

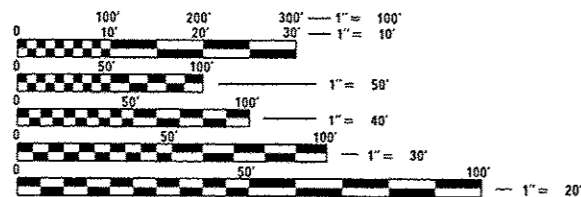
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	ILLINOIS DOT HIGHWAY STANDARDS

LIST OF ILLINOIS DOT HIGHWAY STANDARDS

000001-06	602306-03	701011-04
001001-02	602401-03	701201-04
280001-07	602601-04	701301-04
420001-08	602701-02	701311-03
420101-05	604011-05	701901-06
420106-05	604036-03	720001-01
420406	606001-06	720006-04
424001-09	630001-11	725001-01
515001-03	630301-07	728001-01
542301-03	631031-15	780001-05
542306-03	642001-02	781001-04
542401-02	666001-01	782006
601101-02	701001-02	BLR 21-9
602301-04	701006-05	

LIST OF DISTRICT 4 CADD STANDARDS

205001-D4
205101-D4
406101-D4
406301-D4
605001-D4
630101-D4
667101-D4
780001-D4



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: RICH DOTSON 309-671-3455
PROJECT MANAGER: KEVIN HORST 309-671-3472
CATALOG NO. 033802-00D
CONTRACT NO. 68800

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

04-28-2017 LETTING ITEM 090

DIVISION OF HIGHWAYS

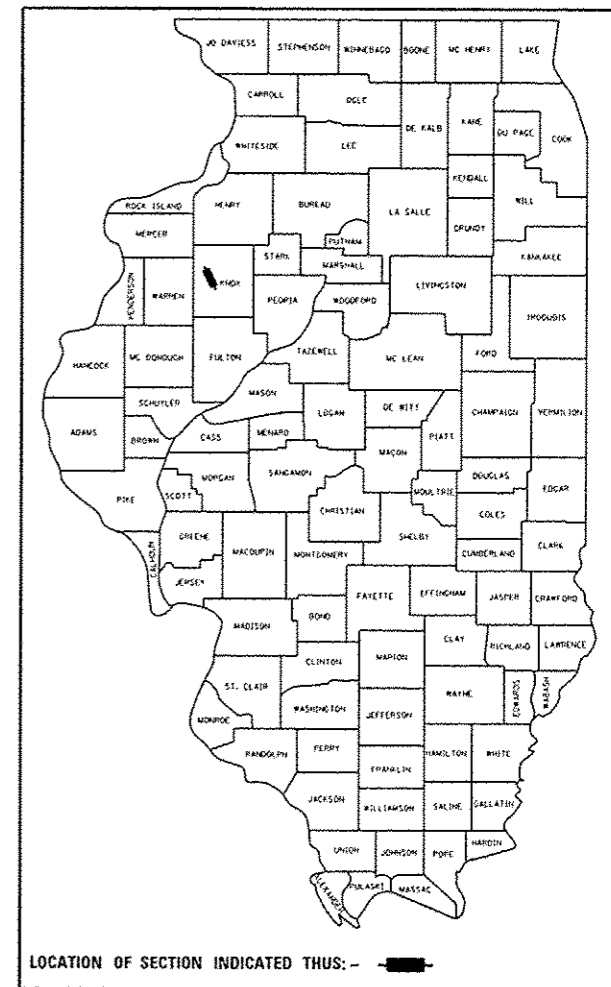
PROPOSED
HIGHWAY PLANS

FAS ROUTE 2401 (US 150) OVER BNSF RAILROAD
SECTION (40V-1)BR
PROJECT ACRS-2401(103)
KNOX COUNTY
C-94-057-08

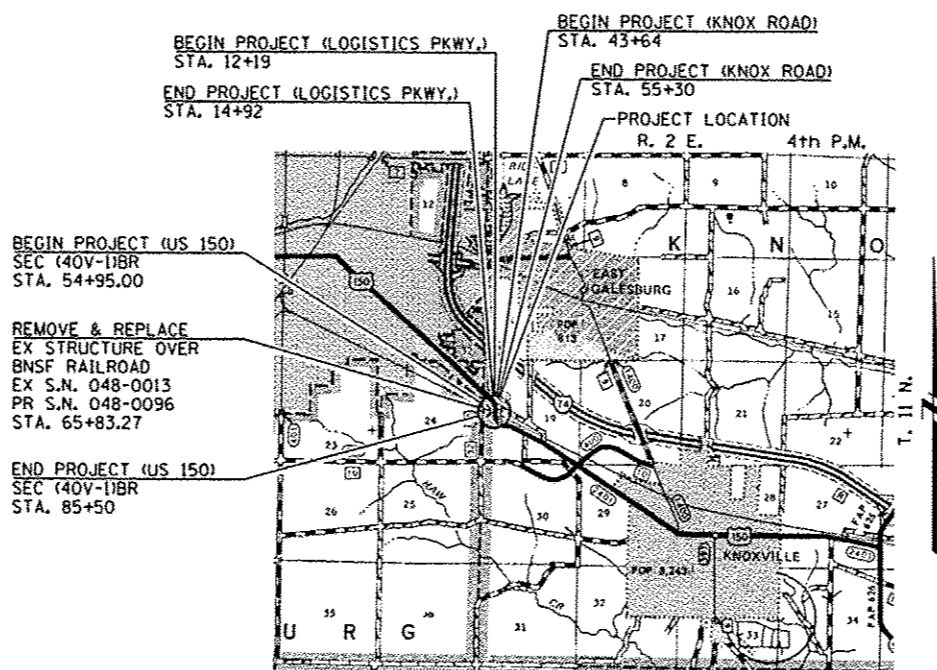
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	1

ILLINOIS CONTRACT NO. 68800

D-94-038-08



ADT = 4200 (2015)
% SU = 7.14 (2015)
% MU = 2.86 (2015)
TOWNSHIP: KNOX
FUNCTIONAL CLASSIFICATION: MINOR ARTERIAL



LOCATION MAP

NET LENGTH OF PROJECT = 3055.00 FEET = 0.579 MILES (US 150)
NET LENGTH OF PROJECT = 1166.00 FEET = 0.221 MILES (KNOX ROAD)
NET LENGTH OF PROJECT = 273.00 FEET = 0.052 MILES (LOGISTICS PKWY.)



Christopher P. Kohlman 11/30/16
EXPIRATION: 11/30/2017

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED December 8, 2016
Paul A. James
REGION THREE ENGINEER

Mar 24, 2017
Maureen M. Addis, PE
ENGINEER OF DESIGN AND ENVIRONMENT

Mar 24, 2017
Maureen M. Addis
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS



Veenstra & Kimm, Inc.
Springfield, IL. Phone: (217)544-8033
IL Design Firm No. 184-001939

COMMITMENTS

1. COMMITMENTS SHALL NOT BE ALTERED WITHOUT THE WRITTEN CONSENT OF ALL PARTIES TO WHICH THE COMMITMENT WAS MADE.
2. NO COMMITMENTS HAVE BEEN MADE ON THIS PROJECT.

I.D.O.T. DISTRICT 4 GENERAL NOTES

105.04 SOIL REPORT AVAILABILITY

THE SOILS REPORT AND ALL SOILS DATA COLLECTED AND PROCESSED IN CONJUNCTION WITH THE DESIGN THIS OF IMPROVEMENT IS ON FILE AT THE DISTRICT OFFICE WHERE IT IS AVAILABLE FOR INSPECTION BY CONTRACTORS OR PROSPECTIVE BIDDERS. BY SUBMITTING A BID, THE CONTRACTOR ACKNOWLEDGES THAT THE SOILS REPORT AND DATA HAVE BEEN MADE AVAILABLE, THAT THE CONTRACTOR IS AWARE OF THE REPORT CONTENTS AND APPENDICES, AND THAT THE SOILS REPORT IS PART OF THE CONTRACT DOCUMENTS.

105.06 AVAILABILITY OF ELECTRONIC FILES

MICRO STATION AND GEOPAK FILES OF THIS PROJECT WILL BE MADE AVAILABLE TO THE CONTRACTOR AFTER CONTRACT AWARD. IF THERE IS A CONFLICT BETWEEN THE ELECTRONIC FILES AND THE PRINTED CONTRACT PLANS AND DOCUMENTS, THE PRINTED CONTRACT PLANS AND DOCUMENTS SHALL TAKE PRECEDENCE OVER THE ELECTRONIC FILES. THE CONTRACTOR SHALL ACCEPT ALL RISK ASSOCIATED WITH USING THE ELECTRONIC FILES AND SHALL HOLD THE DEPARTMENT HARMLESS FOR ANY ERRORS OR OMISSIONS IN THE ELECTRONIC FILES AND THE DATA CONTAINED THEREIN. ERRORS OR DELAYS RESULTING FROM THE USE OF THE ELECTRONIC FILES BY THE CONTRACTOR SHALL NOT RESULT IN AN EXTENSION OF TIME FOR ANY INTERIM OR FINAL COMPLETION DATE OR SHALL NOT BE CONSIDERED CAUSE FOR ADDITIONAL COMPENSATION. THE CONTRACTOR SHALL NOT USE, SHARE, OR DISTRIBUTE THESE ELECTRONIC FILES EXCEPT FOR THE PURPOSE OF CONSTRUCTING THIS CONTRACT. ANY CLAIMS BY THIRD PARTIES DUE TO USE OR ERRORS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL INCLUDE THIS DISCLAIMER WITH THE TRANSFER OF THESE ELECTRONIC FILES TO ANY OTHER PARTIES AND SHALL INCLUDE APPROPRIATE LANGUAGE BINDING THEM TO SIMILAR RESPONSIBILITIES.

105.07 TREE REMOVAL – UTILITY RELOCATION

TREE REMOVAL MAY BE NECESSARY PRIOR TO UTILITY COMPANIES BEING ABLE TO RELOCATE THEIR FACILITIES OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR SHOULD COORDINATE ANY CONTRACT TREE REMOVAL ACTIVITIES WITH THE UTILITY COMPANIES TO ELIMINATE CONFLICTS AND POTENTIAL DELAYS CAUSED BY UTILITY TREE REMOVAL ACTIVITIES OR INCOMPLETE UTILITY RELOCATIONS.

105.09A PLAN ELEVATIONS – U. S. G. S. MEAN SEA LEVEL DATUM

ALL ELEVATIONS SHOWN ON THE PLANS ARE ESTABLISHED FROM U. S. G. S. MEAN SEA LEVEL DATUM.

107.09 PROPERTY OWNER ACCESS REQUIREMENTS

ACCESS MUST BE MAINTAINED TO ALL EXISTING PROPERTIES DURING CONSTRUCTION PER ARTICLE 107.09 UNLESS ARRANGEMENTS ARE MADE IN WRITING BY THE CONTRACTOR WITH THE PROPERTY OWNERS WITH A COPY TO THE ENGINEER FOR SHORT-TERM CLOSURES.

108.02 CRITICAL PATH WORK SCHEDULE REQUIREMENT

THE CONTRACTOR WILL SUBMIT TO THE ENGINEER A SATISFACTORY PROGRESS SCHEDULE AND CRITICAL PATH SCHEDULE WHICH SHALL SHOW THE PROPOSED SEQUENCE OF WORK AT THE TIME OF THE PRE-CONSTRUCTION CONFERENCE.

201.04 TREE REMOVAL

THE DISTRICT FOUR TREE COMMITTEE SHOULD BE CONTACTED AND PRIOR APPROVAL OBTAINED FOR ANY TREE REMOVAL BEYOND THE LIMITS /LOCATIONS INCLUDED IN THE PLANS.

204.00 ENVIRONMENTAL REVIEWS

PRIOR TO THE USE OF ANY PROPOSED BORROW AREAS, USE AREAS (TEMPORARY ACCESS ROADS, DETOURS, RUN-AROUNDS, ETC.) AND/OR WASTE AREAS, THE CONTRACTOR SHALL FILE THE REQUIRED ENVIRONMENTAL RESOURCE REQUEST SURVEYS ACCORDING TO SECTION 107.22 OF THE STANDARD SPECIFICATIONS. THESE SURVEYS ARE REQUIRED IN ORDER FOR THE DEPARTMENT TO CONDUCT CULTURAL AND BIOLOGICAL RESOURCE SURVEYS FOR THE PROPOSED SITE.

THE REQUIRED ENVIRONMENTAL RESOURCE DOCUMENTATION SHALL INCLUDE THE FOLLOWING:

- * BDE FORM 2289 (ENVIRONMENTAL SURVEY REQUEST)
- * BDE FORM 2290 (WASTE/USE AREA REVIEW)
- * A LOCATION MAP SHOWING THE SIZE LIMITS AND LOCATION OF THE USE AREA
- * COLOR PHOTOGRAPHS DEPICTING THE USE AREA
- * BORROW AREA ENTRY AGREEMENT FORM-D4 PI0101

PRIOR TO ANY WASTE MATERIALS BEING REMOVED FROM THE CONSTRUCTION SITE THE REQUIRED ENVIRONMENTAL RESOURCE SURVEYS SHALL BE OBTAINED AND FILED BY THE CONTRACTOR. EXCESS WASTE PRODUCTS REMOVED FROM THE CONSTRUCTION SITE SHALL BE DISPOSED OF AS REQUIRED IN SECTION 202.03 OF THE STANDARD SPECIFICATIONS.

ANY PROTRUDING METAL BARS SHALL BE REMOVED PRIOR TO THE DISPOSAL OF BROKEN CONCRETE AT APPROVED DISPOSAL SITES.

PLEASE NOTE THAT A MINIMUM OF FOUR WEEKS SHALL BE ALLOWED FOR THE DISTRICT TO OBTAIN THE REQUIRED WASTE SITE ENVIRONMENTAL CLEARANCES AND SIX WEEKS FOR THE REQUIRED BORROW SITE ENVIRONMENTAL CLEARANCES.

250.01 SEEDING – SIDE SLOPE RIPPING

ALL SLOPES STEEPER THAN 3 TO 1 AND OVER 15 FT (4.5 M) IN HEIGHT SHALL BE RIPPED. THIS SHALL CONSIST OF RIPPING BETWEEN 18 INCHES TO 24 INCHES (450 MM TO 600 MM) DEEP NORMAL TO THE SLOPE. THE INTERVAL OF RIPPING ALONG THE SLOPE SHALL BE 12 FT. (3.6 M). THIS WORK SHALL BE DONE AFTER THE SEED BED HAS BEEN PREPARED BUT BEFORE ANY FERTILIZER OR SEED HAS BEEN APPLIED. THE FERTILIZER AND SEED SHALL BE APPLIED WITHIN A 24-HOUR PERIOD AFTER THE RIPPING HAS BEEN DONE. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE VARIOUS ITEMS OF SEEDING INVOLVED.

406.03 PAVEMENT STATIONING NUMBERS & PLACEMENT

THE CONTRACTOR SHALL PROVIDE LABOR AND MATERIALS REQUIRED TO IMPRINT PAVEMENT STATION NUMBERS IN THE FINISHED SURFACE OF THE PAVEMENT AND/OR OVERLAY. THE NUMBERS SHALL BE APPROXIMATELY 3/4 INCH (20MM) WIDE, 5 INCHES (125 MM) HIGH AND 5/8 INCH (15 MM) DEEP.

THE PAVEMENT STATION NUMBERS SHALL BE INSTALLED AS SPECIFIED HEREIN:

INTERVAL – 200 FEET (ENGLISH STATIONING) OR 100 METERS (METRIC STATIONING)

BOTTOM OF NUMBERS – 6 INCHES (150 MM) FROM THE INSIDE EDGE OF THE PAVEMENT MARKING

LOCATION:

- * 2,3, & 5 LANE PAVEMENTS – RIGHT EDGE OF PAVEMENT IN DIRECTION OF INCREASING STATIONS
- * MULTI-LANE DIVIDED ROADWAYS – OUTSIDE EDGE OF PAVEMENT IN BOTH DIRECTIONS
- * RAMPS – ALONG BASELINE EDGE OF PAVEMENT

POSITION – STATIONS SHALL BE PLACED SO THEY CAN BE READ FROM THE ADJACENT SHOULDER

FORMAT – ENGLISH (METRIC) PAVEMENT STATIONS SHALL USE THIS FORMAT "XXX (XX+X00)" WHERE X REPRESENTS THE PAVEMENT STATION

THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED INCLUDED IN THE COST OF THE ASSOCIATED PAVEMENT AND/OR OVERLAY PAY ITEMS.



USER NAME =	DESIGNED -	REVISED -
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PLOT DATE =	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, COMMITMENTS &
PROJECT SPECIFIC NOTES**

SCALE: 1" = 50' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	2
CONTRACT NO. 68800				
<small>ILLINOIS FED. AID PROJECT</small>				

- 406.18 BUTT JOINT CUTTING TIME RESTRICTION
BUTT JOINTS SHALL NOT BE MILLED MORE THAN THREE (3) DAYS PRIOR TO PLACEMENT OF THE HMA SURFACE COURSE.
- 406.19 PAVING SURFACE COURSE
CONTINUOUS PAVING OPERATIONS ON THE MAIN ROADWAY SHALL BE MAINTAINED AT ALL TIMES DURING THE CONSTRUCTION OF THE HOT-MIX ASPHALT SURFACE. NO INTERRUPTIONS FOR SIDE ROADS, ENTRANCES, TURN LANES, ETC. WILL BE ALLOWED.
- 542.00 ORDERING LENGTH CONFIRMATION – DRAINAGE ITEMS
THE CONTRACTOR SHALL CONSULT WITH THE ENGINEER IN REGARD TO THE EXACT LENGTH OF THE BOX/PIPE CULVERTS, STORM SEWERS, AND/OR PIPE DRAINS REQUIRED PRIOR TO ORDERING THESE ITEMS.
- 602.00 EXISTING DRAINAGE PIPES CONNECTED TO NEW STRUCTURES
IN ACCORDANCE WITH SECTION 602 OF THE STANDARD SPECIFICATIONS, THE CONNECTING OF EXISTING DRAIN TILES, PIPE CULVERTS, OR STORM SEWERS TO THE PROPOSED DRAINAGE SYSTEM STRUCTURES WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE PAY ITEMS PROVIDED.
- 666.00 RIGHT-OF-WAY MARKERS
WHEN INSTALLING RIGHT-OF-WAY MARKERS, CARE SHALL BE TAKEN TO NOT DISTURB ANY EXISTING PROPERTY/RIGHT-OF-WAY PINS. IF A PROPERTY/RIGHT-OF-WAY PIN IS FOUND AT THE LOCATION OF A PROPOSED RIGHT-OF-WAY MARKER, THE MARKER SHALL BE PLACED ONE (1) FOOT IN FRONT OF THE PIN.
- 670.00 ENGINEERS FIELD OFFICE
ALL OF THE TELEPHONE LINES PROVIDED SHALL HAVE UNPUBLISHED NUMBERS.
- 720.00 SIGNING
SIGN LOCATIONS MAY VARY FROM THE STATIONS SHOWN ON THE PLANS IN ACCORDANCE WITH DIRECTIONS FROM THE ENGINEER AT THE TIME OF CONSTRUCTION. SIGN LOCATIONS MAY BE ADJUSTED IN THE FIELD TO AVOID ANY FOUND UTILITIES.
ALL WOOD POST LOCATIONS SHALL BE VERIFIED WITH THE BUREAU OF OPERATIONS, TRAFFIC SECTION, BEFORE INSTALLATION.
- 780.00 NO PASSING ZONE VERIFICATION
THE RESIDENT SHALL CONTACT OPERATIONS TO VERIFY THE LOCATION OF NO PASSING ZONES PRIOR TO PLACEMENT OF CENTERLINE STRIPING.

PROJECT SPECIFIC NOTES

- WHERE TREE REMOVAL CONFLICTS WITH EXISTING UNDERGROUND UTILITIES, THE CONTRACTOR SHALL CUT THE TREE OFF AT THE GROUND LINE AND GRIND THE STUMP AS DIRECTED BY THE ENGINEER.
- THE THICKNESS OF THE HOT MIX ASPHALT MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT MIXED ASPHALT MIXTURES ARE PLACED.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO ORDERING MATERIALS AND COMMENCING CONSTRUCTION.
- THE ENGINEER SHALL CONFIRM TREE LOCATIONS WITH THE DISTRICT FOUR LANDSCAPE ARCHITECT. PROPOSED TREES SHALL BE PLANTED AT LEAST 50 FEET AWAY FROM THE U.S. ROUTE 150 EDGE OF PAVEMENT.
- SHOULDER RUMBLE STRIPS, 16 INCH SHALL BE CONSTRUCTED WITH THE INNER EDGE OF THE RUMBLE STRIPS 8 INCHES OUTSIDE THE EDGE OF PAVEMENT.
- THE EXISTING PAVEMENT IS 13" THICK.

CALCULATION FACTORS

AGGREGATE SHOULDERS AND BASES: 0.057 TONS /SQ YD /INCH
HOT MIX ASPHALT: 0.056 TONS /SQ YD /INCH
NITROGEN FERTILIZER NUTRIENT: 90 LBS /ACRE
PHOSPHORUS FERTILIZER NUTRIENT: 90 LBS /ACRE
POTASSIUM FERTILIZER NUTRIENT: 90 LBS /ACRE
TEMPORARY EROSION CONTROL SEEDING: 100 LBS /ACRE
STONE DUMPED RIPRAP: 1.5 TONS /CU YD

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE USE(S):	POLYMER SURFACE 2"	TOP POLYMER BINDER 2.25"	LOWER BINDER LIFTS (4" AND 2.25") & VARIABLE DEPTH BINDER	SHOULDERS (SURFACE LIFT)	SHOULDERS (LOWER LIFTS)
AC/PG:	SBS OR SBR 64-28	SBS OR SBR 64-28	PG 64-22	PG 64-22	PG 64-22
DESIGN AIR VOIDS:	4.0% AT N=50	4.0% AT N=50	4.0% AT N=50	4.0% AT N=50	4.0% AT N=50
MIXTURE COMPOSITION: (MIXTURE GRADATION)	IL 9.5	IL 19.0	IL 19.0	IL 9.5FG	IL 19.0
FRICTION AGGREGATE:	MIX D	N.A.	N.A.	MIX C	N.A.
QUALITY MANAGEMENT PROGRAM:	QCP	QCP	QCP	QCQA	QCQA

- NOTE: 1) INDIVIDUAL LIFT THICKNESS OF EACH MIX TYPE WILL BE NO LESS THAT 3 TIMES NOMINAL MAXIMUM AGGREGATE SIZE AND NO MORE THAN 6 TIMES NOMINAL MAXIMUM AGGREGATE SIZE, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
2) FOR DESIGN PURPOSES, MIXTURE WEIGHT FOR ALL MIXES IS DETERMINED TO BE 112.0 LBS./Y./IN. UNLESS OTHERWISE NOTED.
3) SUBLOT SIZES FOR THE FULL DEPTH QCP MIXES WILL BE: ONE SUBLOT FOR POLYMER SURFACE, ONE SUBLOT FOR TOP POLYMER BINDER, AND 700 TONS PER SUBLOT FOR LOWER BINDER LIFTS, UNLESS OTHERWISE AGREED TO BY THE ENGINEER AND THE PAVING CONTRACTOR. THE COMPOSITE FULL DEPTH PAY FACTOR WILL BE BASED ON THE AVERAGE OF EACH INDIVIDUAL MIX TYPE PAY FACTOR.



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, COMMITMENTS &
PROJECT SPECIFIC NOTES

SCALE: 1" = 50' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68800	



Status of Utilities

Name of Utility Company
Century Link

Route FAS 2401 (US 150)
Section (40V-1)BR
County Knox
Contract No. 68800
Catalog No. 033802-00D

Route	Offset	Location	Type of Utility	Type of Conflict	Disposition
US 150	30' RT	Sta 55+00	Fiber Optic	Ditch cut (too shallow)	Relocate
US 150	29' RT	Sta 56+00	Fiber Optic	Ditch cut (too shallow)	Relocate
US 150	26' RT	Sta 57+00	Fiber Optic	Ditch cut (too shallow)	Relocate
US 150	13' RT	Sta 58+00	Fiber Optic	under new pavement	Relocate
US 150	10' LT	Sta 59+00	Fiber Optic	under new pavement	Relocate
US 150	44' LT	Sta 66+00	Fiber Optic	under new embankment	Relocate
US 150	93' LT	Sta 61+00	Fiber Optic	new embankment - toe of slope too shallow	Relocate



Status of Utilities

Name of Utility Company
Ameren - Electric

Route FAS 2401 (US 150)
Section (40V-1)BR
County Knox
Contract No. 68800
Catalog No. 033802-00D

Route	Offset	Location	Type of Utility	Type of Conflict	Disposition
US 150	30' LT	Sta 70+20±	Electric Pole	Embankment	Relocate
US 150	16' RT	Sta 82+28±	Electric Pole	Ditch	Relocate



Status of Utilities

Name of Utility Company
Ameren - Gas

Route FAS 2401 (US 150)
Section (40V-1)BR
County Knox
Contract No. 68800
Catalog No. 033802-00D

Route	Offset	Location	Type of Utility	Type of Conflict	Disposition
US 150	Variable	Sta 55+00 to 67+50	Gas	Embankment/Ditch	Relocate
US 150	Variable	Sta 75+50 to 84+50	Gas	Embankment/Ditch	Relocate



Status of Utilities

Name of Utility Company _____

City of Galesburg

Route FAS 2401 (US 150)
 Section (40V-1)BR
 County Knox
 Contract No. 68800
 Catalog No. 033802-00D

Route	Offset	Location	Type of Utility	Type of Conflict	Disposition
US 150	Variable	Sta 74+00 to 85+50	Water Main	New pavement embankment ditch cut	Relocate



Status of Utilities

Name of Utility Company _____

Galesburg Sanitary District

Route FAS 2401 (US 150)
 Section (40V-1)BR
 County Knox
 Contract No. 68800
 Catalog No. 033802-00D

Route	Offset	Location	Type of Utility	Type of Conflict	Disposition
US 150	28' LT	Sta 55+80±	Sanitary Manhole	Ditch/grading	Caution
US 150	55' LT	Sta 58+70	Sanitary	Ditch/grading	Caution

FILE NAME = Status of Utilities Sheets (2).dgn	USER NAME = diazoa	DESIGNED -	REVISED -
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	PLOT DATE = 12/7/2016	CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

STATUS OF UTILITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	5
CONTRACT NO. 68800			ILLINOIS FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE	100% STATE MOWING
				0004	0011	0004
					S.N. 048-0096	
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	12	12		
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	116	116		
20100500	TREE REMOVAL, ACRES	ACRE	1.9	1.9		
20200100	EARTH EXCAVATION	CU YD	16932	16932		
20400800	FURNISHED EXCAVATION	CU YD	56696	56696		
20800150	TRENCH BACKFILL	CU YD	160	160		
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	13175	13175		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	44094	44094		
* 25000210	SEEDING, CLASS 2A	ACRE	4.4	4.4		
* 25000300	SEEDING, CLASS 3	ACRE	5.1	5.1		
* 25000310	SEEDING, CLASS 4	ACRE	1.8	1.8		
* 25000320	SEEDING, CLASS 5	ACRE	1.8	1.8		
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1017	1017		
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	1017	1017		

* SPECIALTY ITEM

14

V&K
 Veenstra & Kimm, Inc.
 Springfield, IL Phone: (217)544-8033

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PLOT SCALE *	CHECKED -	REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 1 OF 10 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	6
CONTRACT NO. 68800			ILLINOIS FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FEDERAL 20% STATE ROADWAY 0004	80% FEDERAL 20% STATE BRIDGE 0011 S.N. 048-0096	100% STATE MOWING 0004
				* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND
25000750	MOWING	ACRE	33.9			33.9
* 25100115	MULCH, METHOD 2	ACRE	11.3	11.3		
* 25100630	EROSION CONTROL BLANKET	SQ YD	10575	10575		
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	4520	4520		
28000305	TEMPORARY DITCH CHECKS	FOOT	215	215		
28000400	PERIMETER EROSION BARRIER	FOOT	5053	5053		
28000500	INLET AND PIPE PROTECTION	EACH	3	3		
28100225	STONE RIPRAP, CLASS B3	TON	148	148		
28200200	FILTER FABRIC	SQ YD	441	441		
30300106	AGGREGATE SUBGRADE IMPROVEMENT 6"	SQ YD	1368	1368		
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	11808	11808		
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	64	64		
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	26568	26568		

* SPECIALTY ITEM

14



USER NAME *	DESIGNED -	REVISED -
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PLOT DATE *	DRAWN -	REVISED -
	CHECKED -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 2 OF 10 SHEETS STA. TO STA.

F.A.S. RTE. 2401	SECTION 140V-1BR	COUNTY KNOX	TOTAL SHEETS 146	SHEET NO. 7
CONTRACT NO. 68800				ILLINOIS FED. AID PROJECT

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FEDERAL 20% STATE ROADWAY 0004	80% FEDERAL 20% STATE BRIDGE 0011	100% STATE MOWING 0004
					S.N. 048-0096	
40600295	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	15440	15440		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	81	81		
40600990	TEMPORARY RAMP	SQ YD	66	66		
40603230	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	72	72		
40603535	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	64	64		
40701891	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 10 1/2"	SQ YD	7154	7154		
42000070	PAVEMENT CONNECTOR (HMA) FOR BRIDGE APPROACH SLAB	SQ YD	54	54		
42000301	PORTLAND CEMENT CONCRETE PAVEMENT 8" (JOINTED)	SQ YD	4616	4616		
42000401	PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)	SQ YD	1200	1200		
42400100	PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH	SQ FT	2294	2294		
42400800	DETECTABLE WARNINGS	SQ FT	10	10		
44000100	PAVEMENT REMOVAL	SQ YD	8588	8588		
44000400	GUTTER REMOVAL	FOOT	920	920		
44004250	PAVED SHOULDER REMOVAL	SQ YD	3292	3292		

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 3 OF 10 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	140V-11BR	KNOX	146	8
CONTRACT NO. 68800			ILLINOIS FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FEDERAL 20% STATE ROADWAY 0004	80% FEDERAL 20% STATE BRIDGE 0011 S.N. 048-0096	100% STATE MOWING 0004
48101200	AGGREGATE SHOULDERS, TYPE B	TON	89	89		
48203100	HOT-MIX ASPHALT SHOULDERS	TON	1833	1833		
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1		1	
50100300	REMOVAL OF EXISTING STRUCTURES NO. 1	EACH	1	1		
50102400	CONCRETE REMOVAL	CU YD	13.7	13.7		
50105220	PIPE CULVERT REMOVAL	FOOT	76	76		
50200100	STRUCTURE EXCAVATION	CU YD	377		377	
50300225	CONCRETE STRUCTURES	CU YD	369.7		369.7	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	275.7		275.7	
50300260	BRIDGE DECK GROOVING	SQ YD	938		938	
50300300	PROTECTIVE COAT	SQ YD	1221.7		1221.7	
50301350	CONCRETE SUPERSTRUCTURE (APPROACH SLAB)	CU YD	121.3		121.3	
50500105	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1		1	
50500505	STUD SHEAR CONNECTORS	EACH	4842		4842	

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	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 4 OF 10 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	140V-11BR	KNOX	146	9
			CONTRACT NO. 68800	
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FEDERAL 20% STATE ROADWAY 0004	80% FEDERAL 20% STATE BRIDGE 0011	100% STATE MOWING 0004
					S.N. 048-0096	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	149010		149010	
51100100	SLOPE WALL 4 INCH	SQ YD	619		619	
51201700	FURNISHING STEEL PILES HP12X74	FOOT	2199		2199	
51202305	DRIVING PILES	FOOT	2199		2199	
51203700	TEST PILE STEEL HP12X74	EACH	2		2	
51500100	NAME PLATES	EACH	1		1	
52100520	ANCHOR BOLTS, 1"	EACH	48		48	
542A0223	PIPE CULVERTS, CLASS A, TYPE 1 18"	FOOT	36	36		
542A1075	PIPE CULVERTS, CLASS A, TYPE 2 30"	FOOT	112	112		
542A5479	PIPE CULVERTS, CLASS A, TYPE 1 EQUIVALENT ROUND - SIZE 24"	FOOT	50	50		
542D1063	PIPE CULVERTS, CLASS D, TYPE 2 18"	FOOT	58	58		
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	2	2		
54213675	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 30"	EACH	2	2		
54214509	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 24"	EACH	2	2		

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 5 OF 10 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	140V-11BR	KNOX	146	10
			CONTRACT NO. 68800	
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FEDERAL 20% STATE ROADWAY 0004	80% FEDERAL 20% STATE BRIDGE 0011	100% STATE MOWING 0004
				S.N. 048-0096		
54215553	METAL END SECTIONS 18"	EACH	2	2		
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	191	191		
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	297	297		
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	246	246		
550A4000	STORM SEWERS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 18"	FOOT	82	82		
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	89		89	
60219570	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 3V FRAME AND GRATE	EACH	6	6		
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1593	1593		
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	1225	1225		
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4	4		
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	3	3		
* 63100169	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED	EACH	1	1		
63200310	GUARDRAIL REMOVAL	FOOT	1266	1266		
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	5459	5459		

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* SPECIALTY ITEM



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 6 OF 10 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	140V-11BR	KNOX	146	11
			CONTRACT NO. 68800	
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE	100% STATE MOWING
				0004	0011 S.N. 048-0096	0004
66600105	FURNISHING AND ERECTING RIGHT OF WAY MARKERS	EACH	36	36		
66700205	PERMANENT SURVEY MARKERS, TYPE I	EACH	9	9		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	20	20		
67000600	ENGINEER'S FIELD LABORATORY	CAL MO	20	20		
67100100	MOBILIZATION	L SUM	1	1		
66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	5900	5900		
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1		
66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1		
70101830	TRAFFIC CONTROL AND PROTECTION, STANDARD BLR 21	L SUM	1	1		
66900530	SOIL DISPOSAL ANALYSIS	EACH	4	4		
* 72000100	SIGN PANEL - TYPE 1	SQ FT	304	304		
* 72000200	SIGN PANEL - TYPE 2	SQ FT	10	10		
* 72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	73	73		
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4		
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	300	300		
* 73000100	WOOD SIGN SUPPORT	FOOT	84	84		

* SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES	
SCALE: NONE	SHEET NO. 7 OF 10 SHEETS
STA.	TO STA.

F.A.S. RTE. 2401	SECTION 140V-11BR	COUNTY KNOX	TOTAL SHEETS 146	SHEET NO. 12
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

REV

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
				80% FEDERAL ROADWAY	80% FEDERAL BRIDGE	100% STATE MOWING	
				0004	0011	0004	
						S.N. 048-0096	
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	50 FT	108	108			
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	19718	19718			
* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	42	42			
* 78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	477	477			
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	579	579			
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	82	82			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	83	83			
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	27	27			
* 78200011	<i>BARRIER WALL REFLECTORS, TYPE C</i>	EACH	8	8			
	78300200 RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	4	4			
	89502375 REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	2	2			
* D2002260	EVERGREEN, PICEA PUNGENS GLAUCA (COLORADO BLUE SPRUCE), 5' HEIGHT, BALLED AND BURLAPPED	EACH	7	7			
	X0325833 WICK DRAINS	FOOT	33993	33993			

* SPECIALTY ITEM

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V&K
Veenstra & Kimm, Inc.
Springfield, IL. Phone: (217)544-8033

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 8 OF 10 SHEETS STA. TO STA.

F.A.S. RTE. 2401	SECTION (40V-1)BR	COUNTY KNOX	TOTAL SHEETS 146	SHEET NO. 13
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FEDERAL 20% STATE ROADWAY 0004	80% FEDERAL 20% STATE BRIDGE 0011 S.N. 048-0096	100% STATE MOWING 0004
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	468	468		
X5010523	REMOVE CONCRETE END SECTION	EACH	2	2		
X5860110	GRANULAR BACKFILL FOR STRUCTURES	CU YD	149		149	
X6020074	INLETS, TYPE A, TYPE 3V FRAME AND GRATE	EACH	5	5		
X6020075	INLETS, TYPE B, TYPE 3V FRAME AND GRATE	EACH	1	1		
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1		
X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	120	120		
X7830060	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	100	100		
X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	6538	6538		
X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	42	42		
X7830076	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	344	344		
X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	452	452		
X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	82	82		
Z0001002	GUARDRAIL AGGREGATE EROSION CONTROL	TON	274	274		
Z0001900	ASBESTOS BEARING PAD REMOVAL	EACH	44		44	

* SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 9 OF 10 SHEETS STA. TO STA.

F.A.S. RTE. 2401	SECTION 140V-11BR	COUNTY KNOX	TOTAL SHEETS 146	SHEET NO. 14
CONTRACT NO. 68800				ILLINOIS FED. AID PROJECT

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				80% FEDERAL 20% STATE ROADWAY	80% FEDERAL 20% STATE BRIDGE	100% STATE MOWING
				0004	0011	0004
					S.N. 048-0096	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1		
Z0034105	MATERIAL TRANSFER DEVICE	TON	5177	5177		
Z0046304	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	187		187	
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1		
Z0056100	SAND DRAINAGE BLANKET	CU YD	2565	2565		
φ Z0076600	TRAINEES	Hour	1000	1000		
Z0065100	SETTLEMENT PLATFORMS	EACH	3	3		
φ Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	Hour	1000	1000		

6



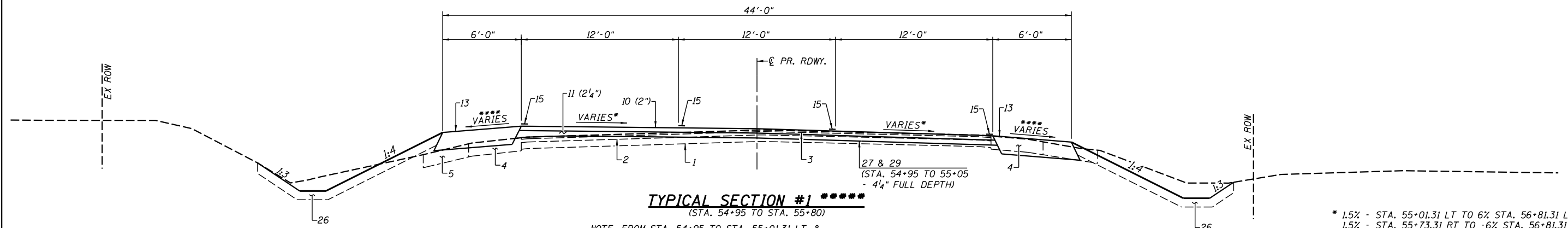
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 10 OF 10 SHEETS STA. _____ TO STA. _____

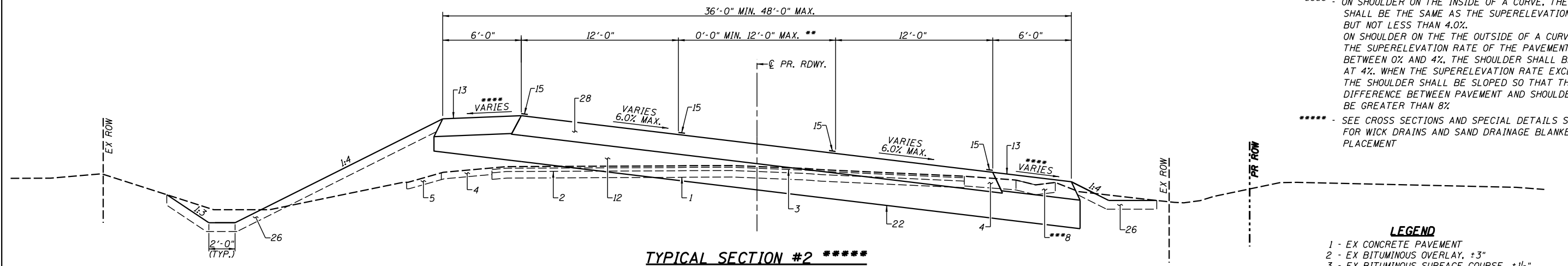
F.A.S. RTE. 2401	SECTION 140V-11BR	COUNTY KNOX	TOTAL SHEETS 146	SHEET NO. 15
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



TYPICAL SECTION #1 *****
(STA. 54+95 TO STA. 55+80)

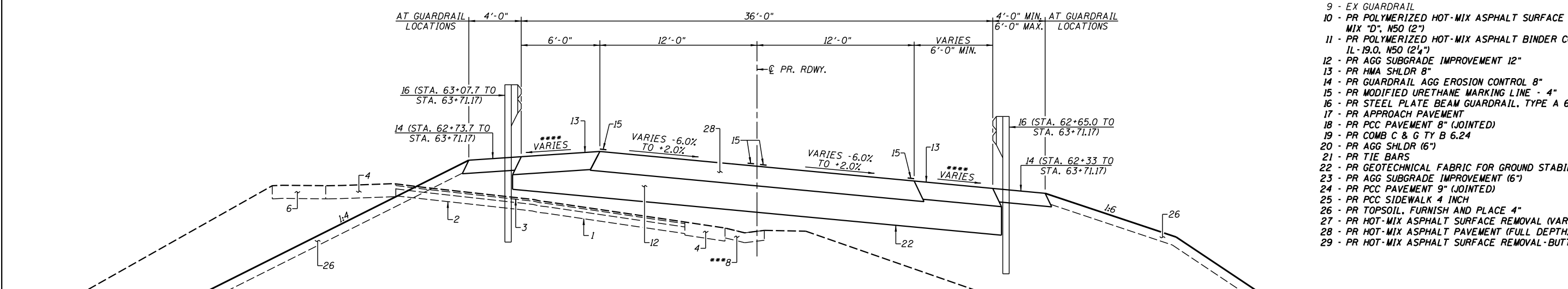
NOTE: FROM STA. 54+95 TO STA. 55+01.31 LT. &
FROM STA. 54+95 TO STA. 55+73.31 RT. - (NO SUPERELEVATION - 1.5% CROSS SLOPE)

- * 1.5% - STA. 55+01.31 LT TO 6% STA. 56+81.31 LT
1.5% - STA. 55+73.31 RT TO -6% STA. 56+81.31 RT.
- ** - 12'-0" - STA. 54+95 TO STA. 57+20
12'-0" - STA. 57+20 TO 0'-0" STA. 59+90
- *** - STA. 58+32 TO STA. 63+51 RT.
- **** - ON SHOULDER ON THE INSIDE OF A CURVE, THE SLOPE SHALL BE THE SAME AS THE SUPERELEVATION RATE, BUT NOT LESS THAN 4.0%.
ON SHOULDER ON THE THE OUTSIDE OF A CURVE, WHERE THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4%, THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE EXCEEDS 4%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER WILL NOT BE GREATER THAN 8%.
- ***** - SEE CROSS SECTIONS AND SPECIAL DETAILS SHEETS FOR WICK DRAINS AND SAND DRAINAGE BLANKET PLACEMENT

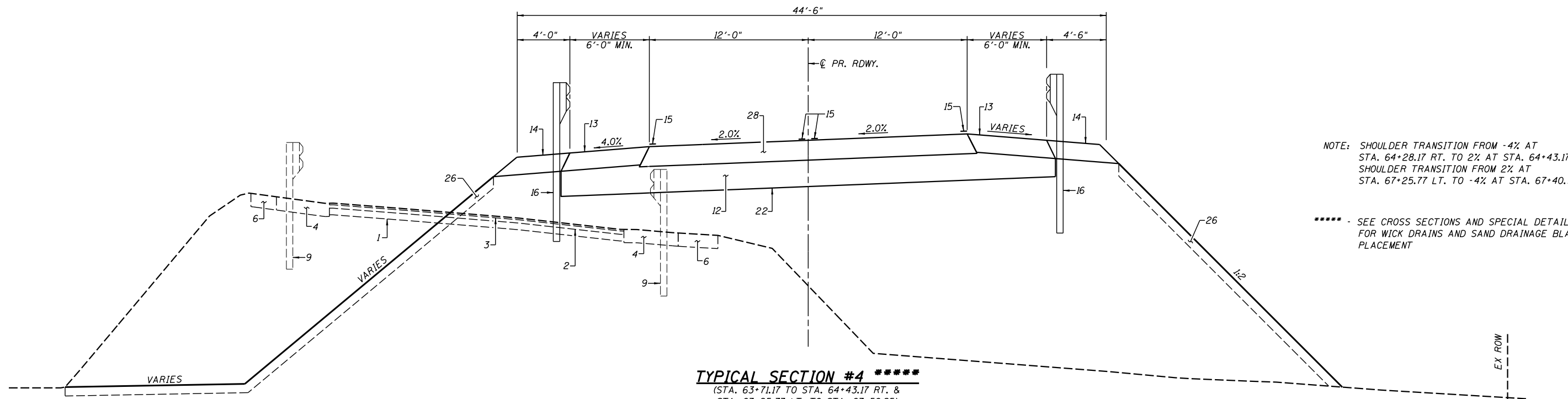


TYPICAL SECTION #2 *****
(STA. 55+80 TO STA. 59+90)

- LEGEND**
- 1 - EX CONCRETE PAVEMENT
 - 2 - EX BITUMINOUS OVERLAY, ±3"
 - 3 - EX BITUMINOUS SURFACE COURSE, ±1½"
 - 4 - EX BITUMINOUS SHOULDERS, 9"
 - 5 - EX AGGREGATE SHOULDERS, 6"
 - 6 - EX AGGREGATE EROSION CONTROL
 - 7 - EX EROSION CONTROL CURB
 - 8 - EX CONCRETE GUTTER, TYPE A
 - 9 - EX GUARDRAIL
 - 10 - PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (2")
 - 11 - PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 (2¼")
 - 12 - PR AGG SUBGRADE IMPROVEMENT 12"
 - 13 - PR HMA SHLDR 8"
 - 14 - PR GUARDRAIL AGG EROSION CONTROL 8"
 - 15 - PR MODIFIED URETHANE MARKING LINE - 4"
 - 16 - PR STEEL PLATE BEAM GUARDRAIL, TYPE A 6' POSTS
 - 17 - PR APPROACH PAVEMENT
 - 18 - PR PCC PAVEMENT 8" (JOINTED)
 - 19 - PR COMB C & G TY B 6.24
 - 20 - PR AGG SHLDR (6")
 - 21 - PR TIE BARS
 - 22 - PR GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - 23 - PR AGG SUBGRADE IMPROVEMENT (6")
 - 24 - PR PCC PAVEMENT 9" (JOINTED)
 - 25 - PR PCC SIDEWALK 4 INCH
 - 26 - PR TOPSOIL, FURNISH AND PLACE 4"
 - 27 - PR HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH)
 - 28 - PR HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 10½"
 - 29 - PR HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT



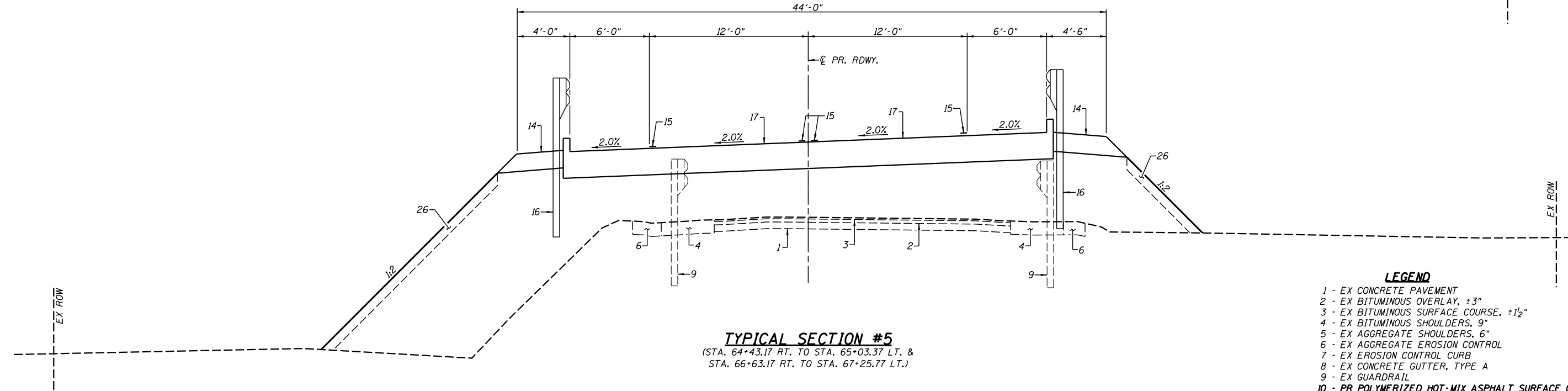
TYPICAL SECTION #3 *****
(STA. 59+90 TO STA. 63+71.17)



TYPICAL SECTION #4 *****
 (STA. 63+71.17 TO STA. 64+43.17 RT. &
 STA. 67+25.77 LT. TO STA. 67+50.95)

NOTE: SHOULDER TRANSITION FROM -4% AT STA. 64+28.17 RT. TO 2% AT STA. 64+43.17 RT. SHOULDER TRANSITION FROM 2% AT STA. 67+25.77 LT. TO -4% AT STA. 67+40.77 LT.

***** - SEE CROSS SECTIONS AND SPECIAL DETAILS SHEETS FOR WICK DRAINS AND SAND DRAINAGE BLANKET PLACEMENT



TYPICAL SECTION #5
 (STA. 64+43.17 RT. TO STA. 65+03.37 LT. &
 STA. 66+63.17 RT. TO STA. 67+25.77 LT.)

- LEGEND**
- 1 - EX CONCRETE PAVEMENT
 - 2 - EX BITUMINOUS OVERLAY, ±3"
 - 3 - EX BITUMINOUS SURFACE COURSE, ±1½"
 - 4 - EX BITUMINOUS SHOULDERS, 9"
 - 5 - EX AGGREGATE SHOULDERS, 6"
 - 6 - EX AGGREGATE EROSION CONTROL
 - 7 - EX EROSION CONTROL CURB
 - 8 - EX CONCRETE GUTTER, TYPE A
 - 9 - EX GUARDRAIL
 - 10 - PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (2")
 - 11 - PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 (2¼")
 - 12 - PR AGG SUBGRADE IMPROVEMENT 12"
 - 13 - PR HMA SHLDR 8"
 - 14 - PR GUARDRAIL AGG EROSION CONTROL 8"
 - 15 - PR MODIFIED URETHANE MARKING LINE - 4"
 - 16 - PR STEEL PLATE BEAM GUARDRAIL, TYPE A 6' POSTS
 - 17 - PR APPROACH PAVEMENT
 - 18 - PR PCC PAVEMENT 8" (JOINTED)
 - 19 - PR COMB C & G TY B 6.24
 - 20 - PR AGG SHLDR (6")
 - 21 - PR TIE BARS
 - 22 - PR GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - 23 - PR AGG SUBGRADE IMPROVEMENT (6")
 - 24 - PR PCC PAVEMENT 9" (JOINTED)
 - 25 - PR PCC SIDEWALK 4 INCH
 - 26 - PR TOPSOIL, FURNISH AND PLACE 4"
 - 27 - PR HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH)
 - 28 - PR HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 10½"
 - 29 - PR HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT



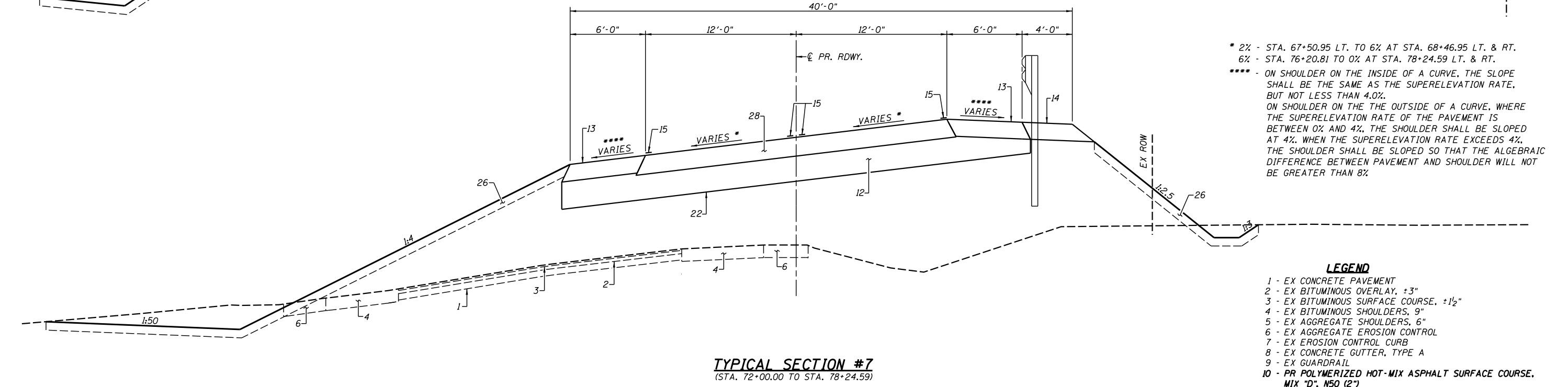
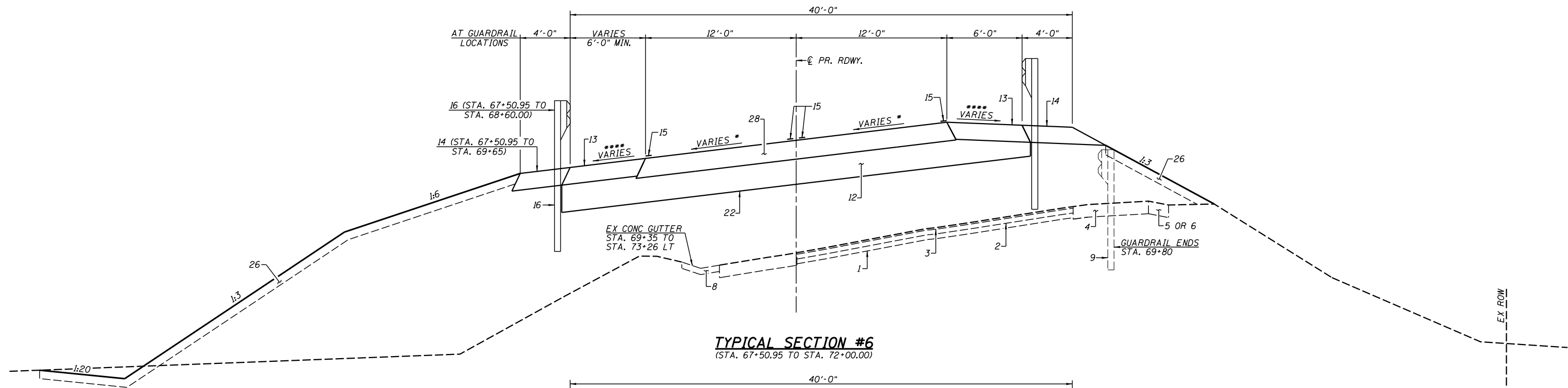
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS - US 150

SCALE: NONE SHEET NO. 2 OF 7 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	17
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



* 2% - STA. 67+50.95 LT. TO 6% AT STA. 68+46.95 LT. & RT.
 6% - STA. 76+20.81 TO 0% AT STA. 78+24.59 LT. & RT.

**** - ON SHOULDER ON THE INSIDE OF A CURVE, THE SLOPE SHALL BE THE SAME AS THE SUPERELEVATION RATE, BUT NOT LESS THAN 4.0%.
 ON SHOULDER ON THE THE OUTSIDE OF A CURVE, WHERE THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4%, THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE EXCEEDS 4%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER WILL NOT BE GREATER THAN 8%.

LEGEND

- 1 - EX CONCRETE PAVEMENT
- 2 - EX BITUMINOUS OVERLAY, ±3"
- 3 - EX BITUMINOUS SURFACE COURSE, ±1½"
- 4 - EX BITUMINOUS SHOULDERS, 9"
- 5 - EX AGGREGATE SHOULDERS, 6"
- 6 - EX AGGREGATE EROSION CONTROL
- 7 - EX EROSION CONTROL CURB
- 8 - EX CONCRETE GUTTER, TYPE A
- 9 - EX GUARDRAIL
- 10 - PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (2")
- 11 - PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 (2¼")
- 12 - PR AGG SUBGRADE IMPROVEMENT 12"
- 13 - PR HMA SHLDR 8"
- 14 - PR GUARDRAIL AGG EROSION CONTROL 8"
- 15 - PR MODIFIED URETHANE MARKING LINE - 4"
- 16 - PR STEEL PLATE BEAM GUARDRAIL, TYPE A 6' POSTS
- 17 - PR APPROACH PAVEMENT
- 18 - PR PCC PAVEMENT 8" (JOINTED)
- 19 - PR COMB C & G TY B 6.24
- 20 - PR AGG SHLDR (6")
- 21 - PR TIE BARS
- 22 - PR GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- 23 - PR AGG SUBGRADE IMPROVEMENT (6")
- 24 - PR PCC PAVEMENT 9" (JOINTED)
- 25 - PR PCC SIDEWALK 4 INCH
- 26 - PR TOPSOIL, FURNISH AND PLACE 4"
- 27 - PR HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH)
- 28 - PR HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 10½"
- 29 - PR HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT



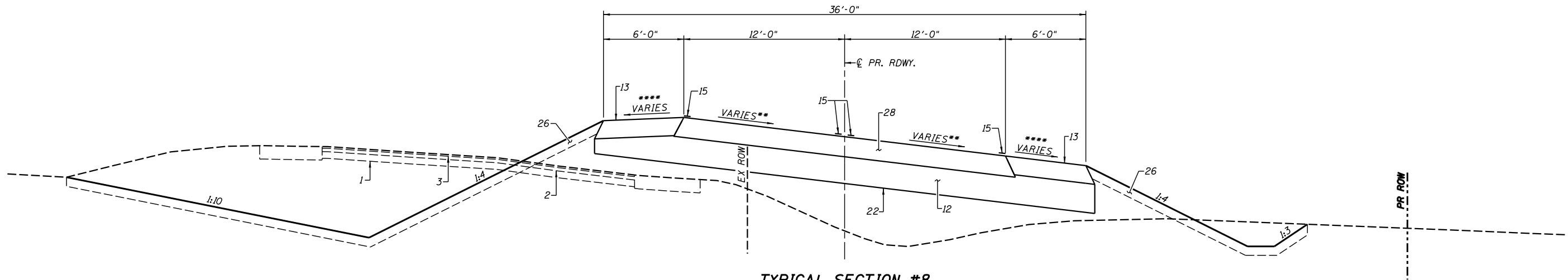
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PLOT DATE =	CHECKED -	REVISED -

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TYPICAL SECTIONS - US 150

SCALE: NONE SHEET NO. 3 OF 7 SHEETS STA. TO STA.

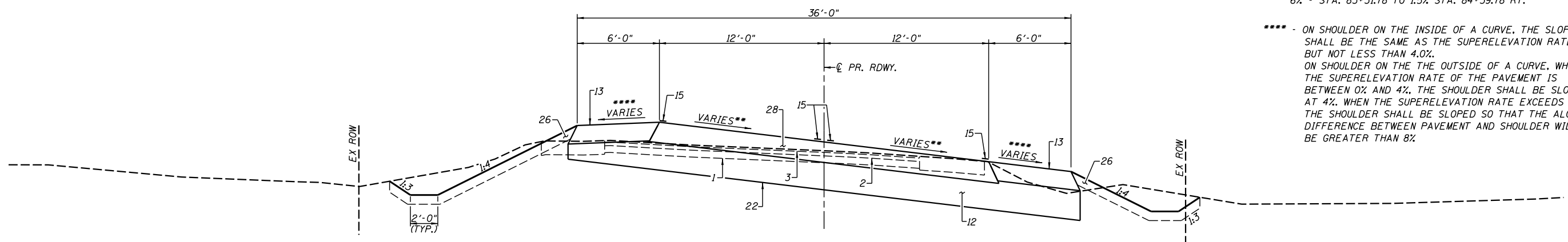
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	18
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



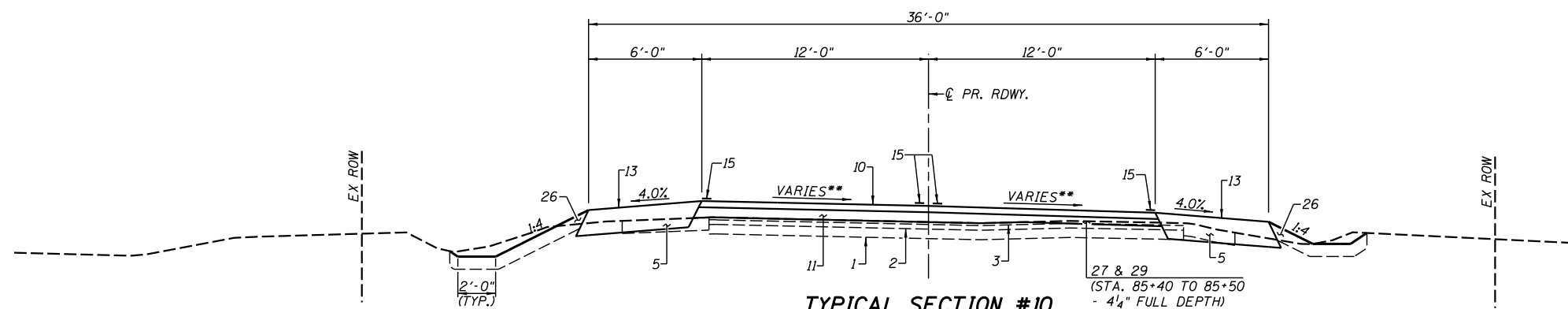
TYPICAL SECTION #8
(STA. 78+24.59 TO STA. 82+00.00)

** 0% - STA. 78+24.59 TO 6% STA. 80+28.36
 6% - STA. 83+31.78 TO -1.5% STA. 85+11.78 LT.
 6% - STA. 83+31.78 TO 1.5% STA. 84+39.78 RT.

**** - ON SHOULDER ON THE INSIDE OF A CURVE, THE SLOPE SHALL BE THE SAME AS THE SUPERELEVATION RATE, BUT NOT LESS THAN 4.0%.
 ON SHOULDER ON THE THE OUTSIDE OF A CURVE, WHERE THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4%, THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE EXCEEDS 4%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER WILL NOT BE GREATER THAN 8%.



TYPICAL SECTION #9
(STA. 82+00.00 TO STA. 84+65 LT)



TYPICAL SECTION #10
(STA. 84+65 TO STA. 85+50 LT. & RT.)

LEGEND

- 1 - EX CONCRETE PAVEMENT
- 2 - EX BITUMINOUS OVERLAY, ±3"
- 3 - EX BITUMINOUS SURFACE COURSE, ±1½"
- 4 - EX BITUMINOUS SHOULDERS, 9"
- 5 - EX AGGREGATE SHOULDERS, 6"
- 6 - EX AGGREGATE EROSION CONTROL
- 7 - EX EROSION CONTROL CURB
- 8 - EX CONCRETE GUTTER, TYPE A
- 9 - EX GUARDRAIL
- 10 - PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (2")
- 11 - PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 (2¼")
- 12 - PR AGG SUBGRADE IMPROVEMENT 12"
- 13 - PR HMA SHLDR 8"
- 14 - PR GUARDRAIL AGG EROSION CONTROL 8"
- 15 - PR MODIFIED URETHANE MARKING LINE - 4"
- 16 - PR STEEL PLATE BEAM GUARDRAIL, TYPE A 6' POSTS
- 17 - PR APPROACH PAVEMENT
- 18 - PR PCC PAVEMENT 8" (JOINTED)
- 19 - PR COMB C & G TY B 6.24
- 20 - PR AGG SHLDR (6")
- 21 - PR TIE BARS
- 22 - PR GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- 23 - PR AGG SUBGRADE IMPROVEMENT (6")
- 24 - PR PCC PAVEMENT 9" (JOINTED)
- 25 - PR PCC SIDEWALK 4 INCH
- 26 - PR TOPSOIL, FURNISH AND PLACE 4"
- 27 - PR HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH)
- 28 - PR HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 10½"
- 29 - PR HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT



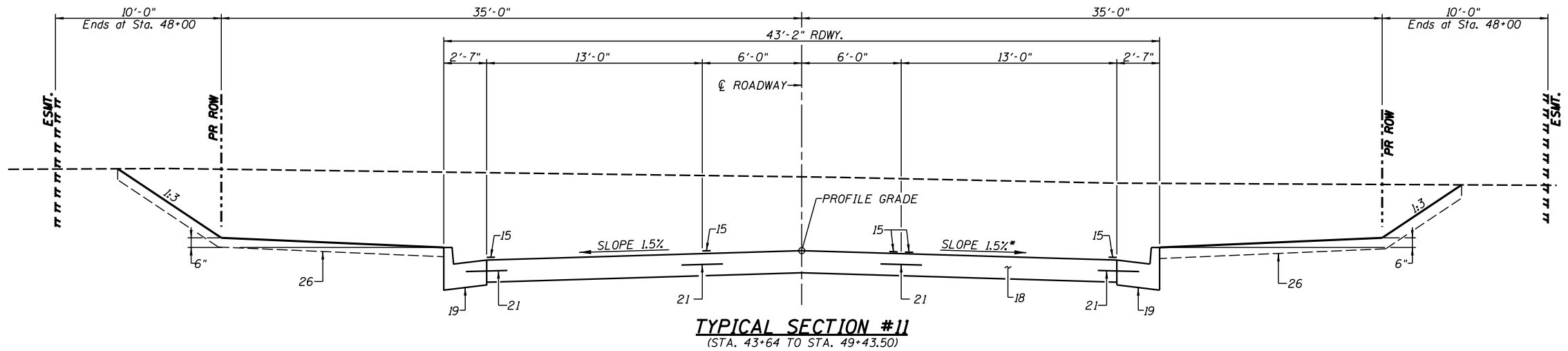
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TYPICAL SECTIONS - US 150

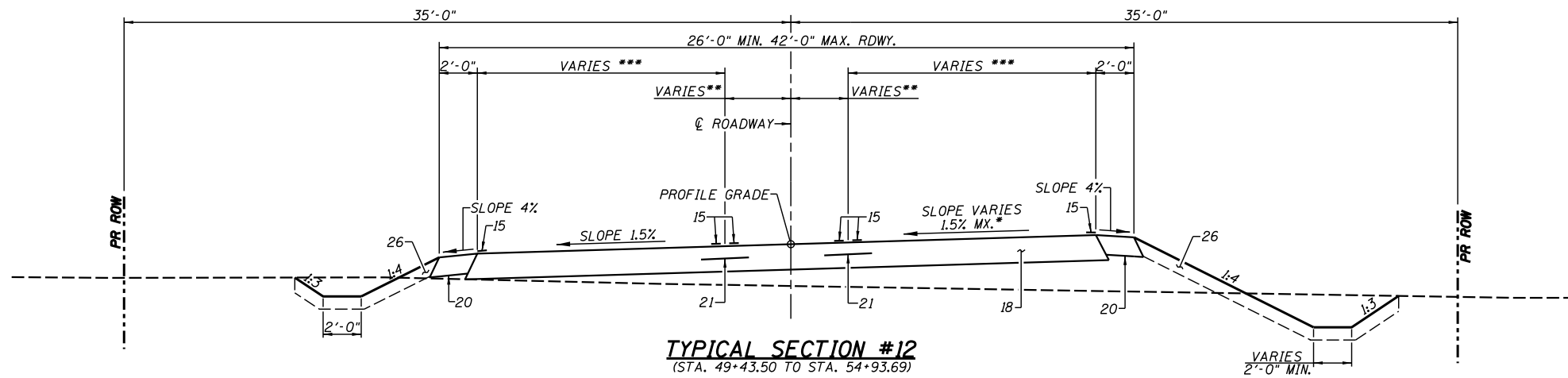
SCALE: NONE SHEET NO. 4 OF 7 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	19
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



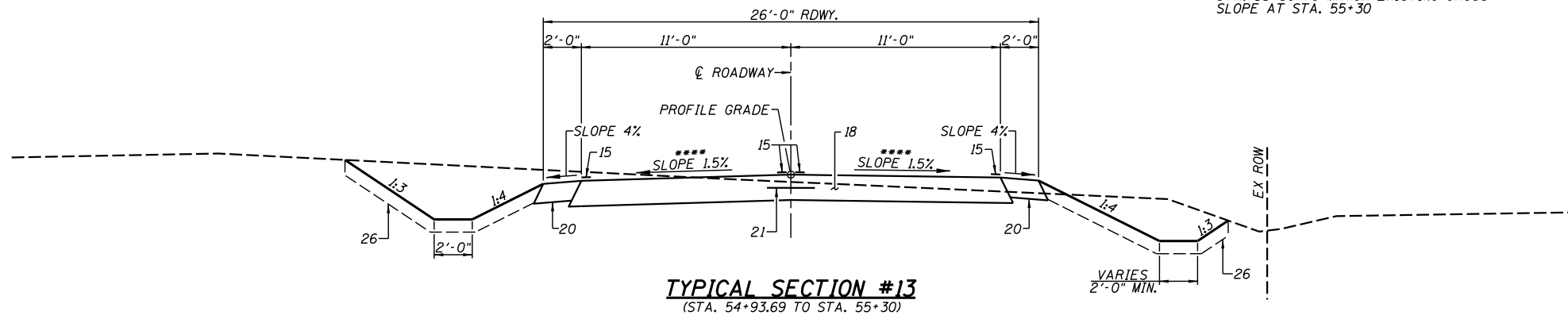
TYPICAL SECTION #11
(STA. 43+64 TO STA. 49+43.50)

* SLOPE VARIES FROM STA. 48+96.74 TO STA. 49+52.74 & STA. 54+37.69 TO STA. 54+93.69



TYPICAL SECTION #12
(STA. 49+43.50 TO STA. 54+93.69)

** - 6'-0" AT STA. 49+43.50 TO 0'-0" AT STA. 54+46.93
 *** - 13'-0" AT STA. 49+43.50 TO 11'-0" AT STA. 54+46.93
 **** - TRANSITION SLOPE FROM STA. 55+00 TO STA. 55+30 TO MATCH EXISTING CROSS SLOPE AT STA. 55+30



TYPICAL SECTION #13
(STA. 54+93.69 TO STA. 55+30)

LEGEND

- 1 - EX CONCRETE PAVEMENT
- 2 - EX BITUMINOUS OVERLAY, ±3"
- 3 - EX BITUMINOUS SURFACE COURSE, ±1½"
- 4 - EX BITUMINOUS SHOULDERS, 9"
- 5 - EX AGGREGATE SHOULDERS, 6"
- 6 - EX AGGREGATE EROSION CONTROL
- 7 - EX EROSION CONTROL CURB
- 8 - EX CONCRETE GUTTER, TYPE A
- 9 - EX GUARDRAIL
- 10 - PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (2")
- 11 - PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 (2¼")
- 12 - PR AGG SUBGRADE IMPROVEMENT 12"
- 13 - PR HMA SHLDR 8"
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- 18 - PR PCC PAVEMENT 8" (JOINTED)
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- 23 - PR AGG SUBGRADE IMPROVEMENT (6")
- 24 - PR PCC PAVEMENT 9" (JOINTED)
- 25 - PR PCC SIDEWALK 4 INCH
- 26 - PR TOPSOIL, FURNISH AND PLACE 4"
- 27 - PR HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH)
- 28 - PR HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 10½"
- 29 - PR HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT

NOTE: REFER TO STANDARDS 420101 AND 420106 FOR TYPICAL SECTIONS 11-15.



