

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	05-00078-00-RS	COOK	17	1

FOR INDEX OF SHEETS, SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED HIGHWAY RESURFACING PLANS

FAU ROUTE 2766 (HARLEM AVENUE)

**FROM FAU ROUTE 1324 (DEMPSTER STREET)
TO FAP ROUTE 339 (GOLF ROAD)**

SECTION: 05-00078-00-RS

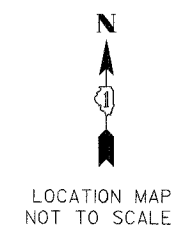
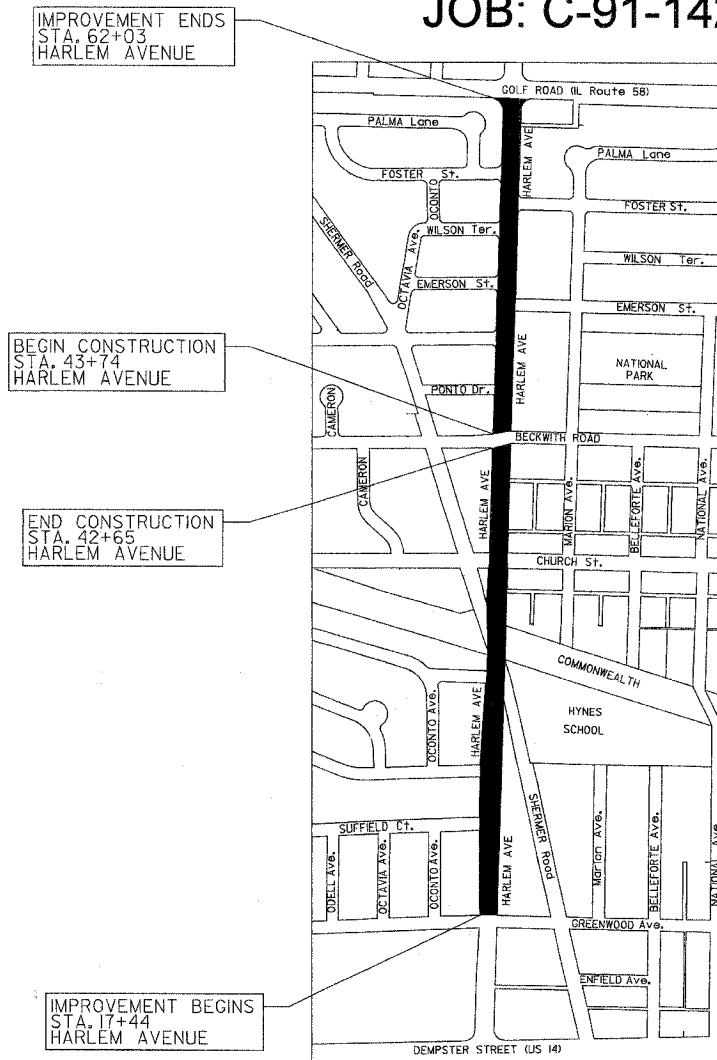
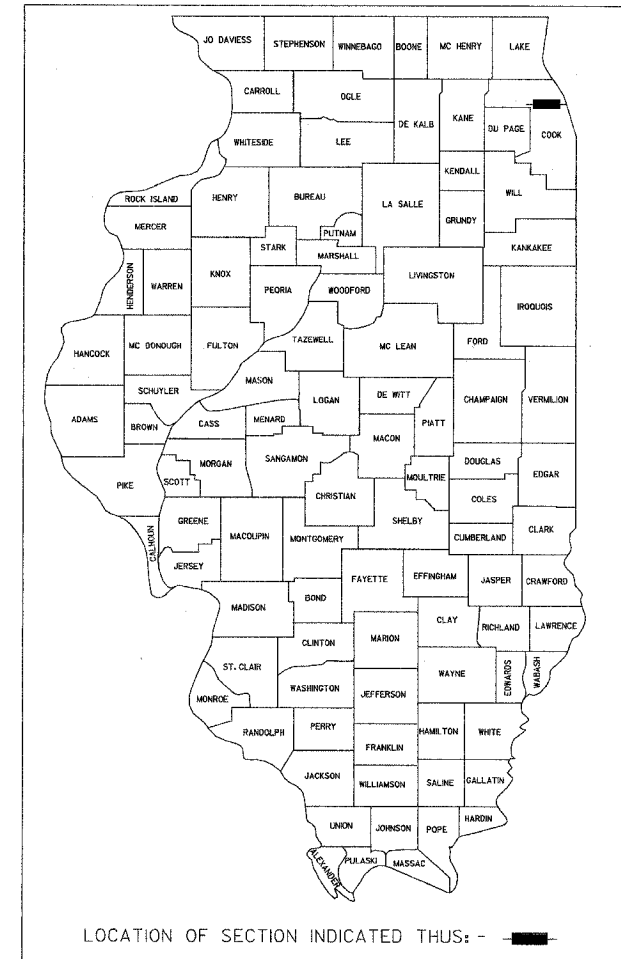
PROJECT: M-8003(565)

VILLAGE OF MORTON GROVE

COOK COUNTY

JOB: C-91-142-06

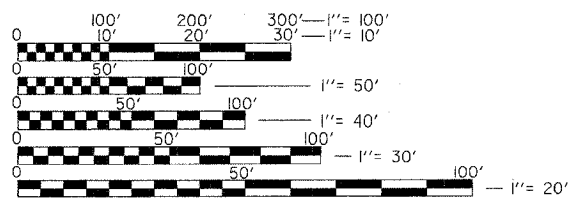
CONTRACT NO.: 83839



TRAFFIC DATA

ADT (2005) = 14,300
POSTED SPEED 30 MPH
DESIGN SPEED 30 MPH

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123



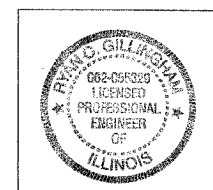
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

FEDERAL AID DESIGN ENGINEER: ABIGAIL BRINKS 847-705-4233

VILLAGE OF MORTON GROVE
SUBMITTED: JANUARY 13, 2006
Ryan Spillingham, P.E.
VILLAGE ENGINEER

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
FEBRUARY 27 2006
[Signature]
LOCAL ROADS ENGINEER
February 27 20 06
Diane M. O'Keefe/Cut
DEPUTY DIRECTOR OF HWYS., REGION ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS



Ryan Spillingham, P.E.
REGISTERED P.E., STATE OF ILLINOIS
11/30/2007
EXPIRATION DATE

LOCATION MAP
TOWNSHIP 41 NORTH, RANGE 9 EAST, MAINE TOWNSHIP AND
TOWNSHIP 41 NORTH, RANGE 13 EAST, NILES TOWNSHIP
GROSS PROJECT LENGTH = 4459 FT OR 0.84 MILES AND
NET PROJECT LENGTH = 4350 FT OR 0.82 MILES

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2786	05-00078-00-RS	COOK	17	2
STA. 17+44		TO STA. 62+03		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NUMBER: 83839				

GENERAL NOTES

A. GENERAL

- EARTHWORK AND PAVING SPECIFICATIONS**
THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", LATEST EDITION, AND ALL ADDENDA THERETO, SHALL GOVERN THE PAVING WORK UNDER THIS CONTRACT, EXCEPT AS MODIFIED BY THESE SPECIFICATIONS.
 - UNDERGROUND SPECIFICATIONS**
THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", LATEST EDITION, SHALL GOVERN THE UNDERGROUND WORK UNDER THIS CONTRACT, EXCEPT AS MODIFIED BY THESE SPECIFICATIONS.
 - WORK**
WORK SHALL MEAN THE FURNISHING OF ALL LABOR, MATERIAL, EQUIPMENT AND OTHER INCIDENTALS NECESSARY OR CONVENIENT TO THE SUCCESSFUL COMPLETION OF THE IMPROVEMENTS DESIGNED BY THE ENGINEER AND SHOWN ON THIS SET OF PLANS. WORK SHALL ALSO INCLUDE CARRYING OUT OF ALL THE DUTIES AND OBLIGATIONS IMPOSED BY THE AGREEMENT BETWEEN THE OWNER AND THE CONTRACTOR FOR THE PROPER INSTALLATION AND ACCEPTANCE OF THE IMPROVEMENTS DESIGNED BY THE ENGINEER AND SHOWN ON THIS SET OF PLANS.
 - COMPLIANCE WITH LOCAL ORDINANCES**
ALL WORK PERFORMED UNDER THIS CONTRACT SHALL BE IN ACCORDANCE WITH THE VILLAGE OF MORTON GROVE'S ORDINANCES AND STANDARDS.
 - EXISTING UTILITIES**
WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION AND ELEVATIONS OF UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO LOCATION AND ELEVATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE BIDDERS. THE ENGINEER AND THE OWNER ASSUME NO RESPONSIBILITY WHATEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION AND ELEVATION OF UTILITY FACILITIES, NOR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT IS EACH CONTRACTOR'S AND SUBCONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND DETERMINE THE ACTUAL LOCATION AND ELEVATION OF ALL UTILITIES. EACH CONTRACTOR AND SUBCONTRACTOR SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION RELATIVE TO THE LOCATION AND ELEVATION OF THEIR FACILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.
- EACH CONTRACTOR AND SUBCONTRACTOR PERFORMING UNDERGROUND WORK SHALL CONTACT J.U.L.I.E. AT 1-800-892-0123 AT LEAST 48 HOURS PRIOR TO START OF WORK.
- SAFETY OF WORK**
EACH CONTRACTOR AND SUBCONTRACTOR SHALL BE RESPONSIBLE FOR INITIATING, MAINTAINING AND SUPERVISING ALL SAFETY PRECAUTIONS AND PROGRAMS IN CONNECTION WITH THE WORK. EACH CONTRACTOR AND SUBCONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE SAFETY OF, AND SHALL PROVIDE THE NECESSARY PROTECTION TO PREVENT DAMAGE, INJURY OR LOSS TO:
 - ALL EMPLOYEES ON THE WORK AND OTHER PERSONS AND ORGANIZATIONS WHO MAY BE AFFECTED THEREBY;
 - ALL THE WORK AND MATERIALS AND EQUIPMENT TO BE INCORPORATED THEREIN, WHETHER IN STORAGE ON OR OFF THE SITE; AND
 - OTHER PROPERTY AT THE SITE OR ADJACENT THERETO, INCLUDING TREES, SHRUBS, LAWNS, WALKS, PAVEMENTS, ROADWAYS, STRUCTURES, UTILITIES AND UNDERGROUND FACILITIES NOT DESIGNATED FOR REMOVAL, RELOCATIONS OR REPLACEMENT IN THE COURSE OF CONSTRUCTION.
 - EACH CONTRACTOR AND SUBCONTRACTOR SHALL DESIGNATE A RESPONSIBLE REPRESENTATIVE AT THE SITE WHOSE DUTY SHALL BE THE PREVENTION OF ACCIDENTS. THIS PERSON SHALL BE THE CONTRACTOR'S SUPERINTENDENT UNLESS OTHERWISE DESIGNATED IN WRITING BY THE CONTRACTOR TO OWNER.

