11A
IDOT LETTING APRIL 28, 2023

PA065
TOTAL SHEETS = 27

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS

CONSTRUCTION PLANS
FOR
CHICAGO EXECUTIVE AIRPORT

EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

ILLINOIS PROJECT: PWK-4795

MARCH 3, 2023

CONTACT THE METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO 2 DAYS BEFORE STARTING WORK

P (708) 588-4055
E WMOJOBSTART@MWRD.ORG

TO THE BEST OF MY KNOWLEDGE AND BELIEF, THE DRAINAGE OF SURFACE WATERS WILL NOT BE CHANGED BY THE PROPOSED DEVELOPMENT. IF ANY DRAINAGE PATTERNS WILL BE CHANGED, REASONABLE PROVISIONS HAVE BEEN MADE FOR THE COLLECTION AND DIVERSION OF SUCH SURFACE WATERS IN TO THE PUBLIC AREA, OR DRAINS APPROVED FOR THE USE BY THE MUNICIPAL ENGINEER, AND THAT SUCH SURFACE WATERS ARE PLANNED FOR IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES SO AS TO REDUCE THE LIKELIHOOD OF DAMAGES TO ADJOINING PROPERTIES.

811 Know what's **below.** **Call** before you dig.

J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

CHICAGO EXECUTIVE AIRPORT

TOWNSHIP: 42 NORTH
RANGE: 11 EAST
COOK COUNTY

WHEELING TOWNSHIP
(SECTION: 13)

CMT
21002031.00
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 062-069052

SUBMITTED BY D. Kyle Peabody
D. KYLE PEABODY, PE

DATE 03/20/2023

CHICAGO EXECUTIVE AIRPORT

APPROVED JEFFREY J. MILLER, A.A.E., ACE — EXECUTIVE DIRECTOR

DATE 3/21/2023

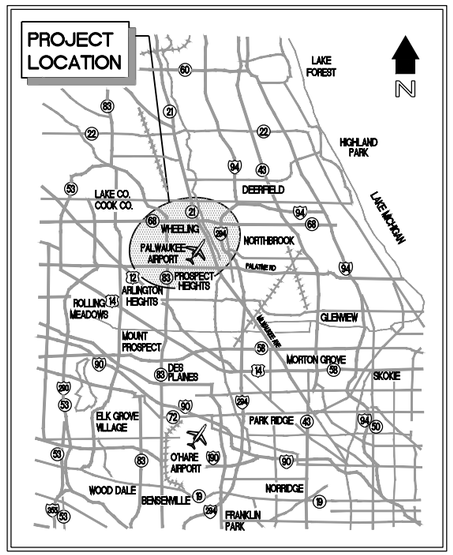
LICENSED PROFESSIONAL ENGINEER
DAVID K. PEABODY
062-048437
STATE OF ILLINOIS

D. Kyle Peabody

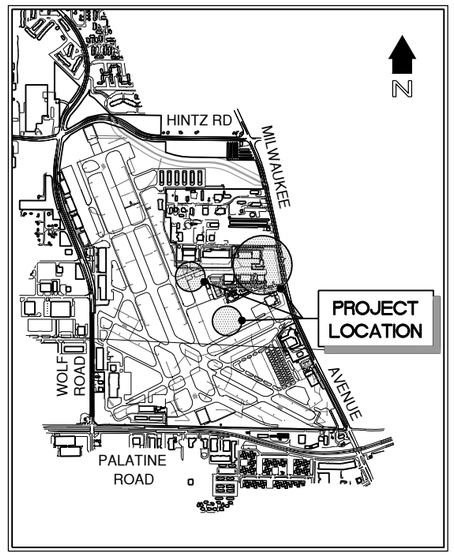
LICENSE EXPIRATION
DATE: 11/30/2023
DATE SIGNED: 03/20/2023

TRUE COPY OF PLANS ON FILE WITH THE METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO WATERSHED MANAGEMENT ORDINANCE PERMIT NO. 2023-0054.

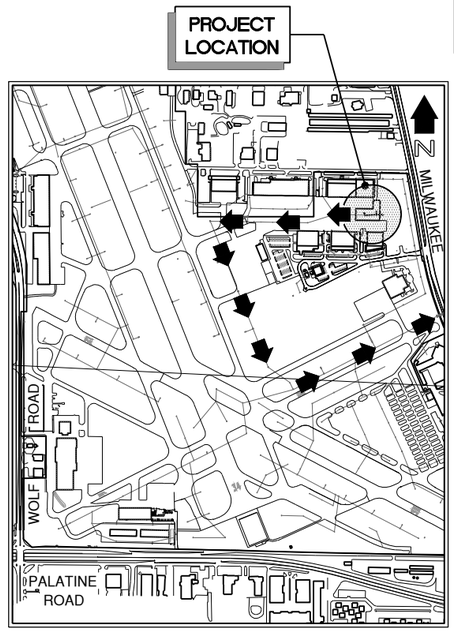
ALL STORM SEWER AND SANITARY SEWER WITHIN THE AIRFIELD PROPERTY IS OWNED BY CHICAGO EXECUTIVE AIRPORT. CHICAGO EXECUTIVE AIRPORT IS CO-OWNED BY BOTH THE VILLAGE OF WHEELING AND THE CITY OF PROSPECT HEIGHTS. THE SITE IS LOCATED WITHIN THE EXISTING SOUTH DETENTION BASIN



LOCATION MAP



SITE PLAN



STORMWATER ROUTING MAP

PROJECT INFORMATION
CONTRACTOR:
RESIDENT ENGINEER:
ORIGINAL CONTRACT AMOUNT:
FINAL CONSTRUCTION COST:
IDOT LETTING DATE:
IDOT AWARD DATE:
NOTICE TO PROCEED:
START OF CONSTRUCTION:
SUBSTANTIAL COMPLETION:

LOCAL AGENCY CONTACT INFORMATION
VILLAGE OF WHEELING - 847.459.2600
CITY OF PROSPECT HEIGHTS - 847.398.6070

ENGINEER'S PROJECT PERMIT LOG
NPDES #
FAA AIRSPACE #
CCDD LPC-663 DATED
MWRDGC PERMIT # 2023 - 0054
VILLAGE APP FOR CONSTRUCTION PERMIT #
VILLAGE FLOODPLAIN PERMIT #
CONTRACTOR'S REGISTRATION WITH VILLAGE
VILLAGE SITE ALTERATION PERMIT #
CITY APPLICATION FOR PERMIT #
CITY FLOODPLAIN PERMIT #
CITY SITE GRADING PERMIT #
CONTRACTOR'S REGISTRATION WITH CITY