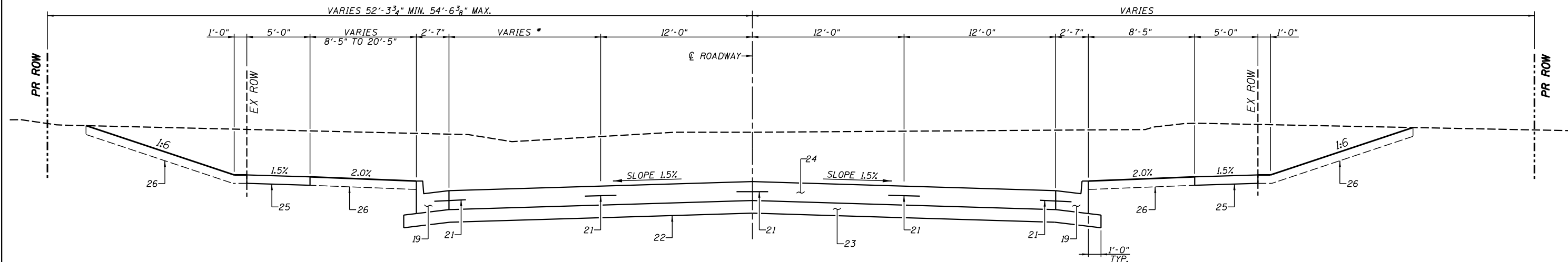
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PLOT DATE =	DRAWN -	REVISED -
	CHECKED -	REVISED -

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TYPICAL SECTIONS - KNOX RD

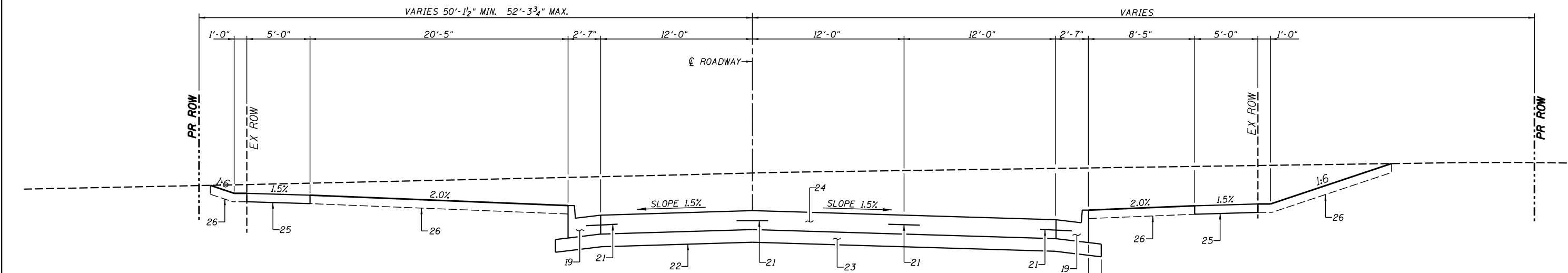
SCALE: NONE SHEET NO. 5 OF 7 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	20
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



TYPICAL SECTION #14
(STA. 12+19 TO STA. 13+75)

* - 12'-0" @ STA. 12+19
- 0'-0" @ STA. 13+75



TYPICAL SECTION #15
(STA. 13+75 TO STA. 14+92)

LEGEND

- 1 - EX CONCRETE PAVEMENT
- 2 - EX BITUMINOUS OVERLAY, ±3"
- 3 - EX BITUMINOUS SURFACE COURSE, ±1½"
- 4 - EX BITUMINOUS SHOULDERS, 9"
- 5 - EX AGGREGATE SHOULDERS, 6"
- 6 - EX AGGREGATE EROSION CONTROL
- 7 - EX EROSION CONTROL CURB
- 8 - EX CONCRETE GUTTER, TYPE A
- 9 - EX GUARDRAIL
- 10 - PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (2")
- 11 - PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 (2¼")
- 12 - PR AGG SUBGRADE IMPROVEMENT 12"
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- 23 - PR AGG SUBGRADE IMPROVEMENT (6")
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- 29 - PR HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT

NOTE: REFER TO STANDARDS 420101 AND 420106 FOR TYPICAL SECTIONS 11-15.



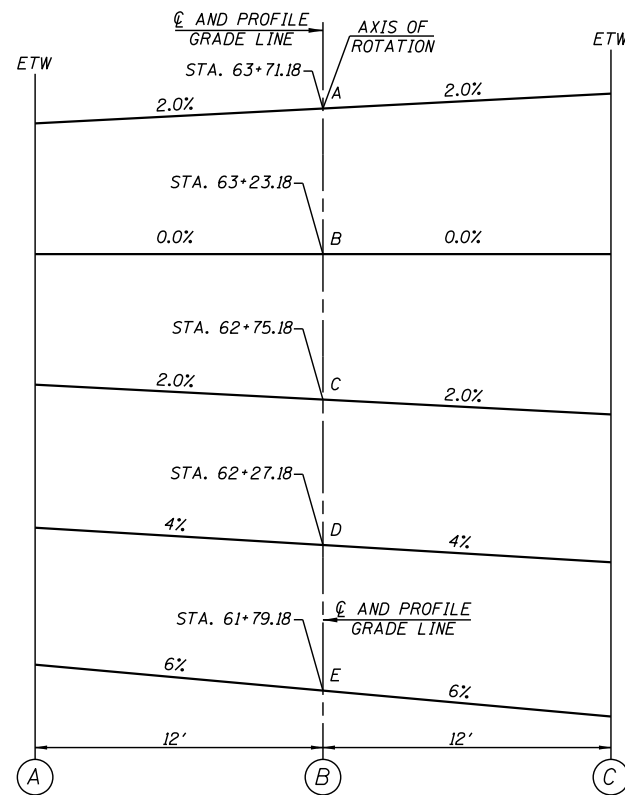
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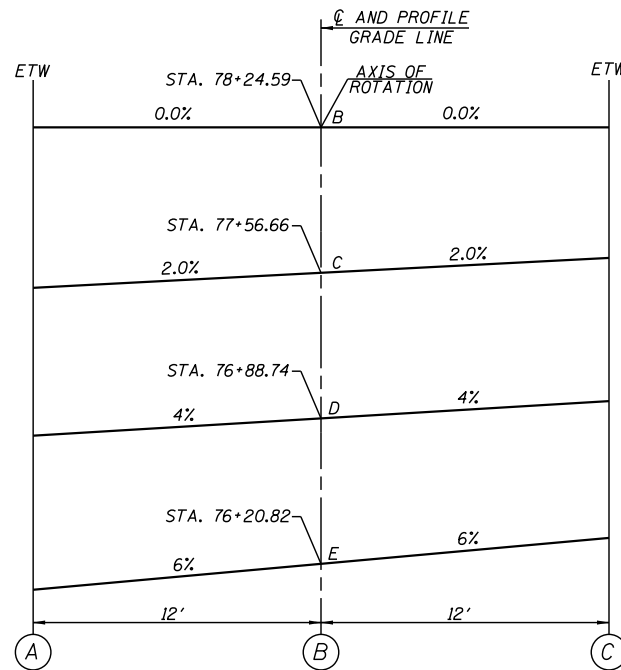
TYPICAL SECTIONS - LOGISTICS PKWY

SCALE: NONE SHEET NO. 6 OF 7 SHEETS STA. TO STA.

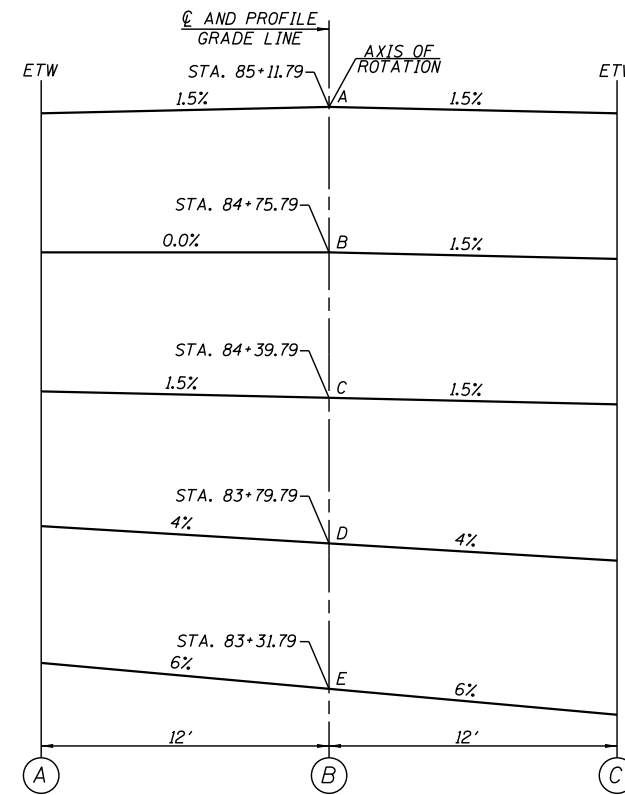
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	21
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



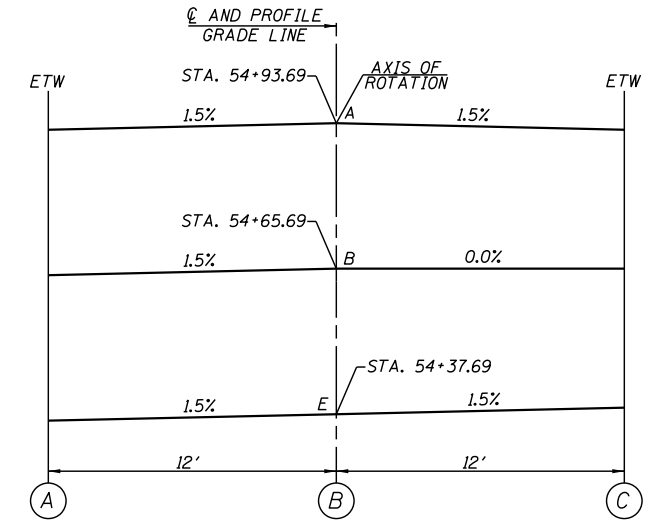
SUPERELEVATION ATTAINMENT
AXIS OF ROTATION ABOUT CENTERLINE
US 150 - CURVE #1
 (COMING OUT OF CURVE INTO BRIDGE)



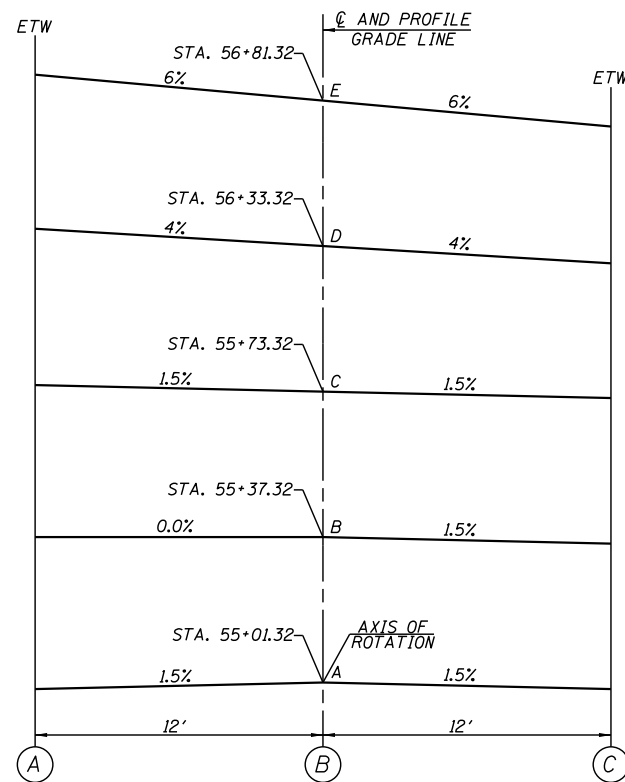
SUPERELEVATION ATTAINMENT
AXIS OF ROTATION ABOUT CENTERLINE
US 150 - CURVE #2
 (COMING OUT OF CURVE)



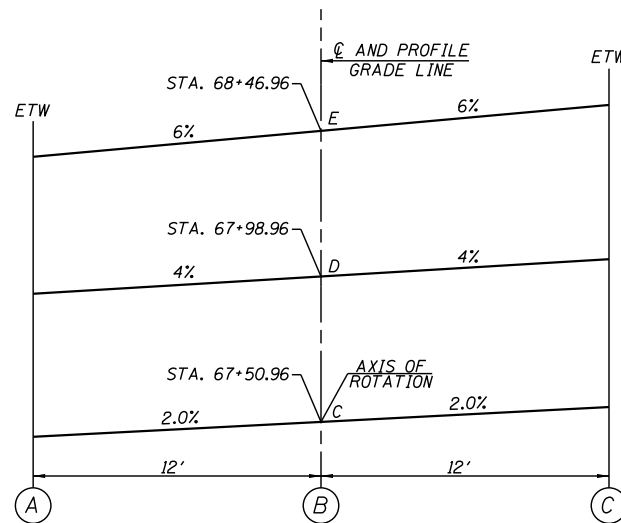
SUPERELEVATION ATTAINMENT
AXIS OF ROTATION ABOUT CENTERLINE
US 150 - CURVE #3
 (COMING OUT OF CURVE)



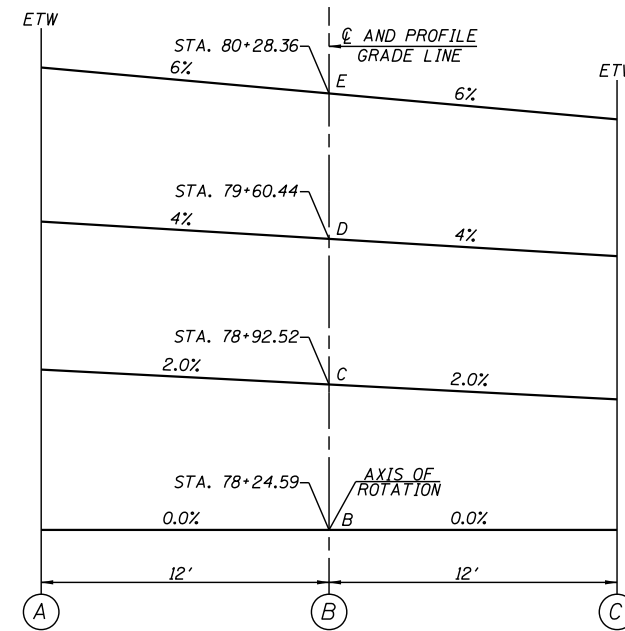
SUPERELEVATION ATTAINMENT
AXIS OF ROTATION ABOUT CENTERLINE
KNOX ROAD - CURVE #1
 (COMING OUT OF CURVE)



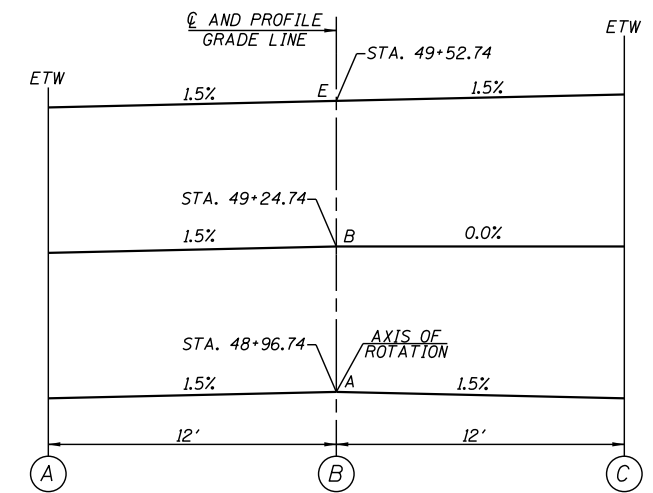
SUPERELEVATION ATTAINMENT
AXIS OF ROTATION ABOUT CENTERLINE
US 150 - CURVE #1
 (GOING INTO CURVE)



SUPERELEVATION ATTAINMENT
AXIS OF ROTATION ABOUT CENTERLINE
US 150 - CURVE #2
 (FROM BRIDGE INTO CURVE)



SUPERELEVATION ATTAINMENT
AXIS OF ROTATION ABOUT CENTERLINE
US 150 - CURVE #3
 (GOING INTO CURVE)



SUPERELEVATION ATTAINMENT
AXIS OF ROTATION ABOUT CENTERLINE
KNOX ROAD - CURVE #1
 (GOING INTO CURVE)



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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS -
 AXIS OF ROTATION ABOUT CENTERLINE
 SCALE: NONE SHEET NO. 7 OF 7 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	22
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

EARTHWORK				
LOCATION	20200100 EARTH EXCAVATION	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	EMBANKMENT	20400800 FURNISHED EXCAVATION EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
	CU YD	CU YD	CU YD	CU YD
US 150				
STA. 54+95 TO STA. 64+88.27	5096	3822	27738	-23916
STA. 66+78.27 TO STA. 85+50	5988	*66	36618	-36552
US 150 SUB-TOTAL	11084	3888	64356	-60468
KNOX ROAD				
STA. 44+00 TO STA. 55+30	2961	2221	614	1607
LOGISTICS ROAD				
STA. 12+50 TO STA. 14+92	2887	2165	0	2165
TOTAL	16932	8274	64970	-56696

*REDUCED ACCORDING TO NON-SPECIAL WASTE DISPOSAL QUANTITY.

TEMPORARY EROSION CONTROL ITEMS					
LOCATION	25100630 EROSION CONTROL BLANKET	28000250 TEMPORARY EROSION CONTROL SEEDING (4 APPLICATIONS)	28000305 TEMPORARY DITCH CHECKS	28000400 PERIMETER EROSION CONTROL BARRIER	28000500 INLET AND PIPE PROTECTION
	SQ YD	POUND	FOOT	FOOT	EACH
US 150					
STA. 56+00 36' LT. & 31' RT.			18		
STA. 58+00 48' LT.			9		
STA. 60+00 68' LT.			30		
STA. 62+00 93' LT.			30		
STA. 64+00 93' LT.			14		
STA. 75+00 56' LT. & 57' RT.			22		
STA. 77+00 50' RT.			8		
STA. 79+00 45' LT. & 45' RT.			23		
STA. 81+00 41' LT. & 36' RT.			19		
STA. 83+00 29' LT. & 32' RT.			18		
STA. 54+95 TO STA. 63+50 RT.	570				
STA. 63+50 TO STA. 65+40 RT.	2160				
STA. 54+95 TO STA. 64+00 LT.	604				
STA. 64+00 TO STA. 65+88 LT.	1643				
STA. 65+71 TO STA. 70+00 RT.	1911				
STA. 70+00 TO STA. 84+50 RT.	967				
STA. 66+56 TO STA. 68+00 LT.	1570				
STA. 68+00 TO STA. 85+50 LT.	1150				
STA. 54+95 TO STA. 85+50		3440			
STA. 62+06 TO STA. 64+00 LT.				200	
STA. 57+00 TO STA. 64+00 RT.				678	
STA. 64+00 TO STA. 65+95 LT.				195	
STA. 65+65 TO STA. 75+00 RT.				1075	
STA. 73+50 68.5' LT.					1
STA. 77+29 48.5' LT.					1
KNOX ROAD					
STA. 43+40 TO STA. 55+30		720			
STA. 43+80 TO STA. 55+30 LT.				1181	
STA. 44+24 TO STA. 50+65 RT.				645	
STA. 50+00 27' LT. & 30' RT.			16		
STA. 53+50 24' LT.			8		
STA. 53+73 TO STA. 55+30 RT.				174	
STA. 52+00 27' LT.					1
LOGISTICS PARKWAY					
STA. 10+50 TO STA. 14+92		360			
STA. 10+50 TO STA. 15+00 LT.				557	
STA. 10+50 TO STA. 13+95 RT.				348	
TOTAL	10575	4520	215	5053	3

PERMANENT SEEDING, MULCH AND MOWING								
LOCATION	25000210 SEEDING, CLASS 2A	25000300 SEEDING, CLASS 3	25000400 SEEDING, CLASS 4 25000500 SEEDING, CLASS 5	25000400 NITROGEN FERTILIZER NUTRIENT	25000500 PHOSPHORUS FERTILIZER NUTRIENT	25000600 POTASSIUM FERTILIZER NUTRIENT	25100115 MULCH, METHOD 2	25000750 MOWING (3 APPLICATIONS)
	ACRE	ACRE	ACRE	POUND	POUND	POUND	ACRE	ACRE
KNOX ROAD								
STA. 43+40 TO STA. 55+30 ROW. ESMT. ROW. LT.	0.52			47	47	47	0.52	1.6
STA. 44+55.8 TO STA. 55+30 ESMT., ROW TO 20' RT.	0.57			52	52	52	0.57	1.7
STA. 55+11 TO STA. 53+72 20' RT TO ROW		0.15		14	14	14	0.15	0.5
US 150								
STA. 54+95 TO STA. 65+69 LT. TO 20'	0.55			50	50	50	0.55	1.7
STA. 54+95 TO STA. 65+33 RT. TO 20'	0.49			44	44	44	0.49	1.5
STA. 57+81 TO STA. 65+84 20' LT. TO ROW		1.33		120	120	120	1.33	4.0
STA. 57+81 TO STA. 65+16 20' RT. TO ROW		0.91		82	82	82	0.91	2.7
STA. 65+80 TO STA. 85+50 20' RT. TO ROW		1.48		134	134	134	1.48	4.4
STA. 66+33 TO STA. 76+91 LT. TO 20'	0.46			42	42	42	0.46	1.4
STA. 66+50 TO STA. 68+00 20' LT. TO ROW		0.17		16	16	16	0.17	0.5
STA. 70+00 TO STA. 85+50 RT. TO 20'	0.88			80	80	80	0.88	2.7
STA. 77+09 TO STA. 85+50 LT. TO DITCHLINE	0.40			36	36	36	0.40	1.2
STA. 68+00 TO STA. 76+91 20' LT. TO DITCHLINE		0.70		63	63	63	0.70	2.1
STA. 68+00 TO STA. 81+87 LT. DITCHLINE TO ROW			1.79	161	161	161	1.79	5.4
LOGISTICS PARKWAY								
STA. 10+50 TO STA. 11+88 20' LT. TO ROW		0.32		29	29	29	0.32	1.0
STA. 10+50 TO STA. 14+92 ROW TO 20' LT.	0.24			22	22	22	0.24	0.7
STA. 10+50 TO STA. 44+55.8 (KNOX) ROW TO ROW RT.	0.27			25	25	25	0.27	0.8
TOTAL	4.38	5.06	1.79	1017	1017	1017	11.23	33.9

USE 4.4 ACRE USE 5.1 ACRE USE 1.8 ACRE

USE 11.3 ACRE

TREE REMOVAL			
LOCATION	20100110 TREE REMOVAL (6 TO 15 UNITS DIA)	20100210 TREE REMOVAL (OVER 15 UNITS DIA)	20100500 TREE REMOVAL ACRE
	UNIT	UNIT	ACRE
STA. 70+01 111' LT.	12		
STA. 68+13 99' LT.		30	
STA. 69+20 123' LT.		22	
STA. 70+00 110' LT.		20	
STA. 70+45 55' LT.		24	
STA. 71+62 84' LT.		20	
KNOX ROAD			
STA. 53+62 TO STA. 54+08 15' TO 35' LT.			0.02
US 150			
STA. 64+00 115' LT. TO STA. 68+00 108' LT.			0.91
STA. 64+00 130' RT. TO STA. 67+65 100' RT.			0.90
TOTAL	12	116	1.83

SAY 1.90 ACRE

20800150 TRENCH BACKFILL	
LOCATION	TRENCH BACKFILL CU YD
KNOX ROAD	
LINE A 12" DIA X 38'	7.4
LINE B 15" DIA X 88'	16.7
LINE C 12" DIA X 38'	7.4
LINE D 15" DIA X 87'	16.6
LINE E 12" DIA X 38'	7.4
LINE F 15" DIA X 87'	16.6
LINE G 12" DIA X 38'	7.4
LINE H 15" DIA X 35'	7.5
LINE I 12" DIA X 39'	8.5
LINE J 18" DIA X 135'	29.5
LOGISTICS PARKWAY	
LINE K 18" DIA X 82'	12.7
LINE L 18" DIA X 111'	21.4
TOTAL	159.1

USE 160 CU. YD.



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

QUANTITY SCHEDULES

SCALE: NONE SHEET NO. 1 OF 6 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	23
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

TABULATION OF RESURFACING QUANTITIES

LOCATION	40600275	40600295	40600982	X4401198	40600990	40603230	40603535	40701891	42000070
	BITUMINOUS MATERIALS (PRIME COAT)	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	HMA SURFACE REMOVAL - BUTT JOINT	HMA SURFACE REMOVAL - VARIABLE DEPTH	TEMPORARY RAMP	POLYMERIZED HMA BINDER COURSE IL-19.0, N50	POLYMERIZED HMA SURFACE COURSE, MIX "D", N50	HMA PAVEMENT (FULL DEPTH) 10% ^{3/4} "	PAVEMENT CONNECTOR (HMA) FOR BRIDGE APPROACH SLAB
	POUND	POUND	SO. YD.	SO. YD.	SO. YD.	TON	TON	SO. YD.	SO. YD.
US 150									
STA. 54+95 TO STA. 55+05			54						
STA. 55+05 TO STA. 55+80				268					
STA. 54+95 TO STA. 55+80		153				43	38		
STA. 55+80 TO STA. 64+48.27								2496	
STA. 55+80 TO STA. 64+55.27	9392	5389							
STA. 55+80 TO STA. 64+58.27									
STA. 64+48.27 TO STA. 64+58.27									27
BRIDGE OMISSION S.N. 048-0096									
STA. 67+08.27 TO STA. 67+18.27									27
STA. 67+08.27 TO STA. 84+65.00		9796							
STA. 67+11.27 TO STA. 84+65.00	17176								
STA. 67+18.27 TO STA. 84+65.00								4658	
STA. 84+65 TO STA. 85+40				200					
STA. 84+65 TO STA. 85+50		102				29	26		
STA. 85+40 TO STA. 85+50			27						
F.E. STA. 77+00 LT.					10				
C.E. STA. 84+99 RT.					56				
TOTAL	26568	15440	81	468	66	72	64	7154	54

AGGREGATE SCHEDULE

LOCATION	40200800	48101200	30300112	30300106
	AGGREGATE SURFACE COURSE, TYPE B	AGGREGATE SHOULDERS, TYPE B 6"	AGGREGATE SUBGRADE IMPROVEMENT 12"	AGGREGATE SUBGRADE IMPROVEMENT 6"
	TON	TON	SO. YD.	SO. YD.
US 150				
FE STA. 77+00 LT.	64			
STA. 55+80 TO STA. 64+55.27			4174	
STA. 67+11.27 TO STA. 84+65			7634	
LOGISTICS PARKWAY				
STA. 12+19 TO STA. 13+75				853
STA. 13+75 TO STA. 14+92				515
KNOX ROAD				
STA. 49+43.50 TO STA. 55+30		89		
TOTAL	64	89	11808	1368

28100225 STONE RIPRAP, CLASS B3 & 28200200 FILTER FABRIC

LOCATION	28100225	28200200
	STONE RIPRAP TYPE B	FILTER FABRIC
	TON	SO. YD.
US 150		
STA. 64+58.7 TO STA. 64+78.7 22' TO 100' LT.	72	214
STA. 67+26.7 TO STA. 67+46.7 22' TO 110' LT.	76	227
TOTAL	148	441

21001000 GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

LOCATION	21001000
	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
	SO YD
LOGISTICS PARKWAY	
STA. 12+19 TO STA. 13+75	852
STA. 13+75 TO STA. 14+92	515
US 150	
STA. 55+80 TO STA. 64+55.27	4174
STA. 67+11.27 TO STA. 84+65	7634
TOTAL	13175

44000100 PAVEMENT REMOVAL

LOCATION	44000100
	PAVEMENT REMOVAL
	SO. YD.
US 150	
STA. 55+80 TO STA. 65+56.27 (EXISTING STRUCTURE)	2825
STA. 66+89.89 (EXISTING STRUCTURE) TO STA. 84+65	4557
KNOX ROAD	
STA. 58+77 35' LT. (US 150) TO STA. 55+30	1206
TOTAL	8588

PORTLAND CEMENT CONCRETE PAVEMENT

LOCATION	42000301	42000401
	PORTLAND CEMENT CONCRETE PAVEMENT 8" (JOINTED)	PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
	SO YD	SO YD
LOGISTICS PARKWAY		
STA. 12+19 TO STA. 14+92		1200
KNOX ROAD		
STA. 43+64 TO STA. 55+30	4616	
TOTAL	4616	1200

21101615 TOPSOIL FURNISH AND PLACE, 4"

LOCATION	21101615
	TOPSOIL FURNISH AND PLACE, 4"
	SO YD
KNOX ROAD	
STA. 44+00 TO STA. 55+30 LT. & RT.	5466
US 150	
STA. 54+95 TO STA. 64+88.27 LT. & RT.	13270
STA. 66+88.27 TO STA. 85+50 LT. & RT.	22930
LOGISTICS PARKWAY	
STA. 10+50 TO STA. 14+92 LT. & RT.	2428
TOTAL	44094

44004250 PAVED SHOULDER REMOVAL

LOCATION	44004250
	PAVED SHOULDER REMOVAL
	SO. YD.
US 150	
STA. 55+80 TO EXISTING STRUCTURE LT.	551
STA. 55+80 TO EXISTING STRUCTURE RT.	617
EXISTING STRUCTURE TO STA. 84+65 LT.	1003
EXISTING STRUCTURE TO STA. 84+65 RT.	1121
TOTAL	3292

X5010523 REMOVE CONCRETE END SECTION

LOCATION	X5010523
	CONCRETE END SECT. REMOVAL
	EACH
US 150	
STA. 73+50 32' LT	1
STA. 73+61 40' RT.	1
TOTAL	2



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PLOT DATE =	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

QUANTITY SCHEDULES

SCALE: NONE SHEET NO. 2 OF 6 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	24
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

STORM SEWERS AND DRAINAGE STRUCTURE SCHEDULE							
LOCATION	550A0050 STORM SEWERS, CLASS A, TYPE 1 12" FOOT	550A0070 STORM SEWERS CLASS A, TYPE 1 15" FOOT	550A0090 STORM SEWERS CLASS A, TYPE 1 18" FOOT	550A4000 STORM SEWERS CLASS A, TYPE 1 EQUIVALENT ROUND SIZE 18"	60219570 MANHOLES, TYPE A 4'-DIAMETER, TYPE 3V FRAME AND GRATE EACH	X6020074 INLETS, TYPE A TYPE 3V FRAME AND GRATE EACH	X6020075 INLETS, TYPE B TYPE 3V FRAME AND GRATE EACH
	LOGISTICS PARKWAY						
STA. 11+57.6 64.2' LT. TO STA. 12+60 21' LT.			111				
STA. 12+60 21' LT.					1		
STA. 13+30 24' RT.					1		
STA. 12+60 21' LT. TO STA. 13+30 24' RT.				82			
STA. 44+71.6 19.3' RT. (KNOX RD.) TO STA. 13+30 24' RT.			135				
KNOX ROAD							
STA. 44+71.6 19' LT.						1	
STA. 44+71.6 19.3' LT. TO 19' RT.	39						
STA. 44+71.6 19' RT.					1		
STA. 44+71.6 19' RT. TO STA. 45+12 19' RT.		35					
STA. 45+12 19' RT.					1		
STA. 45+12 19' LT. TO 19' RT.	38						
STA. 45+12 19' LT.						1	
STA. 45+12 19' RT. TO STA. 46+08 19' RT.		87					
STA. 46+08 19' RT.					1		
STA. 46+08 19' LT. TO 19' RT.	38						
STA. 46+08 19' LT.						1	
STA. 46+08 19' RT. TO STA. 47+04 19' RT.		87					
STA. 47+04 19' RT.					1		
STA. 47+04 19' LT. TO 19' RT.	38						
STA. 47+04 19' LT.						1	
STA. 47+04 19' RT. TO STA. 48+00 19' RT.		88					
STA. 48+00 19' RT.							1
STA. 48+00 19' LT. TO 19' RT.	38						
STA. 48+00 19' LT.						1	
TOTAL	191	297	246	82	6	5	1

60605000 COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24	
LOCATION	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 FOOT
	LOGISTICS PARKWAY
STA. 12+19 TO STA. 14+92 LT.	273
STA. 12+19 TO STA. 13+57 RT.	138
KNOX ROAD	
STA. 43+64 TO STA. 49+43.50 RT.	592
STA. 43+84 TO STA. 49+43.50 LT.	590
TOTAL	1593

42400100 PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH	
LOCATION	PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH SQ. FT.
	LOGISTICS PARKWAY
STA. 12+19 TO STA. 14+92 LT.	1365
STA. 12+19 TO STA. 14+02 RT.	929
TOTAL	2294

50100300 REMOVAL OF EXISTING STRUCTURES NO. 1	
LOCATION	REM. OF EXIST. STRUCTURES NO.1 EACH
	US 150
STA. 78+10 (LARGE GATES CORPORATION SIGN)	1
TOTAL	1

PIPE CULVERTS								
LOCATION	542D1063 PIPE CULVERTS, CLASS D, TYPE 2 18" FOOT	542A0223 PIPE CULVERTS, CLASS A, TYPE 1 18" FOOT	542A1075 PIPE CULVERTS, CLASS A, TYPE 2 30" FOOT	542A5479 PIPE CULVERTS, CLASS A TYPE 1, EQUIVALENT ROUND SIZE 24" FOOT	54215553 METAL END SECTIONS 18" EACH	54213663 PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18" EACH	54213675 PRECAST REINFORCED CONCRETE FLARED END SECTIONS 30" EACH	54214509 PRECAST REINF. CONC. FLARED END SECTIONS EQ. ROUND SIZE 24" EACH
	US 150							
STA. 73+50 62.5' LT. TO 49.5' RT.			112				2	
STA. 76+71 49' LT. TO STA. 77+29 48.5' LT.	58				2			
STA. 84+06 32' RT. TO STA. 84+41 21' RT.		36				1		
KNOX ROAD								
STA. 52+00 22' LT. TO 28' RT.				50				2
LOGISTICS PARKWAY								
STA. 11+57.6 64.2' LT.						1		
TOTAL	58	36	112	50	2	2	2	2

44000400 GUTTER REMOVAL	
LOCATION	GUTTER REMOVAL FOOT
	US 150
STA. 58+31 19' RT. TO STA. 63+51 30' LT.	523
STA. 69+34 8' RT. TO STA. 73+24 27' LT.	397
TOTAL	920

42400800 DETECTABLE WARNINGS	
LOCATION	DETECTABLE WARNINGS SQ. FT.
	LOGISTICS PARKWAY
STA. 13+98 TO STA. 14+00 RT.	10
TOTAL	10

48203100 HOT MIX ASPHALT SHOULDERS	
LOCATION	HMA SHOULDERS TON
	US 150
STA. 54+95 TO STA. 64+58.27 LT. & RT.	626
STA. 67+08.27 TO 85+50 LT. & RT.	1203
STA. 77+00 FE LT. ADDITIONAL SHLDR.	4
TOTAL	1833



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

QUANTITY SCHEDULES

SCALE: NONE SHEET NO. 3 OF 6 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	25
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

GUARDRAIL SCHEDULE						
LOCATION	63000001 SPBGR, TYPE A, 6' POSTS	63100085 TRAFFIC BARRIER TERMINAL, TYPE 6	63100167 TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	63100169 TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED	72501000 TERMINAL MARKER DIRECT APPLIED	63200310 GUARDRAIL REMOVAL
	FOOT	EACH	EACH	EACH	EACH	FOOT
US 150						
STA. 62+65.3 TO STA. 63+15.1 RT.				1		
STA. 63+15.1 TO STA. 64+15.0 RT.	100					
STA. 64+15.0 TO STA. 64+57.7 RT.		1				
STA. 63+07.7 TO STA. 63+57.7 LT.			1			
STA. 63+57.7 TO STA. 64+45.2 LT.	87.5					
STA. 64+45.2 TO STA. 64+88.9 LT.		1				
STA. 66+78.0 TO STA. 67+21.2 RT.		1				
STA. 67+21.2 TO STA. 76+84.4 RT.	987.5					
STA. 76+84.4 TO STA. 77+34.4 RT.			1			
STA. 67+08.9 TO STA. 67+51.6 LT.		1				
STA. 67+51.6 TO STA. 68+01.2 LT.	50					
STA. 68+01.2 TO STA. 68+53.5 LT.			1			
STA. 62+65.3 RT.				1		
STA. 63+07.7 LT.				1		
STA. 77+34.4 RT.				1		
STA. 68+53.5 LT.				1		
STA. 62+66 68' LT. TO STA. 65+51 40' LT.						289
STA. 63+04 34' LT. TO STA. 65+27 11' LT.						228
STA. 66+59 12' RT. TO STA. 70+89 46' RT.						450
STA. 66+82 17' LT. TO STA. 69+79 13' RT.						299
TOTAL	1225	4	3	1	4	1266

66600105 FURNISHING AND ERECTING RIGHT OF WAY MARKERS	
LOCATION	RIGHT OF WAY MARKERS
	EACH
LOGISTICS PARKWAY	
STA. 11+90 160' LT. & 55' LT.	2
STA. 15+00 50' LT. & 50' RT.	2
STA. 11+50 45' RT.	1
STA. 13+50.50 63' RT.	1
KNOX ROAD	
STA. 44+25 35' LT.	1
STA. 44+68 35' RT.	1
STA. 44+85.03 35' LT. & RT.	2
STA. 47+80.84 35' LT. & RT.	2
STA. 49+43.50 35' LT. & RT.	2
STA. 50+00 35' LT.	1
STA. 54+46.93 35' LT.	1
STA. 55+30 25.67' LT.	1
US 150	
STA. 52+09.03 50.75' LT.	1
STA. 54+49.73 50.73' LT.	1
STA. 60+15 82.78' RT.	1
STA. 63+00 130' RT.	1
STA. 64+09.46 130' RT.	1
STA. 65+65.15 100' RT.	1
STA. 67+00 100' RT.	1
STA. 67+98.95 105' RT.	1
STA. 70+00 105' RT.	1
STA. 71+03.69 90.07' RT.	1
STA. 73+19.52 67.16' RT.	1
STA. 74+00 60' RT.	1
STA. 74+50 65' RT.	1
STA. 76+68.82 65' RT.	1
STA. 77+50 65' RT.	1
STA. 79+80.36 50' RT.	1
STA. 83+79.31 50' RT.	1
STA. 85+50 50' RT.	1
STA. 86+00 29.93' RT.	1
TOTAL	36

72400310 REMOVE SIGN PANEL TYPE 1	
LOCATION	REMOVE SIGN PANEL TYPE 1
	SO. FT.
KNOX ROAD	
STA. 58+69 40' LT. (R1-1)	9
US 150	
STA. 59+93 55' LT. (W1-6)	8
STA. 60+83 TO STA. 63+50 LT. (3-W1-8) BOTH SIDES	18
STA. 68+42 TO STA. 72+50 RT. (5-W1-8) BOTH SIDES	30
STA. 73+92 10' RT. (R2-1)	8
TOTAL	73

50105220 PIPE CULVERT REMOVAL	
LOCATION	PIPE CULVERT REMOVAL
	FOOT
US 150	
STA. 76+81 97' LT. TO STA. 77+15 99' LT.	34
KNOX ROAD	
STA. 51+66 42' RT. TO STA. 51+97 67' RT.	42
TOTAL	76

64200116 SHOULDER RUMBLE STRIPS 16"	
LOCATION	SHOULDER RUMBLE STRIPS 16"
	FOOT
US 150	
STA. 54+95 TO STA. 64+48.2 RT.	954
STA. 54+95 TO STA. 64+68.3 LT.	974
STA. 66+98.2 TO 84+45 RT.	1747
STA. 67+18.3 TO 85+50 LT.	1784
TOTAL	5459

66700205 PERMANENT SURVEY MARKERS, TYPE 1	
LOCATION	EACH
P.O.T. STA. 54+00 US 150 CENTERLINE	1
P.C. STA. 56+33.80 US 150 CENTERLINE	1
P.T. STA. 62+27.18 US 150 CENTERLINE	1
P.C. STA. 62+98.96 US 150 CENTERLINE	1
P.T. STA. 76+68.82 US 150 CENTERLINE	1
P.C. STA. 79+80.36 US 150 CENTERLINE	1
P.T. STA. 83+79.31 US 150 CENTERLINE	1
P.O.T. STA. 85+98.44 US 150 CENTERLINE	1
P.O.T. S.N. 048-0096 CENTERLINE	1
TOTAL	9

X7015005 CHANGEABLE MESSAGE SIGN	
LOCATION	CAL. DAY
JOBSITE (2 SIGNS @ 2 MONTHS EACH)	120
TOTAL	120

LOCATION	66900200 NON-SPECIAL WASTE DISPOSAL	66900450 SPECIAL WASTE PLANS AND REPORTS	66900530 SOIL DISPOSAL ANALYSIS
	CU YD	LUMP SUM	EACH
ENTIRE JOB SITE*	5900	1	4
TOTAL	5900	1	4

*SEE "REMOVAL AND DISPOSAL OF REGULATED SUBSTANCES"
SPECIAL PROVISION FOR BREAKDOWN OF LOCATIONS.



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

QUANTITY SCHEDULES		
SCALE: NONE	SHEET NO. 4 OF 6 SHEETS	STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	26
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

PAVEMENT MARKING

LOCATION	78009000	78009004		78009006	78009008		78009012	78009024
	MODIFIED URETHANE PAVEMENT MARKING LETTERS AND SYMBOLS SQ. FOOT	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"		MODIFIED URETHANE PAVEMENT MARKING LINE 6" WHITE 2' LINES 6' DASH FOOT	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"		MODIFIED URETHANE PAVEMENT MARKING LINE 12" FOOT	MODIFIED URETHANE PAVEMENT MARKING LINE 24" FOOT
		WHITE FOOT	YELLOW FOOT		WHITE 2' LINES 6' DASH FOOT	WHITE SOLID LINE FOOT		
LOGISTICS PARKWAY								
STA. 10+56 LT.								46
STA. 10+56 TO STA. 12+19			326	42		163		
STA. 12+19 TO STA. 13+70			622		66		68	
STA. 10+66 - 18' LT., 6' LT., 6' RT.	24.1							
STA. 11+46 - 18' LT., 6' LT., 6' RT.	24.1							
STA. 12+26 - 18' LT., 6' LT., 6' RT.	24.1							
KNOX ROAD								
STA. 43+99 LT.								36
STA. 43+99 TO STA. 45+14		115	230			115		
STA. 45+14 TO STA. 55+30		1173	4072				384	
STA. 44+10 CL	8.8							
STA. 44+90 CL	8.8							
US 150								
STA. 54+70 CL	8.8							
STA. 54+50 TO STA. 54+95			90			45		
STA. 54+95 TO STA. 55+65		140	140			70		
STA. 55+50 CL	8.8							
STA. 55+65 TO STA. 56+33					18			
STA. 55+65 TO STA. 59+90		850	1720				127	
STA. 59+90 TO STA. 85+50		5120	5120					
SUB-TOTAL	107.5	7398	12320	42	84	393	579	82
USE	108							
TOTAL	108		19718	42	477		579	82

Z0065100 SETTLEMENT PLATFORMS

LOCATION	SETTLEMENT PLATFORM EACH
US 150	
STA. 62+00 25' LT. & 25' RT.	2
STA. 64+00 22' RT.	1
TOTAL	3

X0325833 WICK DRAINS

LOCATION	WICKS DRAINS FOOT
US 150	
STA. 60+00 10' LT., 30' RT. TO	
STA. 64+50 13' RT., 65' RT.	33993
TOTAL	33993

Z0056100 SAND DRAINAGE BLANKET

LOCATION	SAND BLANKET CU. YD.
US 150	
STA. 60+00 30' RT., 70' RT. TO	
STA. 64+50 65' RT., 84' RT.	2565
TOTAL	2565

GROOVING FOR RECESSED PAVEMENT MARKING

LOCATION	7830060	7830070		7830074	7830076		7830078	7830090
	GROOVING FOR RECESSED PAVEMENT MARKING LETTERS AND SYMBOLS SQ. FOOT	GROOVING FOR RECESSED PAVEMENT MARKING 5"		GROOVING FOR RECESSED PAVEMENT MARKING 7" 2' LINES 6' DASH FOOT	GROOVING FOR RECESSED PAVEMENT MARKING 9" 2' LINES SOLID LINE 6' DASH FOOT		GROOVING FOR RECESSED MODIFIED URETHANE PAVEMENT MARKING 13" FOOT	GROOVING FOR RECESSED MODIFIED URETHANE PAVEMENT MARKING 25" FOOT
		EDGE FOOT	CENTERLINE FOOT					
LOGISTICS PARKWAY								
STA. 10+56 LT.								46
STA. 10+56 TO STA. 12+19			326	42		163		
STA. 12+19 TO STA. 13+70			622		66		68	
STA. 10+66 - 18' LT., 6' LT., 6' RT.	26.6							
STA. 11+46 - 18' LT., 6' LT., 6' RT.	26.6							
STA. 12+26 - 18' LT., 6' LT., 6' RT.	26.6							
KNOX ROAD								
STA. 43+99 LT.								36
STA. 43+99 TO STA. 45+14		115	230			115		
STA. 45+14 TO STA. 55+30		1173	4072				384	
STA. 44+10 CL	9.7							
STA. 44+90 CL	9.7							
SUB-TOTAL	99.2	1288	5250	42	66	278	452	82
USE	100							
TOTAL	100		6538	42	344		452	82



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

QUANTITY SCHEDULES

SCALE: NONE SHEET NO. 5 OF 6 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	27
			CONTRACT NO. 68800	
ILLINOIS FED. AID PROJECT				

78100100 RAISED REFLECTIVE PAVEMENT MARKERS & 78300200 RAISED REFLECTIVE MARKER REMOVAL				
LOCATION	RAISED REFLECTIVE PAVEMENT MARKER 1 WAY CRYSTAL	RAISED REFLECTIVE PAVEMENT MARKER 1 WAY AMBER	RAISED REFLECTIVE PAVEMENT MARKER 2 WAY AMBER	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL
	EACH	EACH		EACH
US 150				
STA. 54+95 TO STA. 56+33.8 6' LT.	3			
STA. 55+65 TO STA. 59+90 (IN PAINTED MEDIAN)		20		
STA. 54+95 TO STA. 55+65 CENTERLINE			2	
STA. 59+90 TO STA. 64+58.27			12	
STA. 67+08.27 TO STA. 85+50			46	
STA. 54+95 TO STA. 55+80				2
STA. 84+65 TO STA. 85+50				2
SUB-TOTAL	3	20	60	4
TOTAL		83		4

Z0034105 MATERIAL TRANSFER DEVICE	
LOCATION	MATERIAL TRANSFER DEVICE
	TON
US 150	
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	72
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'D', N50	866
HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 10 1/2"	4207
BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	32
TOTAL	5177

SIGN PANEL SCHEDULE		
LOCATION	72000100 SIGN PANEL TYPE 1	72000200 SIGN PANEL TYPE 2
	SQ. FT.	SQ. FT.
LOGISTICS PARKWAY		
STA. 10+56 40' LT. (R1-1)	9	
STA. 14+95 (OM4-1) 4 TOTAL	36	
STA. 14+95 CL (R11-100)		10
KNOX ROAD		
STA. 44+05 35' LT. (R1-1)	9	
STA. 49+00 TO STA. 54+00 LT. (6-W1-8) BOTH SIDES (18"X24")	36	
US 150		
STA. 56+30 TO STA. 64+30 LT. (6-W1-8) BOTH SIDES (24"X30")	60	
STA. 67+50 TO STA. 77+10 RT. (9-W1-8) BOTH SIDES (24"X30")	90	
STA. 79+40 TO STA. 84+20 LT. (4-W1-8) BOTH SIDES (24"X30")	40	
STA. 56+30 TO STA. 64+30 LT. SIGN SUPPORT (BOTH SIDES)	5	
STA. 67+50 TO STA. 77+10 RT. SIGN SUPPORT (BOTH SIDES)	8	
STA. 79+40 TO STA. 84+20 LT. SIGN SUPPORT (BOTH SIDES)	3	
STA. 76+50 20' RT. (R2-1)	8	
TOTAL	304	10

72800100 TELESCOPING STEEL SIGN SUPPORT	
LOCATION	TEL. STEEL SIGN SUPPORT
	FOOT
US 150	
STA. 56+30 TO STA. 64+30 LT. (6-W1-8)	72
STA. 67+50 TO STA. 77+10 RT. (9-W1-8)	108
STA. 79+40 TO STA. 84+20 LT. (4-W1-8)	48
KNOX ROAD	
STA. 49+00 TO STA. 54+00 LT. (6-W1-8)	72
TOTAL	300

89502375 REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	
	QUANTITY REMOVAL
	EACH
US 150	
STA. 59+93 55' LT.	1
STA. 72+50 36' RT.	1
TOTAL	2

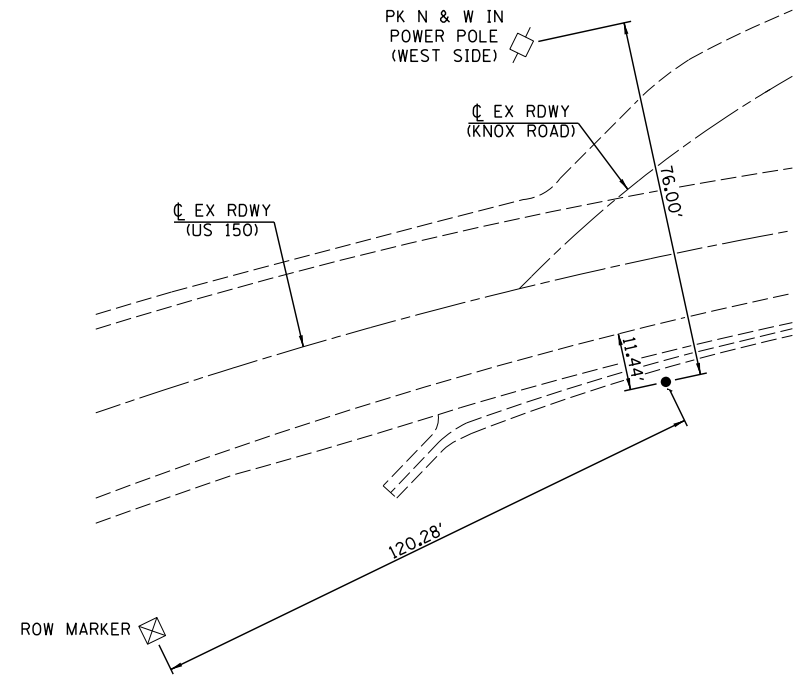
73000100 WOOD SIGN SUPPORT	
LOCATION	WOOD SIGN SUPPORT
	FOOT
LOGISTICS PARKWAY	
STA. 10+56 40' LT. (R1-1)	12
STA. 14+95 (8', 12' LT & 8', 12' RT.) (4-OM4-1)	48
STA. 14+95 CENTERLINE (R11-I100)	12
KNOX ROAD	
STA. 44+05 35' LT. (R1-1)	12
TOTAL	84

Z0001002 GUARDRAIL AGGREGATE EROSION CONTROL	
LOCATION	GUARDRAIL AGGREGATE EROSION CONTROL
	TON
US 150	
STA. 62+36.4 TO STA. 64+71.5 RT.	43
STA. 63+07.7 TO STA. 65+05.1 LT.	28
STA. 66+61.5 TO STA. 77+68.4 RT.	163
STA. 66+95.1 TO STA. 69+67.3 LT.	40
TOTAL	274

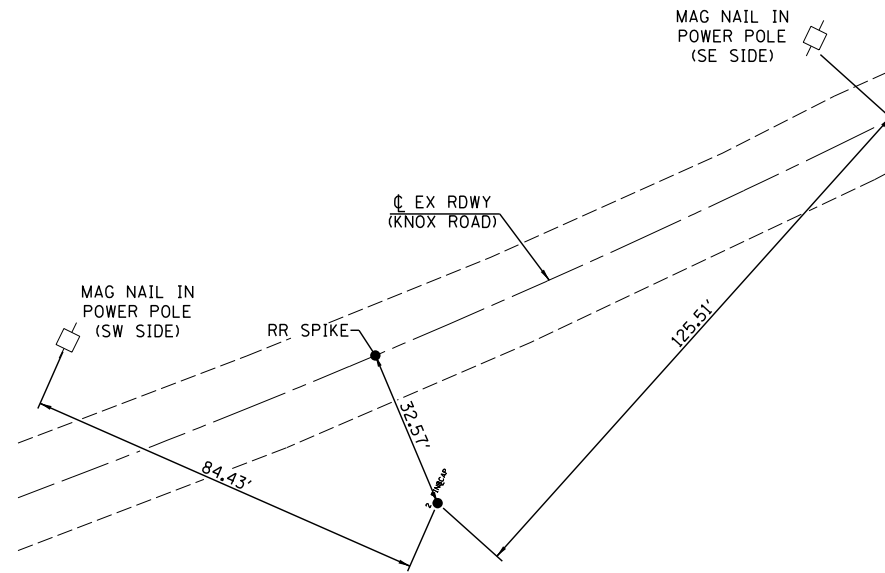
GUARDRAIL REFLECTORS		
LOCATION	78200005 REFLECTORS TYPE A	78200011 REFLECTORS TYPE C
	EACH	EACH
US 150		
STA. 62+65.3 TO STA. 64+58.2 RT.	4	
STA. 63+07.7 TO STA. 64+88.2 LT.	4	
STA. 66+78.4 TO STA. 77+34.4 RT.	15	
STA. 67+09.1 TO STA. 68+53.5 LT.	4	
STA. 64+88.27 TO STA. 66+78.27 LT. & RT.		8
TOTAL	27	8

50102400 CONCRETE REMOVAL	
LOCATION	CONCRETE REMOVAL
	CU. YD.
US 150	
CENTERLINE STA. 73+56 (CONCRETE CULVERT)	13.7
TOTAL	13.7

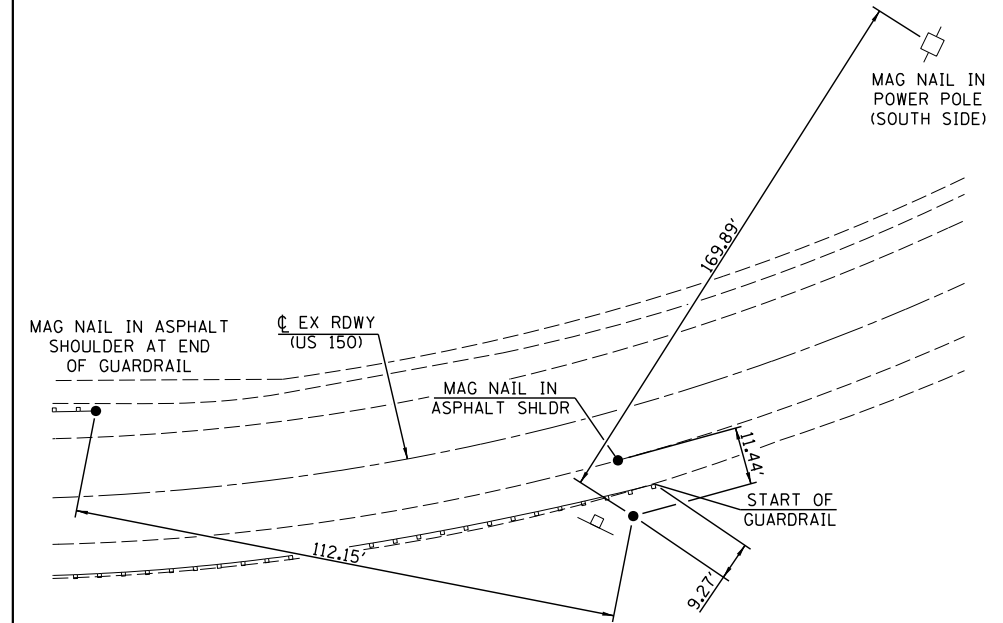
D2002260 EVERGREEN PICEA PUNGENS GLAUCA (COLORADO BLUE SPRUCE) 5' HEIGHT, BALLED AND BURLAPPED	
LOCATION	78200005 REFLECTORS TYPE A
	EACH
US 150	
STA. 58+75 68' LT.	1
STA. 59+00 75' LT.	1
STA. 59+25 75' LT.	1
STA. 59+50 75' LT.	1
STA. 59+75 80' LT.	1
STA. 60+00 85' LT.	1
STA. 60+25 85' LT.	1
TOTAL	7



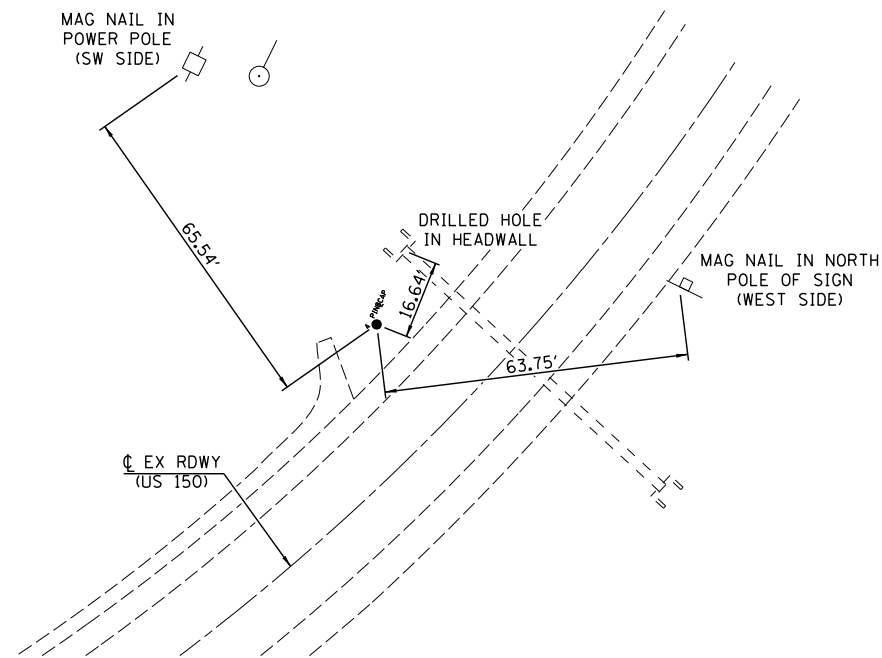
TIE #1
 IRON PIN WITH VEC CAP
 25.77' RT STA. 58+94.91 (EXIST. US 150)
 N 1551624.4000
 E 2252844.3800



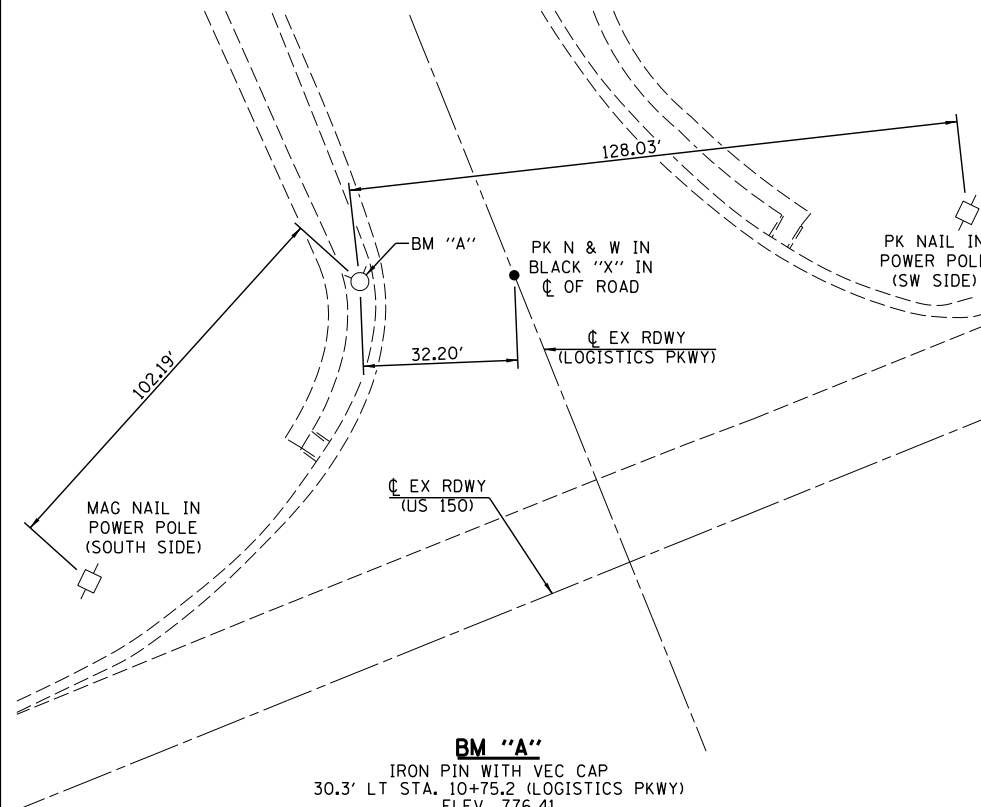
TIE #2
 IRON PIN WITH VEC CAP
 34.41' RT STA. 54+07.00 (PROP. KNOX ROAD)
 N 1551366.3300
 E 2253151.6600



TIE #3
 IRON PIN WITH VEC CAP
 22.15' RT STA. 71+14.07 (EXIST. US 150)
 N 1550486.9300
 E 2253206.9400



TIE #4
 IRON PIN WITH VEC CAP
 24.38' LT STA. 73+83.14 (EXIST. US 150)
 N 1550379.3000
 E 2253453.4900



BM "A"
 IRON PIN WITH VEC CAP
 30.3' LT STA. 10+75.2 (LOGISTICS PKWY)
 ELEV. 776.41
 N 1552078.2990
 E 2252511.4390



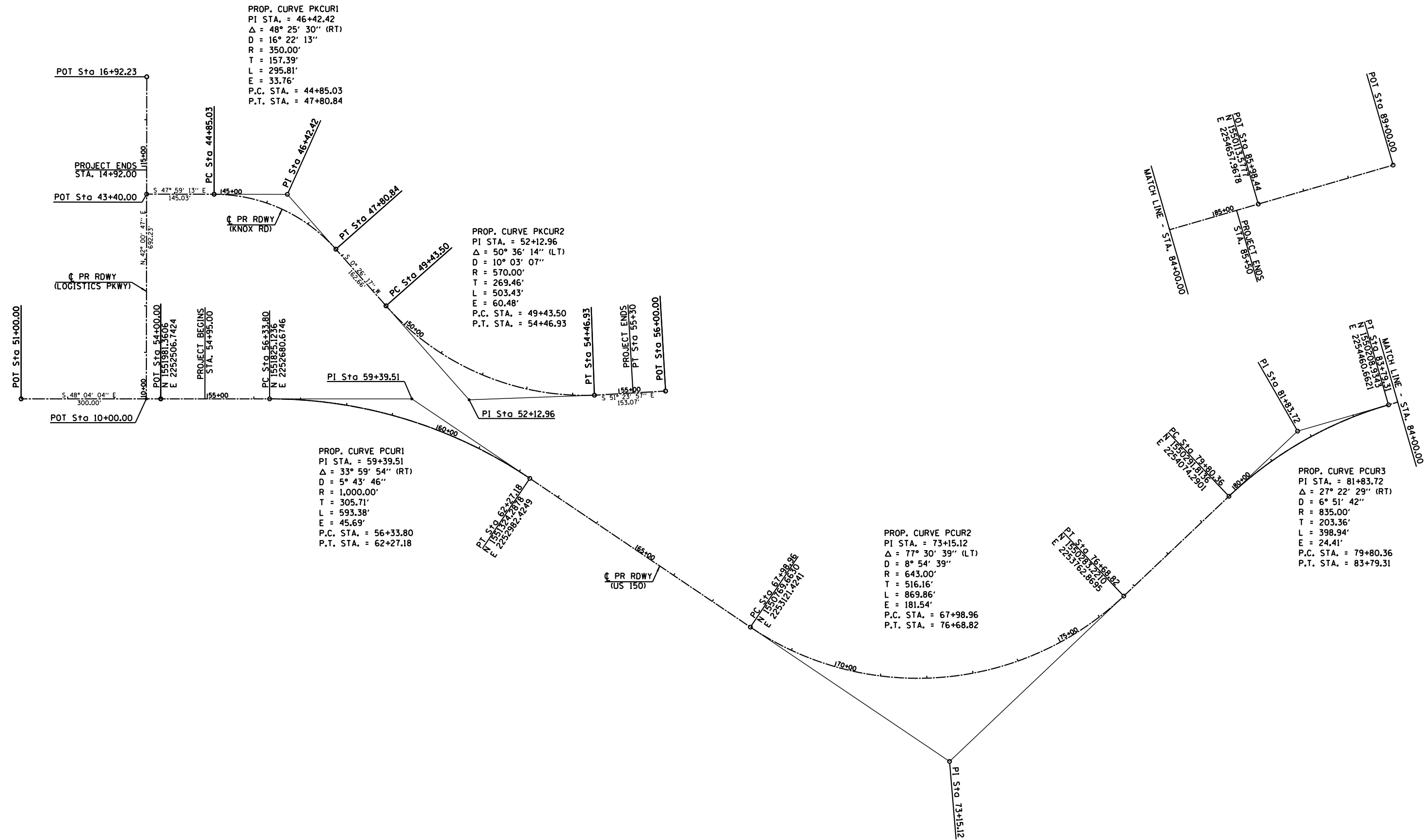
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PLOT SCALE =	CHECKED -	REVISED -
PLOT DATE =	DRAWN -	REVISED -
	CHECKED -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TIES & BENCHMARKS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	29
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



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PLOT DATE =	DRAWN -	REVISED -
	CHECKED -	REVISED -

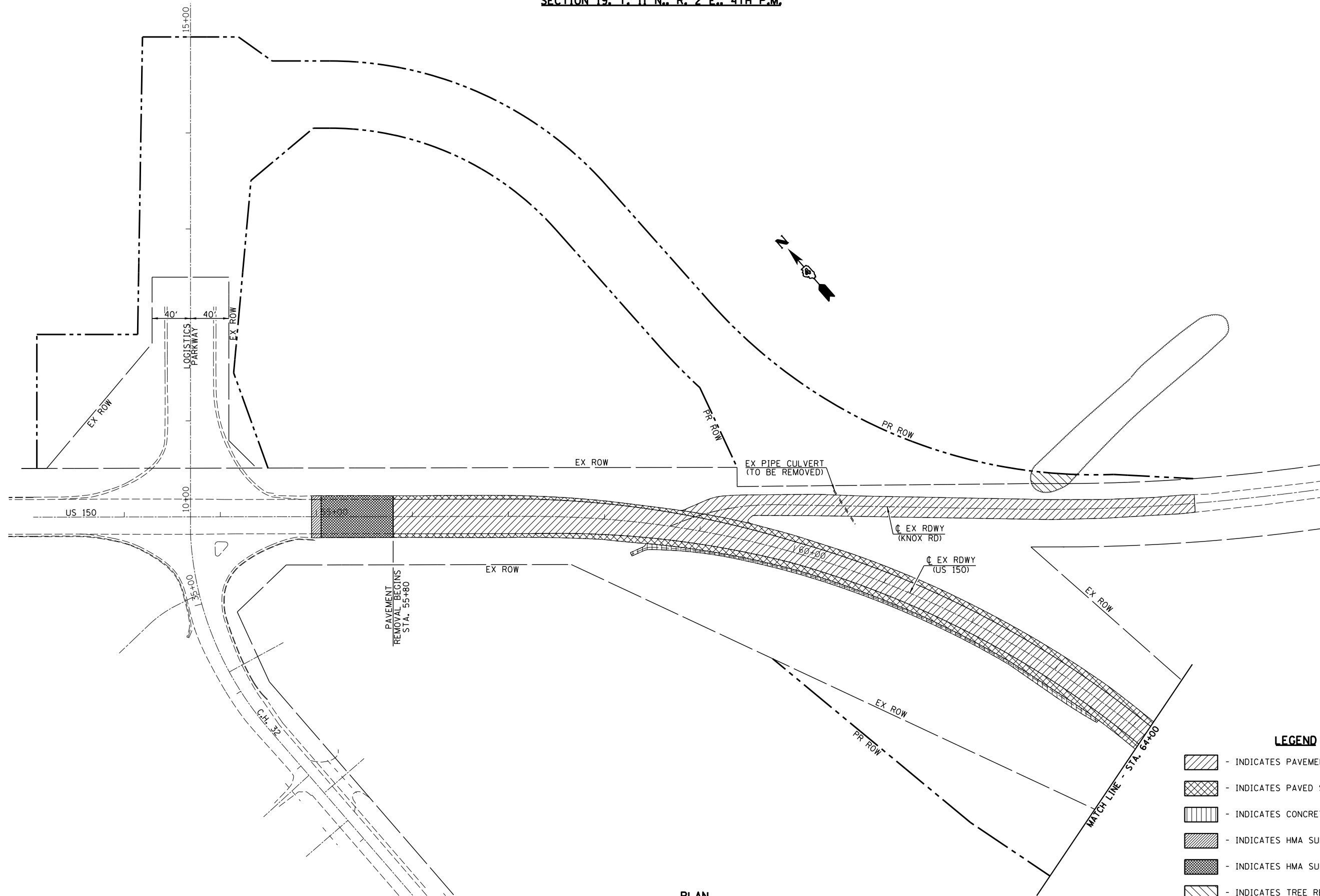
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**








ALIGNMENT PLAN

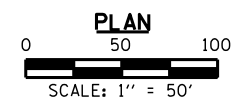
SCALE: 1" = 100' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	30
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

SECTION 19, T. 11 N., R. 2 E., 4TH P.M.



