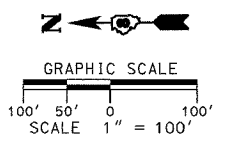


ACCESS ROAD LT. 2  
 PROP. CURVE P\_AR.L2-2-1  
 PI STA. = 85+67.73  
 $\Delta = 131^\circ 02' 13''$  (RT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 219.62'$   
 $L = 228.70'$   
 $E = 141.31'$   
 $\theta = 4%$   
 $T.R. = 28'$   
 $S.E. RUN = 72'$   
 $P.C. STA = 83+48.12$   
 $P.T. STA = 85+76.82$

ACCESS ROAD LT. 2  
 PROP. CURVE P\_AR.L2-2-2  
 PI STA. = 86+70.38  
 $\Delta = 41^\circ 02' 13''$  (LT)  
 $D = 22^\circ 55' 06''$   
 $R = 250.00'$   
 $T = 93.56'$   
 $L = 179.06'$   
 $E = 16.93'$   
 $\theta = 4%$   
 $T.R. = 28'$   
 $S.E. RUN = 72'$   
 $P.C. STA = 85+76.82$   
 $P.T. STA = 87+55.88$

F.A.P. 103  
 PROP. CURVE 2  
 PI STA. = 226+25.75  
 $\Delta = 25^\circ 02' 29''$  (RT)  
 $D = 3^\circ 57' 05''$   
 $R = 1,450.00'$   
 $T = 322.01'$   
 $L = 633.73'$   
 $E = 35.32'$   
 $\theta = 6.00%$   
 $T.R. = 36'$   
 $S.E. RUN = 240'$   
 $P.C. STA = 223+03.74$   
 $P.T. STA = 229+37.47$

PROP. CURVE P\_PE.R2-1  
 PI STA. = 11+22.50  
 $\Delta = 90^\circ 00' 00''$  (RT)  
 $D = 143^\circ 14' 22''$   
 $R = 40.00'$   
 $T = 40.00'$   
 $L = 62.83'$   
 $E = 16.57'$   
 $\theta = \text{SEE INTERSECTION DETAIL}$   
 $P.C. STA = 10+82.50$   
 $P.T. STA = 11+45.33$



PROP. CURVE P\_AR.R2B-1  
 PI STA. = 8+82.10  
 $\Delta = 48^\circ 20' 07''$  (RT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 44.87'$   
 $L = 84.36'$   
 $E = 9.61'$   
 $\theta = 4.00%$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 $P.C. STA = 8+37.23$   
 $P.T. STA = 9+21.59$

PROP. CURVE P\_AR.L2-1  
 PI STA. = 93+43.55  
 $\Delta = 22^\circ 39' 38''$  (RT)  
 $D = 3^\circ 40' 31''$   
 $R = 1,559.00'$   
 $T = 312.38'$   
 $L = 616.59'$   
 $E = 30.99'$   
 $\theta = 2.24%$   
 $T.R. = 28'$   
 $S.E. RUN = 41'$   
 $P.C. STA = 90+31.17$   
 $P.T. STA = 96+47.76$

PROP. CURVE P\_AR.L2-2  
 PI STA. = 96+98.19  
 $\Delta = 16^\circ 52' 31''$  (LT)  
 $D = 16^\circ 51' 08''$   
 $R = 339.99'$   
 $T = 50.43'$   
 $L = 100.14'$   
 $E = 3.72'$   
 $\theta = 3.92%$   
 $T.R. = 28'$   
 $S.E. RUN = 72'$   
 $P.C. STA = 96+47.76$   
 $P.T. STA = 97+47.90$

PROP. CURVE P\_AR.L2-3  
 PI STA. = 99+37.77  
 $\Delta = 19^\circ 15' 21''$  (RT)  
 $D = 17^\circ 21' 44''$   
 $R = 330.00'$   
 $T = 55.98'$   
 $L = 110.91'$   
 $E = 4.71'$   
 $\theta = 3.94%$   
 $T.R. = 28'$   
 $S.E. RUN = 72'$   
 $P.C. STA = 98+81.79$   
 $P.T. STA = 99+92.70$

PROP. CURVE P\_AR.L2-4  
 PI STA. = 100+78.48  
 $\Delta = 22^\circ 13' 49''$  (RT)  
 $D = 17^\circ 21' 44''$   
 $R = 330.00'$   
 $T = 64.83'$   
 $L = 128.04'$   
 $E = 6.31'$   
 $\theta = 3.94%$   
 $T.R. = 28'$   
 $S.E. RUN = 72'$   
 $P.C. STA = 100+13.65$   
 $P.T. STA = 101+41.69$

PROP. CURVE P\_AR.L2-5  
 PI STA. = 102+72.45  
 $\Delta = 22^\circ 13' 48''$  (LT)  
 $D = 17^\circ 21' 44''$   
 $R = 330.00'$   
 $T = 64.83'$   
 $L = 128.03'$   
 $E = 6.31'$   
 $\theta = 3.94%$   
 $T.R. = 28'$   
 $S.E. RUN = 72'$   
 $P.C. STA = 102+07.62$   
 $P.T. STA = 103+35.65$

