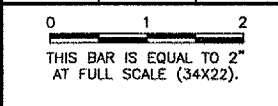


| REVISIONS |    |      |
|-----------|----|------|
| NUMBER    | BY | DATE |
|           |    |      |
|           |    |      |
|           |    |      |



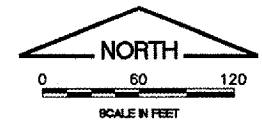
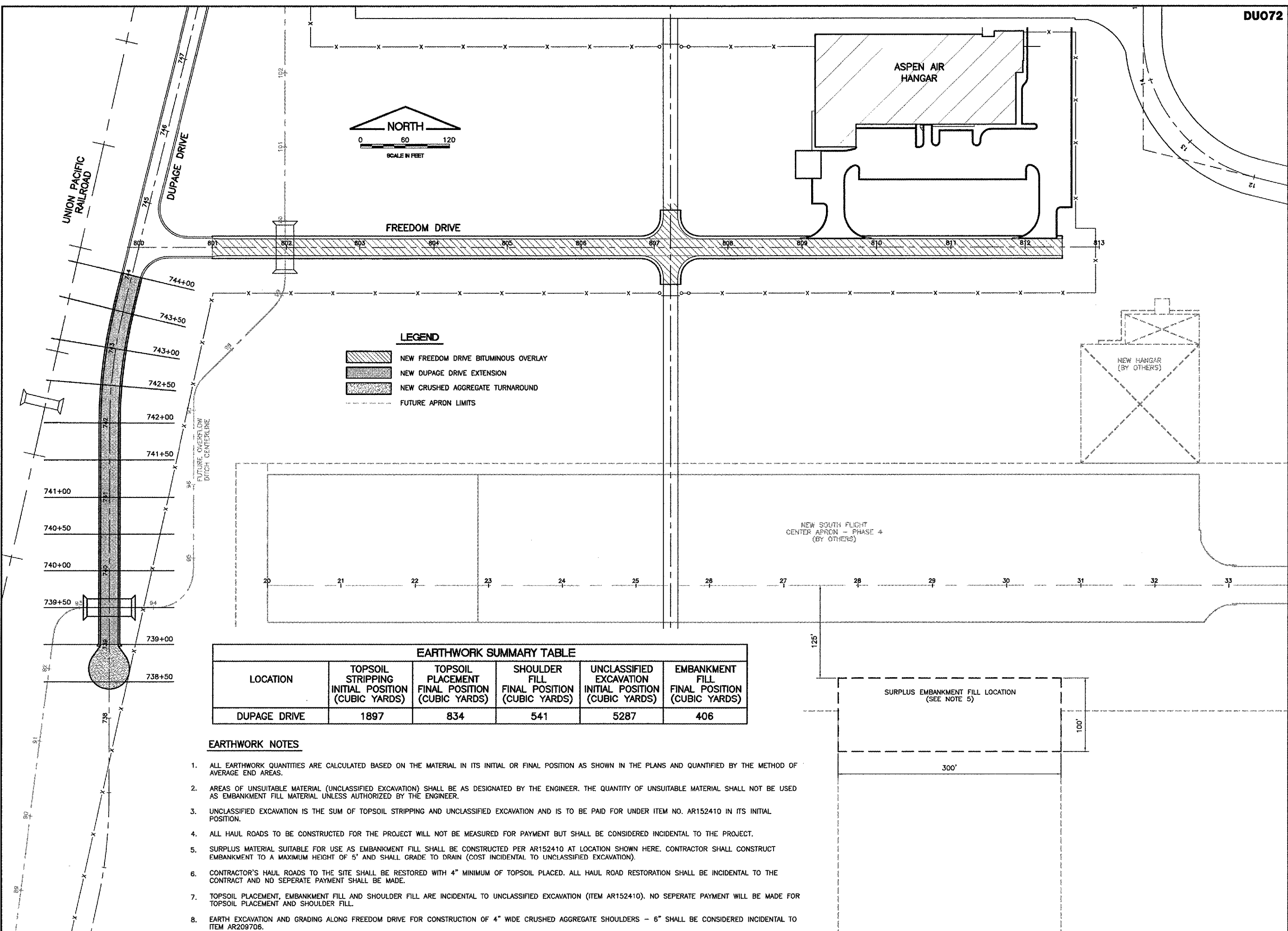
**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1**

