

# DUPAGE AIRPORT AUTHORITY

## WEST CHICAGO, ILLINOIS

### CONSTRUCTION PLANS

### FOR

### DUPAGE AIRPORT

### AIRPORT PERIMETER ROAD RESURFACING - WEST TOWER ROAD AND DUPAGE DRIVE

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ILLINOIS PROJECT: DPA-4979

MARCH 3, 2023

#### DUPAGE AIRPORT

TOWNSHIP: 40 NORTH      WAYNE TOWNSHIP  
 RANGE: 9 EAST          (SECTIONS: 31)  
 DUPAGE COUNTY



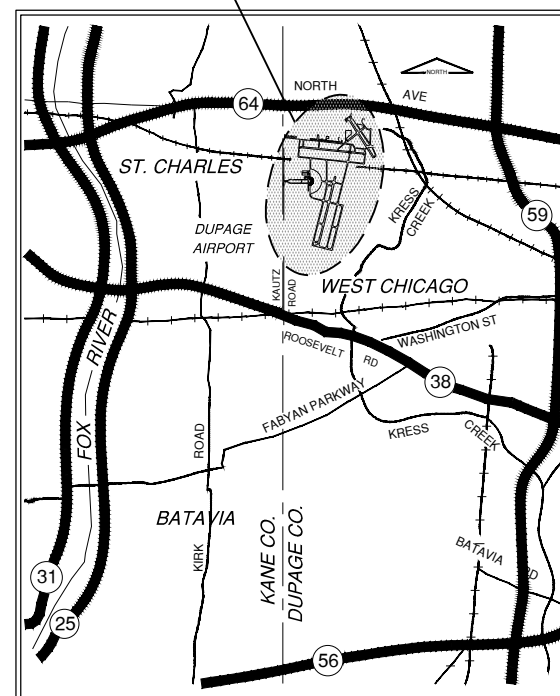
Know what's below.  
 Call before you dig.

J.U.L.I.E.  
 JOINT UTILITY LOCATING  
 INFORMATION FOR EXCAVATORS  
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

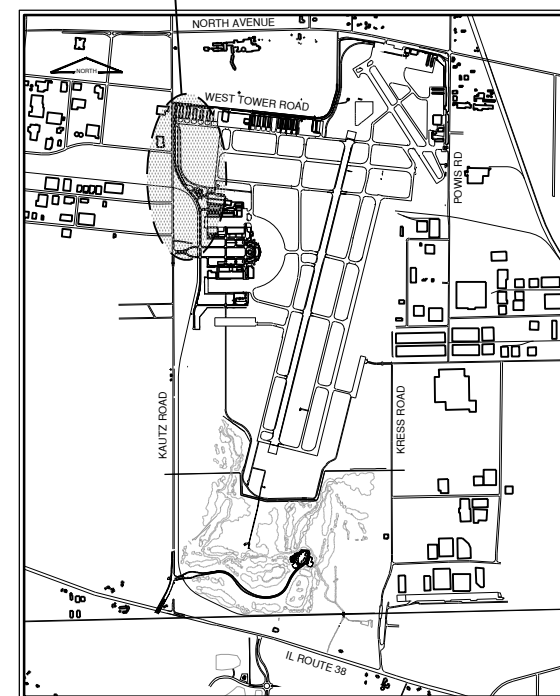
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

PROJECT  
 LOCATION



LOCATION MAP

PROJECT  
 LOCATION



SITE PLAN



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 CONSULTING ENGINEERS  
 License No. 184-000613

21001642.00



SUBMITTED BY *Daniel L. Pape*

DANIEL L. PAPE

DATE MARCH 2, 2023



DuPage Airport

2700 INTERNATIONAL DRIVE  
 SUITE 200  
 WEST CHICAGO, IL 60185

APPROVED BY *Mark Doles*

MARK DOLES  
 EXECUTIVE DIRECTOR

DATE March 2, 2023

## STORM WATER POLLUTION PREVENTION NOTES:

THE FOLLOWING IS THE ESTABLISHED BEST MANAGEMENT PRACTICES TO BE INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLANS CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

### SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING A MILL AND OVERLAY ON WEST TOWER ROAD AND DUPAGE DRIVE AT DUPAGE AIRPORT. THE PROJECT INCLUDES PAVEMENT REHABILITATION AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES:

1. INSTALL TEMPORARY EROSION CONTROL MEASURES.
2. PAVEMENT MILLING AND PAVEMENT REMOVAL/REPLACEMENT.
3. CONSTRUCT NEW OVERLAYS.
4. REMOVAL AND DISPOSAL OF TEMPORARY EROSION CONTROL MEASURES.

### AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS APPROXIMATELY 3.0 ACRES OF WHICH 0.20 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

### DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO STORM SEWER SYSTEMS THAT OUTLET INTO KRESS CREEK.

### SEDIMENTATION AND EROSION CONTROL NOTES

THE RESIDENT ENGINEER WILL PERFORM PERIODIC INSPECTION OF THE SITE TO IDENTIFY POTENTIAL SEDIMENT AND EROSION ISSUES.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED WITHIN SEVEN (7) DAYS OF DISTURBANCE.

### DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
  - A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
  - B. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
5. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF OFF SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.
6. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

### DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

### MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

## SOIL EROSION AND SEDIMENT CONTROL NOTES:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, DUPAGE COUNTY, DUPAGE AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

## STOCKPILING/STAGING AND PROTECTION NOTES

1. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE STORM WATER POLLUTION PREVENTION DEVICES AT THE STAGING AND STOCKPILING AREA. THESE DEVICES, INCLUDING INLET PROTECTIONS, SILT FENCE, BALES, DITCH CHECKS, STABILIZED CONSTRUCTION ENTRANCES, ETC., SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
2. AT THE COMPLETION OF WORK, THE STAGING AND STOCKPILING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS, AT NO ADDITIONAL COST TO THE CONTRACT. THIS WORK SHALL INCLUDE REMOVING ANY AND ALL STORM WATER POLLUTION PREVENTION DEVICES, RESTORING RUTTED AREAS, SEEDING AND MULCHING.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE STAGING AND STOCKPILING AREA(S) IN A MANNER TO PREVENT POLLUTION AND SILTATION OF THE EXISTING STORM SEWER SYSTEM.
4. ALL EQUIPMENT FUELING AND GREASING SHALL BE COMPLETED AT THE STAGING AREA. SPILLS SHALL BE IMMEDIATELY CONTAINED AND THE AREA CLEANED AT NO ADDITIONAL COST THE CONTRACT.
5. ALL EXCESS MATERIAL DEPOSITED AT THE STOCKPILE AREA SHALL BE DUMPED, INTEGRATED INTO THE EXISTING STOCKPILE, AND GRADED TO DRAIN AS APPROVED BY DPAA. THIS WORK SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150520	MOBILIZATION	L SUM	1	
AR152410	UNCLASSIFIED EXCAVATION	CU YD	294	
AR152540	SOIL STABILIZATION FABRIC	SQ YD	840	
AR156520	INLET PROTECTION	EACH	48	
AR208515	POROUS GRANULAR EMBANKMENT	CU YD	294	
AR208604	4" AGGREGATE BASE COURSE	SQ YD	140	
AR208606	6" AGGREGATE BASE COURSE	SQ YD	895	
AR209655	AGGREGATE BASE REMOVAL	SQ YD	1,035	
AR401610	BITUMINOUS SURFACE COURSE	TON	1,797	
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	14,872	
AR401915	REM & REP BIT PAVEMENT - TYPE A	SQ YD	144	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SQ YD	895	
AR603510	BITUMINOUS TACK COAT	GALLON	2,270	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ FT	3,380	

IL CONTRACT: **DU090**

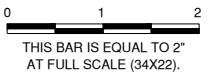
IL LETTING ITEM: **02A**

IL PROJECT: **DPA-4979**

SURVEY BOOK #

### REVISIONS

NUMBER	BY	DATE




**DUPAGE AIRPORT**  
**WEST CHICAGO, ILLINOIS**  
**AIRPORT PERIMETER ROAD RESURFACING -**  
**WEST TOWER ROAD AND DUPAGE DRIVE**


## SUMMARY OF QUANTITIES GENERAL NOTES

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dupageproj\_P\dupage - bdp  
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CONSULTING ENGINEERS  
License No. 184-000613



**DUPAGE AIRPORT**  
**AUTHORITY**

DESIGN BY:	JJM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21001642.00

**FINAL**

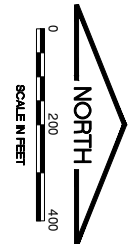
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 6/11/2021 10:30:15 AM  
 6/11/2021 10:30:15 AM

HORIZONTAL CONTROL			
POINT	DESCRIPTION	NORTHING	EASTING
PT#2	IRON ROD	1908491.67	520300.27
PT#3	IRON ROD	1908405.16	519822.40

HORIZONTAL CONTROL COORDINATES EXPRESSED IN NAD27.