\21002031.00\COVER.DWG

SUMMARY OF QUANTITIES

BASE BID

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR125100	ELEVATED RETROREFLECTIVE MARKER	EACH	5	
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1	
AR150520	MOBILIZATION	L SUM	1	
AR152410	UNCLASSIFIED EXCAVATION	CU YD	465	
AR152540	SOIL STABILIZATION FABRIC	SQ YD	2,885	
AR156510	SILT FENCE	FOOT	445	
AR156511	DITCH CHECK	EACH	2	
AR156520	INLET PROTECTION	EACH	8	
AR162618	CLASS E GATE - 18'	EACH	1	
AR162506	CLASS E FENCE 6'	FOOT	50	
AR162570	DETECTOR LOOP	L SUM	1	
AR162604	CLASS E GATE - 4'	EACH	1	
AR162900	REMOVE CLASS E FENCE	FOOT	50	
AR162905	REMOVE GATE	EACH	2	
AR162960	RELOCATE CLASS E FENCE	FOOT	266	
AR208515	POROUS GRANULAR EMBANKMENT	CU YD	990	
AR209606	CRUSHED AGG. BASE COURSE - 6"	SQ YD	2,960	
AR401610	BITUMINOUS SURFACE COURSE	TON	15	
AR401655	BUTT JOINT CONSTRUCTION	SQ YD	70	
AR401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	35	
AR501509	9" PCC PAVEMENT	SQ YD	3,050	
AR501530	PCC TEST BATCH	EACH	1	
AR501900	REMOVE PCC PAVEMENT	SQ YD	125	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ FT	230	
AR620525	PAVEMENT MARKING-BLACK BORDER	SQ FT	230	
AR701512	12" RCP, CLASS IV	FOOT	52	
AR701518	18" RCP, CLASS IV	FOOT	45	
AR701524	24" RCP, CLASS IV	FOOT	102	
AR701900	REMOVE PIPE	FOOT	97	
AR705504	4" PERFORATED UNDERDRAIN	FOOT	90	
AR705506	6" PERFORATED UNDERDRAIN	FOOT	165	
AR751411	INLET-TYPE A	EACH	1	
AR751416	TYPE 1 INLET	EACH	2	
AR751550	MANHOLE 5'	EACH	1	
AR751903	REMOVE MANHOLE	EACH	1	
AR751980	RECONSTRUCT INLET	EACH	1	
AR800084	VOLUME CONTROL INSTALLATION	SQ FT	3,952	
AR800159	SEEDING	SQ YD	1,450	
AR800162	HEAVY-DUTY HYDRAULIC MULCH	SQ YD	1,450	

ADDITIVE ALTERNATE NO. 1 - NON-SPECIAL WASTE MATERIAL TO BE STOCKPILED ON AIRPORT PROPERTY

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AS152410	UNCLASSIFIED EXCAVATION	CU YD	2,900	
AS156510	SILT FENCE	FOOT	1,065	
AS901510	SEEDING	ACRE	1.2	
AS908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	1.2	

ADDITIVE ALTERNATE NO. 2 - NON-SPECIAL WASTE MATERIAL TO BE HAULED OFF-SITE AND DISPOSED OF AT A SUBTITLE D LANDFILL

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AT152410	UNCLASSIFIED EXCAVATION	CU YD	2,900	

INDEX TO SHEETS

EXCAVATION SCOPE OF WORK

ONLY ONE OF THE FOLLOWING ADDITIVE ALTERNATES WILL BE AWARDED FOR THIS PROJECT.

BASE BID

- THE CONTRACTOR SHALL PERFORM EXCAVATION AT THE NEW VOLUME CONTROL SITE (INFILTRATION TRENCH) AND HAUL OFF AND DISPOSE OF MATERIAL OFFSITE. THIS WORK SHALL BE PAID UNDER AR152410 UNCLASSIFIED EXCAVATION.

ADDITIVE ALTERNATE NO. 1

- THE CONTRACTOR SHALL PERFORM EXCAVATION AT THE NEW PCC APRON SITE AND STOCKPILE MATERIAL AT LOCATION AS SHOWN ON SITE PLAN. THIS WORK SHALL BE PAID UNDER AS152410 UNCLASSIFIED EXCAVATION.
- INSTALLATION OF SILT FENCE, SEEDING AND HEAVY-DUTY HYDRAULIC MULCH AT THE STOCKPILE SITE SHALL BE PAID FOR AS AS156510 SILT FENCE, AS901510 SEEDING AND AS908515 HEAVY-DUTY HYDRAULIC MULCH. SEE STORMWATER POLLUTION PREVENTION PLAN FOR MORE NOTES ON STOCKPILE.

ADDITIVE ALTERNATE NO. 2

- THE CONTRACTOR SHALL PERFORM EXCAVATION AT THE NEW PCC APRON SITE AND HAUL OFF AND DISPOSE OF MATERIAL OFFSITE AT A SUBTITLE D LANDFILL. THIS WORK SHALL BE PAID UNDER AT152410 UNCLASSIFIED EXCAVATION.

MUNICIPALITIES GENERAL NOTES

1. THE CHICAGO EXECUTIVE AIRPORT IS A JOINT OWNERSHIP BY BOTH THE VILLAGE OF WHEELING AND CITY OF PROSPECT HEIGHTS. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH VILLAGE AND CITY CODES, ORDINANCES AND STANDARDS AS APPLICABLE.
2. ALL CONTRACTORS AND SUBCONTRACTORS SHALL BE REGISTERED WITH THE VILLAGE AND CITY PRIOR TO THE NOTICE TO PROCEED. ALL REGISTRATION FEES SHALL BE INCIDENTAL TO THE CONTRACT.
3. THE CONTRACTOR SHALL WORK WITH THE AIRPORT AND ENGINEER TO SECURE THE REQUIRED VILLAGE AND CITY LOCAL CONSTRUCTION PERMITS PRIOR TO THE NOTICE TO PROCEED.
4. THE CONTRACTOR SHALL COORDINATE WITH THE VILLAGE AND CITY AT THE WEEKLY PROGRESS MEETINGS AND SHALL NOTIFY THE CITY OF PROSPECT HEIGHTS (847.398.6700) AND THE VILLAGE OF WHEELING (847.459.2600) A MINIMUM OF 48 HOURS PRIOR TO ANY REQUIRED VILLAGE/CITY INSPECTIONS.
5. ALL STORM SEWERS AND SANITARY SEWERS ON THE AIRPORT SITE ARE OWNED, OPERATED AND MAINTAINED BY THE CHICAGO EXECUTIVE AIRPORT UNLESS LABELED OTHERWISE.

NOTES

1. SPECIAL ATTENTION IS NECESSARY WHEN WORKING NEAR FAA POWER AND CONTROL CABLES. ANY FAA UTILITY THAT IS DAMAGED OR CUT DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY. **FAA REQUIRES THAT ANY DAMAGED CABLE BE REPLACED IN ITS ENTIRETY, FROM POWER/CONTROL SOURCE TO THE EQUIPMENT/SERVICE. SPLICES OF ANY KIND WILL NOT BE PERMITTED.** EXPOSURES OF ANY FAA CABLES MUST BE DONE BY HAND DIGGING OR HYDRO-EXCAVATION. NO ADDITIONAL COMPENSATION WILL BE MADE FOR LOCATING, REPLACEMENT OR REPAIR OF FAA FACILITIES OR CABLES BUT, SHALL BE INCIDENTAL TO THE CONTRACT.
2. WHEN FAA CABLES ARE REQUIRED TO BE LOCATED, OR THE CONTRACTOR IS PLANNING ON WORKING ON OR AROUND FAA CABLES, CONDUITS OR EQUIPMENT, A 10 WORKING DAY ADVANCED NOTICE SHALL BE GIVEN TO THE FAA BEFORE ANY SUCH MARKINGS ARE REQUIRED. ONCE FAA MARKS THE CABLES, THE CONTRACTOR WILL BE REQUIRED TO SURVEY THE FAA UTILITIES SO THEY CAN BE REPLACED DURING CONSTRUCTION WITHOUT REMARKING BY THE FAA. THIS SHALL BE INCIDENTAL TO THE CONTRACT. THE FAA PERSONNEL ARE ONLY AVAILABLE FROM 9 AM TO 3 PM, MONDAY THROUGH FRIDAY WITH ADVANCED NOTICE.
3. AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS, AT NO ADDITIONAL COST TO THE CONTRACT.
6. THE CONTRACTOR SHALL PERFORM WORK MEETING THE REQUIREMENTS AS SPECIFIED IN THE MWRD PERMIT SET.

