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PROJECT LOCATED IN THE CITY OF CHICAGO

DAN RYAN EXPRESSWAY:	ADT (2010)	DESIGN SPEED	POSTED SPEED
SB I-94 (DAN RYAN)	140,500	60 MPH	55 MPH
SB I-94 (BISHOP FORD)	87,600	60 MPH	55 MPH
SB I-57	65,800	60 MPH	55 MPH
SB 71ST STREET C-D ENTRANCE RAMP	9,200	45 MPH	40 MPH
SB 75TH STREET C-D EXIT RAMP	8,000	45 MPH	40 MPH
SB 76TH STREET C-D ENTRANCE RAMP	8,000	45 MPH	40 MPH
SB 79TH STREET C-D EXIT RAMP	7,300	45 MPH	40 MPH

**PROJECT DESCRIPTION**

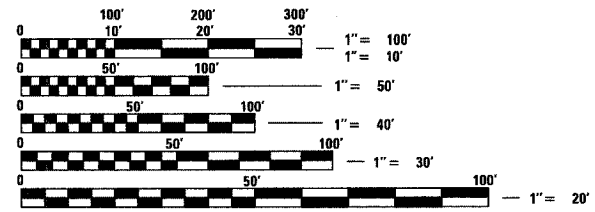
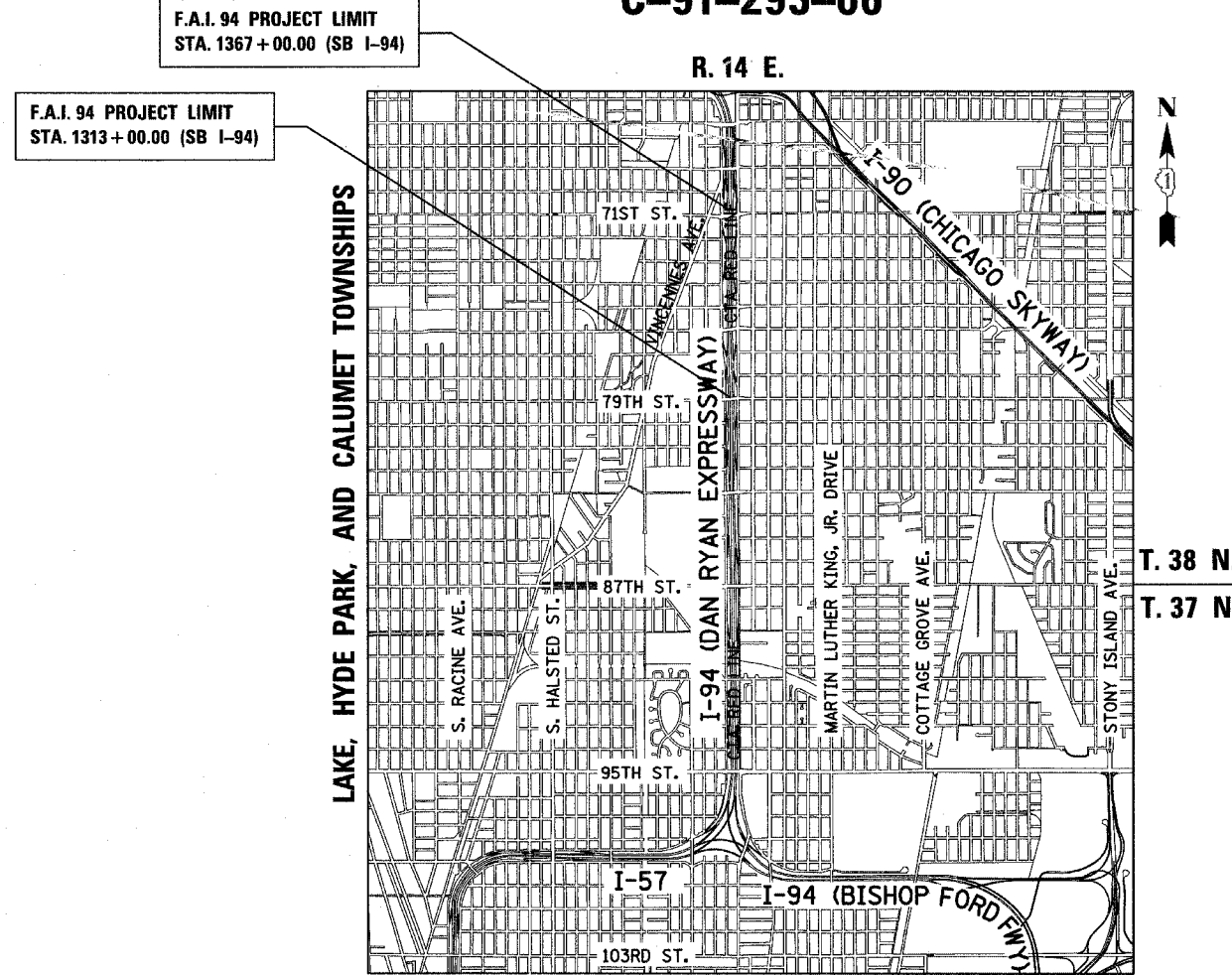
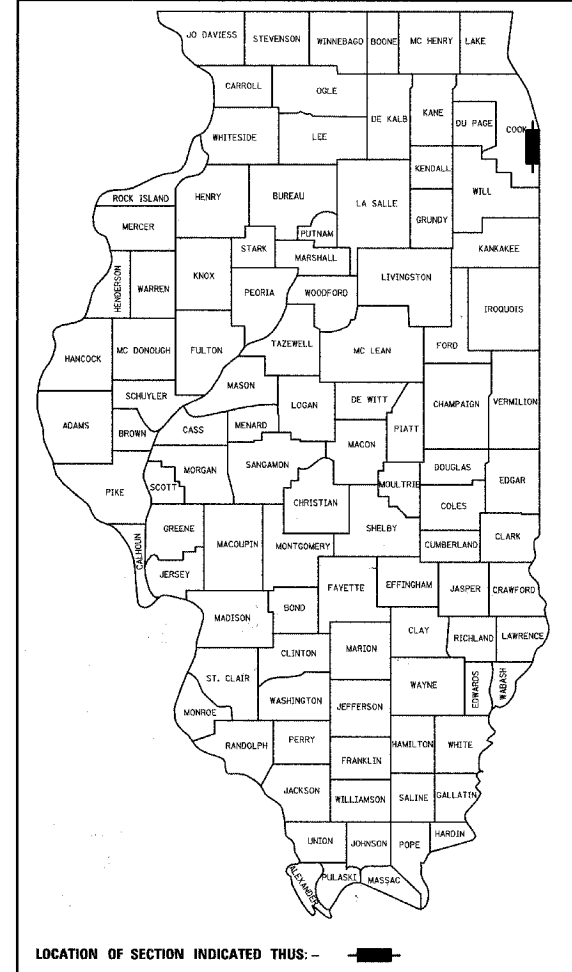
THE PROPOSED IMPROVEMENT CONSISTS OF PAVEMENT AND CTA BARRIER WALL RECONSTRUCTION, WIDENING, RAMP TERMINALS, SHOULDERS, AND DRAINAGE MODIFICATIONS ALONG SB I-94 (DAN RYAN EXPRESSWAY)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED HIGHWAY PLANS**

F.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)  
SECTION 1818 R-6  
PROJECT NO.: IM-94-3(404)060  
79TH STREET TO 71ST STREET  
SB LANES 1 - 5, SHOULDERS & CTA BARRIER WALL  
COOK COUNTY  
C-91-293-06

F.A.I. RTE. 94	SECTION 1818 R-6	COUNTY COOK	TOTAL SHEETS 291*	SHEET NO. 1
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			60B18	



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES; REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED

DIGGER:  
CHICAGO UTILITY ALERT NETWORK  
(312) 744-7000

CTA CONTACT:  
MARVIN A. WATSON,  
GENERAL MANAGER, CONSTRUCTION  
(312) 681-3860

CONTRACT NO. 60B18

MAP SCALE: 1" = 1/2 MILE  
GROSS LENGTH OF PROJECT = 5,400.0 FT. = 1.023 MI.  
NET LENGTH OF PROJECT = 5,400.0 FT. = 1.023 MI.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED March 2, 2006  
Diane M. O'Keefe / cd  
DISTRICT ENGINEER

March 24, 2006  
Mike Hine / cd  
ENGINEER OF DESIGN AND ENVIRONMENT

March 24, 2006  
Milton R. Seo, P.E. / cd  
DIRECTOR, DIVISION OF HIGHWAYS

CONTRACT 23C

DEAN A. KIESLING  
32957  
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS  
Signed Dean A. Kiesling, P.E., Il. Lic. No. 32957  
Expires 11-30-2007.  
Date March 1, 2006

THOMAS MIKOLAJEWICZ  
062-056151  
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS  
Signed Thomas Mikolajewicz, P.E., Il. Lic. No. 062-056151  
Expires 11-30-2007.  
Date March 1, 2006  
For Electrical Drawings 185-202

PHILLIP D. FREY  
081-004826  
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS  
Signed Phillip D. Frey, S.E., Il. Lic. No. 081-004826  
Expires 11-30-2006.  
Date 3/1/06  
For Structural Drawings 208-215

TYLIN INTERNATIONAL  
PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

DISTRICT ONE DESIGN / PROJECT MANAGER:  
BRIAN KUTTAB (847) 705 - 4431

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**INDEX OF SHEETS &  
INDEX OF STATE STANDARDS**  
SCALE: NONE                      DRAWN BY: MPG  
DATE: MARCH 1, 2006            CHECKED BY: JPA

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	3
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

60818

**GENERAL NOTES:**

- UTILITY LOCATIONS SHOWN ON THESE PLANS MAY NOT BE CORRECT OR COMPLETE. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE RESPECTIVE UTILITIES OF THE CITY OF CHICAGO. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL THE CHICAGO UTILITY ALERT NETWORK AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, CABLE, AND GAS FACILITIES (48 HOURS NOTIFICATION IS REQUIRED). CONTACT THE CHICAGO DEPARTMENT OF WATER MANAGEMENT PERMIT SECTION AT (312) 747-7893 FOR WATER AND SEWER LOCATIONS.
- WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKS AND MONUMENTS. THE ENGINEER OR AN AUTHORIZED SURVEYOR AGENT WILL WITNESS OR OTHERWISE REFERENCE AND RESET MONUMENTS AS NECESSARY. ALL PROPERTY CORNERS EXCEPT THOSE WITHIN AREAS WHERE THE SCHEDULE, IF PROVIDED, SHOWS PLACEMENT OF R.O.W. MARKERS SHALL REMAIN UNDISTURBED.
- THE CONTRACTOR SHALL NOT SET UP A YARD OR FIELD OFFICE ON IDOT PROPERTY WITHOUT WRITTEN PERMISSION FROM IDOT.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY SAFETY PRECAUTIONS TO PROTECT AND PROVIDE ACCESS TO ABUTTING PROPERTY, UTILITIES, PEDESTRIANS, AND VEHICULAR TRAFFIC.
- NIGHT OPERATIONS: WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTION IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AS WELL AS THE ADJOINING RESIDENTIAL AREAS.
- ALL ELEVATIONS SHOWN ARE BASED ON THE CHICAGO CITY DATUM OF 0.00, WHICH IS 579.19 FEET ABOVE MEAN TIDE NEW YORK. (NAVD 88)
- THE CONTRACTOR SHALL MAINTAIN THE SURFACE DRAINAGE OF ALL ROADWAYS DURING CONSTRUCTION OF THIS PROJECT. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS, INLETS AND CATCH BASINS. HE SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE SAME. HE SHALL PROVIDE AND MAINTAIN A TEMPORARY OUTLET, AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL INSTALLATION IS COMPLETE INCLUDING PAVEMENT. THIS WORK SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT. COORDINATION WITH IDOT CONTRACT 60B17 REQUIRED.
- ON STATE STANDARD 483001, SUB-BASE GRANULAR MATERIAL, TYPE B 24" SHALL BE USED AS THE IMPROVED SUBGRADE.
- ALL STORM SEWER CONNECTIONS WITH PIPES 27 INCH DIAMETER AND SMALLER SHALL BE MADE WITH PRECAST "TEE" OR "WYE" PIPES. FOR PROPOSED STORM SEWER PIPES LARGER THAN 27 INCH DIAMETER, OPENINGS OF THE SPECIFIED DIAMETER SHALL BE MADE IN THE PIPE AT THE TIME IT IS MANUFACTURED. PRECAST "TEE" AND "WYE" PIPE CONNECTIONS FOR STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST FOR THE STORM SEWERS.
- 10' TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB & GUTTER AND MEDIUM ITEMS TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24" UNLESS OTHERWISE SHOWN.
- NO PAYMENT WILL BE MADE FOR RESTORATION BEYOND THE LIMITS SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2" INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 V:H).
- SPECIAL ATTENTION IS CALLED TO ARTICLE 107.12 AND SPECIAL PROVISIONS ENTITLED "RAILROAD PROTECTIVE LIABILITY INSURANCE" AND "CTA COORDINATION" REGARDING FLAGGERS AND WORK PERFORMED ADJACENT TO THE CTA PASSENGER TRAIN RAIL FACILITIES.
- A PAINT STRIPE SHALL BE APPLIED TO THE FACE OF BARRIER WALL ADJACENT TO ALL DRAINAGE STRUCTURES. THE STRIPE SHALL BE 4" WIDE BY 12" LONG, AND EXTEND VERTICALLY AT 90 DEGREES FROM THE TOP EDGE OF THE BARRIER. THE PAINT WILL BE SPECIFIED AS ORANGE PAINT AND SHALL BE ZINC OXIDE. COST OF THE STRIPE IS TO BE INCLUDED IN THE COST OF THE CONCRETE BARRIER.
- STORM SEWERS TO BE REMOVED SHALL NOT BE SALVAGED.
- THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS SURFACE MIXTURES IS 112 LB/SQ YD/IN.
- ANY WASTE GENERATED AS A SPECIAL WASTE OR A WASTE NOT CERTIFIED AS A NON-SPECIAL WASTE FROM THIS PROJECT SHOULD BE MANIFESTED OFF-SITE USING THE GENERATOR NUMBER ASSOCIATED WITH COOK COUNTY, WHICH IS 0318995023.

**GENERAL NOTES (CONT.):**

- THE CONTRACTOR SHALL BE AWARE THAT MANY CITY OF CHICAGO SEWERS ARE LOCATED IN THE PROPOSED SUB-BASE OR A SHORT DISTANCE BELOW THE SUBGRADE. THE CONTRACTOR SHALL PROTECT THESE FACILITIES FROM DAMAGE DURING CONSTRUCTION OPERATIONS AND SHALL BE RESPONSIBLE FOR ANY DAMAGE AND REPAIR DURING CONSTRUCTION. GAS, ELECTRIC AND TELEPHONE FACILITIES ARE ALSO LOCATED BELOW THE AREA OF PROPOSED CONSTRUCTION. DURING CONSTRUCTION, THE CONTRACTOR SHALL COORDINATE WITH THE PRIVATE UTILITIES (GAS, ELECTRIC AND TELEPHONE) SO THAT THESE UTILITIES MAY PROVIDE APPROPRIATE PROTECTION FOR THEIR FACILITIES. ANY DAMAGE DONE TO EXISTING UTILITIES BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
- THE LOCATIONS OF VARIOUS ITEMS SUCH AS PAVEMENT, BARRIER WALLS AND DRAINAGE STRUCTURES BUILT IN CONTRACTS UNDER CONSTRUCTION DURING THE PREPARATION OF THESE PLANS ARE BASED ON THE PUBLISHED CONTRACT PLAN DRAWINGS AVAILABLE DURING DESIGN. THE CONTRACTOR MUST FIELD VERIFY LIMITS, LOCATIONS AND ELEVATIONS OF THESE PREVIOUSLY CONSTRUCTED ITEMS.
- LOCATIONS OF ACCESS CONTROL FENCING AS SHOWN ON THE PLANS MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER TO BETTER FIT FIELD CONDITIONS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT NO GAP REMAINS BETWEEN PROPOSED FENCING OR WHERE PROPOSED FENCING TERMINATES AND EXISTING FENCE REMAINS IN PLACE.
- TEMPORARY CONCRETE BARRIER: THE BARRIER UNIT AT EACH END OF THE INSTALLATIONS SHALL BE SECURED TO THE PAVEMENT OR SHOULDER USING THREE (3) ANCHORING PINS FOR F SHAPE OR THREE (3) DOWEL BARS FOR NEW JERSEY SHAPE.
- HAMMER DRIVING OF PILES WILL NOT BE ALLOWED.
- CRUSHING PLANT AND CONCRETE PLANT LOCATIONS REQUIRE CITY OF CHICAGO APPROVAL.
- ALL PROPOSED HIGH MAST LIGHT TOWER FOUNDATIONS CONSTRUCTED BY OTHERS ARE SHOWN AS EXISTING IN THIS CONTRACT. HOWEVER, DUE TO THE VARYING PROJECT SCHEDULES ALL HIGH MAST LIGHT TOWER FOUNDATIONS MAY NOT BE CONSTRUCTED AT THE START OF THIS CONTRACT.
- ALL PAVEMENT SHALL BE CONSTRUCTED USING THE SPECIAL PROVISION "EXTENDED LIFE CONCRETE PAVEMENT (30 YEAR)".

**GENERAL NOTES - LANDSCAPE REQUIREMENTS:**

- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD.
- AREAS TO BE SEEDED BETWEEN NOVEMBER 1 AND APRIL 1 SHALL REQUIRE DORMANT SEEDING, WHICH SHALL BE INCLUDED IN THE COST OF SEEDING, CLASS 2A.

**GENERAL NOTES - SEDIMENT AND EROSION CONTROL REQUIREMENTS:**

- EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES, WHICH WILL POTENTIALLY CREATE ERODABLE CONDITIONS.
- THE EROSION CONTROL MEASURE SHOWN ARE BUT A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURE. DEVIATIONS FROM THIS PLAN ARE TO BE EXPECTED PENDING A JOBSITE INSPECTION BETWEEN THE CONTRACTOR AND THE DEPARTMENT.
- THE CONTRACTOR WILL BE REQUIRED TO IMPLEMENT AND MAINTAIN SEDIMENT CONTROL MEASURE PRIOR TO STRIPPING EXISTING VEGETATION.

**PROJECT COMMITMENTS:**

IDOT HAS MADE THE FOLLOWING COMMITMENTS FOR THE PROJECT:

IDOT HAS COMMITTED TO ADDRESS CONSTRUCTION RELATED AIR QUALITY CONCERNS. THESE STRATEGIES INCLUDE REQUIRING DETAILED DUST CONTROL PLANS, REQUIRING THE USE OF CLEANER BURNING DIESEL FUELS ON CERTAIN DIESEL POWERED CONSTRUCTION EQUIPMENT AND/OR THE INSTALLATION OF EXHAUST EMISSION SCRUBBERS, AND THE REDUCTION OF CONSTRUCTION EQUIPMENT IDLING TIMES. THESE STRATEGIES ARE ADDRESSED IN SPECIAL PROVISIONS INCLUDED IN THIS CONTRACT AND DEVELOPED FOR THE DAN RYAN RECONSTRUCTION PROJECT.

IDOT HAS COMMITTED TO REDUCE TIRE-PAVEMENT HIGHWAY TRAFFIC NOISE FOR THE FINISHED PROJECT BY INCLUDING A SPECIAL PROVISION TO INCORPORATE VARIABLE WIDTH AND SKEWED "FINING" OF THE NEW CONCRETE PAVEMENT.

**GENERAL NOTES - CITY OF CHICAGO:**

- ALL CATCH BASINS IN THE CITY OF CHICAGO MUST MEET THE DEPARTMENT OF WATER MANAGEMENT'S STANDARDS.
- SEWER SIZES 21" DIAMETER OR SMALLER MUST BE EXTRA STRENGTH VITRIFIED CLAY PIPE C-700 OR DUCTILE IRON PIPE WITH PUSH-ON OR MECHANICAL JOINTS. SEWER SIZES 24" DIAMETER OR LARGER MUST BE REINFORCED CONCRETE PIPE TYPE C-76, CLASS III, WALL "B" WITH "O-RING" JOINTS.
- PERMITS FROM THE DEPARTMENT OF WATER MANAGEMENT ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY AND COMBINED SEWER SYSTEM CONSTRUCTION, AND FOR ALL WORK INVOLVING ADJUSTMENT OF SEWER STRUCTURES. THE DEPARTMENT OF WATER MANAGEMENT'S PERMIT MUST BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF CONSTRUCTION. THE LICENSED SEWER CONTRACTOR/ SUBCONTRACTOR MUST SUBMIT TWO SETS OF PLANS APPROVED BY THE DEPARTMENT OF WATER MANAGEMENT FOR THE ISSUANCE OF THE SEWER PERMIT TO BUREAU OF ENGINEERING SERVICES-SEWER SECTION, JARDINE PURIFICATION PLANT, EL+51, ROOM 313, 1000 E. OHIO ST., CHICAGO, IL 60611. INSPECTION WILL BE PROVIDED BY THE DEPARTMENT OF WATER MANAGEMENT.
- IF THE SEWER PIPE COVER IS REDUCED TO LESS THAN 3 FT., CONCRETE ENCASUREMENT OF THE SEWER OR REPLACEMENT OF THE SEWER WITH CLASS 52 DUCTILE IRON PIPE WILL BE REQUIRED.
- IN CASE OF DAMAGE TO CITY OF CHICAGO SEWERS, PRIVATE AND PUBLIC DRAINS, SEWER STRUCTURES AND/OR BENCH MONUMENTS, THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE DEPARTMENT OF WATER MANAGEMENT AT (312) 744-0409 OR (312) 744-0408.
- PERFORATED LIDS SHALL BE PLACED ON ALL SEWER MANHOLES AND CATCH BASINS.
- SIDEWALK ACCESSIBILITY RAMPS SHALL NOT BE CONSTRUCTED DIRECTLY OVER EXISTING OR PROPOSED DRAINAGE STRUCTURES.
- CITY OF CHICAGO WATER VALVE VAULTS AND SEWER STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION WITHOUT WRITTEN PERMISSION FROM THE CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT.
- CURB AND GUTTER CONSTRUCTION SHALL PROVIDE A MINIMUM CURB HEIGHT OF 3".
- BACKFILL MATERIAL UNDER SIDEWALKS SHALL BE FA-2.
- PAVEMENT REPLACEMENT AROUND FRAMES AND GRATES OR LIDS WHERE DRAINAGE, WATER MAIN OR ELECTRICAL STRUCTURES ARE ADJUSTED OR RECONSTRUCTED, SHALL BE WITH CLASS S1 CONCRETE.
- ALL PAVEMENT PATCHING ALONG FRONTAGE ROADS SHALL BE CLASS C.
- PRE-CONSTRUCTION VIDEO TAPED INSPECTION REQUIRED PRIOR TO ISSUANCE OF SEWER PERMIT. POST-CONSTRUCTION VIDEO TAPED INSPECTION REQUIRED PRIOR TO ACCEPTANCE OF SEWER BY THE DEPARTMENT OF WATER MANAGEMENT.
- THE CONTRACTOR IS RESPONSIBLE FOR THE ADEQUATE PROTECTION OF THE EXISTING SEWERS, DRAIN CONNECTIONS, SEWER STRUCTURES AND BENCH MONUMENTS DURING CONSTRUCTION OPERATIONS AND USE OF HEAVY EQUIPMENT IN THE LIMITS OF THE PROJECT.
- THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT MUST BE NOTIFIED AT LEAST 48 HOURS IN ADVANCE OF CONSTRUCTION, WITH THE NAME AND TELEPHONE NUMBER OF THE RESIDENT ENGINEER WHO COULD BE CONTACTED FOR ANY SEWER EMERGENCY.
- MANHOLES, CATCH BASINS AND INLETS MUST BE PROTECTED FROM THE ENTRY OF ASPHALT/DEBRIS INTO THE SEWER SYSTEM DURING CONSTRUCTION. THE CONTRACTOR MUST MARK LOCATIONS OF ALL SEWER STRUCTURES ON THE SIDEWALK BEFORE STARTING PAVEMENT REMOVAL/REPLACEMENT. ADJUSTMENT OF FRAMES AND LIDS OF SEWER STRUCTURES MUST BE COMPLETED PRIOR TO STREET RESURFACING.
- THE CONTRACTOR MUST LOCATE AND PROMPTLY CONNECT TO THE NEW SEWERS ALL LIVE HOUSE DRAINS, CATCH BASIN DRAINS AND OTHER EXISTING LATERALS, DRAINS AND SEWERS, OF WHATEVER NATURE, WHICH ARE CONNECTED TO THE EXISTING SEWERS BEING REPAIRED OR REPLACED.
- EXISTING CATCH BASIN LATERALS TO BE REUSED MUST BE RODDED AND FLUSHED IN THE PRESENCE OF THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT INSPECTOR. A NEW CONNECTION TO THE MAIN SEWER IS REQUIRED IF THE EXISTING CATCH BASIN LATERAL IS NOT APPROVED BY THE SEWER INSPECTOR.
- THE FRAMES AND LIDS OF SEWER STRUCTURES TO BE ABANDONED, REMOVED, OR FILLED MUST BE SALVAGED AND THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT NOTIFIED FOR PICKUP.
- WHEN A SEWER STRUCTURE IS ABANDONED, ALL PIPE OPENINGS MUST BE PLUGGED, STRUCTURES FILLED WITH TRENCH BACKFILL, LIDS AND FRAMES REMOVED AND SURFACE RESTORED AS PER THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT STANDARDS AND SPECIFICATIONS.

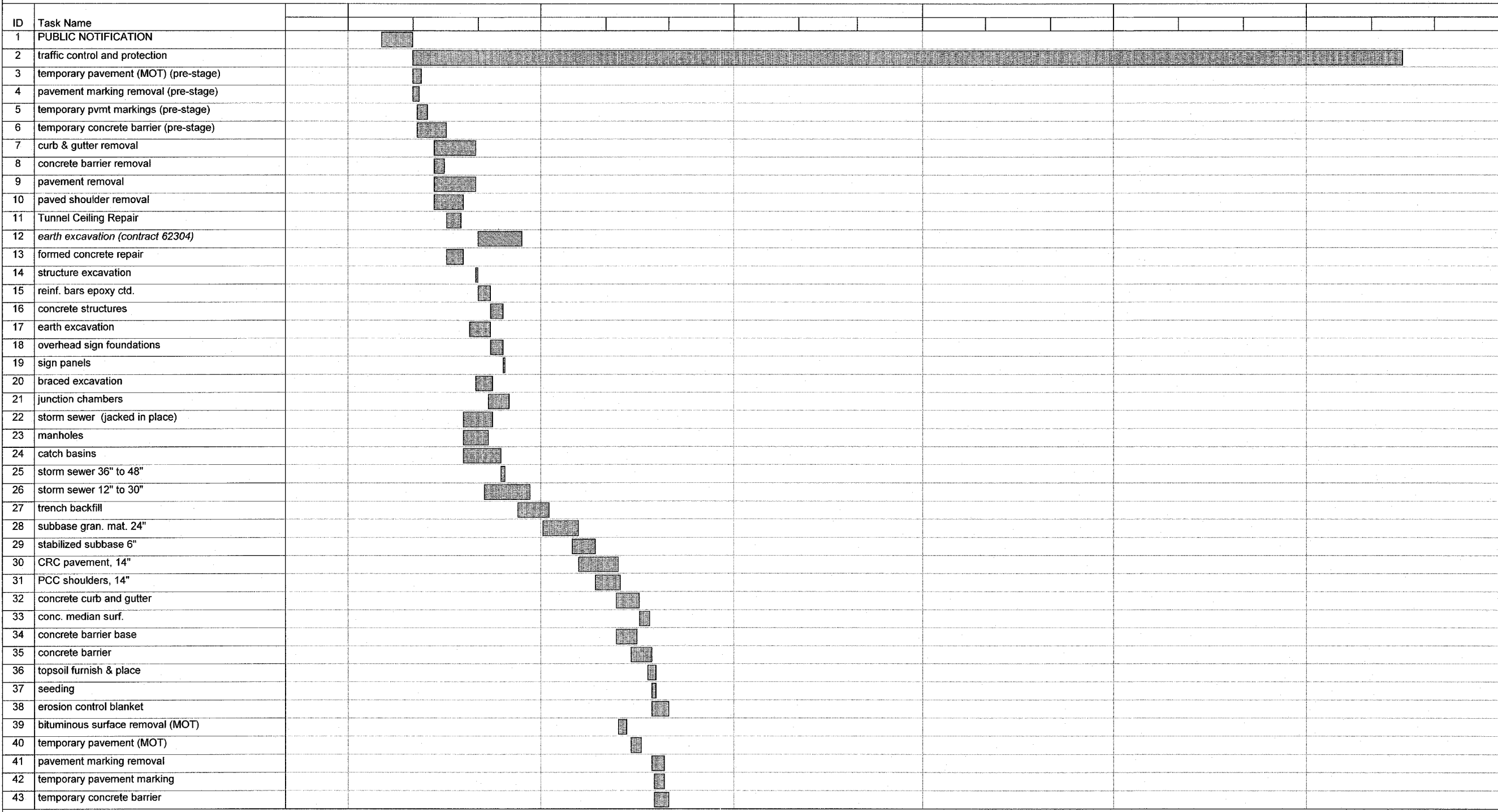
**TYLIN INTERNATIONAL**

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.I. 94 (DAN RYAN EXPRESSWAY)
NAME	DATE	
		GENERAL NOTES & COMMITMENTS

SCALE: NONE      DRAWN BY: MPG  
DATE: MARCH 1, 2006      CHECKED BY: JPA

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CONTRACT 62593



Suggested Sequence of Work  
Date: Sat 2/25/06

Task		Milestone		Rolled Up Task		Rolled Up Progress		External Tasks		Group By Summary	
Progress		Summary		Rolled Up Milestone		Split		Project Summary			

**INCLUDED FOR INFORMATION ONLY.  
SCHEDULE FROM CONTRACT 62593.**

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUGGESTED SEQUENCE OF WORK  
STAGE 1

SCALE: NONE  
DATE: MARCH 1, 2006

DRAWN BY: JJS  
CHECKED BY: TGB

ID	Task Name	Gantt Chart Area																							
1	PUBLIC NOTIFICATION	[Gantt bar]																							
2	traffic control and protection	[Gantt bar]																							
3	pavement marking removal (pre-stage)	[Gantt bar]																							
4	temporary pvmt markings (pre-stage)	[Gantt bar]																							
5	temporary concrete barrier (pre-stage)	[Gantt bar]																							
6	curb & gutter removal	[Gantt bar]																							
7	pavement removal	[Gantt bar]																							
8	earth excavation	[Gantt bar]																							
9	catch basins	[Gantt bar]																							
10	storm sewer 12" to 30"	[Gantt bar]																							
11	trench backfill	[Gantt bar]																							
12	subbase gran. mat. 24"	[Gantt bar]																							
13	stabilized subbase 6"	[Gantt bar]																							
14	CRC pavement, 14"	[Gantt bar]																							
15	concrete barrier base	[Gantt bar]																							
16	concrete barrier	[Gantt bar]																							

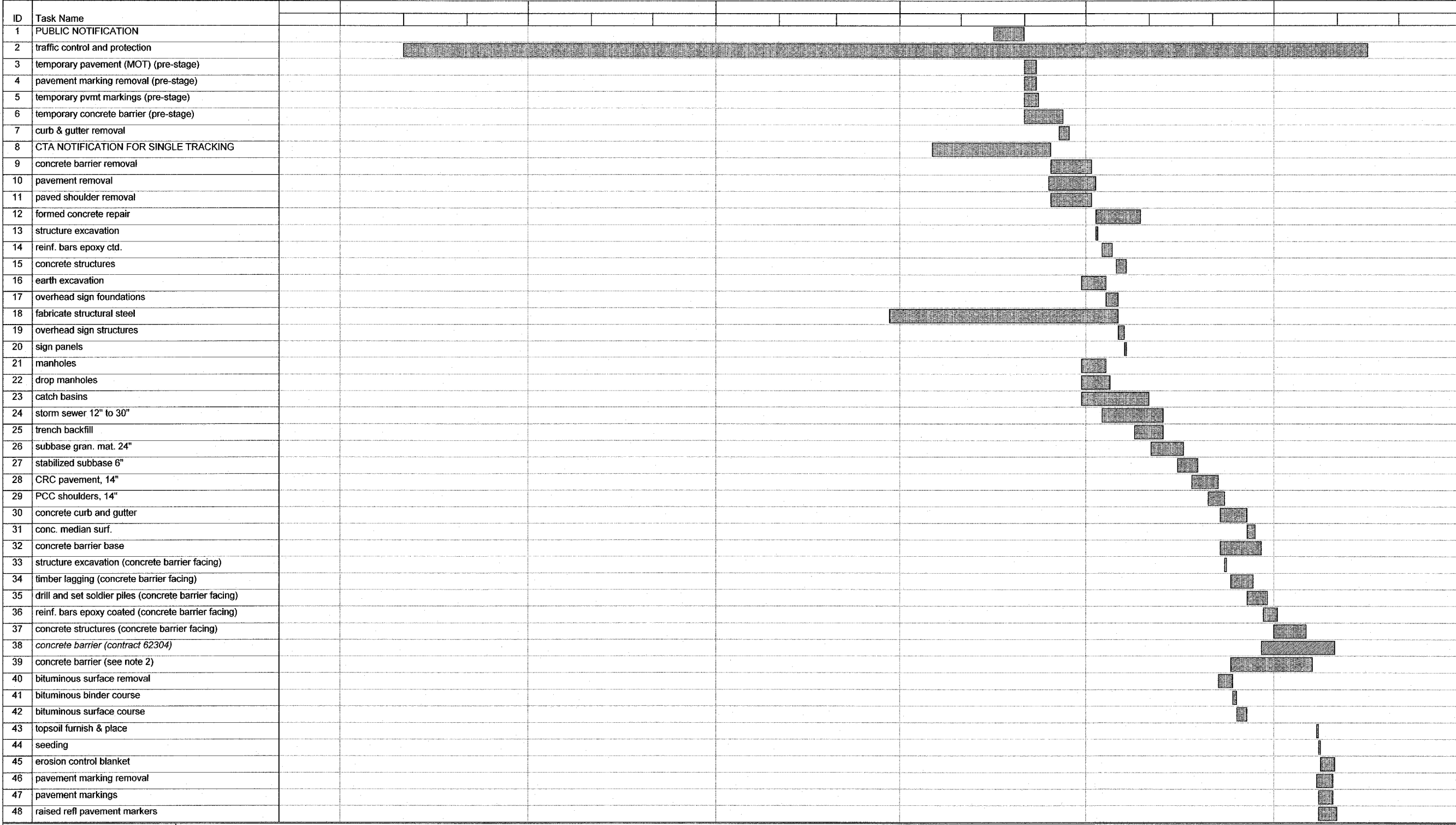
Suggested Sequence of Work Date: Fri 2/24/06

Task		Milestone		Rolled Up Task		Rolled Up Progress		External Tasks		Group By Summary	
Progress		Summary		Rolled Up Milestone		Split		Project Summary			

**INCLUDED FOR INFORMATION ONLY.  
SCHEDULE FROM CONTRACT 62593.**

REVISIONS	
NAME	DATE

CONTRACT 62593



Suggested Sequence of Work Date: Sat 2/25/06

Task		Milestone		Rolled Up Task		Rolled Up Progress		External Tasks		Group By Summary	
Progress		Summary		Rolled Up Milestone		Split		Project Summary			

**INCLUDED FOR INFORMATION ONLY.  
SCHEDULE FROM CONTRACT 62593.**

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUGGESTED SEQUENCE OF WORK  
STAGE 3

SCALE: NONE  
DATE: MARCH 1, 2006

DRAWN BY: JJS  
CHECKED BY: TGB

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE			
				PCC PAVEMENT J000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C
20100110	TREE REMOVAL (6-15 UNITS DIAMETER)	UNIT	25	25			
20101000	TEMPORARY FENCE	FOOT	720	720			
20200100	EARTH EXCAVATION	CU YD	41749	41749			
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	2010	2010			
20700420	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	944	944			
20800150	TRENCH BACKFILL	CU YD	1313	1313			
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	52950	52950			
* 21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	13332	13332			
* 21101645	TOPSOIL FURNISH AND PLACE, 12"	SQ YD	2858	2858			
* 21101825	COMPOST FURNISH AND PLACE, 6"	SQ YD	2858	2858			
* 25000210	SEEDING, CLASS 2A	ACRE	3.00	3.00			
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	272	272			
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	271	271			
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	271	271			
* ⊙ 25000750	MOWING	ACRE	6.50	6.50			
* 25001800	SEEDING, CLASS 4 (MODIFIED)	ACRE	0.50	0.50			
* 25100630	EROSION CONTROL BLANKET	SQ YD	16126	16126			
* 25200200	SUPPLEMENTAL WATERING	UNIT	850	850			
* 28000200	EARTH EXCAVATION FOR EROSION CONTROL	CU YD	15	15			
* 28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	230	230			
* 28000300	TEMPORARY DITCH CHECKS	EACH	14	14			
* 28000510	INLET FILTERS	EACH	13	13			
28001000	AGGREGATE (EROSION CONTROL)	TON	1	1			
31101860	SUB-BASE GRANULAR MATERIAL, TYPE B 24"	SQ YD	54150	54150			
42001300	PROTECTIVE COAT	SQ YD	56018	56018			
42100380	CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"	SQ YD	39810	39810			
44000004	BITUMINOUS SURFACE REMOVAL 1"	SQ YD	7200	7200			
44000006	BITUMINOUS SURFACE REMOVAL 1 1/2"	SQ YD	88	88			
44000100	PAVEMENT REMOVAL	SQ YD	38721	38721			
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	3571	3571			
44001980	CONCRETE BARRIER REMOVAL	FOOT	751	751			
44004250	PAVED SHOULDER REMOVAL	SQ YD	8529	8529			

- \* - SPECIALTY ITEM
- - IDOT PAY CODE SFTY-3N
- △ - IDOT PAY CODE Y080.
- ⊙ - NON-PARTICIPATING

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES  
 SHEET 1 OF 5

SCALE: NONE  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: TGB

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE			
				PCC PAVEMENT J000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C
44004260	PAVED SHOULDER REMOVAL (SPECIAL)	SQ YD	406	406			
44004400	PAVEMENT REMOVAL (SPECIAL)	SQ YD	1435	1435			
50200100	STRUCTURE EXCAVATION	CU YD	455	455			
50300225	CONCRETE STRUCTURES	CU YD	196	196			
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	47570	47570			
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	2144	2144			
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	343	343			
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	335	335			
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	433	433			
550A0680	STORM SEWERS, CLASS A, TYPE 3 18"	FOOT	162	162			
550A0710	STORM SEWERS, CLASS A, TYPE 3 24"	FOOT	60	60			
55100400	STORM SEWER REMOVAL 10"	FOOT	869	869			
55100500	STORM SEWER REMOVAL 12"	FOOT	1728	1728			
55100700	STORM SEWER REMOVAL 15"	FOOT	147	147			
60107700	PIPE UNDERDRAINS 6"	FOOT	9668	9668			
60108200	PIPE UNDERDRAINS 6" (SPECIAL)	FOOT	210	210			
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1			
60201310	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	52	52			
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	6	6			
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3			
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	4	4			
60250200	CATCH BASINS TO BE ADJUSTED	EACH	27	27			
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	2	2			
60255500	MANHOLES TO BE ADJUSTED	EACH	21	21			
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	3	3			
60260100	INLETS TO BE ADJUSTED	EACH	2	2			
60500040	REMOVING MANHOLES	EACH	6	6			
60500050	REMOVING CATCH BASINS	EACH	55	55			
60500060	REMOVING INLETS	EACH	2	2			
60608521	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24	FOOT	56.5	56.5			
60618324	CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)	SQ FT	1108	1108			
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	2	2			

- \* - SPECIALTY ITEM
- - IDOT PAY CODE SFTY-3N
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- - NON-PARTICIPATING

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES  
SHEET 2 OF 5

SCALE: NONE      DRAWN BY: MPC  
DATE: MARCH 1, 2006      CHECKED BY: TGB

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SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE			
				PCC PAVEMENT J000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (TANGENT)	EACH	2	2			
63200310	GUARDRAIL REMOVAL	FOOT	199	199			
63700805	CONCRETE BARRIER TRANSITION	FOOT	256	256			
64200105	SHOULDER RUMBLE STRIP	FOOT	9276	9276			
* 66400560	CHAIN LINK FENCE, 6' (SPECIAL)	FOOT	5400	5400			
* 66402900	CHAIN LINK GATE, 6' X 6' SINGLE	EACH	6	6			
66410300	CHAIN LINK FENCE REMOVAL	FOOT	72	72			
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	14086	14086			
* 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1			
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1			
67100100	MOBILIZATION	L SUM	1	1			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	12540	12540			
70300520	PAVEMENT MARKING TAPE, TYPE III, 4"	FOOT	15556	15556			
70300530	PAVEMENT MARKING TAPE, TYPE III, 5"	FOOT	3795	3795			
70300550	PAVEMENT MARKING TAPE, TYPE III, 8"	FOOT	7510	7510			
70300560	PAVEMENT MARKING TAPE, TYPE III, 12"	FOOT	1028	1028			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	28397	28397			
70400100	TEMPORARY CONCRETE BARRIER	FOOT	6970	6970			
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	13030	13030			
* 72000100	SIGN PANEL - TYPE 1	SQ FT	20			20	
* 72000200	SIGN PANEL - TYPE 2	SQ FT	32			32	
* 72000300	SIGN PANEL - TYPE 3	SQ FT	75			75	
* 72400720	RELOCATE SIGN PANEL - TYPE 2	SQ FT	12			12	
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	14			14	
* 73000100	WOOD SIGN SUPPORT	FOOT	120			120	
* 73700300	REMOVE CONCRETE FOUNDATION - OVERHEAD	EACH	2			2	
* 78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	18934	18934			
* 78005120	EPOXY PAVEMENT MARKING - LINE 5"	FOOT	4424	4424			
* 78005140	EPOXY PAVEMENT MARKING - LINE 8"	FOOT	8749	8749			
* 78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	1608	1608			
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	9399	9399			
* 78008220	POLYUREA PAVEMENT MARKING TYPE I - LINE 5"	FOOT	6649	6649			

- \* - SPECIALTY ITEM
- - IDOT PAY CODE SFTY-3N
- △ - IDOT PAY CODE Y080.
- - NON-PARTICIPATING

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES  
SHEET 3 OF 5

SCALE: NONE      DRAWN BY: MPG  
DATE: MARCH 1, 2006      CHECKED BY: TGB

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SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE			
				PCC PAVEMENT J000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C
* 78008240	POLYUREA PAVEMENT MARKING TYPE I - LINE 8"	FOOT	6358	6358			
* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	905	905			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKERS	EACH	702	702			
* 78200100	MONODIRECTIONAL PRISMATIC BARRIER REFLECTOR	EACH	1024	1024			
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	8	8			
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	66	66			
* 78201000	TERMINAL MARKERS, DIRECT APPLIED	EACH	2	2			
* 78300100	PAVEMENT MARKING REMOVAL	SQ FT	3693	3693			
* 81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	153		153		
* 81023750	CONDUIT ENCASED IN CONCRETE, 3" DIA., PVC	FOOT	138			138	
* 81400200	HEAVY DUTY HANDHOLE	EACH	4		4		
* 81500200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	399		399		
* 84200705	LIGHTING FOUNDATION REMOVAL, PARTIAL	EACH	26		26		
* X2500322	SEEDING, CLASS 5A (MODIFIED)	ACRE	0.50	0.50			
* C2C05818	SHRUB, RHUS AROMATICA GRO-LOW (GROW-LOW FRAGRANT SUMAC), 18" WIDTH, CONTAINER	EACH	225	225			
* E20200G1	VINE-PARTHENOCISSUS QUINQUEFOLIA (VIRGINIA CREEPER), 1-GALLON POT	EACH	95	95			
* K0030400	PERENNIAL PLANTS, DAYLILIES	UNIT	1	1			
⊙ X0320333	ROADWAY CLEANING (SPECIAL)	EACH	28	28			
* X0321866	REMOVE, STORE AND RE-ERECT SIGN PANEL	SQ FT	84				84
* X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	421	32			389
X0322671	STABILIZED CONSTRUCTION ENTRANCE	SQ YD	1050	1050			
* X0322859	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	2	2			
* X0323426	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	26	26			
* X0323973	SEDIMENT CONTROL, SILT FENCE	FOOT	2516	2516			
* X0323974	SEDIMENT CONTROL, SILT FENCE MAINTENANCE	FOOT	629	629			
X0323988	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	2560	2560			
X0324112	BARRIER BASE	FOOT	6351	6351			
* X0324646	CONDUIT ENCASED, REINFORCED CONCRETE, 6 - 4" DIA., CNC	FOOT	246		246		
X0324697	SOIL STABILIZERS	POUND	62000	62000			
X0324698	APPLYING DUST SUPPRESSION AGENTS	UNIT	34	34			
X0325082	CTA BARRIER REMOVAL	FOOT	4981	4981			
* X0325083	CTA FENCE	FOOT	5003	5003			

- \* - SPECIALTY ITEM
- - IDOT PAY CODE SFTY-3N
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- ⊙ - NON-PARTICIPATING

**TYLIN** INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES  
 SHEET 4 OF 5

SCALE: NONE  
 DATE: MARCH 1, 2006

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 CHECKED BY: TCB

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	URBAN - 90% FEDERAL, 10% STATE			
				PCC PAVEMENT J000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C
* X0325084	CTA GATES	EACH	6	6			
* X0325130	TUBULAR TRAFFIC SIGN POST	EACH	1				1
X0712400	TEMPORARY PAVEMENT	SQ YD	2400	2400			
X4066426	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70	TON	8	8			
X4834090	PORTLAND CEMENT CONCRETE SHOULDERS 14"	SQ YD	9965	9965			
X6061001	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48	FOOT	675.5	675.5			
X6063600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	FOOT	1676.0	1676.0			
X6370910	CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT	FOOT	1249	1249			
X6370925	CONCRETE BARRIER, SINGLE FACE, 42" (SPECIAL)	FOOT	4846	4846			
X6640210	TEMPORARY CHAIN LINK FENCE (PORTABLE)	FOOT	775	775			
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1			
X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	457	457			
X7015000	CHANGEABLE MESSAGE SIGN	CAL MO	64	64			
XX001854	STABILIZED SUB-BASE, 6"	SQ YD	52950	52950			
XX004201	PAVEMENT REINFORCEMENT 14"	SQ YD	39810	39810			
Z0002400	BALLAST	TON	1160	1160			
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1			
Z0013825	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	7	7			
<input type="checkbox"/> Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	5	5			
<input type="checkbox"/> Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	10	10			
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1			
<input type="checkbox"/> Z0076600	TRAINEES	HOUR	2000	2000			

- \* - SPECIALTY ITEM
- IDOT PAY CODE SFTY-3N
- IDOT PAY CODE Y080.
- ⊙ - NON-PARTICIPATING

**TYLIN** INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES  
 SHEET 5 OF 5

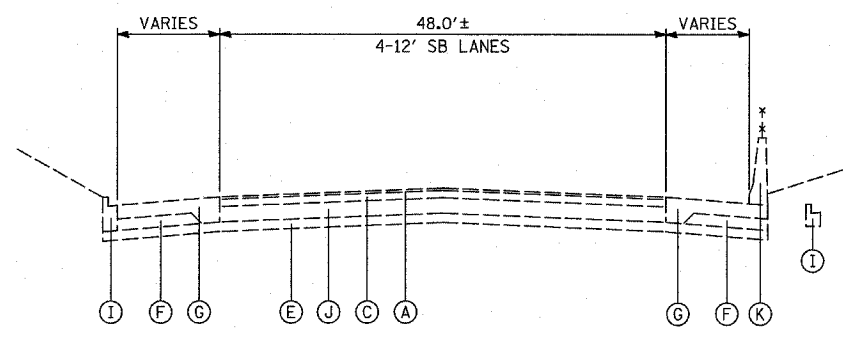
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 DATE: MARCH 1, 2006

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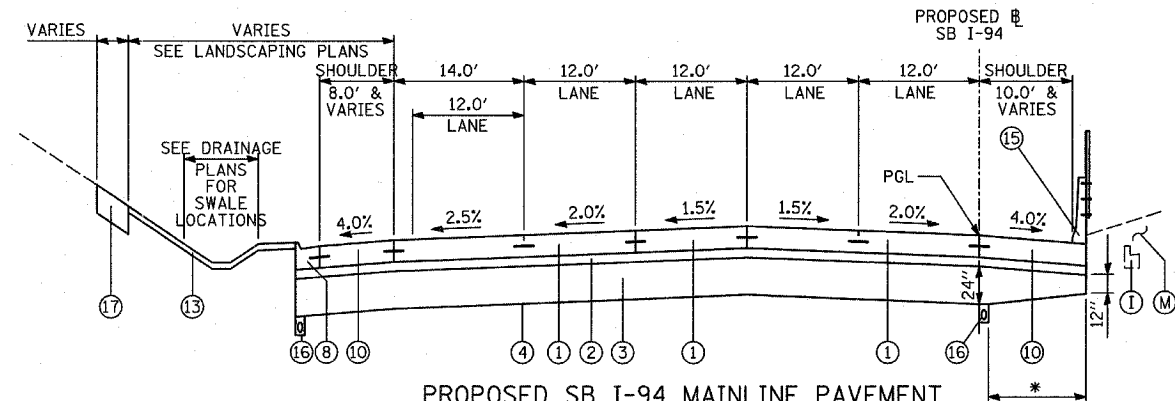
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	12
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

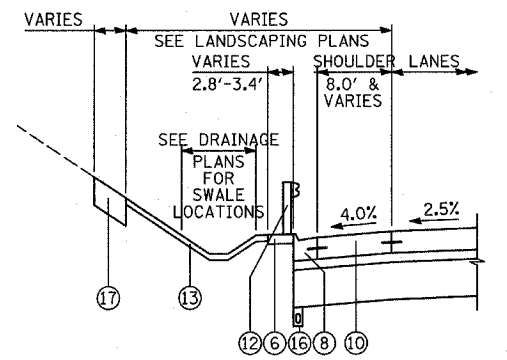
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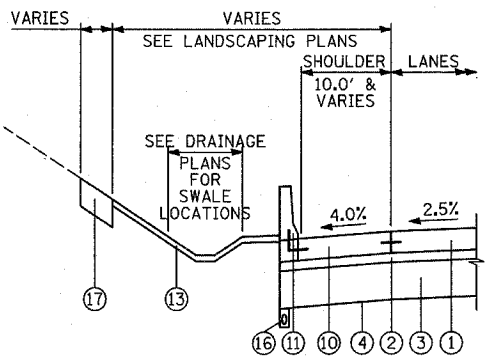
EXISTING SB I-94 MAINLINE PAVEMENT



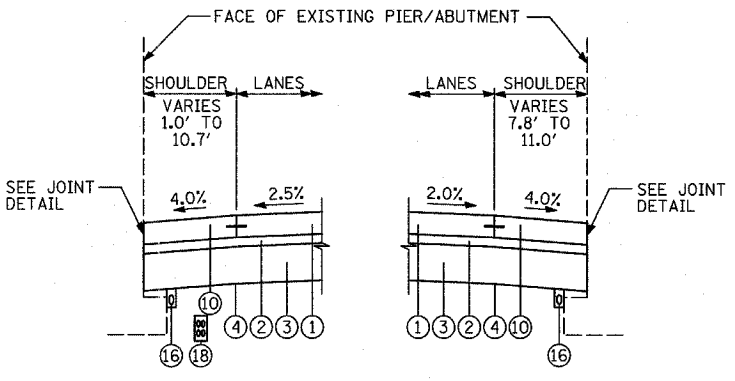
PROPOSED SB I-94 MAINLINE PAVEMENT  
LOCATIONS VARY BETWEEN 95TH STREET AND 71st TO 75th C-D RAMPS



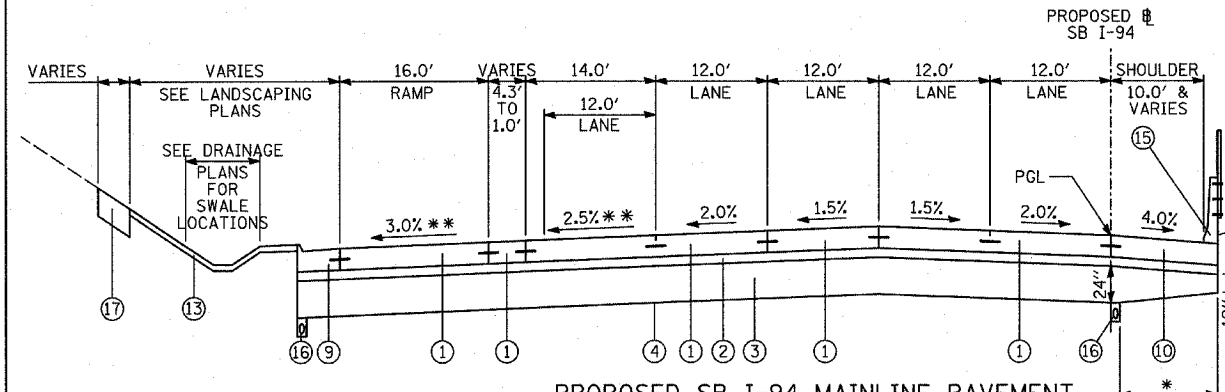
PROPOSED SB I-94 (DAN RYAN EXPWY)  
ALTERNATE WEST PAVEMENT EDGE  
TREATMENTS: GUARDRAIL  
LOCATIONS VARY - SEE EXISTING/PROPOSED PLANS



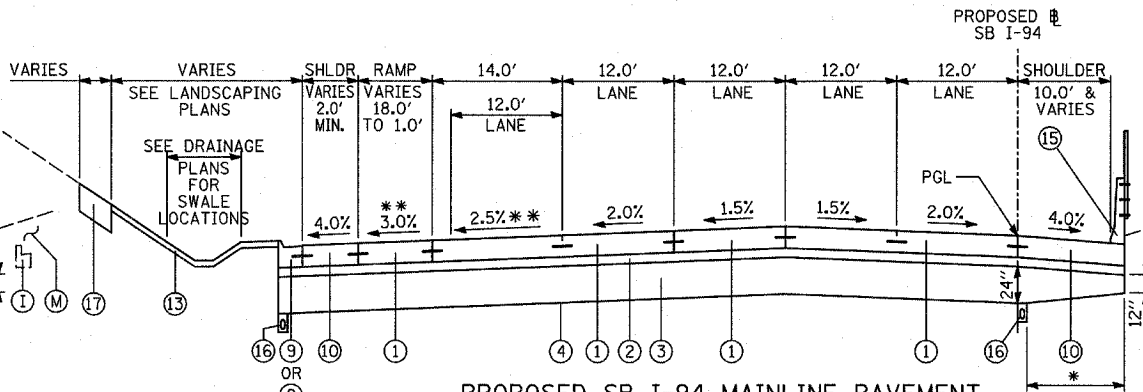
PROPOSED SB I-94 (DAN RYAN EXPWY)  
ALTERNATE WEST PAVEMENT EDGE  
TREATMENTS: CONCRETE BARRIER  
LOCATIONS VARY - SEE EXISTING/PROPOSED PLANS



PROPOSED SB I-94 (DAN RYAN EXPWY)  
ALTERNATE PAVEMENT EDGE  
TREATMENTS: EXISTING PIER/ABUTMENT  
LOCATIONS VARY AT BRIDGES - SEE EXISTING/PROPOSED PLANS FOR LOCATIONS AND CROSS-STREET OVERPASS FOOTING DETAILS FOR ADDITIONAL DETAIL



PROPOSED SB I-94 MAINLINE PAVEMENT  
LOCATIONS VARY BETWEEN 95TH STREET AND 71st TO 75th C-D RAMPS



PROPOSED SB I-94 MAINLINE PAVEMENT  
LOCATIONS VARY BETWEEN 95TH STREET AND 71st TO 75th C-D RAMPS

**BITUMINOUS MIXTURE REQUIREMENT**

ITEM	AC TYPE	VOIDS	RAP %
BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70	PG 64-22	4%±70 Gyr.	10
STABILIZED SUBBASE, 6"	PG 58-22	3%±50 Gyr.	25

THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS SURFACE MIXTURES IS 112 LB/SQ YD/IN.

**NOTES:**

- REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.
- \* - VARYING DEPTH PAID FOR AS "SUB-BASE GRANULAR MATERIAL, TYPE B 24"
- \*\* - CROSS-SLOPES VARY AT 87TH STREET ENTRANCE RAMP

**PROPOSED LEGEND**

- CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14" & PAVEMENT REINFORCEMENT, 14"
- STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
- SUB-BASE GRANULAR MATERIAL, TYPE B 24"
- GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
- CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
- COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
- COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
- PORTLAND CEMENT CONCRETE SHOULDERS 14"
- CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- TRAFFIC BARRIER TERMINAL, TYPE VARIES
- TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
- AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
- CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- PIPE UNDERDRAIN, 6" (SEE DETAILS)
- TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE, 6"; EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
- ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
- CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED); BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- PORTLAND CEMENT CONCRETE SHOULDERS 9"
- 1/2" SHOULDER RESURFACING: BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1/2"
- 1 3/4" SHOULDER RESURFACING: BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 3/4"
- 1 3/4" MAINLINE RESURFACING: POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
- 4" MAINLINE RESURFACING: POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"; POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
- PORTLAND CEMENT CONCRETE PAVEMENT, 14" (JOINTED)

**EXISTING LEGEND** ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE

- BIT CONC SURFACE COURSE, 1 1/2"±
- BIT CONC BINDER COURSE, 1 1/2"±
- BIT CONC BINDER COURSE, 4 3/4"±
- SUB-BASE GRANULAR MATERIAL, 4"±
- SUB-BASE GRANULAR MATERIAL, 6"±
- CRUSHED STONE, 5"±
- PCC SHOULDERS, 9"±
- PCC SHOULDERS, 11"±
- PCC BASE COURSE, 9"±
- COMB CONC CURB & GUTTER
- PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
- CONCRETE BARRIER WALL
- PCC SHOULDERS, 11"±
- CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
- JOINTED PCC PAVEMENT, 11"±
- STABILIZED SUB-BASE, 4"±
- SUB-BASE GRANULAR MATERIAL, 24"±
- SUB-BASE GRANULAR MATERIAL, 12"±

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**EXISTING & PROPOSED TYPICAL SECTIONS  
SB I-94 (DAN RYAN EXPRESSWAY)  
(SHEET 1 OF 2)**  
SCALE: NONE  
DATE: MARCH 1, 2006  
DRAWN BY: MPG  
CHECKED BY: TGB

**PROPOSED LEGEND**

- ① CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14" & PAVEMENT REINFORCEMENT, 14"
- ② STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
- ③ SUB-BASE GRANULAR MATERIAL, TYPE B 24"
- ④ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ⑤ CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
- ⑥ CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
- ⑦ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
- ⑧ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ⑨ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
- ⑩ PORTLAND CEMENT CONCRETE SHOULDERS 14"
- ⑪ CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- ⑫ TRAFFIC BARRIER TERMINAL, TYPE VARIES
- ⑬ TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
- ⑭ AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
- ⑮ CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- ⑯ PIPE UNDERDRAIN, 6" (SEE DETAILS)
- ⑰ TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE, 6"; EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
- ⑱ ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
- ⑲ CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED); BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- ⑳ PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ㉑ 1 1/2" SHOULDER RESURFACING; BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 1/2"
- ㉒ 1 3/4" SHOULDER RESURFACING; BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 3/4"
- ㉓ 1 3/4" MAINLINE RESURFACING; POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
- ㉔ 4" MAINLINE RESURFACING; POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"; POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
- ㉕ PORTLAND CEMENT CONCRETE PAVEMENT, 14" (JOINTED)

**EXISTING LEGEND** ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE

- (A) BIT CONC SURFACE COURSE, 1 1/2"±
- (B) BIT CONC BINDER COURSE, 1 1/2"±
- (C) BIT CONC BINDER COURSE, 4 3/4"±
- (D) SUB-BASE GRANULAR MATERIAL, 4"±
- (E) SUB-BASE GRANULAR MATERIAL, 6"±
- (F) CRUSHED STONE, 5"±
- (G) PCC SHOULDERS, 9"±
- (H) PCC BASE COURSE, 9"±
- (I) COMB CONC CURB & GUTTER
- (J) PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
- (K) CONCRETE BARRIER WALL
- (L) PCC SHOULDERS, 11"±
- (M) CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
- (N) JOINTED PCC PAVEMENT, 11"±
- (O) STABILIZED SUB-BASE, 4"±
- (P) SUB-BASE GRANULAR MATERIAL, 24"±
- (Q) SUB-BASE GRANULAR MATERIAL, 12"±
- (R) PIPE UNDERDRAIN

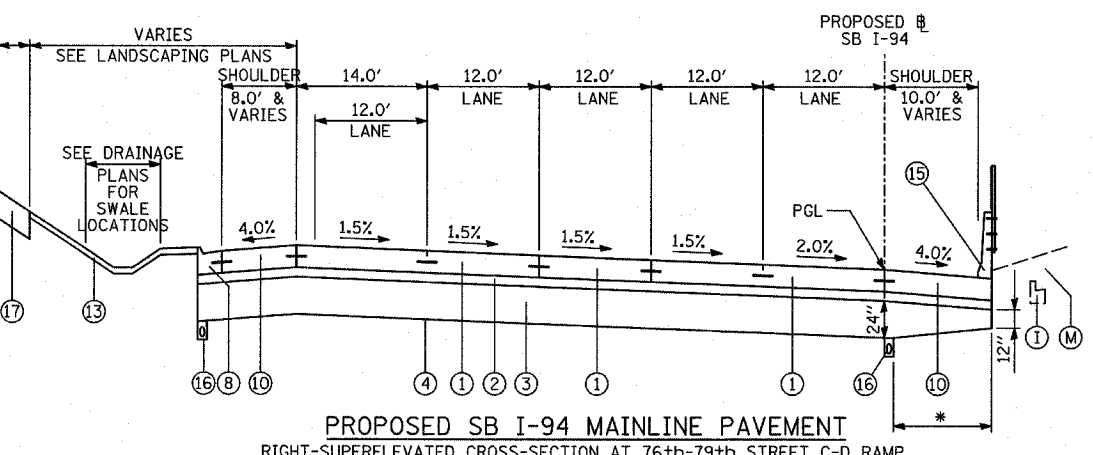
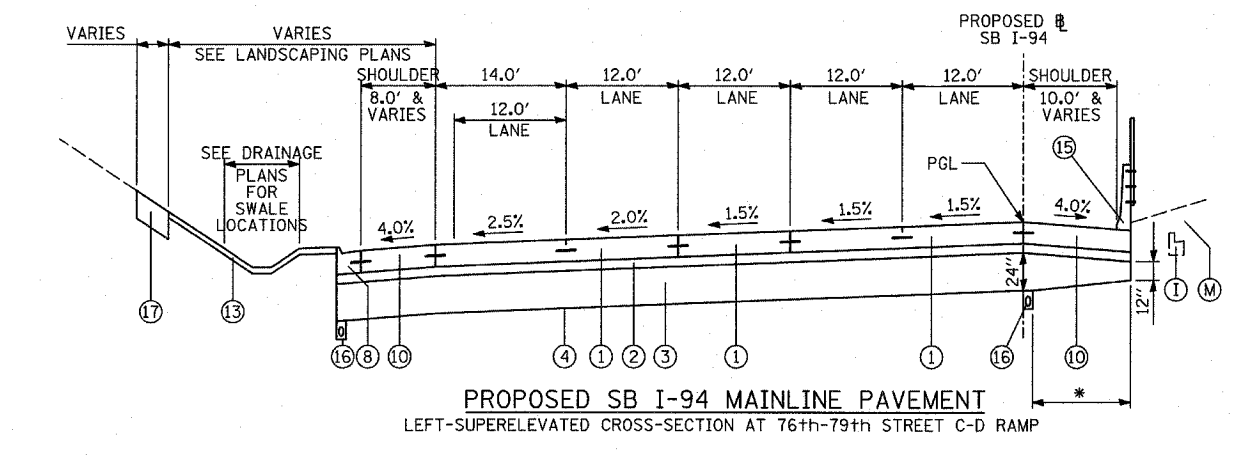
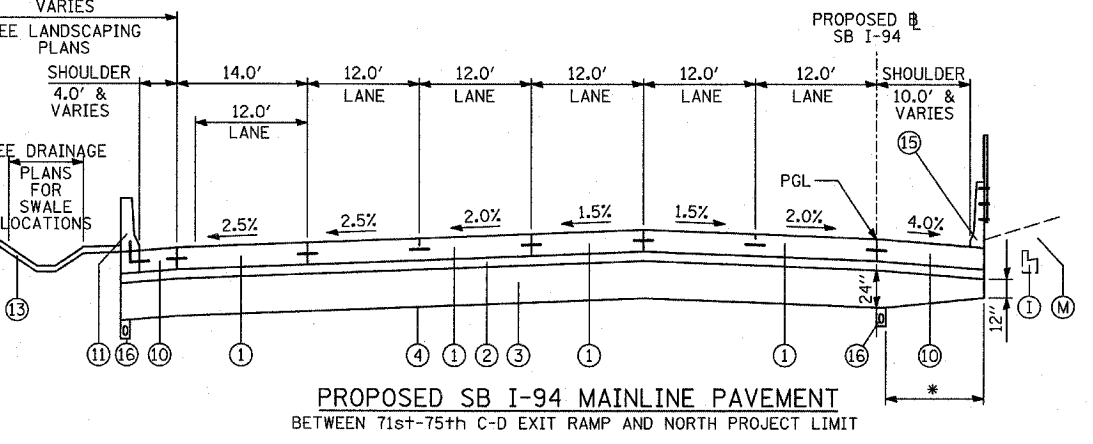
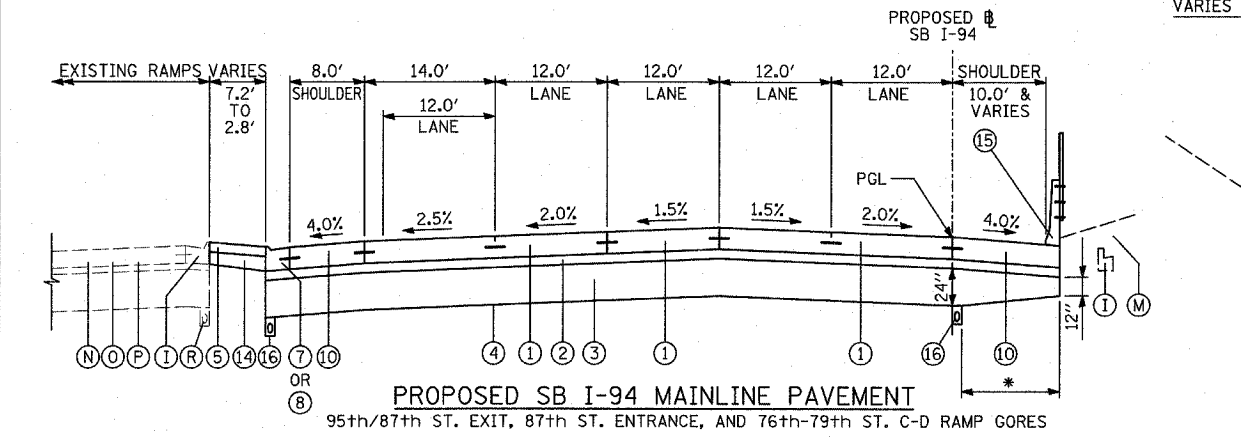
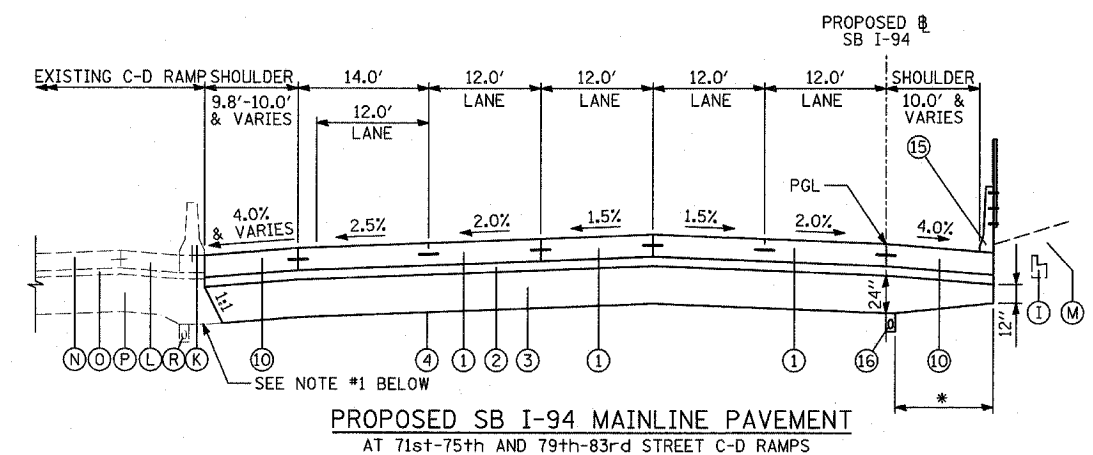
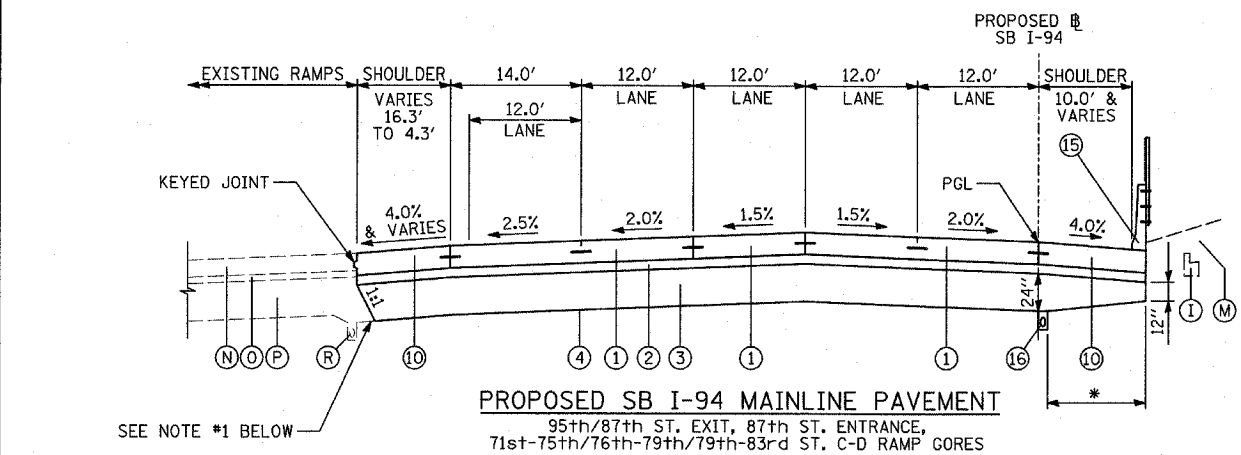
**NOTES:**

1. REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.
2. ANY REQUIRED REGRADING OF EXISTING ADJACENT SUB-BASE GRANULAR MATERIAL SHALL BE INCLUDED IN THE COST OF "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

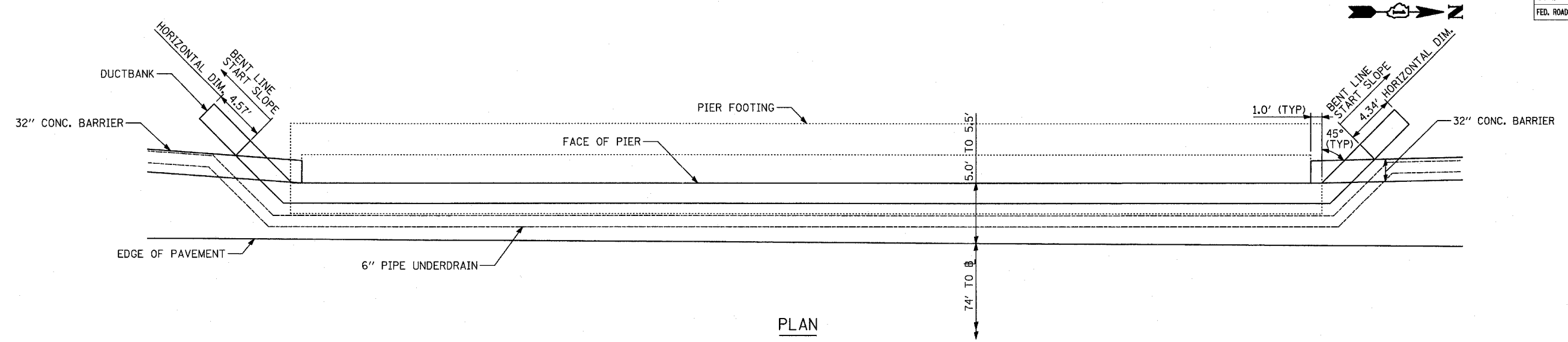
\* - VARYING DEPTH PAID FOR AS "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

REVISIONS	
NAME	DATE

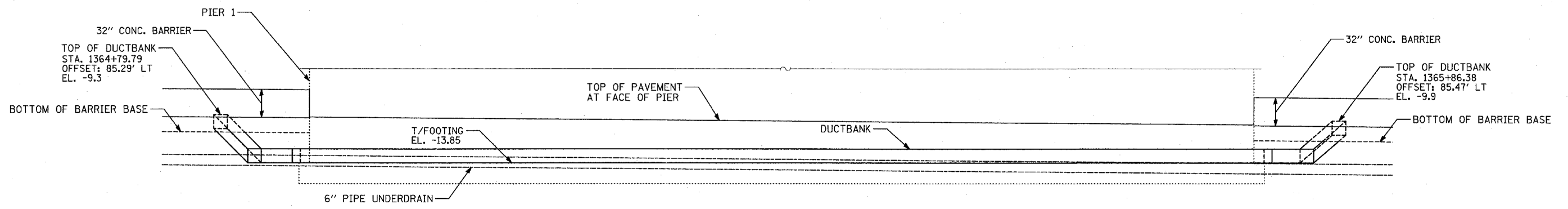
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**PROPOSED TYPICAL SECTIONS**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**(SHEET 2 OF 2)**  
 SCALE: NONE      DRAWN BY: MPG  
 DATE: MARCH 1, 2006      CHECKED BY: TGB