**INDEX TO CROSS SECTIONS**

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|              |          |
|--------------|----------|
| DESIGN BY:   | JRL      |
| DRAWN BY:    | JRO      |
| CHECKED BY:  | MJS      |
| APPROVED BY: | DLP      |
| DATE:        | 03/03/06 |
| JOB No:      | 05257-02 |



**LEGEND**

|  |                                      |
|--|--------------------------------------|
|  | NEW FREEDOM DRIVE BITUMINOUS OVERLAY |
|  | NEW DUPAGE DRIVE EXTENSION           |
|  | NEW CRUSHED AGGREGATE TURNAROUND     |
|  | FUTURE APRON LIMITS                  |

| EARTHWORK SUMMARY TABLE |  |  |  |  |  |
|-------------------------|--|--|--|--|--|
| LOCATION                | TOPSOIL STRIPPING<br>INITIAL POSITION<br>(CUBIC YARDS) | TOPSOIL PLACEMENT<br>FINAL POSITION<br>(CUBIC YARDS) | SHOULDER FILL<br>FINAL POSITION<br>(CUBIC YARDS) | UNCLASSIFIED EXCAVATION<br>INITIAL POSITION<br>(CUBIC YARDS) | EMBANKMENT FILL<br>FINAL POSITION<br>(CUBIC YARDS) |
| DUPAGE DRIVE            | 1897   | 834  | 541  | 5287   | 406  |

- EARTHWORK NOTES**
- ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
  - AREAS OF UNSUITABLE MATERIAL (UNCLASSIFIED EXCAVATION) SHALL BE AS DESIGNATED BY THE ENGINEER. THE QUANTITY OF UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT FILL MATERIAL UNLESS AUTHORIZED BY THE ENGINEER.
  - UNCLASSIFIED EXCAVATION IS THE SUM OF TOPSOIL STRIPPING AND UNCLASSIFIED EXCAVATION AND IS TO BE PAID FOR UNDER ITEM NO. AR152410 IN ITS INITIAL POSITION.
  - ALL HAUL ROADS TO BE CONSTRUCTED FOR THE PROJECT WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
  - SURPLUS MATERIAL SUITABLE FOR USE AS EMBANKMENT FILL SHALL BE CONSTRUCTED PER AR152410 AT LOCATION SHOWN HERE. CONTRACTOR SHALL CONSTRUCT EMBANKMENT TO A MAXIMUM HEIGHT OF 5' AND SHALL GRADE TO DRAIN (COST INCIDENTAL TO UNCLASSIFIED EXCAVATION).
  - CONTRACTOR'S HAUL ROADS TO THE SITE SHALL BE RESTORED WITH 4" MINIMUM OF TOPSOIL PLACED. ALL HAUL ROAD RESTORATION SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPERATE PAYMENT SHALL BE MADE.
  - TOPSOIL PLACEMENT, EMBANKMENT FILL AND SHOULDER FILL ARE INCIDENTAL TO UNCLASSIFIED EXCAVATION (ITEM AR152410). NO SEPERATE PAYMENT WILL BE MADE FOR TOPSOIL PLACEMENT AND SHOULDER FILL.
  - EARTH EXCAVATION AND GRADING ALONG FREEDOM DRIVE FOR CONSTRUCTION OF 4" WIDE CRUSHED AGGREGATE SHOULDERS - 6" SHALL BE CONSIDERED INCIDENTAL TO ITEM AR209706.