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

INDEX OF SHEETS, SUMMARY OF QUANTITIES AND GENERAL NOTES

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

UPDATE BY: Sheue Torng Lee
LAYOUT: Layout1
DATE: Wednesday, March 22, 2023 12:55:08 PM
FILE: K:\ChicagoExecAp\21002031-00_ExpandEQuadApron\Draw\Sheets\expansion-quantities.dwg

HORIZONTAL CONTROL

POINT	DESCRIPTION	NORTHING	EASTING
CP-1	X CUT ON CURB	1985345.623	618830.492
CP-2	X CUT ON CURB	1985287.657	618458.634
CP-3	X CUT ON CURB	1985327.383	618150.558
CP-4	X CUT ON CURB	1985264.927	617942.456
CP-5	X CUT ON CURB	1985057.726	618034.423

HORIZONTAL CONTROL COORDINATES EXPRESSED IN NAV27

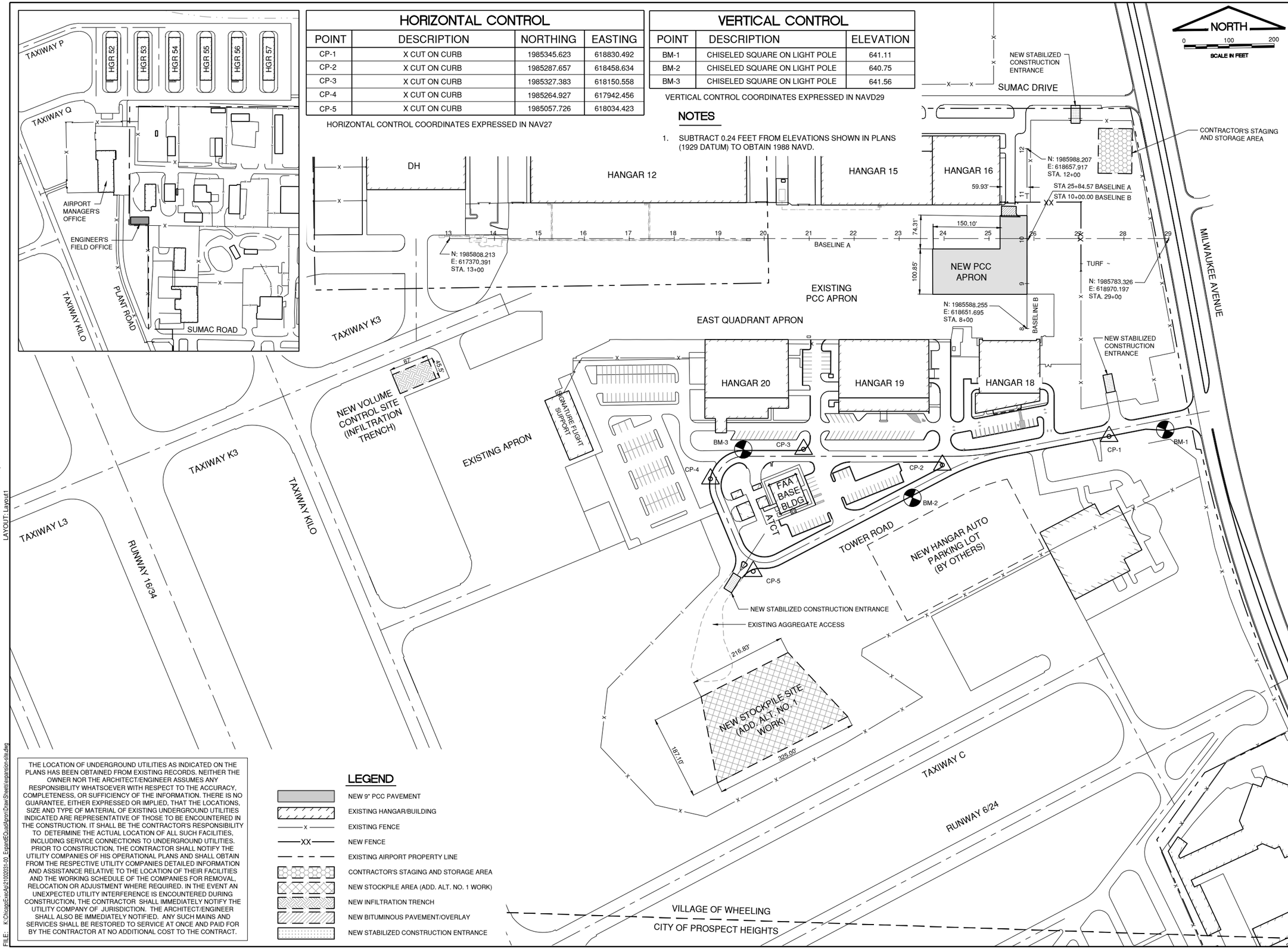
VERTICAL CONTROL

POINT	DESCRIPTION	ELEVATION
BM-1	CHISELED SQUARE ON LIGHT POLE	641.11
BM-2	CHISELED SQUARE ON LIGHT POLE	640.75
BM-3	CHISELED SQUARE ON LIGHT POLE	641.56

VERTICAL CONTROL COORDINATES EXPRESSED IN NAVD29

NOTES

1. SUBTRACT 0.24 FEET FROM ELEVATIONS SHOWN IN PLANS (1929 DATUM) TO OBTAIN 1988 NAVD.



THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ARCHITECT/ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER WITH RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL, RELOCATION OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ARCHITECT/ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

LEGEND

- NEW 9" PCC PAVEMENT
- EXISTING HANGAR/BUILDING
- EXISTING FENCE
- NEW FENCE
- EXISTING AIRPORT PROPERTY LINE
- CONTRACTOR'S STAGING AND STORAGE AREA
- NEW STOCKPILE AREA (ADD. ALT. NO. 1 WORK)
- NEW INFILTRATION TRENCH
- NEW BITUMINOUS PAVEMENT/OVERLAY
- NEW STABILIZED CONSTRUCTION ENTRANCE

IL. CONTRACT: PA065
IL. LETTING ITEM: 11A
IL. PROJECT: PWK-4795
S.B.G. PROJECT: N/A

REVISIONS

NUMBER	BY	DATE

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CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

SITE PLAN AND PROJECT CONTROL PLAN

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CHICAGO EXECUTIVE AIRPORT

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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
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JOB No:	21002031.00