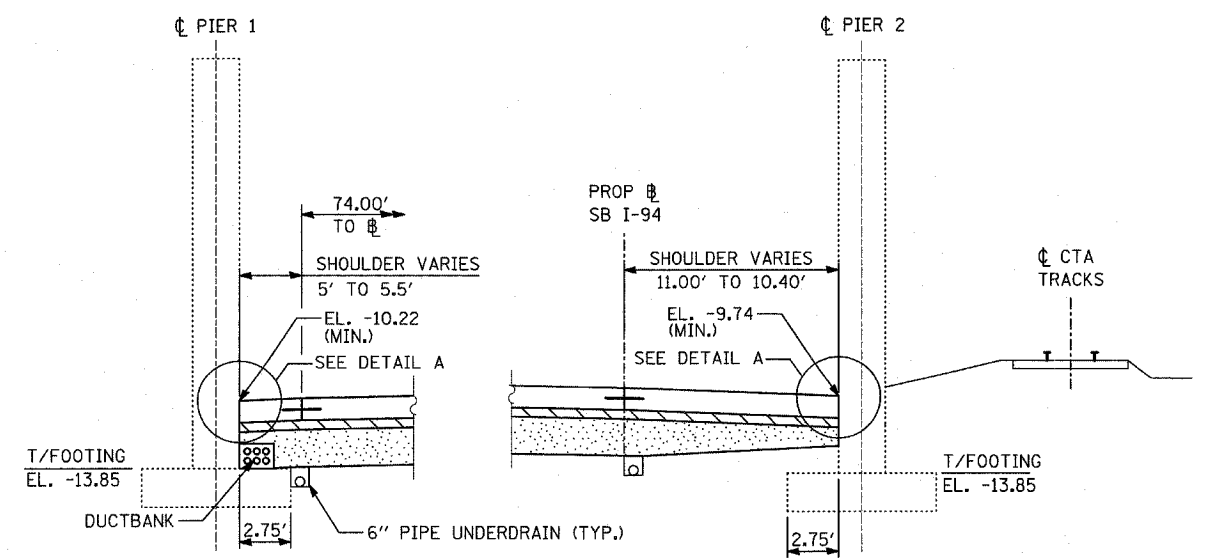
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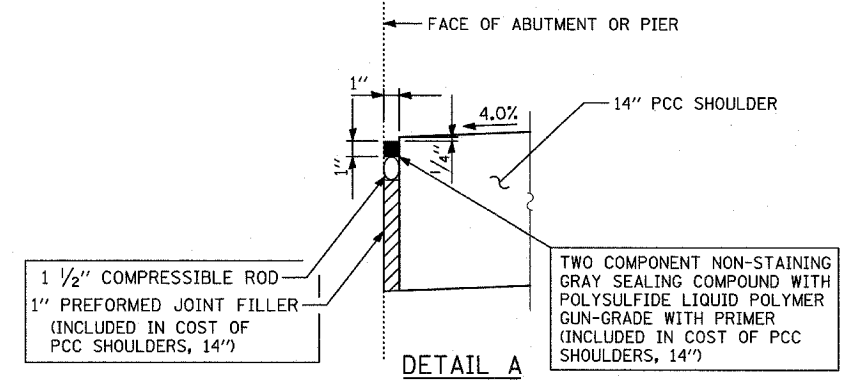
PLAN



ELEVATION



SHOULDER DETAIL AT 71st STREET



DETAIL A

NOTES

- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
- ALL VIEWS LOOKING UPSTATION.
- CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
- ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
- THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
- REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
- MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

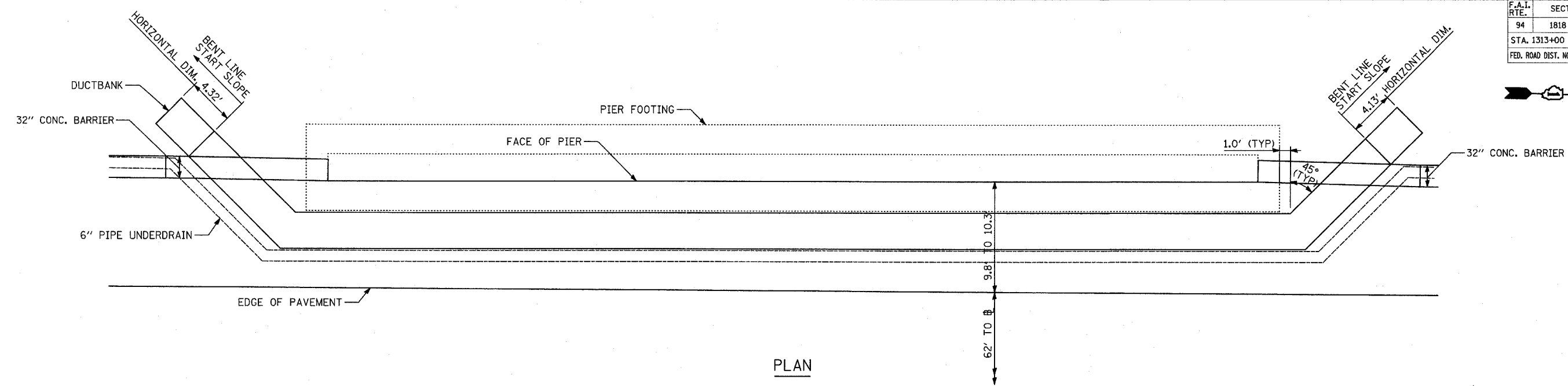
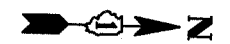
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 TYPICAL SECTIONS AND DETAILS:  
 71ST. STREET OVERPASS FOOTINGS,  
 DUCTBANK & UNDERDRAIN DETAILS

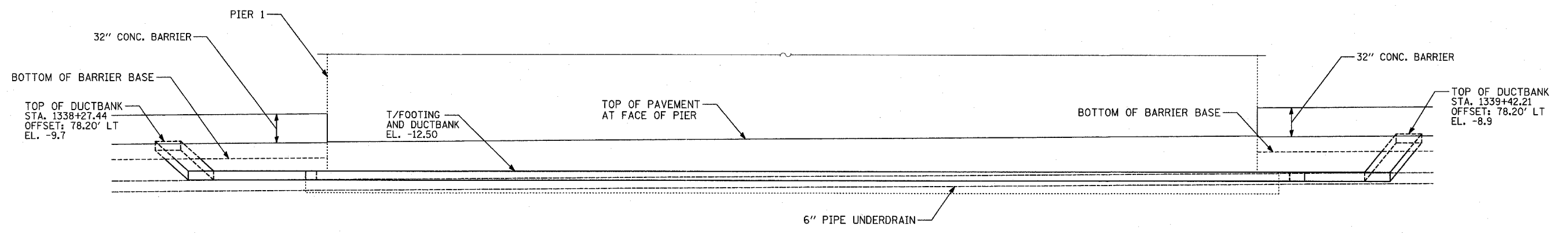
SCALE: NONE  
 DATE: MARCH 1, 2006  
 DRAWN BY: TB  
 CHECKED BY: TGB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	15
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

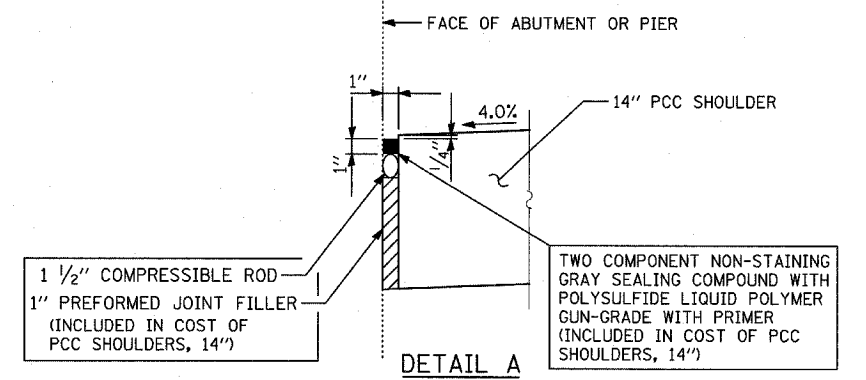
60B18



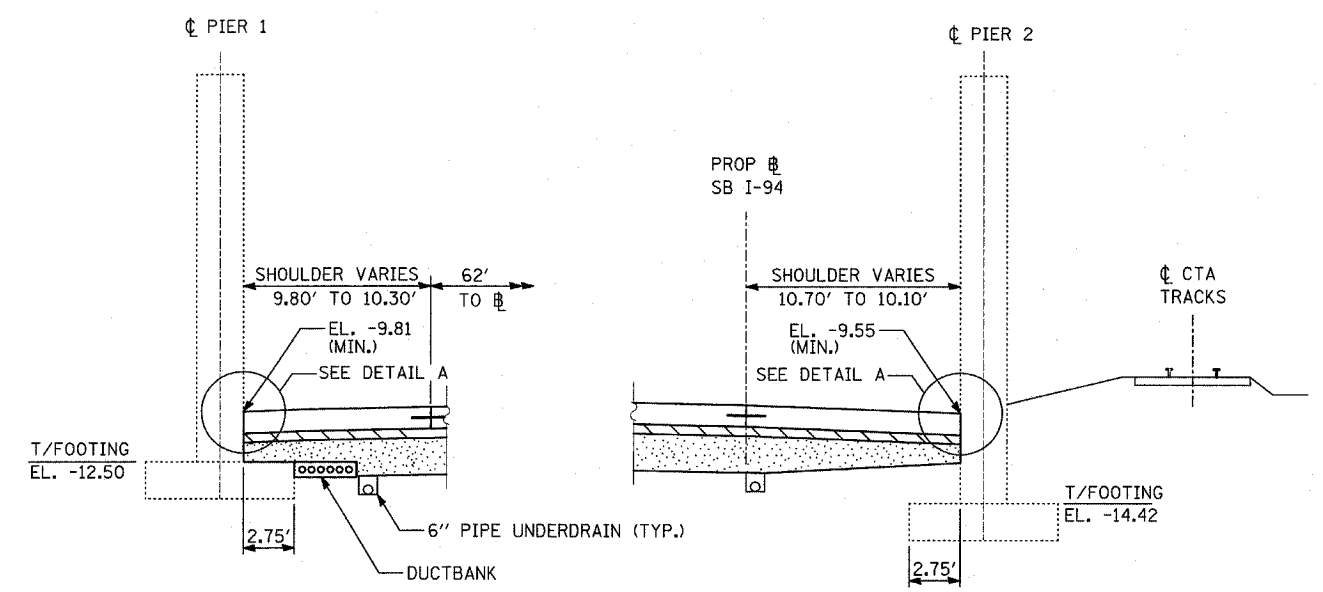
PLAN



ELEVATION



SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"



SHOULDER DETAIL AT 75th STREET

NOTES

- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
- ALL VIEWS LOOKING UPSTATION.
- CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
- ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
- THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
- REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
- MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
 TYPICAL SECTIONS AND DETAILS:  
 75TH. STREET OVERPASS FOOTINGS,  
 DUCTBANK & UNDERDRAIN DETAILS

SCALE: NONE  
 DATE: MARCH 1, 2006  
 DRAWN BY: TB  
 CHECKED BY: TGB



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	17
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

PAVED SHOULDER REMOVAL

DESCRIPTION OF EXISTING LOCATION	OFFSET DIRECTION	FROM		TO		PAVED SHLD REMOVAL (SQ YD)	PAVED SHLD REM SPL (SQ YD)
		ALIGNMENT	STATION	ALIGNMENT	STATION		
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1313+00.0	SB I-94(RYAN)	1313+24.6	38	
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1313+24.6	SB I-94(RYAN)	1313+60.1		52
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1313+60.1	SB I-94(RYAN)	1313+89.0	40	
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1313+88.9	SB I-94(RYAN)	1314+39.3		63
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1314+39.2	SB I-94(RYAN)	1316+37.1	230	
76TH-79TH ST. ENTRANCE RAMP SHLDR	LT	SB I-94(RYAN)	1316+37.1	SB I-94(RYAN)	1317+46.1	38	
76TH-79TH ST. ENTRANCE RAMP GORE	LT	SB I-94(RYAN)	1319+48.6	SB I-94(RYAN)	1320+17.4	34	
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1326+21.5	SB I-94(RYAN)	1343+41.8	1657	
76TH-79TH ST. EXIT RAMP GORE	LT	SB I-94(RYAN)	1331+92.5	SB I-94(RYAN)	1333+47.0	165	
76TH-79TH ST. EXIT RAMP SHOULDER	LT	SB I-94(RYAN)	1333+79.4	SB I-94(RYAN)	1335+24.6	49	
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1356+96.1	SB I-94(RYAN)	1358+68.1	227	
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1358+68.0	SB I-94(RYAN)	1359+18.1		65
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1359+18.0	SB I-94(RYAN)	1366+00.1	934	
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1313+00.0	SB I-94(RYAN)	1313+24.6	31	
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1313+24.6	SB I-94(RYAN)	1313+59.9		39
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1313+59.9	SB I-94(RYAN)	1313+88.6	31	
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1313+88.6	SB I-94(RYAN)	1314+38.6		52
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1314+38.5	SB I-94(RYAN)	1325+50.6	1130	
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1325+50.1	SB I-94(RYAN)	1326+08.7		59
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1326+08.3	SB I-94(RYAN)	1345+55.1	1917	
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1345+55.1	SB I-94(RYAN)	1345+85.1		29
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1345+85.1	SB I-94(RYAN)	1358+68.5	1245	
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1358+68.5	SB I-94(RYAN)	1359+18.5		47
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1359+18.5	SB I-94(RYAN)	1367+00.0	763	
I000-2A TOTAL							
J000-2A TOTAL						8529	406
TOTAL						8529	406

PAVEMENT REMOVAL

DESCRIPTION OF EXISTING LOCATION	FROM		TO		PAVEMENT REM (SQ YD)	PAVT REMOVAL SPL (SQ YD)
	ALIGNMENT	STATION	ALIGNMENT	STATION		
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1313+00.0	SB I-94(RYAN)	1313+24.6	129	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1313+24.6	SB I-94(RYAN)	1313+60.1		186
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1313+59.9	SB I-94(RYAN)	1313+88.9	153	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1313+88.6	SB I-94(RYAN)	1314+39.2		276
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1314+38.6	SB I-94(RYAN)	1320+00.0	4140	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1320+00.0	SB I-94(RYAN)	1325+53.5	3762	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1325+50.6	SB I-94(RYAN)	1326+11.1		398
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1326+08.7	SB I-94(RYAN)	1330+00.0	2408	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1330+00.0	SB I-94(RYAN)	1336+72.9	883	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1330+00.0	SB I-94(RYAN)	1340+00.0	5355	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1340+00.0	SB I-94(RYAN)	1345+55.1	3923	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1345+55.1	SB I-94(RYAN)	1345+85.1		260
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1345+85.1	SB I-94(RYAN)	1350+00.0	3512	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1350+00.0	SB I-94(RYAN)	1358+68.5	6829	
71ST TO 75TH STREET EXIT RAMP	SB I-94(RYAN)	1358+67.9	SB I-94(RYAN)	1359+18.0		45
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1358+68.1	SB I-94(RYAN)	1359+18.5		270
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1359+17.9	SB I-94(RYAN)	1363+70.7	658	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1359+18.1	SB I-94(RYAN)	1367+00.0	4569	
REMOVAL OF TEMPORARY PAVEMENT	-	-	-	-	2400	
I000-2A TOTAL						
J000-2A TOTAL					38721	1435
TOTAL					38721	1435

COMBINATION CONCRETE CURB AND GUTTER REMOVAL

DESCRIPTION OF EXISTING LOCATION	FROM			TO			COMB CURB GUTTER REM (FOOT)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1316+37.1	73.6 LT	SB I-94(RYAN)	1318+82.2	86.3 LT	244
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1319+46.7	65.0 LT	SB I-94(RYAN)	1333+49.0	65.0 LT	1409
76TH TO 79TH ST. ENTRANCE RAMP	SB I-94(RYAN)	1319+46.7	65.0 LT	SB I-94(RYAN)	1321+24.6	76.0 LT	180
76TH TO 79TH ST. EXIT RAMP	SB I-94(RYAN)	1332+44.3	76.0 LT	SB I-94(RYAN)	1333+49.0	65.0 LT	107
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1333+79.3	86.3 LT	SB I-94(RYAN)	1338+37.7	61.2 LT	460
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1342+07.1	70.7 LT	SB I-94(RYAN)	1348+21.4	86.1 LT	615
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1359+25.0	86.3 LT	SB I-94(RYAN)	1364+79.8	61.7 LT	556
I000-2A TOTAL							
J000-2A TOTAL							3571
TOTAL							3571

TYLIN INTERNATIONAL

NOTES:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES  
REMOVAL QUANTITIES - SHEET 1

SCALE: NONE  
DATE: MARCH 1, 2006  
DRAWN BY: MPG  
CHECKED BY: RTM



**CONCRETE BARRIER REMOVAL**

DESCRIPTION OF EXISTING LOCATION	BARRIER TYPE	FROM			TO			CONC BARRIER REMOV (FOOT)
		ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
SB I-94 (DAN RYAN EXPRESSWAY)	PERMANENT	SB I-94(RYAN)	1313+24.0	65.5 LT	SB I-94(RYAN)	1316+37.1	73.6 LT	312
SB I-94 (DAN RYAN EXPRESSWAY)	PERMANENT	SB I-94(RYAN)	1339+32.2	62.6 LT	SB I-94(RYAN)	1342+07.1	71.1 LT	276
SB I-94 (DAN RYAN EXPRESSWAY)	TEMPORARY	SB I-94(RYAN)	1355+55.6	9.5 RT	SB I-94(RYAN)	1355+85.6	11.0 RT	30
SB I-94 (DAN RYAN EXPRESSWAY)	TEMPORARY	SB I-94(RYAN)	1355+96.5	11.0 RT	SB I-94(RYAN)	1356+16.4	9.4 RT	20
SB I-94 (DAN RYAN EXPRESSWAY)	PERMANENT	SB I-94(RYAN)	1365+87.5	79.7 LT	SB I-94(RYAN)	1367+00.0	81.0 LT	113
							I000-2A TOTAL	
							J000-2A TOTAL	751
							TOTAL	751

**GUARDRAIL REMOVAL**

DESCRIPTION OF EXISTING LOCATION	FROM			TO			GUARDRAIL REMOV (FOOT)	
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET		
76TH TO 79TH ST. ENTRANCE RAMP	SB I-94(RYAN)	1316+37.1	73.6 LT	SB I-94(RYAN)	1317+18.2	77.8 LT	81	
76TH TO 79TH ST. EXIT RAMP	SB I-94(RYAN)	1333+79.3	86.3 LT	SB I-94(RYAN)	1334+96.7	77.0 LT	118	
							I000-2A TOTAL	
							J000-2A TOTAL	199
							TOTAL	199

**CTA BARRIER REMOVAL**

DESCRIPTION OF EXISTING LOCATION	FROM			TO			CTA BARRIER REMOV (FOOT)	
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET		
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1313+24.4	7.7 RT	SB I-94(RYAN)	1320+35.0	9.6 RT	711	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1320+85.0	9.5 RT	SB I-94(RYAN)	1331+97.8	10.5 RT	1113	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1332+79.5	10.8 RT	SB I-94(RYAN)	1338+38.1	11.4 RT	559	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1339+32.4	10.3 RT	SB I-94(RYAN)	1355+55.6	9.2 RT	1623	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1356+16.4	9.2 RT	SB I-94(RYAN)	1364+79.6	11.7 RT	864	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1365+89.0	11.0 RT	SB I-94(RYAN)	1367+00.0	8.9 RT	111	
							I000-2A TOTAL	
							J000-2A TOTAL	4981
							TOTAL	4981

**CHAIN LINK FENCE REMOVAL**

DESCRIPTION OF EXISTING LOCATION	FROM			TO			CH LK FENCE REMOV (FOOT)	
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET		
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1355+51.0	10.0 RT	SB I-94(RYAN)	1356+21.1	10.0 RT	72	
							I000-2A TOTAL	
							J000-2A TOTAL	72
							TOTAL	72

**TREE REMOVAL (6 TO 15 UNITS)**

LOCATION					TREE REMOV 6-15 (UNIT)
ALIGNMENT	STATION	OFFSET	NORTHING	EASTING	
SB I-94(RYAN)	1332+73.1	113.8 LT	1854645.7	1177202.4	14
SB I-94(RYAN)	1339+39.0	75.0 LT	1855312.7	1177245.5	11
TOTAL (6-15 UNITS)					25

**TYLIN**INTERNATIONAL

**NOTES:**

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

**SCHEDULE OF QUANTITIES  
REMOVAL QUANTITIES - SHEET 2**

SCALE: NONE  
DATE: MARCH 1, 2006  
DRAWN BY: MPG  
CHECKED BY: RTM

**PROPOSED RECONSTRUCTED MAINLINE PAVEMENT**

LOCATION DESCRIPTION	FROM		TO		CONT REINF PCC PVT 14 (SQ YD)	PAVT REINFORCEMENT 14 (SQ YD)	SUB GRAN MAT B 24 (SQ YD)	GEOTECH FAB F/GR STAB (SQ YD)	STAB SUB-BASE 6	PROTECTIVE COAT (SQ YD)
	ALIGNMENT	STATION	ALIGNMENT	STATION						
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1313+00.0	SB I-94(RYAN)	1330+00.0	12282	12282	15946	15946	15946	12282
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1330+00.0	SB I-94(RYAN)	1350+00.0	14711	14711	19163	19163	19163	14711
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1350+00.0	SB I-94(RYAN)	1367+00.0	12817	12817	16641	16641	16641	12817
ADDITIONAL QUANTITY FOR STAGING OVERDIG	-	-	-	-			2400	1200	1200	
I000-2A TOTAL										
J000-2A TOTAL					39810	39810	54150	52950	52950	39810
TOTAL					39810	39810	54150	52950	52950	39810

**CONCRETE BARRIER**

LOCATION DESCRIPTION	FROM			TO			CONC BAR SIN FACE 32 (FOOT)	CONC BAR SIN FACE 42 SPL (FOOT)	CONC BAR TRANS (FOOT)	BARRIER BASE (FOOT)	BAR WALL MKR TYPE C (EACH)	PROTECTIVE COAT (SQ YD)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET						
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1313+27.1	64.4 LT	SB I-94(RYAN)	1313+42.1	64.8 LT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1313+42.1	64.8 LT	SB I-94(RYAN)	1317+42.8	80.8 LT	399			399	5	304
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1333+79.3	86.3 LT	SB I-94(RYAN)	1338+27.2	72.0 LT	449			449	6	342
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1338+27.2	72.0 LT	SB I-94(RYAN)	1338+42.2	71.8 LT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1339+28.2	72.3 LT	SB I-94(RYAN)	1339+43.2	72.0 LT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1339+43.2	72.0 LT	SB I-94(RYAN)	1341+93.2	72.0 LT	250			250	5	190
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1364+30.1	80.0 LT	SB I-94(RYAN)	1364+73.3	80.0 LT	44			44	5	33
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1364+73.3	80.0 LT	SB I-94(RYAN)	1364+88.3	79.0 LT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1365+78.3	79.6 LT	SB I-94(RYAN)	1365+93.3	80.0 LT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1365+93.3	80.0 LT	SB I-94(RYAN)	1367+00.0	80.0 LT	107			107	5	81
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1313+25.5	7.8 RT	SB I-94(RYAN)	1313+40.4	6.3 RT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1313+40.4	6.3 RT	SB I-94(RYAN)	1319+97.5	10.0 RT		658		658	6	572
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1319+97.5	10.0 RT	SB I-94(RYAN)	1320+35.0	8.9 RT			38	38		30
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1320+85.0	8.2 RT	SB I-94(RYAN)	1321+22.5	9.6 RT			38	38		30
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1321+22.5	9.6 RT	SB I-94(RYAN)	1331+82.3	10.0 RT		1060		1060	7	922
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1331+82.3	10.0 RT	SB I-94(RYAN)	1331+97.3	10.7 RT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1332+79.3	10.7 RT	SB I-94(RYAN)	1332+94.3	10.0 RT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1332+94.3	10.0 RT	SB I-94(RYAN)	1338+21.7	10.0 RT		528		528	6	459
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1338+21.7	10.0 RT	SB I-94(RYAN)	1338+36.7	10.7 RT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1339+34.7	10.1 RT	SB I-94(RYAN)	1339+49.7	10.0 RT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1339+49.7	10.0 RT	SB I-94(RYAN)	1355+85.6	10.0 RT		1636		1636	9	1422
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1355+96.4	10.0 RT	SB I-94(RYAN)	1364+63.4	10.2 RT		867		867	7	754
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1364+63.4	10.2 RT	SB I-94(RYAN)	1364+78.4	11.0 RT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1365+88.4	10.4 RT	SB I-94(RYAN)	1366+03.4	10.0 RT			15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1366+03.4	10.0 RT	SB I-94(RYAN)	1367+00.0	10.0 RT		97		97	5	84
I000-2A TOTAL												
J000-2A TOTAL							1249	4846	256	6351	66	5365
TOTAL							1249	4846	256	6351	66	5365

**PROPOSED GUARDRAIL**

LOCATION DESCRIPTION	ALIGNMENT	END OF TYPE 6 TERMINAL		JOINT BETWEEN TERMINALS		END OF TYPE 1 TERMINAL		TR BAR TRM T1 SPL TAN (EACH)	TRAF BAR TERM T6 (EACH)	GUARDRAIL MKR TYPE A (EACH)	TERMINAL MARKER - DA (EACH)
		STATION	OFFSET	STATION	OFFSET	STATION	OFFSET				
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1317+42.8	80.8 LT	1317+73.6	82.0 LT	1318+23.9	85.0 LT	1	1	4	1
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1341+93.2	72.0 LT	1342+23.8	72.0 LT	1342+73.8	73.0 LT	1	1	4	1
I000-2A TOTAL											
J000-2A TOTAL								2	2	8	2
TOTAL								2	2	8	2

**TYLIN**INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES  
PROPOSED QUANTITIES - SHEET 1

SCALE: NONE                      DRAWN BY: MPG  
DATE: MARCH 1, 2006            CHECKED BY: RTM

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24

LOCATION DESCRIPTION	FROM			TO			COMB CC&G TM2.24 (FOOT)	PROTECTIVE COAT (SQ YD)	
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET			
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1331+94.1	72.0 LT	SB I-94(RYAN)	1332+44.2	76.0 LT	56.5	16	
							I000-2A TOTAL		
							J000-2A TOTAL	56.5	16
							TOTAL	56.5	16

CTA GATES

LOCATION DESCRIPTION	LOCATION			CTA GATES (EACH)	
	ALIGNMENT	STATION	OFFSET		
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1317+16.2	11.8 RT	1	
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1321+92.2	11.8 RT	1	
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1336+75.1	11.8 RT	1	
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1347+40.6	11.8 RT	1	
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1356+75.9	11.8 RT	1	
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1366+84.0	11.8 RT	1	
				I000-2A TOTAL	
				J000-2A TOTAL	6
				TOTAL	6

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24

LOCATION DESCRIPTION	FROM			TO			COMB CC&G TM4.24 (FOOT)	PROTECTIVE COAT (SQ YD)	
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET			
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1321+25.4	76.0 LT	SB I-94(RYAN)	1331+94.1	72.0 LT	1079.0	327	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1341+93.2	72.0 LT	SB I-94(RYAN)	1344+00.7	72.0 LT	207.5	63	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1360+40.6	80.0 LT	SB I-94(RYAN)	1364+30.1	80.0 LT	389.5	118	
							I000-2A TOTAL		
							J000-2A TOTAL	1676.0	507
							TOTAL	1676.0	507

CTA FENCE

LOCATION DESCRIPTION	FROM			TO			CTA FENCE (FOOT)	
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET		
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1313+25.7	9.5 RT	SB I-94(RYAN)	1317+16.2	11.8 RT	391	
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1317+22.2	11.8 RT	SB I-94(RYAN)	1320+35.0	11.2 RT	313	
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1320+85.0	10.4 RT	SB I-94(RYAN)	1321+92.2	11.8 RT	108	
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1321+98.2	11.8 RT	SB I-94(RYAN)	1331+97.7	12.4 RT	999	
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1332+79.0	12.4 RT	SB I-94(RYAN)	1336+75.1	11.8 RT	397	
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1336+81.1	11.8 RT	SB I-94(RYAN)	1338+36.8	12.5 RT	156	
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1339+34.6	11.9 RT	SB I-94(RYAN)	1347+40.6	11.8 RT	806	
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1347+46.6	11.8 RT	SB I-94(RYAN)	1356+75.9	11.8 RT	930	
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1356+81.9	11.8 RT	SB I-94(RYAN)	1364+78.5	12.8 RT	797	
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1365+88.3	12.1 RT	SB I-94(RYAN)	1366+84.0	11.8 RT	96	
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1366+90.0	11.8 RT	SB I-94(RYAN)	1367+00.0	11.8 RT	10	
							I000-2A TOTAL	
							J000-2A TOTAL	5003
							TOTAL	5003

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48

LOCATION DESCRIPTION	FROM			TO			COMB CC&G TM4.48 (FOOT)	PROTECTIVE COAT (SQ YD)	
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET			
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1317+42.8	80.8 LT	SB I-94(RYAN)	1318+82.2	86.3 LT	138.5	73	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1344+00.7	72.0 LT	SB I-94(RYAN)	1348+21.4	86.0 LT	421.0	221	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1359+25.0	86.3 LT	SB I-94(RYAN)	1360+40.6	80.0 LT	116.0	61	
							I000-2A TOTAL		
							J000-2A TOTAL	675.5	355
							TOTAL	675.5	355

CONCRETE MEDIAN SURFACE, 6" (SPECIAL)

LOCATION DESCRIPTION	OFFSET DIRECTION	FROM		TO		CONC MEDIAN SURF 6 SP (SQ FT)	
		ALIGNMENT	STATION	ALIGNMENT	STATION		
SB I-94 (RYAN) - GUARDRAIL	LT	SB I-94(RYAN)	1317+42.7	SB I-94(RYAN)	1318+48.6	306	
SB I-94 (RYAN) - 75TH ENTRANCE GORE	LT	SB I-94(RYAN)	1321+25.4	SB I-94(RYAN)	1321+79.4	242	
SB I-94 (RYAN) - 75TH EXIT GORE	LT	SB I-94(RYAN)	1331+94.1	SB I-94(RYAN)	1332+44.2	255	
SB I-94 (RYAN) - GUARDRAIL	LT	SB I-94(RYAN)	1341+93.2	SB I-94(RYAN)	1342+98.3	305	
						I000-2A TOTAL	
						J000-2A TOTAL	1108
						TOTAL	1108

PORTLAND CEMENT CONCRETE SHOULDERS 14"

LOCATION DESCRIPTION	OFFSET DIRECTION	FROM		TO		PCC SHOULDERS 14 (SQ YD)	PROTECTIVE COAT (SQ YD)	
		ALIGNMENT	STATION	ALIGNMENT	STATION			
SB I-94 (RYAN) - 79TH ST TO 75TH ENT	LT	SB I-94(RYAN)	1313+00.0	SB I-94(RYAN)	1317+43.0	188	188	
SB I-94 (RYAN) - 75TH ENT TO 79TH EXIT	LT	SB I-94(RYAN)	1318+83.0	SB I-94(RYAN)	1333+77.6	1405	1405	
SB I-94 (RYAN) - 79TH EXIT TO 71ST ENT	LT	SB I-94(RYAN)	1333+79.0	SB I-94(RYAN)	1344+00.8	901	901	
SB I-94 (RYAN) - 71ST ENT TO 75TH EXIT	LT	SB I-94(RYAN)	1348+22.1	SB I-94(RYAN)	1359+23.9	1249	1249	
SB I-94 (RYAN) - 75TH EXIT TO NORTH LIMIT	LT	SB I-94(RYAN)	1360+40.5	SB I-94(RYAN)	1367+00.0	341	341	
SB I-94 (DAN RYAN) MEDIAN	RT	SB I-94(RYAN)	1313+00.0	SB I-94(RYAN)	1330+00.0	1747	1747	
SB I-94 (DAN RYAN) MEDIAN	RT	SB I-94(RYAN)	1330+00.0	SB I-94(RYAN)	1350+00.0	2235	2235	
SB I-94 (DAN RYAN) MEDIAN	RT	SB I-94(RYAN)	1350+00.0	SB I-94(RYAN)	1367+00.0	1899	1899	
						I000-2A TOTAL		
						J000-2A TOTAL	9965	9965
						TOTAL	9965	9965

REVISIONS	
NAME	DATE

**TEMPORARY CONCRETE BARRIER**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	21
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

60B1

FROM		TO		TOTAL BARRIER REQUIRED (FOOT)	STAGE *	REMAINS THROUGH STAGE *	TEMPORARY CONCRETE BARRIER (FOOT)†	RELOCATE TEMPORARY CONC BARR (FOOT)†	BARRIER REMAIN FROM STAGE-STAGE (FOOT)	BARRIER RELOCATED @ END STAGE (FOOT)	BARRIER REMOVED @ END STAGE (FOOT)	
ALIGNMENT	STATION	ALIGNMENT	STATION									
SB I-94	1313+00	SB I-94	1314+95	200	1	1A	200		200			
SB I-94	1319+13	SB I-94	1333+06	1420	1	1A	1420		1420			
SB I-94	1338+51	SB I-94	1342+81	430	1	1A	430		430			
SB I-94	1349+16	SB I-94	1359+85	1080	1	1A	1080		1080			
SB I-94	1364+01	SB I-94	1367+00	300	1	1A	300		300			
STAGE 1 RAMP TERMINALS				1000	1	1	1000			1000		
SB I-94	1313+00	SB I-94	1314+95	200	1A	1					200	
SB I-94	1319+13	SB I-94	1333+06	1420	1A	1				1420		
SB I-94	1338+51	SB I-94	1342+81	430	1A	1				430		
SB I-94	1349+16	SB I-94	1359+85	1080	1A	1				1080		
SB I-94	1364+01	SB I-94	1367+00	300	1A	1				300		
STAGE 1 RAMP TERMINALS (RELOCATE, STAGE 1A)				1000	1A	1A		1000				1000
SB I-94	1319+15	SB I-94	1333+22	1420	W	W		1420		1420		
SB I-94	1337+72	SB I-94	1341+92	430	W	W		430		430		
SB I-94	1348+72	SB I-94	1359+42	1080	W	W		1080		1080		
SB I-94	1363+29	SB I-94	1366+29	300	W	W		300		300		
SB I-94	1313+00	SB I-94	1345+30	3230	2	2		3230		3230		
SB I-94	1345+30	SB I-94	1370+70	2540	2	2	2540			2340	200	
SB I-94	1313+00	SB I-94	1368+60	5570	3	3		5570			5570	
STAGE 1 TOTALS:				4430			4430		3430	1000		
STAGE 1A TOTALS:				4430				1000		3230	1200	
WINTER TOTALS:				3230				3230		3230		
STAGE 2 TOTALS:				5770			2540	3230		5570	200	
STAGE 3 TOTALS:				5570				5570			5570	
FINAL TOTALS:				23430			6970	13030	3430	13030	6970	

† - DENOTES IDOT PAY ITEMS

**TEMPORARY IMPACT ATTENUATORS**

LOCATION		IMPACT ATTENUATORS REQUIRED (EACH)	STAGE *	REMAINS THROUGH STAGE *	TEMPORARY IMPACT ATTENUATOR (EACH)†	RELOCATE IMPACT ATTENUATOR (EACH)†	ATTENUATOR REMAIN FROM STAGE-STAGE (EACH)	ATTENUATOR RELOCATED @ END STAGE (EACH)	ATTENUATOR REMOVED @ END STAGE (EACH)
ALIGNMENT	STATION								
SB I-94	1315+22	1	1	1	1			1	
SB I-94	1333+49	1	1	1	1			1	
SB I-94	1343+11	1	1	1	1			1	
SB I-94	1360+11	1	1	1	1			1	
SB I-94	1313+25	1	W	W		1			1
SB I-94	1333+48	1	W	W		1		1	
SB I-94	1342+19	1	W	W		1		1	
SB I-94	1359+68	1	W	W		1			1
SB I-94	1366+55	1	W	W	1			1	
SB I-94	1370+97	1	2	2		1		1	
STAGE 2 WORK ZONE		2	2	2		2		2	
SB I-94	1368+88	1	3	3		1			1
STAGE 3 WORK ZONE		2	3	3		2			2
STAGE 1 TOTALS:		4			4			4	
WINTER TOTALS:		5			1	4		3	2
STAGE 2 TOTALS:		3				3		3	
STAGE 3 TOTALS:		3				3			3
FINAL TOTALS:		15			5	10		10	5

† - DENOTES IDOT PAY ITEMS

**NOTES:**

- "ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS
- STAGE 1A REFERS ONLY TO THE RELOCATION OF TEMPORARY CONCRETE BARRIER REQUIRED FOR RAMP TERMINAL CONSTRUCTION.

**TYLIN** INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES  
TEMPORARY CONCRETE BARRIER AND  
TEMPORARY IMPACT ATTENUATOR  
QUANTITIES - SHEET 1

SCALE: NONE                      DRAWN BY: RTM  
DATE: MARCH 1, 2006            CHECKED BY: MPG

EARTHWORK SCHEDULE

LOCATION DESCRIPTION	FROM		TO		EARTH EXCAVATION (CU YD)	EMBANKMENT (CU YD)	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL (CU YD)
	ALIGNMENT	STATION	ALIGNMENT	STATION			
79TH TO 75TH	SB I-94	1313+00	SB I-94	1339+00	18260	190	855
75TH TO 71ST	SB I-94	1339+50	SB I-94	1363+50	22545	35	1155
12" PGES - SEE SEPARATE SCHEDULE					944		
TOTAL					41749	225	2010

POROUS GRANULAR EMBANKMENT, SUBGRADE

ALIGNMENT	DESCRIPTION	STATION		LENGTH (FOOT)	WIDTH (FOOT)	DEPTH (INCH)	VOLUME (CU YD)
		FROM	TO				
SB I-94	MAINLINE DAN RYAN	1336+50	1339+50	300	85	12	944
TOTAL (CU YD)							944

TYLIN INTERNATIONAL

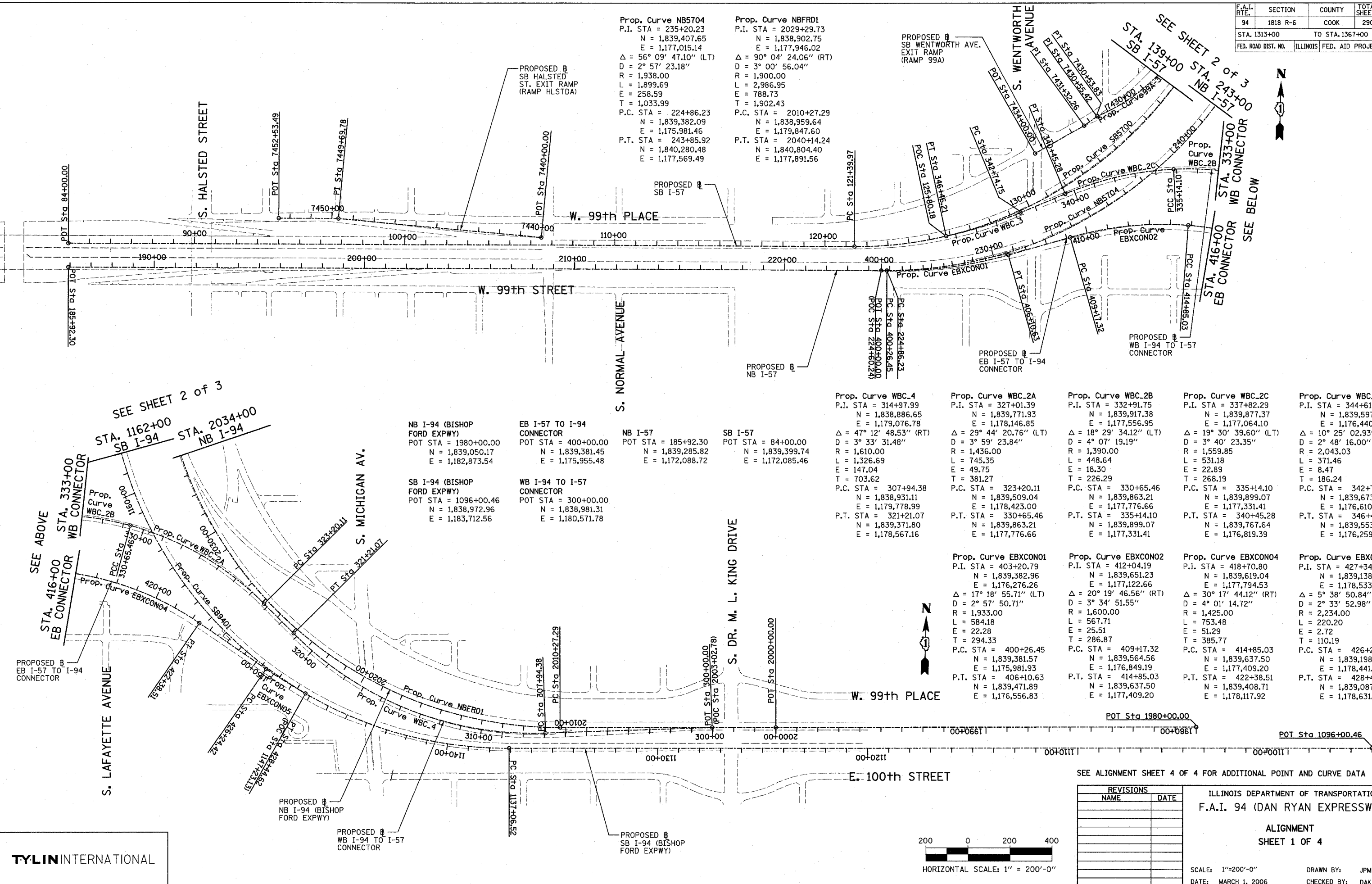
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES  
EARTHWORK QUANTITIES - SHEET 1

SCALE: NONE  
DATE: MARCH 1, 2006

DRAWN BY: MPG  
CHECKED BY: JPM



**Prop. Curve NB5704**  
P.I. STA = 235+20.23  
N = 1,839,407.65  
E = 1,177,015.14  
 $\Delta = 56^\circ 09' 47.10''$  (LT)  
D = 2° 57' 23.18"  
R = 1,938.00  
L = 1,899.69  
E = 258.59  
T = 1,033.99  
P.C. STA = 224+86.23  
N = 1,839,382.09  
E = 1,175,981.46  
P.T. STA = 243+85.92  
N = 1,840,280.48  
E = 1,177,569.49

**Prop. Curve NBFDR1**  
P.I. STA = 2029+29.73  
N = 1,838,902.75  
E = 1,177,946.02  
 $\Delta = 90^\circ 04' 24.06''$  (RT)  
D = 3° 00' 56.04"  
R = 1,900.00  
L = 2,986.95  
E = 788.73  
T = 1,902.43  
P.C. STA = 2010+27.29  
N = 1,838,959.64  
E = 1,179,847.60  
P.T. STA = 2040+14.24  
N = 1,840,804.40  
E = 1,177,891.56

**NB I-94 (BISHOP FORD EXPWY)**  
POT STA = 1980+00.00  
N = 1,839,050.17  
E = 1,182,873.54

**EB I-57 TO I-94 CONNECTOR**  
POT STA = 400+00.00  
N = 1,839,381.45  
E = 1,175,955.48

**NB I-57**  
POT STA = 185+92.30  
N = 1,839,285.82  
E = 1,172,088.72

**SB I-57**  
POT STA = 84+00.00  
N = 1,839,399.74  
E = 1,172,085.46

**SB I-94 (BISHOP FORD EXPWY)**  
POT STA = 1096+00.46  
N = 1,838,972.96  
E = 1,183,712.56

**WB I-94 TO I-57 CONNECTOR**  
POT STA = 300+00.00  
N = 1,838,981.31  
E = 1,180,571.78

**Prop. Curve WBC.4**  
P.I. STA = 314+97.99  
N = 1,838,886.65  
E = 1,179,076.78  
 $\Delta = 47^\circ 12' 48.53''$  (RT)  
D = 3° 33' 31.48"  
R = 1,610.00  
L = 1,326.69  
E = 147.04  
T = 703.62  
P.C. STA = 307+94.38  
N = 1,838,931.11  
E = 1,179,778.99  
P.T. STA = 321+21.07  
N = 1,839,371.80  
E = 1,178,567.16

**Prop. Curve WBC.2A**  
P.I. STA = 327+01.39  
N = 1,839,771.93  
E = 1,178,146.85  
 $\Delta = 29^\circ 44' 20.76''$  (LT)  
D = 3° 59' 23.84"  
R = 1,436.00  
L = 745.35  
E = 49.75  
T = 381.27  
P.C. STA = 323+20.11  
N = 1,839,509.04  
E = 1,178,423.00  
P.T. STA = 330+65.46  
N = 1,839,863.21  
E = 1,177,776.66

**Prop. Curve WBC.2B**  
P.I. STA = 332+91.75  
N = 1,839,917.38  
E = 1,177,556.95  
 $\Delta = 18^\circ 29' 34.12''$  (LT)  
D = 4° 07' 19.19"  
R = 1,390.00  
L = 448.64  
E = 18.30  
T = 226.29  
P.C. STA = 330+65.46  
N = 1,839,863.21  
E = 1,177,776.66  
P.T. STA = 335+14.10  
N = 1,839,899.07  
E = 1,177,331.41

**Prop. Curve WBC.2C**  
P.I. STA = 337+82.29  
N = 1,839,877.37  
E = 1,177,064.10  
 $\Delta = 19^\circ 30' 39.60''$  (LT)  
D = 3° 40' 23.35"  
R = 1,559.85  
L = 531.18  
E = 22.89  
T = 268.19  
P.C. STA = 335+14.10  
N = 1,839,899.07  
E = 1,177,331.41  
P.T. STA = 340+45.28  
N = 1,839,767.64  
E = 1,176,819.39

**Prop. Curve WBC.3**  
P.I. STA = 344+61.00  
N = 1,839,597.54  
E = 1,176,440.07  
 $\Delta = 10^\circ 25' 02.93''$  (RT)  
D = 2° 48' 16.00"  
R = 2,043.03  
L = 371.46  
E = 8.47  
T = 186.24  
P.C. STA = 342+74.75  
N = 1,839,673.75  
E = 1,176,610.01  
P.T. STA = 346+46.21  
N = 1,839,553.32  
E = 1,176,259.15

**Prop. Curve EBXCNO1**  
P.I. STA = 412+04.19  
N = 1,839,382.96  
E = 1,176,276.26  
 $\Delta = 17^\circ 18' 55.71''$  (LT)  
D = 2° 57' 50.71"  
R = 1,933.00  
L = 584.18  
E = 22.28  
T = 294.33  
P.C. STA = 400+26.45  
N = 1,839,381.57  
E = 1,175,981.93  
P.T. STA = 406+10.63  
N = 1,839,471.89  
E = 1,176,556.83

**Prop. Curve EBXCNO2**  
P.I. STA = 418+70.80  
N = 1,839,651.23  
E = 1,177,122.66  
 $\Delta = 20^\circ 19' 46.56''$  (RT)  
D = 3° 34' 51.55"  
R = 1,600.00  
L = 567.71  
E = 25.51  
T = 286.87  
P.C. STA = 409+17.32  
N = 1,839,564.56  
E = 1,177,409.20  
P.T. STA = 414+85.03  
N = 1,839,637.50  
E = 1,177,409.20

**Prop. Curve EBXCNO4**  
P.I. STA = 414+85.03  
N = 1,839,637.50  
E = 1,177,794.53  
 $\Delta = 30^\circ 17' 44.12''$  (RT)  
D = 4° 01' 14.72"  
R = 1,425.00  
L = 753.48  
E = 51.29  
T = 385.77  
P.C. STA = 414+85.03  
N = 1,839,637.50  
E = 1,177,409.20  
P.T. STA = 422+38.51  
N = 1,839,408.71  
E = 1,178,117.92

**Prop. Curve EBXCNO5**  
P.I. STA = 427+34.61  
N = 1,839,138.24  
E = 1,178,533.79  
 $\Delta = 5^\circ 38' 50.84''$  (LT)  
D = 2° 33' 52.98"  
R = 2,234.00  
L = 220.20  
E = 2.72  
T = 110.19  
P.C. STA = 426+24.42  
N = 1,839,198.31  
E = 1,178,441.42  
P.T. STA = 428+44.62  
N = 1,839,087.55  
E = 1,178,631.63

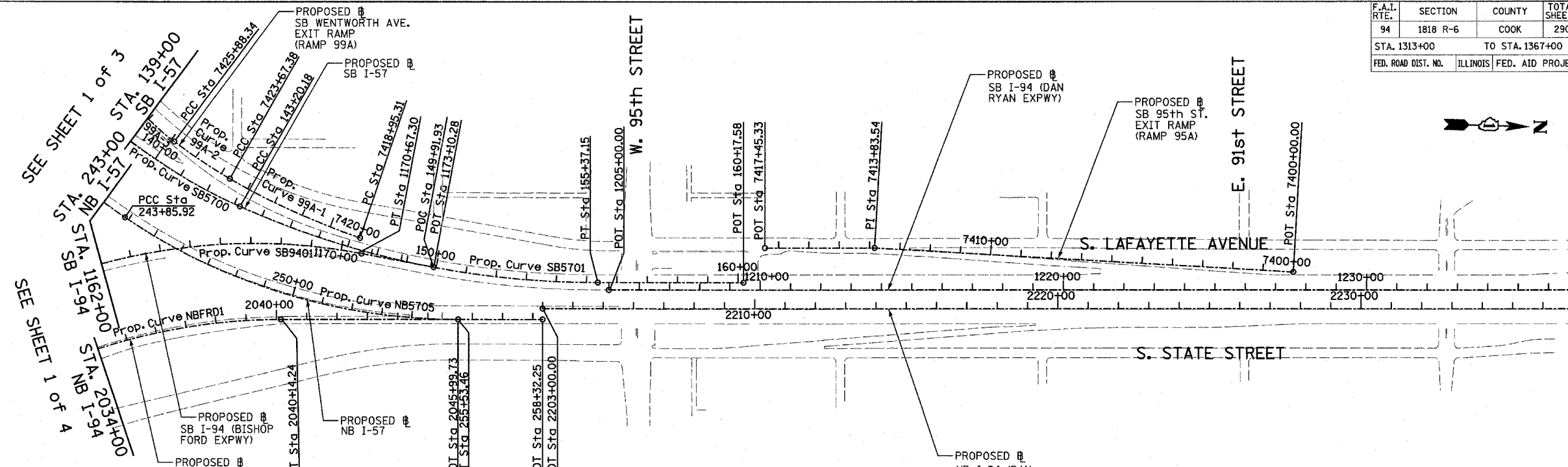
SEE ALIGNMENT SHEET 4 OF 4 FOR ADDITIONAL POINT AND CURVE DATA

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**

**ALIGNMENT SHEET 1 OF 4**

SCALE: 1"=200'-0"  
DATE: MARCH 1, 2006  
DRAWN BY: JPM  
CHECKED BY: DAK



Prop. Curve SB5700  
P.I. STA = 133+70.06  
N = 1,839,521.70  
E = 1,177,054.03  
 $\Delta = 66^\circ 11' 55.15''$  (LT)  
 $D = 3^\circ 02' 10.83''$   
R = 1,887.00  
L = 2,180.21  
E = 365.53  
T = 1,230.09  
P.C. STA = 121+39.97  
N = 1,839,491.52  
E = 1,175,824.31  
P.T. STA = 143+20.18  
N = 1,840,659.02  
E = 1,177,522.68

Prop. Curve SB5701  
P.I. STA = 149+37.75  
N = 1,841,230.00  
E = 1,177,757.97  
 $\Delta = 24^\circ 02' 07.49''$  (LT)  
 $D = 1^\circ 58' 30.13''$   
R = 2,901.00  
L = 1,216.96  
E = 65.01  
T = 617.56  
P.C. STA = 143+20.18  
N = 1,840,659.02  
E = 1,177,522.68  
P.T. STA = 155+37.15  
N = 1,841,847.31  
E = 1,177,740.29

Prop. Curve NB5705  
P.I. STA = 249+87.51  
N = 1,840,788.31  
E = 1,177,892.02  
 $\Delta = 34^\circ 03' 37.98''$  (LT)  
 $D = 2^\circ 55' 02.28''$   
R = 1,964.00  
L = 1,167.54  
E = 90.07  
T = 601.59  
P.C. STA = 243+85.92  
N = 1,840,280.48  
E = 1,177,569.49  
P.T. STA = 255+53.46  
N = 1,841,389.65  
E = 1,177,874.80

Prop. Curve SB9401  
P.I. STA = 1160+15.97  
N = 1,838,783.95  
E = 1,177,299.83  
 $\Delta = 100^\circ 48' 58.07''$  (RT)  
 $D = 2^\circ 59' 59.20''$   
R = 1,910.00  
L = 3,360.79  
E = 1,086.95  
T = 2,309.46  
P.C. STA = 1137+06.52  
N = 1,838,851.99  
E = 1,179,608.28  
P.T. STA = 1170+67.30  
N = 1,841,064.16  
E = 1,177,666.20

SB I-94 (DAN RYAN EXPWY)  
POT STA = 1205+00.00  
N = 1,841,884.04  
E = 1,177,763.25

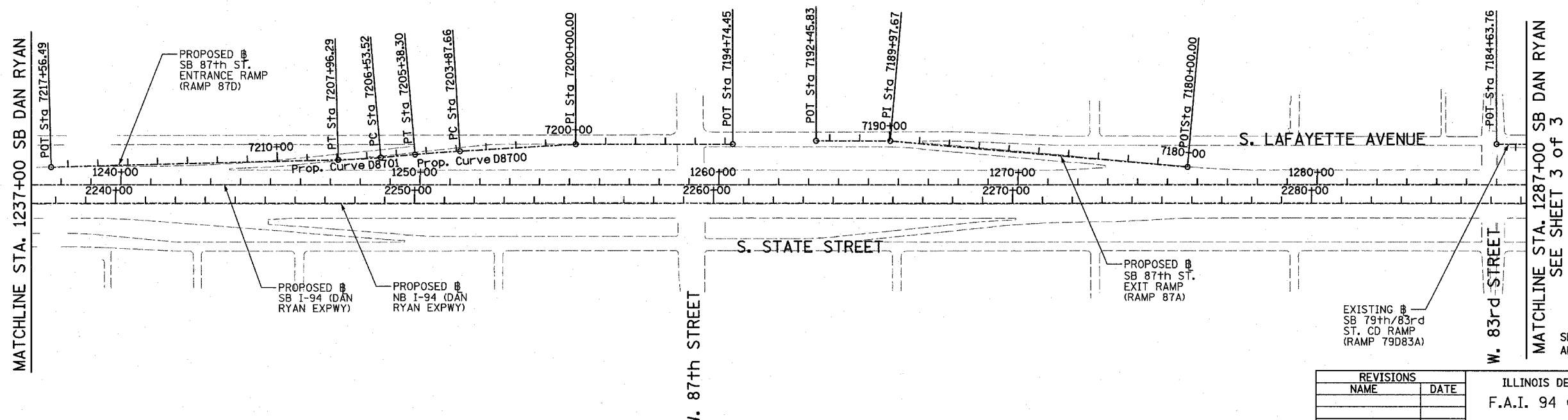
SB I-57  
POT STA = 160+17.58  
N = 1,842,327.55  
E = 1,177,726.54

NB I-57  
POT STA = 258+32.25  
N = 1,841,668.33  
E = 1,177,866.82

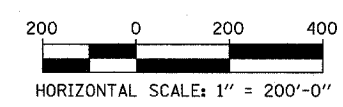
NB I-94 (DAN RYAN EXPWY)  
POT STA = 2203+00.00  
N = 1,841,667.31  
E = 1,177,830.83

SB I-94 (BISHOP FORD EXPWY)  
POT STA = 1173+10.28  
N = 1,841,304.06  
E = 1,177,704.74

NB I-94 (BISHOP FORD EXPWY)  
POT STA = 2045+99.73  
N = 1,841,389.65  
E = 1,177,874.80



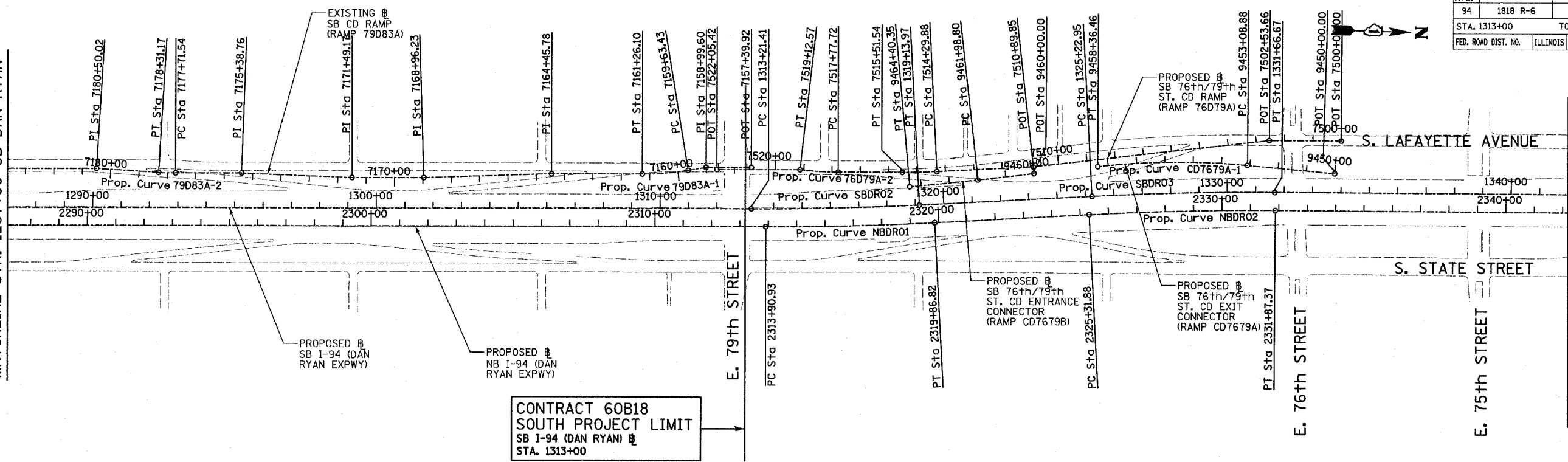
TYLIN INTERNATIONAL



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
ALIGNMENT  
SHEET 2 OF 4  
SCALE: 1"=200'  
DATE: MARCH 1, 2006  
DRAWN BY: JPM  
CHECKED BY: DAK

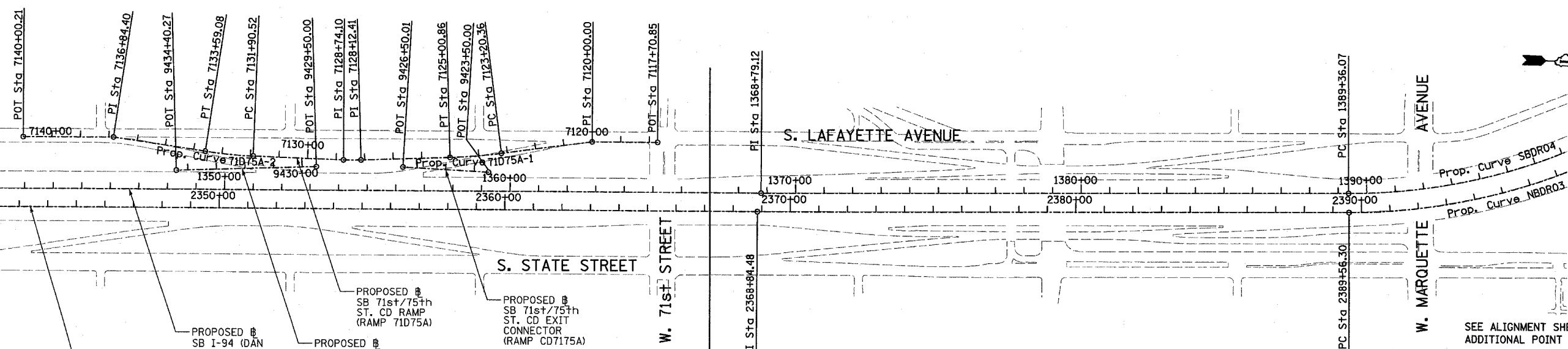
SEE SHEET 2 OF 3  
MATCHLINE STA. 1287+00 SB DAN RYAN



**CONTRACT 60B18**  
**SOUTH PROJECT LIMIT**  
SB I-94 (DAN RYAN) @  
STA. 1313+00

<b>Prop. Curve NBDRO1</b> P.I. STA = 2316+88.94 N = 1,853,051.58 E = 1,177,504.84 $\Delta = 2^\circ 58' 38.60''$ (LT) $D = 0^\circ 29' 58.77''$ R = 11,467.00 L = 595.89 E = 3.87 T = 298.01 P.C. STA = 2313+90.93 N = 1,852,753.69 E = 1,177,513.37 P.T. STA = 2319+86.82 N = 1,853,348.62 E = 1,177,480.85	<b>Prop. Curve SBDR02</b> P.I. STA = 2328+59.71 N = 1,854,218.69 E = 1,177,410.58 $\Delta = 3^\circ 16' 48.34''$ (RT) $D = 0^\circ 30' 01.44''$ R = 11,450.00 L = 655.49 E = 4.69 T = 327.84 P.C. STA = 2325+31.88 N = 1,853,891.91 E = 1,177,436.97 P.T. STA = 2331+87.37 N = 1,854,546.43 E = 1,177,402.92	<b>Prop. Curve NBDRO3</b> P.I. STA = 2398+13.58 N = 1,861,170.38 E = 1,177,231.19 $\Delta = 44^\circ 36' 17.12''$ (LT) $D = 2^\circ 44' 29.13''$ R = 2,090.00 L = 1,627.06 E = 168.99 T = 857.27 P.C. STA = 2389+56.30 N = 1,860,313.48 E = 1,177,256.19 P.T. STA = 2405+83.37 N = 1,861,762.92 E = 1,176,611.65	<b>Prop. Curve SBDR02</b> P.I. STA = 1316+17.76 N = 1,852,997.25 E = 1,177,445.02 $\Delta = 2^\circ 58' 38.60''$ (LT) $D = 0^\circ 30' 08.86''$ R = 11,403.00 L = 592.56 E = 3.85 T = 296.35 P.C. STA = 1313+21.41 N = 1,852,701.02 E = 1,177,453.50 P.T. STA = 1319+13.97 N = 1,853,292.63 E = 1,177,421.16	<b>Prop. Curve SBDR03</b> P.I. STA = 1328+44.90 N = 1,854,220.54 E = 1,177,346.22 $\Delta = 3^\circ 16' 11.04''$ (RT) $D = 0^\circ 30' 28.59''$ R = 11,280.00 L = 643.72 E = 4.59 T = 321.95 P.C. STA = 1325+22.95 N = 1,853,899.63 E = 1,177,372.14 P.T. STA = 1331+66.67 N = 1,854,542.40 E = 1,177,338.64	<b>Prop. Curve SBDR04</b> P.I. STA = 1397+75.78 N = 1,861,149.16 E = 1,177,163.97 $\Delta = 44^\circ 32' 58.56''$ (LT) $D = 2^\circ 47' 41.70''$ R = 2,050.00 L = 1,593.95 E = 165.31 T = 839.71 P.C. STA = 1389+36.07 N = 1,860,309.83 E = 1,177,189.29 P.T. STA = 1405+30.02 N = 1,861,729.54 E = 1,176,557.11	<b>NB I-94 (DAN RYAN EXPRESSWAY)</b> P.I. STA = 2368+84.48 N = 1,858,242.53 E = 1,177,316.62  <b>SB I-94 (DAN RYAN EXPRESSWAY)</b> P.I. STA = 1368+79.12 N = 1,858,253.81 E = 1,177,251.32
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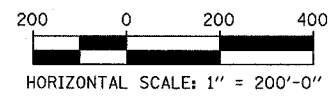
MATCHLINE STA. 1342+00 SB DAN RYAN



**CONTRACT 60B18**  
**NORTH PROJECT LIMIT**  
SB I-94 (DAN RYAN) @  
STA. 1367+00

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**ALIGNMENT**  
SHEET 3 OF 4  
SCALE: 1"=200'  
DATE: MARCH 1, 2006  
DRAWN BY: JPM  
CHECKED BY: DAK





SB HALSTED ST.  
EXIT RAMP  
(RAMP HLSTDA)  
POT STA = 7440+00.00  
N = 1,839,490.94  
E = 1,174,333.70  
P.I. STA = 7449+69.78  
N = 1,839,551.73  
E = 1,173,365.83  
POT STA = 7452+53.49  
N = 1,839,544.76  
E = 1,173,082.20

SB WENTWORTH AVE.  
EXIT RAMP  
(RAMP 99A)  
POT STA = 7430+55.42  
N = 1,840,141.17  
E = 1,176,961.02  
POT STA = 7431+32.26  
N = 1,840,094.48  
E = 1,176,899.99  
POT STA = 7434+00.00  
N = 1,839,955.89  
E = 1,176,670.92

SB 95th ST. EXIT RAMP  
(RAMP 95A)  
POT STA = 7400+00.00  
N = 1,844,139.73  
E = 1,177,638.63  
P.I. STA = 7413+83.54  
N = 1,842,756.77  
E = 1,177,598.60  
POT STA = 7417+45.33  
N = 1,842,395.13  
E = 1,177,608.95

SB 87th ST.  
ENTRANCE RAMP  
(RAMP 87D)  
POT STA = 7194+74.45  
N = 1,847,424.06  
E = 1,177,468.58  
P.I. STA = 7200+00.00  
N = 1,846,898.74  
E = 1,177,484.09  
POT STA = 7217+56.49  
N = 1,845,147.17  
E = 1,177,609.78

SB 87th ST. EXIT RAMP  
(RAMP 87A)  
POT STA = 7180+00.00  
N = 1,848,946.30  
E = 1,177,501.00  
P.I. STA = 7189+97.67  
N = 1,847,950.35  
E = 1,177,442.53  
POT STA = 7192+45.83  
N = 1,847,702.29  
E = 1,177,449.63

Prop. Curve 99A-1  
P.I. STA = 7421+32.05  
N = 1,840,832.05  
E = 1,177,544.75  
 $\Delta = 10^\circ 49' 08.53''$  (RT)  
D =  $2^\circ 17' 30.59''$   
R = 2,500.00  
L = 472.07  
E = 11.18  
T = 236.74  
P.C. STA = 7418+95.31  
N = 1,841,058.12  
E = 1,177,615.03  
P.T. STA = 7423+67.38  
N = 1,840,623.20  
E = 1,177,433.29

Prop. Curve 99A-2  
P.I. STA = 7424+78.02  
N = 1,840,525.59  
E = 1,177,381.20  
 $\Delta = 7^\circ 26' 49.68''$  (RT)  
D =  $3^\circ 22' 13.22''$   
R = 1,700.00  
L = 220.96  
E = 3.60  
T = 110.64  
P.C. STA = 7423+67.38  
N = 1,840,623.20  
E = 1,177,433.29  
P.T. STA = 7425+88.34  
N = 1,840,435.56  
E = 1,177,316.90

Prop. Curve 99A-3  
P.I. STA = 7428+26.42  
N = 1,840,241.82  
E = 1,177,178.53  
 $\Delta = 29^\circ 38' 02.27''$  (RT)  
D =  $6^\circ 21' 58.31''$   
R = 900.00  
L = 465.49  
E = 30.96  
T = 238.08  
P.C. STA = 7425+88.34  
N = 1,840,435.56  
E = 1,177,316.90  
P.T. STA = 7430+53.83  
N = 1,840,141.84  
E = 1,176,962.46

Prop. Curve D8700  
P.I. STA = 7204+62.99  
N = 1,846,437.54  
E = 1,177,524.69  
 $\Delta = 1^\circ 30' 03.72''$  (LT)  
D =  $0^\circ 59' 47.21''$   
R = 5,750.00  
L = 150.64  
E = 0.49  
T = 75.32  
P.C. STA = 7203+87.66  
N = 1,846,512.58  
E = 1,177,518.09  
P.T. STA = 7205+38.30  
N = 1,846,362.71  
E = 1,177,533.26

Prop. Curve D8701  
P.I. STA = 7207+24.93  
N = 1,846,177.29  
E = 1,177,554.49  
 $\Delta = 3^\circ 27' 36.43''$  (RT)  
D =  $2^\circ 25' 25.25''$   
R = 2,364.00  
L = 142.76  
E = 1.08  
T = 71.40  
P.C. STA = 7206+53.52  
N = 1,846,248.23  
E = 1,177,546.37  
P.T. STA = 7207+96.29  
N = 1,846,105.99  
E = 1,177,558.32

Prop. Curve 79D83A-1  
P.I. STA = 7160+44.94  
N = 1,852,394.55  
E = 1,177,340.32  
 $\Delta = 9^\circ 19' 12.90''$  (RT)  
D =  $5^\circ 43' 46.48''$   
R = 1,000.00  
L = 162.67  
E = 3.32  
T = 81.51  
P.C. STA = 7159+63.43  
N = 1,852,474.58  
E = 1,177,324.82  
P.T. STA = 7161+26.10  
N = 1,852,313.07  
E = 1,177,342.65

SB 76th/79th ST.  
CD RAMP  
(RAMP 76D79A)  
POT STA = 7500+00.00  
N = 1,854,771.42  
E = 1,177,153.21  
P.I. STA = 7502+53.66  
N = 1,854,517.84  
E = 1,177,159.17  
P.I. STA = 7510+89.85  
N = 1,853,689.73  
E = 1,177,275.17  
POT STA = 7522+05.42  
N = 1,852,575.71  
E = 1,177,319.03

SB 71st/75th ST.  
CD RAMP  
(RAMP 71D75A)  
POT STA = 7117+70.85  
N = 1,857,886.40  
E = 1,177,086.26  
P.I. STA = 7120+00.00  
N = 1,857,657.31  
E = 1,177,091.65  
P.I. STA = 7128+12.41  
N = 1,856,850.29  
E = 1,177,177.64  
P.I. STA = 7128+74.10  
N = 1,856,788.62  
E = 1,177,179.10  
P.I. STA = 7136+84.40  
N = 1,855,981.27  
E = 1,177,124.75  
POT STA = 7140+00.21  
N = 1,855,665.56  
E = 1,177,132.18

Prop. Curve 79D83A-2  
P.I. STA = 7178+01.36  
N = 1,850,638.94  
E = 1,177,390.59  
 $\Delta = 3^\circ 24' 57.69''$  (RT)  
D =  $5^\circ 43' 46.48''$   
R = 1,000.00  
L = 59.62  
E = 0.44  
T = 29.82  
P.C. STA = 7177+71.54  
N = 1,850,668.74  
E = 1,177,389.73  
P.T. STA = 7178+31.17  
N = 1,850,609.13  
E = 1,177,389.66

Prop. Curve 76D79A-1  
P.I. STA = 7514+90.72  
N = 1,853,290.06  
E = 1,177,306.25  
 $\Delta = 2^\circ 48' 22.72''$  (RT)  
D =  $2^\circ 18' 23.74''$   
R = 2,484.00  
L = 121.66  
E = 0.75  
T = 60.84  
P.C. STA = 7514+29.88  
N = 1,853,350.72  
E = 1,177,301.54  
P.T. STA = 7515+51.54  
N = 1,853,229.24  
E = 1,177,307.99

SB C-D RAMP  
(RAMP 79D83A)  
POT STA = 7157+39.92  
N = 1,852,696.86  
E = 1,177,308.11  
P.I. STA = 7158+99.60  
N = 1,852,537.24  
E = 1,177,312.68  
P.I. STA = 7164+45.78  
N = 1,851,993.52  
E = 1,177,351.80  
P.I. STA = 7168+96.23  
N = 1,851,543.85  
E = 1,177,378.34  
P.I. STA = 7171+49.17  
N = 1,851,291.02  
E = 1,177,385.58  
P.I. STA = 7175+38.76  
N = 1,850,901.43  
E = 1,177,383.07  
P.I. STA = 7180+50.02  
N = 1,850,390.38  
E = 1,177,382.88  
POT STA = 7184+63.76  
N = 1,849,976.81  
E = 1,177,394.72

Prop. Curve 76D79A-2  
P.I. STA = 7518+45.22  
N = 1,852,935.69  
E = 1,177,316.40  
 $\Delta = 6^\circ 31' 33.29''$  (RT)  
D =  $4^\circ 50' 21.01''$   
R = 1,184.00  
L = 134.86  
E = 1.92  
T = 67.50  
P.C. STA = 7517+77.72  
N = 1,853,003.16  
E = 1,177,314.47  
P.T. STA = 7519+12.57  
N = 1,852,868.44  
E = 1,177,310.65

SB 76th/79th ST.  
CD ENTRANCE CONNECTOR  
(CD7679A)  
POT STA = 9450+00.00  
N = 1,854,751.70  
E = 1,177,267.37  
Prop. Curve CD7679A-1  
P.I. STA = 9455+73.58  
N = 1,854,179.28  
E = 1,177,230.83  
 $\Delta = 11^\circ 37' 34.33''$  (LT)  
D =  $2^\circ 12' 13.26''$   
R = 2,600.00  
L = 527.58  
E = 13.44  
T = 264.70  
P.C. STA = 9453+08.88  
N = 1,854,443.44  
E = 1,177,247.69  
P.T. STA = 9458+36.46  
N = 1,853,917.14  
E = 1,177,267.55

SB 76th/79th ST.  
CD ENTRANCE CONNECTOR  
(CD7679B)  
POT STA = 9460+00.00  
N = 1,853,693.06  
E = 1,177,298.94  
Prop. Curve CD7679B-1  
P.I. STA = 9463+19.58  
N = 1,853,376.57  
E = 1,177,343.27  
 $\Delta = 1^\circ 13' 13.54''$  (RT)  
D =  $0^\circ 30' 18.91''$   
R = 11,340.00  
L = 241.55  
E = 0.64  
T = 120.78  
P.C. STA = 9461+98.80  
N = 1,853,496.18  
E = 1,177,326.52  
P.T. STA = 9464+40.35  
N = 1,853,256.63  
E = 1,177,357.48

Prop. Curve 71D75A-1  
P.I. STA = 7124+10.68  
N = 1,857,251.52  
E = 1,177,154.81  
 $\Delta = 5^\circ 35' 23.99''$  (RT)  
D =  $3^\circ 05' 49.45''$   
R = 1,850.00  
L = 180.49  
E = 2.20  
T = 90.32  
P.C. STA = 7123+20.36  
N = 1,857,340.76  
E = 1,177,140.92  
P.T. STA = 7125+00.86  
N = 1,857,161.34  
E = 1,177,159.94

Prop. Curve 71D75A-2  
P.I. STA = 7132+74.89  
N = 1,856,387.85  
E = 1,177,175.17  
 $\Delta = 6^\circ 30' 28.18''$  (RT)  
D =  $3^\circ 51' 39.25''$   
R = 1,484.00  
L = 168.56  
E = 2.40  
T = 84.37  
P.C. STA = 7131+90.52  
N = 1,856,472.22  
E = 1,177,176.00  
P.T. STA = 7133+59.08  
N = 1,856,304.13  
E = 1,177,164.79

SB 71st/75th ST.  
CD ENTRANCE CONNECTOR  
(RAMP CD7175A)  
POT STA = 9423+50.00  
N = 1,857,297.2866  
E = 1,177,207.4713  
POT STA = 9426+50.01  
N = 1,856,997.4225  
E = 1,177,198.1891  
CD ENTRANCE CONNECTOR  
(RAMP CD7175B)  
POT STA = 9429+50.00  
N = 1,856,694.2083  
E = 1,177,205.3235  
POT STA = 9434+40.27  
N = 1,856,204.7281  
E = 1,177,233.1786