- LEGEND**
-  - INDICATES PAVEMENT REMOVAL
 -  - INDICATES PAVED SHOULDER REMOVAL
 -  - INDICATES CONCRETE GUTTER REMOVAL
 -  - INDICATES HMA SURFACE REMOVAL-BUTT JOINT
 -  - INDICATES HMA SURFACE REMOVAL, VARIABLE DEPTH
 -  - INDICATES TREE REMOVAL, ACRES
 -  - INDICATES TREE REMOVAL, (UNITS)



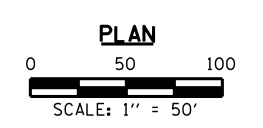
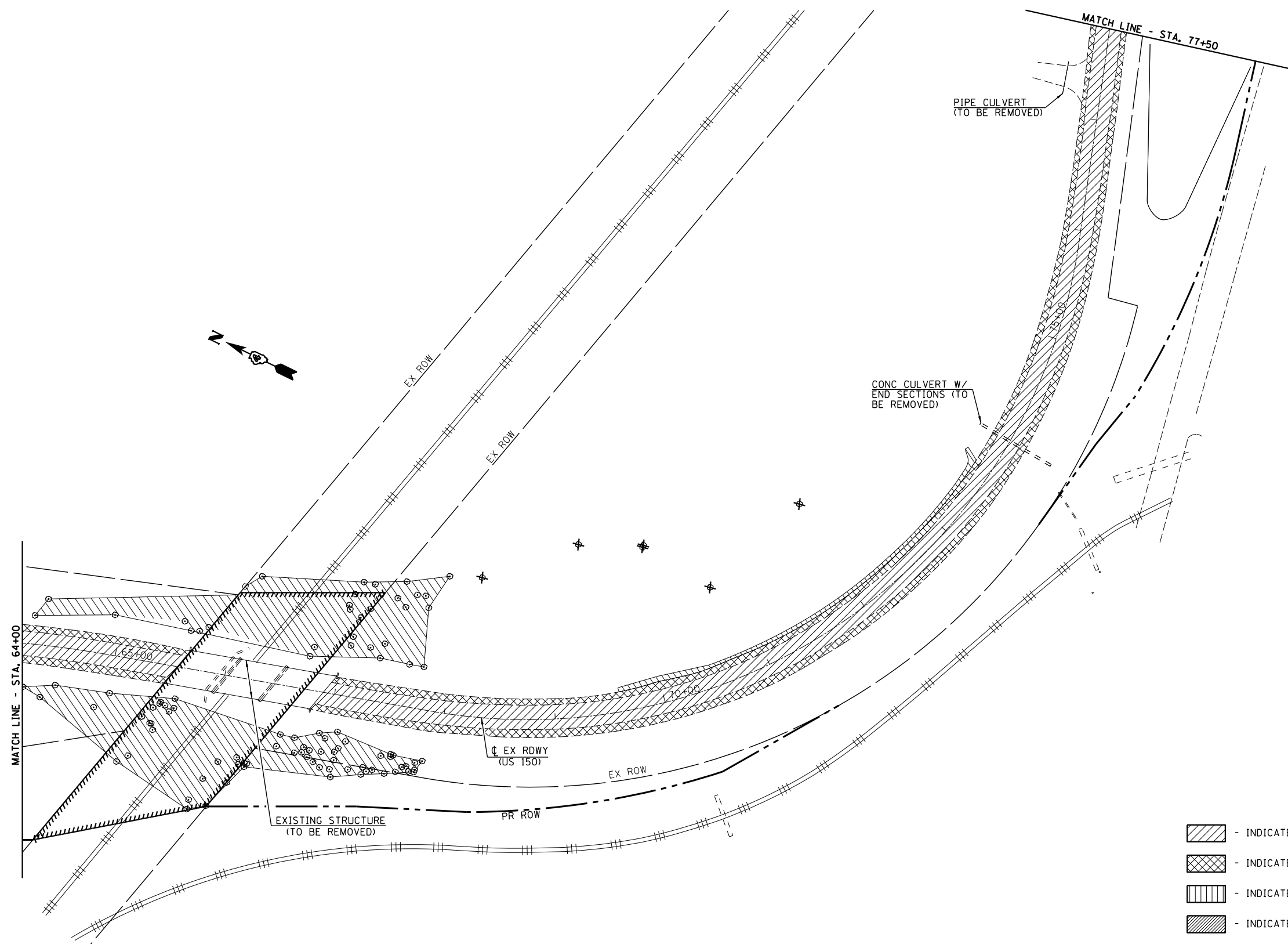
USER NAME =	DESIGNED -	REVISD -
CHECKED -	REVISD -	
PLOT SCALE =	DRAWN -	REVISD -
PLOT DATE =	CHECKED -	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REMOVAL PLAN - US 150 /KNOX RD.

SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	31
CONTRACT NO. 68800				
<small>ILLINOIS FED. AID PROJECT</small>				



- LEGEND**
- INDICATES PAVEMENT REMOVAL
 - INDICATES PAVED SHOULDER REMOVAL
 - INDICATES CONCRETE GUTTER REMOVAL
 - INDICATES HMA SURFACE REMOVAL-BUTT JOINT
 - INDICATES HMA SURFACE REMOVAL, VARIABLE DEPTH
 - INDICATES TREE REMOVAL, ACRES
 - INDICATES TREE REMOVAL, (UNITS)



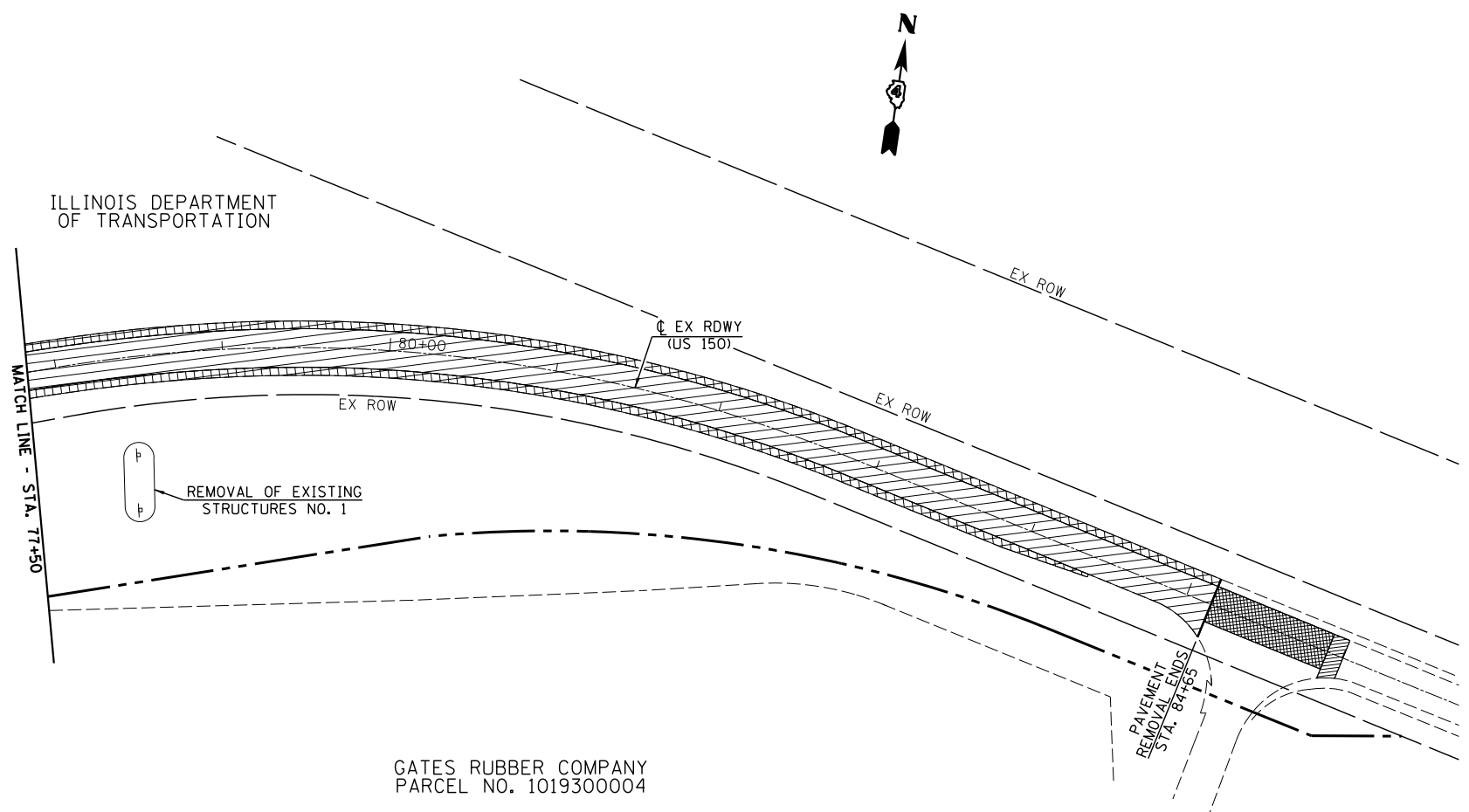
USER NAME =	DESIGNED -	REVISED -
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PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**








REMOVAL PLAN - US 150

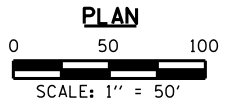
SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	32
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



LEGEND

-  - INDICATES PAVEMENT REMOVAL
-  - INDICATES PAVED SHOULDER REMOVAL
-  - INDICATES CONCRETE GUTTER REMOVAL
-  - INDICATES HMA SURFACE REMOVAL-BUTT JOINT
-  - INDICATES HMA SURFACE REMOVAL, VARIABLE DEPTH
-  - INDICATES TREE REMOVAL, ACRES
-  - INDICATES TREE REMOVAL, (UNITS)



USER NAME =	DESIGNED -	REVISED -
	CHECKED -	REVISED -
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

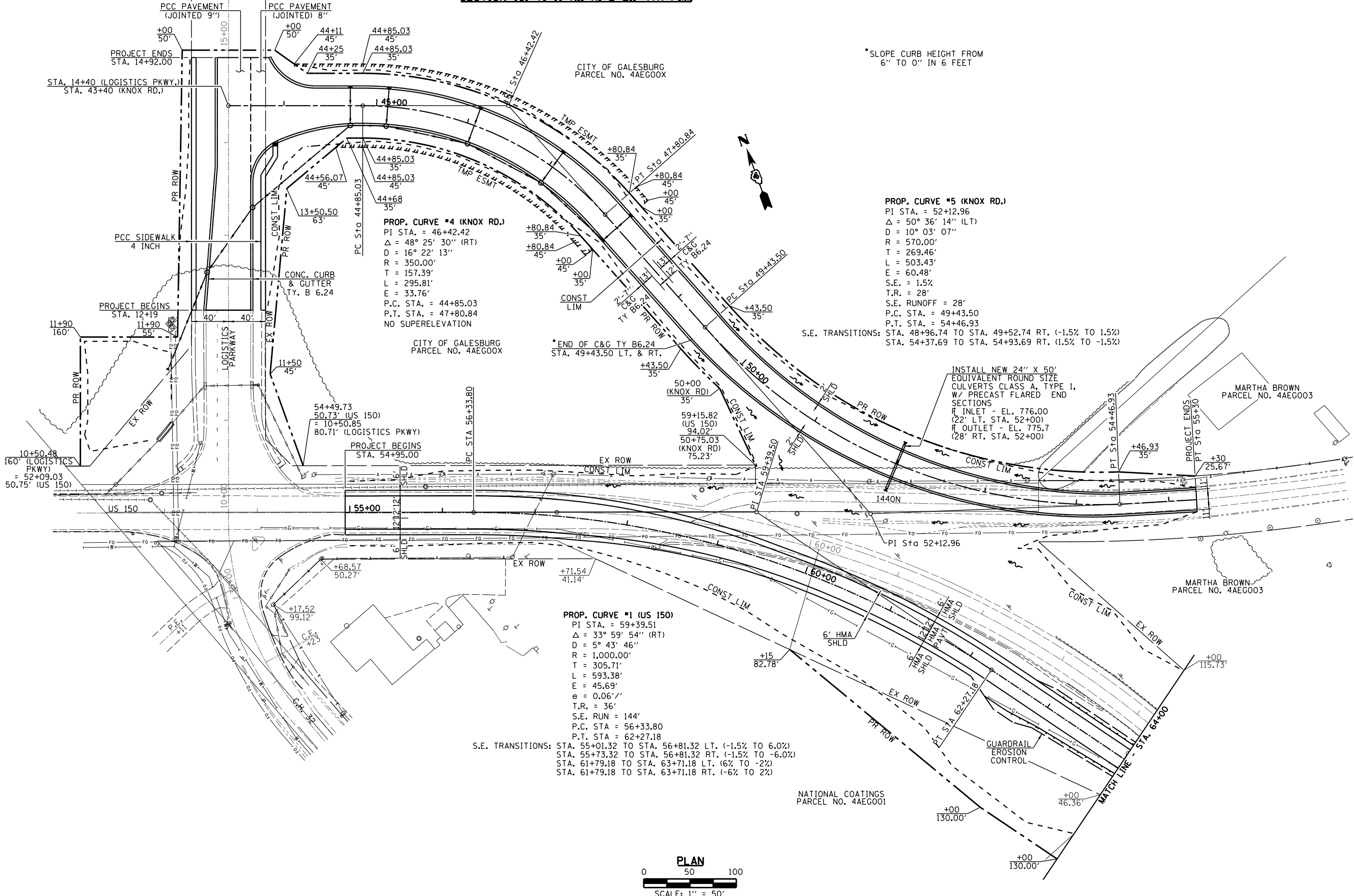
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REMOVAL PLAN - US 150

SCALE: 1" = 50' SHEET NO. 3 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	33
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

SECTION 19, T. 11 N., R. 2 E., 4TH P.M.

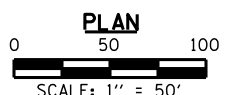


*SLOPE CURB HEIGHT FROM 6" TO 0" IN 6 FEET

PROP. CURVE #5 (KNOX RD.)
 PI STA. = 52+12.96
 Δ = 50° 36' 14" (LT)
 D = 10° 03' 07"
 R = 570.00'
 L = 269.46'
 E = 503.43'
 P.C. STA. = 49+43.50
 P.T. STA. = 54+46.93
 S.E. TRANSITIONS: STA. 48+96.74 TO STA. 49+52.74 RT. (-1.5% TO 1.5%)
 STA. 54+37.69 TO STA. 54+93.69 RT. (1.5% TO -1.5%)

INSTALL NEW 24" X 50' EQUIVALENT ROUND SIZE CULVERTS CLASS A, TYPE 1, W/ PRECAST FLARED END SECTIONS
 R INLET - EL. 776.00 (22' LT. STA. 52+00)
 R OUTLET - EL. 775.7 (28' RT. STA. 52+00)

PROP. CURVE #1 (US 150)
 PI STA. = 59+39.51
 Δ = 33° 59' 54" (RT)
 D = 5° 43' 46"
 R = 1,000.00'
 L = 305.71'
 E = 593.38'
 P.C. STA. = 56+33.80
 P.T. STA. = 62+27.18
 S.E. TRANSITIONS: STA. 55+01.32 TO STA. 56+81.32 LT. (-1.5% TO 6.0%)
 STA. 55+73.32 TO STA. 56+81.32 RT. (-1.5% TO -6.0%)
 STA. 61+79.18 TO STA. 63+71.18 LT. (6% TO -2%)
 STA. 61+79.18 TO STA. 63+71.18 RT. (-6% TO 2%)

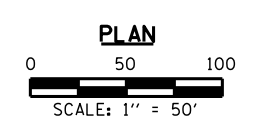
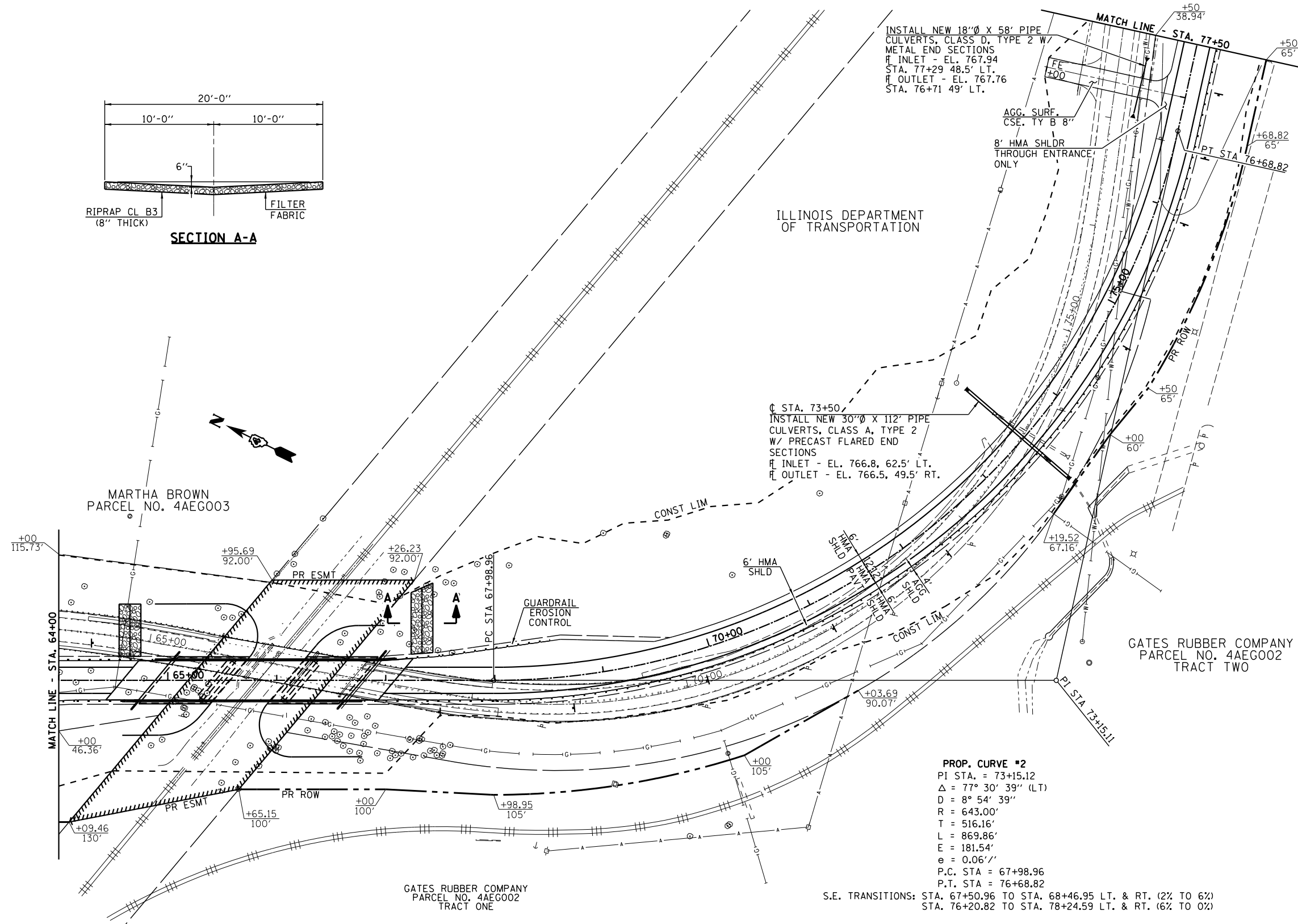
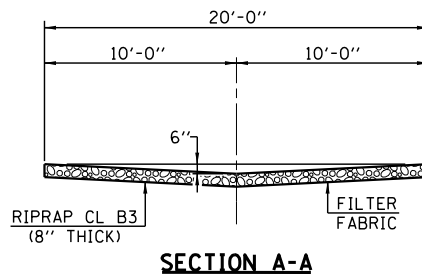


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	CHECKED -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PLAN - US 150 / KNOX RD.
 SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	34
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

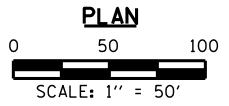
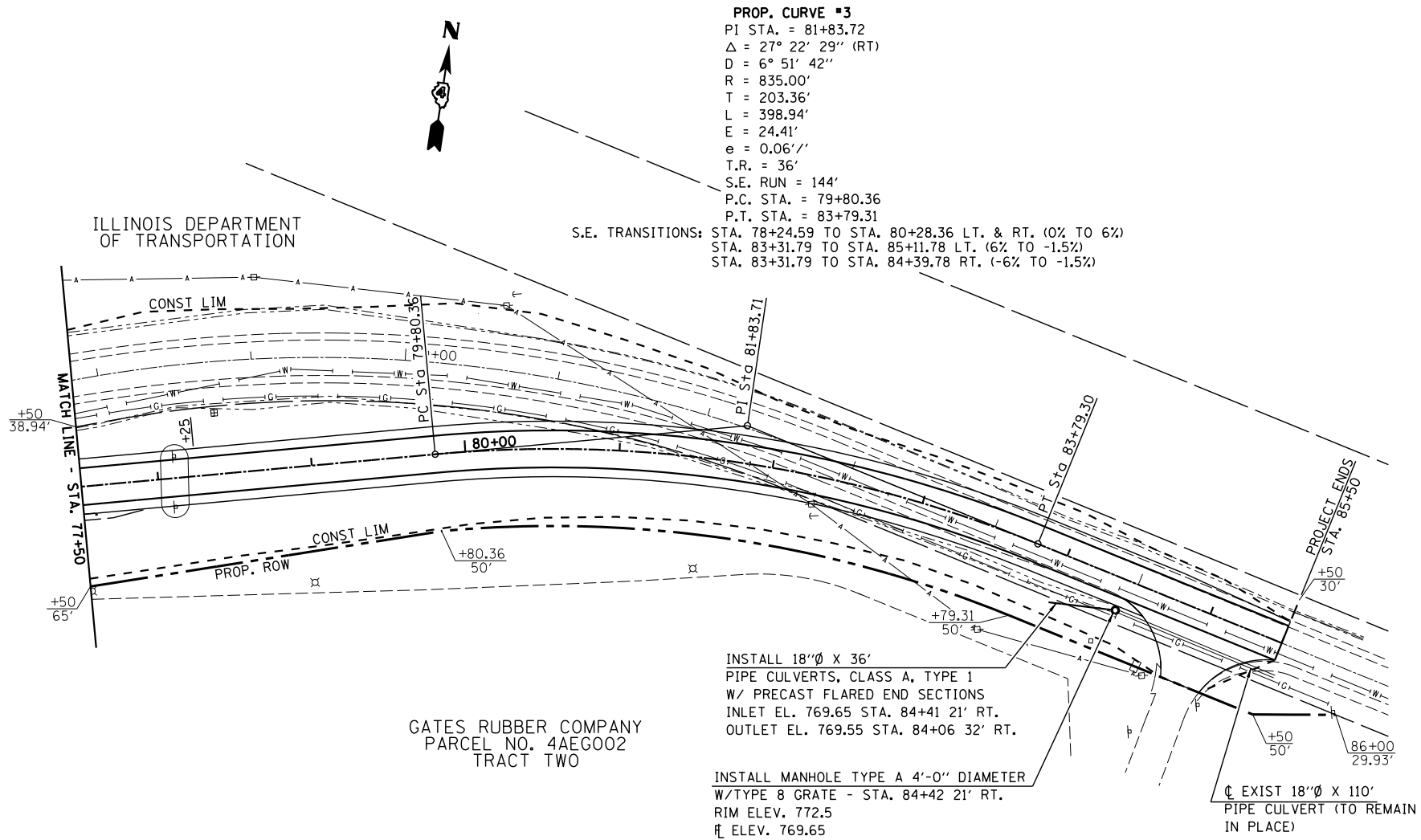


USER NAME =	DESIGNED -	REVISED -
PLOT SCALE =	CHECKED -	REVISED -
PLOT DATE =	DRAWN -	REVISED -
	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN - US 150	
SCALE: 1" = 50'	SHEET NO. 2 OF 3 SHEETS
STA.	TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	35
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



USER NAME =	DESIGNED -	REVISED -
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PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

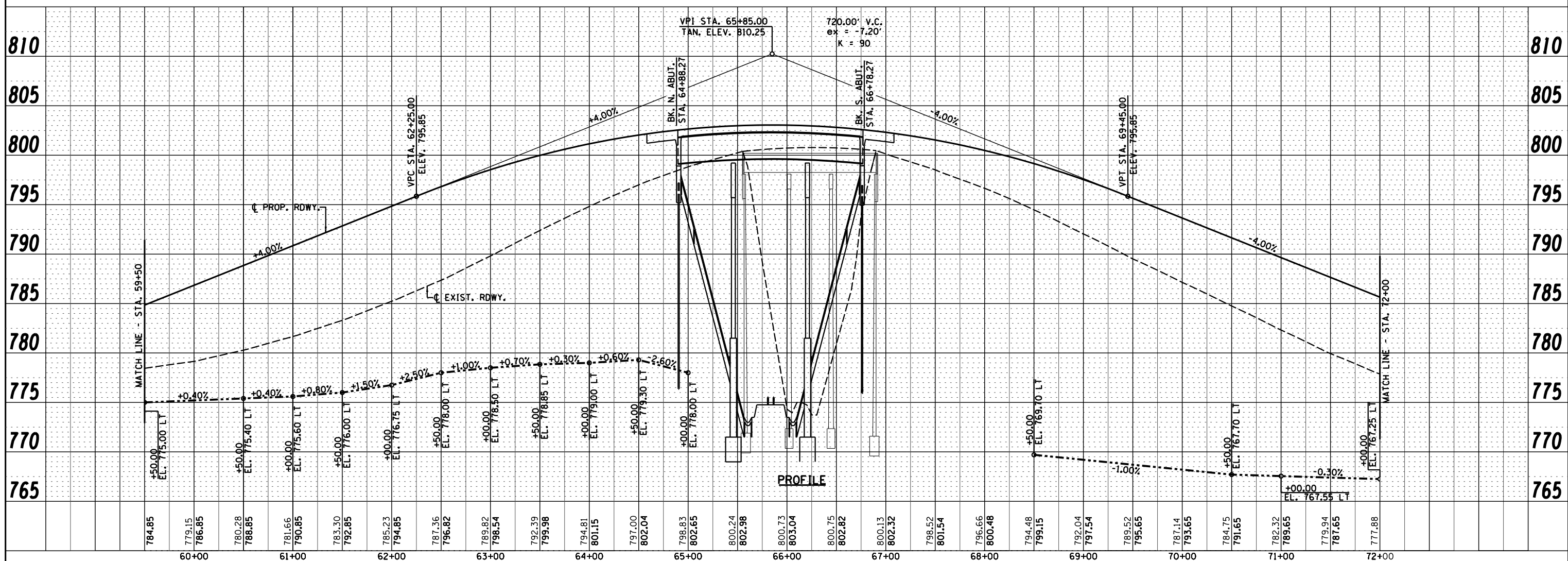
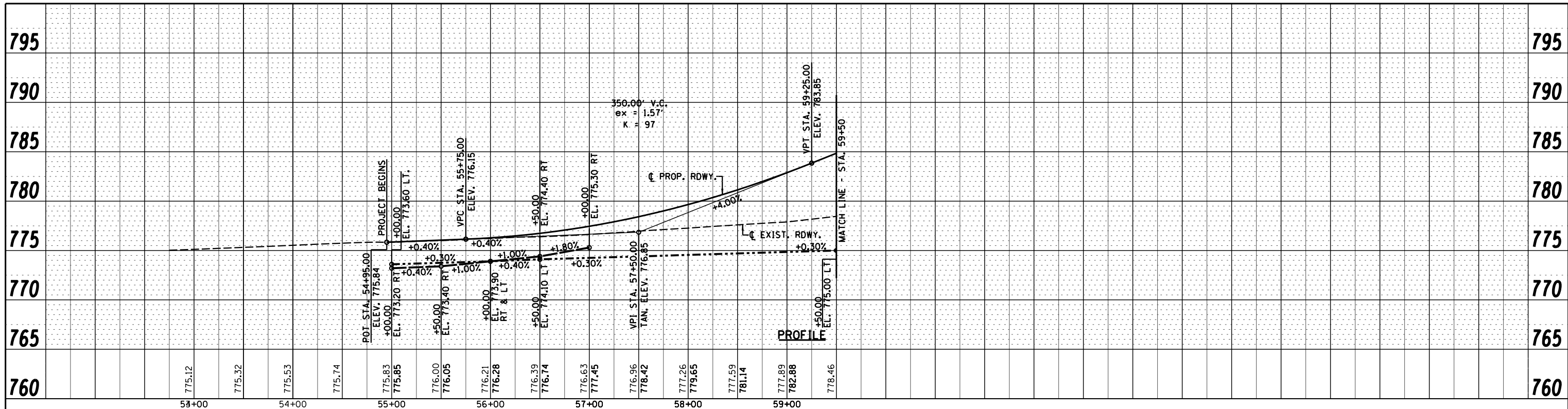
PLAN - US 150

SCALE: 1" = 50' SHEET NO. 3 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	36
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	DESIGNED		
	NO. _____		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS OK'D		
	NO. _____		



USER NAME =	DESIGNED -	REVISED -
PLOT SCALE =	CHECKED -	REVISED -
PLOT DATE =	DRAWN -	REVISED -
	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

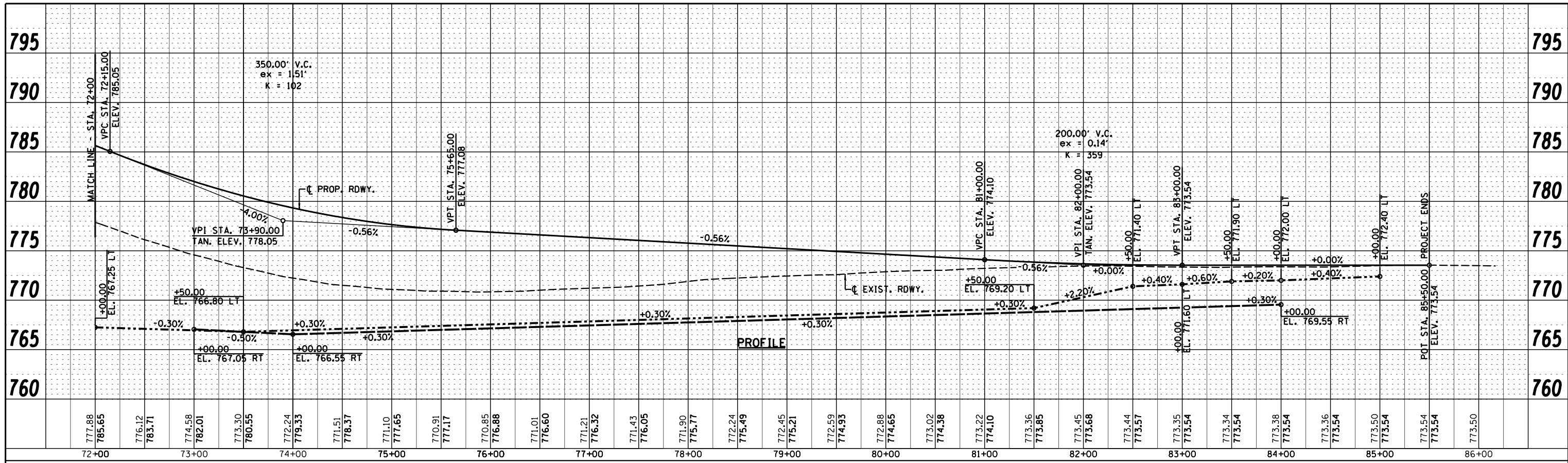
PROFILE - U.S. 150

SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	37
CONTRACT NO. 68800			ILLINOIS FED. AID PROJECT	

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	ALIGNMENT CHECKED		
	STRUCTURE NOTATIONS CHKD		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	ALIGNMENT CHECKED		
	STRUCTURE NOTATIONS CHKD		
	NO.		



777.88	776.12	774.58	772.24	771.51	770.91	770.85	771.01	771.21	771.43	771.90	772.24	772.59	772.88	773.02	773.22	773.36	773.45	773.44	773.35	773.34	773.38	773.36	773.50	773.54	773.54	773.50	
785.65	783.71	782.01	780.55	779.33	778.37	777.65	777.17	776.88	776.60	776.32	776.05	775.77	775.49	775.21	774.93	774.65	774.38	774.10	773.85	773.68	773.57	773.54	773.54	773.54	773.54	773.54	773.54
72+00	73+00	74+00	75+00	76+00	77+00	78+00	79+00	80+00	81+00	82+00	83+00	84+00	85+00	86+00													



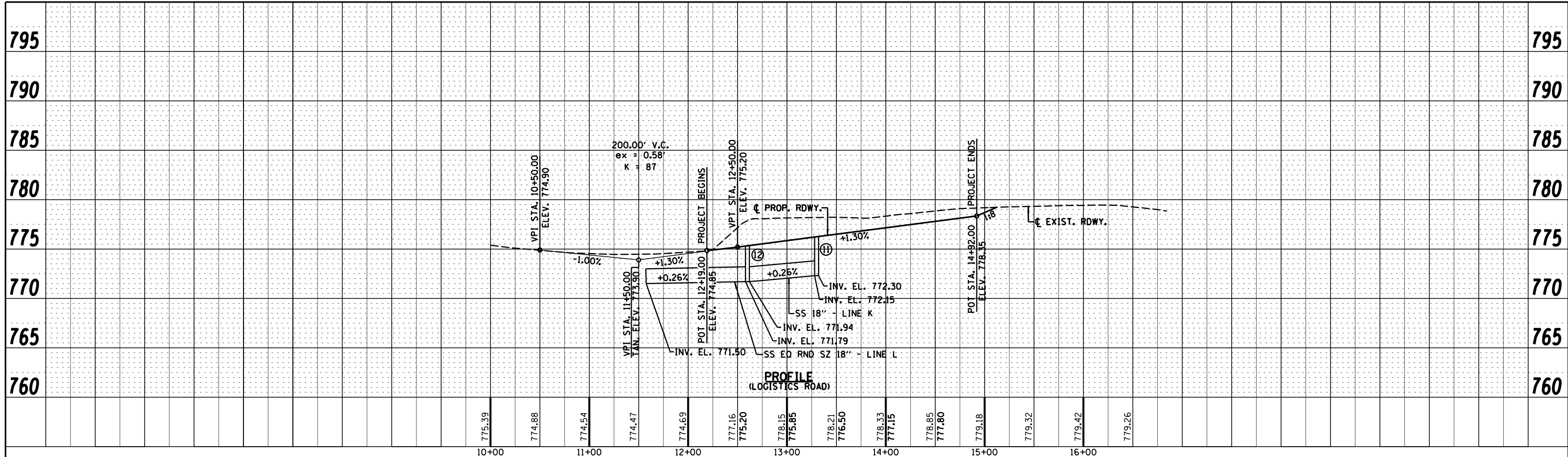
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

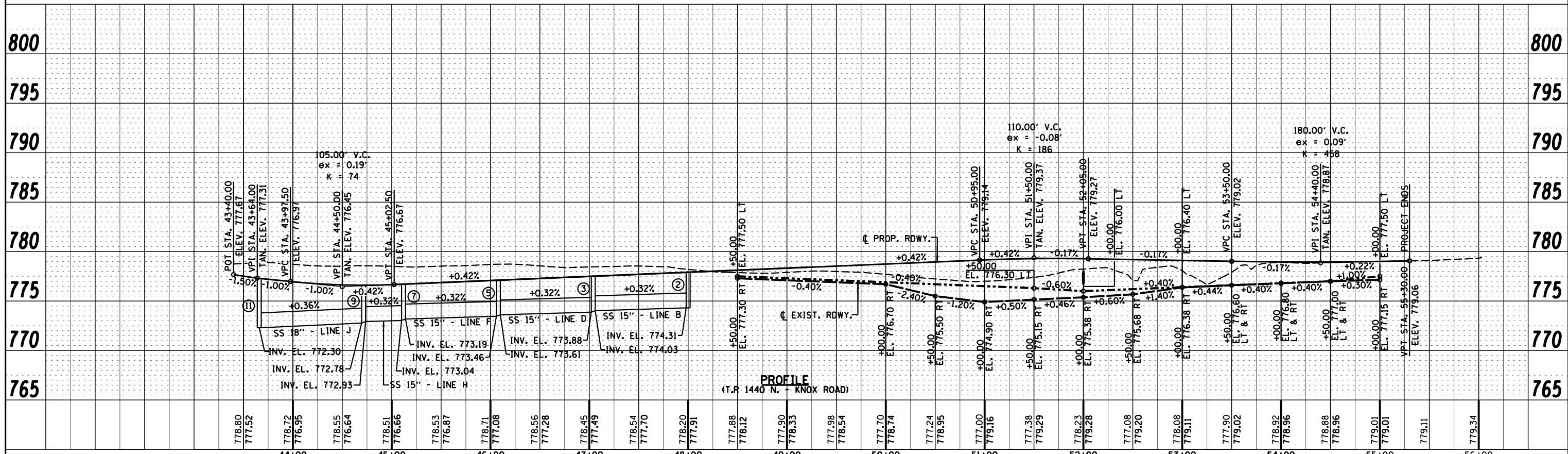
PROFILE - U.S. 150
SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. TO STA.


F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	38
			CONTRACT NO. 68800	
ILLINOIS FED. AID PROJECT				

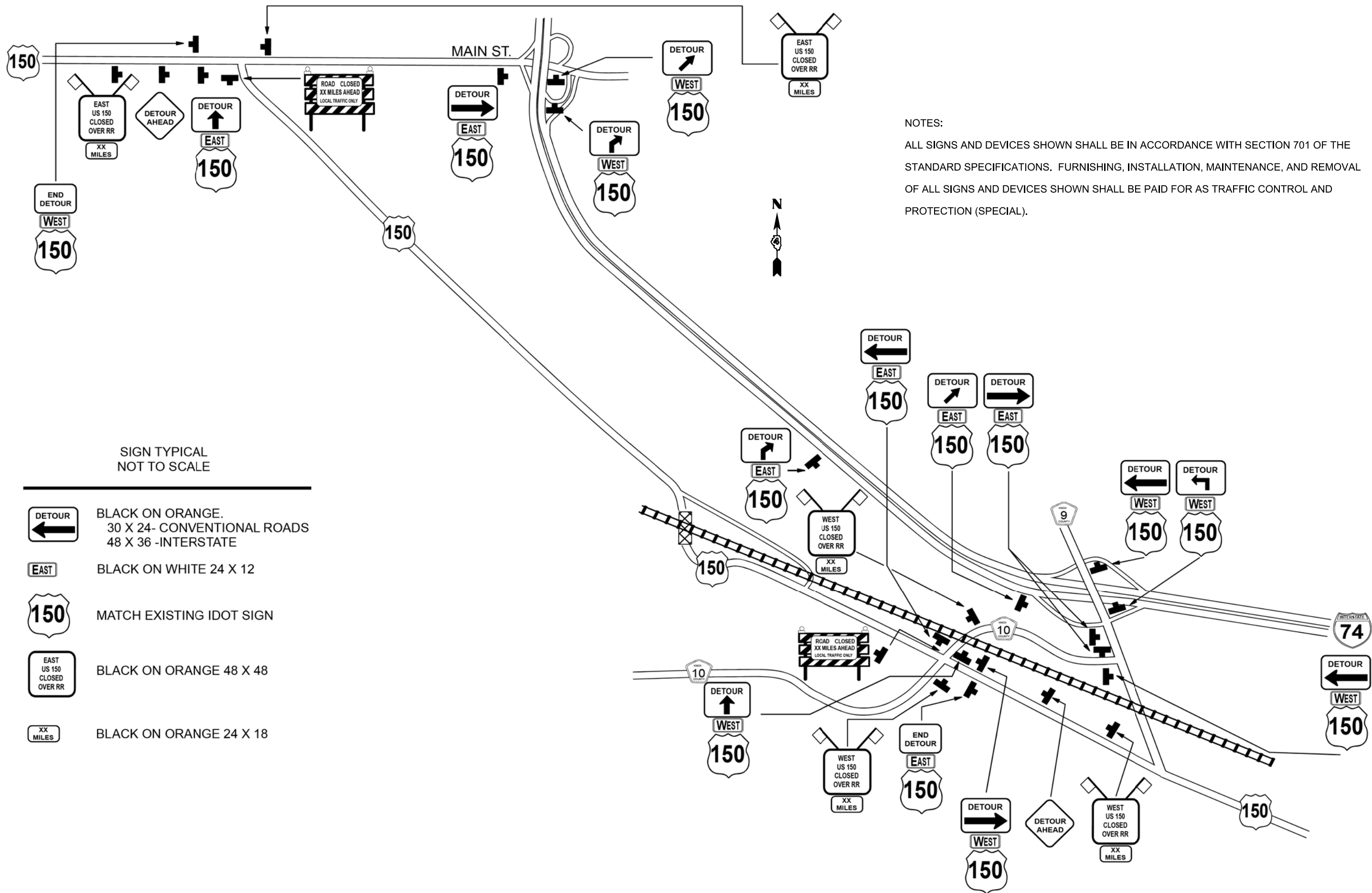
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	PLOTTED	BY
	NOTE BOOK	
	NO.	
	CHECKED	
	CAD FILE NAME	



PROFILE	SURVEYED	DATE
	PLOTTED	BY
	NOTE BOOK	
	NO.	
	CHECKED	
	STRUCTURE	
	NOTATIONS	
	CHFD	








 Veenstra & Kimm, Inc. Springfield, IL. Phone: (217)544-8033	USER NAME =	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROFILE - LOGISTICS PARKWAY & KNOX ROAD		F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
	PLOT SCALE =	CHECKED -	REVISED -		2401	140V-1BR	KNOX	146	39				
	PLOT DATE =	DRAWN -	REVISED -		SCALE: 1" = 50'			SHEET NO. 3 OF 3 SHEETS		STA. TO STA.		CONTRACT NO. 68800	
		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT								



NOTES:
 ALL SIGNS AND DEVICES SHOWN SHALL BE IN ACCORDANCE WITH SECTION 701 OF THE STANDARD SPECIFICATIONS. FURNISHING, INSTALLATION, MAINTENANCE, AND REMOVAL OF ALL SIGNS AND DEVICES SHOWN SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION (SPECIAL).

SIGN TYPICAL
 NOT TO SCALE

-  BLACK ON ORANGE.
 30 X 24- CONVENTIONAL ROADS
 48 X 36 -INTERSTATE
-  BLACK ON WHITE 24 X 12
-  MATCH EXISTING IDOT SIGN
-  BLACK ON ORANGE 48 X 48
-  BLACK ON ORANGE 24 X 18



USER NAME =	DESIGNED -	REVISED -
	CHECKED -	REVISED -
PLOT SCALE =	DRAWN -	REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

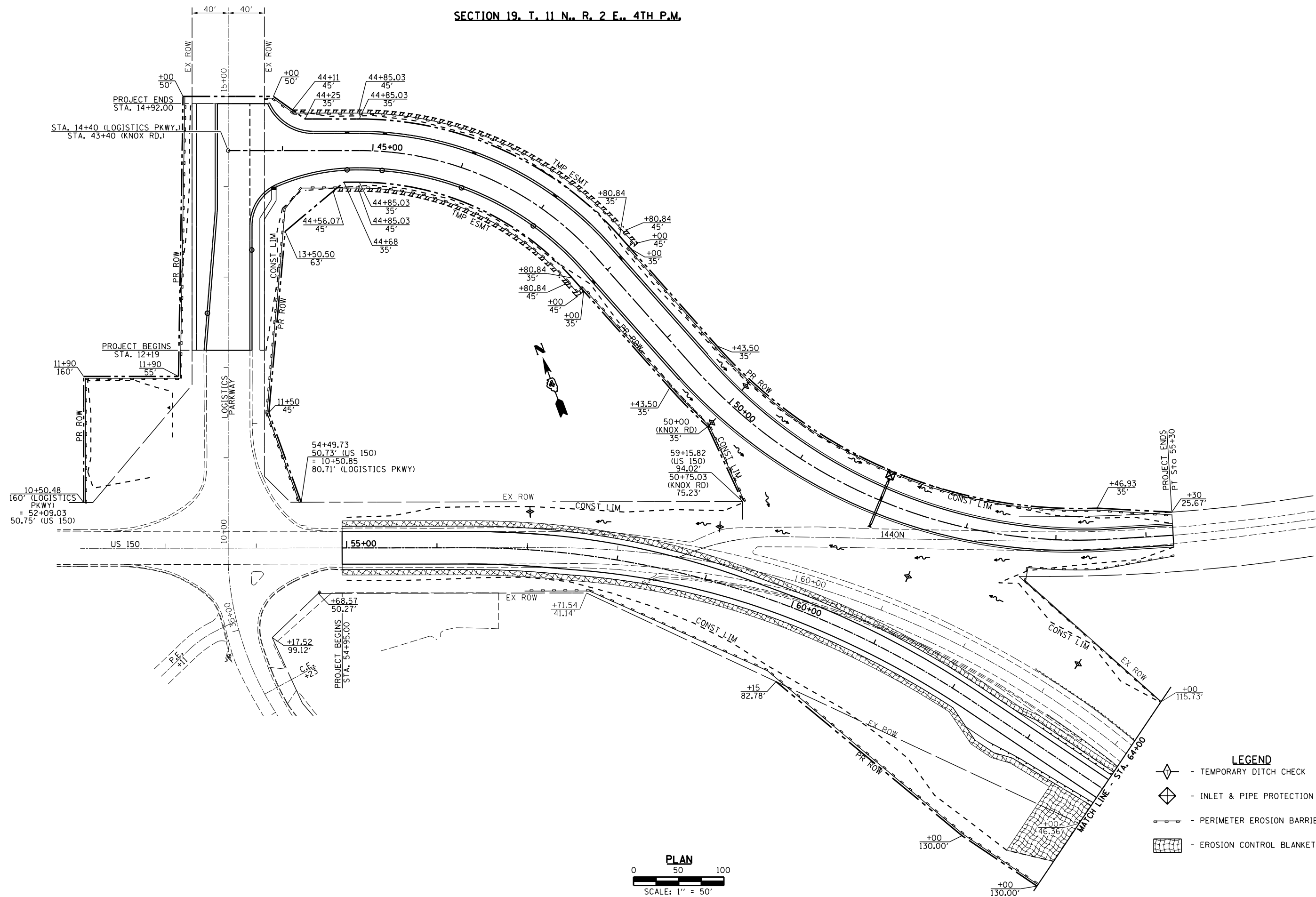
DETOUR DETAILS

SCALE: 1" = 50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

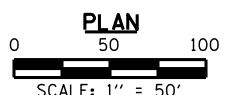
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	40
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

STAGE	PROPOSED CONSTRUCTION	PROPOSED TRAFFIC CONTROL
1	<p>COMPLETE ALL TREE REMOVAL. INSTALL NECESSARY SETTLEMENT PLATFORMS, CONSTRUCT PROPOSED WICK DRAINS, AND CONSTRUCT STAGE 1 EMBANKMENT FROM U.S. ROUTE 150 STA. 60+00 TO STA. 70+50 AS SHOWN ON THE CROSS SECTIONS.</p> <p>COMPLETE PROPOSED EARTHWORK AND INSTALL PROPOSED STORM SEWER ON LOGISTICS PARKWAY AND ON KNOX RD. 1440N FROM LOGISTICS PARKWAY TO KNOX RD. 1440N STA. 51+50. CONSTRUCT THE PROPOSED DETENTION BASIN ON LOGISTICS PARKWAY.</p>	<p>USE TC&P STANDARDS 701006, 701201, AND 701301 AS NEEDED.</p> <p>CLOSE LOGISTICS PARKWAY USING TC&P STANDARD BLR 21.</p>
2	<p>REMOVE EXISTING PAVEMENT AND STRUCTURE ON US ROUTE 150. COMPLETE THE PROPOSED EMBANKMENT, STRUCTURE, PAVEMENT, SHOULDERS, GUARDRAIL, AND ALL OTHER PROPOSED WORK ON THE U.S. 150 ROADWAY.</p> <p>COMPLETE ALL PROPOSED WORK ON LOGISTICS PARKWAY AND KNOX RD. 1440N.</p>	<p>CLOSE U.S. ROUTE 150 USING TC&P STANDARD BLR 21. USE MESSAGE BOARDS ONE WEEK IN ADVANCE OF CLOSURE TO NOTIFY MOTORISTS OF THE CLOSURE. SIGN THE DETOUR AS SHOWN IN THE DETOUR DETAILS.</p> <p>CLOSE LOGISTICS PARKWAY AND KNOX RD. 1440N USING TC&P STANDARD BLR 21. THESE ROADS SHALL REMAIN CLOSED UNTIL U.S. ROUTE 150 IS RE-OPENED TO TRAFFIC.</p>
3	<p>COMPLETE LANDSCAPING WORK.</p>	<p>USE TC&P STANDARD 701006, 701201, 701301, AND 701311 AS NEEDED.</p>

SECTION 19, T. 11 N., R. 2 E., 4TH P.M.



- LEGEND**
- TEMPORARY DITCH CHECK
 - INLET & PIPE PROTECTION
 - PERIMETER EROSION BARRIER
 - EROSION CONTROL BLANKET

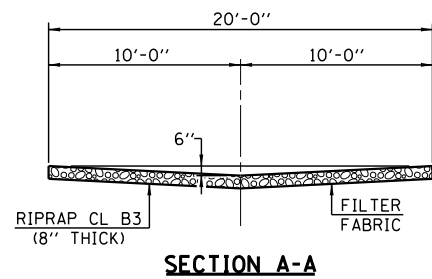


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PLOT SCALE =	CHECKED -	REVISED -
PLOT DATE =	DRAWN -	REVISED -
	CHECKED -	REVISED -

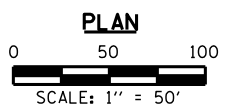
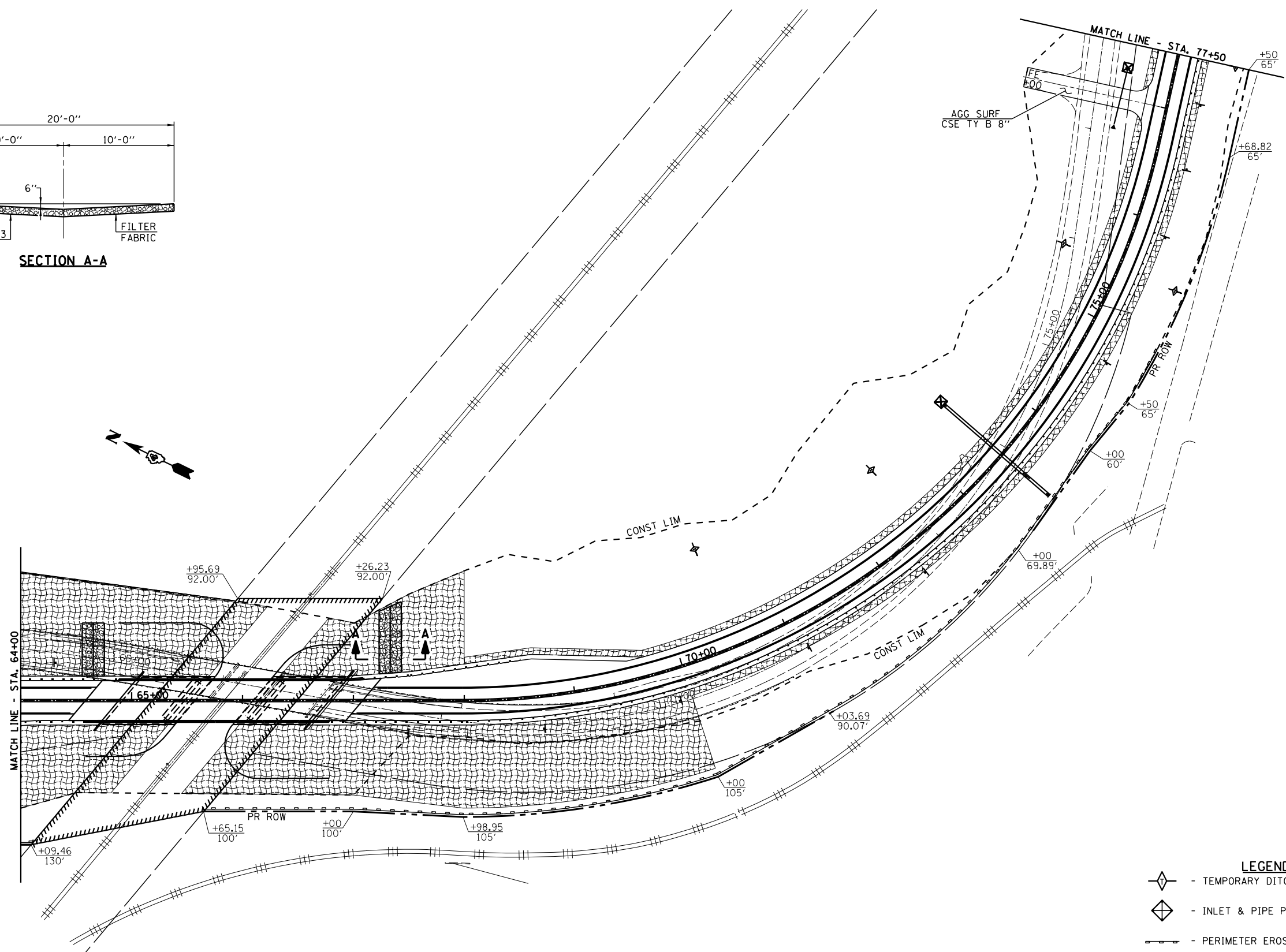
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL - US 150 / KNOX RD.
SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	42
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



SECTION A-A



- LEGEND**
- TEMPORARY DITCH CHECK
 - INLET & PIPE PROTECTION
 - PERIMETER EROSION BARRIER
 - EROSION CONTROL BLANKET

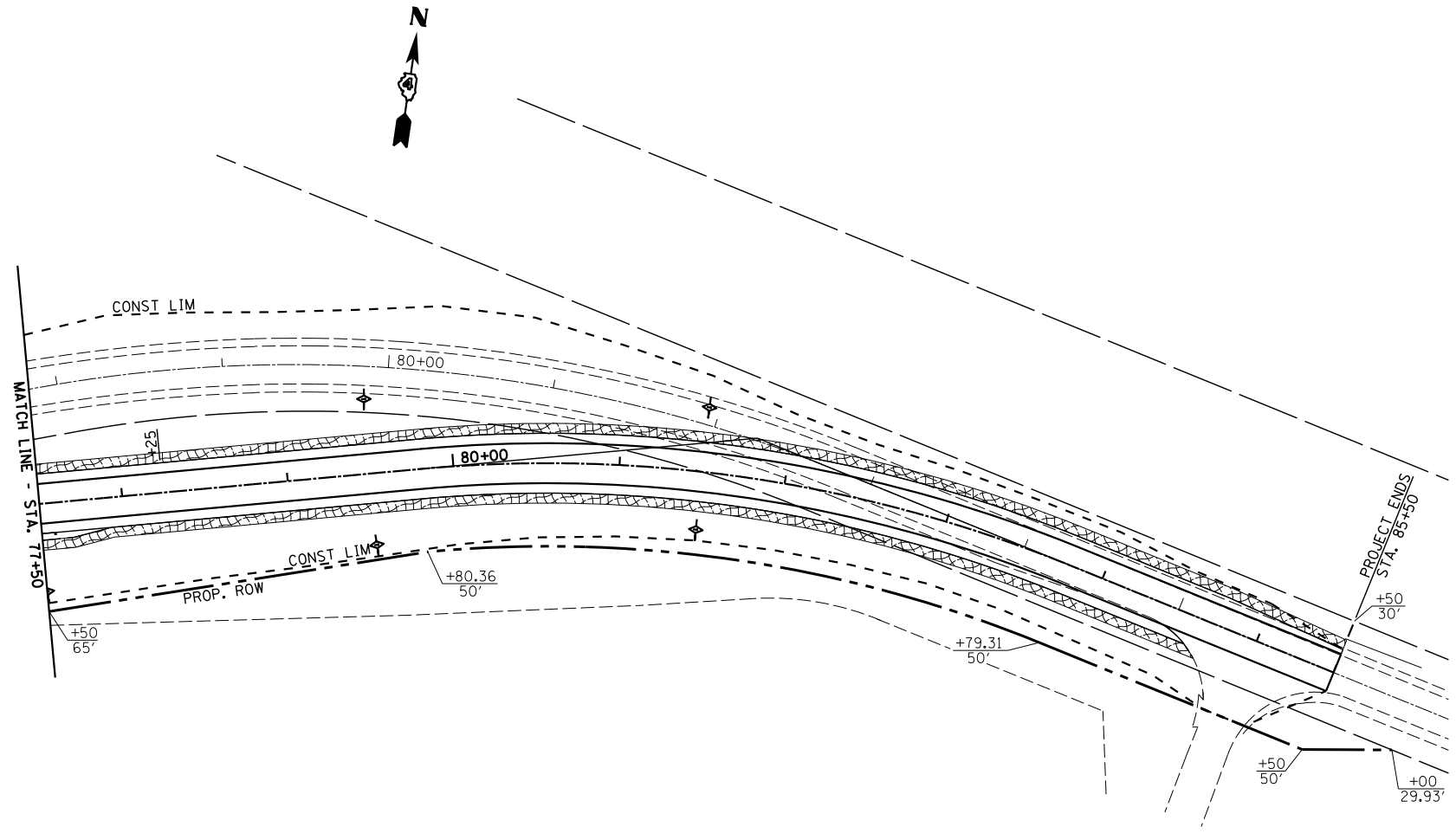






USER NAME =	DESIGNED -	REVISED -
	CHECKED -	REVISED -
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PLOT DATE =	CHECKED -	REVISED -

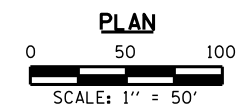
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EROSION CONTROL - US 150 /KNOX RD.
SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	43
CONTRACT NO. 68800				
<small>ILLINOIS FED. AID PROJECT</small>				

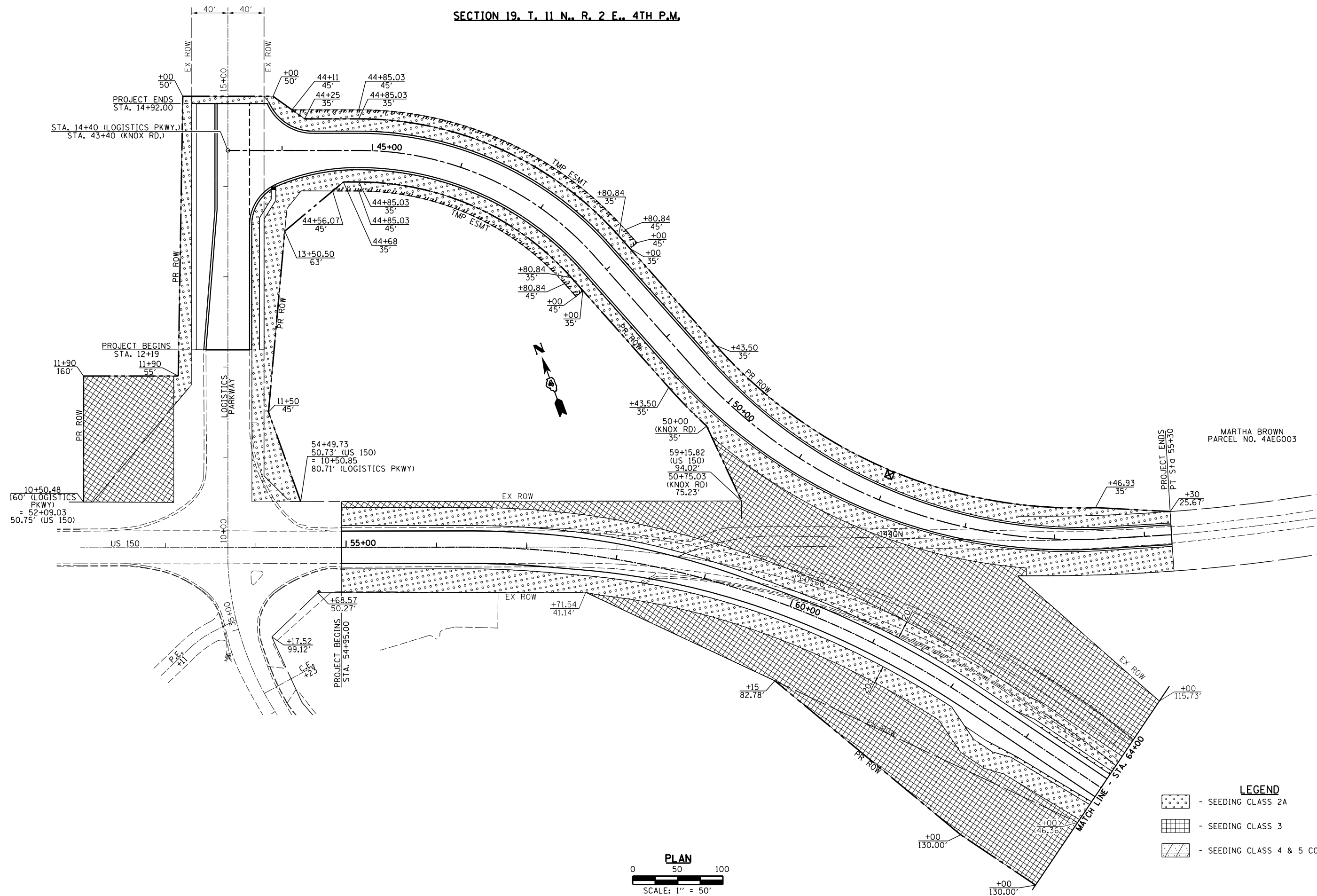


- LEGEND**
-  - TEMPORARY DITCH CHECK
 -  - INLET & PIPE PROTECTION
 -  - PERIMETER EROSION BARRIER
 -  - EROSION CONTROL BLANKET






FILE NAME = *FILEL*	USER NAME = *USER*	DESIGNED -	REVISED -		Allen Henderson & Associates A Division of Veenstra & Kimm, Inc. Springfield, IL. Phone: (217)544-8033 IL. Design Firm No. 184-001939	EROSION CONTROL - US 150 /KNOX RD.			F.A.S. RTE. 2401	SECTION (40V-1)BR	COUNTY KNOX	TOTAL SHEETS 146	SHEET NO. 44	
		DRAWN -	REVISED -			SCALE: 1" = 50'	SHEET NO. 3 OF 3 SHEETS	STA. _____ TO STA. _____	CONTRACT NO. 68800					
		CHECKED -	REVISED -						FED. ROAD DIST. NO. - [ILLINOIS] FED. AID PROJECT					
		DATE -	REVISED -											

SECTION 19, T. 11 N., R. 2 E., 4TH P.M.



LEGEND

-  - SEEDING CLASS 2A
-  - SEEDING CLASS 3
-  - SEEDING CLASS 4 & 5 COMBINED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

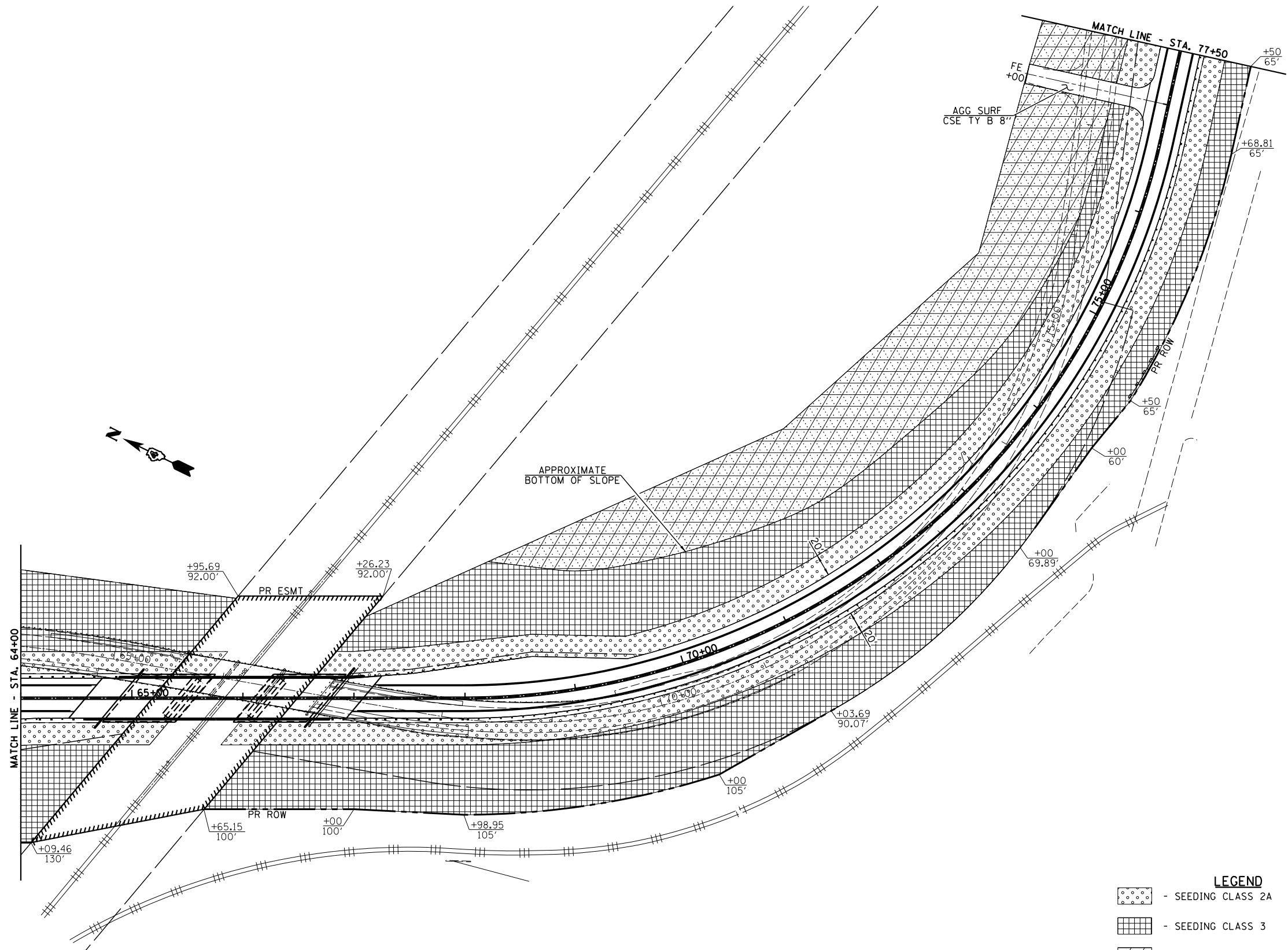
SEEDING PLAN - US 150 / KNOX RD.

SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. TO STA.




F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	45
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

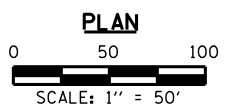


USER NAME =	DESIGNED -	REVISED -
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PLOT DATE =	DRAWN -	REVISED -
	CHECKED -	REVISED -



APPROXIMATE
BOTTOM OF SLOPE

- LEGEND**
-  - SEEDING CLASS 2A
 -  - SEEDING CLASS 3
 -  - SEEDING CLASS 4 & 5 COMBINED



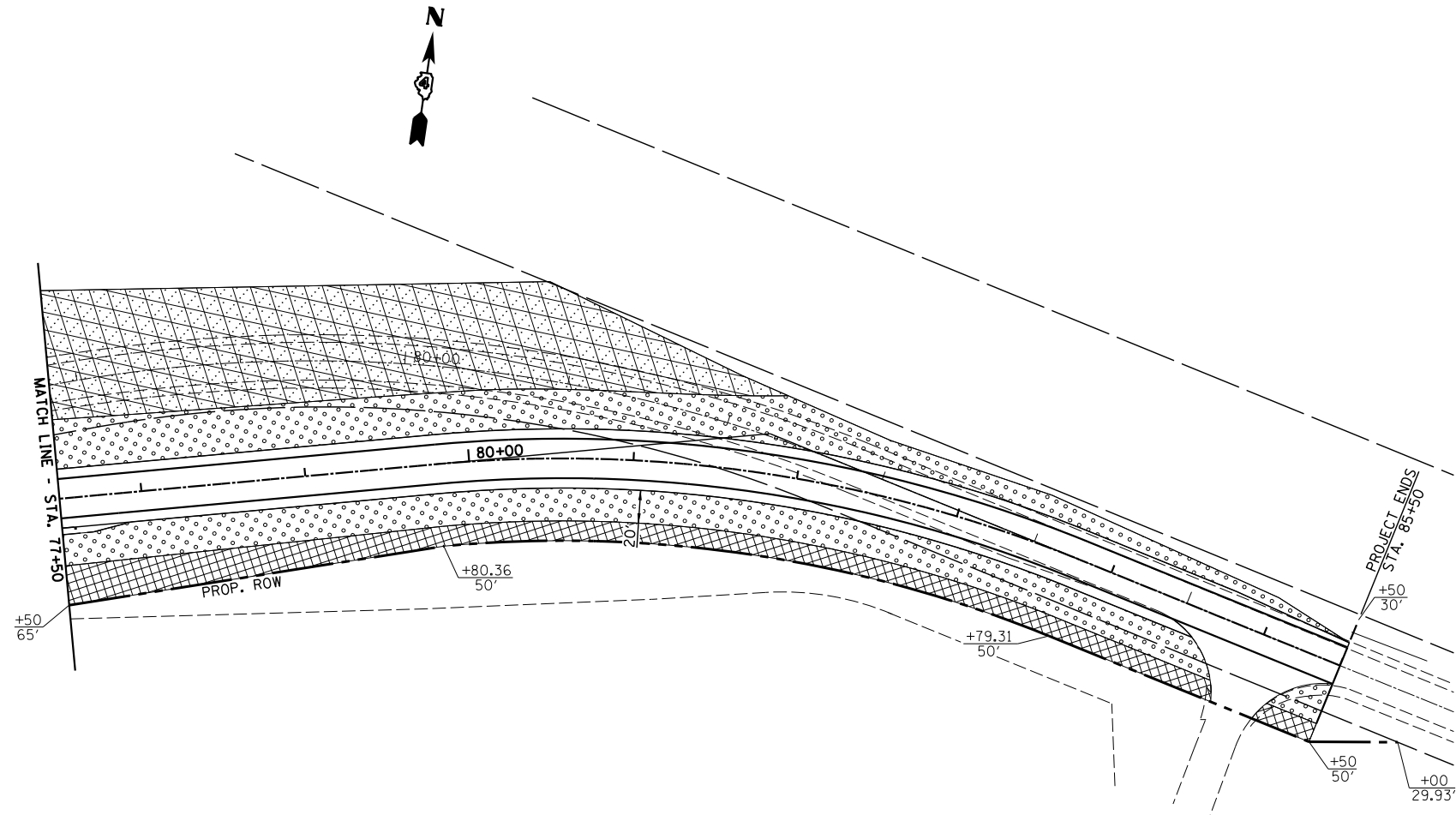
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CHECKED -	REVISIED -	
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PLOT DATE =	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**




SEEDING PLAN - US 150 /KNOX RD.

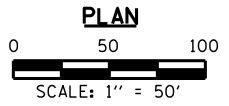
SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	46
CONTRACT NO. 68800				
<small>ILLINOIS FED. AID PROJECT</small>				



LEGEND

	- SEEDING CLASS 2A
	- SEEDING CLASS 3
	- SEEDING CLASS 4 & 5 COMBINED



USER NAME =	DESIGNED -	REVISED -
CHECKED -	REVISED -	
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SEEDING PLAN - US 150 /KNOX RD.

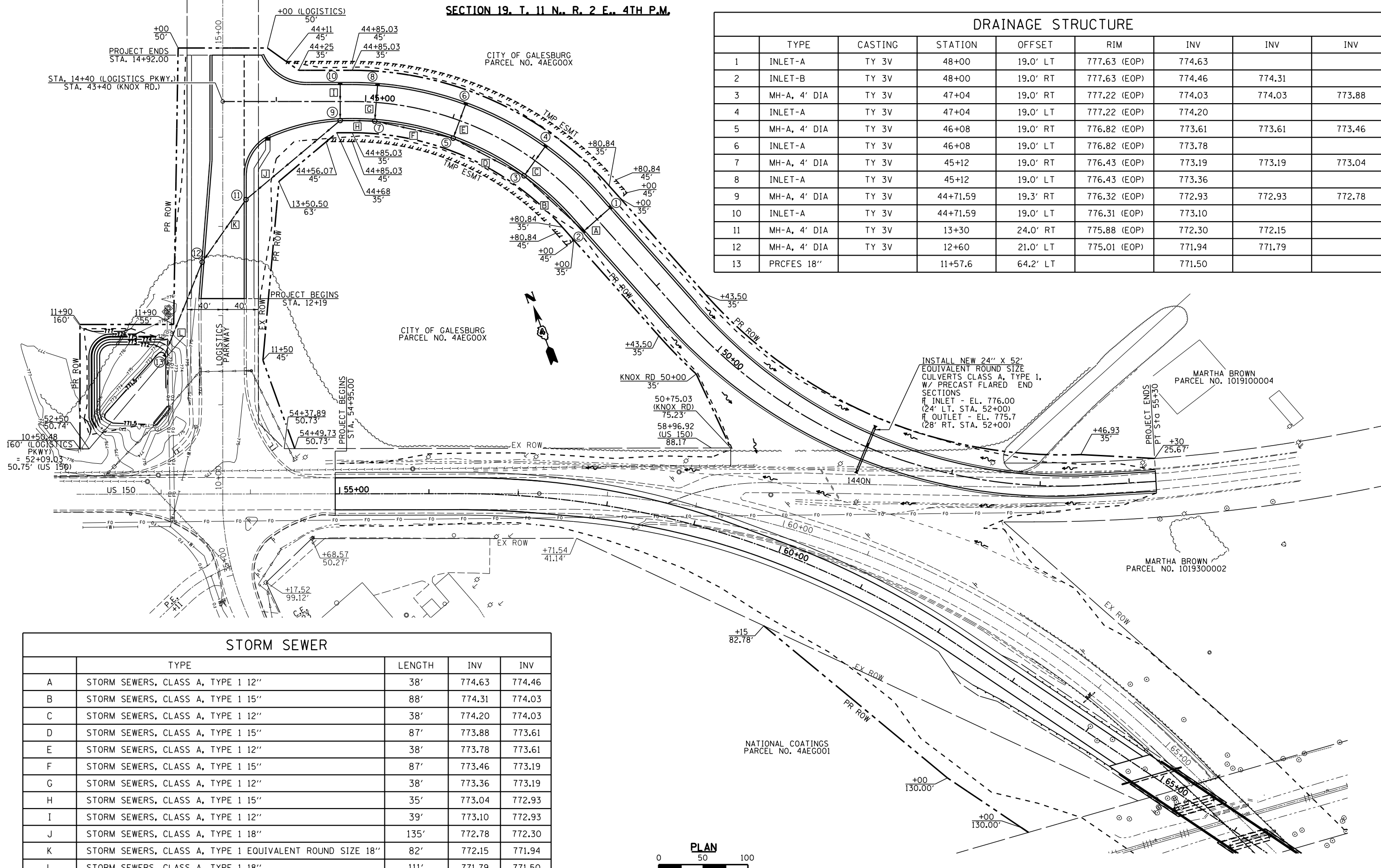
SCALE: 1" = 50' SHEET NO. 3 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	47
CONTRACT NO. 68800				
<small>ILLINOIS FED. AID PROJECT</small>				

SECTION 19. T. 11 N., R. 2 E., 4TH P.M.

