

GENERAL NOTES

F.A.U. RTE. 2605 1233	SECTION 02-00170-09-CH	COUNTY LAKE	TOTAL SHEET 66	SHEET NO. 2
GENERAL NOTES AND LEGEND				
FEB. ROAD DIST. NO. 5 ILLINOIS PROJECT CMM-8003(207)				
CONTRACT NO. 83637				

GENERAL

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED JANUARY 1, 2002 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS," ADOPTED MARCH 1, 2005; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS," THE "STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS," FIFTH EDITION; THE DETAILS IN THE PLANS; AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS AND SPECIFICATIONS SHALL BE INTERPRETED TO MEAN THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS REQUIRED BY THE APPLICATION OF TRAFFIC CONTROL DEVICES, THE STANDARD SPECIFICATIONS AND THE PLANS.

ALL WORK INVOLVING EXISTING SIGNS SHALL BE GOVERNED BY THE FOLLOWING:

- A) SIGNS SHALL NOT BE REMOVED UNTIL THE PROGRESS OF WORK NECESSITATES IT.
- B) EACH SIGN TO BE REMOVED MUST BE RE-ERECTED AT A TEMPORARY LOCATION APPROVED BY THE ENGINEER IN A WORKMANLIKE MANNER AND SHALL BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. ALL SUCH SIGNS SHALL BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING.
- C) ALL SIGNS SHALL BE RE-ERECTED AT PERMANENT LOCATIONS AS THE COMPLETION OF THE ROADWAY IMPROVEMENTS PERMIT. LOCATIONS SHALL BE APPROVED BY THE ENGINEER.
- D) ALL UNUSED SIGNS SHALL BE STORED ON THE JOBSITE FOR PICKUP BY THE COUNTY.
- E) LONGER POSTS MAY BE REQUIRED AT SOME TEMPORARY LOCATIONS TO MAINTAIN PROPER SIGN HEIGHT. IN SUCH CASES, POSTS SHALL BE FURNISHED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL MAILBOXES WHICH INTERFERE WITH OR BECOME INACCESSIBLE DUE TO CONSTRUCTION OPERATIONS, AS MANY TIMES AS NECESSARY, AND RESET THEM AT THEIR PERMANENT LOCATIONS AS SHOWN IN THE DETAIL IN THE PLANS OR AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH ARTICLE 107.20 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL COOPERATE WITH THE DIVISION OF TRANSPORTATION IN ANY CONSTRUCTION THE DIVISION MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, FIELD TILES AND ANY OTHER PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THEIR EXACT LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR WITH NO ADDITIONAL COMPENSATION TO THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS REQUIRED BY THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED TO THE SATISFACTION OF THE OWNER OR REPLACED. THIS WORK WILL BE AT THE CONTRACTOR'S EXPENSE.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION MEETING.

ALL ELEVATIONS SHOWN ON THE PLANS ARE U.S.G.S. DATUM.

WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE THE MONUMENTS ARE DISTURBED. THE CONTRACTOR SHALL CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE BACK OF CURB, UNLESS OTHERWISE NOTED. ELEVATIONS SHOWN AT POINTS OF CURVE, ETC. ARE TO THE EDGE OF PAVEMENT, UNLESS OTHERWISE NOTED.

STRUCTURE LOCATIONS GIVEN ON THE PLANS ARE AS FOLLOWS:

- A) FOR STRUCTURES FALLING IN THE CURB & GUTTER - TO THE EDGE OF PAVEMENT.
- B) FOR OTHER LOCATIONS - TO THE CENTER OF THE STRUCTURE.
- C) FOR FLARED END SECTIONS - TO THE END OF THE END SECTION.

FLAT TOPS AND CONES ARE TO BE TURNED SO THAT THE FRAME IS CLOSEST TO THE CENTER LINE OF THE ROAD, UNLESS OTHERWISE NOTED ON THE STRUCTURE IN THE PLANS. ALL FLAT TOPS AND CONES ARE ASSUMED TO BE ECCENTRIC.

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS FOR STRUCTURES, EDGES OF PAVEMENT, ETC. ARE FROM THE CENTERLINE.

FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATION AND CROSS-SLOPE OF THE AREA IN WHICH THEY ARE LOCATED. THIS ADJUSTMENT SHALL BE INCLUDED IN THE COST OF THE STRUCTURE.

CONSTRUCTION

ALL SUITABLE EXCESS MATERIAL FROM SEWER TRENCHES, SIDE ROADS, ENTRANCES AND OTHER EXCAVATIONS SHALL BE USED IN THE CONSTRUCTION OF THE ROADWAY. PLACEMENT AND COMPACTION OF THIS MATERIAL SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR DRAINAGE STRUCTURES WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN IN AN OPERATING CONDITION TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES WHICH HAVE THE CAPACITY TO RECEIVE AND DISCHARGE THE STORM FLOWS NORMALLY ACCEPTED AND RELEASED BY THE EXISTING FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.