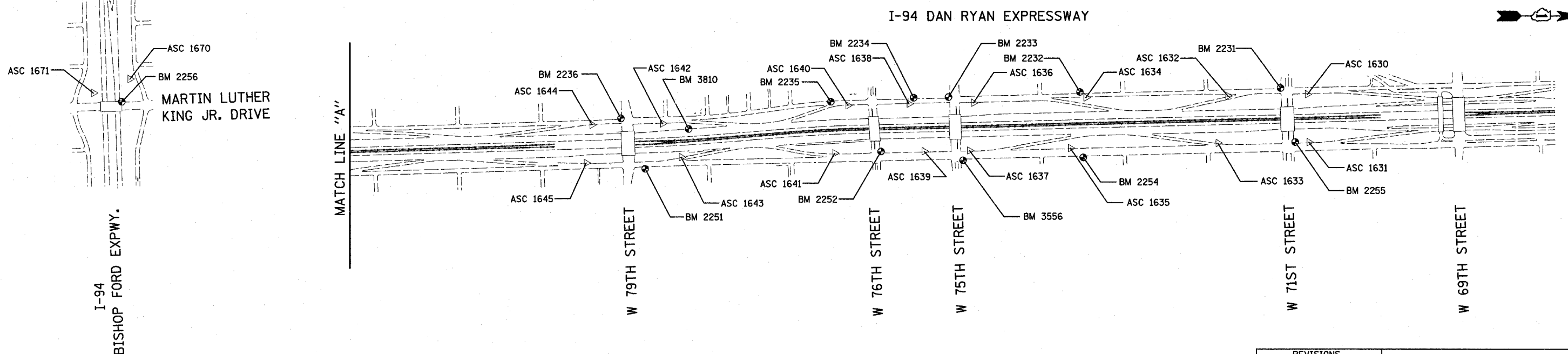
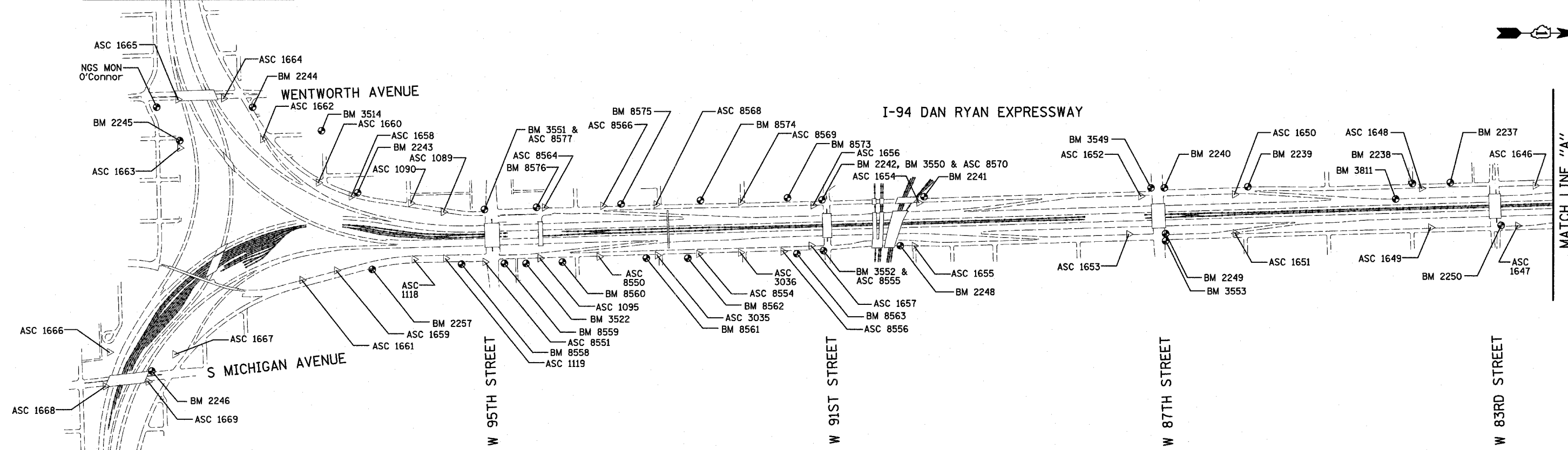
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

ALIGNMENT  
SHEET 4 OF 4

SCALE: NO SCALE  
DATE: MARCH 1, 2006  
DRAWN BY: JPA  
CHECKED BY: JPM

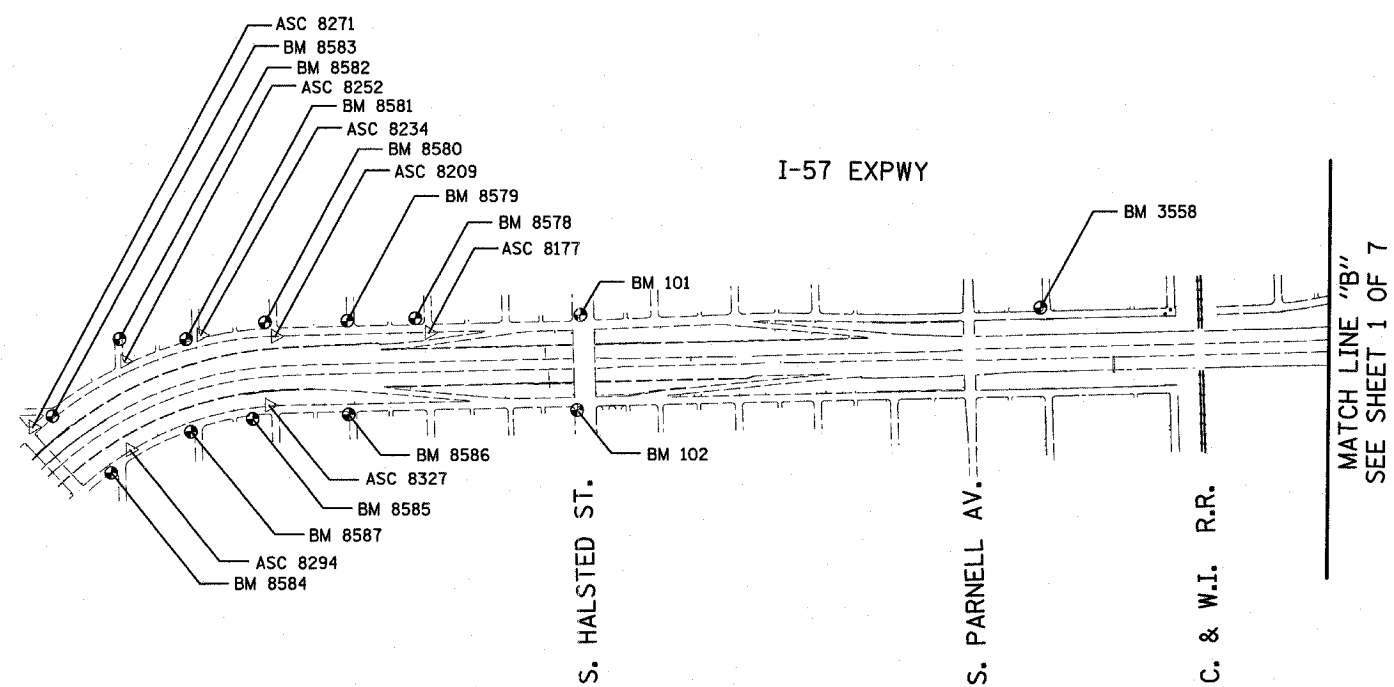
I-57  
EXPWY  
SEE SHEET 2 OF 7  
MATCH LINE "B"



REVISIONS	
NAME	DATE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	28
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

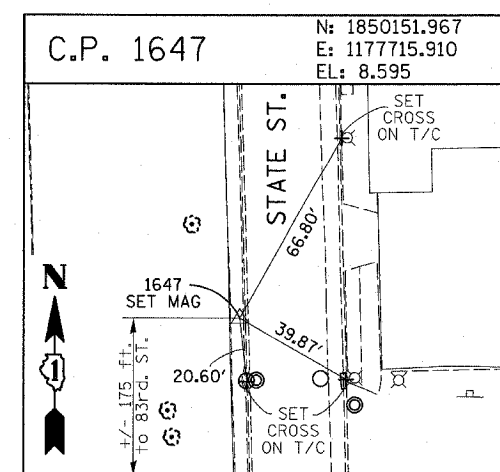
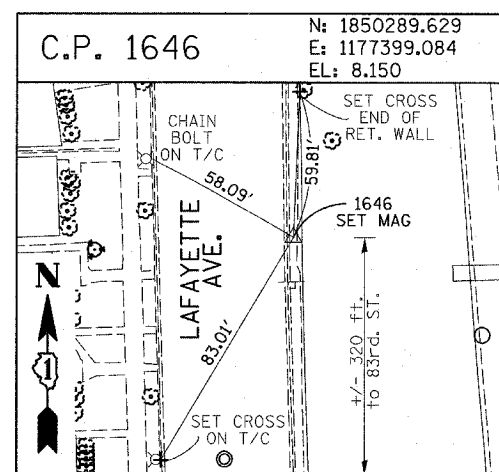
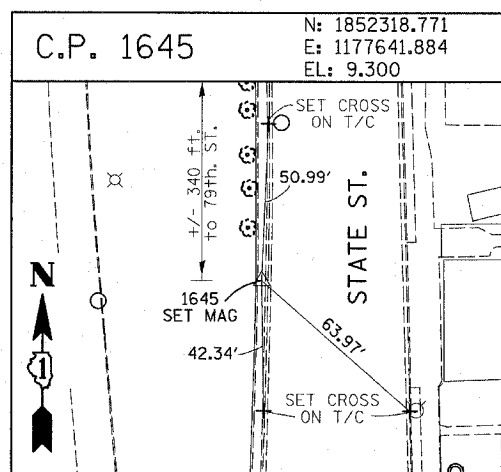
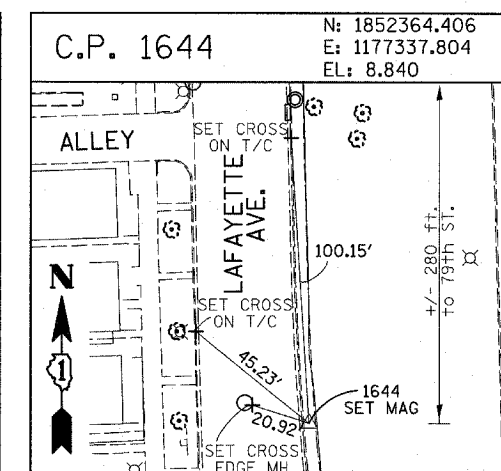
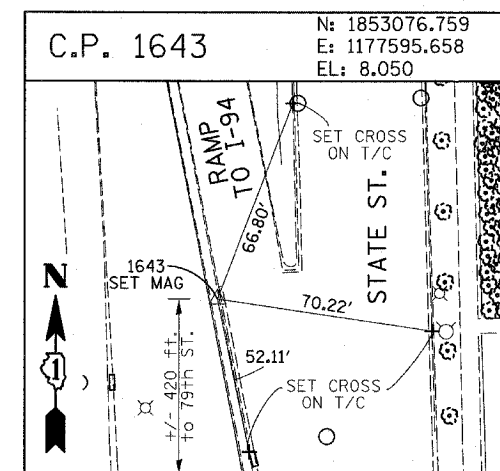
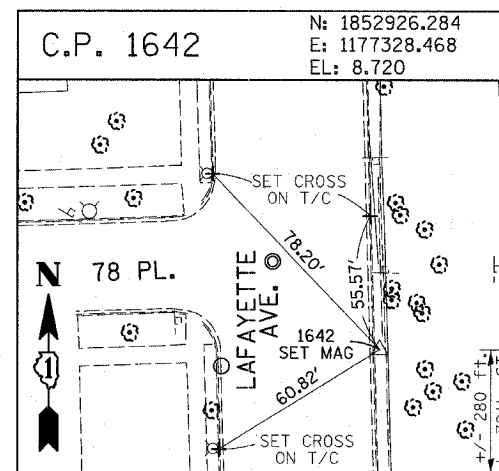
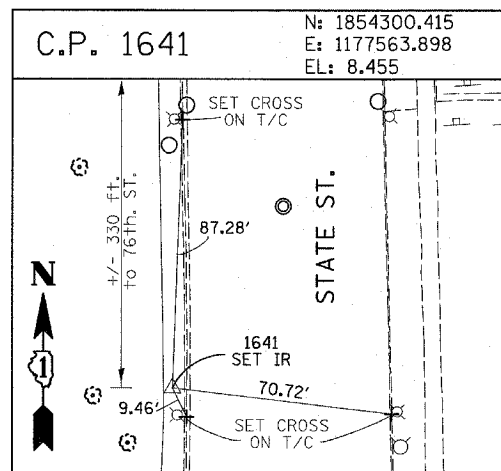
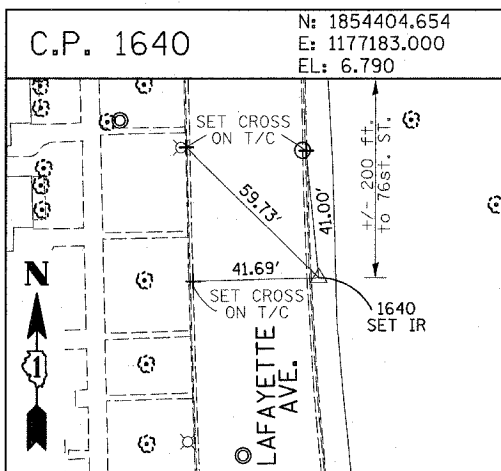
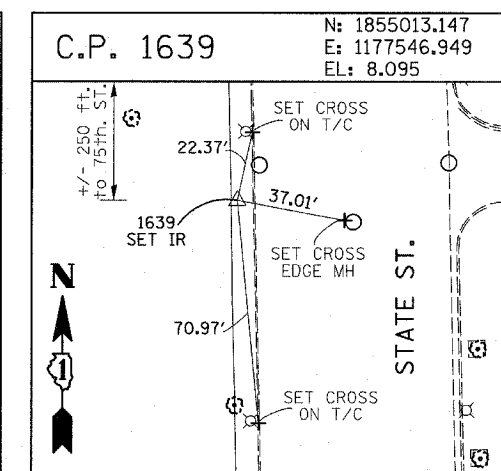
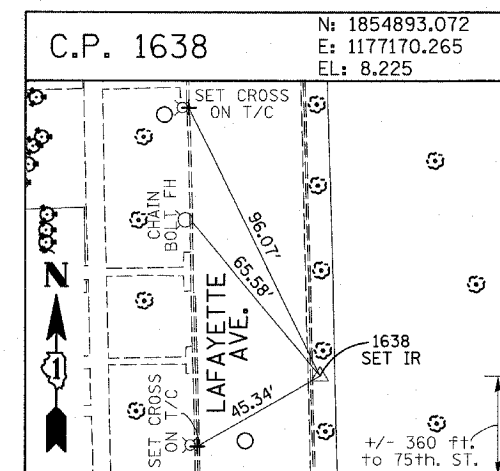
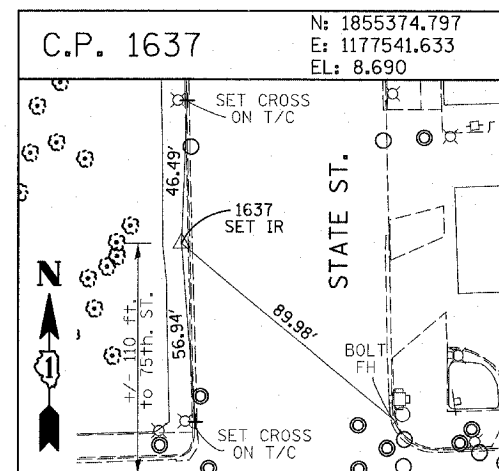
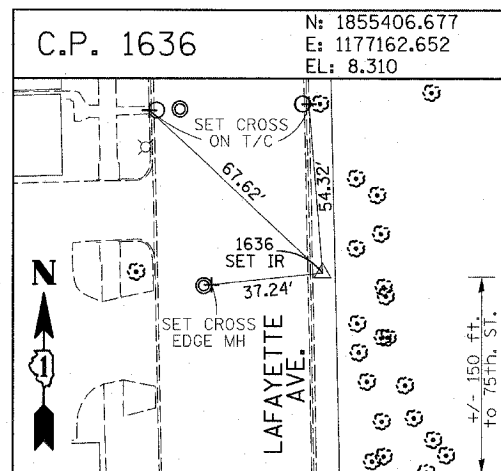
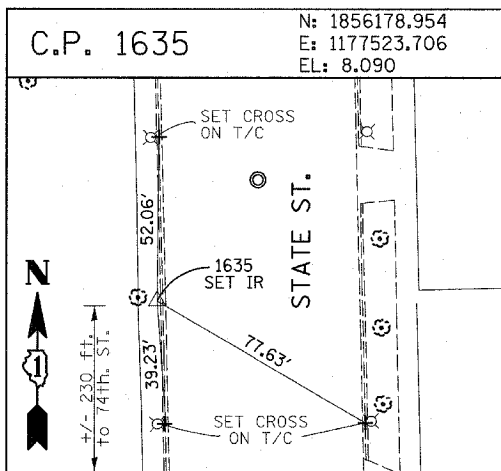
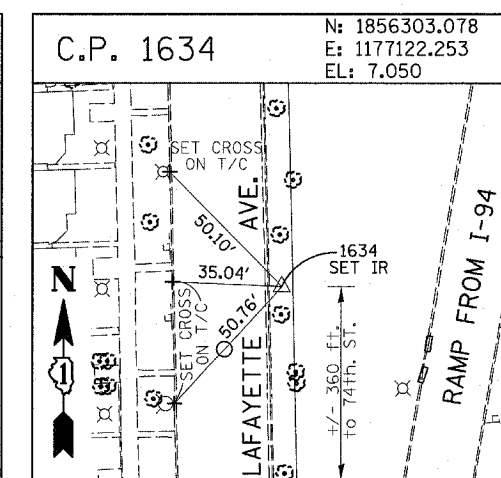
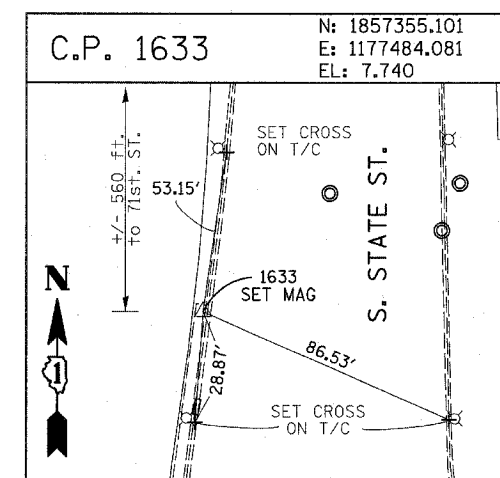
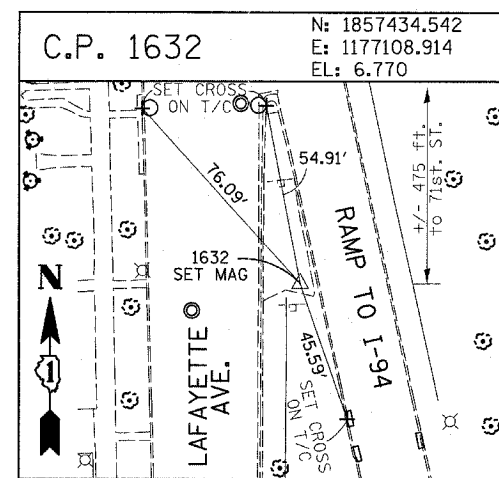
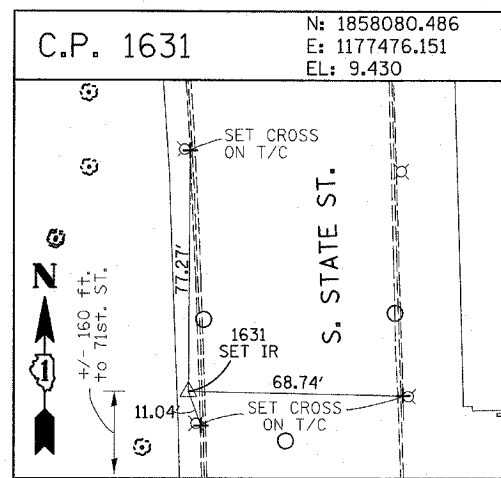
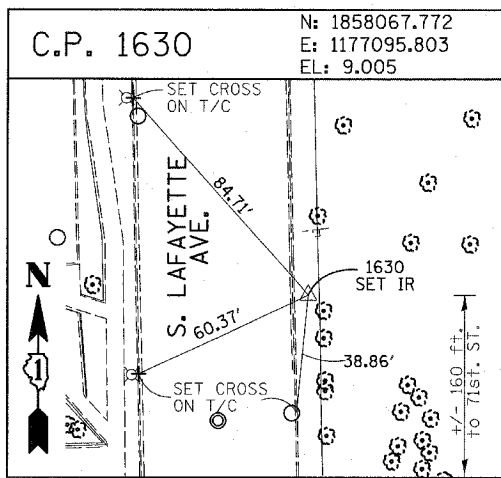
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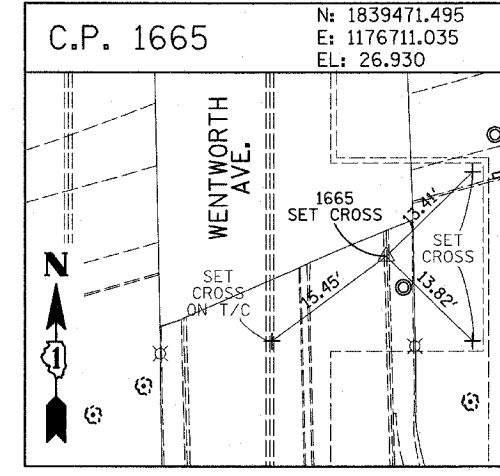
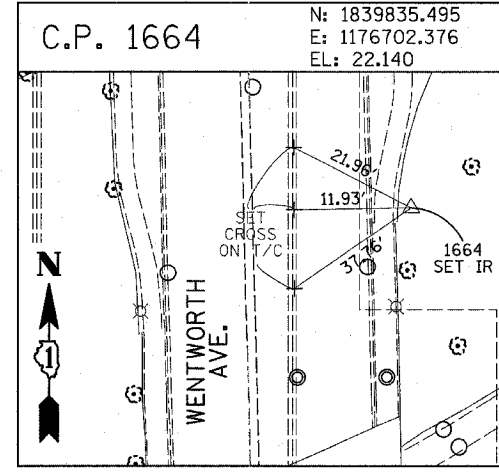
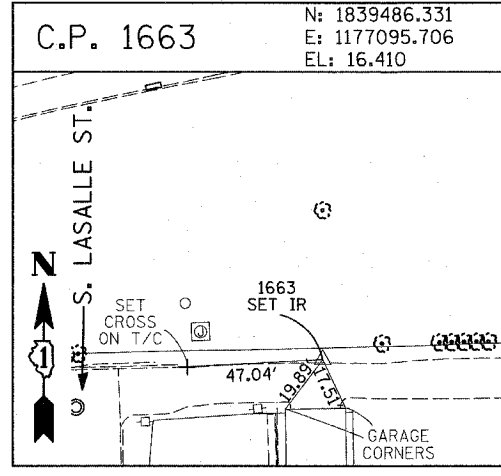
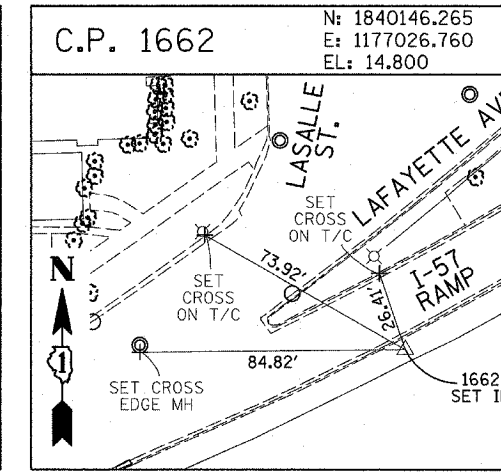
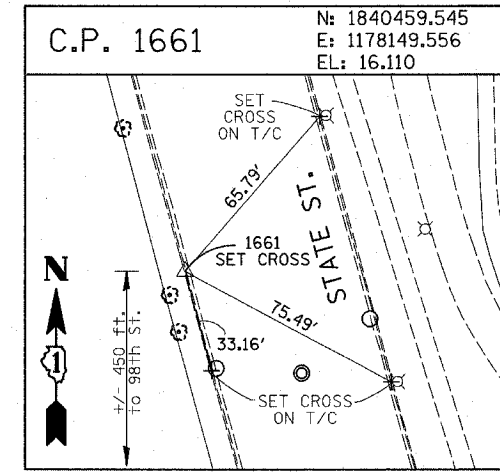
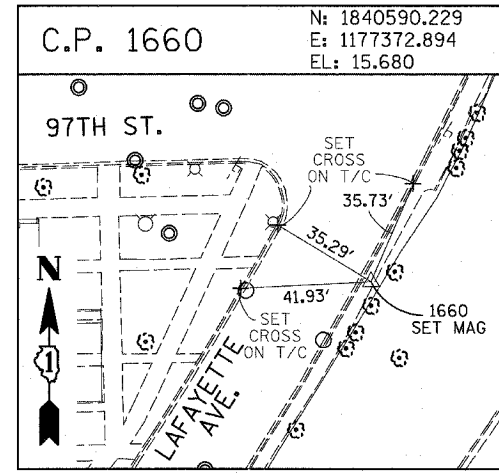
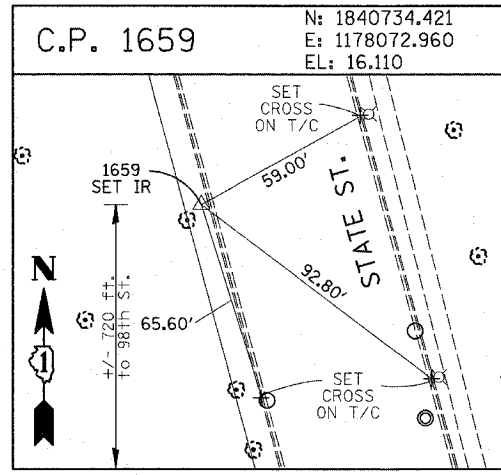
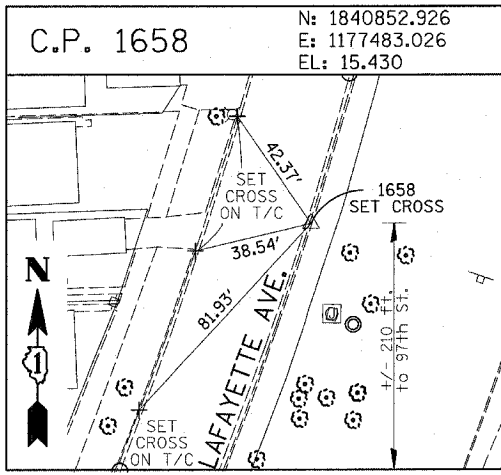
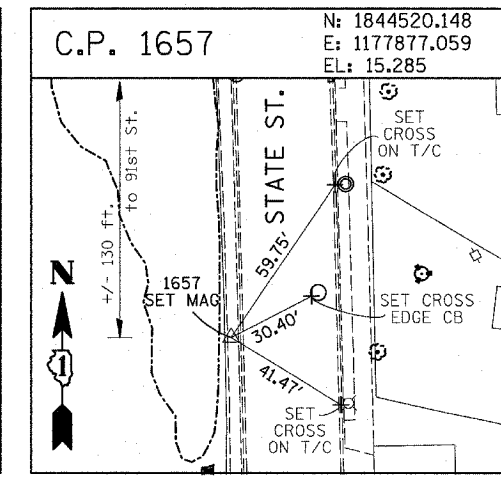
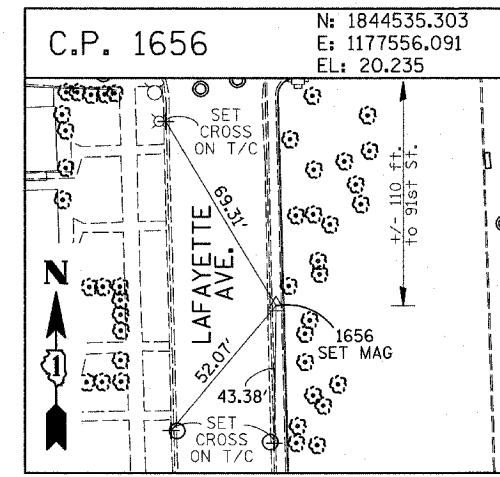
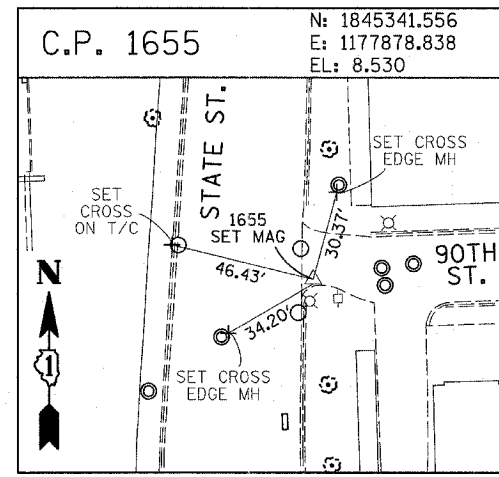
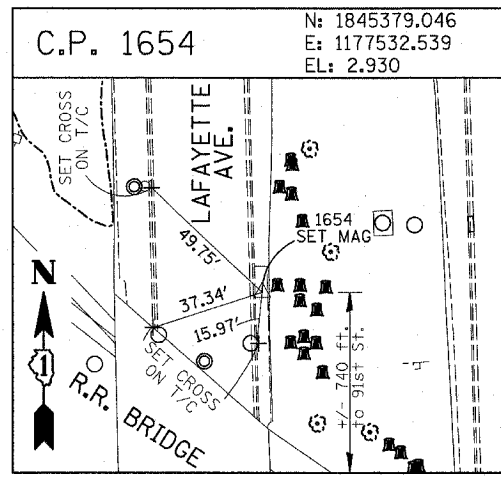
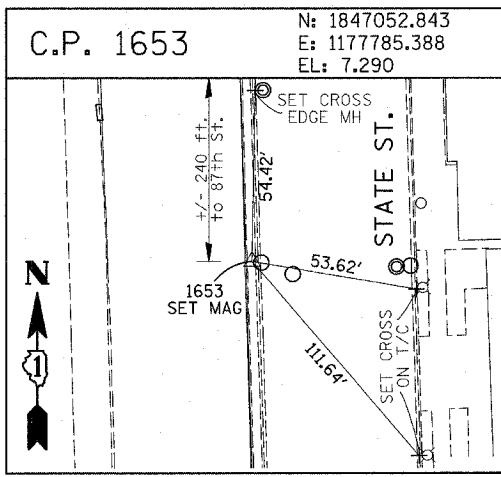
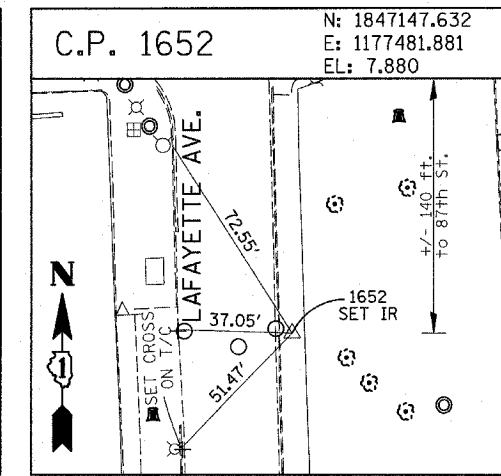
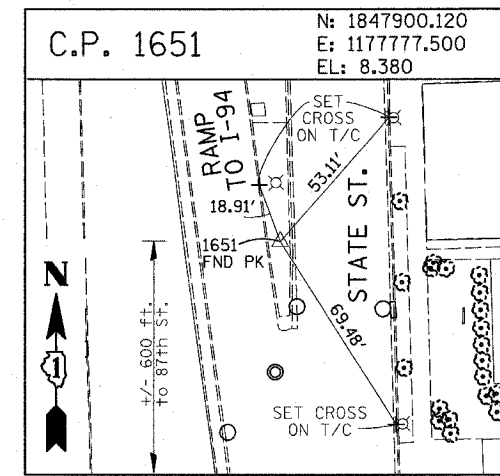
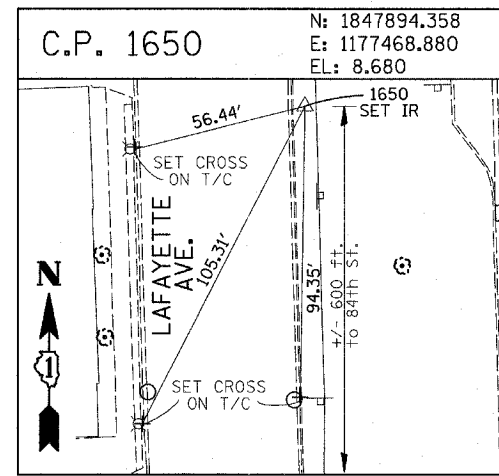
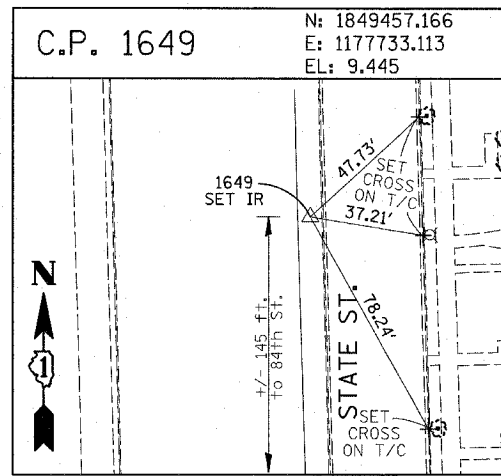
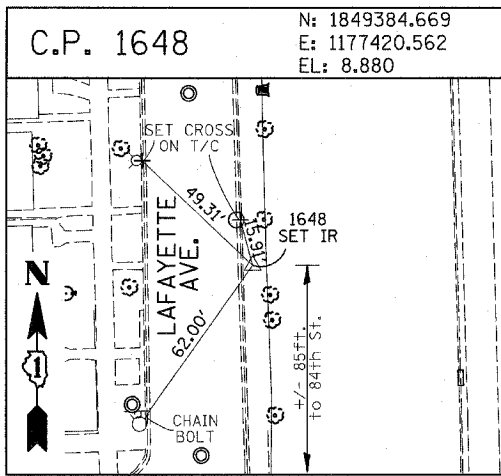
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 SURVEY TIES FOR CONTROL POINTS  
 SHEET 2 OF 7

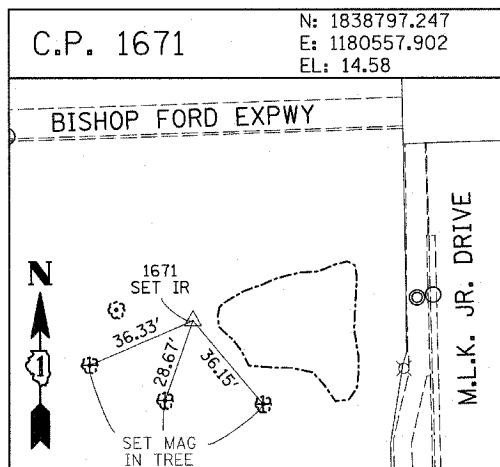
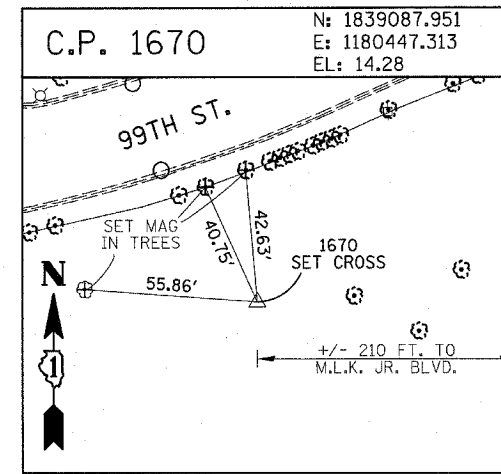
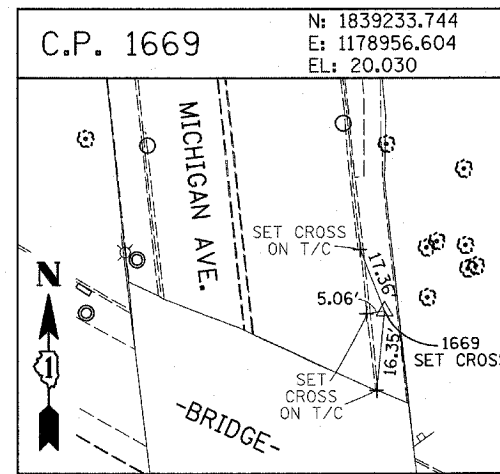
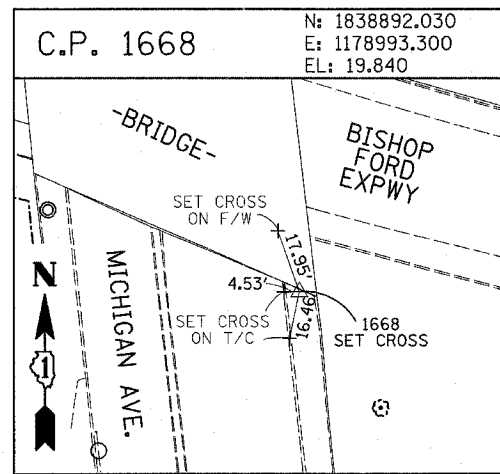
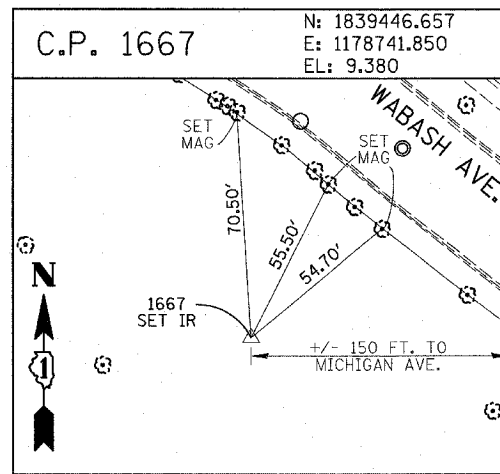
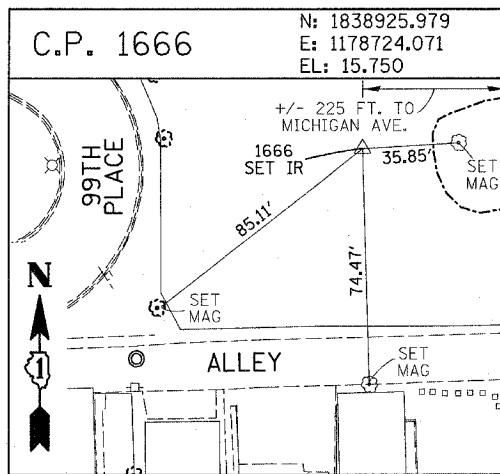
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NAME	DATE

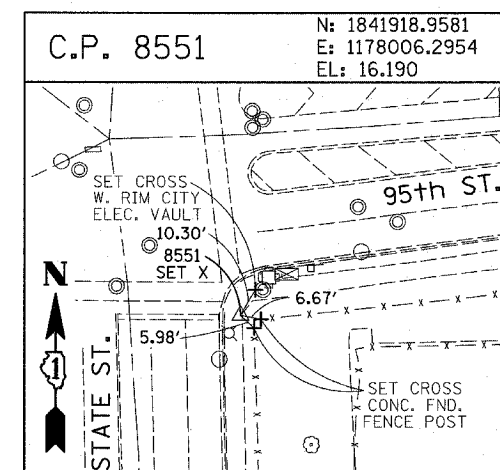
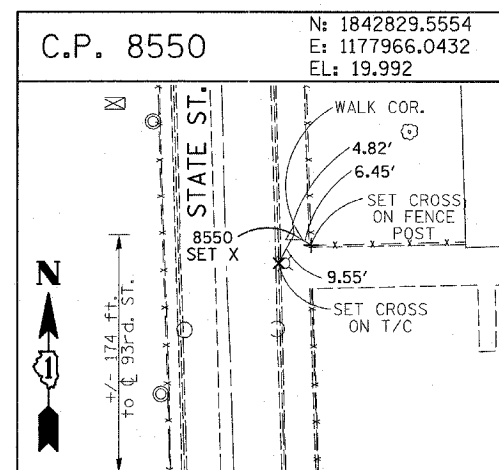
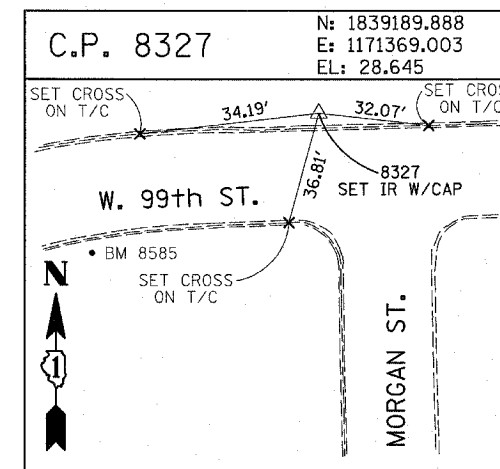
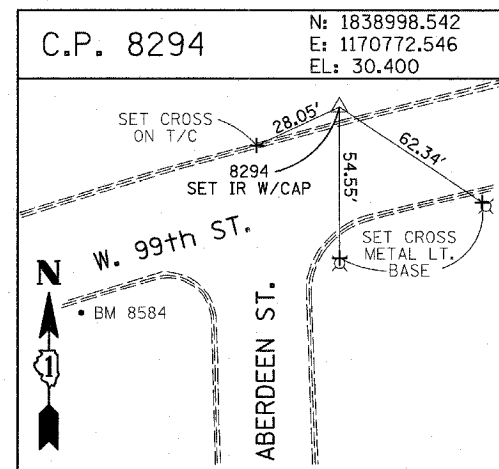
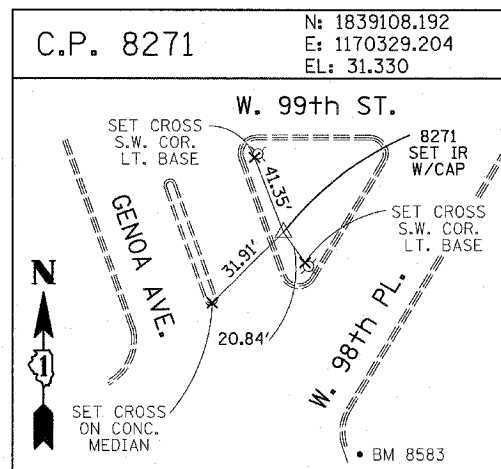
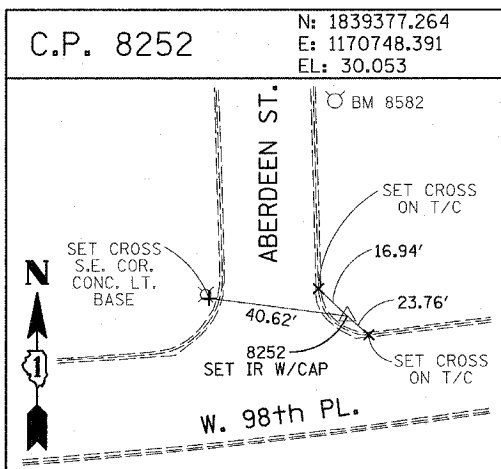
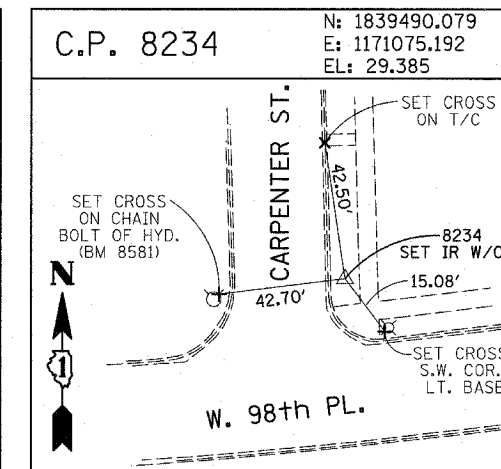
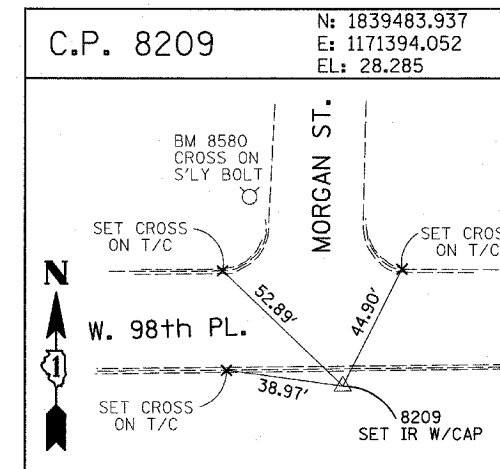
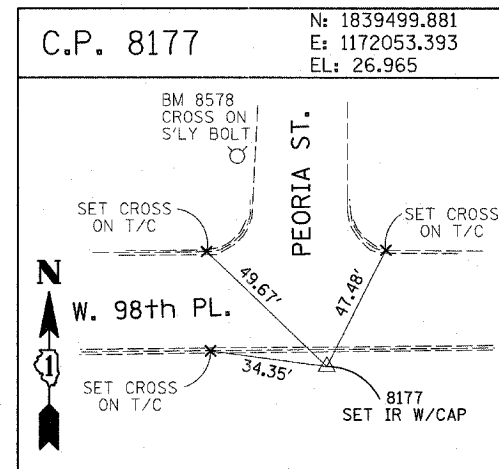
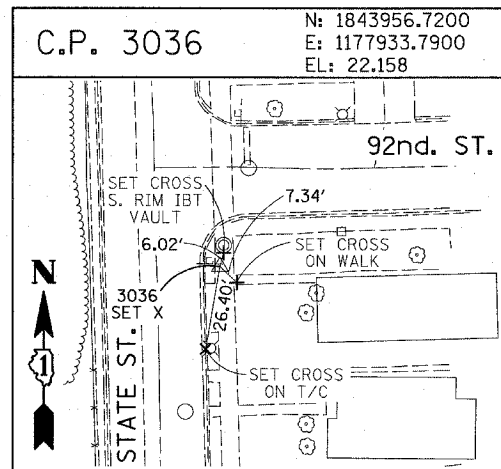
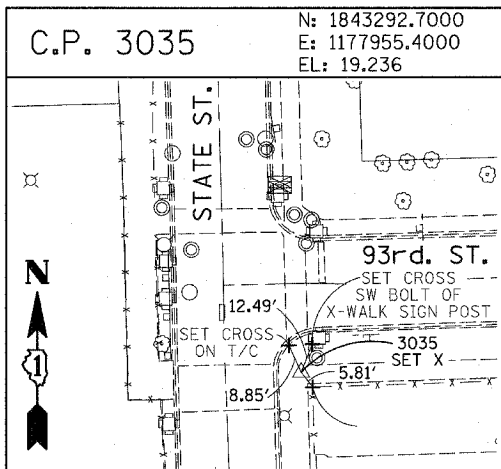
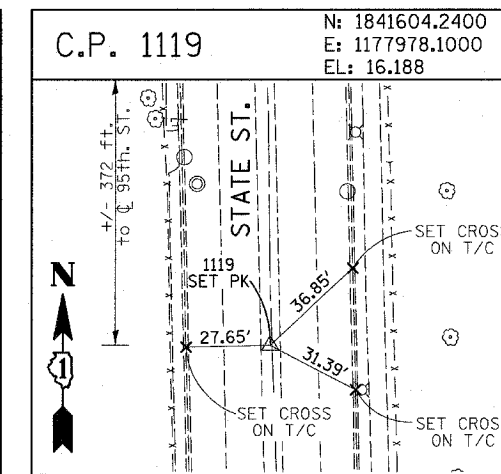
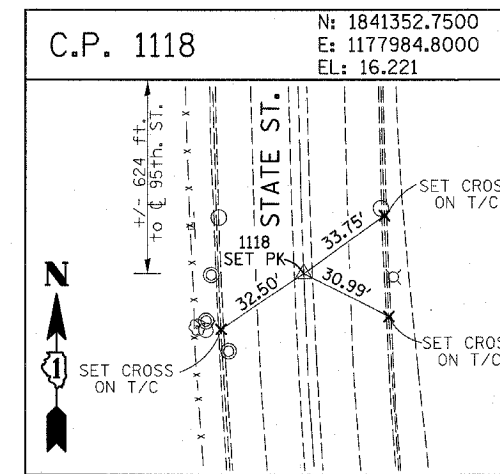
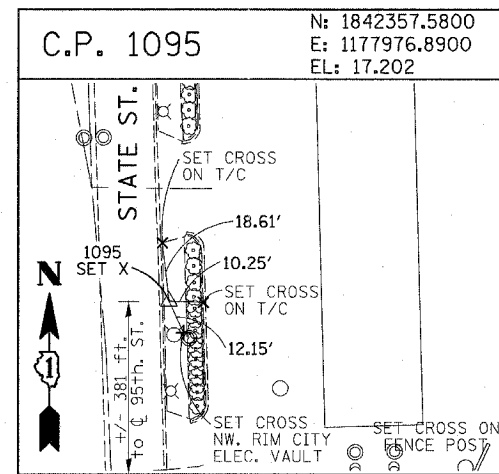
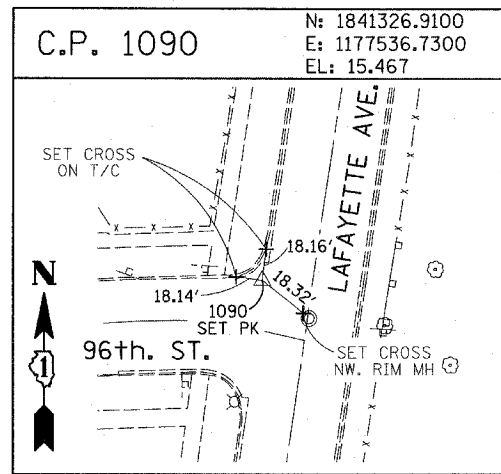
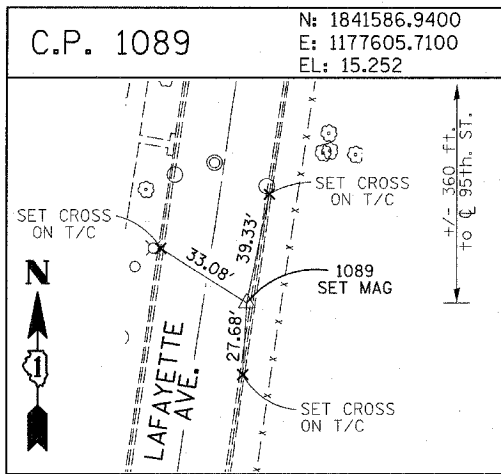


**PROJECT BENCHMARKS**

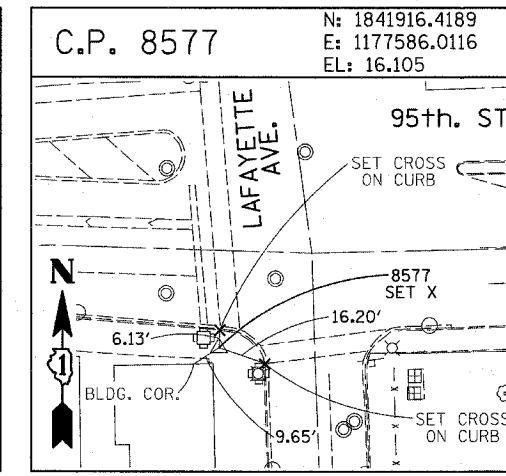
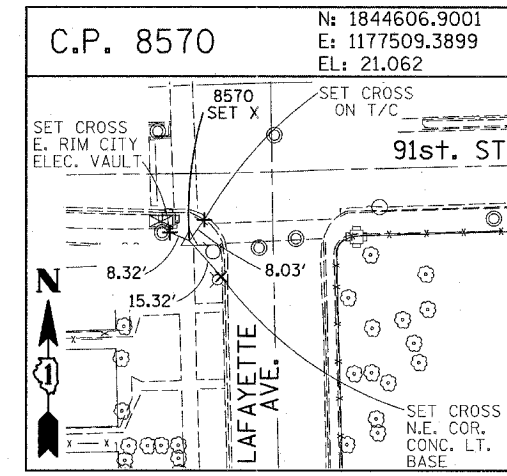
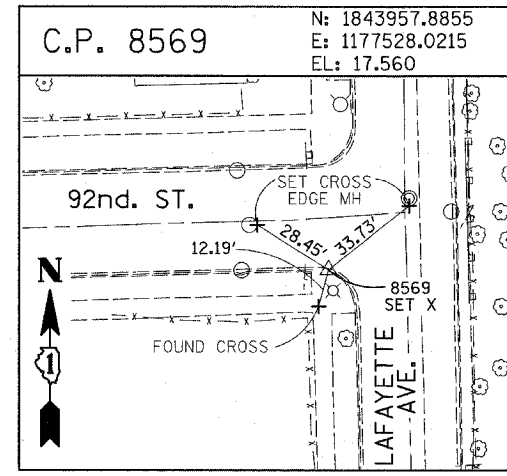
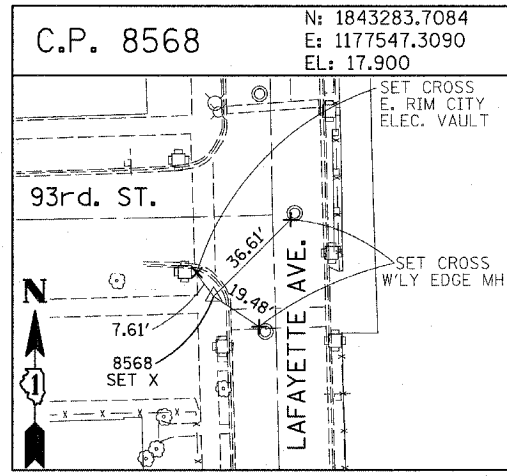
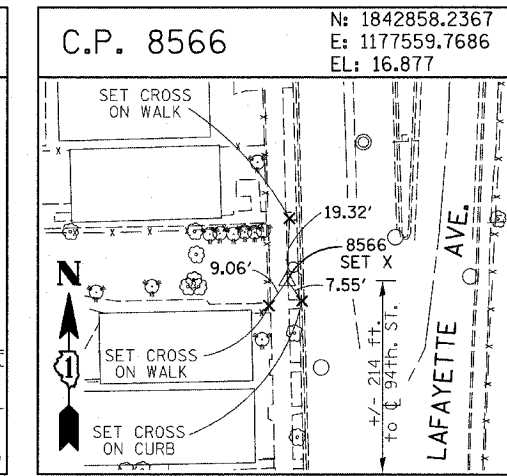
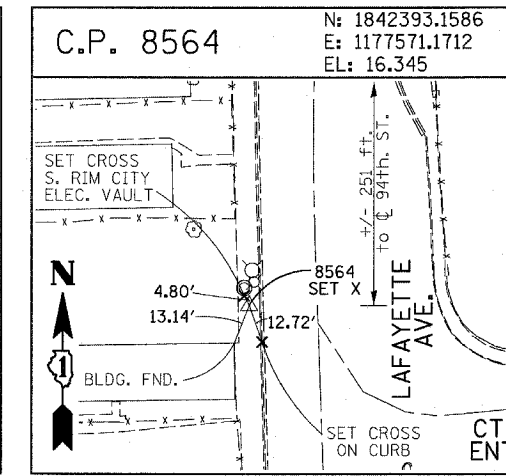
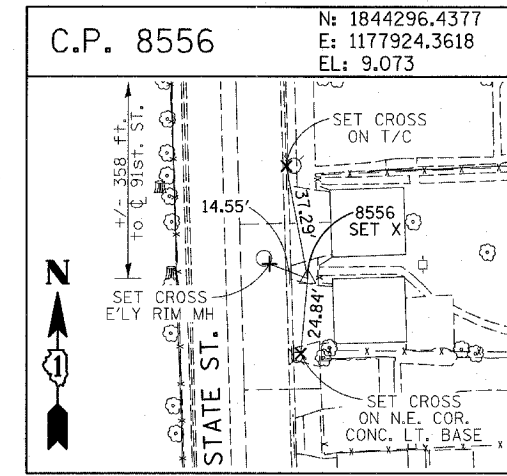
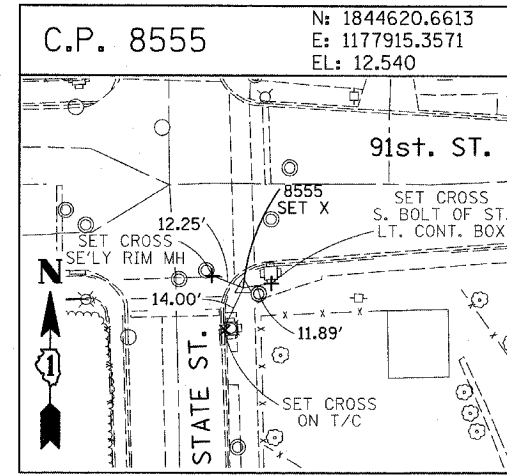
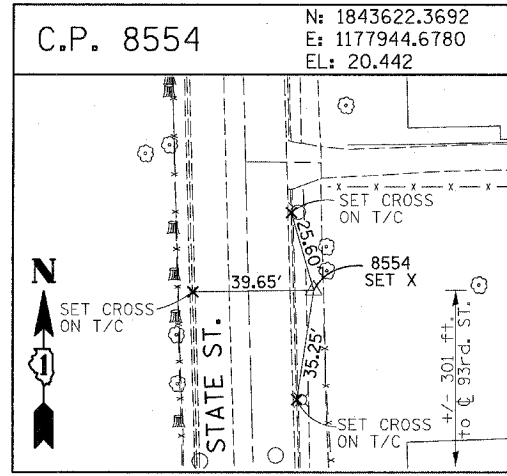
- BM 2231 ELEVATION= 10.24 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 71ST STREET AND LAFAYETTE AVENUE.
- BM 2232 ELEVATION= 8.46 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 73RD STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 330 FEET SOUTH OF 73RD STREET.
- BM 2233 ELEVATION= 9.90 FEET  
SET CROSS ON NORTHWEST BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 75TH STREET AND LAFAYETTE AVENUE.
- BM 2234 ELEVATION= 9.56 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 75TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 320 FEET SOUTH OF 75TH STREET.
- BM 2235 ELEVATION= 7.50 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 76TH STREET ON THE WEST SIDE OF LAFAYETTE. APPROXIMATELY 330 FEET SOUTH OF 76TH STREET.
- BM 2236 ELEVATION= 10.41 FEET  
SET CROSS ON WEST BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 79TH STREET AND LAFAYETTE AVENUE.
- BM 2237 ELEVATION= 9.20 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF 83RD STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 350 FEET NORTH OF 83RD STREET.
- BM 2238 ELEVATION= 10.04 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT OF THE NORTHWEST CORNER OF 4TH STREET AND LAFAYETTE AVENUE.
- BM 2239 ELEVATION= 9.59 FEET  
SET CROSS ON CHAIN BOLT OF SECOND FIRE HYDRANT NORTH OF 87TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 720 FEET NORTH OF 87TH STREET.
- BM 2240 ELEVATION= 9.49 FEET  
SET CROSS ON THE NORTHWEST BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 78TH STREET AND LAFAYETTE AVENUE.
- BM 2241 ELEVATION= 3.63 FEET  
SET SQUARE CUT ON TOP OF CURB NEXT TO LIGHT POLE ON THE NORTHWEST CORNER OF LAFAYETTE AVENUE AND RAILROAD BRIDGE. APPROXIMATELY 770 FEET NORTH OF 91ST STREET.
- BM 2242 ELEVATION= 22.82 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 91ST STREET AND LAFAYETTE AVENUE.
- BM 2243 ELEVATION= 15.68 FEET  
SET SQUARE CUT ON THE TOP OF CURB NEXT TO THIRD LIGHT POLE NORTH OF 97TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 260 FEET NORTH OF 97TH STREET.
- BM 2244 ELEVATION= 15.94 FEET  
SET SQUARE CUT ON TOP OF CURB NEXT TO LIGHT POLE ON THE NORTH SIDE OF 98TH STREET. APPROXIMATELY 120 FEET EAST OF WENTWORTH AVENUE.

- BM 2245 ELEVATION= 16.35 FEET  
SET SQUARE CUT ON THE EAST END OF CURB AT THE NORTHEAST CORNER OF FIRST ALLEY NORTH OF 99TH STREET. APPROXIMATELY 35 FEET EAST OF LASALLE STREET.
- BM 2246 ELEVATION= 20.00 FEET  
SET SQUARE CUT WITH CROSS ON TOP OF ABUTMENT WALL ON THE NORTHWEST CORNER OF STATE STREET BRIDGE OVER THE BISHOP FORD EXPRESSWAY. APPROXIMATELY 20 FEET NORTH OF THE NORTH EDGE OF THE BRIDGE.
- BM 2248 ELEVATION= 9.77 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF RAILROAD BRIDGE ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 110 FEET SOUTH OF 90TH STREET.
- BM 2249 ELEVATION= 9.67 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 87TH AND STATE STREET.
- BM 2250 ELEVATION= 10.03 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 83RD ST. AND STATE STREET.
- BM 2251 ELEVATION= 10.95 FEET  
FOUND CROSS ON EAST BOLT OF FIRE HYDRANT ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 135 FEET NORTH OF 79TH STREET.
- BM 2252 ELEVATION= 10.02 FEET  
SET CROSS ON WEST BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF STATE STREET AND 76TH STREET.
- BM 2254 ELEVATION= 9.17 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 73RD STREET ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 330 FEET SOUTH OF 73RD STREET.
- BM 2255 ELEVATION= 11.25 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 71ST STREET AND STATE STREET.
- BM 2256 ELEVATION= 16.36 FEET  
SET SQUARE CUT ON TOP OF ABUTMENT WALL ON THE NORTHWEST CORNER OF MARTIN LUTHER KING DRIVE BRIDGE OVER THE BISHOP FORD EXPRESSWAY. APPROXIMATELY 1 FOOT NORTH OF THE NORTH END OF THE BRIDGE.
- BM 2257 ELEVATION= 18.25 FEET  
SET CROSS ON NORTHEAST BOLT OF LIGHT POLE BASE ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 970 FEET SOUTH OF 95TH STREET.
- BM 2258 ELEVATION= 10.53 FEET  
SET CROSS ON NORTHWEST BOLT OF LIGHT BASE ON NORTHWEST CORNER OF 71ST STREET AND LAFAYETTE AVENUE.

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**PROJECT BENCHMARKS**

- BM 101 ELEVATION= 24.89 FEET  
SET SQUARE CUT ON SOUTHERLY CURB OF WEST 98TH PLACE. APPROXIMATELY 70 FEET EAST OF THE CENTERLINE OF GREEN STREET.
- BM 102 ELEVATION= 27.36 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF WEST 99TH STREET AND GREEN STREET.
- BM 8558 ELEVATION= 17.73 FEET  
SET SQUARE CUT ON SOUTHEAST CORNER OF FOURTH LIGHT BASE SOUTH OF 95TH STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8559 ELEVATION= 17.10 FEET  
SET SQUARE CUT ON SOUTHWEST CORNER OF CONCRETE SIGN BASE AT THE NORTHWEST CORNER OF THE MOBIL GAS STATION PROPERTY AT THE NORTHEAST CORNER OF 95TH STREET AND STATE STREET.
- BM 8560 ELEVATION= 17.50 FEET  
SET SQUARE CUT ON SOUTHEAST CORNER OF CONCRETE BUS SLAB ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 50 FEET SOUTH OF THE CENTERLINE OF 94TH STREET.
- BM 8561 ELEVATION= 19.77 FEET  
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT SOUTH OF 93RD STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8562 ELEVATION= 22.29 FEET  
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT NORTH OF 93RD STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8563 ELEVATION= 20.47 FEET  
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT SOUTH OF 91ST STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8573 ELEVATION= 19.89 FEET  
SET SQUARE CUT ON EAST SIDE OF SIDEWALK OPPOSITE TO THE SECOND FIRE HYDRANT SOUTH OF 91ST STREET, ON THE WEST SIDE OF LAFAYETTE AVENUE.
- BM 8574 ELEVATION= 18.90 FEET  
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT APPROXIMATELY AT 9224 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.
- BM 8575 ELEVATION= 17.77 FEET  
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT APPROXIMATELY AT 9326 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.

- BM 8576 ELEVATION= 17.43 FEET  
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT AT THE VACANT LOT BETWEEN 9416 AND 9422 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.
- BM 8578 ELEVATION= 27.92 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND PEORIA STREET.
- BM 8579 ELEVATION= 28.59 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND SANGAMON STREET.
- BM 8580 ELEVATION= 29.24 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND MORGAN STREET.
- BM 8581 ELEVATION= 30.82 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF CARPENTER STREET AND WEST 98TH PLACE.
- BM 8582 ELEVATION= 30.69 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF WEST 98TH PLACE, ON THE EAST SIDE OF ABERDEEN STREET.
- BM 8583 ELEVATION= 33.14 FEET  
SET SQUARE CUT ON CONCRETE BRIDGE WALL AT THE SOUTHEASTERLY CORNER OF GENOA AVENUE AND 99TH STREET.
- BM 8584 ELEVATION= 32.36 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT WEST OF ABERDEEN STREET, ON THE SOUTHERLY SIDE OF 99TH STREET.
- BM 8585 ELEVATION= 30.28 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRST FIRE HYDRANT WEST OF MORGAN STREET, ON THE SOUTH SIDE OF 99TH STREET.
- BM 8586 ELEVATION= 28.75 FEET  
SET CROSS ON NORTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF 99TH STREET AND SANGAMON STREET.
- BM 8587 ELEVATION= 31.19 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF 99TH STREET AND CARPENTER STREET.

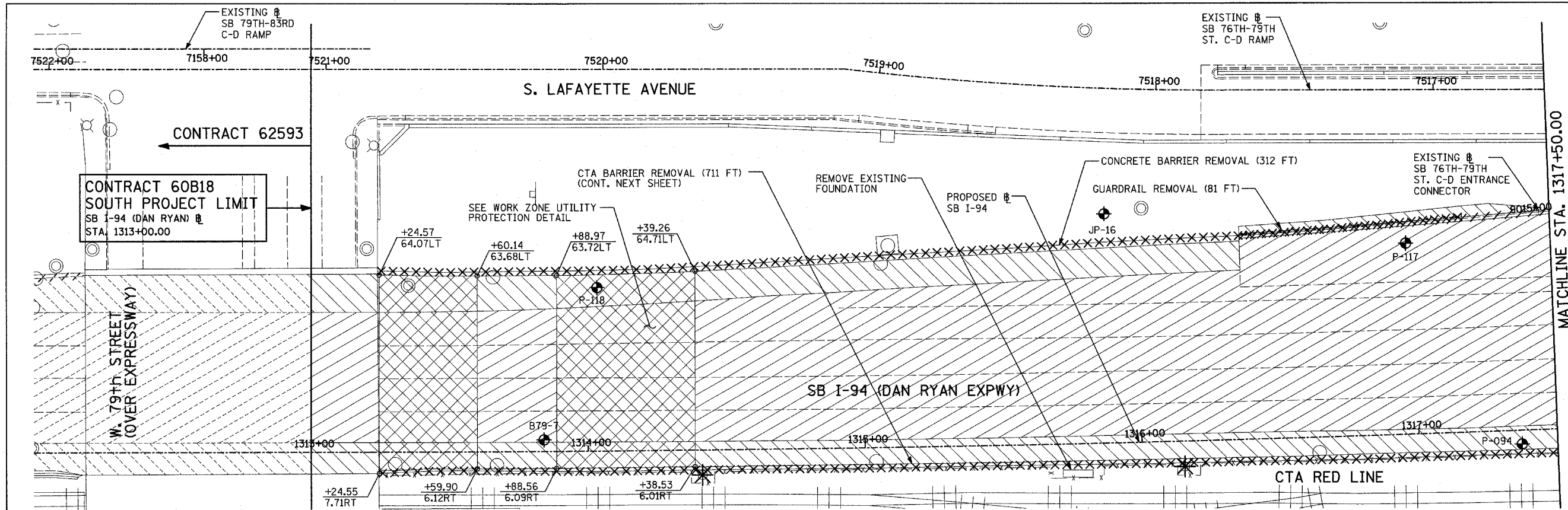
- BM 3514 ELEVATION= 17.56 FEET  
SET CROSS ON CHAIN BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF LASALLE ST. AND 97TH ST.
- BM 3522 ELEVATION= 18.06 FEET  
SET CROSS ON SOUTH SOUTHEAST FLANGE BOLT OF HYDRANT ± HALFWAY BETWEEN NORTH AND SOUTH ENTRANCE TO CITGO, EAST SIDE OF STATE ST. AND NORTH OF 95TH ST.
- BM 3549 ELEVATION= 9.49 FEET  
SET CROSS WESTERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 87TH ST. AND LAFAYETTE AVE.
- BM 3550 ELEVATION= 22.88 FEET  
SET CROSS WESTERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 91ST ST. AND LAFAYETTE AVE.
- BM 3551 ELEVATION= 17.50 FEET  
SET CROSS NORTHERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 95TH ST. AND LAFAYETTE AVE.
- BM 3552 ELEVATION= 14.81 FEET  
SET CROSS NORTHEAST BOLT OF TRAFFIC SIGNAL/LIGHT BASE AT SOUTHEAST INTERSECTION OF 91ST ST. AND STATE ST.
- BM 3553 ELEVATION= 8.47 FEET  
SET CROSS NORTHWEST BOLT OF TRAFFIC SIGNAL/LIGHT BASE AT NORTHEAST INTERSECTION OF 87TH ST. AND STATE ST.
- BM 3556 ELEVATION= 9.06 FEET  
SET CROSS EASTERLY FLANGE BOLT OF HYDRANT AT NORTHEAST CORNER OF 75TH ST. AND STATE ST.
- BM 3558 ELEVATION= 18.15 FEET  
SET CROSS SOUTHWESTERLY FLANGE BOLT OF HYDRANT AT NORTHWEST INTERSECTION OF NORMAL ST. AND 98TH PLACE.
- BM 3810 ELEVATION= 0.29 FEET  
CUT SQUARE ON SOUTHERLY CORNER OF METAL BASE FOR OVERHEAD SIGN, ON THE WESTERLY SIDE OF SOUTHBOUND DAN RYAN, OPPOSITE OF WEST 77TH PLACE, APPROXIMATELY 1000 FEET NORTH OF 79TH STREET BRIDGE.
- BM 3811 ELEVATION= 1.95 FEET  
CUT SQUARE ON SOUTHEASTERLY CORNER OF METAL BASE FOR OVERHEAD SIGN, ON THE WESTERLY SIDE OF SOUTHBOUND DAN RYAN APPROXIMATELY 840 FEET SOUTHERLY OF 83RD STREET.