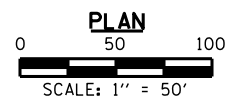
DRAINAGE STRUCTURE

	TYPE	CASTING	STATION	OFFSET	RIM	INV	INV	INV
1	INLET-A	TY 3V	48+00	19.0' LT	777.63 (EOP)	774.63		
2	INLET-B	TY 3V	48+00	19.0' RT	777.63 (EOP)	774.46	774.31	
3	MH-A, 4' DIA	TY 3V	47+04	19.0' RT	777.22 (EOP)	774.03	774.03	773.88
4	INLET-A	TY 3V	47+04	19.0' LT	777.22 (EOP)	774.20		
5	MH-A, 4' DIA	TY 3V	46+08	19.0' RT	776.82 (EOP)	773.61	773.61	773.46
6	INLET-A	TY 3V	46+08	19.0' LT	776.82 (EOP)	773.78		
7	MH-A, 4' DIA	TY 3V	45+12	19.0' RT	776.43 (EOP)	773.19	773.19	773.04
8	INLET-A	TY 3V	45+12	19.0' LT	776.43 (EOP)	773.36		
9	MH-A, 4' DIA	TY 3V	44+71.59	19.3' RT	776.32 (EOP)	772.93	772.93	772.78
10	INLET-A	TY 3V	44+71.59	19.0' LT	776.31 (EOP)	773.10		
11	MH-A, 4' DIA	TY 3V	13+30	24.0' RT	775.88 (EOP)	772.30	772.15	
12	MH-A, 4' DIA	TY 3V	12+60	21.0' LT	775.01 (EOP)	771.94	771.79	
13	PRCFES 18"		11+57.6	64.2' LT		771.50		



STORM SEWER

	TYPE	LENGTH	INV	INV
A	STORM SEWERS, CLASS A, TYPE 1 12"	38'	774.63	774.46
B	STORM SEWERS, CLASS A, TYPE 1 15"	88'	774.31	774.03
C	STORM SEWERS, CLASS A, TYPE 1 12"	38'	774.20	774.03
D	STORM SEWERS, CLASS A, TYPE 1 15"	87'	773.88	773.61
E	STORM SEWERS, CLASS A, TYPE 1 12"	38'	773.78	773.61
F	STORM SEWERS, CLASS A, TYPE 1 15"	87'	773.46	773.19
G	STORM SEWERS, CLASS A, TYPE 1 12"	38'	773.36	773.19
H	STORM SEWERS, CLASS A, TYPE 1 15"	35'	773.04	772.93
I	STORM SEWERS, CLASS A, TYPE 1 12"	39'	773.10	772.93
J	STORM SEWERS, CLASS A, TYPE 1 18"	135'	772.78	772.30
K	STORM SEWERS, CLASS A, TYPE 1 EQUIVALENT ROUND SIZE 18"	82'	772.15	771.94
L	STORM SEWERS, CLASS A, TYPE 1 18"	111'	771.79	771.50



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE PLAN - US 150 / KNOX RD.

SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	48
CONTRACT NO. 68800				

V&K
Veenstra & Kimm, Inc.
Springfield, IL. Phone: (217)544-8033

USER NAME =	DESIGNED -	REVISOR -
PLOT SCALE =	CHECKED -	REVISIONS -
PLOT DATE =	DRAWN -	REVISIONS -
	CHECKED -	REVISIONS -

ILLINOIS FED. AID PROJECT

CITY OF GALESBURG
PARCEL NO. 9019300003

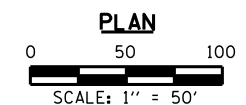
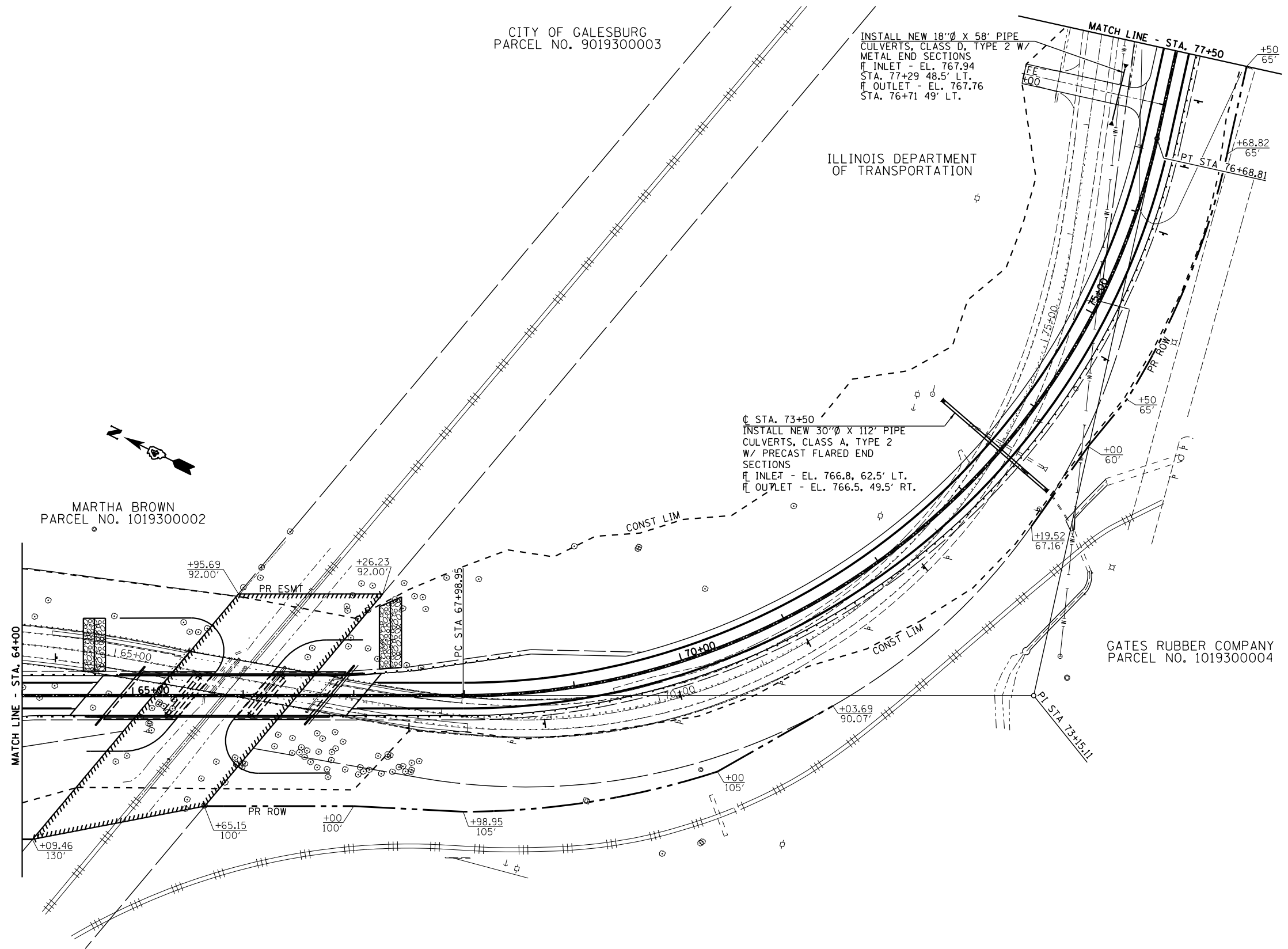
ILLINOIS DEPARTMENT
OF TRANSPORTATION

MARTHA BROWN
PARCEL NO. 1019300002

GATES RUBBER COMPANY
PARCEL NO. 1019300004

INSTALL NEW 18"Ø X 58' PIPE
CULVERTS, CLASS D, TYPE 2 W/
METAL END SECTIONS
H INLET - EL. 767.94
STA. 77+29 48.5' LT.
H OUTLET - EL. 767.76
STA. 76+71 49' LT.

CL STA. 73+50
INSTALL NEW 30"Ø X 112' PIPE
CULVERTS, CLASS A, TYPE 2
W/ PRECAST FLARED END
SECTIONS
H INLET - EL. 766.8, 62.5' LT.
H OUTLET - EL. 766.5, 49.5' RT.



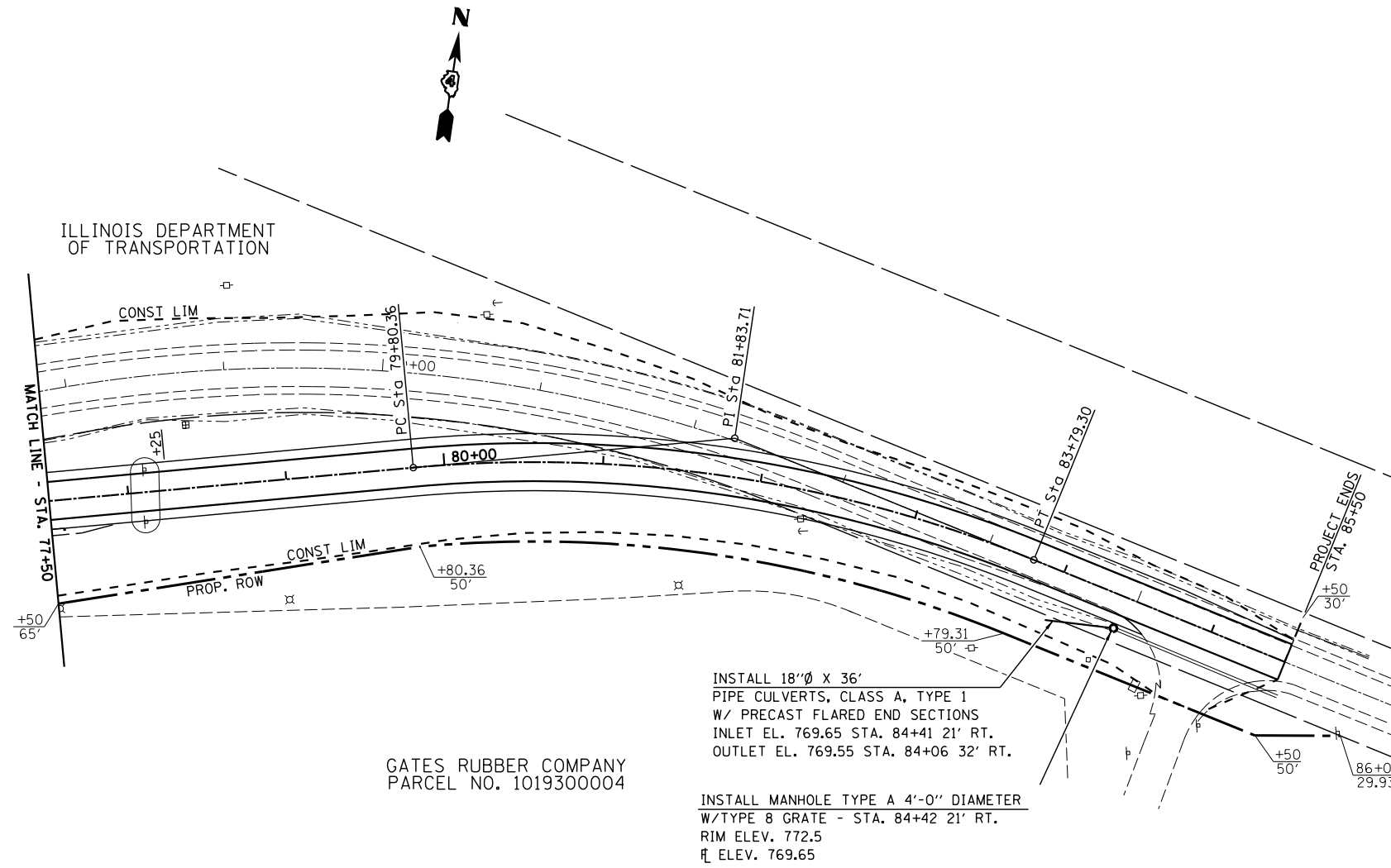
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	CHECKED -	REVISED -
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRAINAGE PLAN - US 150 /KNOX RD.

SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

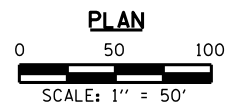
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	49
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



GATES RUBBER COMPANY
PARCEL NO. 1019300004

INSTALL 18"Ø X 36'
PIPE CULVERTS, CLASS A, TYPE 1
W/ PRECAST FLARED END SECTIONS
INLET EL. 769.65 STA. 84+41 21' RT.
OUTLET EL. 769.55 STA. 84+06 32' RT.

INSTALL MANHOLE TYPE A 4'-0" DIAMETER
W/TYPE 8 GRATE - STA. 84+42 21' RT.
RIM ELEV. 772.5
FL ELEV. 769.65



USER NAME =	DESIGNED -	REVISED -
	CHECKED -	REVISED -
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRAINAGE PLAN - US 150 /KNOX RD.	
SCALE: 1" = 50'	SHEET NO. 3 OF 3 SHEETS
STA.	TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	50
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

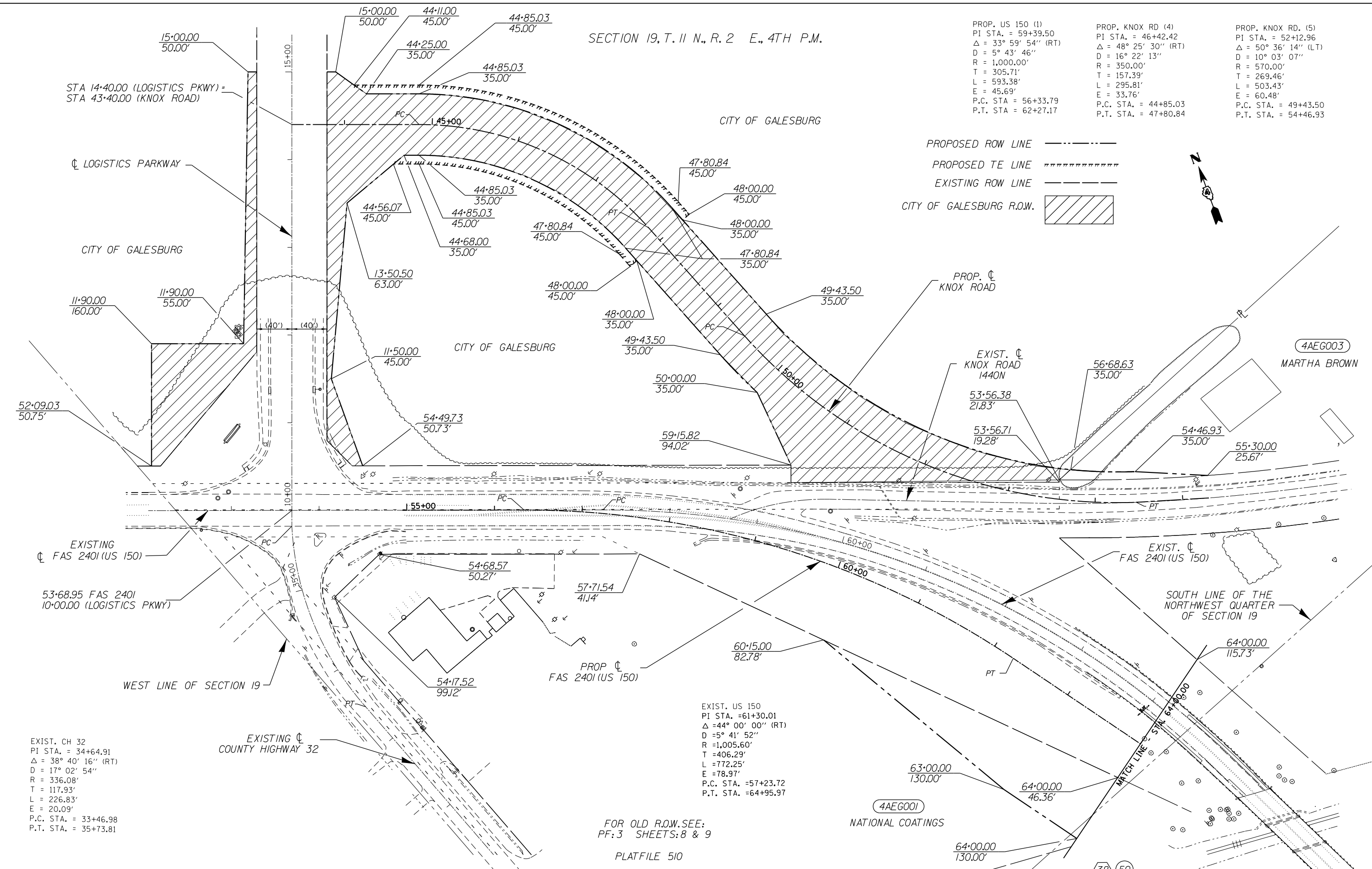
SECTION 19, T. 11 N., R. 2 E., 4TH P.M.

PROP. US 150 (1)
 PI STA. = 59+39.50
 $\Delta = 33^\circ 59' 54''$ (RT)
 $D = 5^\circ 43' 46''$
 $R = 1,000.00'$
 $T = 305.71'$
 $L = 593.38'$
 $E = 45.69'$
 P.C. STA. = 56+33.79
 P.T. STA. = 62+27.17

PROP. KNOX RD (4)
 PI STA. = 46+42.42
 $\Delta = 48^\circ 25' 30''$ (RT)
 $D = 16^\circ 22' 13''$
 $R = 350.00'$
 $T = 157.39'$
 $L = 295.81'$
 $E = 33.76'$
 P.C. STA. = 44+85.03
 P.T. STA. = 47+80.84

PROP. KNOX RD. (5)
 PI STA. = 52+12.96
 $\Delta = 50^\circ 36' 14''$ (LT)
 $D = 10^\circ 03' 07''$
 $R = 570.00'$
 $T = 269.46'$
 $L = 503.43'$
 $E = 60.48'$
 P.C. STA. = 49+43.50
 P.T. STA. = 54+46.93

PROPOSED ROW LINE - - - - -
 PROPOSED TE LINE - - - - -
 EXISTING ROW LINE - - - - -
 CITY OF GALESBURG R.O.W. [Hatched Box]



EXIST. CH 32
 PI STA. = 34+64.91
 $\Delta = 38^\circ 40' 16''$ (RT)
 $D = 17^\circ 02' 54''$
 $R = 336.08'$
 $T = 117.93'$
 $L = 226.83'$
 $E = 20.09'$
 P.C. STA. = 33+46.98
 P.T. STA. = 35+73.81

EXIST. US 150
 PI STA. = 61+30.01
 $\Delta = 44^\circ 00' 00''$ (RT)
 $D = 5^\circ 41' 52''$
 $R = 1,005.60'$
 $T = 406.29'$
 $L = 772.25'$
 $E = 78.97'$
 P.C. STA. = 57+23.72
 P.T. STA. = 64+95.97

FOR OLD R.O.W. SEE:
 PF: 3 SHEETS: 8 & 9
 PLATFILE 510

USER NAME =	DESIGNED -	REVISED -
CHECKED -	REVISOR -	REVISOR -
PLOT SCALE =	DRAWN -	REVISOR -
PLOT DATE =	CHECKED -	REVISOR -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

RIGHT OF WAY PLAN - US 150 / KNOX RD.

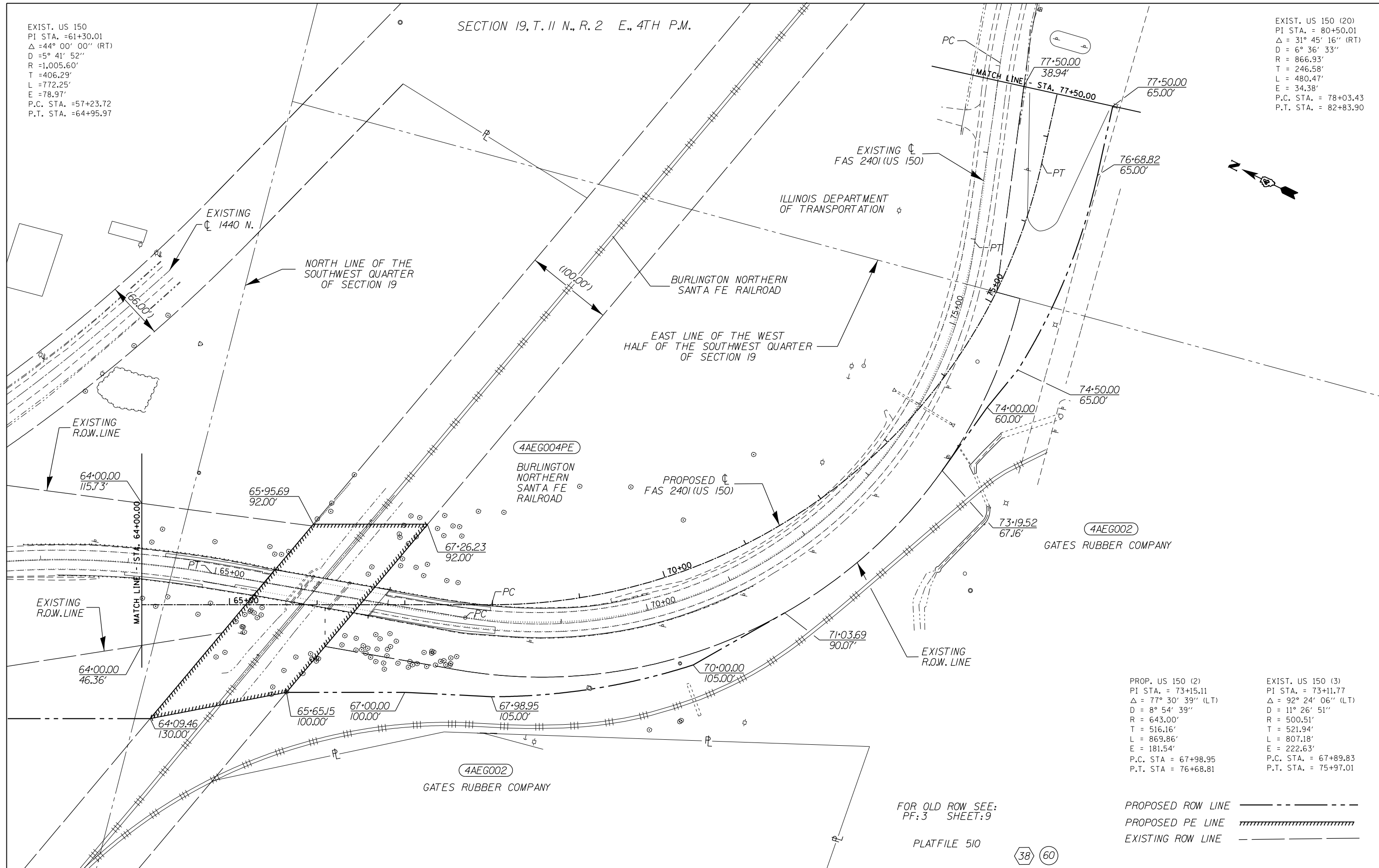
SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. 53+00.00 TO STA. 64+00.00

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	51
R-94-004-15		CONTRACT NO. 68800		
ILLINOIS FED. AID PROJECT				

SECTION 19, T. 11 N., R. 2 E., 4TH P.M.

EXIST. US 150
 PI STA. = 61+30.01
 $\Delta = 44^\circ 00' 00''$ (RT)
 $D = 5^\circ 41' 52''$
 $R = 1,005.60'$
 $T = 406.29'$
 $L = 772.25'$
 $E = 78.97'$
 P.C. STA. = 57+23.72
 P.T. STA. = 64+95.97

EXIST. US 150 (2)
 PI STA. = 80+50.01
 $\Delta = 31^\circ 45' 16''$ (RT)
 $D = 6^\circ 36' 33''$
 $R = 866.93'$
 $T = 246.58'$
 $L = 480.47'$
 $E = 34.38'$
 P.C. STA. = 78+03.43
 P.T. STA. = 82+83.90

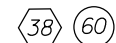


PROP. US 150 (2)
 PI STA. = 73+15.11
 $\Delta = 77^\circ 30' 39''$ (LT)
 $D = 8^\circ 54' 39''$
 $R = 643.00'$
 $T = 516.16'$
 $L = 869.86'$
 $E = 181.54'$
 P.C. STA. = 67+98.95
 P.T. STA. = 76+68.81

EXIST. US 150 (3)
 PI STA. = 73+11.77
 $\Delta = 92^\circ 24' 06''$ (LT)
 $D = 11^\circ 26' 51''$
 $R = 500.51'$
 $T = 521.94'$
 $L = 807.18'$
 $E = 222.63'$
 P.C. STA. = 67+89.83
 P.T. STA. = 75+97.01

FOR OLD ROW SEE:
 PF: 3 SHEET: 9

PLATFILE 510



PROPOSED ROW LINE

PROPOSED PE LINE

EXISTING ROW LINE

USER NAME =	DESIGNED -	REVISED -
CHECKED -	REVISOR -	REVISOR -
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -



STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

RIGHT OF WAY PLAN - US 150 /KNOX RD.

SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. 64+00.00 TO STA. 77+50.00

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	52
R-94-004-15			CONTRACT NO. 68800	
ILLINOIS FED. AID PROJECT				

SECTION 19, T. 11 N., R. 2 E., 4TH P.M.

PROPOSED ROW LINE 
 EXISTING ROW LINE 

EXIST. US 150 (20)	PROP. US 150 (3)
PI STA. = 80+50.01	PI STA. = 81+83.71
$\Delta = 31^\circ 45' 16''$ (RT)	$\Delta = 27^\circ 22' 29''$ (RT)
D = 6° 36' 33"	D = 6° 51' 42"
R = 866.93'	R = 835.00'
T = 246.58'	T = 203.36'
L = 480.47'	L = 398.94'
E = 34.38'	E = 24.41'
P.C. STA. = 78+03.43	P.C. STA. = 79+80.36
P.T. STA. = 82+83.90	P.T. STA. = 83+79.30

EAST LINE OF THE
SOUTHWEST QUARTER
OF SECTION 19



ILLINOIS DEPARTMENT
OF TRANSPORTATION

BURLINGTON NORTHERN
SANTA FE R.R.

EXISTING \square F.A.S. 2401 (US 150)

PT

77+50.00
38.94'

PC

80+00

PC

80+00

PT

79+80.36
50.00'

77+50.00
65.00'

PROPOSED \square
F.A.S. 2401 (US 150)

4AEG002

GATES RUBBER COMPANY

83+79.31
50.00'

86+00.00
29.93'

85+50.00
50.00'

WEST LINE OF THE
EAST HALF OF THE SOUTHWEST
QUARTER OF SECTION 19

EXIST. US 150 (3)
PI STA. = 73+11.77
 $\Delta = 92^\circ 24' 06''$ (LT)
D = 11° 26' 51"
R = 500.51'
T = 521.94'
L = 807.18'
E = 222.63'
P.C. STA. = 67+89.83
P.T. STA. = 75+97.01

FOR OLD R.O.W. SEE:
PF: 3 SHEET: 9

38 61

PLATFILE 510

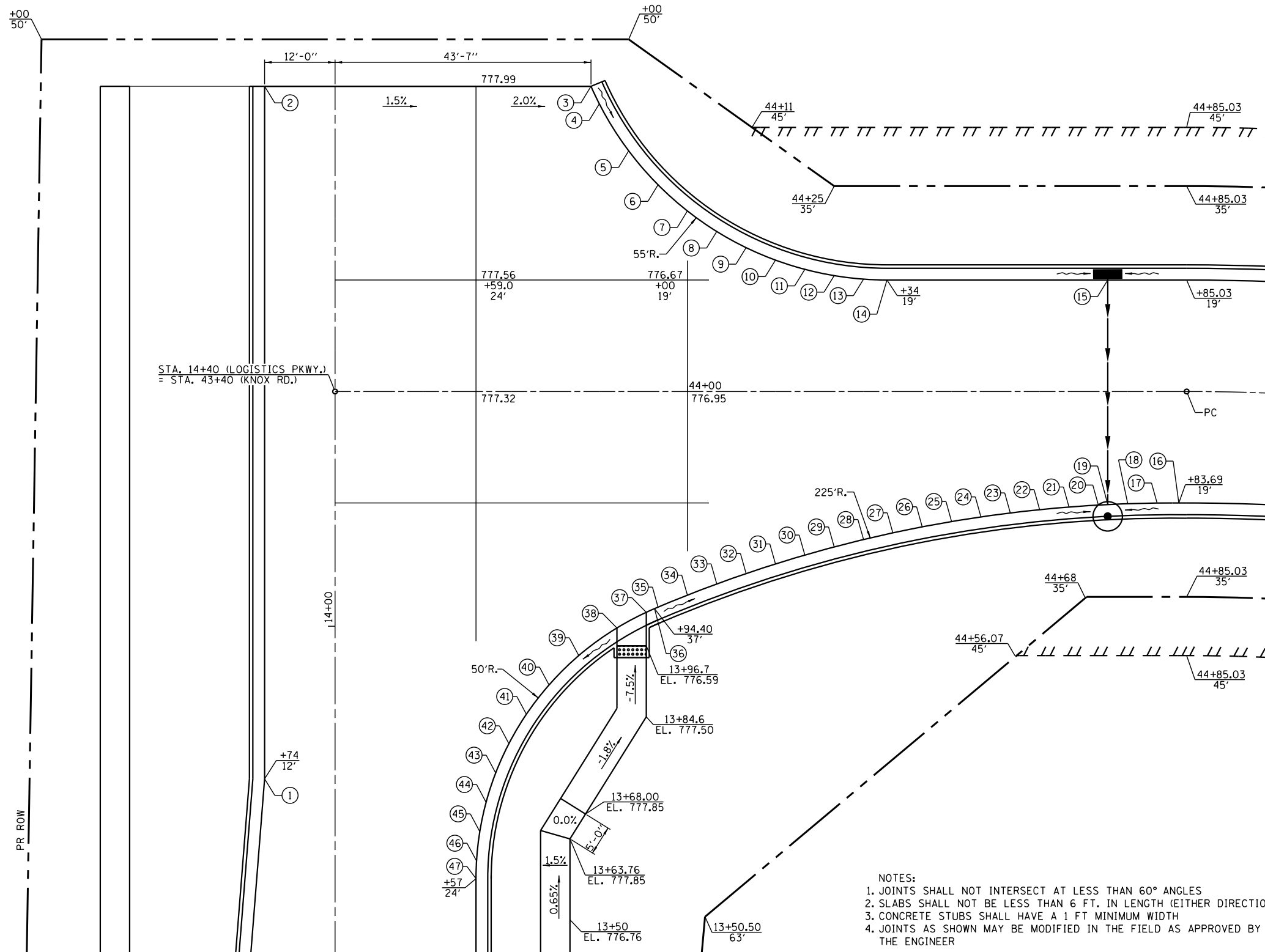
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PLOT DATE =	CHECKED -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RIGHT OF WAY PLAN - US 150 / KNOX RD.

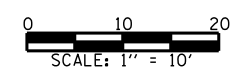
SCALE: 1" = 50' SHEET NO. 3 OF 3 SHEETS STA. 77+50.00 TO STA. 85+98.44

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	53
CONTRACT NO. 68800			ILLINOIS FED. AID PROJECT	



NOTES:
 1. JOINTS SHALL NOT INTERSECT AT LESS THAN 60° ANGLES
 2. SLABS SHALL NOT BE LESS THAN 6 FT. IN LENGTH (EITHER DIRECTION)
 3. CONCRETE STUBS SHALL HAVE A 1 FT MINIMUM WIDTH
 4. JOINTS AS SHOWN MAY BE MODIFIED IN THE FIELD AS APPROVED BY THE ENGINEER

	STA.	OFFSET	ELEVATION
1	13+74	12.0' LT	776.63
2	14+92	12.0' LT	778.17
3	14+92 C	43.6' RT	777.60
4	14+89	45.0' RT	777.55
5	14+81	50.0' RT	777.33
6	43+95	35.2' LT	777.15
7	44+00	30.8' LT	776.90
8	44+05	27.3' LT	776.65
9	44+10	24.5' LT	776.59
10	44+15	22.4' LT	776.53
11	44+20	20.8' LT	776.47
12	44+25	19.7' LT	776.45
13	44+30	19.1' LT	776.43
14	44+34	19.0' LT	776.41
15	44+71.6	19.0' LT	776.32
16	44+83.7	19.0' RT	776.33
17	44+80	19.0' RT	776.33
18	44+75	19.1' RT	776.32
19	44+71.6	19.3' RT	776.30
20	44+70	19.4' RT	776.31
21	44+65	19.7' RT	776.32
22	44+60	20.1' RT	776.33
23	44+55	20.7' RT	776.34
24	44+50	21.4' RT	776.35
25	44+45	22.2' RT	776.36
26	44+40	23.1' RT	776.37
27	44+35	24.1' RT	776.38
28	44+30	25.2' RT	776.39
29	44+25	26.5' RT	776.40
30	44+20	27.9' RT	776.41
31	44+15	29.4' RT	776.42
32	44+10	31.0' RT	776.43
33	44+05	32.8' RT	776.44
34	44+00	34.7' RT	776.46
35	43+95	36.7' RT	776.48
36	43+94.4	37.0' RT	776.49
37	43+93	37.6' RT	776.50
38	13+99.7	48.0' RT	776.50
39	13+95	41.5' RT	776.48
40	13+90	36.4' RT	776.46
41	13+85	32.6' RT	776.44
42	13+80	29.6' RT	776.42
43	13+75	27.4' RT	776.40
44	13+70	25.7' RT	776.38
45	13+65	24.6' RT	776.33
46	13+60	24.1' RT	776.27
47	13+57	24.0' RT	776.23



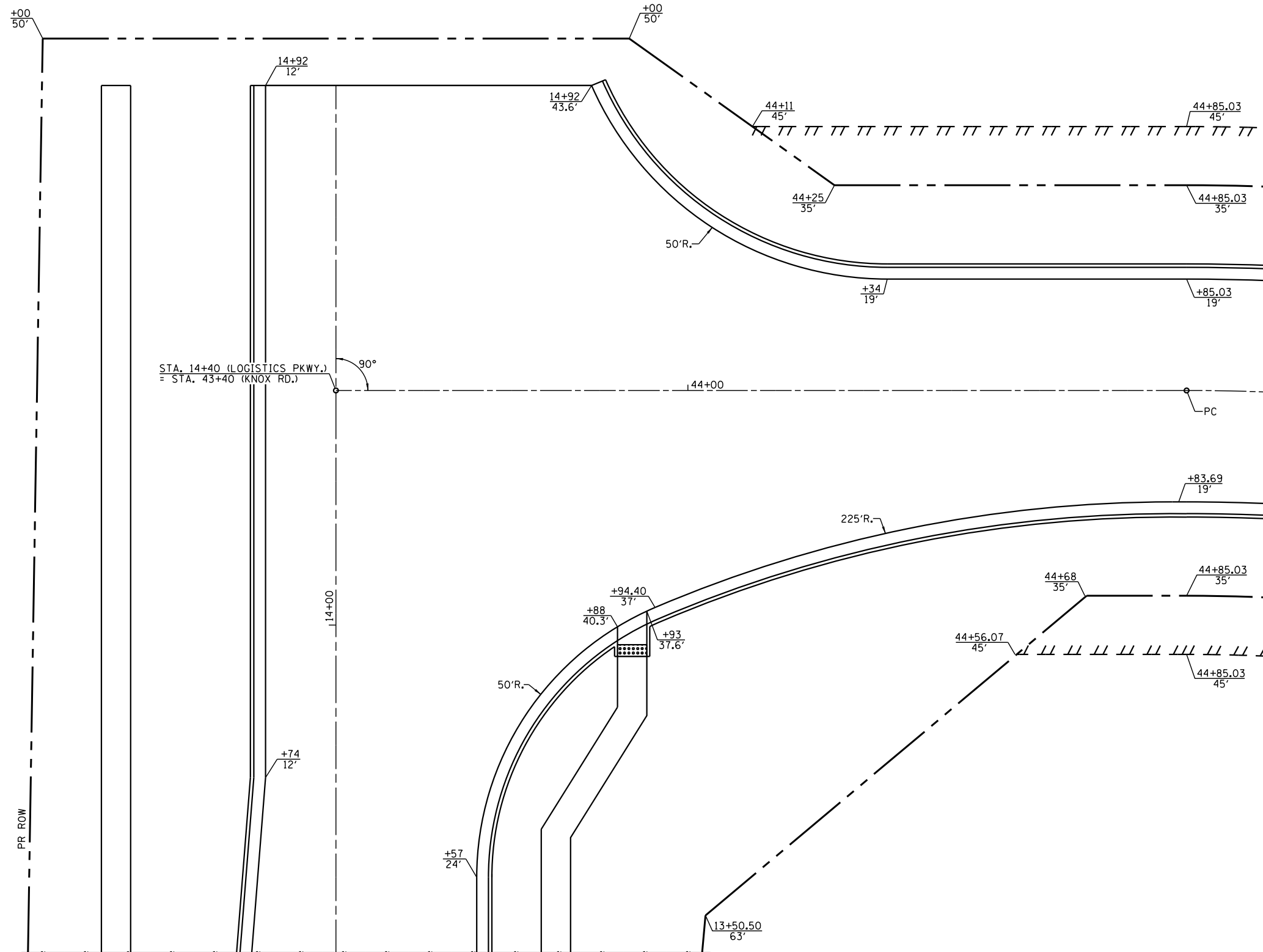
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CHECKED -	REVISOR -	REVISED -
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

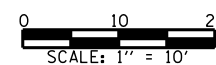
INTERSECTION VERTICAL CONTROL

SCALE: 1" = 10' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	54
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



PLAN
(LOGISTICS PARKWAY & KNOX ROAD INTERSECTION)



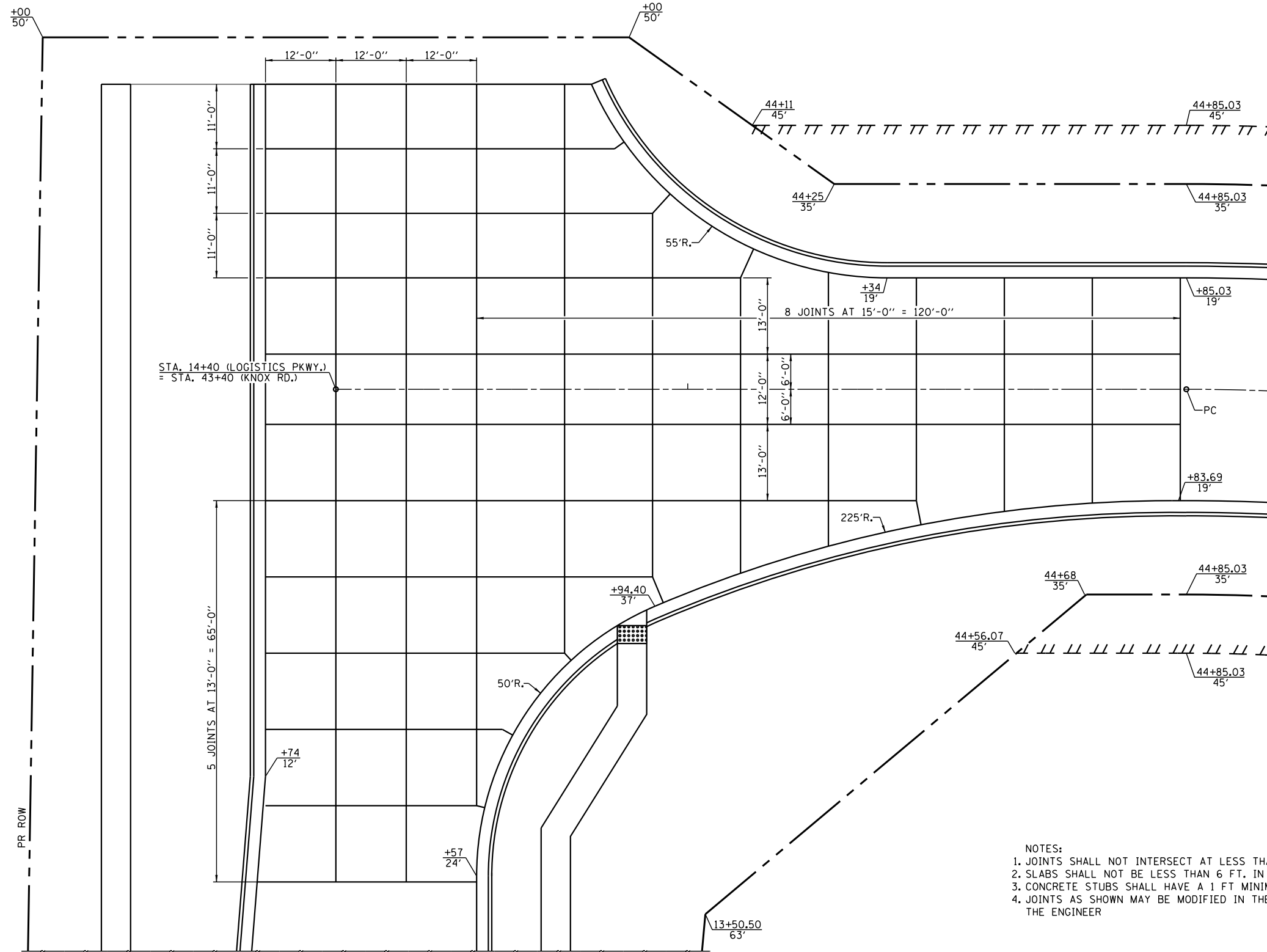
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	CHECKED -	REVISED -
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

INTERSECTION HORIZONTAL CONTROL

SCALE: 1" = 10' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

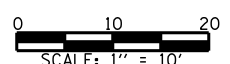
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	55
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



STA. 14+40 (LOGISTICS PKWY.)
= STA. 43+40 (KNOX RD.)

- NOTES:
1. JOINTS SHALL NOT INTERSECT AT LESS THAN 60° ANGLES
 2. SLABS SHALL NOT BE LESS THAN 6 FT. IN LENGTH (EITHER DIRECTION)
 3. CONCRETE STUBS SHALL HAVE A 1 FT MINIMUM WIDTH
 4. JOINTS AS SHOWN MAY BE MODIFIED IN THE FIELD AS APPROVED BY THE ENGINEER

PLAN
(LOGISTICS PARKWAY & KNOX ROAD INTERSECTION)



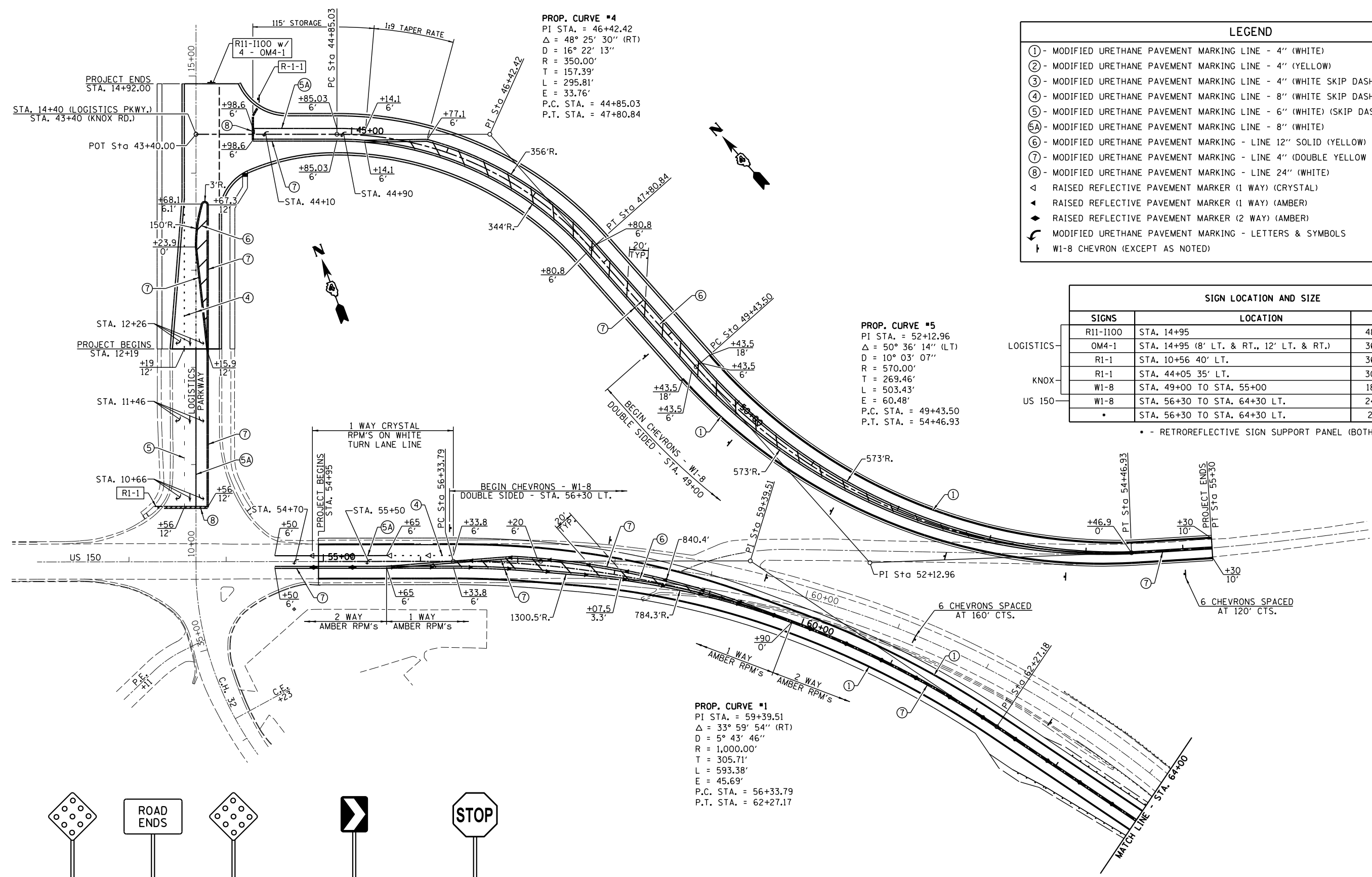
USER NAME =	DESIGNED -	REVISED -
	CHECKED -	REVISED -
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INTERSECTION PAVEMENT JOINT DETAILS

SCALE: 1" = 10' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	56
CONTRACT NO. 68800				
<small>ILLINOIS FED. AID PROJECT</small>				



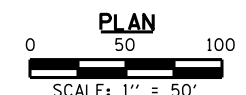
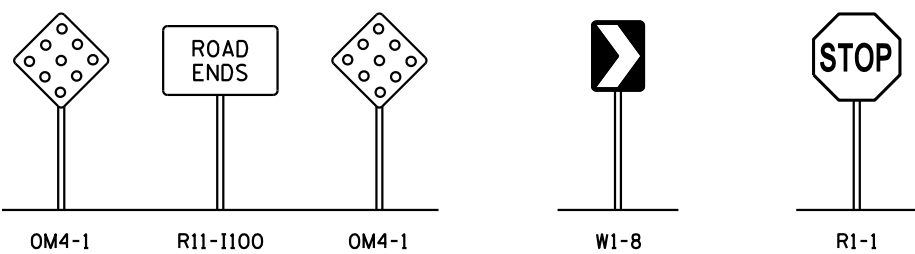
LEGEND

- ① - MODIFIED URETHANE PAVEMENT MARKING LINE - 4" (WHITE)
- ② - MODIFIED URETHANE PAVEMENT MARKING LINE - 4" (YELLOW)
- ③ - MODIFIED URETHANE PAVEMENT MARKING LINE - 4" (WHITE SKIP DASH)
- ④ - MODIFIED URETHANE PAVEMENT MARKING LINE - 8" (WHITE SKIP DASH) 2' LINES 6' DASH
- ⑤ - MODIFIED URETHANE PAVEMENT MARKING LINE - 6" (WHITE) (SKIP DASH)
- ⑤A - MODIFIED URETHANE PAVEMENT MARKING LINE - 8" (WHITE)
- ⑥ - MODIFIED URETHANE PAVEMENT MARKING - LINE 12" SOLID (YELLOW)
- ⑦ - MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW NO PASSING)
- ⑧ - MODIFIED URETHANE PAVEMENT MARKING - LINE 24" (WHITE)
- ◁ RAISED REFLECTIVE PAVEMENT MARKER (1 WAY) (CRYSTAL)
- ◄ RAISED REFLECTIVE PAVEMENT MARKER (1 WAY) (AMBER)
- ◆ RAISED REFLECTIVE PAVEMENT MARKER (2 WAY) (AMBER)
- ↵ MODIFIED URETHANE PAVEMENT MARKING - LETTERS & SYMBOLS
- † W1-8 CHEVRON (EXCEPT AS NOTED)

SIGN LOCATION AND SIZE

	SIGNS	LOCATION	SIZE	QUANTITY
LOGISTICS	R11-I100	STA. 14+95	48"x30"	1
	OM4-1	STA. 14+95 (8' LT. & RT., 12' LT. & RT.)	36"x36"	4
KNOX	R1-1	STA. 10+56 40' LT.	36"x36"	1
	R1-1	STA. 44+05 35' LT.	30"x30"	1
US 150	W1-8	STA. 49+00 TO STA. 55+00	18"x24"	12
	W1-8	STA. 56+30 TO STA. 64+30 LT.	24"x30"	12
	*	STA. 56+30 TO STA. 64+30 LT.	2"x60"	12

* - RETROREFLECTIVE SIGN SUPPORT PANEL (BOTH SIDES)



USER NAME =	DESIGNED -	REVISD -
PLOT SCALE =	CHECKED -	REVISD -
PLOT DATE =	DRAWN -	REVISD -
	CHECKED -	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING - US 150 / KNOX RD.

SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	57
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

LEGEND

- ① - MODIFIED URETHANE PAVEMENT MARKING LINE - 4" (WHITE)
- ② - MODIFIED URETHANE PAVEMENT MARKING LINE - 4" (YELLOW)
- ③ - MODIFIED URETHANE PAVEMENT MARKING LINE - 4" (WHITE SKIP DASH)
- ④ - MODIFIED URETHANE PAVEMENT MARKING LINE - 8" (WHITE SKIP DASH) 2' LINES 6' DASH
- ⑤ - MODIFIED URETHANE PAVEMENT MARKING LINE - 6" (WHITE) (SKIP DASH)
- ⑤A - MODIFIED URETHANE PAVEMENT MARKING LINE - 8" (WHITE)
- ⑥ - MODIFIED URETHANE PAVEMENT MARKING - LINE 12" SOLID (YELLOW)
- ⑦ - MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW NO PASSING)
- ⑧ - MODIFIED URETHANE PAVEMENT MARKING - LINE 24" (WHITE)
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- ◄ RAISED REFLECTIVE PAVEMENT MARKER (1 WAY) (AMBER)
- ◆ RAISED REFLECTIVE PAVEMENT MARKER (2 WAY) (AMBER)
- ↵ MODIFIED URETHANE PAVEMENT MARKING - LETTERS & SYMBOLS
- ⊥ W1-8 CHEVRON (EXCEPT AS NOTED)

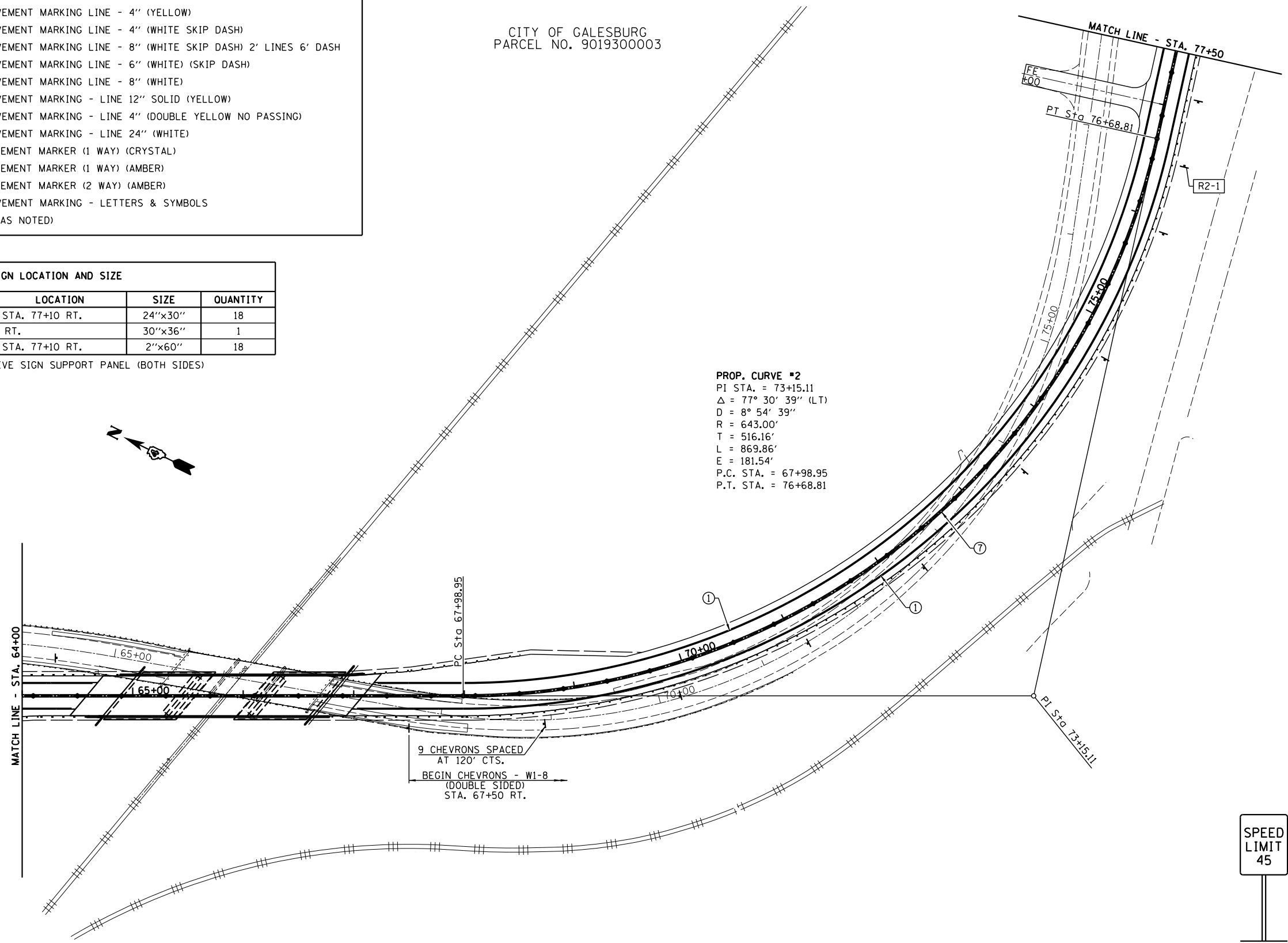
SIGN LOCATION AND SIZE

SIGNS	LOCATION	SIZE	QUANTITY
W1-8	STA. 67+50 TO STA. 77+10 RT.	24"x30"	18
R2-1	STA. 76+50 20' RT.	30"x36"	1
•	STA. 67+50 TO STA. 77+10 RT.	2"x60"	18

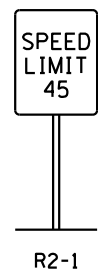
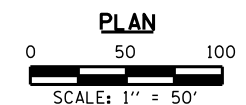
• - RETROREFLECTIVE SIGN SUPPORT PANEL (BOTH SIDES)

CITY OF GALESBURG
PARCEL NO. 9019300003

PROP. CURVE #2
 PI STA. = 73+15.11
 $\Delta = 77^\circ 30' 39''$ (LT)
 $D = 8^\circ 54' 39''$
 $R = 643.00'$
 $T = 516.16'$
 $L = 869.86'$
 $E = 181.54'$
 P.C. STA. = 67+98.95
 P.T. STA. = 76+68.81



9 CHEVRONS SPACED
AT 120' CTS.
BEGIN CHEVRONS - W1-8
(DOUBLE SIDED)
STA. 67+50 RT.



USER NAME =	DESIGNED -	REVISED -
	CHECKED -	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING - US 150 /KNOX RD.

SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	58
CONTRACT NO. 68800				

ILLINOIS FED. AID PROJECT

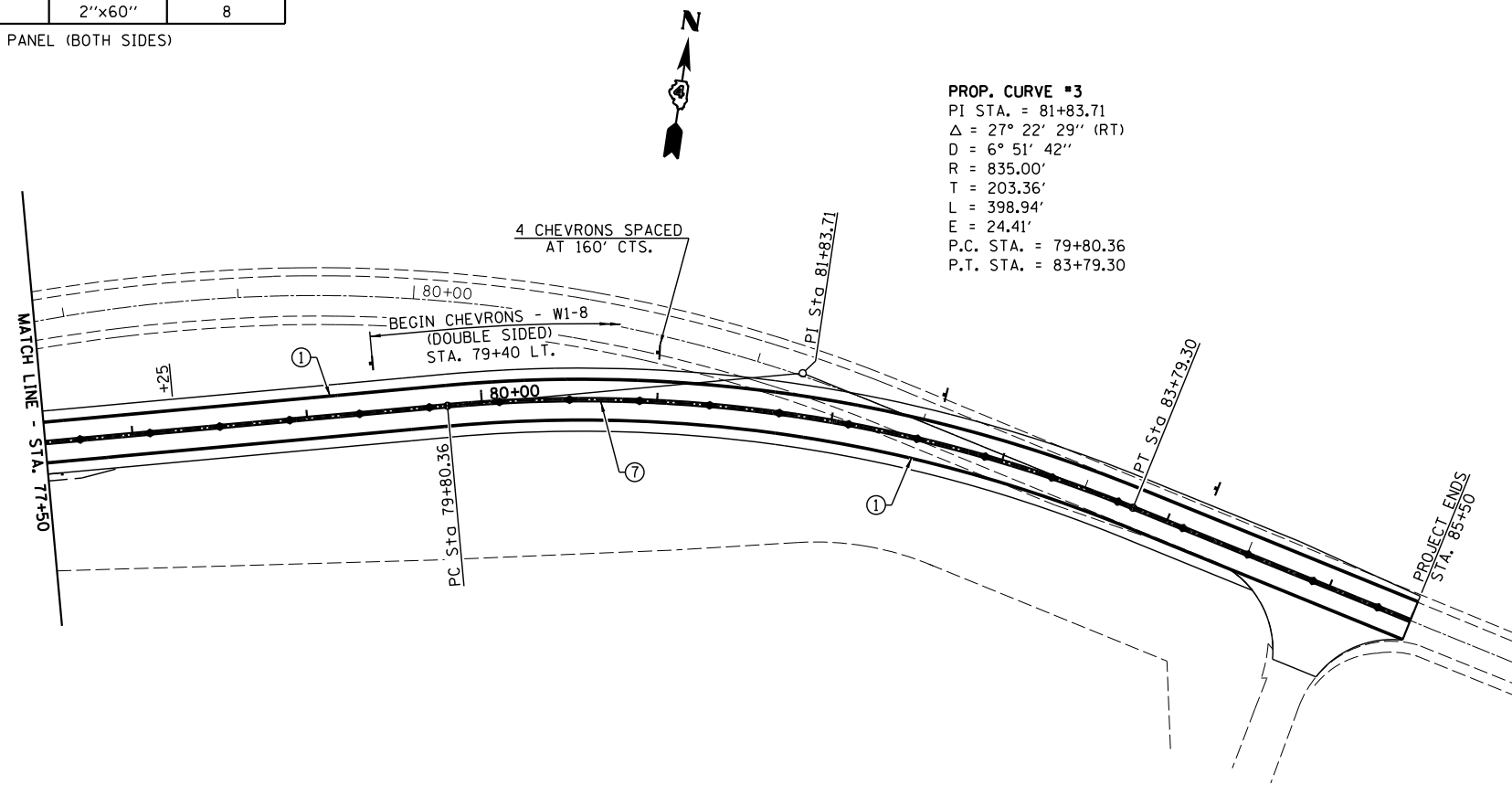
LEGEND

- ① - MODIFIED URETHANE PAVEMENT MARKING LINE - 4" (WHITE)
- ② - MODIFIED URETHANE PAVEMENT MARKING LINE - 4" (YELLOW)
- ③ - MODIFIED URETHANE PAVEMENT MARKING LINE - 4" (WHITE SKIP DASH)
- ④ - MODIFIED URETHANE PAVEMENT MARKING LINE - 8" (WHITE SKIP DASH) 2' LINES 6' DASH
- ⑤ - MODIFIED URETHANE PAVEMENT MARKING LINE - 6" (WHITE) (SKIP DASH)
- ⑥A - MODIFIED URETHANE PAVEMENT MARKING LINE - 8" (WHITE)
- ⑥ - MODIFIED URETHANE PAVEMENT MARKING - LINE 12" SOLID (YELLOW)
- ⑦ - MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW NO PASSING)
- ⑧ - MODIFIED URETHANE PAVEMENT MARKING - LINE 24" (WHITE)
- ◁ RAISED REFLECTIVE PAVEMENT MARKER (1 WAY) (CRYSTAL)
- ◄ RAISED REFLECTIVE PAVEMENT MARKER (1 WAY) (AMBER)
- ◆ RAISED REFLECTIVE PAVEMENT MARKER (2 WAY) (AMBER)
- ↪ MODIFIED URETHANE PAVEMENT MARKING - LETTERS & SYMBOLS
- † W1-8 CHEVRON (EXCEPT AS NOTED)

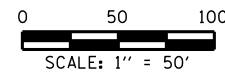
SIGN LOCATION AND SIZE

SIGNS	LOCATION	SIZE	QUANTITY
W1-8	STA. 79+40 TO STA. 84+20 LT.	24"x30"	8
*	STA. 79+40 TO STA. 84+20 LT.	2"x60"	8

* - RETROREFLECTIVE SIGN SUPPORT PANEL (BOTH SIDES)



PLAN



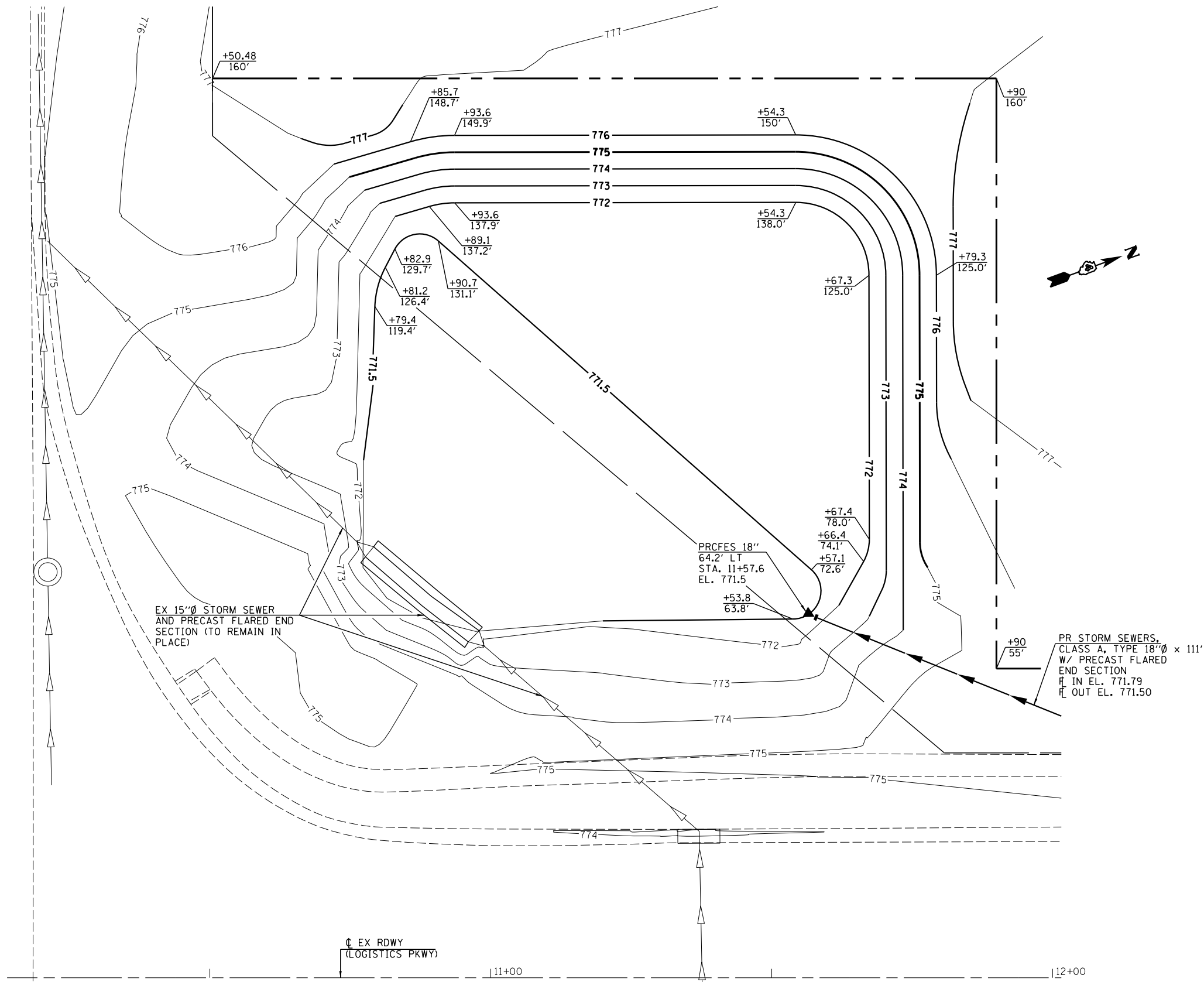
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PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING - US 150 /KNOX RD.

SCALE: 1" = 50' SHEET NO. 3 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	59
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

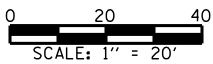


EX 15"Ø STORM SEWER
AND PRECAST FLARED END
SECTION (TO REMAIN IN
PLACE)

PRCFES 18"
64.2' LT
STA. 11+57.6
EL. 771.5

PR STORM SEWERS,
CLASS A, TYPE 18"Ø x 111'
W/ PRECAST FLARED
END SECTION
℄ IN EL. 771.79
℄ OUT EL. 771.50

PLAN
(LOGISTICS PARKWAY DETENTION BASIN)

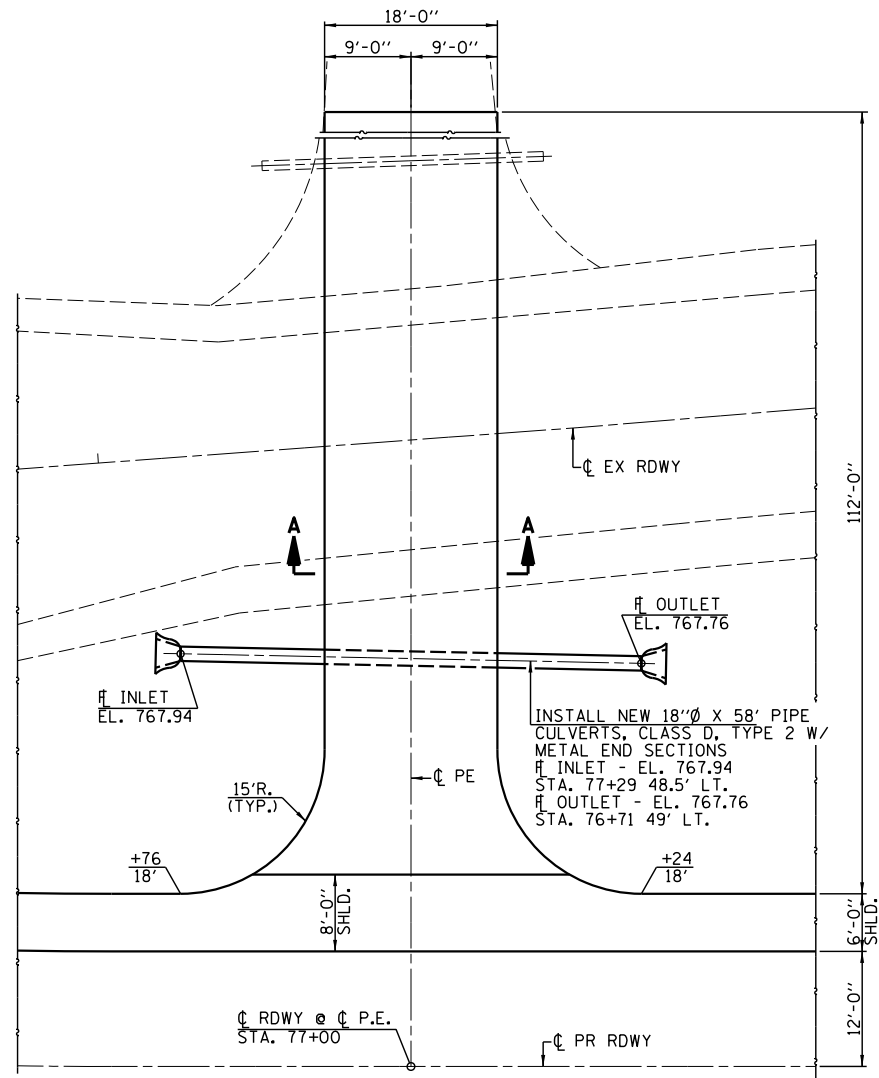


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PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

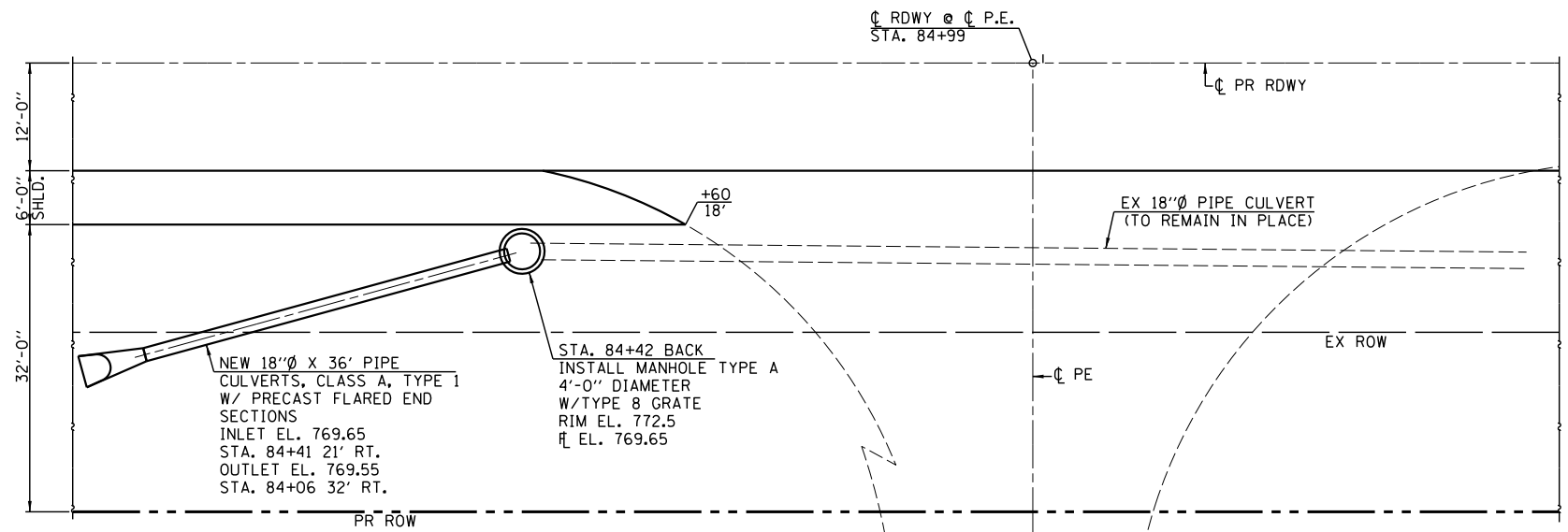
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN - DETENTION BASIN
SCALE: 1" = 20' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

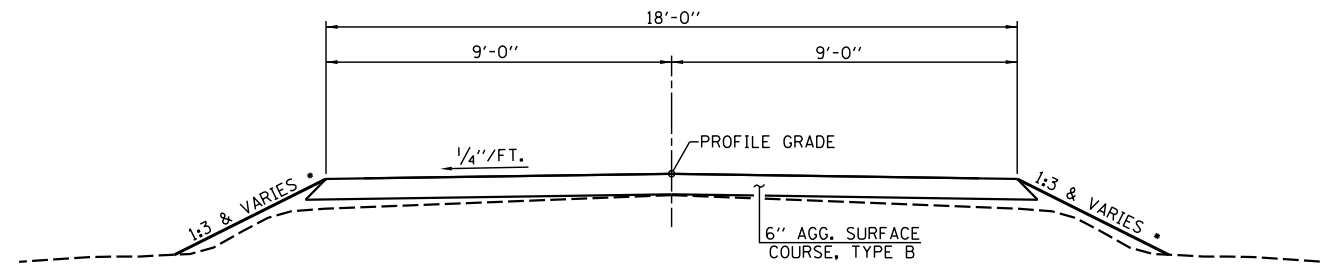
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	60
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



PLAN
(F.E. STA. 77+00 LT.)