- EXISTING ELEVATIONS AND LOCATIONS**
EVERY CONTRACTOR AND SUBCONTRACTOR SHALL VERIFY THE ELEVATIONS AND LOCATIONS OF ALL EXISTING INFORMATION AS SHOWN ON THE PLANS AND NOTIFY THE ENGINEER OF ALL DISCREPANCIES PRIOR TO THE COMMENCEMENT OF THE WORK. EXISTING UNDERGROUND UTILITIES SHALL BE EXPOSED BY THE CONTRACTORS AND SUB-CONTRACTORS PRIOR TO THE START OF CONSTRUCTION TO DETERMINE IF A PROBLEM OR CONFLICT EXISTS WITH THE PROPOSED IMPROVEMENTS AND TO AVOID DELAYS IN THE PROGRESS OF THE WORK ONCE THE WORK COMMENCES.
- NOTIFICATION**
THE VILLAGE OF MORTON GROVE'S ENGINEER SHALL BE NOTIFIED BY EVERY CONTRACTOR AND SUBCONTRACTOR AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- COMPLIANCE WITH LAWS AND REGULATIONS**
EVERY CONTRACTOR SHALL COMPLY WITH ALL LOCAL AND STATE SAFETY LAWS, REGULATIONS AND ORDINANCES; AND FEDERAL SAFETY REGULATIONS AS OUTLINED IN THE LATEST REVISIONS OF THE FEDERAL CONSTRUCTION SAFETY STANDARDS AND WITH ALL PROVISIONS AND REGULATIONS OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) STANDARDS. EACH CONTRACTOR AND SUBCONTRACTOR IS RESPONSIBLE FOR PROVIDING A SAFE WORKING PLACE FOR HIS EMPLOYEES. EACH CONTRACTOR AND SUBCONTRACTOR IS RESPONSIBLE FOR THE SUPERVISION, DIRECTION AND CONDUCT OF THEIR EMPLOYEES, AGENTS, MATERIAL SUPPLIERS AND VENDORS.
- GRASS AREAS WITHIN RIGHT-OF-WAY**
ALL GRASS AREAS WITHIN THE RIGHT-OF-WAY DISTURBED DURING CONSTRUCTION SHALL BE RESTORED BY PLACING A MINIMUM 4" TOPSOIL AND SALT TOLERANT SOD. 4" TOPSOIL AND SALT TOLERANT SOD ARE INCIDENTAL TO THE CONTRACT.
- PAVEMENT DISTURBED DURING CONSTRUCTION**
ANY EXISTING ROADWAY PAVEMENT OR SHOULDER DISTURBED DURING CONSTRUCTION SHALL BE RESTORED IN ACCORDANCE WITH I.D.O.T. AND VILLAGE OF MORTON GROVE STANDARDS.
- ITEMS NOT SPECIFICALLY SHOWN**
ALL ITEMS THAT ARE NOT SPECIFICALLY SHOWN ON THE PLANS OR IN THE SUMMARY OF QUANTITIES BUT CAN REASONABLY BE INTERPRETED TO BE INCLUDED IN THE WORK DESCRIBED SHALL BE INCIDENTAL TO THE COST OF THE CONTRACT.
- FINAL FRAME ADJUSTMENTS**
THE PAVING CONTRACTOR SHALL MAKE ALL NECESSARY FINAL ADJUSTMENTS TO EXISTING AND PROPOSED FRAMES, GRATES, LIDS AND STRUCTURES TO MEET FINAL FINISHED GRADES.
- RECORD DRAWINGS**
EACH CONTRACTOR AND SUBCONTRACTOR SHALL MAINTAIN AND KEEP UP TO DATE A SET OF "RECORD DRAWINGS" SHOWING ALL CHANGES FROM THE ORIGINAL PLANS. ALL CONTRACTORS AND SUBCONTRACTORS SHALL DELIVER THE "RECORD DRAWINGS" TO THE ENGINEER AT THE CONCLUSION OF THE PROJECT. THE ENGINEER WILL TRANSFER THE INFORMATION TO THE ORIGINAL PLANS.
- CONTRACTOR(S)**
FOR THE PURPOSES OF THESE SPECIFICATIONS, THE TERMS CONTRACTOR AND SUBCONTRACTOR SHALL MEAN ANY PERSON OR ENTITY THAT PROVIDES WORK FOR THE IMPROVEMENTS AS SHOWN ON THESE PLANS. THE ENGINEER, OWNER AND THE ARCHITECT ARE NOT CONSIDERED CONTRACTORS OR SUBCONTRACTORS.
- ANY SIGN LOCATED IN THE PUBLIC RIGHT-OF-WAY WHICH INTERFERES WITH CONSTRUCTION OF THE PROPOSED ROADWAY WORK OR LIGHTING SYSTEM THAT IS INTENDED TO BE MAINTAINED SHALL BE RELOCATED. THIS WORK IS INCIDENTAL TO THIS PROJECT. ALL WORK INVOLVING SIGN REMOVAL SHALL BE GOVERNED BY THE FOLLOWING REQUIREMENTS:**

EVERY SIGN RELOCATED MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO TRAFFIC FOR WHICH IT WAS INTENDED. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF TEMPORARY SETTING. THIS WORK INCLUDES PROVIDING A SIGN POST FOR THE TEMPORARY LOCATION, SHOULD SUCH A POST BE NECESSARY.

ALL SIGNS SHALL BE RE-ERECTED IN THE ORIGINAL LOCATION AS THE IMPROVEMENTS ARE COMPLETED.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY SIGNS DAMAGED BY HIS/HER CONSTRUCTION ACTIVITIES AND WILL REPLACE THEM AT NO COST TO THE CITY. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.

B. PAVING

- THE PAVING CONTRACTOR IS RESPONSIBLE FOR MAINTAINING PROPER DRAINAGE ON THE PROPOSED PAVEMENT. WHERE THE CONTRACTOR IS IN DOUBT ABOUT THE PROPER DIRECTION OF DRAINAGE, HE SHALL REQUEST ASSISTANCE FROM THE ENGINEER.
- FULL DEPTH SAW CUTS SHALL BE USED TO REMOVE EXISTING PAVEMENT, CURB AND GUTTER, SIDEWALK, DRIVEWAYS, BUTT JOINTS AND APPURTENANCES FROM MATERIAL TO REMAIN, IN ACCORDANCE WITH SECTION 440 OF THE "STANDARD SPECIFICATIONS". THE COST OF THE SAWING SHALL BE INCLUDED IN THE COST OF THE ITEM REMOVED.
- BASE COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
- PRIOR TO PLACING BITUMINOUS CONCRETE MIX ADJACENT TO EXISTING PAVEMENT TO REMAIN, THE EXPOSED EDGE SHALL BE CLEANED OF LOOSE MATERIAL TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE BITUMINOUS CONCRETE BEING PLACED.
- THE CONTRACTOR SHALL NOT CROSS COMPLETED BASE COURSE, OR EXISTING PAVEMENT NOT SCHEDULED TO BE REMOVED WITH TRACK EQUIPMENT.
- PRIME COAT - PRIME COAT MUST BE INSTALLED NO EARLIER THAN TWENTY-FOUR (24) HOURS PRIOR TO PLACEMENT OF BITUMINOUS CONCRETE.
- BUTT JOINTS - BUTT JOINT WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE LOCATION OF PAVEMENT PATCHING AND COMBINATION CONCRETE CURB AND GUTTER ARE AS SHOWN ON THE PLANS OR AS DETERMINED BY THE ENGINEER. THESE SHALL NOT EXCEED THE QUANTITIES LISTED IN THE SUMMARY OF QUANTITIES.

C. RESTORATION AND TREE PRESERVATION

- ALL DISTURBED LAWN AREAS WITHIN THE PUBLIC RIGHT-OF-WAY/EASEMENTS WILL BE RESTORED WITH 4" OF TOPSOIL AND SALT TOLERANT SOD. THE 4" TOPSOIL AND SALT TOLERANT SOD ARE INCIDENTAL TO THE CONTRACT.
- SODDING SHALL INCLUDE A MINIMUM OF 4 INCHES OF TOPSOIL AND SHALL BE DONE IN CONFORMANCE WITH SECTION 252 OF I.D.O.T. STANDARDS.
- ALL PARKWAY TREES SHALL BE PRESERVED. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY DAMAGE TO SUCH TREES AND SHALL PROVIDE TEMPORARY FENCES AND OTHER MEASURES, AS NECESSARY, WITH ALL SUCH MEASURES CONSIDERED AS INCIDENTAL TO THE CONTRACT.

NOTE: BOXED NOTES ARE INCIDENTAL ITEMS.





LIST OF IDOT STANDARD DRAWINGS

000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-01	CLASS C AND D PATCHES
606001-02	CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER
701301-02	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-03	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701606-04	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-04	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-03	LANE CLOSURE, MULTILANE IW OR 2W CROSSWALK OR SIDEWALK CLOSURE
702001-06	TRAFFIC CONTROL DEVICES
886001	DETECTOR LOOP INSTALLATIONS
886006	TYPICAL LAYOUT FOR DETECTION LOOPS

INDEX OF SHEETS

- COVER SHEET
- INDEX OF SHEETS, LEGEND
- GENERAL NOTES, I.D.O.T. STANDARD DRAWINGS
- SUMMARY OF QUANTITIES
- EXISTING AND PROPOSED TYPICAL CROSS SECTIONS
- EXISTING ROADWAY & PROPOSED IMPROVEMENT PLAN
- DRIVEWAY DETAILS
- DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
- PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT DETAILS
- CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT DETAILS
- BUTT JOINT AND BITUMINOUS TAPER DETAILS
- DISTRICT ONE TYPICAL PAVEMENT MARKINGS DETAILS
- DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING
- DETECTOR LOOP REPLACEMENT - HARLEM AV. @ IL RTE 58 (GOLF RD)
- TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS

LEGEND OF SYMBOLS

-  COMB. CONC. CURB & GUTTER REMOVAL & REPLACEMENT
-  BITUMINOUS SURFACE REMOVAL - BUTT JOINT
-  BITUMINOUS PATCH, TYPE D, CLASS IV
-  BITUMINOUS SURFACE REMOVAL - 2". BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70. POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