FINAL

DATE: Wednesday, March 22, 2023 12:55:17 PM
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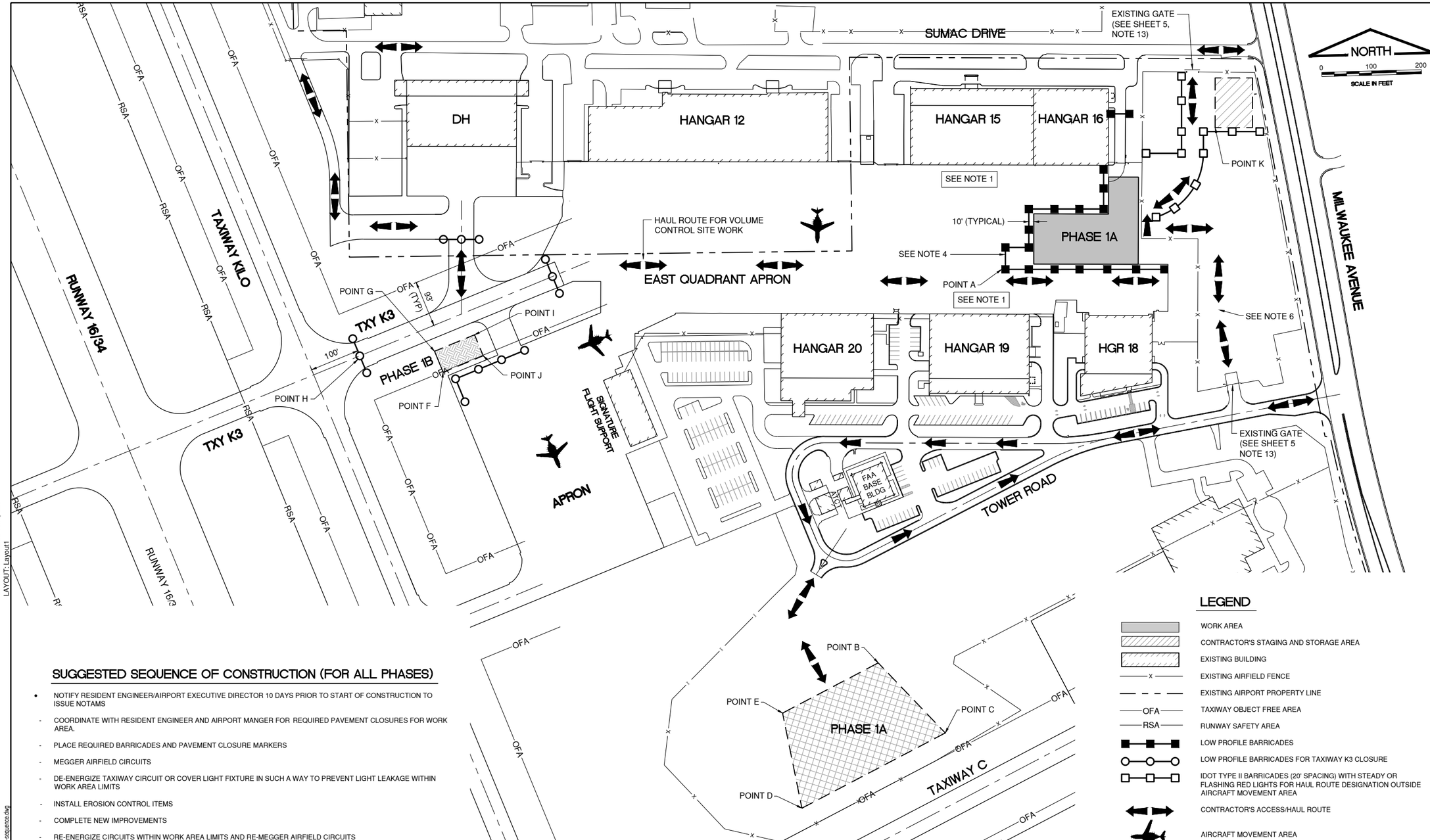
UPDATE BY: Sheue Tomg Lee
LAYOUT: Layout1



SUGGESTED SEQUENCE OF CONSTRUCTION (FOR ALL PHASES)

- NOTIFY RESIDENT ENGINEER/AIRPORT EXECUTIVE DIRECTOR 10 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE NOTAMS
- COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
- PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS
- MEGGER AIRFIELD CIRCUITS
- DE-ENERGIZE TAXIWAY CIRCUIT OR COVER LIGHT FIXTURE IN SUCH A WAY TO PREVENT LIGHT LEAKAGE WITHIN WORK AREA LIMITS
- INSTALL EROSION CONTROL ITEMS
- COMPLETE NEW IMPROVEMENTS
- RE-ENERGIZE CIRCUITS WITHIN WORK AREA LIMITS AND RE-MEGGER AIRFIELD CIRCUITS
- CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA
- RESTORE STAGING AREAS

GENERAL NOTES

1. NO AIRCRAFT WILL BE ALLOWED TO PARK IN THIS AREA TO ALLOW FOR AIRCRAFT TRAFFIC.
2. CONTRACTOR WILL BE REQUIRED TO HAVE A CROSSING GUARD AND/OR VEHICULAR ESCORT FOR CONTRACTOR'S VEHICLES AND EQUIPMENT CROSSING AIRCRAFT MOVEMENT AREA AND CROSSING ACTIVE AIRCRAFT APRON AREAS.
3. CONTRACTOR TO EXPEDITE PHASE 1B FOR WORK TO BE COMPLETED IN THIS AREA.
4. WORK AREAS PHASE 1A AND 1B SHALL BE CONSTRUCTED CONCURRENTLY.
5. NO WORK SHALL OCCUR BETWEEN JUNE 19TH AND JUNE 24TH.
6. CONTRACTOR CAN ONLY USE THIS ACCESS/HAUL ROUTE TO THE STOCKPILE AREA

PHASE 1A - SEQUENCE OF CONSTRUCTION

- COORDINATE ACCESS TO THE APRON.
- CONSTRUCT PROPOSED IMPROVEMENTS IN PHASE 1A WORK AREA.

PHASE 1B - SEQUENCE OF CONSTRUCTION

- COORDINATE CLOSURE OF TAXIWAY K3 WITH RESIDENT ENGINEER.
- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES FOR TAXIWAY CLOSURES.
- CONSTRUCT PROPOSED IMPROVEMENTS IN PHASE 1B WORK AREA INCLUDING SEEDING AND MULCHING. MAXIMUM 4 CONSECUTIVE DAYS TO COMPLETE THIS WORK.
- UPON COMPLETION OF WORK ITEMS, CLEAN PAVEMENTS, REMOVE BARRICADES, AND OPEN TAXIWAY K3.

PHASE	FACILITY STATUS	FACILITY STATUS - CLOSED
1A	ALL RUNWAYS AND TAXIWAYS OPEN	NONE
1B	TAXIWAY K3 - CLOSED ALL OTHER TAXIWAYS OPEN; ALL RUNWAYS OPEN	TAXIWAY K3

NOTE: PHASE 1B SHALL BE RESTRICTED TO 4 CONSECUTIVE CALENDAR DAYS.