PLAN
(P.E. STA. 84+99 RT.)



SECTION A-A

• 1:6 FROM 18' TO 32' LEFT OF US 150 CL



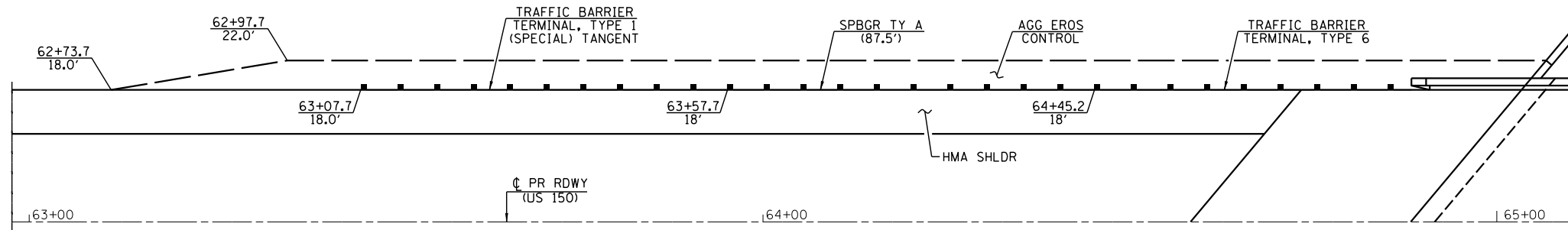
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

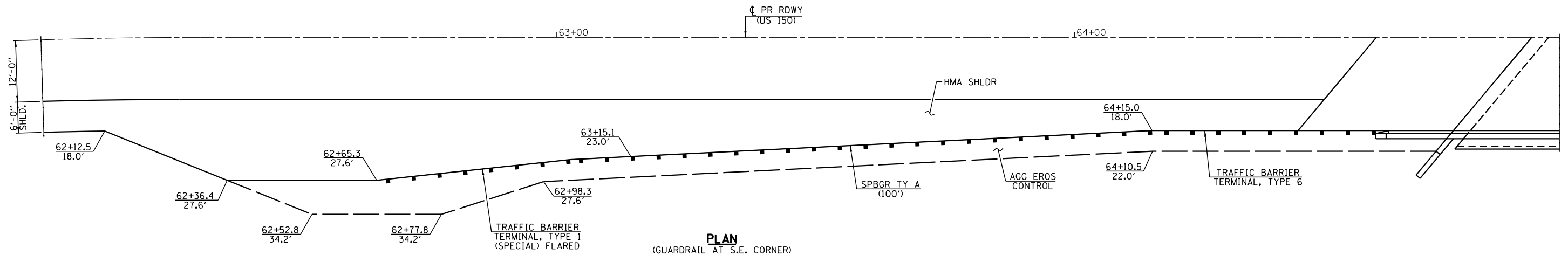
ROADWAY DETAILS

SCALE: 1" = 10' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

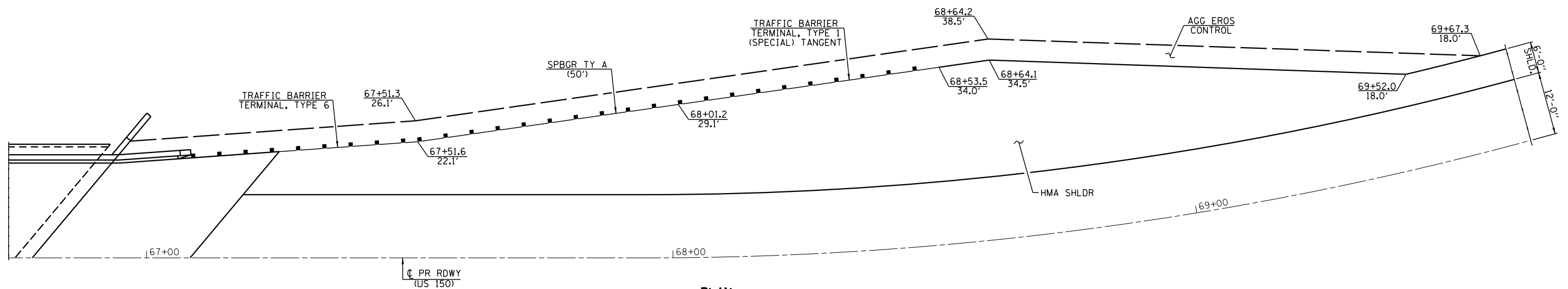
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	61
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



PLAN
(GUARDRAIL AT S.W. CORNER)



PLAN
(GUARDRAIL AT S.E. CORNER)



PLAN
(GUARDRAIL AT N.W. CORNER)



USER NAME =	DESIGNED -	REVISED -
CHECKED -	REVISOR -	
PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

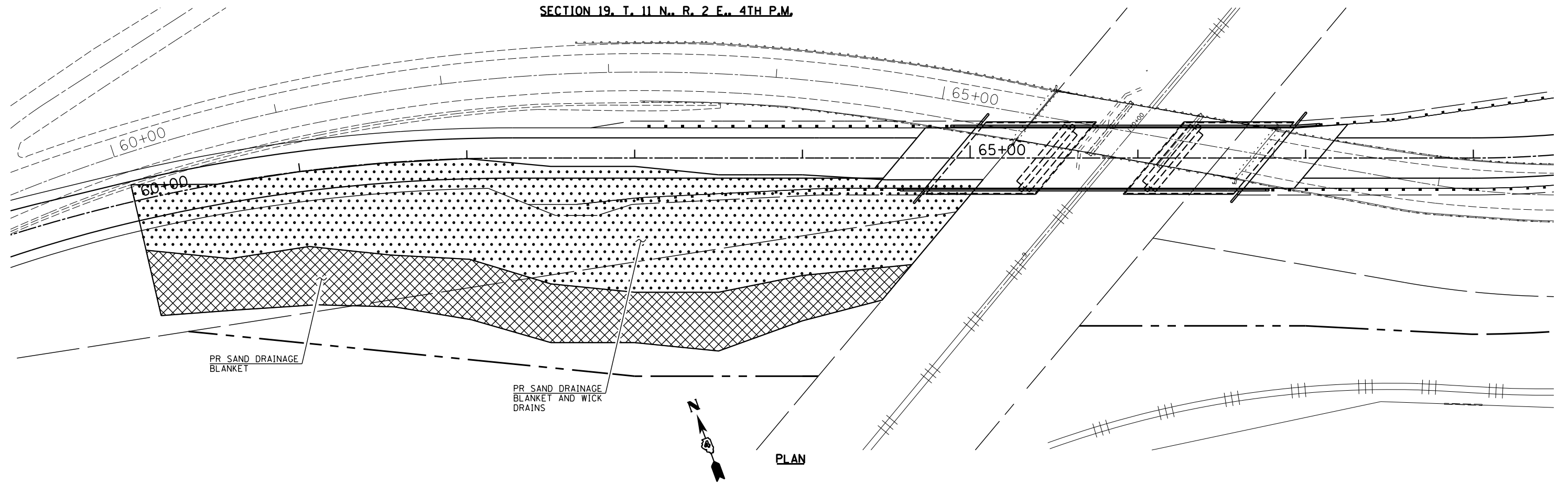
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY GUARDRAIL DETAILS

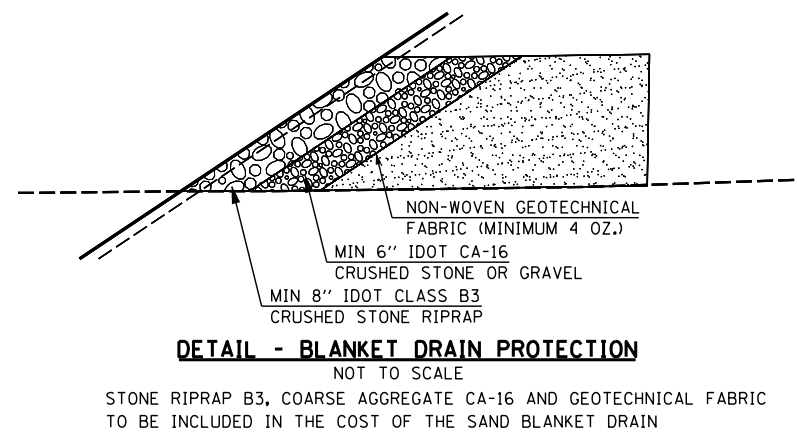
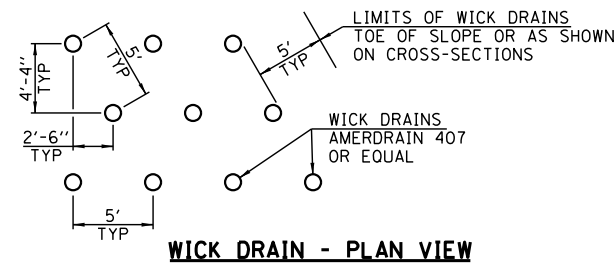
SCALE: 1" = 10' SHEET NO. 3 OF 3 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	62
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

SECTION 19, T. 11 N., R. 2 E., 4TH P.M.

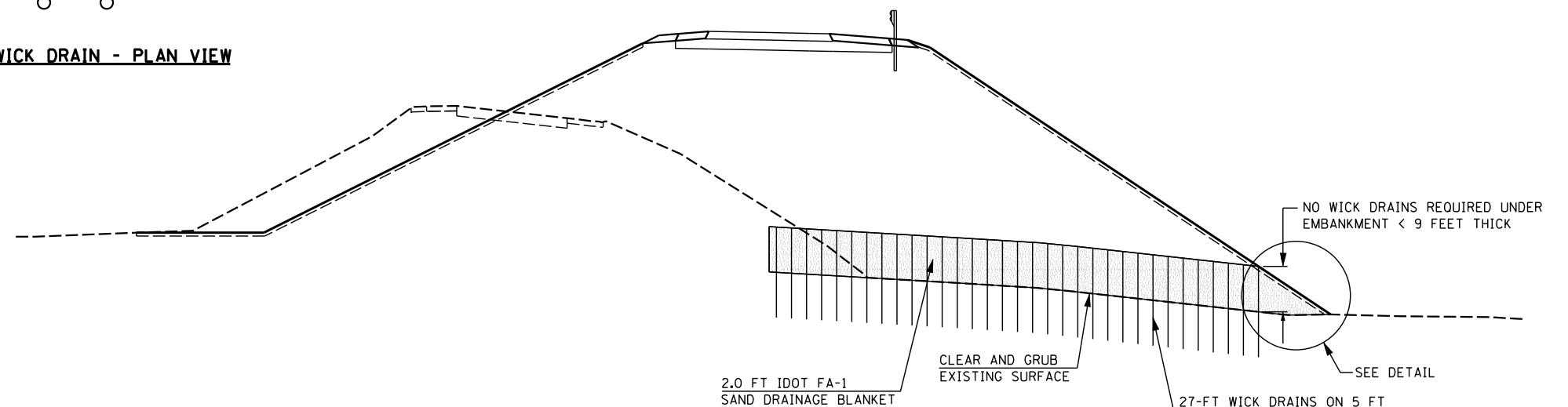


LOCATION	WICK DRAINS & SAND BLANKET	SAND BLANKET
60+00	10' LT TO 30' RT	30' RT TO 70' RT
60+50	CL TO 45' RT	45' RT TO 76' RT
61+00	CL TO 45' RT	45' RT TO 80' RT
61+50	CL TO 55' RT	55' RT TO 86' RT
62+00	CL TO 60' RT	60' RT TO 92' RT
62+50	5' RT TO 75' RT	75' RT TO 110' RT
63+00	5' RT TO 80' RT	80' RT TO 110' RT
63+50	10' RT TO 80' RT	80' RT TO 115' RT
64+00	10' RT TO 70' RT	70' RT TO 97' RT
64+50	13' RT TO 65' RT	65' RT TO 84' RT



DETAIL - BLANKET DRAIN PROTECTION

NOT TO SCALE
STONE RIPRAP B3, COARSE AGGREGATE CA-16 AND GEOTECHNICAL FABRIC TO BE INCLUDED IN THE COST OF THE SAND BLANKET DRAIN



DETAIL - SAND DRAINAGE BLANKET - CROSS-SECTION

NOT TO SCALE



USER NAME =	DESIGNED -	REVISED -
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PLOT DATE =	DRAWN -	REVISED -
	CHECKED -	REVISED -

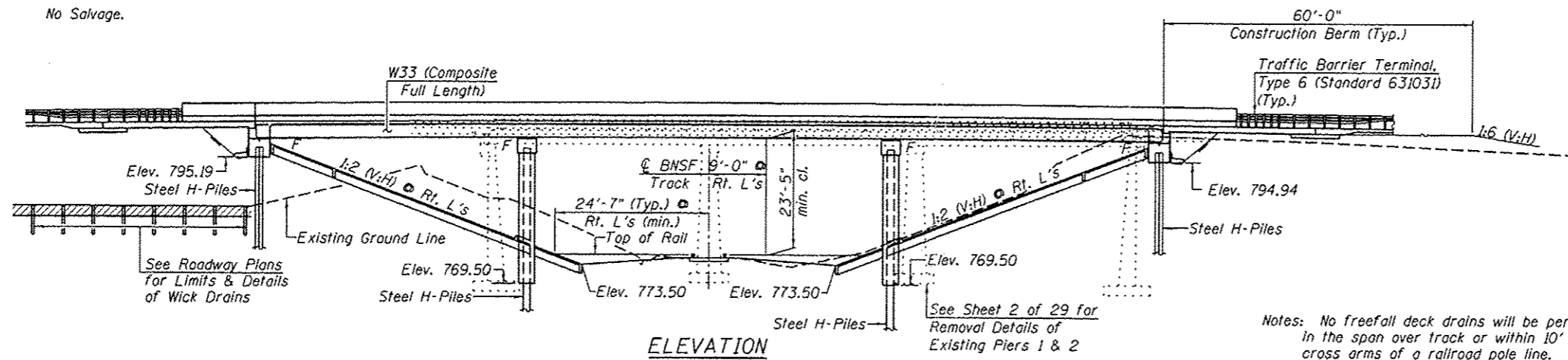
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WICK DRAINS & SAND BLANKET DETAILS	
SCALE: 1" = 30'	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	63
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

Benchmark: "M" in Mueller on fire hydrant at N.W. Corner of Grand Ave. (U.S. Route 150) and Knox Road (600 E).
Sta. 53+39, 75' Lt. Elev. 776.39

Existing Structure: S.N. 048-0013 was built as S.B.I. Route 8, Section 40 in 1927 at Sta. 66+15.00. The original superstructure was replaced in 1977 as S.B.I. Route 8, Section 40V-1, RS. The structure consists of three simple span PPC deck beams supported on spill-thru counter-fort abutments and multi-column piers. 133'-7 1/2" Bk.-Bk. Abutments. 33'-0" out.-out. deck. Existing structure to be removed. Road will be closed and traffic detoured during construction.
No Salvage.

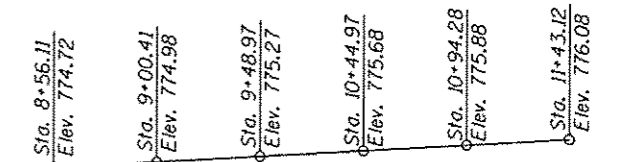


LOADING HL 93
Allow 50#/sq. ft. for future wearing surface.

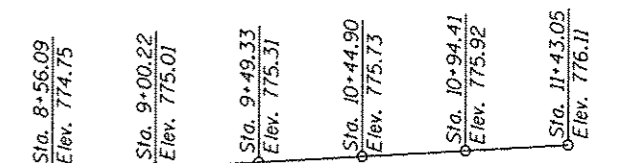
DESIGN SPECIFICATIONS
2014 AASHTO LRFD Bridge Design Specifications, 7th Edition with 2015 and 2016 Interims.

DESIGN STRESSES
FIELD UNITS
f'c = 3,500 psi
f'c = 4,000 psi (superstructure concrete)
fy = 60,000 psi (reinforcement)
fy = 50,000 psi (M270 Grade 50) - Primary
fy = 36,000 psi (M270 Grade 36)

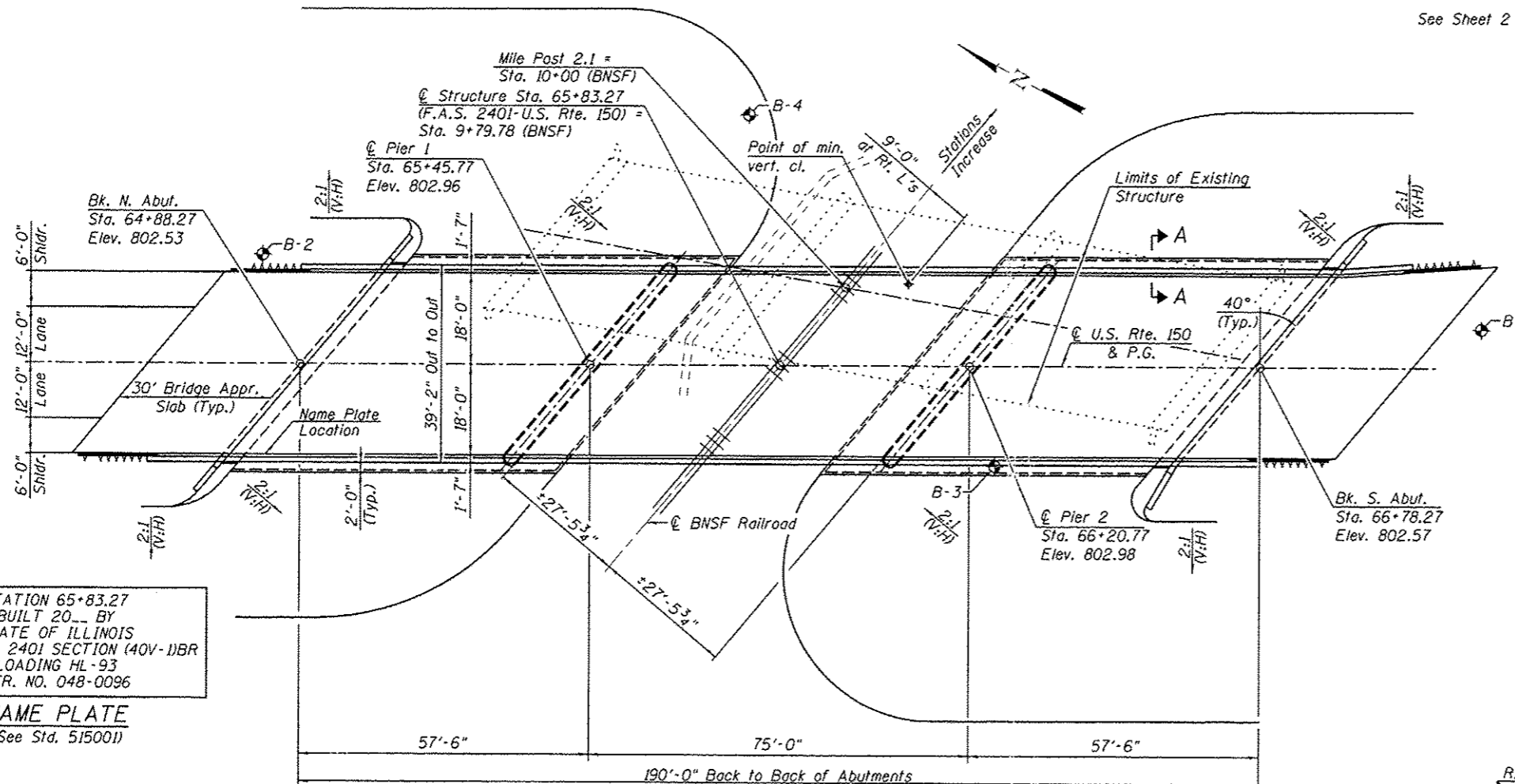
SEISMIC DATA
Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (S₀₁) = 0.101g
Design Spectral Acceleration at 0.2 sec. (S₀₅) = 0.151g
Soil Site Class = D



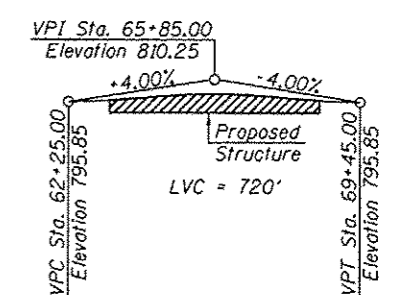
EXISTING TOP OF RAIL ELEVATIONS
(North Rail Along BNSF Track)



EXISTING TOP OF RAIL ELEVATIONS
(South Rail Along BNSF Track)



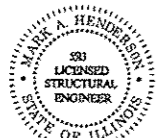
- INDEX OF SHEETS**
- 1 - General Plan & Elevation
 - 2 - General Data
 - 3-6 - Top of Slab Elevations
 - 7-8 - Top of Approach Slab Elevations
 - 9 - Superstructure
 - 10 - Superstructure Details
 - 11-12 - Diaphragm Details
 - 13-17 - Bridge Approach Slab Details
 - 18 - Structural Steel
 - 19 - Structural Steel Details
 - 20 - Bearing Details
 - 21 - North Abutment
 - 22 - South Abutment
 - 23 - Pier 1
 - 24 - Pier 2
 - 25 - HP Pile Details
 - 26 - Concrete Parapet Slipforming Option
 - 27-29 - Boring Logs



PROFILE GRADE
(Along U.S. Rte. 150)

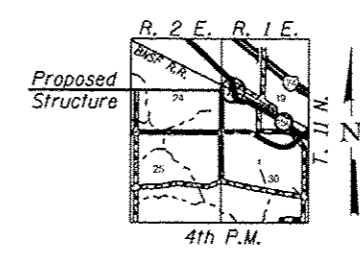
STATION 65+83.27
BUILT 20__ BY
STATE OF ILLINOIS
F.A.S. RTE 2401 SECTION (40V-1)BR
LOADING HL-93
STR. NO. 048-0096

NAME PLATE
(See Std. 515001)



APPROVED
For Structural Adequacy Only
Sh. Carl Kruger
Engineer of Bridges & Structures

Mark A. Henderson 12/11/2016
Expiration Date: 11/30/2018



LOCATION SKETCH

GENERAL PLAN & ELEVATION
U.S. ROUTE 150 OVER
BNSF RAILROAD
F.A.S. ROUTE 2401 - SECTION (40V-1)BR
KNOX COUNTY
STA. 65+83.27
S.N. 048-0096



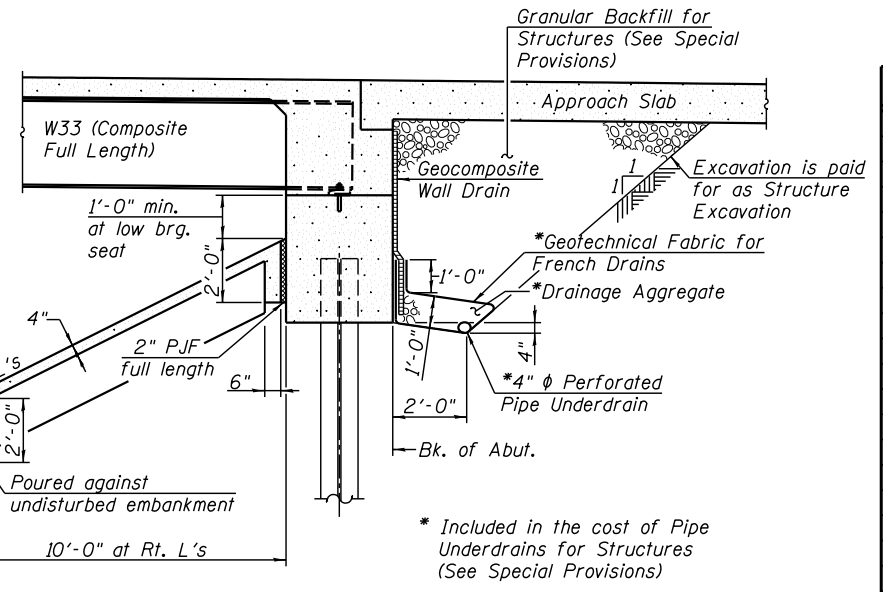
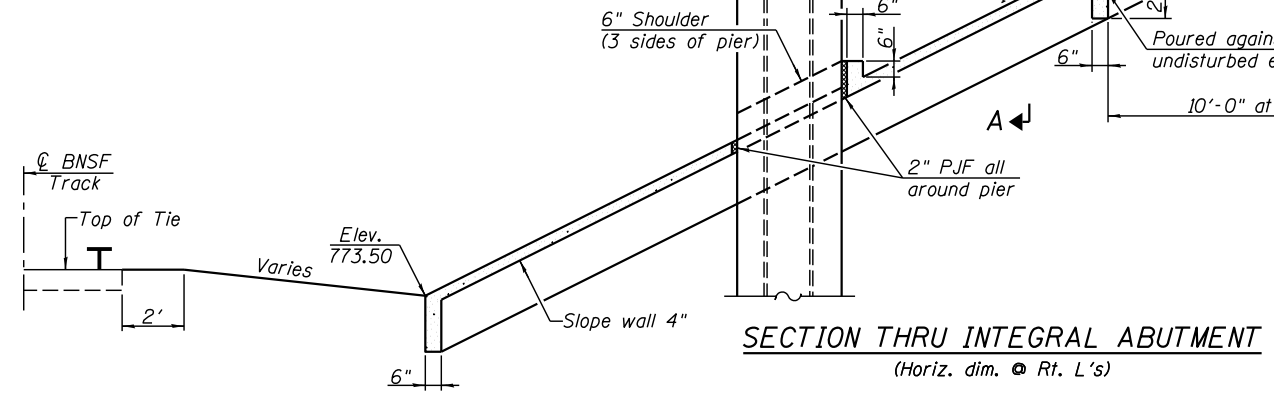
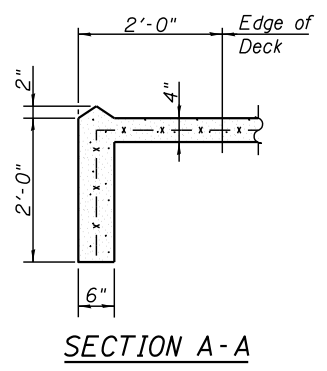
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PLOT SCALE *	CHECKED - MAH	REVISIONS
PLOT DATE *	DRAWN - JRP	REVISIONS
	CHECKED - GBR	REVISIONS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 048-0096
SHEET NO. 1 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	64
			CONTRACT NO. 68800	

ILLINOIS FED. AID PROJECT



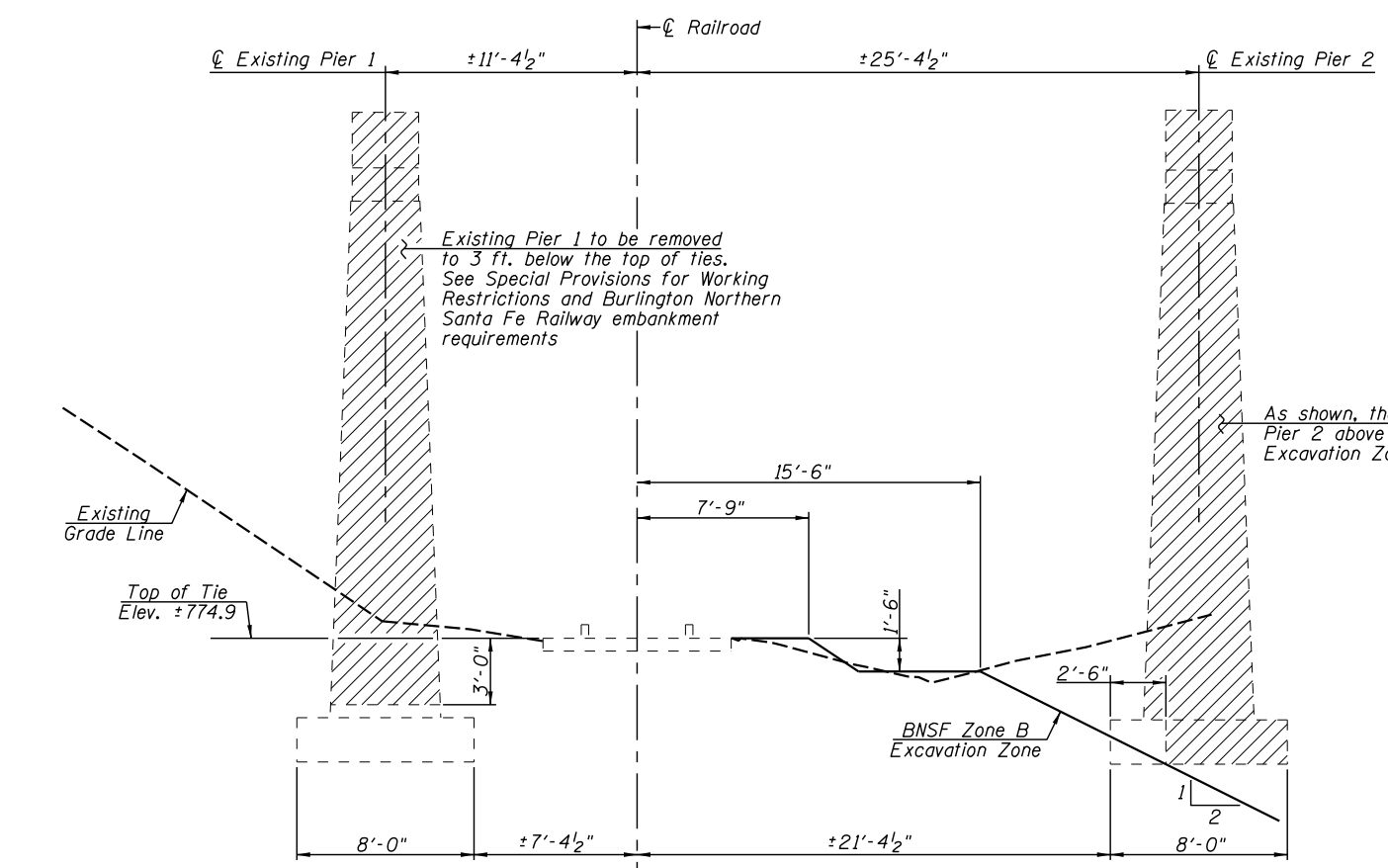
TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Removal of Existing Structures	Each			1
Structure Excavation	Cu. Yd.		377	377
Concrete Structures	Cu. Yd.		369.7	369.7
Concrete Superstructure	Cu. Yd.	275.7		275.7
Bridge Deck Grooving	Sq. Yd.	938		938
Protective Coat	Sq. Yd.	1221.7		1221.7
Concrete Superstructure (Approach Slab)	Cu. Yd.	121.3		121.3
Furnishing and Erecting Structural Steel	L. Sum	1		1
Stud Shear Connectors	Each	4842		4842
Reinforcement Bars, Epoxy Coated	Pound	109820	39190	149010
Slope Wall 4 Inch	Sq. Yd.		619	619
Furnishing Steel Piles HP 12x74	Foot		2199	2199
Driving Piles	Foot		2199	2199
Test Pile Steel HP 12x74	Each		2	2
Name Plates	Each	1		1
Anchor Bolts, 1"	Each		48	48
Geocomposite Wall Drains	Sq. Yd.		89	89
Granular Backfill for Structures	Cu. Yd.		149	149
Asbestos Bearing Pad Removal	Each			44
Pipe Underdrains for Structures 4"	Foot		187	187

GENERAL NOTES

All new structural steel shall be hot-dip galvanized. See Special Provision for "Hot-Dip Galvanizing for Structural Steel".
 Fasteners shall be ASTM A325 Type 1, hot dip galvanized bolts. Bolts 7/8 in. ϕ , holes 11/16 in. ϕ unless noted otherwise.
 Calculated weight of Structural Steel = 176,440 lbs. (M270 Grade 50)
 12,590 lbs. (M270 Grade 36)
 No field welding is permitted except as specified in the contract documents.
 Reinforcement bars designated (E) shall be epoxy coated.
 Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of 1/8 in. (0.01 ft.). Adjustment shall be made either by grinding the surface or by shimming the bearings.
 The embankment configuration shown shall be the minimum that must be placed and compacted prior to construction of the abutments.
 Wick drains shall be used to expedite the embankment settlement. See roadway plans for locations, details and quantities.

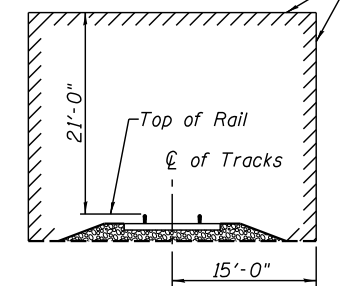
Note:
 Slope wall shall be reinforced with Galvanized Welded Wire Fabric, 6 in. x 6 in. - W4.0 x W4.0 weighing 58 lbs. per 100 sq. ft.
 All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).



PIER REMOVAL DETAILS - LOOKING ALONG RAILROAD

(All dimensions are at Rt. L's to ϕ Railroad)
 (Looking East)
 - Indicates Limits of Structure Removal

No Construction Activities or Other Obstructions Shall Be Placed Within These Limits



MINIMUM CONSTRUCTION CLEARANCE ENVELOPE
 (Normal to Railroad)



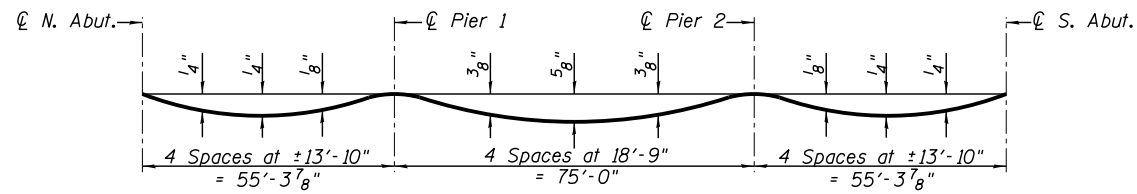
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CHECKED - MAH	REVISIONS -	
PLOT SCALE =	DRAWN - JRP	REVISED -
PLOT DATE =	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL DATA
STRUCTURE NO. 048-0096

SHEET NO. 2 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	65
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

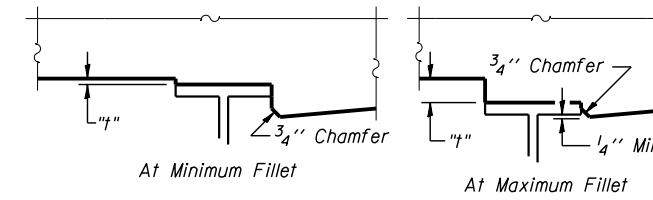


DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

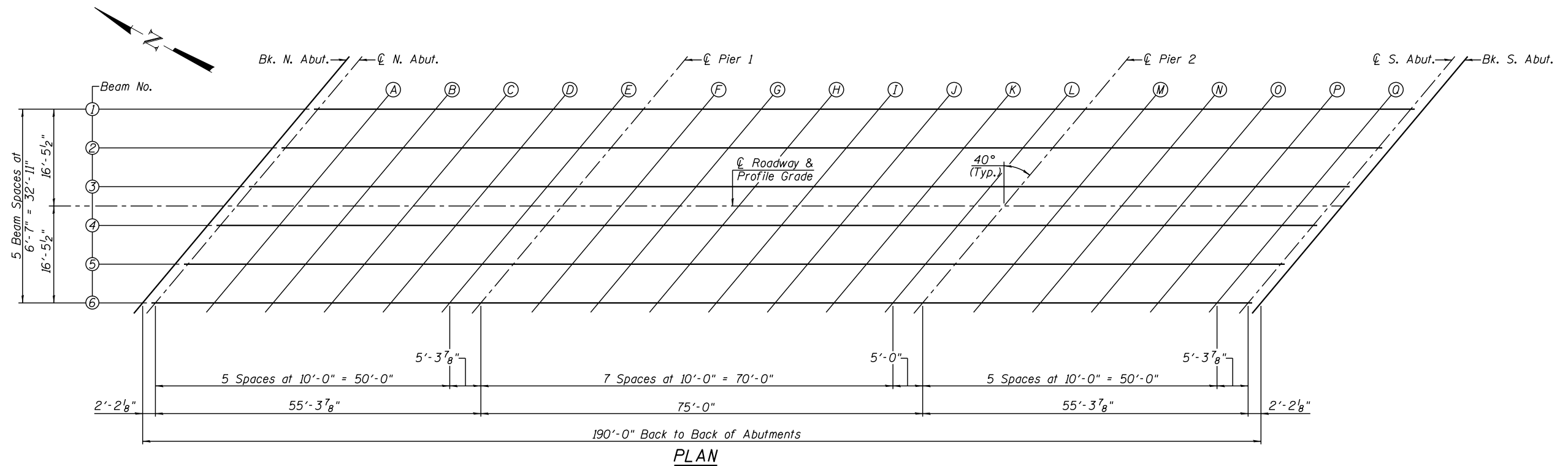
Note:

The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on Sheets 4,5 & 6 of 29.



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on Sheets 4,5 & 6 of 29, minus slab thickness, equals the fillet heights "t" above top flange of beams.

FILLET HEIGHTS



PLAN



USER NAME =	DESIGNED - GBR	REVISED -
	CHECKED - MAH	REVISED -
PLOT SCALE =	DRAWN - JRP	REVISED -
PLOT DATE =	CHECKED - GBH	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOP OF SLAB ELEVATIONS
STRUCTURE NO. 048-0096**

SHEET NO. 3 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	66
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

BEAM 1				
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. N. Abut.	65+02.08	-16.46	802.34	802.34
CL N. Abut.	65+04.26	-16.46	802.36	802.36
A	65+14.26	-16.46	802.44	802.45
B	65+24.26	-16.46	802.52	802.54
C	65+34.26	-16.46	802.58	802.60
D	65+44.26	-16.46	802.63	802.64
E	65+54.26	-16.46	802.67	802.67
CL Pier 1	65+59.58	-16.46	802.68	802.68
F	65+69.58	-16.46	802.71	802.72
G	65+79.58	-16.46	802.72	802.75
H	65+89.58	-16.46	802.72	802.77
I	65+99.58	-16.46	802.71	802.76
J	66+09.58	-16.46	802.69	802.73
K	66+19.58	-16.46	802.65	802.67
L	66+29.58	-16.46	802.61	802.62
CL Pier 2	66+34.58	-16.46	802.58	802.58
M	66+44.58	-16.46	802.52	802.52
N	66+54.58	-16.46	802.45	802.46
O	66+64.58	-16.46	802.37	802.39
P	66+74.58	-16.46	802.28	802.30
Q	66+84.58	-16.46	802.17	802.18
CL S. Abut.	66+89.90	-16.46	802.11	802.11
Bk. S. Abut.	66+92.08	-16.46	802.08	802.08

BEAM 2				
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. N. Abut.	64+96.56	-9.88	802.42	802.42
CL N. Abut.	64+98.74	-9.88	802.44	802.44
A	65+08.74	-9.88	802.53	802.54
B	65+18.74	-9.88	802.61	802.63
C	65+28.74	-9.88	802.68	802.70
D	65+38.74	-9.88	802.73	802.74
E	65+48.74	-9.88	802.78	802.78
CL Pier 1	65+54.06	-9.88	802.80	802.80
F	65+64.06	-9.88	802.83	802.84
G	65+74.06	-9.88	802.85	802.88
H	65+84.06	-9.88	802.85	802.90
I	65+94.06	-9.88	802.85	802.90
J	66+04.06	-9.88	802.83	802.87
K	66+14.06	-9.88	802.81	802.83
L	66+24.06	-9.88	802.77	802.78
CL Pier 2	66+29.06	-9.88	802.74	802.74
M	66+39.06	-9.88	802.69	802.69
N	66+49.06	-9.88	802.62	802.63
O	66+59.06	-9.88	802.55	802.57
P	66+69.06	-9.88	802.46	802.48
Q	66+79.06	-9.88	802.36	802.37
CL S. Abut.	66+84.38	-9.88	802.30	802.30
Bk. S. Abut.	66+86.56	-9.88	802.28	802.28

BEAM 3				
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. N. Abut.	64+91.03	-3.29	802.49	802.49
CL N. Abut.	64+93.21	-3.29	802.52	802.52
A	65+03.21	-3.29	802.61	802.62
B	65+13.21	-3.29	802.70	802.72
C	65+23.21	-3.29	802.77	802.79
D	65+33.21	-3.29	802.84	802.85
E	65+43.21	-3.29	802.89	802.89
CL Pier 1	65+48.53	-3.29	802.91	802.91
F	65+58.53	-3.29	802.95	802.96
G	65+68.53	-3.29	802.97	803.00
H	65+78.53	-3.29	802.98	803.03
I	65+88.53	-3.29	802.98	803.03
J	65+98.53	-3.29	802.97	803.01
K	66+08.53	-3.29	802.95	802.97
L	66+18.53	-3.29	802.92	802.93
CL Pier 2	66+23.53	-3.29	802.90	802.90
M	66+33.53	-3.29	802.85	802.85
N	66+43.53	-3.29	802.79	802.80
O	66+53.53	-3.29	802.72	802.74
P	66+63.53	-3.29	802.64	802.66
Q	66+73.53	-3.29	802.55	802.56
CL S. Abut.	66+78.85	-3.29	802.49	802.49
Bk. S. Abut.	66+81.03	-3.29	802.47	802.47



USER NAME =	DESIGNED - GBR	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS
STRUCTURE NO. 048-0096

SHEET NO. 4 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	67
			CONTRACT NO. 68800	
ILLINOIS FED. AID PROJECT				

CENTERLINE ROADWAY & P.G.				
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. N. Abut.	64+88.27	0.00	802.53	802.53
CL N. Abut.	64+90.45	0.00	802.55	802.55
A	65+00.45	0.00	802.65	802.66
B	65+10.45	0.00	802.74	802.76
C	65+20.45	0.00	802.82	802.84
D	65+30.45	0.00	802.88	802.89
E	65+40.45	0.00	802.94	802.94
CL Pier 1	65+45.77	0.00	802.96	802.96
F	65+55.77	0.00	803.00	803.01
G	65+65.77	0.00	803.03	803.06
H	65+75.77	0.00	803.05	803.10
I	65+85.77	0.00	803.05	803.10
J	65+95.77	0.00	803.04	803.08
K	66+05.77	0.00	803.03	803.05
L	66+15.77	0.00	803.00	803.01
CL Pier 2	66+20.77	0.00	802.98	802.98
M	66+30.77	0.00	802.93	802.93
N	66+40.77	0.00	802.88	802.89
O	66+50.77	0.00	802.81	802.83
P	66+60.77	0.00	802.73	802.75
Q	66+70.77	0.00	802.64	802.65
CL S. Abut.	66+76.09	0.00	802.59	802.59
Bk. S. Abut.	66+78.27	0.00	802.57	802.57

BEAM 4				
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. N. Abut.	64+85.51	3.29	802.57	802.57
CL N. Abut.	64+87.69	3.29	802.59	802.59
A	64+97.69	3.29	802.69	802.70
B	65+07.69	3.29	802.78	802.80
C	65+17.69	3.29	802.86	802.88
D	65+27.69	3.29	802.93	802.94
E	65+37.69	3.29	802.99	802.99
CL Pier 1	65+43.01	3.29	803.02	803.02
F	65+53.01	3.29	803.06	803.07
G	65+63.01	3.29	803.09	803.12
H	65+73.01	3.29	803.11	803.16
I	65+83.01	3.29	803.12	803.17
J	65+93.01	3.29	803.11	803.15
K	66+03.01	3.29	803.10	803.12
L	66+13.01	3.29	803.07	803.08
CL Pier 2	66+18.01	3.29	803.06	803.06
M	66+28.01	3.29	803.01	803.01
N	66+38.01	3.29	802.96	802.97
O	66+48.01	3.29	802.90	802.92
P	66+58.01	3.29	802.82	802.84
Q	66+68.01	3.29	802.73	802.74
CL S. Abut.	66+73.33	3.29	802.68	802.68
Bk. S. Abut.	66+75.51	3.29	802.66	802.66

BEAM 5				
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. N. Abut.	64+79.98	9.88	802.63	802.63
CL N. Abut.	64+82.16	9.88	802.66	802.66
A	64+92.16	9.88	802.77	802.78
B	65+02.16	9.88	802.87	802.89
C	65+12.16	9.88	802.95	802.97
D	65+22.16	9.88	803.03	803.04
E	65+32.16	9.88	803.09	803.09
CL Pier 1	65+37.48	9.88	803.12	803.12
F	65+47.48	9.88	803.17	803.18
G	65+57.48	9.88	803.21	803.24
H	65+67.48	9.88	803.23	803.28
I	65+77.48	9.88	803.24	803.29
J	65+87.48	9.88	803.25	803.29
K	65+97.48	9.88	803.24	803.26
L	66+07.48	9.88	803.22	803.23
CL Pier 2	66+12.48	9.88	803.21	803.21
M	66+22.48	9.88	803.17	803.17
N	66+32.48	9.88	803.12	803.13
O	66+42.48	9.88	803.06	803.08
P	66+52.48	9.88	802.99	803.01
Q	66+62.48	9.88	802.91	802.92
CL S. Abut.	66+67.80	9.88	802.87	802.87
Bk. S. Abut.	66+69.98	9.88	802.85	802.85



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS
STRUCTURE NO. 048-0096

SHEET NO. 5 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	68
			CONTRACT NO. 68800	
ILLINOIS FED. AID PROJECT				

BEAM 6				
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. N. Abut.	64+74.46	16.46	802.70	802.70
CL N. Abut.	64+76.64	16.46	802.73	802.73
A	64+86.64	16.46	802.84	802.85
B	64+96.64	16.46	802.95	802.97
C	65+06.64	16.46	803.04	803.06
D	65+16.64	16.46	803.12	803.13
E	65+26.64	16.46	803.19	803.19
CL Pier 1	65+31.96	16.46	803.22	803.22
F	65+41.96	16.46	803.28	803.29
G	65+51.96	16.46	803.32	803.35
H	65+61.96	16.46	803.35	803.40
I	65+71.96	16.46	803.37	803.42
J	65+81.96	16.46	803.38	803.42
K	65+91.96	16.46	803.38	803.40
L	66+01.96	16.46	803.36	803.37
CL Pier 2	66+06.96	16.46	803.35	803.35
M	66+16.96	16.46	803.32	803.32
N	66+26.96	16.46	803.28	803.29
O	66+36.96	16.46	803.23	803.25
P	66+46.96	16.46	803.17	803.19
Q	66+56.96	16.46	803.09	803.10
CL S. Abut.	66+62.28	16.46	803.05	803.05
Bk. S. Abut.	66+64.46	16.46	803.03	803.03



USER NAME =	DESIGNED - GBR	REVISED -
	CHECKED - MAH	REVISED -
PLOT SCALE =	DRAWN - JRP	REVISED -
PLOT DATE =	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS
STRUCTURE NO. 048-0096

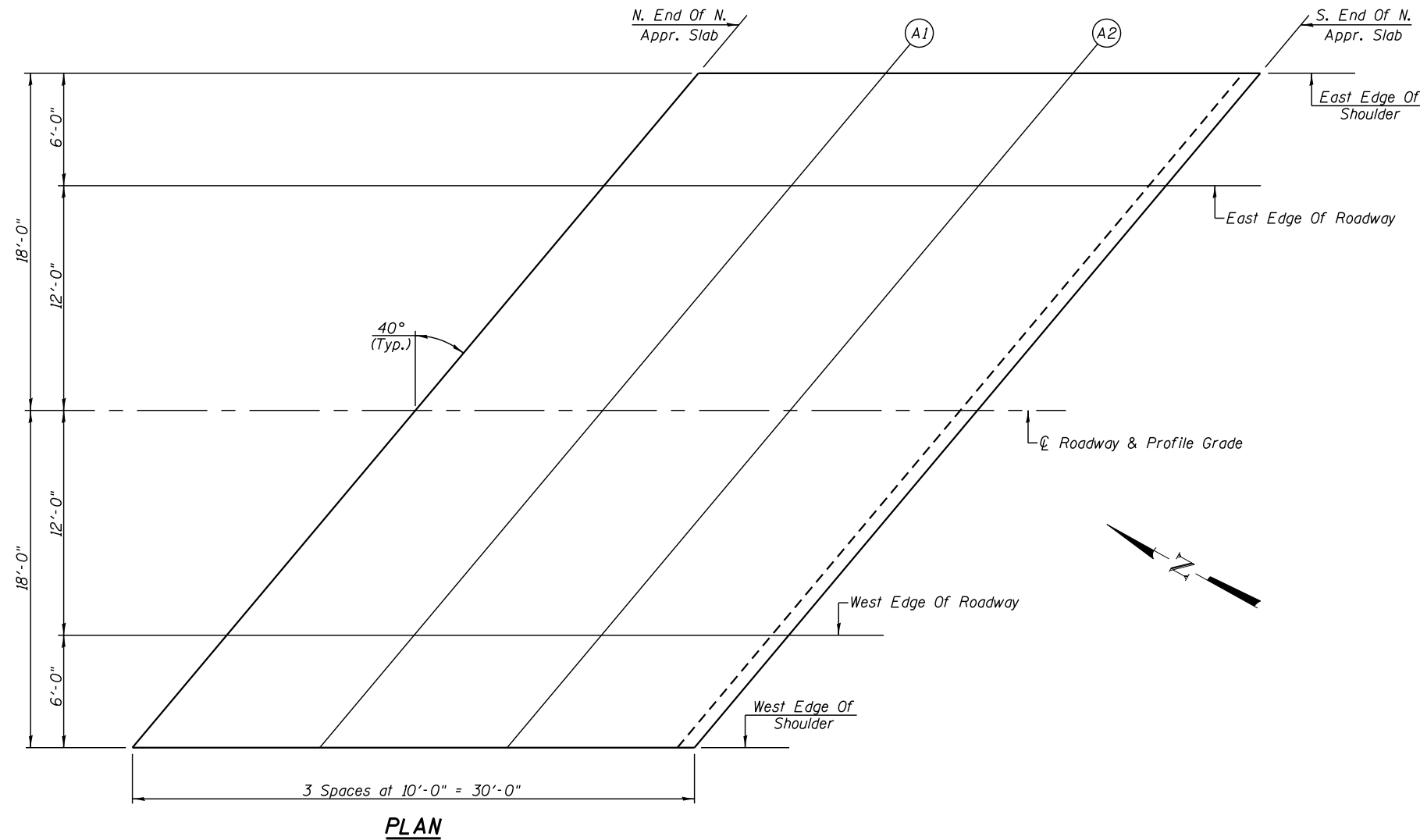
SHEET NO. 6 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	69
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68800	

EAST EDGE OF SHOULDER			
Location	Station	Offset	Theoretical Grade Elevations
N. End of N. Appr. Slab	64+74.68	-18.00	802.01
A1	64+84.68	-18.00	802.13
A2	64+94.68	-18.00	802.24
S. End of N. Appr. Slab	65+04.68	-18.00	802.33

EAST EDGE OF ROADWAY			
Location	Station	Offset	Theoretical Grade Elevations
N. End of N. Appr. Slab	64+69.65	-12.00	802.07
A1	64+79.65	-12.00	802.19
A2	64+89.65	-12.00	802.30
S. End of N. Appr. Slab	64+99.65	-12.00	802.41

CENTERLINE ROADWAY & P.G.			
Location	Station	Offset	Theoretical Grade Elevations
N. End of N. Appr. Slab	64+59.58	0.00	802.18
A1	64+69.58	0.00	802.31
A2	64+79.58	0.00	802.43
S. End of N. Appr. Slab	64+89.58	0.00	802.54



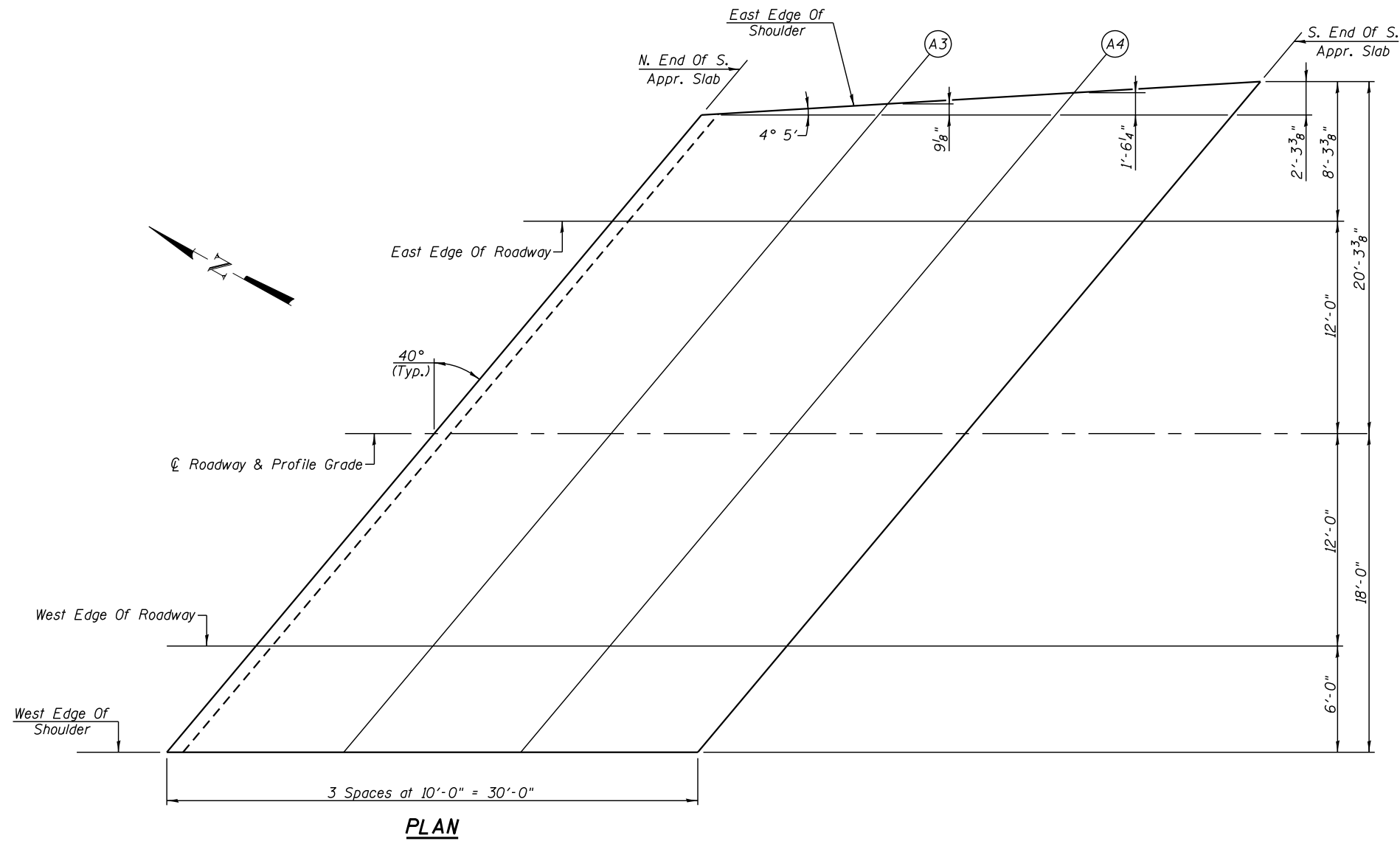
WEST EDGE OF ROADWAY			
Location	Station	Offset	Theoretical Grade Elevations
N. End of N. Appr. Slab	64+49.51	12.00	802.27
A1	64+59.51	12.00	802.42
A2	64+69.51	12.00	802.55
S. End of N. Appr. Slab	64+79.51	12.00	802.67

WEST EDGE OF SHOULDER			
Location	Station	Offset	Theoretical Grade Elevations
N. End of N. Appr. Slab	64+44.47	18.00	802.31
A1	64+54.47	18.00	802.46
A2	64+64.47	18.00	802.60
S. End of N. Appr. Slab	64+74.47	18.00	802.73

EAST EDGE OF SHOULDER			
Location	Station	Offset	Theoretical Grade Elevations
N. End of S. Appr. Slab	66+92.07	-18.00	802.05
A3	67+02.71	-18.76	801.91
A4	67+13.35	-19.52	801.74
S. End of S. Appr. Slab	67+23.99	-20.28	801.57

EAST EDGE OF ROADWAY			
Location	Station	Offset	Theoretical Grade Elevations
N. End of S. Appr. Slab	66+87.03	-12.00	802.23
A3	66+97.03	-12.00	802.11
A4	67+07.03	-12.00	801.98
S. End of S. Appr. Slab	67+17.03	-12.00	801.84

CENTERLINE ROADWAY & P.G.			
Location	Station	Offset	Theoretical Grade Elevations
N. End of S. Appr. Slab	66+76.97	0.00	802.58
A3	66+86.97	0.00	802.47
A4	66+96.97	0.00	802.35
S. End of S. Appr. Slab	67+06.97	0.00	802.22



WEST EDGE OF ROADWAY			
Location	Station	Offset	Theoretical Grade Elevations
N. End of S. Appr. Slab	66+66.90	12.00	802.92
A3	66+76.90	12.00	802.82
A4	66+86.90	12.00	802.71
S. End of S. Appr. Slab	66+96.90	12.00	802.59

WEST EDGE OF SHOULDER			
Location	Station	Offset	Theoretical Grade Elevations
N. End of S. Appr. Slab	66+61.86	18.00	803.08
A3	66+71.86	18.00	802.99
A4	66+81.86	18.00	802.89
S. End of S. Appr. Slab	66+91.86	18.00	802.78



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PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -

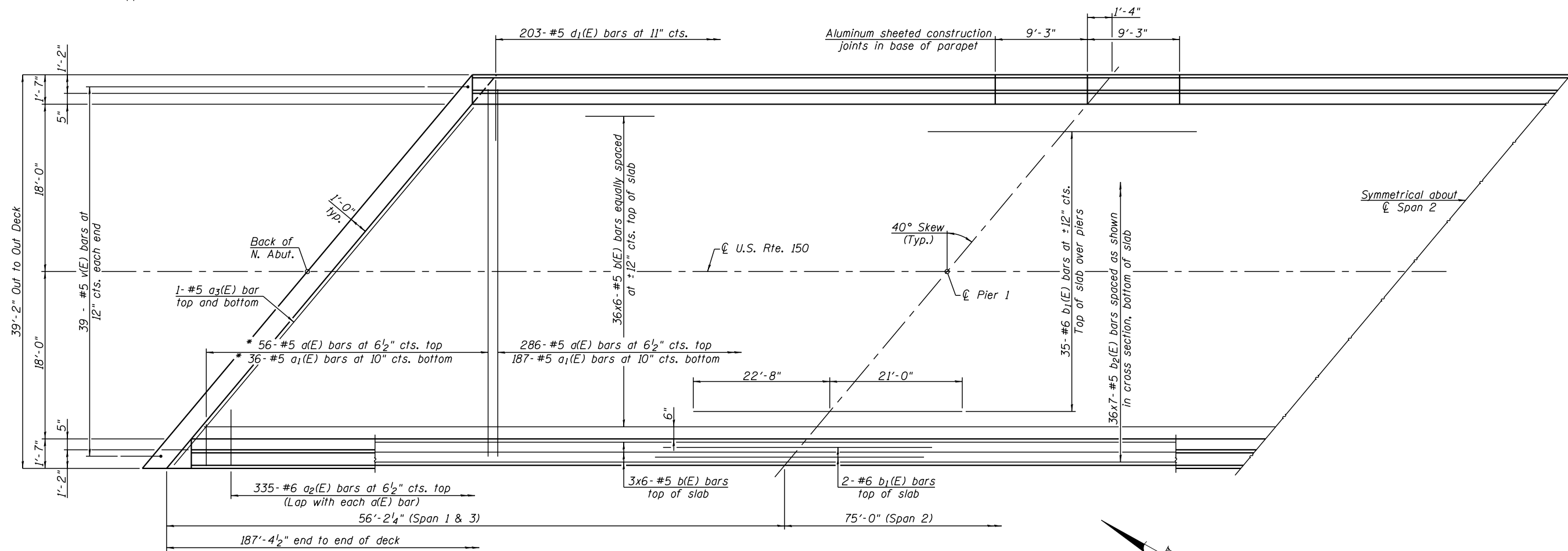
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SOUTH APPROACH SLAB ELEVATIONS
STRUCTURE NO. 048-0096

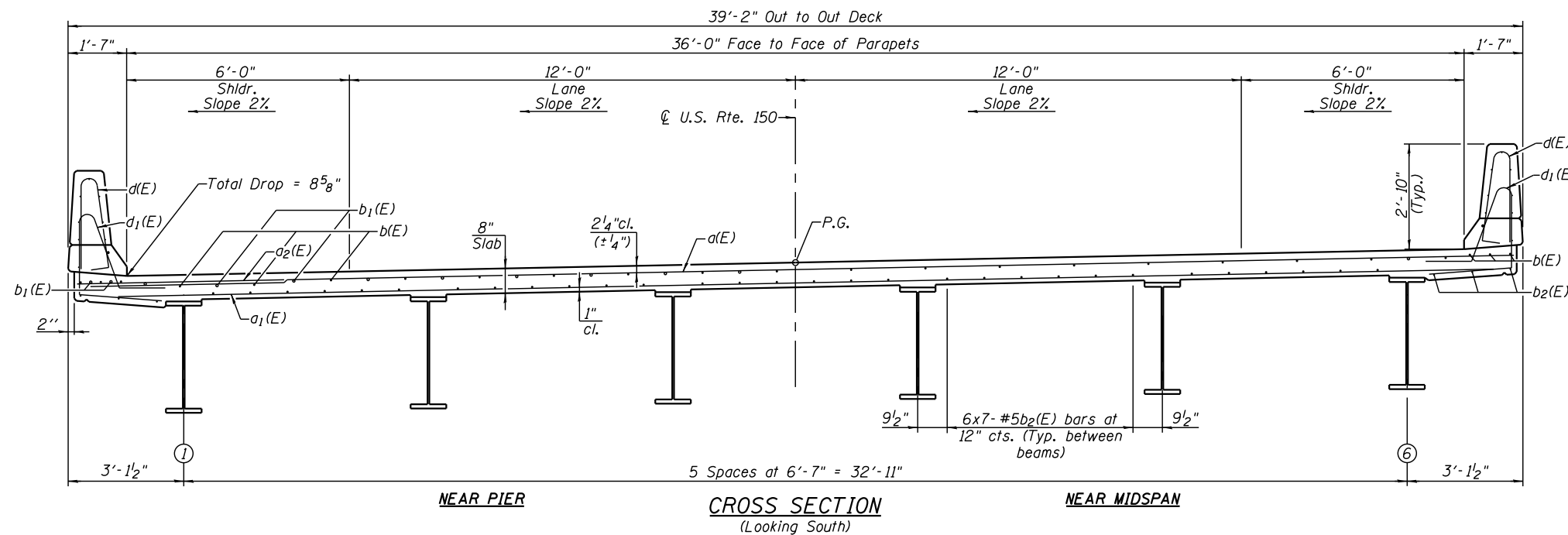
SHEET NO. 8 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	71
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68800	

* Order $a(E)$ and $a_1(E)$ bars full length. Cut to fit skew and use remainder of bars in opposite end.



PARTIAL PLAN



NEAR PIER

CROSS SECTION
(Looking South)

NEAR MIDSPAN

MIN. BAR LAP

#5 = 3'-6"

Notes:
See Sheet 10 of 29 for superstructure details and Bill of Material.

Bars indicated thus 36 x 6-#5 etc. indicates 36 lines of bars with 6 lengths per line.

See Sheet 10 of 29 for parapet reinforcement.



