04-28-2017 LETTING ITEM 015

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	1
	ILLINOIS	CONTRACT NO. 62C93		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED
HIGHWAY PLANS

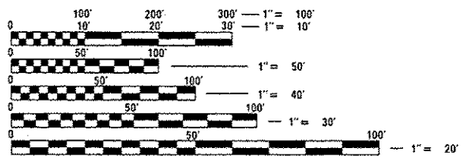
FAP 339 /IL 62 (ALGONQUIN ROAD)
E /O ROSELLE ROAD TO IL 58 (GOLF ROAD)
SECTION 116Y(1&2)R-RS-6
RESURFACING(3P), PEDESTRIAN RAMPS
PROJECT: NHPP-0339(035)
COOK COUNTY
C-91-411-16

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN:
THE VILLAGE OF ARLINGTON HEIGHTS
THE VILLAGE OF PALATINE
THE CITY OF ROLLING MEADOWS
THE VILLAGE OF SCHAUMBURG

TRAFFIC DATA

ROUTE SEGMENT	SPEED	ADT(YEAR)
IL 62 (ALGONQUIN RD)		
ROSELLE ROAD TO QUENTIN ROAD	45 MPH	22,500(2015)
QUENTIN ROAD TO MEACHAM ROAD	40 MPH	37,800(2015)
MEACHAM ROAD TO IL 53	35,40 MPH	35,800(2015)
IL 53 TO NEW WILKE ROAD	35 MPH	29,700(2015)
NEW WILKE ROAD TO IL 58 (GOLF ROAD)	35 MPH	30,700(2015)
IL 58 (GOLF RD) S. OF PROJECT LIMITS	45 MPH	29,900(2015)
IL 58 (GOLF ROAD)		
WEST OF IL 62 (ALGONQUIN RD)	40 MPH	30,900(2015)
EAST OF IL 62 (ALGONQUIN RD)	40 MPH	29,800(2015)

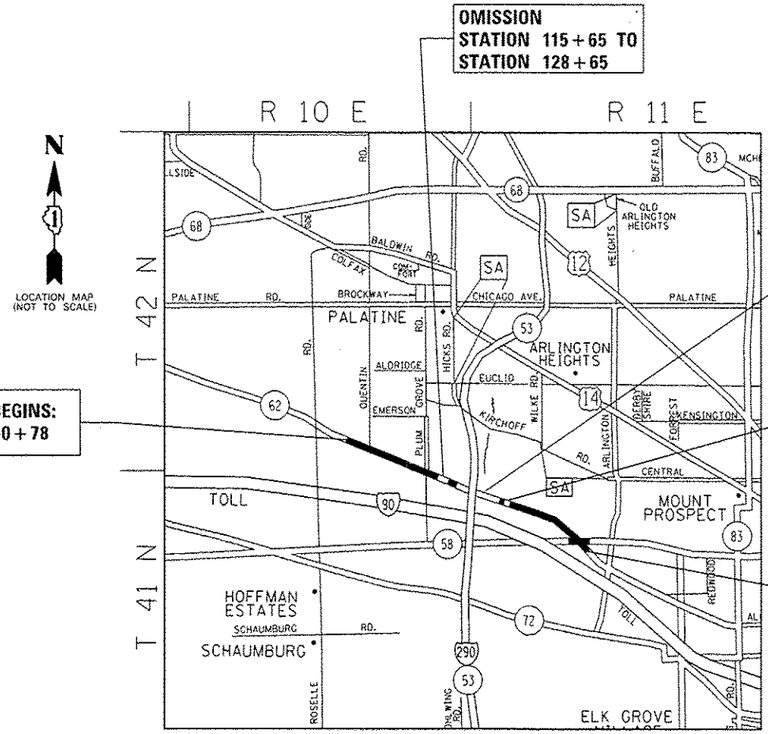


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62C93



GROSS LENGTH = 22,606.5 FT. = 4.29 MILES
NET LENGTH = 15,993.8 FT. = 3.02 MILES

D-91-411-16

LOCATION OF SECTION INDICATED THUS: —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED February 6 20 17
REGIONAL ENGINEER
Mar 24 20 17
ENGINEER OF DESIGN AND ENVIRONMENT
Mar 24 20 17
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-09	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-02	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
424026-01	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS, TYPE 1
604091-03	FRAME AND GRATE TYPE 24
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS >= 45 MPH
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS <= 40 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701602-08	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-06	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF ARLINGTON HEIGHTS, PALATINE, SCHAUMBURG, AND THE CITY OF ROLLING MEADOWS.
3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
4. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
5. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
6. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
7. ALL PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, DRAINAGE ADJUSTMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
8. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
9. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
10. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
11. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
12. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
13. THE RESIDENT ENGINEER SHALL CONTACT CORY JUCIUS, ARTERIAL TRAFFIC FIELD ENGINEER, AT CORY.JUCIUS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
14. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
15. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
16. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
17. UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
18. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
19. ALL PROPOSED SIDEWALK RAMPS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
20. CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS, & GENERAL NOTES
IL 62 (ALGONQUIN ROAD) – E/O ROSELLE ROAD TO IL 58(GOLF ROAD)

SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	11GY(1&2)R-RS-6	COOK	58	2
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

URBAN

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE: 80% FEDERAL 20% STATE 0005					
20200100	EARTH EXCAVATION	CU YD	53	53					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	601	601					
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	11	11					
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	11	11					
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	11	11					
25200110	SODDING, SALT TOLERANT	SQ YD	601	601					
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	10	10					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	78764	78764					
40600400	MIXTURE FOR CRACKS, JOINTS, AND	TON	185	185					
	FLANGEWAYS								
40600827	POLYMERIZED LEVELING BINDER (MACHINE	TON	4273	4273					
	METHOD), IL-4.75, N50								
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	910	910					
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	2	2					
42001300	PROTECTIVE COAT	SQ YD	1658	1658					
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY	SQ YD	32	32					
	PAVEMENT, 8 INCH								
14 * SPECIALTY	ITEMS								

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE: 80% FEDERAL 20% STATE 0005					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	7234	7234					
42400800	DETECTABLE WARNINGS	SQ FT	666	666					
44000100	PAVEMENT REMOVAL	SQ YD	199	199					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	103599	103599					
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	42	42					
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	385	385					
44000600	SIDEWALK REMOVAL	SQ FT	6680	6680					
44003100	MEDIAN REMOVAL	SQ FT	800	800					
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	697	697					
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	1906	1906					
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	240	240					
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	150	150					
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	5	5					
60255500	MANHOLES TO BE ADJUSTED	EACH	5	5					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	13	13					
15 * SPECIALTY	ITEMS								

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL RTE. 62 (ALGONQUIN RD.) (E/O ROSELLE RD. TO IL RTE. 58 (GOLF RD.)
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	3
		CONTRACT NO. 62C93		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

	SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
	CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005					
	60404950	FRAMES AND GRATES, TYPE 24	EACH	4	4					
	60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2	2					
	60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	2	2					
	60619200	CONCRETE MEDIAN, TYPE SB-6.06	SQ FT	1112	1112					
	60626300	STABILIZED MEDIAN SURFACE	SQ YD	4	4					
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	53	53					
*	66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1					
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	4	4					
	67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
	67100100	MOBILIZATION	LSUM	1	1					
	70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	LSUM	1	1					
	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	LSUM	1	1					
	70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	LSUM	1	1					
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1					
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1					
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	25801	25801					
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	8592	8592					
	70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	2285	2285					
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	52608	52608					
	70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	12069	12069					
	70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	267	267					
	70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	3161	3161					
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1613	1613					
	70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	12984	12984					
*	72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	1	1					
*	72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1					
*	72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	1	1					
*	SPECIALTY ITEMS									

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL RTE. 62 (ALGONQUIN RD.) (E/O ROSELLE RD. TO IL RTE. 58 (GOLF RD.)
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	4
		CONTRACT NO. 62C93		
FED. ROAD DIST. NO.)	ILLINOIS	FED. AID PROJECT		

14

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FEDERAL 20% STATE 0005					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	2285	2285					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	52608	52608					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	12069	12069					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	267	267					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	3161	3161					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1613	1613					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1881	1881					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1429	1429					
85000200	MAINTENANCE OF TRAFFIC SIGNAL INSTALLATION	EACH	6	6					
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	4348	4348					
89502376	REBUILD EXISTING HANDHOLE	EACH	8	8					
89502378	REBUILD EXISTING HANDHOLE TO HEAVY-DUTY HANDHOLE	EACH	2	2					
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1					
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	12497	12497					

* SPECIALTY ITEMS

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FEDERAL 20% STATE 0005					
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	19394	19394					
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	71	71					
X6061311	CONCRETE MEDIAN SURFACE, 5 INCH	SQ FT	300	300					
X6350120	DELINEATOR REMOVAL	EACH	1	1					
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	36727	36727					
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	2053	2053					
△ Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	160	160					
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4					
Z0038120	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1 3/4"	SQ YD	249	249					
Ø Z0076600	TRAINEES	HOUR	500	500					
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	500	500					

△ NON-PARTICIPATING (100% STATE)
* SPECIALTY ITEMS
Ø 0042

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL RTE. 62 (ALGONQUIN RD.) (E/O ROSELLE RD. TO IL RTE. 58 (GOLF RD.)
SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	5

CONTRACT NO. 62C93

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

Rev

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

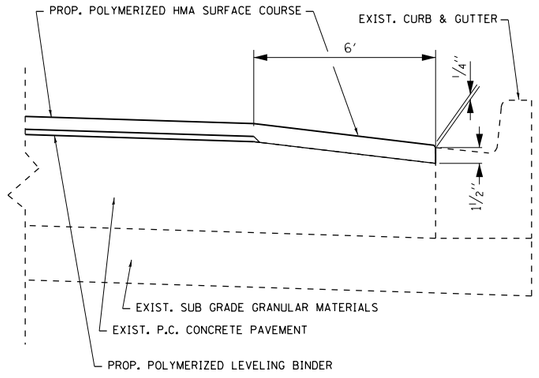
HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS(%) @ $N_{DES.}$	
PAVEMENT RESURFACING		
POLYMERIZED HMA SURFACE COURSE, SMA, N80 (IL 9.5 mm), 1¾''	3.5% @ 80 GYR	PFP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾''	3.5% @ 50 GYR	QCP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm), 10''	4% @ 70 GYR	QC/QA
DRIVEWAYS		
HOT-MIX ASPHALT SURFACE COURSE, MIX ''D'', N50 (IL 9.5 mm), 2''	4% @ 50 GYR	QC/QA
HMA BASE COURSE (HMA BINDER IL-19 mm), CE - 8''	4% @ 50 GYR	QC/QA
STABILIZED MEDIAN SURFACE		
HOT-MIX ASPHALT SURFACE COURSE, MIX ''D'', N50 (IL 9.5 mm), 2''	4% @ 50 GYR	QC/QA
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 10''	4% @ 50 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE(QA/QC); QUALITY CONTROL FOR PERFORMANCE(QCP); PAY FOR PERFORMANCE(PFP)		

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

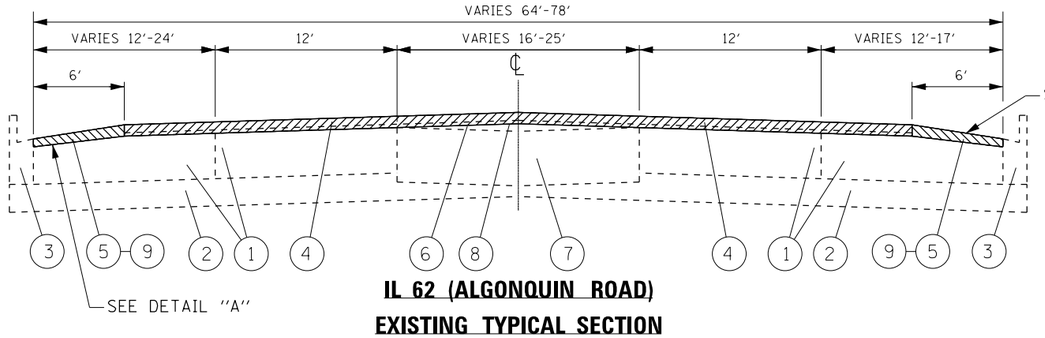
THE ''AC TYPE'' FOR POLYMERIZED HMA MIXES SHALL BE ''SBS/SBR PG 76-22'' AND FOR NON-POLYMERIZED HMA THE ''AC TYPE'' SHALL BE ''PG 64-22'' UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

LEGEND

1. EXISTING PCC PAVEMENT, 10'' (±)
2. EXISTING SUB-BASE GRANULAR MATERIALS, 6'' (±)
3. EXISTING COMBINATION CONCRETE CURB & GUTTER
4. EXISTING HMA SURFACE, 1 ¾''±
5. EXISTING HMA SURFACE, VARIABLE 1 ¾'' - 2 ½''±
6. EXISTING HMA LEVELING BINDER, 3/4''± AND VARIES
7. EXISTING STABILIZED MEDIAN SURFACE, 12''± / PC CONCRETE MEDIAN /PCC CORRUGATED MEDIAN
8. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 ½''
9. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH, 1 ¾'' - 2 ½''
10. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, N80, 1 ¾''
11. PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾''



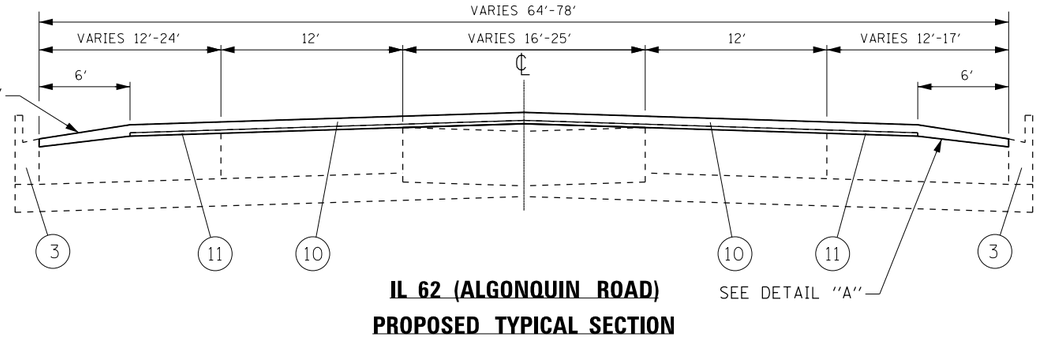
DETAIL "A"



IL 62 (ALGONQUIN ROAD)

EXISTING TYPICAL SECTION

STA. 50+78 TO STA. 82+90
STA. 84+00 TO STA. 93+19
STA. 96+30 TO STA. 103+64
STA. 104+72 TO STA. 115+65
STA. 136+35 TO STA. 146+14.5
STA. 195+71.4 TO STA. 201+19.5
STA. 205+65.3 TO STA. 207+05
STA. 208+02 TO STA. 241+00
STA. 245+13 TO STA. 252+66



IL 62 (ALGONQUIN ROAD)

PROPOSED TYPICAL SECTION

STA. 50+78 TO STA. 82+90
STA. 84+00 TO STA. 93+19 ← SEE DETAIL ''B''
STA. 96+30 TO STA. 103+64 ← SEE DETAIL ''B''
STA. 104+72 TO STA. 115+65
STA. 136+35 TO STA. 146+14.5
STA. 195+71.4 TO STA. 201+19.5
STA. 205+65.3 TO STA. 207+05
STA. 208+02 TO STA. 241+00
STA. 245+13 TO STA. 252+66

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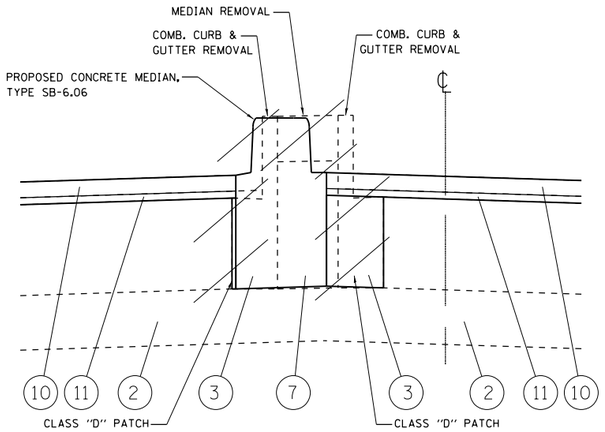
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS
IL 62 (ALGONQUIN ROAD) – E/O ROSELLE ROAD TO IL 58(GOLF ROAD)

SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	6
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

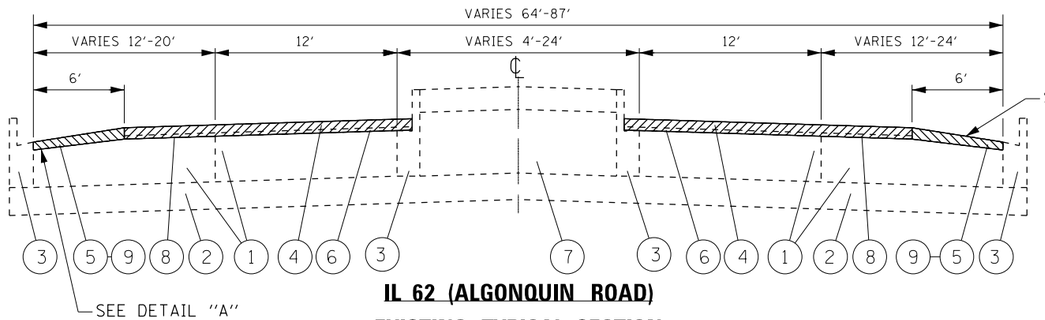
NOTE:
EXISTING MEDIAN TO BE REMOVED IS BETWEEN STA. 93+19 AND STA. 96+30. PROPOSED CONCRETE MEDIAN TO BE PLACED BETWEEN STA. 91+85 AND STA. 97+47. ANY VOIDS BETWEEN EXISTING MEDIAN REMOVAL AND PROPOSED MEDIAN PLACEMENT SHALL BE FILLED WITH CLASS "D" PATCH.



DETAIL "B"

LEGEND

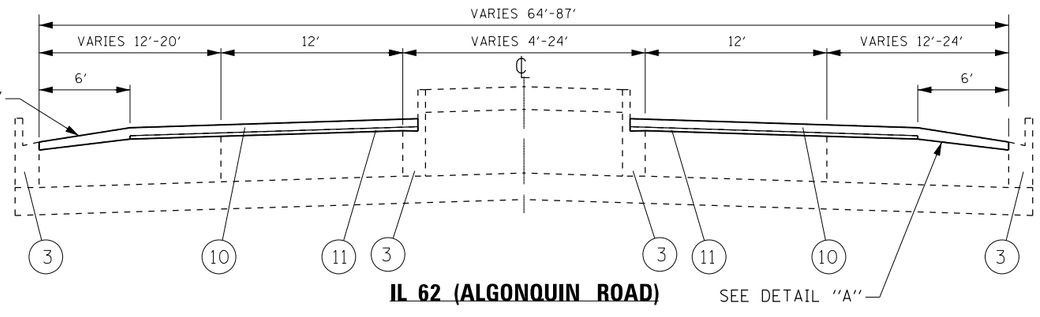
1. EXISTING PCC PAVEMENT, 10" (±)
2. EXISTING SUB-BASE GRANULAR MATERIALS, 6" (±)
3. EXISTING COMBINATION CONCRETE CURB & GUTTER
4. EXISTING HMA SURFACE, 1 ¾"±
5. EXISTING HMA SURFACE, VARIABLE 1 ¾" - 2 ½"±
6. EXISTING HMA LEVELING BINDER, 3/4"± AND VARIES
7. EXISTING STABILIZED MEDIAN SURFACE, 12"± / PC CONCRETE MEDIAN /PCC CORRUGATED MEDIAN
8. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 ½"
9. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH, 1 ¾" - 2 ½"
10. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, N80, 1 ¾"
11. PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"



IL 62 (ALGONQUIN ROAD)

EXISTING TYPICAL SECTION

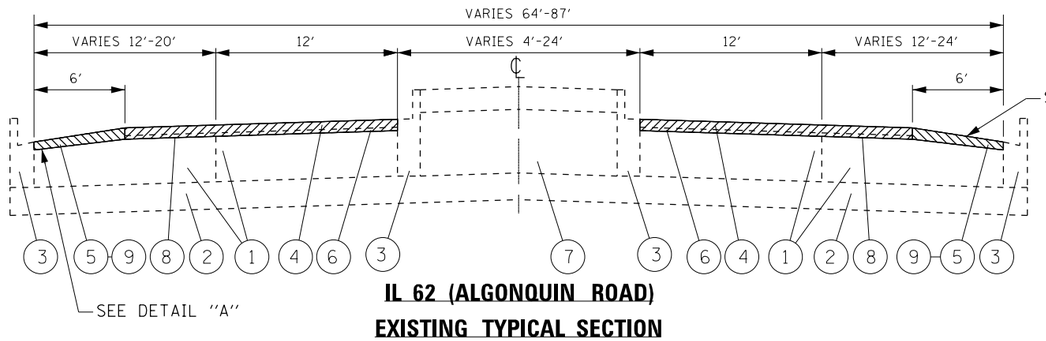
STA. 82+90 TO STA. 84+00
STA. 93+19 TO STA. 96+30
STA. 103+64 TO STA. 104+72
STA. 134+75 TO STA. 136+35
STA. 241+00 TO STA. 245+13
STA. 252+66 TO STA. 253+94
EB STA. 253+94 TO STA. 257+92
STA. 257+92 TO STA. 261+82



IL 62 (ALGONQUIN ROAD)

EXISTING TYPICAL SECTION

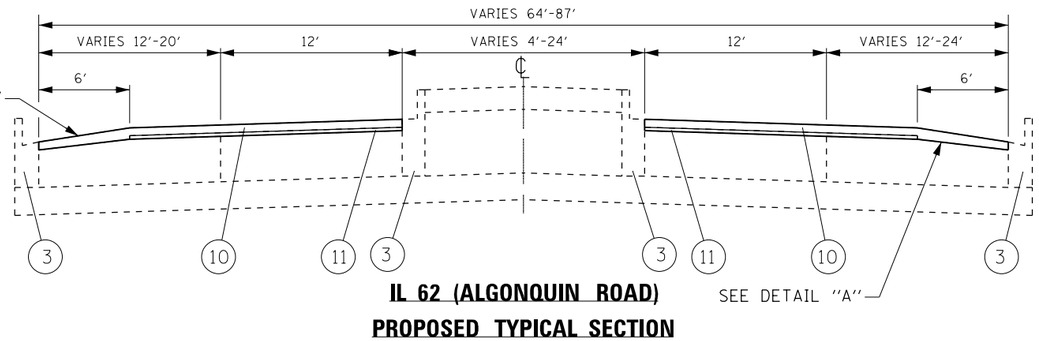
STA. 82+90 TO STA. 84+00
STA. 93+19 TO STA. 96+30 ← SEE DETAIL "B"
STA. 103+64 TO STA. 104+72
STA. 134+75 TO STA. 136+35
STA. 241+00 TO STA. 245+13
STA. 252+66 TO STA. 253+94
EB STA. 253+94 TO STA. 257+92
STA. 257+92 TO STA. 261+82



IL 62 (ALGONQUIN ROAD)

EXISTING TYPICAL SECTION

STA. 128+65 TO STA. 134+75
STA. 207+05 TO STA. 208+02
WB STA. 253+94 TO STA. 257+92
STA. 261+82 TO STA. 276+84.5



IL 62 (ALGONQUIN ROAD)

PROPOSED TYPICAL SECTION

STA. 128+65 TO STA. 134+75
STA. 207+05 TO STA. 208+02
WB STA. 253+94 TO STA. 257+92
STA. 261+82 TO STA. 276+84.5

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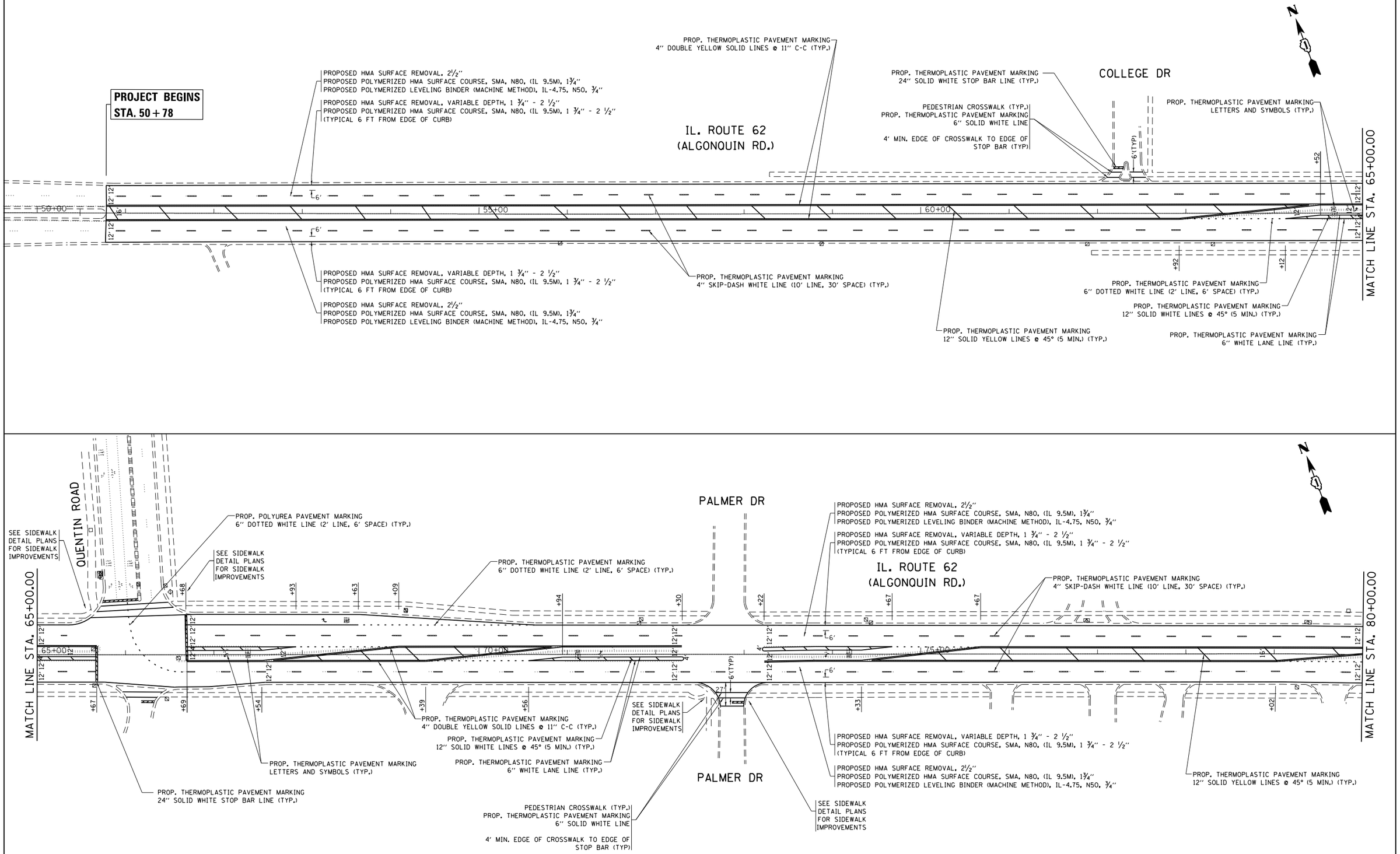
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS
IL 62 (ALGONQUIN ROAD) – E/O ROSELLE ROAD TO IL 58(GOLF ROAD)

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	7
		CONTRACT NO. 62C93		
		ILLINOIS	FED. AID PROJECT	



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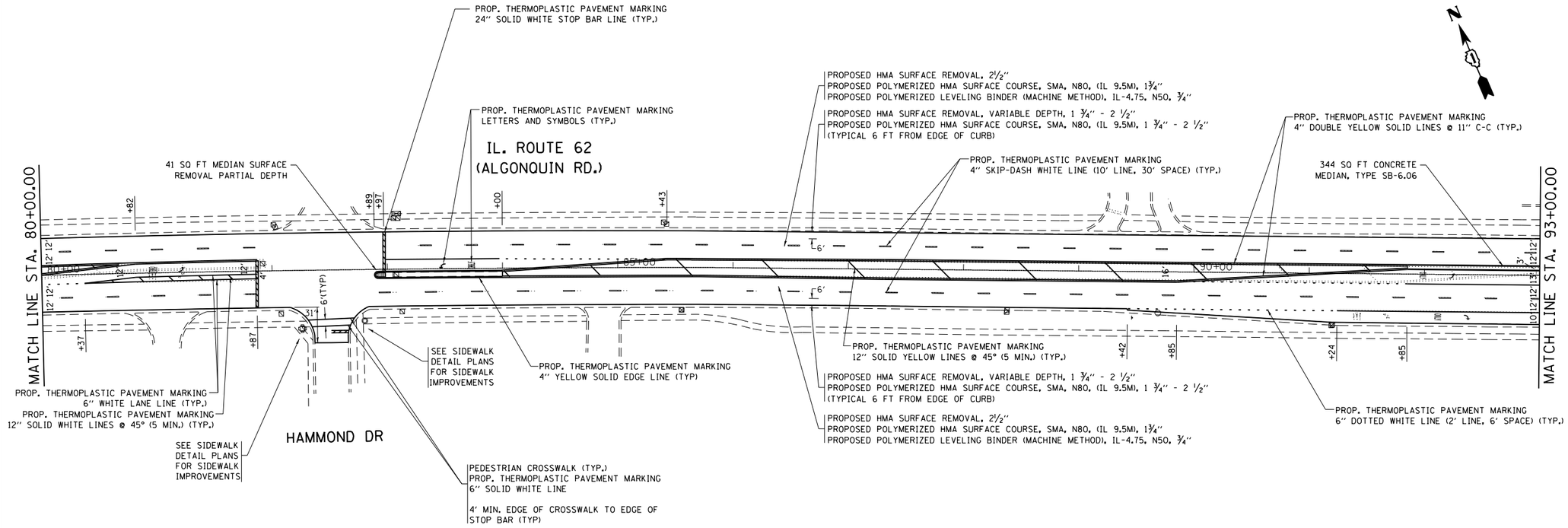
Default	PLOT DATE = 2/4/2017	DATE -	REVISED -

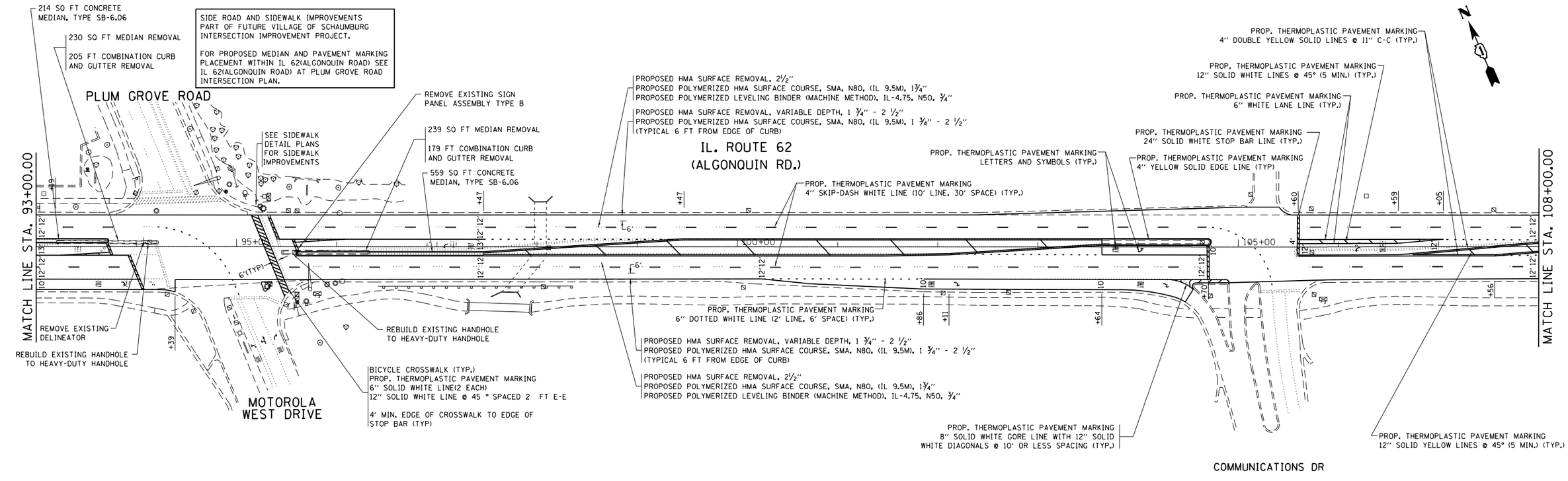
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
IL RTE. 62 (ALGONQUIN RD.) (E/O ROSELLE RD. TO IL RTE. 58 (GOLF RD.)

