

























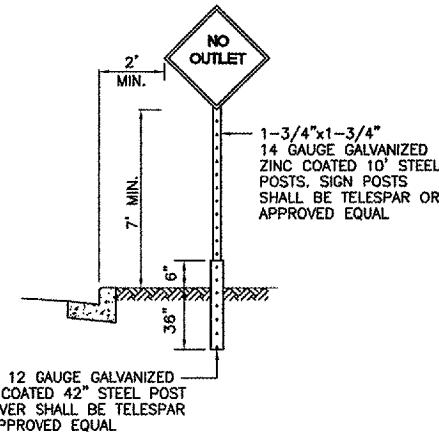
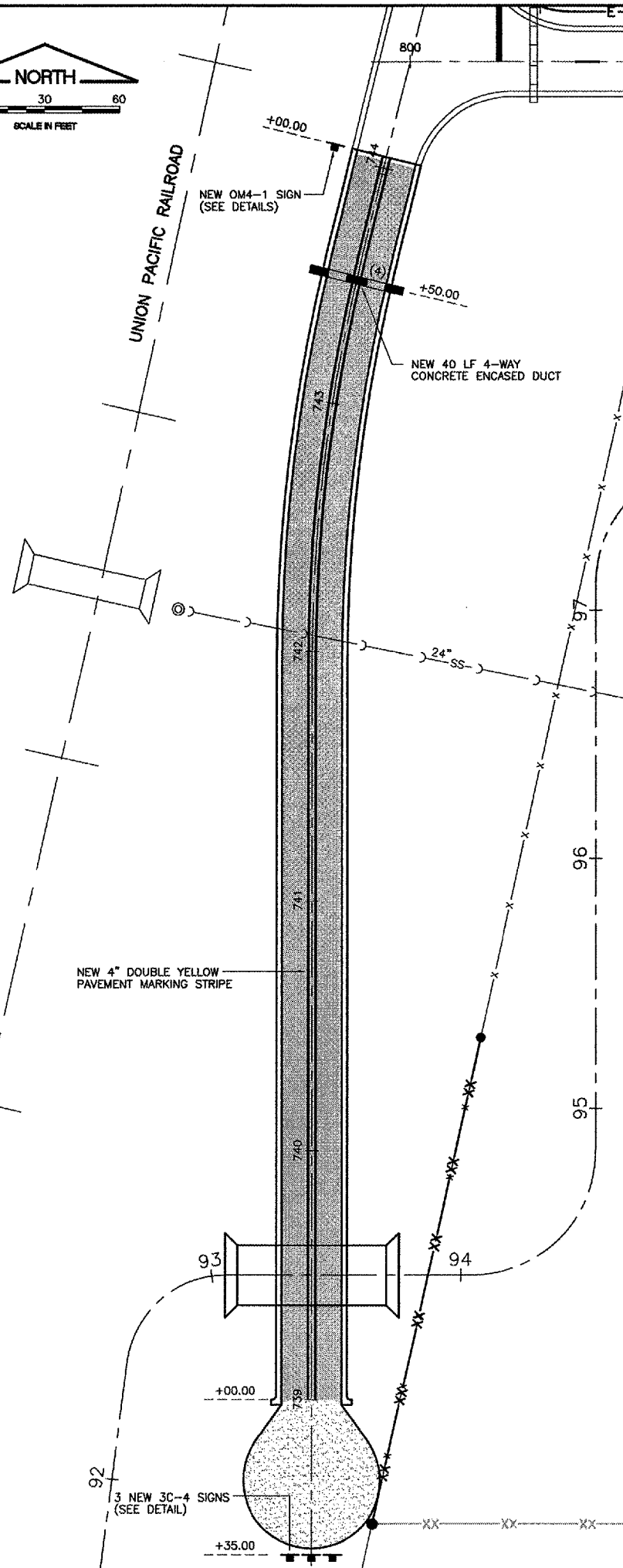
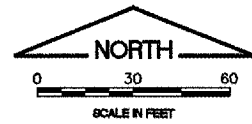