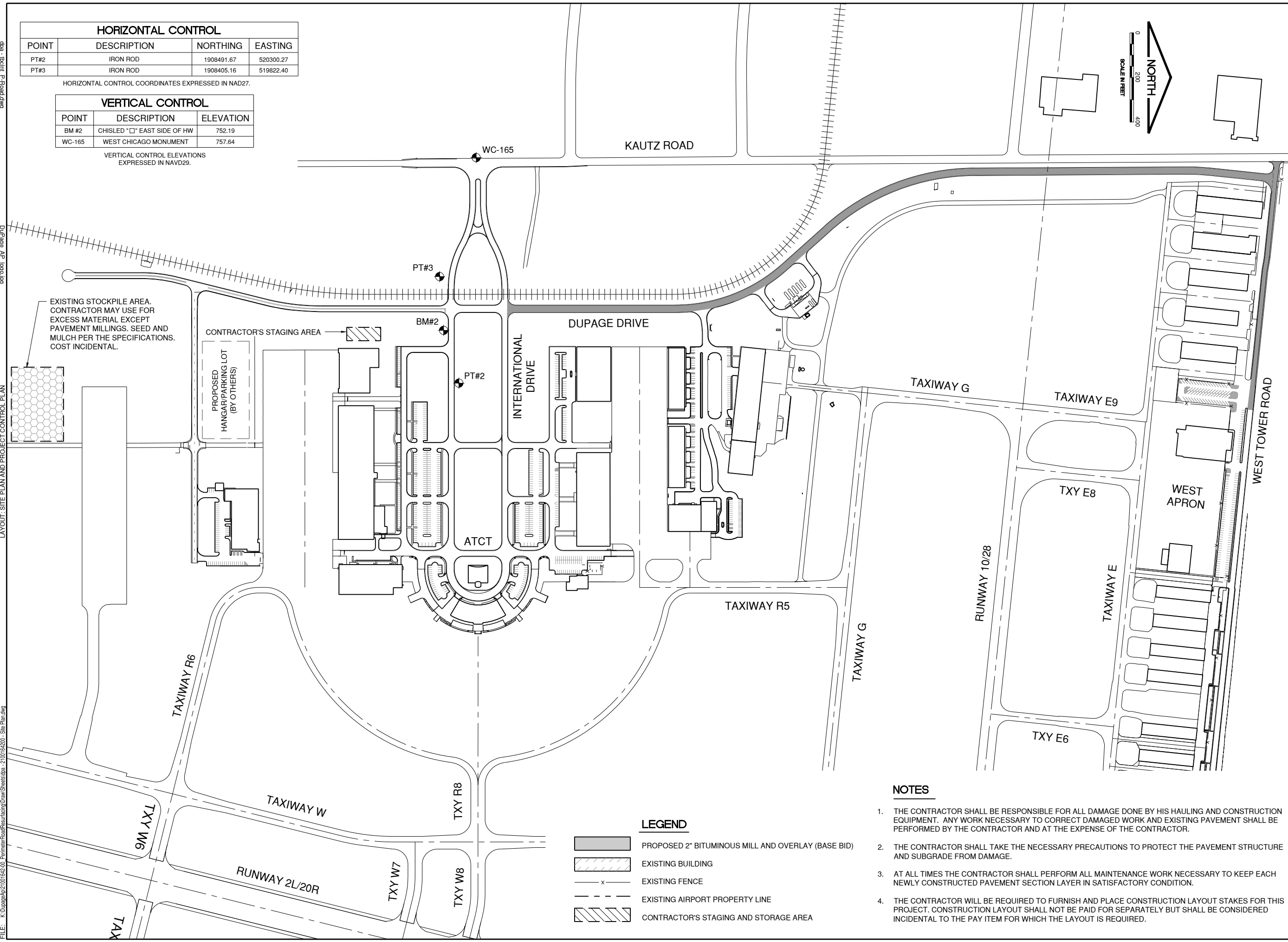
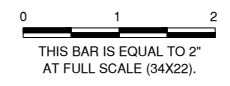
VERTICAL CONTROL		
POINT	DESCRIPTION	ELEVATION
BM #2	CHISLED "C" EAST SIDE OF HW	752.19
WC-165	WEST CHICAGO MONUMENT	757.64

VERTICAL CONTROL ELEVATIONS EXPRESSED IN NAVD29.



IL CONTRACT: **DU090**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **DPA-4979**

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EXISTING STOCKPILE AREA. CONTRACTOR MAY USE FOR EXCESS MATERIAL EXCEPT PAVEMENT MILLINGS, SEED AND MULCH PER THE SPECIFICATIONS. COST INCIDENTAL.

CONTRACTOR'S STAGING AREA

PROPOSED HANGAR/PARKING LOT (BY OTHERS)

LEGEND	
	PROPOSED 2" BITUMINOUS MILL AND OVERLAY (BASE BID)
	EXISTING BUILDING
	EXISTING FENCE
	EXISTING AIRPORT PROPERTY LINE
	CONTRACTOR'S STAGING AND STORAGE AREA

**NOTES**

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
2. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE.
3. AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN SATISFACTORY CONDITION.
4. THE CONTRACTOR WILL BE REQUIRED TO FURNISH AND PLACE CONSTRUCTION LAYOUT STAKES FOR THIS PROJECT. CONSTRUCTION LAYOUT SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM FOR WHICH THE LAYOUT IS REQUIRED.

**DUPAGE AIRPORT**  
 WEST CHICAGO, ILLINOIS  
**AIRPORT PERIMETER ROAD RESURFACING -**  
 WEST TOWER ROAD AND DUPAGE DRIVE  
**SITE PLAN AND PROJECT CONTROL PLAN**

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**DUPAGE AIRPORT**  
 AUTHORITY

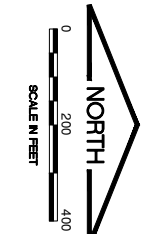
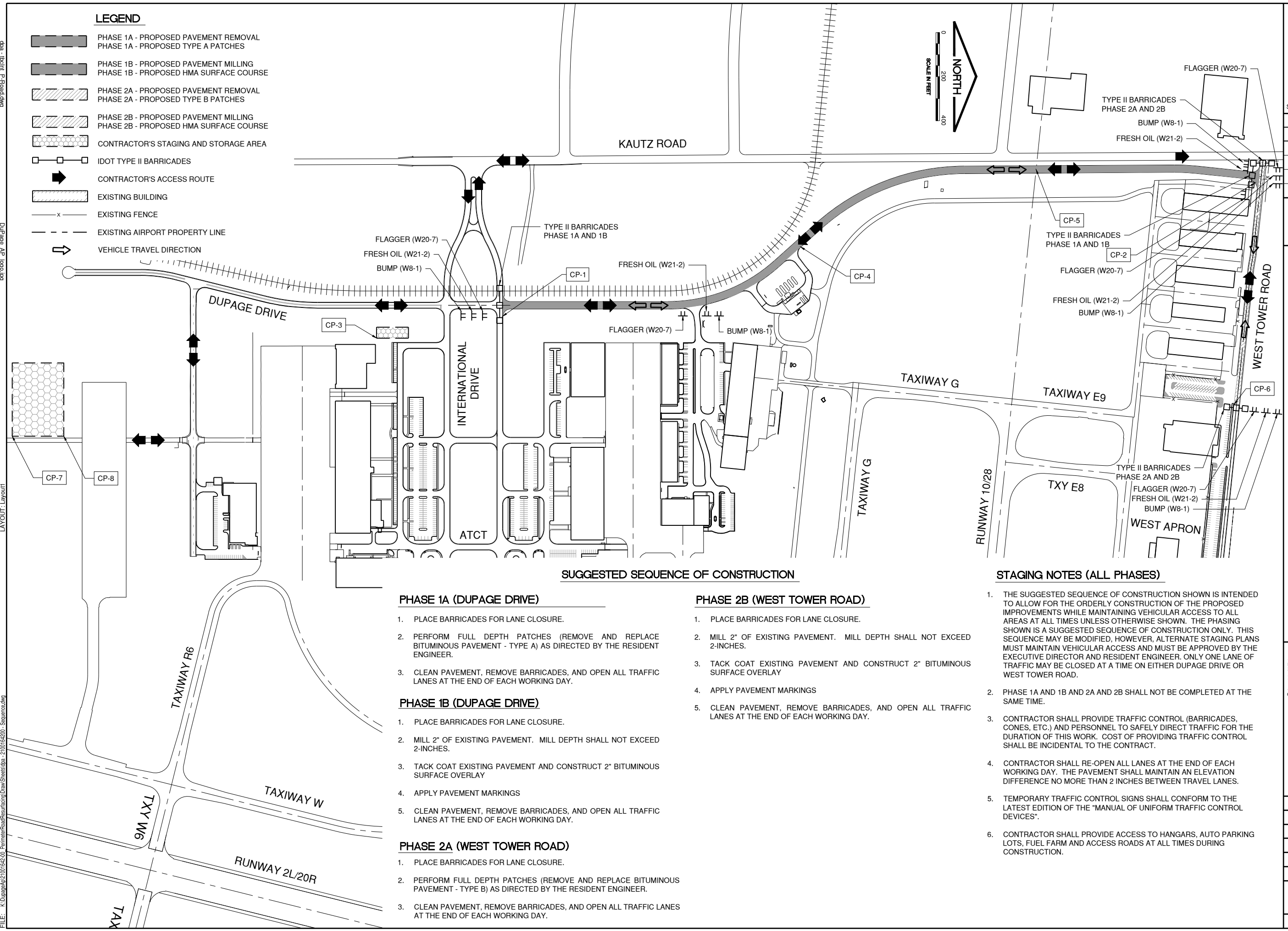
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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21001642.00

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 6/9/2021 10:49 AM

**LEGEND**

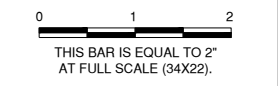
- PHASE 1A - PROPOSED PAVEMENT REMOVAL
- PHASE 1A - PROPOSED TYPE A PATCHES
- PHASE 1B - PROPOSED PAVEMENT MILLING
- PHASE 1B - PROPOSED HMA SURFACE COURSE
- PHASE 2A - PROPOSED PAVEMENT REMOVAL
- PHASE 2A - PROPOSED TYPE B PATCHES
- PHASE 2B - PROPOSED PAVEMENT MILLING
- PHASE 2B - PROPOSED HMA SURFACE COURSE
- CONTRACTOR'S STAGING AND STORAGE AREA
- IDOT TYPE II BARRICADES
- CONTRACTOR'S ACCESS ROUTE
- EXISTING BUILDING
- EXISTING FENCE
- EXISTING AIRPORT PROPERTY LINE
- VEHICLE TRAVEL DIRECTION



IL CONTRACT: **DU090**  
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**SUGGESTED SEQUENCE OF CONSTRUCTION**

**PHASE 1A (DUPAGE DRIVE)**

1. PLACE BARRICADES FOR LANE CLOSURE.
2. PERFORM FULL DEPTH PATCHES (REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A) AS DIRECTED BY THE RESIDENT ENGINEER.
3. CLEAN PAVEMENT, REMOVE BARRICADES, AND OPEN ALL TRAFFIC LANES AT THE END OF EACH WORKING DAY.