VILLAGE OF MORTON GROVE

HARLEM AVENUE

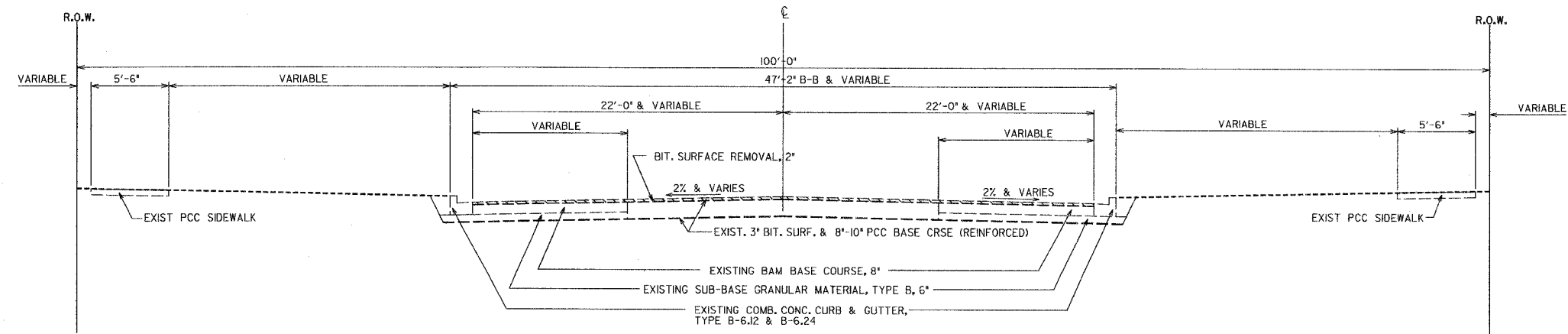
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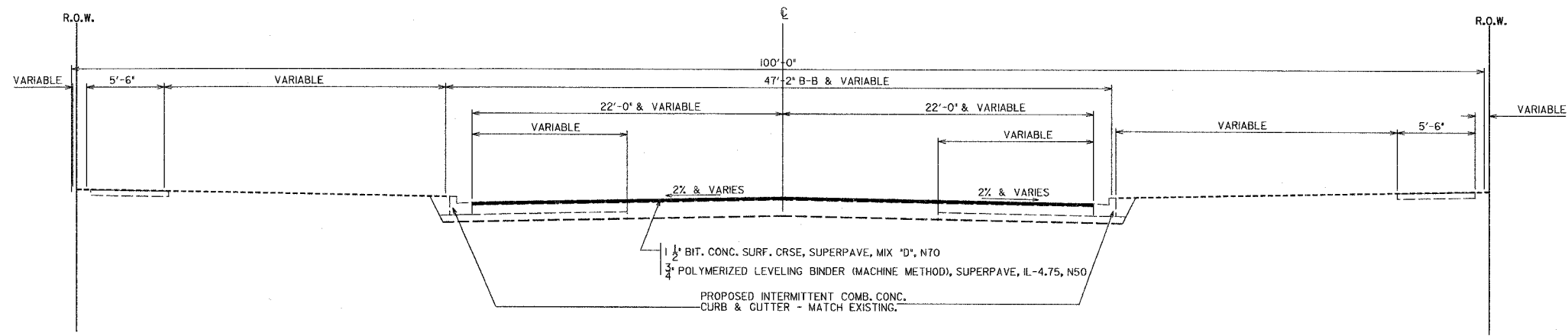
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DRAWN BY: JFG
CHECKED BY: RCG

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2786	05-00078-00-RS	COOK	17	4
STA. 17+44		TO STA. 62+03		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NUMBER: 83839				



EXISTING TYPICAL SECTION
HARLEM AVENUE - STA. 17+44 TO STA. 62+03



PROPOSED TYPICAL SECTION
HARLEM AVENUE - STA. 17+44 TO STA. 62+03

BITUMINOUS MIXTURE REQUIREMENTS			
PAY ITEM	ACTYPE	VOIDS	MAX RAP %
BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX D, N70	PG 64-22	4% @ 70 GYR	10
POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50	SBS/SBR PG 76-28	2.5% @ 50 GYR	0
CLASS D PATCH, SPECIAL, 13 INCH	PG 64-22	4% @ 70 GYR	15

**THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS SURFACE MIXTURE IS 112 LBS/SQ YD/IN

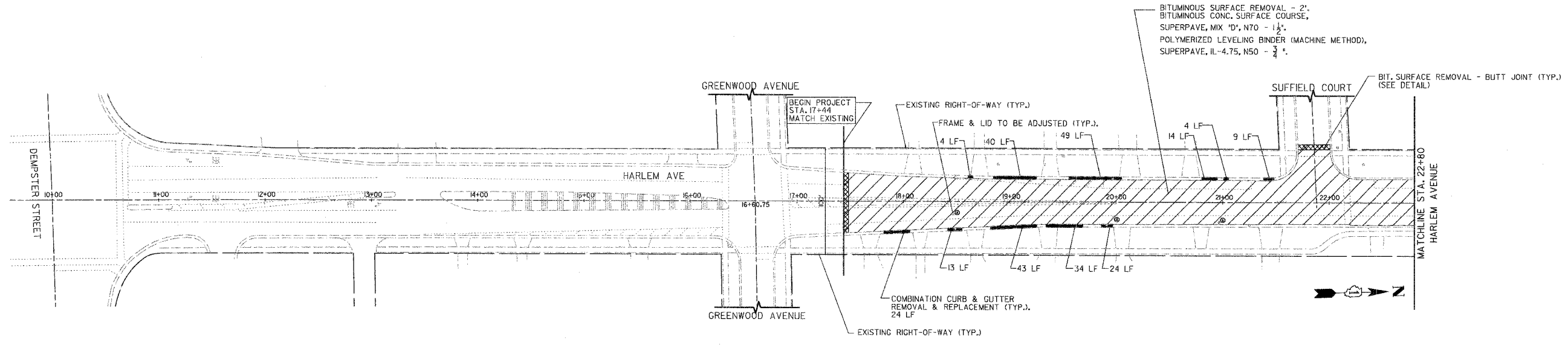
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
VILLAGE OF MORTON GROVE
HARLEM AVENUE
TYPICAL CROSS SECTIONS

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HORIZ. NTS
DATE: 1/13/06

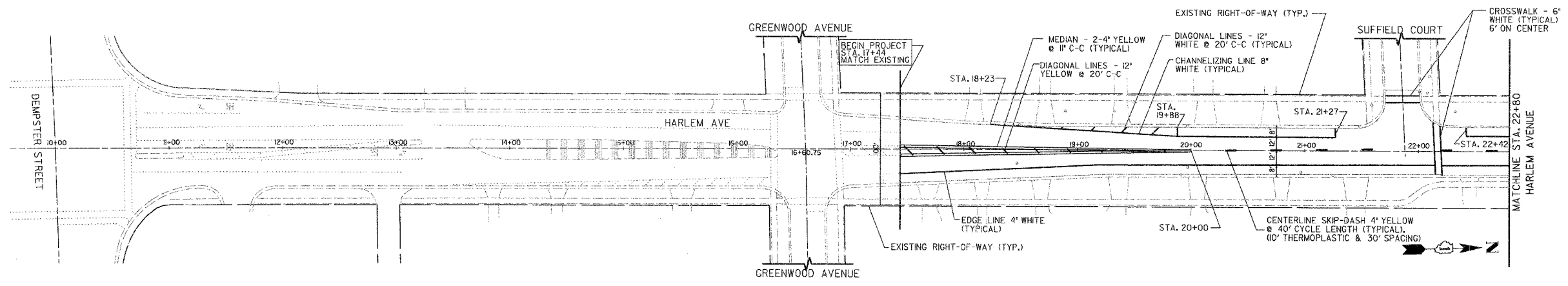
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	05-00078-00-RS	COOK	17	5
STA. 17+44		TO STA. 22+80		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NUMBER: 83839				



NOTE:
 1. FOR ALL SIDE STREETS, THE RESURFACING LIMITS WILL BE MARKED BY THE ENGINEER IN THE FIELD.
 2. CURB REMOVAL WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED THE QUANTITY STATED IN THE SUMMARY OF QUANTITIES.

EXISTING CONDITIONS AND
 PROPOSED REMOVAL & REPLACEMENT

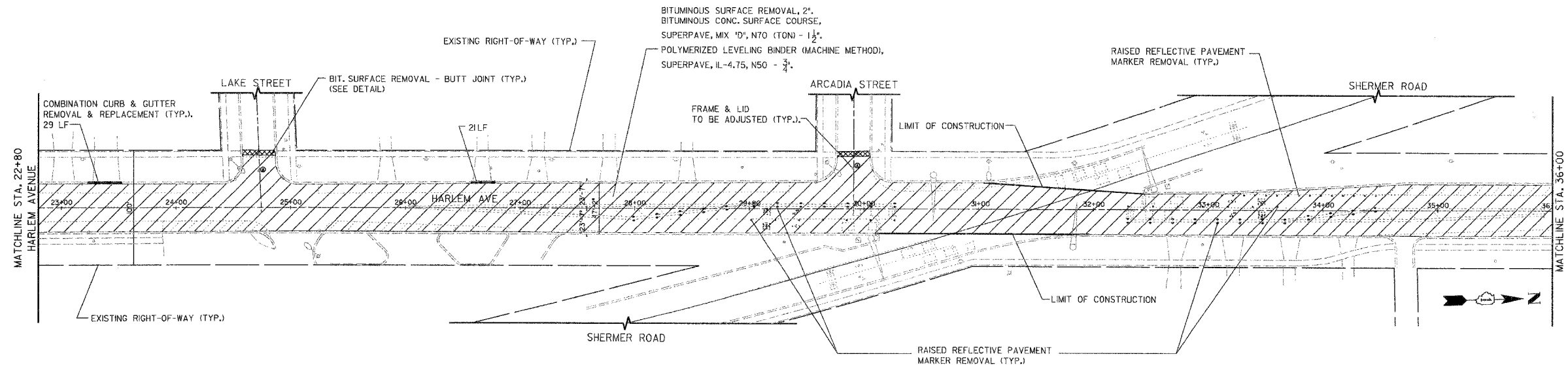


PROPOSED PAVEMENT MARKINGS

REVISIONS	
NAME	DATE

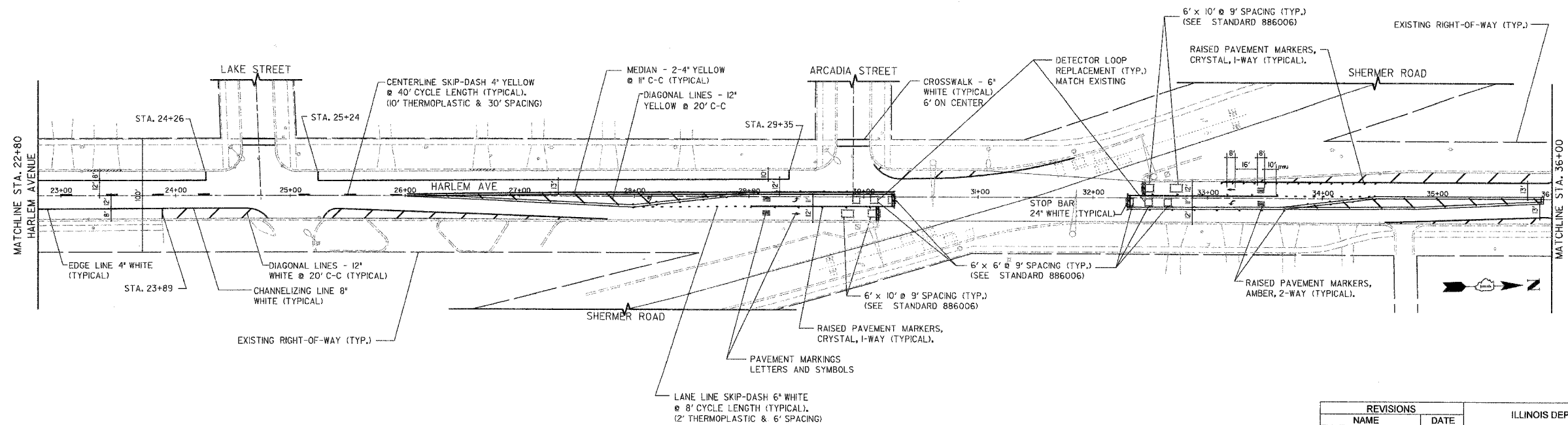
ILLINOIS DEPARTMENT OF TRANSPORTATION
 VILLAGE OF MORTON GROVE
 HARLEM AVENUE
 ROADWAY PLANS
 STA. 17+44 TO STA. 22+80
 SCALE: VERT. 1"=50'
 HORIZ. 1"=60'
 DATE: 1/13/06
 DRAWN BY: JFG
 CHECKED BY: RCG

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2786	05-00078-00-RS	COOK	17	6
STA. 22+80		TO STA. 36+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NUMBER: 83839				



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 3. STREET DIMENSIONS ARE TO EDGE-OF-PAVEMENT UNLESS OTHERWISE NOTED.

