

INDEX OF SHEETS

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PROJECT LOCATED IN THE CITY OF CHICAGO

DAN RYAN EXPRESSWAY:	ADT (2010)	DESIGN SPEED	POSTED SPEED
SB I-94 (DAN RYAN)	140,500	60 MPH	55 MPH
SB I-94 (BISHOP FORD)	87,600	60 MPH	55 MPH
SB I-57	65,800	60 MPH	55 MPH
SB 79TH STREET C-D ENTRANCE RAMP	15,300	45 MPH	40 MPH
SB 83RD STREET C-D EXIT RAMP	6,800	45 MPH	40 MPH
SB 87TH STREET EXIT RAMP	12,600	45 MPH	40 MPH
SB 87TH STREET ENTRANCE RAMP	13,400	45 MPH	40 MPH
SB 95TH STREET EXIT RAMP	11,900	45 MPH	40 MPH
SB WENTWORTH AVE. EXIT RAMP	4,800	45 MPH	40 MPH
SB HALSTED STREET EXIT RAMP	21,300	45 MPH	40 MPH
EB I-57 TO I-94 CROSS-CONNECTOR	22,800	50 MPH	50 MPH

PROPOSED HIGHWAY PLANS

F.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
SECTION (1516.1, 1717, & 1818) R-5
PROJECT NO.: *IM-094-3(401)061*
I-57 TO 79TH STREET

SB LANES 1 - 5, SHOULDERS & CTA BARRIER WALL
COOK COUNTY
C-91-095-03

VOL. 1 OF 2

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	1
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
D-91-421-01	(1516.1, 1717, & 1818) R-5		62593	



PROJECT DESCRIPTION

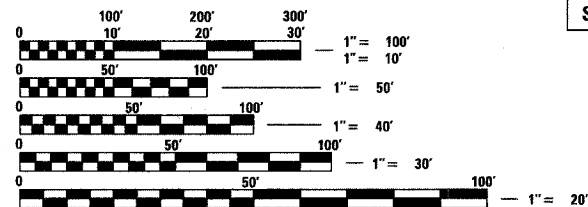
THE PROPOSED IMPROVEMENT CONSISTS OF PAVEMENT AND CTA BARRIER WALL RECONSTRUCTION, WIDENING, RAMP TERMINALS, SHOULDERS, AND DRAINAGE MODIFICATIONS ALONG SB I-94 (DAN RYAN & BISHOP FORD) AND SB I-57; AND PAVEMENT RESURFACING ALONG SB I-94 (BISHOP FORD) AND EB I-57 TO I-94 CONNECTOR.

INTERIOR WALL AND CEILING REPAIRS TO:

S.N. 016-0073: SOUTHBOUND BISHOP FORD TUNNEL AND ADJACENT CTA TUNNEL UNDER NORTHBOUND I-57.

SUBSTRUCTURE AND ABUTMENT REPAIRS TO:

S.N. 016-0143: BELT RAILWAY & NORFOLK SOUTHERN BRIDGE OVER SOUTHBOUND AND NORTHBOUND I-94 (DAN RYAN EXPRESSWAY).
S.N. 016-0144 & S.N. 016-0145: CHICAGO RAIL LINK BRIDGES OVER SOUTHBOUND AND NORTHBOUND I-94 (DAN RYAN EXPRESSWAY).