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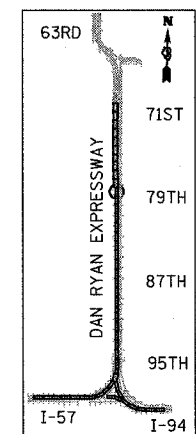
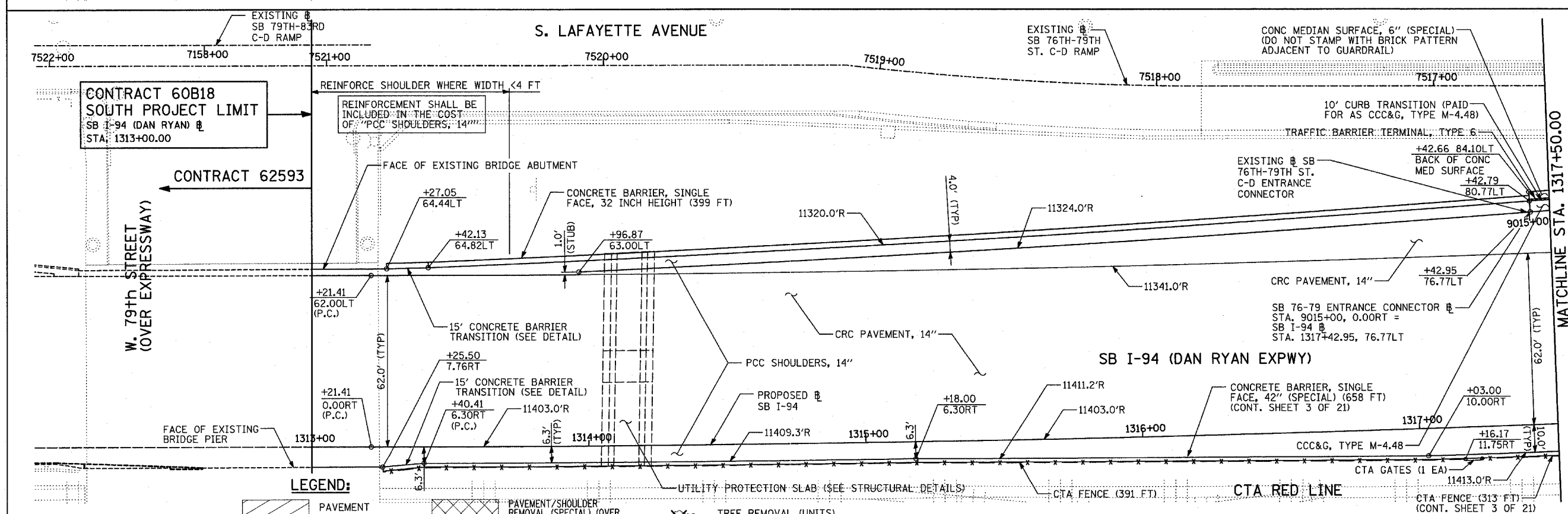
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 SURVEY TIES FOR CONTROL POINTS  
 SHEET 7 OF 7

SCALE: 1"=30'  
 DATE: MARCH 1, 2006  
 DRAWN BY: GSP  
 CHECKED BY: MMW





EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS

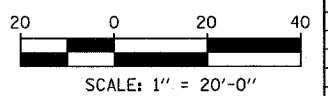


LOCATION MAP

**LEGEND:**

- |  |                                    |  |   |  |   |
|--|------------------------------------|--|---|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |  | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)   |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |  | MAINLINE #  |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |  | STATION/OFFSET  |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |  | <b>PLAN NOTES:</b>  |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |  | - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |  | - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED                        |

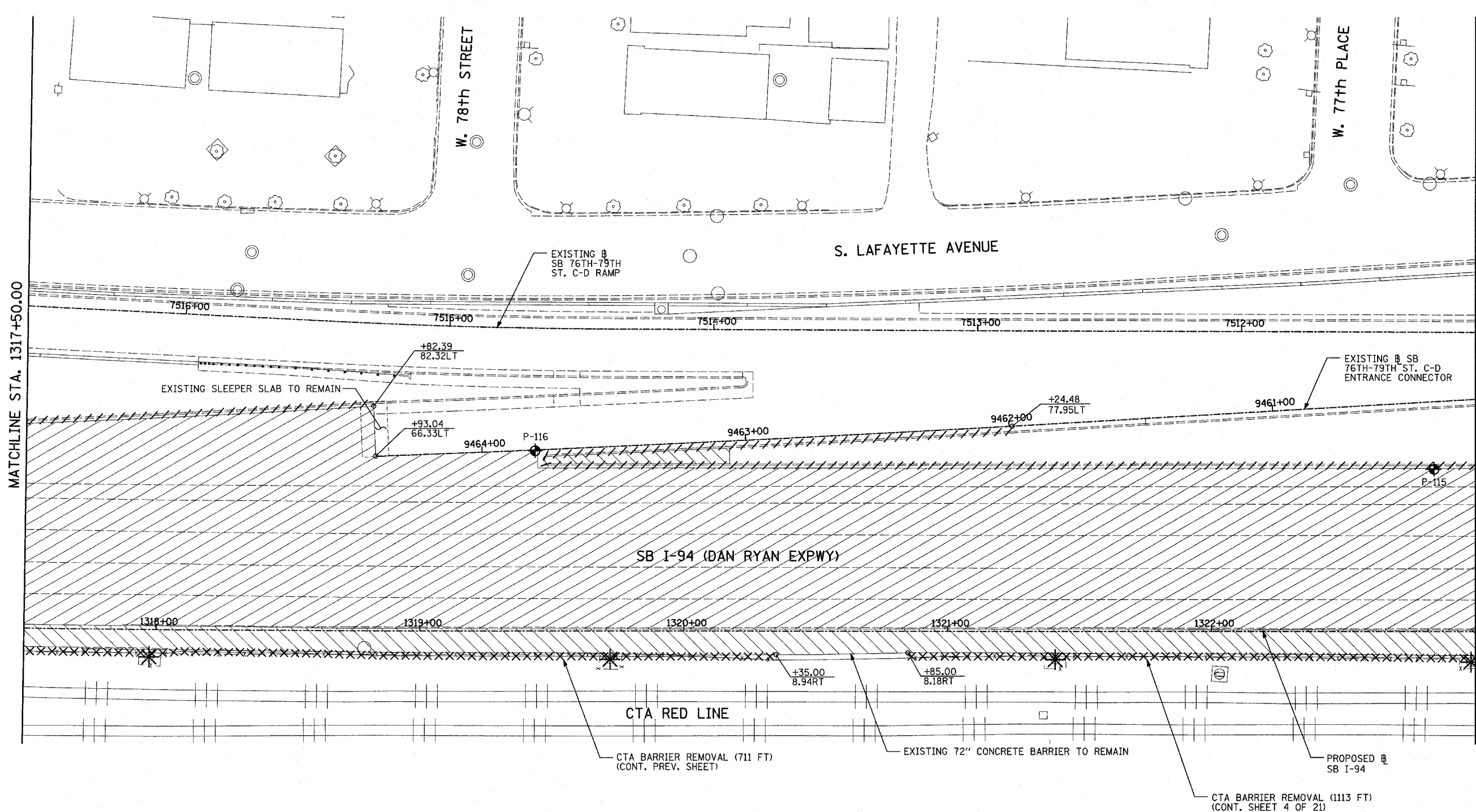
REVISIONS	
NAME	DATE



TYLIN INTERNATIONAL

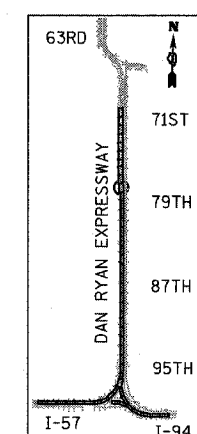
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
SB I-94 (DAN RYAN EXPRESSWAY)  
SB I-94 STA. 1313+00.00 TO 1317+50.00

SCALE: 1"=20'  
DATE: MARCH 1, 2006  
DRAWN BY: MPG  
CHECKED BY: RTM



MATCHLINE STA. 1317+50.00

MATCHLINE STA. 1323+00.00



LOCATION MAP

**LEGEND:**

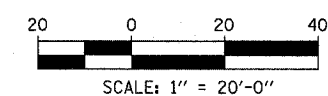
- |  |                                    |  |   |
|--|------------------------------------|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

- TREE REMOVAL (UNITS) (NO UNIT SHOWN = 6)
- ++XX.XX** MAINLINE #  
**XX.XXRT** STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN** INTERNATIONAL

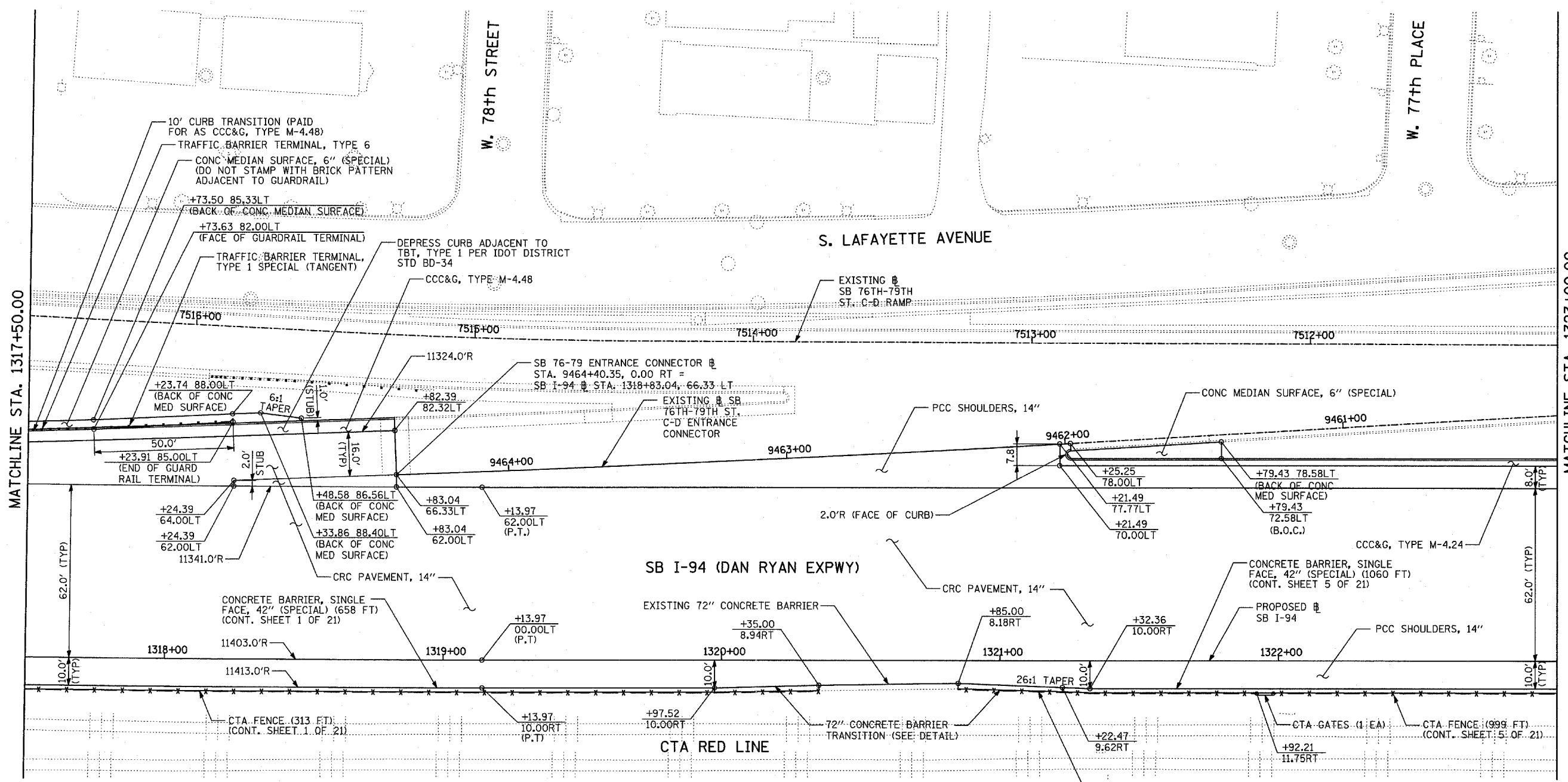


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NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**EXISTING PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**DAN RYAN STA. 1317+50.00 TO 1323+00.00**

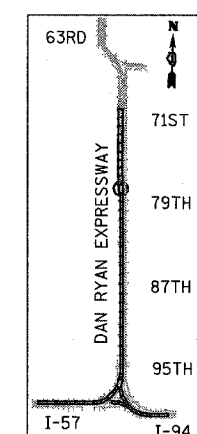
SCALE: 1"=20'  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM



MATCHLINE STA. 1323+00.00

MATCHLINE STA. 1317+50.00



LOCATION MAP

**LEGEND:**

	PAVEMENT REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL
	BITUMINOUS SURFACE REMOVAL, 4"		CHAIN LINK FENCE REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CONCRETE BARRIER/GUARDRAIL REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
			LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)

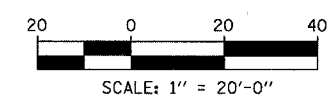
TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6>)

MAINLINE #  
 STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN** INTERNATIONAL

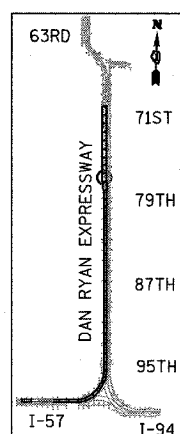
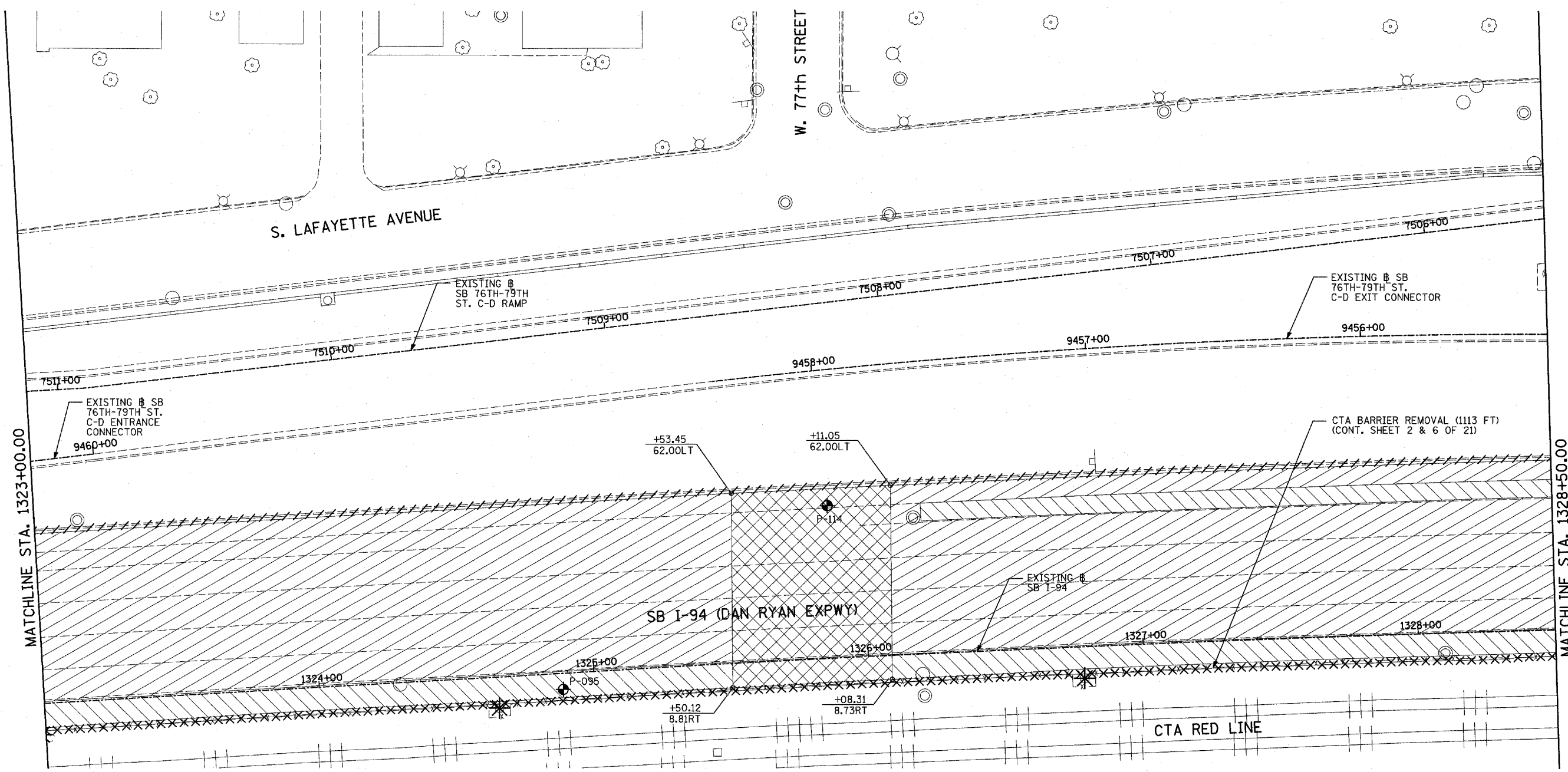
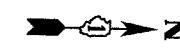


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**PROPOSED PLAN**  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 DAN RYAN STA. 1317+50.00 TO 1323+00.00

SCALE: 1"=20'  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM



LOCATION MAP

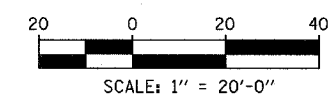
**LEGEND:**

- |  |                                    |  |   |
|--|------------------------------------|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

- TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6>)
- PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN INTERNATIONAL**

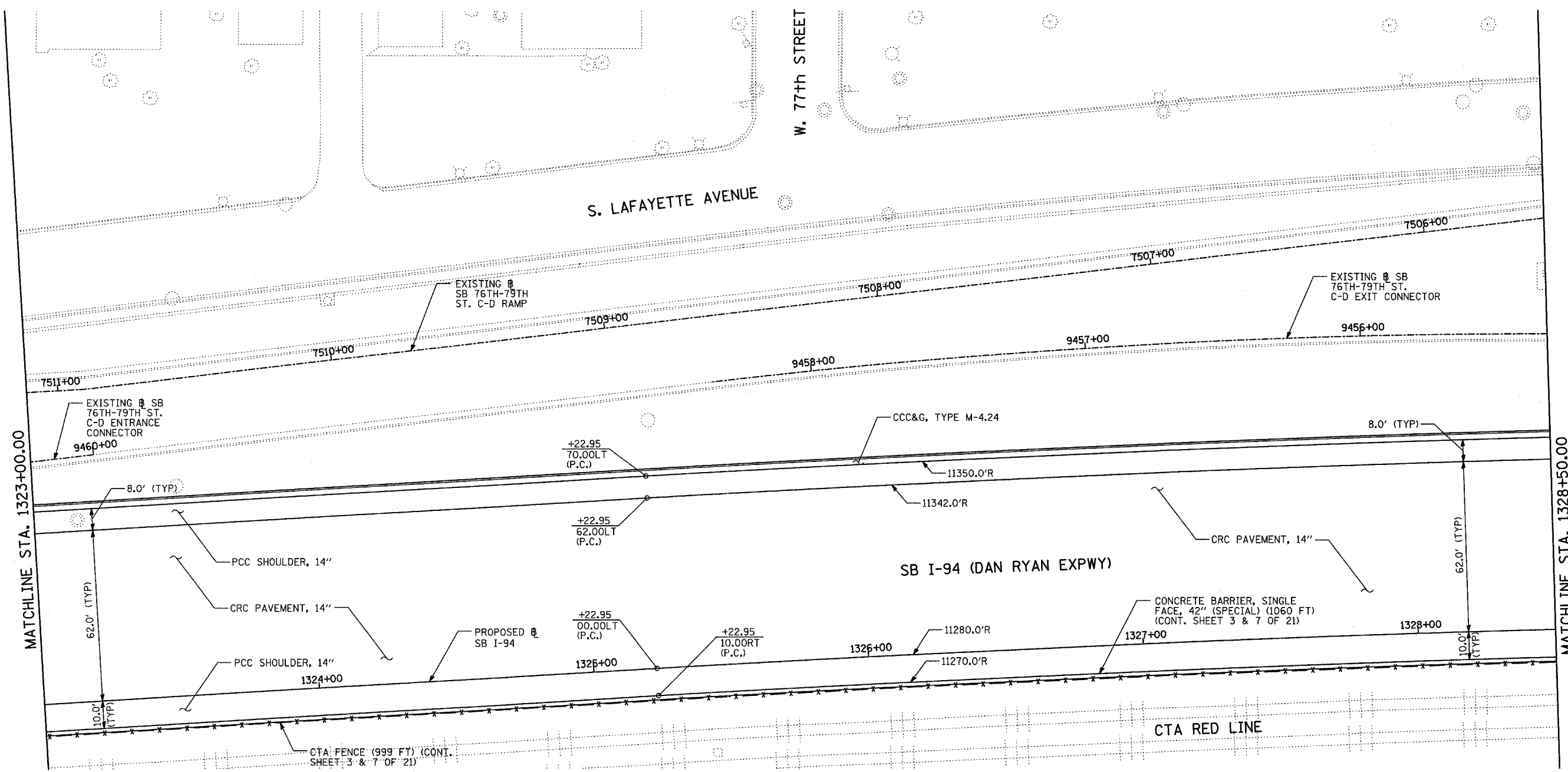


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**EXISTING PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**DAN RYAN STA. 1323+00.00 TO 1328+50.00**

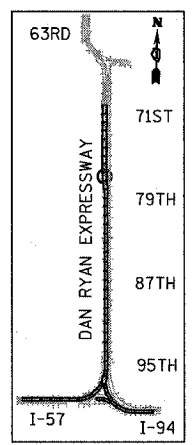
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DATE: MARCH 1, 2006

DRAWN BY: MPG  
CHECKED BY: RTM



MATCHLINE STA. 1323+00.00

MATCHLINE STA. 1328+50.00

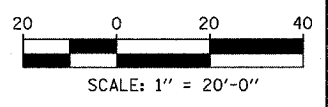


LOCATION MAP

**LEGEND:**

- |  |                                    |  |   |  |   |
|--|------------------------------------|--|---|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |  | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)   |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |  | XXX.XX MAINLINE @   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |  | XX.XXRT STATION/OFFSET  |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |  | <b>PLAN NOTES:</b>  |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |  | - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |  | - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED                        |

**TYLIN** INTERNATIONAL



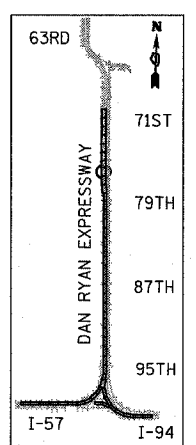
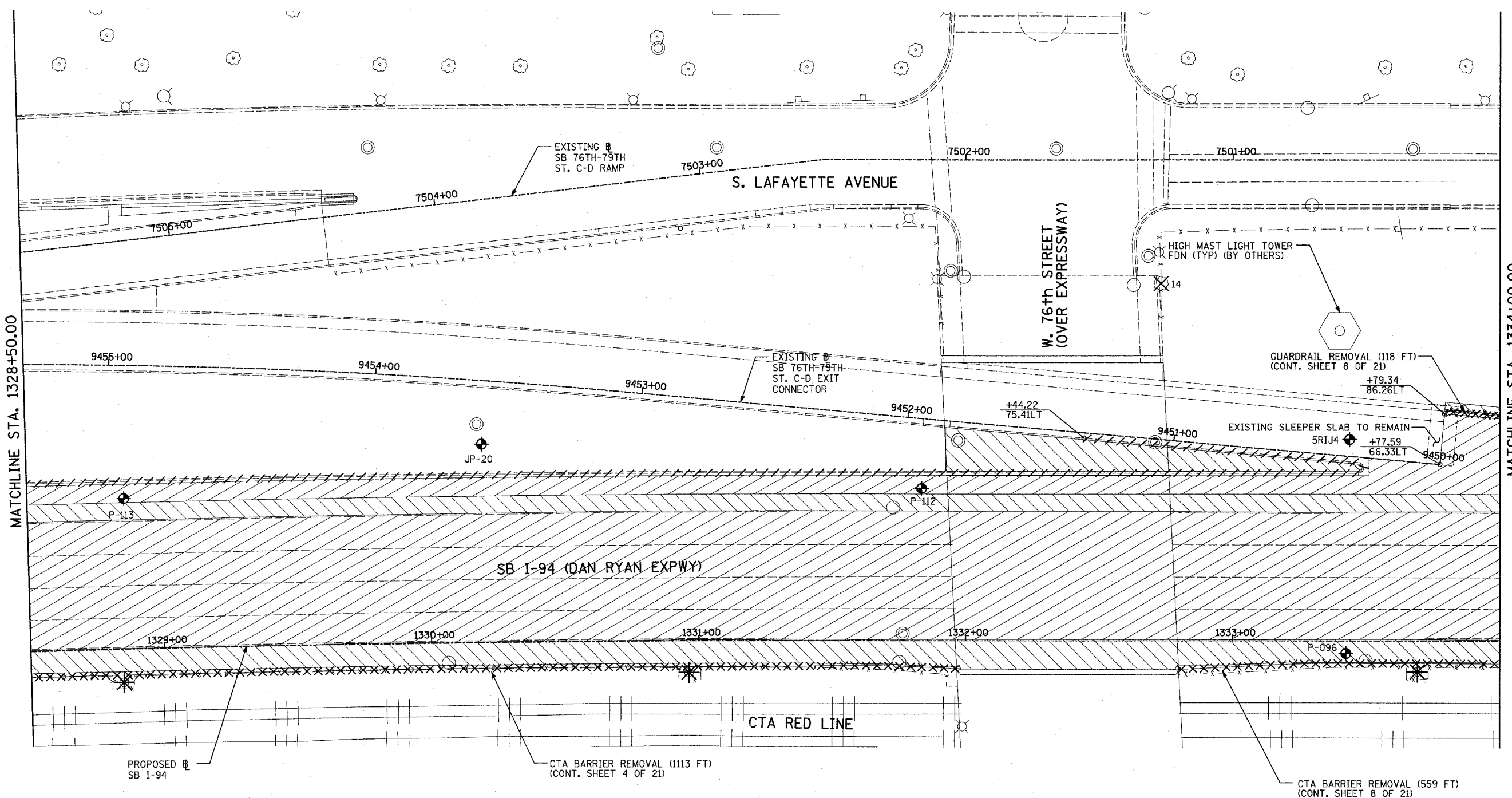
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**PROPOSED PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**DAN RYAN STA. 1323+00.00 TO 1328+50.00**

SCALE: 1"=20'  
DATE: MARCH 1, 2006

DRAWN BY: MPG  
CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	39
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		60B18	

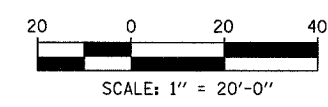


LOCATION MAP

**LEGEND:**

- |  |                                    |  |   |  |   |
|--|------------------------------------|--|---|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |  | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)   |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |  | MAINLINE #  |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |  | STATION/OFFSET  |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |  | <b>PLAN NOTES:</b>  |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |  | - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |  | - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED                        |

TYLIN INTERNATIONAL

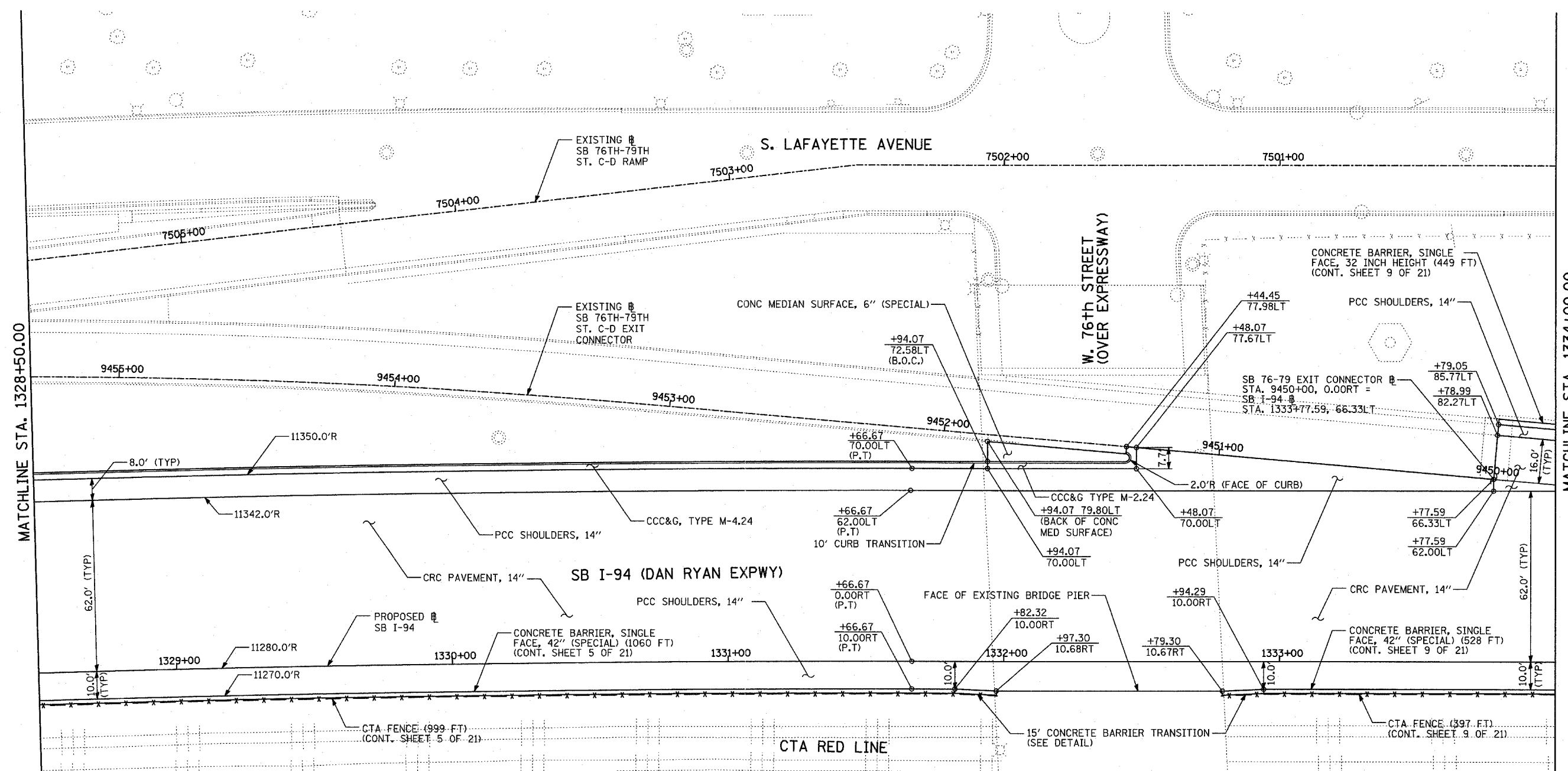


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 DAN RYAN STA. 1328+50.00 TO 1334+00.00

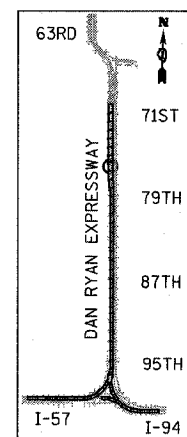
SCALE: 1"=20'  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM



MATCHLINE STA. 1328+50.00

MATCHLINE STA. 1334+00.00



LOCATION MAP

**LEGEND:**

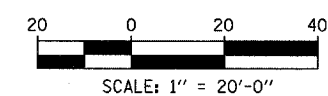
	PAVEMENT REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL
	BITUMINOUS SURFACE REMOVAL, 4"		CHAIN LINK FENCE REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CONCRETE BARRIER/GUARDRAIL REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
			LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)

TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6>)

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN INTERNATIONAL**

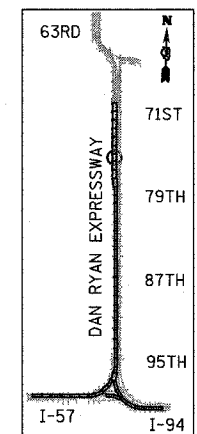
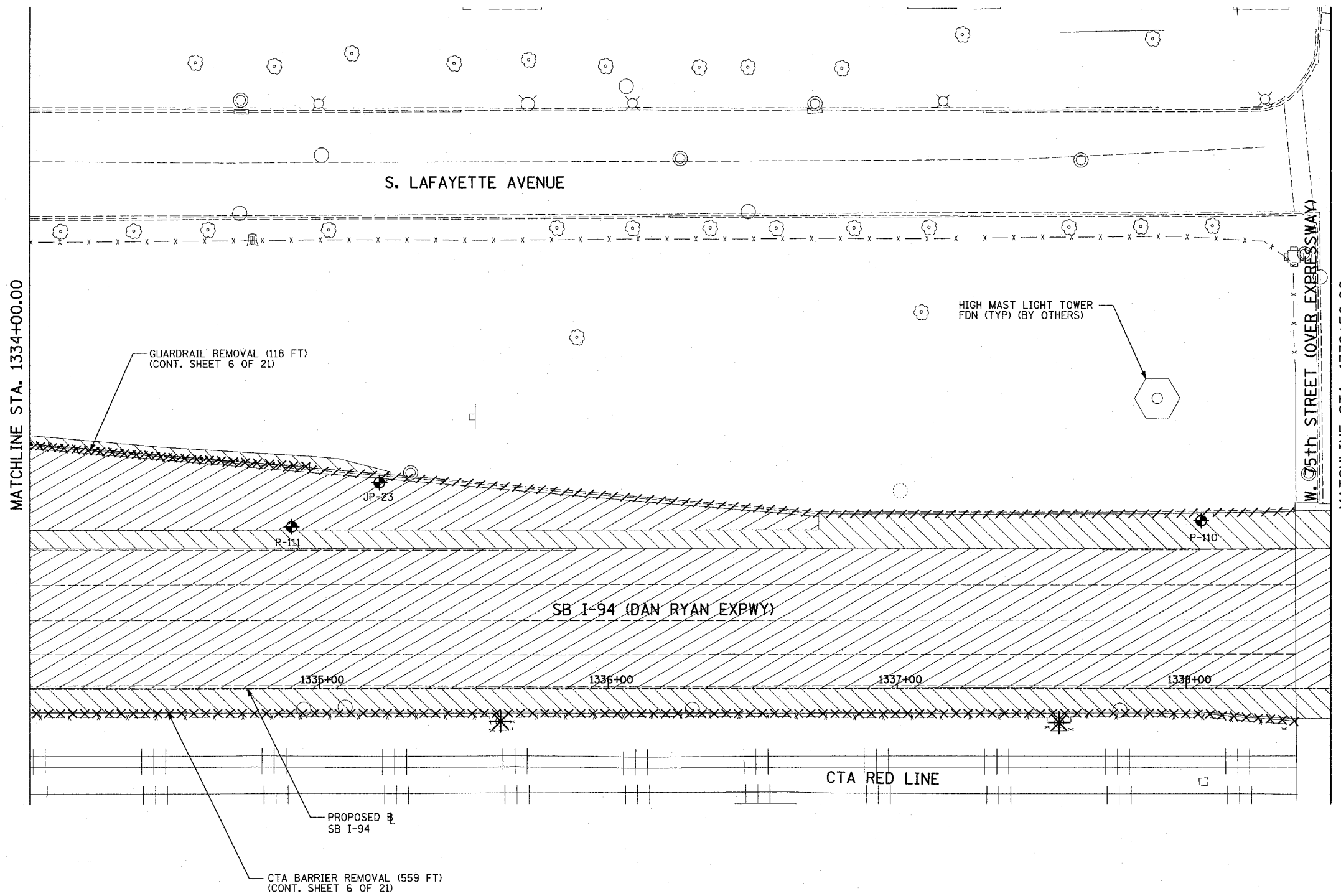


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**PROPOSED PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**DAN RYAN STA. 1328+50.00 TO 1334+00.00**

SCALE: 1"=20'  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM



LOCATION MAP

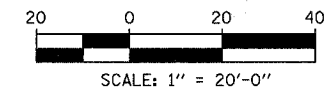
**LEGEND:**

- |  |                                    |  |   |  |   |
|--|------------------------------------|--|---|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) |  | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)                                 |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL                               |  | SOIL BORING LOCATIONS   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL                                      |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL                            |  |   |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  |   |  |   |

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ ABUTMENTS TO BE FIELD VERIFIED

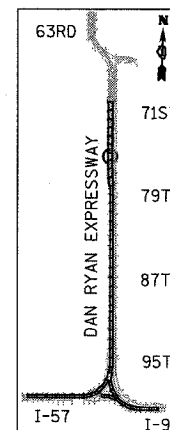
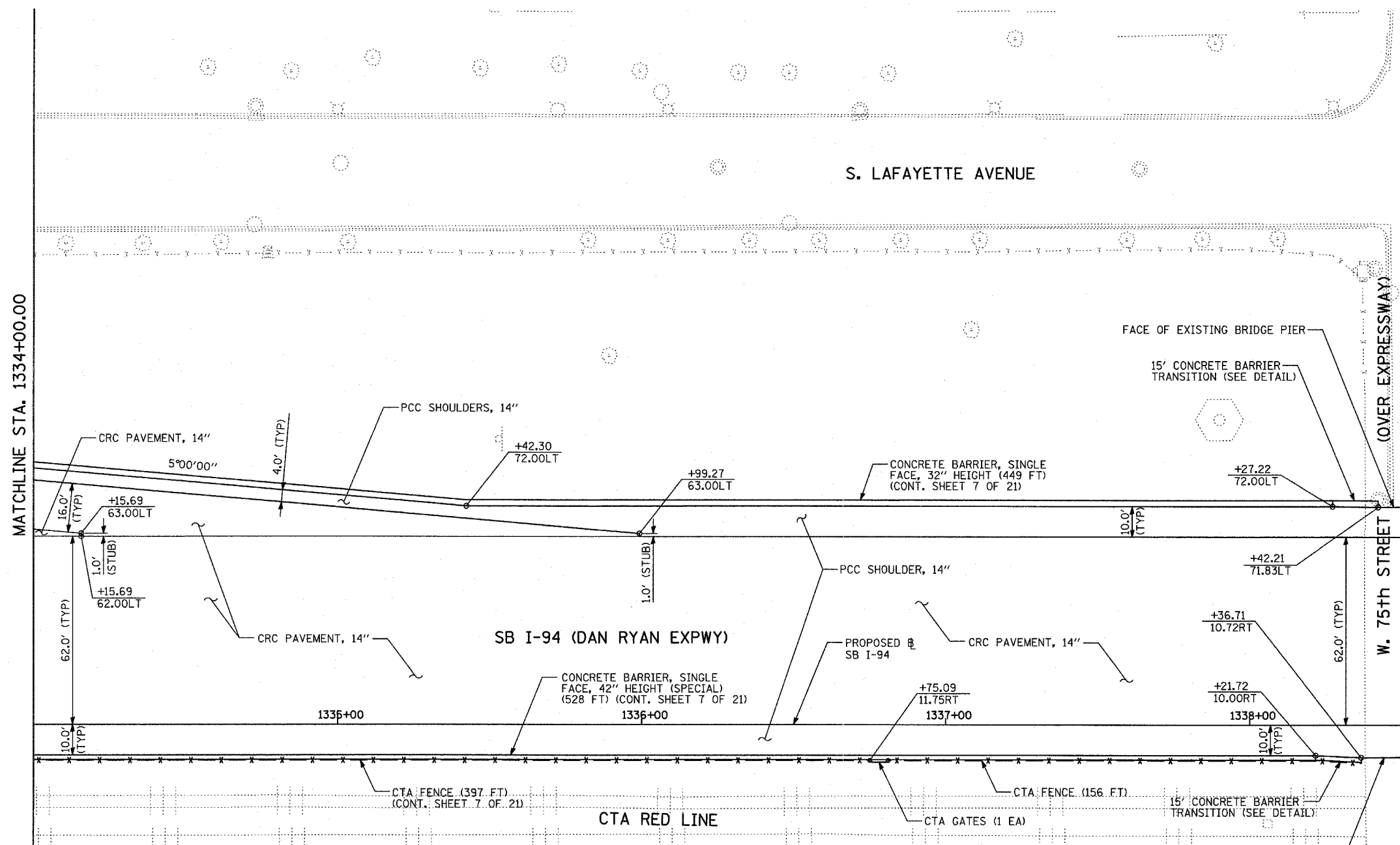
**TYLIN** INTERNATIONAL



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
 EXISTING PLAN  
**SB 76th TO 79th STREET C-D RAMP**  
**DAN RYAN STA. 1334+00.00 TO 1338+50.00**  
 SCALE: 1"=20'  
 DATE: MARCH 1, 2006  
 DRAWN BY: MPG  
 CHECKED BY: RTM





LOCATION MAP

**LEGEND:**

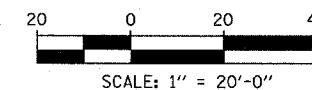
	PAVEMENT REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL
	BITUMINOUS SURFACE REMOVAL, 4"		CHAIN LINK FENCE REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CONCRETE BARRIER/GUARDRAIL REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
			LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)

10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)

+XX.XX MAINLINE #  
XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

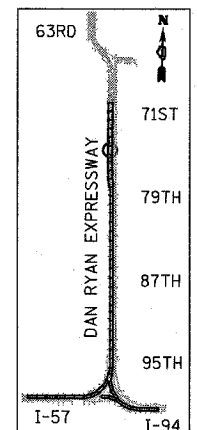
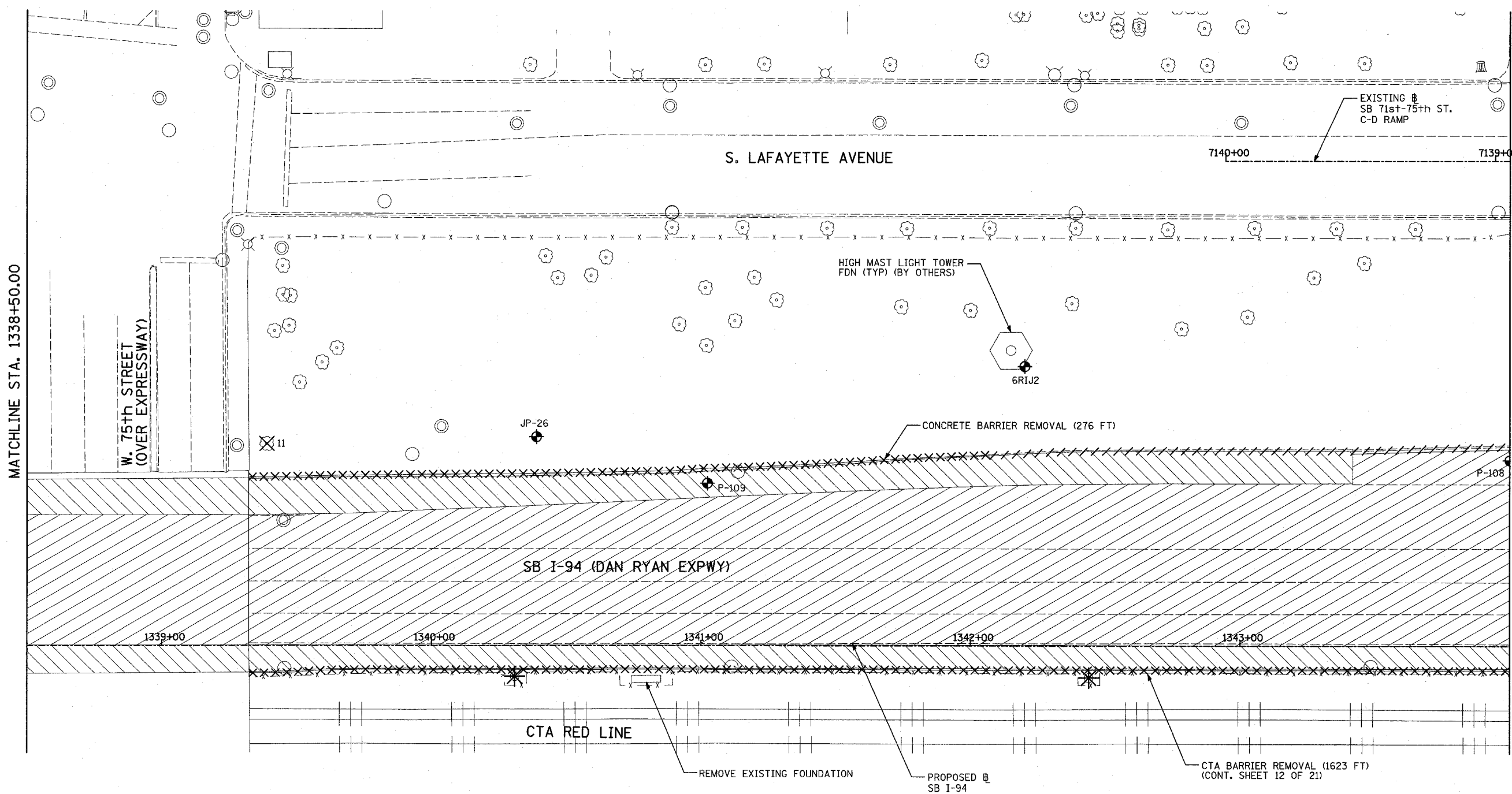


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**PROPOSED PLAN**  
SB I-94 (DAN RYAN EXPRESSWAY)  
DAN RYAN STA. 1334+00.00 TO 1338+50.00

SCALE: 1"=20'  
DATE: MARCH 1, 2006

DRAWN BY: MPG  
CHECKED BY: RTM



LOCATION MAP

**LEGEND:**

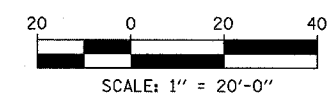
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|--|------------------------------------|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)

+XX.XX MAINLINE #  
XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



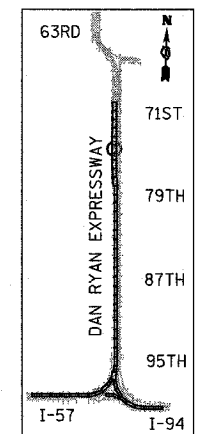
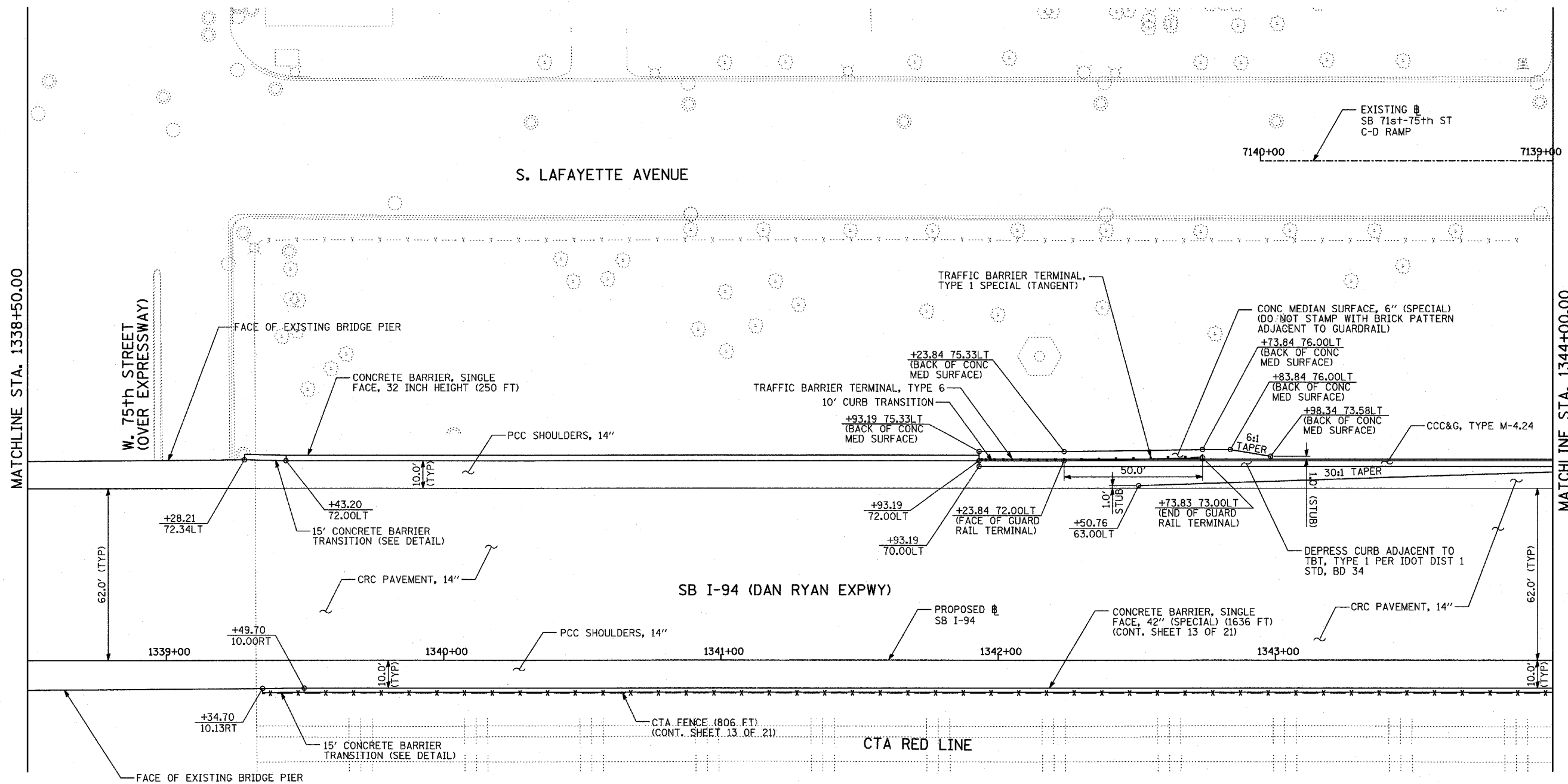
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 SB I-94 STA. 1338+50.00 TO 1344+00.00

SCALE: 1"=20'  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM

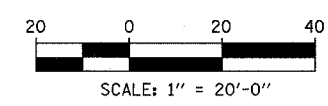
**TYLIN** INTERNATIONAL



**LEGEND:**

- |  |                                    |  |   |   |   |
|--|------------------------------------|--|---|---|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |   | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |   | MAINLINE @ STATION/OFFSET                 |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  | <b>PLAN NOTES:</b>  |   |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  | - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |   |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   | - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED                        |   |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |   |   |

**TYLIN INTERNATIONAL**

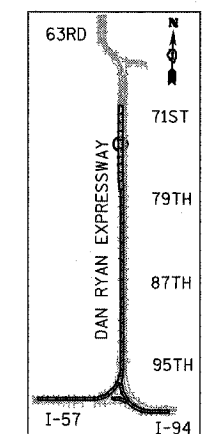
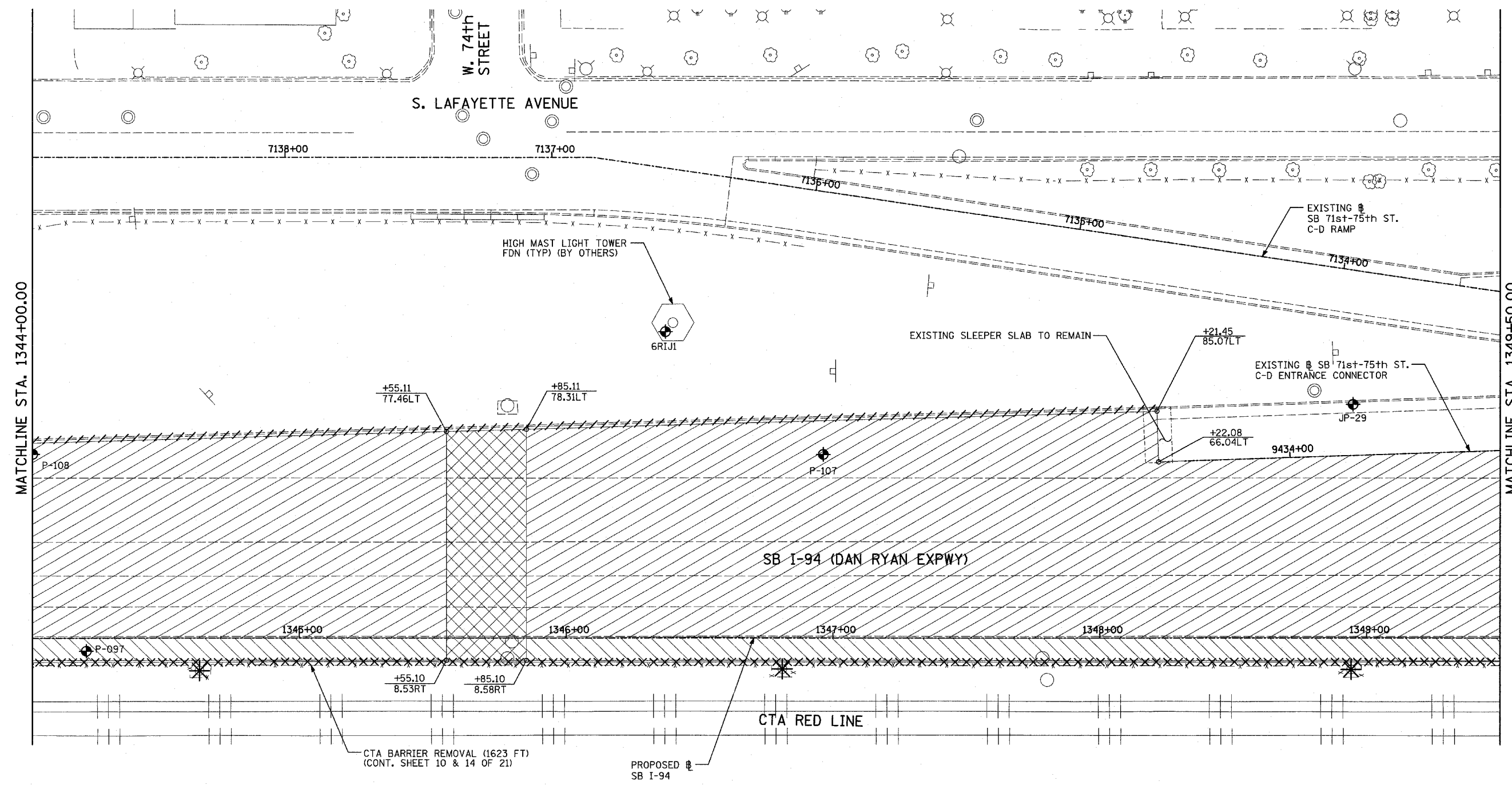


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**PROPOSED PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**SB I-94 STA. 1338+50.00 TO 1344+00.00**

SCALE: 1"=20'  
DATE: MARCH 1, 2006

DRAWN BY: MPG  
CHECKED BY: RTM



LOCATION MAP

**LEGEND:**

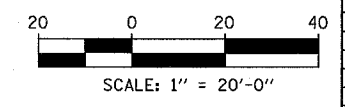
- |  |                                    |  |   |
|--|------------------------------------|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

- 10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6>)
- ++XX.XX** MAINLINE  $\Phi$
- XX.XXRT** STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN INTERNATIONAL**

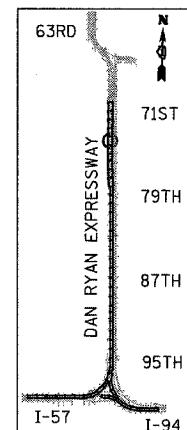
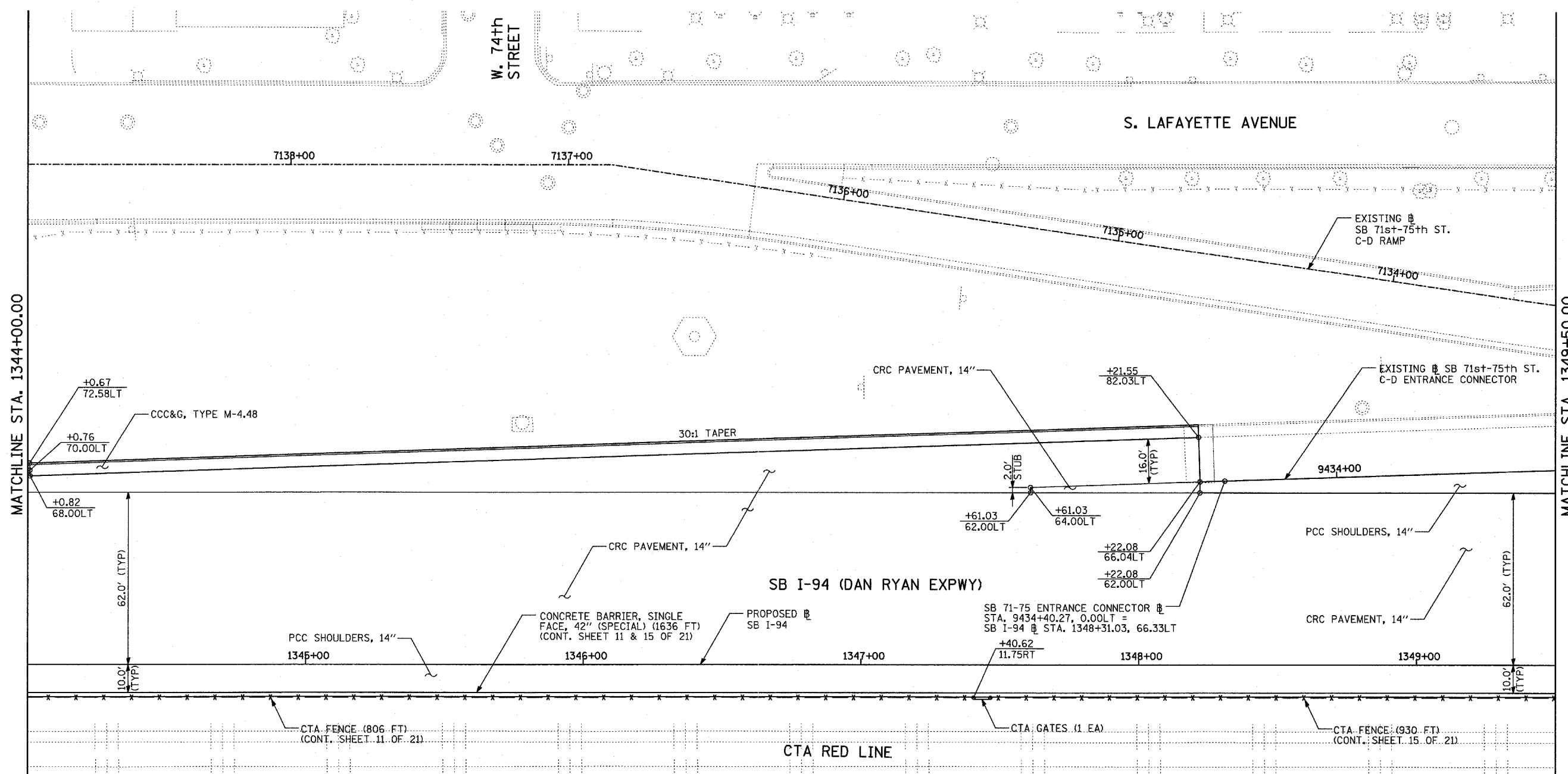


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**EXISTING PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**SB I-94 STA. 1344+00.00 TO 1349+50.00**

SCALE: 1"=20'  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM



LOCATION MAP

**LEGEND:**

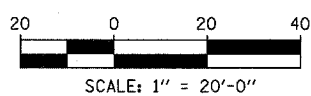
	PAVEMENT REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL
	BITUMINOUS SURFACE REMOVAL, 4"		CHAIN LINK FENCE REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CONCRETE BARRIER/GUARDRAIL REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
			LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)

10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)

XX.XX MAINLINE #  
XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

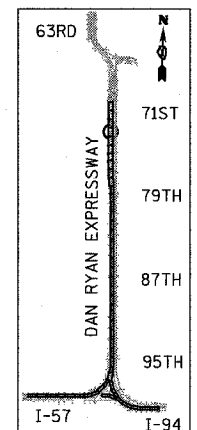
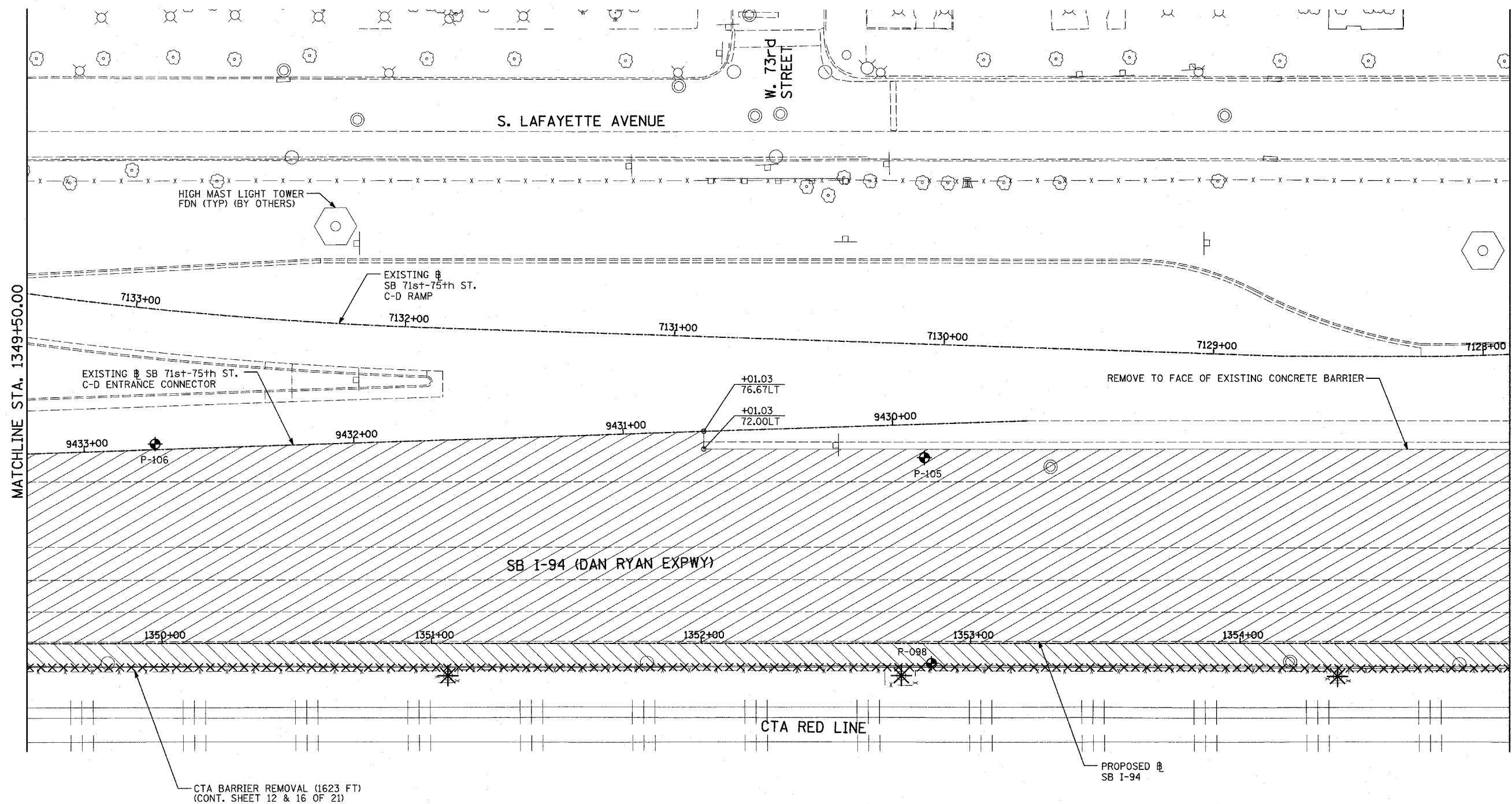


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**PROPOSED PLAN**  
SB I-94 (DAN RYAN EXPRESSWAY)  
SB I-94 STA. 1344+00.00 TO 1349+50.00

SCALE: 1"=20'  
DATE: MARCH 1, 2006  
DRAWN BY: MPG  
CHECKED BY: RTM

**TYLIN** INTERNATIONAL



LOCATION MAP

**LEGEND:**

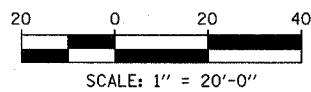
- |  |                                    |  |   |
|--|------------------------------------|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)

+XX.XX MAINLINE @  
XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



REVISIONS	
NAME	DATE

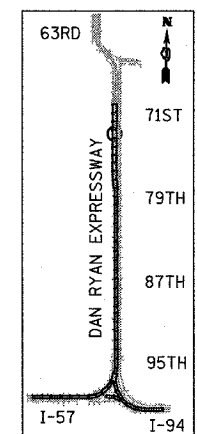
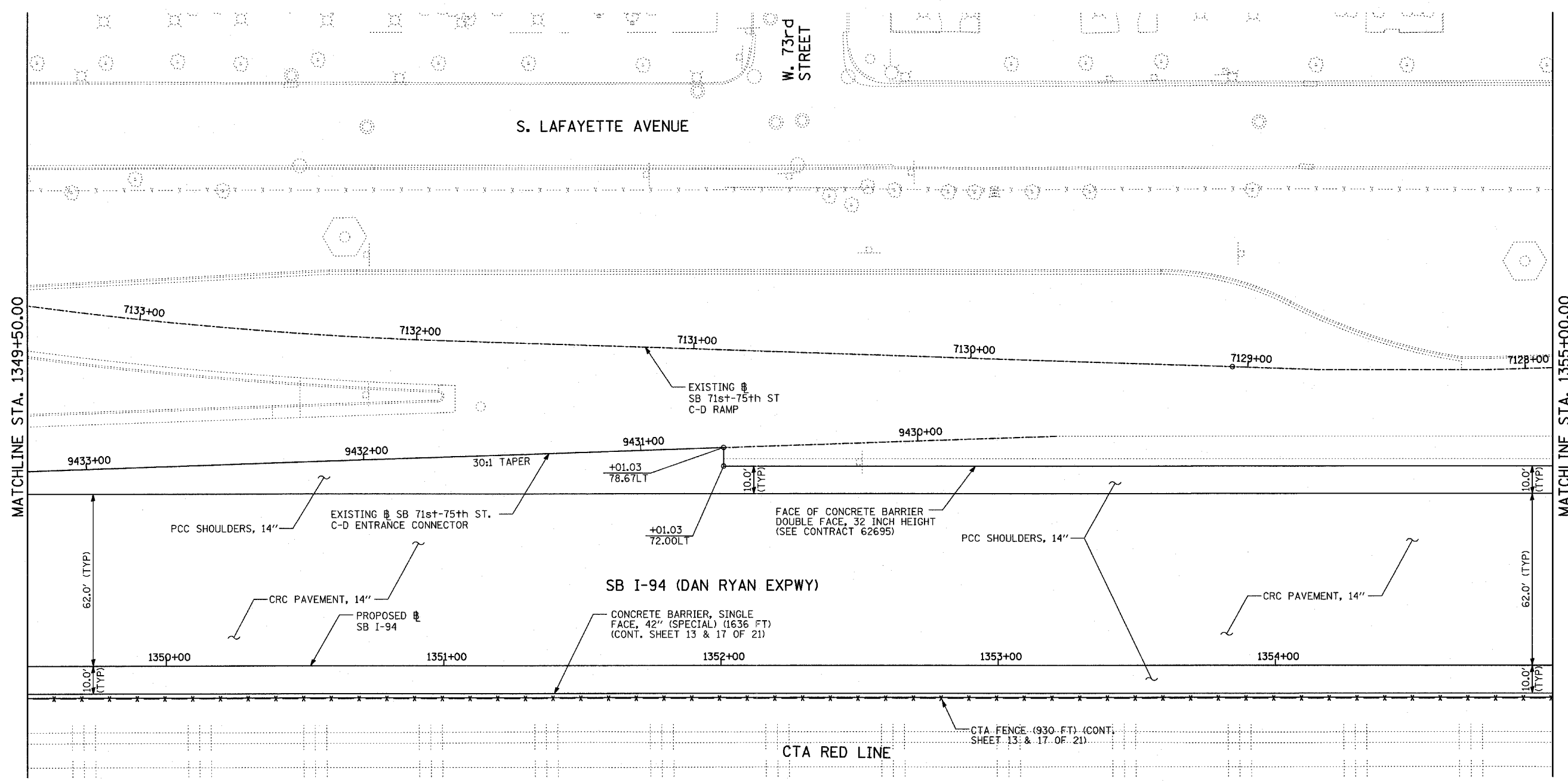
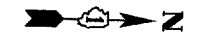
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING PLAN  
SB I-94 (DAN RYAN EXPRESSWAY)  
SB I-94 STA. 1349+50.00 TO 1355+00.00

SCALE: 1"=20'  
DATE: MARCH 1, 2006  
DRAWN BY: MPG  
CHECKED BY: RTM

**TYLIN** INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	48
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

60B18



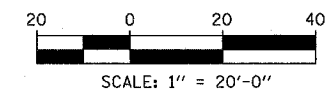
LOCATION MAP

**LEGEND:**

- |  |                                    |  |   |  |   |
|--|------------------------------------|--|---|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) |  | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6>)                                |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL                               |  | SOIL BORING LOCATIONS   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL                                      |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL                            |  | EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED                             |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  |   |  |   |

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



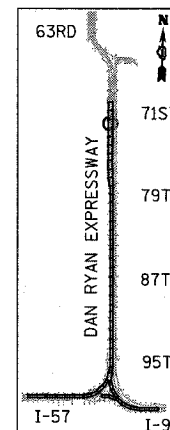
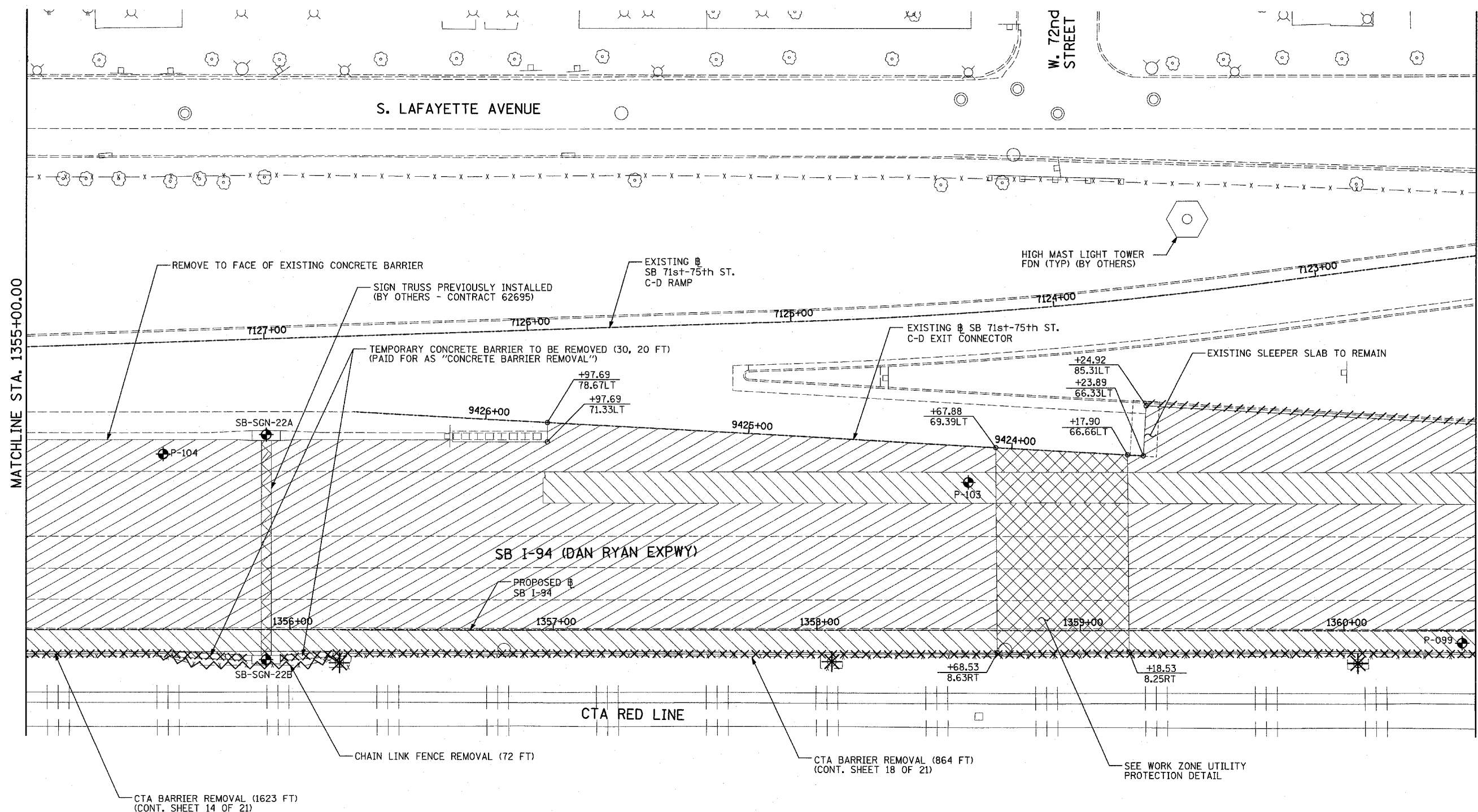
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**PROPOSED PLAN**  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 SB I-94 STA. 1349+50.00 TO 1355+00.00

SCALE: 1"=20'  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM

**TYLIN** INTERNATIONAL



LOCATION MAP

**LEGEND:**

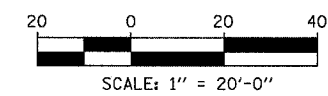
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|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

- 10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6>  
 +XX.XX MAINLINE #  
 XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN INTERNATIONAL**



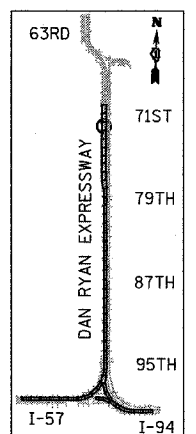
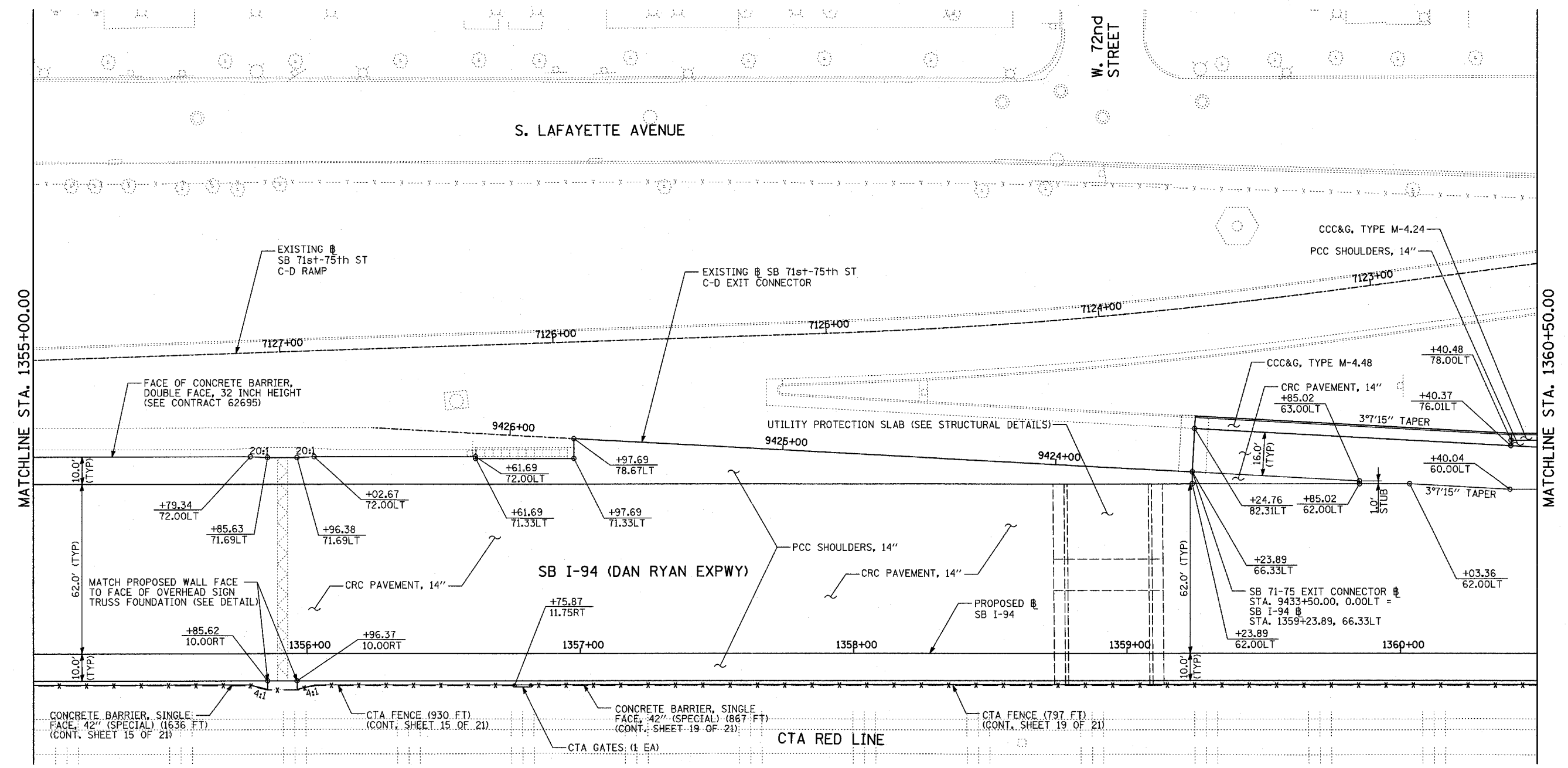
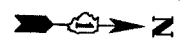
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 SB I-94 STA. 1355+00.00 TO 1360+50.00

SCALE: 1"=20'  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM





LOCATION MAP

**LEGEND:**

- |  |                                    |  |   |  |   |
|--|------------------------------------|--|---|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) |  | TREE REMOVAL (UNITS) (NO UNIT SHOWN = 6)                                  |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL                               |  | SOIL BORING LOCATIONS   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL                                      |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL                            |  |   |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  |   |  |   |
- +XX.XX MAINLINE #  
 XX.XXRT STATION/OFFSET

**PLAN NOTES:**

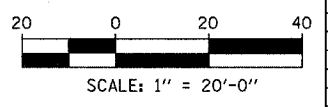
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