EXISTING CONDITIONS AND
PROPOSED REMOVAL & REPLACEMENT

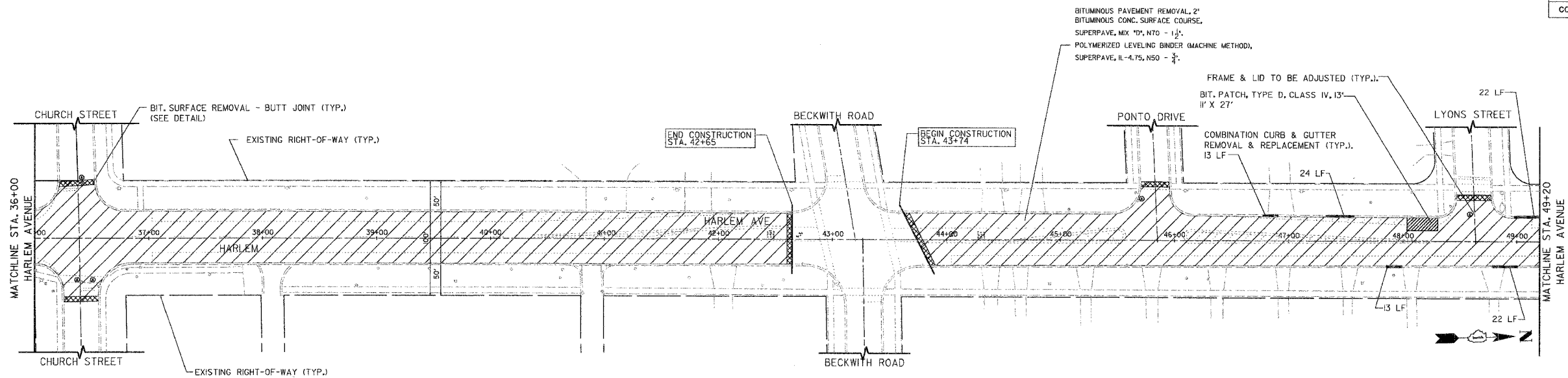


PROPOSED PAVEMENT MARKINGS

REVISIONS	
NAME	DATE

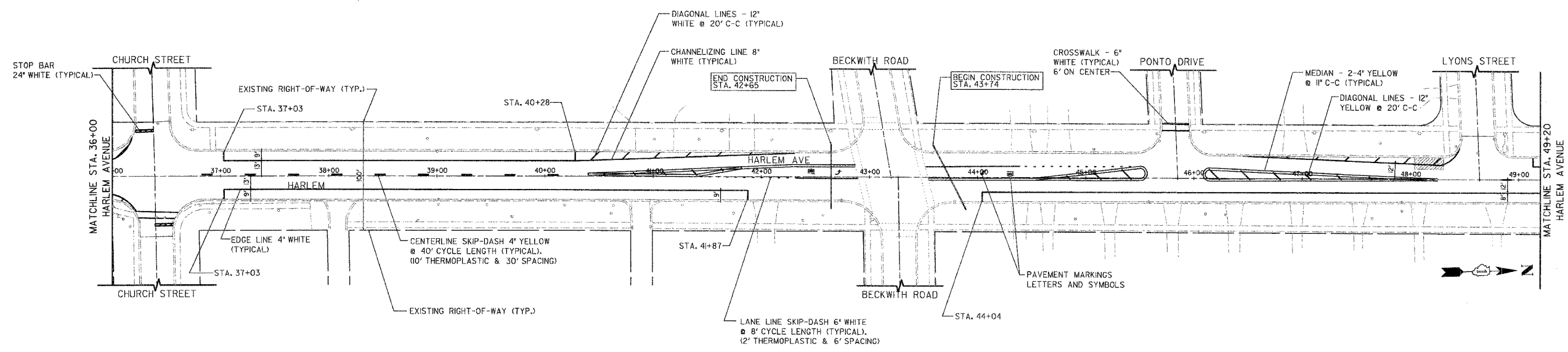
ILLINOIS DEPARTMENT OF TRANSPORTATION
 VILLAGE OF MORTON GROVE
 HARLEM AVENUE
 ROADWAY PLANS
 STA. 22+80 TO STA. 36+00
 SCALE: VERT. 1"=8'
 HORIZ. 1"=50'
 DATE: 1/13/08
 DRAWN BY: JFG
 CHECKED BY: RCG

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2786	05-00078-00-RS	COOK	17	7
STA. 36+00		TO STA. 49+20		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NUMBER: 83839				



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EXISTING CONDITIONS AND
 PROPOSED REMOVAL & REPLACEMENT

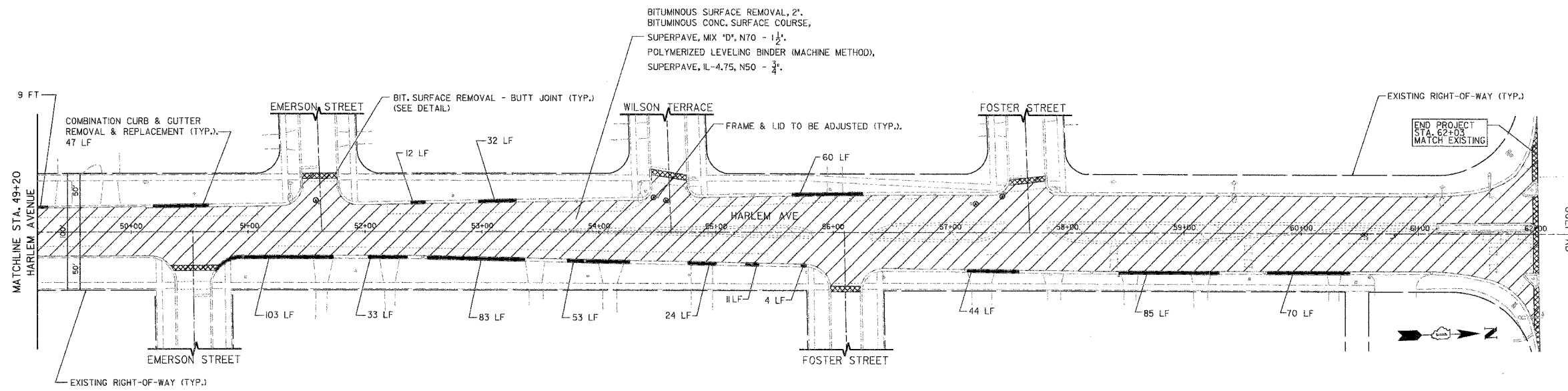


PROPOSED PAVEMENT MARKINGS

REVISIONS	
NAME	DATE

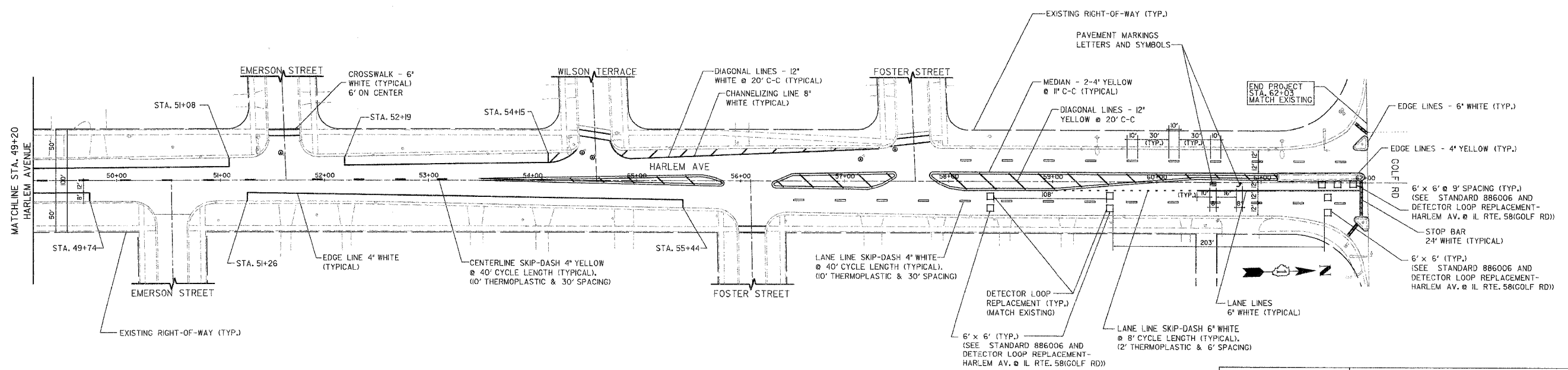
ILLINOIS DEPARTMENT OF TRANSPORTATION
 VILLAGE OF MORTON GROVE
 HARLEM AVENUE
 ROADWAY PLANS
 STA. 36+00 TO STA. 49+20
 SCALE: VERT. 1"=8'
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 DATE: 1/13/06
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	05-0078-00-RS	COOK	17	8
STA. 49+20		TO STA. 62+03		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NUMBER: 83839				



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EXISTING CONDITIONS AND
 PROPOSED REMOVAL & REPLACEMENT



PROPOSED PAVEMENT MARKINGS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

VILLAGE OF MORTON GROVE

HARLEM AVENUE

ROADWAY PLANS

STA. 49+20 TO STA. 62+03

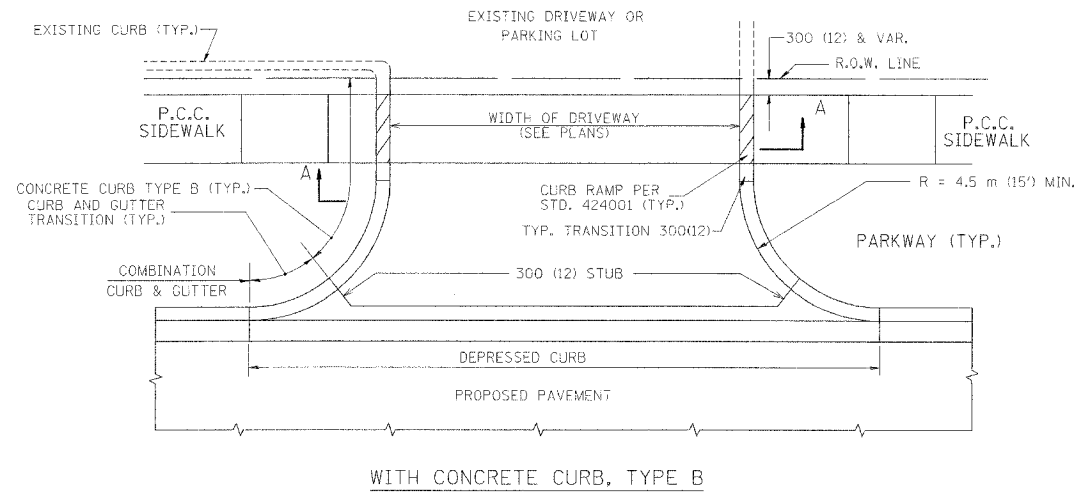
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DATE: 1/13/08