LEGEND

- WORK AREA
- CONTRACTOR'S STAGING AND STORAGE AREA
- EXISTING BUILDING
- EXISTING AIRFIELD FENCE
- EXISTING AIRPORT PROPERTY LINE
- TAXIWAY OBJECT FREE AREA
- RUNWAY SAFETY AREA
- LOW PROFILE BARRICADES
- LOW PROFILE BARRICADES FOR TAXIWAY K3 CLOSURE
- IDOT TYPE II BARRICADES (20' SPACING) WITH STEADY OR FLASHING RED LIGHTS FOR HAUL ROUTE DESIGNATION OUTSIDE AIRCRAFT MOVEMENT AREA
- CONTRACTOR'S ACCESS/HAUL ROUTE
- AIRCRAFT MOVEMENT AREA
- POINT A — CRITICAL POINT FOR AIRSPACE
- NEW INFILTRATION TRENCH
- NEW STOCKPILE AREA

IL. CONTRACT: PA065
IL. LETTING ITEM: 11A
IL. PROJECT: PWK-4795
S.B.G. PROJECT: N/A

REVISIONS

NUMBER	BY	DATE

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CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

SEQUENCE OF CONSTRUCTION

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CHICAGO EXECUTIVE AIRPORT

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JOB No:	21002031.00

FINAL

UPDATE BY: Sheue Tomg Lee
LAYOUT: Layout1
DATE: Wednesday, March 22, 2023 12:55:26 PM
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GENERAL NOTES

1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT EXECUTIVE DIRECTOR AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.

2. ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".

3. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS. NO EXCEPTIONS FOR SLOW MOVING EQUIPMENT SHALL BE ALLOWED.

4. THE AIRPORT EXECUTIVE DIRECTOR SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATION PHASING AND SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.

5. ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT EXECUTIVE DIRECTOR.

6. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER AND AIRPORT.

7. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.

8. ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFECIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

9. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

10. MATERIALS REMOVED FROM THE PROJECT WILL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.

11. PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, CONSTRUCTION FENCE, SIGNING, RUNWAY AND TAXIWAY CLOSED MARKERS, SAFETY AND OBJECT FREE AREAS, LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. LOW PROFILE BARRICADES SHALL CONFORM TO THE DETAILS IN THE PLANS AND SEQUENCE OF CONSTRUCTION. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE WORK AREA BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS AND OBJECT FREE AREAS.

12. THE CONTRACTOR SHALL CONTACT THE AIRPORT EXECUTIVE DIRECTOR THROUGH THE RESIDENT ENGINEER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.

13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON-WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

14. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS OR A WORKING BEACON LIGHT ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.

15. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT EXECUTIVE DIRECTOR AND THE RESIDENT ENGINEER IMMEDIATELY.

16. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.

17. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.

18. IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.

19. CHICAGO EXECUTIVE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.

20. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.

21. MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

22. LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO PREVIOUS EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.

23. COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

24. THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.

25. DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.

26. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA AND RUNWAY SAFETY AREA OF ACTIVE TAXIWAYS AND RUNWAYS.

27. CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.

28. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

29. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 70-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT EXECUTIVE DIRECTOR.

CONTRACTOR CROSSING RUNWAY SAFETY AREAS (RSA) AND TAXIWAY OBJECT FREE AREAS (TOFA)

31. ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES ($1,000 PER OCCURENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.

32. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEPT TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY SAFETY AREAS (RSA) / RUNWAY OBSTACLE FREE ZONE (OFZ) AND TAXIWAY OBJECT FREE AREAS (TOFA)

RUNWAYS:

33. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. ANY WORK WITHIN THE RUNWAY SAFETY AREA OR RUNWAY OBSTACLE FREE AREA WILL REQUIRE A RUNWAY CLOSURE. UNLESS OTHERWISE NOTED FOR OVERNIGHT CLOSURE PER SEQUENCE OF CONSTRUCTION SHEET, WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING PERIOD THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED PER FAA REQUIREMENTS. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY, STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA AT NO ADDITIONAL COST TO THE CONTRACT. NO MATERIAL SHALL BE STOCKPILED WITHIN THE RSA. IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN, EQUIPMENT AND BARRICADES.

TAXIWAYS:

34. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE FOR WORK WITHIN THE TAXIWAY OBJECT FREE AREA. ANY WORK WITHIN THE TAXIWAY OBJECT FREE AREA WILL REQUIRE A TAXIWAY CLOSURE. WORK WITHIN THE TAXIWAY OBJECT FREE AREA SHALL BE EXPEDITED AND AT THE END OF EACH WORKING PERIOD THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE TAXIWAY TO BE REOPENED PER FAA REQUIREMENTS. IF NECESSARY, STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE TOFA AT NO ADDITIONAL COST TO THE CONTRACT. NO MATERIAL OR EQUIPMENT SHALL BE STOCKPILED OR STORED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT.

ALLOWABLE CONSTRUCTION HOURS

1. THE ALLOWABLE CONSTRUCTION HOURS FOR THE VILLAGE OF WHEELING AND THE CITY OF PROSPECT HEIGHTS ARE FROM 7 AM TO 6 PM, MONDAY THROUGH SATURDAY. THE AIRPORT WILL SEEK A WAIVER WITH THE VILLAGE AND CITY TO ALLOW CONSTRUCTION OUTSIDE OF THOSE HOURS FOR THE PHASES SHOWN TO BE COMPLETED OVER WEEKENDS ONLY. AT ALL OTHER TIMES, IT IS EXPECTED THE CONTRACTOR WILL ADHERE TO THE VILLAGE AND CITY NOISE ORDINANCE AND ALLOWABLE CONSTRUCTION HOUR POLICIES. SHOULD THE CONTRACTOR REQUIRE ADDITIONAL WORKING HOURS, HE SHALL REQUEST, THROUGH THE RESIDENT ENGINEER, THAT THE VILLAGE AND CITY BE CONTACTED TO REQUEST ADDITIONAL WAIVER OF THE NOISE ORDINANCE POLICY. ANY FINES LEVIED BY THE VILLAGE OR CITY TO THE AIRPORT FOR VIOLATIONS OF THE NOISE ORDINANCE AND ALLOWABLE CONSTRUCTION HOURS SHALL BE PAID BY THE CONTRACTOR.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS THE TAXIWAY SITE DUE TO ADJACENT BUILDING CONSTRUCTION

GROUND CONTROL FREQUENCY: 121.7 MHz
AIR CONTROL FREQUENCY: 119.9 MHz

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: DUMP TRUCK IN DUMP POSITION - 25'

IN THE EVENT THE CONTRACTOR PROPOSES TO UTILIZE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN WHAT IS LISTED, THE CONTRACTOR WILL BE RESPONSIBLE TO SUBMIT FAA FORM 7460 FOR AIRSPACE APPROVAL. THE RESIDENT ENGINEER WILL PROVIDE BASE AIRPORT INFORMATION FOR THE CONTRACTOR'S USE.