REVISIONS	
NAME	DATE

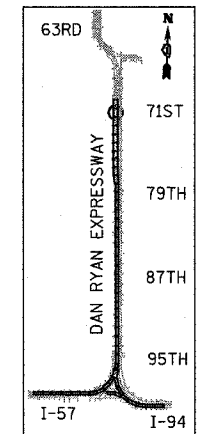
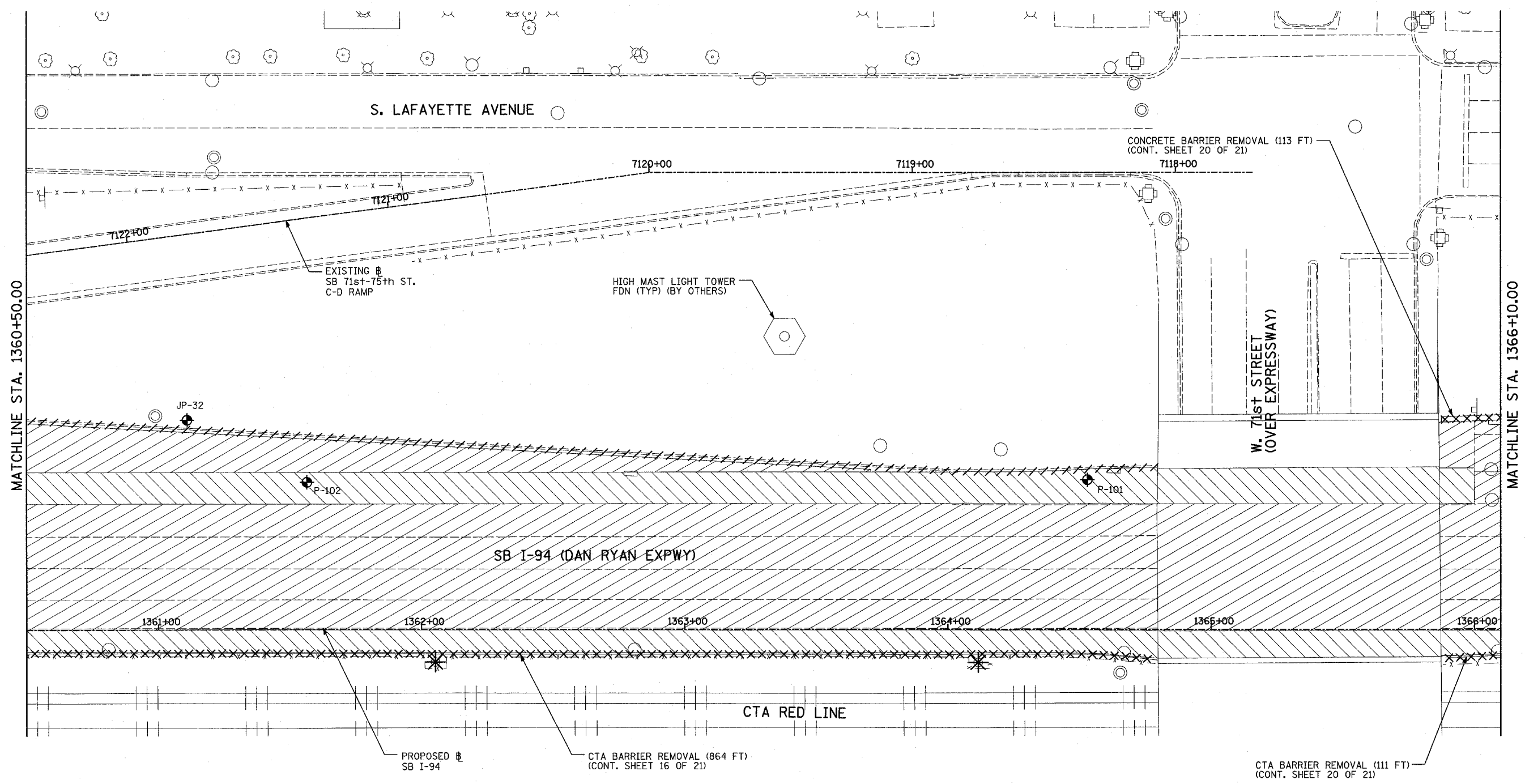
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**PROPOSED PLAN**  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 SB I-94 STA. 1355+00.00 TO 1360+50.00

SCALE: 1"=20'  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM



**TYLIN** INTERNATIONAL

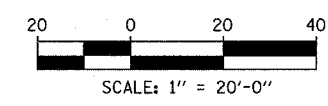


LOCATION MAP

**LEGEND:**

- |  |                                    |  |   |   |   |
|--|------------------------------------|--|---|---|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) OVER EXISTING UTILITIES               |   | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |   | MAINLINE STATION/OFFSET                   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  | <b>PLAN NOTES:</b>  |   |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  | - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |   |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   | - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED                        |   |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |   |   |

**TYLIN** INTERNATIONAL

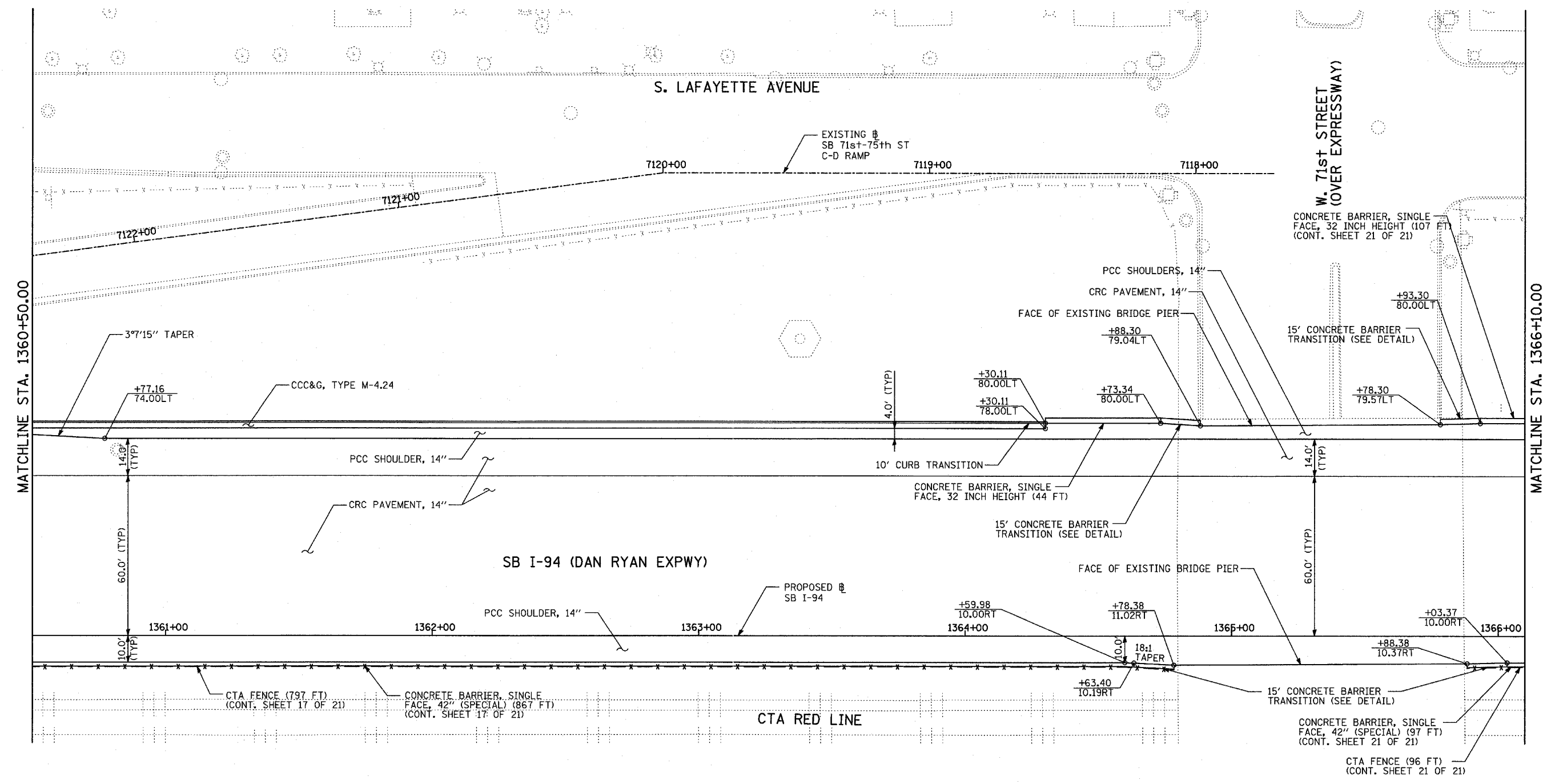
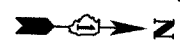


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**EXISTING PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**SB I-94 STA. 1360+50.00 TO 1366+10.00**

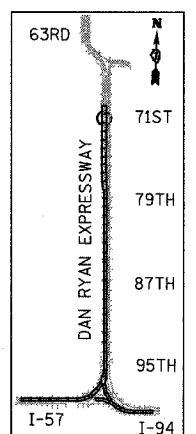
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 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM



MATCHLINE STA. 1360+50.00

MATCHLINE STA. 1366+10.00



LOCATION MAP

**LEGEND:**

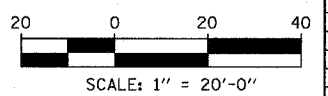
- |  |                                    |  |   |
|--|------------------------------------|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

- 10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- +XX.XX MAINLINE @
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN INTERNATIONAL**

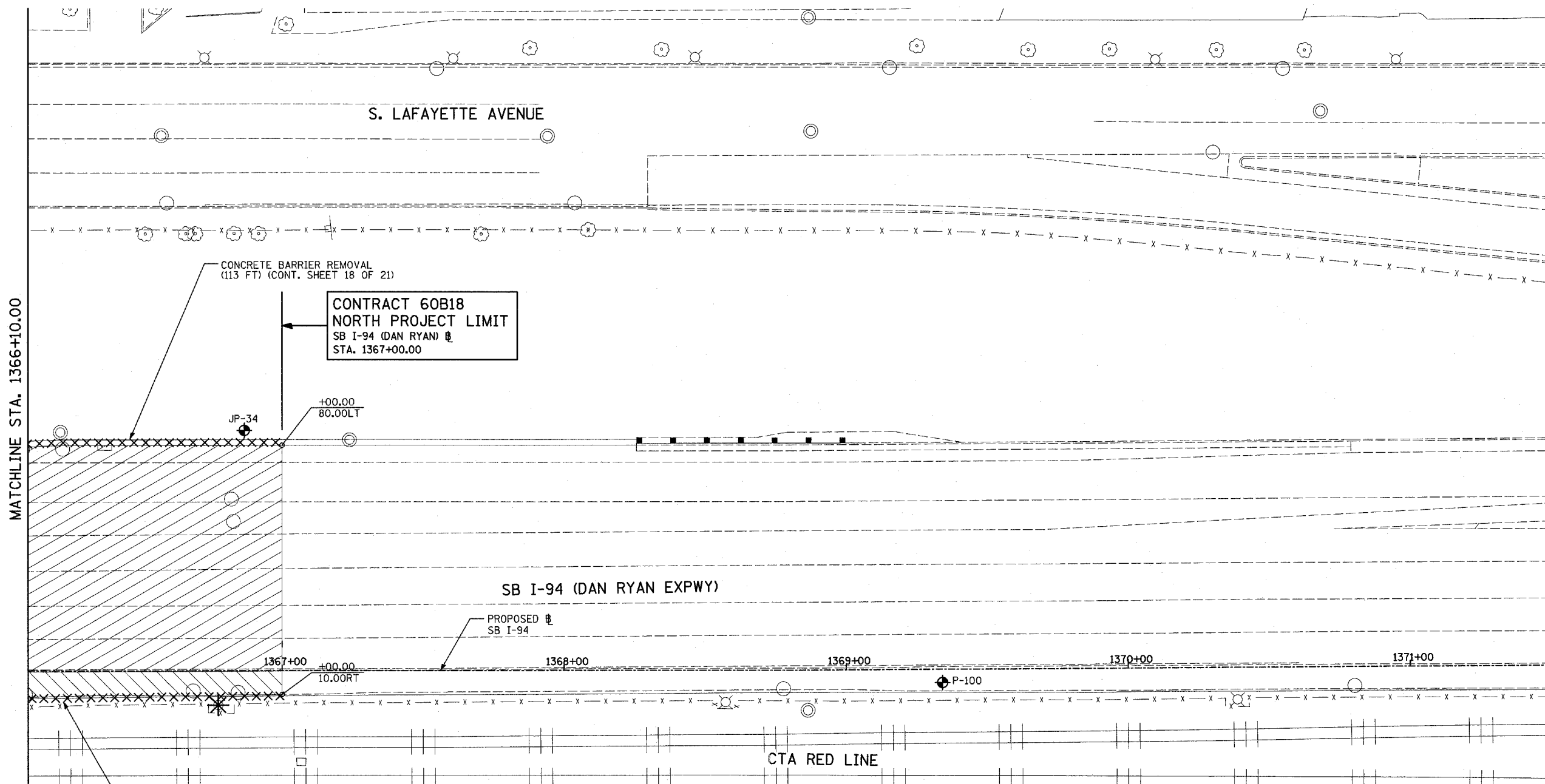


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 PROPOSED PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 SB I-94 STA. 1360+50.00 TO 1366+10.00

SCALE: 1"=20'  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM

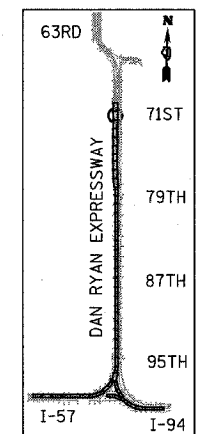


MATCHLINE STA. 1366+10.00

**CONTRACT 60B18**  
**NORTH PROJECT LIMIT**  
 SB I-94 (DAN RYAN) #  
 STA. 1367+00.00

CONCRETE BARRIER REMOVAL  
 (113 FT) (CONT. SHEET 18 OF 21)

CTA BARRIER REMOVAL  
 (111 FT) (CONT. SHEET 18 OF 21)



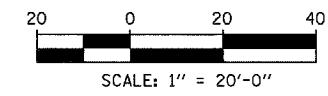
LOCATION MAP

**LEGEND:**

- |  |                                    |  |   |  |   |
|--|------------------------------------|--|---|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |  | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |  | MAINLINE #                                |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |  | STATION/OFFSET                            |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |  |   |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |  |   |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |  |   |

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ ABUTMENTS TO BE FIELD VERIFIED



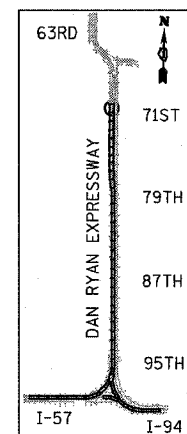
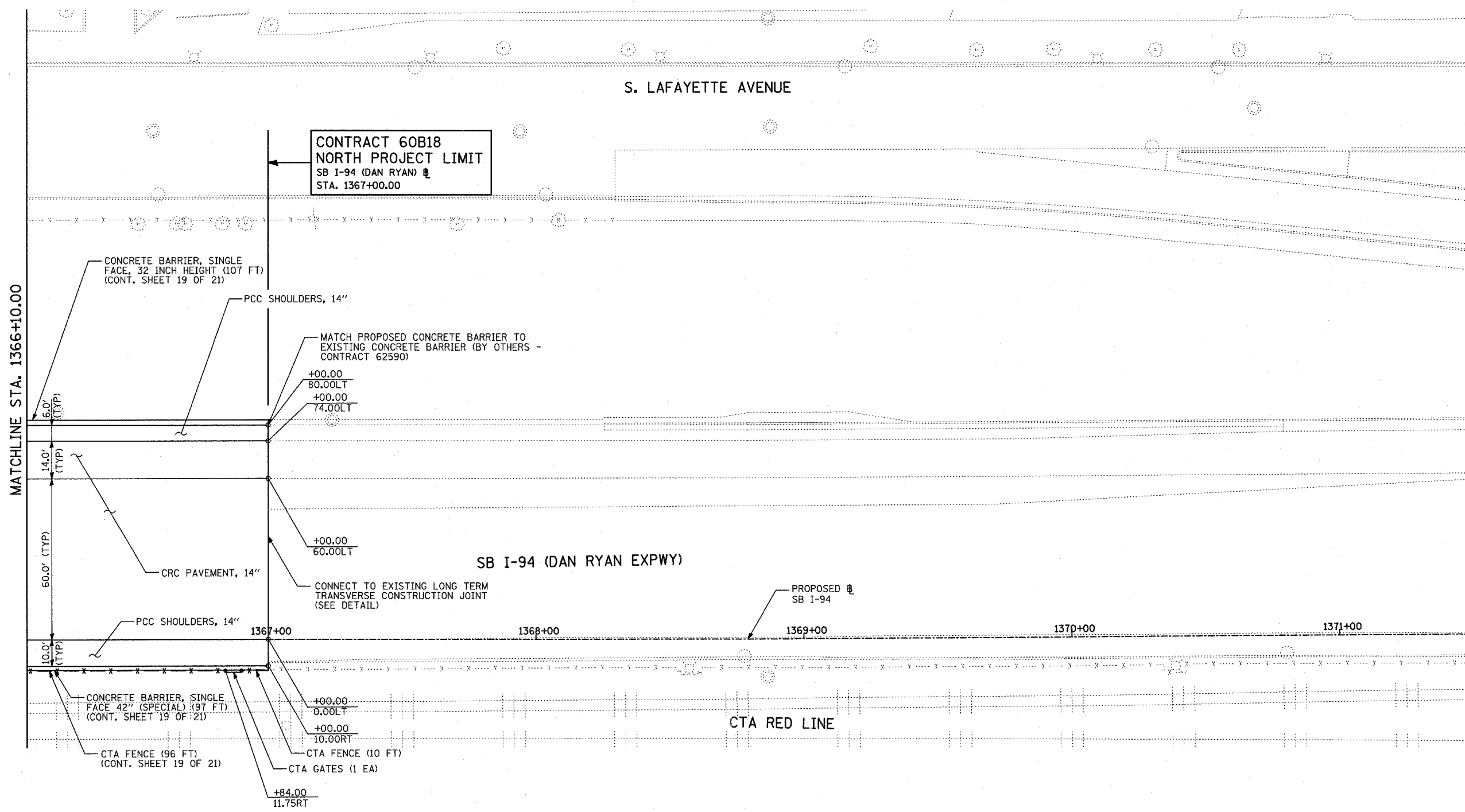
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 SB I-94 STA. 1366+10.00 TO 1367+00.00

SCALE: 1"=20'  
 DATE: MARCH 1, 2006

DRAWN BY: MPG  
 CHECKED BY: RTM

**TYLIN INTERNATIONAL**

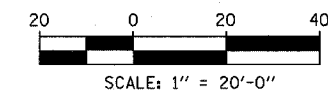


LOCATION MAP

**LEGEND:**

- |  |                                    |  |   |  |   |
|--|------------------------------------|--|---|--|---|
|  | PAVEMENT REMOVAL                   |  | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)             |  | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6>)  |
|  | PAVED SHOULDER REMOVAL             |  | COMB CONC CURB & GUTTER REMOVAL   |  | XXX.XX MAINLINE #   |
|  | BITUMINOUS SURFACE REMOVAL, 4"     |  | CHAIN LINK FENCE REMOVAL  |  | XX.XXRT STATION/OFFSET  |
|  | BITUMINOUS SURFACE REMOVAL, 1 1/2" |  | CONCRETE BARRIER/GUARDRAIL REMOVAL  |  | <b>PLAN NOTES:</b>  |
|  | BITUMINOUS SURFACE REMOVAL, 1 3/4" |  | SOIL BORING LOCATIONS   |  | - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |
|  |                                    |  | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |  | - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED                        |

TYLIN INTERNATIONAL



REVISIONS	
NAME	DATE

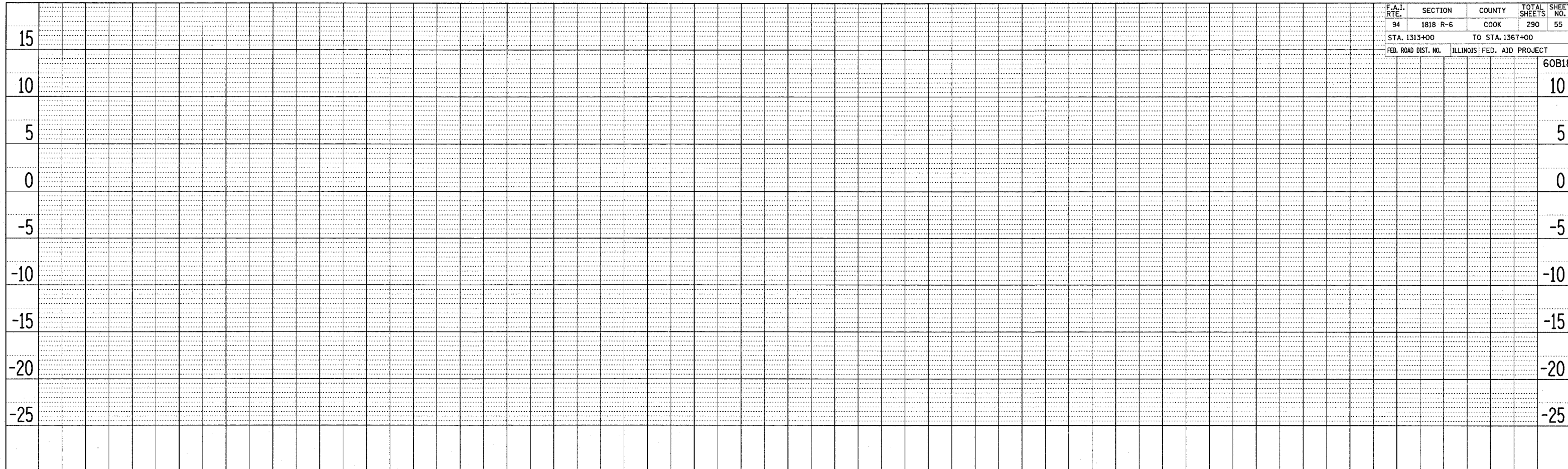
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**PROPOSED PLAN**  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 SB I-94 STA. 1366+10.00 TO 1367+00.00

SCALE: 1"=20'  
 DATE: MARCH 1, 2006

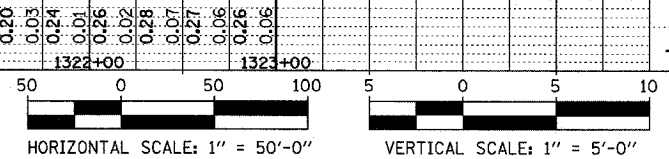
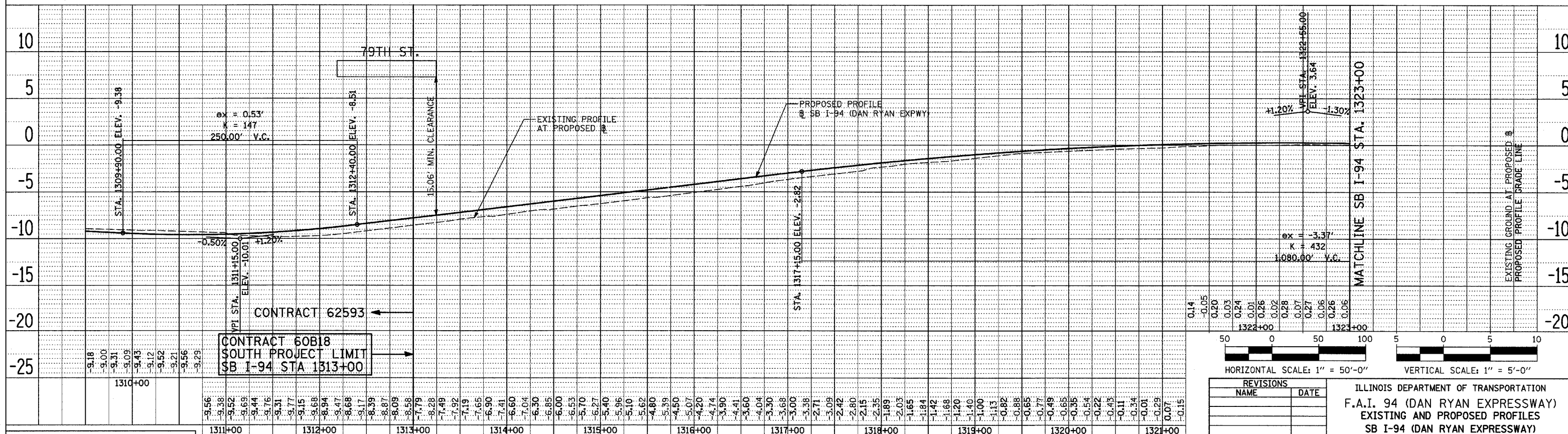
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	55
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
			60B18	

PLAN	BY	DATE
REVISIONS		
NOTED		
PLOTTED		
ALIGNMENT CHECKED		
AS BUILT CHECKED		
CADD FILE NAME		
NO.		



PLAN	BY	DATE
REVISIONS		
NOTED		
PLOTTED		
ALIGNMENT CHECKED		
AS BUILT CHECKED		
CADD FILE NAME		
NO.		



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PROFILES  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1295+00.00 TO 1323+00.00

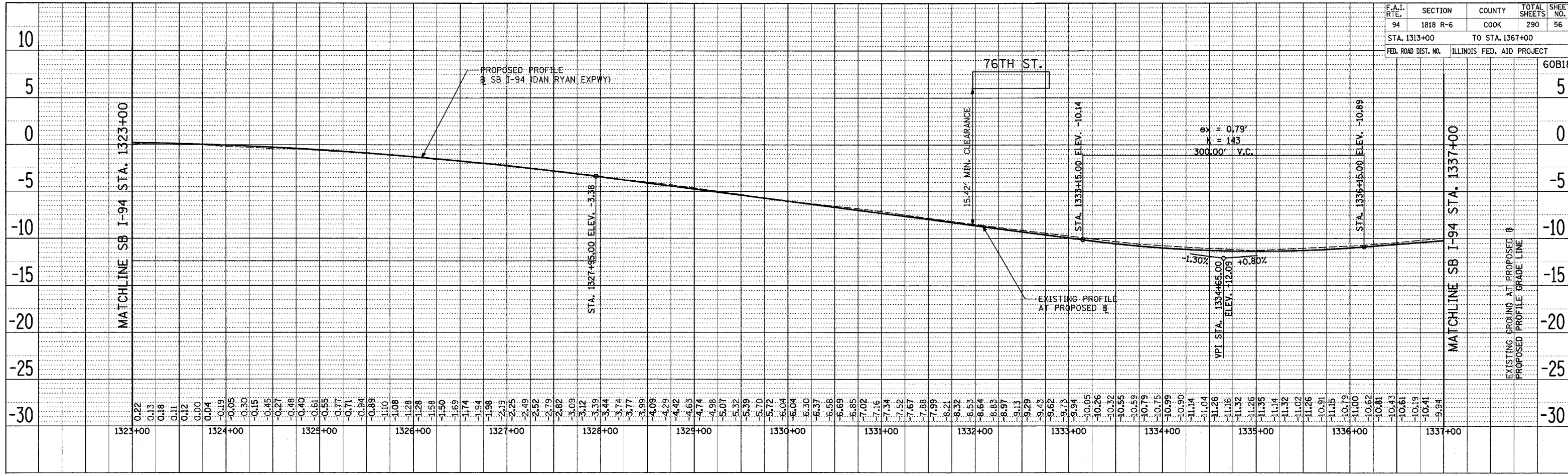
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 1"=5' VERT.  
 DATE: MARCH 1, 2006

DRAWN BY: JPM  
 CHECKED BY: MPG

**TYLINTERNATIONAL**

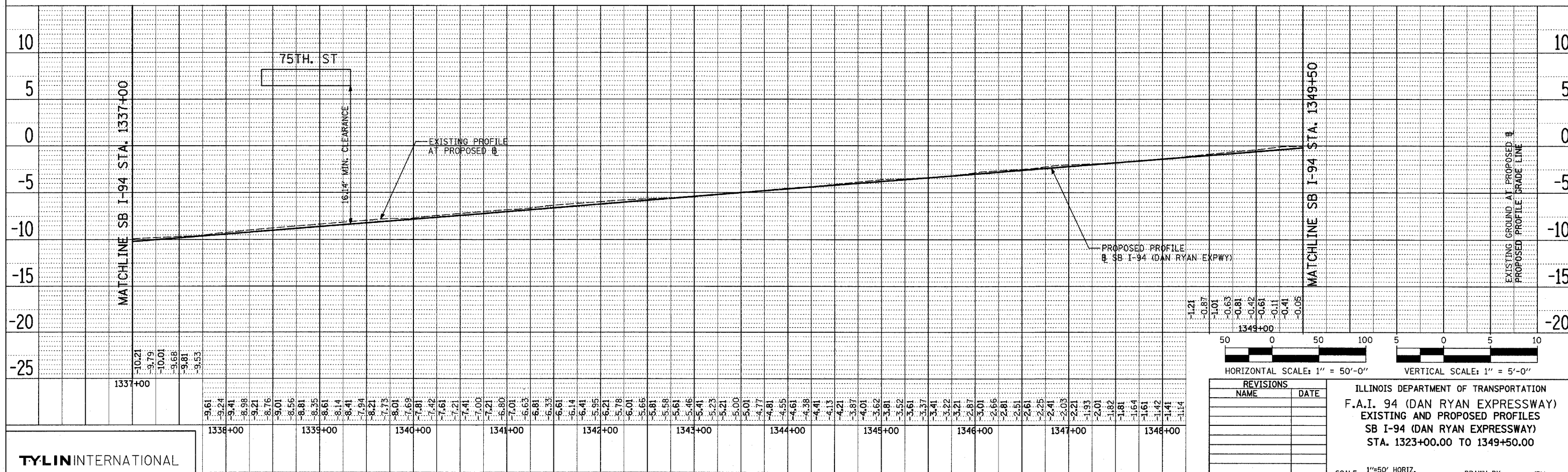
SB I-94 (DAN RYAN EXPRESSWAY)

DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 REVISIONS:  
 1. \_\_\_\_\_  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_  
 4. \_\_\_\_\_  
 5. \_\_\_\_\_

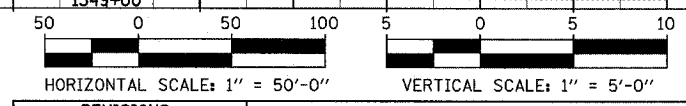


SB I-94 (DAN RYAN EXPRESSWAY)

DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 REVISIONS:  
 1. \_\_\_\_\_  
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 5. \_\_\_\_\_



SB I-94 (DAN RYAN EXPRESSWAY)



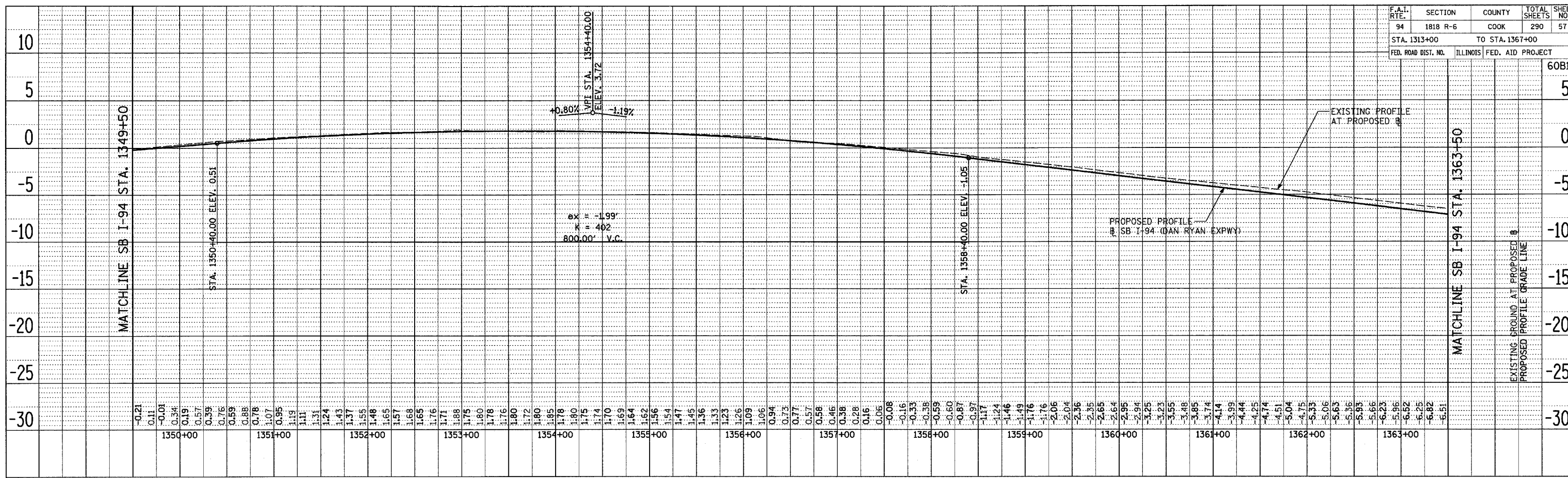
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PROFILES  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1323+00.00 TO 1349+50.00

SCALE: 1"=50' HORIZ.  
 1"=5' VERT.  
 DATE: MARCH 1, 2006  
 DRAWN BY: JPM  
 CHECKED BY: MPG

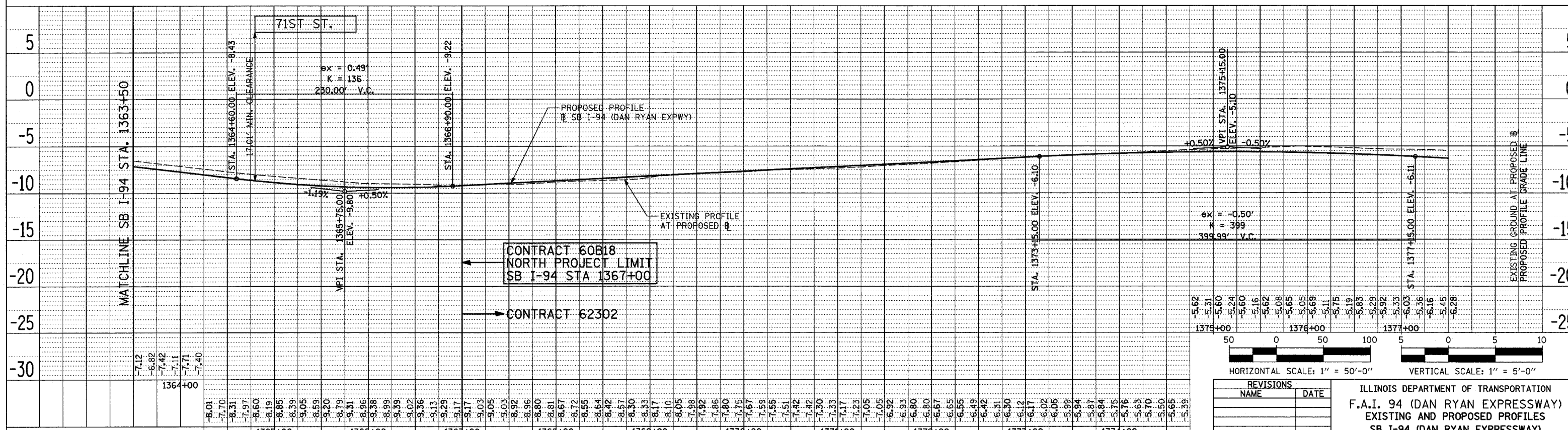
**TYLIN** INTERNATIONAL

DATE: \_\_\_\_\_  
BY: \_\_\_\_\_  
PROJECT: \_\_\_\_\_  
DRAWN BY: \_\_\_\_\_  
CHECKED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

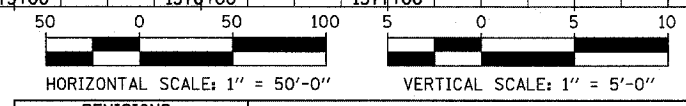


SB I-94 (DAN RYAN EXPRESSWAY)

DATE: \_\_\_\_\_  
BY: \_\_\_\_\_  
PROJECT: \_\_\_\_\_  
DRAWN BY: \_\_\_\_\_  
CHECKED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_



SB I-94 (DAN RYAN EXPRESSWAY)



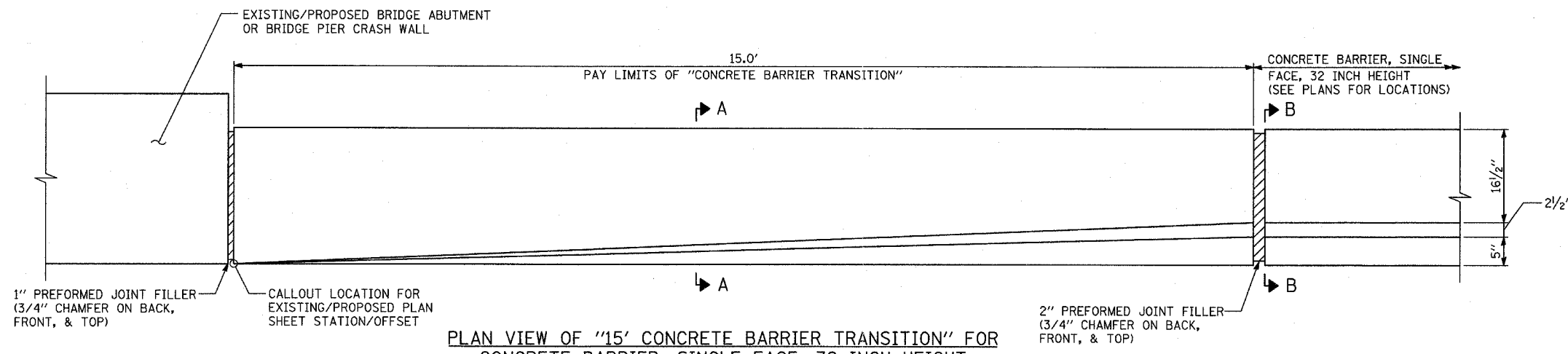
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PROFILES  
SB I-94 (DAN RYAN EXPRESSWAY)  
STA. 1349+50.00 TO 1367+00.00

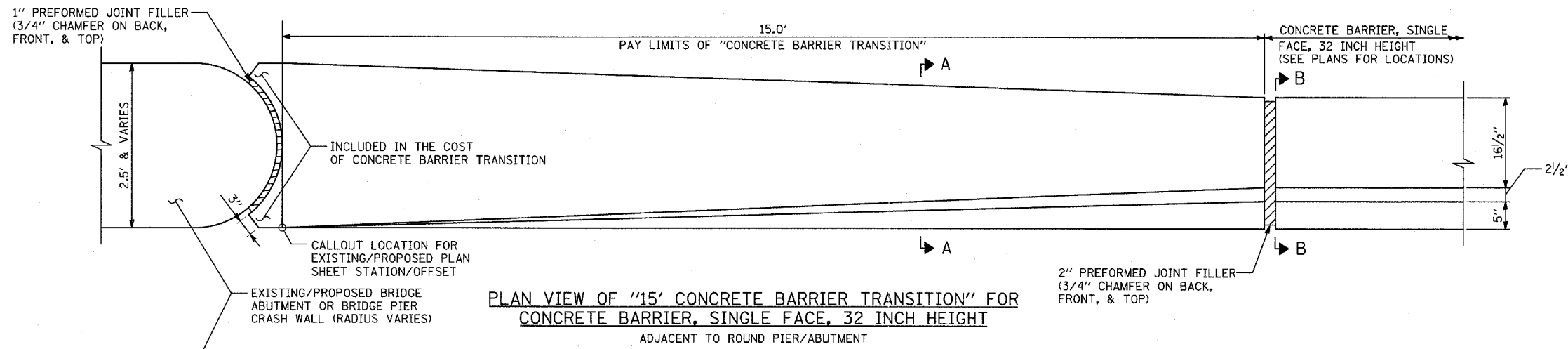
SCALE: 1"=50' HORIZ.  
1"=5' VERT.  
DATE: MARCH 1, 2006  
DRAWN BY: JPM  
CHECKED BY: MPG

TYLIN INTERNATIONAL

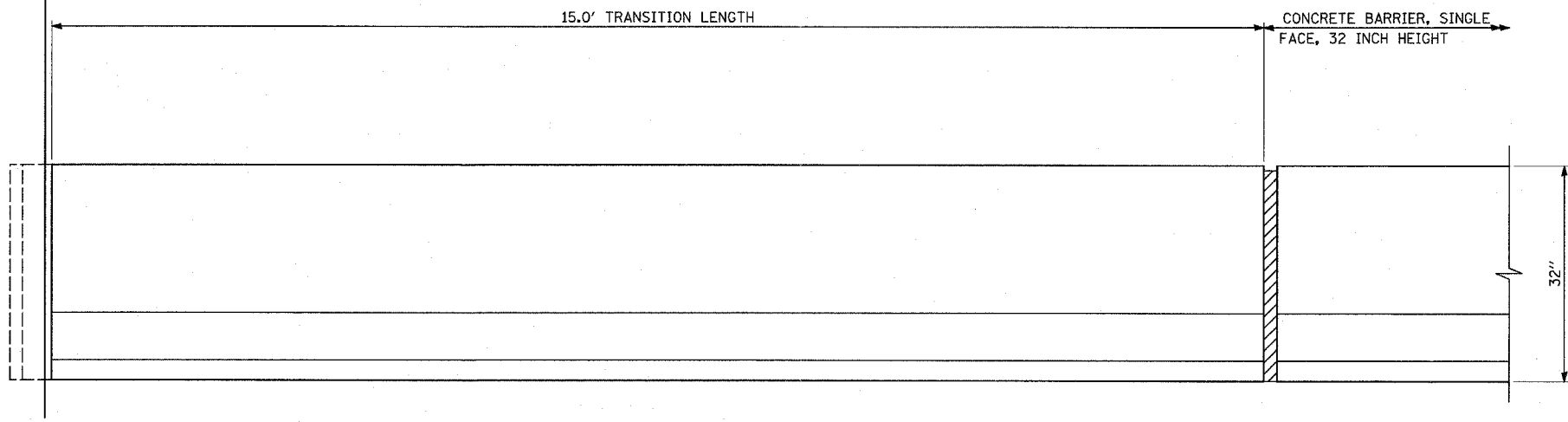




PLAN VIEW OF "15' CONCRETE BARRIER TRANSITION" FOR CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT ADJACENT TO SQUARE PIER/ABUTMENT



PLAN VIEW OF "15' CONCRETE BARRIER TRANSITION" FOR CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT ADJACENT TO ROUND PIER/ABUTMENT



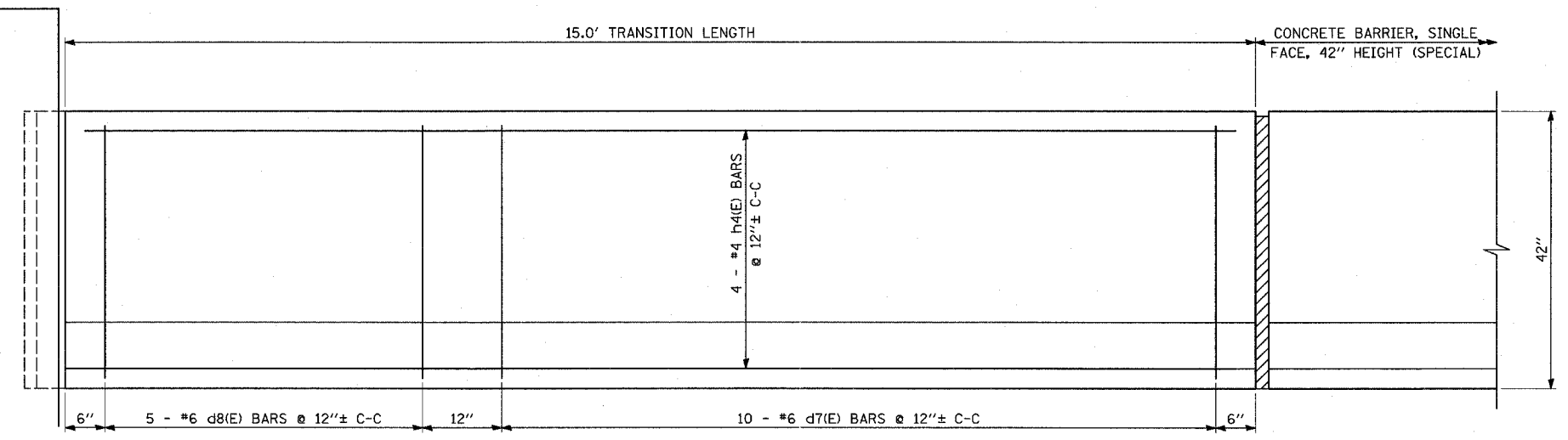
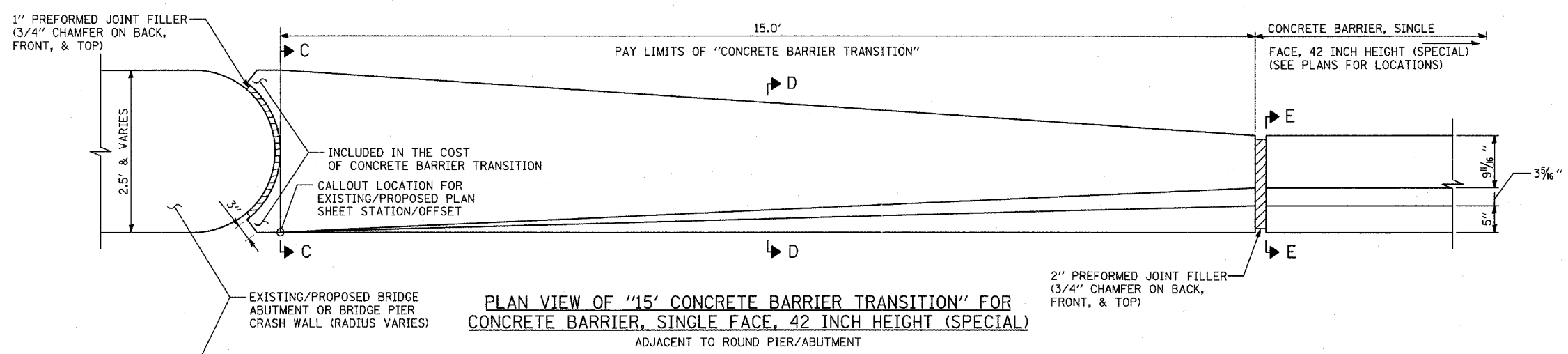
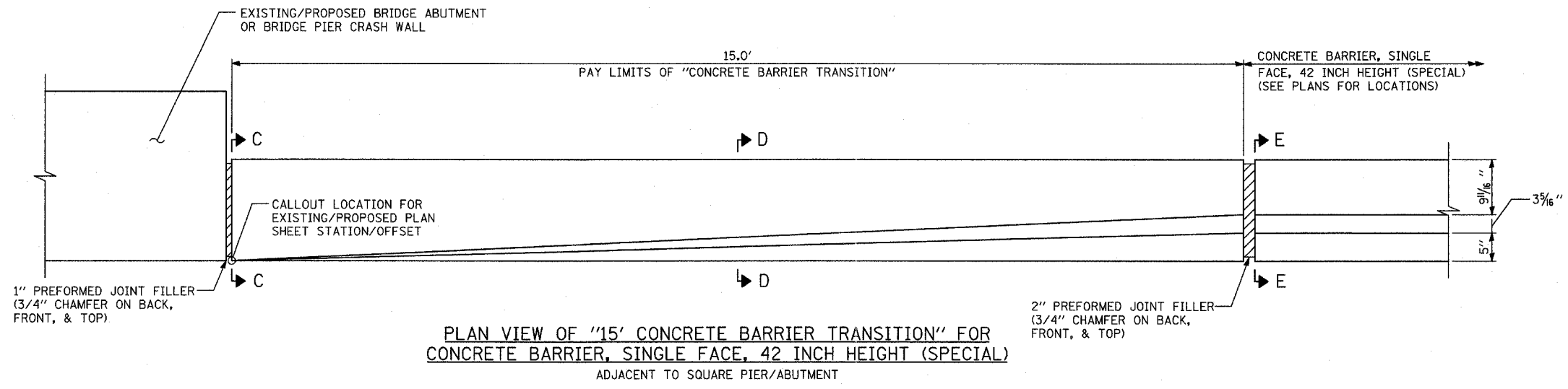
ELEVATION VIEW OF "15' CONCRETE BARRIER TRANSITION" FOR CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT

NOTE:  
ALL PREFORMED JOINT FILLER INCLUDED IN THE COST OF "CONCRETE BARRIER TRANSITION"

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
  
MISCELLANEOUS DETAILS  
CONCRETE BARRIER TRANSITION  
FOR CONCRETE BARRIER, 32 INCH HEIGHT  
  
SCALE: NONE  
DATE: MARCH 1, 2006  
  
DRAWN BY: MPG  
CHECKED BY: TGB



NOTE:  
ALL PREFORMED JOINT FILLER INCLUDED IN THE COST OF "CONCRETE BARRIER TRANSITION"

REVISIONS	
NAME	DATE

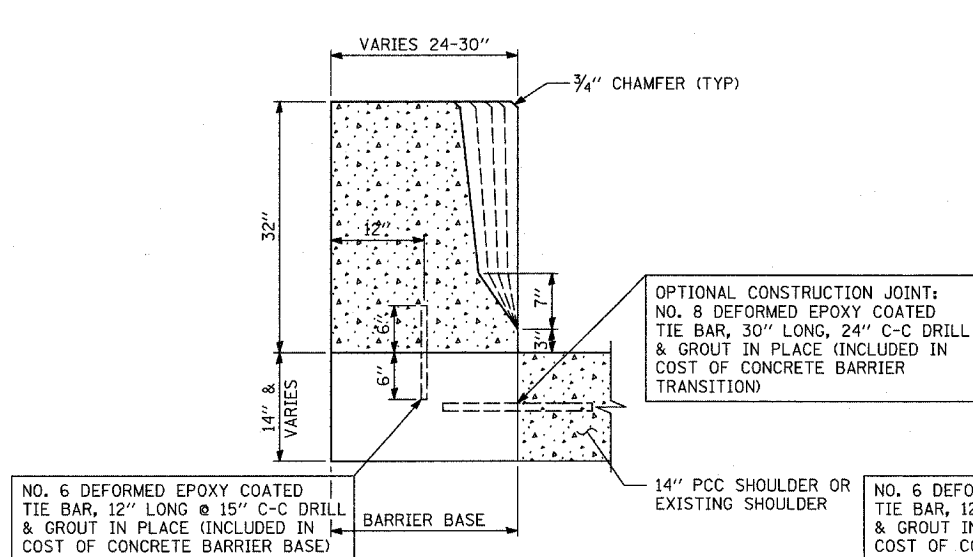
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS  
CONCRETE BARRIER TRANSITION FOR  
CONCRETE BARRIER, 42 INCH HEIGHT (SPECIAL)

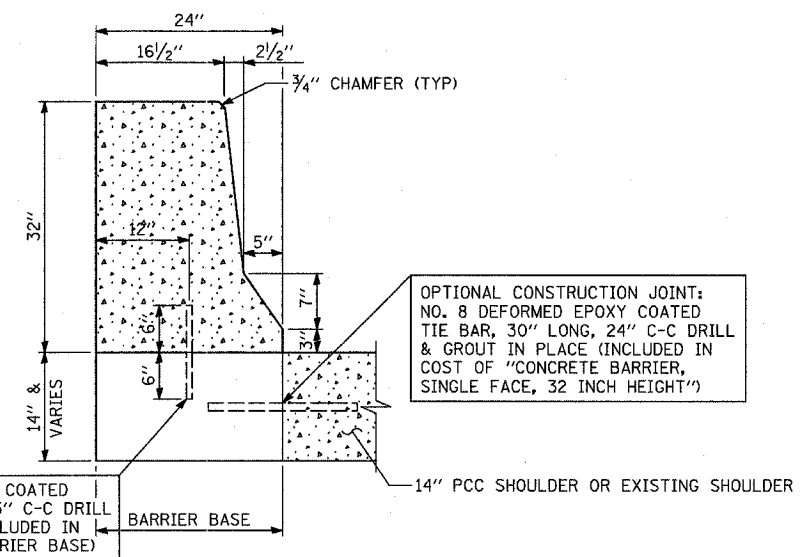
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DATE: MARCH 1, 2006

DRAWN BY: MPG  
CHECKED BY: TGB

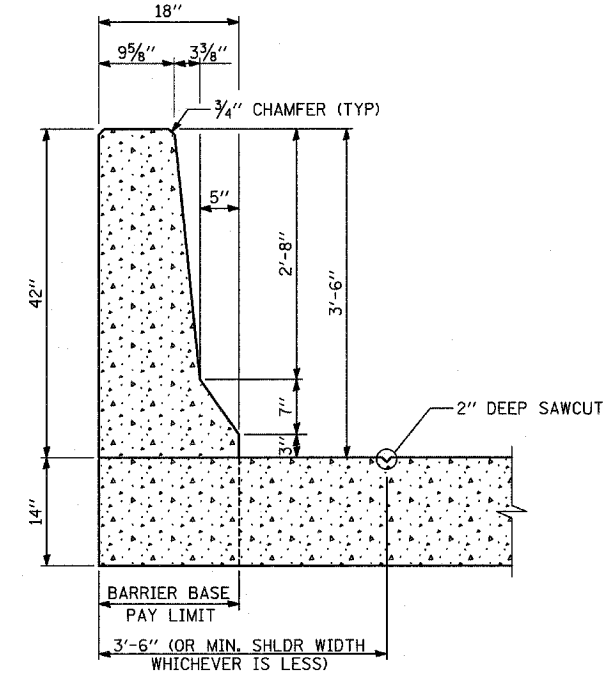
**TYLIN** INTERNATIONAL



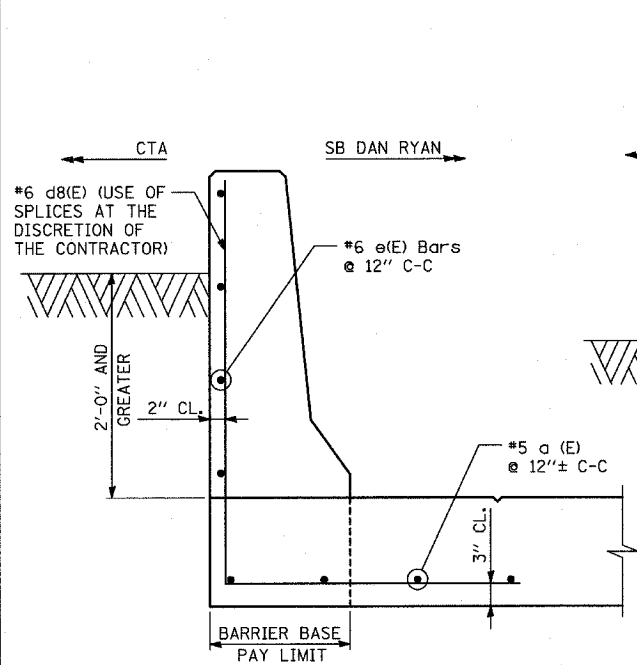
**SECTION A-A**  
**15' CONCRETE BARRIER TRANSITION**  
 CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT



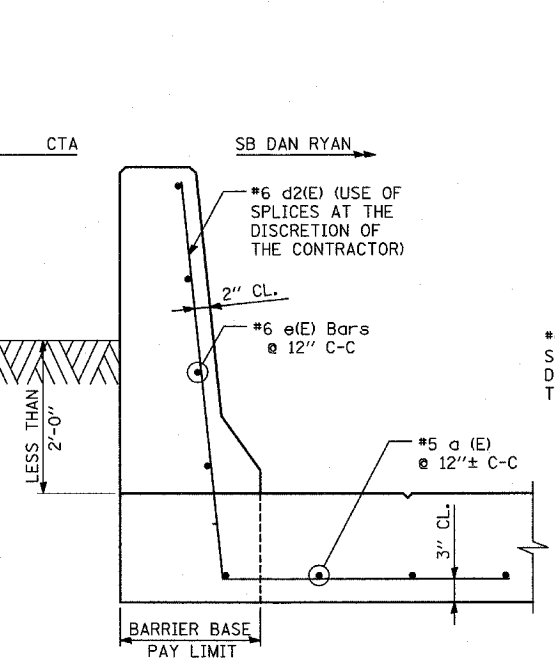
**SECTION B-B**  
**CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT**  
 & CONCRETE BARRIER BASE



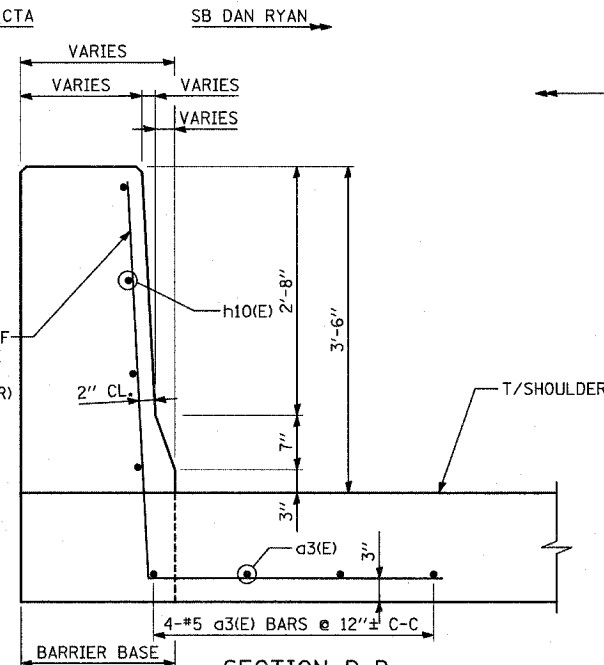
**SECTION E-E**  
**CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT**  
**(SPECIAL) DIMENSIONS**  
 SEE BELOW FOR REINFORCEMENT OPTIONS



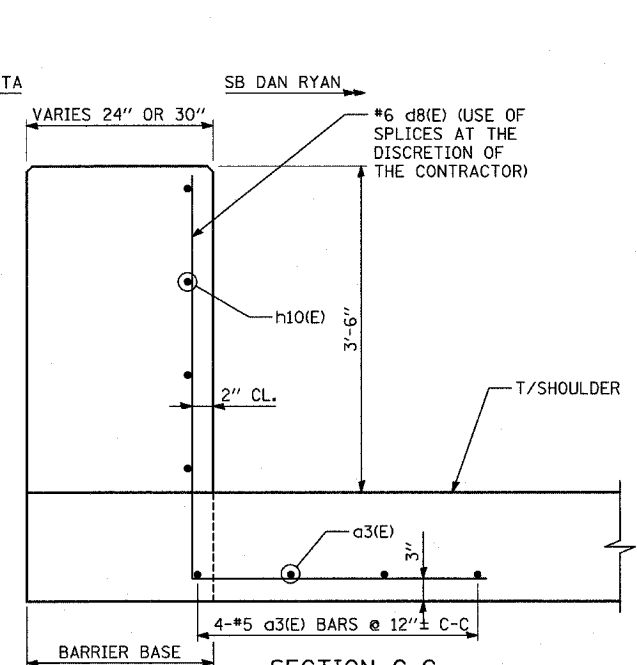
**REINFORCEMENT #1**  
 RETAINED HEIGHT GREATER THAN 2'-0"  
**SECTION E-E: CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)**



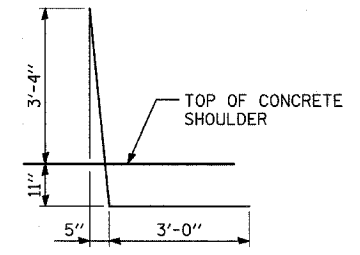
**REINFORCEMENT #2**  
 RETAINED HEIGHT LESS THAN 2'-0"



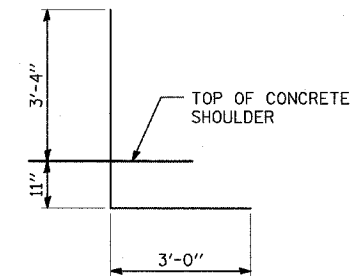
**SECTION D-D**  
**15' CONCRETE BARRIER TRANSITION**  
 CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)



**SECTION C-C**  
**15' CONCRETE BARRIER TRANSITION**  
 CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)



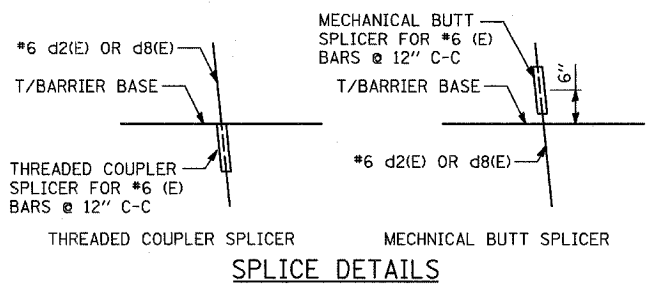
**d2(E) BAR**



**d8(E) BAR**

**NOTES:**

1. BAR SPLICERS SHALL BE CAPABLE OF DEVELOPING A MINIMUM OF 125% OF THE YIELD STRENGTH OF A #6 BAR.
2. ALL REINFORCING BARS, BAR SPLICERS, TIE BARS AND DOWEL BARS SHALL BE SEATED IN THE FINAL POSITION PRIOR TO THE CONCRETE OPERATIONS. BARS CANNOT BE MUCKED INTO PLACE.
3. MAINTAIN SLOPE OF FACE AS SHOWN ON DETAILS.
4. ALL REINFORCING BARS, BAR SPLICERS, TIE BARS AND DOWEL BARS SHALL BE INCLUDED IN THE COST OF "CONCRETE BARRIER, SINGLE FACE" OF THE TYPE SPECIFIED



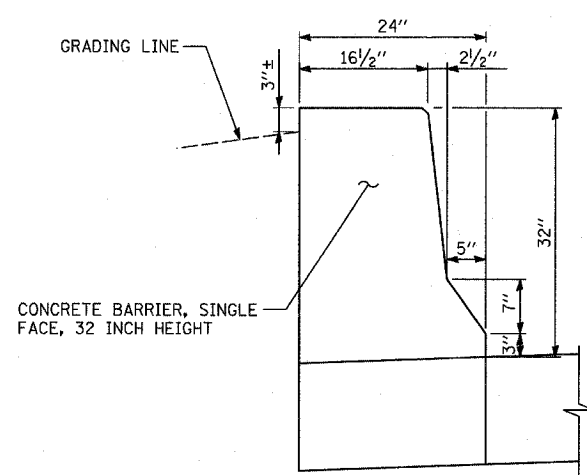
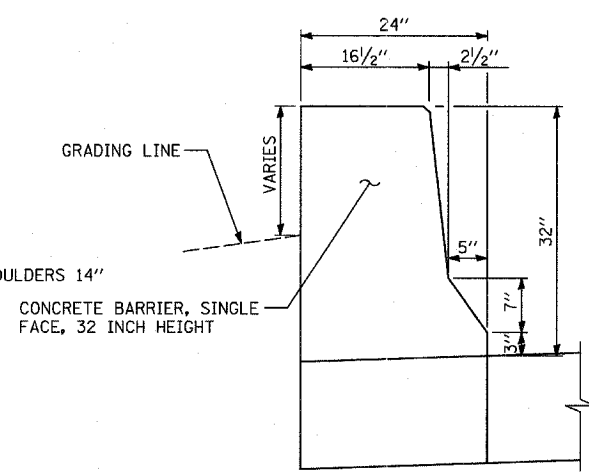
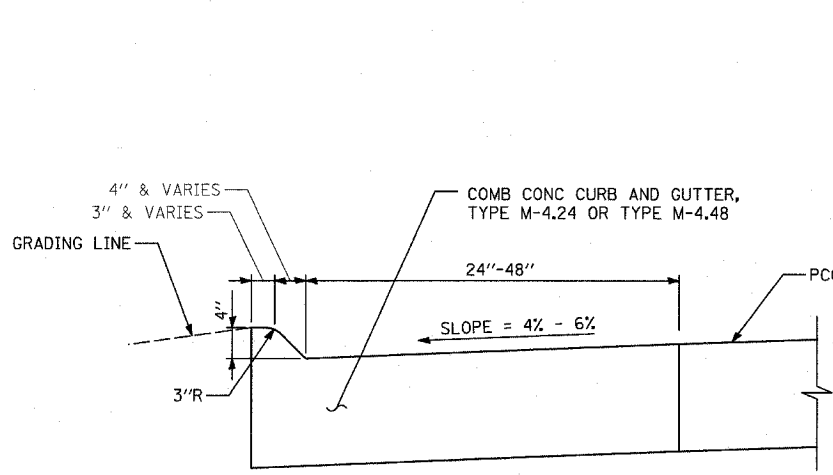
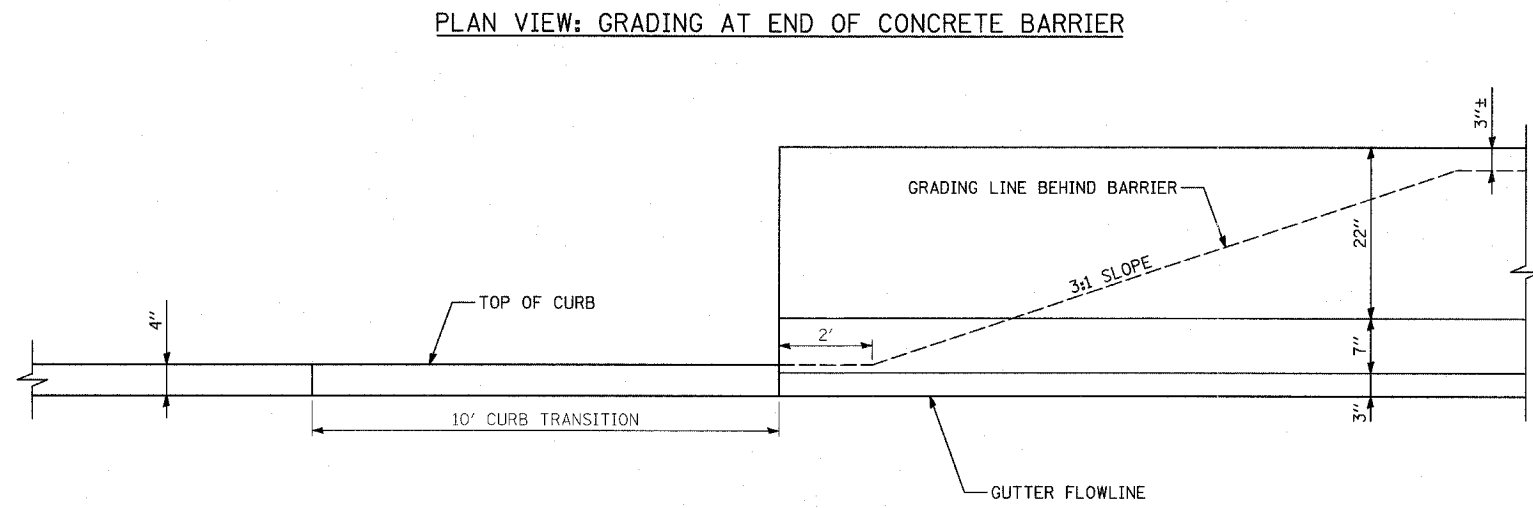
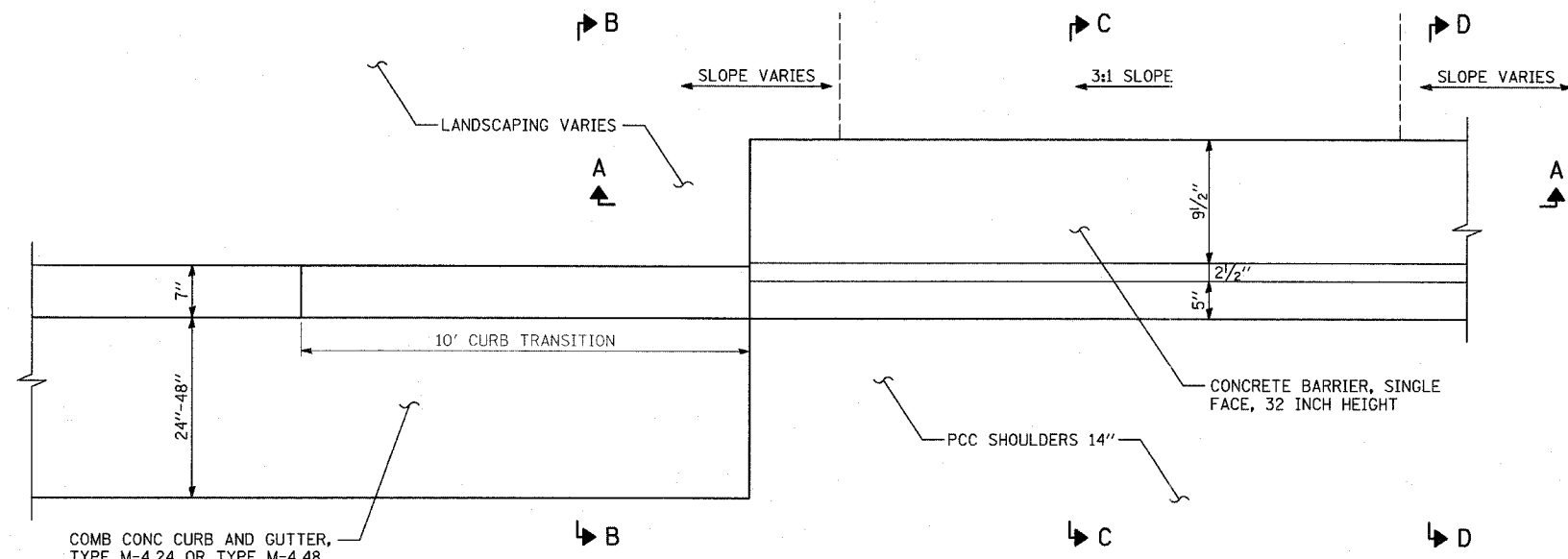
**SPlice DETAILS**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS  
 CONCRETE BARRIER TRANSITION  
 TYPICAL SECTIONS

SCALE: NONE  
 DATE: MARCH 1, 2006  
 DRAWN BY: MFG  
 CHECKED BY: TGB



- NOTES:**
- SEE DRAINAGE AND UTILITY PLANS, CROSS-SECTIONS, AND LANDSCAPING PLANS FOR GRADING LIMITS AND DETAILS.

