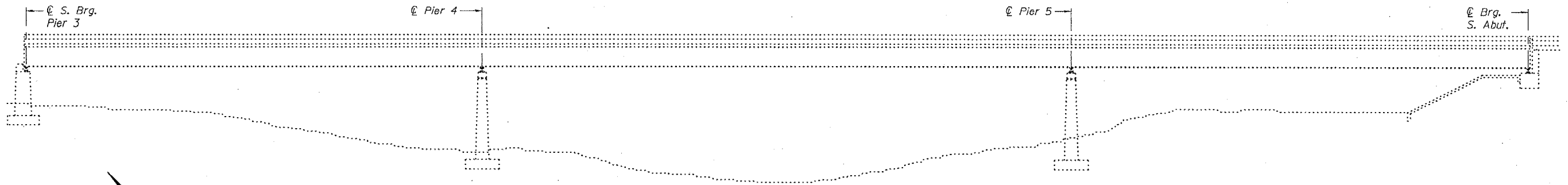


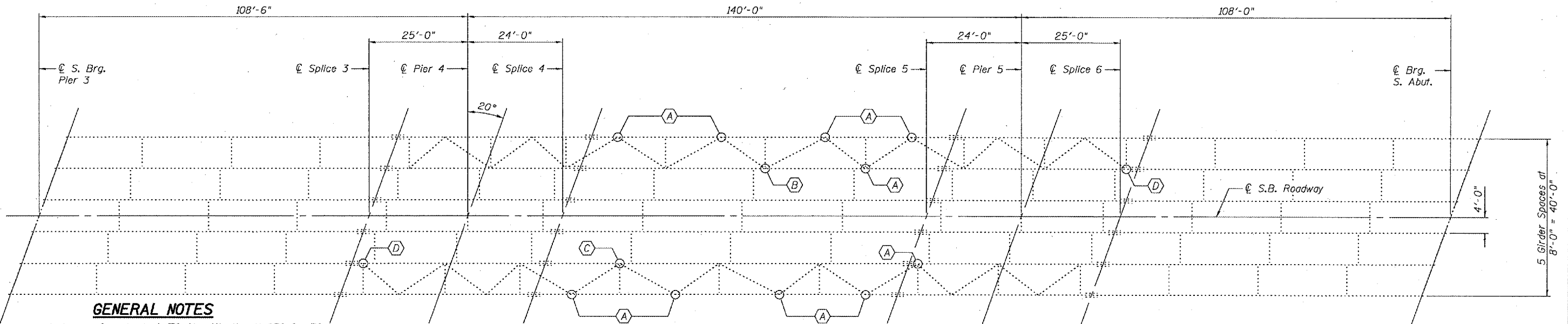
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 1
		Tazewell	9	6	2 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract Number: 68585



ELEVATION



GENERAL NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.
Fasteners shall be high strength bolts. Bolts $7/8"$ ϕ , open holes $15/16"$ ϕ , unless otherwise noted.
Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
Cost of removal and/or re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included in the cost of Structural Steel Repair. Cost of temporarily supported lateral bracing is included with Structural Steel Repair.
The existing structural steel coating contains lead. The Contractor should take appropriate precautions to deal with the presence of lead on this project.
Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".
All structural steel shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Structural Steel Repair.

PLAN

- (A) Repair Detail A See Sheet 2 of 2.
- (B) Repair Detail B See Sheet 2 of 2.
- (C) Repair Detail C See Sheet 2 of 2.
- (D) Repair Detail D See Sheet 2 of 2.

Procedure for Hoan Repair Detail
1. Remove existing $1/2"$ connecting ϕ . The minimum distance from cut to face of web shall be the larger of $1/4"$ or web to ϕ weld size, with removal of remaining material by grinding as described below. The cut shall be made parallel to the web without angling the cut towards the web. Equipment and method of cutting shall be approved by the Engineer. Any method of removal to be used shall ensure that no damage is done to the existing web, vertical stiffener or welds connecting these elements. Cutting shall be done in a manner such that the point on the opposite face of the web is not damaged. If damage occurs, the damaged area shall be repainted at the contractor's expense and procedures shall be modified to prevent damage at subsequent removal locations.

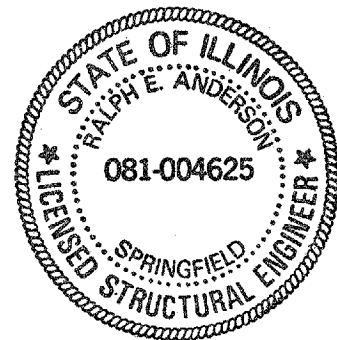
2. Remove material between cut and web by grinding and grind smooth at web surface. Web ϕ surfaces shall have a roughness average (Ra) of $250 \mu\text{in.}$ or less. Grinding equipment shall be approved by the Engineer. The grinding operation should not gouge the girder web ϕ .

3. The web surface at the modification shall be inspected using dye penetrant or magnetic particle methods. Any cracks found shall be identified and reported to the Bureau of Bridges and Structures for further disposition

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Steel Repair	Pound	1,890

PLAN AND ELEVATION
I-155 (S.B.) / MACKINAW RIVER
TAZEWELL COUNTY
SN 090-0092



Expires: November 30, 2006

DESIGNED: *Adam T. Holloway*
CHECKED: *Paul H. G...*
DRAWN: *[Signature]*
CHECKED: A.T.H. P.S.J.

March 1, 2006
EXAMINED: *John A. Morrison*
PASSED: *Ralph E. Anderson*
ENGINEER OF BRIDGES AND STRUCTURES