**PHASE 1B (DUPAGE DRIVE)**

1. PLACE BARRICADES FOR LANE CLOSURE.
2. MILL 2" OF EXISTING PAVEMENT. MILL DEPTH SHALL NOT EXCEED 2-INCHES.
3. TACK COAT EXISTING PAVEMENT AND CONSTRUCT 2" BITUMINOUS SURFACE OVERLAY
4. APPLY PAVEMENT MARKINGS
5. CLEAN PAVEMENT, REMOVE BARRICADES, AND OPEN ALL TRAFFIC LANES AT THE END OF EACH WORKING DAY.

**PHASE 2A (WEST TOWER ROAD)**

1. PLACE BARRICADES FOR LANE CLOSURE.
2. PERFORM FULL DEPTH PATCHES (REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B) AS DIRECTED BY THE RESIDENT ENGINEER.
3. CLEAN PAVEMENT, REMOVE BARRICADES, AND OPEN ALL TRAFFIC LANES AT THE END OF EACH WORKING DAY.

**PHASE 2B (WEST TOWER ROAD)**

1. PLACE BARRICADES FOR LANE CLOSURE.
2. MILL 2" OF EXISTING PAVEMENT. MILL DEPTH SHALL NOT EXCEED 2-INCHES.
3. TACK COAT EXISTING PAVEMENT AND CONSTRUCT 2" BITUMINOUS SURFACE OVERLAY
4. APPLY PAVEMENT MARKINGS
5. CLEAN PAVEMENT, REMOVE BARRICADES, AND OPEN ALL TRAFFIC LANES AT THE END OF EACH WORKING DAY.

**STAGING NOTES (ALL PHASES)**

1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING VEHICULAR ACCESS TO ALL AREAS AT ALL TIMES UNLESS OTHERWISE SHOWN. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED, HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN VEHICULAR ACCESS AND MUST BE APPROVED BY THE EXECUTIVE DIRECTOR AND RESIDENT ENGINEER. ONLY ONE LANE OF TRAFFIC MAY BE CLOSED AT A TIME ON EITHER DUPAGE DRIVE OR WEST TOWER ROAD.
2. PHASE 1A AND 1B AND 2A AND 2B SHALL NOT BE COMPLETED AT THE SAME TIME.
3. CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL (BARRICADES, CONES, ETC.) AND PERSONNEL TO SAFELY DIRECT TRAFFIC FOR THE DURATION OF THIS WORK. COST OF PROVIDING TRAFFIC CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.
4. CONTRACTOR SHALL RE-OPEN ALL LANES AT THE END OF EACH WORKING DAY. THE PAVEMENT SHALL MAINTAIN AN ELEVATION DIFFERENCE NO MORE THAN 2 INCHES BETWEEN TRAVEL LANES.
5. TEMPORARY TRAFFIC CONTROL SIGNS SHALL CONFORM TO THE LATEST EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
6. CONTRACTOR SHALL PROVIDE ACCESS TO HANGARS, AUTO PARKING LOTS, FUEL FARM AND ACCESS ROADS AT ALL TIMES DURING CONSTRUCTION.

**DUPAGE AIRPORT**  
**WEST CHICAGO, ILLINOIS**  
**AIRPORT PERIMETER ROAD RESURFACING -**  
**WEST TOWER ROAD AND DUPAGE DRIVE**  
**SEQUENCE OF CONSTRUCTION**

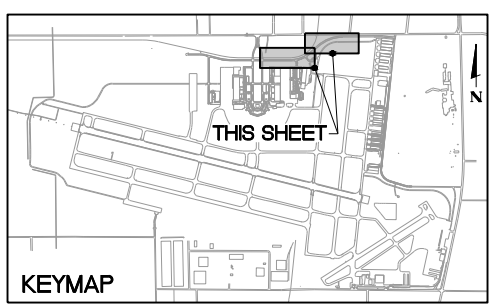
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 License No. 184-000613

**DUPAGE AIRPORT**  
**AUTHORITY**

DESIGN BY:	JJM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21001642.00

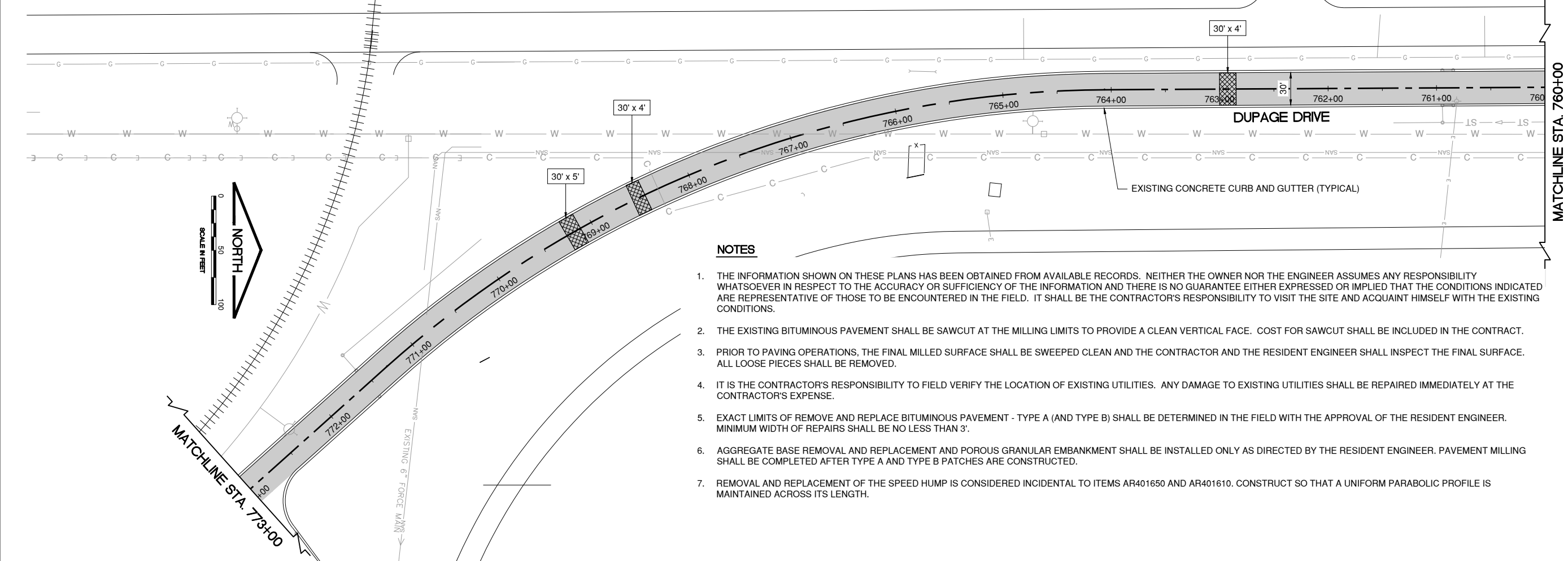
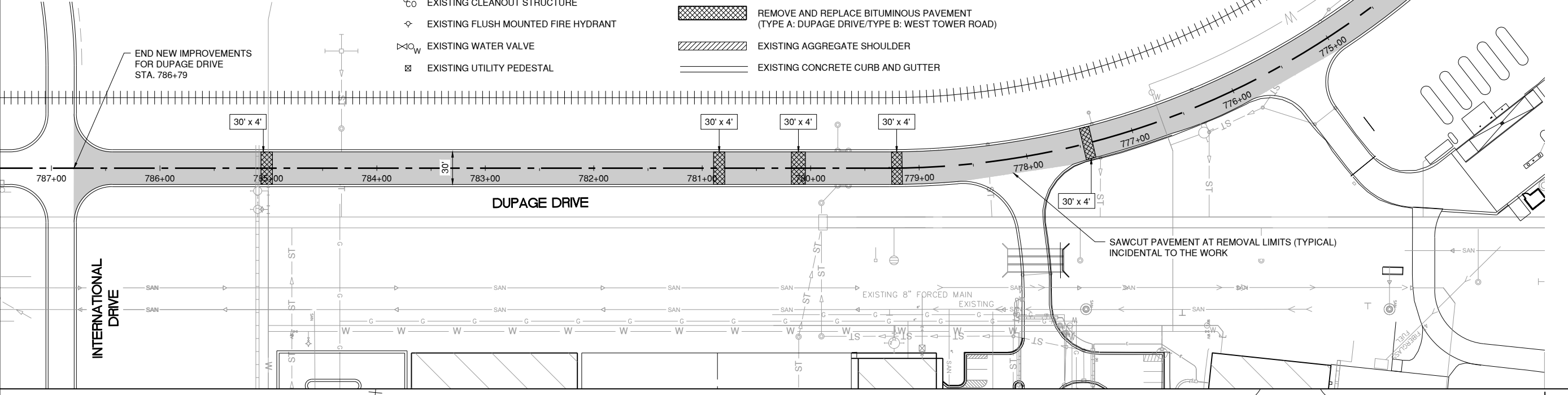
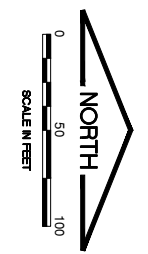