THE COST OF INTERCONNECTIONS BETWEEN THE PROPOSED AND EXISTING SEWER SYSTEMS AND PROPOSED AND EXISTING WATER MAIN SHALL BE INCLUDED IN THE VARIOUS UNIT PRICES OF THE PROPOSED UNITS OF WORK.

ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT SHALL HAVE CAST INTO THE LID THE WORD "STORM," "SANITARY" OR "WATER," AS APPROPRIATE TO THE TYPE OF STRUCTURE INVOLVED.

TRENCH BACKFILL QUANTITIES HAVE BEEN COMPUTED IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF HIGHWAYS, BUREAU OF CONSTRUCTION TRENCH BACKFILL TABLE, BASED ON PLAN INVERT DEPTH FROM SUBGRADE. ANY TRENCH BACKFILL REQUIRED IN EXCESS OF THE QUANTITY ESTABLISHED ABOVE, INCLUDING BEDDING MATERIAL, SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

ALL EXISTING DRAINAGE FACILITIES, HEADWALLS AND FENCES NO LONGER REQUIRED, IN THE OPINION OF THE ENGINEER, SHALL BE REMOVED. THE COST OF REMOVAL OF EXISTING PIPE CULVERTS, STORM SEWERS, DRAINAGE STRUCTURES, CONCRETE HEADWALLS, FENCING OR OTHER OBSTRUCTIONS WHICH INTERFERE WITH THE PROPOSED IMPROVEMENTS AND WHICH ARE NOT SHOWN TO BE REMOVED AS A SEPARATE PAY ITEM SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

WHEN SLOPES ARE TO BE WIDENED, STEPS SHALL BE CUT INTO THE EXISTING SLOPES IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS AT NO ADDITIONAL COST TO THE CONTRACT.

ANY OF THESE MATERIALS CONSIDERED SUITABLE FOR SALVAGE BY THE ENGINEER SHALL BE STORED WITHIN THE RIGHT-OF-WAY FOR LATER REMOVAL BY THE LAKE COUNTY DIVISION OF TRANSPORTATION. UNUSABLE MATERIALS SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. TRENCH BACKFILL AND/OR PAVEMENT REPLACEMENT AND/OR AGGREGATE BASE COURSE TYPE A WILL BE PAID FOR WHEN THE WORK LIES UNDER EXISTING PAVEMENT AREAS.

AT LOCATIONS WHERE THE PROPOSED STORM SEWER CROSSES OVER A UTILITY, A FOUR-INCH STYROFOAM CUSHION SHALL BE PLACED BENEATH THE STORM SEWER WHEN DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF THE STORM SEWER BEING PLACED.

TRENCHES ACROSS PAVED SURFACES SHALL BE PATCHED WITH EITHER PERMANENT OR TEMPORARY PAVEMENT AT THE END OF EACH WORK DAY. TEMPORARY PATCHING OF TRENCHES WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE ITEM PLACED IN THE TRENCH.

MISCELLANEOUS

THE CONTRACTOR SHALL PROVIDE ACCESS TO ADJUTING PROPERTIES AT ALL TIMES DURING CONSTRUCTION OF THIS PROJECT. ANY COST INCURRED BY THE CONTRACTOR TO MEET THIS REQUIREMENT THAT IS NOT COVERED BY A SPECIFIC PAY ITEM WILL BE INCLUDED IN THE COST OF THE CONTRACT.

SAWING OF REMOVAL ITEMS AS NOTED ON THE PLANS, SPECIFIED IN THE STANDARD SPECIFICATIONS OR AS REQUIRED BY THE ENGINEER SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED, UNLESS NOTED OTHERWISE. LONGITUDINAL FULL-DEPTH SAW CUTTING OF EXISTING PAVEMENT WILL BE PAID FOR SEPARATELY.

THE EXISTING BITUMINOUS SURFACE SHALL BE SAW CUT TO A DEPTH OF TWO INCHES AT ALL BUTT JOINTS.

WHERE NEW WORK IS PROPOSED TO MEET EXISTING FEATURES, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD CHECK ALL DIMENSIONS AND ELEVATIONS AND NOTIFY THE ENGINEER OF DISCREPANCIES BEFORE PROCEEDING WITH CONSTRUCTION.

WHERE PROPOSED CURB AND/OR CURB AND GUTTER MEETS EXISTING, THE PROPOSED SHALL TRANSITION TO THE EXISTING IN A DISTANCE OF TEN FEET OR AS DIRECTED BY THE ENGINEER. THE TRANSITION LENGTH WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM.

ADA COMPLAINT SIDEWALK RAMP FOR THE HANDICAPPED SHALL BE INSTALLED AT ALL INTERSECTIONS AND DRIVEWAYS IN ACCORDANCE WITH CURRENT LCDOT STANDARDS.