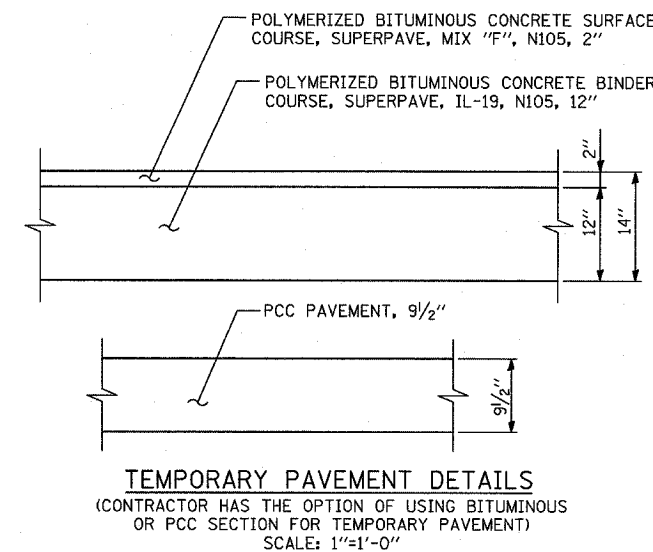
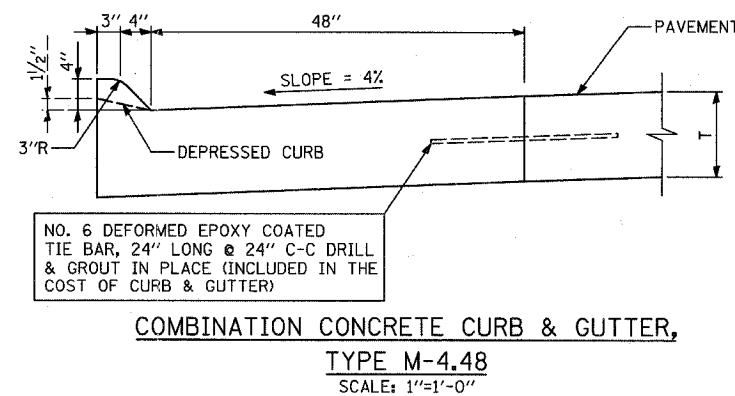
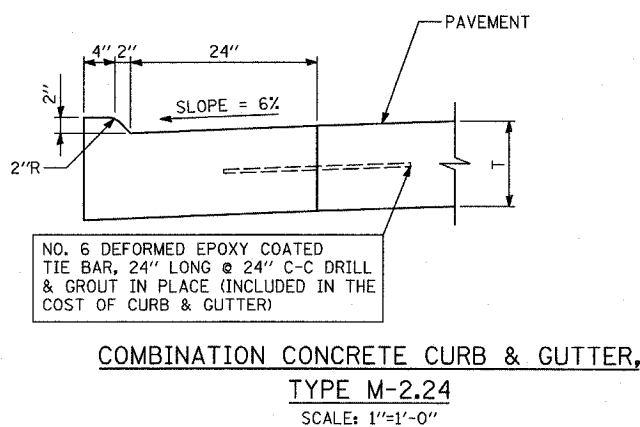
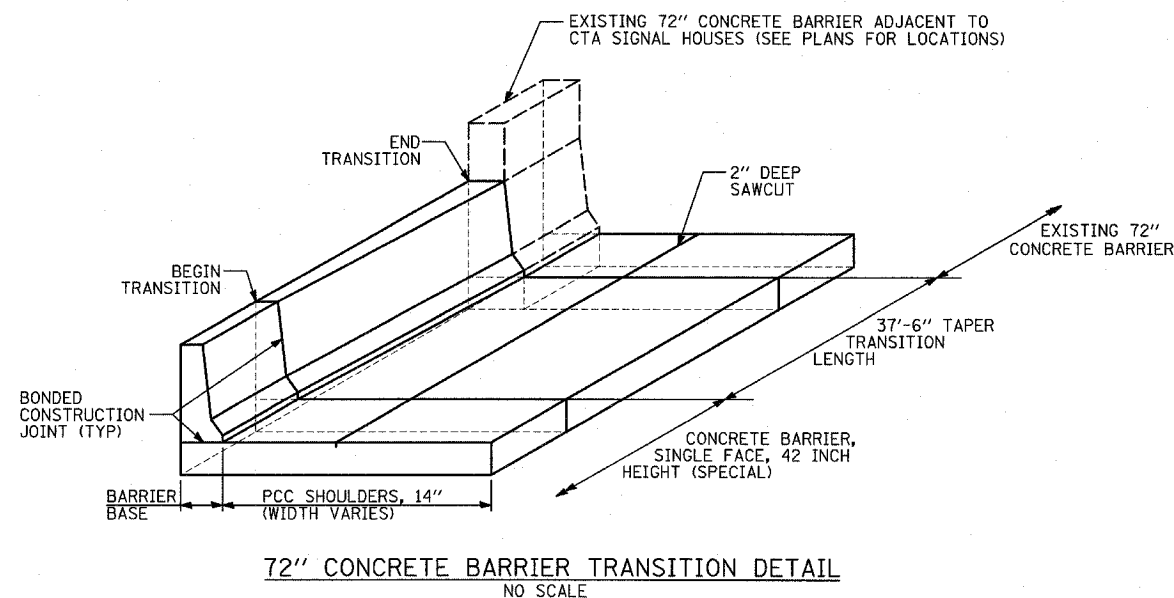
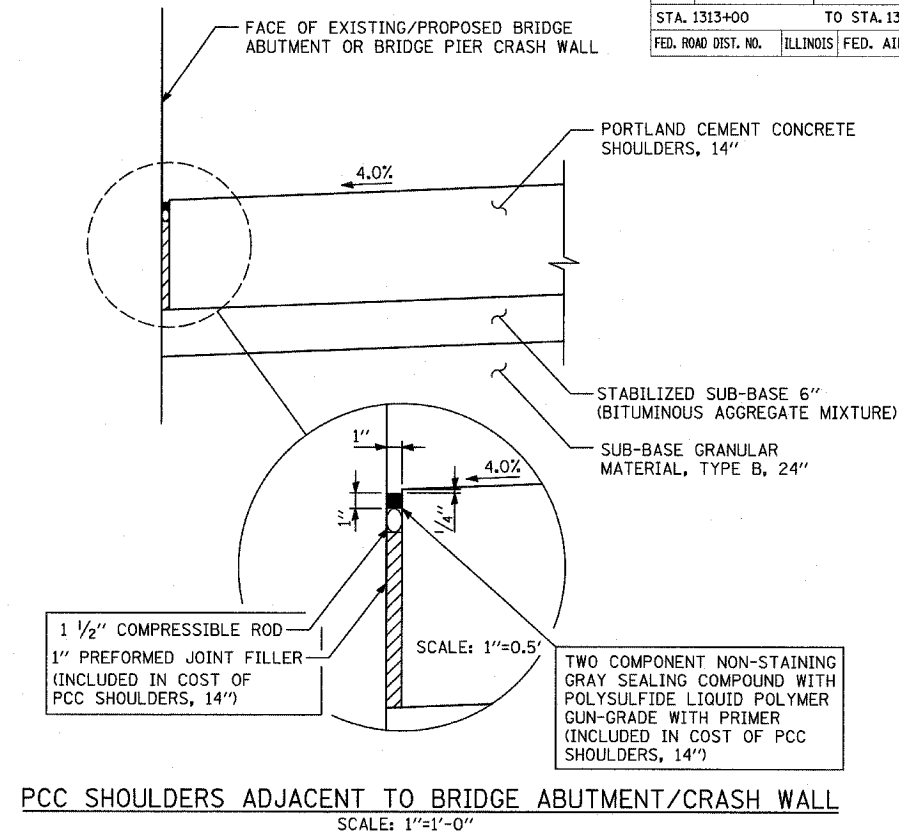
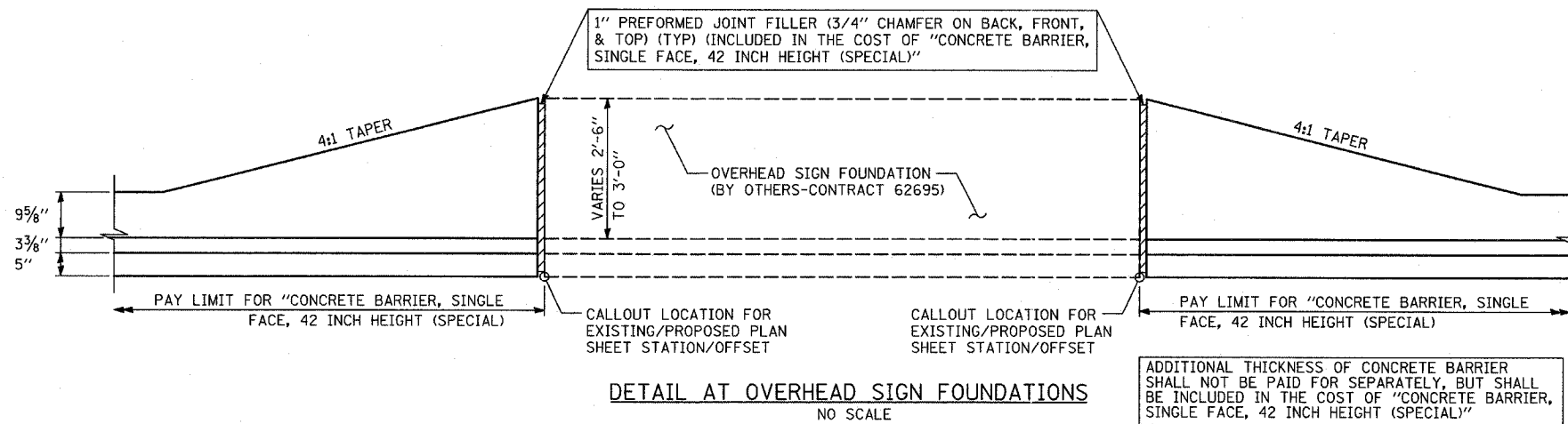
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS  
 GRADING DETAIL AT END  
 OF CONCRETE BARRIER

SCALE: NONE  
 DATE: MARCH 1, 2006

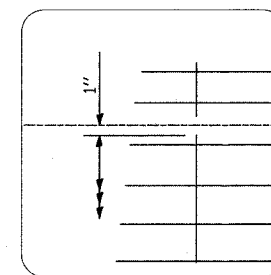
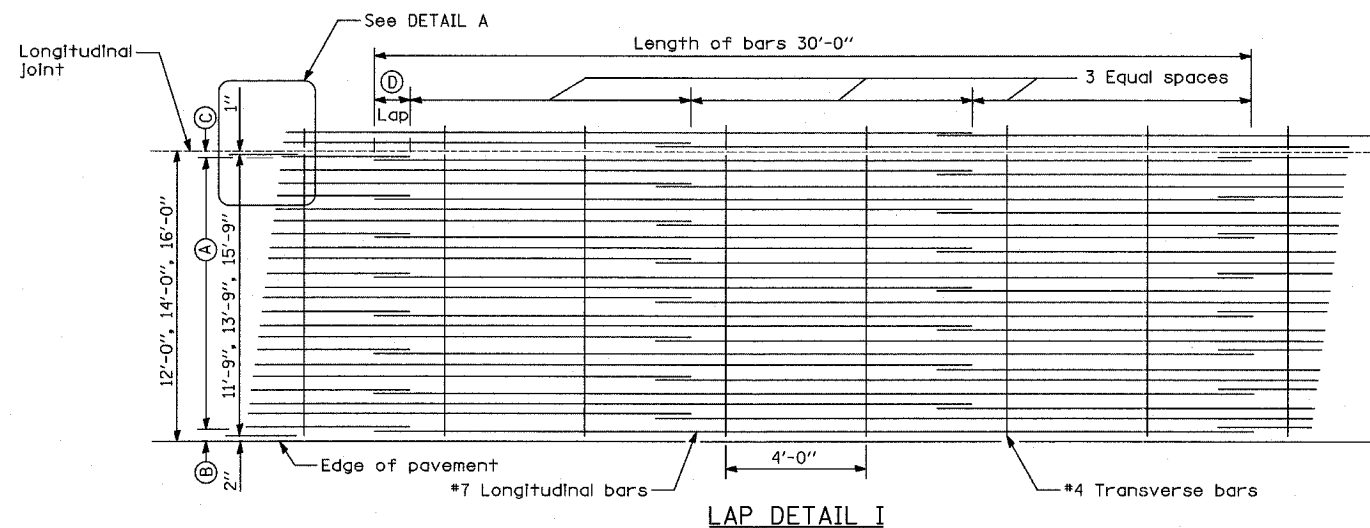
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 CHECKED BY: TGB



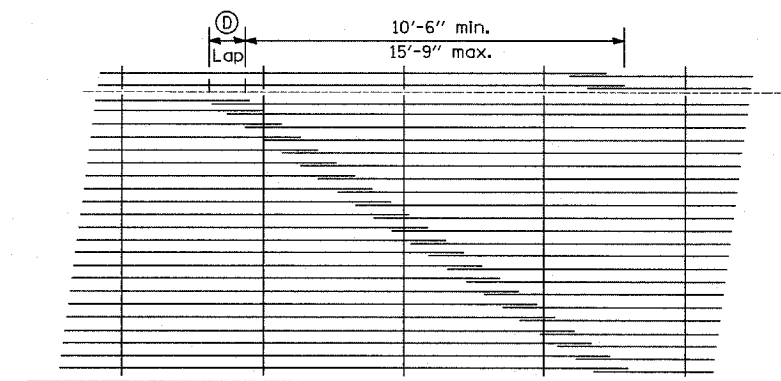
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**MISCELLANEOUS DETAILS**  
PLAN AND TYPICAL SECTION DETAILS

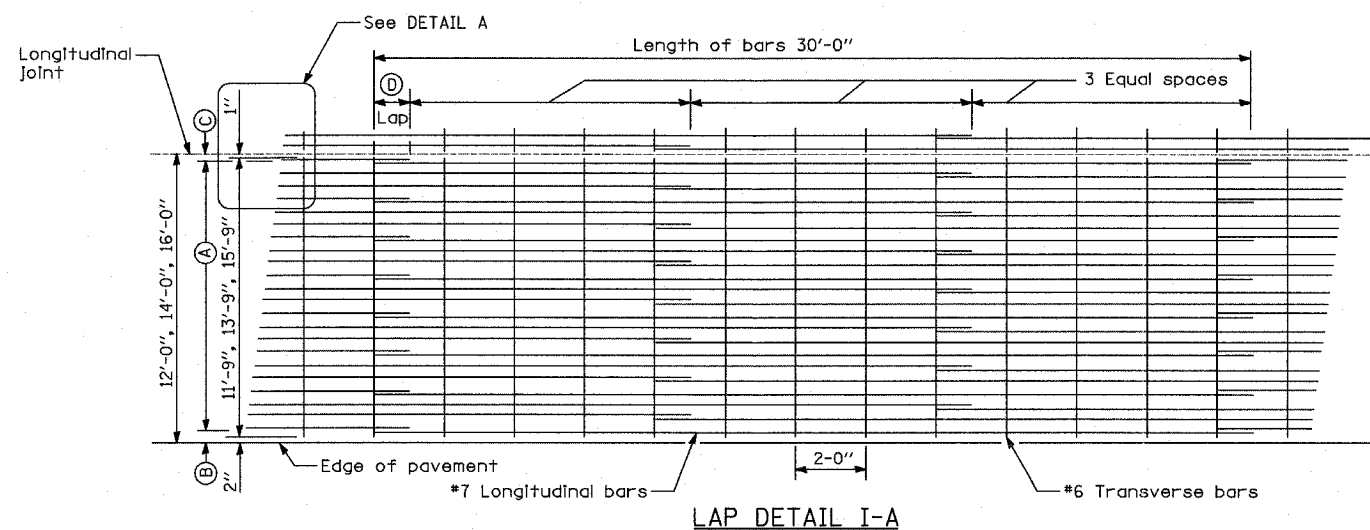
SCALE: AS SHOWN  
DATE: MARCH 1, 2006  
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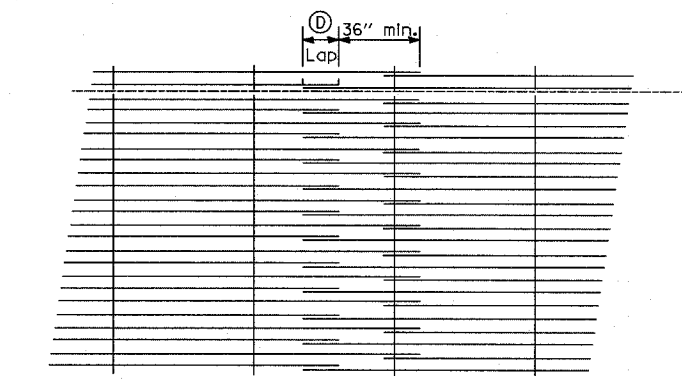
DETAIL A



LAP DETAIL II



ADDITIONAL REINFORCEMENT IN GORE AREAS - SEE PAVEMENT JOINTING AND ELEVATION PLANS FOR LOCATIONS



LAP DETAIL III

GENERAL NOTES

1. THE PAVEMENT REINFORCEMENT SHALL BE 4.5" FROM THE TOP OF PAVEMENT.
2. EXCEPT AS NOTED OR SHOWN, THE DIMENSIONS AND NOTES SPECIFIED FOR LAP DETAIL I ARE TYPICAL FOR LAP DETAIL II AND III.
3. THE (B) DIMENSION AND THE DISTANCE FROM THE END OF THE TRANSVERSE BAR TO THE EDGE OF PAVEMENT MAY BE INCREASED BY 1" FOR SLIP FORM PAVING.

Pavement Width	Bar Size	Pavement Thickness	(A) (Approx. Spacing)	(B)	(C)	(D)
12 feet	#7	14"	23 spaces (24 bars) @ 6"	3 1/2"	3"	26"
14 feet	#7	14"	27 spaces (28 bars) @ 6"	3 1/2"	3"	26"
16 feet	#7	14"	31 spaces (32 bars) @ 6"	3 1/2"	3"	26"

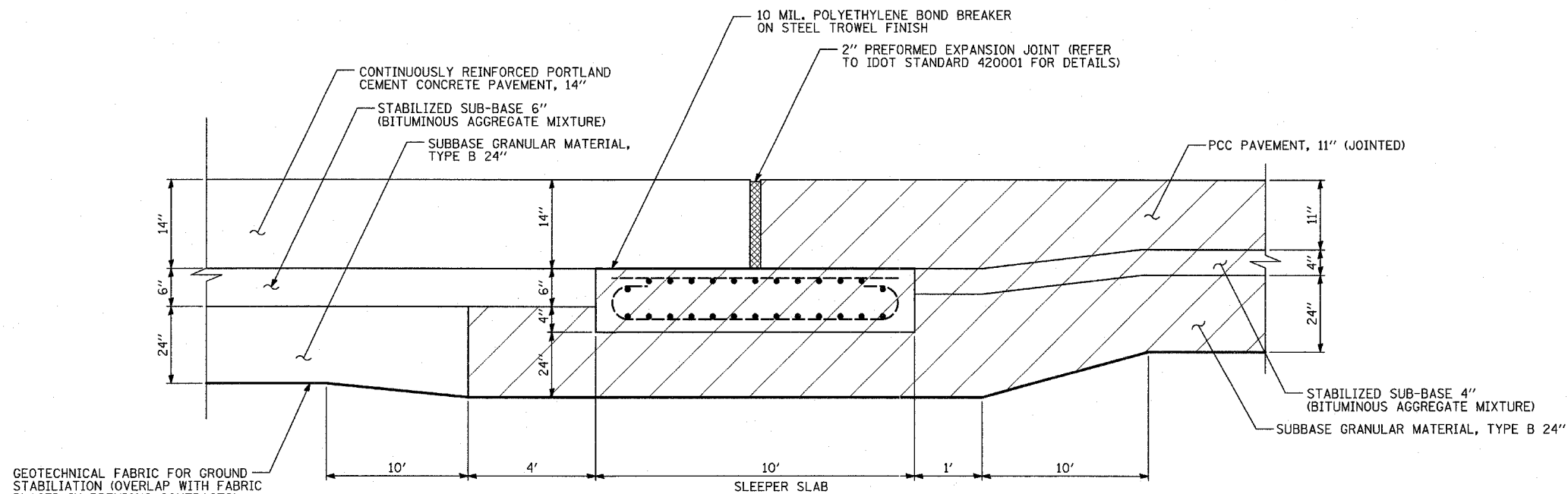
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS  
EXTENDED LANE REINFORCEMENT FOR  
CONTINUOUSLY REINFORCED PCC PAVEMENT

SCALE: NONE  
DATE: MARCH 1, 2006

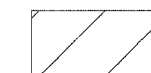
DRAWN BY: CTE, MPG  
CHECKED BY: TGB



**RAMP TERMINAL DETAILS AT EXISTING SLEEPER SLAB**

**NOTES:**

1. THE THICKENED EDGE OF THE SUB-BASE SHALL BE INCLUDED IN THE COST OF "SUB-BASE GRANULAR MATERIAL, TYPE B 24"



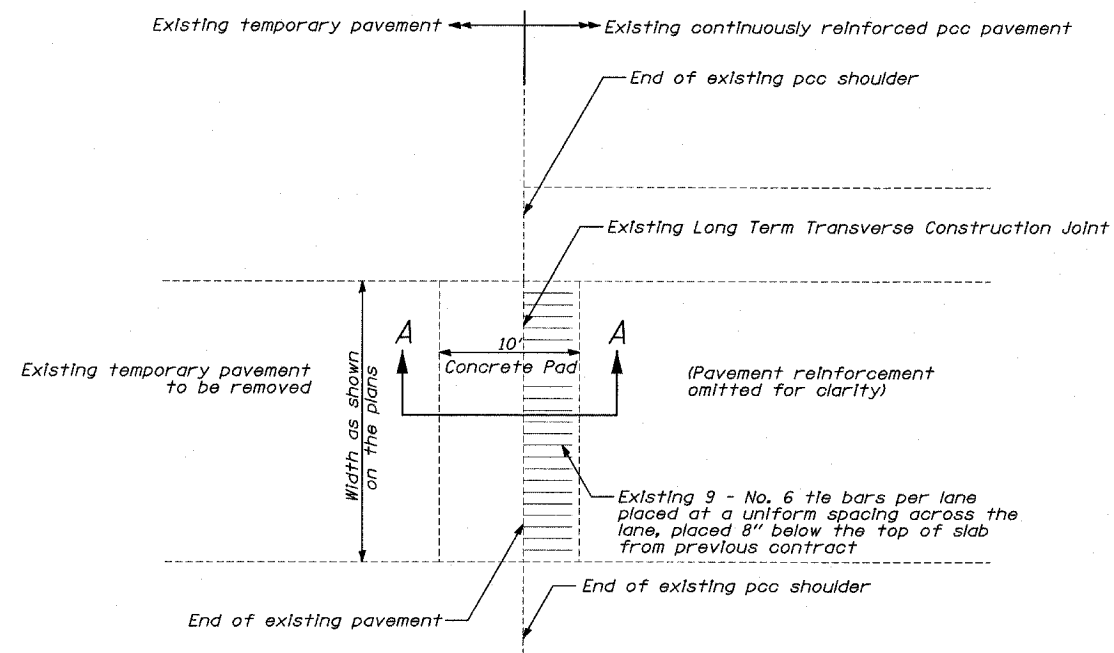
EXISTING PAVEMENT ITEMS CONSTRUCTED IN PREVIOUS CONTRACTS.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

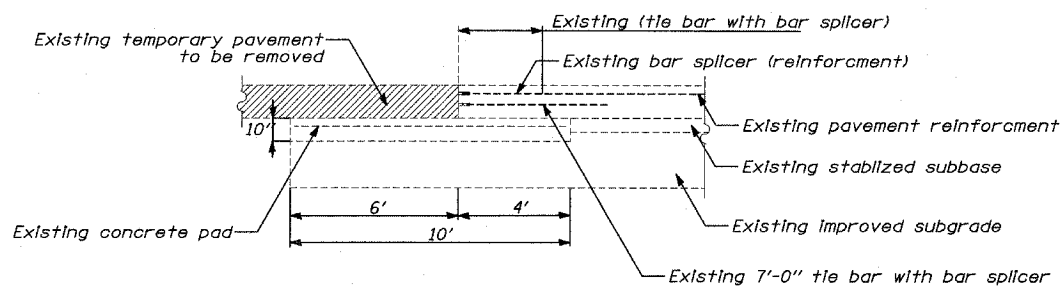
MISCELLANEOUS DETAILS:  
MISCELLANEOUS PAVEMENT ELEVATION  
AND JOINTING DETAILS

SCALE: NONE  
DATE: MARCH 1, 2006  
DRAWN BY: MPG  
CHECKED BY: TGB

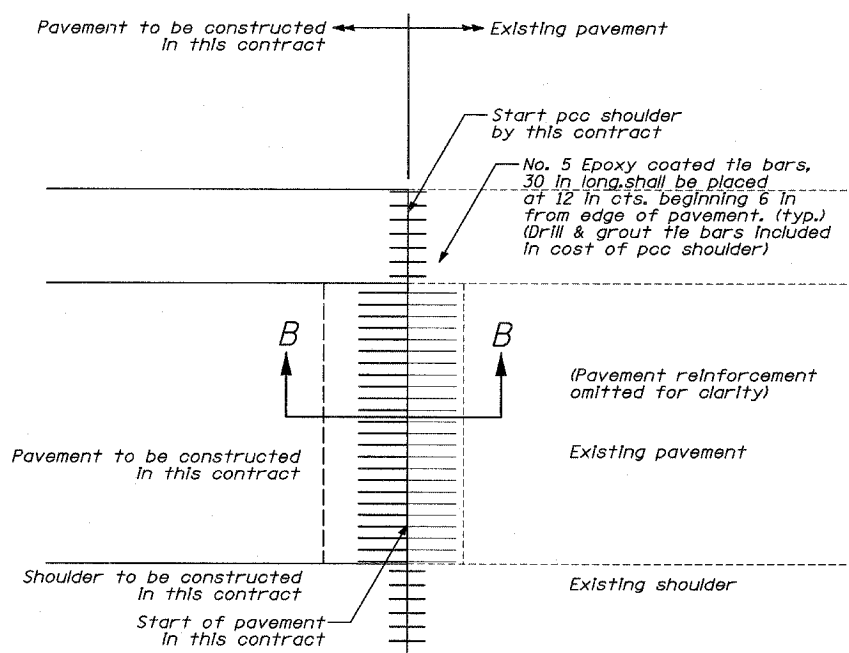


**PLAN**

(EXISTING LONG TERM TRANSVERSE CONSTRUCTION JOINT)

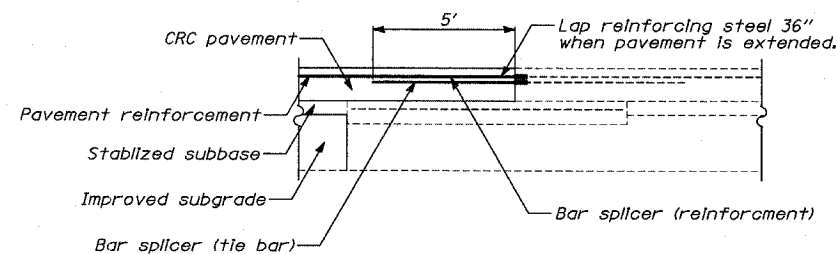


**LONG TERM TRANSVERSE CONSTRUCTION JOINT SECTION A-A**

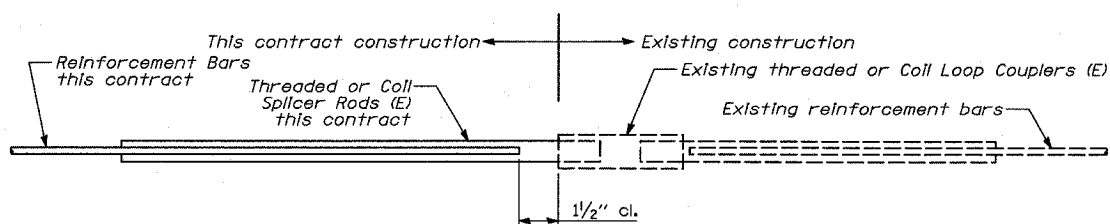


**PLAN**

(CONNECTION TO EXISTING LONG TERM TRANSVERSE CONSTRUCTION JOINT)



**LONG TERM TRANSVERSE CONSTRUCTION JOINT SECTION B-B**



**BAR SPLICER ASSEMBLY DETAIL**

(E) : Indicates epoxy coating.



**NOTES**

1. This detail shows connection of proposed CRC pavement to existing pavement at an existing long term transverse construction joint.
2. Bar splicer assemblies shall be of an IDOT approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
3. Bar splicers shall be of the "coupler" type, and shall not have flanges.
4. Splicer rods shall be of minimum 60 ksi yield strength, threaded or colled full length.
5. All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
6. Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
7. Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- A. Minimum Capacity (Tension in ksi) = 1.25 x fy x A(t)
- B. Minimum Pull-out Strength (Tension in ksi) = 1.25 x fs (allow) x A(t)

Where:

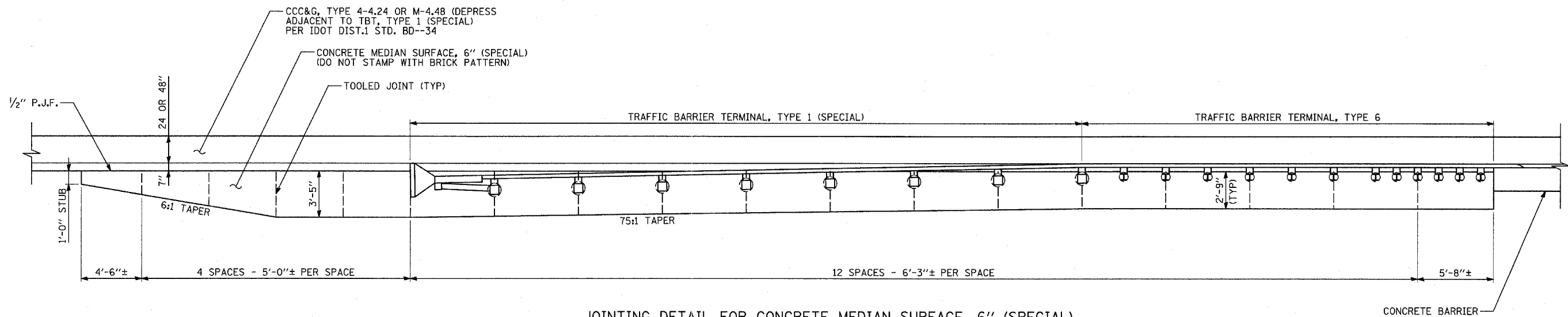
- fy = Yield strength of lapped reinforcement bars in ksi.
- fs(allow) = Allowable tensile stress in lapped reinforcement bars in ksi (Service Load)
- A(t) = Tensile stress area of lapped reinforcement bars (in<sup>2</sup>).
- \* = 28 day concrete

Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	STRENGTH REQUIREMENTS	
		Min. Capacity (kips) tension	Min. Pull-Out Strength (kips) tension
#5	2'-0"	23.0	9.2
#6	2'-7"	33.1	13.3
#7	3'-5"	45.1	18.0
#8	4'-6"	58.9	23.6
#9	5'-9"	75.0	30.0

8. Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted.
9. Connection to long term transverse construction joint work includes the installation of the bar splicers, payment for this work will be included in the cost of CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT, 14". Tie bars to be drilled and grouted shall not be paid for separately but included in the cost of PORTLAND CEMENT CONCRETE SHOULDERS, 14".

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NAME	DATE



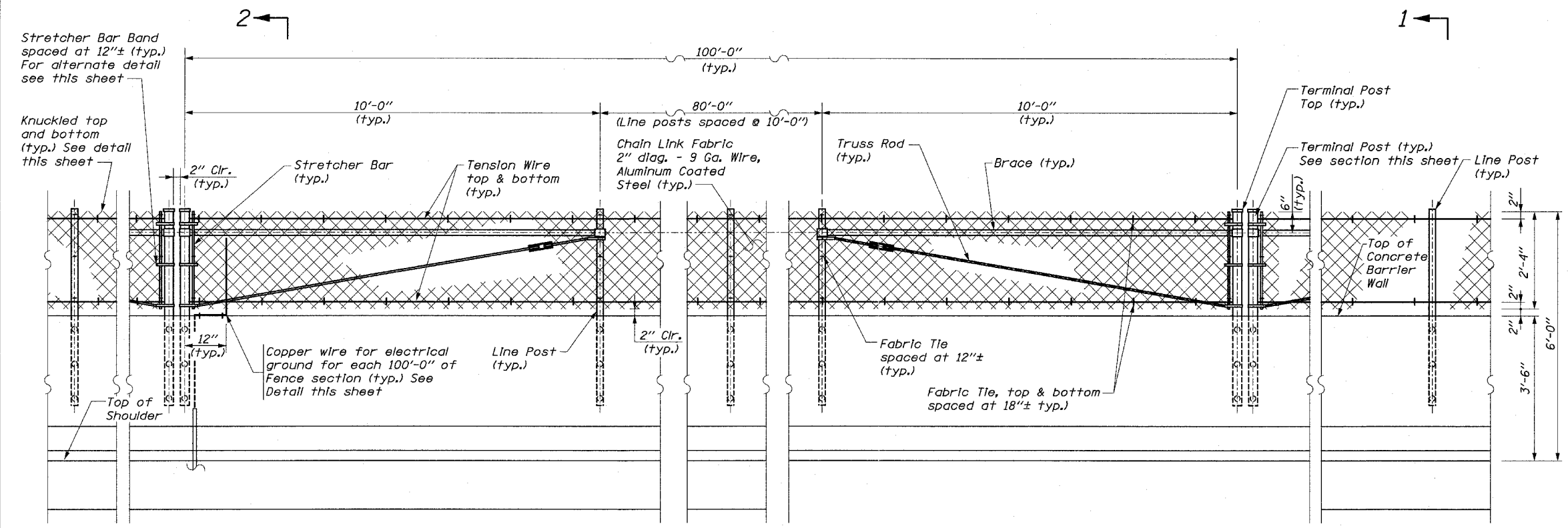


JOINTING DETAIL FOR CONCRETE MEDIAN SURFACE, 6" (SPECIAL)  
AT TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) AND TYPE 6

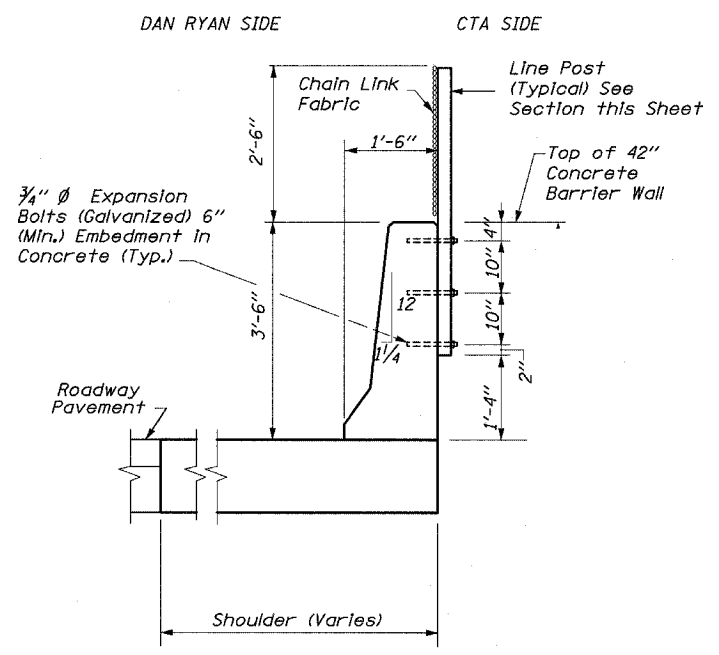
**TYLIN** INTERNATIONAL

REVISIONS	
NAME	DATE

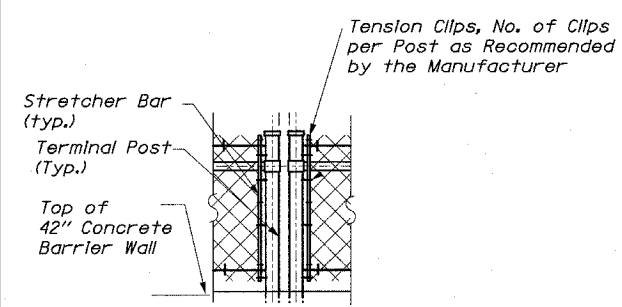
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 MISCELLANEOUS DETAILS  
 PAVEMENT JOINTING DETAILS  
 FOR CONCRETE MEDIAN SURFACE, 6" (SPECIAL)  
 AT TRAFFIC BARRIER TERMINALS  
 SCALE: NONE DRAWN BY: MPG  
 DATE: MARCH 1, 2006 CHECKED BY: TGB



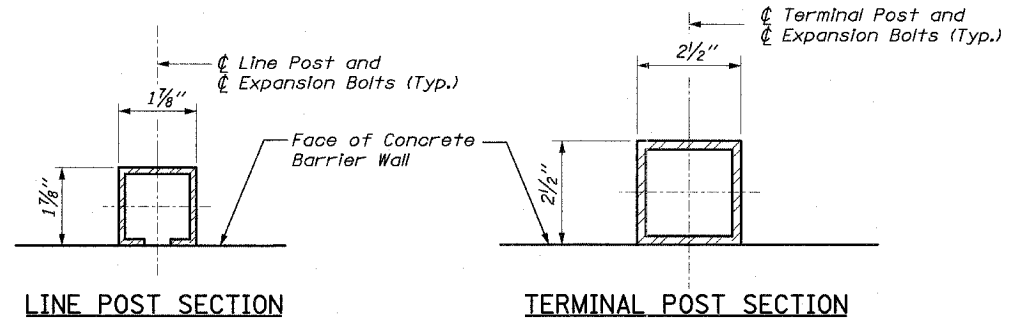
**ELEVATION**  
**CTA FENCE ON CONCRETE BARRIER WALL**



**SECTION 1-1**

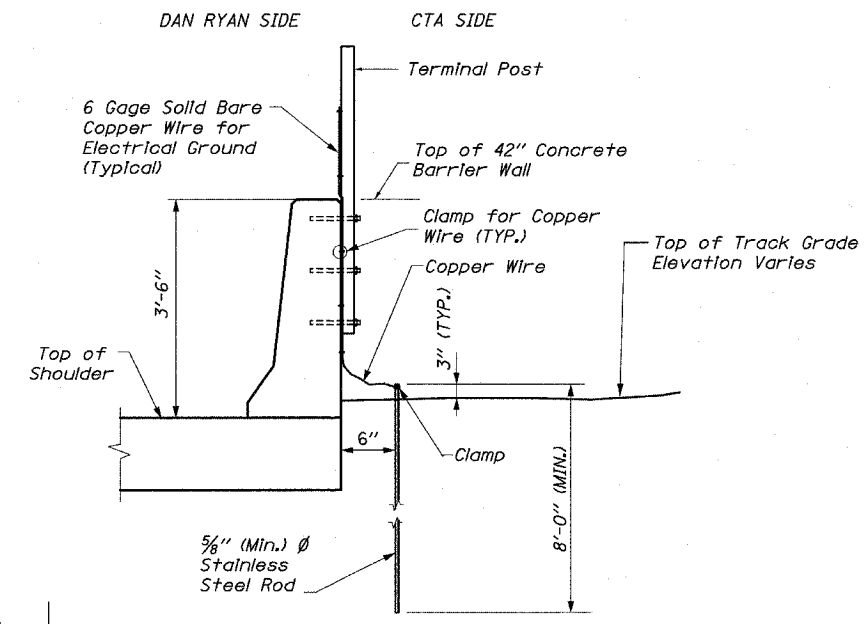


**ALTERNATE DETAIL FOR**  
**STRETCHER BAR BAND**

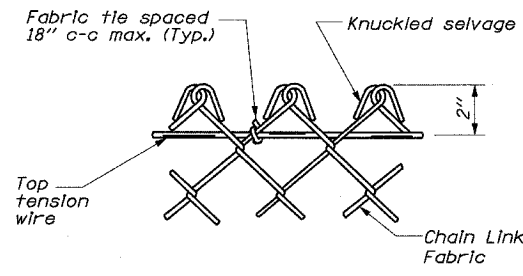


**NOTE:**  
The Contractor May Use Another C Section as per the Standards of the Illinois Department of Transportation.

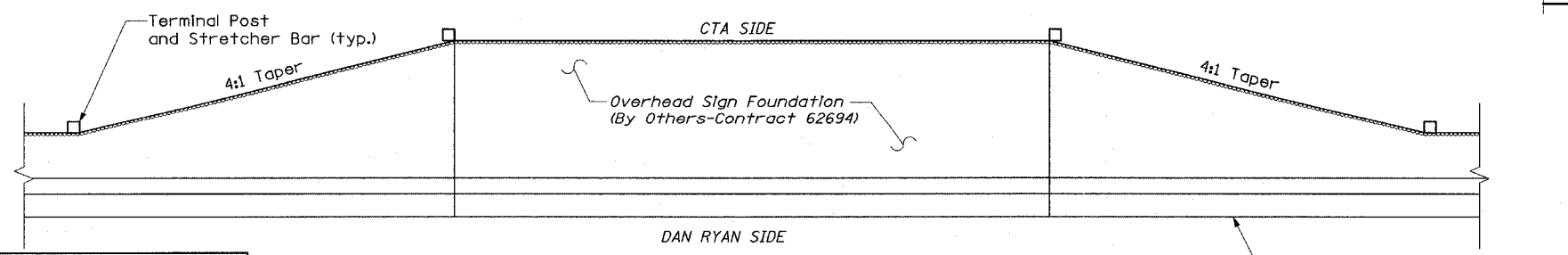
**NOTE:**  
Where the fence meets bridge piers and other vertical structures taller than the fence a maximum gap of 2" clear between the terminal post and the structure shall be provided.



**SECTION 2-2**  
**ELECTRICAL GROUND DETAIL**



**KNUCKLED DETAIL**



**DETAIL AT OVERHEAD SIGN FOUNDATIONS**

Fence, hardware and posts around the overhead sign foundation are paid for as CTA Fence.

42" Concrete Barrier, Single Face, 42 Inch Height (Special)

**Edwards AND Kelcey**  
ONE NORTH FRANKLIN CHICAGO, IL 60606  
PHONE: (312) 251-3000  
FAX: (312) 251-3015  
WEB: WWW.EKCORP.COM

**TYLIN INTERNATIONAL**

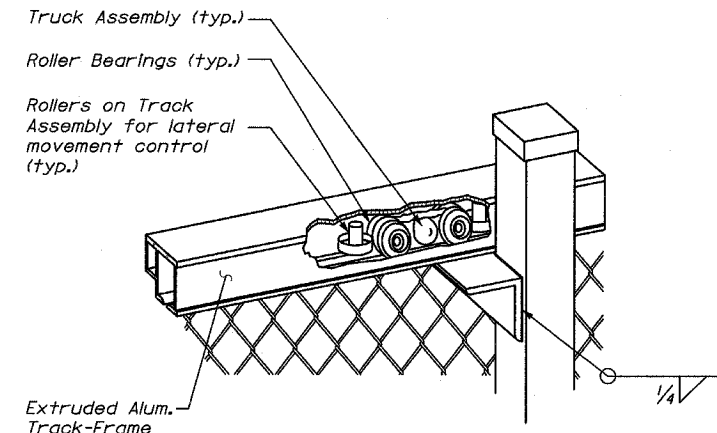
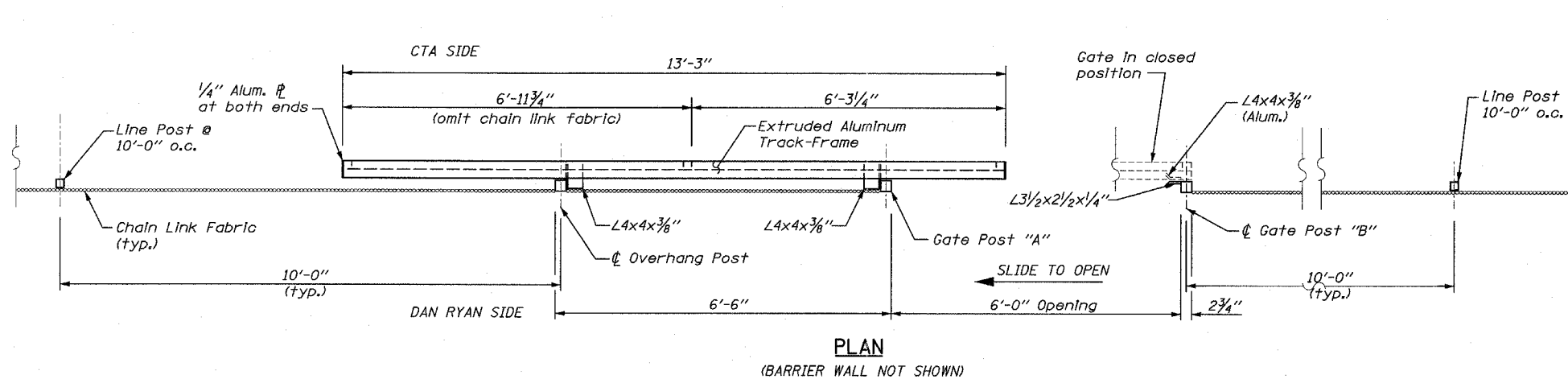
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

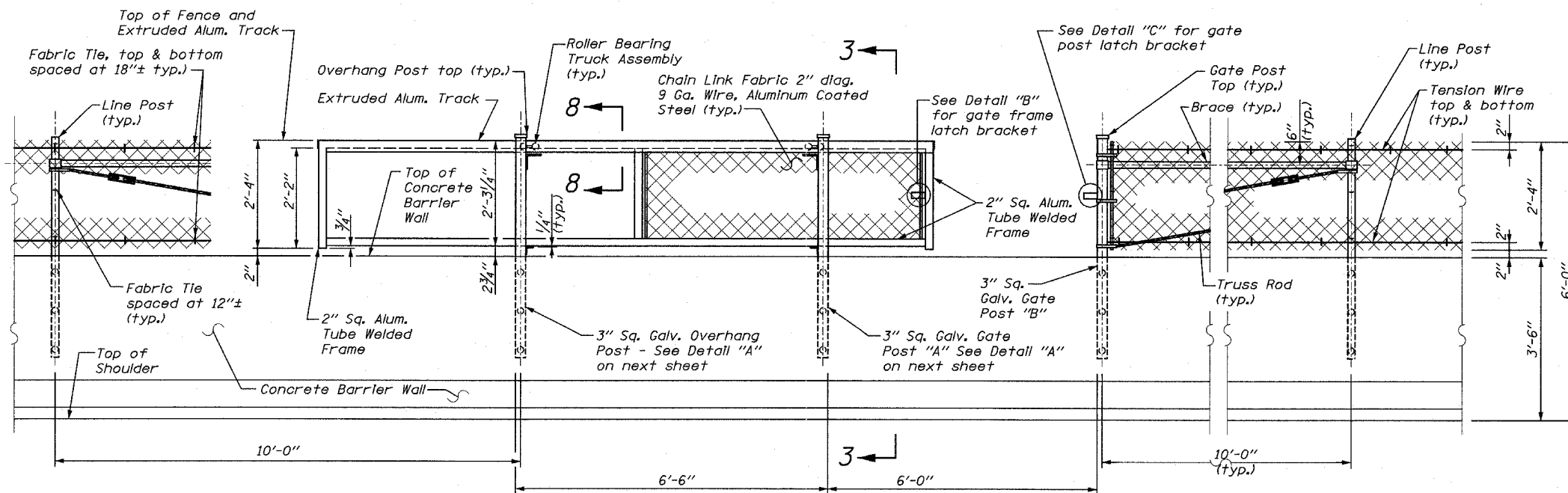
**CTA FENCE**  
**ELEVATIONS AND DETAILS**  
**SHEET 1 OF 3**

SCALE: NONE  
DATE: MARCH 1, 2006

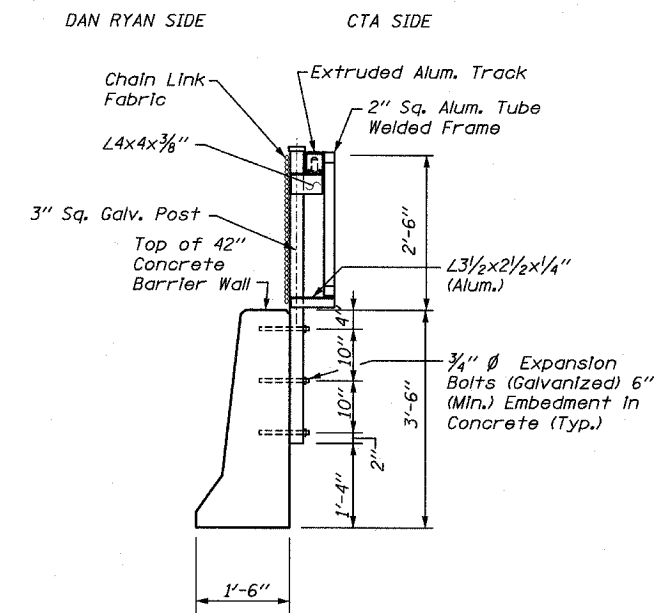
DRAWN BY: RLK  
CHECKED BY: EL



**ROLLER BEARING TRUCK ASSEMBLY DETAIL**

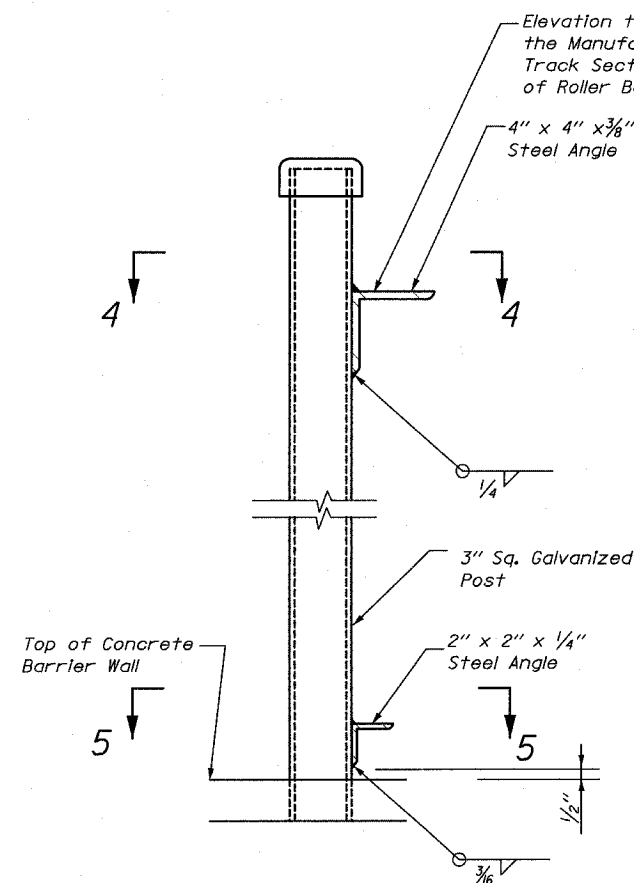


**ELEVATION CTA FENCE & CTA GATE**  
(FOOTING NOT SHOWN)

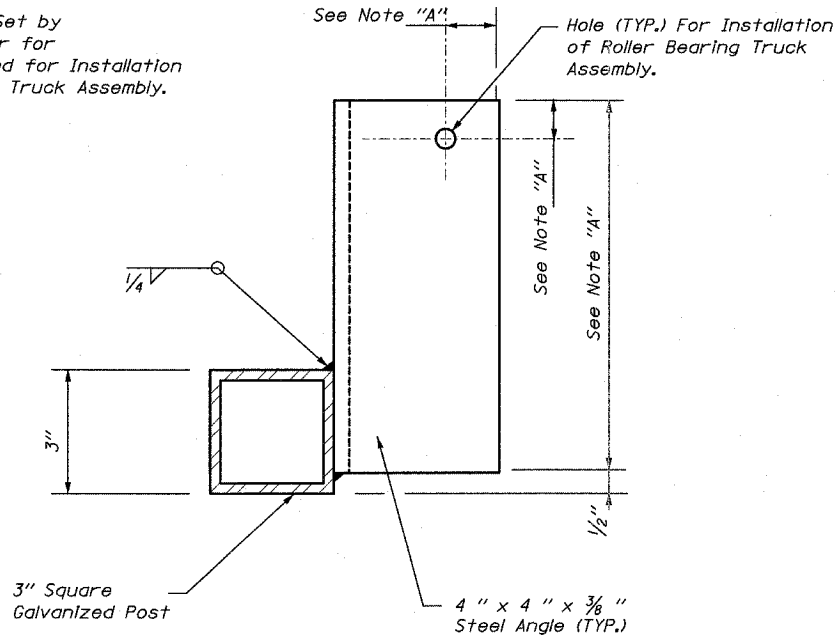


**SECTION 3-3**

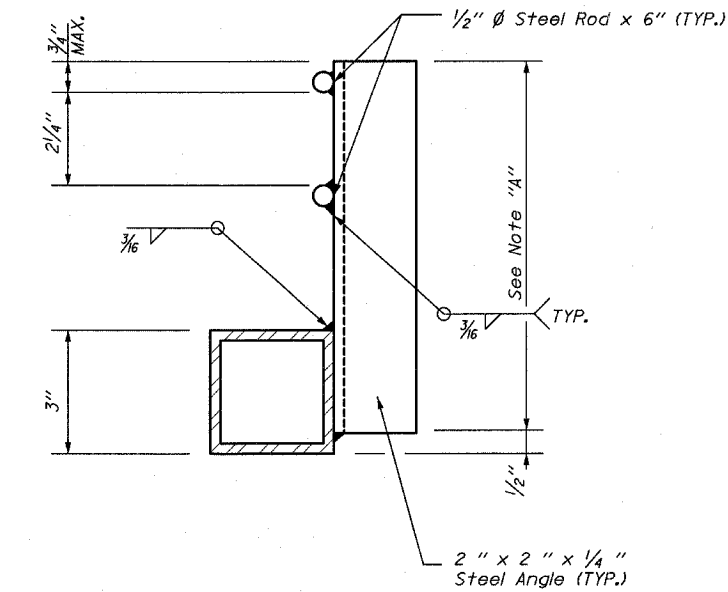
REVISIONS	
NAME	DATE



**ELEVATION**



**SECTION 4-4**



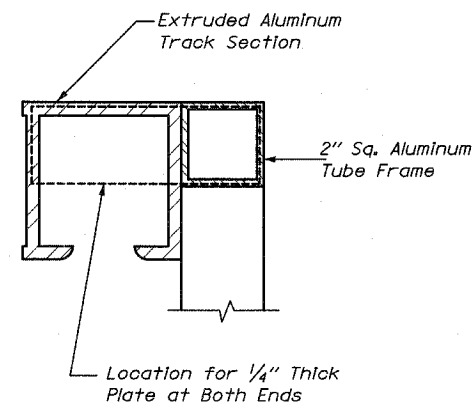
**SECTION 5-5**

**DETAIL A**

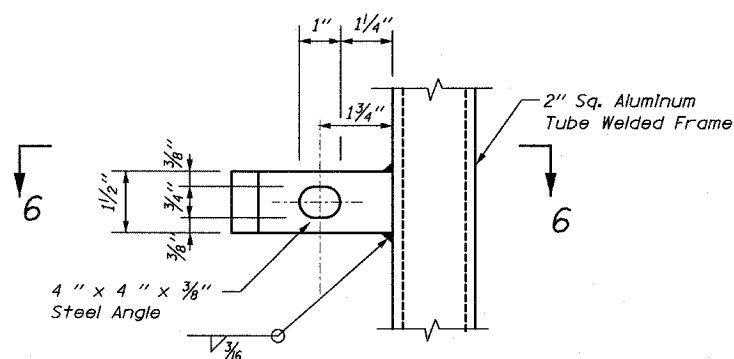
For Overhang Post - As Shown  
For Gate Post "A" - Opposite Hand

NOTE "A": The Manufacturer of the Sliding Gate Shall Determine the Length of Angles and Location of Hole for Installation of Roller Bearing Truck Assembly.

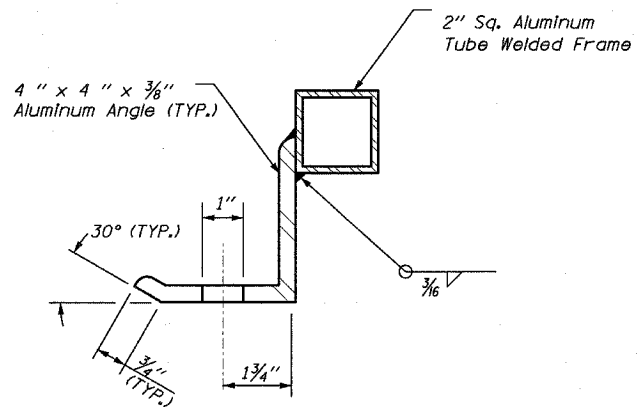
NOTE: The Slide Gates Shall be of the Cantilevered Type with no Track or Support Across the Opening.



**SECTION 8-8**



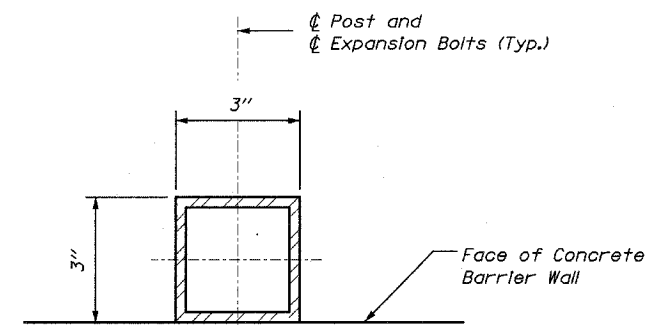
**ELEVATION**



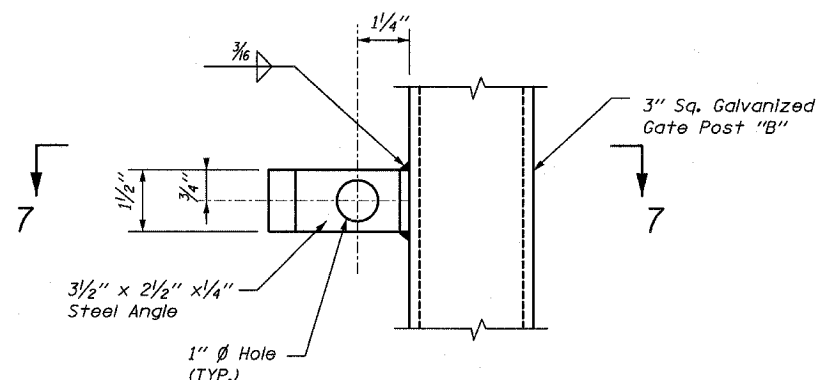
**SECTION 6-6**

**DETAIL B**

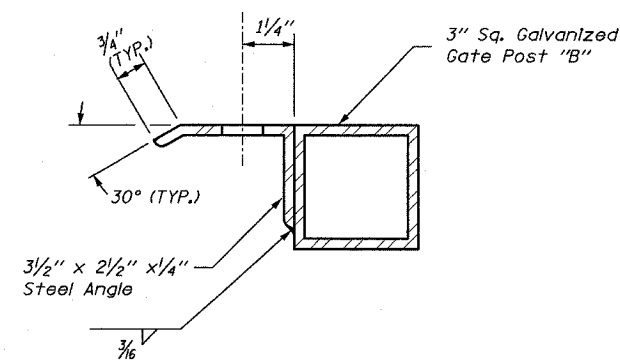
NOTE "B": The Manufacturer of the Sliding Gate Shall Set the Angle Based on the Size of the Track Frame



**GATE AND OVERHANG POST SECTION**



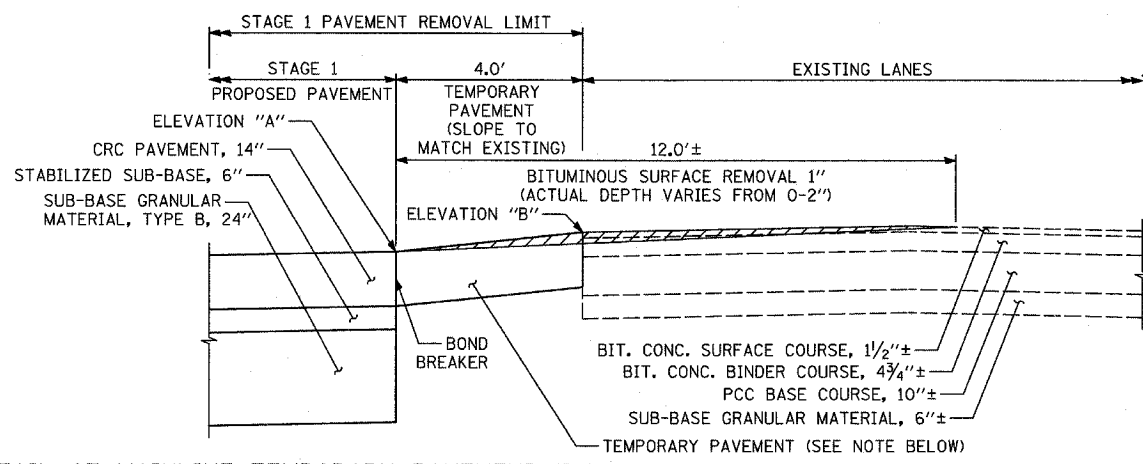
**ELEVATION**



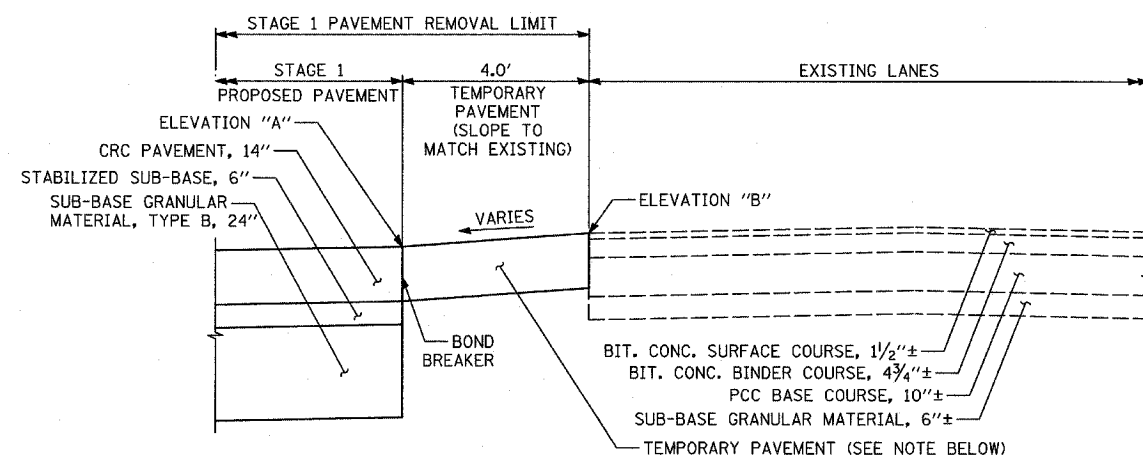
**SECTION 7-7**

**DETAIL C**

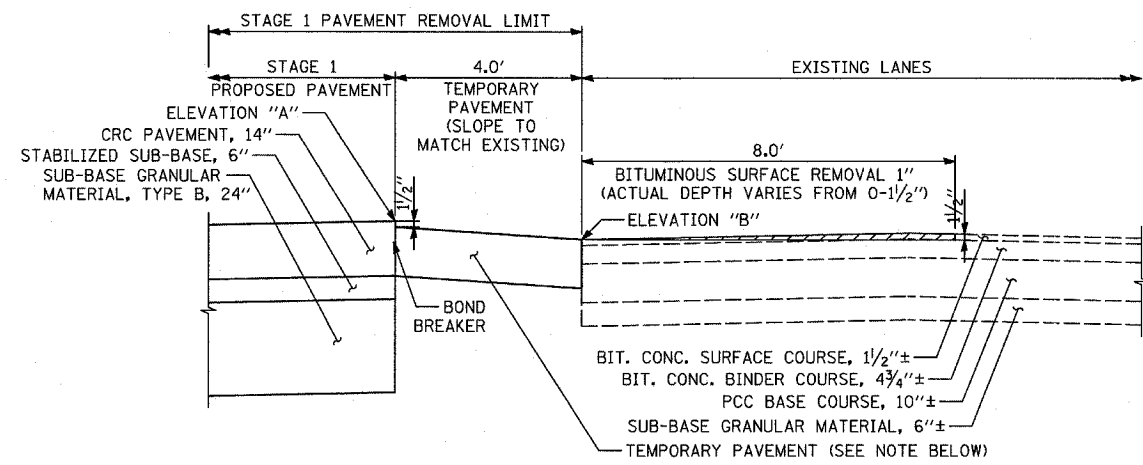
REVISIONS	
NAME	DATE



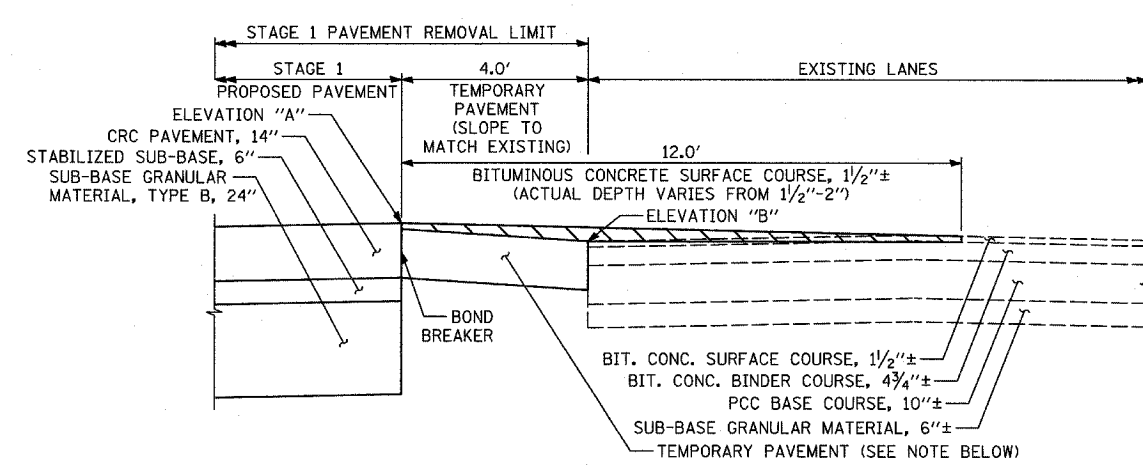
**DETAIL OF MAINLINE TEMPORARY PAVEMENT FOR POST-STAGE 1 WINTER LANE CONFIGURATION**  
 USE WHERE ELEVATION "A" IS MORE THAN 2" BELOW ELEVATION "B"  
 SEE MAINTENANCE OF TRAFFIC PLANS FOR LOCATIONS



**DETAIL OF MAINLINE TEMPORARY PAVEMENT FOR POST-STAGE 1 WINTER LANE CONFIGURATION**  
 USE WHERE ELEVATION "A" IS 0-2" BELOW ELEVATION "B"  
 SEE MAINTENANCE OF TRAFFIC PLANS FOR LOCATIONS



**STEP 1**  
 BITUMINOUS SURFACE REMOVAL

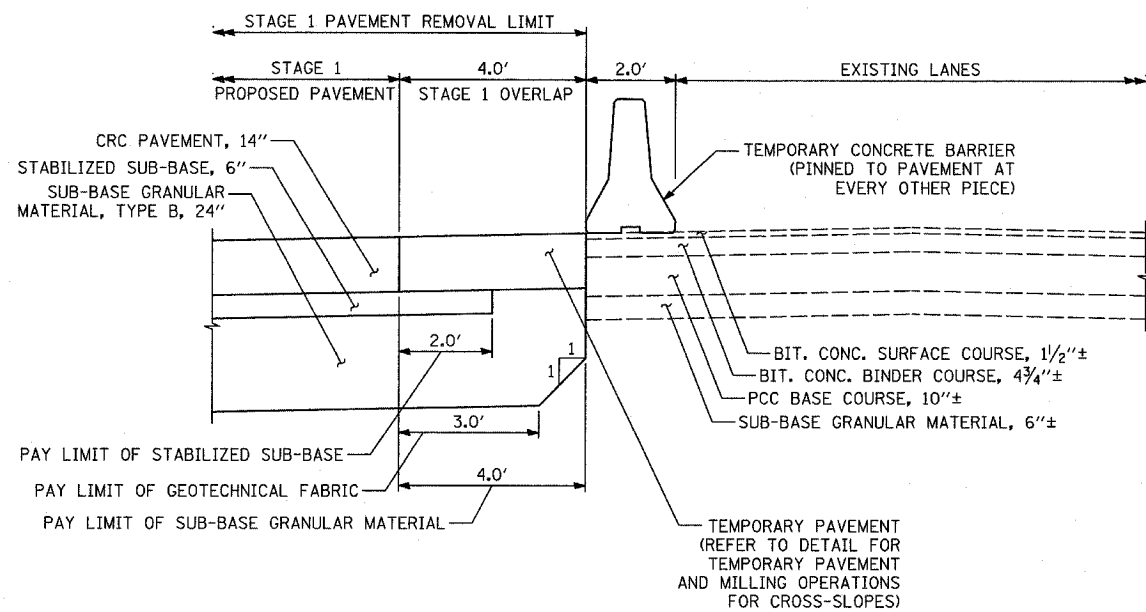


**STEP 2**  
 PLACEMENT OF BITUMINOUS SURFACE COURSE

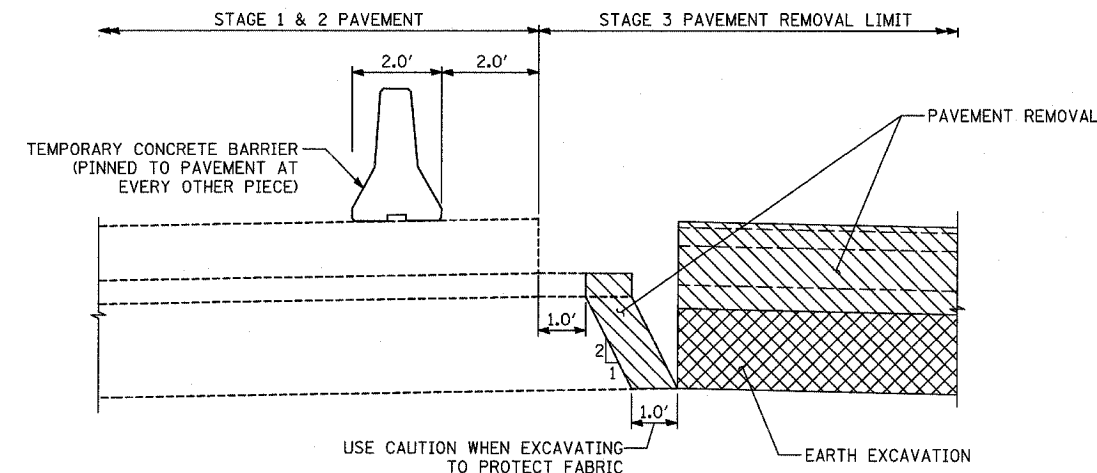
**NOTE:**  
 CONTRACTOR MUST USE A 14" THICK CONCRETE TEMPORARY PAVEMENT OPTION. ADDITIONAL THICKNESS OF CONCRETE TEMPORARY PAVEMENT WILL NOT BE PAID FOR SEPARATELY BUT BE INCLUDED IN THE COST OF THE TEMPORARY PAVEMENT.

REVISIONS	
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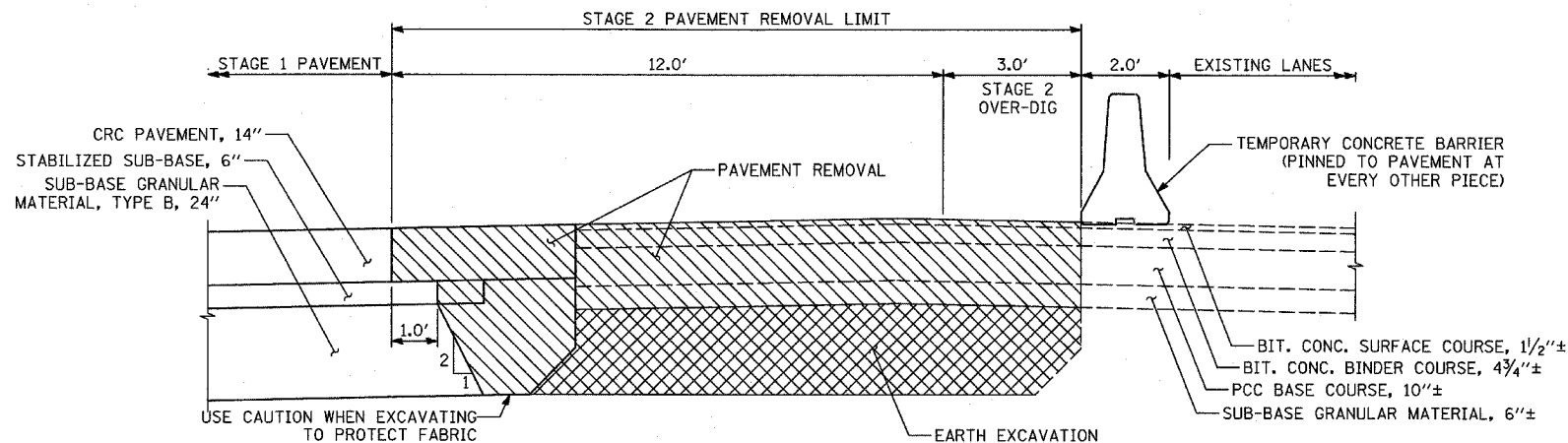
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**MISCELLANEOUS DETAILS**  
 TEMPORARY PAVEMENT & MILLING OPERATIONS  
 SCALE: AS SHOWN  
 DATE: MARCH 1, 2006  
 DRAWN BY: MPG  
 CHECKED BY: TGB



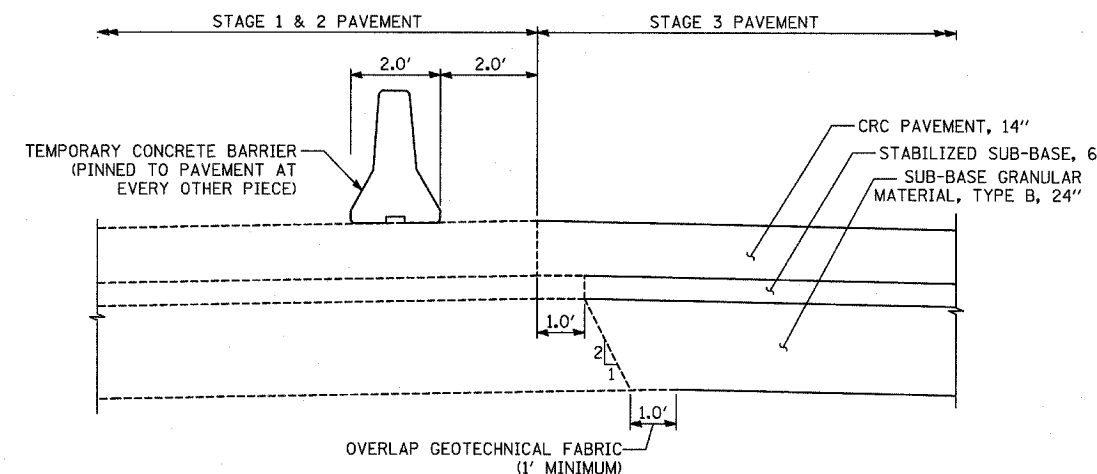
STAGE 1 PAVEMENT



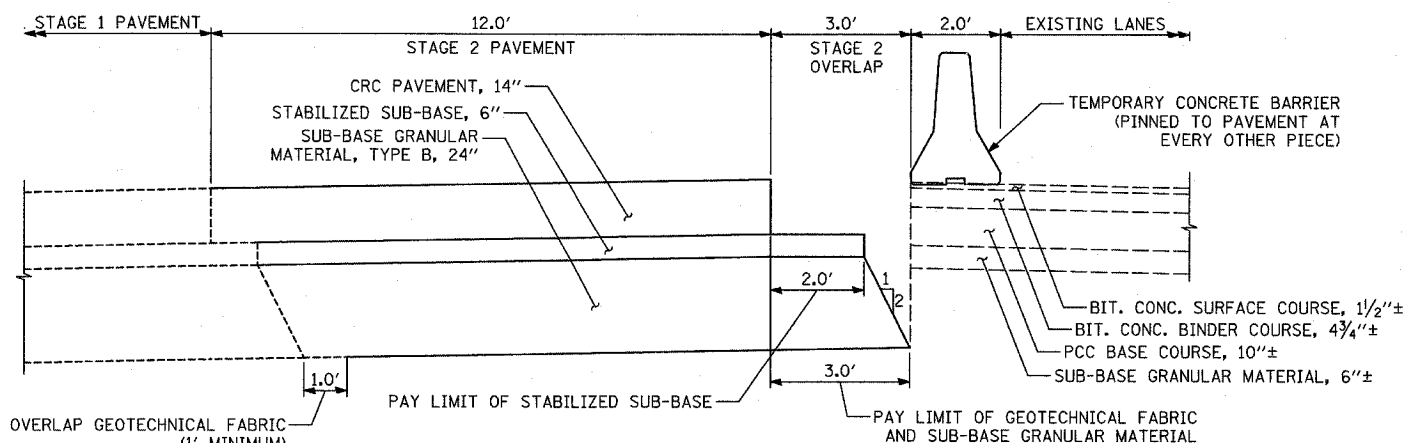
STAGE 3 REMOVAL



STAGE 2 REMOVAL & OVER-DIG



STAGE 3 PAVEMENT



STAGE 2 PAVEMENT

SEE NOTE #2

NOTES:

1. THESE SECTIONS DEPICT MAINLINE DAN RYAN. SIMILAR SITUATIONS OCCUR ALONG I-57 WITH A LESSER NUMBER OF LANES, BUT SAME CONSTRUCTION REQUIREMENTS.
2. STAGE 2A WILL REQUIRE A 3.0' WIDE STRIP OF TEMPORARY PAVEMENT BETWEEN SB I-94 STA. 1260+00 AND STA. 1270+00. ADDITIONAL THICKNESS OF CONCRETE WILL BE INCLUDED IN THE COST OF THE TEMPORARY PAVEMENT.