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**LEGEND**

- ⊙ EXISTING ELECTRICAL/STORM/SANITARY/TELEPHONE MANHOLE OR EXISTING WATER VALVE VAULT
- ⊠ EXISTING ELECTRICAL HANDHOLE
- EXISTING STORM INLET
- ▭ EXISTING SLOPE BOX
- ▷ EXISTING FLARED END SECTION
- ⌋ EXISTING HEADWALL
- ⊕ EXISTING CLEANOUT STRUCTURE
- ⚡ EXISTING FLUSH MOUNTED FIRE HYDRANT
- ⊕<sub>W</sub> EXISTING WATER VALVE
- ⊠ EXISTING UTILITY PEDESTAL
- ⊠ EXISTING UTILITY PEDESTAL
- ST EXISTING STORM SEWER
- UD EXISTING UNDERDRAIN
- W EXISTING WATERMAIN
- SAN EXISTING SANITARY SEWER
- - - EXISTING AIRPORT PROPERTY LINE
- ▭ HMA PAVEMENT MILLING - 2"
- ▨ REMOVE AND REPLACE BITUMINOUS PAVEMENT (TYPE A: DUPAGE DRIVE/TYPE B: WEST TOWER ROAD)
- ▨ EXISTING AGGREGATE SHOULDER
- ▬ EXISTING CONCRETE CURB AND GUTTER



**NOTES**

1. THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.
2. THE EXISTING BITUMINOUS PAVEMENT SHALL BE SAWCUT AT THE MILLING LIMITS TO PROVIDE A CLEAN VERTICAL FACE. COST FOR SAWCUT SHALL BE INCLUDED IN THE CONTRACT.
3. PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE SWEEPED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED.
4. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
5. EXACT LIMITS OF REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A (AND TYPE B) SHALL BE DETERMINED IN THE FIELD WITH THE APPROVAL OF THE RESIDENT ENGINEER. MINIMUM WIDTH OF REPAIRS SHALL BE NO LESS THAN 3'.
6. AGGREGATE BASE REMOVAL AND REPLACEMENT AND POROUS GRANULAR EMBANKMENT SHALL BE INSTALLED ONLY AS DIRECTED BY THE RESIDENT ENGINEER. PAVEMENT MILLING SHALL BE COMPLETED AFTER TYPE A AND TYPE B PATCHES ARE CONSTRUCTED.
7. REMOVAL AND REPLACEMENT OF THE SPEED HUMP IS CONSIDERED INCIDENTAL TO ITEMS AR401650 AND AR401610. CONSTRUCT SO THAT A UNIFORM PARABOLIC PROFILE IS MAINTAINED ACROSS ITS LENGTH.

IL. CONTRACT: **DU090**  
 IL. LETTING ITEM: **02A**  
 IL. PROJECT: **DPA-4979**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DUPAGE AIRPORT**  
**WEST CHICAGO, ILLINOIS**  
**AIRPORT PERIMETER ROAD RESURFACING -**  
**WEST TOWER ROAD AND DUPAGE DRIVE**  
**EXISTING CONDITIONS/PROPOSED REMOVALS 1**

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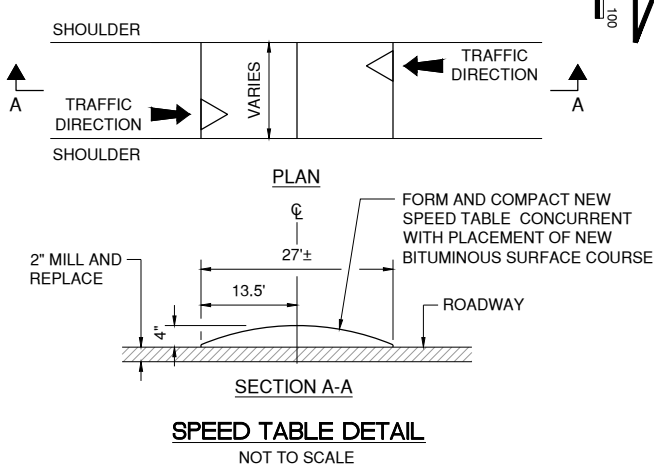
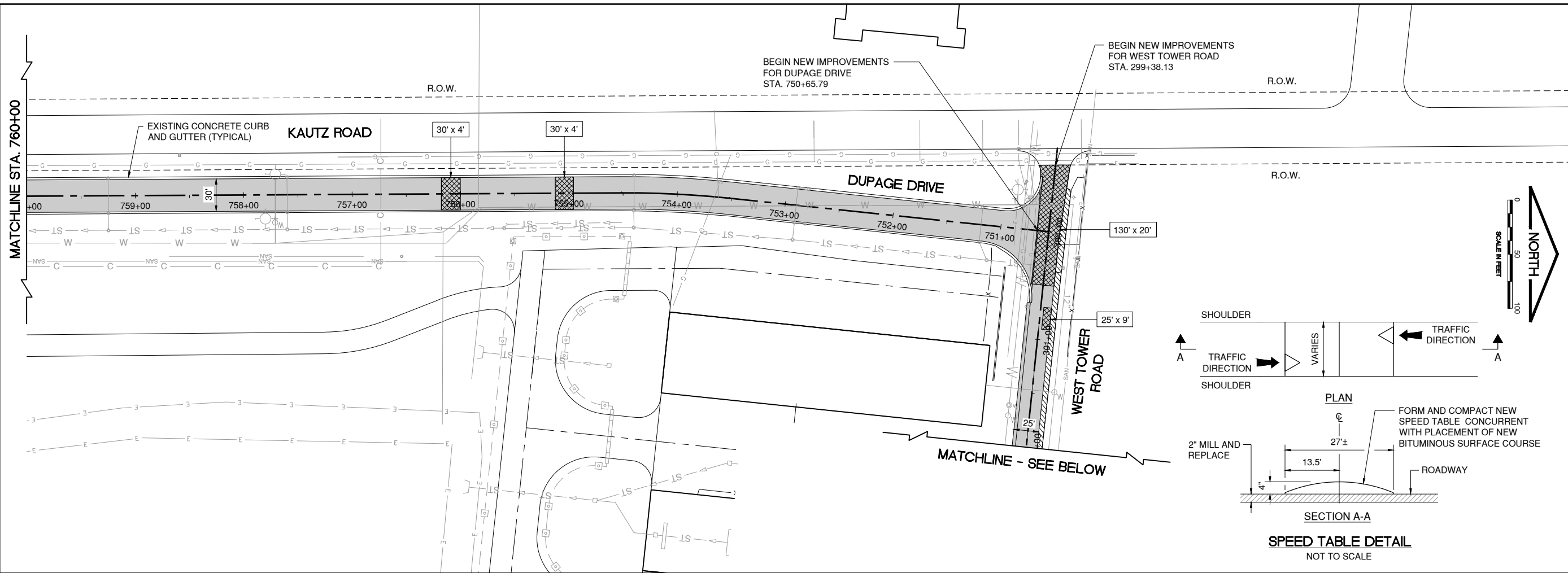
**DUPAGE AIRPORT**  
**AUTHORITY**

DESIGN BY:	JJM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21001642.00

**FINAL**

SHEET 6 OF 12 SHEETS

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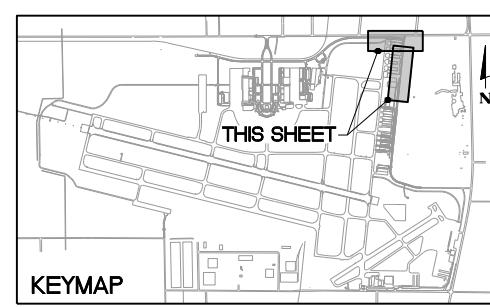


IL. CONTRACT: **DU090**  
 IL. LETTING ITEM: **02A**  
 IL. PROJECT: **DPA-4979**

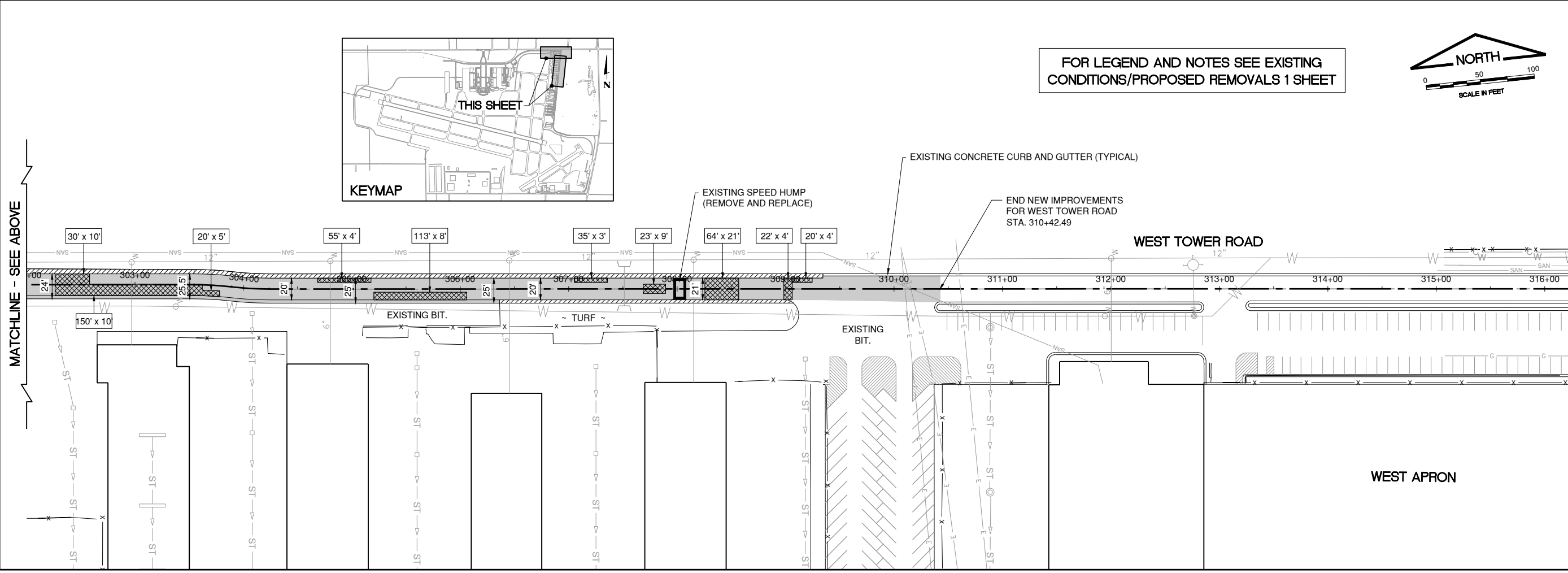
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REVISIONS		
NUMBER	BY	DATE