ELECTRICAL NOTES - ALL PHASES

1. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

2. WHEN FAA CABLES ARE REQUIRED TO BE LOCATED, A 10 WORKING DAY ADVANCED NOTICE SHALL BE GIVEN TO THE FAA BEFORE ANY SUCH MARKINGS ARE REQUIRED. ONCE FAA MARKS THE CABLES, THE CONTRACTOR WILL BE REQUIRED TO SURVEY THE FAA UTILITIES SO THEY CAN BE REPLACED DURING CONSTRUCTION WITHOUT REMARKING BY THE FAA. THIS SHALL BE INCIDENTAL AND AT THE CONTRACTOR'S EXPENSE. THE FAA PERSONNEL ARE ONLY AVAILABLE FROM 9 AM TO 3 PM, MONDAY THROUGH FRIDAY WITH ADVANCED NOTICE.

3. SPECIAL ATTENTION IS NECESSARY WHEN WORKING NEAR FAA POWER AND CONTROL CABLES. ANY FAA UTILITY THAT IS DAMAGED OR CUT DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY. FAA REQUIRES THAT ANY DAMAGED CABLE BE REPLACED IN ITS ENTIRETY, FROM POWER/CONTROL SOURCE TO THE EQUIPMENT/SERVICE. SPLICES OF ANY KIND WILL NOT BE PERMITTED. EXPOSURES OF ANY FAA CABLES MUST BE DONE BY HAND DIGGING OR HYDRO-EXCAVATION. NO ADDITIONAL COMPENSATION WILL BE MADE FOR LOCATING, REPLACEMENT OR REPAIR OF FAA FACILITIES OR CABLES BUT, SHALL BE INCIDENTAL AND AT THE CONTRACTOR'S EXPENSE.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS THE WORK SITE DUE TO ADJACENT CONSTRUCTION

- REHABILITATE ACCESS ROAD AT NW QUADRANT HANGARS.
- 2023 AIRFIELD PAVEMENT REPAIR AND REMARKING.
- REHABILITATE AIRFIELD LIGHTING - PHASE 1 PROJECT.
- MWRD PROJECT #06-360-3SR UPPER DES PLAINES INTERCEPTING SEWER 14B REHABILITATION, NSA

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS - 1

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

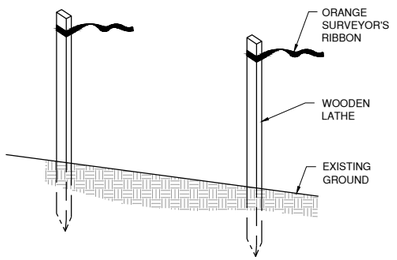
UPDATE BY: Sheue Torng Lee
LAYOUT: Layout1
DATE: Wednesday, March 22, 2023 12:55:34 PM
FILE: K:\ChicagoExecAvp\21002031-00_ExpandEQuadApron\Draw\Sheets expansion-see Notes Details.dwg

PHASING NOTES (ALL PHASES)

1. THE INTENT OF THE PHASING PLANS IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. THE CONTRACTOR SHALL CONSTRUCT THE PROJECT IN CONSECUTIVE PHASES AS OUTLINED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT EXECUTIVE DIRECTOR.
2. WHEN HAUL ROUTE IS IN USE ON ACTIVE AIRFIELD PAVEMENTS, THE CONTRACTOR WILL BE REQUIRED TO BE UNDER CONTROL BY A CROSSING GUARD OR ESCORT FOR CONSTRUCTION PERSONNEL, CONSTRUCTION TRAFFIC, CONTRACTORS VEHICLES AND EQUIPMENT CROSSING BY, TO OR FROM WORK ZONE. STOP SIGNS SHALL BE IN PLACE AT ALL TIMES IN THIS AREA. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
3. THE CONTRACTOR SHALL SUBMIT A CONSTRUCTION SCHEDULE. STRICT ADHERENCE TO THE APPROVED SCHEDULE WILL BE ENFORCED TO AVOID CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES ON THE AIRPORT AND THE ADVERSE EFFECTS THEY COULD HAVE ON AIRPORT OPERATIONS.
4. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT STAFF TO SCHEDULE THE RUNWAY/TAXIWAY CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY AND MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THIS CRITICAL CLOSURE. THE AIRPORT EXECUTIVE DIRECTOR AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE EXACT DATES AND TIMES OF THE CLOSURE(S).
5. CONTRACTOR MUST MAINTAIN ACCESS TO ALL ACTIVE AND OPEN AREAS AT ALL TIMES. CONTRACTOR SHALL RELOCATE EQUIPMENT AT NO ADDITIONAL COST TO CONTRACT TO ALLOW AIRCRAFT TO PASS. CONTRACTOR SHALL COORDINATE CONSTRUCTION OPERATIONS AT ALL ACTIVE AND OPEN AREAS TO PROVIDE MINIMAL DISRUPTIONS TO AIRCRAFT MOVEMENT IN THAT AREA.
6. FAA AND AIRPORT ACCESS ROAD(S) SHALL NOT BE USED AS A HAUL ROAD BY THE CONTRACTOR WITHOUT PRIOR APPROVAL.
7. TO THE EXTENT POSSIBLE THE CONTRACTOR SHALL HAVE ALL EMPLOYEE PARKING OUTSIDE OF AIRPORT FENCE OR AS INDICATED AT THE LOCATION SHOWN.
8. THE AIRPORT RESERVES THE RIGHT TO MODIFY THE SEQUENCE OF CONSTRUCTION INCLUDING BUT NOT LIMITED TO PHASING, WORK AREAS, BARRICADE PLACEMENT, ACCESS AND HAUL ROUTES, AND CONTRACTOR MOVEMENTS AT ANY TIME DURING THE PROJECT WITH FAA, IDA AND ATCT APPROVAL.

AIRFIELD LIGHTS AND SIGNS NOTES

1. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
2. CONTRACTOR SHALL TURN OFF RUNWAY/TAXIWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES. CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY/TAXIWAY, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT EXECUTIVE DIRECTOR/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

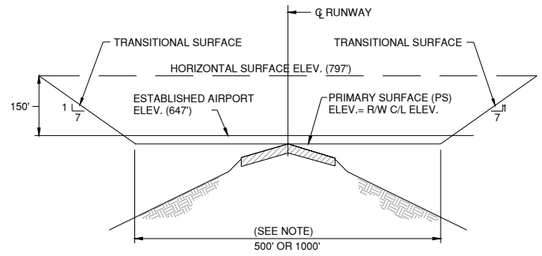


CONSTRUCTION SETBACK LINE DETAIL

NOT TO SCALE

CONSTRUCTION SETBACK NOTES

1. CONTRACTOR SHALL MARK THE RUNWAY SAFETY AREA, TAXIWAY OBJECT FREE AREA AND RUNWAY OBSTACLE FREE ZONE PER THE CONSTRUCTION SETBACK DETAIL AS DIRECTED BY THE RESIDENT ENGINEER.
2. ALL COST ASSOCIATED WITH THE CONSTRUCTION SETBACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



TYPICAL SECTION F.A.R. PART 77 IMAGINARY SURFACES

NO SCALE

NOTE:
IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY
R/W 6/24 & 12/30 500' PS (250' LT & RT OF ℄) R/W 16/34 1000' PS (500' LT & RT OF ℄)