ANY SHEETING AND/OR SHORING USED ON THIS IMPROVEMENT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

PROTECTIVE COAT SHALL BE APPLIED TO ALL CURBS, GUTTERS AND P.C.C. PAVEMENTS, SIDEWALKS AND DRIVEWAYS.

ALL TYPE II BARRICADES SHALL BE WEIGHTED DOWN WITH TWO SANDBAGS EACH.

SEDIMENTATION AND EROSION CONTROL

SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL BE UTILIZED IN CONSIDERATION OF TIME OF YEAR, SITE CONDITIONS AND THE SUITABILITY OF TEMPORARY VERSUS PERMANENT MEASURES.

SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.

DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN FOURTEEN CALENDAR DAYS OF THE END OF THE ACTIVE HYDROLOGIC DISTURBANCE.

AREAS OR EMBANKMENTS HAVING SLOPES OF 3H:1V OR STEEPER SHALL BE STABILIZED WITH SOD, MAT OR BLANKET IN COMBINATION WITH SEEDING.

ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY APPROPRIATE EROSION CONTROL MEASURES.

ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY DAYS AFTER THE FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.

ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED BY THE CONTRACTOR AS REQUIRED BY THE SPECIAL PROVISIONS.

A STABILIZED MAT OF AGGREGATE UNDERLAIN WITH FILTER CLOTH (OR OTHER APPROPRIATE MEASURE) SHALL BE LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA. ANY SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.

SOIL STOCKPILES SHALL NOT BE LOCATED IN FLOOD-PRONE AREAS OR DESIGNATED BUFFERS PROTECTING WATERS OF THE UNITED STATES.

IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH EFFECTIVE SEDIMENT CONTROL MEASURES (e.g., SEDIMENT TRAPS, SEDIMENT BASINS OR OTHER APPROPRIATE MEASURES).

THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.

DRIVEWAYS OR ENTRANCES

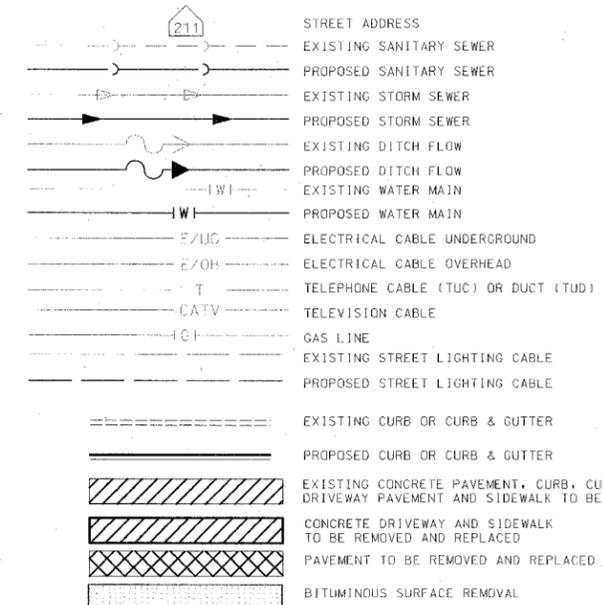
EXISTING BITUMINOUS, CONCRETE, AND GRAVEL DRIVEWAYS AND ENTRANCES SHALL BE SURFACED TO ONE FOOT INSIDE THE RIGHT-OF-WAY WITH BITUMINOUS CONCRETE SURFACE COURSE AS SCHEDULED IN THE PLANS.

EXISTING FIELD ENTRANCES SHALL BE BUILT UP IN PLACE TO THE RIGHT-OF-WAY WITH AGGREGATE BASE COURSE

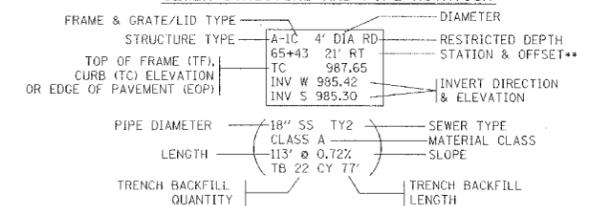
THE CONTRACTOR SHALL CONSTRUCT ALL COMMERCIAL AND PRIVATE DRIVEWAYS IN ACCORDANCE WITH THE DETAILS IN THE PLANS.

SUPPLEMENTAL LEGEND

SEE STANDARDS FOR ADDITIONAL INFORMATION



SEWER STRUCTURE AND PIPE NOTATION



- ** NOTE:
- OFFSET FOR STRUCTURES FALLING IN THE CURB LINE IS TO EDGE OF PAVEMENT
- OFFSET FOR ALL OTHER STRUCTURES IS TO CENTER OF STRUCTURE.

STRUCTURE ADJUSTMENT/RECONSTRUCTION/REMOVAL NOTATION