0 1 2  
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FOR LEGEND AND NOTES SEE EXISTING CONDITIONS/PROPOSED REMOVALS 1 SHEET



DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 AIRPORT PERIMETER ROAD RESURFACING -  
 WEST TOWER ROAD AND DUPAGE DRIVE  
**EXISTING CONDITIONS/PROPOSED REMOVALS 2**

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**DUPAGE AIRPORT AUTHORITY**

DESIGN BY:	JJM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21001642.00

**FINAL**

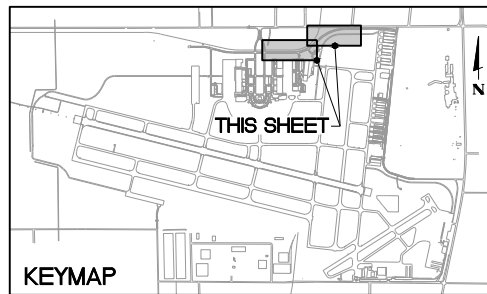
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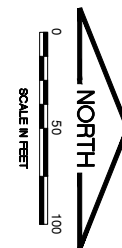
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**NOTES**

1. INSTALL HMA WEDGE AS DIRECTED BY THE RESIDENT ENGINEER TO MINIMIZE PAVEMENT ELEVATION DIFFERENCE FROM THE MILLED AND EXISTING PAVEMENT SURFACE. THE HMA WEDGE SHALL EXTEND FULL WIDTH OF THE TRAVEL LANE. COST OF HMA WEDGE SHALL BE INCIDENTAL TO THE CONTRACT.
2. COORDINATE STOP BAR LOCATIONS IN FIELD. LOCATIONS SHALL BE LAID OUT AND APPROVED BY THE ENGINEER PRIOR TO MARKING THE STOP BAR.



IL CONTRACT: **DU090**  
IL LETTING ITEM: **02A**  
IL PROJECT: **DPA-4979**

SURVEY BOOK #

**REVISIONS**

NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DUPAGE AIRPORT  
WEST CHICAGO, ILLINOIS  
AIRPORT PERIMETER ROAD RESURFACING -  
WEST TOWER ROAD AND DUPAGE DRIVE

**PROPOSED IMPROVEMENTS 1**

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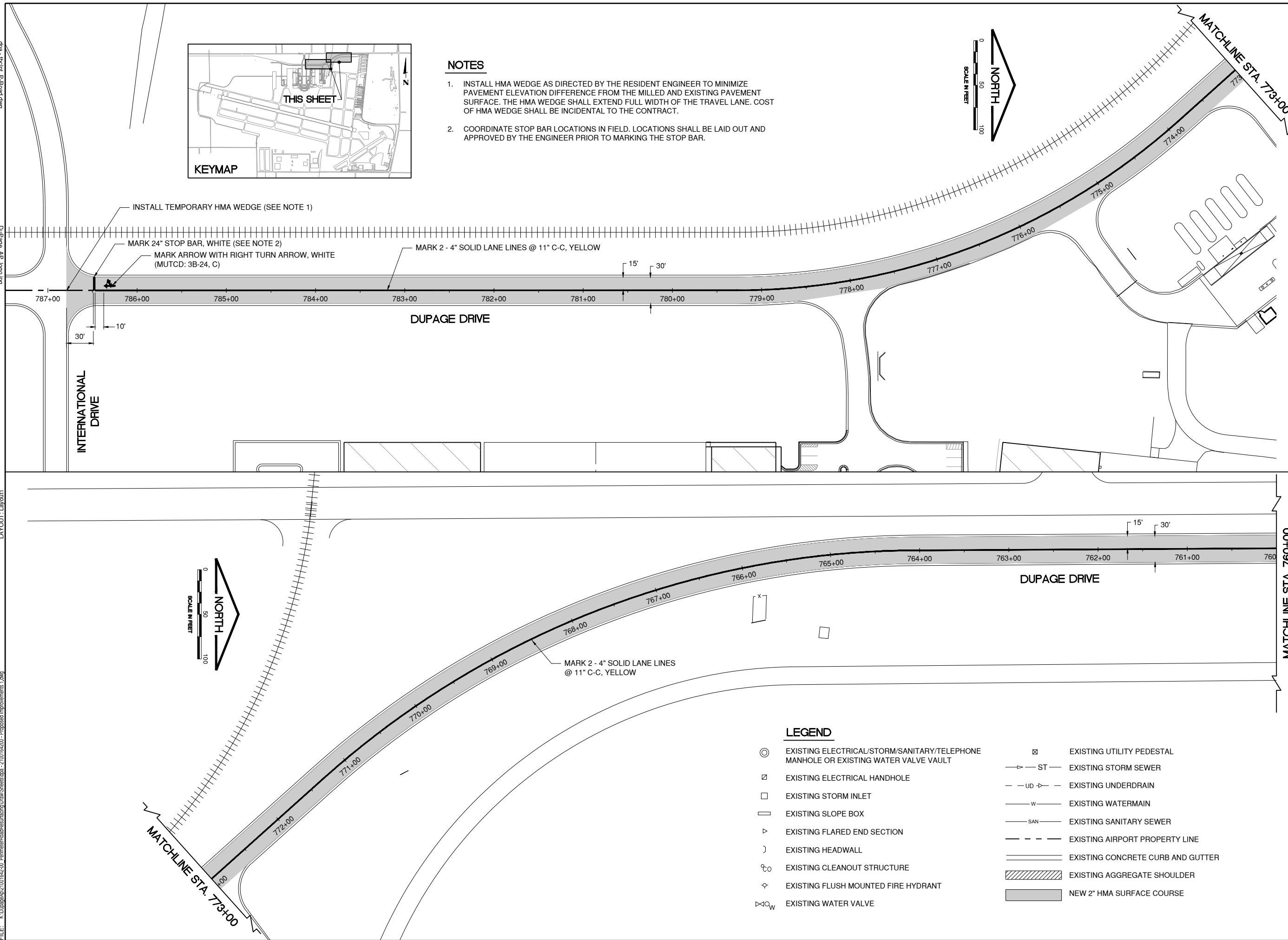
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DATE: 03/03/2023