SCALE: 1"= 50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	8
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		





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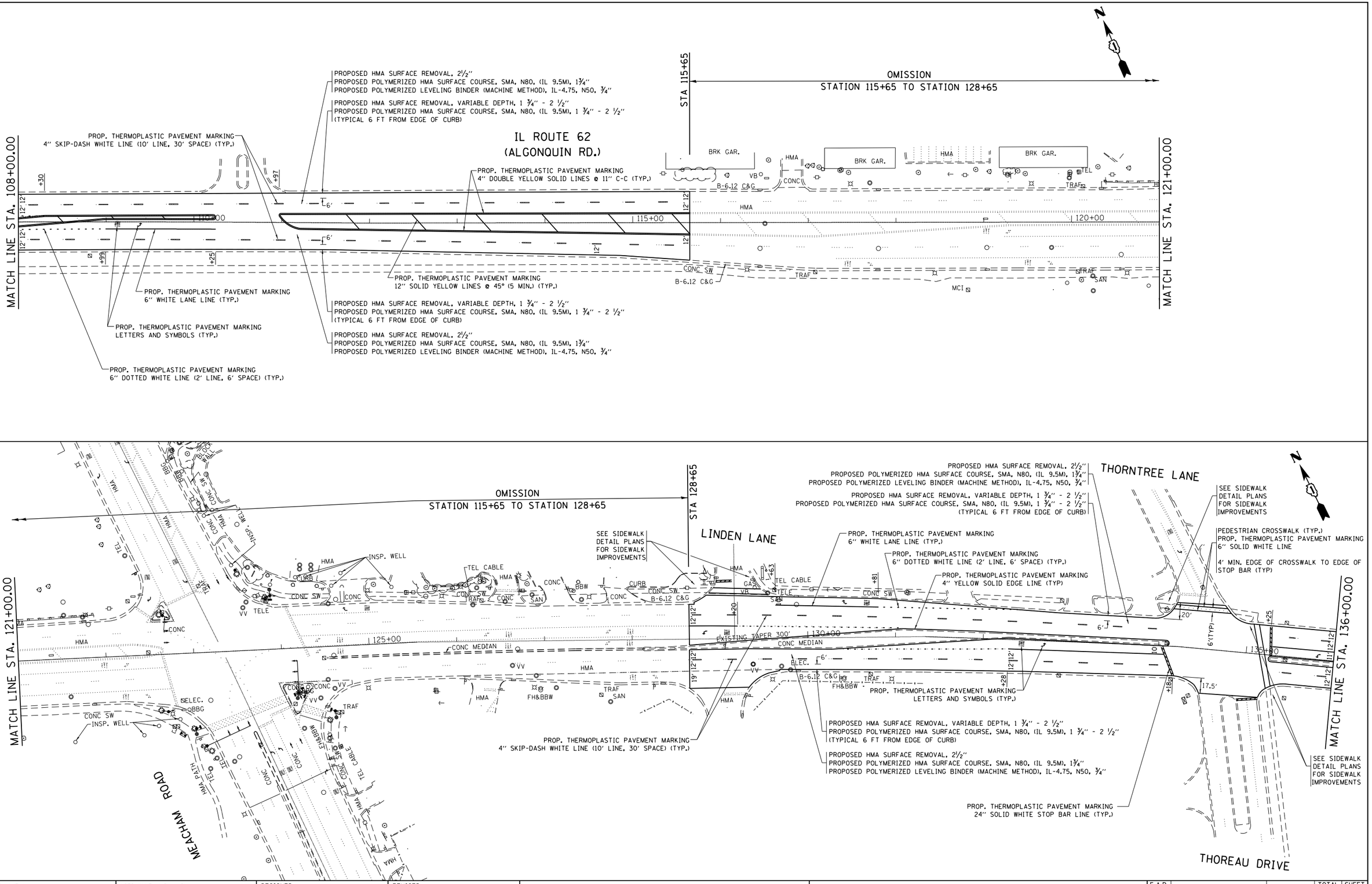
Default		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
IL RTE. 62 (ALGONQUIN RD.) (E/O ROSELLE RD. TO IL RTE. 58 (GOLF RD.)

SCALE: 1"= 50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	9
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		



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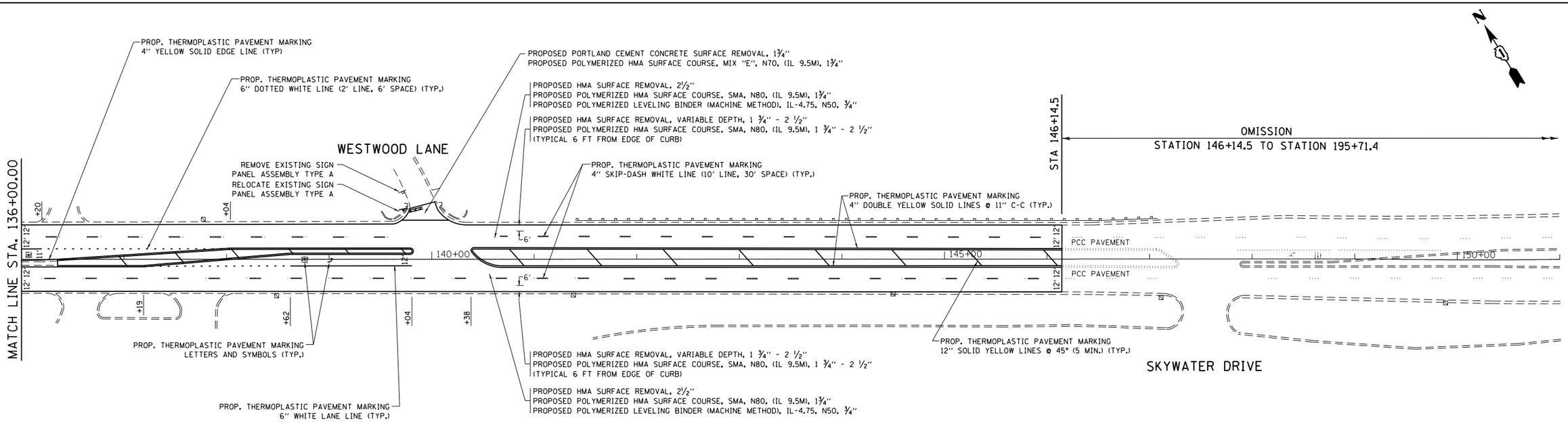
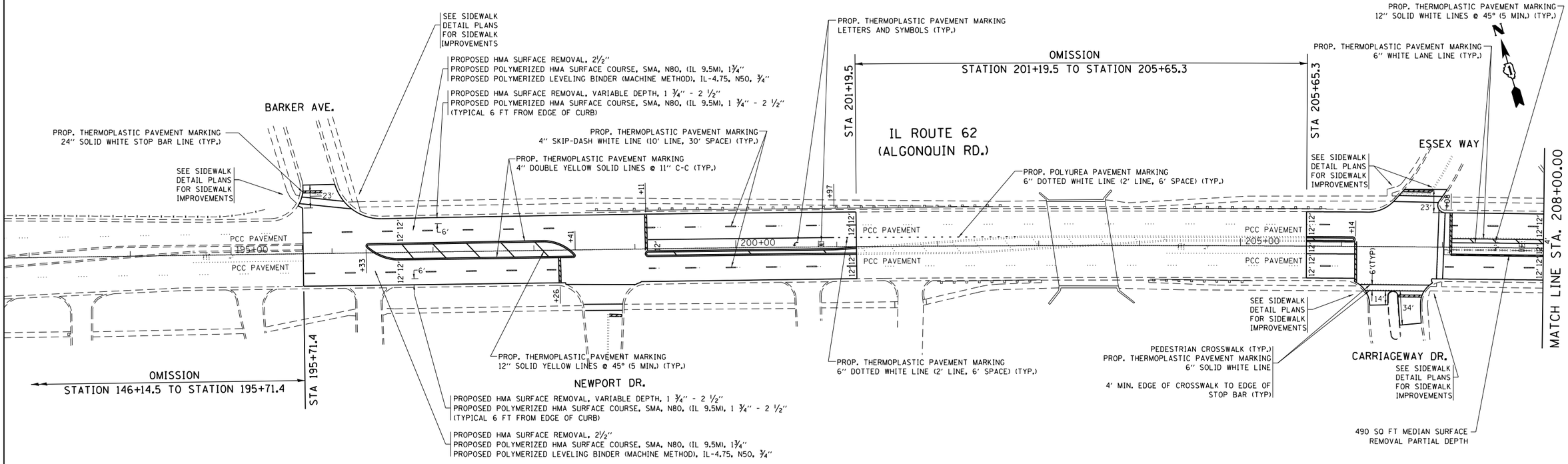
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
IL RTE. 62 (ALGONQUIN RD.) (E/O ROSELLE RD. TO IL RTE. 58 (GOLF RD.)

SCALE: 1"= 50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	10
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		





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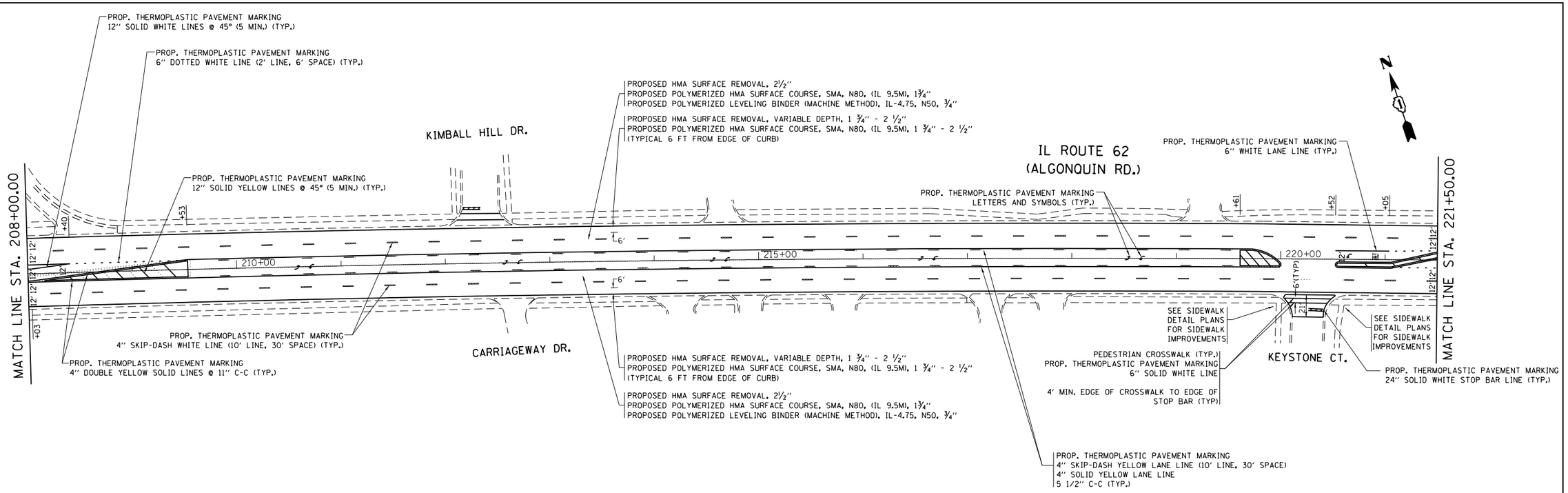
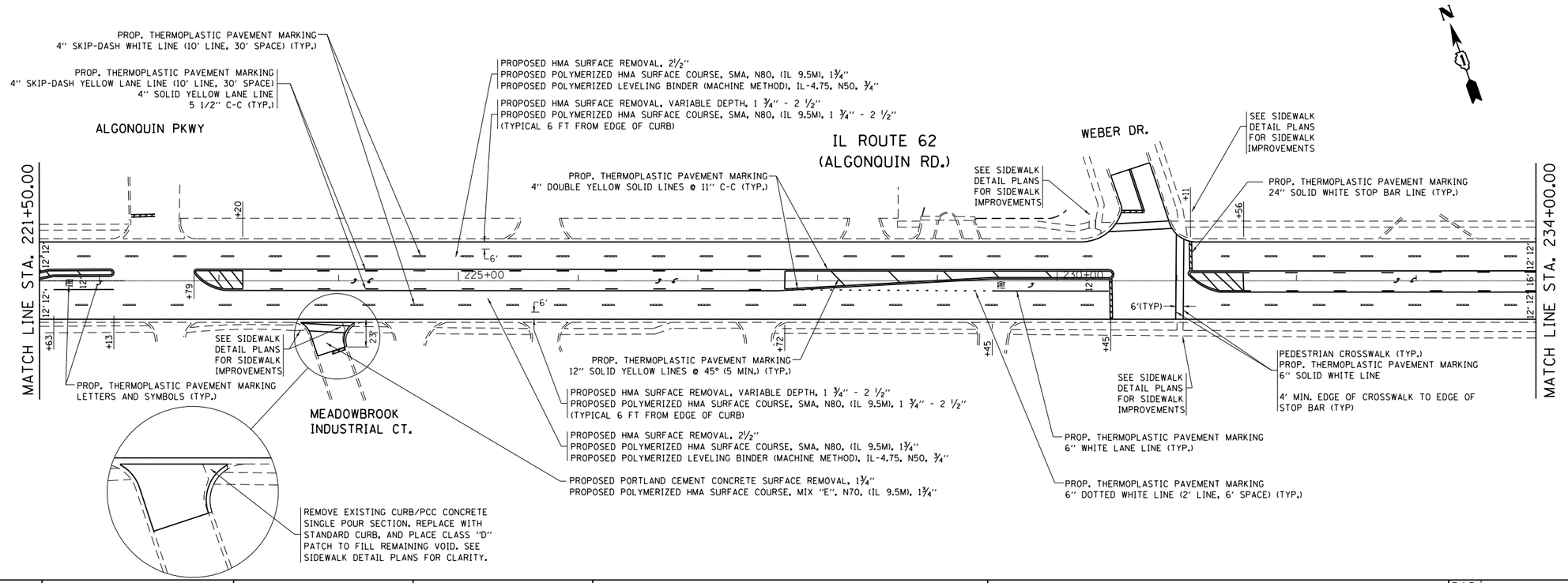
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -		**IL RTE. 62 (ALGONQUIN RD.) (E/O ROSELLE RD. TO IL RTE. 58 (GOLF RD.)**	339	116Y(1&2)R-RS-6	COOK	58	11
Default	PLOT DATE = 2/4/2017	DATE -	REVISED -		SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 62C93		
							ILLINOIS FED. AID PROJECT			





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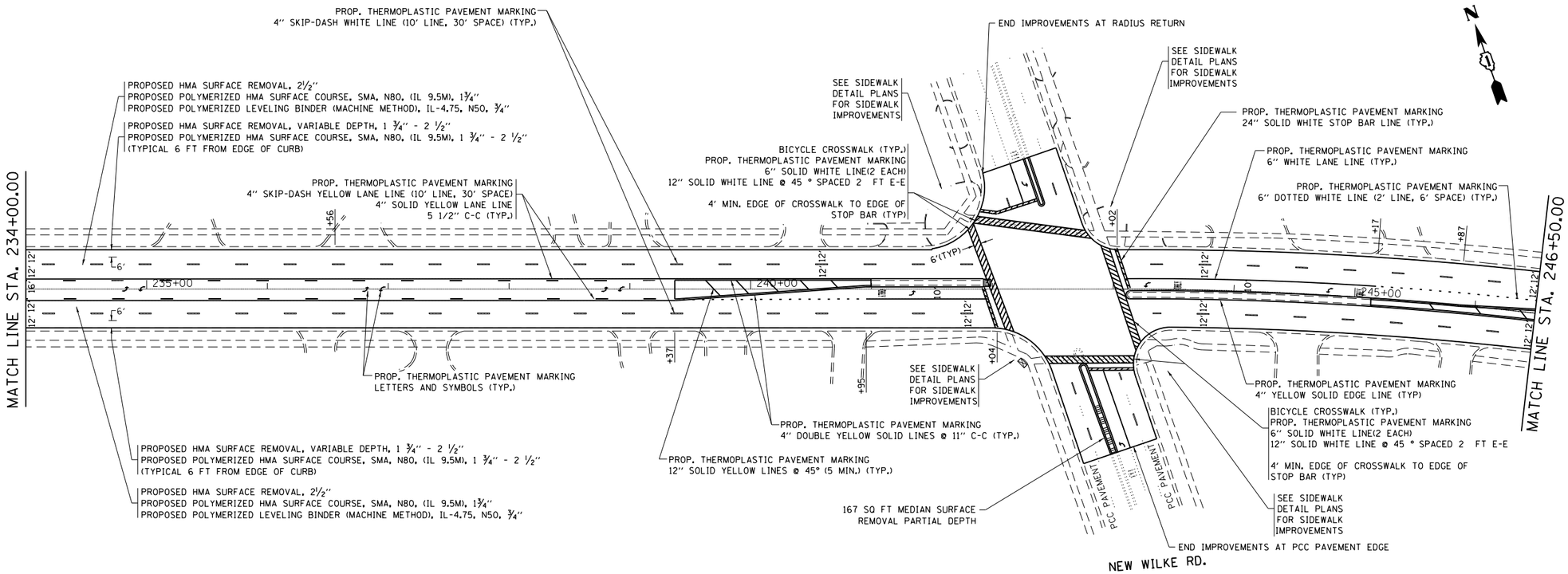
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
IL RTE. 62 (ALGONQUIN RD.) (E/O ROSELLE RD. TO IL RTE. 58 (GOLF RD.)

SCALE: 1"= 50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	12
		CONTRACT NO. 62C93		
		ILLINOIS	FED. AID PROJECT	



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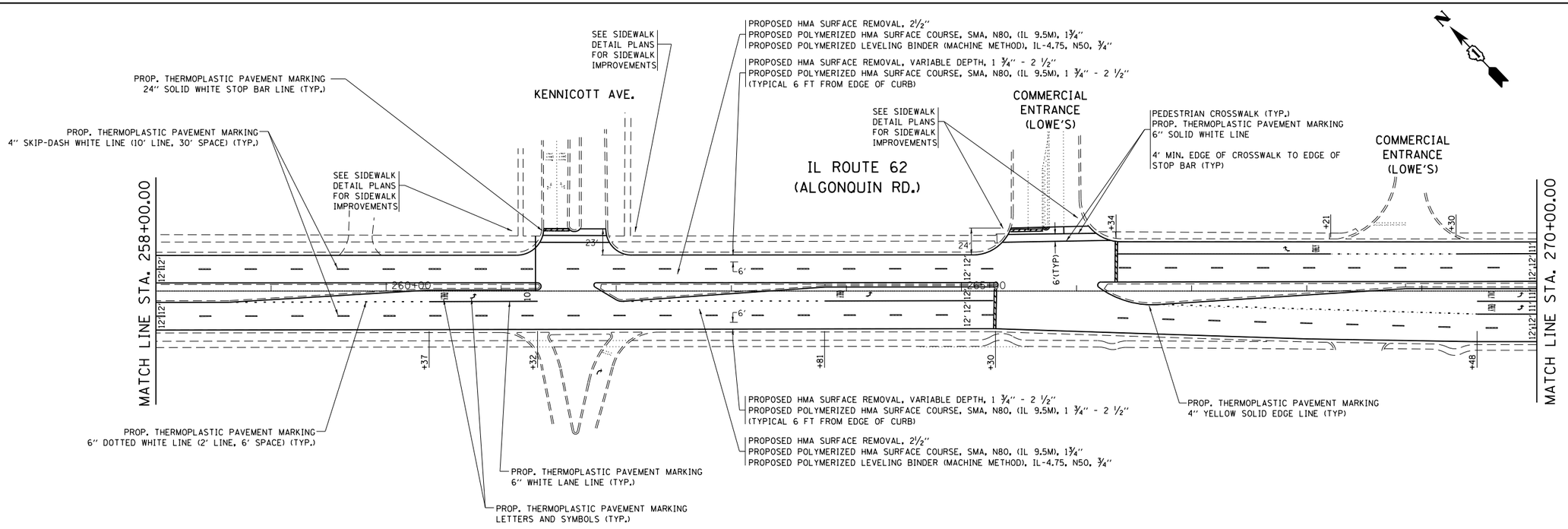
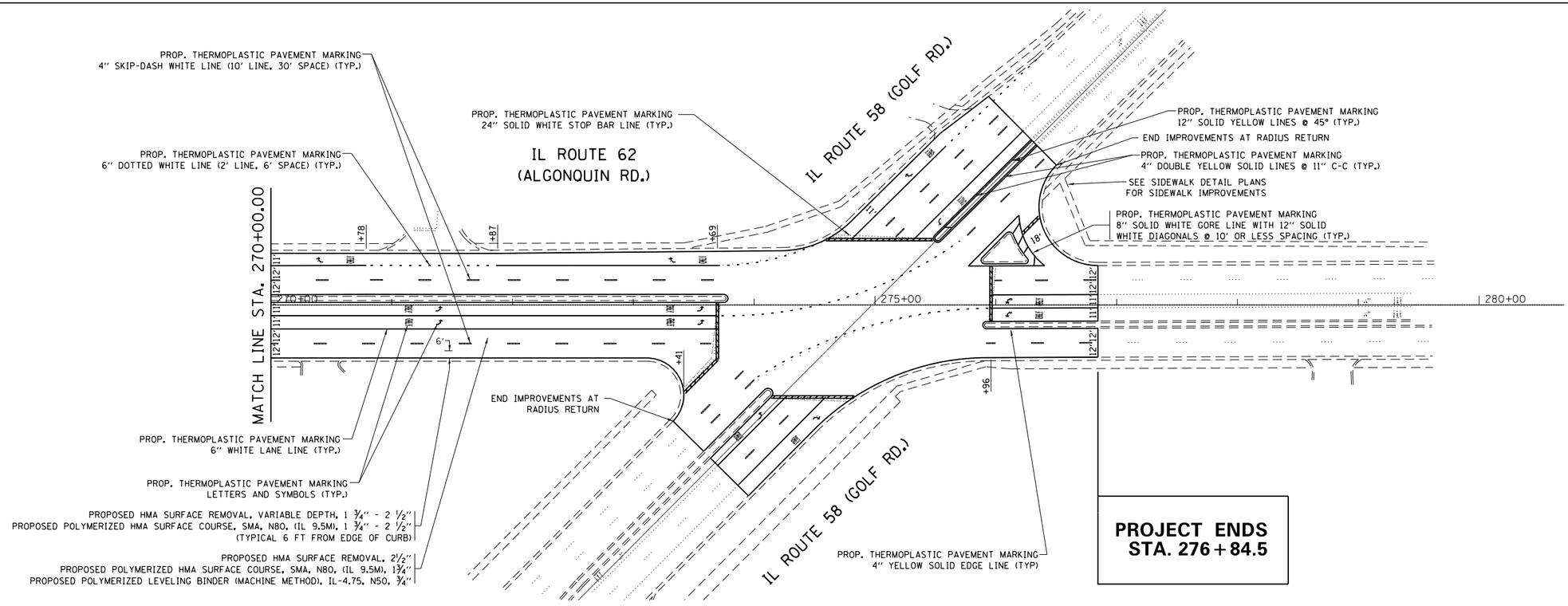
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
IL RTE. 62 (ALGONQUIN RD.) (E/O ROSELLE RD. TO IL RTE. 58 (GOLF RD.)

SCALE: 1"= 50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	56	13
		CONTRACT NO. 62C93		
		ILLINOIS	FED. AID PROJECT	





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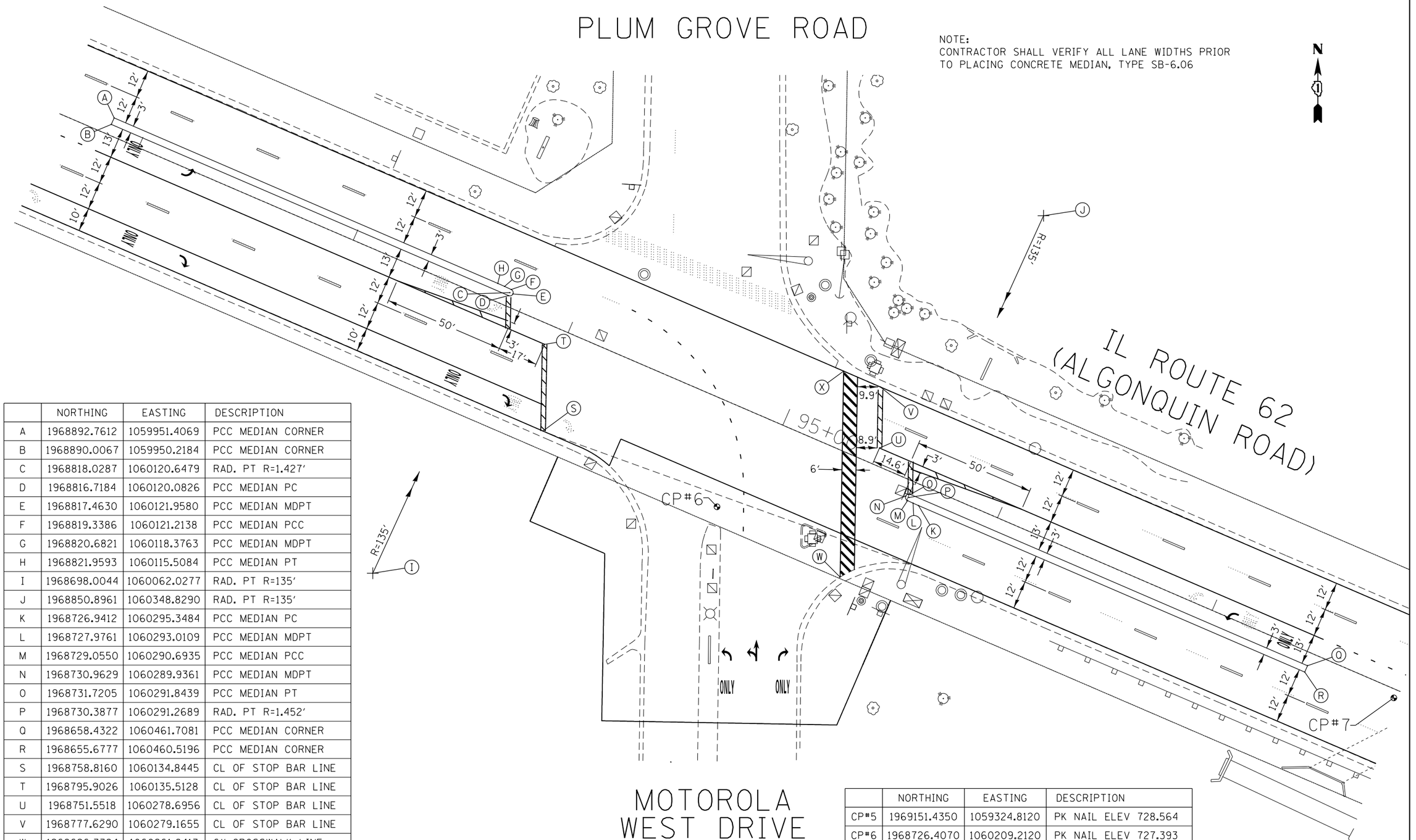
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
IL RTE. 62 (ALGONQUIN RD.) (E/O ROSELLE RD. TO IL RTE. 58 (GOLF RD.)
SCALE: 1"= 50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	14
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		



	NORTHING	EASTING	DESCRIPTION
A	1968892.7612	1059951.4069	PCC MEDIAN CORNER
B	1968890.0067	1059950.2184	PCC MEDIAN CORNER
C	1968818.0287	1060120.6479	RAD. PT R=1.427'
D	1968816.7184	1060120.0826	PCC MEDIAN PC
E	1968817.4630	1060121.9580	PCC MEDIAN MDPT
F	1968819.3386	1060121.2138	PCC MEDIAN PCC
G	1968820.6821	1060118.3763	PCC MEDIAN MDPT
H	1968821.9593	1060115.5084	PCC MEDIAN PT
I	1968698.0044	1060062.0277	RAD. PT R=135'
J	1968850.8961	1060348.8290	RAD. PT R=135'
K	1968726.9412	1060295.3484	PCC MEDIAN PC
L	1968727.9761	1060293.0109	PCC MEDIAN MDPT
M	1968729.0550	1060290.6935	PCC MEDIAN PCC
N	1968730.9629	1060289.9361	PCC MEDIAN MDPT
O	1968731.7205	1060291.8439	PCC MEDIAN PT
P	1968730.3877	1060291.2689	RAD. PT R=1.452'
Q	1968658.4322	1060461.7081	PCC MEDIAN CORNER
R	1968655.6777	1060460.5196	PCC MEDIAN CORNER
S	1968758.8160	1060134.8445	CL OF STOP BAR LINE
T	1968795.9026	1060135.5128	CL OF STOP BAR LINE
U	1968751.5518	1060278.6956	CL OF STOP BAR LINE
V	1968777.6290	1060279.1655	CL OF STOP BAR LINE
W	1968696.3384	1060261.8413	6" CROSSWALK LINE
X	1968784.4405	1060263.3587	6" CROSSWALK LINE

	NORTHING	EASTING	DESCRIPTION
CP#5	1969151.4350	1059324.8120	PK NAIL ELEV 728.564
CP#6	1968726.4070	1060209.2120	PK NAIL ELEV 727.393
CP#7	1968644.6670	1060498.6170	PK NAIL ELEV 724.109
CP#8	1968297.8800	1061144.0970	PK NAIL ELEV 724.055

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 62(ALGONQUIN ROAD) AT PLUM GROVE ROAD INTERSECTION PLAN
IL RTE. 62 (ALGONQUIN RD.) (E/O ROSELLE RD. TO IL RTE. 58 (GOLF RD.)
SCALE: 1"= 20' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	15
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

IL 62 (ALONGQUIN RD) INTERSECTION	EARTH EXCAVATION	TOPSOIL FURNISH AND PLACE, 4"	NITROGEN FERTILIZER NUTRIENT	PHOSPHORUS FERTILIZER NUTRIENT	POTASSIUM FERTILIZER NUTRIENT	SODDING, SALT TOLERANT	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	DETECTABLE WARNINGS	SIDEWALK REMOVAL	CLASS D PATCHES, TYPE II, 12 INCH	MANHOLES TO BE ADJUSTED	REMOVE SIGN PANEL ASSEMBLY - TYPE A	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	REBUILD EXISTING HANDHOLE	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
	CU YD	SQ YD	POUND	POUND	POUND	SQ YD	SQ FT	SQ FT	SQ FT	SQ YD	EACH	EACH	EACH	EACH	FOOT
	20200100	21101615	25000400	25000500	25000600	25200110	42400200	42400800	44000600	44201789	60255500	72400100	72400500	89502376	Z0004562
QUENTIN RD	6.3	43.8	0.9	0.9	0.9	43.8	721.0	34.0	638.0	0.0	0.0	0.0	0.0	0.0	36.0
PALMER DR	3.0	34.0	0.8	0.8	0.8	34.0	331.0	20.0	397.0	0.0	0.0	0.0	0.0	1.0	35.0
HAMMOND DR	7.8	36.1	0.8	0.8	0.8	36.1	540.0	28.0	456.0	0.0	1.0	0.0	0.0	0.0	42.0
PLUM GROVE RD / WEST DR	1.4	14.3	0.4	0.4	0.4	14.3	165.0	22.0	180.0	0.0	0.0	1.0	1.0	0.0	33.0
LINDEN LN	3.0	18.6	0.5	0.5	0.5	18.6	410.0	48.0	374.0	0.0	1.0	0.0	0.0	0.0	51.0
THORNTREE LN / THOREAU DR	3.1	29.5	0.8	0.8	0.8	29.5	354.0	47.0	329.0	0.0	0.0	0.0	0.0	1.0	53.0
BARKER AVE	4.8	33.9	0.7	0.7	0.7	33.9	434.0	30.0	393.0	0.0	0.0	0.0	0.0	0.0	28.0
CARRIAGE WAY DR/ ESSEX WAY	2.2	25.3	0.7	0.7	0.7	25.3	495.0	84.0	462.0	0.0	0.0	0.0	0.0	1.0	91.0
KEYSTONE CT	1.7	29.2	0.6	0.6	0.6	29.2	392.0	33.0	358.0	0.0	1.0	0.0	0.0	0.0	39.0
MEADOWBROOK CT	1.2	11.9	0.3	0.3	0.3	11.9	190.0	29.0	190.0	10.0	0.0	0.0	0.0	0.0	30.0
WEBER DR	2.8	32.2	0.7	0.7	0.7	32.2	483.0	40.0	469.0	0.0	0.0	0.0	0.0	2.0	57.0
NEW WILKE RD	7.2	60.7	1.3	1.3	1.3	60.7	1111.0	110.0	1042.0	0.0	2.0	0.0	0.0	3.0	100.0
MERIDIAN PARKING	1.1	13.8	0.3	0.3	0.3	13.8	189.0	39.0	210.0	0.0	0.0	0.0	0.0	0.0	42.0
KENNICOTT AVE	4.3	38.4	0.8	0.8	0.8	38.4	515.0	20.0	456.0	0.0	0.0	0.0	0.0	0.0	33.0
LOWES PARKING NORTH	1.1	13.0	0.4	0.4	0.4	13.0	213.0	38.0	183.0	0.0	0.0	0.0	0.0	0.0	39.0
LOWES PARKING SOUTH	1.2	23.3	0.5	0.5	0.5	23.3	360.0	24.0	359.0	0.0	0.0	0.0	0.0	0.0	99.0
GOLF RD	0.6	15.2	0.3	0.3	0.3	15.2	184.0	20.0	184.0	0.0	0.0	0.0	0.0	0.0	15.0
TOTAL	52.8	473.2	10.8	10.8	10.8	473.2	7087	666	6680	10	5	1	1	8	823

NOTE: RESTORATION (SODDING, NUTRIENTS AND TOPSOIL) LIMITS ARE SHOWN ON THE SITE PLANS WHEN SIDEWALK IS REMOVED AND REPLACED WITH SOD. HOWEVER, FOR RESTORATION AROUND NEW SIDEWALK, QUANTITY HAS BEEN PROVIDED BUT IS NOT SHOWN ON THE SITE PLANS.