LEGEND

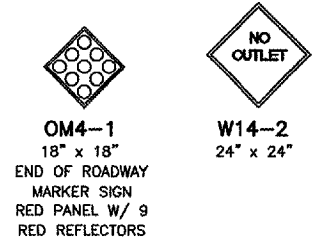
-  BUTT JOINT CONSTRUCTION
-  NEW 4' WIDE AGGREGATE SHOULDER - 6"
-  NEW BITUMINOUS PAVEMENT
-  NEW 6" THICK CRUSHED AGGREGATE TURNAROUND
-  EXISTING BASE MOUNTED TAXI LIGHT
-  EXISTING DUCT BANK
-  EXISTING COMED CABLE(S)
-  EXISTING LIGHTING CIRCUIT
-  EXISTING LIGHTING CIRCUIT TO BE ABANDONED
-  EXISTING WATERMAIN
-  EXISTING GAS LINE
-  EXISTING SANITARY SEWER
-  EXISTING STORM SEWER
-  EXISTING UNDERDRAIN
-  EXISTING MANHOLE/INLET/FLARED END SECTION
-  EXISTING FENCE
-  EXISTING SBC COMMUNICATION CABLE(S)
-  EXISTING ROADWAY SIGN
-  NEW ROADWAY SIGN
-  EXISTING ROADWAY LIGHT
-  EXISTING HANDHOLE
-  NEW STORM SEWER
-  NEW MANHOLE/INLET/FLARED END SECTION
-  NEW DUCT BANK OR STEEL CONDUIT
-  NEW LIGHTING CIRCUIT
-  NEW ROADWAY LIGHT (SEE DETAIL)

ELECTRICAL NOTES

1. CONTRACTOR SHALL SPLICE EXISTING LIGHTING CABLE WITH NEW CABLE. COST SHALL BE INCIDENTAL TO AR800140.
2. THE ROUTING OF THE PROPOSED CABLES AND CONDUIT SHOWN IS FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE COORDINATED WITH THE ENGINEER. THE MINIMUM BURIAL DEPTH OF THE CABLES SHALL BE 30" BELOW FINISHED GRADE. PROPOSED CONDUITS SHALL BE INSTALLED AT A DEPTH LOWER THAN THE PROPOSED UNDERDRAIN BY A MINIMUM OF 1 FOOT.
3. IT IS CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF THE EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT CONTRACTOR'S EXPENSE.
4. ANY SPLICES TO THE PROPOSED CABLES SHALL BE MADE INSIDE THE HANDHOLE OR POLE BASES. NO DIRECT BURIED SPLICES WILL BE ALLOWED. ALL SPLICES SHALL BE WATER PROOF AND SHALL HAVE HEAT SHRINK TUBING OVER ENTIRE SPLICE.
5. ALL AREAS DISTURBED AS A RESULT OF THE WORK UNDER THIS CONTRACT SHALL BE RESTORED. THE COST OF RESTORATION SHALL BE INCIDENTAL TO THE RESPECTIVE ITEMS.
6. WHEN CROSSING THE EXISTING UNDERGROUND UTILITIES, CONTRACTOR SHALL HAND DIG THE TRENCH TO AVOID ANY DAMAGE. CONTRACTOR MAY BE REQUIRED TO ROUTE THE PROPOSED CABLES UNDER THE EXISTING UTILITIES AS DIRECTED BY THE ENGINEER. NO ADDITIONAL COMPENSATION WILL BE MADE FOR WORK REQUIRED TO PLACE CABLES BENEATH EXISTING UTILITIES.
7. CONTRACTOR SHALL CORE HOLE INTO EXISTING MANHOLE/HANDHOLE TO ROUTE PROPOSED UNIT DUCT. COST SHALL BE INCIDENTAL TO THE INSTALLATION OF CABLE IN UNIT DUCT.
8. INSTALL PROPOSED CABLE IN UNIT DUCT IN EXISTING CONDUIT. CONTRACTOR SHALL USE EXISTING CONDUIT ALREADY USED FOR LIGHTING CIRCUITS.
9. THE CONTRACTOR SHALL LOCATE, REMOVE EXISTING DUCT MARKERS AND INSTALL THEM UPON COMPLETION OF THE BITUMINOUS PAVEMENT OVERLAY. REMOVAL AND INSTALLATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. THE EXISTING LIGHTING CABLE SHALL REMAIN IN SERVICE UNTIL THE NEW CIRCUIT AND LIGHTS ARE COMPLETED AND WORKING.



POST MOUNTED SIGN LOCATION DETAIL
N.T.S.