DATE \_\_\_\_\_ BY \_\_\_\_\_  
 PLAN SURVEYED \_\_\_\_\_  
 NOTE BOOK \_\_\_\_\_  
 RT. OF WAY CHECKED \_\_\_\_\_  
 NO. \_\_\_\_\_

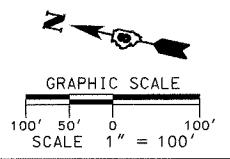
DATE \_\_\_\_\_ BY \_\_\_\_\_  
 PLAN SURVEYED \_\_\_\_\_  
 NOTE BOOK \_\_\_\_\_  
 RT. OF WAY CHECKED \_\_\_\_\_  
 NO. \_\_\_\_\_

F.A.P. 103  
 PROP. CURVE 2A  
 PI STA. = 250+98.51  
 $\Delta = 0^\circ 16' 54''$  (RT)  
 $D = 0^\circ 16' 54''$   
 $R = 20,343.43'$   
 $T = 50.00'$   
 $L = 100.00'$   
 $E = 0.06'$   
 $\theta = \text{NC}$   
 $P.C. STA = 250+48.51$   
 $P.T. STA = 251+48.51$

ACCESS ROAD RT. 3  
 PROP. CURVE P\_AR.R3-1  
 PI STA. = 102+08.36  
 $\Delta = 115^\circ 19' 24''$  (LT)  
 $D = 88^\circ 08' 50''$   
 $R = 65.00'$   
 $T = 102.67'$   
 $L = 130.83'$   
 $E = 56.51'$   
 $\theta = 4.00%$   
 $T.R. = 28'$   
 $S.E. RUN = 73'$   
 $P.C. STA = 101+05.69$   
 $P.T. STA = 102+36.52$

ACCESS ROAD RT. 3  
 PROP. CURVE P\_AR.R3-2  
 PI STA. = 103+77.85  
 $\Delta = 25^\circ 19' 23''$  (RT)  
 $D = 17^\circ 21' 44''$   
 $R = 330.00'$   
 $T = 74.14'$   
 $L = 145.85'$   
 $E = 8.22'$   
 $\theta = 3.94%$   
 $T.R. = 28'$   
 $S.E. RUN = 72'$   
 $P.C. STA = 103+03.72$   
 $P.T. STA = 104+49.57$

ACCESS ROAD RT. 3  
 PROP. CURVE P\_AR.R3-3  
 PI STA. = 110+44.37  
 $\Delta = 65^\circ 29' 19''$  (RT)  
 $D = 301^\circ 33' 24''$   
 $R = 19.00'$   
 $T = 12.22'$   
 $L = 21.72'$   
 $E = 3.59'$   
 $\theta = \text{NONE}$   
 $T.R. = \text{---}$   
 $S.E. RUN = \text{---}$   
 $P.C. STA = 110+32.15$   
 $P.T. STA = 110+53.87$



ACCESS ROAD LT. 3  
 PROP. CURVE P\_AR.L3-1  
 PI STA. = 93+02.79  
 $\Delta = 7^\circ 29' 47''$  (LT)  
 $D = 8^\circ 43' 15''$   
 $R = 657.00'$   
 $T = 43.04'$   
 $L = 85.96'$   
 $E = 1.41'$   
 $\theta = 3.29%$   
 $T.R. = 28'$   
 $S.E. RUN = 60'$   
 $P.C. STA = 92+59.75$   
 $P.T. STA = 93+45.71$

ACCESS ROAD LT. 3  
 PROP. CURVE P\_AR.L3-2  
 PI STA. = 96+00.57  
 $\Delta = 14^\circ 53' 04''$  (LT)  
 $D = 17^\circ 21' 44''$   
 $R = 330.00'$   
 $T = 43.11'$   
 $L = 85.73'$   
 $E = 2.80'$   
 $\theta = 3.94%$   
 $T.R. = 28'$   
 $S.E. RUN = 72'$   
 $P.C. STA = 95+57.46$   
 $P.T. STA = 96+43.19$

F.A.P. 103  
 PROP. CURVE 3  
 PI STA. = 262+94.84  
 $\Delta = 5^\circ 00' 00''$  (RT)  
 $D = 1^\circ 37' 12''$   
 $R = 3,537.00'$   
 $T = 154.43'$   
 $L = 308.66'$   
 $E = 3.37'$   
 $\theta = 3.90%$   
 $T.R. = 39'$   
 $S.E. RUN = 156'$   
 $P.C. STA = 261+40.41$   
 $P.T. STA = 264+49.07$

F.A.P. 103  
 CURVE 4  
 PI STA. = 271+70.13  
 $\Delta = 4^\circ 59' 34''$  (LT)  
 $D = 1^\circ 37' 12''$   
 $R = 3,537.00'$   
 $T = 154.20'$   
 $L = 308.21'$   
 $E = 3.36'$   
 $\theta = 3.90%$   
 $T.R. = 39'$   
 $S.E. RUN = 156'$   
 $P.C. STA = 270+15.93$   
 $P.T. STA = 273+24.14$

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**HORIZONTAL ALIGNMENT**  
**SHEET 2 OF 3 SHEETS**  
 F.A.P. 103 (IL. RTE. 13/15)  
 SECTION 27-1R - ST. CLAIR CO.  
 SCALE: 1" = 100'  
 DATE February 11, 2005  
 DRAWN BY KEB  
 CHECKED BY RCY

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