JOB No: 21001642.00

**FINAL**

SHEET 8 OF 12 SHEETS



**LEGEND**

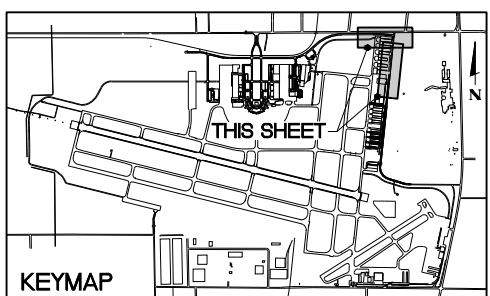
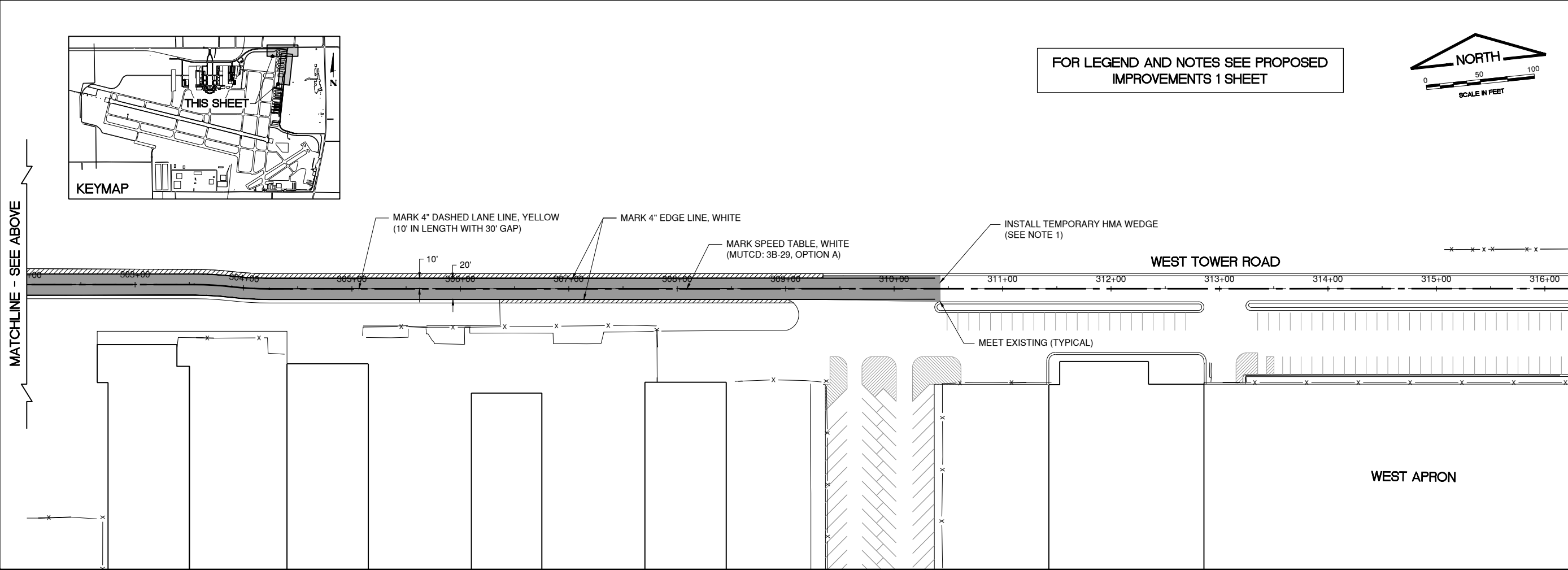
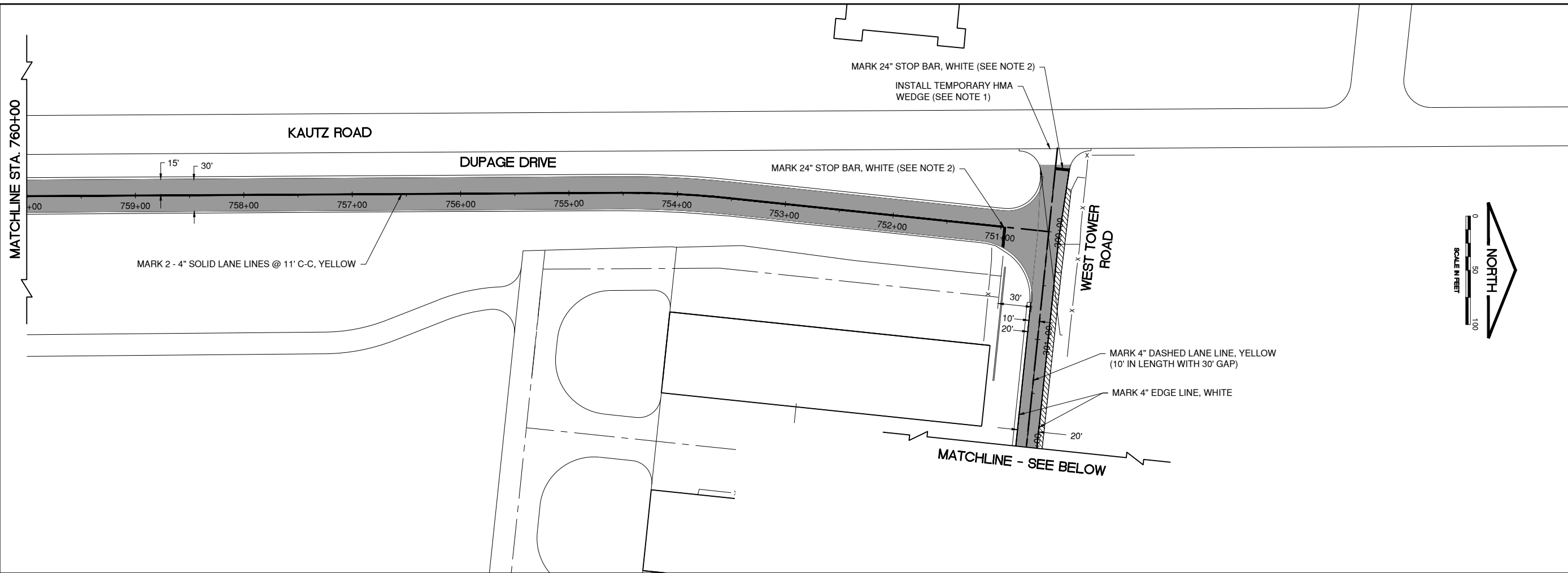
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|---|--|------------|-----------------------------------|
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| ⊠ | EXISTING ELECTRICAL HANDHOLE   | — ST —     | EXISTING STORM SEWER              |
| □ | EXISTING STORM INLET   | - - UD - - | EXISTING UNDERDRAIN               |
| ▭ | EXISTING SLOPE BOX   | — W —      | EXISTING WATERMAIN                |
| ▷ | EXISTING FLARED END SECTION  | — SAN —    | EXISTING SANITARY SEWER           |
| ) | EXISTING HEADWALL  | - - - - -  | EXISTING AIRPORT PROPERTY LINE    |
| ⊕ | EXISTING CLEANOUT STRUCTURE  | ▬▬▬▬▬▬     | EXISTING CONCRETE CURB AND GUTTER |
| ⊕ | EXISTING FLUSH MOUNTED FIRE HYDRANT  | ▨▨▨▨▨▨     | EXISTING AGGREGATE SHOULDER       |
| ⊕ | EXISTING WATER VALVE   | ▬▬▬▬▬▬     | NEW 2" HMA SURFACE COURSE         |



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 2021.03.03  
 11:58:42 AM



FOR LEGEND AND NOTES SEE PROPOSED IMPROVEMENTS 1 SHEET

IL CONTRACT: **DU090**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **DPA-4979**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 AIRPORT PERIMETER ROAD RESURFACING -  
 WEST TOWER ROAD AND DUPAGE DRIVE

**PROPOSED IMPROVEMENTS 2**

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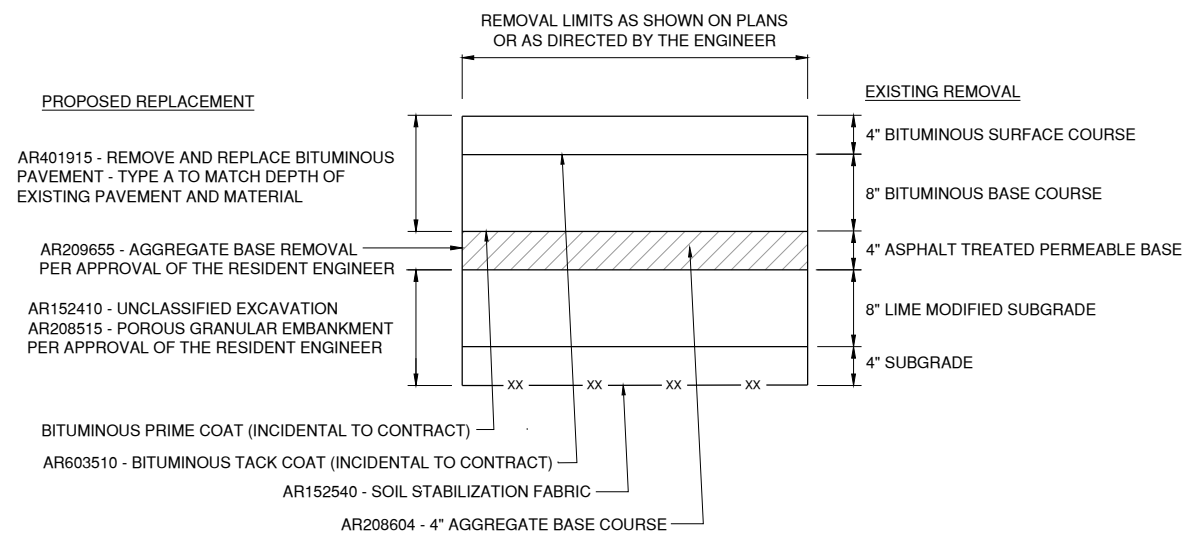
DUPAGE AIRPORT  
 AUTHORITY

DESIGN BY: JIM  
 DRAWN BY: JRO  
 CHECKED BY: DKP  
 APPROVED BY: DLP  
 DATE: 03/03/2023  
 JOB No: 21001642.00

**FINAL**

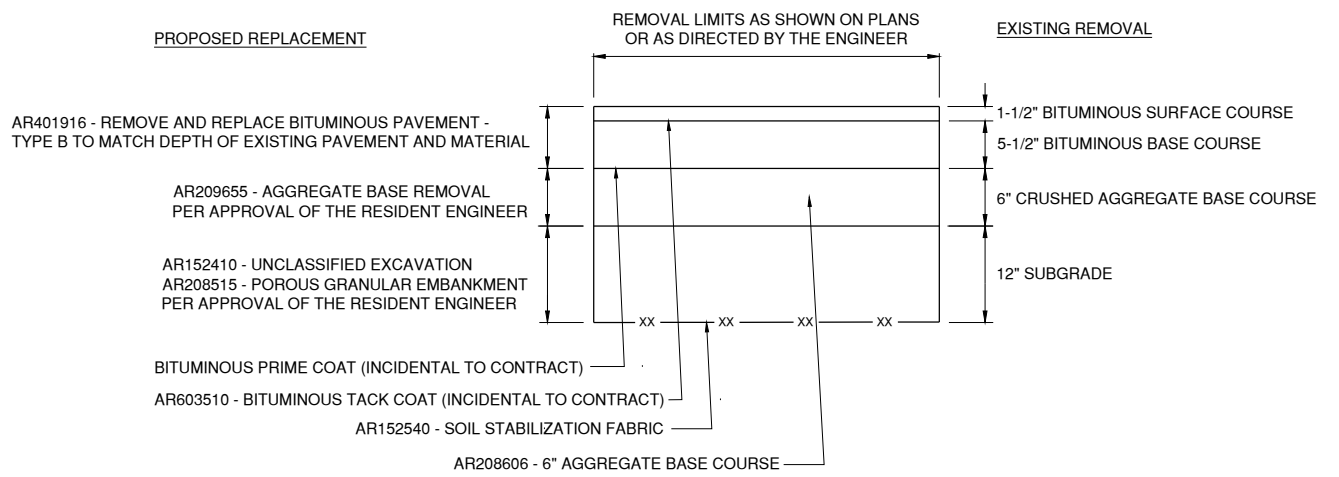
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**REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A DETAIL**

NOT TO SCALE  
(DUPAGE DRIVE)