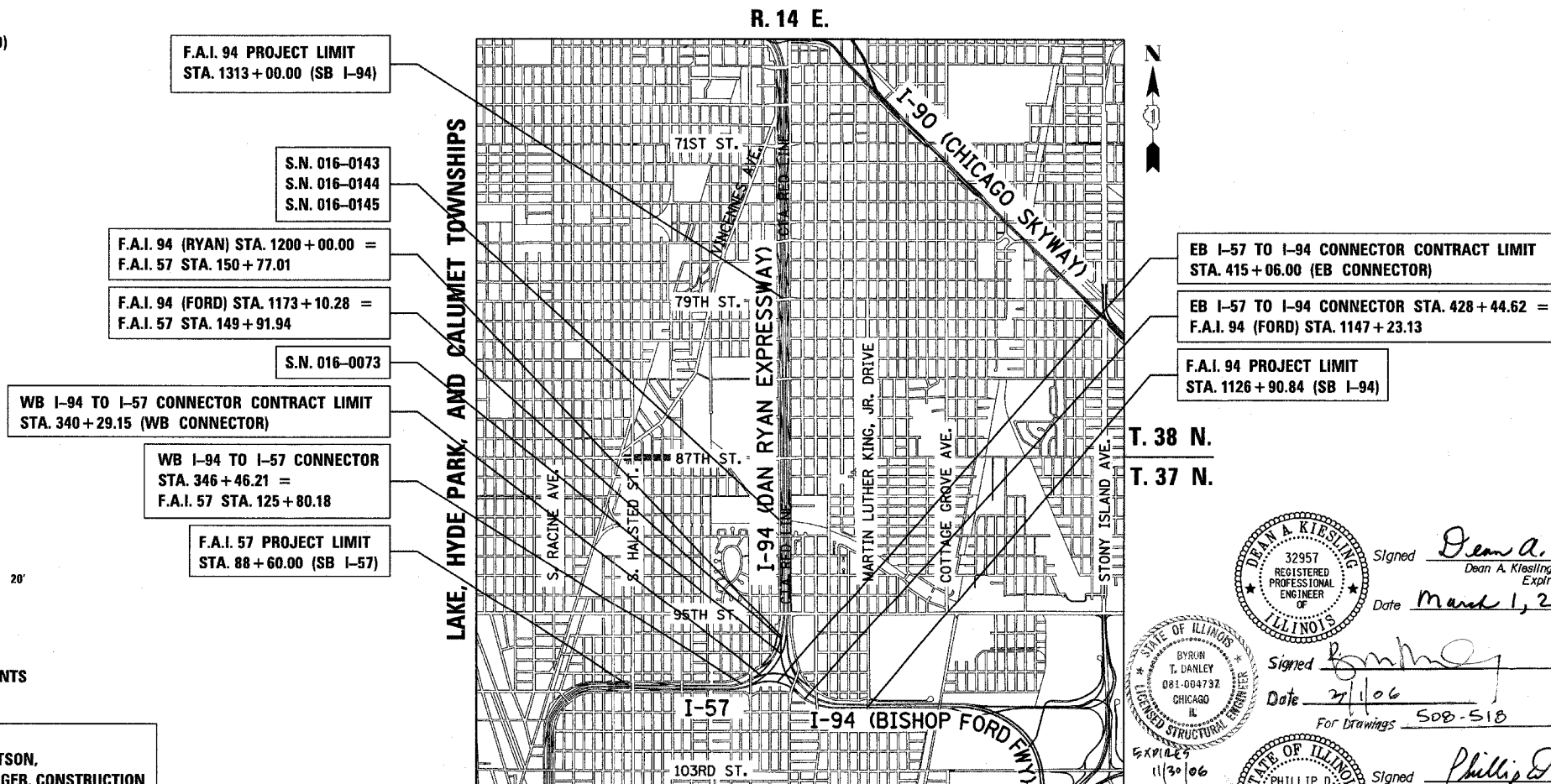
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES; REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED

DIGGER:
CHICAGO UTILITY ALERT NETWORK
(312) 744-7000

CTA CONTACT:
MARVIN A. WATSON,
GENERAL MANAGER, CONSTRUCTION
(312) 681-3860

CONTRACT NO. 62593

DISTRICT ONE DESIGN / PROJECT MANAGER:
BRIAN KUTTAB (847) 705 - 4431



F.A.I. 94 PROJECT LIMIT
STA. 1313 + 00.00 (SB I-94)

S.N. 016-0143
S.N. 016-0144
S.N. 016-0145

F.A.I. 94 (RYAN) STA. 1200 + 00.00 =
F.A.I. 57 STA. 150 + 77.01

F.A.I. 94 (FORD) STA. 1173 + 10.28 =
F.A.I. 57 STA. 149 + 91.94

S.N. 016-0073

WB I-94 TO I-57 CONNECTOR CONTRACT LIMIT
STA. 340 + 29.15 (WB CONNECTOR)

WB I-94 TO I-57 CONNECTOR
STA. 346 + 46.21 =
F.A.I. 57 STA. 125 + 80.18

F.A.I. 57 PROJECT LIMIT
STA. 88 + 60.00 (SB I-57)

EB I-57 TO I-94 CONNECTOR CONTRACT LIMIT
STA. 415 + 06.00 (EB CONNECTOR)

EB I-57 TO I-94 CONNECTOR STA. 428 + 44.62 =
F.A.I. 94 (FORD) STA. 1147 + 23.13

F.A.I. 94 PROJECT LIMIT
STA. 1126 + 90.84 (SB I-94)

LOCATION MAP

GROSS LENGTH OF PROJECT = 24088.3 FT. = 4.562 MI.
NET LENGTH OF PROJECT = 24088.3 FT. = 4.562 MI.
MAP SCALE: 1" = 1/2 MILE

CONTRACT 23

Signed *Dean A. Kiesel*
Dean A. Kiesel, P.E., Ill. Lic. No. 32957
Expires 11-30-2007.
Date *March 1, 2006*

Signed *Thomas Mikolajewski*
Thomas Mikolajewski, P.E., Ill. Lic. No. 062-056151
Expires 11-30-2007.
Date *March 1, 2006*
For Electrical Drawings 441-485

Signed *Phillip D. Frey*
Phillip D. Frey, S.E., Ill. Lic. No. 081-004826
Expires 11-30-2006.
Date *3/1/06*
For Structural Drawings 342-347 AND 519-534

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