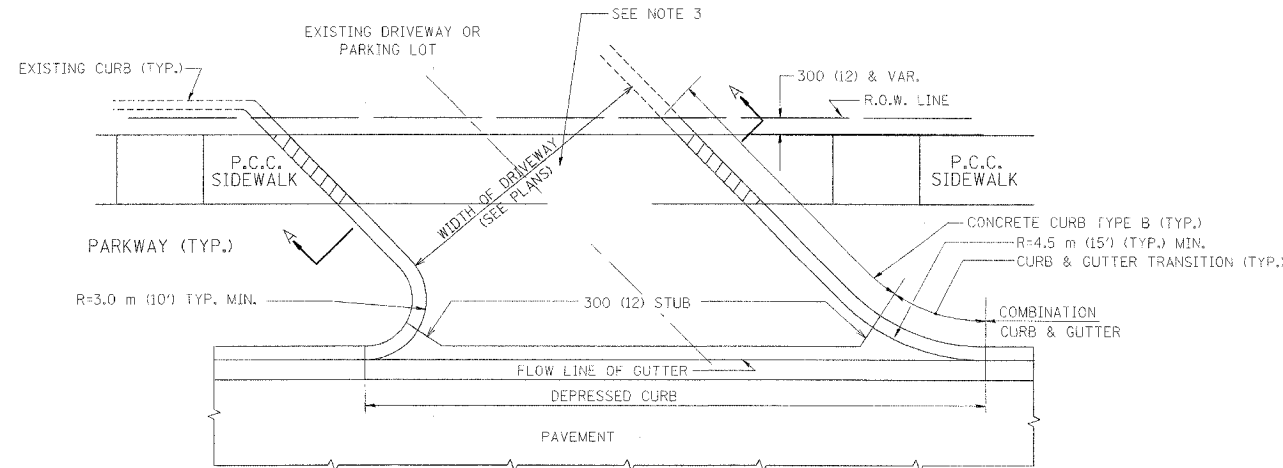
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SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766 05-00078-00-RS	COOK	17	9

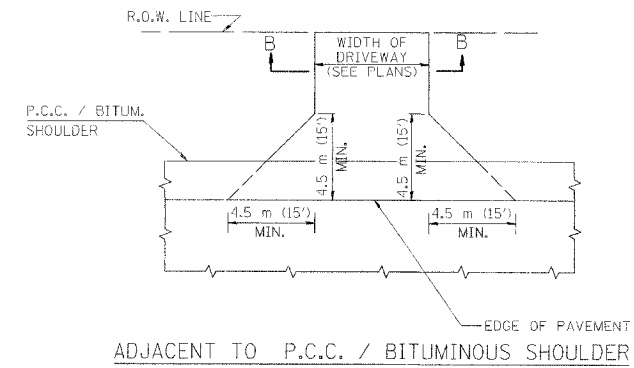
Contract No: 83B39



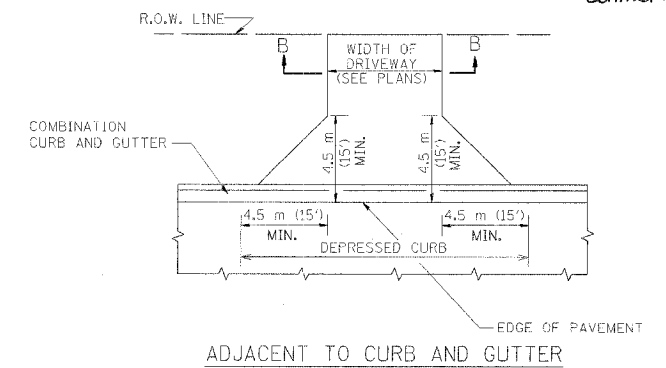
WITH CONCRETE CURB, TYPE B



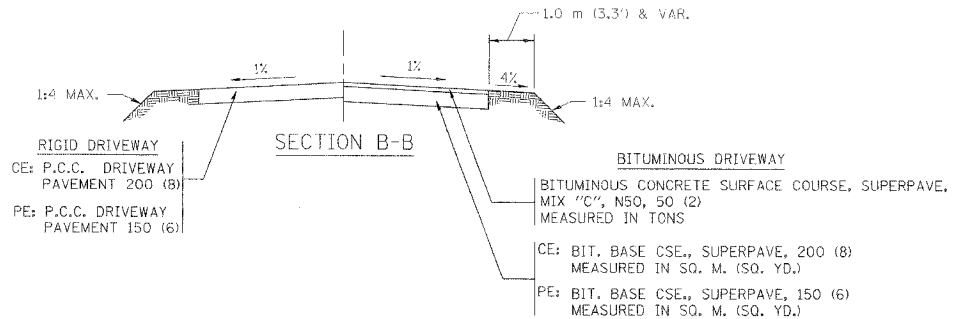
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / BITUMINOUS SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)
 BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX "C", N50, 50 (2) MEASURED IN TONS
 AGGREGATE BASE CSE., TYPE A 200 (8) MEASURED IN SQ. M. (SQ. YD.)

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

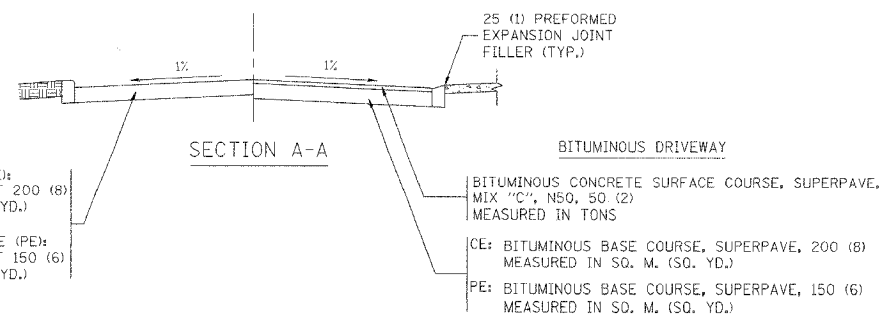
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 1.2 METERS (4 FEET) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

25 (1) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



RIGID DRIVEWAY
 COMMERCIAL ENTRANCE (CE): P.C.C. DRIVEWAY PAVEMENT 200 (8) MEASURED IN SQ. M. (SQ. YD.)
 NON-COMMERCIAL ENTRANCE (PE): P.C.C. DRIVEWAY PAVEMENT 150 (6) MEASURED IN SQ. M. (SQ. YD.)

BITUMINOUS DRIVEWAY
 BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50, 50 (2) MEASURED IN TONS
 CE: BITUMINOUS BASE COURSE, SUPERPAVE, 200 (8) MEASURED IN SQ. M. (SQ. YD.)
 PE: BITUMINOUS BASE COURSE, SUPERPAVE, 150 (6) MEASURED IN SQ. M. (SQ. YD.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED

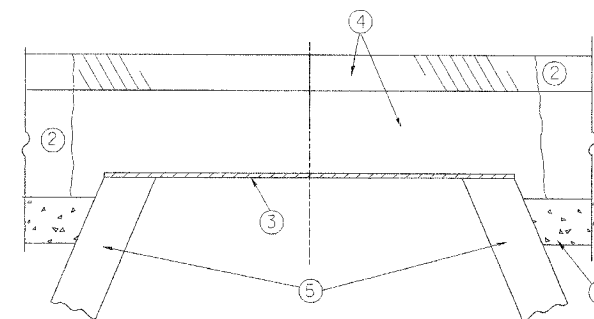
ILLINOIS DEPARTMENT OF TRANSPORTATION
DRIVEWAY DETAILS
 DISTANCE BETWEEN R.O.W. AND FACE OF CURB / EDGE OF SHOULDER >= 4.5 m (15')

REVISIONS	
NAME	DATE
P. LAFLEUR	04-15-03
R. SHAH	11-04-95
J. POLLASTRINI	08-12-96
J. POLLASTRINI	12-14-96
A. ABBAS	03-21-97
T. HOLTZ	04-08-97
M. GOMEZ	04-06-01

SCALE: NONE
 DATE PLOTTED: 04/17/2003
 DRAWN BY: SC
 CHECKED BY: JFP

F. A. REC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	05-00078-00-RS	COOK	17	10
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

Contract No. 03839



CONSTRUCTION PROCEDURES

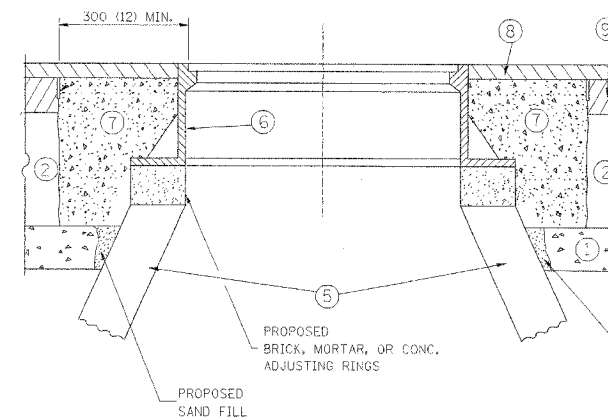
STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 300 (12) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 900 (36) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 40 (1 1/2) THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.



LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 900 (36) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND BITUMINOUS MATERIAL
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL
- ⑧ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE
- ⑨ PROPOSED BITUMINOUS CONCRETE BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: FRAMES AND LIDS TO BE ADJUSTED, SPECIAL EACH

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

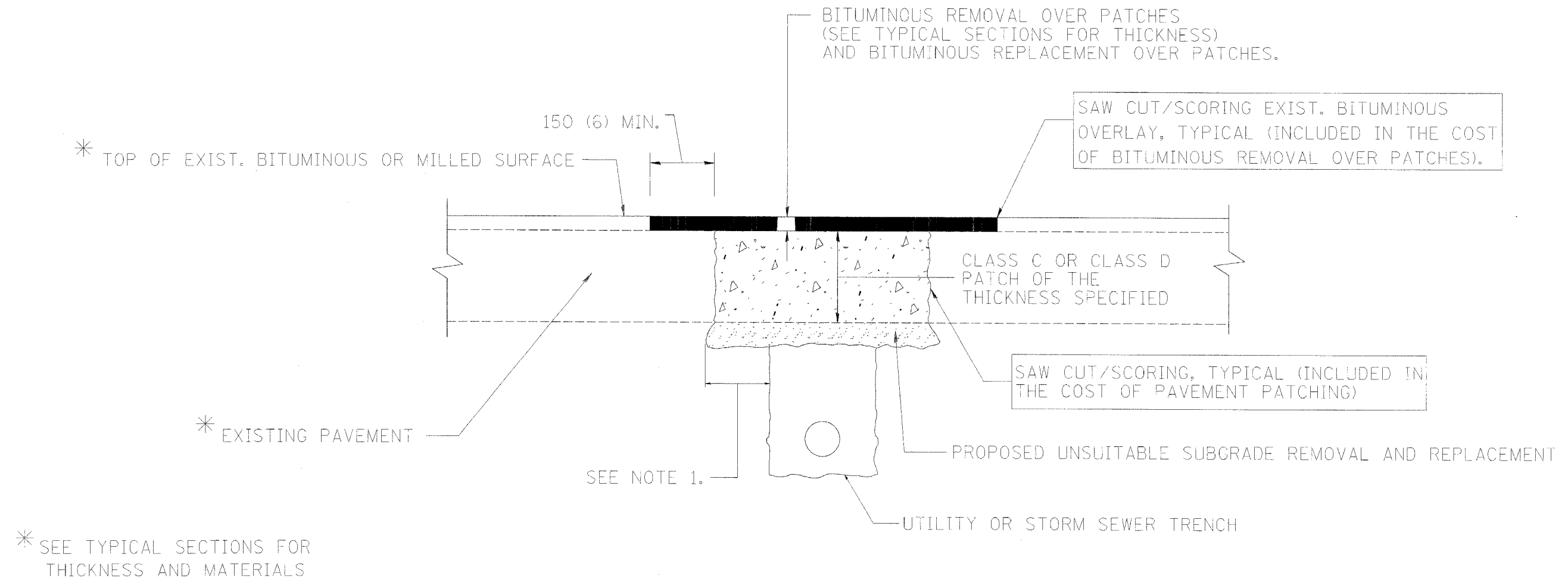
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04