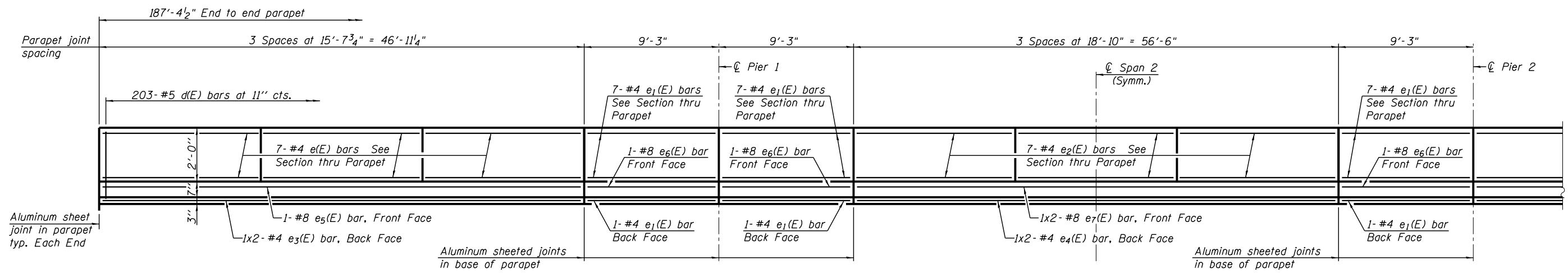
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PLOT SCALE =	CHECKED - MAH	REVISED -
PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
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SUPERSTRUCTURE
STRUCTURE NO. 048-0096

SHEET NO. 9 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	72
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

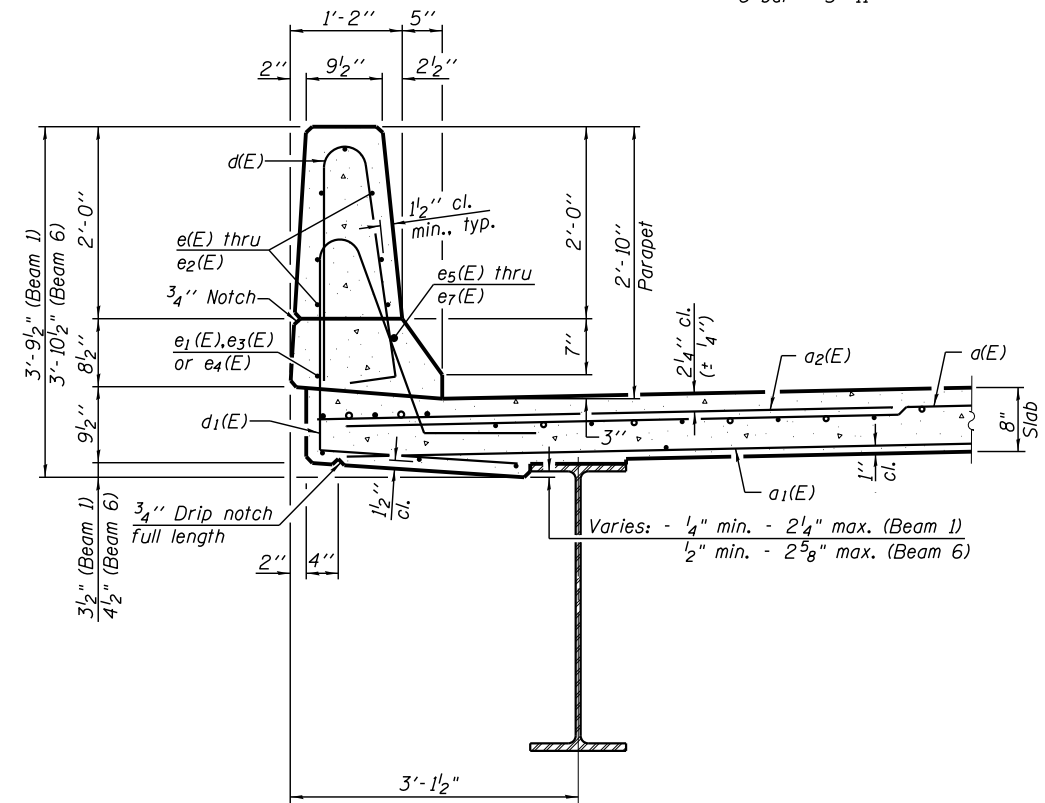


INSIDE ELEVATION OF EAST PARAPET
(West Parapet Similar)

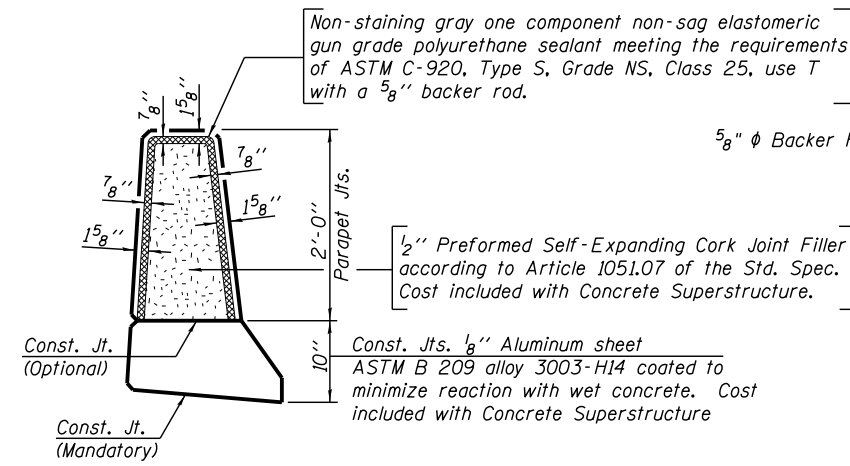
MINIMUM BAR LAP
(Parapet)
#4 bar = 2'-8"
#8 bar = 5'-11"

SUPERSTRUCTURE BILL OF MATERIAL

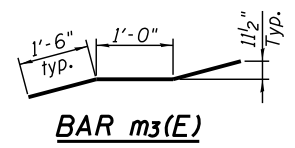
Bar	No.	Size	Length	Shape
a(E)	342	#5	38'-6"	—
a1(E)	223	#5	37'-6"	—
a2(E)	670	#6	6'-6"	—
a3(E)	4	#5	49'-0"	—
b(E)	252	#5	34'-8"	—
b1(E)	78	#6	43'-8"	—
b2(E)	252	#5	30'-3"	—
d(E)	406	#5	5'-7"	⌋
d1(E)	406	#5	7'-11"	⌋
e(E)	84	#4	15'-4"	—
e1(E)	64	#4	9'-0"	—
e2(E)	42	#4	18'-6"	—
e3(E)	8	#4	24'-10"	—
e4(E)	4	#4	29'-6"	—
e5(E)	4	#8	46'-9"	—
e6(E)	8	#8	9'-0"	—
e7(E)	4	#8	31'-1"	—
m(E)	16	#6	27'-7"	—
m1(E)	30	#6	8'-0"	—
m2(E)	12	#6	3'-7"	—
m3(E)	36	#5	4'-0"	—
s(E)	92	#5	9'-2"	⌋
s1(E)	82	#5	10'-1"	⌋
v(E)	78	#5	3'-1"	⌋
Reinforcement Bars, Epoxy Coated			Pound	63,330
Concrete Superstructure			Cu. Yds.	269.1



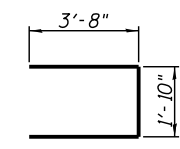
SECTION THRU PARAPET



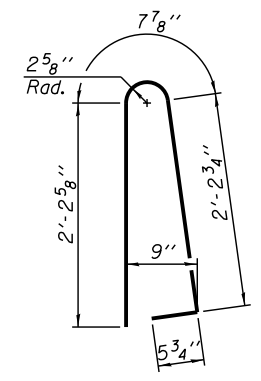
PARAPET JOINT DETAILS



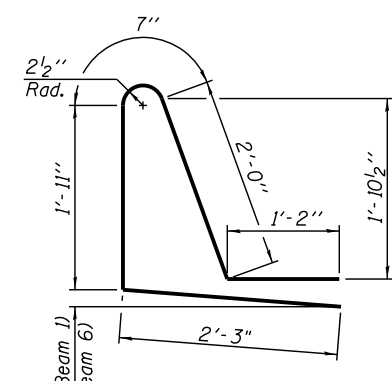
BAR m3(E)



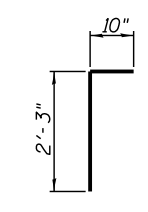
BAR s(E)



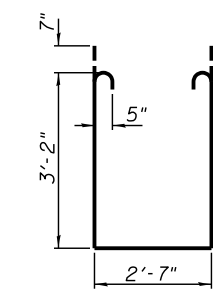
BAR d(E)



BAR d1(E)



BAR v(E)



BAR s1(E)

SI-D2-LR

6-8-15



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PLOT DATE =	DRAWN - JRP	REVISED -
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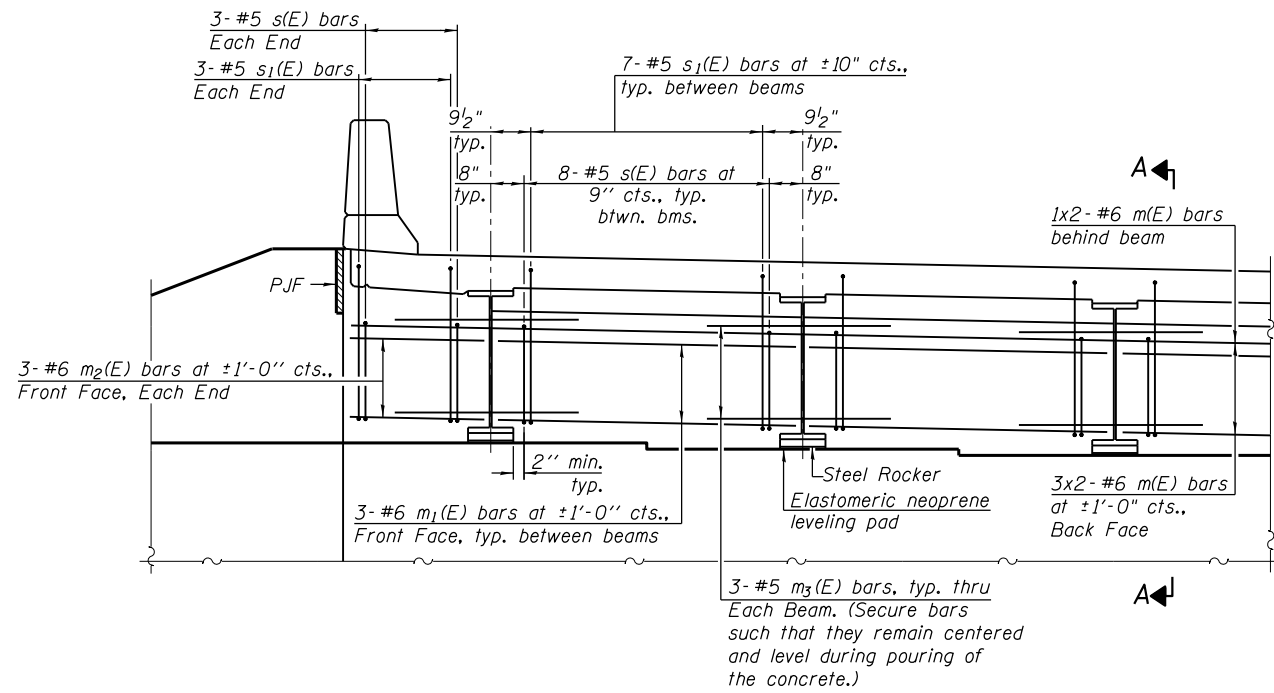
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS
STRUCTURE NO. 048-0096

SHEET NO. 10 OF 29 SHEETS

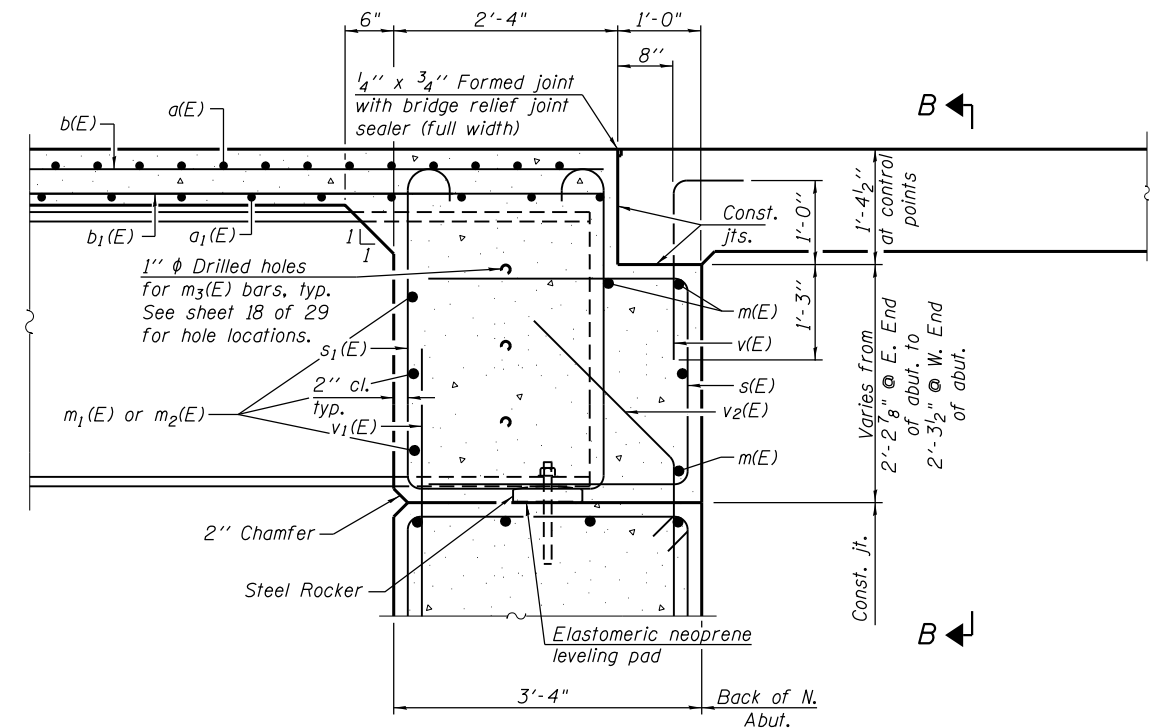
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	73
CONTRACT NO. 68800				

ILLINOIS FED. AID PROJECT

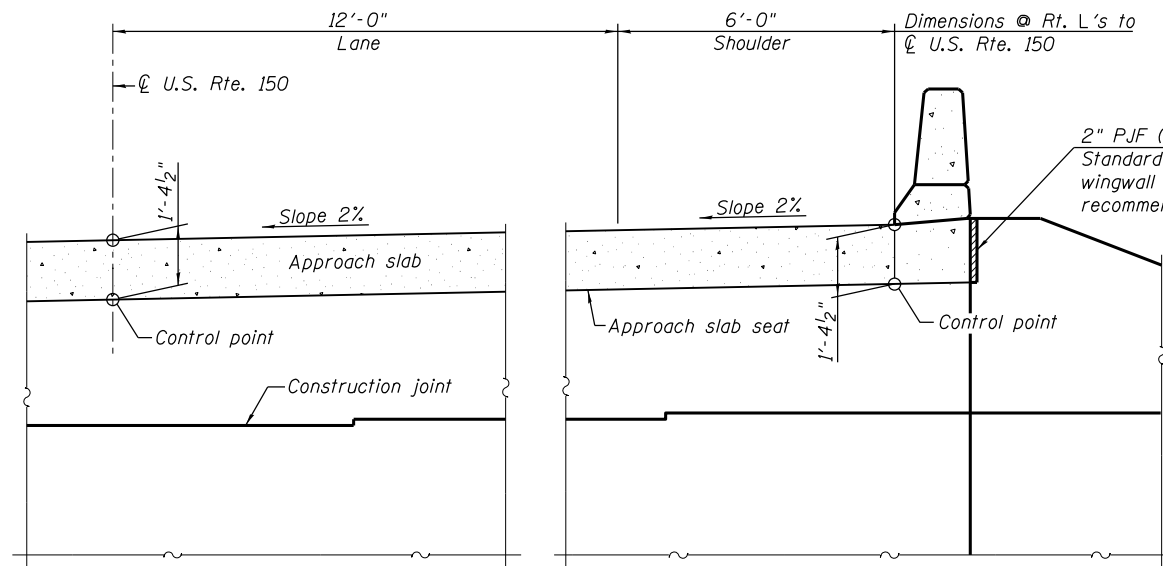


DIAPHRAGM ELEVATION AT NORTH ABUTMENT
(Looking North)

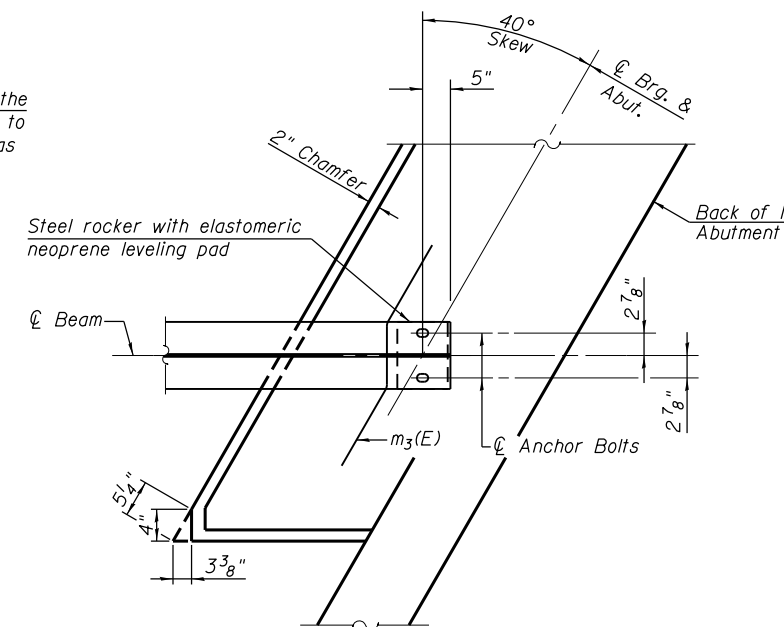
MINIMUM BAR LAP
#6 bar = 4'-4"



SECTION A-A
(at Rt. L's)



SECTION B-B



PARTIAL PLAN AT ABUTMENT
(Showing bottom flange of beam)

Notes:
 Reinforcement bars in diaphragm are billed with superstructure on sheet 10 of 29.
 Concrete in diaphragm is included with Concrete Superstructure on sheet 10 of 29.
 For details of bars s(E), s1(E) and v(E) see sheet 10 of 29.
 The s(E) and s1(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.
 The approach slab seat shall have a constant slope determined from the control points shown.
 For bearing details see sheet 20 of 29.
 Bars indicated 3x2-#6 etc. indicates 3 lines of bars with 2 lengths per line.

DSI-2440-L 8-31-12



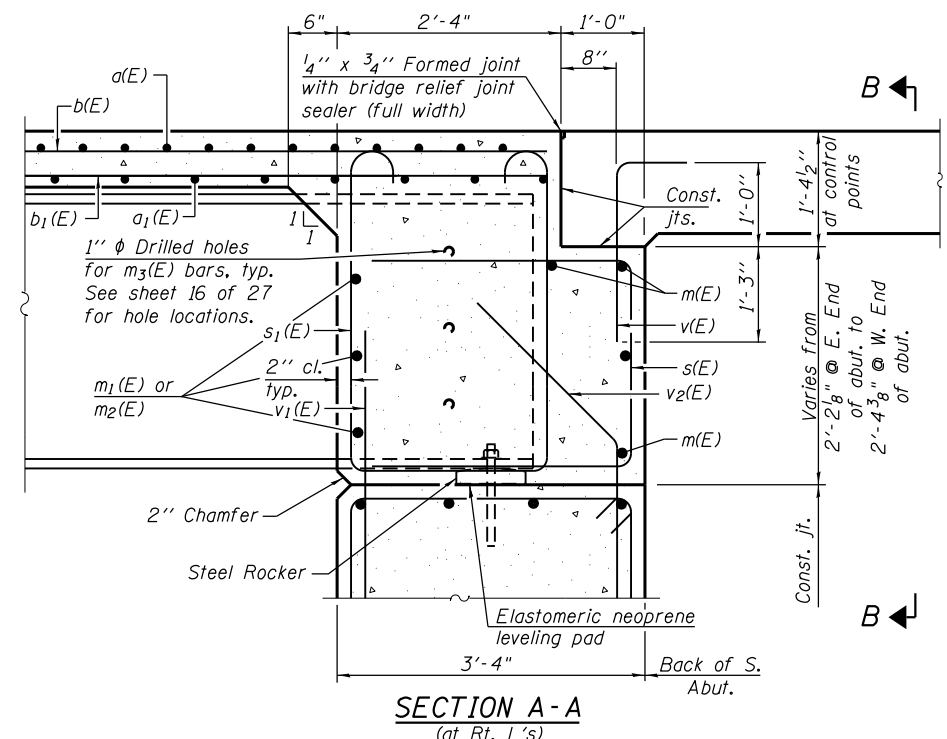
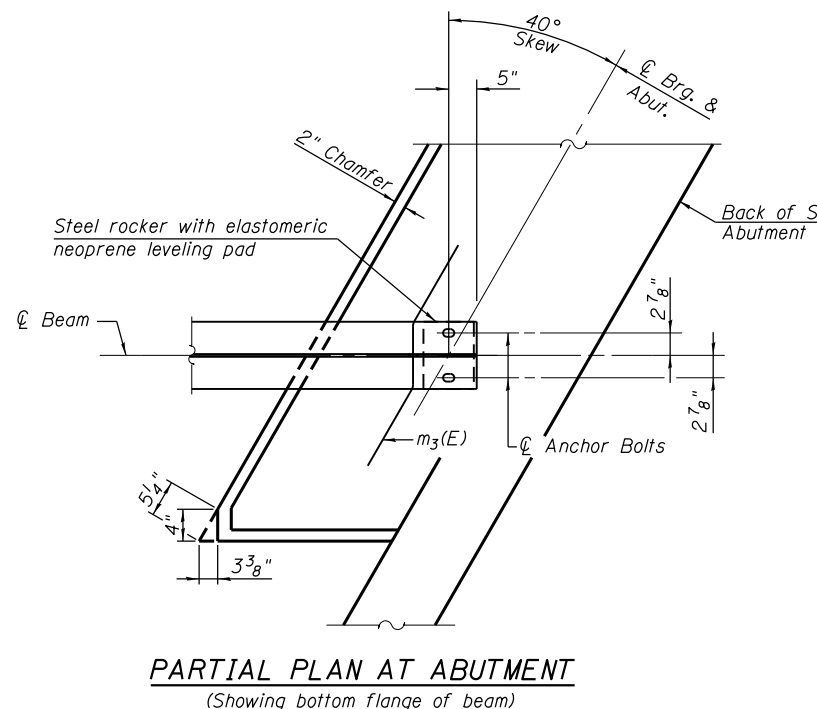
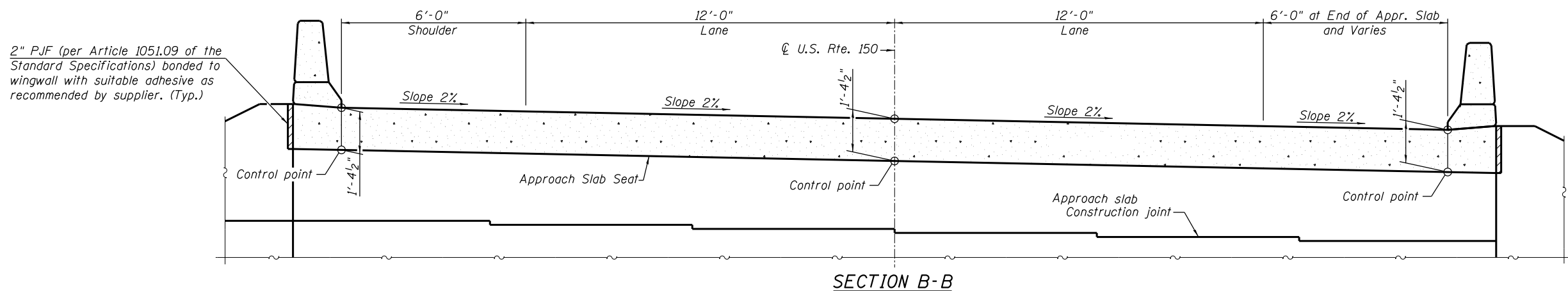
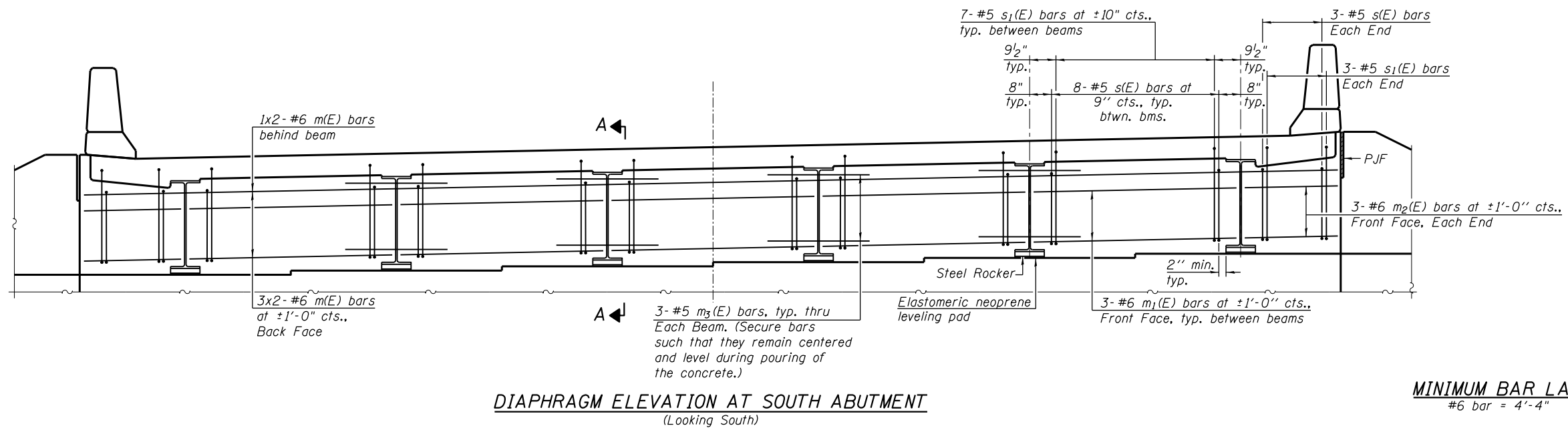
USER NAME =	DESIGNED - GBR	REVISED -
PLOT SCALE =	CHECKED - MAH	REVISED -
PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH ABUTMENT DIAPHRAGM DETAILS
STRUCTURE NO. 048-0096

SHEET NO. 11 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	74
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



Notes:
 Reinforcement bars in diaphragm are billed with superstructure on sheet 10 of 29.
 Concrete in diaphragm is included with Concrete Superstructure on sheet 10 of 29.
 For details of bars s(E), s1(E) and v(E) see sheet 10 of 29.
 The s(E) and s1(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.
 The approach slab seat shall have a constant slope determined from the control points shown.
 For bearing details see sheet 20 of 29.
 Bars indicated 3x2-#6 etc. indicates 3 lines of bars with 2 lengths per line.



USER NAME =	DESIGNED - GBR	REVISED -
CHECKED - MAH	REVISIONS -	
PLOT SCALE =	DRAWN - JRP	REVISIONS -
PLOT DATE =	CHECKED - GBR	REVISIONS -

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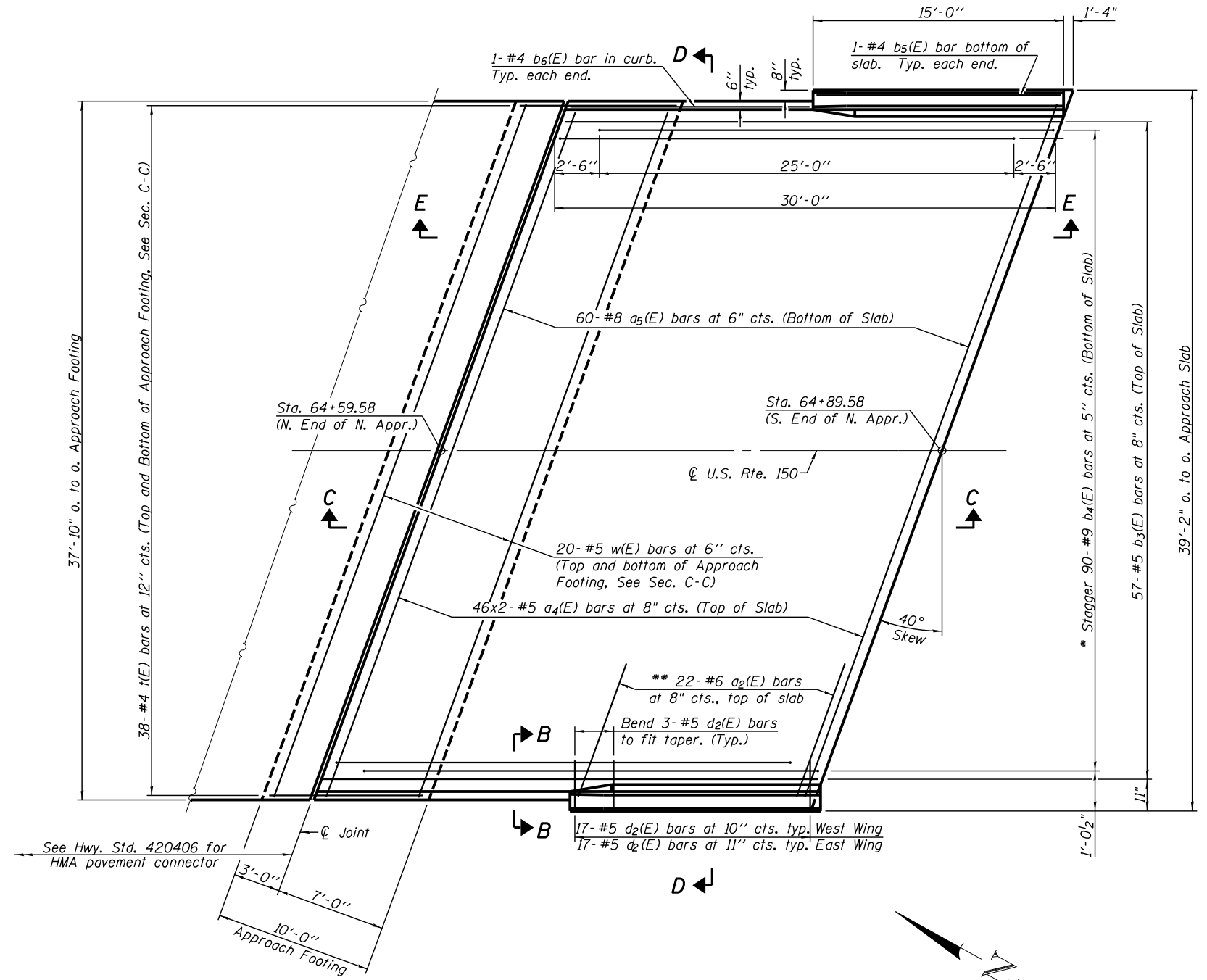
SOUTH ABUTMENT DIAPHRAGM DETAILS
STRUCTURE NO. 048-0096

SHEET NO. 12 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	75
CONTRACT NO. 68800				

ILLINOIS FED. AID PROJECT

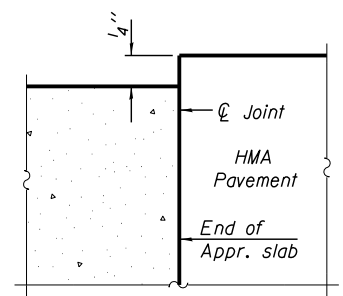
Notes:
 See sheet 14 of 29 for Sections C-C & D-D and View E-E.
 a₄(E) and a₅(E) bar spacings measured along \varnothing Rdwy.
 Bars indicated 46x2-#5 etc. indicates 46 lines of bars with 2 lengths per line.



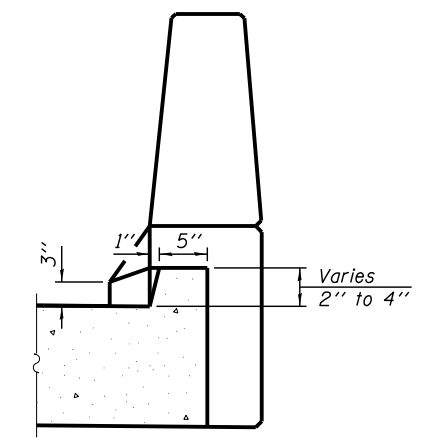
PLAN

* Tilt #9 b₄(E) bars as required to maintain clearance.
 ** Space between a₄(E) bars, typ. each parapet.

MINIMUM BAR LAP
 #5 bar = 3'-6"



**FLEXIBLE PAVEMENT
 DETAIL A**



VIEW B-B

(Sheet 1 of 2)



USER NAME =	DESIGNED - GBR	REVISED -
	CHECKED - MAH	REVISED -
PLOT SCALE =	DRAWN - JRP	REVISED -
PLOT DATE =	CHECKED - GBR	REVISED -

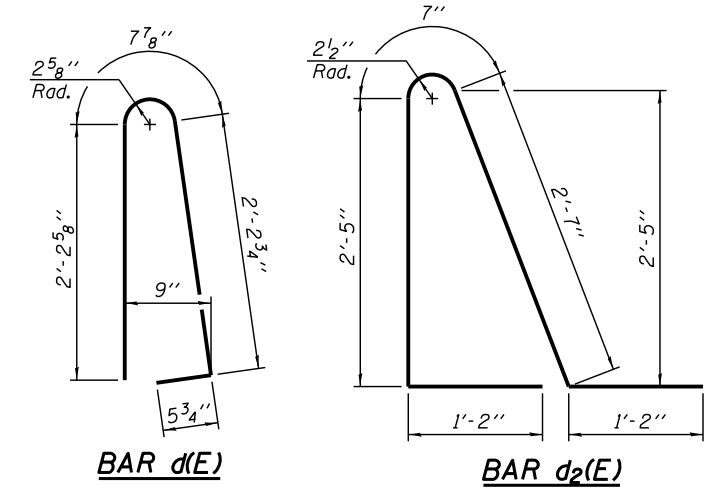
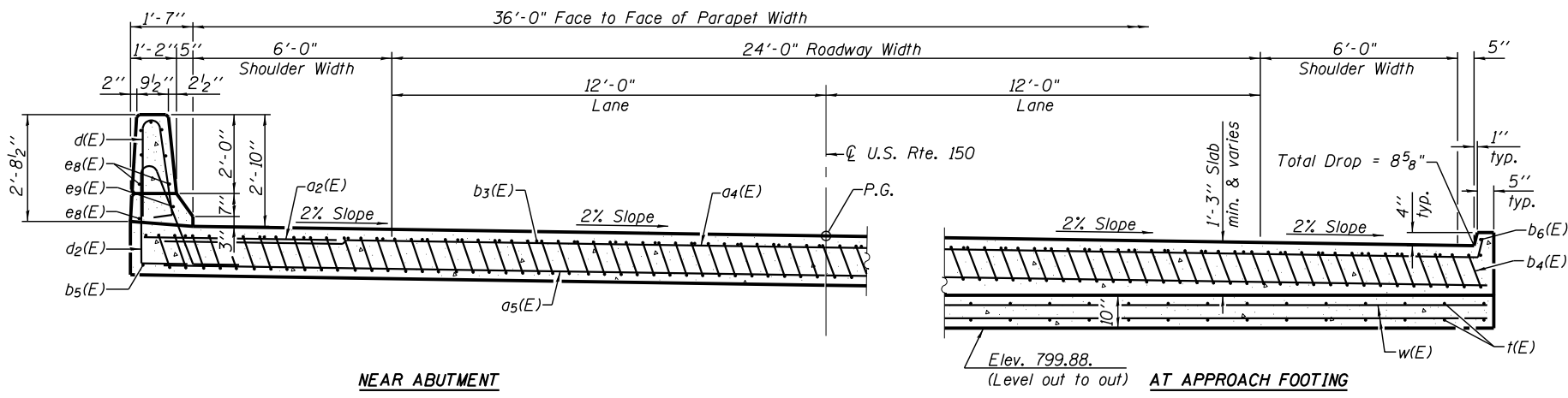
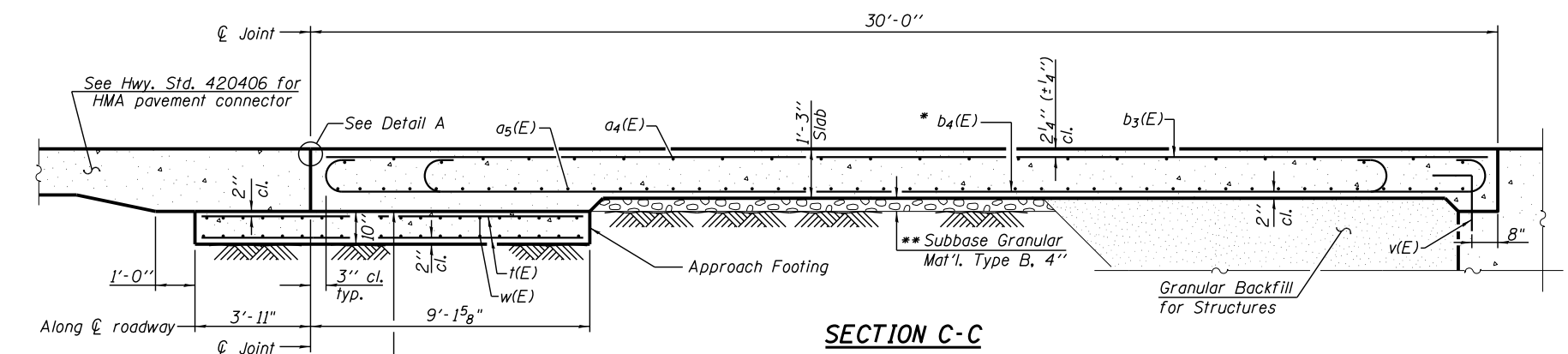
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**NORTH BRIDGE APPROACH SLAB DETAILS
 STRUCTURE NO. 048-0096**

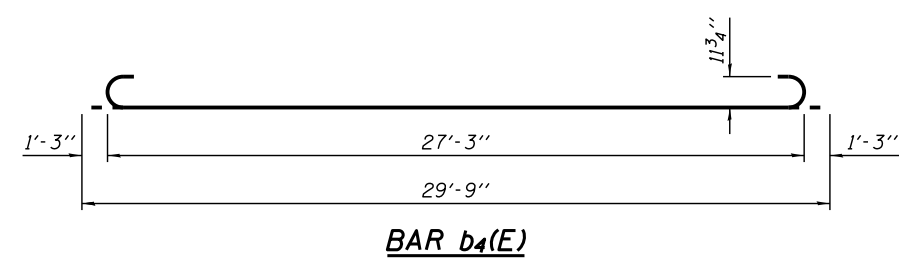
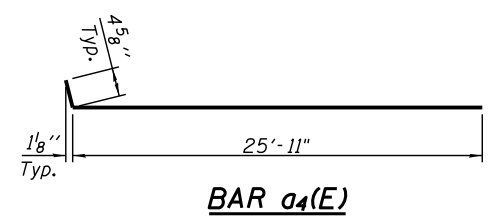
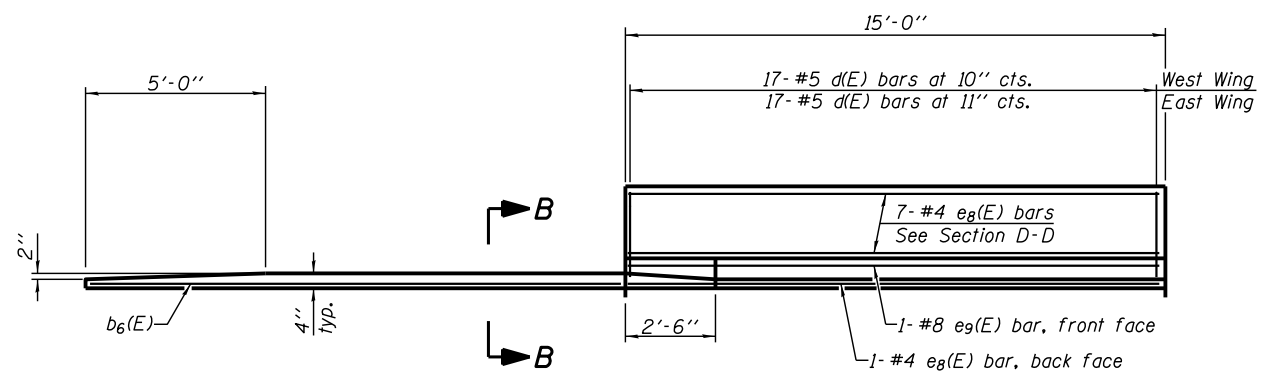
SHEET NO. 13 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	76
CONTRACT NO. 68800				
<small>ILLINOIS FED. AID PROJECT</small>				

Notes:
 See sheet 13 of 29 for Detail A and View B-B.
 Approach slab shall be paid for as Concrete Superstructure (Approach Slab).
 Parapet concrete shall be paid for as Concrete Superstructure.
 Approach footing concrete shall be paid for as Concrete Structures.
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
 For v(E) bar details, see sheet 10 of 29.
 The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
 Cost of excavation for approach footing included with Concrete Structures.
 For Granular Backfill for Structures and drainage treatment details, see sheet 2 of 29.
 For additional parapet details, see sheet 10 of 29.



* Tilt #9 b4(E) bars as required to maintain clearance.
 ** Cost included with Concrete Superstructure (Approach Slab).



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a2(E)	44	#6	6'-6"	—
a4(E)	92	#5	26'-4"	—
a5(E)	60	#8	49'-0"	—
b3(E)	57	#5	29'-8"	—
b4(E)	90	#9	29'-9"	—
b5(E)	2	#4	14'-8"	—
b6(E)	2	#4	14'-2"	—
d(E)	34	#5	5'-7"	—
d2(E)	34	#5	7'-11"	—
e8(E)	16	#4	14'-8"	—
e9(E)	2	#8	14'-8"	—
t(E)	76	#4	12'-8"	—
w(E)	40	#5	49'-0"	—
Concrete Superstructure			Cu. Yd.	3.3
Concrete Superstructure (Approach Slab)			Cu. Yd.	56.6
Concrete Structures			Cu. Yd.	15.3
Reinforcement Bars, Epoxy Coated			Pound	25120

(Sheet 2 of 2)



USER NAME =	DESIGNED - GBR	REVISED -
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PLOT DATE =	CHECKED - GBR	REVISIONS -

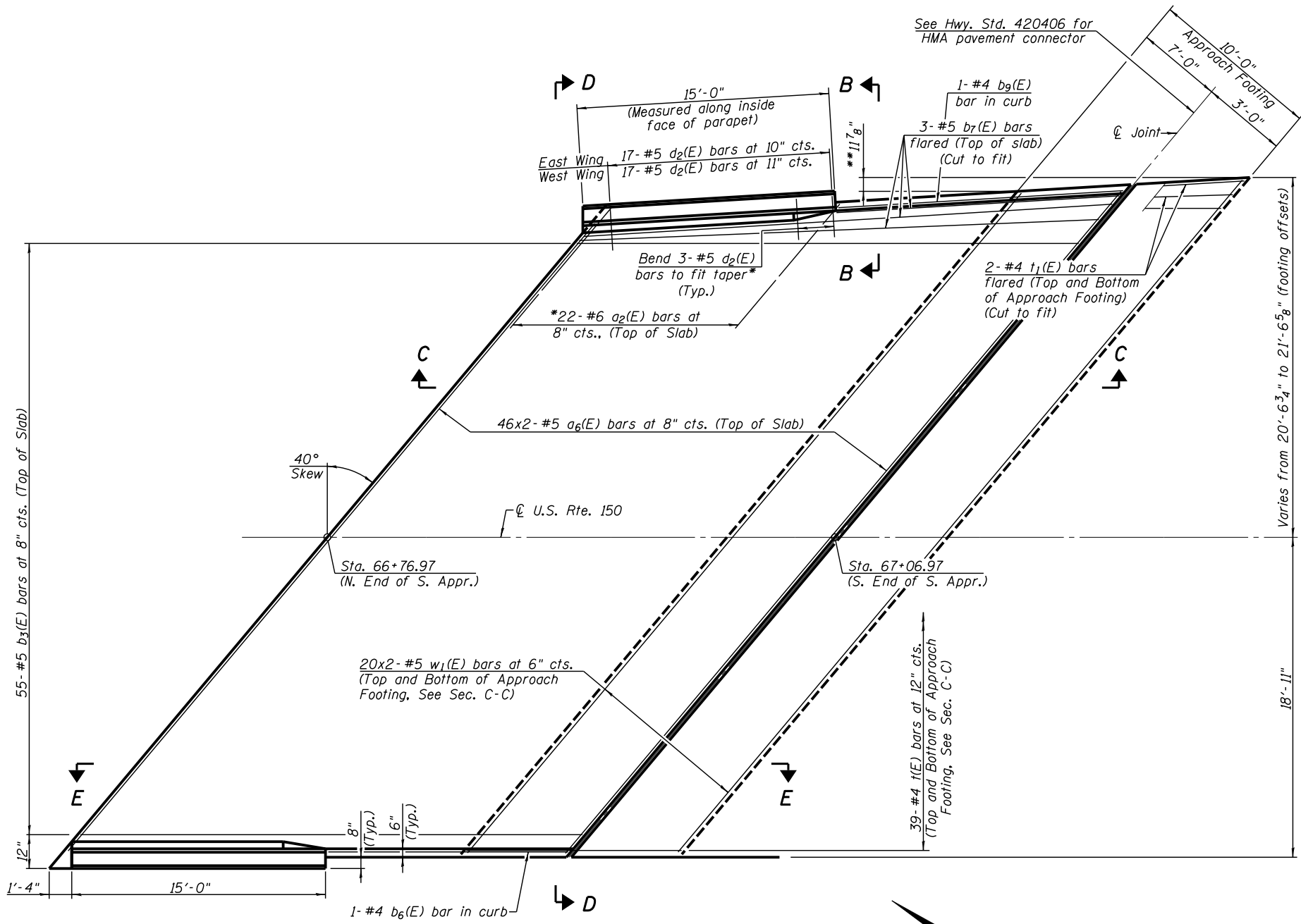
STATE OF ILLINOIS
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NORTH BRIDGE APPROACH SLAB DETAILS
 STRUCTURE NO. 048-0096

SHEET NO. 14 OF 29 SHEETS

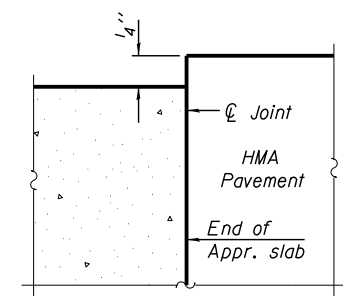
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	77
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

Notes:
 See sheet 17 of 29 for Sections C-C & D-D and View E-E.
 $a_6(E)$ bar spacings measured along C.R.
 Bars indicated 46x2-#5 etc. indicates 46 lines of bars with 2 lengths per line.

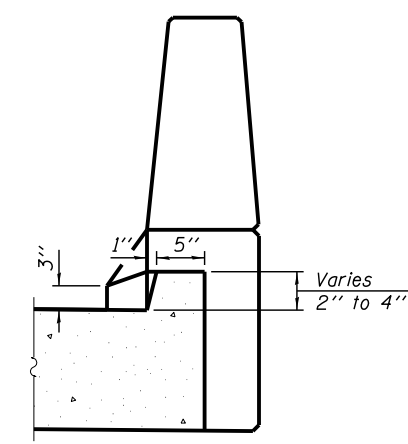


PLAN
 (Showing Top of Slab Reinforcement & Footing Reinforcement)
 * Space between $a_6(E)$ bars, typ. each parapet.
 ** Difference in offset values for NE & SE corners of footing

MINIMUM BAR LAP
 #5 bar = 3'-6"



FLEXIBLE PAVEMENT
DETAIL A



VIEW B-B

(Sheet 1 of 3)



USER NAME =	DESIGNED - GBR	REVISED -
	CHECKED - MAH	REVISED -
PLOT SCALE =	DRAWN - JRP	REVISED -
PLOT DATE =	CHECKED - GBR	REVISED -

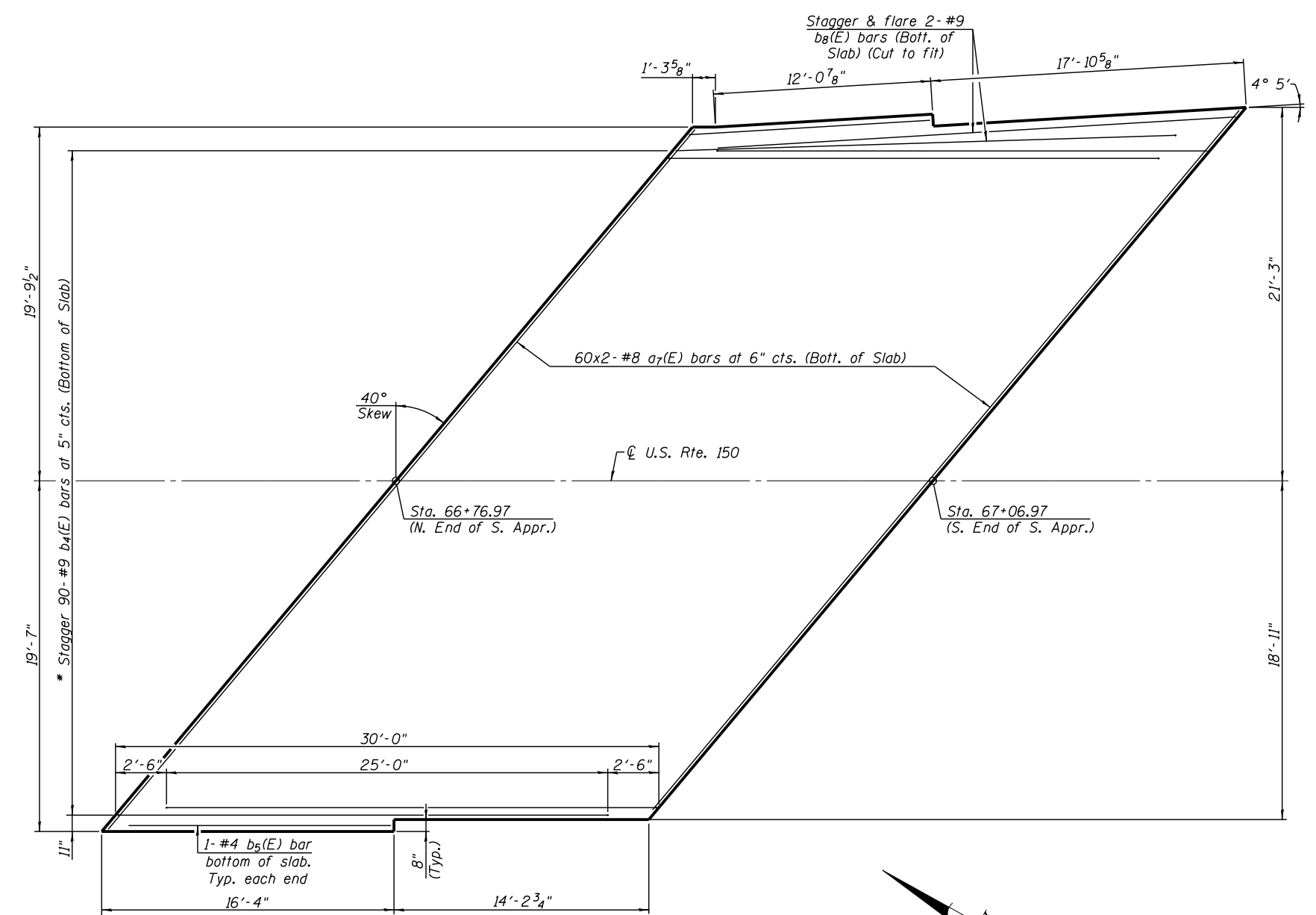
STATE OF ILLINOIS
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SOUTH BRIDGE APPROACH SLAB DETAILS
STRUCTURE NO. 048-0096

SHEET NO. 15 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	78
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

Notes:
 $a_7(E)$ bar spacings measured along \varnothing Rdwy.
 Bars indicated 60x2-#8 etc. indicates 60 lines of bars with 2 lengths per line.



PLAN
 (Showing Bottom of Slab Reinforcement)
 (Parapets & Footing not shown for clarity)
 * Tilt #9 $b_4(E)$ & $b_8(E)$ bars as required to maintain clearance.

MINIMUM BAR LAP
 #8 bar = 4'-9"

(Sheet 2 of 3)



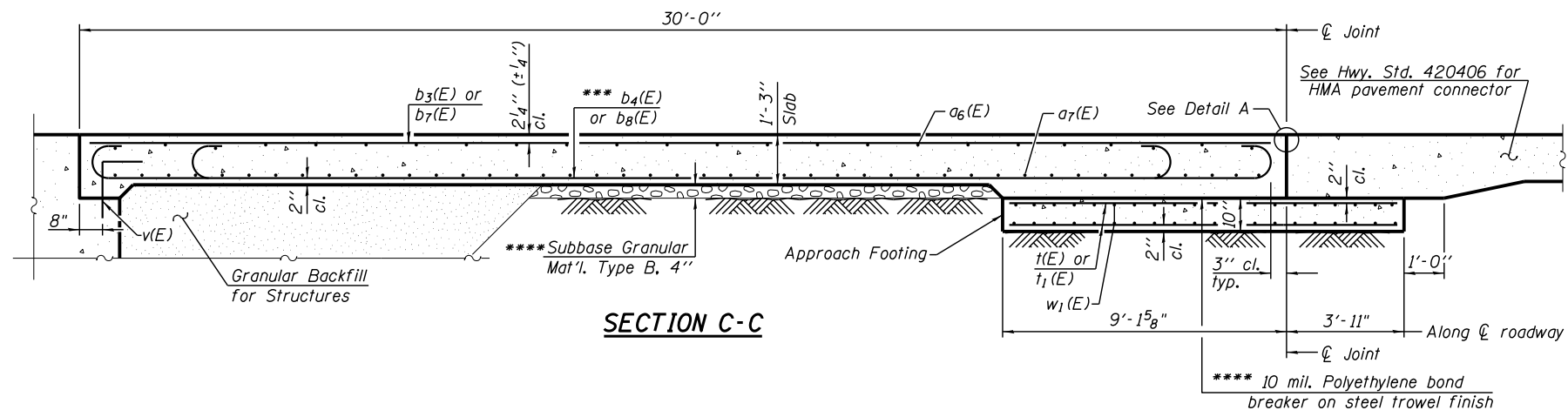
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	CHECKED - MAH	REVISED -
PLOT SCALE =	DRAWN - JRP	REVISED -
PLOT DATE =	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
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SOUTH BRIDGE APPROACH SLAB DETAILS
STRUCTURE NO. 048-0096

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	79
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68800	

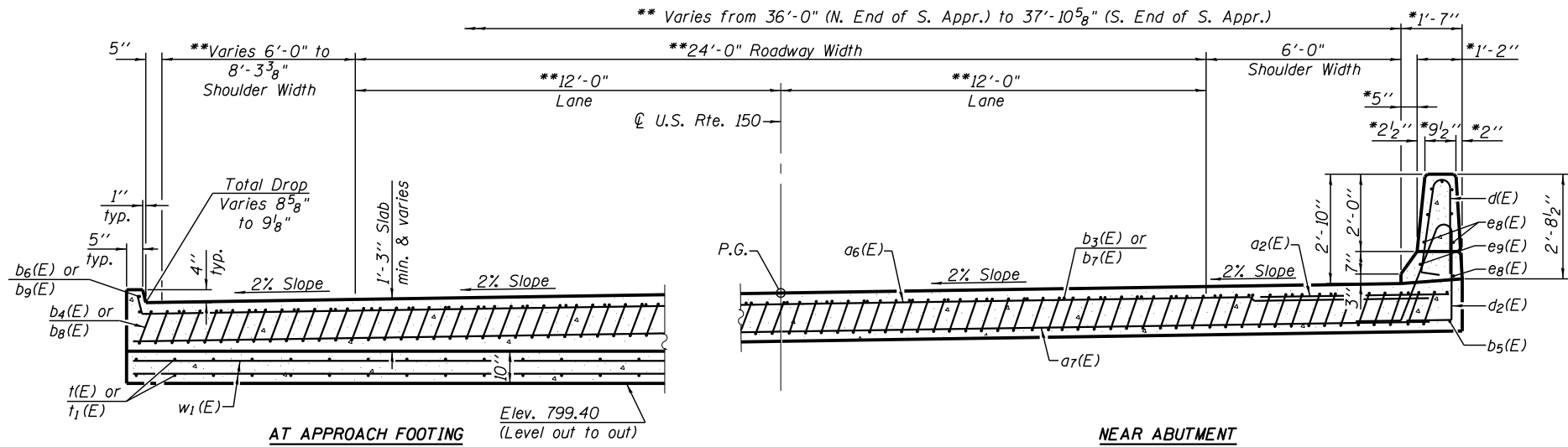
SHEET NO. 16 OF 29 SHEETS



SECTION C-C

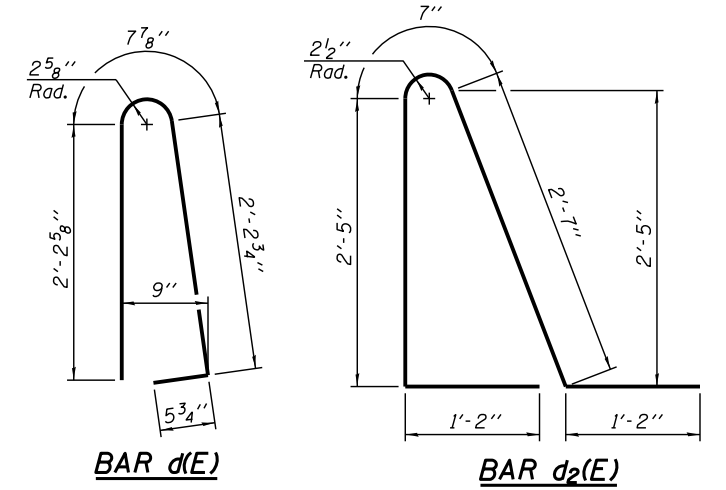
Notes:

See sheet 15 of 29 for Detail A and View B-B.
 Parapet concrete shall be paid for as Concrete Superstructure.
 Approach slab shall be paid for as Concrete Superstructure (Approach Slab).
 Approach footing concrete shall be paid for as Concrete Structures.
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
 For v(E) bar details, see sheet 10 of 29.
 The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
 Cost of excavation for approach footing included with Concrete Structures.
 For Granular Backfill for Structures and drainage treatment details, see sheet 2 of 29.
 For additional parapet details, see sheet 10 of 29.



SECTION D-D

(See Plan for dimensions not shown)



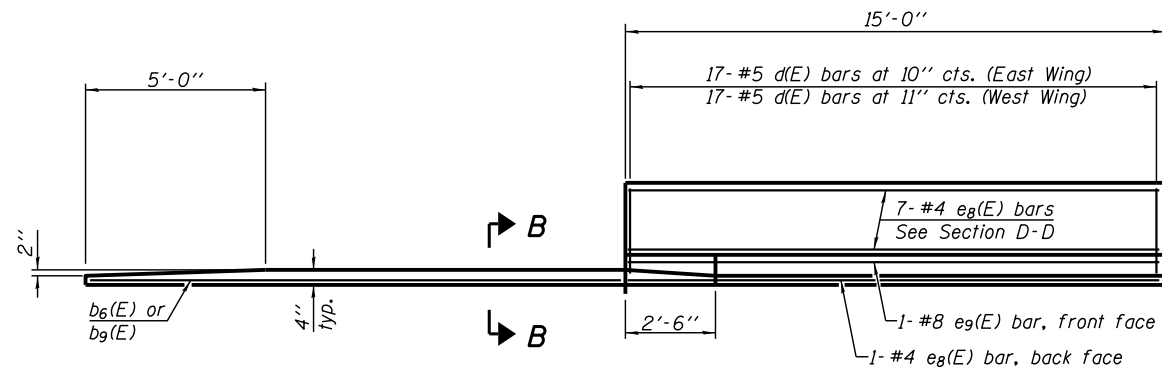
BAR d(E)

BAR d2(E)

AT APPROACH FOOTING

NEAR ABUTMENT

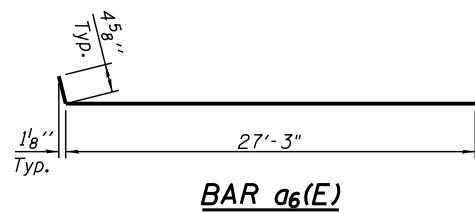
- * Dimension shown at Rt. L's to inside face of parapet
- ** Dimension shown at Rt. L's to \bar{C} U.S. Rte. 150
- *** Tilt #9 b4(E) or b8(E) bars as required to maintain clearance.
- **** Cost included with Concrete Superstructure (Approach Slab).



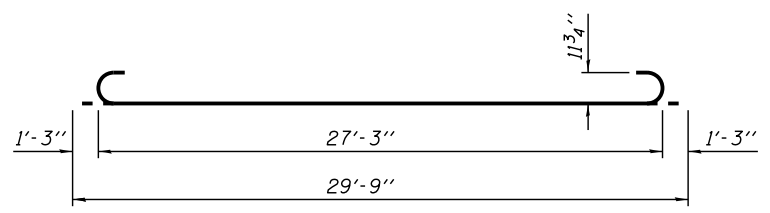
VIEW E-E

BILL OF MATERIAL

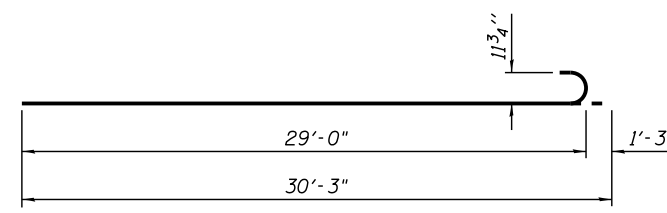
Bar	No.	Size	Length	Shape	
a2(E)	44	#6	6'-6"	—	
a6(E)	92	#5	27'-8"	—	
a7(E)	120	#8	28'-5"	—	
b3(E)	55	#5	29'-8"	—	
b4(E)	90	#9	29'-9"	—	
b5(E)	2	#4	14'-8"	—	
b6(E)	1	#4	14'-2"	—	
b7(E)	3	#5	31'-2"	—	
b8(E)	2	#9	30'-3"	—	
b9(E)	1	#4	16'-11"	—	
d(E)	34	#5	5'-7"	—	
d2(E)	34	#5	7'-11"	—	
e8(E)	16	#4	14'-8"	—	
e9(E)	2	#8	14'-8"	—	
t(E)	78	#4	12'-8"	—	
t1(E)	4	#4	13'-5"	—	
w1(E)	80	#5	28'-0"	—	
Concrete Superstructure				Cu. Yd.	3.3
Concrete Superstructure (Approach Slab)				Cu. Yd.	64.7
Concrete Structures				Cu. Yd.	16.1
Reinforcement Bars, Epoxy Coated				Pound	27090



BAR a6(E)



BAR b4(E)



BAR b8(E)

(Sheet 3 of 3)



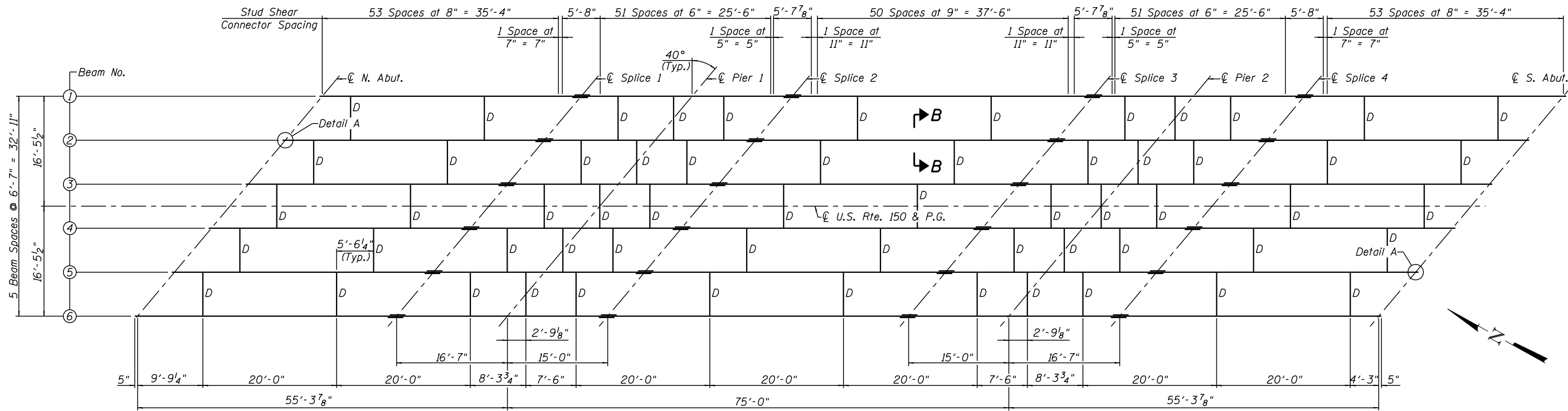
USER NAME =	DESIGNED - GBR	REVISED -
PLOT SCALE =	CHECKED - MAH	REVISED -
PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SOUTH BRIDGE APPROACH SLAB DETAILS
STRUCTURE NO. 048-0096**

SHEET NO. 17 OF 29 SHEETS

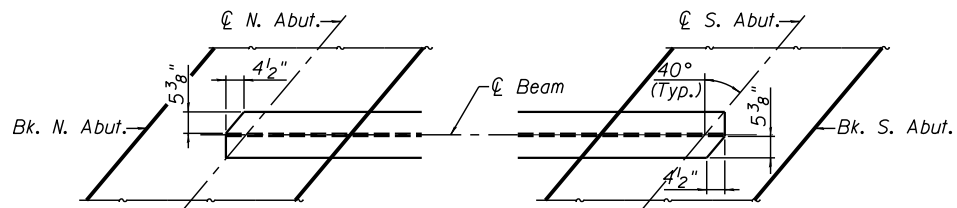
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	80
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



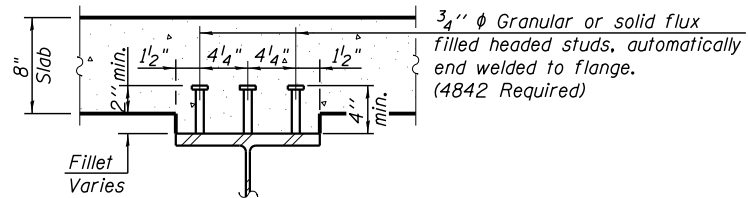
FRAMING PLAN

All beams are W33x141, AASHTO M 270, Grade 50, NTR
 Note: Load carrying components designated "NTR" shall conform to the Impact Testing Requirement, Zone 2. All new structural steel shall be hot-dip galvanized. See Special Provision for "Hot-Dip Galvanizing for Structural Steel".

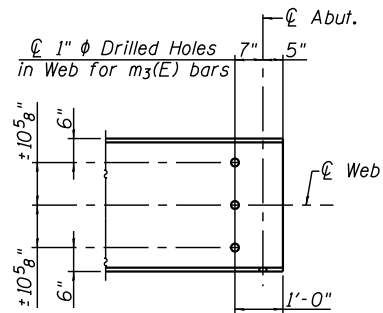
In order to facilitate shipping and/or erection, the Contractor has the option of either: (a) replacing one or more bolted field splices with shop-welded splices (after galvanizing), or (b) fully-torquing one or more bolted splices in the shop.



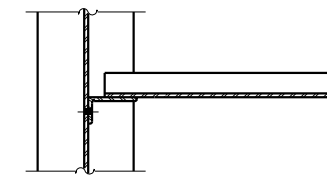
DETAIL A
(Top Flange Clip Detail)



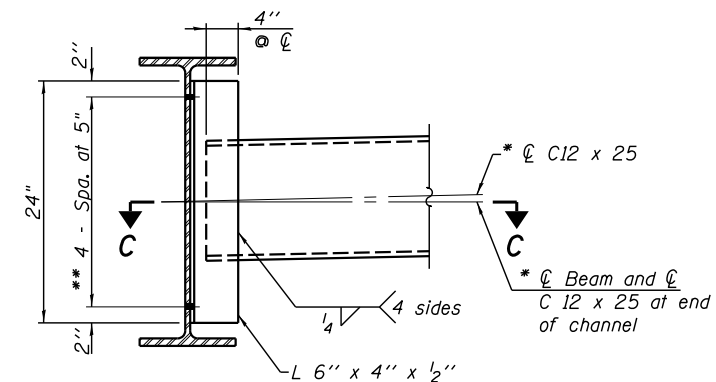
SECTION B-B



TYP. END OF BEAM ELEVATION



SECTION C-C



INTERIOR DIAPHRAGM D
(60 Required)

***** TOP OF BEAM ELEVATIONS**

	CL N. ABUT.	CL SPLICE 1	CL PIER 1	CL SPLICE 2	CL SPLICE 3	CL PIER 2	CL SPLICE 4	CL S. ABUT.
BEAM 1	801.64	801.82	801.87	801.92	801.85	801.77	801.68	801.39
BEAM 2	801.72	801.93	801.99	802.04	802.01	801.93	801.85	801.58
BEAM 3	801.80	802.03	802.10	802.16	802.15	802.09	802.02	801.77
BEAM 4	801.87	802.13	802.21	802.28	802.30	802.24	802.18	801.96
BEAM 5	801.94	802.22	802.31	802.39	802.44	802.39	802.34	802.15
BEAM 6	802.01	802.31	802.41	802.50	802.58	802.54	802.50	802.33

*** For Fabrication Only

Note:

All diaphragms shall be installed as steel is erected and secured with erection pins and bolts except as otherwise noted. Individual diaphragms at supports may be temporarily disconnected to install bearing anchor rods.

Two hardened washers required for each set of oversized holes. *Alternate channels C 12 x 30 are permitted to facilitate material acquisition. Calculated weight of structural steel is based on the lighter section.

The alternate, if utilized, shall be provided at no additional cost to the Department.

*** 3/4" φ HS bolts, 1 5/16" φ holes.

INTERIOR BEAM MOMENT TABLE				
		0.4 Sp. 1 0.6 Sp. 3	Pier 1 or 2	0.5 Sp. 2
I_s	(in ⁴)	7450	7450	7450
$I_c(n)$	(in ⁴)	19485	19485	19485
$I_c(3n)$	(in ⁴)	14464	14464	14464
$I_c(cr)$	(in ⁴)	-	9897	-
S_s	(in ³)	448	448	448
$S_c(n)$	(in ³)	646	646	646
$S_c(3n)$	(in ³)	587	587	587
$S_c(cr)$	(in ³)	-	507	-
DC1	(k/')	0.840	0.840	0.840
MDC1	('k)	160.6	-369.8	220.7
DC2	(k/')	0.150	0.150	0.150
MDC2	('k)	28.7	-66.1	39.4
DW	(k/')	0.330	0.330	0.330
MDW	('k)	63.1	-145.3	86.7
LLDF		0.610	0.592	0.578
$M_L + IM$	('k)	600.2	-626.2	645.5
M_u (Strength I)	('k)	1382	-1859	1585
$\phi_r M_n$	('k)	3366	-	3313
f_s DC1	(ksi)	4.3	-9.9	5.9
f_s DC2	(ksi)	0.6	-1.4	0.8
f_s DW	(ksi)	1.3	-3.0	1.8
f_s ($\frac{1}{4} + IM$)	(ksi)	11.1	-11.6	12.0
f_s (Service II)	(ksi)	20.6	-29.4	24.1
$0.95R_h F_y f$	(ksi)	47.5	47.5	47.5
f_s (Total)(Strength I)	(ksi)	-	-45.5	-
$\phi_r F_n$	(ksi)	-	50.0	-
Vr	(k)	23.3	27.0	27.0

BEAM REACTION TABLE				
	Abutments		Piers	
	Interior	Exterior	Interior	Exterior
LLDF	0.713	0.538	0.713	0.538
OCF		1.17		
RDC1 (k)	16.6	17.3	61.4	64.3
RDC2 (k)	3.0	3.0	11.0	11.0
RDW (k)	5.9	5.9	21.9	21.9
R _L (k)	52.5	46.3	84.8	64.0
R _{IM} (k)	13.5	11.9	16.4	12.4
R _{Total} (k)	91.5	84.4	195.5	173.6

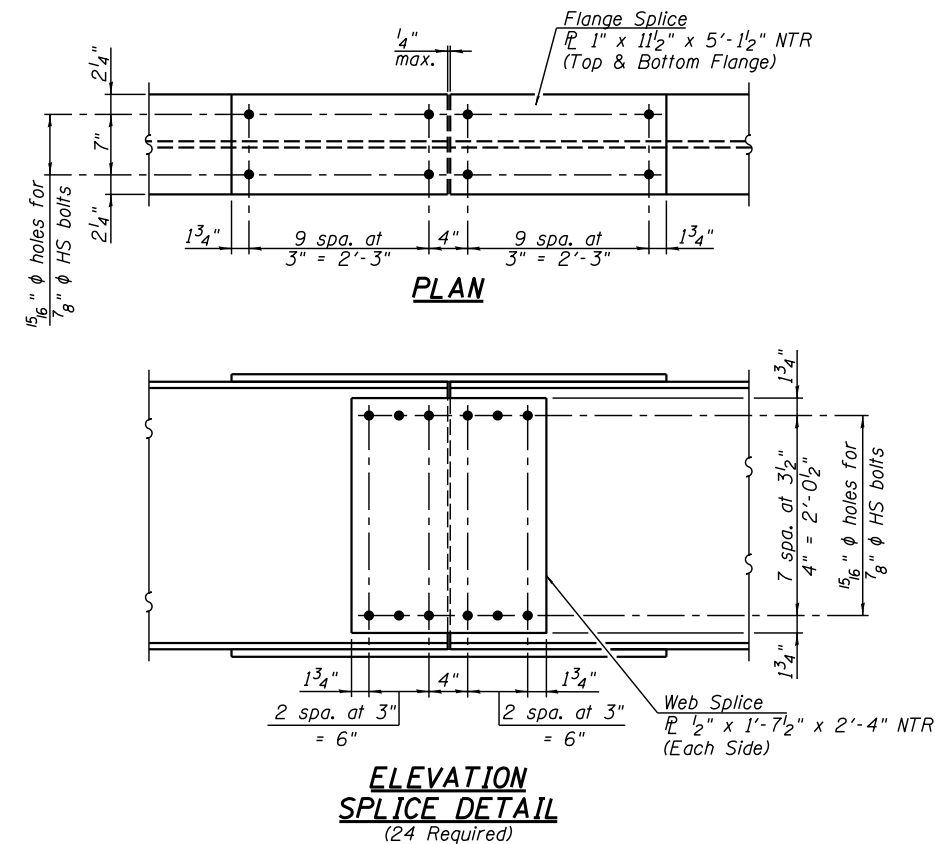
I_s, S_s : Non-composite moment of inertia and section modulus of the steel section used for computing f_s (Total-Strength I, and Service II) due to non-composite dead loads (in⁴ and in³).

$I_c(n), S_c(n)$: Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing f_s (Total-Strength I, and Service II) in uncracked sections due to short-term composite live loads (in⁴ and in³).

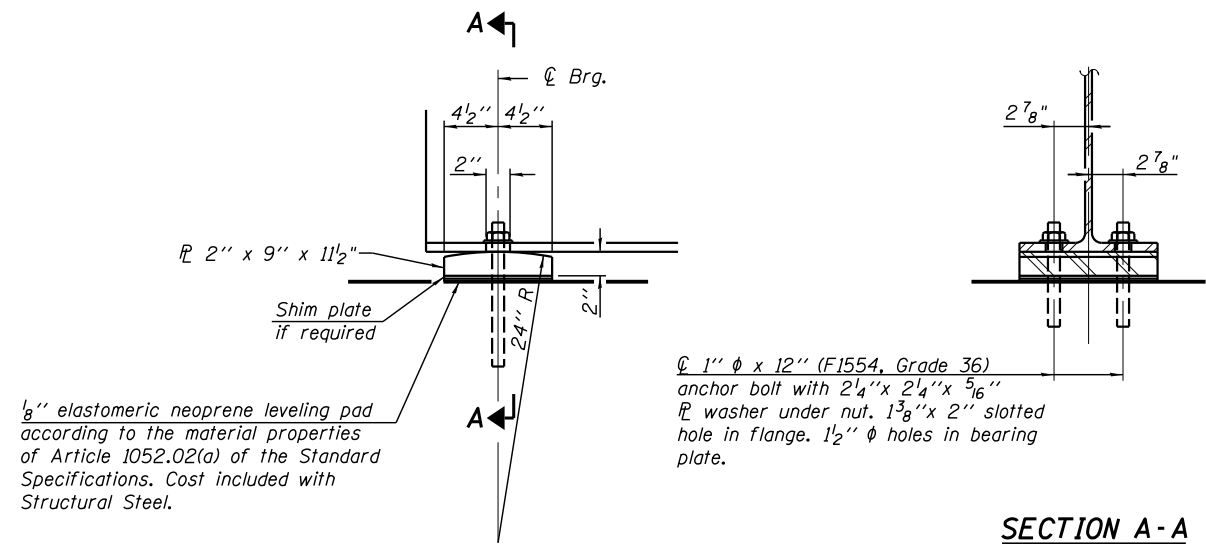
$I_c(3n), S_c(3n)$: Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing f_s (Total-Strength I, and Service II) in uncracked sections, due to long-term composite (superimposed) dead loads (in⁴ and in³).