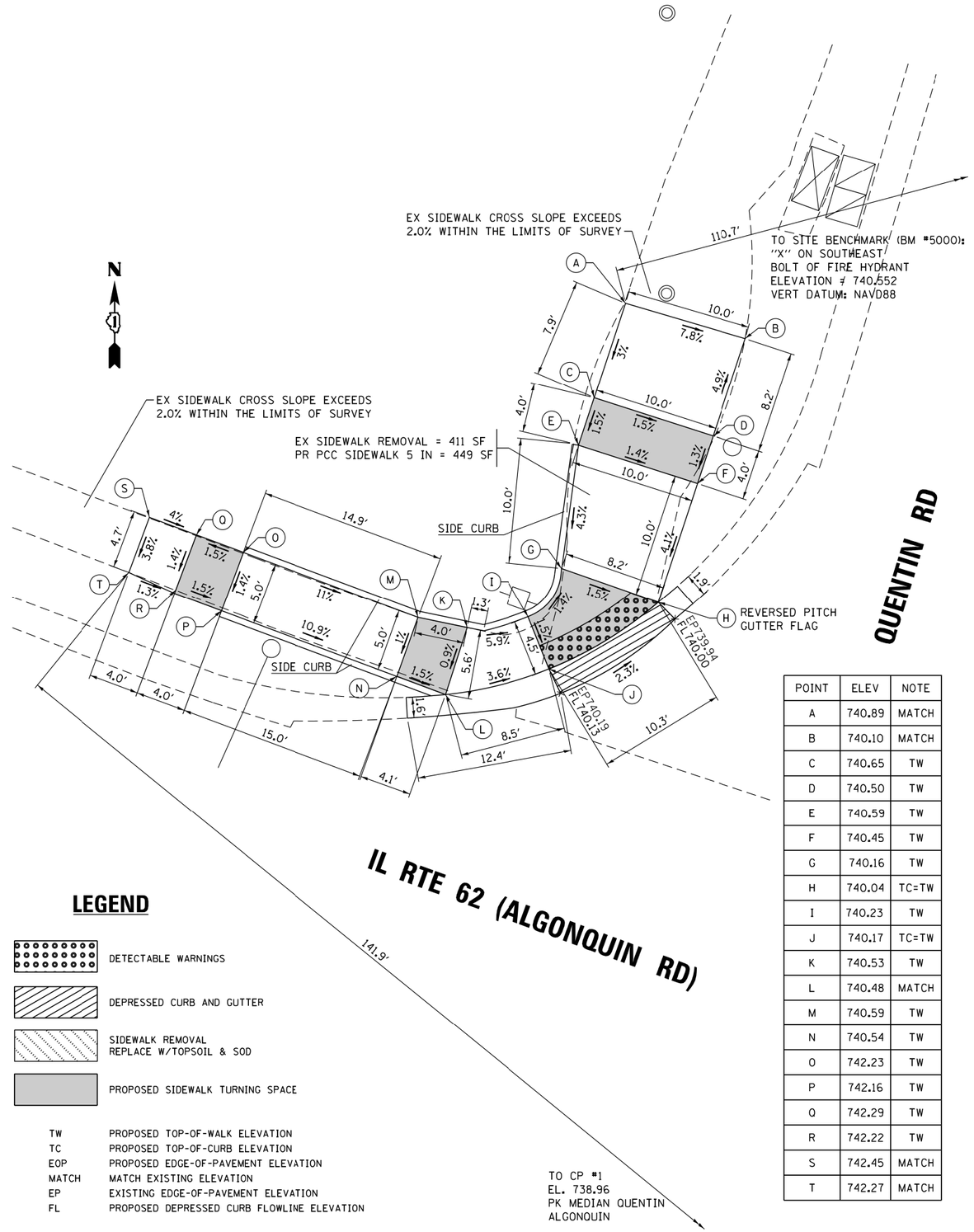
FILE NAME = 62C93-sht-S0001.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 2.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN – SCHEDULE OF QUANTITIES
IL 62 (ALGONQUIN RD.) (QUENTIN RD. TO GOLF RD.)

SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	16
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

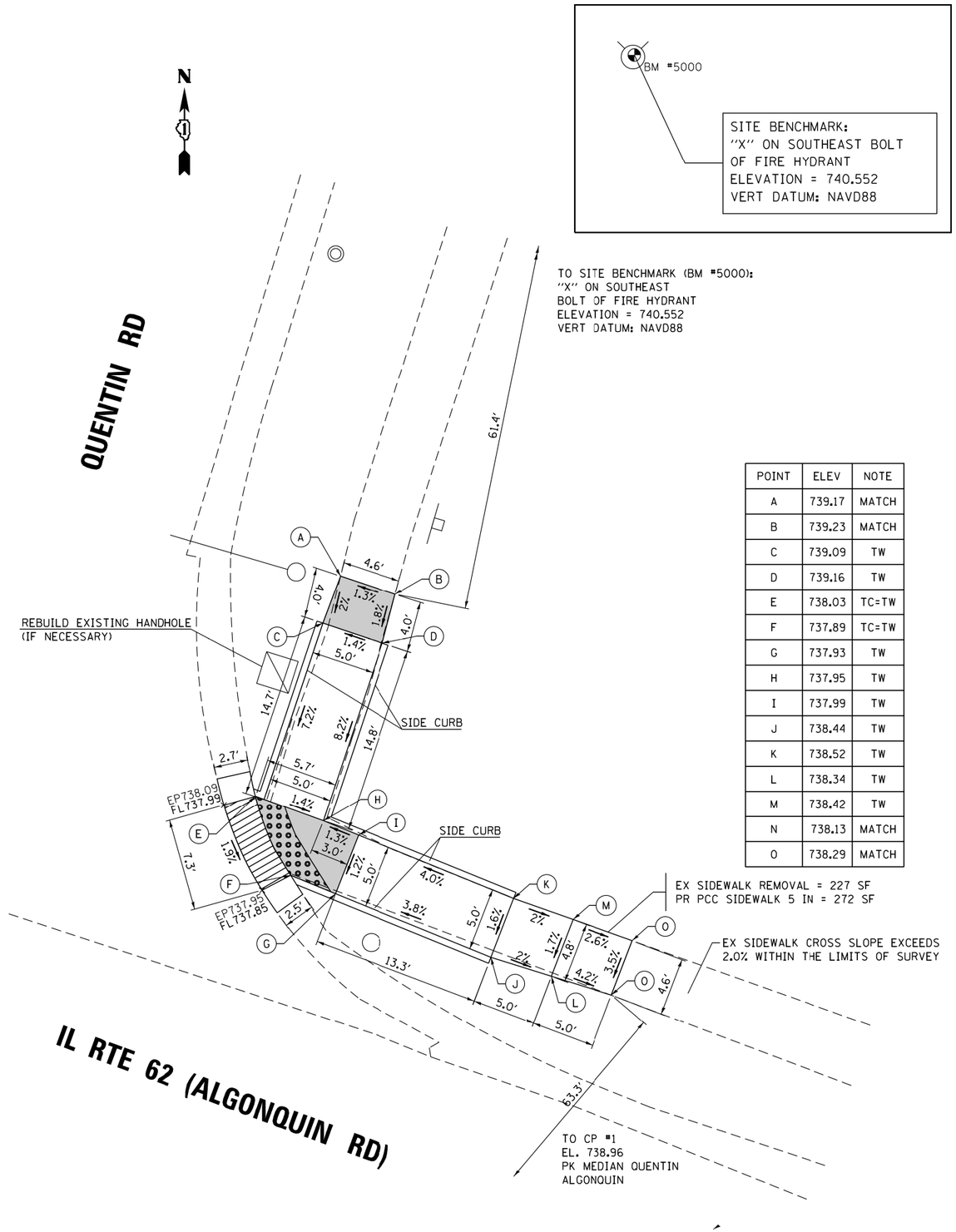


POINT	ELEV	NOTE
A	740.89	MATCH
B	740.10	MATCH
C	740.65	TW
D	740.50	TW
E	740.59	TW
F	740.45	TW
G	740.16	TW
H	740.04	TC=TW
I	740.23	TW
J	740.17	TC=TW
K	740.53	TW
L	740.48	MATCH
M	740.59	TW
N	740.54	TW
O	742.23	TW
P	742.16	TW
Q	742.29	TW
R	742.22	TW
S	742.45	MATCH
T	742.27	MATCH

LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL
  REPLACE W/TOPSOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION



POINT	ELEV	NOTE
A	739.17	MATCH
B	739.23	MATCH
C	739.09	TW
D	739.16	TW
E	738.03	TC=TW
F	737.89	TC=TW
G	737.93	TW
H	737.95	TW
I	737.99	TW
J	738.44	TW
K	738.52	TW
L	738.34	TW
M	738.42	TW
N	738.13	MATCH
O	738.29	MATCH

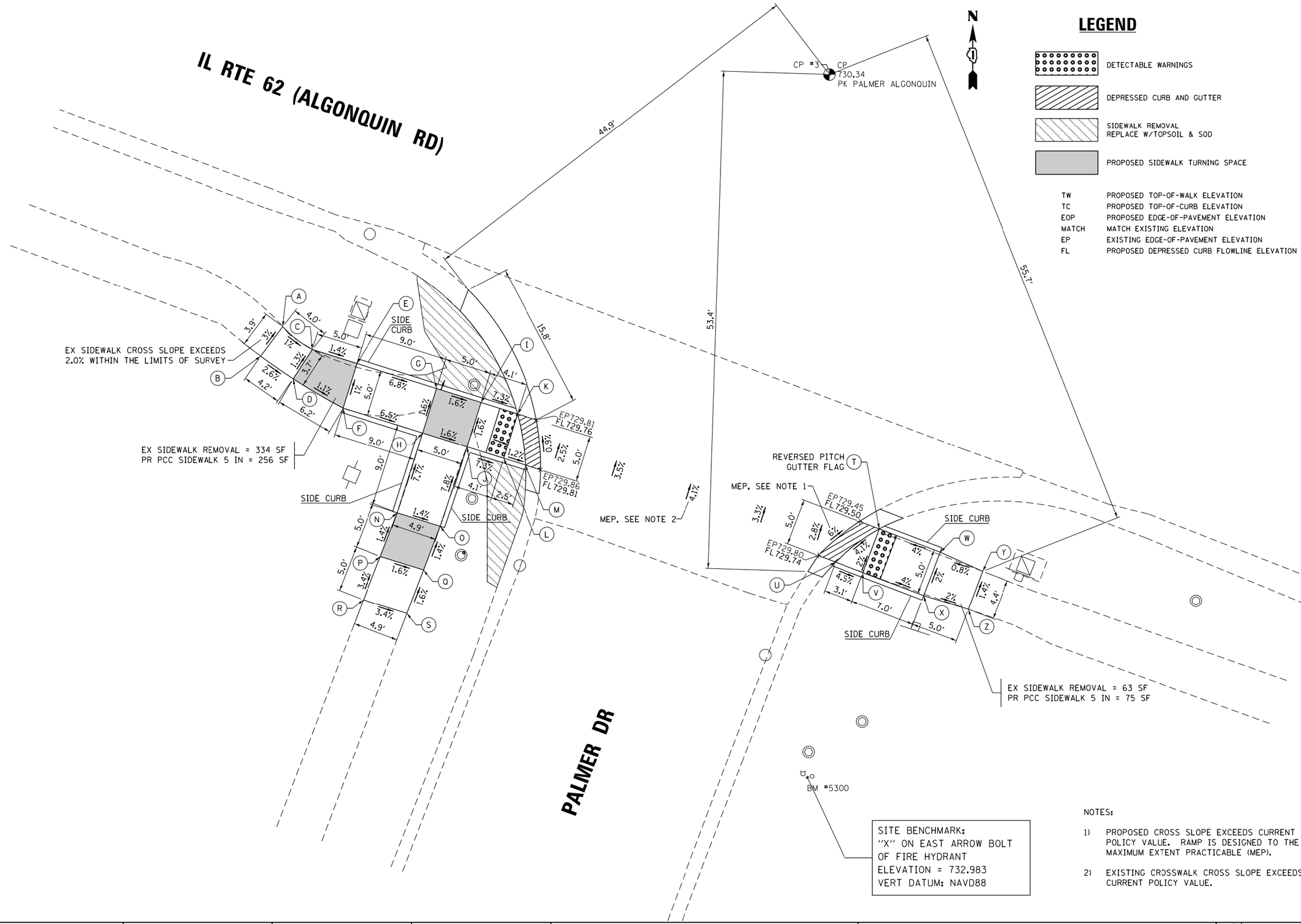
FILE NAME = 62C93-sht-plan01.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT QUENTIN ROAD

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	17
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			



LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

POINT	ELEV	NOTE
A	730.90	MATCH
B	731.02	MATCH
C	730.86	TW
D	730.91	TW
E	730.79	TW
F	730.84	TW
G	730.18	TW
H	730.26	TW
I	730.10	TW
J	730.18	TW
K	729.80	TC=TW
L	729.88	TW
M	729.85	TC=TW
N	730.95	TW
O	730.88	TW
P	731.03	TW
Q	730.95	TW
R	731.20	MATCH
S	731.03	MATCH
T	729.54	TC=TW
U	729.78	TC=TW
V	729.64	TW
W	729.82	TW
X	729.92	TW
Y	729.86	MATCH
Z	730.02	MATCH

NOTES:

1) PROPOSED CROSS SLOPE EXCEEDS CURRENT POLICY VALUE. RAMP IS DESIGNED TO THE MAXIMUM EXTENT PRACTICABLE (MEP).

2) EXISTING CROSSWALK CROSS SLOPE EXCEEDS CURRENT POLICY VALUE.

FILE NAME = 62C93-sht-plan02.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT PALMER DRIVE

SCALE: 1" = 5' | SHEET OF SHEETS | STA. TO STA.

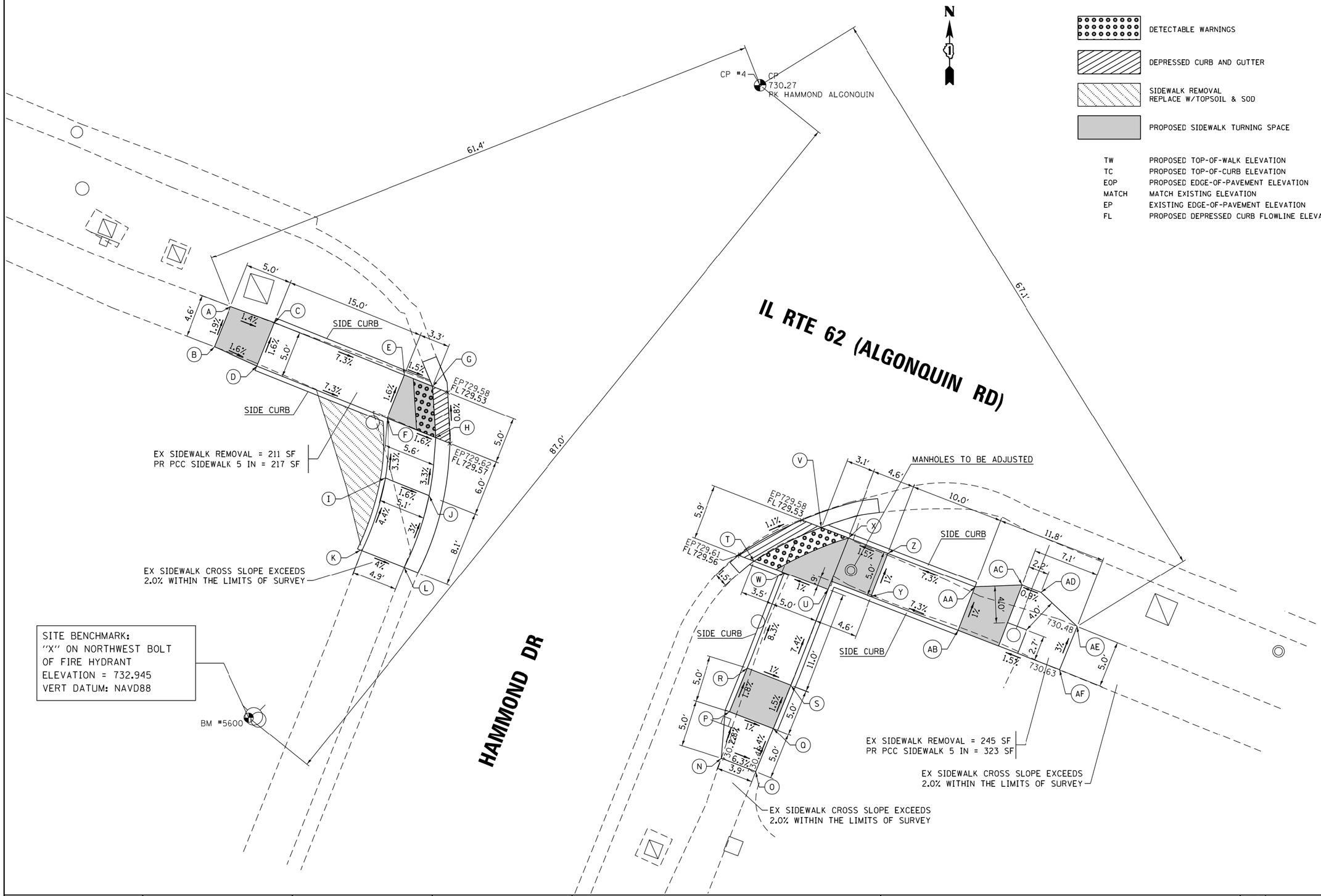
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	18
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE

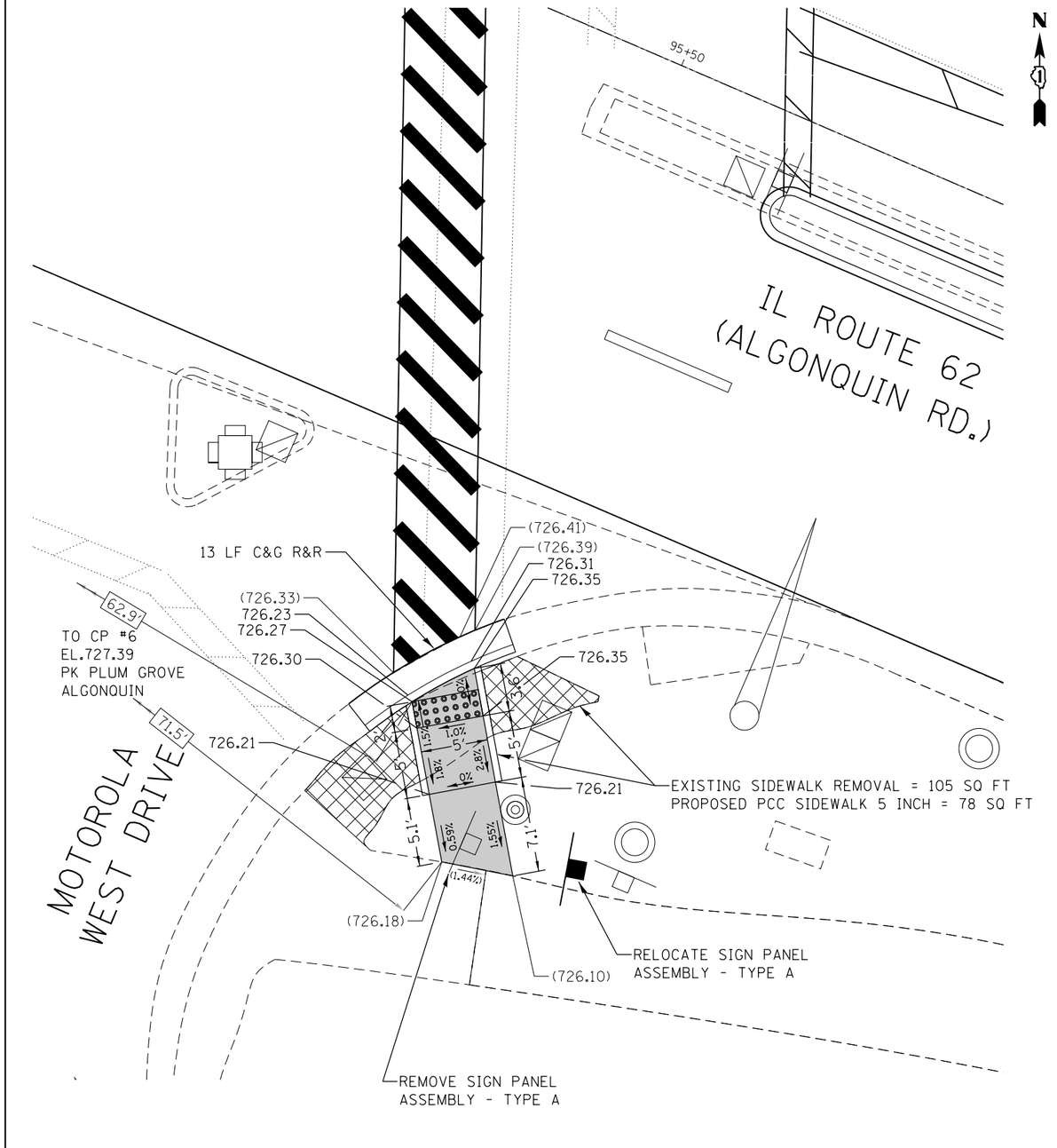
TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

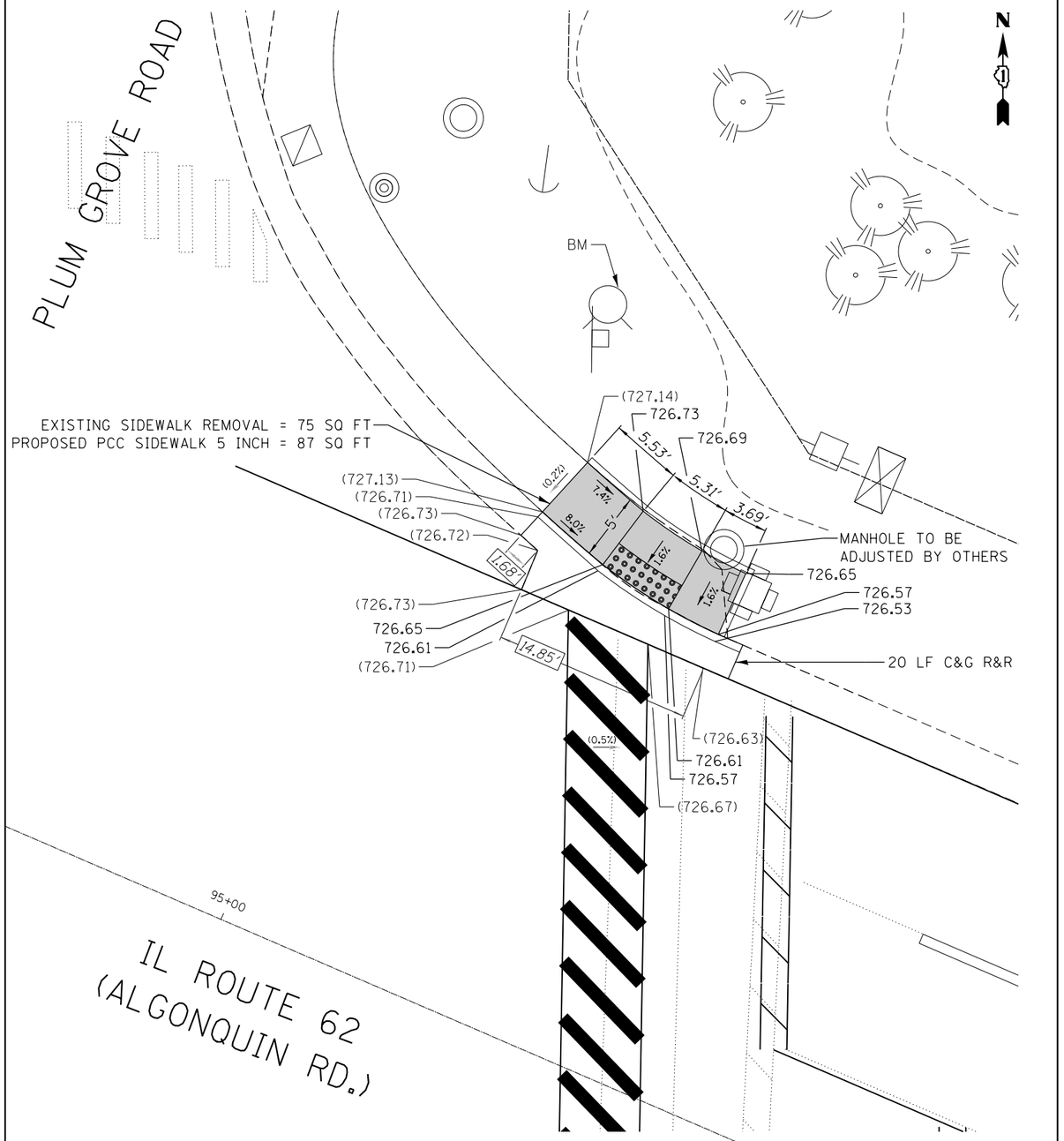
POINT	ELEV	NOTE
A	730.79	MATCH
B	730.88	MATCH
C	730.72	TW
D	730.80	TW
E	729.62	TW
F	729.70	TW
G	729.57	TW=TC
H	729.61	TW
I	729.90	TW
J	729.82	TW
K	730.26	MATCH
L	730.06	MATCH
M	NOT USED	
N	730.78	MATCH
O	730.52	MATCH
P	730.64	TW
Q	730.59	TW
R	730.55	TW
S	730.50	TW
T	729.60	TC=TW
U	729.69	TW
V	729.57	TC=TW
W	729.64	TW
X	729.62	TW
Y	729.74	TW
Z	729.69	TW
AA	730.40	TW
AB	730.45	TW
AC	730.41	TW
AD	730.43	TW
AE	730.48	MATCH
AF	730.63	MATCH



FILE NAME = 62C93-sht-plan03.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**SIDEWALK DETAIL PLAN**
		DRAWN -	REVISED -		**IL ROUTE 62 (ALGONQUIN ROAD) AT HAMMOND DRIVE**
Default	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -		SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.
	PLOT DATE = 10/19/2016	DATE -	REVISED -		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	19
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		





REFERENCE BENCHMARK ELEV 728.327 (NAVD88)

BENCHMARK : "X" CUT ON WEST BOLT OF FIRE HYDRANT

LOCATION : ON SOUTH WEST CORNER OF IL 62(ALGONQUIN RD) AND PLUM GROVE RD

LEGEND

xx.xx'	EXISTING LENGTH
———	PROPOSED SIDE CURB
( )	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 730.123 (NAVD88)

BENCHMARK : "X" CUT ON NORTH BOLT OF FIRE HYDRANT

LOCATION : ON NORTH EAST CORNER OF IL 62(ALGONQUIN RD) AND PLUM GROVE RD

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\D141116\Data\Design\D141116-sht-details.dgn	USER NAME = PencePL	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 1/31/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT PLUM GROVE ROAD / WEST DRIVE

SCALE: 1"=5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	20
		CONTRACT NO. 62C93		
	ILLINOIS	FED. AID PROJECT		

SITE BENCHMARK:
SPIKE IN NORTH FACE OF
POWER POLE
ELEVATION = 741.013
VERT DATUM: NAVD88

TO CP #10
EL. 739.60
PK N SIDE OF
WENDYS IN XHATCH

184.9'

BM #6600

EX SIDEWALK CROSS SLOPE EXCEEDS
2.0% WITHIN THE LIMITS OF SURVEY

SIDE CURB

EX SIDEWALK REMOVAL = 125 SF
PR PCC SIDEWALK 5 IN = 146 SF

EP738.48
FL738.43

EP738.39
FL738.34

TO CP #10
EL. 739.60
PK N SIDE OF
WENDYS IN XHATCH

180.8'

LINDEN LN

TO CP #10
EL. 739.60
PK N SIDE OF
WENDYS IN XHATCH

172.5'

N

LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

POINT	ELEV	NOTE
A	739.83	MATCH
B	739.51	MATCH
C	739.76	TW
D	739.67	TW
E	739.70	TW
F	739.61	TW
G	738.48	TW
H	738.38	TW=TC
I	738.47	TW=TC
L	739.16	MATCH
M	739.24	MATCH
N	739.14	TW
O	739.19	TW
P	738.75	TC=TW
Q	738.79	TW
R	738.75	TC=TW
S	738.79	TW
T	738.55	TC=TW
U	738.66	TW
V	738.89	MATCH
W	739.00	TW
X	738.93	MATCH
Y	739.04	TW
Z	738.92	MATCH
AA	739.14	MATCH

EX SIDEWALK REMOVAL = 249 SF
PR PCC SIDEWALK 5 IN = 264 SF

EP738.76
FL696.71

EP738.76
FL738.71

HANDHOLE TO BE ADJUSTED
BY OTHERS (FIBER OPTIC)

SIDE CURB

MANHOLES TO BE ADJUSTED

EP738.56
FL738.51

EX SIDEWALK CROSS SLOPE EXCEEDS
2.0% WITHIN THE LIMITS OF SURVEY

IL RTE 62 (ALGONQUIN RD)

FILE NAME = 62C93-sht-plan07.dgn	USER NAME = DWhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT LINDEN LANE

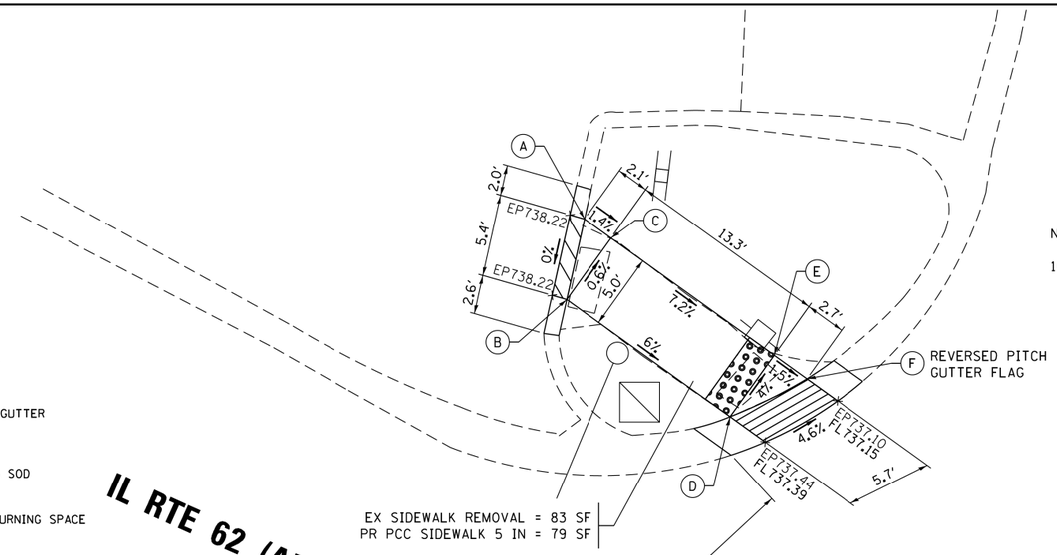
SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	21
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

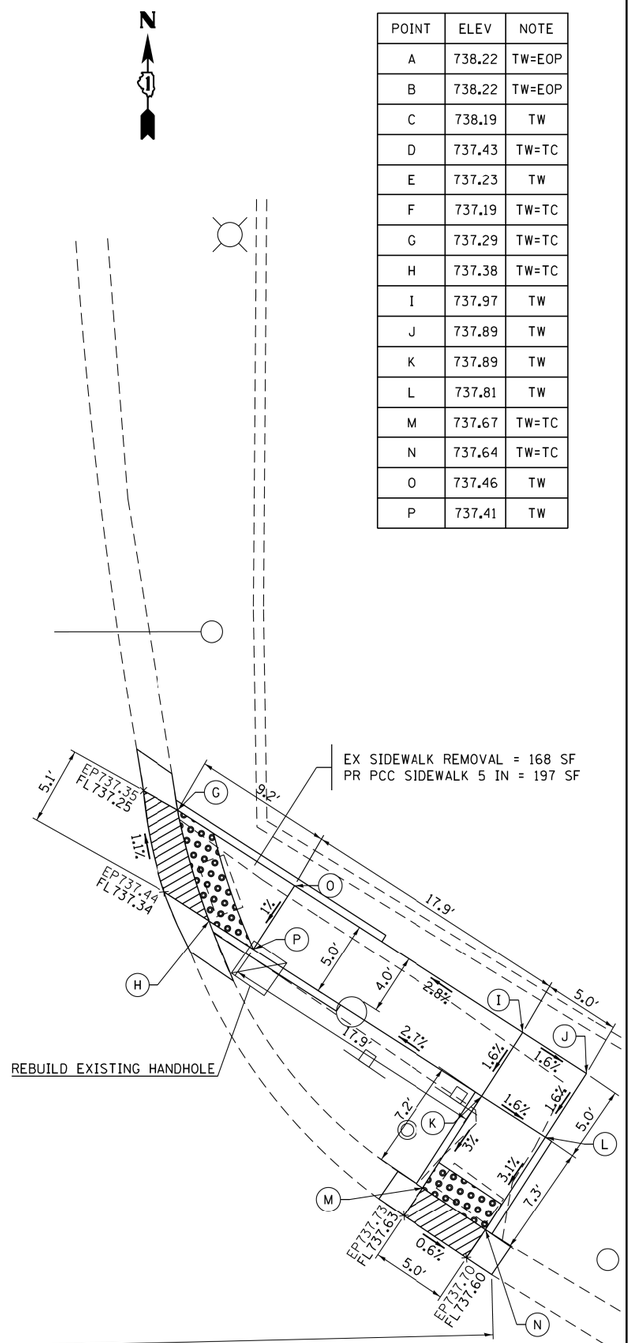


NOTES:

1) PROPOSED CROSS SLOPE EXCEEDS CURRENT POLICY VALUE. RAMP IS DESIGNED TC THE MAXIMUM EXTENT PRACTICABLE (MEP).