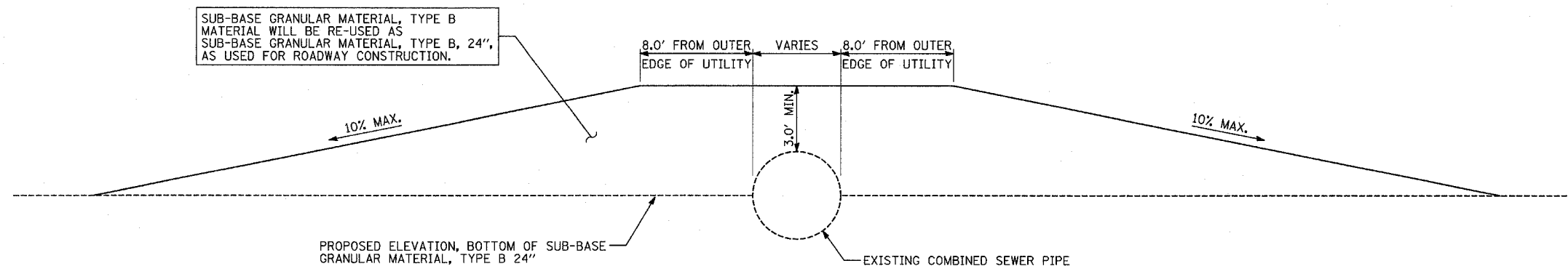
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS:  
OVER-DIG AREAS

SCALE: AS SHOWN  
DATE: MARCH 1, 2006

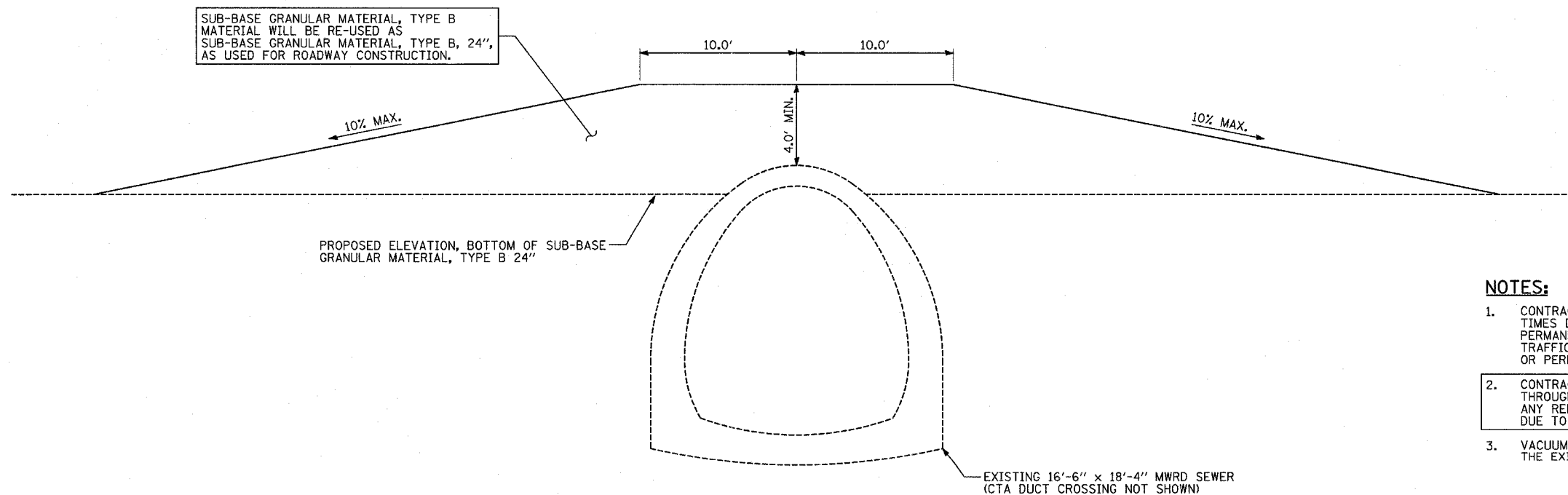
DRAWN BY: RTM  
CHECKED BY: TGB



**COMBINED SEWER CONSTRUCTION PROTECTION**  
(SEE COMBINED SEWER CROSSING STATIONS)

**COMBINED SEWER CROSSING STATIONS:**

1. SB I-57 STA. 90+32.99 (60")
2. SB I-57 STA. 130+21.70/  
WB I-94 TO I-57 CONN. STA. 342+09.91 (5'-8" x 3'-7" BOX)
3. SB I-57 STA. 143+31.38 (7'-6" x 6'-0" DOUBLE BOX)
4. SB I-94 STA. 1245+59.70 (48")
5. SB I-94 STA. 1265+92.81 (DOUBLE 48")
6. SB I-94 STA. 1279+30.52 (TRIPLE 48")
7. SB I-94 STA. 1325+70.55 (10'-0" x 8'-0" BOX)
8. SB I-94 STA. 1345+70.10 (60")



**16'-6" X 18'-4" MWRD SEWER - SB I-94 STA. 1358+93.46**  
**CTA DUCT - SB I-94 STA. 1314+13.59**  
**CTA DUCT - SB I-57 STA. 151+94.38**  
**CONSTRUCTION PROTECTION**

**NOTES:**

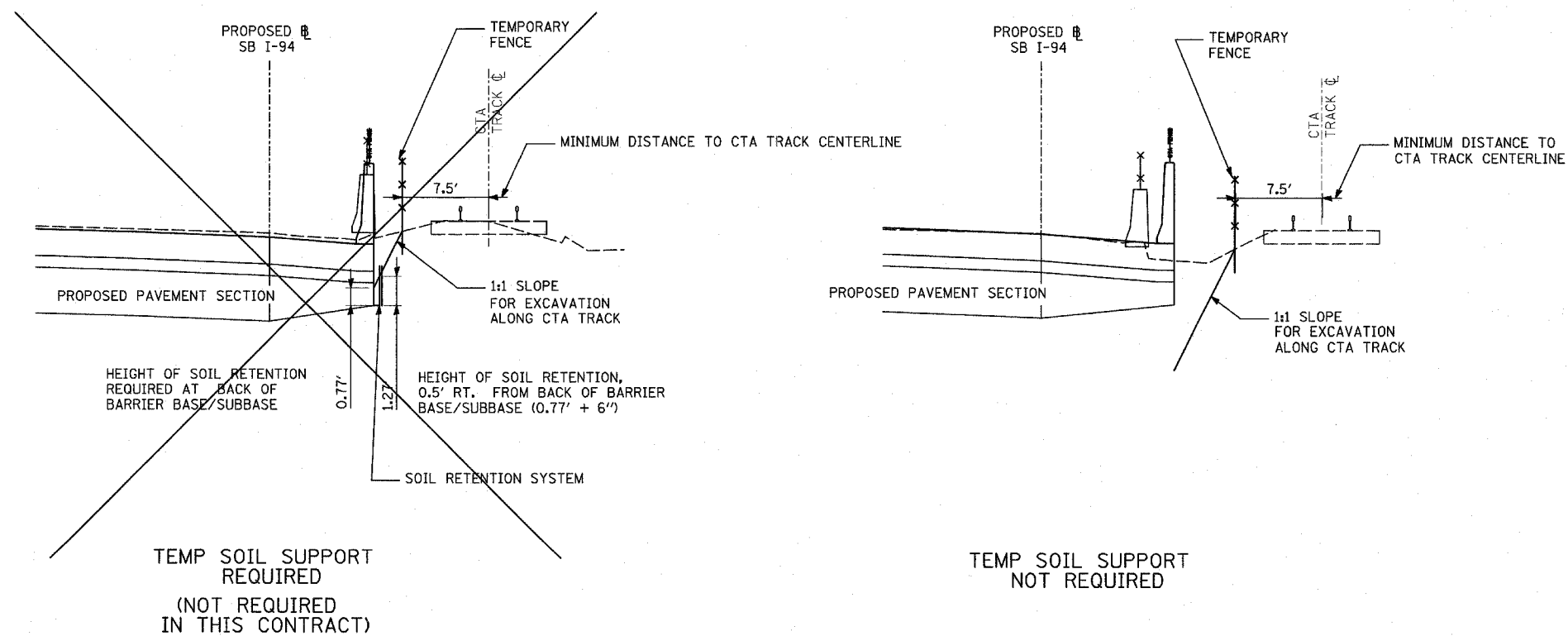
1. CONTRACTOR WILL PROVIDE A MINIMUM COVER, AS SHOWN ABOVE AT ALL TIMES DURING CONSTRUCTION AND PRIOR TO THE INSTALLATION OF THE PERMANENT PROTECTION SLAB AND/OR ROADWAY. CONSTRUCTION TRAFFIC SHALL NOT CROSS THE UTILITIES WITHOUT TEMPORARY COVER OR PERMANENT SLAB IN PLACE.
2. CONTRACTOR IS RESPONSIBLE FOR THE INTEGRITY OF SEWER PIPE THROUGH THE DURATION OF THE PROJECT, AND WILL NOT BE PAID FOR ANY REPAIRS REQUIRED, AS DETERMINED BY THE ENGINEER, TO THE PIPE, DUE TO CONSTRUCTION ACTIVITIES.
3. VACUUM EXCAVATION OR HAND DIGGING MUST BE USED WITHIN 2'-0" OF THE EXISTING 16'-6" x 18'-4" MWRD SEWER AND CTA DUCTS ONLY.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS:  
WORK ZONE UTILITY PROTECTION

SCALE: NONE  
DATE: MARCH 1, 2006  
DRAWN BY: MPG  
CHECKED BY: TCB



CALCULATION OF HEIGHT OF TEMP SOIL SUPPORT

SOIL RETENTION SCHEDULE

		SOIL RETENTION AREA SF
NUMBER OF DRAINAGE STRUCTURES	32	2560
	TOTAL	2560

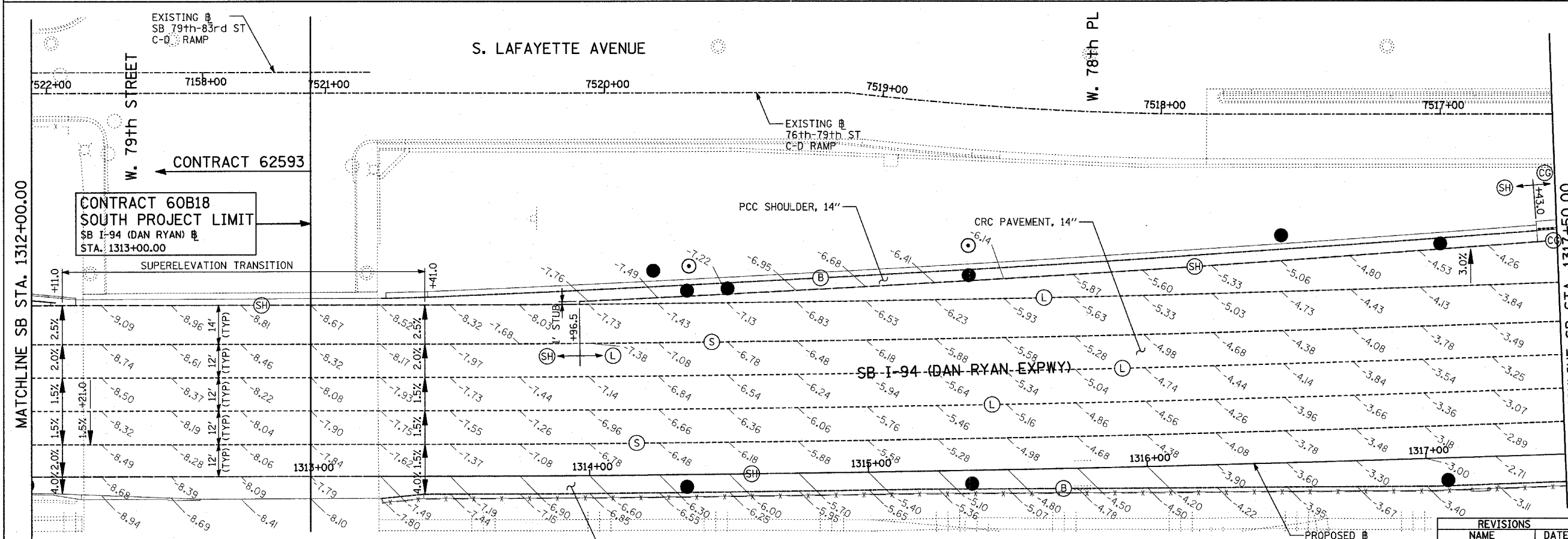
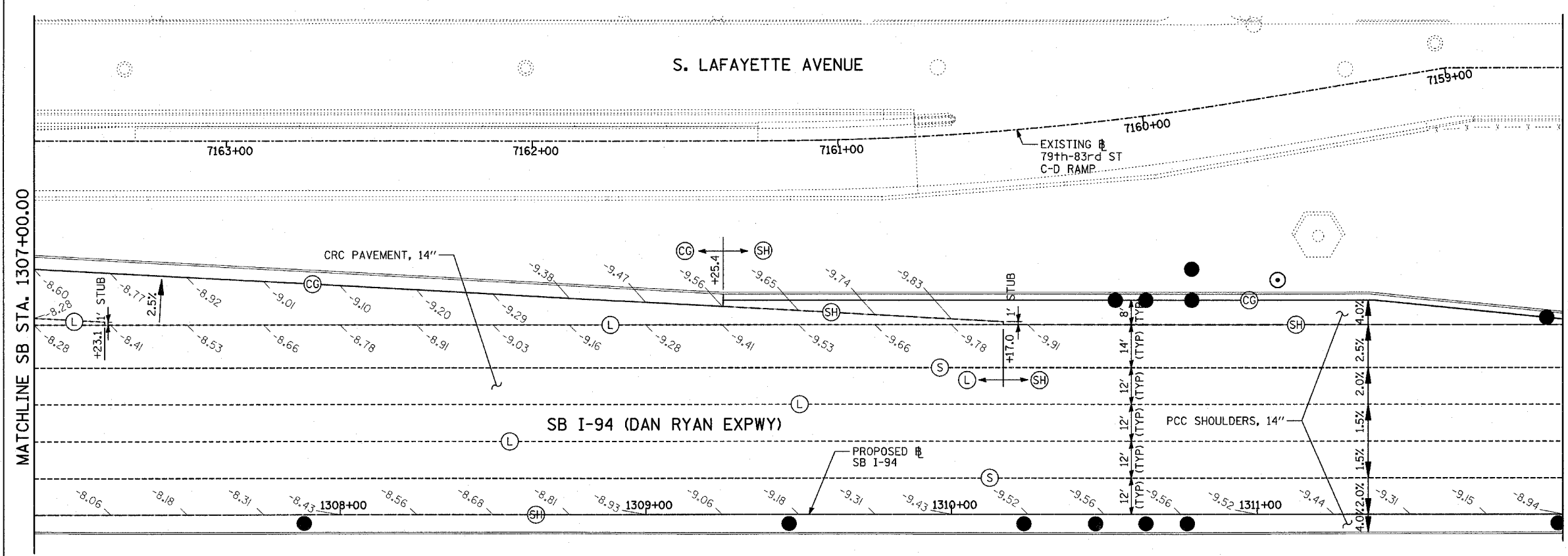
\* 80 SF OF SOIL RETENTION IS REQUIRED FOR EACH DRAINAGE STRUCTURE. SEE DRAINAGE SCHEDULES FOR DRAINAGE STRUCTURES REQUIRING SOIL RETENTION





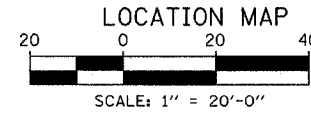
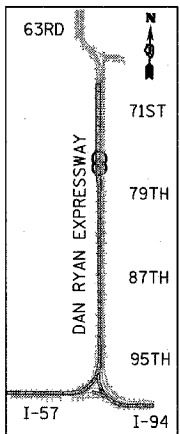
**LEGEND:**

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



CONTRACT 60B18  
SOUTH PROJECT LIMIT  
SB I-94 (DAN RYAN) @  
STA. 1313+00.00

CONTRACT 62593



**NOTES:**

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

REVISIONS	
NAME	DATE

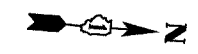
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
PAVEMENT JOINTING AND ELEVATION PLAN  
SB I-94 (DAN RYAN EXPRESSWAY)  
STA. 1307+00.00 TO 1317+50.00  
SHEET 1 OF 11

SCALE: 1"=20'  
DATE: MARCH 1, 2006  
DRAWN BY: JJS  
CHECKED BY: MPG

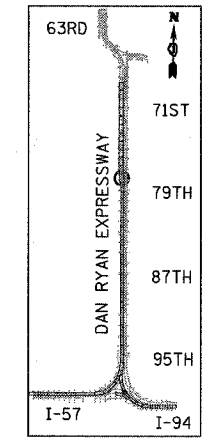
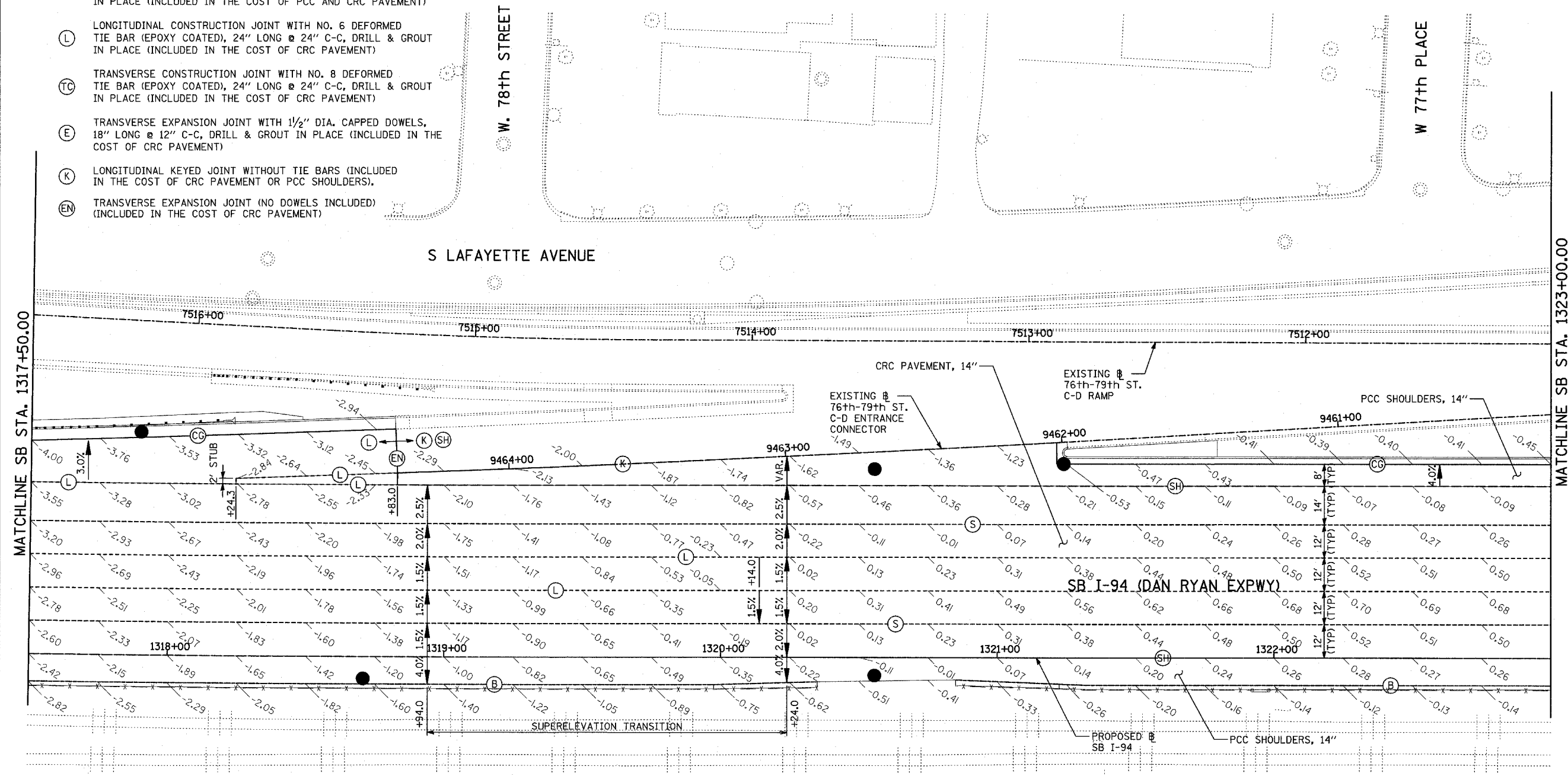
TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	75
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

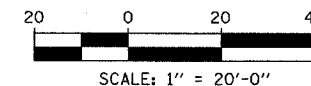
60B18



- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
  - (CC) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
  - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
  - (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
  - (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
  - (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



LOCATION MAP



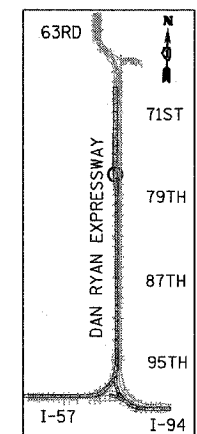
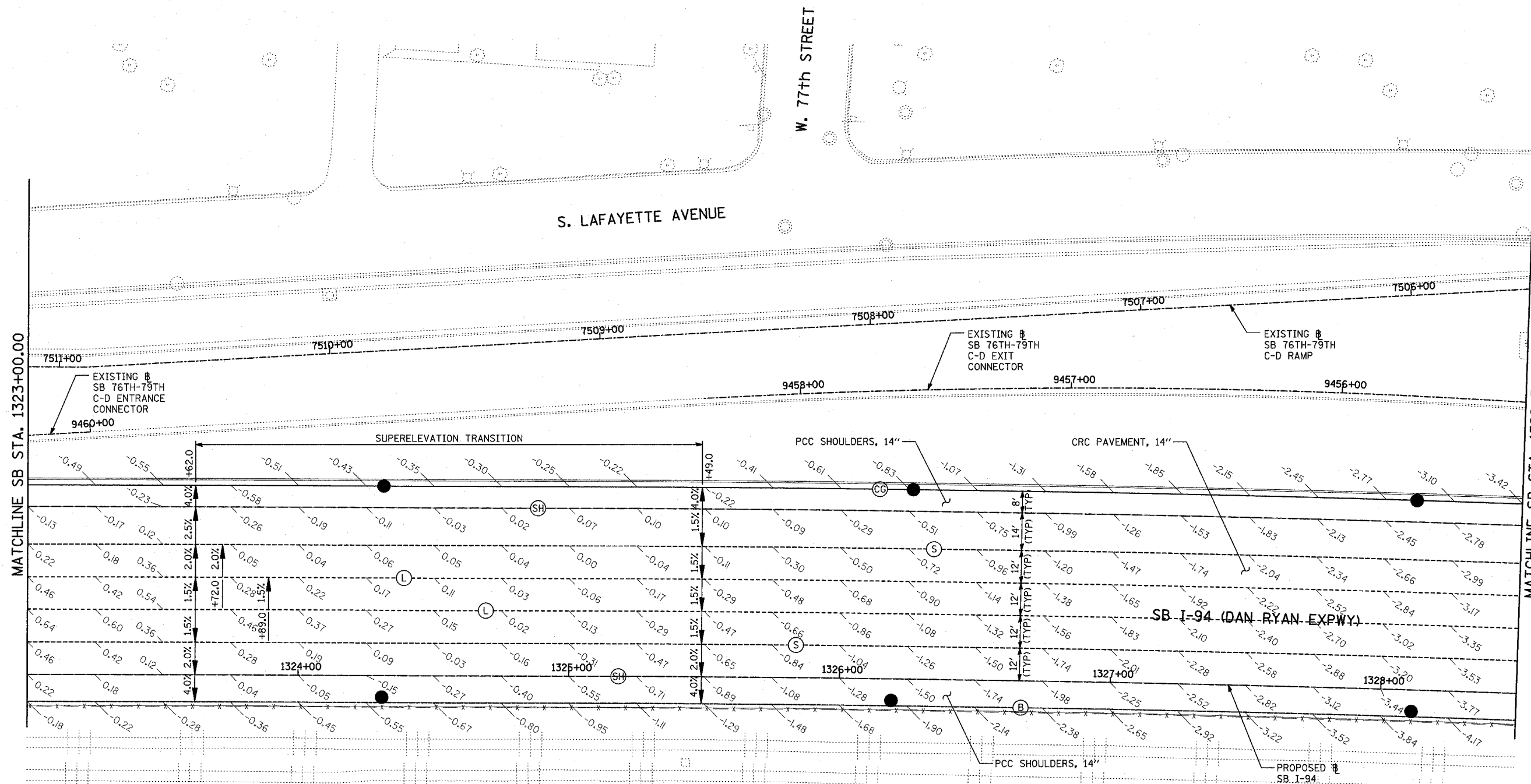
- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
  - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
  - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 PAVEMENT JOINTING AND ELEVATION PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1317+50.00 TO 1323+00.00  
 SHEET 2 OF 11

SCALE: 1"=20'  
 DATE: MARCH 1, 2006  
 DRAWN BY: JJS  
 CHECKED BY: MPG

**TYLIN** INTERNATIONAL

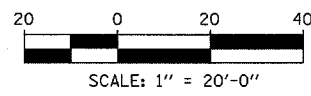


**LEGEND:**

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

**NOTES:**

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

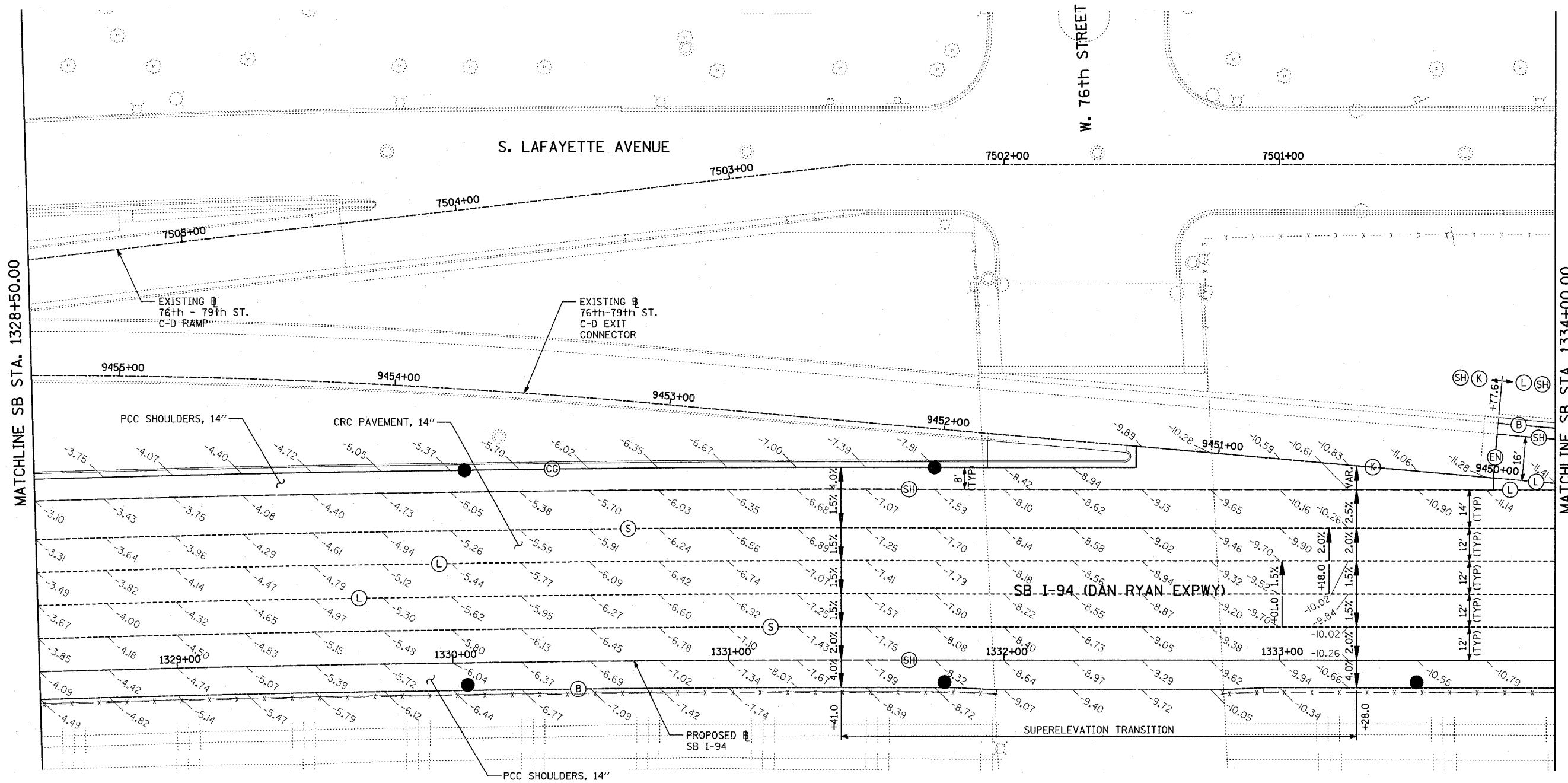
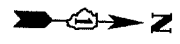


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**PAVEMENT JOINTING AND ELEVATION PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**STA. 1323+00.00 TO 1328+50.00**  
**SHEET 3 OF 11**

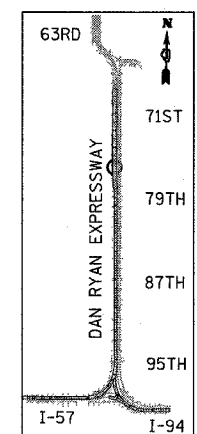
SCALE: 1"=20'  
 DATE: MARCH 1, 2006  
 DRAWN BY: JJS  
 CHECKED BY: MPG

**TYLIN INTERNATIONAL**

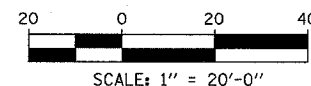


MATCHLINE SB STA. 1328+50.00

MATCHLINE SB STA. 1334+00.00



LOCATION MAP



- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
  - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
  - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
  - (CC) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
  - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
  - (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
  - (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
  - (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

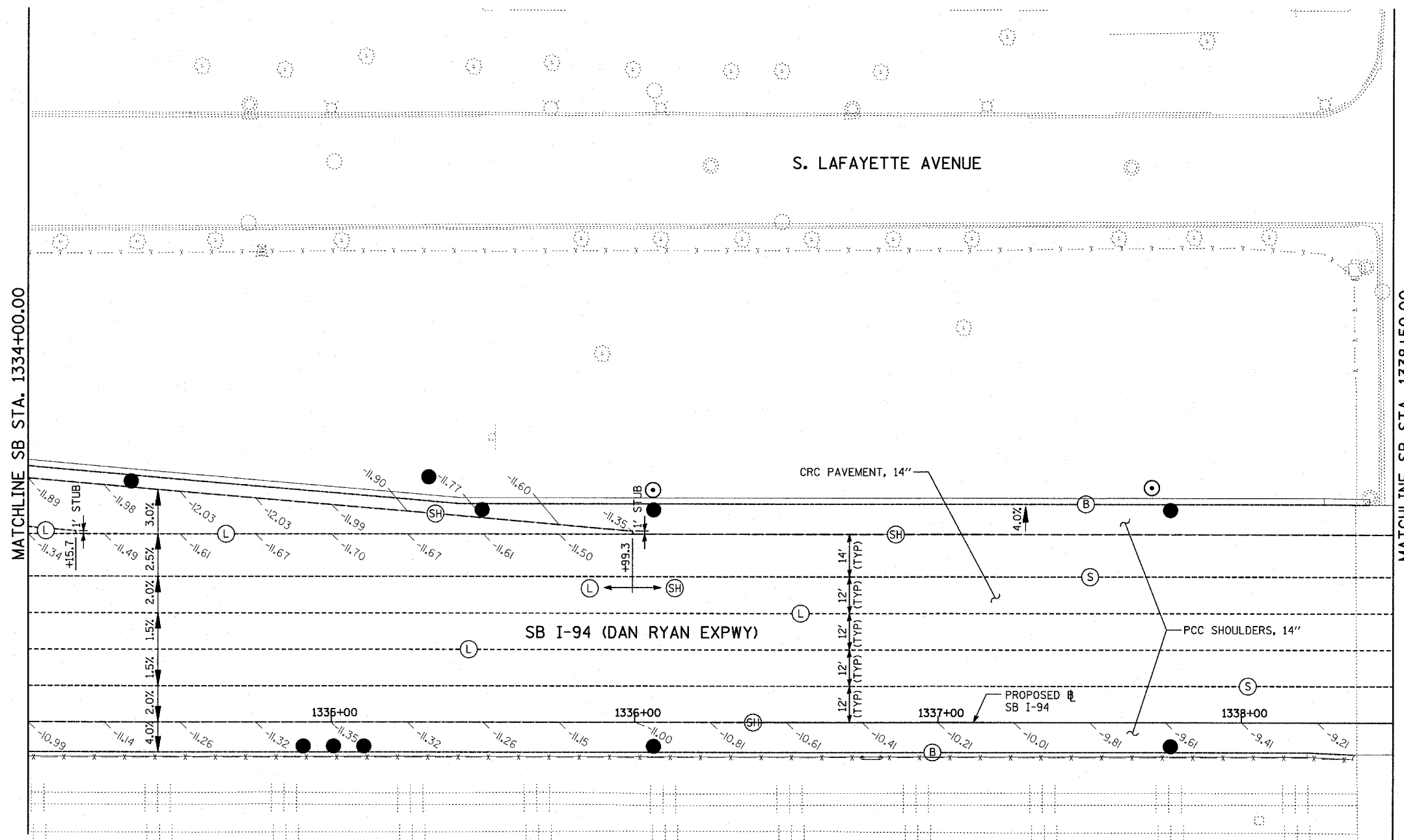
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**PAVEMENT JOINTING AND ELEVATION PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**STA. 1328+50.00 TO 1334+00.00**  
**SHEET 4 OF 11**

SCALE: 1"=20'  
DATE: MARCH 1, 2006

DRAWN BY: JJS  
CHECKED BY: MPG

**TYLIN**INTERNATIONAL

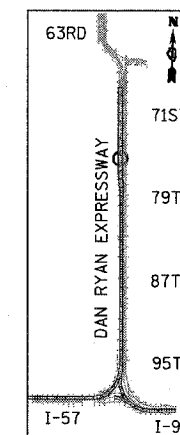


**LEGEND:**

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CC) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

**NOTES:**

- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
- ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
- CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



LOCATION MAP

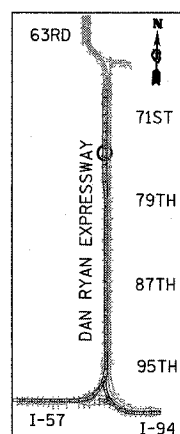
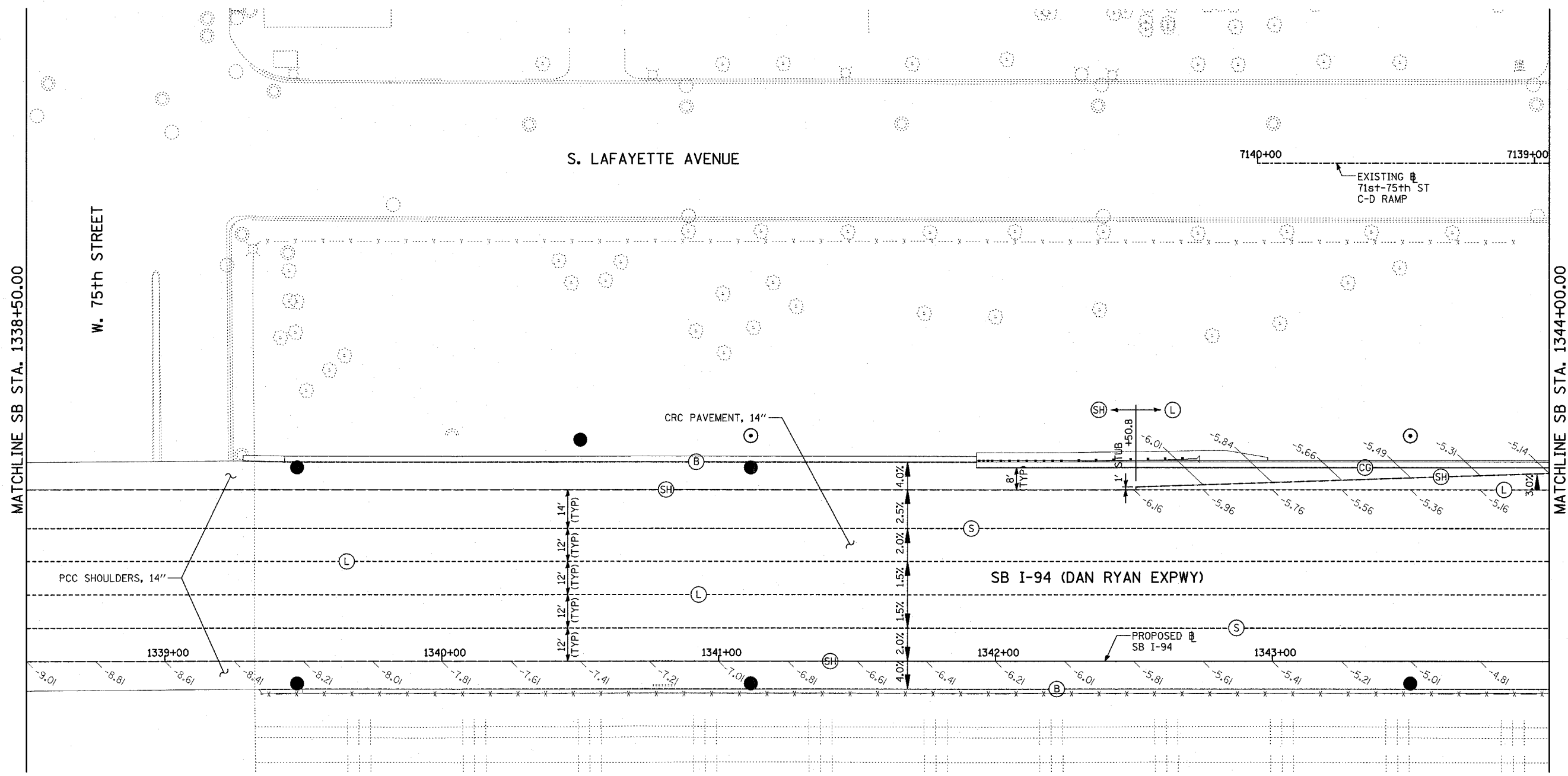


SCALE: 1" = 20'-0"

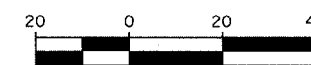
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 PAVEMENT JOINTING AND ELEVATION PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1334+00.00 TO 1338+50.00  
 SHEET 5 OF 11

SCALE: 1"=20'  
 DATE: MARCH 1, 2006  
 DRAWN BY: JJS  
 CHECKED BY: MPG



LOCATION MAP



SCALE: 1" = 20'-0"

**NOTES:**

- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
- ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
- CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

**LEGEND:**

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**PAVEMENT JOINTING AND ELEVATION PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**STA. 1338+50.00 TO 1344+00.00**  
**SHEET 6 OF 11**

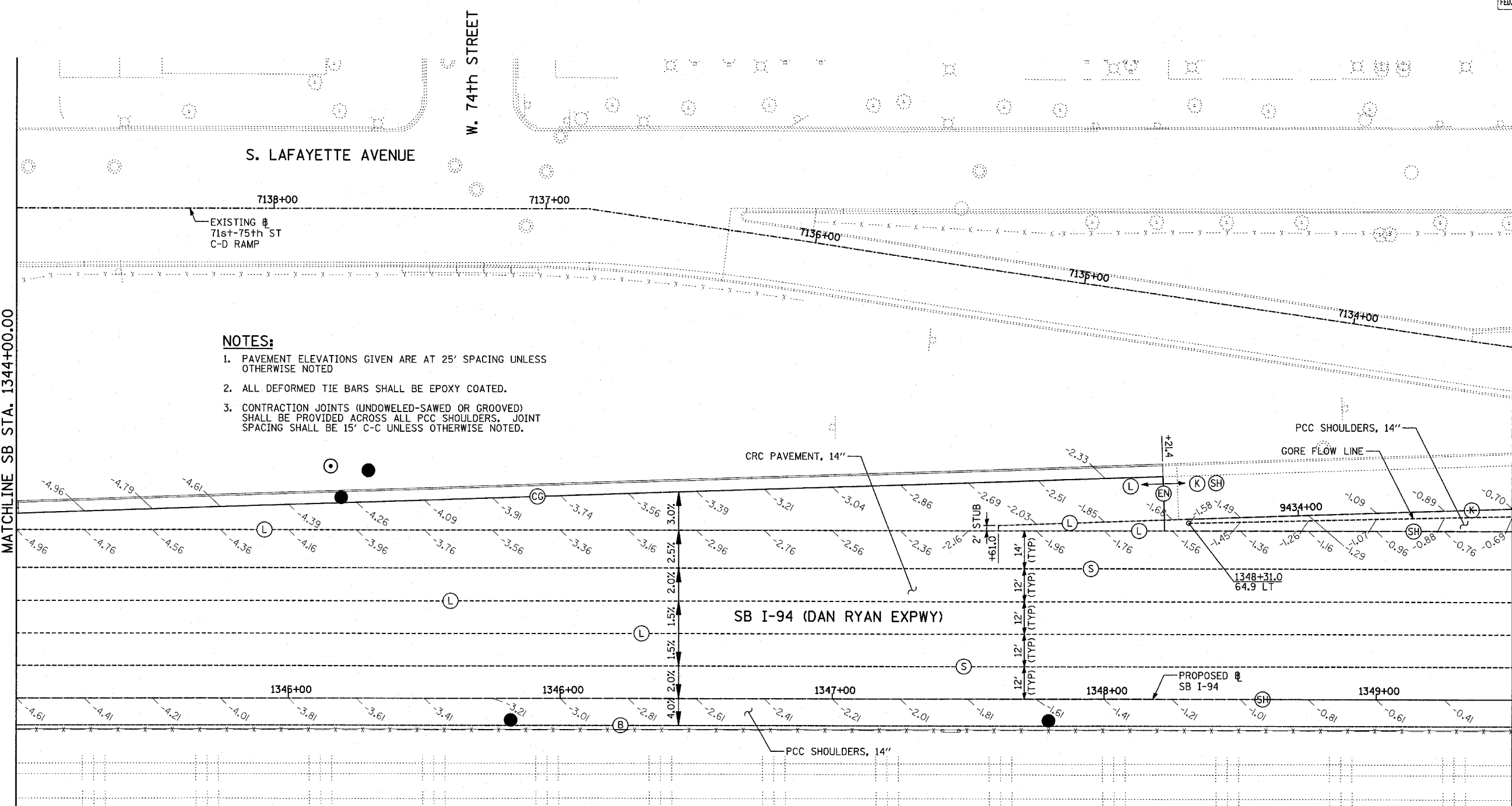
SCALE: 1"=20'  
 DATE: MARCH 1, 2006  
 DRAWN BY: JJS  
 CHECKED BY: MPG





MATCHLINE SB STA. 1344+00.00

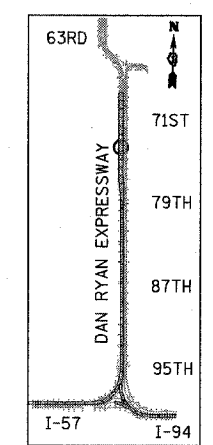
MATCHLINE SB STA. 1349+50.00



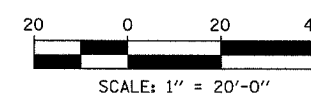
- NOTES:**
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
  2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
  3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

**LEGEND:**

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



LOCATION MAP

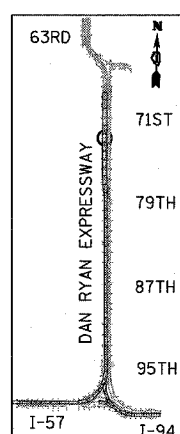
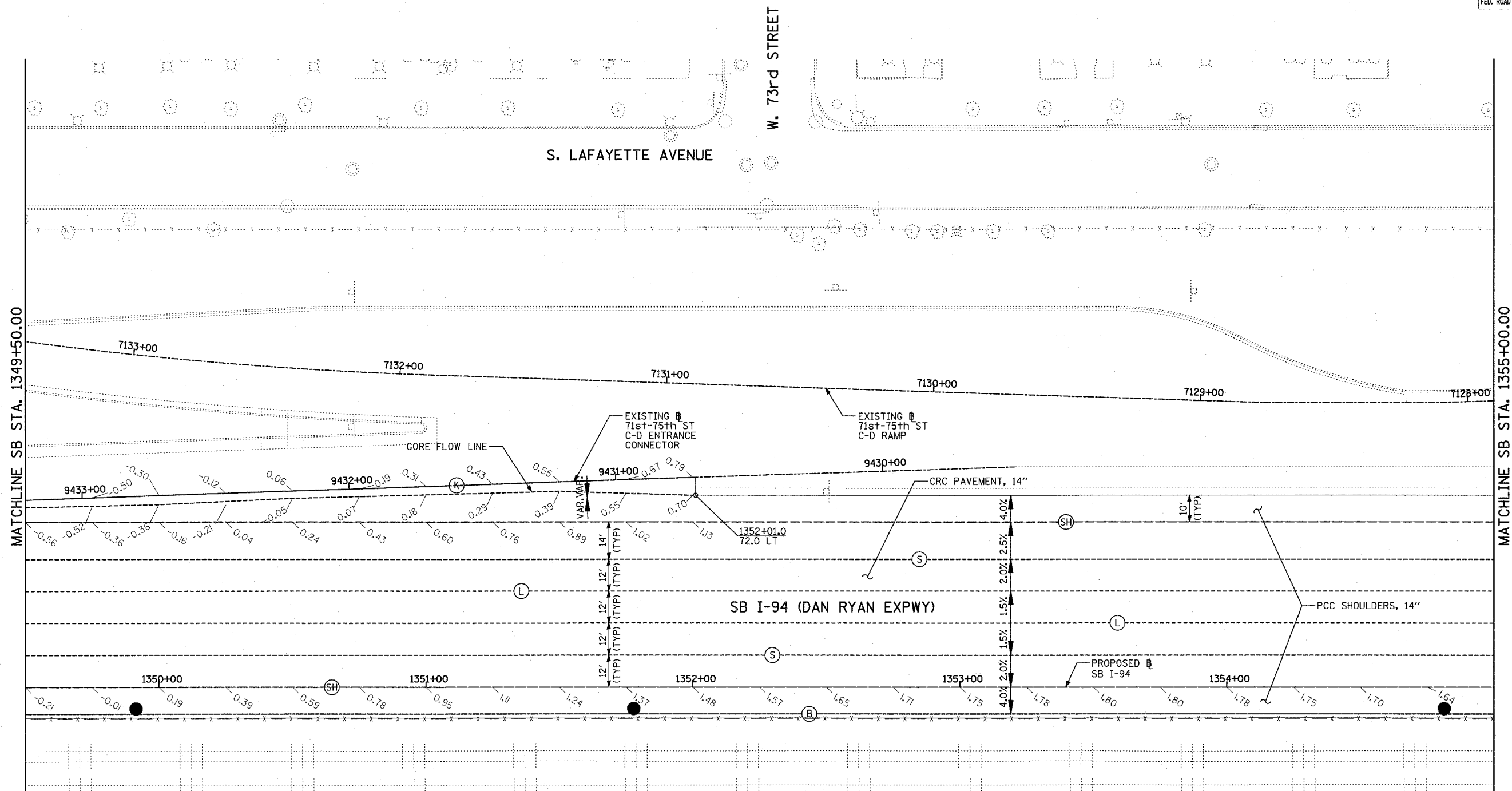


REVISIONS	
NAME	DATE

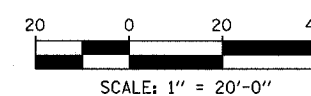
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 PAVEMENT JOINTING AND ELEVATION PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1344+00.00 TO 1349+50.00  
 SHEET 7 OF 11

SCALE: 1"=20'  
 DATE: MARCH 1, 2006  
 DRAWN BY: JJS  
 CHECKED BY: MPG

**TYLIN INTERNATIONAL**



LOCATION MAP



SCALE: 1" = 20'-0"

- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
  - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
  - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

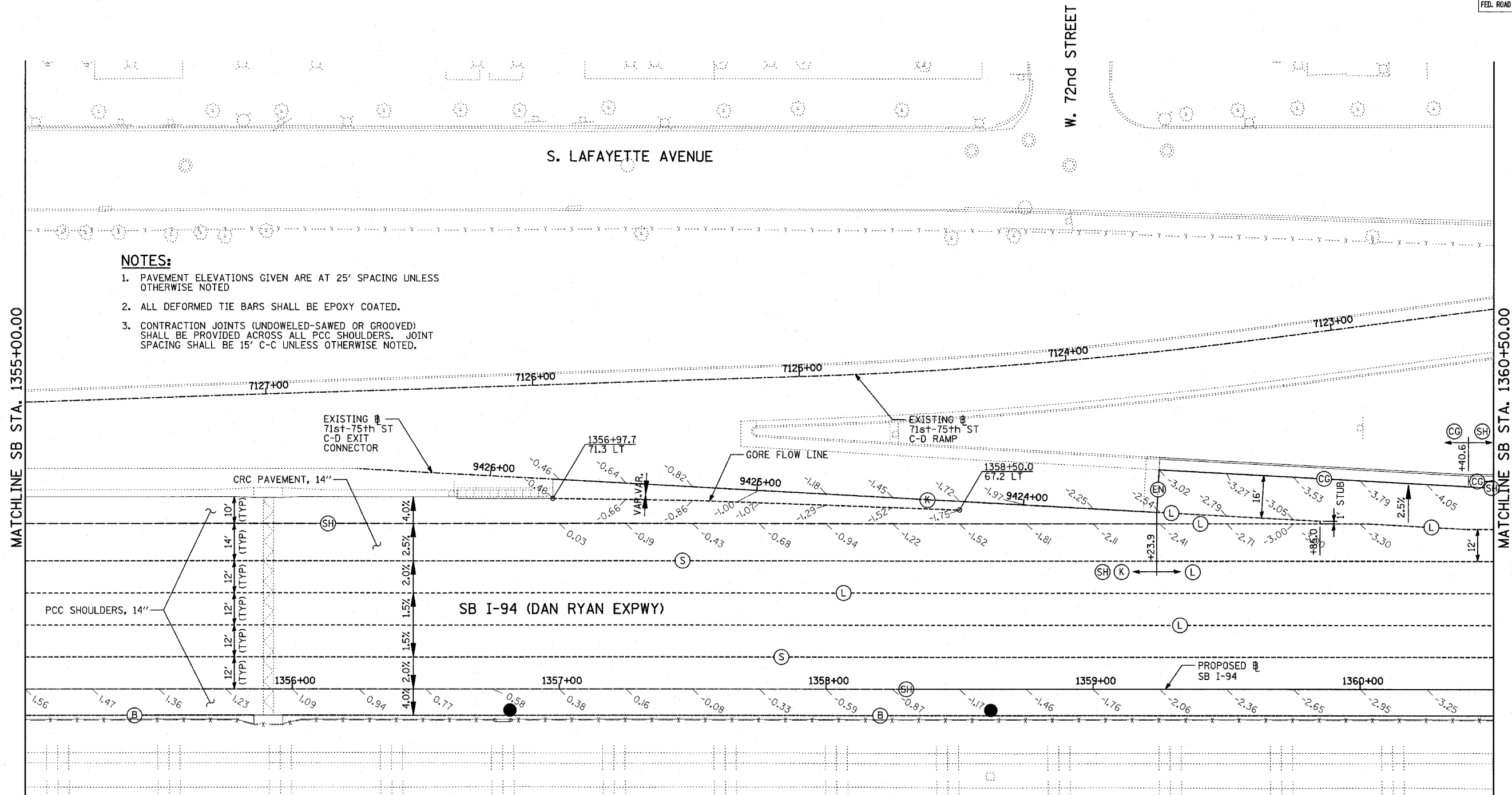
- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
  - (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
  - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
  - (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
  - (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
  - (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
  - (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**PAVEMENT JOINTING AND ELEVATION PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**STA. 1349+50.00 TO 1355+00.00**  
**SHEET 8 OF 11**

SCALE: 1"=20'  
 DATE: MARCH 1, 2006  
 DRAWN BY: JJS  
 CHECKED BY: MPG



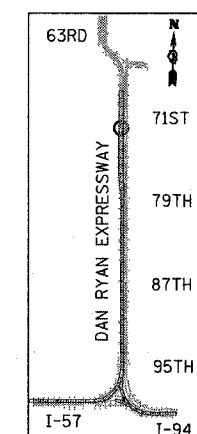


**NOTES:**

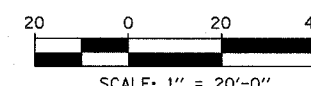
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

**LEGEND:**

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



LOCATION MAP

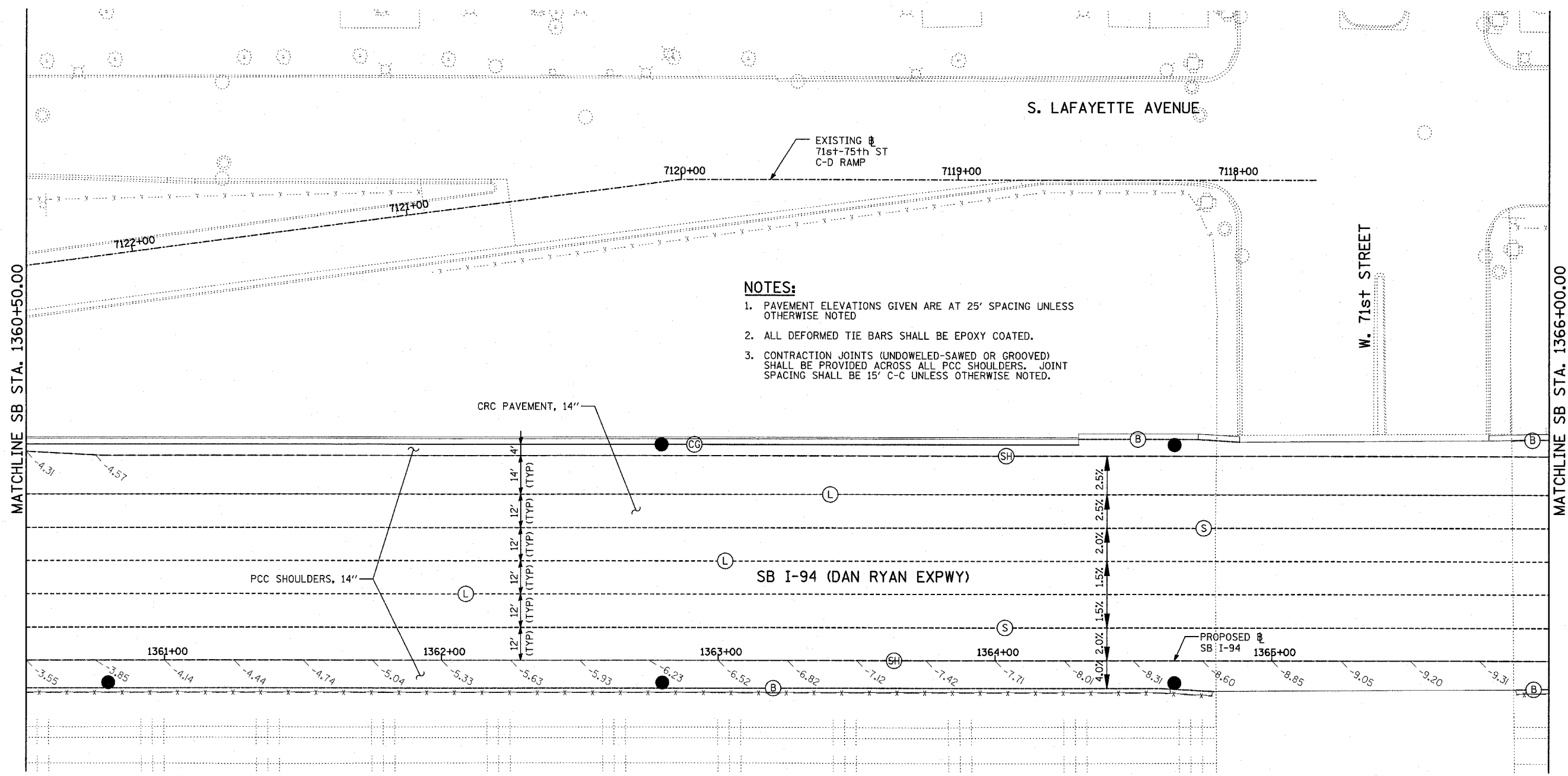
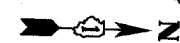


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 PAVEMENT JOINTING AND ELEVATION PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1355+00.00 TO 1360+50.00  
 SHEET 9 OF 11

SCALE: 1"=20'  
 DATE: MARCH 1, 2006  
 DRAWN BY: JJS  
 CHECKED BY: MPG

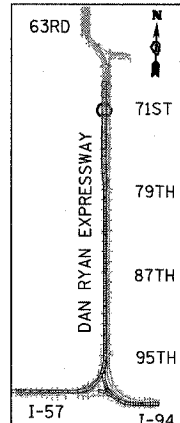




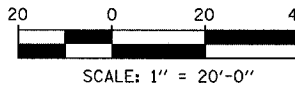
- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
  - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
  - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

MATCHLINE SB STA. 1360+50.00

MATCHLINE SB STA. 1366+00.00



LOCATION MAP



**LEGEND:**

- |  |  |
|--|--|
| <ul style="list-style-type: none"> <li>(B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.</li> <li>(CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL &amp; GROUT IN PLACE (INCLUDED IN THE COST OF CURB &amp; GUTTER)</li> <li>(S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)</li> <li>(T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL &amp; GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)</li> <li>(SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL &amp; GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)</li> </ul> | <ul style="list-style-type: none"> <li>(L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL &amp; GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</li> <li>(TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL &amp; GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</li> <li>(E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL &amp; GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</li> <li>(K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).</li> <li>(EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)</li> </ul> |
|--|--|

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**PAVEMENT JOINTING AND ELEVATION PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**SB I-94 STA. 1360+50.00 TO 1366+00.00**  
**SHEET 10 OF 11**

SCALE: 1"=20'  
DATE: MARCH 1, 2006  
DRAWN BY: JJS  
CHECKED BY: MPG



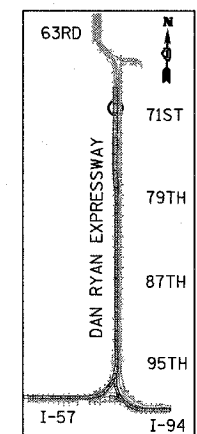
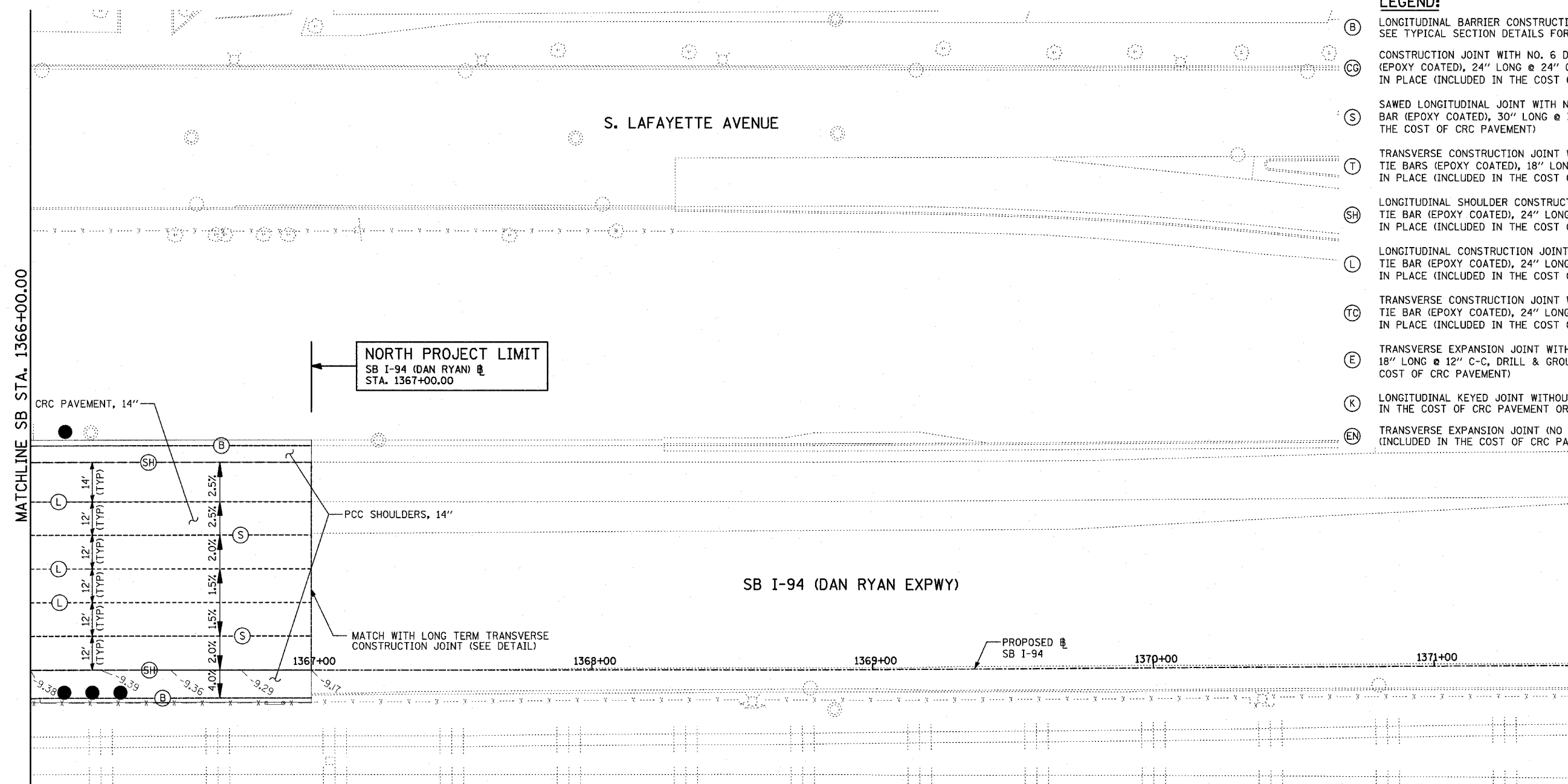
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	84
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

60B18

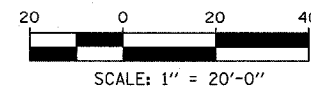


**LEGEND:**

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
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- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



LOCATION MAP



**NOTES:**

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 PAVEMENT JOINTING AND ELEVATION PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1366+00.00 TO 1367+00.00  
 SHEET 11 OF 11

SCALE: 1"=20'  
 DATE: MARCH 1, 2006  
 DRAWN BY: JJS  
 CHECKED BY: MPG

**TYLIN**INTERNATIONAL

**GENERAL NOTES:**

SEE SPECIAL PROVISION TITLED TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)

A WORK ZONE SPEED LIMIT OF 45 M.P.H. MUST BE MAINTAINED AT ALL TIMES.

QUANTITY FOR TEMPORARY PAVEMENT MARKING - LINE 6" WAS ASSUMED EQUAL TO THE LENGTH OF TEMPORARY CONCRETE BARRIER. UTILIZED WHEN THE BARRIER IS LOCATED ONE FOOT OR LESS FROM THE EDGE OF TRAVELED WAY.

ALL TEMPORARY PAVEMENT MARKINGS PLACED DURING THE WINTER LANE CONFIGURATION OR ON EXISTING PAVEMENT SHALL BE PAID FOR AS EPOXY PAVEMENT MARKING OF THE LINE TYPE AND WIDTH SPECIFIED.

ALL TEMPORARY PAVEMENT MARKINGS PLACED ON NEW PAVEMENT SHALL BE PAID FOR AS PAVEMENT MARKING TAPE, TYPE III OF THE LINE TYPE AND WIDTH SPECIFIED EXCEPT WHEN DIRECTED OTHERWISE.

A TOTAL OF 4 CHANGEABLE MESSAGE SIGNS SHALL BE LOCATED ALONG THE MAINLINE AND NEAR RAMP CLOSURES FOR THE DURATION OF THIS CONTRACT. EXACT PLACEMENT OF THE SIGNS SHALL BE DETERMINED BY THE RESIDENT ENGINEER.

ANY RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH THE TEMPORARY TRAFFIC LANES MUST HAVE THE REFLECTIVE LENSES REMOVED AS DIRECTED BY THE RESIDENT ENGINEER. MEASURE OF PAYMENT FOR REMOVAL OF REFLECTORS IS INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).

ALL CONSTRUCTION VEHICLES MUST STAY WITHIN THE HAUL ROAD. CROSSING EXIT RAMP TRAFFIC IS PROHIBITED.

THE CONTRACTOR SHALL BE RESPONSIBLE TO DIFFERENTIATE BETWEEN THE RAMP EXIT AND HAUL ROAD ENTRANCE.

THE CONTRACTOR SHALL UTILIZE EITHER THE A.I.S. OR C-D ROADWAY TO MERGE HAUL ROAD AND RAMP TRAFFIC AT ALL EXIT RAMP LOCATIONS. A STOP SIGN SHALL BE PLACED AT THE UPSTREAM END OF THE C-D ROADWAY TO STOP TRAFFIC ON THE RAMP FROM THE FRONTAGE ROAD.

ALL SIGNING FROM DISTRICT 1 DETAIL TC-18 SHALL BE APPLIED TO ALL WORK ZONE ACCESS OPTIONS WHICH MAY BE USED BY THE CONTRACTOR.

FOR ACCESS CONTROL, THE CONTRACTOR IS REQUIRED TO PLACE CHAIN LINK FENCE 6' (SPECIAL), 7'-2" CLEAR FROM THE CENTERLINE OF CTA TRACK, PRIOR TO REMOVING EXISTING CTA CHAIN LINK FENCE AND BARRIER WALL. THE CONTRACTOR MUST ALSO PLACE CHAIN LINK GATES, 6' X 6' SINGLE AT ALL LOCATIONS WHERE THERE ARE EXISTING CTA FENCE GATES. THE CHAIN LINK FENCE 6' (SPECIAL) MUST REMAIN IN PLACE UNTIL THE NEW PERMANENT CTA FENCE IS COMPLETED.

SEE MISCELLANEOUS DETAILS FOR MAINLINE TEMPORARY PAVEMENT AND MILLING OPERATIONS FOR THE POST-STAGE 1 WINTER LANE CONFIGURATION.

THE RESIDENT ENGINEER SHALL ASSESS THE EXISTING CONDITION OF THE PAVEMENT LOCATED ALONG THE INSIDE SHOULDER. DAMAGED AREAS WILL REQUIRE SHOULDER REHABILITATION INCLUDING MILLING AND RESURFACING. ADDITIONAL QUANTITIES OF 88 SQ YD FOR BITUMINOUS SURFACE REMOVAL 1 1/2" AND 8.0 TONS FOR BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", NTO HAVE BEEN INCLUDED IN THE PLANS.

**CONSTRUCTION STAGING NOTES**

**PRE STAGE 1 - NIGHT TIME OPERATIONS**

INSTALL RAMP CLOSURE ADVANCE INFORMATION SIGNS ON STAGE 1 RAMP CLOSURES A MINIMUM OF 2 WEEKS IN ADVANCE OF THE CLOSURE. SEE STAGE 1 RAMP CLOSURE GUIDELINES IN THE SPECIAL PROVISIONS.

COORDINATE 3 LANES OF TRAFFIC AT SB I-94 (DAN RYAN) STA. 1313+00 WITH CONTRACT #62593 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE MULTI-LANE WEAVE AT SB I-94 (DAN RYAN) STA. 1367+00 WITH CONTRACT #62302 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COMPLETE SHOULDER REHABILITATION FOR AREAS OF DAMAGED PAVEMENT PRIOR TO SHIFTING TRAFFIC.

REMOVE EXISTING PAVEMENT MARKINGS AND REPLACE WITH EPOXY PAVEMENT MARKINGS.

REMOVE LENSES FROM RAISED REFLECTIVE PAVEMENT MARKERS.

INSTALL TEMPORARY CONCRETE BARRIER WALL AND REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR STAGE 1 CONSTRUCTION.

COVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

**STAGE 1 CONSTRUCTION (AUGUST 1ST, 2006 - NOVEMBER 30, 2006)**

STAGE 1 CONSTRUCTS LANES 4, 5, & THE OUTSIDE SHOULDER ALONG THE MAINLINE SB I-94 (DAN RYAN).

MAINLINE SB I-94 (DAN RYAN) TRAFFIC WILL BE SHIFTED LEFT TO THE INSIDE SHOULDER IN 3-11' LANES.

**PRE STAGE WINTER LANE CONFIGURATION - NIGHT TIME OPERATIONS**

PERFORM TEMPORARY PAVEMENT AND COLD MILLING OPERATIONS ALONG SB I-94 (DAN RYAN) PRIOR TO THE START OF THE WINTER LANE CONFIGURATION.

COORDINATE 4 LANES OF TRAFFIC AT SB I-94 (DAN RYAN) STA. 1313+00 WITH CONTRACT #62593 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE MULTI-LANE WEAVE AT SB I-94 (DAN RYAN) STA. 1367+00 WITH CONTRACT #62302 IN ORDER TO MAINTAIN TRAFFIC FLOW.

REMOVE CONFLICTING TEMPORARY PAVEMENT MARKINGS FROM STAGE 1 AND REPLACE WITH EPOXY PAVEMENT MARKINGS.

RELOCATE TEMPORARY CONCRETE BARRIER WALL TO CREATE ADEQUATE SHOULDERS FOR SNOW STORAGE AND RELOCATE REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR THE WINTER LANE CONFIGURATION.

COVER & UNCOVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

**WINTER LANE CONFIGURATION (DECEMBER 1, 2006 - APRIL 1, 2007)**

MAINLINE SB I-94 (DAN RYAN) TRAFFIC WILL BE SHIFTED LEFT TO THE INSIDE SHOULDER IN 4-11' LANES.

**PRE STAGE 2 - NIGHT TIME OPERATIONS**

COORDINATE 3 LANES OF SPLIT TRAFFIC AT SB I-94 (DAN RYAN) STA. 1313+00 WITH CONTRACT #62593 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE MULTI-LANE WEAVE AND TRAFFIC SPLIT AT SB I-94 (DAN RYAN) STA. 1378+90 WITH CONTRACTS #60A63 AND #62303 IN ORDER TO MAINTAIN TRAFFIC FLOW.

REMOVE CONFLICTING TEMPORARY PAVEMENT MARKINGS FROM THE WINTER LANE CONFIGURATION AND REPLACE WITH EPOXY OR PAVEMENT MARKING TAPE DEPENDING IF THE PAVEMENT IS EXISTING, NEWLY BUILT, OR UNLESS DIRECTED OTHERWISE.

RELOCATE TEMPORARY CONCRETE BARRIER WALL AND REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR STAGE 2.

COVER & UNCOVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

**STAGE 2 CONSTRUCTION (APRIL 2, 2007 - MAY 31, 2007)**

STAGE 2 CONSTRUCTS LANE 3 ALONG THE MAINLINE SB I-94 (DAN RYAN) FROM STA. 1313+00 TO STA. 1367+00.

MAINLINE SB I-94 (DAN RYAN) TRAFFIC WILL BE SPLIT INTO 2-11' EXPRESS LANES SHIFTED LEFT TO THE INSIDE SHOULDER AND 1-11' LOCAL LANE SHIFTED RIGHT TO THE OUTSIDE SHOULDER.

**PRE STAGE 3 - NIGHT TIME OPERATIONS**

COORDINATE 3 LANES OF TRAFFIC AT SB I-94 (DAN RYAN) STA. 1313+00 WITH CONTRACT #62593 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE MULTI-LANE WEAVE AT SB I-94 (DAN RYAN) STA. 1379+60 WITH CONTRACTS #60A63 AND #62303 IN ORDER TO MAINTAIN TRAFFIC FLOW.

REMOVE CONFLICTING TEMPORARY PAVEMENT MARKINGS FROM STAGE 2 AND REPLACE WITH PAVEMENT MARKING TAPE.

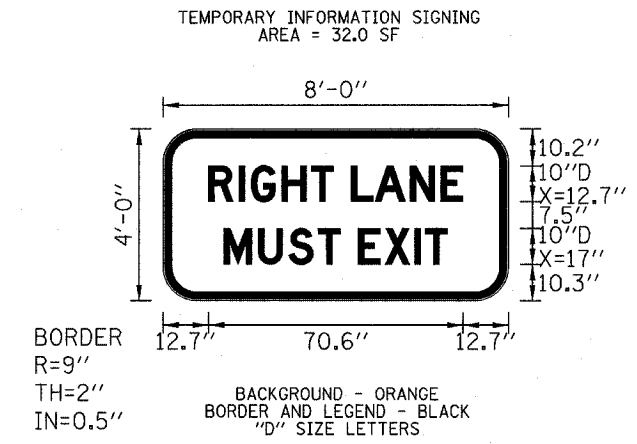
RELOCATE TEMPORARY CONCRETE BARRIER WALL AND REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR STAGE 3.

COVER & UNCOVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

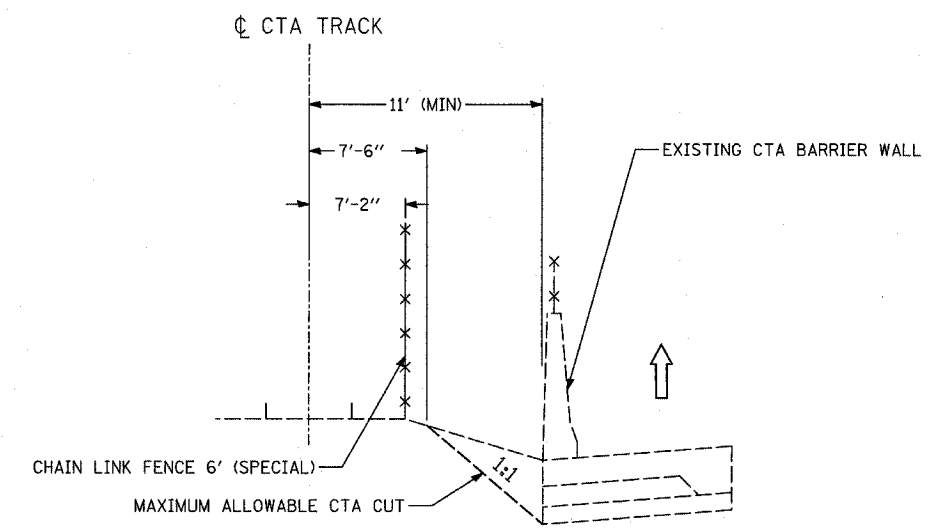
**STAGE 3 CONSTRUCTION (JUNE 1, 2007 - OCTOBER 31, 2007)**

STAGE 3 CONSTRUCTS LANES 1, 2, & THE INSIDE SHOULDER ALONG THE MAINLINE SB I-94 (DAN RYAN) FROM STA. 1313+00 TO STA. 1367+00.

MAINLINE SB I-94 (DAN RYAN) TRAFFIC WILL BE SHIFTED RIGHT TO THE OUTSIDE SHOULDER IN 3-11' LANES.