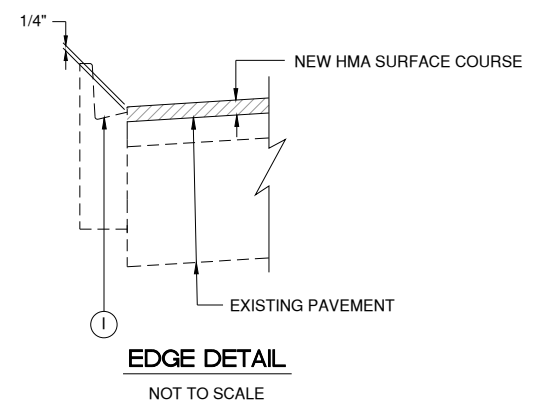


**REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B DETAIL**

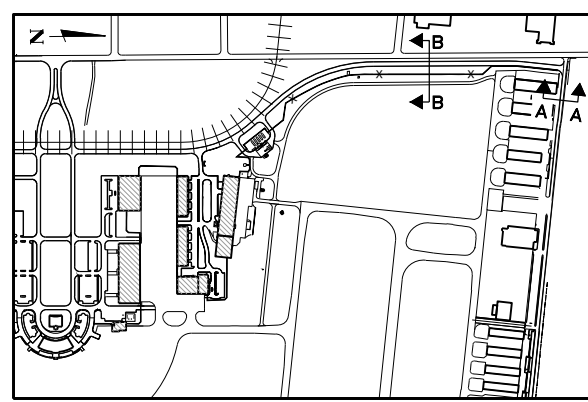
NOT TO SCALE  
(WEST TOWER ROAD)

**NOTES**

1. DEPTHS OF EXISTING PAVEMENT SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRAVERSE CRACKS ARE SPACED AT VARIABLE INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
3. AS FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION, FURNISHING AND PLACING OF POROUS GRANULAR EMBANKMENT AND/OR REMOVAL AND REPLACEMENT OF AGGREGATE BASE COURSE SHALL BE DONE AT SECTIONS WHERE THE REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A OR B IS UTILIZED. THIS SHALL BE DONE TO REPAIR SOFT SUBGRADE AS DETERMINED BY THE RESIDENT ENGINEER BASED ON RESULTS OF THE GEOTECHNICAL TESTING OR PROOF ROLL. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES.



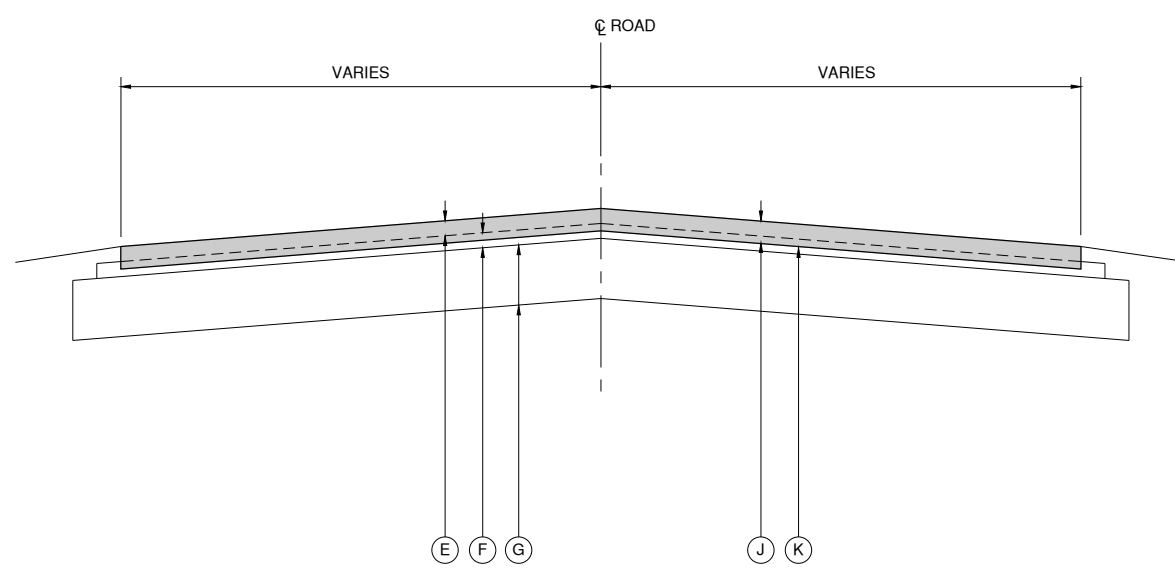
**EDGE DETAIL**  
NOT TO SCALE



**KEY MAP**

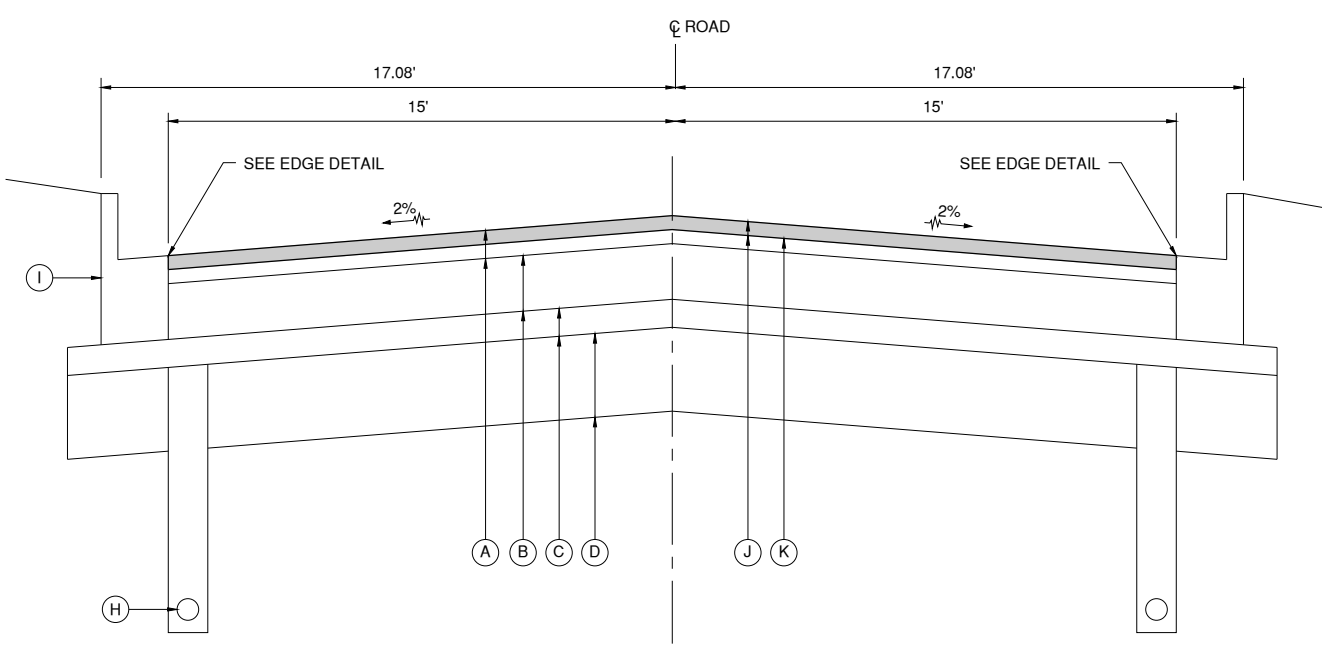
**LEGEND**

- (A) EXISTING 4" BITUMINOUS SURFACE COURSE
- (B) EXISTING 8" BITUMINOUS BASE COURSE
- (C) EXISTING 4" ASPHALT TREATED PERMEABLE BASE
- (D) EXISTING 8" LIME MODIFIED SUBGRADE
- (E) EXISTING 1-1/2" BITUMINOUS SURFACE COURSE
- (F) EXISTING 5-1/2" BITUMINOUS BASE COURSE
- (G) EXISTING 6" CRUSHED AGGREGATE BASE COURSE
- (H) EXISTING 6" PERFORATED UNDERDRAIN
- (I) EXISTING M-6.18 CURB AND GUTTER
- (J) NEW BITUMINOUS OVERLAY BITUMINOUS PAVEMENT MILLING, 2" (401) 2" BITUMINOUS SURFACE COURSE (401)
- (K) NEW BITUMINOUS TACK COAT (603)



**WEST TOWER ROAD SECTION A-A**

NOT TO SCALE  
(STA. 299+38.13 TO STA. 310+42.49)



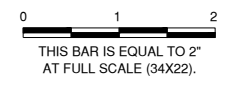
**DUPAGE DRIVE SECTION B-B**

NOT TO SCALE  
(STA. 750+65.79 TO STA. 786+79)

IL CONTRACT: **DU090**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **DPA-4979**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



**DUPAGE AIRPORT**  
**WEST CHICAGO, ILLINOIS**  
**AIRPORT PERIMETER ROAD RESURFACING -**  
**WEST TOWER ROAD AND DUPAGE DRIVE**