IL RTE 62 (ALGONQUIN RD)

THORNTREE LN



POINT	ELEV	NOTE
A	738.22	TW=EOP
B	738.22	TW=EOP
C	738.19	TW
D	737.43	TW=TC
E	737.23	TW
F	737.19	TW=TC
G	737.29	TW=TC
H	737.38	TW=TC
I	737.97	TW
J	737.89	TW
K	737.89	TW
L	737.81	TW
M	737.67	TW=TC
N	737.64	TW=TC
O	737.46	TW
P	737.41	TW

SITE BENCHMARK:
"X" ON SOUTHEAST BOLT
OF FIRE HYDRANT
ELEVATION = 740.806
VERT DATUM: NAVD88

89.4'

95.0'

146.5'

TM #6800

TO CP #12
EL. 737.56
X S SIDE ALGONQUIN
IN CURB AT THOREAU

FILE NAME = 62C93-sht-plan08.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT THORNTREE LANE

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	22
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

LEGEND

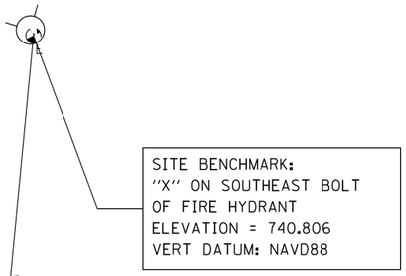
DETECTABLE WARNINGS

DEPRESSED CURB AND GUTTER

SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION



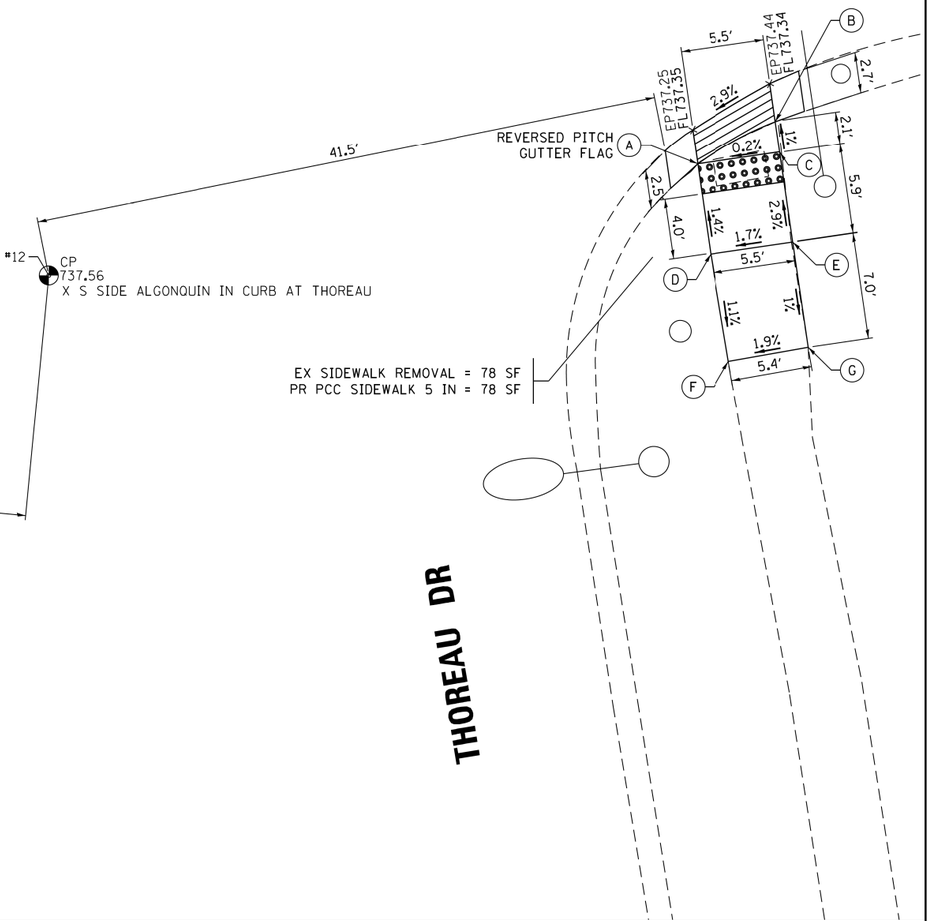
SITE BENCHMARK:
"X" ON SOUTHEAST BOLT
OF FIRE HYDRANT
ELEVATION = 740.806
VERT DATUM: NAVD88

95.0'

POINT	ELEV	NOTE
A	737.39	TW=TC
B	737.38	TW=TC
C	737.40	TW
D	737.47	TW
E	737.56	TW
F	737.39	MATCH
G	737.49	MATCH

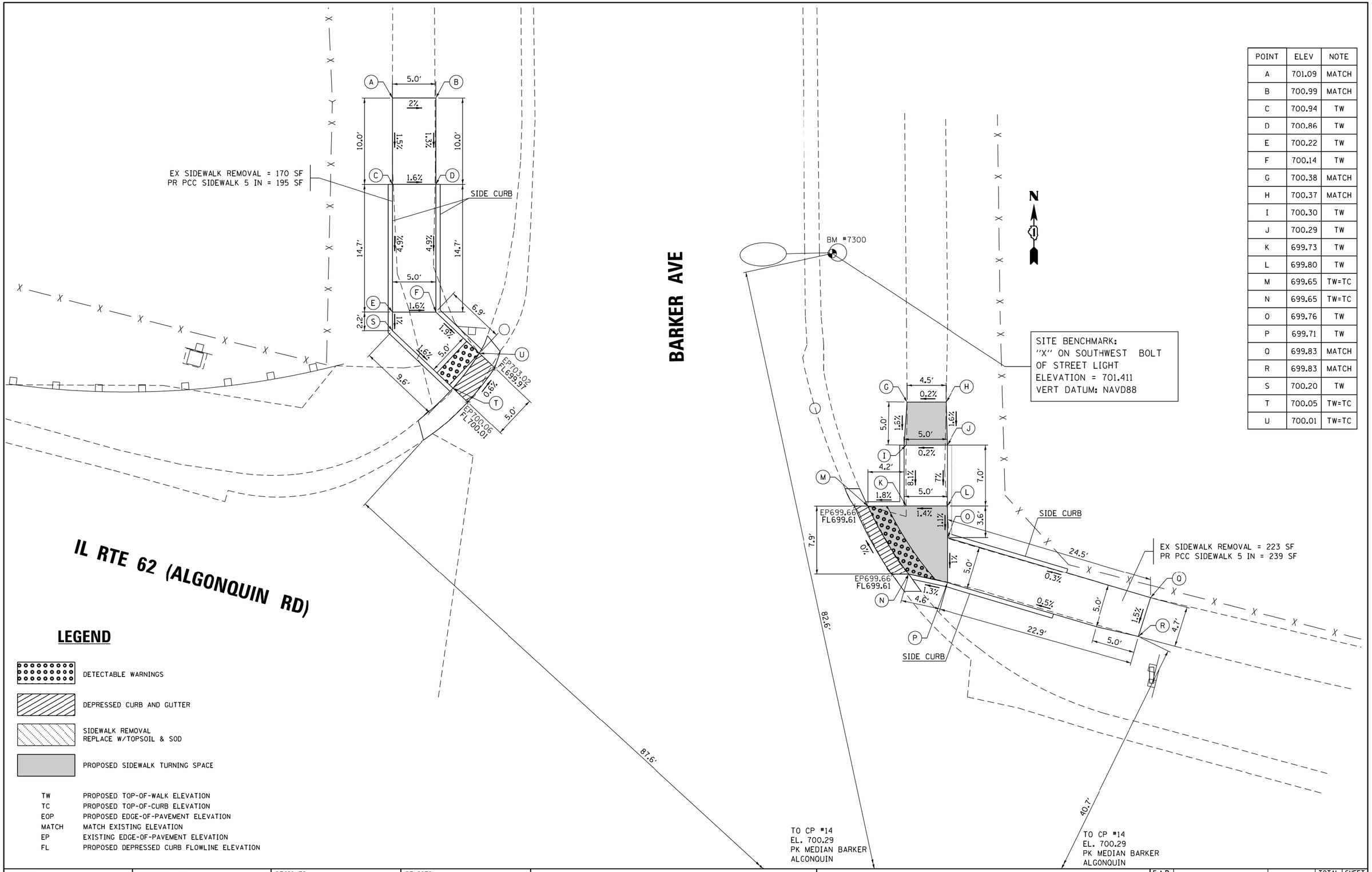
CP #12
CP
737.56
X S SIDE ALGONQUIN IN CURB AT THOREAU

IL RTE 62 (ALGONQUIN RD)



THOREAU DR

FILE NAME = 62C93-sht-plan09.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SIDEWALK DETAIL PLAN IL ROUTE 62 (ALGONQUIN ROAD) AT THOREAU DRIVE	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -			339	116Y(1&2)R-RS-6	COOK	58	23
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -					CONTRACT NO. 62C93		
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -		SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT		



POINT	ELEV	NOTE
A	701.09	MATCH
B	700.99	MATCH
C	700.94	TW
D	700.86	TW
E	700.22	TW
F	700.14	TW
G	700.38	MATCH
H	700.37	MATCH
I	700.30	TW
J	700.29	TW
K	699.73	TW
L	699.80	TW
M	699.65	TW=TC
N	699.65	TW=TC
O	699.76	TW
P	699.71	TW
Q	699.83	MATCH
R	699.83	MATCH
S	700.20	TW
T	700.05	TW=TC
U	700.01	TW=TC

LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL
  REPLACE W/TOPSOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

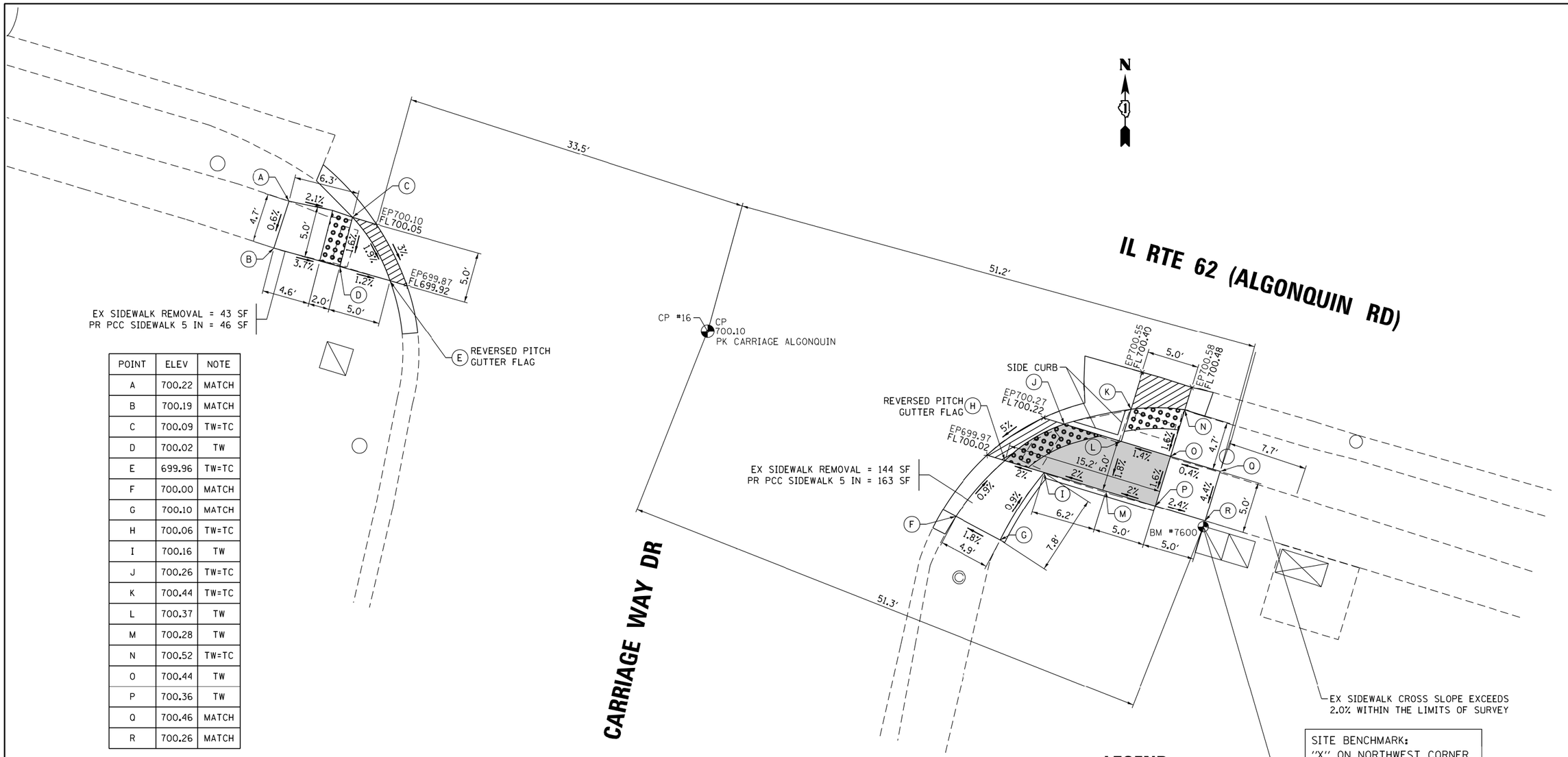
FILE NAME = 62C93-sht-plan10.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT BARKER AVENUE

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	24
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		



POINT	ELEV	NOTE
A	700.22	MATCH
B	700.19	MATCH
C	700.09	TW=TC
D	700.02	TW
E	699.96	TW=TC
F	700.00	MATCH
G	700.10	MATCH
H	700.06	TW=TC
I	700.16	TW
J	700.26	TW=TC
K	700.44	TW=TC
L	700.37	TW
M	700.28	TW
N	700.52	TW=TC
O	700.44	TW
P	700.36	TW
Q	700.46	MATCH
R	700.26	MATCH

LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL
  REPLACE W/TOPSOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

FILE NAME = 62C93-sht-plan11.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

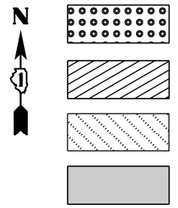
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT CARRIAGE WAY DRIVE

SCALE: 1" = 5' | SHEET OF SHEETS | STA. TO STA.

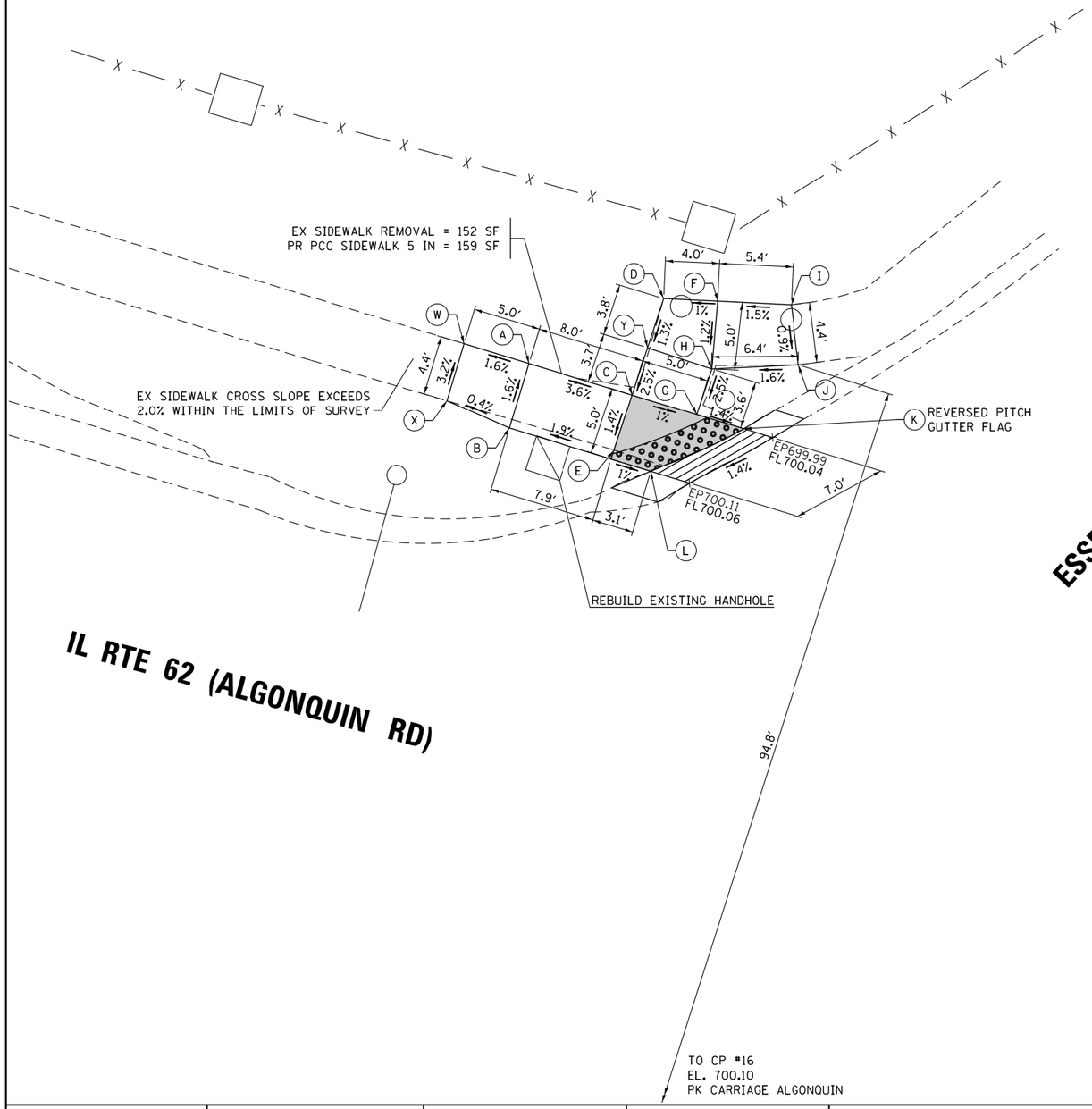
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	25
		CONTRACT NO. 62C93		
	ILLINOIS	FED. AID PROJECT		

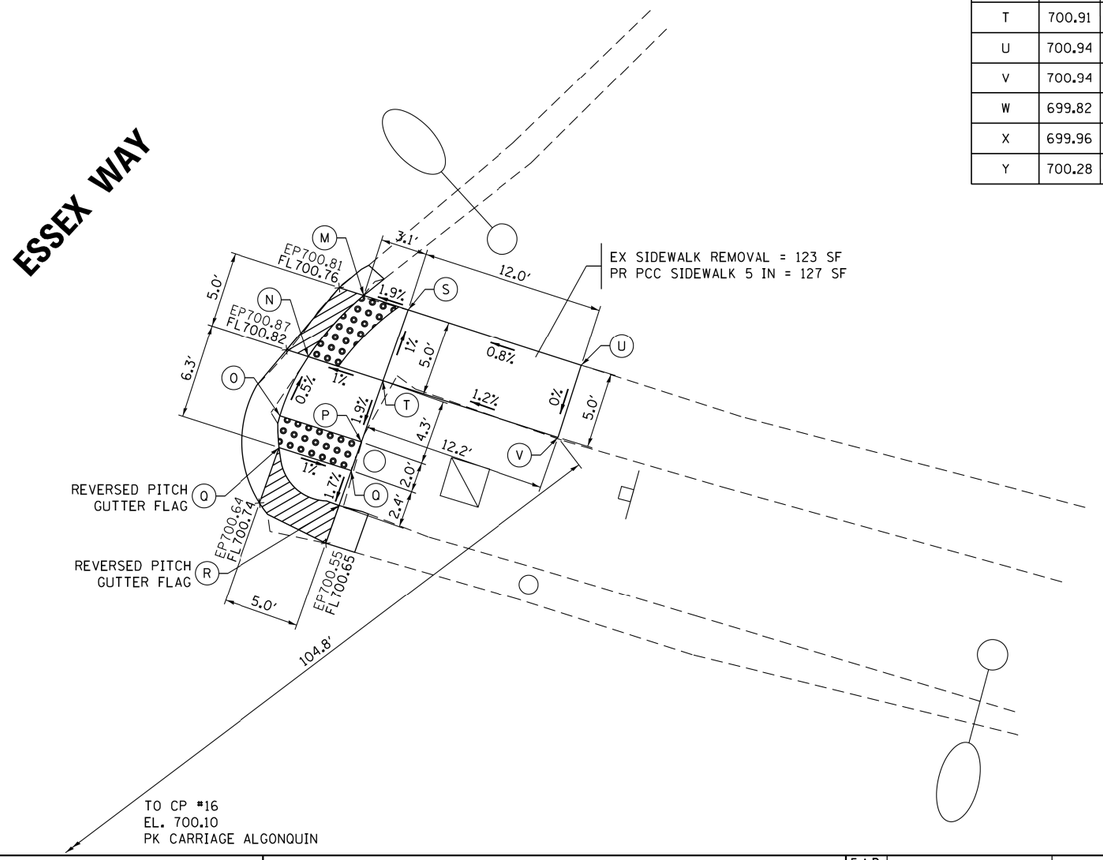
LEGEND



TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

POINT	ELEV	NOTE
A	699.90	TW
B	699.98	TW
C	700.19	TW
D	700.33	TW
E	700.13	TW
F	700.29	TW
G	700.14	TW
H	700.23	TW
I	700.37	MATCH
J	700.33	MATCH
K	700.09	TW=TC
L	700.10	TW=TC
M	700.80	TW=TC
N	700.86	TW=TC
O	700.88	TW
P	700.83	TW
Q	700.78	TW=TC
R	700.69	TW=TC
S	700.86	TW
T	700.91	TW
U	700.94	MATCH
V	700.94	MATCH
W	699.82	MATCH
X	699.96	MATCH
Y	700.28	TW





FILE NAME = 62C93-sht-plan12.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SIDEWALK DETAIL PLAN
		DRAWN -	REVISED -		IL ROUTE 62 (ALGONQUIN ROAD) AT ESSEX WAY
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -		SCALE: 1" = 5' SHEET OF SHEETS STA. TO STA.
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	26
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

LEGEND

DETECTABLE WARNINGS

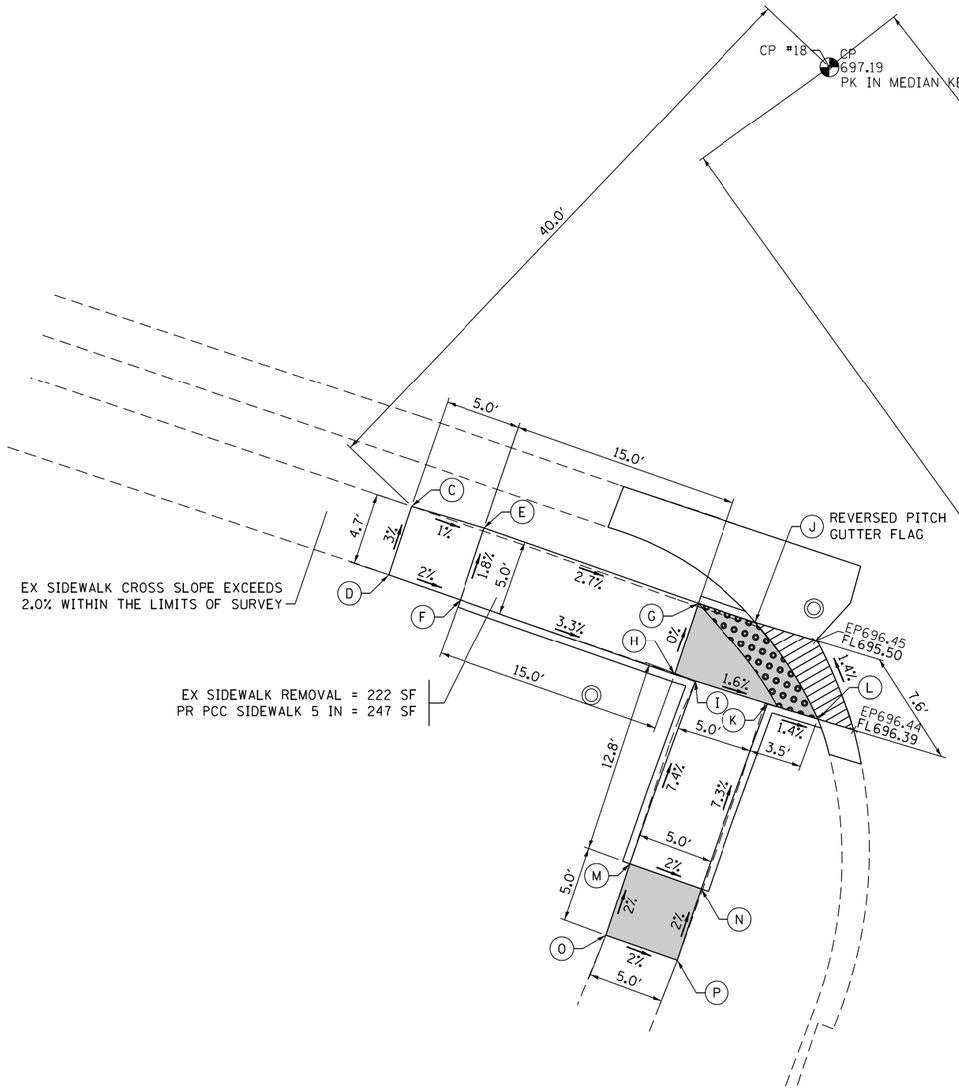
DEPRESSED CURB AND GUTTER

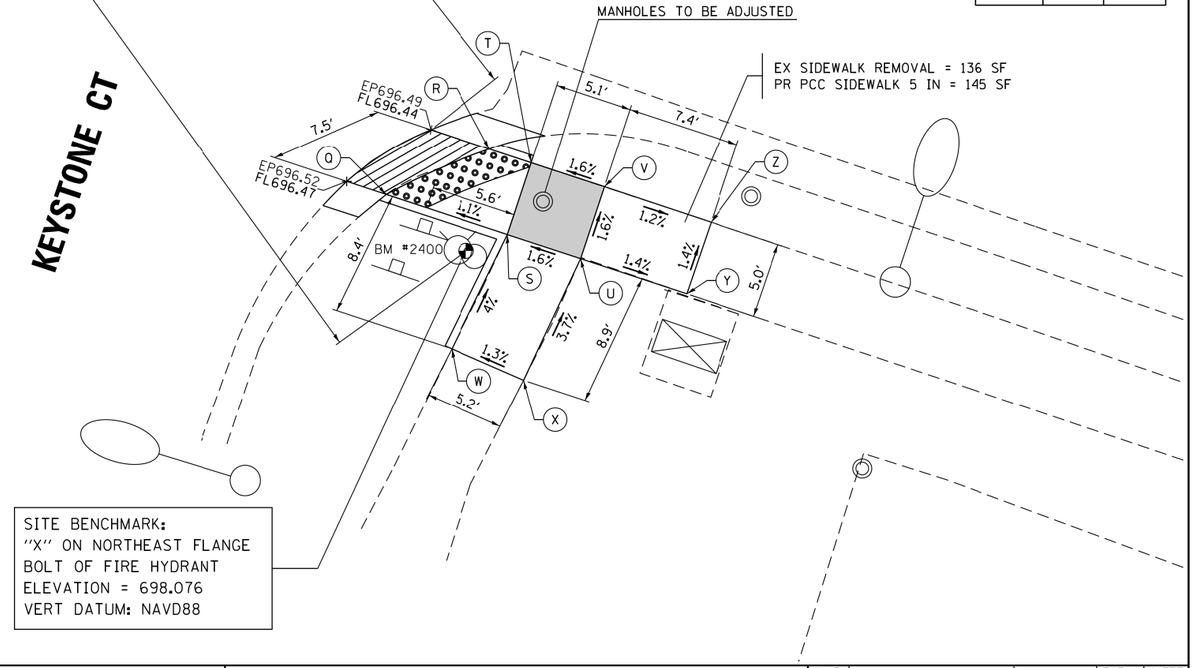
SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

POINT	ELEV	NOTE
A	NOT USED	
B	NOT USED	
C	697.03	MATCH
D	697.17	MATCH
E	696.98	TW
F	697.07	TW
G	696.58	TW
H	696.58	TW
I	696.56	TW
J	696.54	TW=TC
K	696.48	TW
L	696.43	TW=TC
M	697.51	TW
N	697.41	TW
O	697.61	MATCH
P	697.51	MATCH
Q	696.51	TW=TC
R	696.48	TW=TC
S	696.60	TW
T	696.52	TW
U	696.68	TW
V	696.60	TW
W	696.94	MATCH
X	697.01	MATCH
Y	696.58	MATCH
Z	696.51	MATCH





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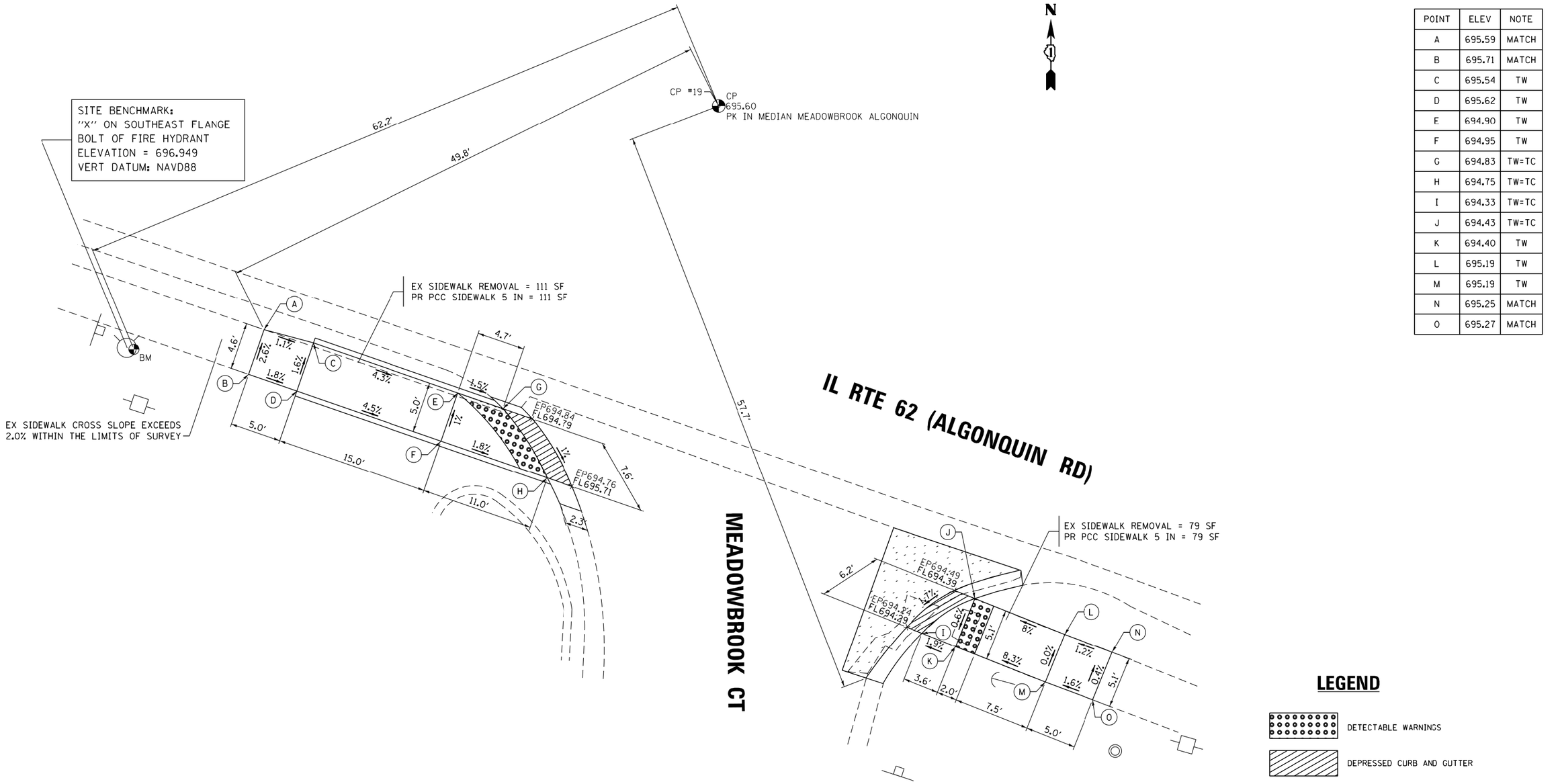
		DRAWN -	REVISED -
Default	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT KEYSTONE COURT