SCALE: NONE
DATE: 05/17/2004

DRAWN BY
CHECKED BY
BD600-03 (BD-8)
REVISION DATE: 05/17/04



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 300 (12) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE SPECIAL PROVISION "PATCHING WITH BITUMINOUS OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT

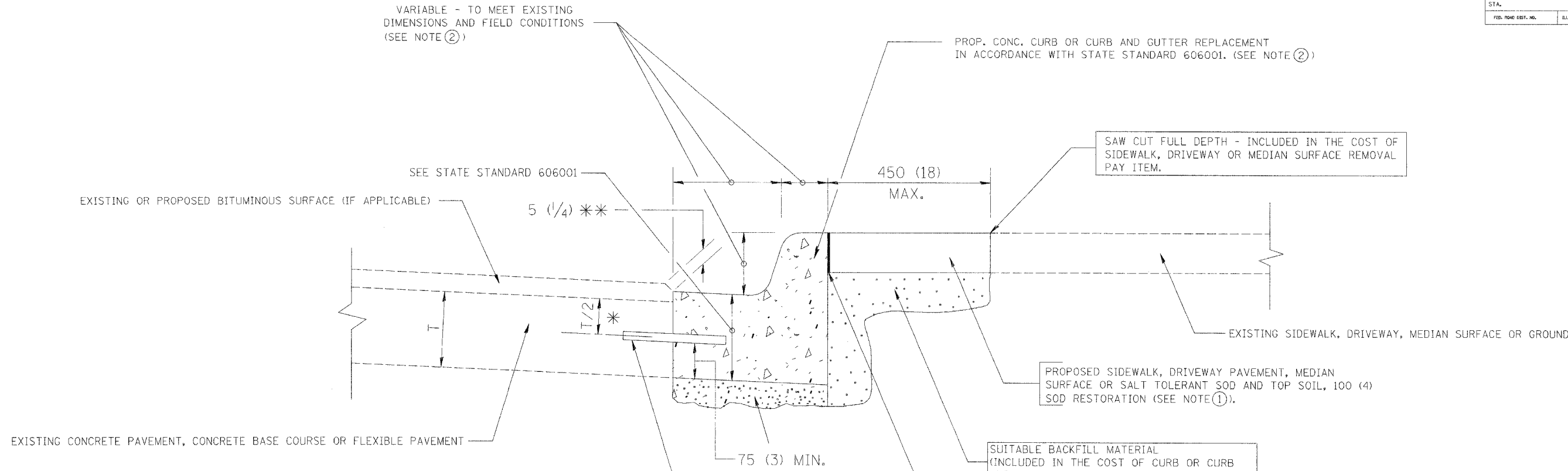
REVISIONS		REVISIONS	
NAME	DATE	NAME	DATE
R. SHAH	10/25/94	ART ABBAS	04/27/98
R. SHAH	01/14/95		
R. SHAH	03/23/95		
R. SHAH	04/24/95		
A. HOUSEH	03/15/96		
A. ABBAS	03/21/97		
A. ABBAS	01/20/98		

SCALE: NONE
DATE: 10/18/2002

DRAWN BY
CHECKED BY

BD400-04 (BD-22)
REVISION DATE: 04/27/98

Contract No. 83859



* 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE:
- ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
 - ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
 - ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
 - ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑤ THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
 - ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SALT TOLERANT SOD AND TOP SOIL, 100 (4) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT 100 (4) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 100 (4) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED NO. 20 (NO. 6) EPOXY COATED TIE BARS 600 (24) LONG AT 600 (24) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

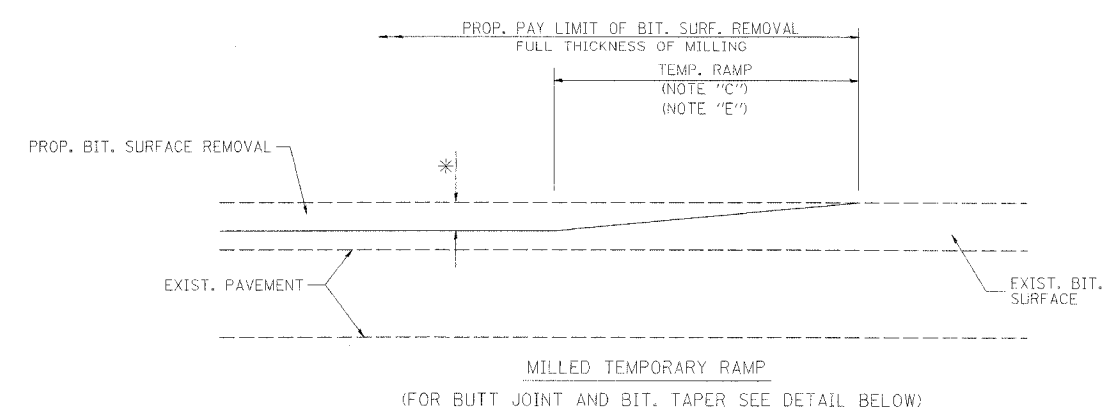
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

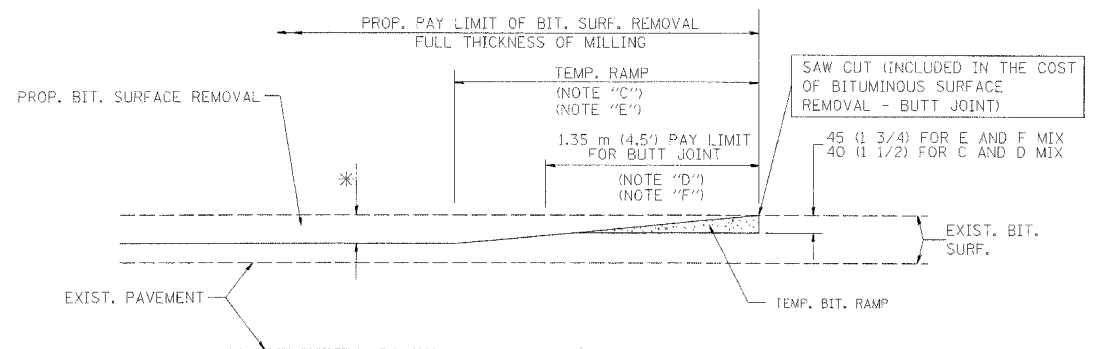
REVISIONS	
NAME	DATE
M. DE YONG	05/28/91
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01

ILLINOIS DEPARTMENT OF TRANSPORTATION
**CURB OR
 CURB AND GUTTER
 REMOVAL AND REPLACEMENT**

SCALE: NONE
 DATE: 10/18/2002
 DRAWN BY:
 CHECKED BY:
 BD600-06 (BD-24)

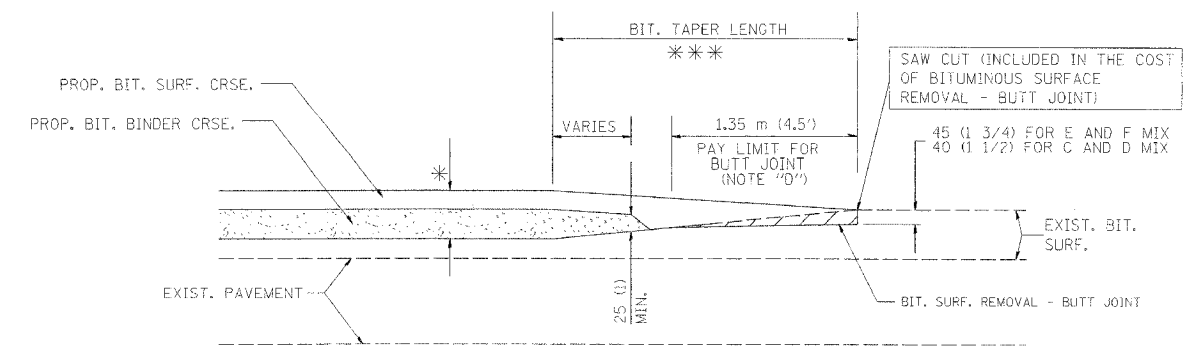


OPTION 1

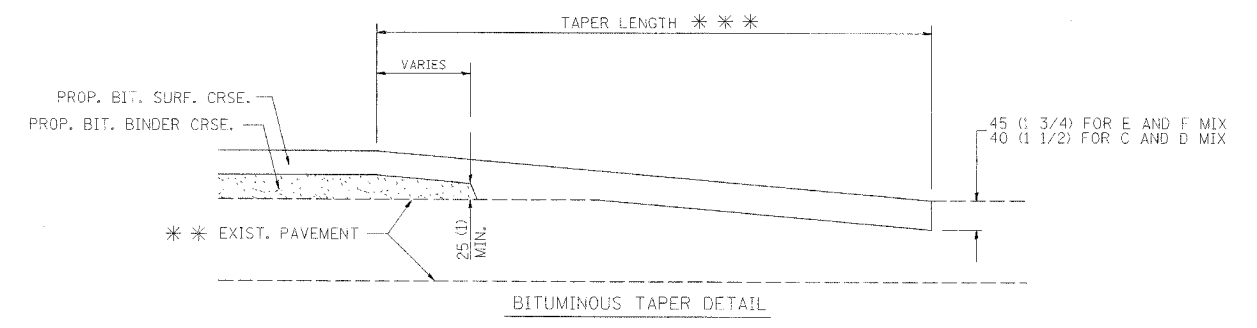
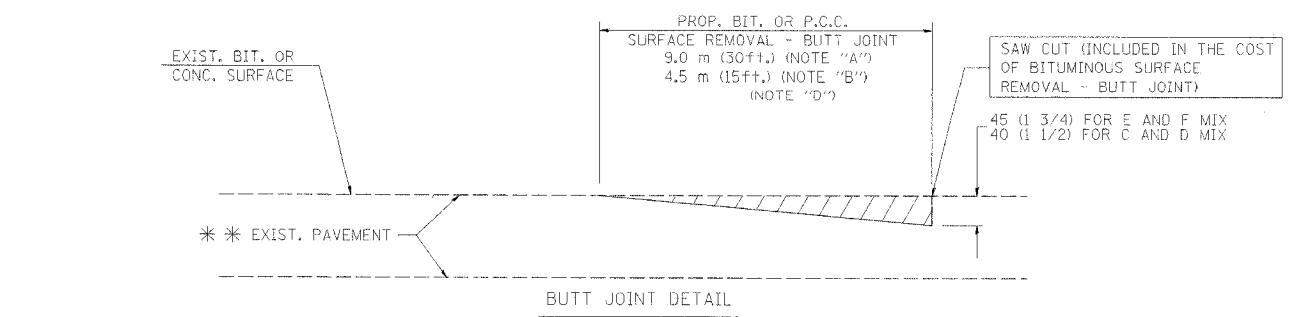


BITUMINOUS CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT AND BITUMINOUS TAPER
TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR RESURFACING ONLY

*** PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 FT.) PER INCH OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".
 - G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 6.1 m (20') PER 25 (1) RESURFACING (NOTE "A")
3.0 m (10') PER 25 (1) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