**TYPICAL SECTIONS**

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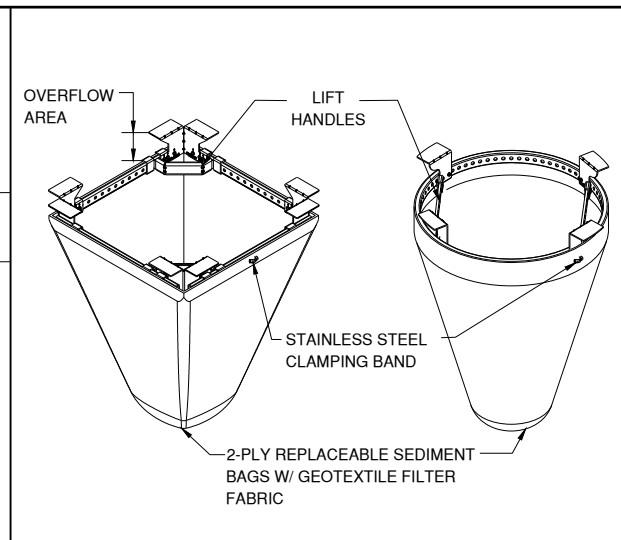
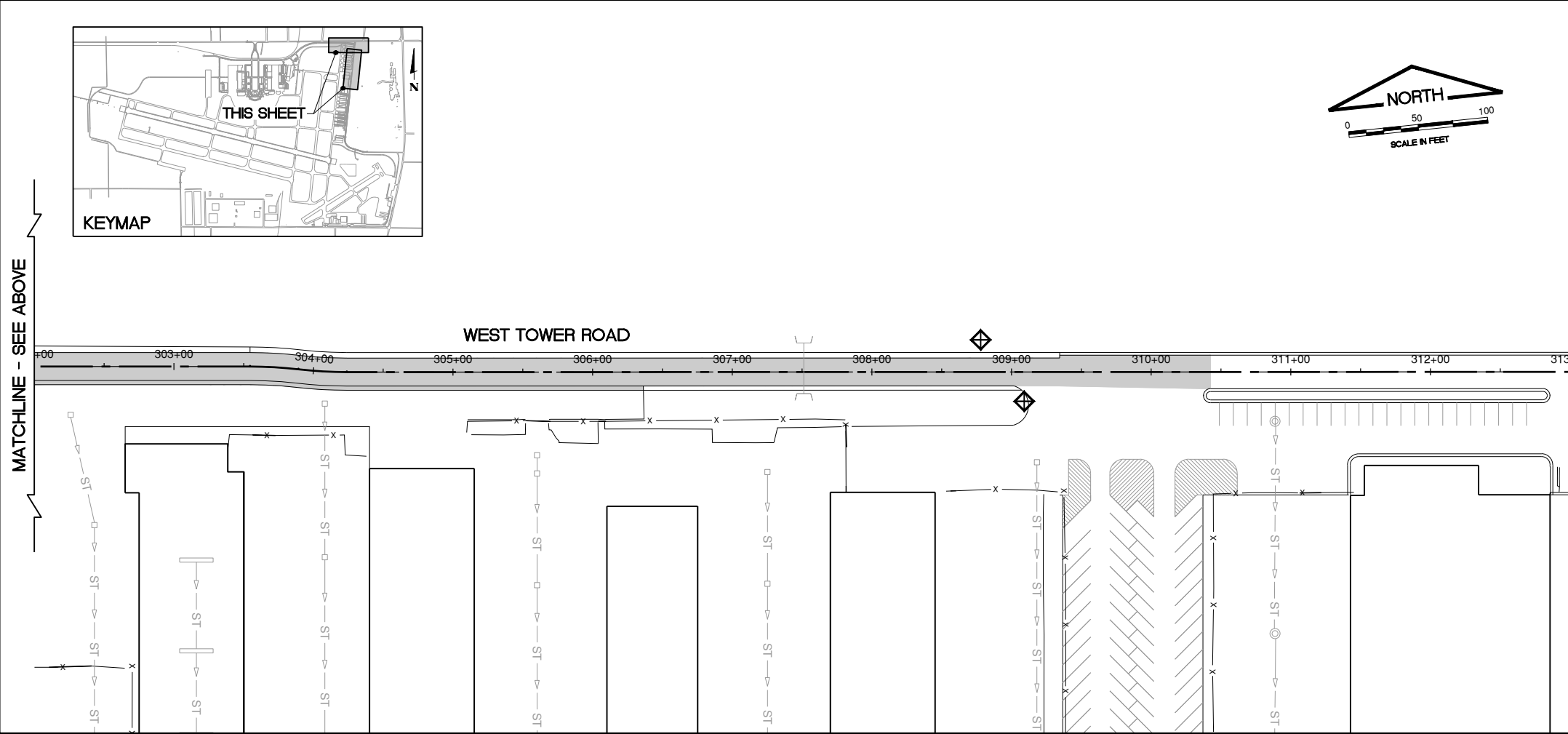
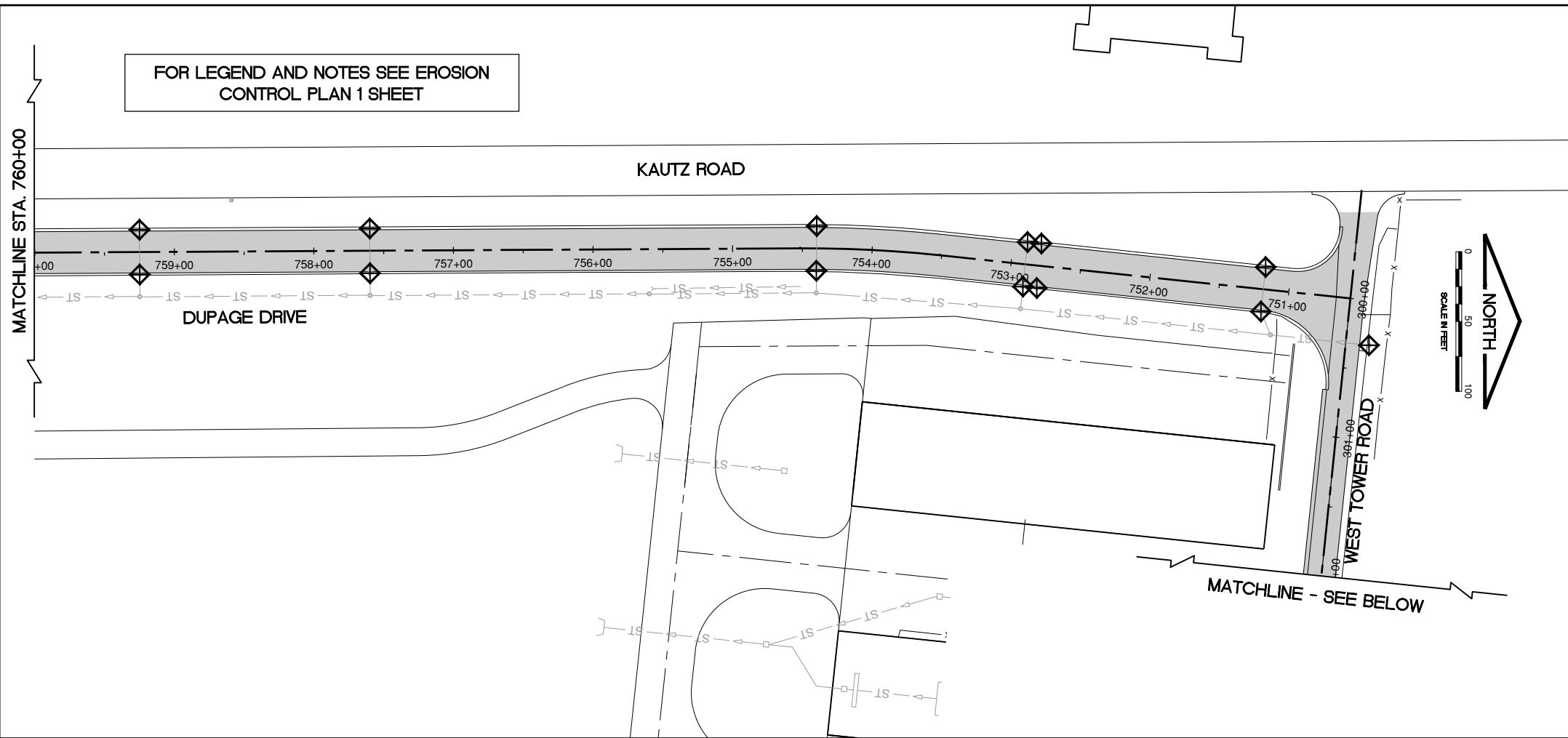
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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21001642.00

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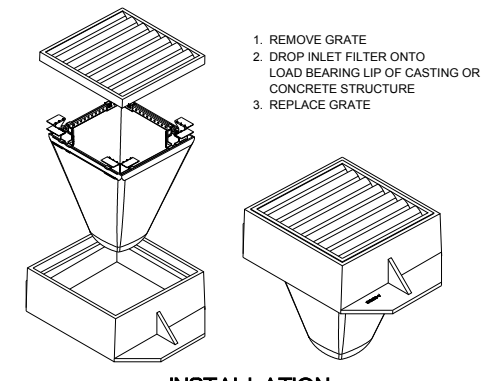
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**INLET PROTECTION - SILT BASKET (PAVEMENT)**  
 NOT TO SCALE  
 FOR ALL RECTANGULAR AND CIRCULAR INLETS

**NOTES FOR INLET PROTECTION DETAILS**

1. FILTER WRAP TO BE PLACED IN ALL SLOPE BOX INLETS, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
2. FABRIC SHALL BE IN CONFORMANCE WITH ARTICLE 1080.03 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2022.
3. FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
5. FABRIC SHALL REMAIN IN PLACE UNTIL COMPLETION OF PAVEMENT REHABILITATION.
6. COST OF FILTER WRAP AND MAINTENANCE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
7. ONLY THOSE INLET PROTECTIONS SHOWN ON THE STORM WATER POLLUTION PREVENTION PLAN SHALL BE PAID UNDER INLET PROTECTION (156). INLET PROTECTION REQUIRED DUE TO CONTRACTOR REQUIREMENTS AT THE STAGING AREA SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



**INSTALLATION**  
 NOT TO SCALE

IL CONTRACT: **DU090**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **DPA-4979**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DUPAGE AIRPORT**  
**WEST CHICAGO, ILLINOIS**  
**AIRPORT PERIMETER ROAD RESURFACING -**  
**WEST TOWER ROAD AND DUPAGE DRIVE**  
**EROSION CONTROL PLAN 2**

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**DUPAGE AIRPORT**  
**AUTHORITY**

DESIGN BY:	JUM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21001642.00
<b>FINAL</b>	
SHEET 12 OF 12 SHEETS	