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	27
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		



POINT	ELEV	NOTE
A	695.59	MATCH
B	695.71	MATCH
C	695.54	TW
D	695.62	TW
E	694.90	TW
F	694.95	TW
G	694.83	TW=TC
H	694.75	TW=TC
I	694.33	TW=TC
J	694.43	TW=TC
K	694.40	TW
L	695.19	TW
M	695.19	TW
N	695.25	MATCH
O	695.27	MATCH

LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL
  REPLACE W/TOPSOIL & SOD
- CLASS D PATCHES
- PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

FILE NAME = 62C93-sht-plan14.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT MEADOWBROOK COURT

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	28
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

LEGEND

DETECTABLE WARNINGS

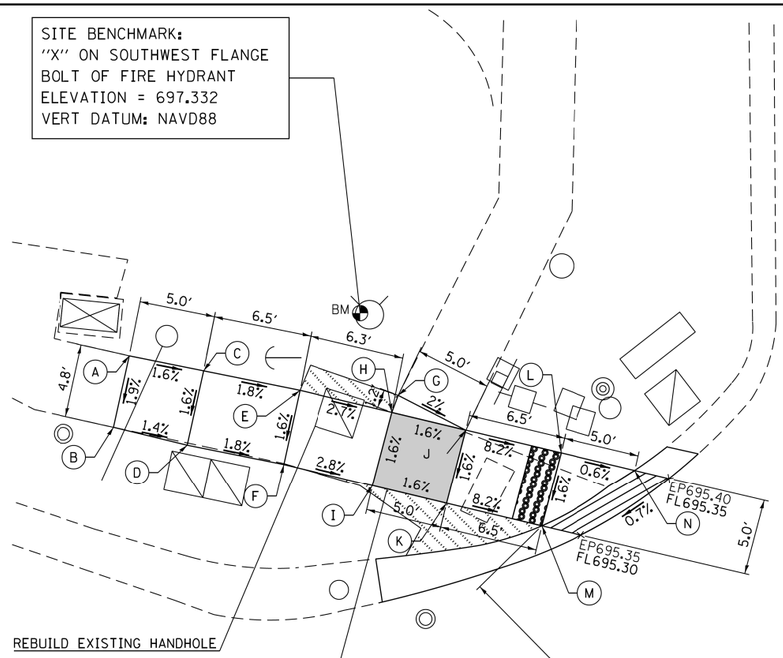
DEPRESSED CURB AND GUTTER

SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

SITE BENCHMARK:
"X" ON SOUTHWEST FLANGE
BOLT OF FIRE HYDRANT
ELEVATION = 697.332
VERT DATUM: NAVD88

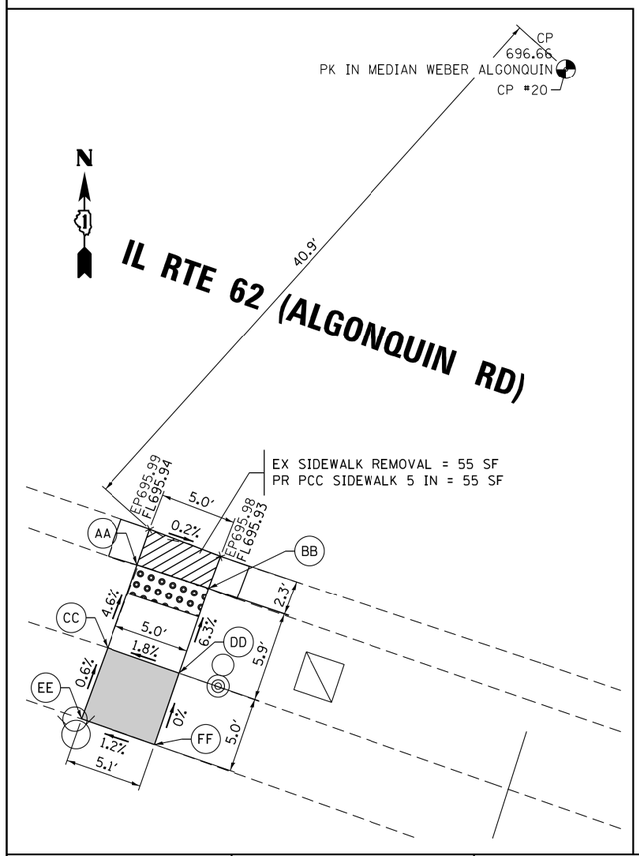


EX SIDEWALK REMOVAL = 182 SF
PR PCC SIDEWALK 5 IN = 161 SF

POINT	ELEV	NOTE
A	696.37	MATCH
B	696.28	MATCH
C	696.29	TW
D	696.21	TW
E	696.17	TW
F	696.09	TW
G	696.05	MATCH
H	696.03	TW
I	695.95	TW
J	695.95	MATCH
K	695.87	TW
L	695.42	TW
M	695.34	TW
N	695.39	TW
O	696.19	MATCH
P	696.20	MATCH

POINT	ELEV	NOTE
Q	696.10	TW
R	696.15	TW
S	695.83	TW
T	695.91	TW
U	695.75	TW
V	695.72	TW=TC
W	695.80	TW=TC
X	695.88	TW
Y	695.96	TW
Z	695.92	TW
AA	695.98	TW=TC
BB	695.97	TW=TC
CC	696.25	MATCH
DD	696.34	MATCH
EE	696.28	MATCH
FF	696.34	MATCH

POINT	ELEV	NOTE
GG	695.84	TW=TC
HH	695.89	TW=TC
II	696.67	TW
JJ	696.62	TW
KK	696.72	MATCH
LL	696.70	MATCH



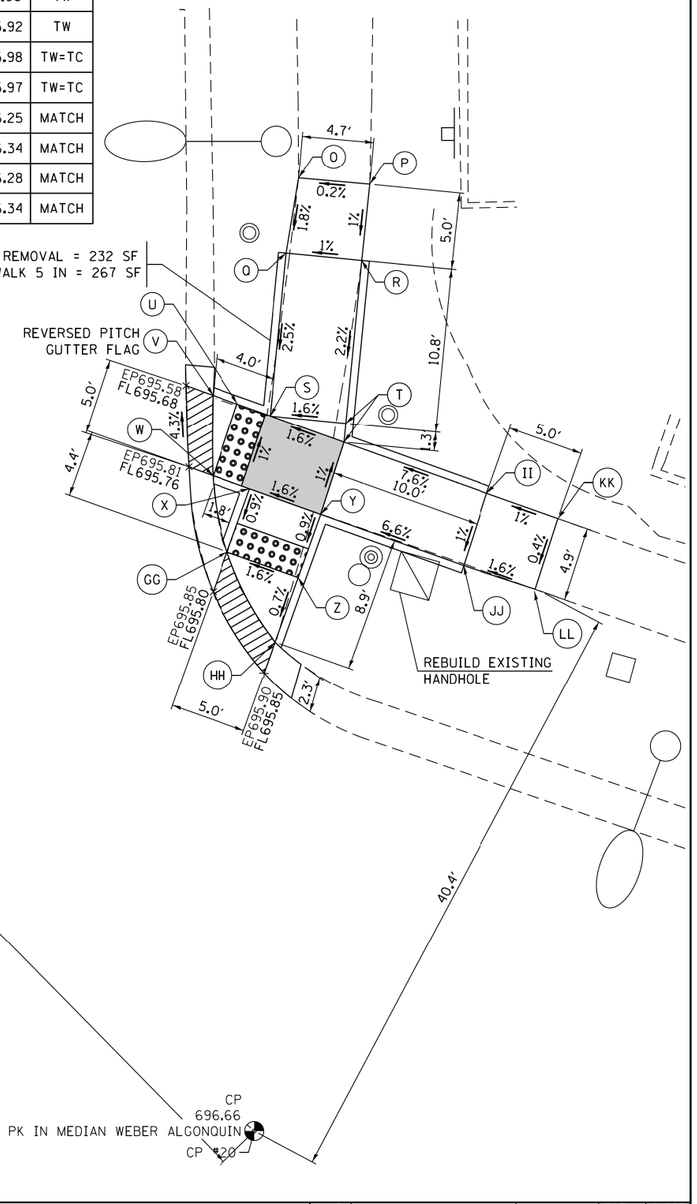
EX SIDEWALK REMOVAL = 55 SF
PR PCC SIDEWALK 5 IN = 55 SF

WEBER DR

IL RTE 62 (ALGONQUIN RD)

82.4'

40.4'



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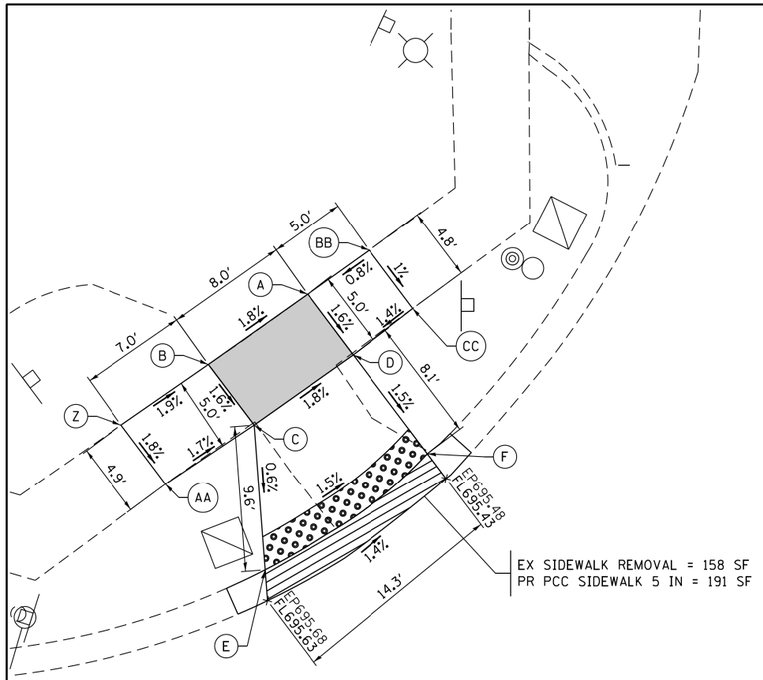
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT WEBER DRIVE

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

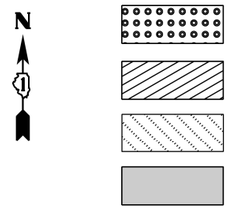
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	29
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			



NEW WILKE RD

IL RTE 62 (ALGONQUIN RD)

LEGEND



DETECTABLE WARNINGS

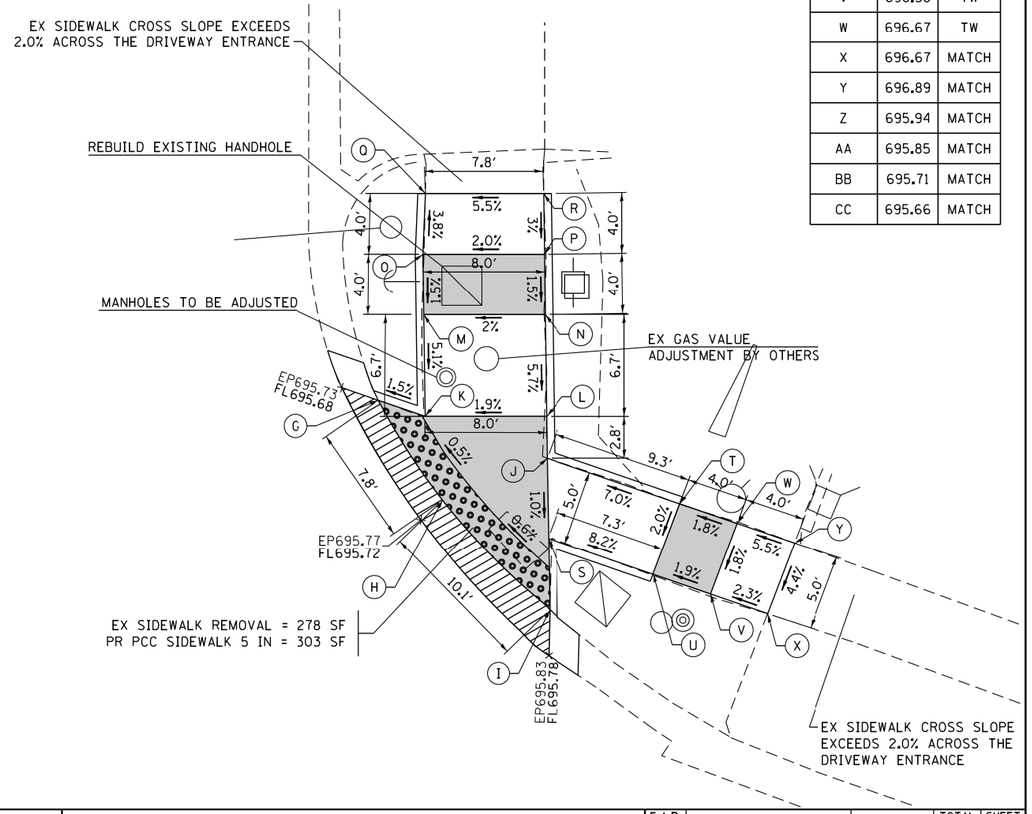
DEPRESSED CURB AND GUTTER

SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

POINT	ELEV	NOTE
A	695.67	TW
B	695.81	TW
C	695.73	TW
D	695.59	TW
E	695.67	TW=TC
F	695.47	TW=TC
G	695.72	TW=TC
H	695.76	TW=TC
I	695.82	TW=TC
J	695.95	TW
K	695.77	TW
L	695.92	TW
M	696.11	TW
N	696.27	TW
O	696.17	TW
P	696.33	TW
Q	696.02	MATCH
R	696.45	MATCH
S	695.90	TW
T	696.60	TW
U	696.50	TW
V	696.58	TW
W	696.67	TW
X	696.67	MATCH
Y	696.89	MATCH
Z	695.94	MATCH
AA	695.85	MATCH
BB	695.71	MATCH
CC	695.66	MATCH



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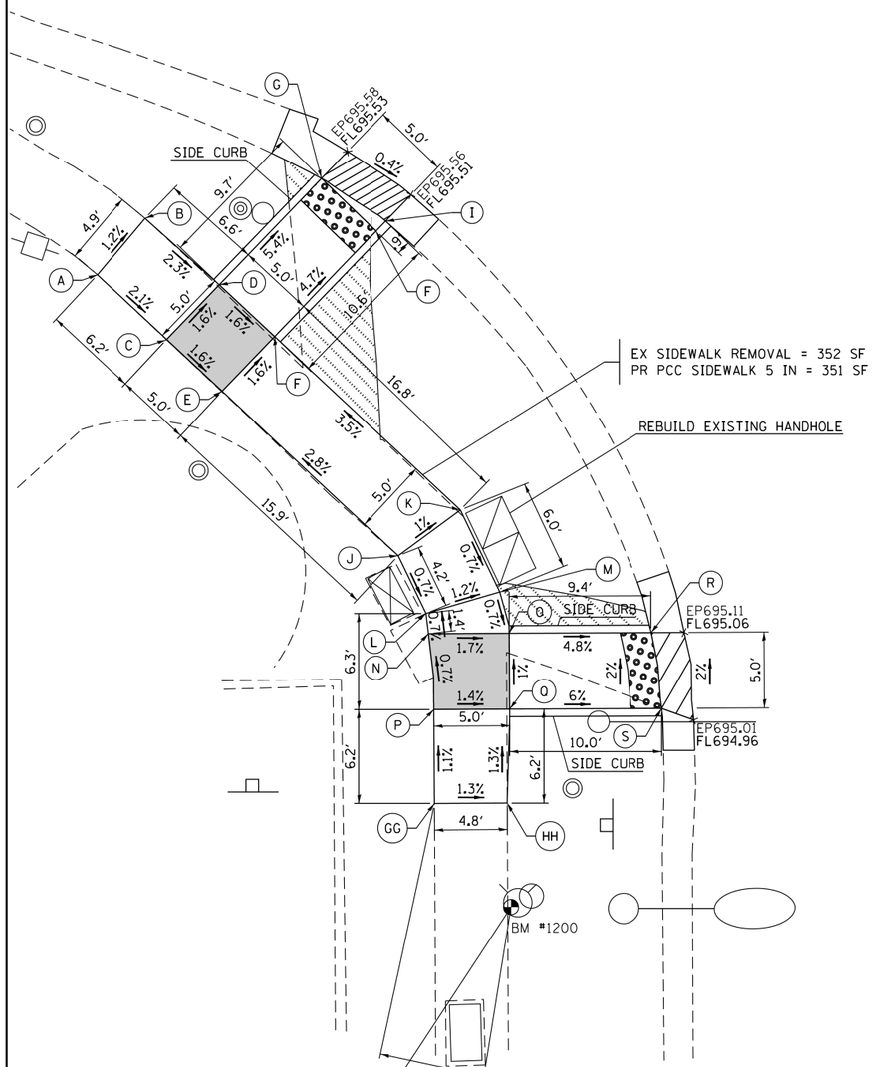
	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT NEW WILKE ROAD

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	30
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		



SITE BENCHMARK:
"X" ON WEST FLANGE BOLT
OF FIRE HYDRANT
ELEVATION = 697.041
VERT DATUM: NAVD88

POINT	ELEV	NOTE
A	696.31	MATCH
B	696.25	MATCH
C	696.18	TW
D	696.10	TW
E	696.10	TW
F	696.02	TW
G	695.57	TW=TC
H	695.57	TW
I	695.55	TW=TC
J	695.66	MATCH
K	695.61	TW
L	695.63	MATCH
M	695.57	TW
N	695.64	MATCH
O	695.55	TW
P	695.67	TW
Q	695.60	TW
R	695.10	TW=TC
S	695.00	TW=TC
T	695.10	TW=TC
U	695.12	TW=TC
V	695.19	TW=TC
W	695.33	TW
X	695.43	TW
Y	695.17	TW
Z	695.26	TW
AA	695.26	TW
BB	695.88	TW
CC	695.80	TW
DD	695.21	TW
EE	695.37	MATCH
FF	695.53	MATCH
GG	695.74	MATCH
HH	695.68	MATCH

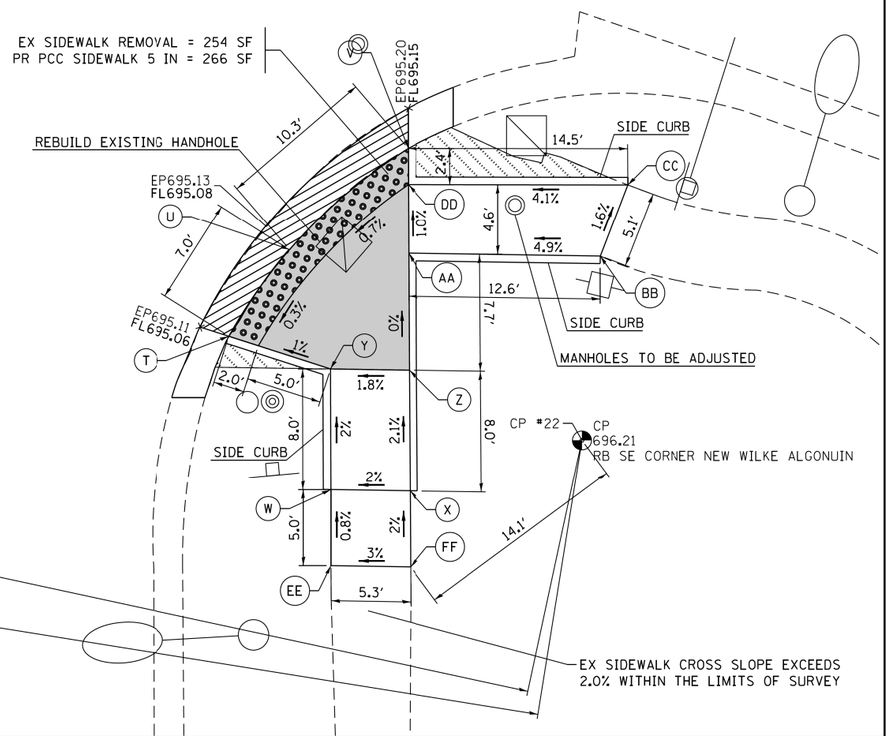
NEW WILKE RD

IL RTE 62 (ALGONQUIN RD)

LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION



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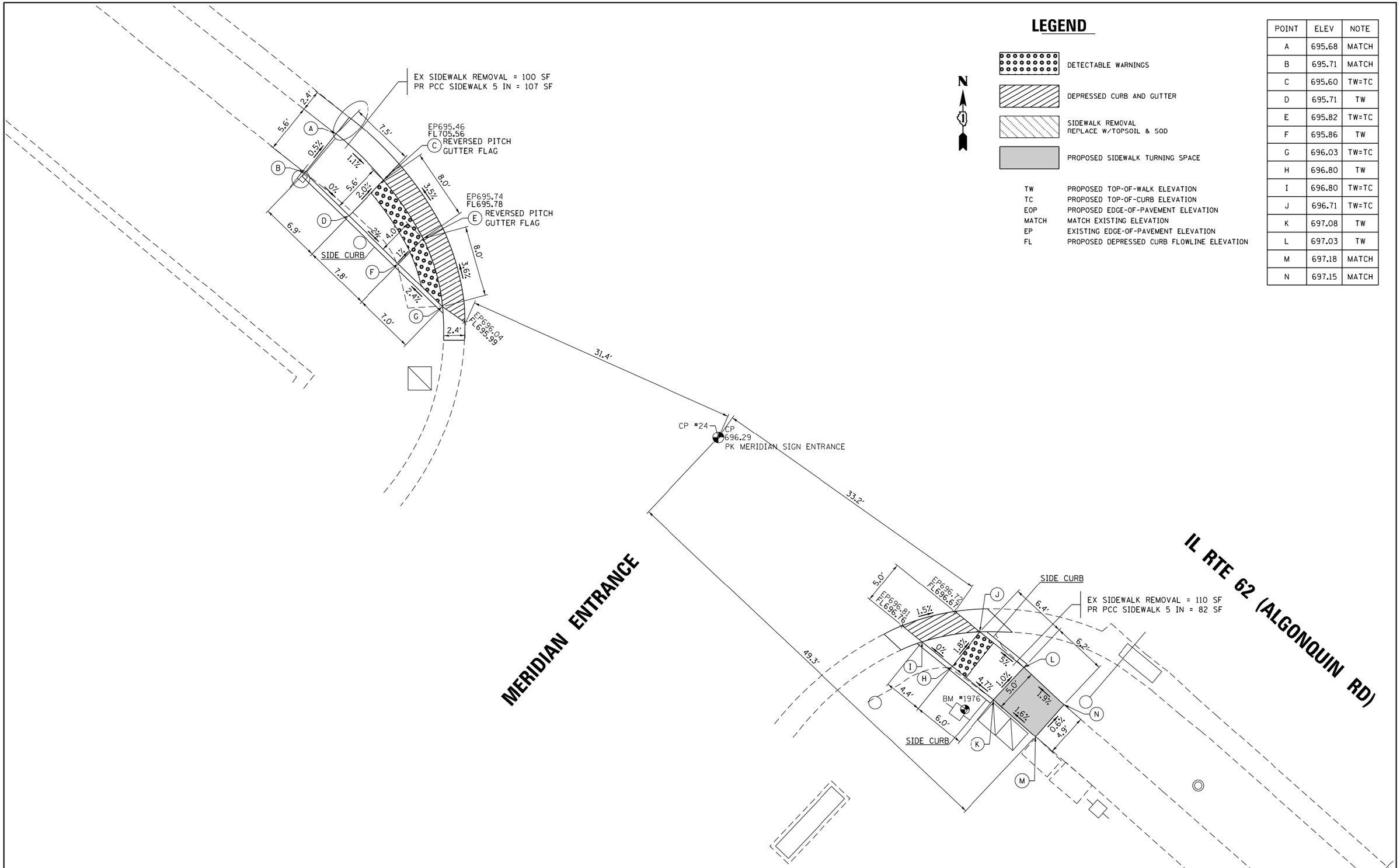
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT NEW WILKE ROAD

SCALE: 1" = 5' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	31
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			



LEGEND

DETECTABLE WARNINGS

DEPRESSED CURB AND GUTTER

SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

POINT	ELEV	NOTE
A	695.68	MATCH
B	695.71	MATCH
C	695.60	TW=TC
D	695.71	TW
E	695.82	TW=TC
F	695.86	TW
G	696.03	TW=TC
H	696.80	TW
I	696.80	TW=TC
J	696.71	TW=TC
K	697.08	TW
L	697.03	TW
M	697.18	MATCH
N	697.15	MATCH

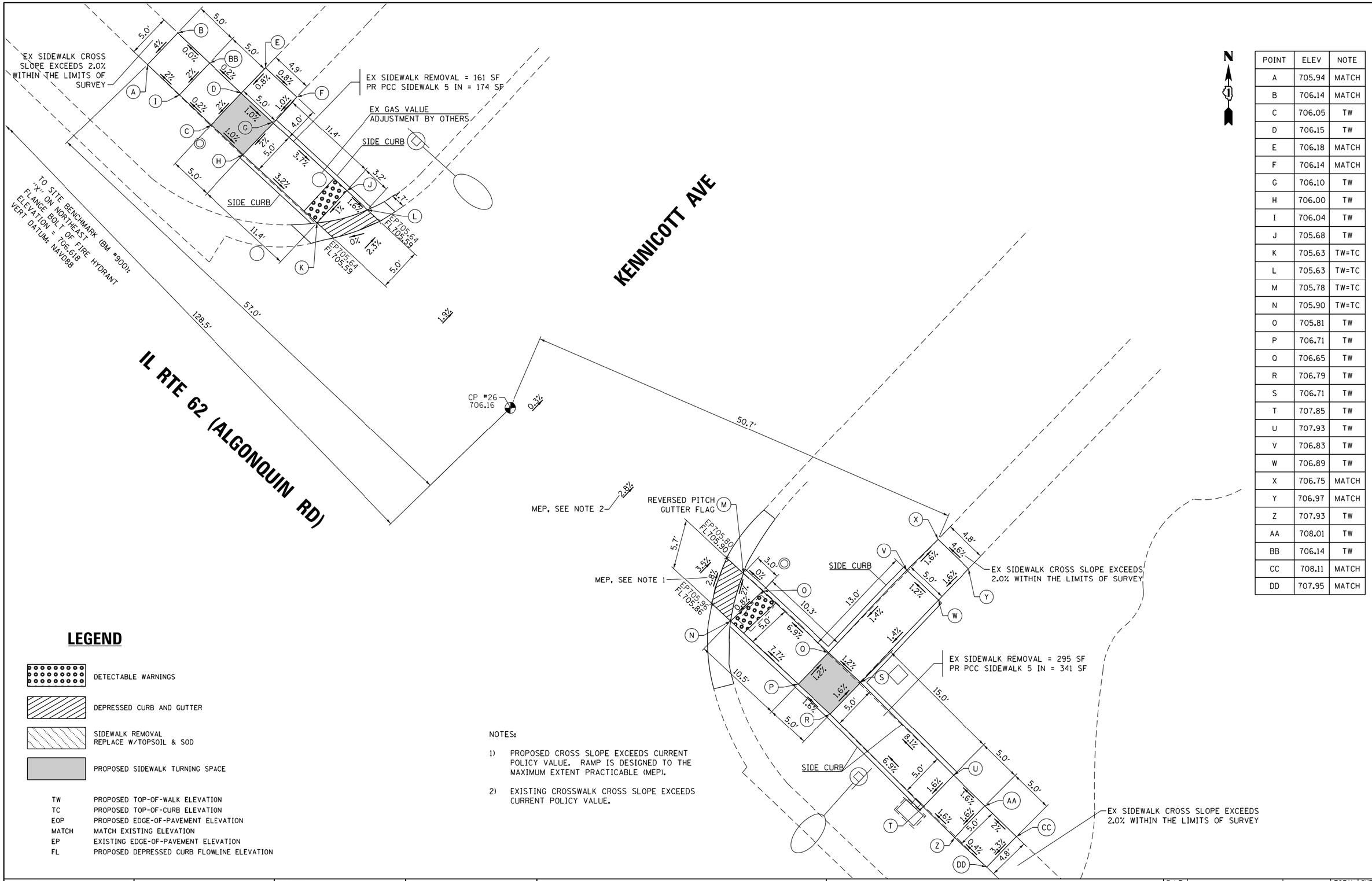
FILE NAME = 62C93-sht-plan18.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT MERIDIAN ENTRANCE

SCALE: 1" = 5' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	32
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			



POINT	ELEV	NOTE
A	705.94	MATCH
B	706.14	MATCH
C	706.05	TW
D	706.15	TW
E	706.18	MATCH
F	706.14	MATCH
G	706.10	TW
H	706.00	TW
I	706.04	TW
J	705.68	TW
K	705.63	TW=TC
L	705.63	TW=TC
M	705.78	TW=TC
N	705.90	TW=TC
O	705.81	TW
P	706.71	TW
Q	706.65	TW
R	706.79	TW
S	706.71	TW
T	707.85	TW
U	707.93	TW
V	706.83	TW
W	706.89	TW
X	706.75	MATCH
Y	706.97	MATCH
Z	707.93	TW
AA	708.01	TW
BB	706.14	TW
CC	708.11	MATCH
DD	707.95	MATCH

LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

NOTES:

1) PROPOSED CROSS SLOPE EXCEEDS CURRENT POLICY VALUE. RAMP IS DESIGNED TO THE MAXIMUM EXTENT PRACTICABLE (MEP).

2) EXISTING CROSSWALK CROSS SLOPE EXCEEDS CURRENT POLICY VALUE.

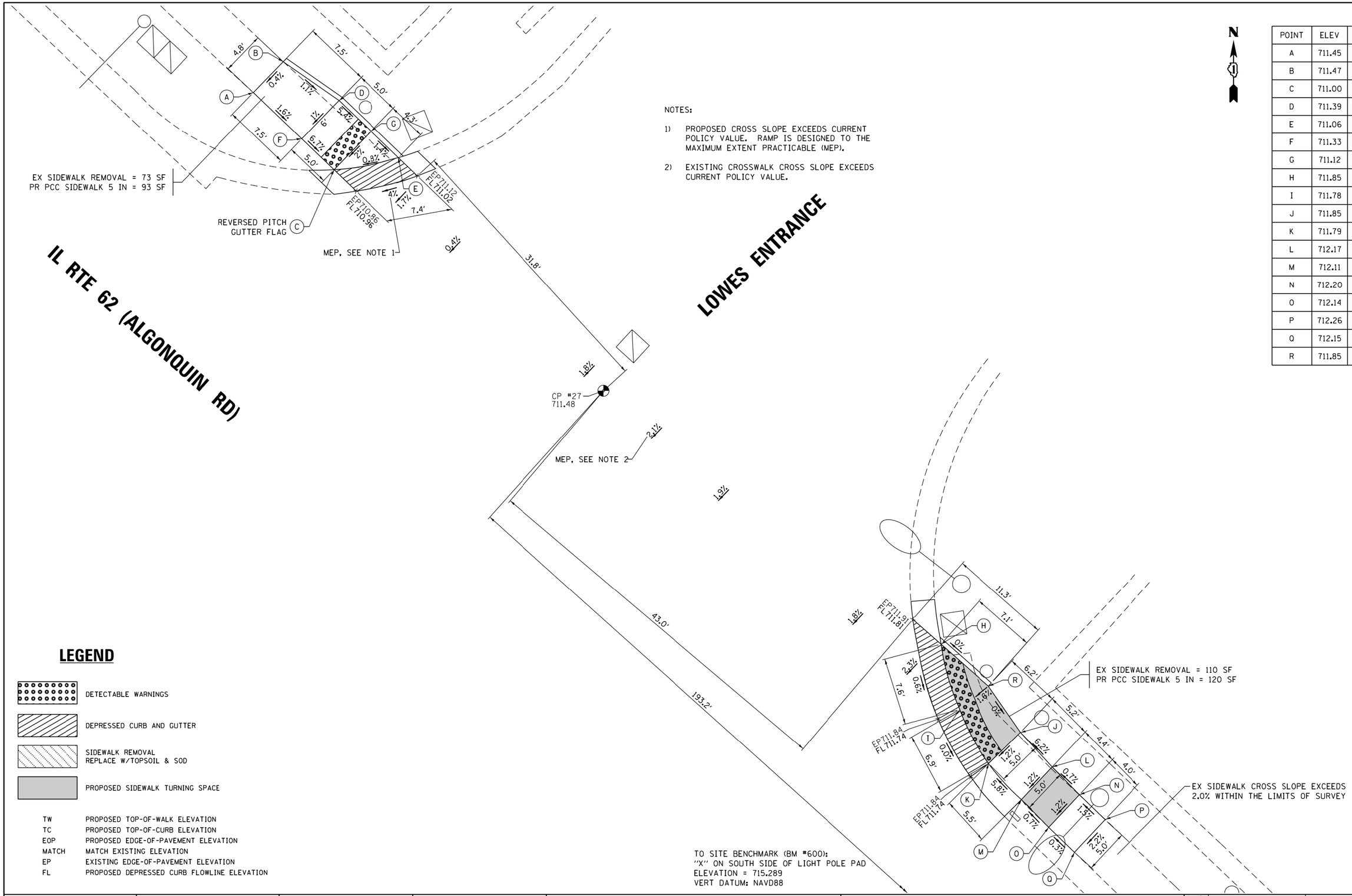
FILE NAME = 62C93-sht-plan19.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT KENNICOTT AVENUE

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	33
		CONTRACT NO. 62C93		
	ILLINOIS	FED. AID PROJECT		



NOTES:

1) PROPOSED CROSS SLOPE EXCEEDS CURRENT POLICY VALUE. RAMP IS DESIGNED TO THE MAXIMUM EXTENT PRACTICABLE (MEP).