SIGN LEGEND
NO SCALE

SIGN NOTES

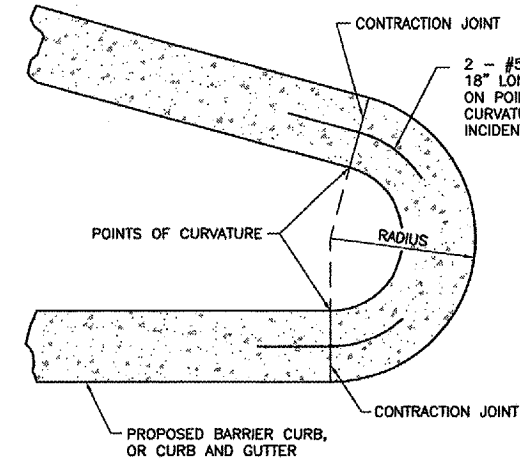
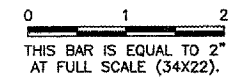
1. ALL SIGNS SHALL BE 3M DIAMOND GRADE VIP REFLECTIVE SHEETING SERIES 3990 OR APPROVED EQUAL.
2. ALL SIGNS ARE STANDARD COLORS AND LETTER STYLE AND SIZE AS SHOWN IN THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", LATEST EDITION.
3. ALL NEW SIGN POSTS SHALL HAVE 4"x72" DIAMOND GRADE, COLOR MATCHED, SQUARE TUBE REFLECTIVE POST PANELS INSTALLED (COST INCIDENTAL TO NEW SIGNS).

DU072

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UPDATE BY: .
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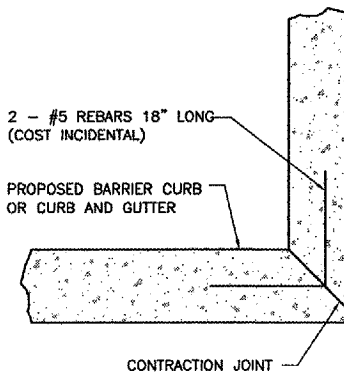
REVISIONS

NUMBER	BY	DATE



ISLAND RADII REINFORCEMENT DETAIL
N.T.S.

INSTALL TIE BARS IN LIEU OF DOWEL BARS WHEN RADII ARE 7.5' AND LESS.



CORNER REINFORCEMENT DETAIL
N.T.S.

REINFORCEMENTS SHALL BE INSTALLED AT ALL CORNERS.

CURB AND GUTTER TYPE M-6.18 NOTES

1. CONTRACTION JOINTS SHALL BE PLACED AT TEN (10) FOOT MINIMUM ON CENTERS AND SHALL BE SAW CUT TO MINIMUM DEPTH OF TWO (2) INCHES FROM FRONT TO BACK AS SOON AS THE CONCRETE IS SUFFICIENTLY CURED TO ALLOW CUTTING. AS A MAXIMUM, THE CURB SHALL BE SAWED WITHIN TWENTY-FOUR (24) HOURS OF CONCRETE PLACEMENT. CONTRACTION JOINTS SHALL BE FILLED WITH GRAY NP1 OR EQUIVALENT.
2. EXPANSION JOINTS SHALL BE CONSTRUCTED AT A 50' MAXIMUM SPACING.
3. ALL CURBS SHALL BE CURED AND PROTECTED TO THE REQUIREMENTS OF ARTICLE 606 OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION. MEMBRANE CURING WITH W.R. MEADOWS CS 309, OR APPROVED EQUAL, WILL BE ALLOWED WITH A WHITE FUGITIVE DYE, AS PER TYPE II MEMBRANE CURING.
4. CURB AND GUTTER SHALL HAVE A LIGHT BROOM FINISH.
5. EXPANSION JOINTS SHALL ALSO BE PLACED AT ALL POINTS OF CURVATURE, AT 5' EACH SIDE OF ALL DRAINAGE STRUCTURES, AT THE END OF THE DAYS POUR, OR AS DIRECTED BY THE ENGINEER.
6. TWO (2) NO. 5 REBARS (TEN) 10' LONG SHALL BE PLACED ON EITHER SIDE OF ALL PROPOSED UTILITY TRENCHES.
7. THREE (3) NO. 5 REBARS SPACED 5" APART SHALL BE PLACED THE LENGTH OF ALL DEPRESSED CURB.

DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER
DEVELOPMENT - PHASE 1
PAVEMENT MARKING / SIGNING PLAN /
MISCELLANEOUS DETAILS - DUPAGE DRIVE

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DESIGN BY:	JRL
DRAWN BY:	JRO
CHECKED BY:	JRL
APPROVED BY:	DLP
DATE:	03/03/06
JOB No:	05257-02