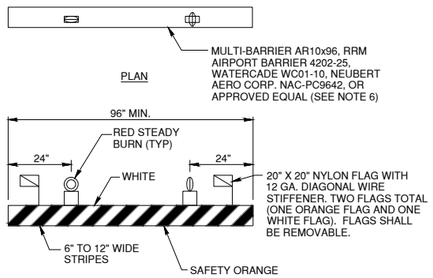
DESIGN AIRCRAFT APPROACH CATEGORY: D DESIGN AIRPORT GROUP: III
EAST QUADRANT APRON MAXIMUM ANTICIPATED WINGSPAN OF ADG III GULFSTREAM G550 - WINGSPAN = 93.5'

EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS

RUNWAY	16/34	12/30	6/24
APPROACH CATEGORY	D	B	B
DESIGN GROUP	III	II	I
DESIGN AIRCRAFT	GULFSTREAM 550	KING AIR B200	CESSNA 421
APPROACH SPEED	150 KNOTS	107 KNOTS	96 KNOTS
WINGSPAN	94 FEET	58 FEET	44 FEET
TAIL HEIGHT	25.8 FEET	14.3 FEET	11.5 FEET
STRENGTH (MGTW)	91,000 LBS.	12,500 LBS.	7,450 LBS.
LENGTH	97 FEET	47 FEET	37 FEET
AOA @ RUNWAY SAFETY AREA WIDTH (RSA)	500	150	120
RUNWAY OBJECT FREE AREA WIDTH (ROFA)	800	500	400
TAXIWAY SAFETY AREA WIDTH (TSA)	118	79	49
AOA @ TAXIWAY OBJECT FREE AREA WIDTH (TOFA)	171	124	89

AOA = AIRCRAFT OPERATIONS AREA

DATA FROM 2021 CEA APPROVED ALP

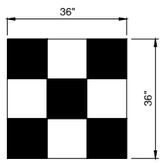


LOW PROFILE LIGHTED BARRICADE

NOT TO SCALE

BARRICADE NOTES:

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OR SOLAR POWER OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90º.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED END TO END AS INDICATED AT THE LOCATIONS SHOWN ON THE PLANS ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION OR AS DIRECTED BY THE RESIDENT ENGINEER OR AIRPORT. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90º.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN.



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG

NOT TO SCALE

CRITICAL POINTS TABLE

WORK AREA	POINT	APPROXIMATE ELEVATION OF GROUND (1929 DATUM)	ANTICIPATED EQUIPMENT AND HEIGHT	APPROXIMATE ELEVATION OF EQUIPMENT (1929 DATUM)	LATITUDE (NAD 83)	LONGITUDE (NAD 83)
PHASE 1	A	642.0	SEMI/DUMP TRUCK - 25'	667.0	42° 07' 03.05"	87° 53' 49.43"
PHASE 1	B	644.0	SEMI/DUMP TRUCK - 25'	669.0	42° 06' 55.19"	87° 53' 53.00"
PHASE 1	C	642.4	SEMI/DUMP TRUCK - 25'	667.4	42° 06' 53.77"	87° 53' 51.25"
PHASE 1	D	643.0	SEMI/DUMP TRUCK - 25'	668.0	42° 06' 52.33"	87° 53' 55.10"
PHASE 1	E	644.0	SEMI/DUMP TRUCK - 25'	669.0	42° 06' 54.23"	87° 53' 55.57"
PHASE 2	F	640.0	SEMI/DUMP TRUCK - 25'	665.0	42° 07' 01.10"	87° 54' 04.45"
PHASE 2	G	640.0	SEMI/DUMP TRUCK - 25'	665.0	42° 07' 01.52"	87° 54' 04.67"
PHASE 2	H	640.0	SEMI/DUMP TRUCK - 25'	665.0	42° 07' 01.46"	87° 54' 06.66"
PHASE 2	I	640.0	SEMI/DUMP TRUCK - 25'	665.0	42° 07' 01.83"	87° 54' 03.60"
PHASE 2	J	640.1	SEMI/DUMP TRUCK - 25'	665.1	42° 07' 01.42"	87° 54' 03.38"
STAGING	K	642.1	SEMI/DUMP TRUCK - 25'	667.1	42° 07' 05.57"	87° 53' 43.68"

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS - 2

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CHICAGO EXECUTIVE AIRPORT

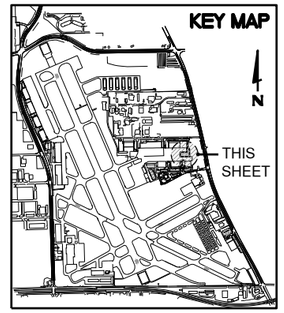
DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

UPDATE BY: Sheue Torng Lee
LAYOUT: Layout1
DATE: Wednesday, March 22, 2023 12:55:39 PM
FILE: K:\Chicago\Exec\Ap\21002031-00_ExpandEQuadApron\Draw\Sheets expansion-seq Notes Details - 2.dwg

NOTES

1. THE SITE DISTURBANCE IS MORE THAN 1 ACRE. THE PROJECT REQUIRES A NPDES GENERAL PERMIT NO. ILR10. THE CONTRACTOR WILL BE REQUIRED TO USE BEST CONSTRUCTION PRACTICES TO COMPLY WITH THE REQUIREMENTS OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) REQUIREMENTS FOR CONSTRUCTION SITE ACTIVITIES.
2. ALL PROJECT AREAS, INCLUDING STOCKPILES, HAUL ROADS AND STAGING AREAS, AS SHOWN ON THE PLANS OR COORDINATED IN THE FIELD, SHALL HAVE 4 INCHES OF TOPSOIL PLACED, BE SEEDED AND MULCHED IN ACCORDANCE WITH THE SPECIFICATIONS. AREAS DISTURBED WITHIN THE PROJECT LIMITS AND OUTSIDE THE PROJECT LIMITS WILL BE SEEDED AND MULCHED BY THE CONTRACTOR AT THEIR COST AND RESTORED TO ORIGINAL CONDITIONS.
3. IN AREAS WHERE WORK IS COMPLETE, PERMANENT STABILIZATION SHALL OCCUR WITHIN 7 DAYS OF COMPLETION, AND IN AREAS WHERE WORK WILL BE TEMPORARILY CEASED FOR 14 DAYS OR MORE, TEMPORARY STABILIZATION SHALL OCCUR BY THE 14TH DAY AFTER WORK HAS CEASED. TEMPORARY STABILIZATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. CONTRACTOR SHALL PROVIDE CONCRETE WASHOUT BOX FOR ALL CONCRETE READY MIX TRUCKS OR EACH READY MIX TRUCK SHALL BE EQUIPPED WITH A WASH OUT SYSTEM.
5. THE CONTRACTOR SHALL PERMANENTLY SEED AND HEAVY-DUTY HYDRAULIC MULCH ALL AREAS OF DISTURBED SOIL FOR ALL PROJECT IMPROVEMENT WORK.
6. THE CONTRACTOR SHALL TAKE PROVISIONS TO PREVENT EROSION AND STORM WATER POLLUTION WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS, SUCH AS STABILIZED CONSTRUCTION ENTRANCES, SILT FENCE, INLET PROTECTIONS, ETC. COSTS FOR INSTALLATION, MAINTENANCE AND REMOVAL OF EROSION CONTROL DEVICES WITHIN THE PROJECT LIMITS AND AT STAGING AREAS SHALL BE BORNE BY THE CONTRACTOR UNLESS OTHERWISE SHOWN ON THE PLANS.
7. THE GENERAL LAYOUT OF THE EARTH STOCKPILE TO ACCOMMODATE THE ESTIMATED QUANTITY OF EXCESS EARTH MATERIAL IS SHOWN. THE DIMENSIONS MAY BE MODIFIED BY THE RESIDENT ENGINEER.
8. THE HAULING, PLACEMENT, GRADING OF THE UNCLASSIFIED EXCAVATION, EARTH STORM SEWER SPOILS, TOPSOIL STRIPPING AND VOLUME CONTROL SITE EXCAVATION SPOILS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM ASSOCIATED WITH SAID WORK. SILT FENCE, SEEDING AND MULCHING ARE THE ONLY PAY ITEMS FOR THE EARTH STOCKPILE WORK.
9. THE FILL HEIGHT OF THE EARTH STOCKPILE SHALL BE A MAXIMUM OF 3'. THE PERIMETER SIDE SLOPES SHALL BE 6H:1V MAXIMUM. THE TOP OF THE BERM SHALL BE PEAKED IN THE CENTER AND DRAIN TO THE PERIMETER AT A 1.5% SLOPE.
10. EARTH MATERIALS SHALL BE SEPARATED BY TOPSOIL AND CLAY MATERIALS AS DIRECTED BY THE RESIDENT ENGINEER.
11. CONTRACTOR SHALL PLACE A 4" MINIMUM OF TOPSOIL AS NECESSARY TO ESTABLISH TURF.
12. CONTRACTOR'S HAUL ROAD RESTORATION TO ORIGINAL CONDITION SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
13. EXCESS UNCLASSIFIED EXCAVATION SHALL BE PLACED AT THE BOTTOM OF THE STOCKPILE. EXCESS TOPSOIL SHALL BE PLACED AT THE TOP AND ARE NOT TO INTERMINGLE.