2) EXISTING CROSSWALK CROSS SLOPE EXCEEDS CURRENT POLICY VALUE.

POINT	ELEV	NOTE
A	711.45	MATCH
B	711.47	MATCH
C	711.00	TW=TC
D	711.39	TW
E	711.06	TW=TC
F	711.33	TW
G	711.12	TW
H	711.85	TW=TC
I	711.78	TW=TC
J	711.85	TW
K	711.79	TW=TC
L	712.17	TW
M	712.11	MATCH
N	712.20	TW
O	712.14	MATCH
P	712.26	MATCH
Q	712.15	MATCH
R	711.85	TW

TO SITE BENCHMARK (BM #600):
"X" ON SOUTH SIDE OF LIGHT POLE PAD
ELEVATION = 715.289
VERT DATUM: NAVD88

LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL
  REPLACE W/TOPSOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

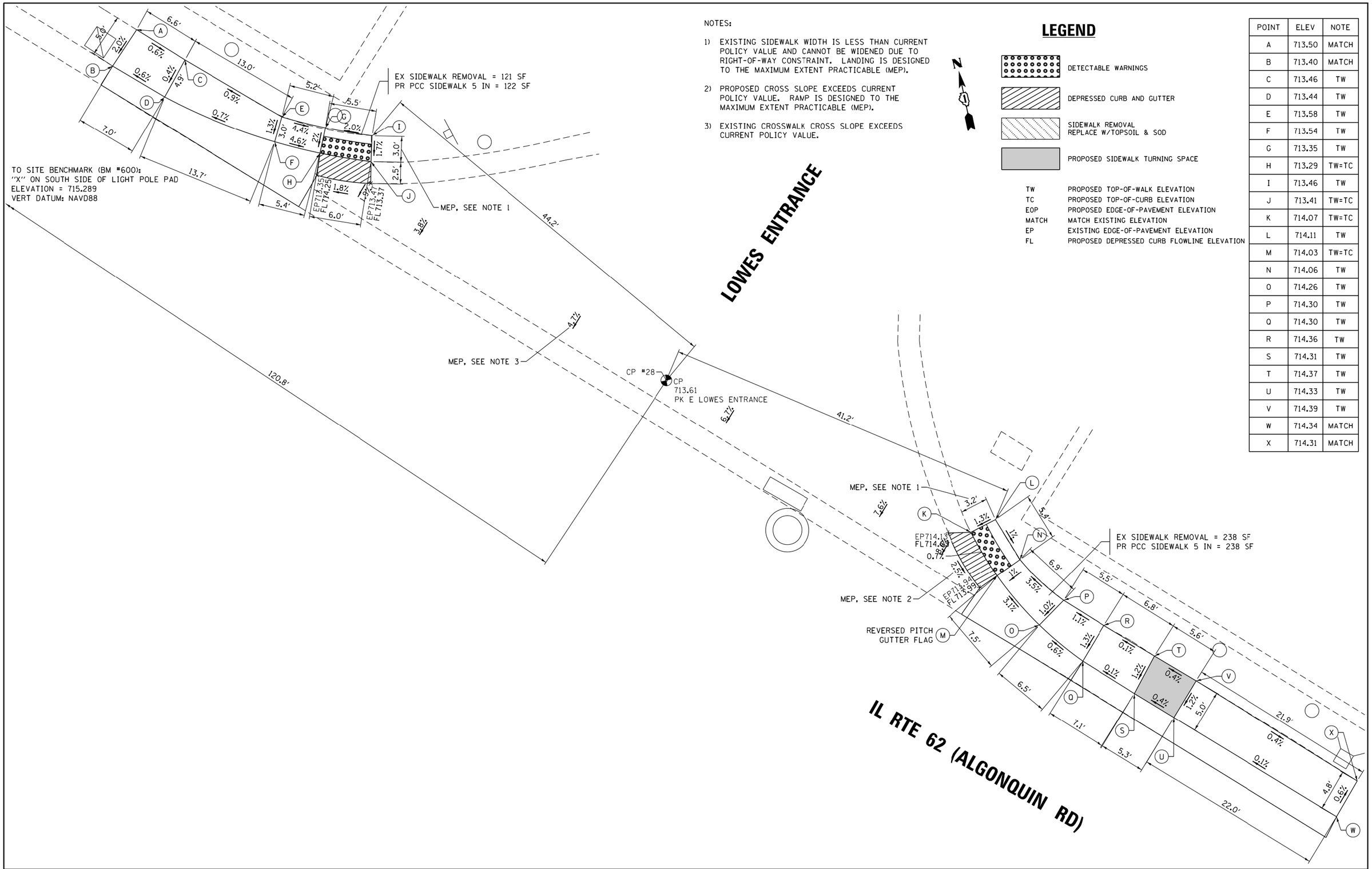
FILE NAME = 62C93-sht-plan20.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT LOWES ENTRANCE

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	34
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		



POINT	ELEV	NOTE
A	713.50	MATCH
B	713.40	MATCH
C	713.46	TW
D	713.44	TW
E	713.58	TW
F	713.54	TW
G	713.35	TW
H	713.29	TW=TC
I	713.46	TW
J	713.41	TW=TC
K	714.07	TW=TC
L	714.11	TW
M	714.03	TW=TC
N	714.06	TW
O	714.26	TW
P	714.30	TW
Q	714.30	TW
R	714.36	TW
S	714.31	TW
T	714.37	TW
U	714.33	TW
V	714.39	TW
W	714.34	MATCH
X	714.31	MATCH

FILE NAME = 62C93-sht-plan21.dgn	USER NAME = Dwhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
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Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

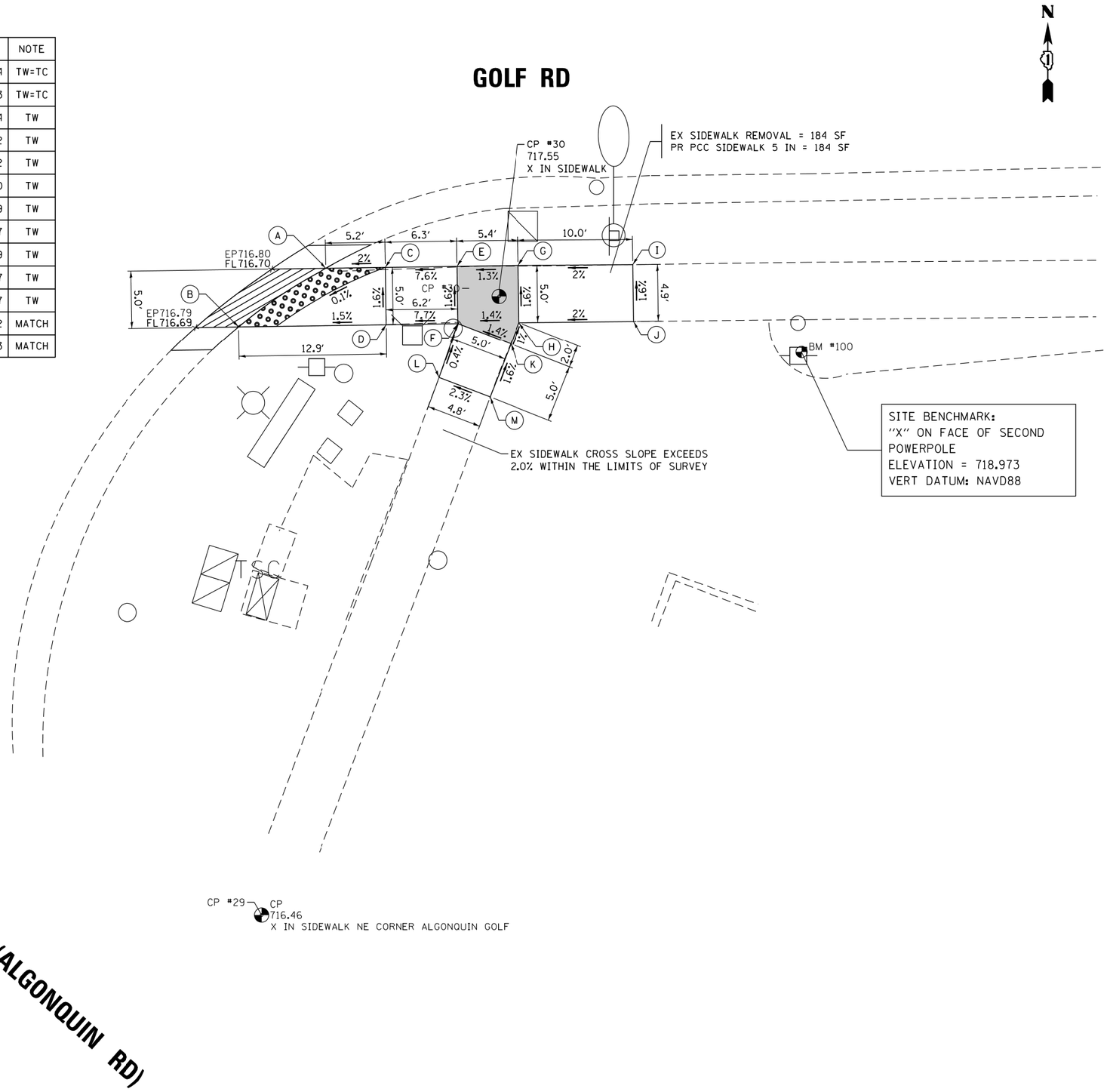
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT LOWES ENTRANCE

SCALE: 1" = 5' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	35
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		

POINT	ELEV	NOTE
A	716.74	TW=TC
B	716.73	TW=TC
C	716.84	TW
D	716.92	TW
E	717.32	TW
F	717.40	TW
G	717.39	TW
H	717.47	TW
I	717.59	TW
J	717.67	TW
K	717.47	TW
L	717.42	MATCH
M	717.53	MATCH



LEGEND

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL
  REPLACE W/TOPSOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE

TW	PROPOSED TOP-OF-WALK ELEVATION
TC	PROPOSED TOP-OF-CURB ELEVATION
EOP	PROPOSED EDGE-OF-PAVEMENT ELEVATION
MATCH	MATCH EXISTING ELEVATION
EP	EXISTING EDGE-OF-PAVEMENT ELEVATION
FL	PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

FILE NAME = 62C93-sht-plan22.dgn	USER NAME = DWhite	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 10/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
IL ROUTE 62 (ALGONQUIN ROAD) AT GOLF ROAD

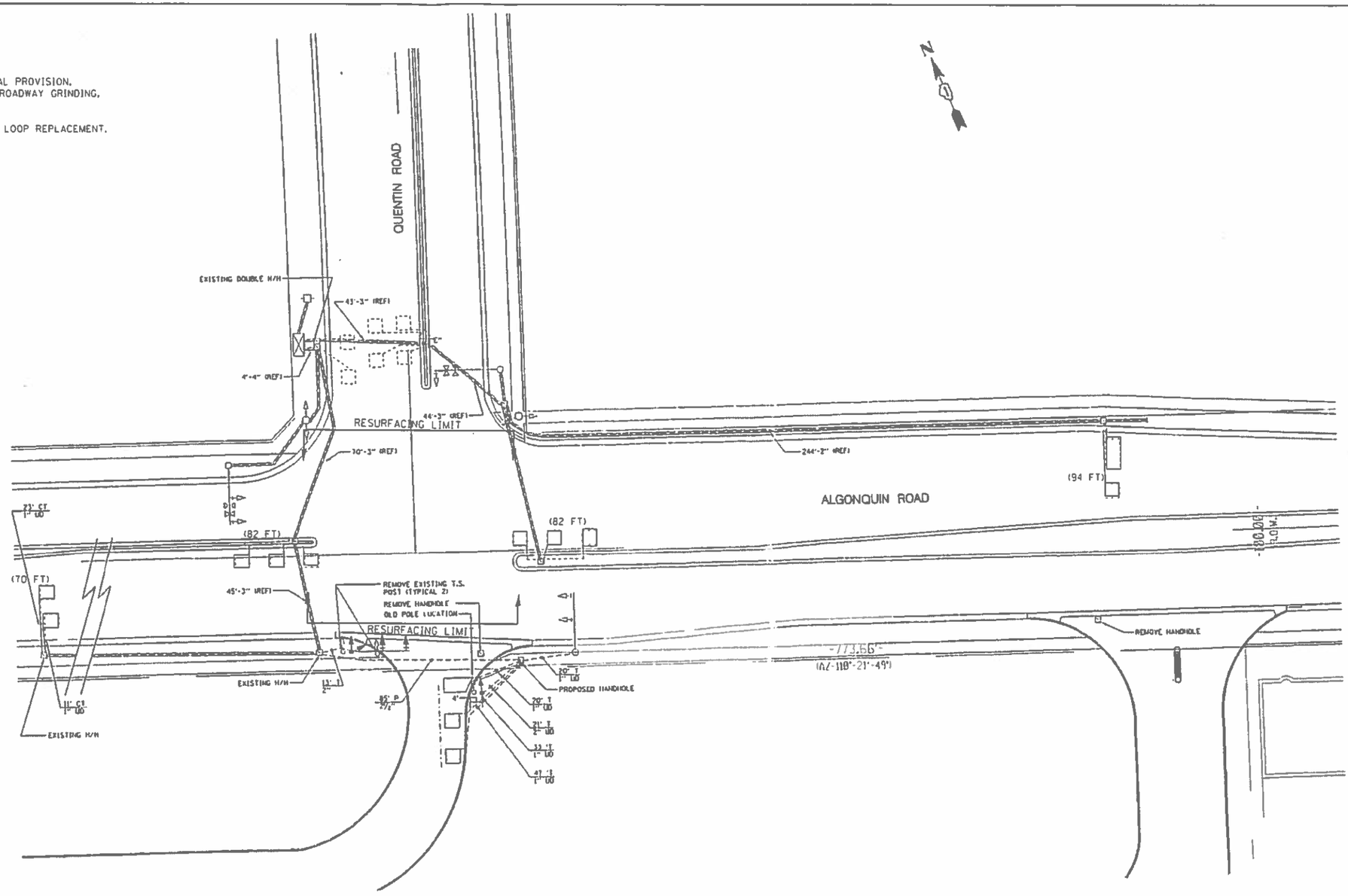
SCALE: 1" = 5′ | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	36
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS.

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	328	FOOT

FILE NAME = S:\WP\Design\JC\Detector Loop Details\2216\62C93\DetectorLoopReplacementSheet_Blank_DRAWN6.dgn	USER NAME = curryje	DESIGNED - IP	REVISED -
		DRAWN - IP	REVISED -
	PLOT SCALE = 48.0468 ' / in.	CHECKED - LP	REVISED -
Default	PLOT DATE = 9/21/2016	DATE - 8/15/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT PLAN
IL 62 AT QUENTIN

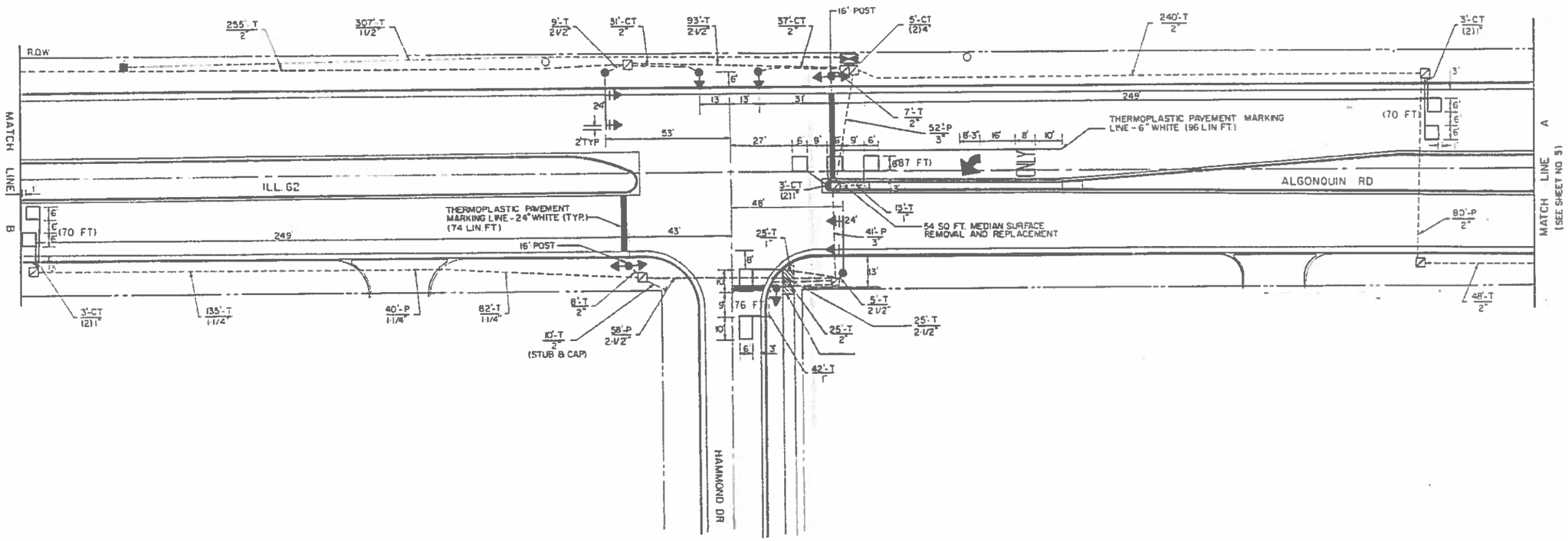
SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y11&21R-RS-6	COOK	58	37
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS.

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	303	FOOT

FILE NAME = S:\WP\Design\JC\Detector Loop Details\2016\62C93\DetectorLoopReplacementSheet.Blank.dgn	USER NAME = curryje	DESIGNED - IP	REVISED -
		DRAWN - IP	REVISED -
	PLOT SCALE = 40.0460 ' / in.	CHECKED - LP	REVISED -
Default	PLOT DATE = 9/21/2016	DATE - 8/15/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT PLAN
IL 62 AT HAMMOND DR.

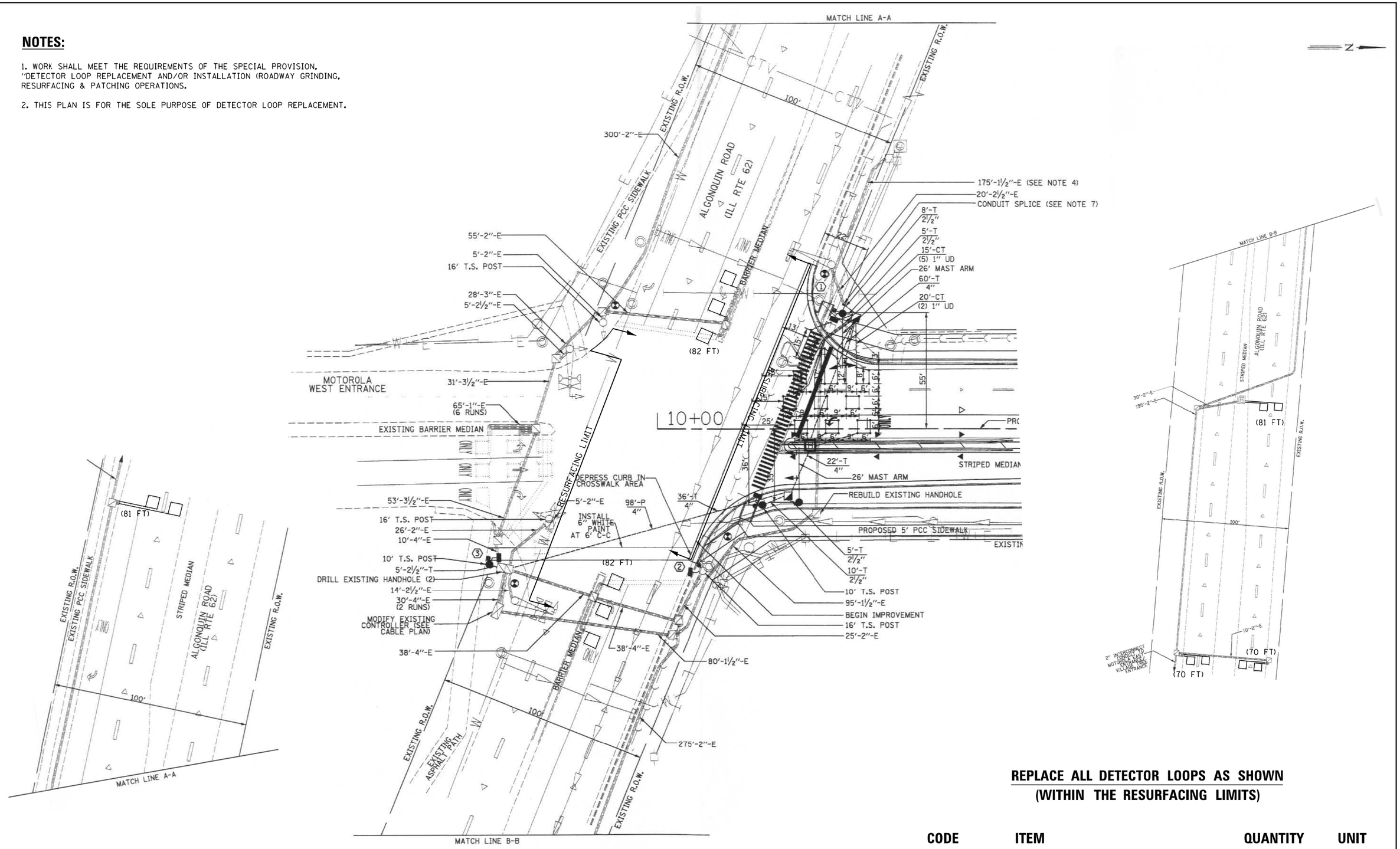
SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	38
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS.

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	466	FOOT

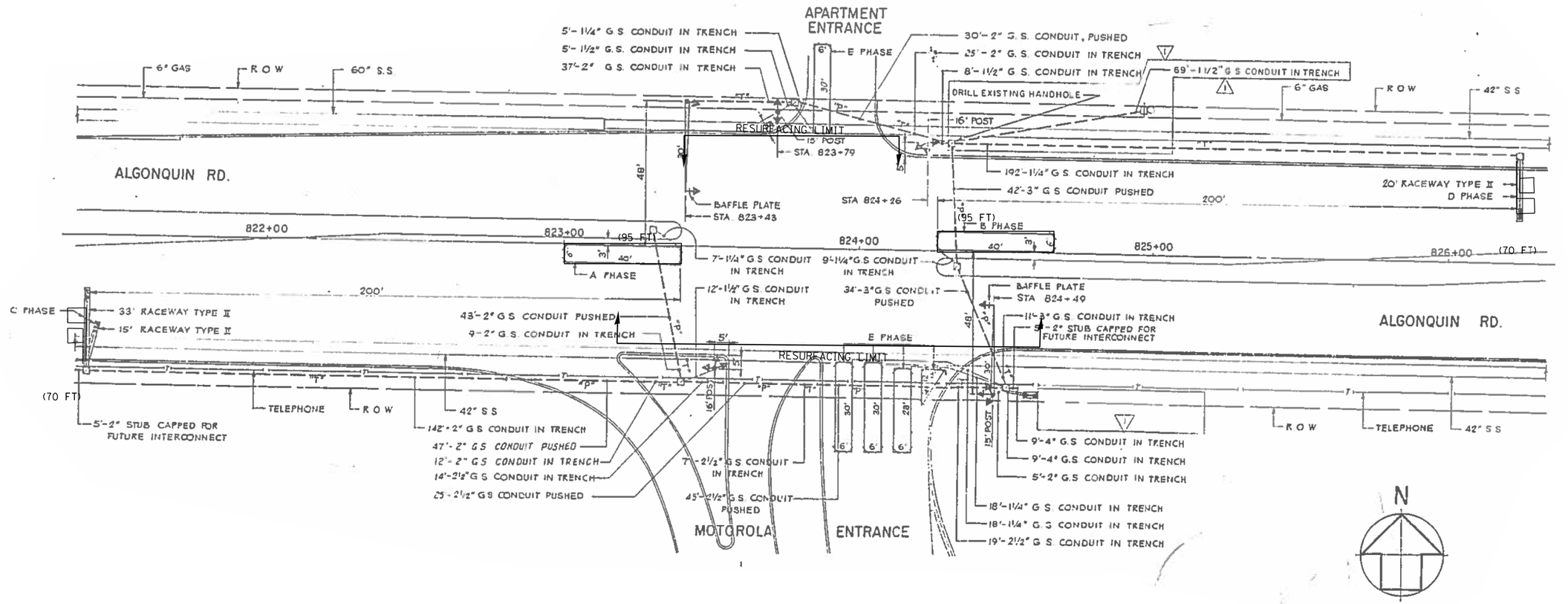
FILE NAME = S:\WP\Design\JC\Detector Loop Details\2016\62C93\DetectorLoopReplacementSheet_Blank_DRAWING6.dgn	USER NAME = curryja	DESIGNED - IP	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN IL 62 AT PLUM GROVE
	PLOT SCALE = 40.0000 ' / in.	DRAWN - IP	REVISED -		
Default	PLOT DATE = 1/25/2017	CHECKED - LP	REVISED -		
		DATE - 8/15/2016	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	39
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS.

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

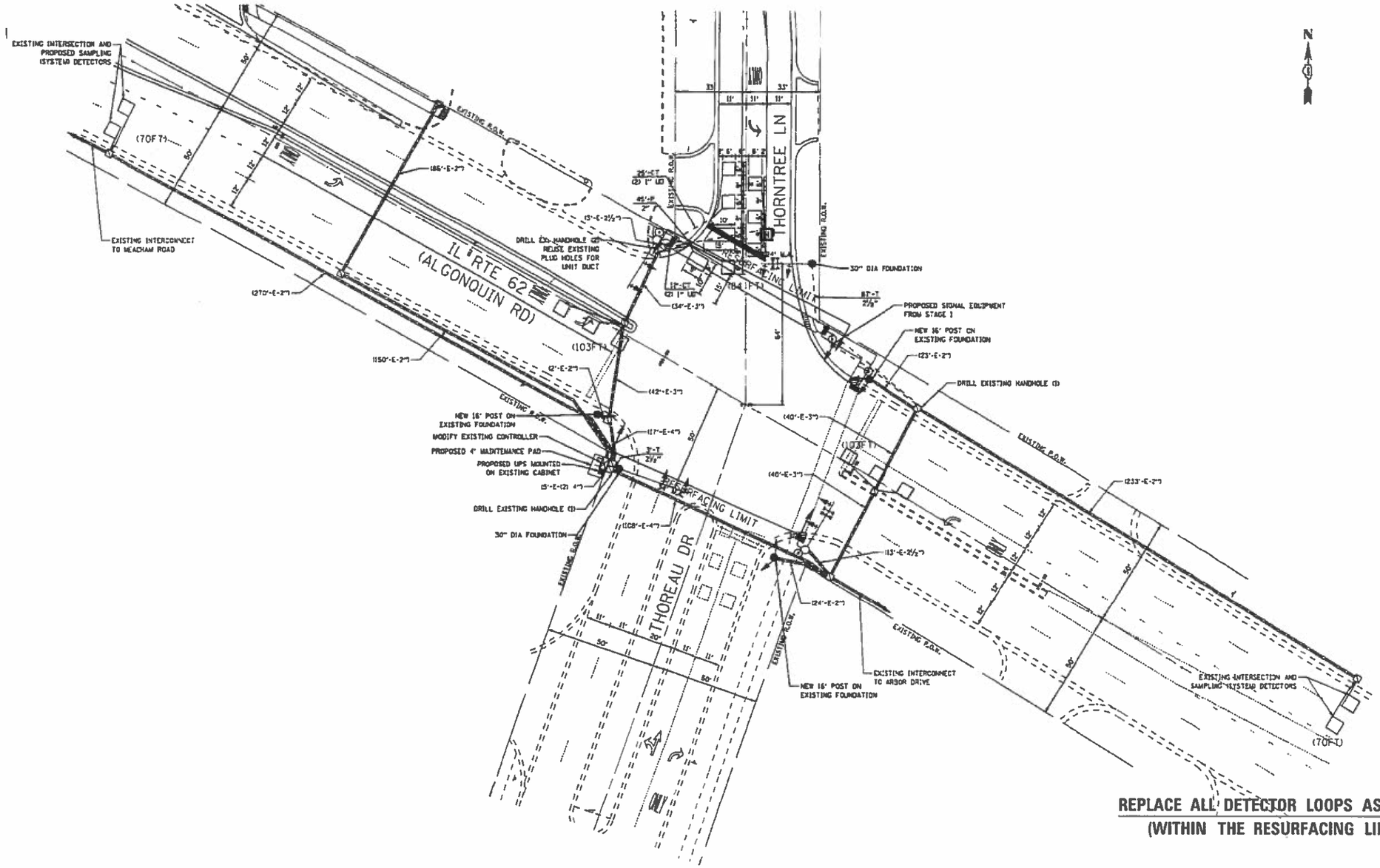
CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	330	FOOT

FILE NAME = S:\WP\Design\JC\Detector Loop Details\2016\62C93\DetectorLoopReplacementSheet_Blank_DRAWING6.dgn	USER NAME = curryja	DESIGNED - IP	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN IL 62 AT COMMUNICATIONS DR	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - IP	REVISED -			339	116Y(1&2)R-RS-6	COOK	58	40
Default	PLOT SCALE = 40.0000 ' / in.	CHECKED - LP	REVISED -					CONTRACT NO. 62C93		
	PLOT DATE = 1/25/2017	DATE - 8/15/2016	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT		

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS.

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	430	FOOT

FILE NAME = S:\MP\Design\JC\Detector Loop Details\2216\62C93\Detector-LoopReplacementSheet_Blank.dgn Default	USER NAME = currya	DESIGNED - IP	REVISED -
	PLOT SCALE = 48.0468 ' / in.	DRAWN - IP	REVISED -
	PLOT DATE = 9/21/2016	CHECKED - LP	REVISED -
		DATE - 8/15/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT PLAN
IL 62 AT THORNTREE /THOREAU

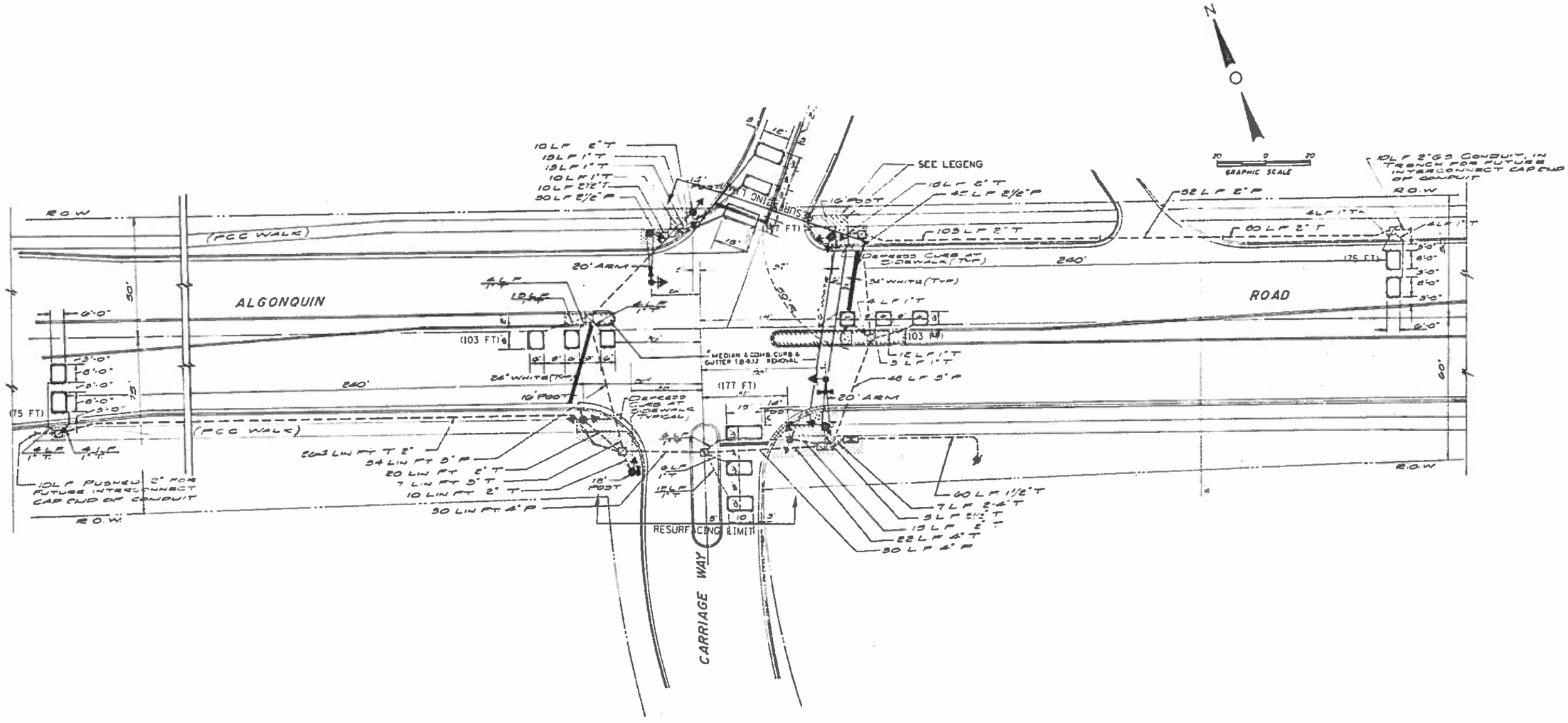
SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	41
		CONTRACT NO. 62C93		
	(ILLINOIS) FED. AID PROJECT			

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS.