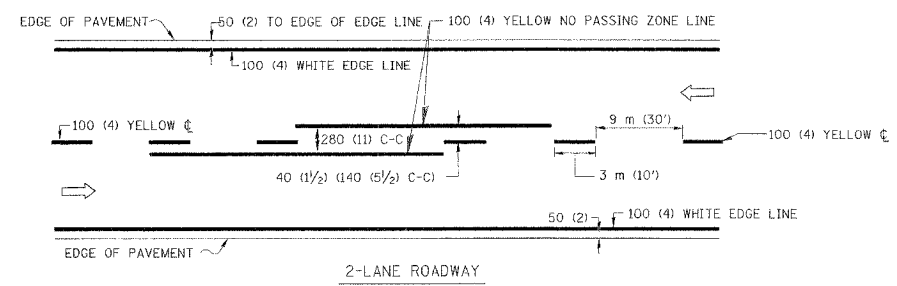
THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01

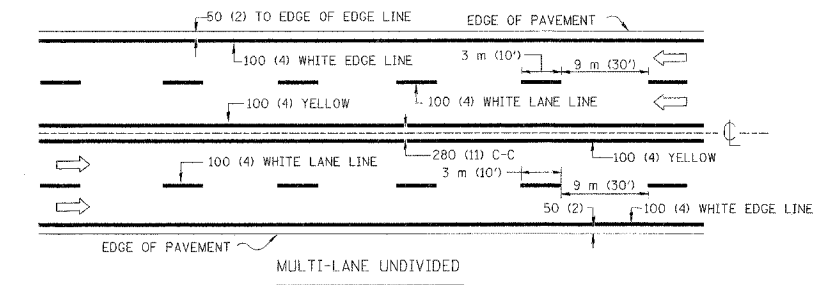
ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND BITUMINOUS TAPER DETAILS

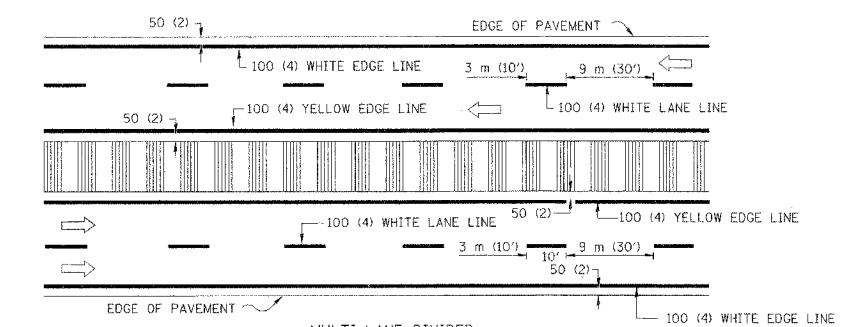
SCALE: NONE
DATE PLOTTED: 10/18/2002
DRAWN BY
CHECKED BY
BD400-05 (VI:BD32)
REVISION DATE: 04/06/01



2-LANE ROADWAY



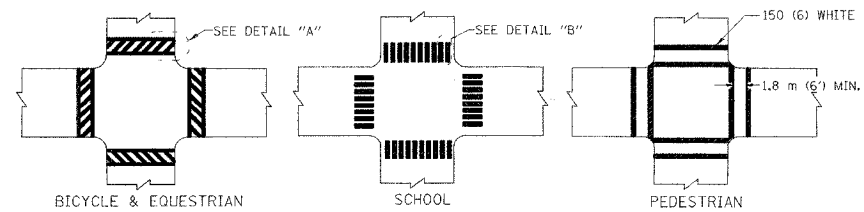
MULTI-LANE UNDIVIDED



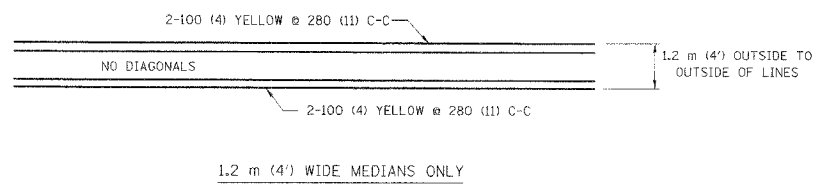
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

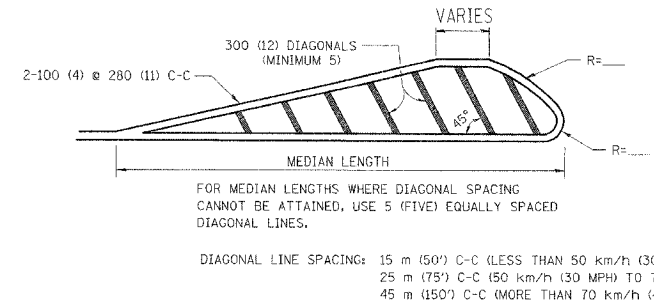
TYPICAL LANE AND EDGE LINE MARKING



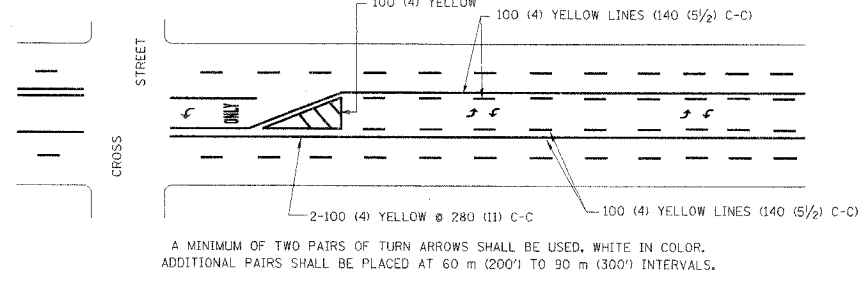
TYPICAL CROSSWALK MARKING



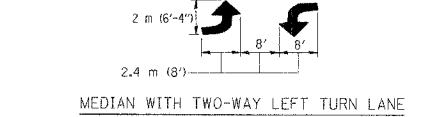
1.2 m (4') WIDE MEDIANS ONLY



MEDIANS OVER 1.2 m (4') WIDE

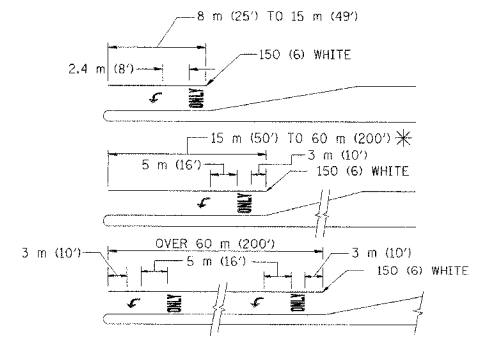


TYPICAL PAINTED MEDIAN MARKING



MEDIAN WITH TWO-WAY LEFT TURN LANE

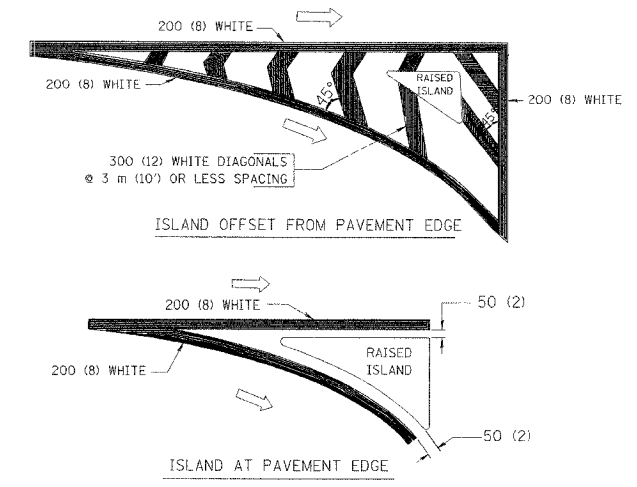
TYPICAL PAINTED MEDIAN MARKING



TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINES FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33m ² (3.6 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.)
SHOULDER DIAGONALS	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))	SOLID	WHITE - RIGHT YELLOW - LEFT	

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT
MARKINGS

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

SCALE: NONE
DATE 10/18/2002

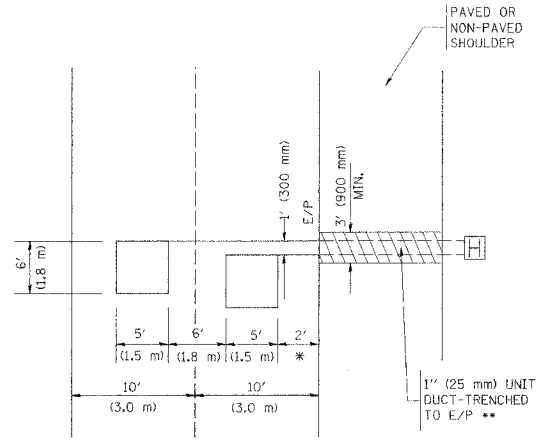
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CHECKED BY

F.A. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	05-00078-00-RS	COOK	17	15
STA.	TO STA.		FED. AID PROJECT	
FED. ROAD DIST. NO. 7	B.L.N.O.S.	FED. AID PROJECT		

Contract No. B3839

LOOPS NEXT TO SHOULDERS

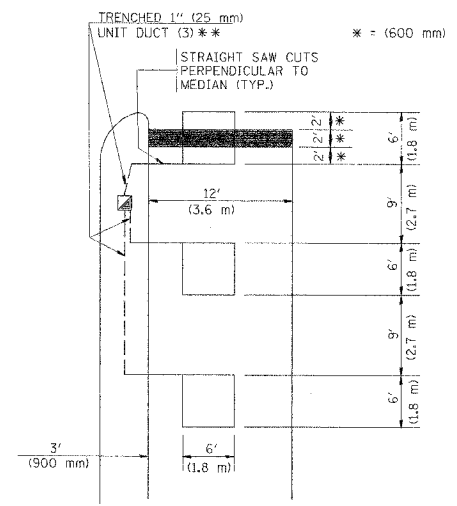
PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



* = (600 mm)
 ** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

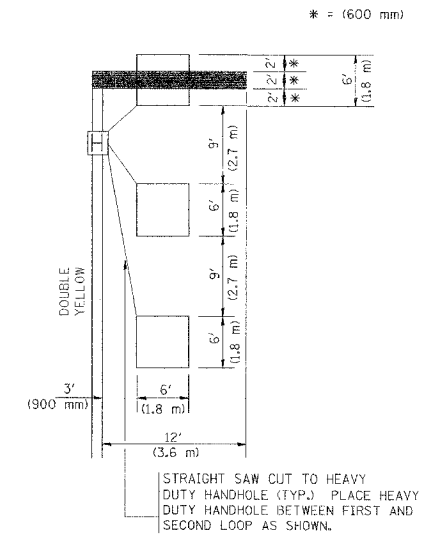
LEFT TURN LANES WITH MEDIANS
 VOLUME DENSITY ("FAR OUT" DETECTION)
 ON SAME APPROACH
 (PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD B14001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.
 NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
 VOLUME DENSITY ("FAR OUT" DETECTION)
 ON SAME APPROACH
 (PROTECTED / PERMITTED LEFT TURN PHASING)