$I_c(cr), S_c(cr)$: Composite moment of inertia and section modulus of the steel and longitudinal deck reinforcement, used for computing f_s (Total-Strength I and Service II) in cracked sections, due to both short-term composite live loads and long-term composite (superimposed) dead loads (in⁴ and in³).

DC1: Un-factored non-composite dead load (kips/ft.).
MDC1: Un-factored moment due to non-composite dead load (kip-ft.).
DC2: Un-factored long-term composite (superimposed excluding future wearing surface) dead load (kips/ft.).
MDC2: Un-factored moment due to long-term composite (superimposed excluding future wearing surface) dead load (kip-ft.).
DW: Un-factored long-term composite (superimposed future wearing surface only) dead load (kips/ft.).
MDW: Un-factored moment due to long-term composite (superimposed future wearing surface only) dead load (kip-ft.).
 $M_L + IM$: Un-factored live load moment plus dynamic load allowance (impact) (kip-ft.).
 M_u (Strength I): Factored design moment (kip-ft.).
 $1.25 (M_{DC1} + M_{DC2}) + 1.5 M_{DW} + 1.75 M_L + IM$
 $\phi_r M_n$: Compact composite positive moment capacity computed according to Article 6.10.7.1 or non-slender negative moment capacity according to Article A6.1.1 or A6.1.2 (kip-ft.).
 f_s DC1: Un-factored stress at edge of flange for controlling steel flange due to vertical non-composite dead loads as calculated below (ksi).
 M_{DC1} / S_{nc}
 f_s DC2: Un-factored stress at edge of flange for controlling steel flange due to vertical composite dead loads as calculated below (ksi).
 $M_{DC2} / S_c(3n)$ or $M_{DC2} / S_c(cr)$ as applicable.
 f_s DW: Un-factored stress at edge of flange for controlling steel flange due to vertical composite future wearing surface loads as calculated below (ksi).
 $M_{DW} / S_c(3n)$ or $M_{DW} / S_c(cr)$ as applicable.
 f_s ($\frac{1}{4} + IM$): Un-factored stress at edge of flange for controlling steel flange due to vertical composite live load plus impact loads as calculated below (ksi).
 $M_L + IM / S_c(n)$ or $M_{LL+I} / S_c(cr)$ as applicable.
 f_s (Service II): Sum of stresses as computed below (ksi).
 $f_{sDC1} + f_{sDC2} + f_{sDW} + 1.3 f_s(\frac{1}{4} + IM)$
 $0.95R_h F_y f$: Composite stress capacity for Service II loading according to Article 6.10.4.2 (ksi).
 f_s (Total)(Strength I): Sum of stresses as computed below on non-compact section (ksi).
 $1.25 (f_{sDC1} + f_{sDC2}) + 1.5 f_{sDW} + 1.75 f_s(\frac{1}{4} + IM)$
 $\phi_r F_n$: Non-Compact composite positive or negative stress capacity for Strength I loading according to Article 6.10.7 or 6.10.8 (ksi).
Vr: Maximum factored shear range in span computed according to Article 6.10.10.
LLDF: Live Load Distribution Factor
OCF: Obtuse Correction Factor

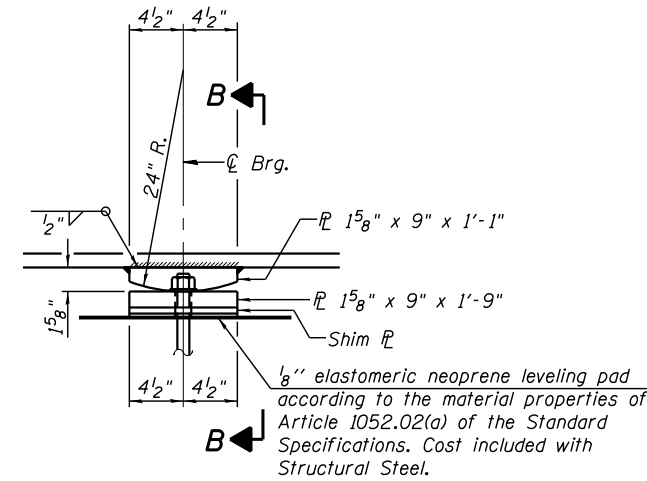


All splice plates shall be AASHTO M 270, Grade 50, NTR Load carrying components designated "NTR" shall conform to the Impact Testing Requirement, Zone 2.
All new structural steel shall be hot-dip galvanized.
See Special Provision for "Hot-Dip Galvanizing for Structural Steel".



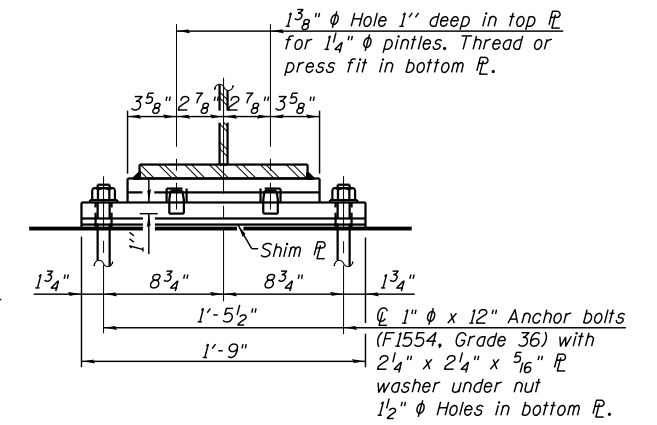
ELEVATION AT ABUTMENT

FIXED BEARING AT ABUTMENTS
(12 Required)

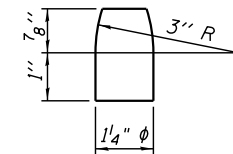


ELEVATION AT PIER

FIXED BEARING AT PIERS
(12 Required)



SECTION B-B



PINTLE

Notes:

Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

The structural steel plates of the bearings shall conform to the requirements of AASHTO M 270, Grade 50.

All bearing plates, anchor bolts, nuts, washers and pintles shall be galvanized according to AASHTO M 111 or M 232 as applicable.

Beams shall be braced for stability during erection and remain braced until deck is poured and cured.

Anchor bolts at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.

BILL OF MATERIAL

Item	Unit	Total
Anchor Bolts, 1"	Each	48

I-2E-1

1-27-12



USER NAME =	DESIGNED - GBR	REVISED -
	CHECKED - MAH	REVISED -
PLOT SCALE =	DRAWN - JRP	REVISED -
PLOT DATE =	CHECKED - GBR	REVISED -

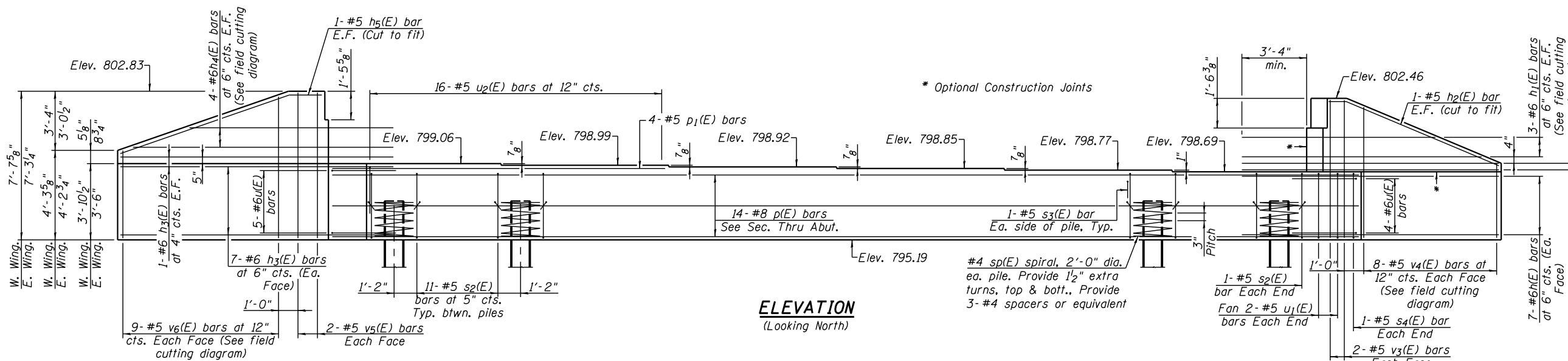
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BEARING DETAILS
STRUCTURE NO. 048-0096

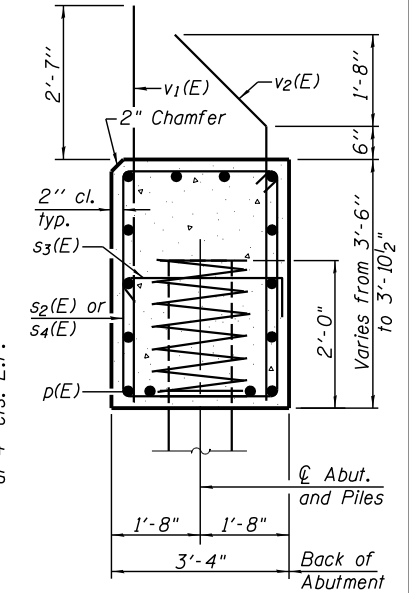
SHEET NO. 20 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	83
			CONTRACT NO. 68800	
ILLINOIS FED. AID PROJECT				

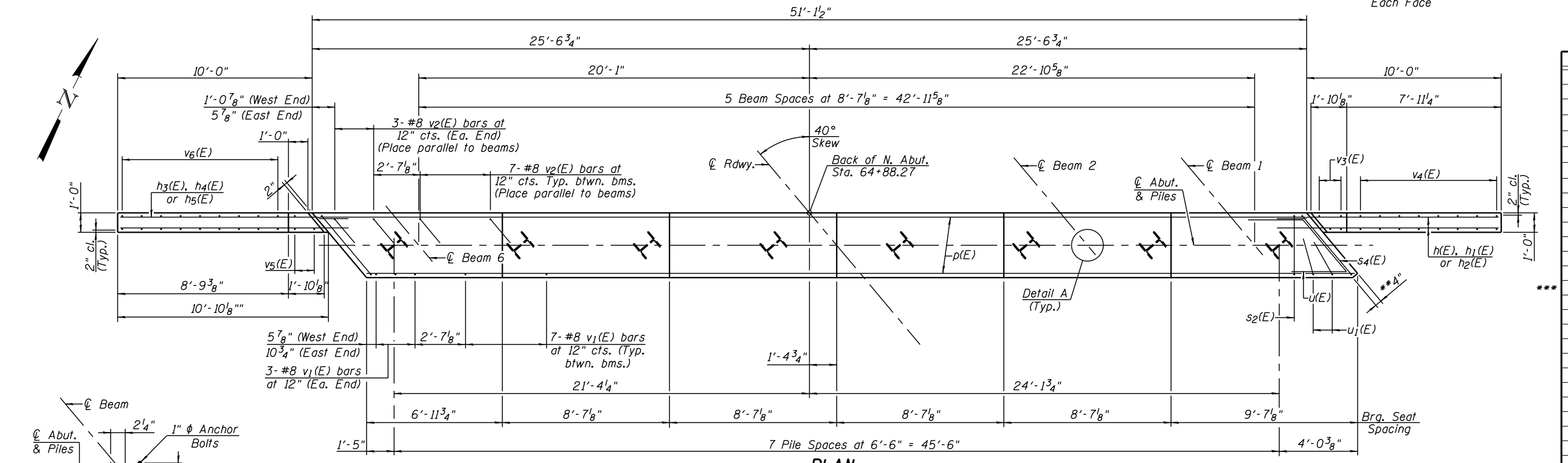
Notes:
Space reinforcement in cap to miss anchor bolts.
Four steps monolithically with cap.



ELEVATION
(Looking North)



SEC. THRU ABUT.
Dimensions at right angles to abutment.

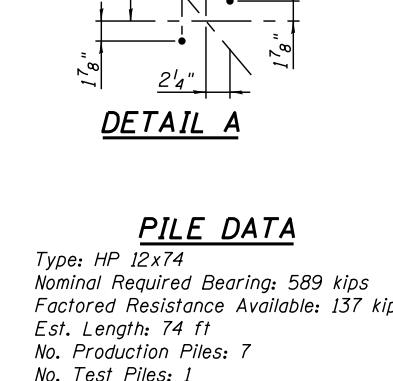


PLAN

BILL OF MATERIAL

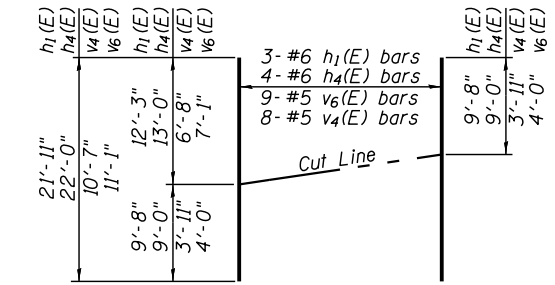
Bar	No.	Size	Length	Shape
h(E)	18	#6	13'-2"	—
h1(E)	3	#6	21'-11"	—
h2(E)	2	#5	9'-7"	—
h3(E)	16	#6	14'-0"	—
h4(E)	4	#6	22'-0"	—
h5(E)	2	#5	10'-7"	—
p(E)	14	#8	50'-7"	—
p1(E)	4	#5	15'-4"	—
s2(E)	79	#5	13'-3"	□
s3(E)	16	#5	4'-0"	□
s4(E)	2	#5	15'-1"	□
sp(E)	8	#4	2'-0"	WWM
u(E)	9	#6	11'-5"	—
u1(E)	4	#5	8'-2"	—
u2(E)	16	#5	8'-0"	—
v1(E)	41	#8	5'-11"	—
v2(E)	41	#8	6'-2"	—
v3(E)	4	#5	6'-11"	—
v4(E)	8	#5	10'-7"	—
v5(E)	4	#5	7'-4"	—
v6(E)	9	#5	11'-1"	—
Structure Excavation		Cu. Yd.	20.0	
Concrete Structures		Cu. Yd.	28.0	
Reinforcement Bars, Epoxy Coated		Pound	6380	
Furnishing Steel Piles HP 12x74		Foot	518	
Driving Piles		Foot	518	
Test Pile Steel HP 12x74		Each	1	

*** Length is height of spiral.
For details of piles see sheet 25 of 29.



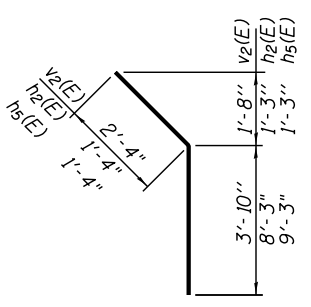
DETAIL A

PILE DATA
Type: HP 12x74
Nominal Required Bearing: 589 kips
Factored Resistance Available: 137 kips
Est. Length: 74 ft
No. Production Piles: 7
No. Test Piles: 1

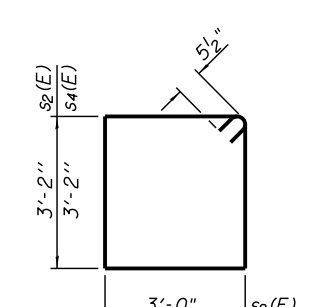


FIELD CUTTING DIAGRAM

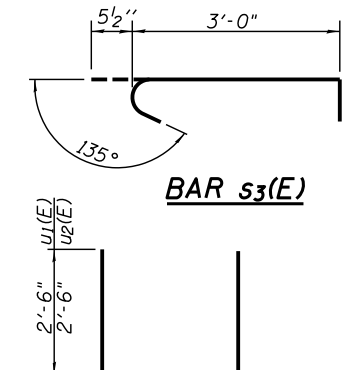
Order h1(E), h4(E), v4(E) and v6(E) full length. Cut as shown and use remainder of bars in opposite face.



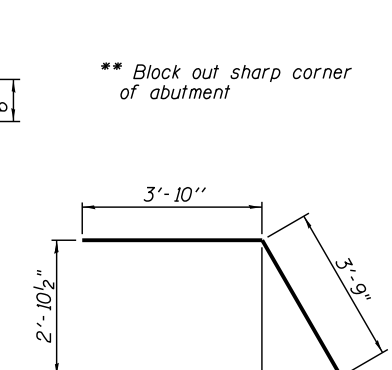
BAR v2(E), h2(E) & h5(E)



BAR s2(E) & s4(E)



BAR u1(E) & u2(E)



BAR u(E)



USER NAME =	DESIGNED - GBR	REVISED -
PLOT SCALE =	CHECKED - MAH	REVISED -
PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

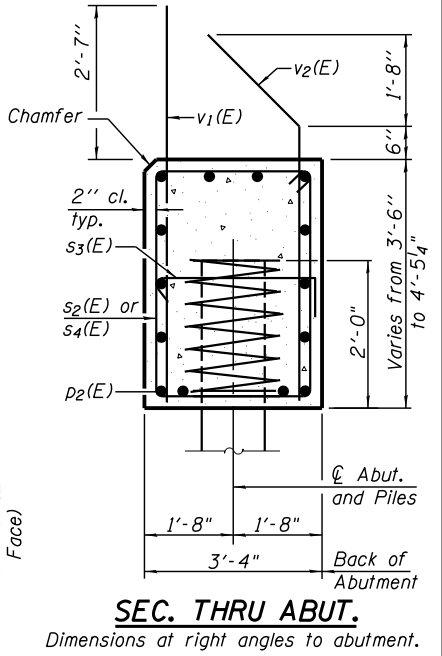
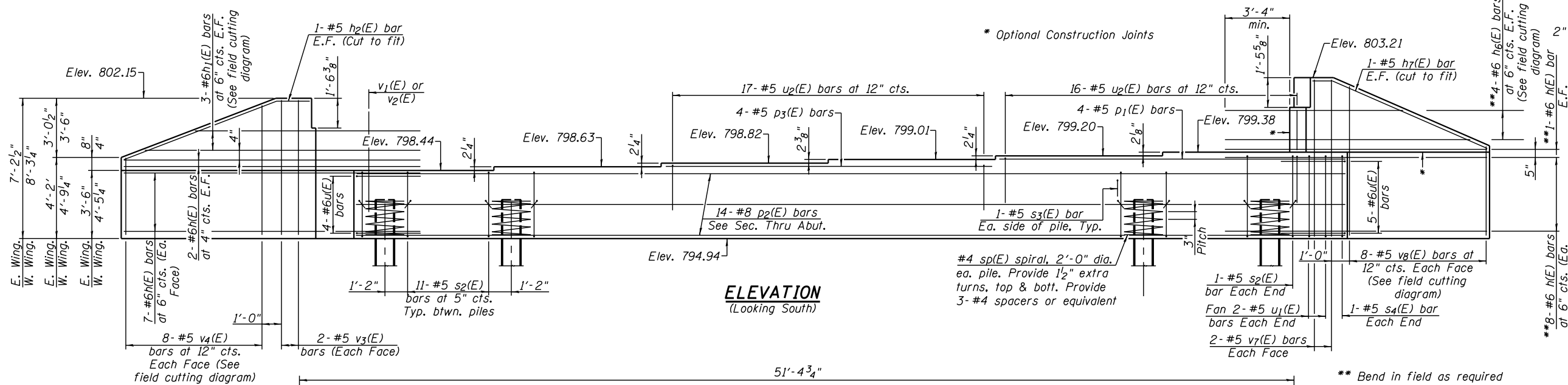
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH ABUTMENT
STRUCTURE NO. 048-0096

SHEET NO. 21 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	84
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

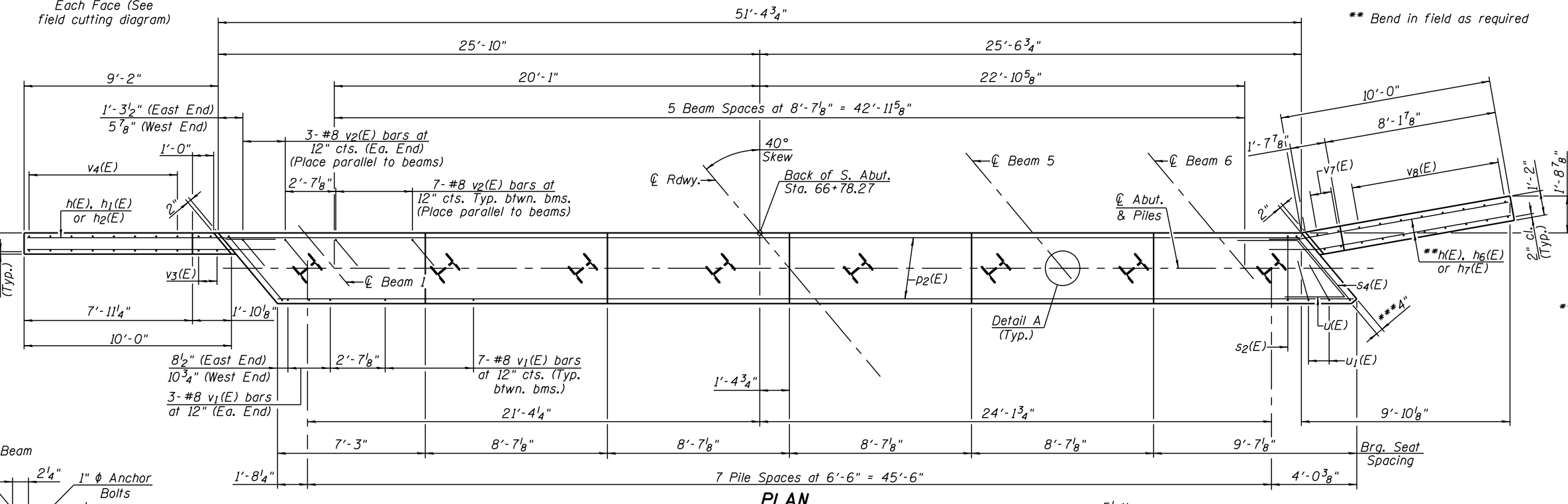
Notes:
 Pour steps monolithically with cap.
 Space reinforcement in cap to miss anchor bolts.



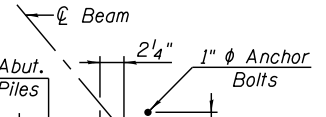
ELEVATION
 (Looking South)

SEC. THRU ABUT.

Dimensions at right angles to abutment.



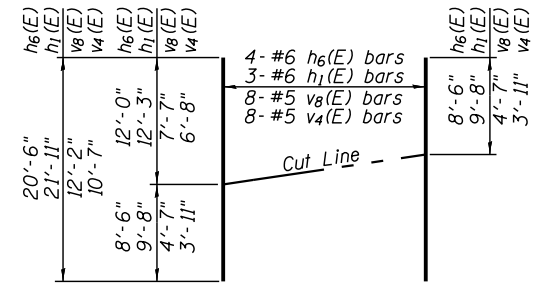
PLAN



DETAIL A

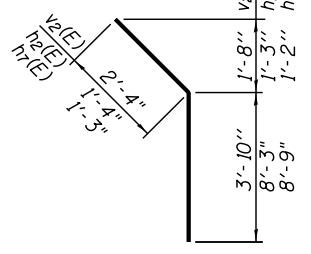
PILE DATA

Type: HP 12x74
 Nominal Required Bearing: 589 kips
 Factored Resistance Available: 138 kips
 Est. Length: 71 ft
 No. Production Piles: 7
 No. Test Piles: 1

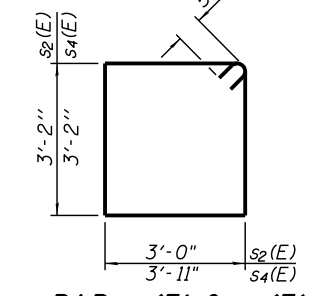


FIELD CUTTING DIAGRAM

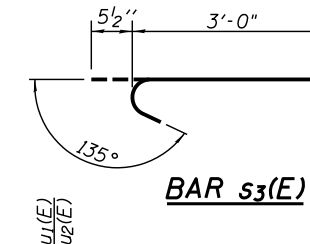
Order h6(E), h1(E), v8(E) and v4(E) full length. Cut as shown and use remainder of bars in opposite face.



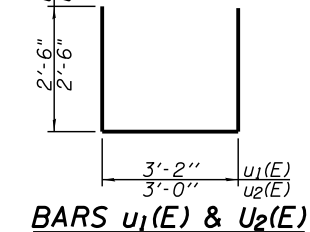
BAR v2(E), h2(E) & h7(E)



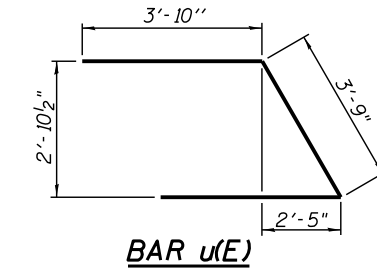
BAR s2(E) & s4(E)



BAR s3(E)



BARS u1(E) & u2(E)



BAR u(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	36	#6	13'-2"	—
h1(E)	3	#6	21'-11"	—
h2(E)	2	#5	9'-7"	—
h6(E)	4	#6	20'-6"	—
h7(E)	2	#5	10'-0"	—
p1(E)	4	#5	15'-4"	—
p2(E)	14	#8	50'-10"	—
p3(E)	4	#5	16'-10"	—
s2(E)	79	#5	13'-3"	□
s3(E)	16	#5	4'-0"	□
s4(E)	2	#5	15'-1"	□
sp(E)	8	#4	2'-0"	≡≡≡
u(E)	9	#6	11'-5"	—
u1(E)	4	#5	8'-2"	—
u2(E)	33	#5	8'-0"	—
v1(E)	41	#8	5'-11"	—
v2(E)	41	#8	6'-2"	—
v3(E)	4	#5	6'-11"	—
v4(E)	8	#5	10'-7"	—
v7(E)	4	#5	7'-11"	—
v8(E)	8	#5	12'-2"	—
Structure Excavation	Cu. Yd.	73		
Concrete Structures	Cu. Yd.	30.3		
Reinforcement Bars, Epoxy Coated	Pound	6610		
Furnishing Steel Piles				
HP 12x74	Foot	497		
Driving Piles	Foot	497		
Test Pile Steel HP 12x74	Each	1		

**** Length is height of spiral.
 For details of piles see sheet 25 of 29.

AI-2440S-L 8-31-12



USER NAME =	DESIGNED - GBR	REVISIONS -
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PLOT DATE =	DRAWN - JRP	REVISIONS -
	CHECKED - GBR	REVISIONS -

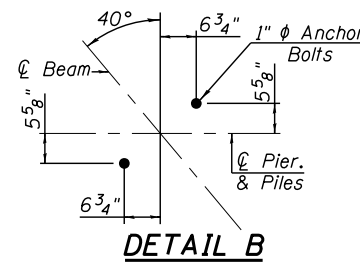
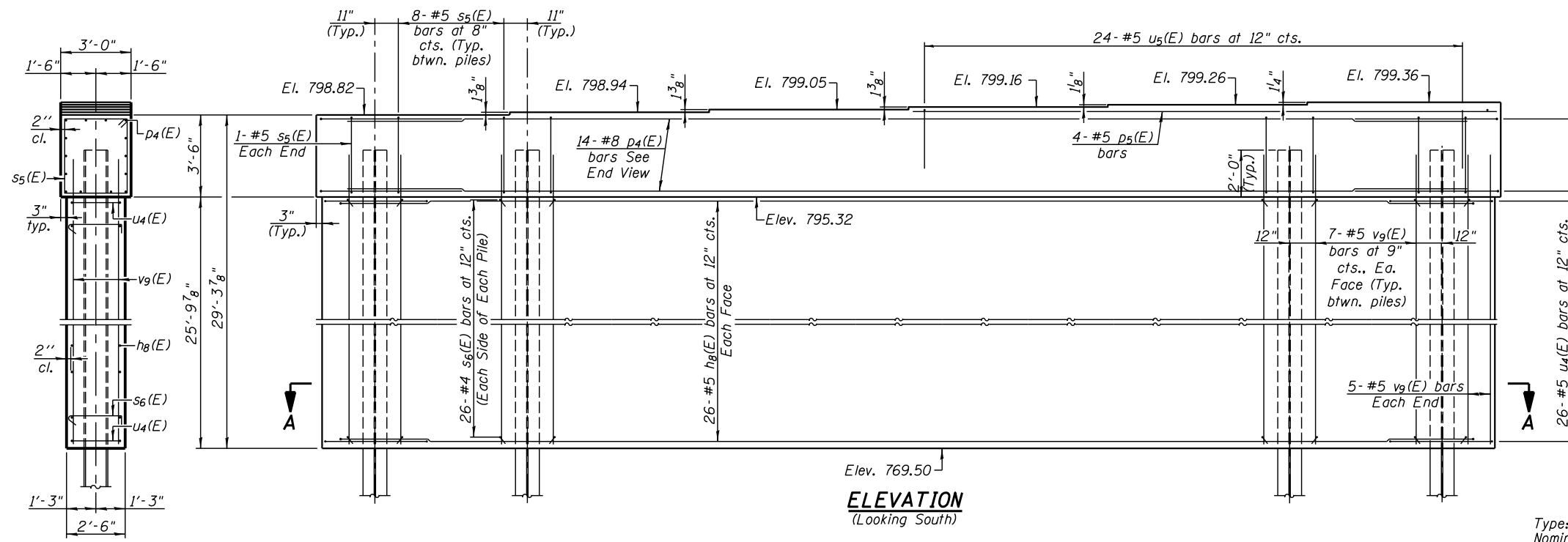
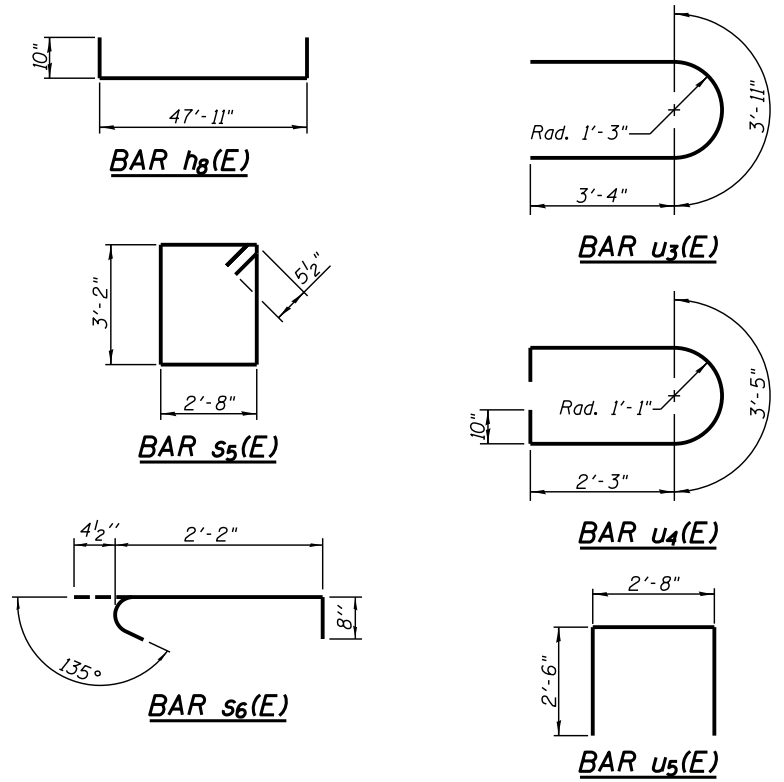
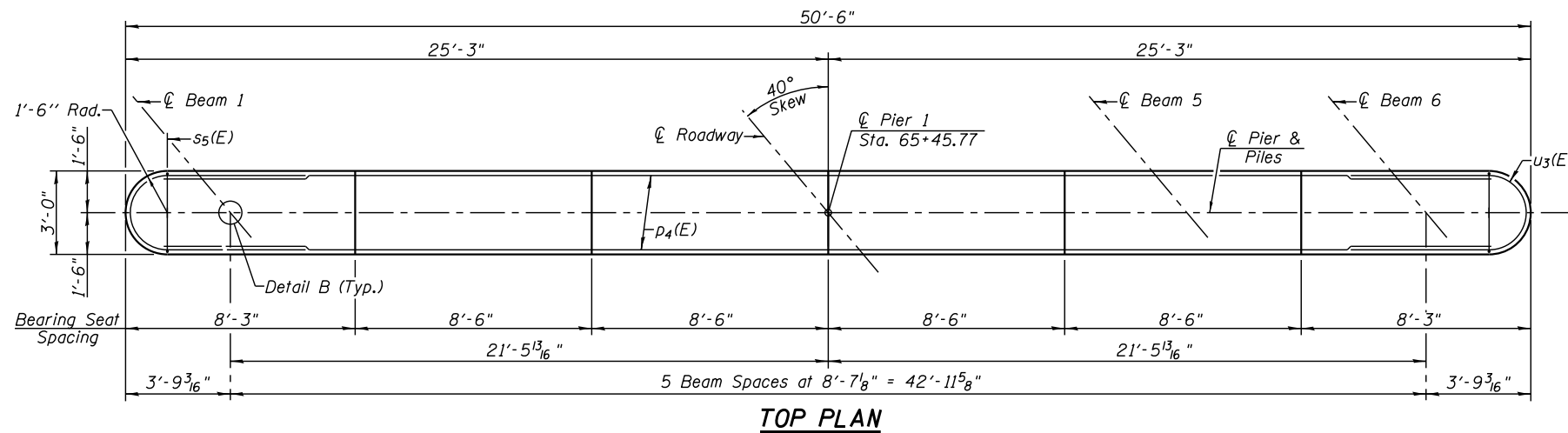
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SOUTH ABUTMENT
 STRUCTURE NO. 048-0096

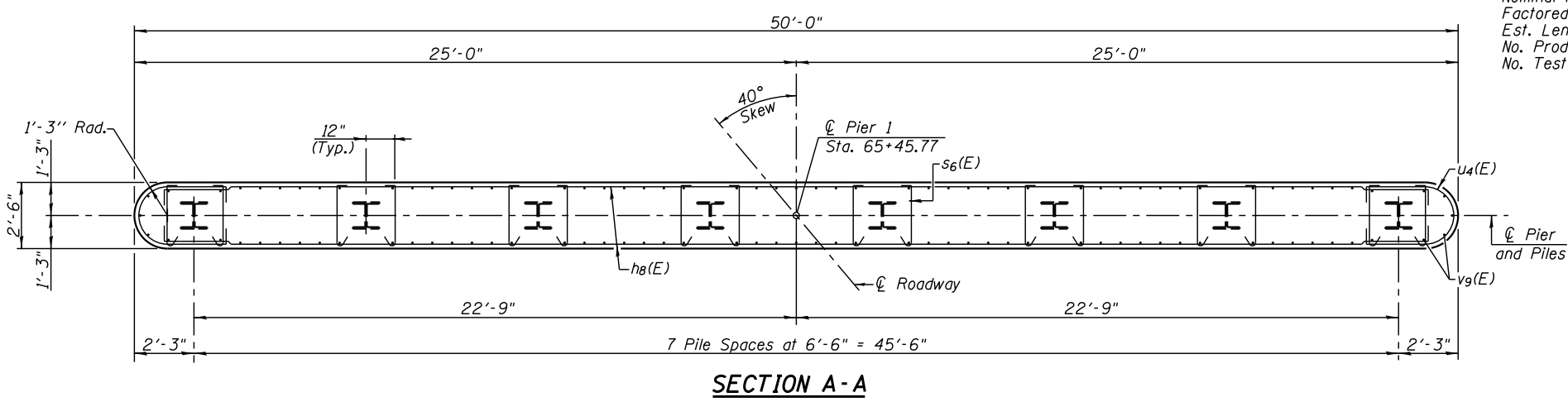
SHEET NO. 22 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	85
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

Notes:
Space reinforcement in cap to miss anchor bolts.
Four steps monolithically with cap.



END VIEW



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
hg(E)	52	#5	49'-7"	□
p4(E)	14	#8	47'-6"	—
p5(E)	4	#5	23'-6"	—
s5(E)	58	#5	12'-7"	□
s6(E)	416	#4	3'-3"	┌┐
u3(E)	9	#6	10'-7"	U
u4(E)	52	#5	9'-7"	U
u5(E)	24	#5	7'-8"	U
vg(E)	108	#5	28'-0"	—
Structure Excavation		Cu. Yd.	183	
Concrete Structures		Cu. Yd.	140	
Reinforcement Bars, Epoxy Coated		Pound	10,240	
Furnishing Steel Piles HP 12x74		Foot	592	
Driving Piles		Foot	592	

For details of piles see sheet 25 of 29.

PILE DATA

Type: HP 12x74
Nominal Required Bearing: 589 kips
Factored Resistance Available: 324 kips
Est. Length: 74 ft.
No. Production Piles: 8
No. Test Piles: 0



USER NAME =	DESIGNED - GBR	REVISED -
PLOT SCALE =	CHECKED - MAH	REVISED -
PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

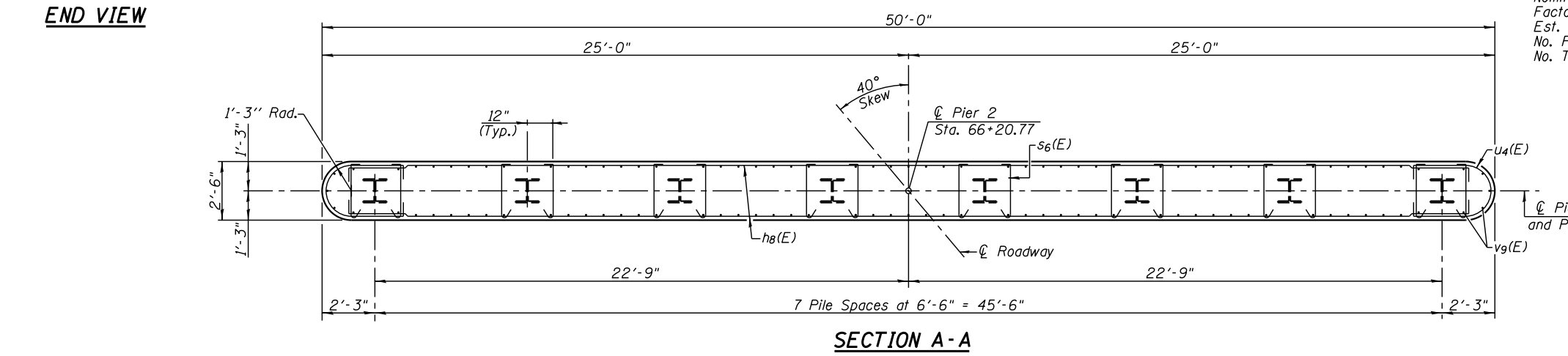
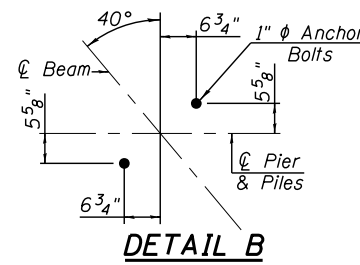
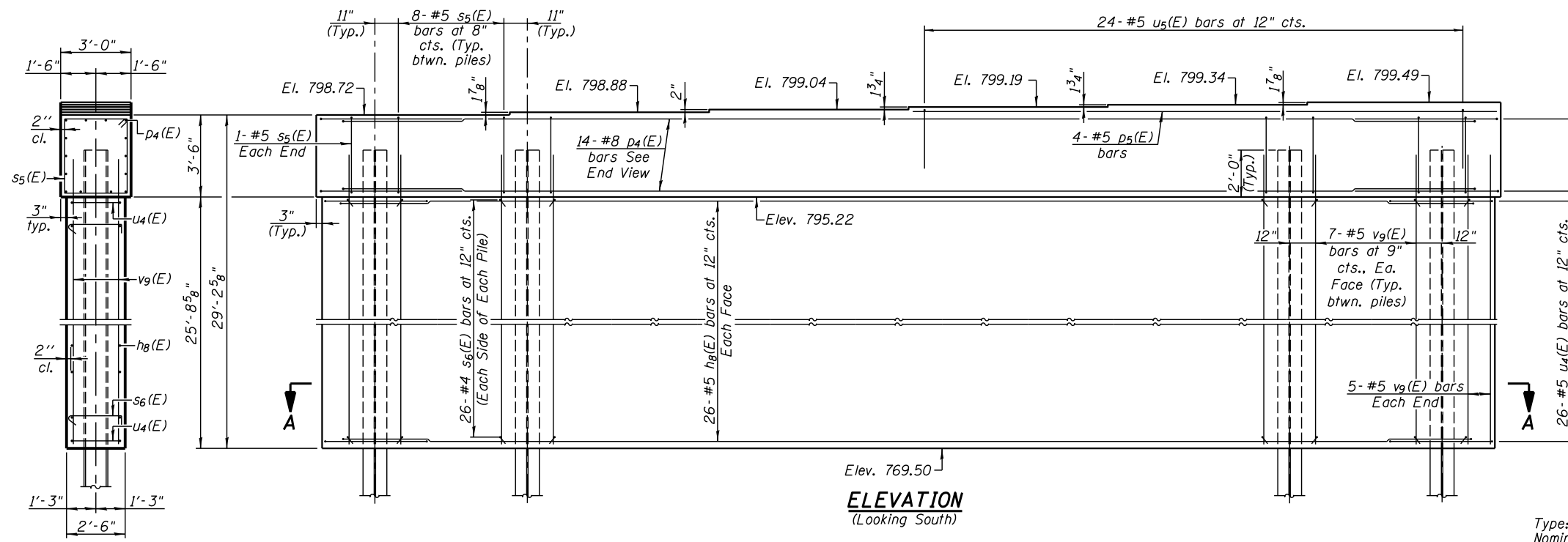
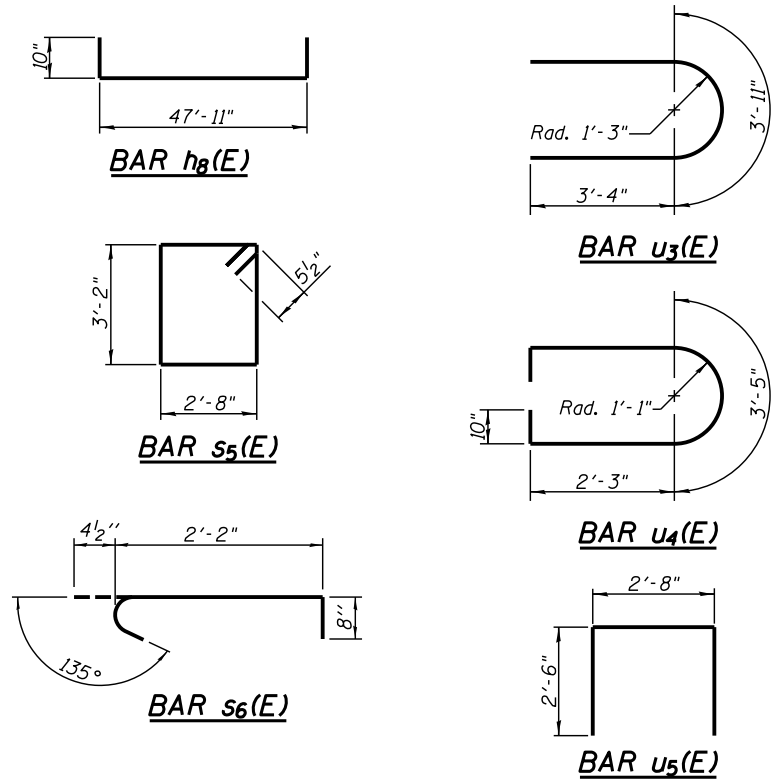
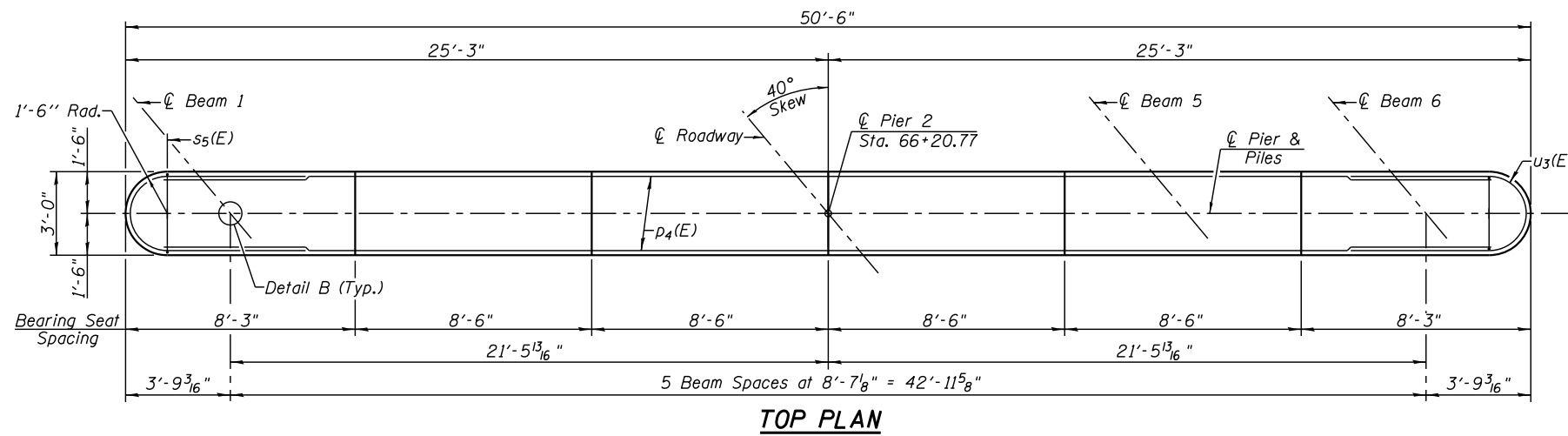
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 1
STRUCTURE NO. 048-0096

SHEET NO. 23 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	86
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

Notes:
Space reinforcement in cap to miss anchor bolts.
Pour steps monolithically with cap.



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
hg(E)	52	#5	49'-7"	U
p4(E)	14	#8	47'-6"	—
p5(E)	4	#5	23'-6"	—
s5(E)	58	#5	12'-7"	□
s6(E)	416	#4	3'-3"	┌┐
u3(E)	9	#6	10'-7"	U
u4(E)	52	#5	9'-7"	U
u5(E)	24	#5	7'-8"	U
v9(E)	108	#5	28'-0"	—
Structure Excavation	Cu. Yd.		101	
Concrete Structures	Cu. Yd.		140.0	
Reinforcement Bars, Epoxy Coated	Pound		10,240	
Furnishing Steel Piles HP 12x74	Foot		592	
Driving Piles	Foot		592	

For details of piles see sheet 25 of 29.

PILE DATA

Type: HP 12x74

Nominal Required Bearing: 589 kips

Factored Resistance Available: 324 kips

Est. Length: 74 ft.

No. Production Piles: 8

No. Test Piles: 0

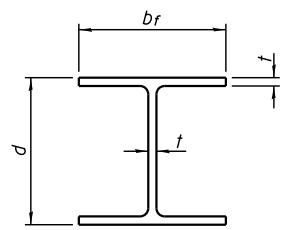


USER NAME =	DESIGNED - GBR	REVISED -
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PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
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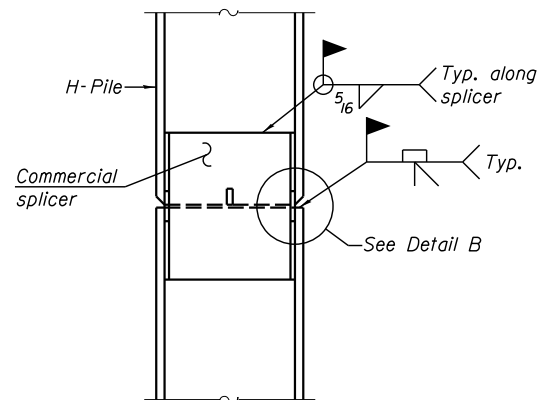
PIER 2
STRUCTURE NO. 048-0096
SHEET NO. 24 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	87
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

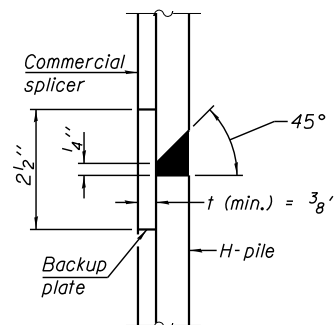


STEEL PILE TABLE

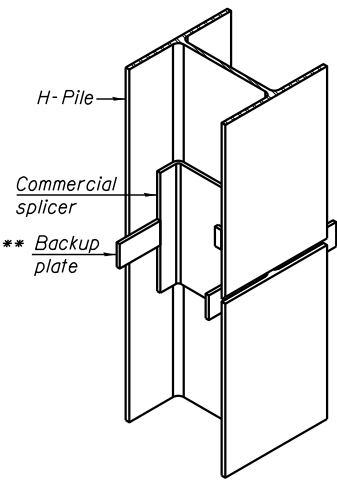
Designation	Depth d	Flange width br	Web and Flange thickness t	Encasement diameter A
HP 14x117	14 1/4"	14 7/8"	1 3/16"	30"
x102	14"	14 3/4"	1/16"	30"
x89	13 7/8"	14 3/4"	5/8"	30"
x73	13 5/8"	14 5/8"	1/2"	30"
HP 12x84	12 1/4"	12 1/4"	1/16"	24"
x74	12 1/8"	12 1/4"	5/8"	24"
x63	12"	12 1/8"	1/2"	24"
x53	11 3/4"	12"	7/16"	24"
HP 10x57	10"	10 1/4"	9/16"	24"
x42	9 3/4"	10 1/8"	7/16"	24"
HP 8x36	8"	8 1/8"	7/16"	18"



ELEVATION

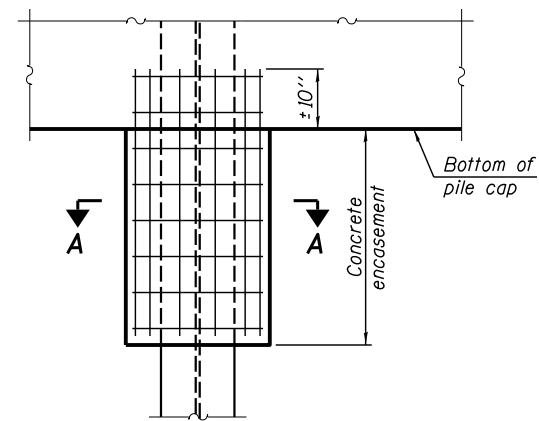


DETAIL "B"



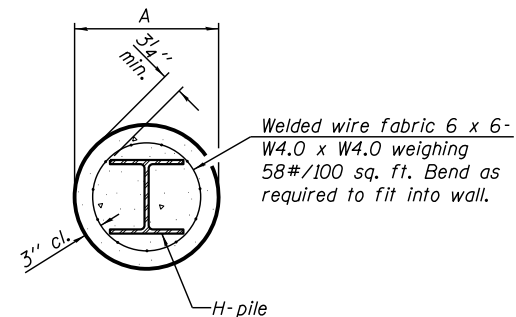
ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE



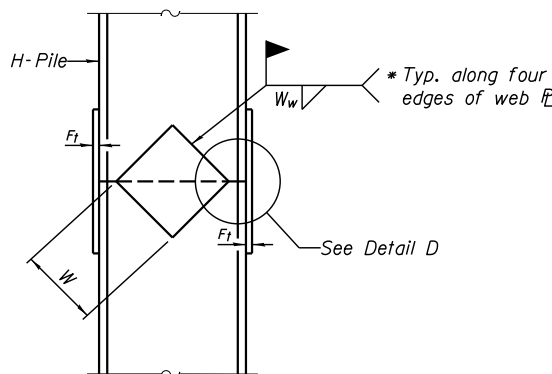
ELEVATION

PILE ENCASEMENT

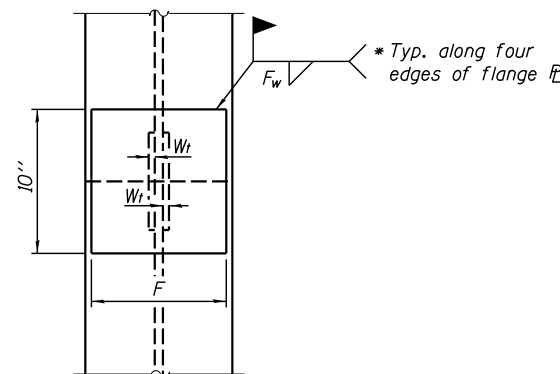


SECTION A-A

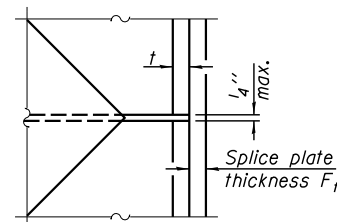
Note:
Forms for encasement may be omitted when soil conditions permit.



ELEVATION



END VIEW



DETAIL D

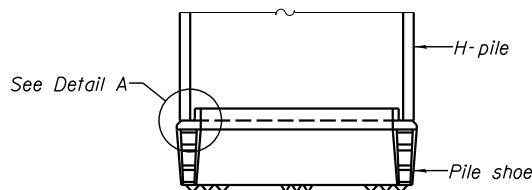
WELDED PLATE FIELD SPLICE

Designation	F	F _t	F _w	W	W _t	W _w
HP 14x117	12 1/2"	1"	7/8"	7 3/4"	5/8"	1/2"
x102	12 1/2"	7/8"	3/4"	7 3/4"	5/8"	1/2"
x89	12 1/2"	3/4"	1/16"	7 3/4"	5/8"	1/2"
x73	12 1/2"	5/8"	9/16"	7 3/4"	5/8"	1/2"
HP 12x84	10"	7/8"	1/16"	6 1/2"	5/8"	1/2"
x74	10"	7/8"	1/16"	6 1/2"	5/8"	1/2"
x63	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
x53	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
HP 10x57	8"	3/4"	9/16"	5 1/4"	1/2"	3/8"
x42	8"	5/8"	9/16"	5 1/4"	1/2"	3/8"
HP 8x36	7"	5/8"	7/16"	4 1/4"	1/2"	3/8"

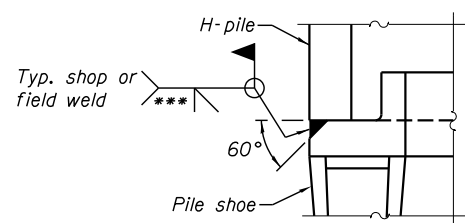
WELDED COMMERCIAL SPLICE ALTERNATE

- * Interrupt welds 1/4" from end of web and/or each flange.
- ** Remove portions of backup plates that extend outside the flanges.
- *** Weld size per pile shoe manufacturer (5/16" min.).

Note:
The steel H-piles shall be according to AASHTO M270 Grade 50.

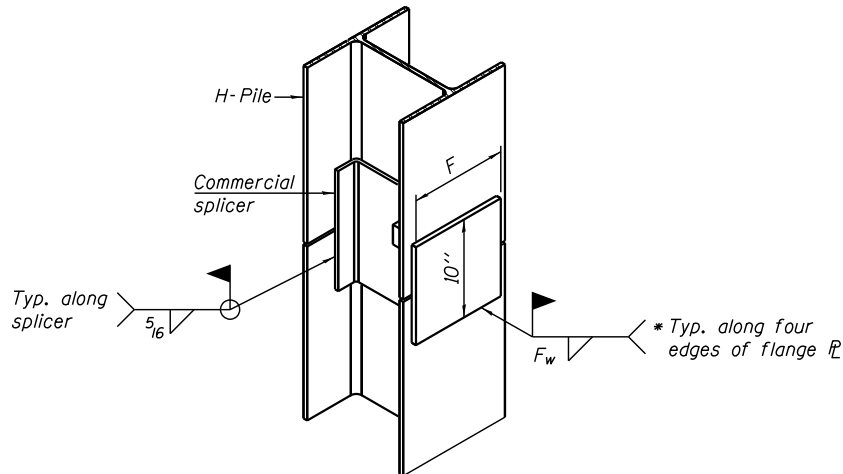


ELEVATION



DETAIL A

H-PILE SHOE ATTACHMENT



ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE ALTERNATE

F-HP 1-27-12



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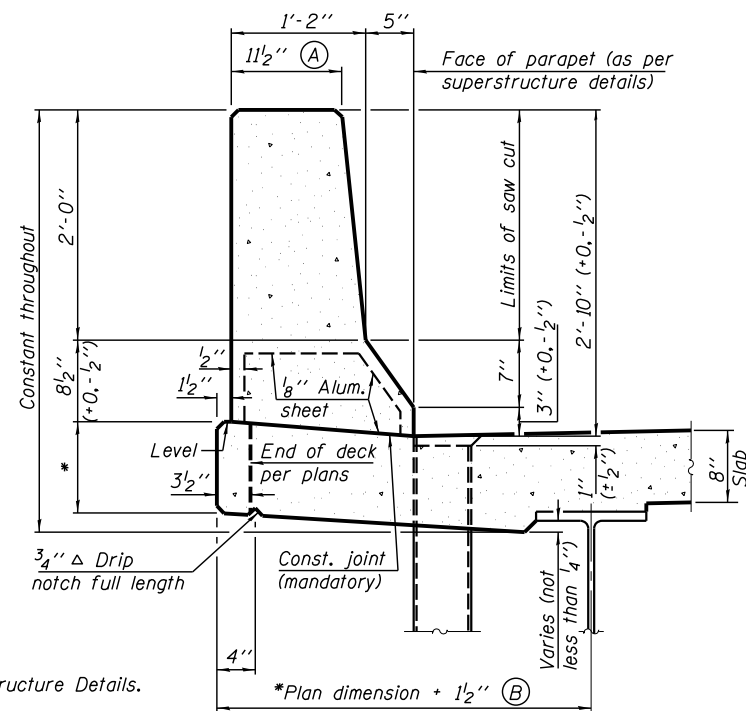
HP PILE DETAILS
STRUCTURE NO. 048-0096

SHEET NO. 25 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	88
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

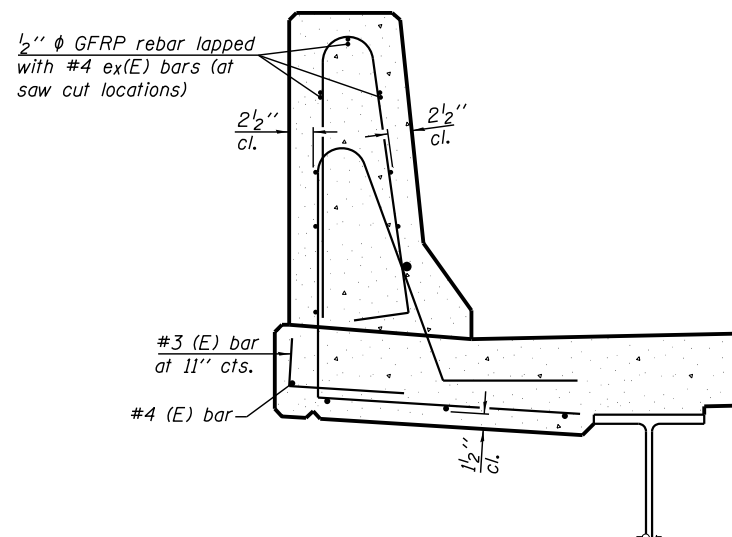
GENERAL NOTES

All dimensions shall remain the same as shown on superstructure details, except dimensions A and B which are to be revised as shown to provide additional clearance. Additional concrete needed to revise dimension A and B = 0.0165 cu. yds./ft. for 34" parapet or = 0.0223 cu. yds./ft. for 42" parapet. Place aluminum sheet in curb portion at and near piers. Full thickness saw cut at all joint locations in lieu of cork joint filler. Steel superstructure shown. Other superstructure types similar.



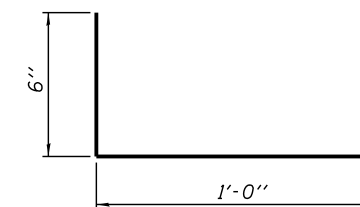
34" F SHAPE PARAPET SECTION
(Showing dimensions)

*See Superstructure Details.

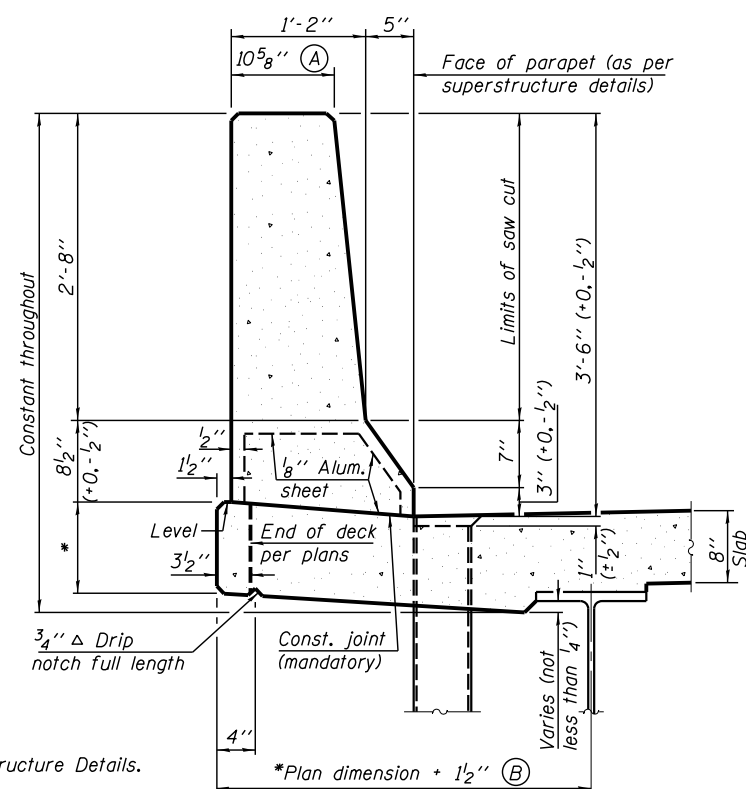


SECTION

(34" parapet shown - 42" parapet similar)
(Showing reinforcement clearances for slip forming and additional reinforcement bars)

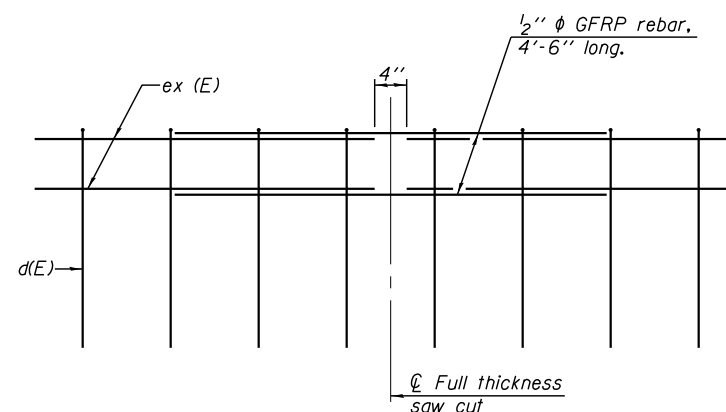


#3 (E) BAR



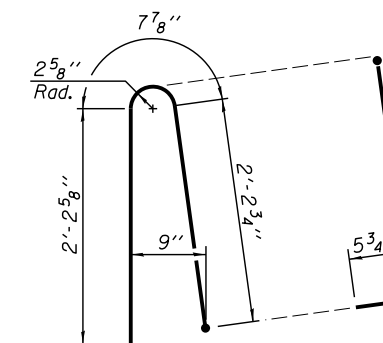
42" F SHAPE PARAPET SECTION
(Showing dimensions)

*See Superstructure Details.

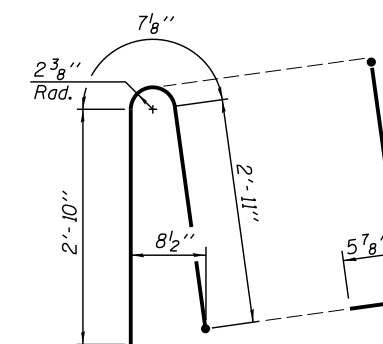


GFRP REBAR STIFFENING DETAIL

(Place as shown in parapet section at each parapet joint location.)