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	710	FOOT

FILE NAME = S:\MP\Design\JC\Detector Loop Details\2016\62C93\Detector-LoopReplacementSheet_Blank_DRAWING.dgn	USER NAME = curryjc	DESIGNED - IP	REVISED -
	PLOT SCALE = 40.0460 ' / in.	DRAWN - IP	REVISED -
Default	PLOT DATE = 9/21/2016	CHECKED - LP	REVISED -
		DATE - 8/15/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT PLAN
IL 62 AT CARRIAGEWAY DR.

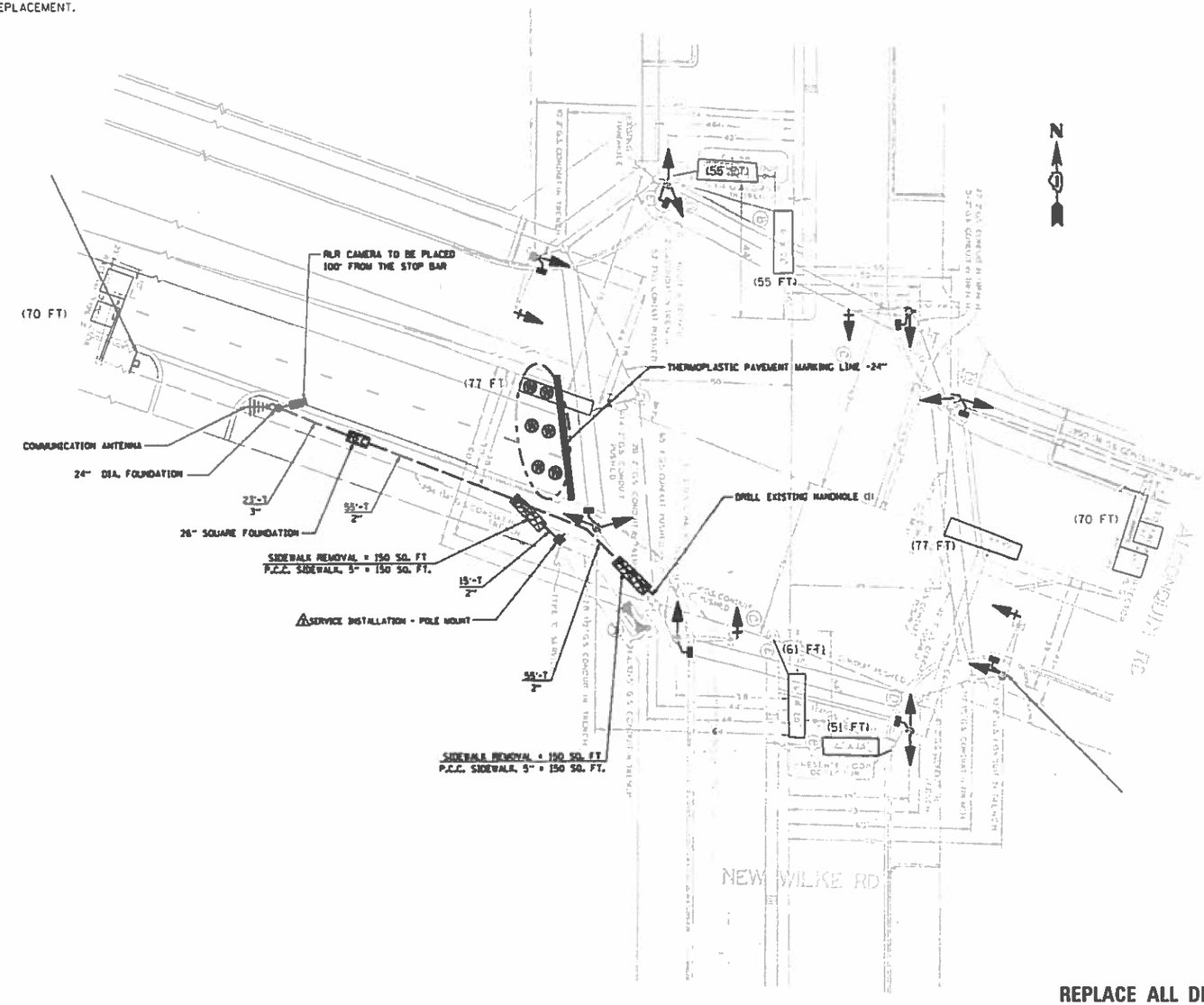
SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	42
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS.

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	516	FOOT

FILE NAME = S:\WP\Design\JC\Detector Loop Details\2016\62C93\DetectorLoopReplacementSheet.Blank.dgn Default	USER NAME = curryje	DESIGNED - JP	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
	PLOT SCALE = 40.0468 ' / in.	DRAWN - JP	REVISED -	
	PLOT DATE = 9/21/2016	CHECKED - LP	REVISED -	
		DATE - 8/15/2016	REVISED -	

DETECTOR LOOP REPLACEMENT PLAN
IL 62 AT NEW WILKE

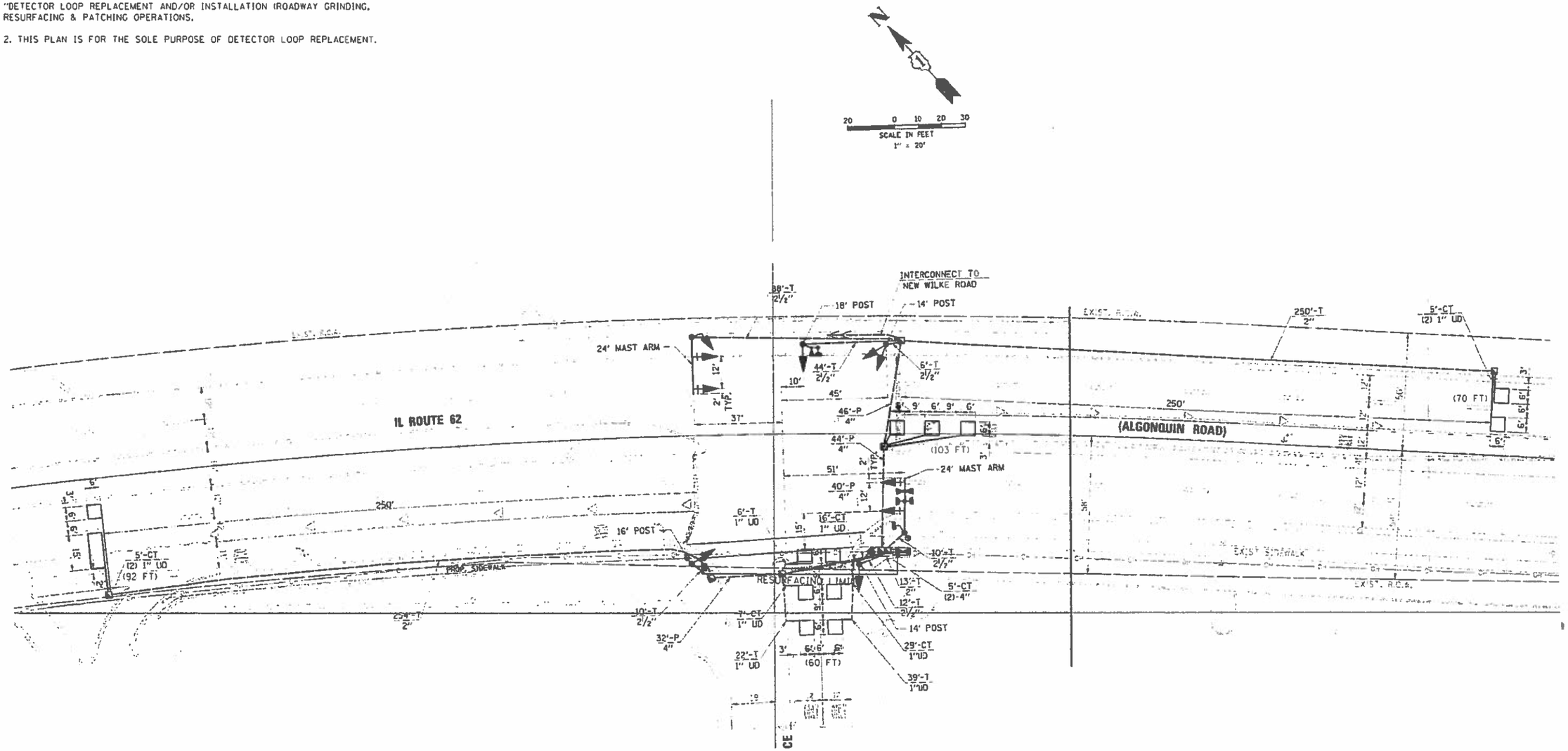
SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	43
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS.

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	265	FOOT

FILE NAME = S:\WP\Design\JC\Detector Loop Details\2016\62C93\DetectorLoopReplacementSheet_Blank.dgn	USER NAME = curry_e	DESIGNED - IP	REVISED -
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Default	PLOT DATE = 9/21/2016	CHECKED - LP	REVISED -
		DATE - 8/15/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT PLAN
IL 62 AT MARKET PLACE

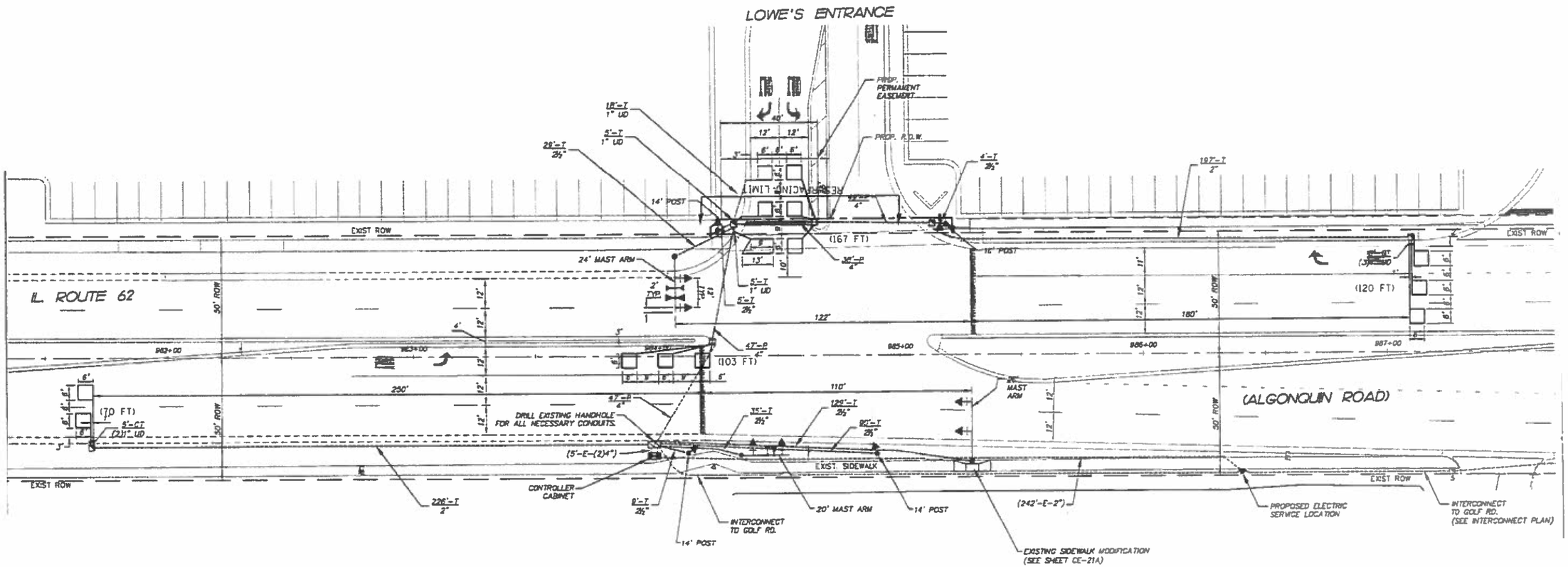
SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1)&2)R-RS-6	COOK	58	44
		CONTRACT NO. 62C93		
		ILLINOIS FED. AID PROJECT		

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS.

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	460	FOOT

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Default	PLOT DATE = 9/21/2016	DATE - 8/15/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT PLAN
IL 62 AT LOWES

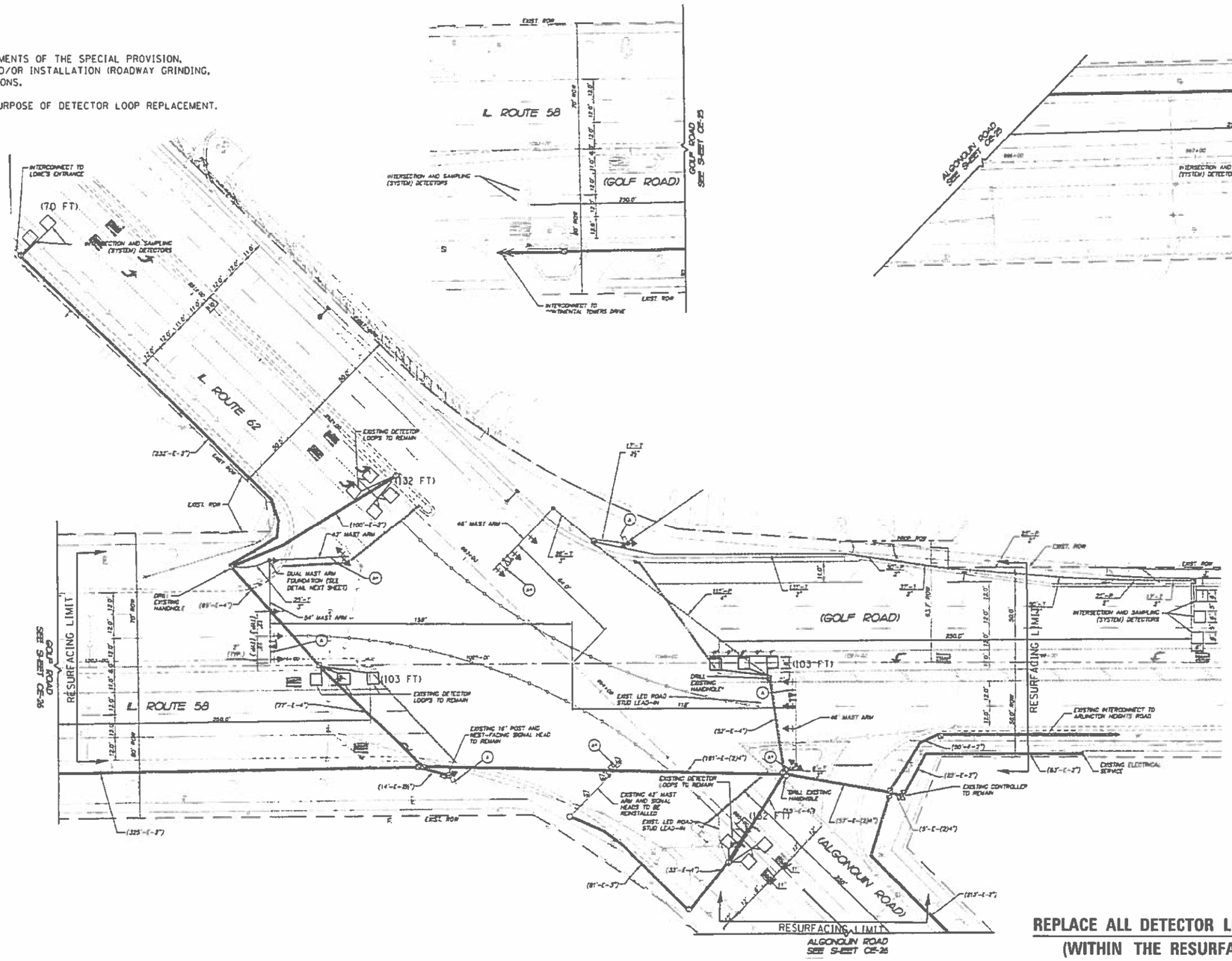
SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	45
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS.

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	540	FOOT

FILE NAME = S:\WP\Design\JC\Detector Loop Details\2016\62C93\DetectorLoopReplacementSheet_Blank_DRAWING.dgn	USER NAME = curryje	DESIGNED - IP	REVISED -
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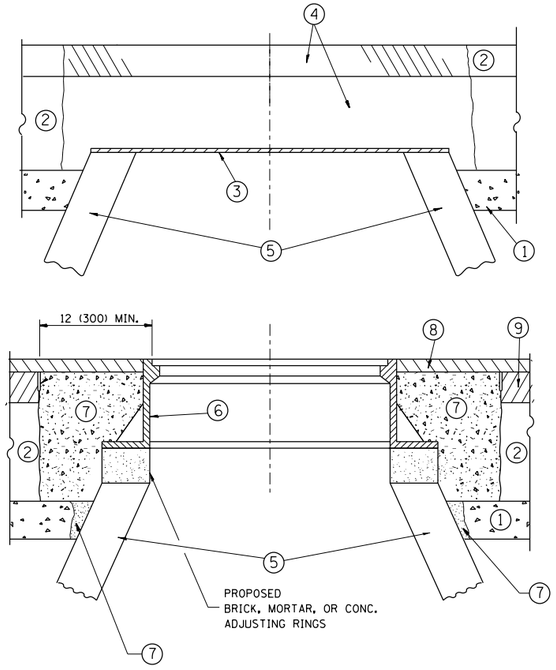
Default	PLOT DATE = 9/21/2016	DATE - 8/15/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT PLAN
IL 62 AT IL 58

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	46
		CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-1* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

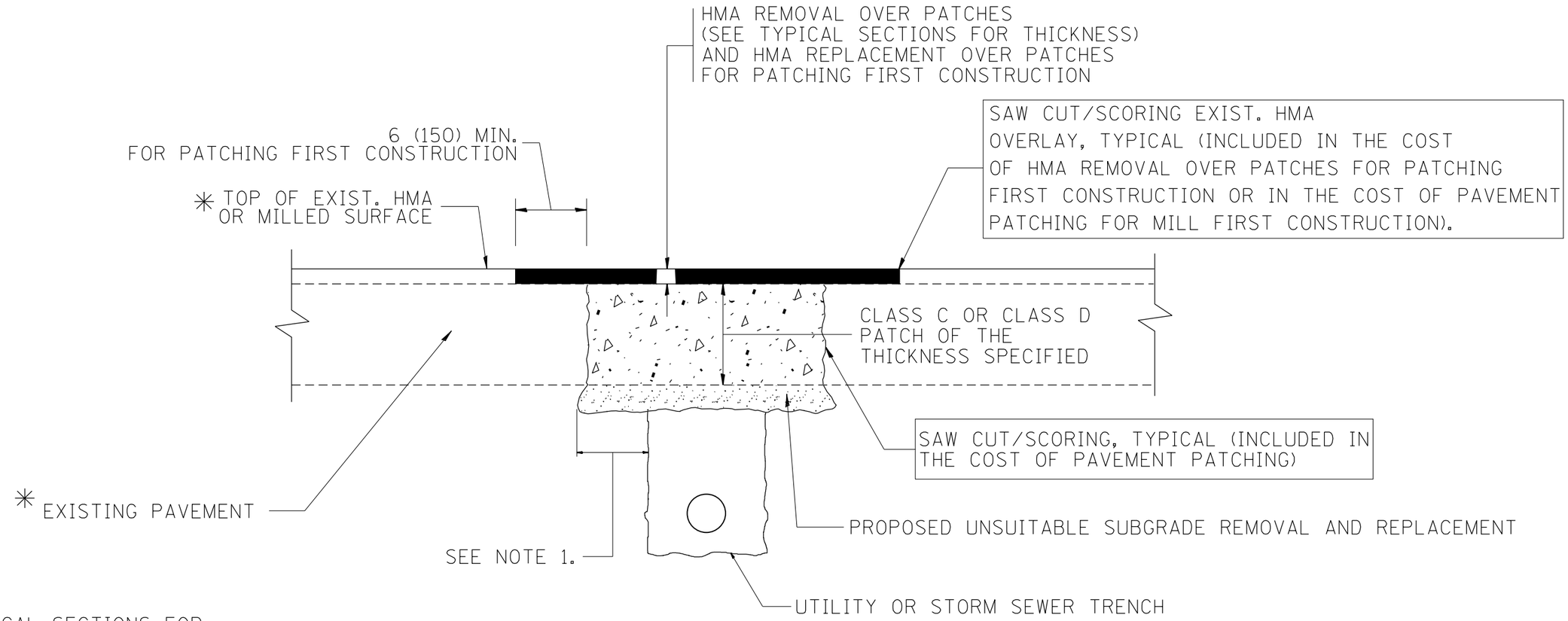
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\D141[illegible]\Data\Design\Diststd.dgn	USER NAME = PencePL	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 2/4/2017	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	47
	BD600-03 (BD-8)	CONTRACT NO. 62C93		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



✳ SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

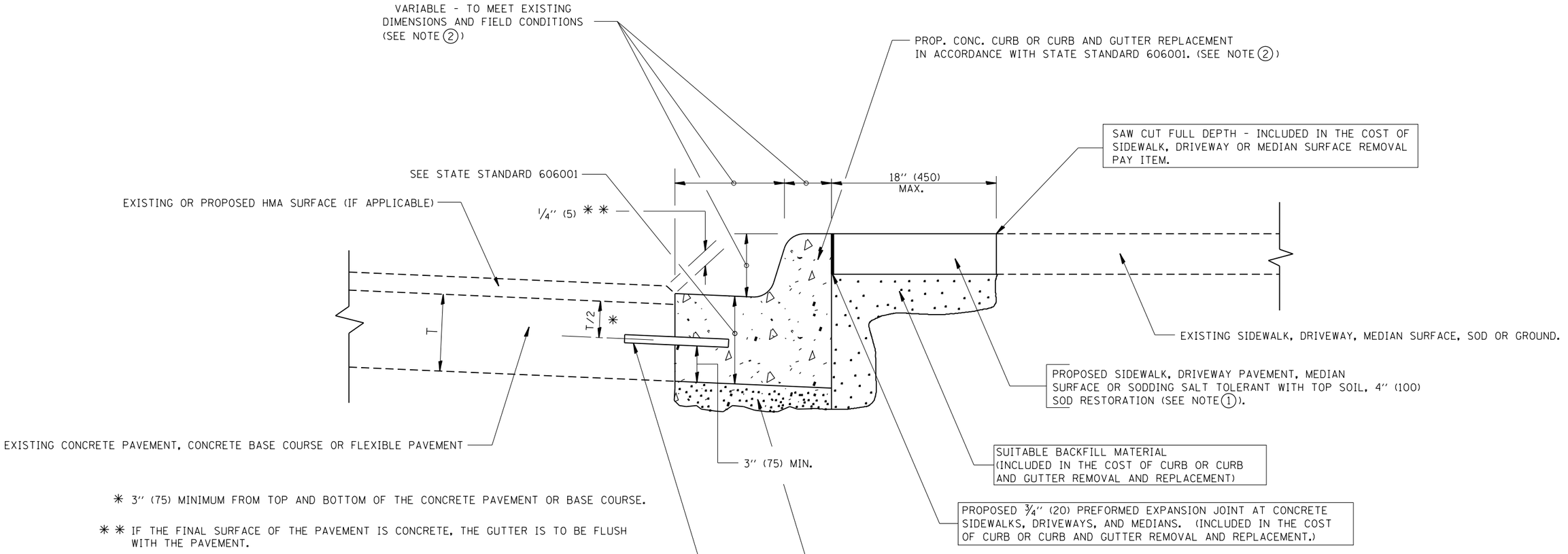
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\D141[illegible]\Design\Diststd.dgn	USER NAME = PencePL	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
	PLOT SCALE = 100.0000 ' / in.	DRAWN -	REVISED - R. BORO 01-01-07		
	PLOT DATE = 2/4/2017	CHECKED -	REVISED - R. BORO 09-04-07		SCALE: NONE / SHEET NO. 1 OF 1 SHEETS / STA. / TO STA.
		DATE - 10-25-94	REVISED - K. ENG 10-27-08		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	48
	BD400-04 (BD-22)	CONTRACT NO. 62C93		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

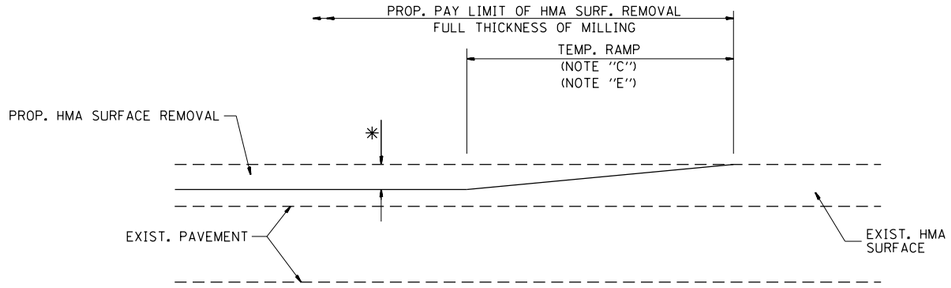
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

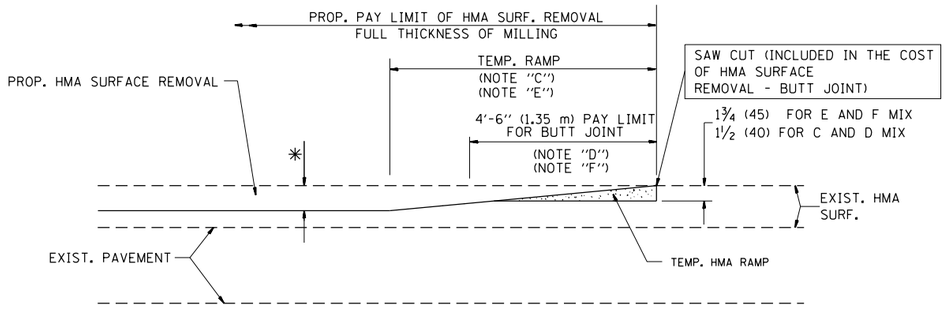
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\D141[illegible]\Design\Diststd.dgn	USER NAME = PencePL	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000 ' / in.	DRAWN -	REVISED - A. ABBAS 03-21-97				339	116Y(1&2)R-RS-6	COOK	58	49
	PLOT DATE = 2/4/2017	CHECKED -	REVISED - M. GOMEZ 01-22-01					BD600-06 (BD-24)		CONTRACT NO. 62C93	
		DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

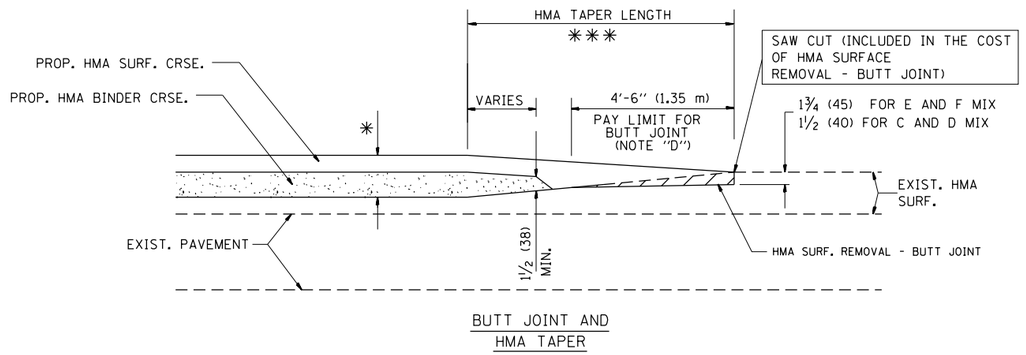


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

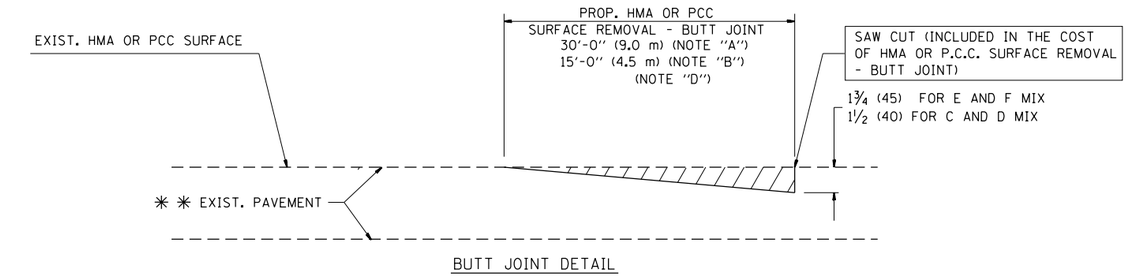
OPTION 2

TYPICAL TEMPORARY RAMP

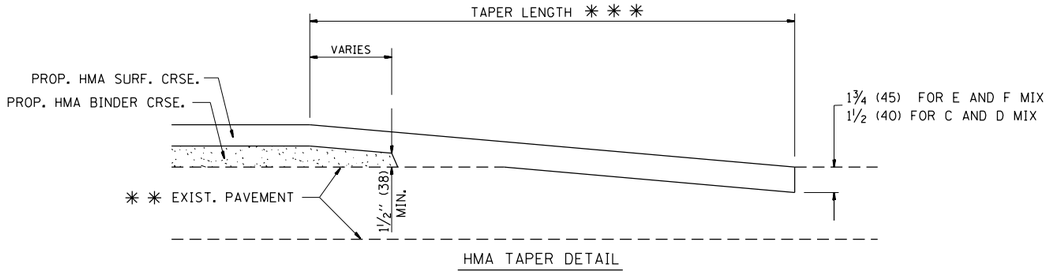


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

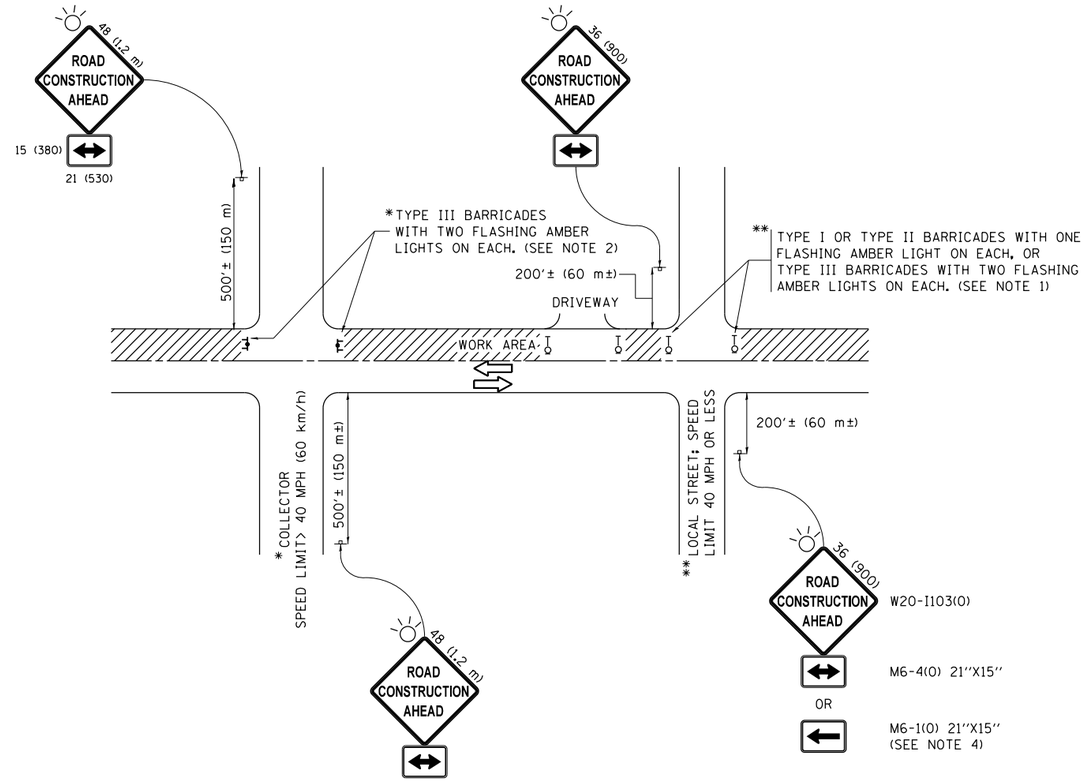
BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\D1413\Data\Design\Diststd.dgn	USER NAME = PencePL	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS
	PLOT SCALE = 100.0000 '/ in.	DRAWN -	REVISED - A. ABBAS 03-21-97		
	PLOT DATE = 2/4/2017	CHECKED -	REVISED - M. GOMEZ 04-06-01		
		DATE - 06-13-90	REVISED - R. BORO 01-01-07		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	50
	BD400-05 BD32	CONTRACT NO. 62C93		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ''ROAD CONSTRUCTION AHEAD'' SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ''ROAD CONSTRUCTION AHEAD'' SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