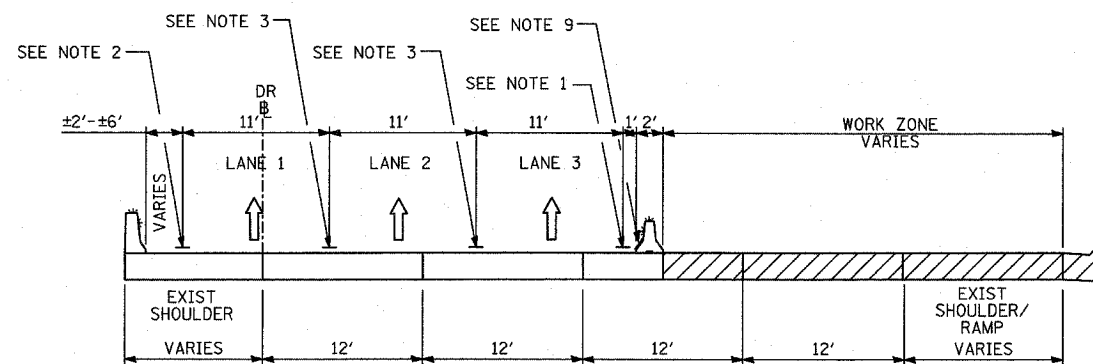


**'RIGHT LANE MUST EXIT' SIGN DETAIL**



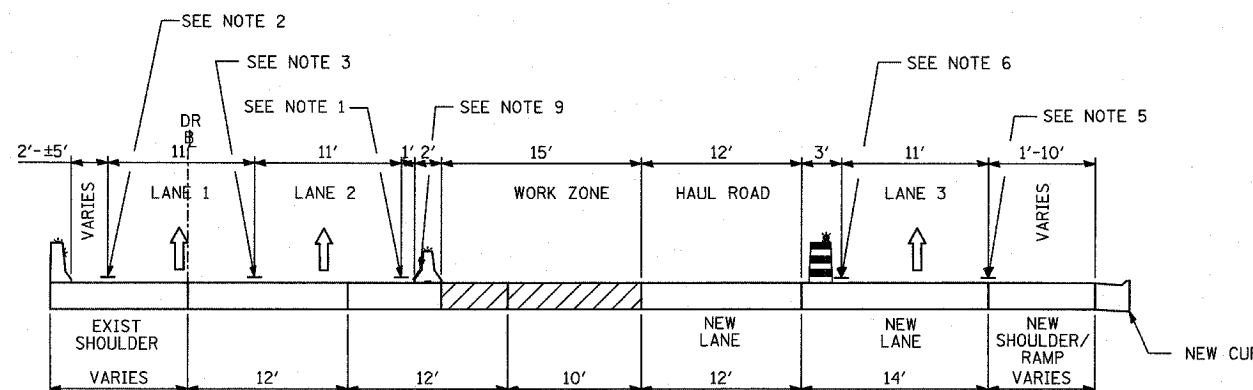
**LOCATION OF CHAIN LINK FENCE 6' (SPECIAL)**

REVISIONS	
NAME	DATE



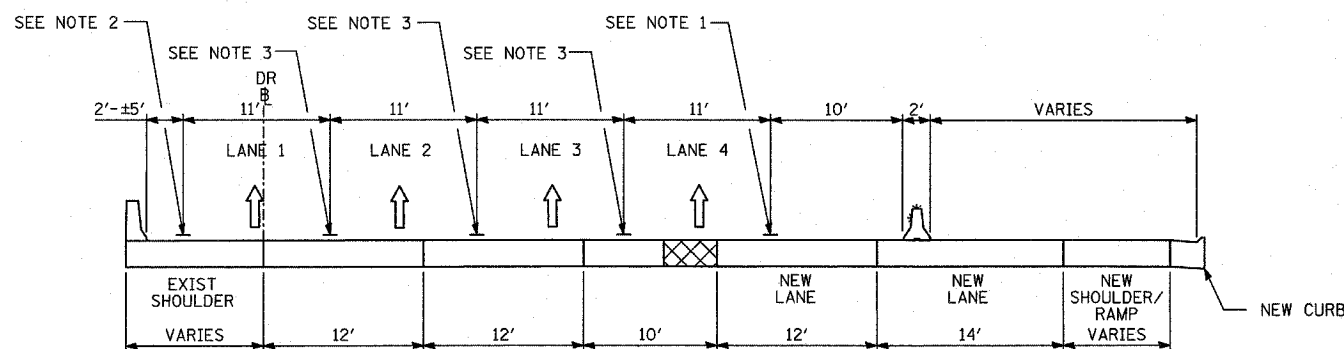
C-C

STAGE 1: SB I-94 DAN RYAN; STA. 1313+00 TO STA. 1367+00



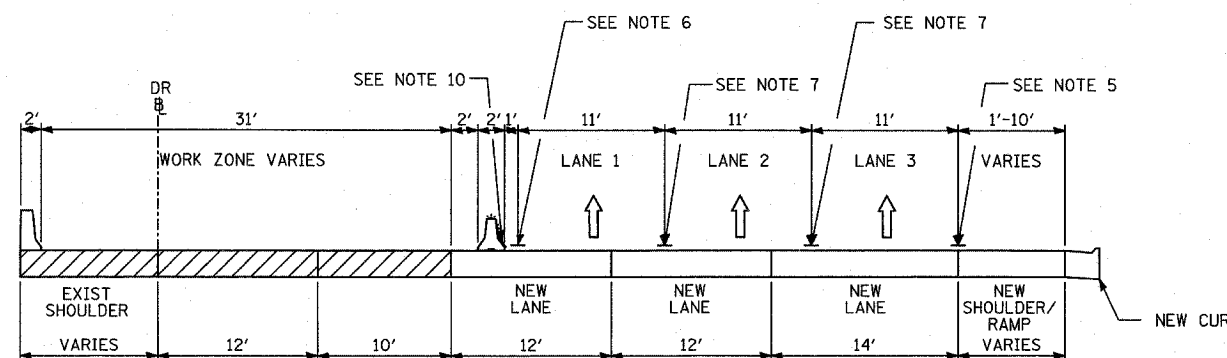
L-L

STAGE 2: SB I-94 DAN RYAN; STA. 1313+00 TO STA. 1367+00



K-K

WINTER LANE CONFIGURATION: SB I-94 DAN RYAN; STA. 1313+00 TO 1367+00



P-P

STAGE 3: SB I-94 DAN RYAN; STA. 1313+00 TO STA. 1367+00

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

**LEGEND:**

- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

\*ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

**TYLIN** INTERNATIONAL

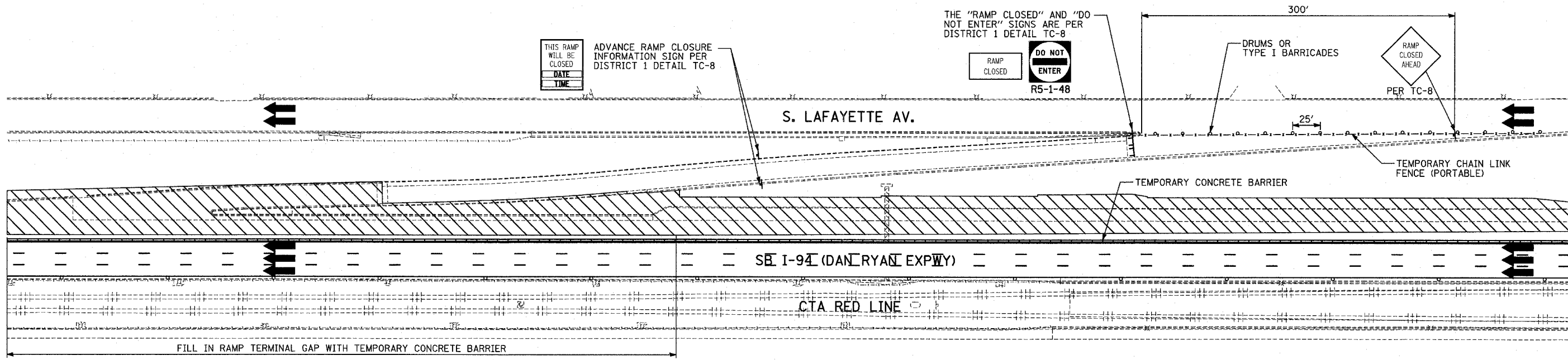
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 GENERAL NOTES  
 CONSTRUCTION STAGING NOTES  
 MAINTENANCE OF TRAFFIC DETAILS  
 SHEET 2 OF 4

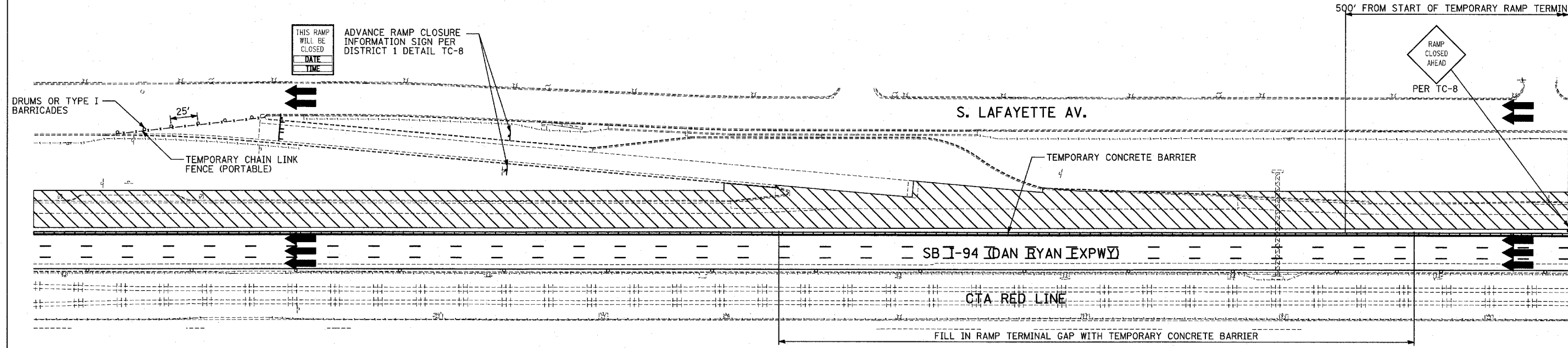
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 DATE: MARCH 1, 2006  
 DRAWN BY: RTM  
 CHECKED BY: TGB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	87
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

60B1E



TYPICAL ENTRANCE RAMP CLOSURE FOR STAGE 1



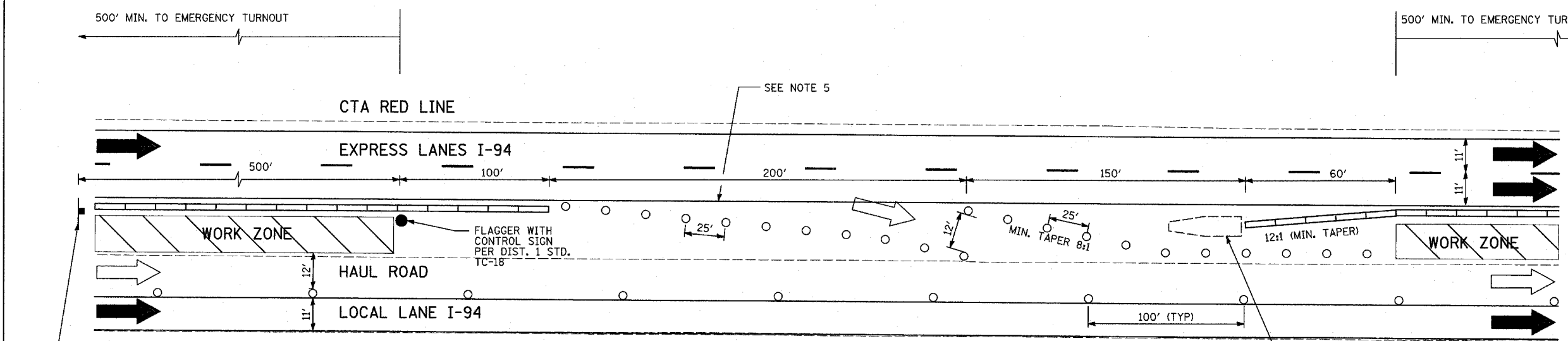
TYPICAL EXIT RAMP CLOSURE FOR STAGE 1

**TYLIN INTERNATIONAL**

WORK ZONE FOR LANES 4, 5, OUTSIDE SHOULDER, AND RAMP TERMINAL

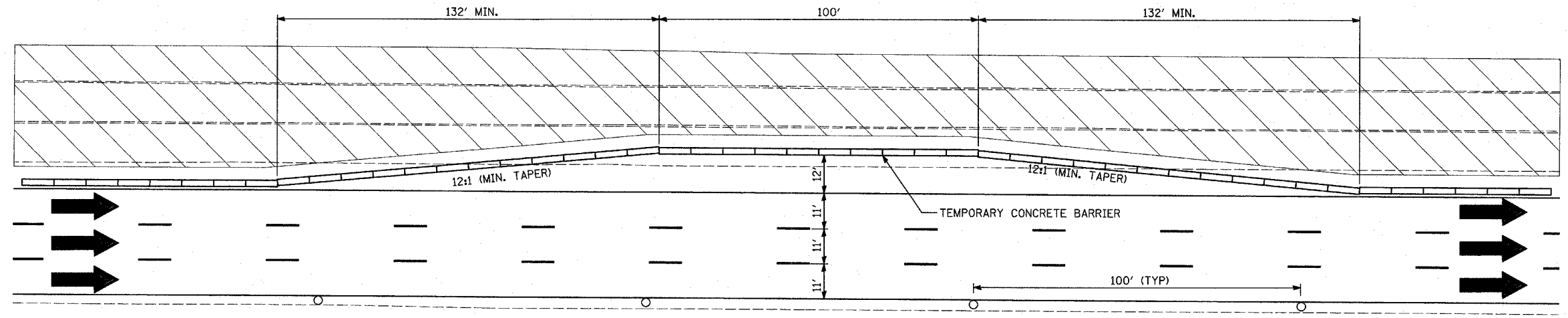
REVISIONS	
NAME	DATE
REVISOR	04/15/05

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
GENERAL NOTES  
CONSTRUCTION STAGING NOTES  
MAINTENANCE OF TRAFFIC DETAILS  
SHEET 3 OF 4  
SCALE: NO SCALE  
DATE: MARCH 1, 2006  
DRAWN BY: RTM  
CHECKED BY: TGB



**SUGGESTED ACCESS FROM EXPRESS LANES**

- LEGEND:**
- TEMPORARY CONCRETE BARRIER
  - TRAFFIC LANE
  - CONSTRUCTION TRAFFIC
  - WORK ZONE
  - DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
  - ARROW BOARD



**MAINTENANCE OF TRAFFIC EMERGENCY TURNOUT DETAIL**  
EMERGENCY TURNOUTS WILL BE LOCATED APPROXIMATELY EVERY HALF MILE

**NOTES:**

1. THERE CAN BE NO MORE THAN ONE (1) WORK ZONE ACCESS/EGRESS COMBINATIONS AND THE CONTRACTOR MUST MAINTAIN AT LEAST ONE (1) EXPRESS LANE ACCESS DURING STAGE 2 CONSTRUCTION.
2. THE CONTRACTOR SHALL NOT ENTER OR EXIT THE HAUL ROAD WITHIN THE 100' BARREL-SPACING AREA. WORK ZONE ACCESS AND EGRESS WILL ONLY BE PERMITTED AT THE DESIGNATED LOCATIONS.
3. FOR EXPRESS LANE WORK ZONE EGRESS, TAPER LENGTHS SHALL FOLLOW DISTRICT 1 STANDARD TC-18.
4. THE CONTRACTOR SHALL CLOSE OPENINGS WITH BARRELS WHEN NOT BEING USED FOR ACCESS.
5. TEMPORARY PAVEMENT MARKINGS SHALL BE CARRIED THRU THE OPENING.

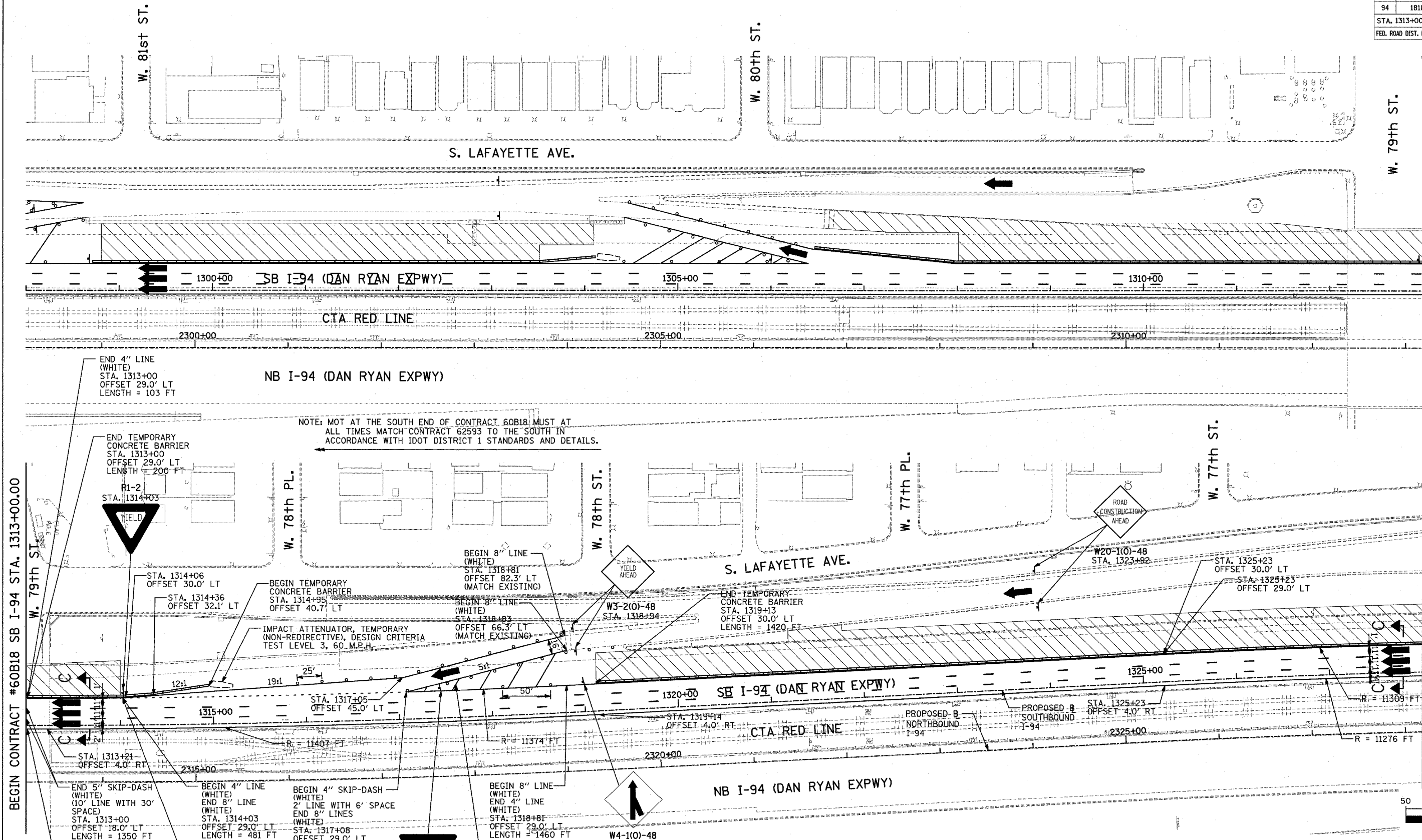
**TYLIN**INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
GENERAL NOTES  
CONSTRUCTION STAGING NOTES  
MAINTENANCE OF TRAFFIC DETAILS  
SHEET 4 OF 4

SCALE: NO SCALE  
DATE: MARCH 1, 2006

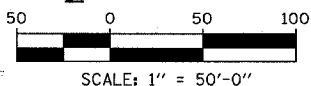
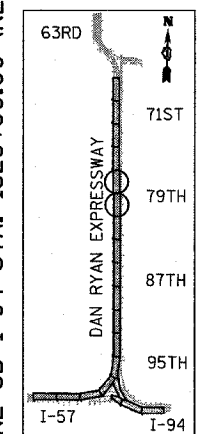
DRAWN BY: RTM  
CHECKED BY: TGB



NOTE: MOT AT THE SOUTH END OF CONTRACT 60B18 MUST AT ALL TIMES MATCH CONTRACT 62593 TO THE SOUTH IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARDS AND DETAILS.

BEGIN CONTRACT #60B18 SB I-94 STA. 1313+00.00

MATCHLINE SB I-94 STA. 1328+00.00 (NEXT SHEET)



TYLIN INTERNATIONAL

**NOTE:**  
 ON ALL THE STAGE 1 PLAN SHEETS, CALLOUTS FOR TEMPORARY CONCRETE BARRIER ARE TO THE FRONT FACE OF THE WALL.

- LEGEND:**
- TEMPORARY CONCRETE BARRIER
  - TRAFFIC LANE
  - WORK ZONE
  - BARRICADE, TYPE III
  - DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
  - ARROW BOARD
  - TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 MAINTENANCE OF TRAFFIC PLAN  
 STAGE 1 - SB I-94  
 SB I-94 STA. 1298+00 TO 1328+00  
 SHEET 1 OF 3

SCALE: 1"=50'-0"  
 DATE: MARCH 1, 2006

DRAWN BY: JDF  
 CHECKED BY: RTM

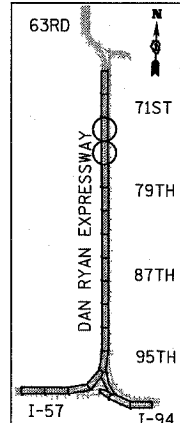
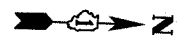
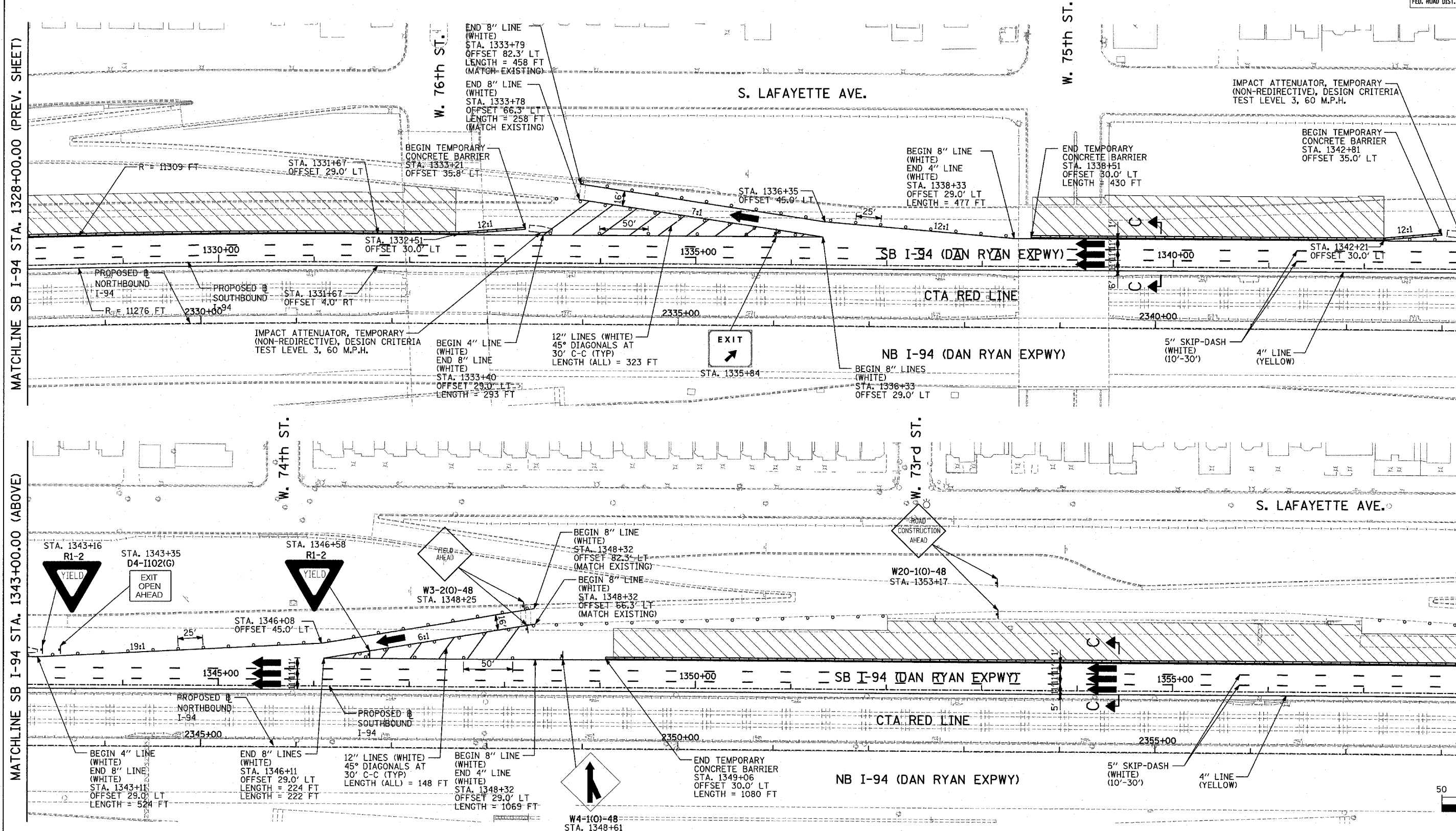


MATCHLINE SB I-94 STA. 1328+00.00 (PREV. SHEET)

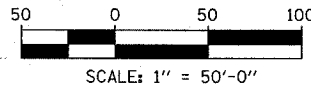
MATCHLINE SB I-94 STA. 1343+00.00 (BELOW)

MATCHLINE SB I-94 STA. 1343+00.00 (ABOVE)

MATCHLINE SB I-94 STA. 1358+00.00 (NEXT SHEET)



LOCATION MAP



**TYLIN INTERNATIONAL**

- LEGEND:**
- TEMPORARY CONCRETE BARRIER
  - TRAFFIC LANE
  - WORK ZONE
  - BARRICADE, TYPE III
  - DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
  - ARROW BOARD
  - TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

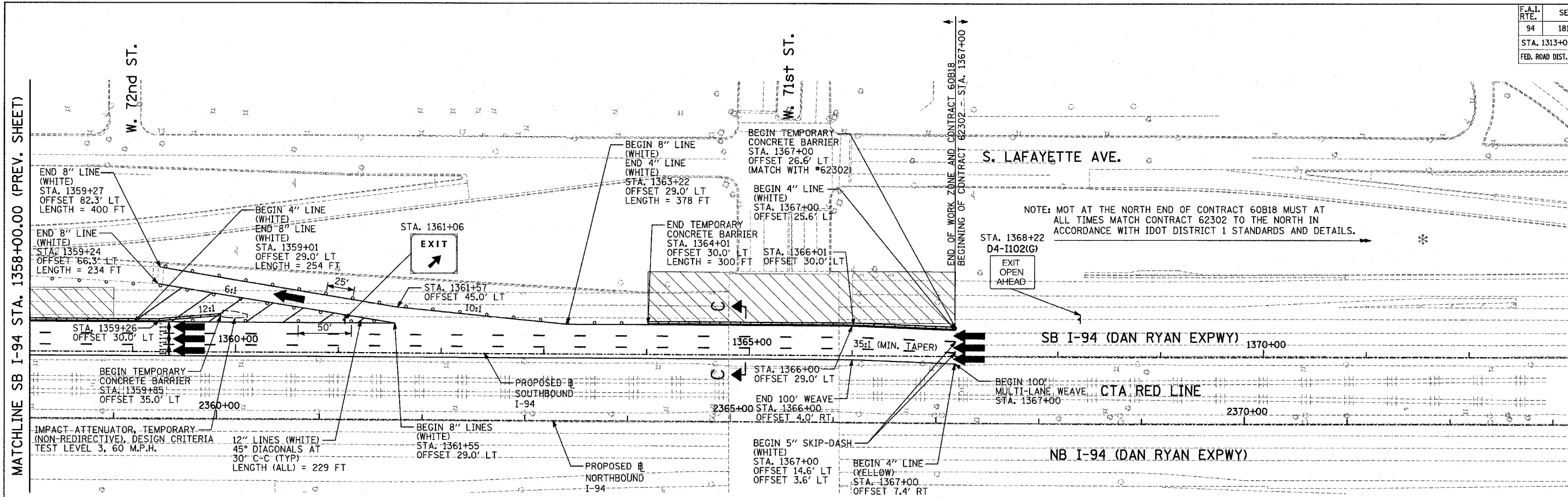
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**MAINTENANCE OF TRAFFIC PLAN**  
 STAGE 1 - SB I-94  
 SB I-94 STA. 1328+00 TO 1358+00  
 SHEET 2 OF 3

SCALE: 1"=50'-0"  
 DATE: MARCH 1, 2006

DRAWN BY: JDF  
 CHECKED BY: RTM

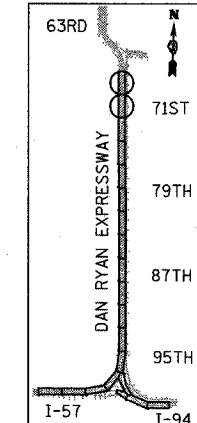
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	91
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

60B18

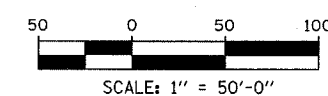


NOTE: MOT AT THE NORTH END OF CONTRACT 60B18 MUST AT ALL TIMES MATCH CONTRACT 62302 TO THE NORTH IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARDS AND DETAILS.

MATCHLINE SB I-94 STA. 1358+00.00 (PREV. SHEET)



LOCATION MAP



**LEGEND:**

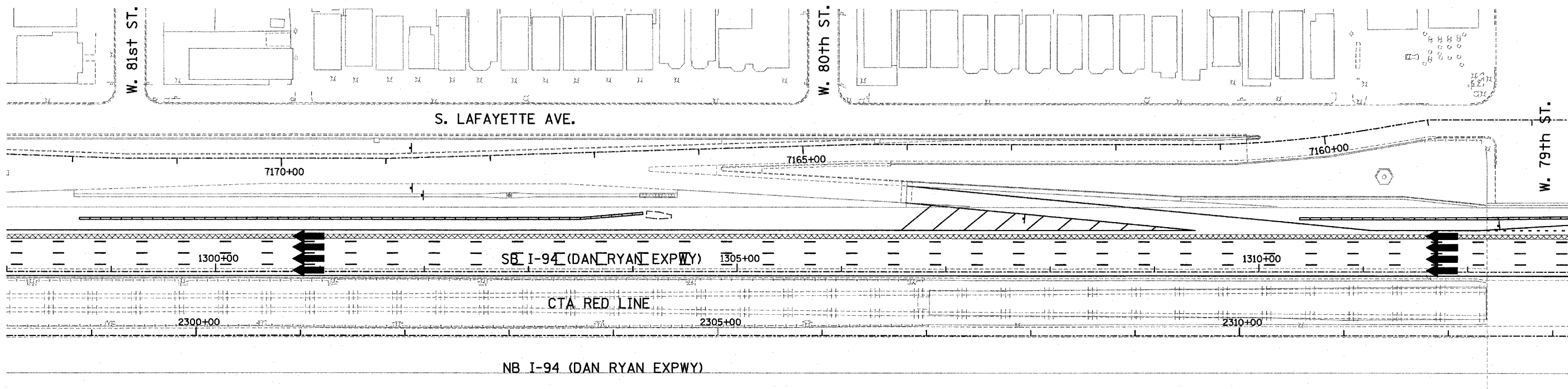
	TEMPORARY CONCRETE BARRIER		DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
	TRAFFIC LANE		ARROW BOARD
	WORK ZONE		TEMPORARY PAVEMENT
	BARRICADE, TYPE III		

REVISIONS	
NAME	DATE

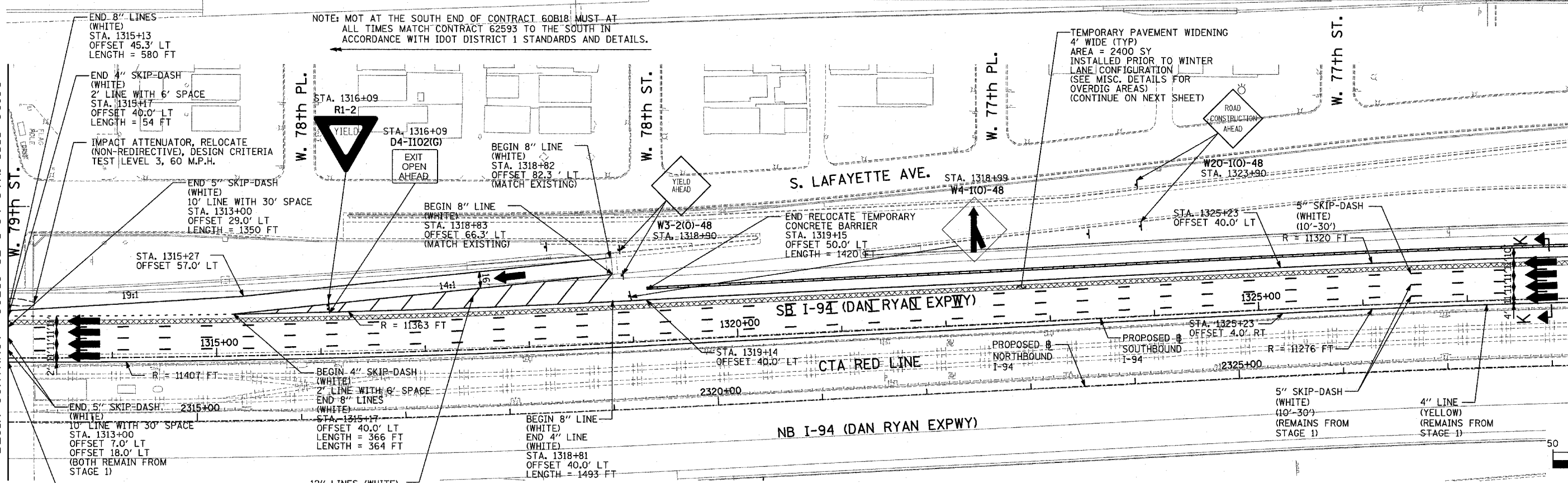
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**MAINTENANCE OF TRAFFIC PLAN**  
**STAGE 1 - SB I-94**  
**SB I-94 STA. 1358+00 TO 1388+00**  
**SHEET 3 OF 3**

SCALE: 1"=50'-0"  
 DATE: MARCH 1, 2006  
 DRAWN BY: JDF  
 CHECKED BY: RTM

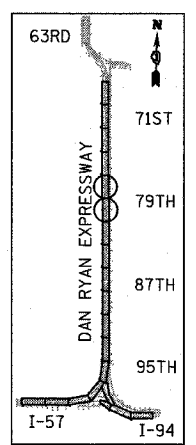
**TYLIN** INTERNATIONAL



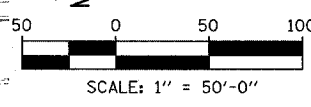
END CONTRACT #62593 SB I-94 STA. 1313+00.00



MATCHLINE SB I-94 STA. 1328+00.00 (NEXT SHEET)



LOCATION MAP



BEGIN CONTRACT #60B18 SB I-94 STA. 1313+00.00

NOTE: MOT AT THE SOUTH END OF CONTRACT 60B18 MUST AT ALL TIMES MATCH CONTRACT 62593 TO THE SOUTH IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARDS AND DETAILS.

TEMPORARY PAVEMENT WIDENING 4' WIDE (TYP) AREA = 2400 SY INSTALLED PRIOR TO WINTER LANE CONFIGURATION (SEE MISC. DETAILS FOR OVERDIG AREAS) (CONTINUE ON NEXT SHEET)

END 4" LINES (YELLOW) STA. 1315+13 OFFSET 4.0' RT (REMAIN FROM STAGE 1)

TYLIN INTERNATIONAL

**NOTE:**  
ON ALL THE WINTER LANE CONFIGURATION PLAN SHEETS, CALLOUTS FOR TEMPORARY CONCRETE BARRIER ARE TO THE FRONT FACE OF THE WALL.

- LEGEND:**
- TEMPORARY CONCRETE BARRIER
  - TRAFFIC LANE
  - WORK ZONE
  - BARRICADE, TYPE III
  - DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
  - ARROW BOARD
  - TEMPORARY PAVEMENT

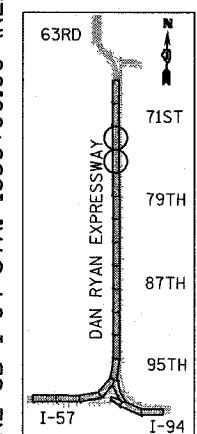
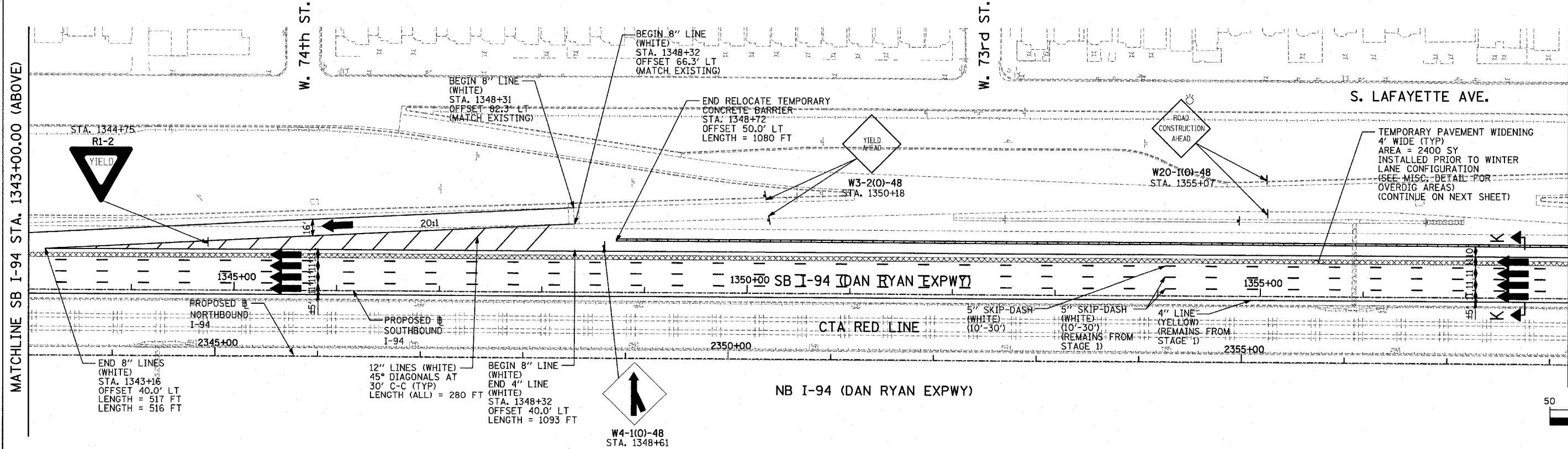
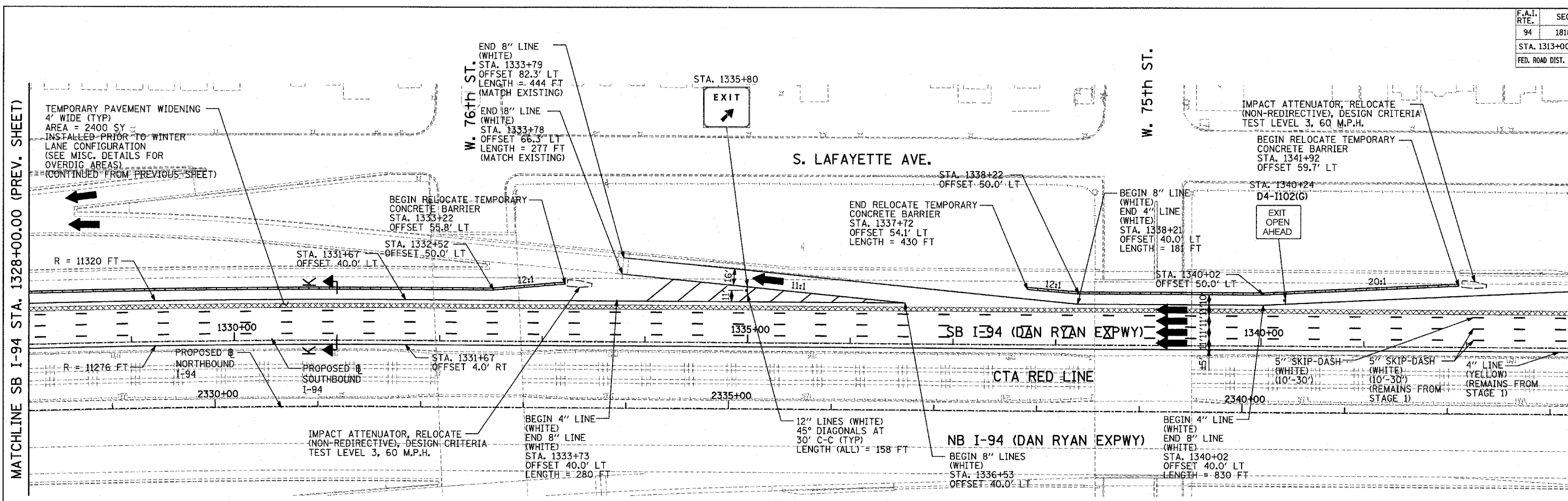
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**MAINTENANCE OF TRAFFIC PLAN**  
**WINTER LANE CONFIGURATION - SB I-94**  
**SB I-94 STA. 1298+00 TO 1328+00**  
**SHEET 1 OF 3**

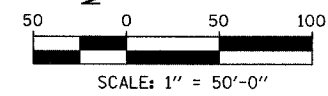
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 DATE: MARCH 1, 2006  
 DRAWN BY: JDF  
 CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	93
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

60B18



LOCATION MAP



**LEGEND:**

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 MAINTENANCE OF TRAFFIC PLAN  
 WINTER LANE CONFIGURATION - SB I-94  
 SB I-94 STA. 1328+00 TO 1358+00  
 SHEET 2 OF 3

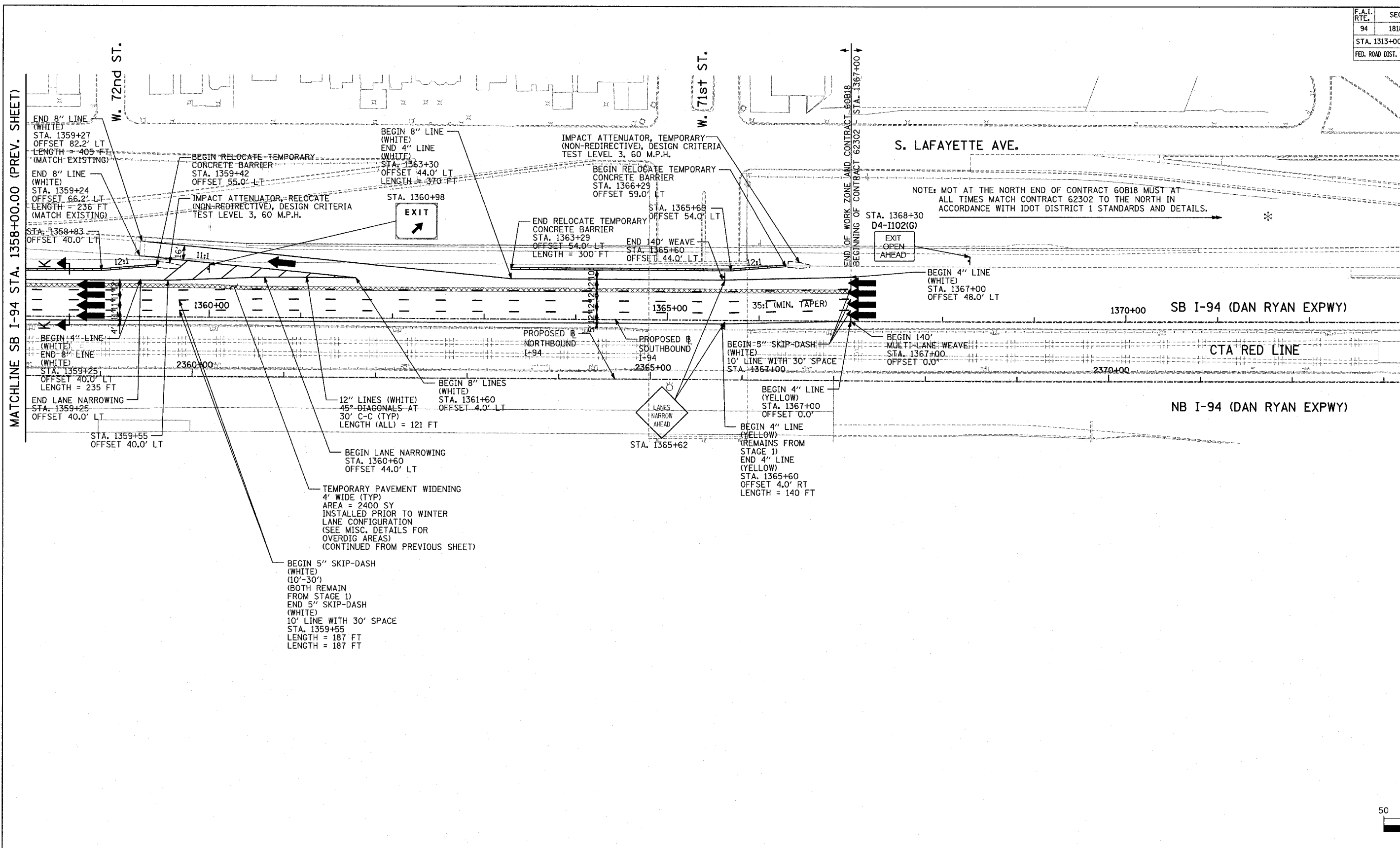
SCALE: 1"=50'-0"  
 DATE: MARCH 1, 2006

DRAWN BY: JDF  
 CHECKED BY: RTM

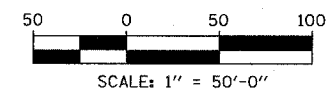
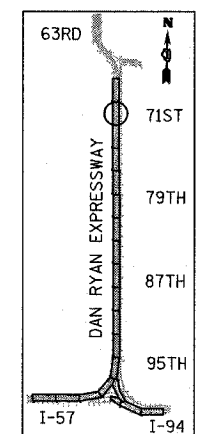
TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	94
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

60B18



NOTE: MOT AT THE NORTH END OF CONTRACT 60B18 MUST AT ALL TIMES MATCH CONTRACT 62302 TO THE NORTH IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARDS AND DETAILS.



**LEGEND:**

	TEMPORARY CONCRETE BARRIER		DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
	TRAFFIC LANE		ARROW BOARD
	WORK ZONE		TEMPORARY PAVEMENT
	BARRICADE, TYPE III		

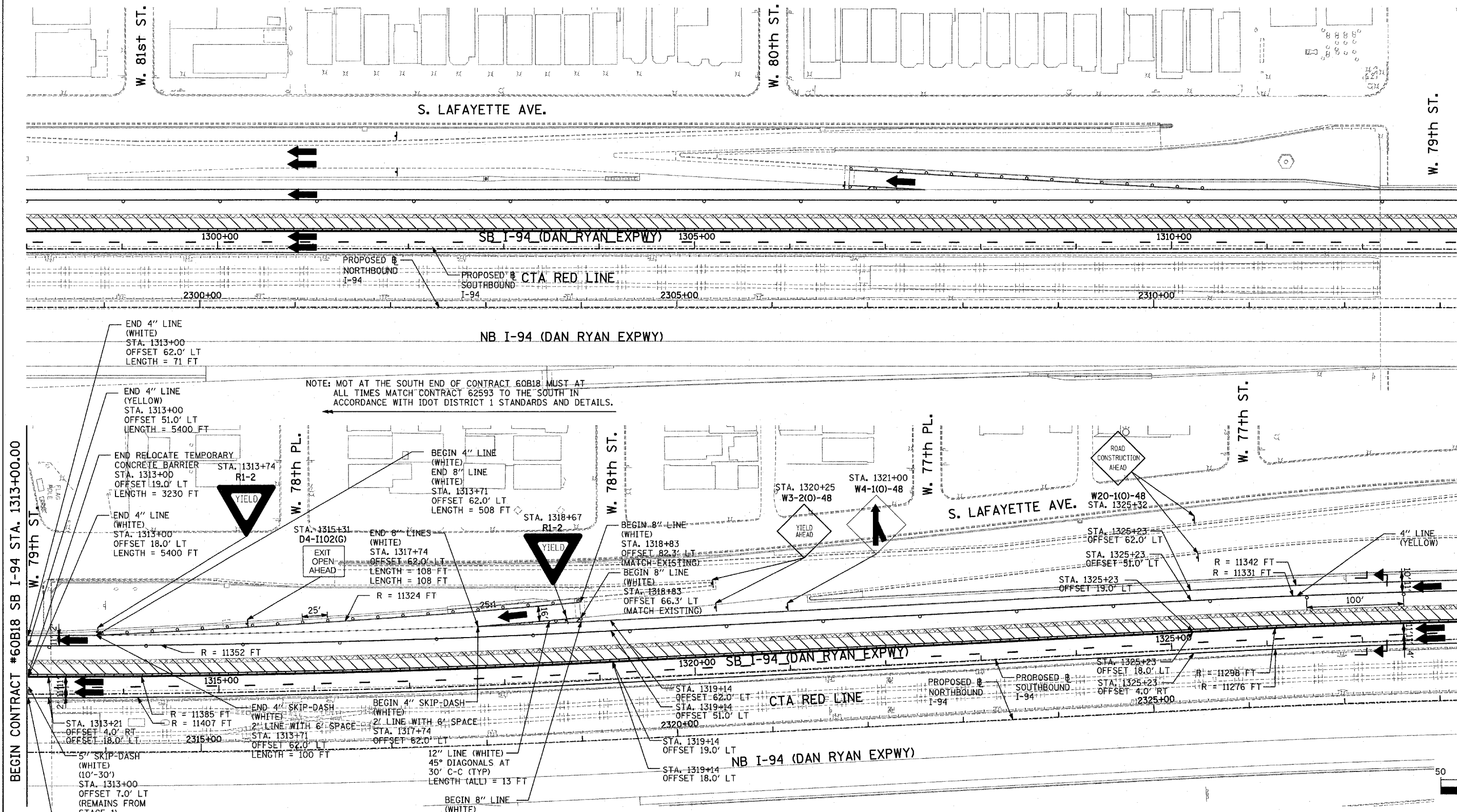
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 MAINTENANCE OF TRAFFIC PLAN  
 WINTER LANE CONFIGURATION - SB I-94  
 SB I-94 STA. 1358+00 TO 1388+00  
 SHEET 3 OF 3

SCALE: 1"=50'-0"  
 DATE: MARCH 1, 2006

DRAWN BY: JDF  
 CHECKED BY: RTM

TYLIN INTERNATIONAL

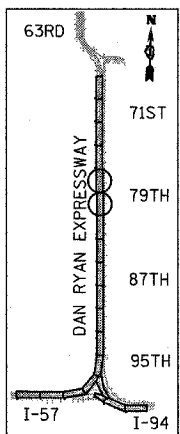


NOTE: MOT AT THE SOUTH END OF CONTRACT 60B18 MUST AT ALL TIMES MATCH CONTRACT 62593 TO THE SOUTH IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARDS AND DETAILS.

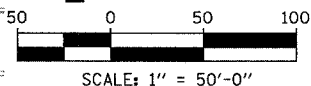
BEGIN CONTRACT #60B18 SB I-94 STA. 1313+00.00

END CONTRACT #62593 SB I-94 STA. 1313+00.00

MATCHLINE SB I-94 STA. 1328+00.00 (NEXT SHEET)



LOCATION MAP



TYLIN INTERNATIONAL

**NOTE:**  
ON ALL THE STAGE 2 PLAN SHEETS, CALLOUTS FOR TEMPORARY CONCRETE BARRIER ARE TO THE FRONT FACE OF THE WALL.

**LEGEND:**

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

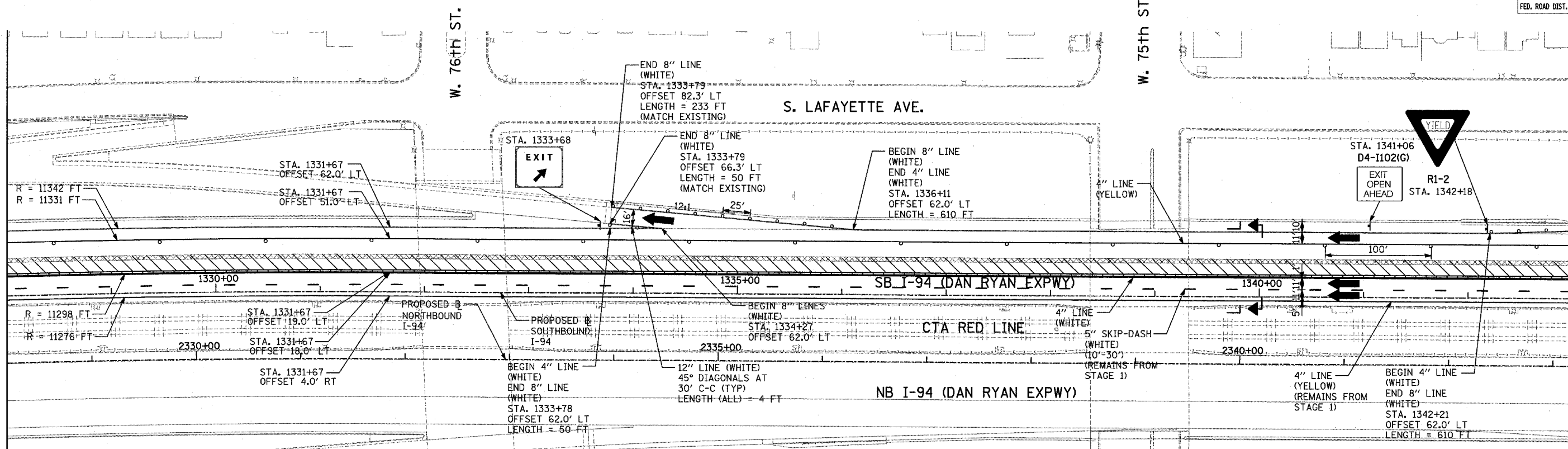
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
 MAINTENANCE OF TRAFFIC PLAN  
 STAGE 2 - SB I-94  
 SB I-94 STA. 1298+00 TO 1328+00  
 SHEET 1 OF 3

SCALE: 1"=50'-0"  
 DATE: MARCH 1, 2006

DRAWN BY: JDF  
 CHECKED BY: RTM

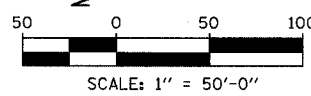
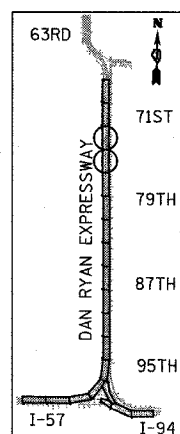
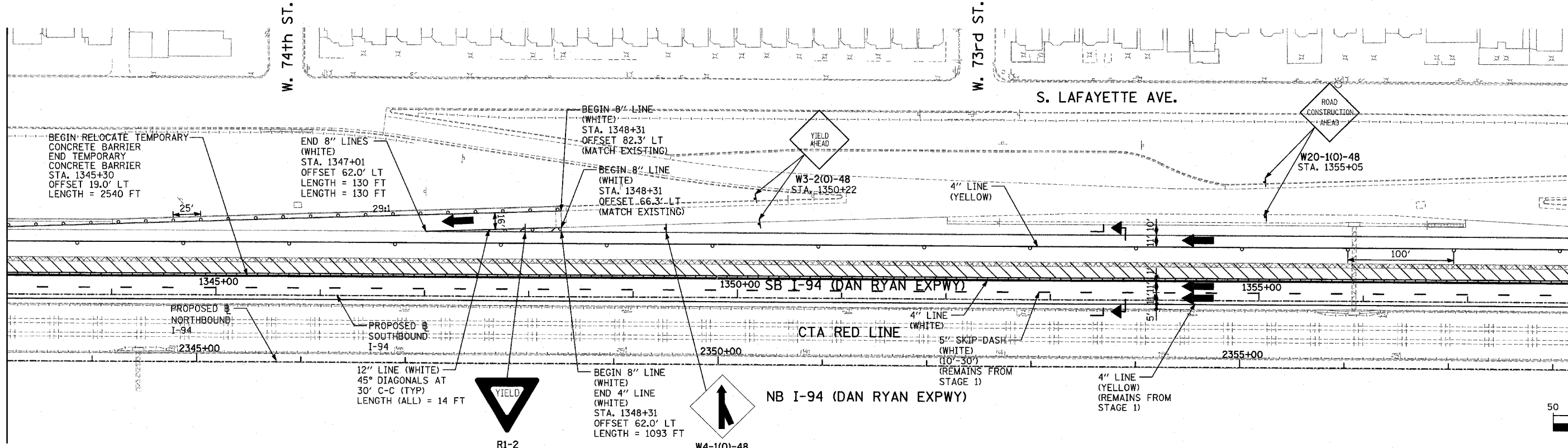
MATCHLINE SB I-94 STA. 1328+00.00 (PREV. SHEET)

MATCHLINE SB I-94 STA. 1343+00.00 (BELOW)



MATCHLINE SB I-94 STA. 1343+00.00 (ABOVE)

MATCHLINE SB I-94 STA. 1358+00.00 (NEXT SHEET)



**TYLIN** INTERNATIONAL

- LEGEND:**
- TEMPORARY CONCRETE BARRIER
  - TRAFFIC LANE
  - WORK ZONE
  - BARRICADE, TYPE III
  - DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
  - ARROW BOARD
  - TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**MAINTENANCE OF TRAFFIC PLAN**  
**STAGE 2 - SB I-94**  
**SB I-94 STA. 1328+00 TO 1358+00**  
**SHEET 2 OF 3**

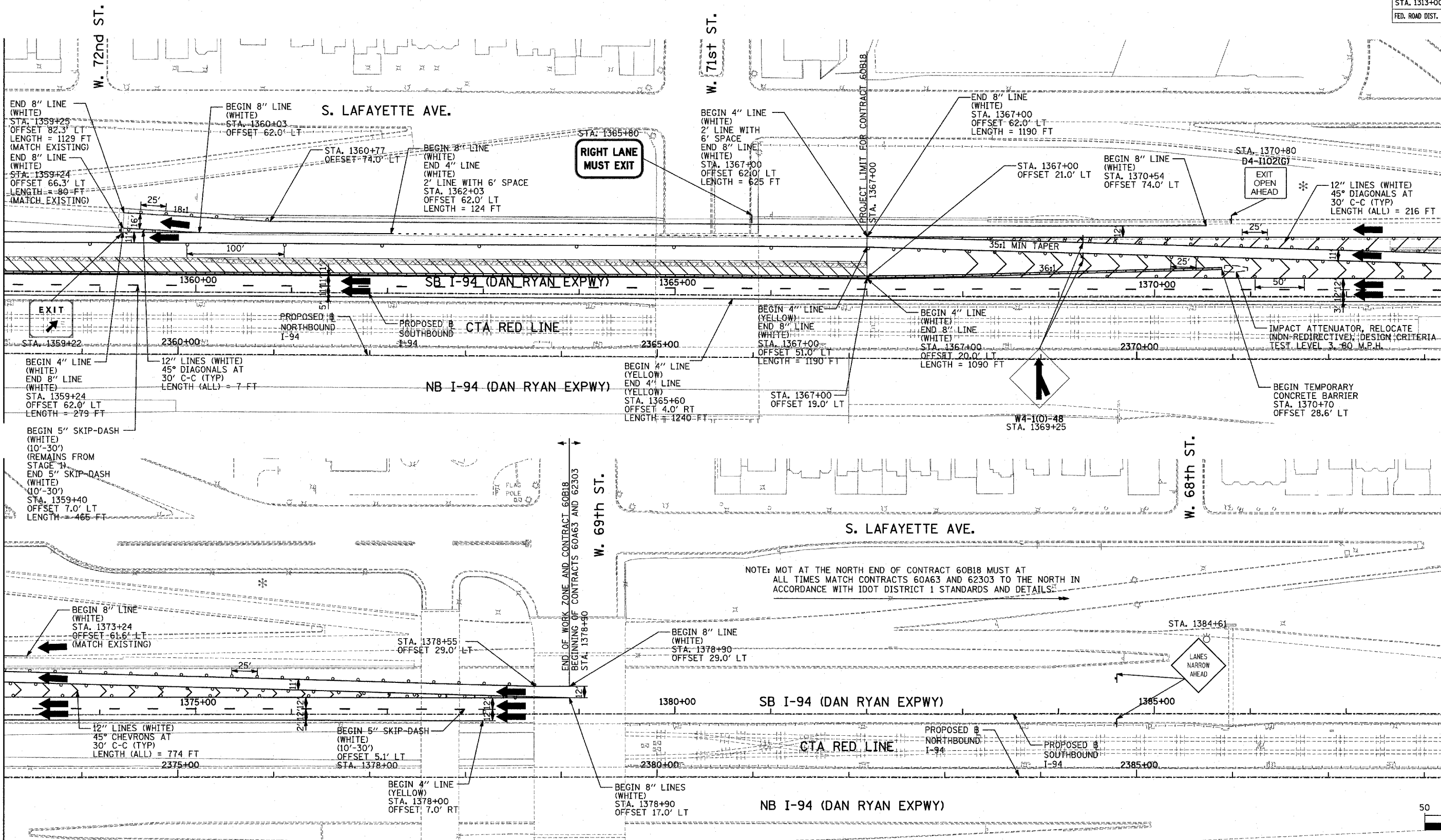
SCALE: 1"=50'-0"  
 DATE: MARCH 1, 2006

DRAWN BY: JDF  
 CHECKED BY: RTM

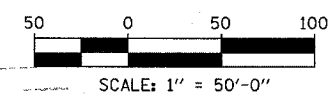
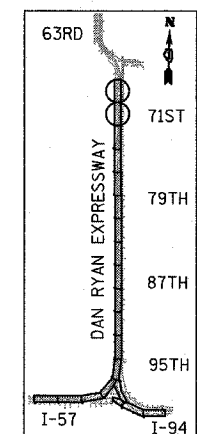
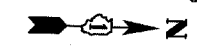
MATCHLINE SB I-94 STA. 1358+00.00 (PREV. SHEET)

MATCHLINE SB I-94 STA. 1373+00.00 (ABOVE)

MATCHLINE SB I-94 STA. 1373+00.00 (BELOW)



NOTE: MOT AT THE NORTH END OF CONTRACT 60B18 MUST AT ALL TIMES MATCH CONTRACTS 60A63 AND 62303 TO THE NORTH IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARDS AND DETAILS.



**LEGEND:**

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 MAINTENANCE OF TRAFFIC PLAN  
 STAGE 2 - SB I-94  
 SB I-94 STA. 1358+00 TO 1388+00  
 SHEET 3 OF 3

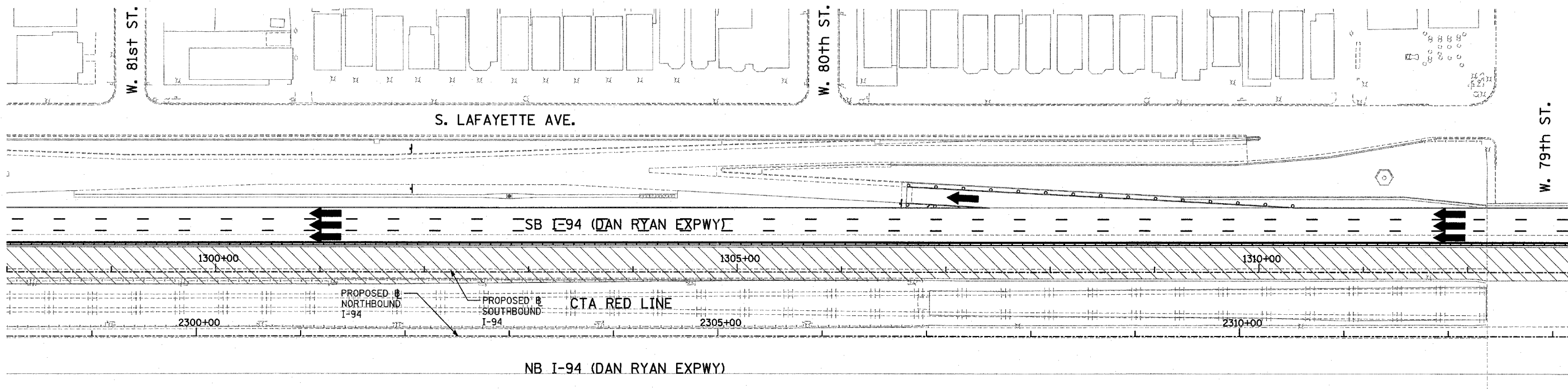
SCALE: 1"=50'-0"  
 DATE: MARCH 1, 2006

DRAWN BY: JDF  
 CHECKED BY: RTM



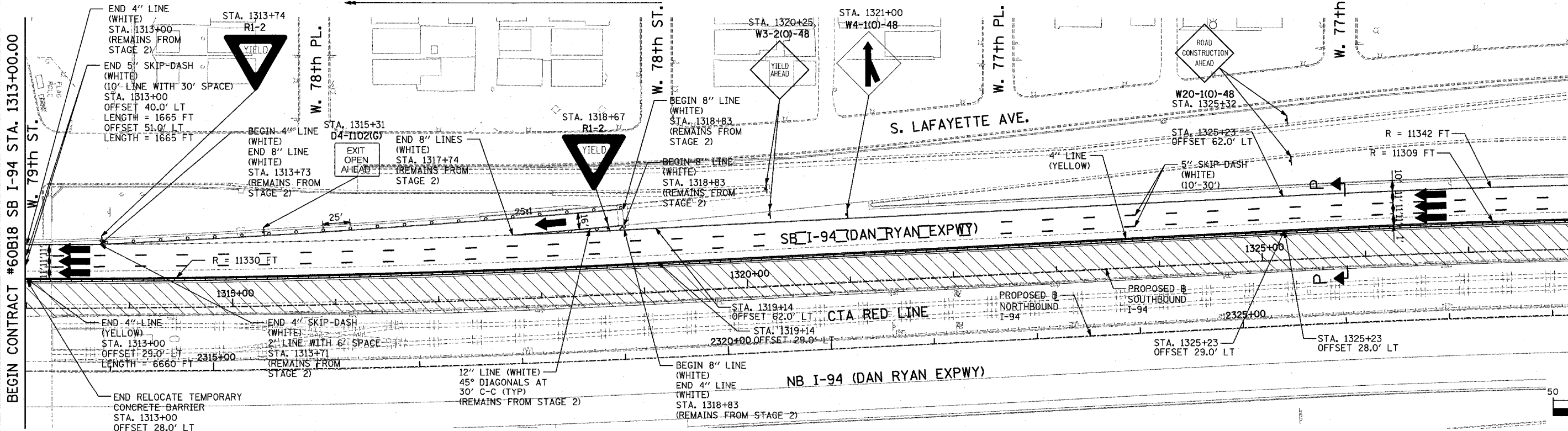
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	98
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

60B18



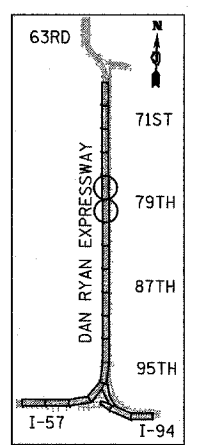
END CONTRACT #62593 SB I-94 STA. 1313+00.00

NOTE: NOT AT THE SOUTH END OF CONTRACT 60B18 MUST AT ALL TIMES MATCH CONTRACT 62593 TO THE SOUTH IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARDS AND DETAILS.

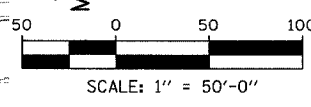


BEGIN CONTRACT #60B18 SB I-94 STA. 1313+00.00

MATCHLINE SB I-94 STA. 1328+00.00 (NEXT SHEET)



LOCATION MAP



**TYLIN** INTERNATIONAL

**NOTE:**  
ON ALL THE STAGE 3 PLAN SHEETS, CALLOUTS FOR TEMPORARY CONCRETE BARRIER ARE TO THE FRONT FACE OF THE WALL.

- LEGEND:**
- TEMPORARY CONCRETE BARRIER
  - TRAFFIC LANE
  - WORK ZONE
  - BARRICADE, TYPE III
  - DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
  - ARROW BOARD
  - TEMPORARY PAVEMENT

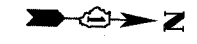
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**MAINTENANCE OF TRAFFIC PLAN**  
**STAGE 3 - SB I-94**  
**SB I-94 STA. 1298+00 TO 1328+00**  
**SHEET 1 OF 3**

SCALE: 1"=50'-0"  
 DATE: MARCH 1, 2006  
 DRAWN BY: JDF  
 CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	99
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

60B18

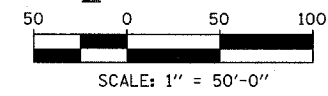
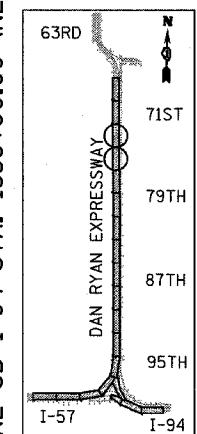
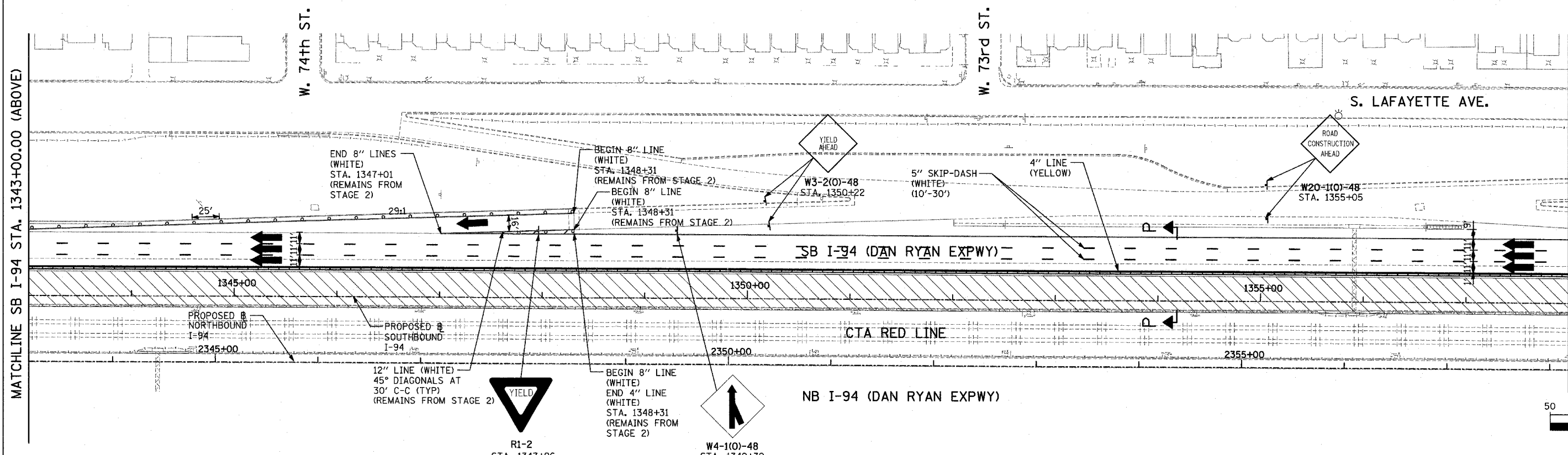
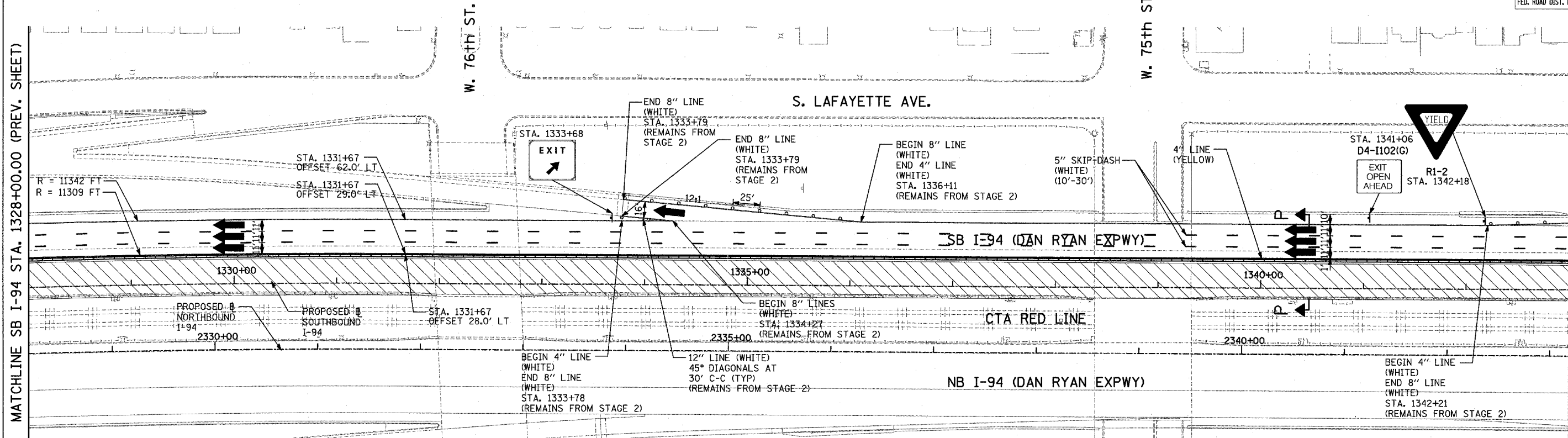


MATCHLINE SB I-94 STA. 1328+00.00 (PREV. SHEET)

MATCHLINE SB I-94 STA. 1343+00.00 (BELOW)

MATCHLINE SB I-94 STA. 1343+00.00 (ABOVE)

MATCHLINE SB I-94 STA. 1358+00.00 (NEXT SHEET)



**TYLIN INTERNATIONAL**

**LEGEND:**

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

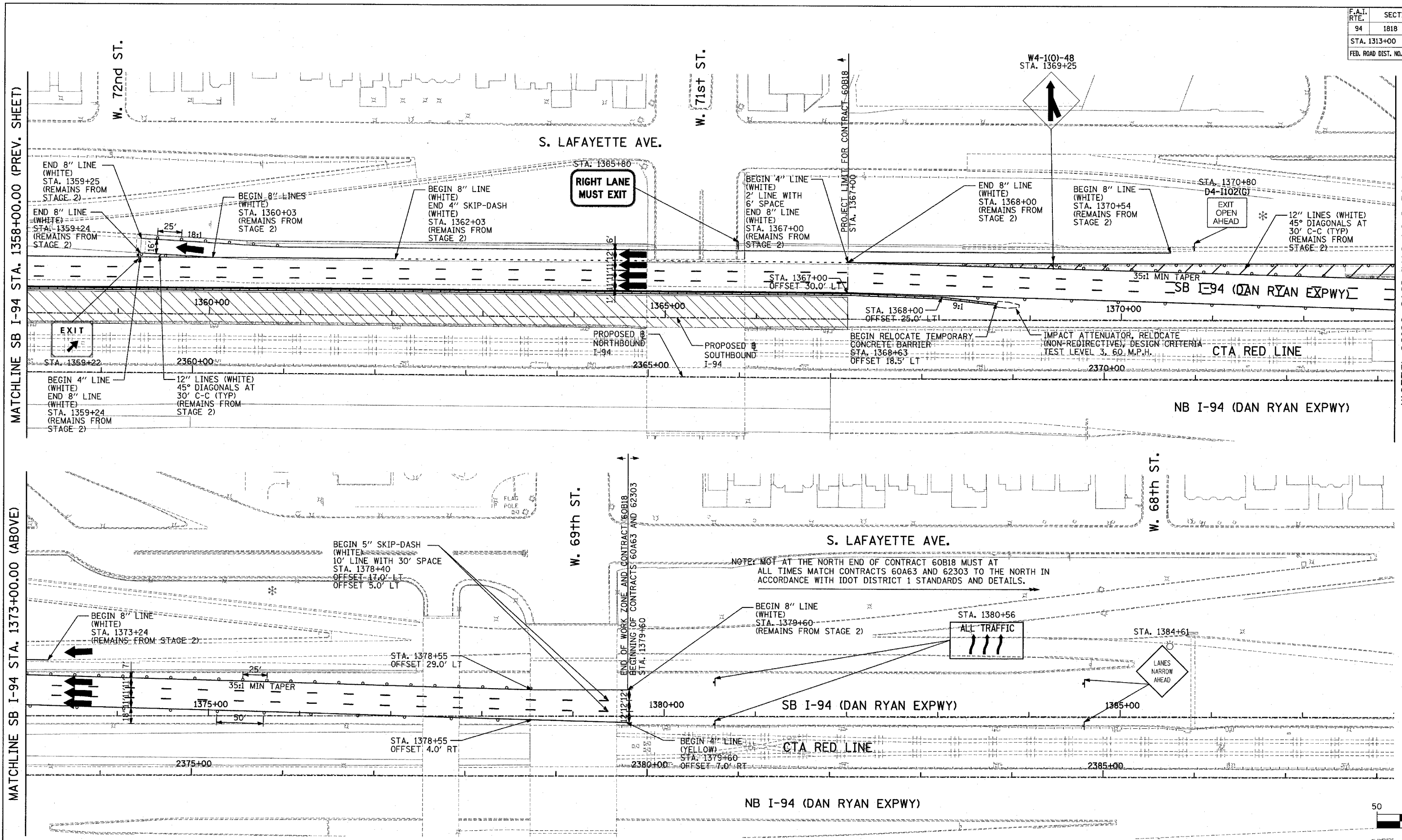
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
 MAINTENANCE OF TRAFFIC PLAN  
 STAGE 3 - SB I-94  
 SB I-94 STA. 1328+00 TO 1358+00  
 SHEET 2 OF 3

SCALE: 1"=50'-0"      DRAWN BY: JDF  
 DATE: MARCH 1, 2006      CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1818 R-6	COOK	290	100
STA. 1313+00		TO STA. 1367+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

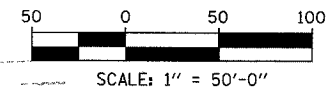
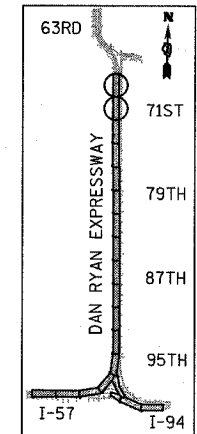
60B18



MATCHLINE SB I-94 STA. 1373+00.00 (BELOW)

MATCHLINE SB I-94 STA. 1358+00.00 (PREV. SHEET)

MATCHLINE SB I-94 STA. 1373+00.00 (ABOVE)



**TYLIN** INTERNATIONAL

**LEGEND:**

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**MAINTENANCE OF TRAFFIC PLAN**  
**STAGE 3 - SB I-94**  
**SB I-94 STA. 1358+00 TO 1388+00**  
**SHEET 3 OF 3**

SCALE: 1"=50'-0"  
 DATE: MARCH 1, 2006  
 DRAWN BY: JDF  
 CHECKED BY: RTM