ALTERNATE BAR d(E)
(For 34" parapet when conduit is present)



ALTERNATE BAR d(E)
(For 42" parapet when conduit is present)

SFP 34-42

8-16-12



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CONCRETE PARAPET SLIPFORMING OPTION
STRUCTURE NO. 048-0096

SHEET NO. 26 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	89
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

SOIL BORING LOG

Date 3/2/10

ROUTE FAS 2401 DESCRIPTION US Route 150 over BNSF R.R. LOGGED BY KEG

SECTION (40V-1) BR LOCATION Galesburg, SEC. 19, TWP. 11N, RNG. 2E, Latitude, Longitude

COUNTY Knox DRILLING METHOD HSA HAMMER TYPE Automatic

STRUCT. NO.	Station	B	U	M	Surface Water Elev.	ft	D	B	U	M
BORING NO.	Station	H	S	Q	Stream Bed Elev.	ft	P	O	S	I
Offset	Ground Surface Elev.	(ft)	(#)	(tsf)	(%)	ft	(ft)	(#)	(tsf)	(%)
048-0013 (existing)	66+15.00									
B-1 (S. Abut.)	67+22				759.0					
7.7 ft Lt	797.5									
CONCRETE - 14 inches										
FILL: Dark brown, clay, trace crushed rock (A-7)										
FILL: Brown and gray, clay (A-7)										
Becomes dark brown										
Becomes brown and gray										
Becomes dark brown and brown										
Becomes dark brown										
FILL: Dark brown, silty clay (A-7)										
FILL: Dark brown and gray, clay (A-7)										

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)

SOIL BORING LOG

Date 3/2/10

ROUTE FAS 2401 DESCRIPTION US Route 150 over BNSF R.R. LOGGED BY KEG

SECTION (40V-1) BR LOCATION Galesburg, SEC. 19, TWP. 11N, RNG. 2E, Latitude, Longitude

COUNTY Knox DRILLING METHOD HSA HAMMER TYPE Automatic

STRUCT. NO.	Station	B	U	M	Surface Water Elev.	ft	D	B	U	M
BORING NO.	Station	H	S	Q	Stream Bed Elev.	ft	P	O	S	I
Offset	Ground Surface Elev.	(ft)	(#)	(tsf)	(%)	ft	(ft)	(#)	(tsf)	(%)
048-0013 (existing)	66+15.00									
B-1 (S. Abut.)	67+22				759.0					
7.7 ft Lt	797.5									
CLAY: Brown and reddish brown, trace sand, gravel (A-7) (continued)										
Becomes brown, some sand										
Becomes grayish brown										
SILT: Gray (A-4)										
SANDY CLAY: Grayish brown (A-4)										

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)

ROCK CORE LOG

Date 3/2/10

ROUTE FAS 2401 DESCRIPTION US Route 150 over BNSF R.R. LOGGED BY KEG

SECTION (40V-1) BR LOCATION Galesburg, SEC. 19, TWP. 11N, RNG. 2E, Latitude, Longitude

COUNTY Knox CORING METHOD Rotary, surface set diamond bit

STRUCT. NO.	Station	CORING BARREL TYPE & SIZE	NX conv dbl bbl split inner	D	C	R	R	CORE	S	M
BORING NO.	Station	Core Diameter	Top of Rock Elev.	P	T	H	Y	DEPTH	TRENGTH	TEST
Offset	Ground Surface Elev.	in	ft	(ft)	(#)	(%)	(%)	(min/ft)	(tsf)	(%)
048-0013 (existing)	66+15.00	2	729.46					1	98	93
B-1 (S. Abut.)	67+22	2	728.96							
7.7 ft Lt	797.5									
SILTSTONE: Gray										
End of Boring										

Color pictures of the cores Yes
 Cores will be stored for examination until Completion of Const.
 The "Strength" column represents the uniaxial compressive strength of the core sample (ASTM D-2938) BBS, form 138 (Rev. 8-99)



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PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BORING LOGS
STRUCTURE NO. 048-0096
 SHEET NO. 27 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	90
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

ROUTE FAS 2401 DESCRIPTION US Route 150 over BNSF R.R. LOGGED BY KEG
 SECTION (40V-1) BR LOCATION Galesburg, SEC. 19, TWP. 11N, RNG. 2E,
 Latitude, Longitude
 COUNTY Knox DRILLING METHOD HSA HAMMER TYPE Automatic

STRUCT. NO.	Station	DEPTH	DIAMETER	SOIL TYPE	WATER	DEPTH	DIAMETER	SOIL TYPE	WATER
048-0013 (existing)	66+15.00	(ft)	(in)	(tsf)	(ft)	(in)	(tsf)	(ft)	(in)
BORING NO. B-2 (N. Abut.)		Station 64+81		Groundwater Elev. 763.7		First Encounter		Upon Completion	
Offset 21.6 ft Lt		Ground Surface Elev. 797.2		After - Hrs.					
CONCRETE - 12 inches									
FILL: Dark brown and gray, clay, trace gravel, crushed rock (A-7)									
CLAY: Brown (A-7)									
FILL: Brown and gray, clay (A-7)									
Becomes brown, gray, and dark brown									
Becomes brown									
Becomes dark brown, trace sand									
Trace gravel									

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)

ROUTE FAS 2401 DESCRIPTION US Route 150 over BNSF R.R. LOGGED BY KEG
 SECTION (40V-1) BR LOCATION Galesburg, SEC. 19, TWP. 11N, RNG. 2E,
 Latitude, Longitude
 COUNTY Knox DRILLING METHOD HSA HAMMER TYPE Automatic

STRUCT. NO.	Station	DEPTH	DIAMETER	SOIL TYPE	WATER	DEPTH	DIAMETER	SOIL TYPE	WATER
048-0013 (existing)	66+15.00	(ft)	(in)	(tsf)	(ft)	(in)	(tsf)	(ft)	(in)
BORING NO. B-2 (N. Abut.)		Station 64+81		Groundwater Elev. 763.7		First Encounter		Upon Completion	
Offset 21.6 ft Lt		Ground Surface Elev. 797.2		After - Hrs.					
CLAY: Brown, trace to some sand (A-7) (continued)									
SAND: Brown, fine to medium, trace gravel (A-3)									
SILT: Gray (A-4)									
CLAYEY SHALE: Gray									
SAND & GRAVEL (A-1)									
CLAY: Grayish brown, some sand, trace gravel (A-7)									

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) AASHTO Classifications are based on visual classifications unless otherwise noted BBS, form 137 (Rev. 8-99)

ROUTE FAS 2401 (US 150 over BNSF RR) DESCRIPTION US Route 150 over BNSF R.R. LOGGED BY KEG
 SECTION (40V-1) BR LOCATION Galesburg, SEC. , TWP. , RNG. ,
 Latitude, Longitude
 COUNTY Knox DRILLING METHOD CME 55 w/HSA HAMMER TYPE Automatic

STRUCT. NO.	Station	DEPTH	DIAMETER	SOIL TYPE	WATER	DEPTH	DIAMETER	SOIL TYPE	WATER
048-0013 (existing)	66+50	(ft)	(in)	(tsf)	(ft)	(in)	(tsf)	(ft)	(in)
BORING NO. B-3 (PIER 2)		Station 66+50		Groundwater Elev. 767.5		First Encounter		Upon Completion	
Offset 30.0 ft Rt		Ground Surface Elev. 778.45		After 24 Hrs. Not observed					
Shrub ground cover & Topsoil+									
FILL: Dark brown, silty clay, trace sand (A-6)									
FILL: Brown, silty clay, trace organics (A-7)									
FILL: Dark brown, silty clay, trace sand, organics (A-6)									
SILT: Brown (A-4)									
SILT: Gray (A-4)									
Becomes brown and grayish brown									
Becomes brown, some sand									
Becomes gray									
CLAY: Brown, some sand (A-7)									
SANDY CLAY: Gray, trace fine gravel (A-6)									
SANDY CLAY: Brown, trace fine gravel (A-7)									

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T266) BBS, form 137 (Rev. 8-99)

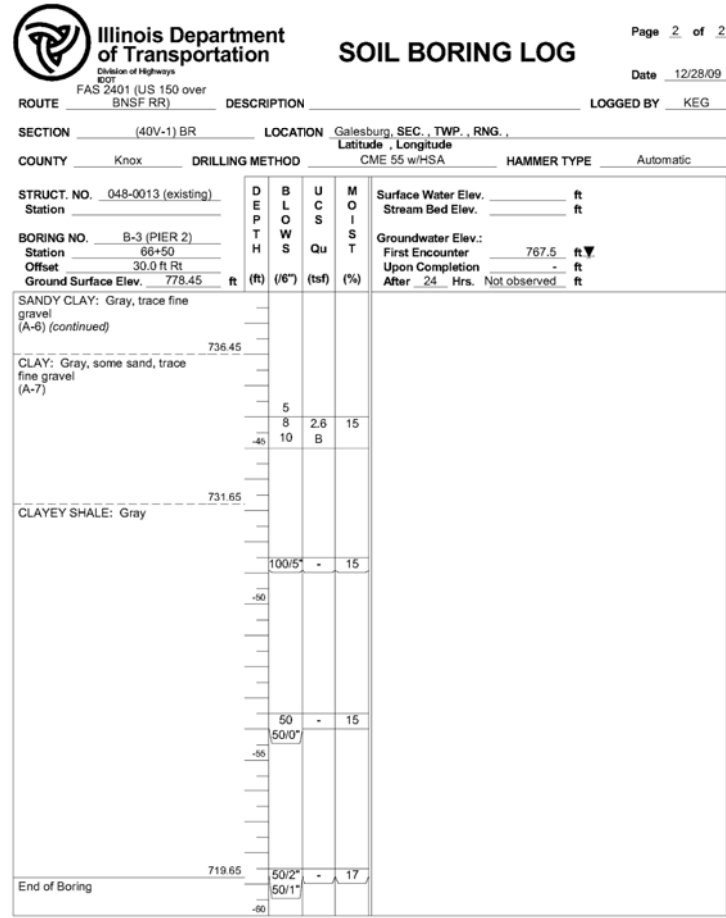


USER NAME =	DESIGNED - GBR	REVISED -
PLOT SCALE =	CHECKED - MAH	REVISED -
PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

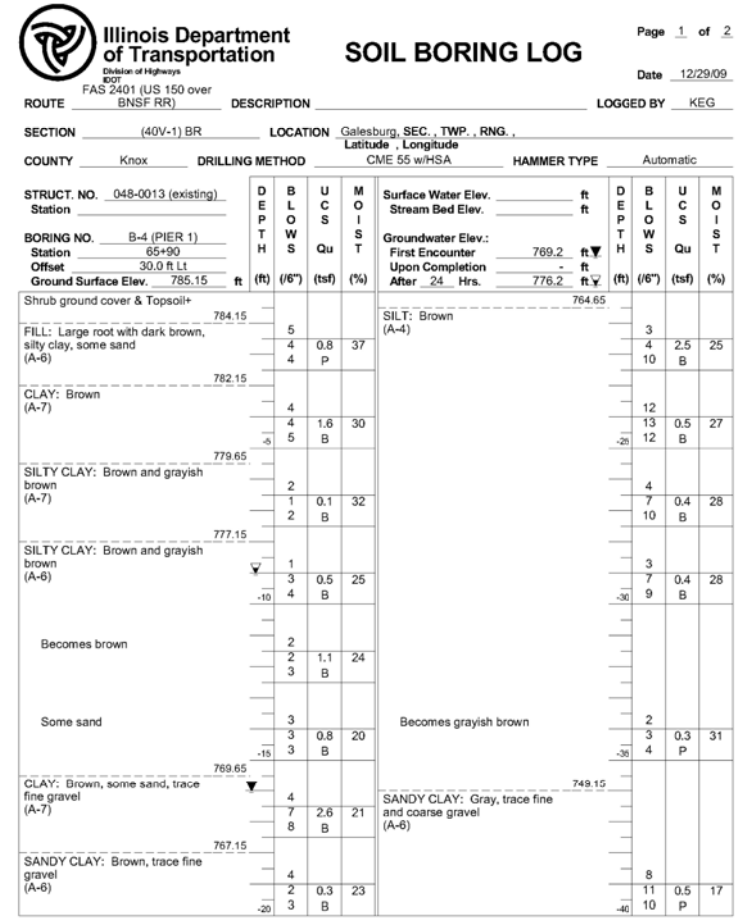
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BORING LOGS
STRUCTURE NO. 048-0096
 SHEET NO. 28 OF 29 SHEETS

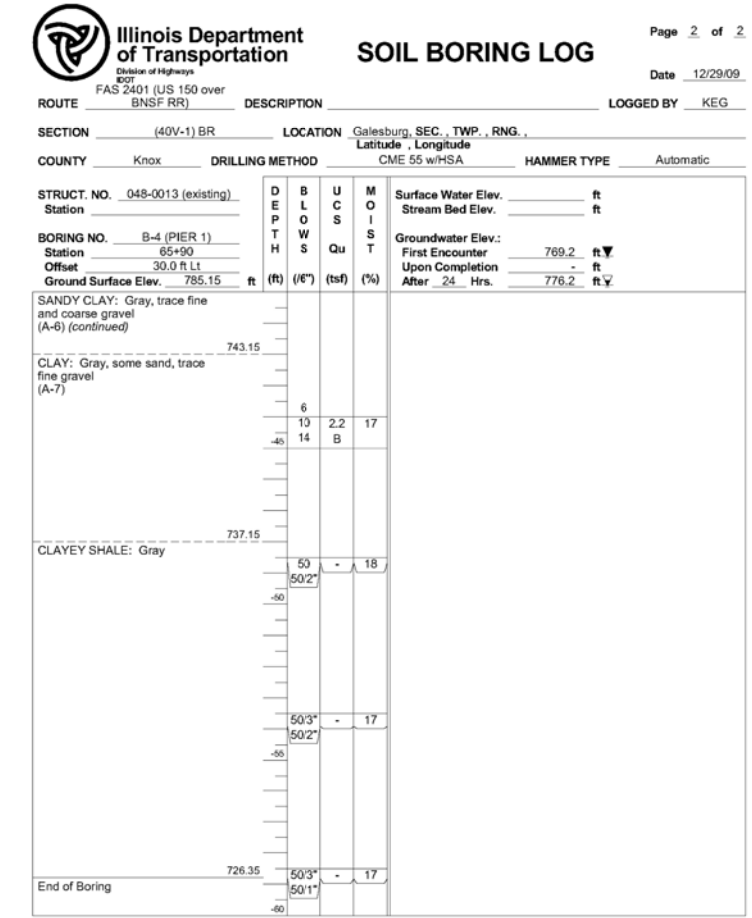
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	91
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				



The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
 BBS, form 137 (Rev. 8-99)



The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
 BBS, form 137 (Rev. 8-99)



The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
 BBS, form 137 (Rev. 8-99)



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PLOT DATE =	CHECKED - GBR	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BORING LOGS
STRUCTURE NO. 048-0096**

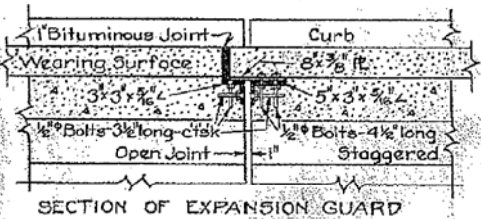
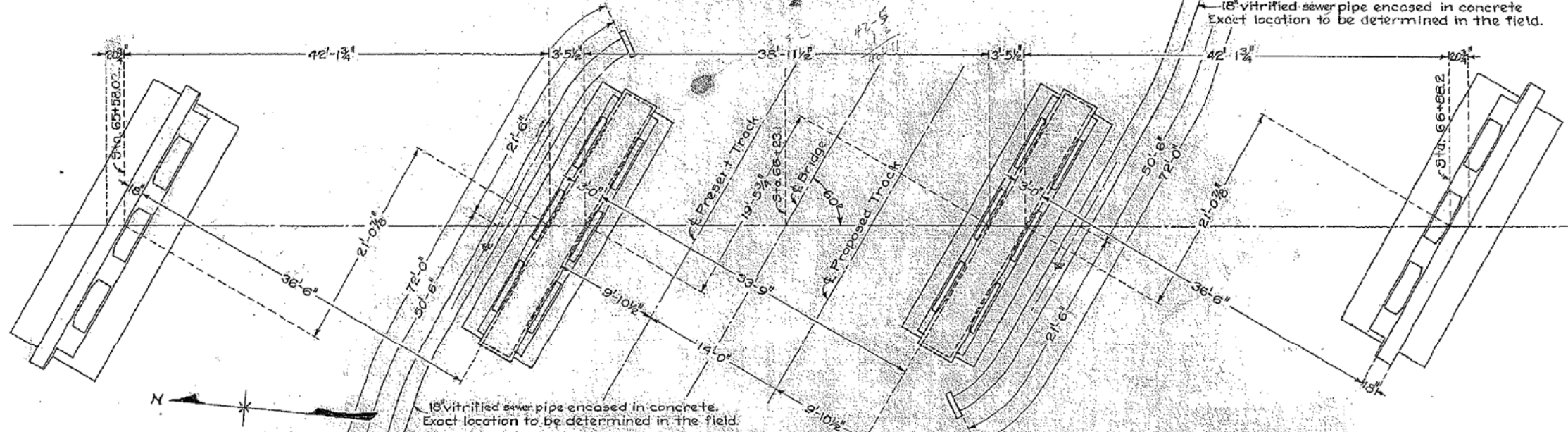
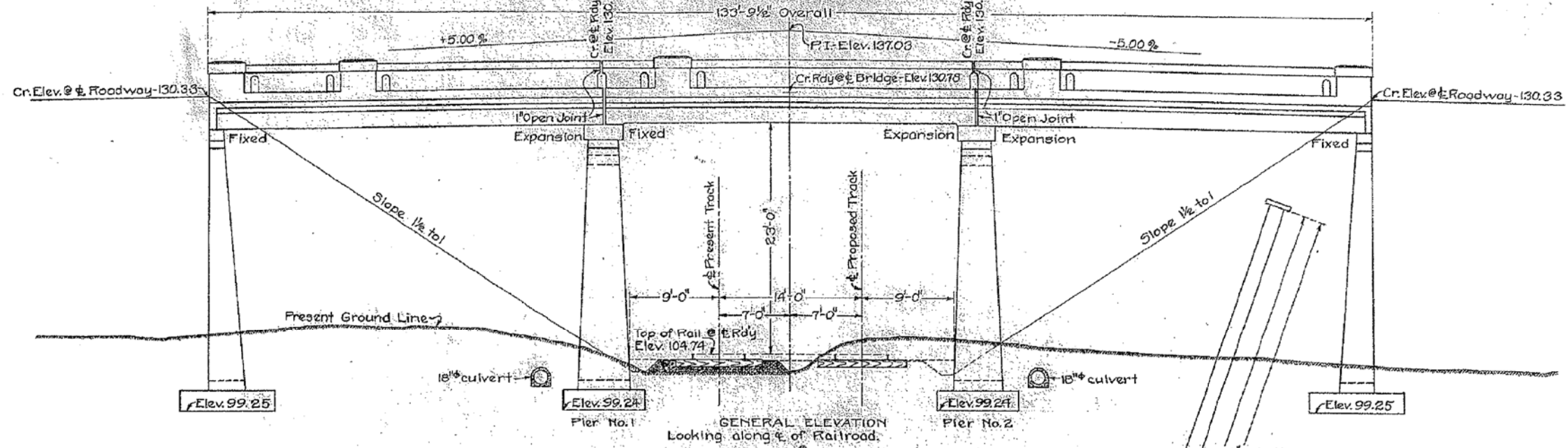
SHEET NO. 29 OF 29 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	92
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

BM 8-N&W in Gate Post
Rt. Sta. 65+00-Elev. 106.30.
There is no existing structure.

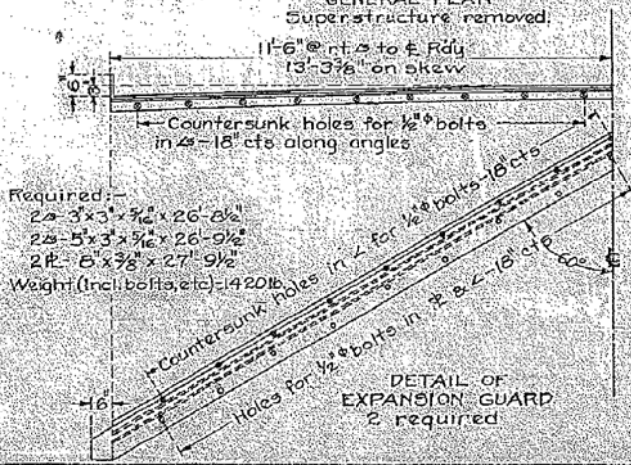
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

GROUP NO.	COUNTY	SEC.	TOTAL SHEETS	SHEET NO.
8	KNOX	40V	6	2



COMPUTED	—
CHECKED	—
DRAWN	—
CHECKED	—
SPECIAL ASSEMBLED	—
CHECKED	—

EXAMINED Oct 8 1925
BRIDGE ENGINEER
APPROVED
CHIEF HIGHWAY ENGINEER



SUMMARY BILL OF MATERIAL

Item	Span 1-3	Span 2	Abut. Piers	Piers	Culverts	Total
Class A Concrete-Cu.Yds.	105.6	164	101.7	130.0	15.8	399.5
Class X Concrete-Cu.Yds.	14.0	6.1				20.1
Reinforcing Steel-Lbs.	38930	17420	13820	11270		81440
Rockers & Plates-Lbs.	1900	950				2850
Str. Steel (Exp. Device)-Lbs.		1420				1420
Name Plate	1					1

FOR INFORMATION ONLY

STATION 66+15
STATE ROAD 155UE-ROUTE 8
SECTION 40V - KNOX COUNTY



USER NAME =	DESIGNED - GBR	REVISED -
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PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING GENERAL PLAN & ELEVATION
1925 PLANS (FOR INFORMATION ONLY)

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	93

CONTRACT NO. 68800

SHEET NO. 1 OF 6 SHEETS

ILLINOIS FED. AID PROJECT

B.M.-U.S.G.S. (CRAIG RM 1) 796.112
 Existing Structure: 3 span R.C.G.D with P.C.C. Piers & Abuts, Rdwy width 23'0" F to F of curb
 Remove & replace Superstructure and designated portions of the Substructure No salvage
 Temporary Bridge not required. Traffic to be detoured.

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
U.S. 150	40V-1	KNOX	12	7
FED. ROAD DIST. NO. 7	SECTION	FED. AID PROJECT		

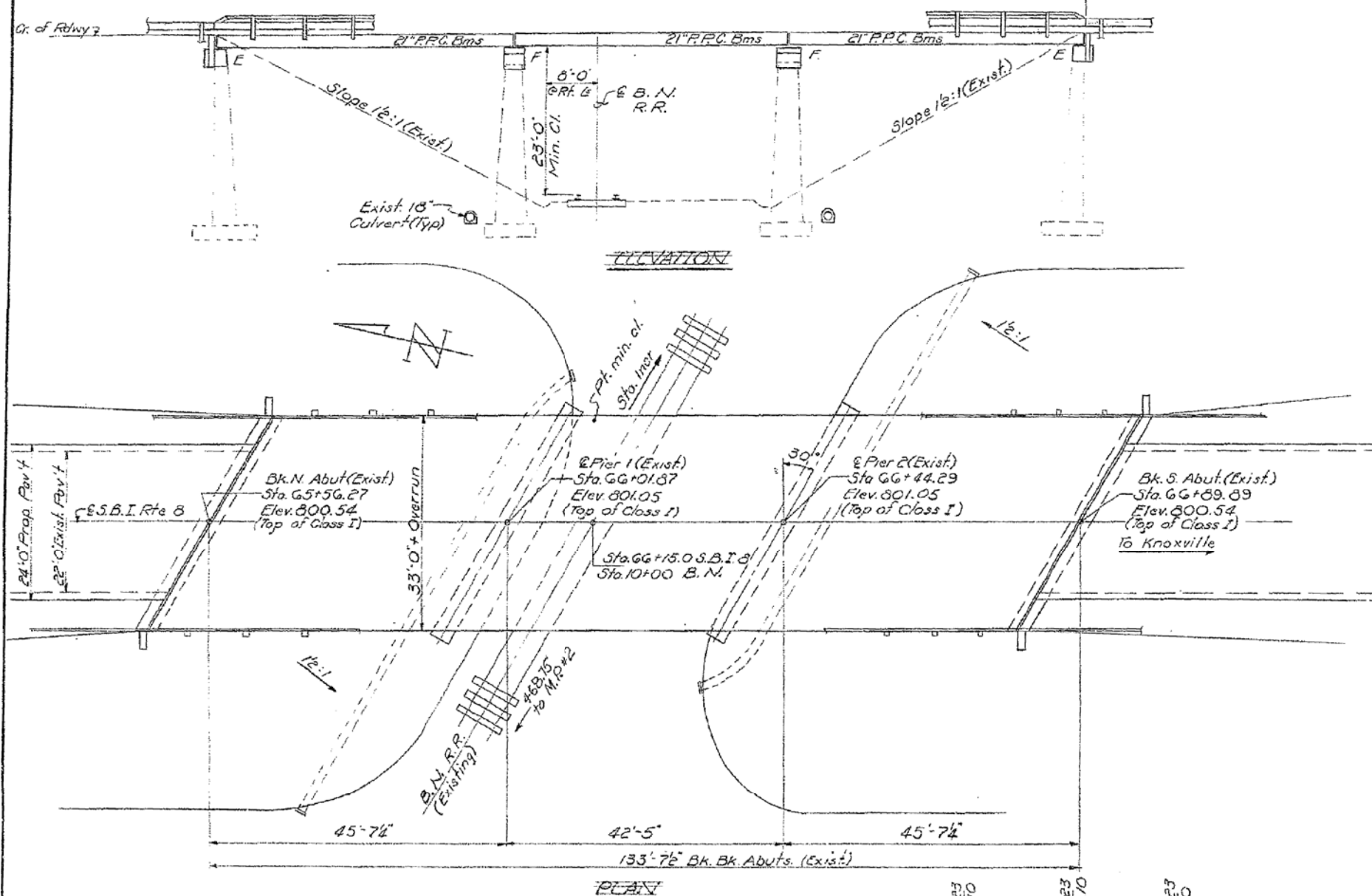
GENERAL NOTES

- All structural steel shall be shop painted with two coats of basic lead silico chromate paint.
- Expansion guards which are not cast in the precast unit shall be fabricated and erected in accordance with Article 503.07(c) of the Standard Specifications and are included in quantity of structural steel.
- It shall be the responsibility of the Contractor to verify all dimensions and conditions existing in the field prior to construction and ordering of materials.
- The top surface of the beams shall be finished in accordance with Article 505.06 of the Standard Specifications except that the surface shall not be roughened by brooming. The finished surface shall be free of depressions or high spots with sharp corners.
- Protective Coat shall not be applied to surfaces to which Waterproofing Membrane System is applied.

TOTAL BILL OF MATERIAL

Item	Unit	Super	Sub	Total
Bituminous Concrete Surface Course, Class I	Tons	23		23
Portland Cement Mortar Bedding Course	Lin. Ft.	1331		1331
Waterproofing Membrane System	Sq. Yds.	462		462
Concrete Removal	Cu. Yds.		25	25
Structural Steel	Lbs.	5010		5010
Class A Concrete	Cu. Yds.	14.6	61.8	76.4
Steel Shell Piles (12" x 12")	Lin. Ft.		160	160
Precast Prestressed Concrete Deck Beams (27)	Sq. Ft.	4385		4385
Steel Rolling, Type (1)	Lin. Ft.	266		266
Reinforcement Bars	Lbs.	590	7710	8300
Pavement Removal (P.C.C. Replacement Type 210)	Sq. Yds.	1		1
Removal of Existing Superstructures	Each	1		1
Name Plates	Each		1	1
Preformed Joint Sealer (2")	Lin. Ft.	75		75
Protective Coat	Sq. Yds.	45		45

* See Special Provisions



STATION GG+15.00
 REBUILT BY
 STATE OF ILLINOIS
 S.B.I. RT. 8 SEC. 40V-1
 LOADING HS 20

NAME PLATE
 See Std. 2113

DESIGNED: *John Schindler*
 CHECKED: *James Ponce*
 DRAWN: *W.E.D.*
 CHECKED: *JP*

EXAMINED: *November 3 1971*
 PASSED: *[Signature]*
 APPROVED: *[Signature]*
 DIRECTOR OF HIGHWAYS

PRECAST PRESTRESSED UNITS

$f_c = 5000$ psi
 $f_{cu} = 4000$ psi
 $f_s = 270,000$ psi (Strands 7/8")
 $f_{sc} = 183,000$ psi (Strands 7/8")

FIELD UNITS

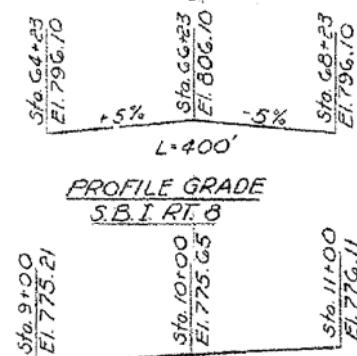
$f_c = 1400$ psi Sub
 $f_s = 20000$ psi Reinf.

n = 10

LOADING HS 20-44

Allow 25% for future Wearing Surface

PROFILE GRADE
 S.B.I. RT. 8



PROFILE GRADE N. RAIL B.N. R.R.



FOR INFORMATION ONLY

GENERAL PLAN & ELEVATIONS
 S.B.I. RT. 8 OVER B.N. R.R.
 S.B.I. RT. 8 (U.S. 150) SEC. 40V-1
 KNOX COUNTY
 STA. GG+15.00

Rev. J.A.M. 6-3-76

V&K
 Veenstra & Kimm, Inc.
 Springfield, IL Phone: (217)544-8033

USER NAME =	DESIGNED - GBR	REVISED -
PLOT SCALE =	CHECKED - MAH	REVISED -
PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXISTING GENERAL PLAN & ELEVATION
 1971 PLANS (FOR INFORMATION ONLY)

SHEET NO. 2 OF 6 SHEETS

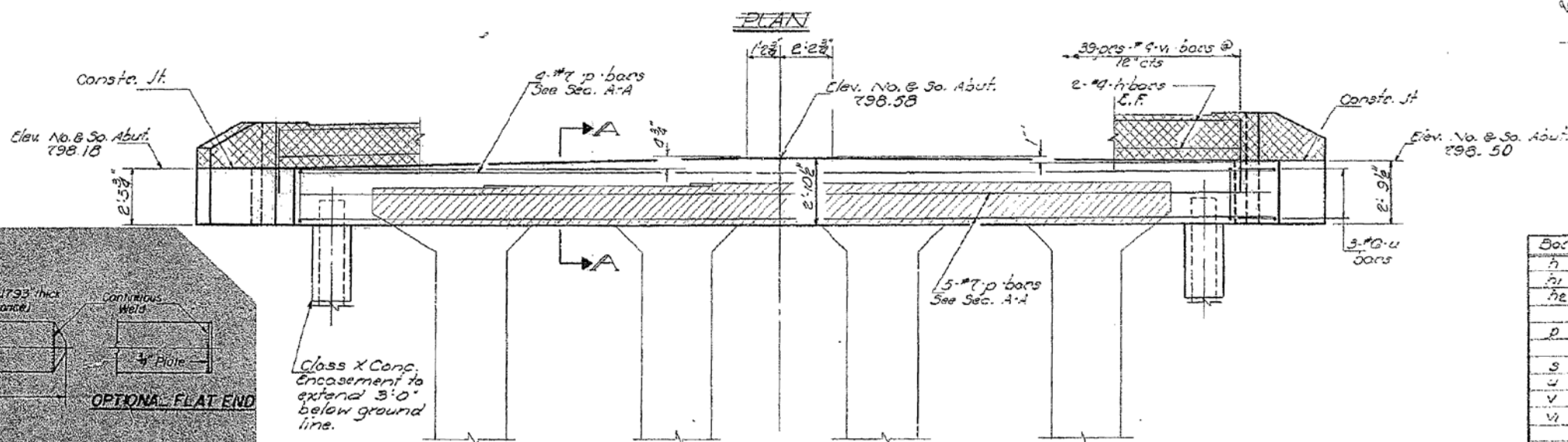
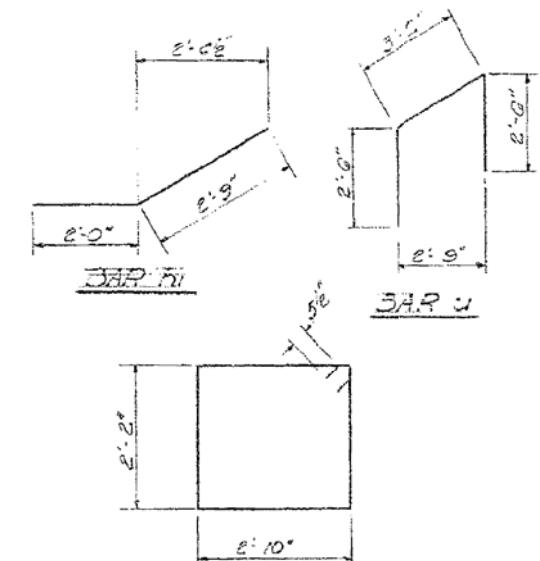
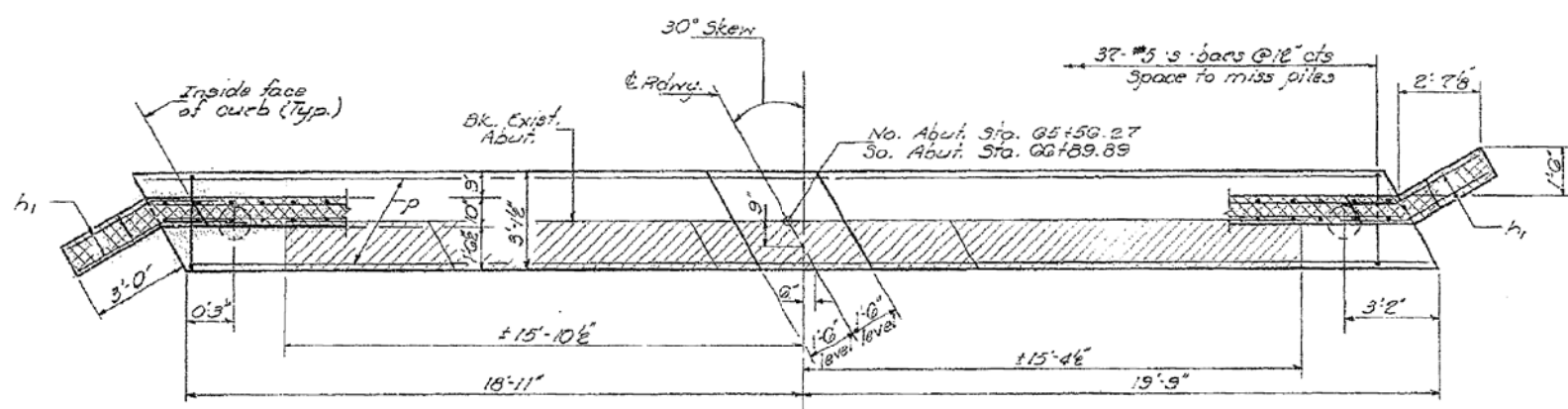
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	94
CONTRACT NO. 68800				

ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	95

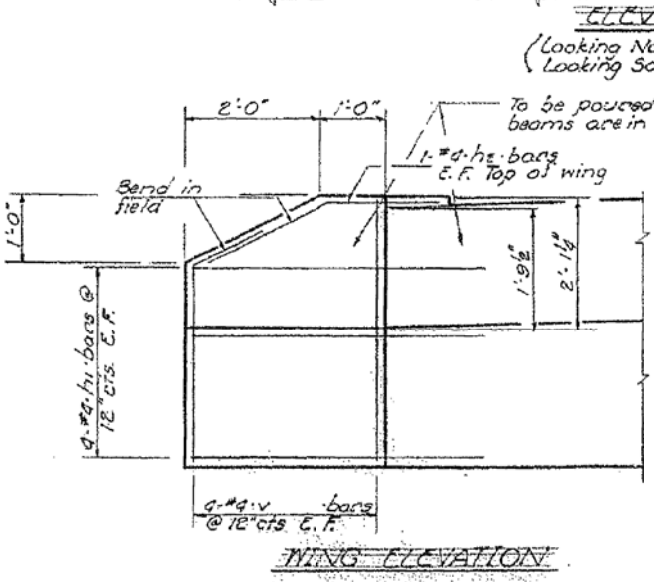
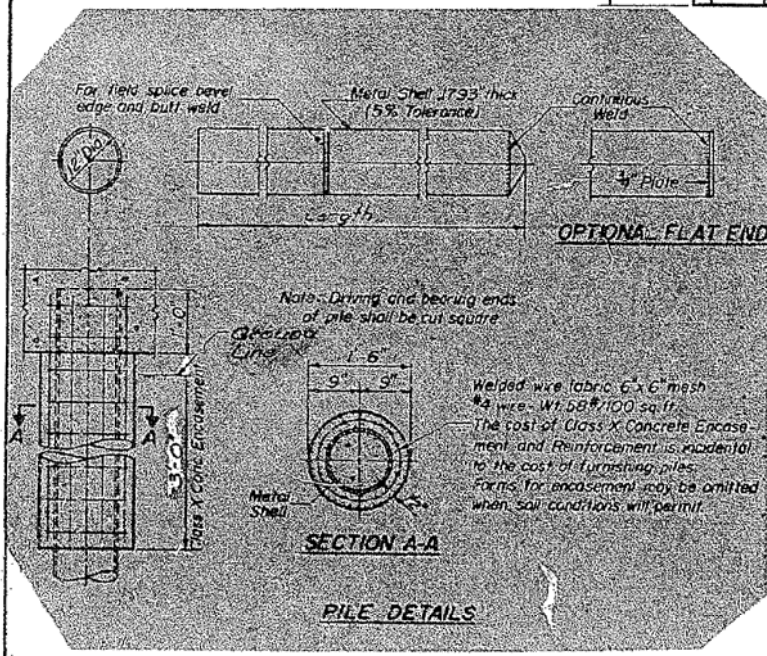
Notes:
Patched area indicates Concrete Removal.
Reinforcement extending into removed area shall be cleaned and incorporated into the new construction.
Cross-hatched area to be poured after beams are in place. Class X Concrete filled with superstructure.



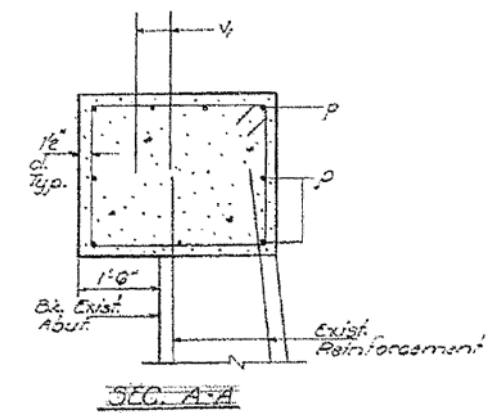
**TYPICAL ABUTMENTS
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h	8	#4	38'-5"	—
h1	32	#4	2'-9"	—
h2	8	#6	3'-0"	—
p	18	#7	38'-5"	—
s	74	#5	10'-11"	□
u	12	#6	8'-5"	—
v	32	#6	6'-0"	—
v1	156	#4	3'-0"	—

Class X Concrete	Cu. Yds	26.0
Concrete Removal	Cu. Yds	6
Reinforcement Bars	Lbs	3120
Metal Shell Piles (12")	Lin. Ft.	100



PILE DATA
Type: Metal Shell
Capacity: 35 Tons
Est. Length: 60 Ft.
No. Req'd.: 4



FOR INFORMATION ONLY

ABUTMENTS
S.B.T. 97-8 SEC. 40V-1
KNOX COUNTY
STA. 66+15.00

DESIGNED: [Signature]
CHECKED: James P. [Signature]
DRAWN: J. SCHNELLER
CHECKED: JP

EXAMINED: [Signature] Nov. 8 1971
PASSED: [Signature]
APPROVED: [Signature] DIRECTOR OF HIGHWAYS



USER NAME =	DESIGNED - GBR	REVISED -
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	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING ABUTMENTS
1971 PLANS (FOR INFORMATION ONLY)

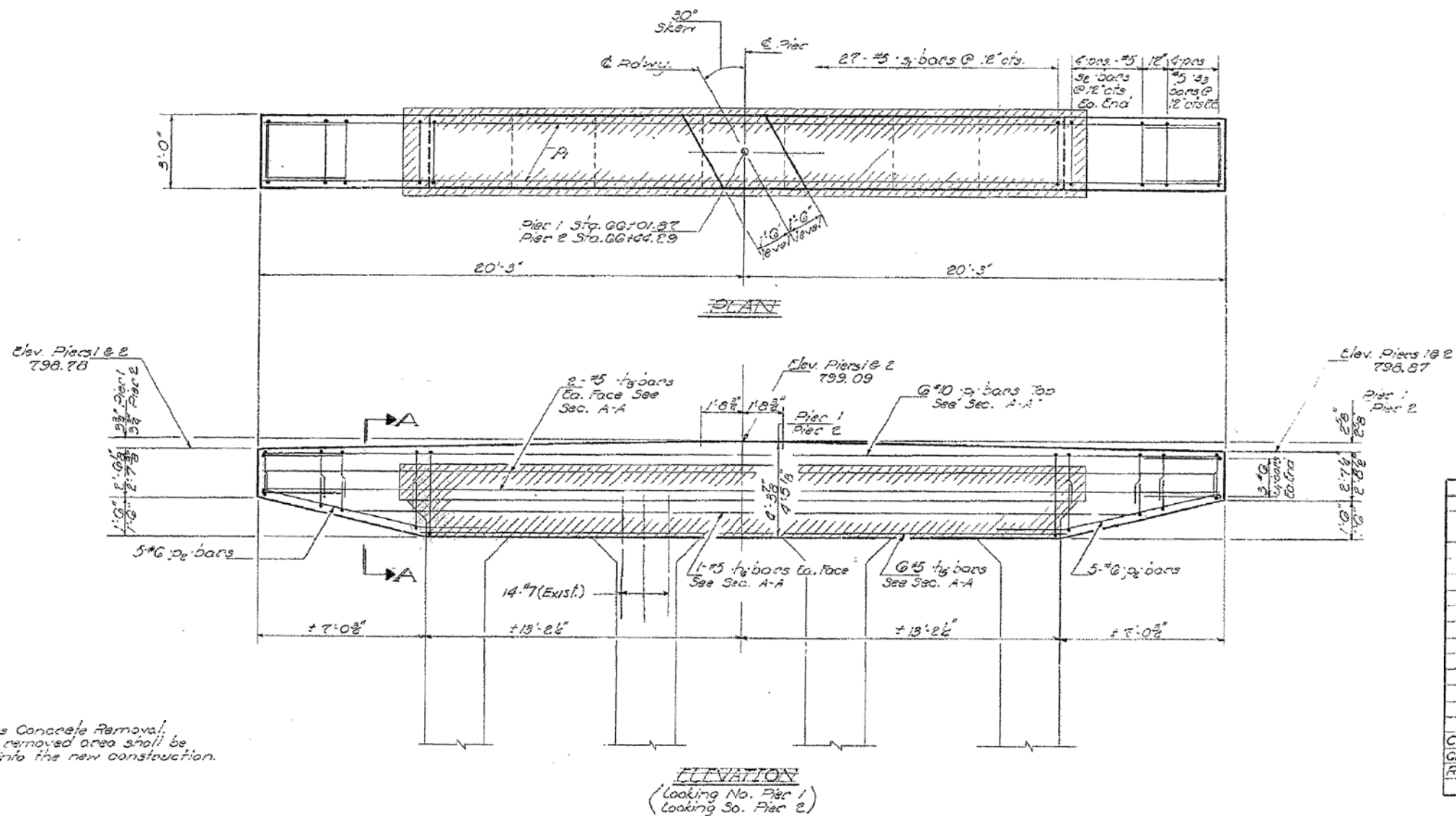
SHEET NO. 3 OF 6 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	95

CONTRACT NO. 68800
ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	13	13
SHEET NO. 7 7 SHEETS				



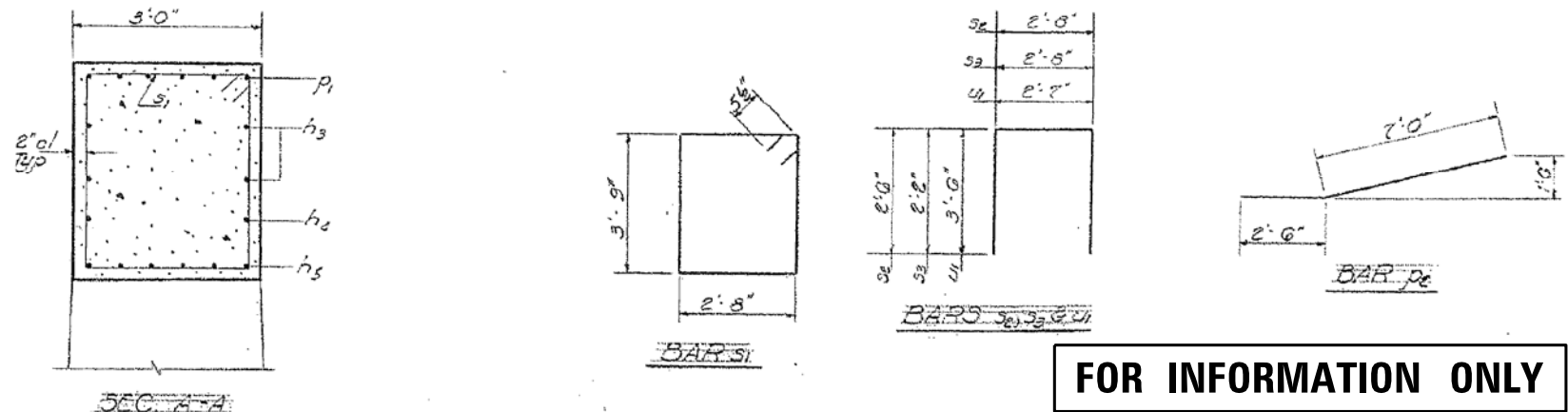
**TWO PIERS
BILL OF MATERIAL**

Size	No.	Size	Length	Shape	
#3	8	#3	20'-2"	—	
#4	4	#5	30'-9"	—	
#8	12	#3	26'-0"	—	
#1	12	#10	20'-5"	—	
#2	20	#6	9'-6"	—	
S1	50	#5	13'-9"	□	
S2	32	#5	7'-8"	□	
S3	32	#5	7'-0"	□	
L1	12	#6	9'-2"	□	
Class X Concrete				Cu. Yds.	35.5
Concrete Removal				Cu. Yds.	19
Reinforcement Bars				Lbs.	4590

Notes: Hatched area indicates Concrete Removal. Reinforcement extending into removed area shall be cleaned and incorporated into the new construction.

DESIGNED: *[Signature]*
 CHECKED: James Ruce
 DRAWN: J. SCHNELLE
 CHECKED: JP

EXAMINED: *[Signature]*
 PASSED
 APPROVED: *[Signature]*
 DIRECTOR OF HIGHWAYS



FOR INFORMATION ONLY

PIERS 1 & 2
 35.5 CU. YDS. 550.00 YDS
 KNOX COUNTY
 574.08.15.00



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PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

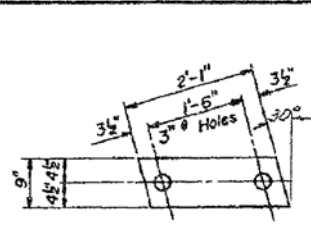
EXISTING PIERS
1971 PLANS (FOR INFORMATION ONLY)

SHEET NO. 4 OF 6 SHEETS

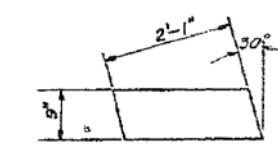
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	96
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

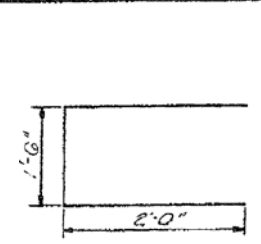
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
201 B	40V-1	KNOX	13	8
FED. ROAD DIST. NO. 1	141-107	FED. AID PROJECT		



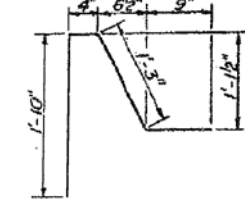
FABRIC BEARING PAD



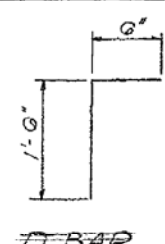
GRAPHITED ASBESTOS BEARING PAD



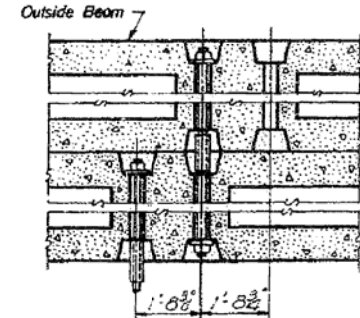
U₁ BAR



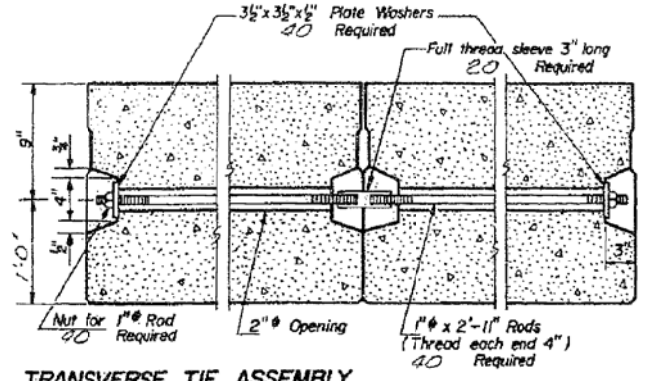
U BAR



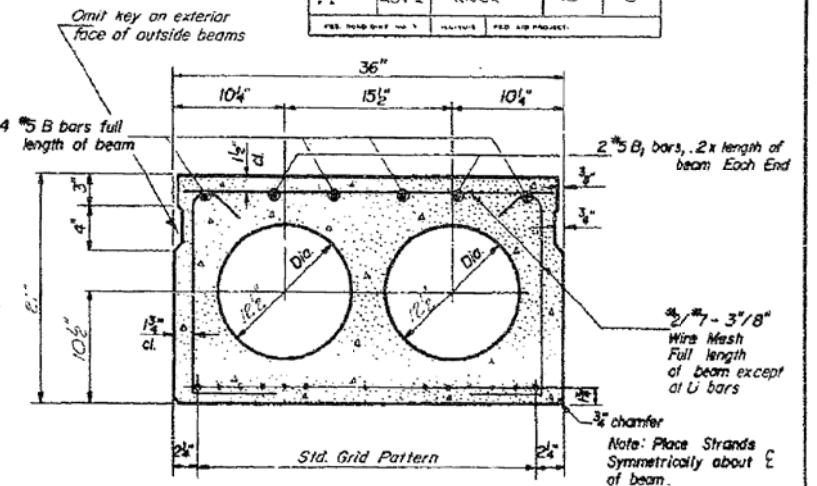
D BAR



Outside Beam

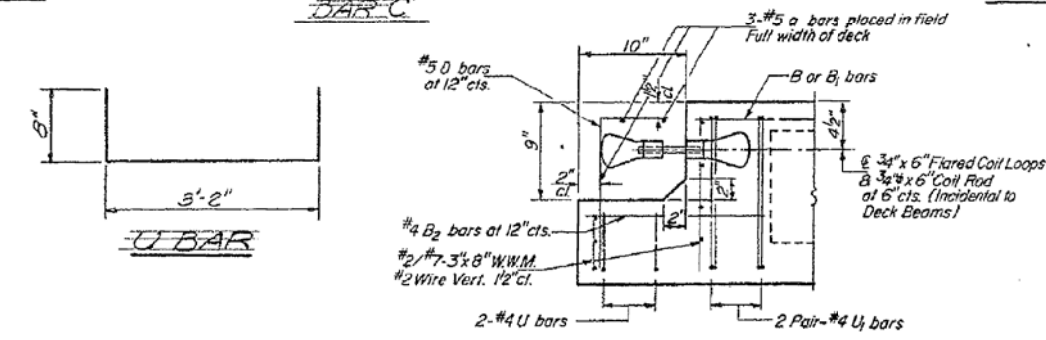


TYPICAL TRANSVERSE TIE ASSEMBLY

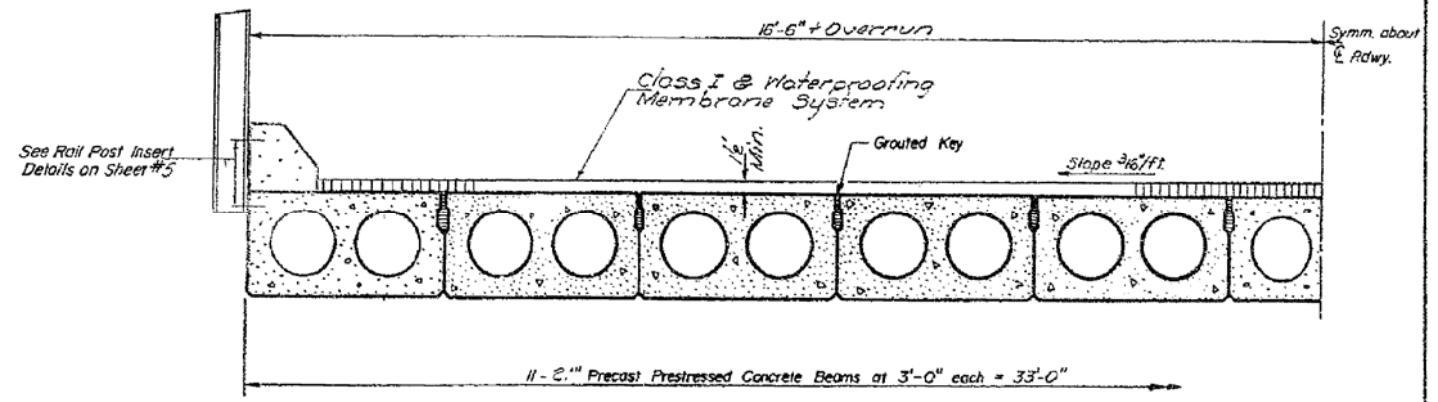


TYPICAL SECTION

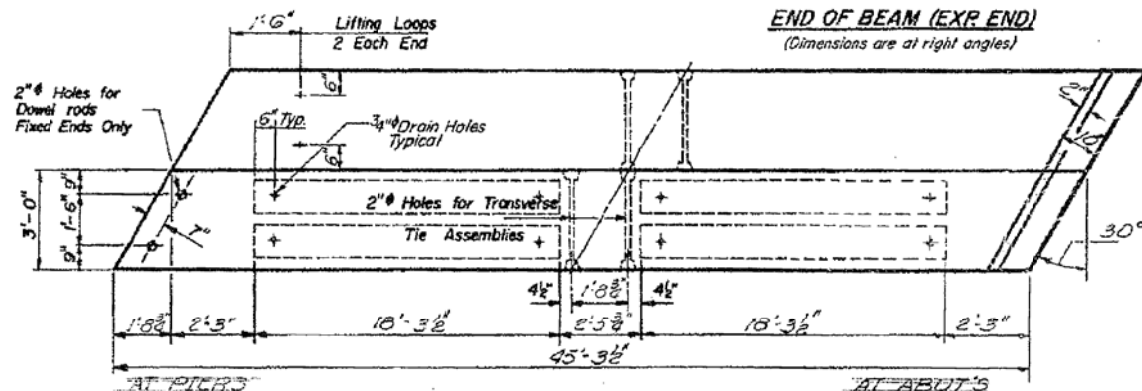
7/8" Strands Each Strand Stressed to 21,700 lbs.
13 Strands 1 1/2" up



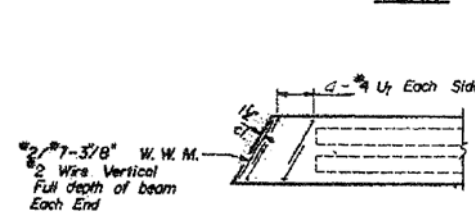
END OF BEAM (EXP. END)
(Dimensions are at right angles)



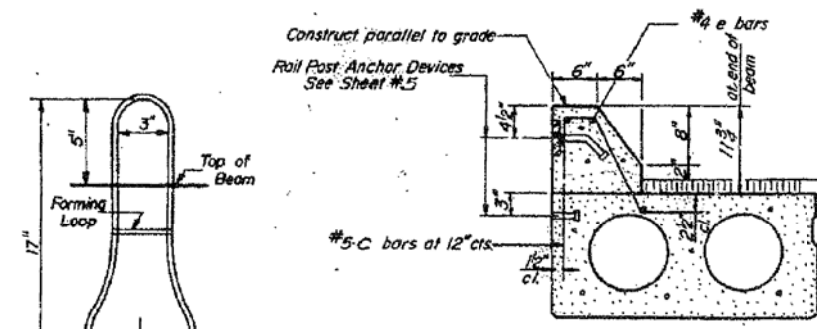
HALF CROSS SECTION



PLAN



END PLAN (Fixed End)



SECTION THRU CURB

Curbs shall be poured in the field. Class X Concrete & #4 bars for curbs are billed on Sheet #5

GENERAL NOTES

Prestressing steel shall be non-galvanized high strength, stress-relieved 7-wire strand, Grade 270. The nominal diameter shall be 7/8" and the nominal cross-sectional area shall be 0.115 sq. in.. Lifting loops shall be 3/4" diameter, 6x25 class wire rope with fiber core and shall have a minimum ultimate tensile strength of 33,000 lbs. The 1" rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets that receive transverse tie bar on outside beam shall be filled with grout after transverse tie assembly is in place. Longitudinal shear keys shall be packed with a very dry mix of 2:1 sand and P.C. mortar. After beams have been erected, holes for the dowel anchors shall be drilled into the sub-structure and the anchor dowels shall be grouted in place.

After fabrication the transverse tie assemblies (ie rods, nuts, washers and sleeves) shall be hot-dipped galvanized in accordance with AASHTO M-232. Cost of reinforcement and accessories cast into the beam, or bearing pads, or dowel rods and of grouting longitudinal shear keys is included in unit price bid for "Precast Prestressed Concrete Deck Beams."

BILL OF MATERIAL

Q	G	#5	37' 10"	
Precast Prestressed Concrete Deck Beams (27)	Sq. Ft.	1,989		
Class X Concrete	Cu. Yds	6.8		
Reinforcement Bars	Lbs.	270		

FOR INFORMATION ONLY

5L SUPERSTRUCTURE
SPANS 1 & 3
S.B.I. R.T. 6 SEC. 60V-1
KNOX COUNTY
STA. 60+15.00

DESIGNED: *James Kimm*
CHECKED: *James Kimm*
DRAWN: J. SCHNELLER
CHECKED: JP
EXAMINED: *Nov 8 1971*
PASSED: _____
APPROVED: _____
DIRECTOR OF HIGHWAYS

FD-1-L 11-19-65 Rev. 6-20-69
SR-2 (5-15-71)



USER NAME =	DESIGNED - GBR	REVISED -
PLOT SCALE =	CHECKED - MAH	REVISED -
PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

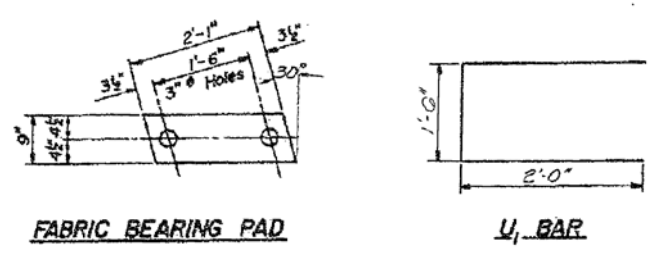
EXISTING SUPERSTRUCTURE
1971 PLANS (FOR INFORMATION ONLY)

SHEET NO. 5 OF 6 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	97
			CONTRACT NO. 68800	
ILLINOIS FED. AID PROJECT				

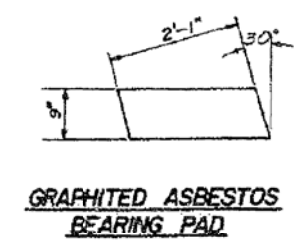
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEETS
40V-L	KNOX	5	5	3	
SHEET NO. 3					

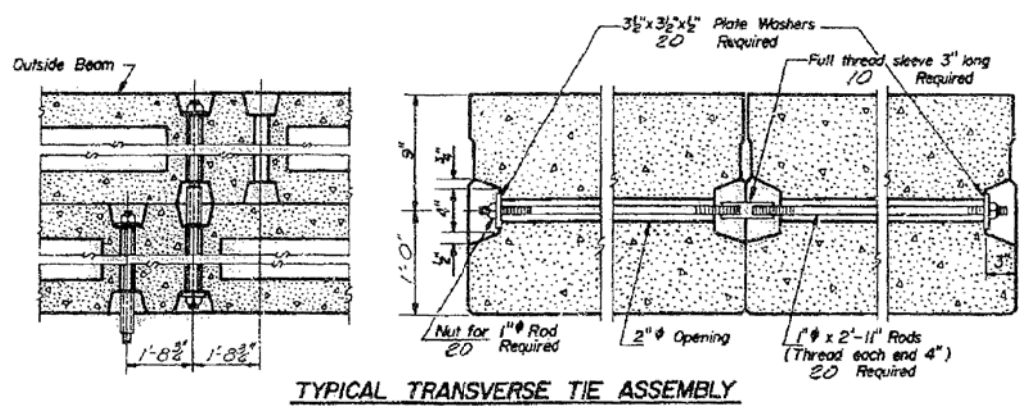


FABRIC BEARING PAD

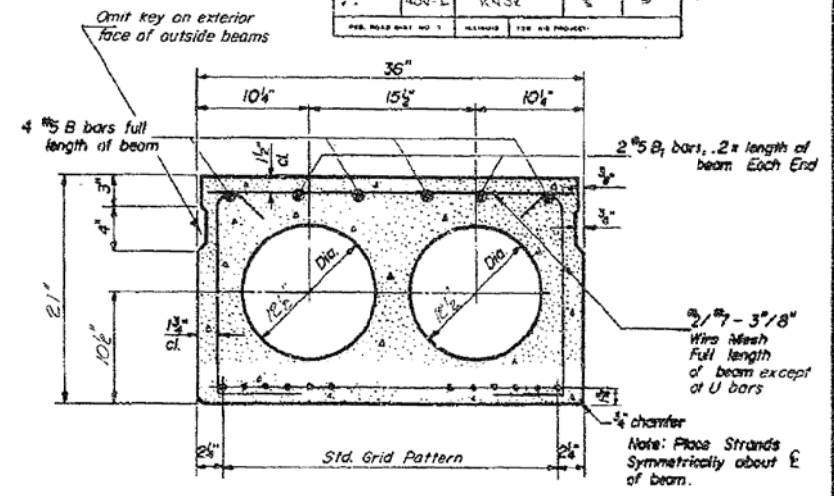
U BAR



GRAPHITED ASBESTOS BEARING PAD

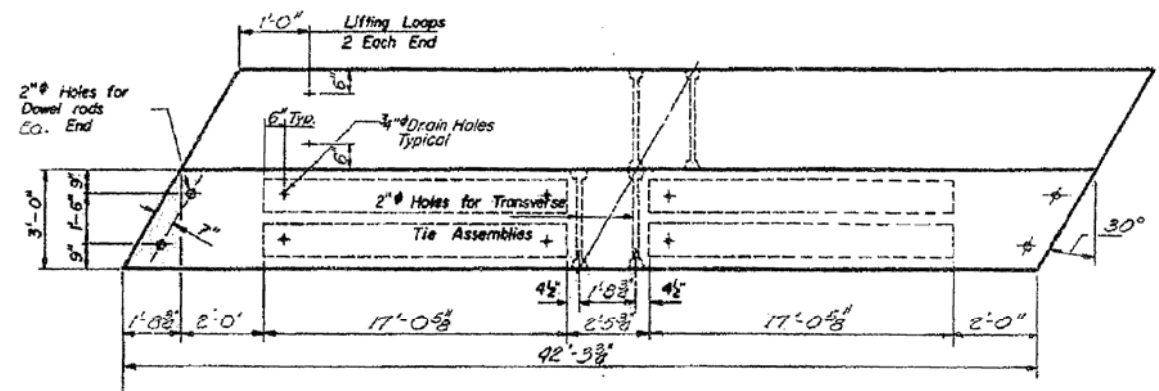


TYPICAL TRANSVERSE TIE ASSEMBLY

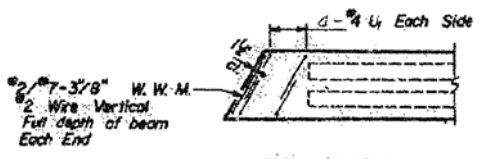


TYPICAL SECTION

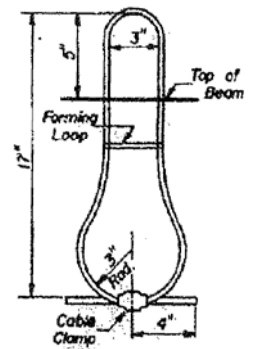
7/8" Strands Each Strand Stressed to 21,700 lbs.
12 Strands 1 3/4" up



PLAN



END PLAN



LIFTING LOOP DETAIL

GENERAL NOTES

Prestressing steel shall be non-galvanized high strength, stress-relieved 7-wire strand, Grade E70. The nominal diameter shall be 7/8" and the nominal cross-sectional area shall be 0.115 sq. in. Lifting loops shall be 1/2" diameter, 6 x 25 class wire rope with fiber core and shall have a minimum ultimate tensile strength of 21,000 lbs. The 1" rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets that receive transverse tie bar on outside beam shall be filled with grout after transverse tie assembly is in place. Longitudinal shear keys shall be packed with a very dry mix of 2-1 sand and P.C. mortar. After beams have been erected, holes for the dowel anchors shall be drilled into the sub-structure and the anchor dowels shall be grouted in place.

After fabrication the transverse tie assemblies (tie rods, nuts, washers and sleeves) shall be hot-dipped galvanized in accordance with AASHTO M-23E. Cost of reinforcement and accessories cast into the beam, of bearing pads, of dowel rods and of grouting longitudinal shear keys is included in unit price bid for Precast Prestressed Concrete Deck Beams.

BILL OF MATERIAL

Item	Quantity	Unit	Price
Precast Prestressed Concrete Deck Beams (2")		Sq. Ft.	13.96

DESIGNED	John Schneller	EXAMINED	May 5 1971
CHECKED	James Deuel	PASSED	
DRAWN	J. SCHNELLER	APPROVED	
CHECKED	JP		

FOR INFORMATION ONLY

SUPERSTRUCTURE
SPAN 2
S.B.F. 878 SEC. 40V-L
KNOX COUNTY
STA. 6675.00



USER NAME =	DESIGNED - GBR	REVISED -
PLOT SCALE =	CHECKED - MAH	REVISED -
PLOT DATE =	DRAWN - JRP	REVISED -
	CHECKED - GBR	REVISED -

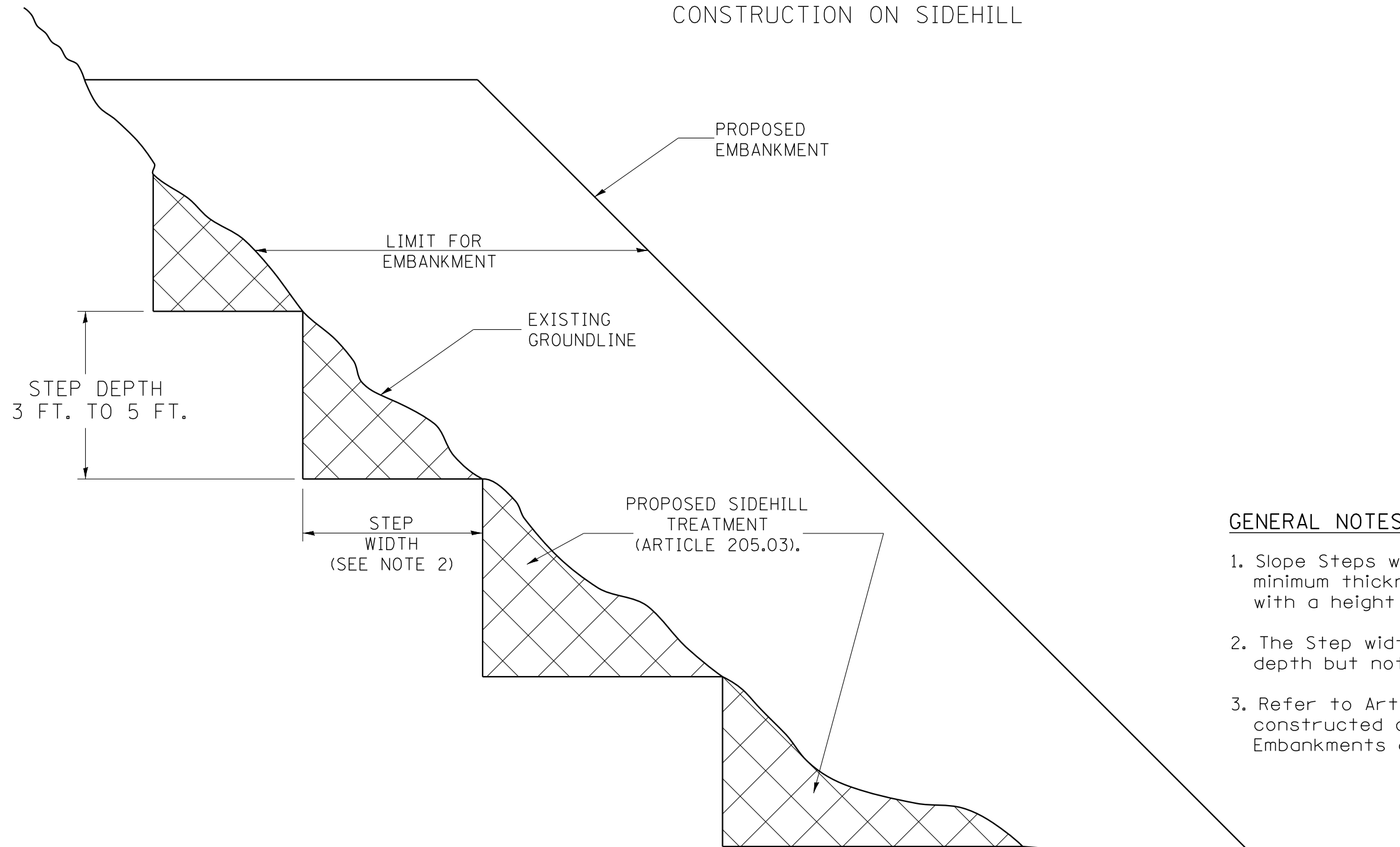
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING SUPERSTRUCTURE
1971 PLANS (FOR INFORMATION ONLY)

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2401	(40V-1)BR	KNOX	146	98
CONTRACT NO. 68800				
ILLINOIS FED. AID PROJECT				

SLOPE STEPS DETAIL

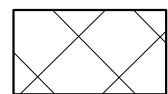
TYPICAL CROSS-SECTION EMBANKMENT CONSTRUCTION ON SIDEHILL



GENERAL NOTES:

1. Slope Steps will be required for all 12(300) minimum thickness "silver fills" and on a fills with a height of 10'(3.0m).
2. The Step width shall be twice the Step depth but not less than 6 feet.
3. Refer to Article 205.03 for Embankment to be constructed on Hillside or Slopes, or if existing Embankments are to be widened.

REPLACEMENT MATERIAL:



STANDARD EMBANKMENT
(IN ACCORDANCE WITH
205 OF THE STANDARD SPECIFICATION).

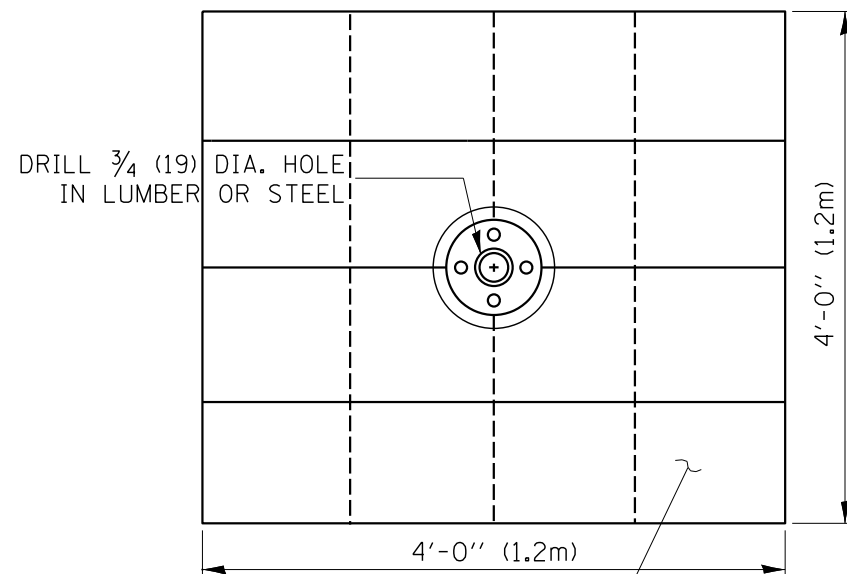
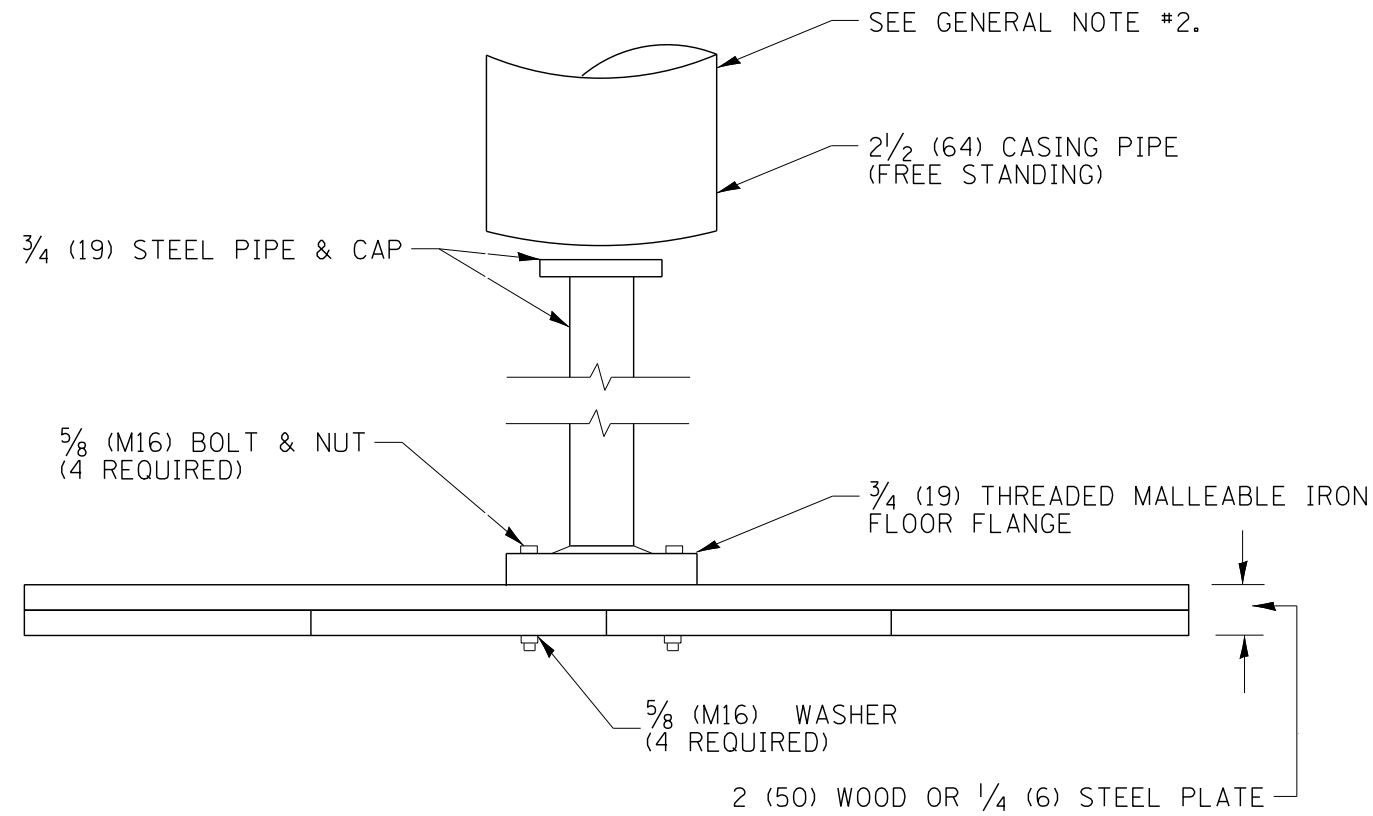
All dimensions are in inches (millimeters)
unless otherwise noted.

1-1-97	RENUM. L-5.03, NEW REVISION BOX, REVISED TITLE BOX, REVISED GENERAL NOTES.	T.P.				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SLOPE STEPS DETAIL	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
10-16-06	REVISED TO 2007 SPEC.	M.A.						2401	(40V-1)BR	KNOX	146	99
CONTRACT NO. 68800												

NOT TO SCALE

CADD STD. 205001-D4

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



SOUND LUMBER - 1(25) x 12(300) NAILED TOGETHER OR 1/4(6) THICK BY 4'(1.2m) SQUARE STEEL PLATE

GENERAL NOTES:

1. Settlement Platform shall be in accordance with the applicable portions of Article 204.06 of the Standard Specifications.
2. Do Not install casing pipe until after one section of 3/4"(19 mm) has been covered with earth. The casing pipe should not rest on platform.

All dimensions are in inches (millimeters) unless otherwise noted.

1-1-97	RENUM. L-5.04, NEW REVISION BOX, REVISED NOTES, REVISED TITLE BOX	T.P.	8-23-01	UPDATE FOR NEW SPEC.	M.A.	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SETTLEMENT PLATFORM	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4-14-99	ADDED "CASING PIPE" REQUIREMENT	J.A.	10-16-06	REVISED TO 2007 SPEC.	M.A.			2401	(40V-1)BR	KNOX	146	100
5-19-99	CORRECTIONS TO CASING PIPE	J.A.						CONTRACT NO. 68800				
							NOT TO SCALE	CADD STD. 205101-D4	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			