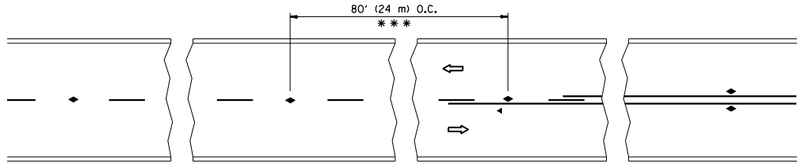
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\D141[illegible]\Data\Design\Diststd.dgn	USER NAME = PencePL	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
		DRAWN -	REVISED - T. RAMMACHER 01-06-00
	PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
Default	PLOT DATE = 2/4/2017	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

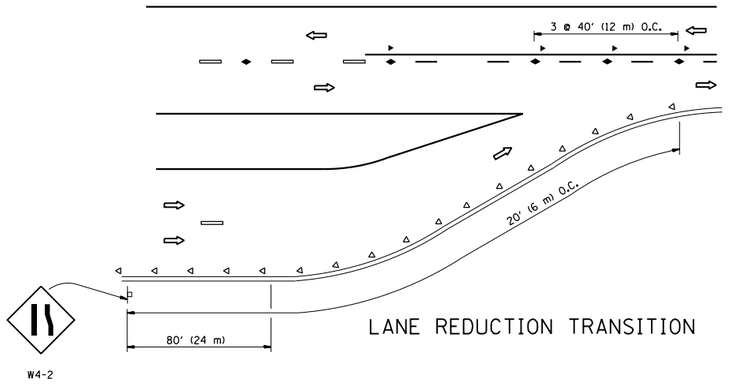
SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	51
	TC-10	CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

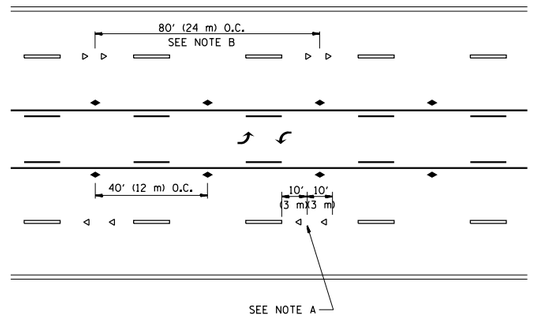


* * * REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

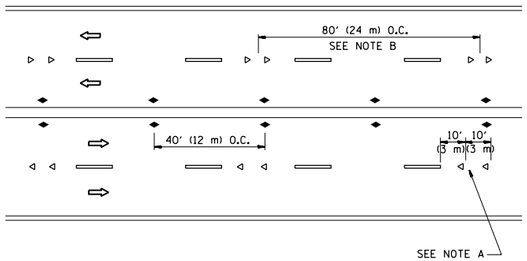
TWO-LANE/TWO-WAY



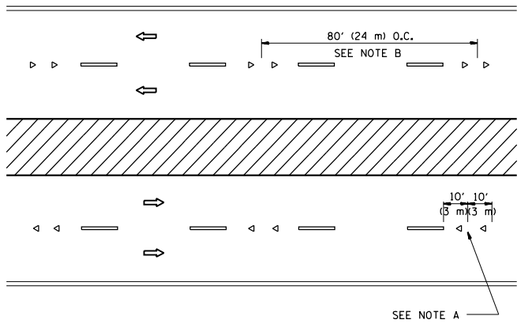
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

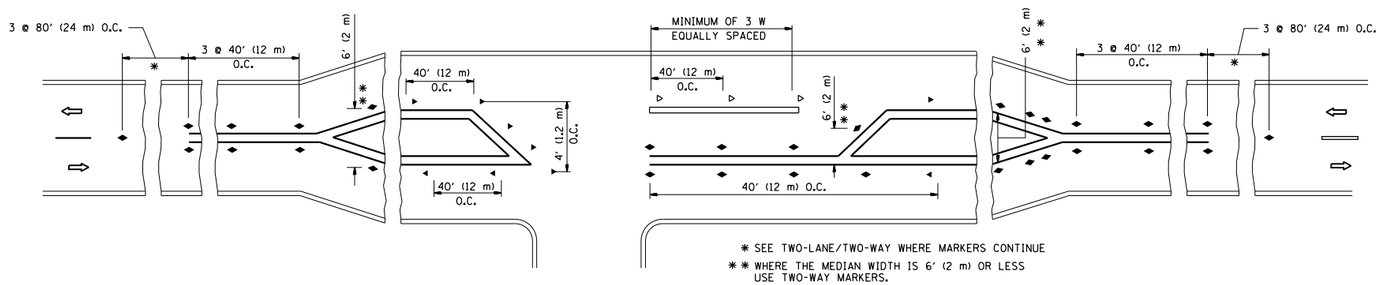
LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



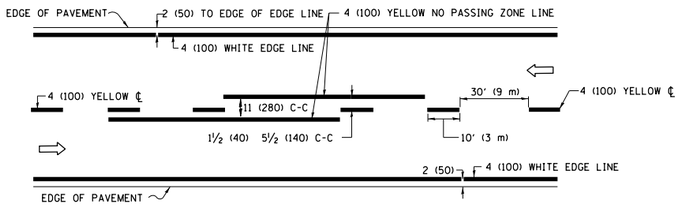
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

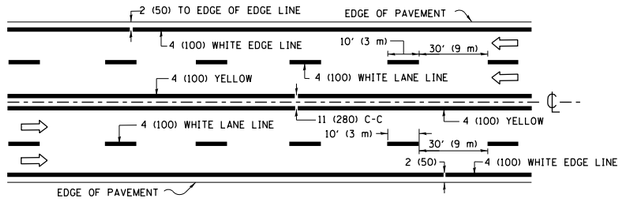
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

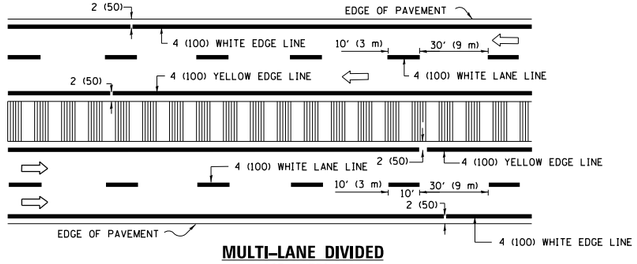
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\D141[illegible]\Data\Design\Diststd.dgn	USER NAME = PencePL	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - T. RAMMACHER 03-12-99			339	116Y(1&2)R-RS-6	COOK	58	52
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00				TC-11		CONTRACT NO. 62C93	
	PLOT DATE = 2/4/2017	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	



2–LANE ROADWAY

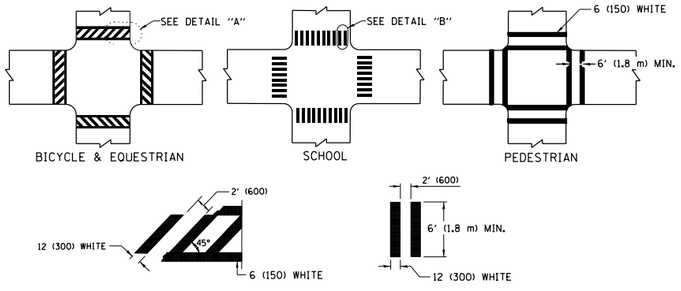


MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

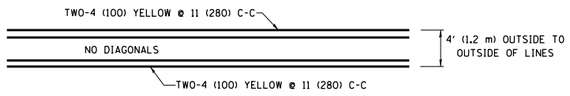
TYPICAL LANE AND EDGE LINE MARKING



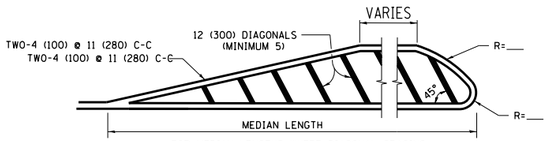
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



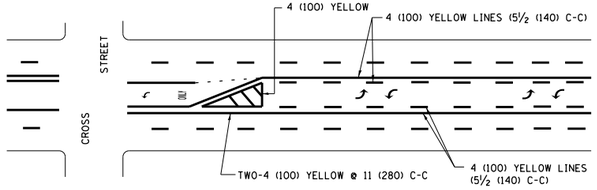
4' (1.2 m) WIDE MEDIANS ONLY



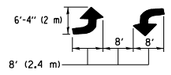
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

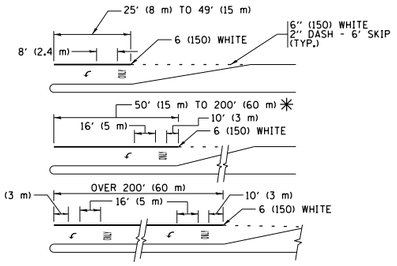


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

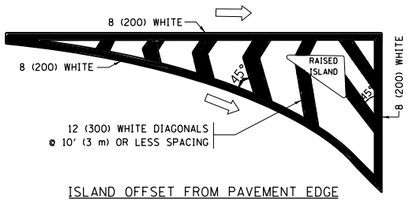


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

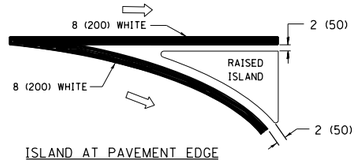
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

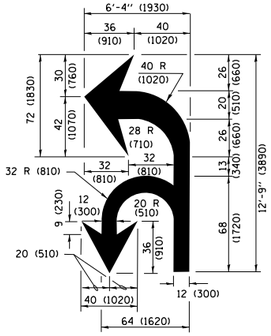


ISLAND OFFSET FROM PAVEMENT EDGE

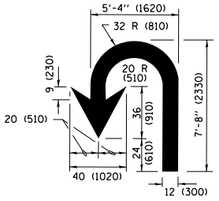


ISLAND AT PAVEMENT EDGE

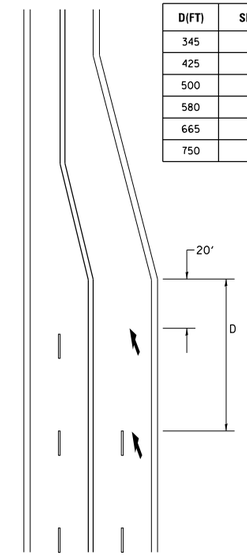
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ⌀ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ⌀ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ⌀ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ⌀ 6 (150) 12 (300) ⌀ 45° 12 (300) ⌀ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ⌀ 4 (100) WITH 12 (300) DIAGONALS ⌀ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ⌀ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) ⌀ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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Default	PLOT DATE = 2/4/2017	DRAWN - DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
			REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	53
	TC-13	CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

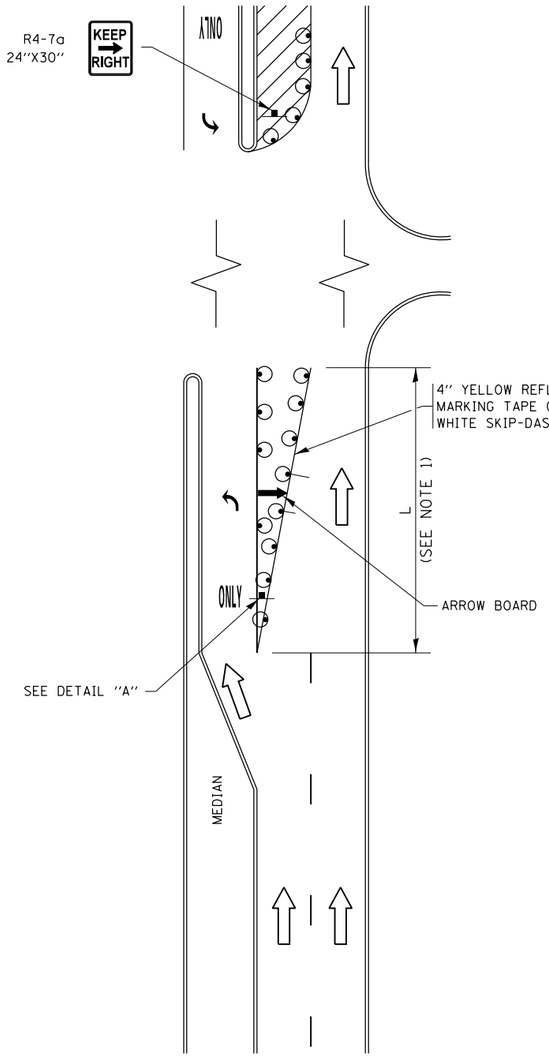


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.

   B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.

2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.

4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.

5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-110OR 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.

6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.

7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.

8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

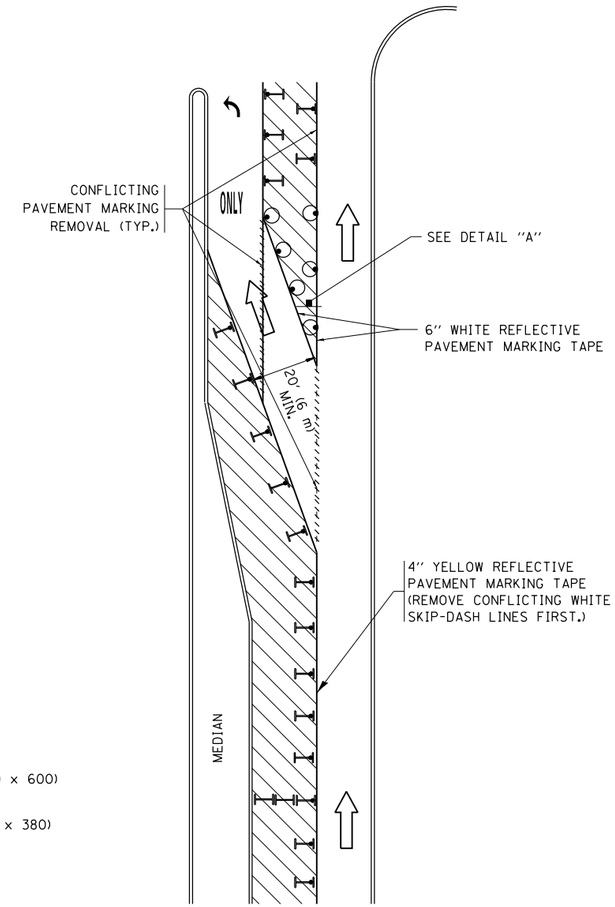
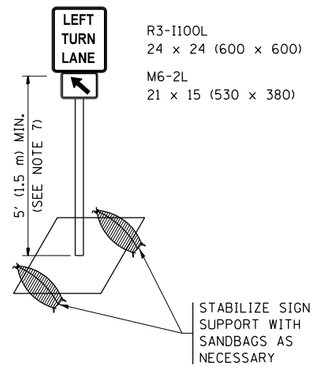


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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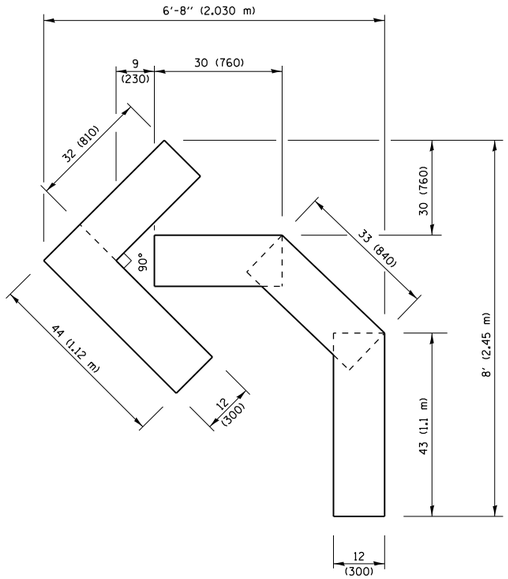
	REVISED - R. BORO 09-14-09	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 09-15-16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)

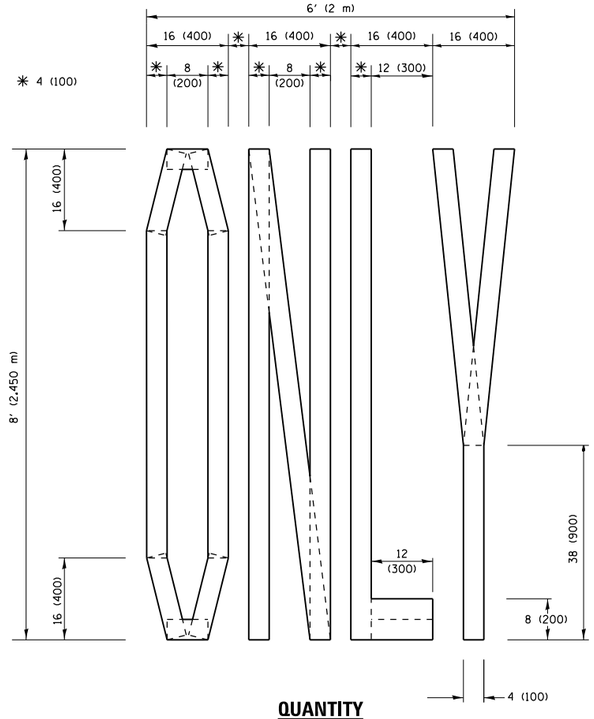
SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	54
	TC-14	CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			



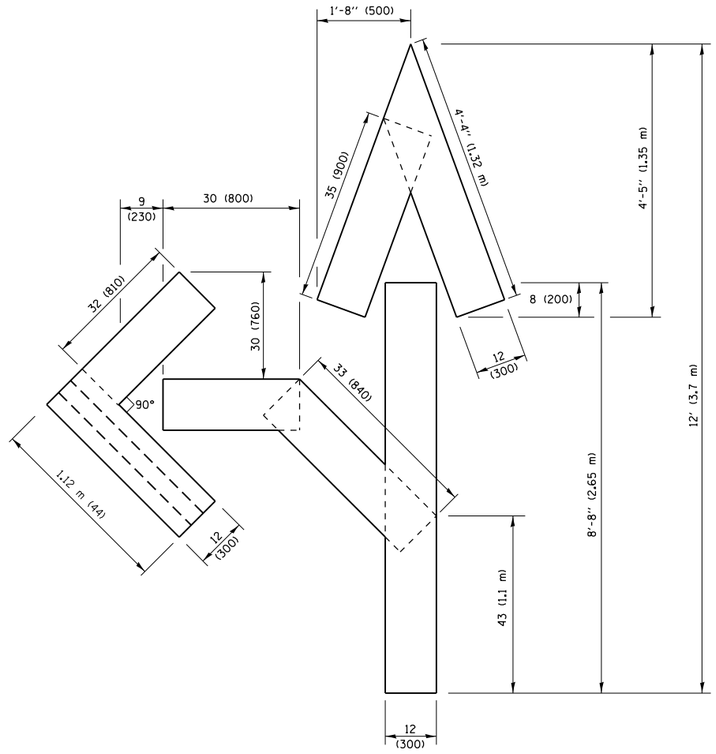
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

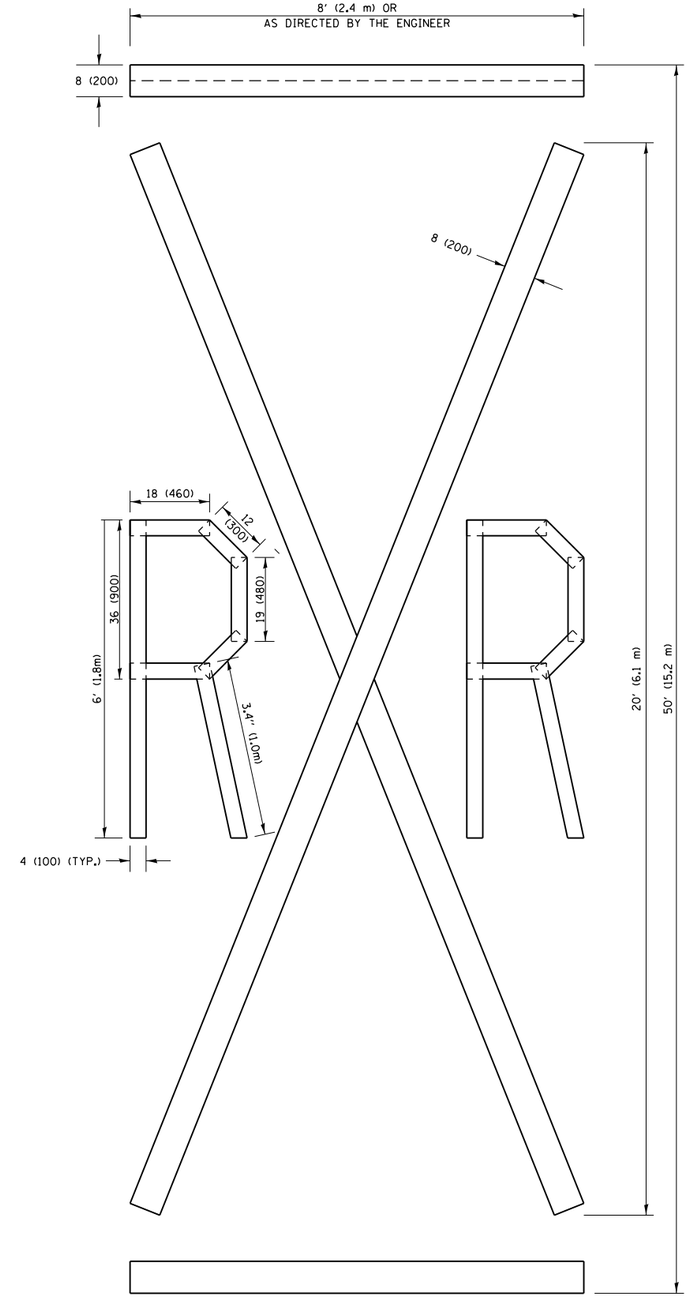


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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		DRAWN -	REVISED - E. GOMEZ 08-28-00
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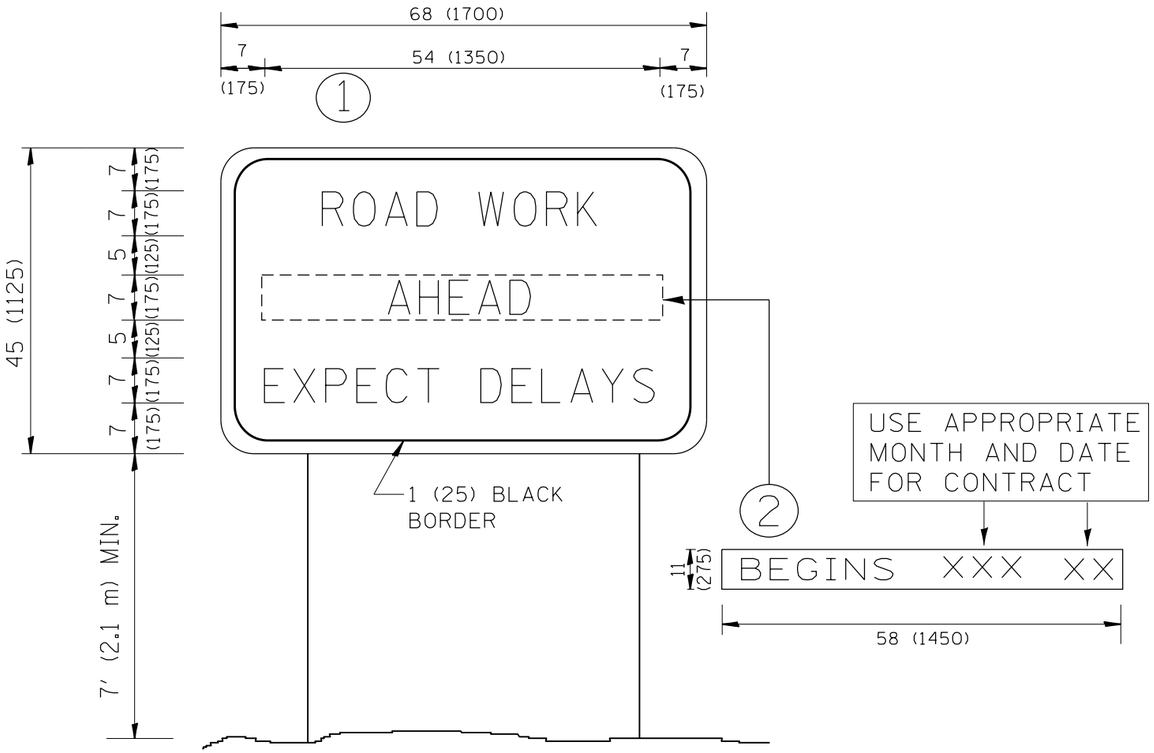
	PLOT DATE = 2/4/2017	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	55
	TC-16	CONTRACT NO. 62C93		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE ''ROAD CONSTRUCTION AHEAD'' SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR ''TEMPORARY INFORMATION SIGNING'' FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\D141[illegible]\Design\Diststd.dgn	USER NAME = PencePL	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
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	PLOT DATE = 2/4/2017	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	56
	TC-22	CONTRACT NO. 62C93		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

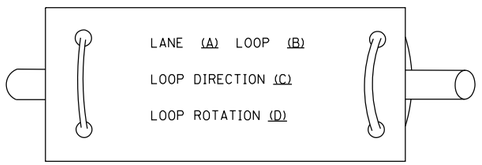
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

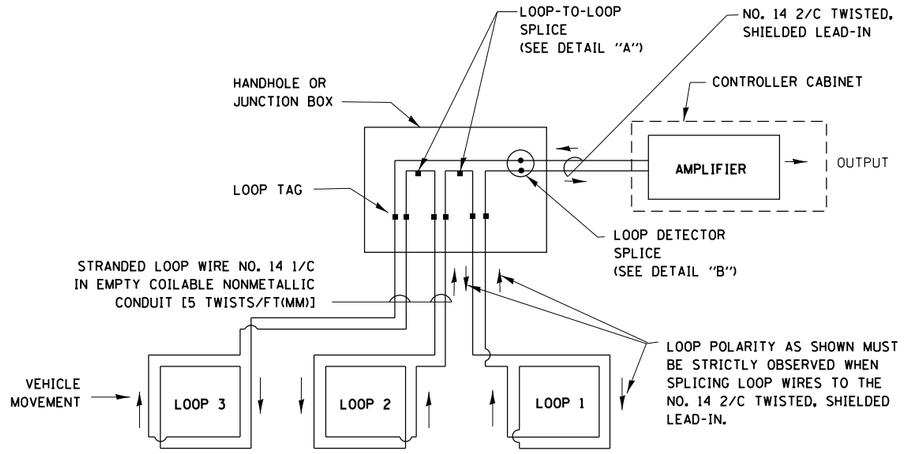


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

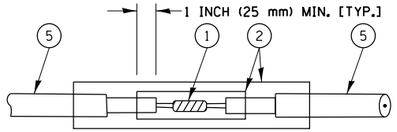
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

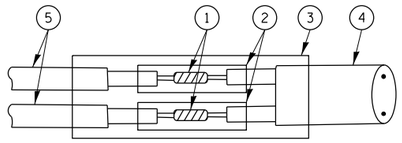


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

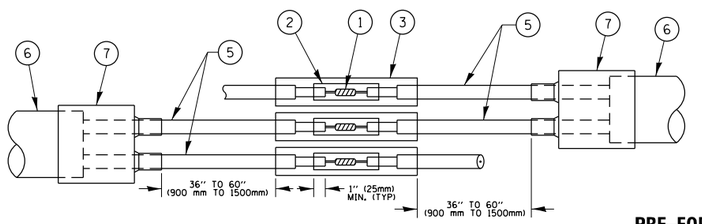


DETAIL "A"
LOOP-TO-LOOP SPLICE



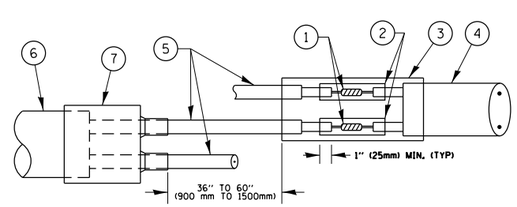
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

(6) PRE-FORMED LOOP

(7) XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

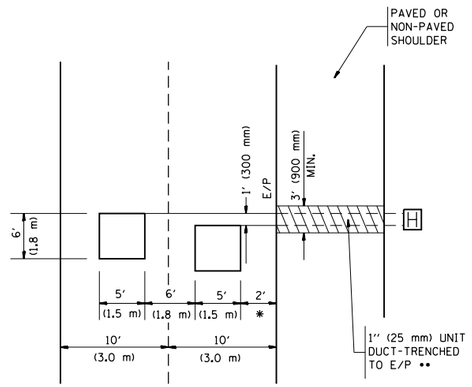
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 2 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	57
	TS-05	CONTRACT NO. 62C93		
	ILLINOIS FED. AID PROJECT			

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

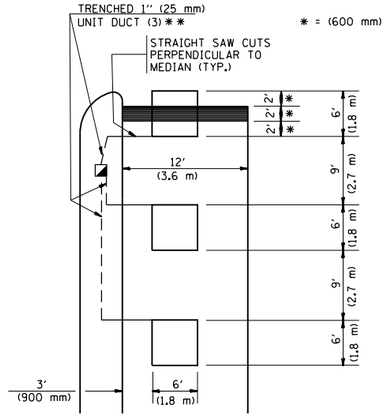


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

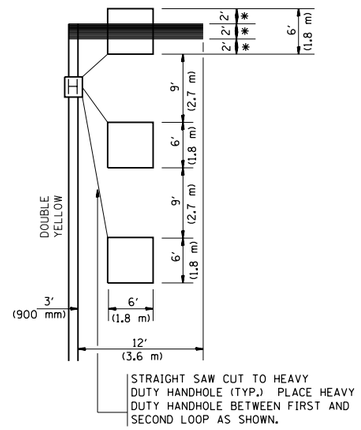


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

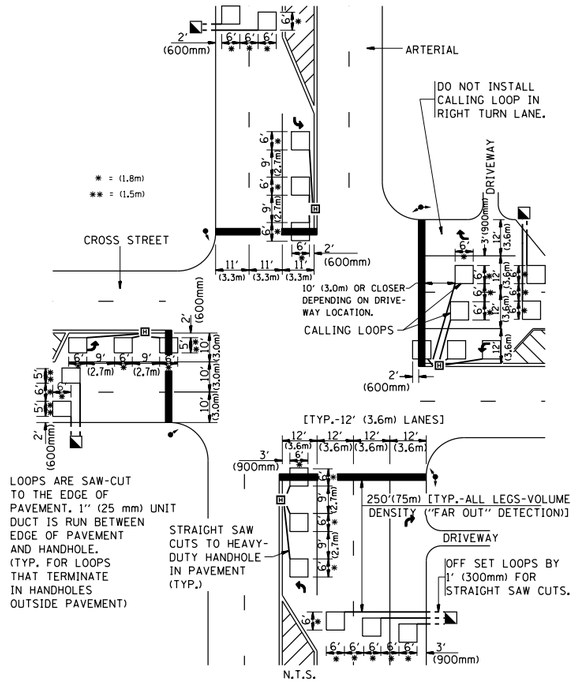
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

* = (600 mm)



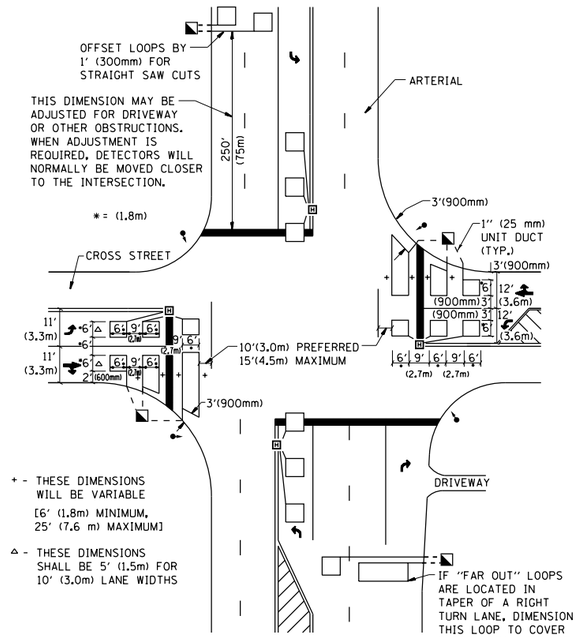
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
339	116Y(1&2)R-RS-6	COOK	58	58
	TS-07	CONTRACT NO. 62C93		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		