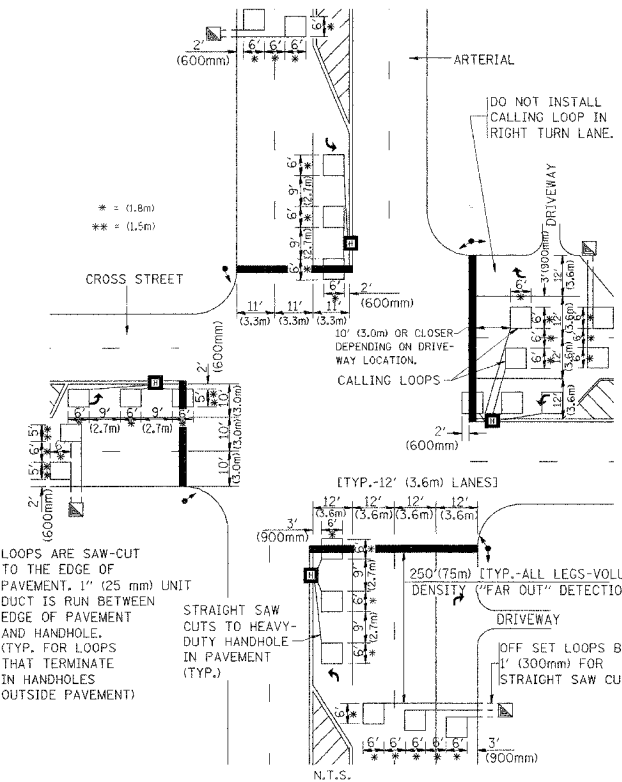


NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

- VEHICLES LOOP DETECTORS
- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
 - * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
 - * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
 - * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
 - * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
 - * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
 - * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

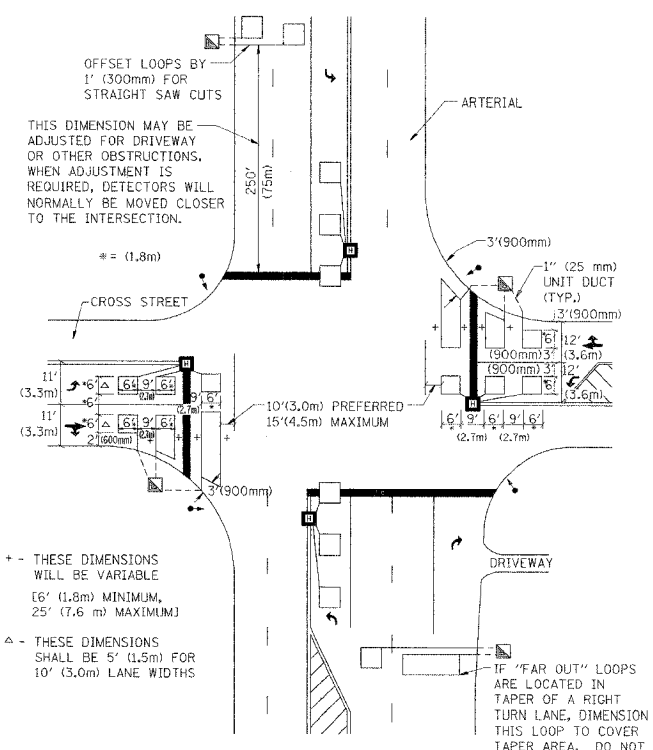
ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
 CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)
 STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)
 OFF SET LOOPS BY 1' (300mm) FOR STRAIGHT SAW CUTS.
 N.T.S.

DETAIL 1
 N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
 CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



OFF SET LOOPS BY 1' (300mm) FOR STRAIGHT SAW CUTS
 THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.
 * = (1.8m)
 + - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
 Δ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS
 N.T.S.

DETAIL 2
 N.T.S.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

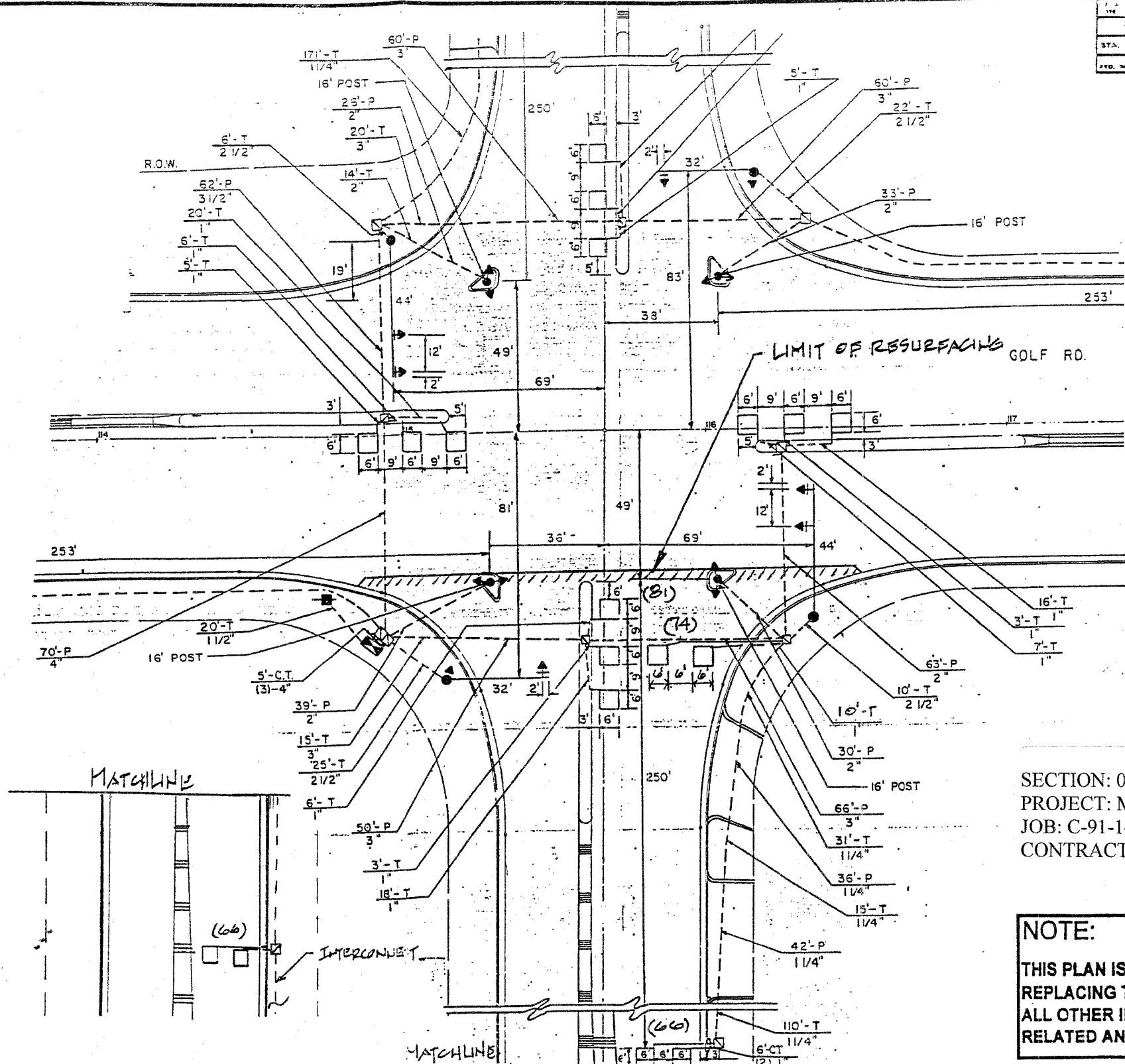
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT 1
 DETECTOR LOOP
 INSTALLATION DETAILS
 FOR ROADWAY RESURFACING

SCALE: NONE
 DATE: 10/18/2002
 DRAWN BY: CADD
 DESIGNED BY:
 CHECKED BY: R.K.F.
 TSO7
 REVISION DATE:



NORTH

SECTION: 05-00078-00-RS
 PROJECT: M-8003(565)
 JOB: C-91-142-06
 CONTRACT: 83839

NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

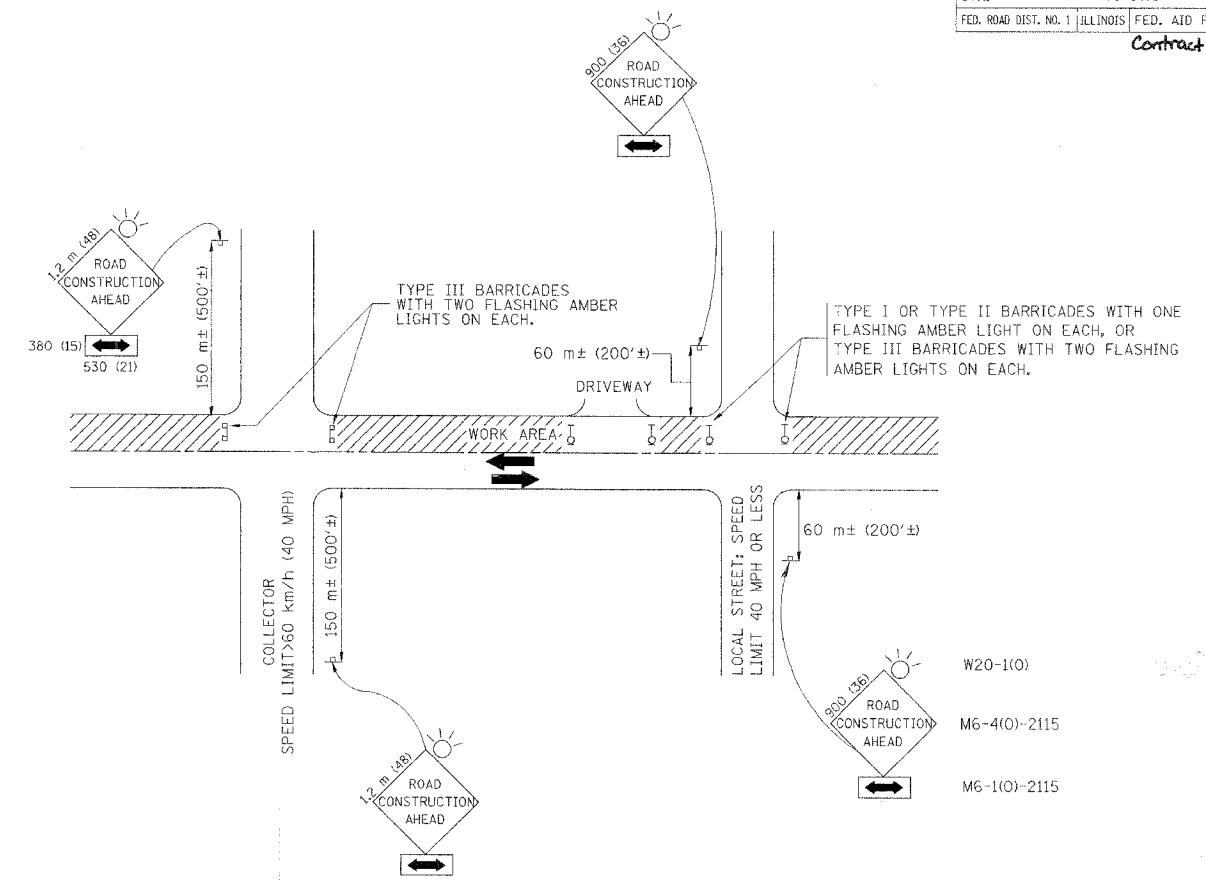
REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86500600	237	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 HARLEM AV. @ IL. RT. 58 (GOLF RD.)
 SCALE: NONE
 DATE: DEC. 05
 DRAWN BY: J.E.
 DESIGNED BY: J.E.
 CHECKED BY: D.R.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	105-0078-00-RS	COOK	17	17
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
Contract Ab: 83839				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).**
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
**TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS**

SCALE: VERT. _____
 HORIZ. _____
 DATE 10/18/2002

DRAWN BY _____
 CHECKED BY _____
 TC-10