LEGEND

- CONTRACTOR'S STORAGE AND STAGING AREA
- NEW BITUMINOUS PAVEMENT
- EXISTING AIRFIELD FENCE
- EXISTING VEHICULAR GATE
- EXISTING STORM SEWER
- EXISTING MANHOLE
- EXISTING INLET
- NEW STORM SEWER
- NEW MANHOLE
- NEW INLET
- NEW UNDERDRAIN
- DITCH FLOW LINE
- EXISTING BUILDING
- NEW PCC PAVEMENT
- EXISTING GAS
- EXISTING WATERMAIN
- EXISTING FUEL DISPENSING LINES
- EXISTING MONITORING WELL
- EXISTING TREE/SHRUB
- NEW TOPSOILING, SEEDING, AND HEAVY-DUTY MULCHING
- NEW INLET PROTECTION
- NEW SILT FENCE
- NEW DITCH CHECKS
- EXISTING CONTOUR
- NEW CONTOUR
- NEW STABILIZED CONSTRUCTION ENTRANCE (SEE SITE PLAN FOR OTHER LOCATIONS)
- CONCRETE WASHOUT FACILITY

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

STORM WATER POLLUTION PREVENTION PLAN

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

UPDATE BY: Sheue Tomg Lee
LAYOUT: Layout1
DATE: Wednesday, March 22, 2023 12:56:07 PM
FILE: K:\ChicagoExec\Apt\21002031-00 Expand\EQuadApron\Draw\Sheets expansion-swppp.dwg

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING NEW PCC PAVEMENT AT THE EAST QUADRANT GENERAL AVIATION APRON AT THE CHICAGO EXECUTIVE AIRPORT. THE PROJECT INCLUDES EARTH EXCAVATION, VARIOUS PAVEMENT ITEMS AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL. SUCH AS INLET PROTECTION AND SILT FENCE.
2. REMOVAL OF A PORTION OF THE BITUMINOUS ACCESS ROAD AND REMOVAL OF SELECTED EXISTING PCC PANELS.
3. EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS TO GRADE OUT FOR THE PROPOSED PAVEMENT IMPROVEMENTS.
4. PAVEMENT CONSTRUCTION, VOLUME CONTROL INSTALLATION AND STOCKPILE CONSTRUCTION.
5. PAVEMENT MARKING AND OTHER MISCELLANEOUS ITEMS.
6. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING WITHIN PROPOSED PROJECT GRADING LIMITS AND IN AREAS DISTURBED BY CONTRACTOR STAGING, STORAGE AND HAUL OPERATIONS.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 2 ACRES WHICH WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE DES PLAINES RIVER THROUGH A STORM SEWER SYSTEM.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTIONS SHALL BE INSTALLED AS CALLED OUT IN THE PLAN AND DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN (14) CALENDAR DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCLUDED INCLUDED IN THE UNIT BID PRICE FOR THE VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

DOCUMENTATION:

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF INTENT (NOI)" PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF THIS IS NOT POSSIBLE, THEN IT MAY BE PERMITTED TO POST THIS NOTICE IN A LOCAL PUBLIC BUILDING. THE SIGN OR NOTICE MUST CONTAIN THE FOLLOWING:

1. A COPY OF THE COMPLETED NOTICE OF INTENT (NOI) AS SUBMITTED TO THE IEPA
2. THE LOCATION OF THE SWPPP AND NAME AND 24/7 TELEPHONE NUMBER OF THE CONTACT PERSON.

THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AND UPDATE AN "AS-BUILT" SET OF STORM WATER POLLUTION PREVENTION PLANS IN THE PROJECT FILES. THE SWPPP SHALL BE UPDATED WITHIN 7-DAYS OF ANY MODIFICATIONS TO THE PLANS. THE SWPPP AND ALL REVISIONS SHALL BE RETAINED FOR THREE YEARS AFTER FINAL STABILIZATION OF THE SITE, WHICH SHALL BE DEFINED AS VEGETATION COVER OF AT LEAST 70% OF HISTORIC CONDITIONS.

A STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (FORM BC 2259) SHALL BE BE COMPLETED WITH INSPECTION FREQUENCIES AS OUTLINED HEREIN. SWPPP REPORTS SHALL BE RETAINED FOR THREE YEARS AFTER THE DATE OF FINAL STABILIZATION AS DEFINED HEREIN.

IF ANY VIOLATION OF THE PROVISIONS OF THE PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION COVERED IN THIS PLAN, THE ENGINEER AND/OR CONTRACTOR SHALL COMPLETE AND FILE AN "INCIDENT OF NONCOMPLIANCE (ION)" REPORT FOR THE IDENTIFIED VIOLATION. THE FORMS SHALL BE AS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND SHALL INCLUDE SPECIFIC INFORMATION ON THE INCIDENT THAT CAUSED NONCOMPLIANCE, ACTIONS THAT WERE TAKEN TO CORRECT THE NONCOMPLIANCE AND TO PREVENT ITS' REOCCURRENCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G. OF THE GENERAL PERMIT.

AFTER PROJECT FINAL ACCEPTANCE, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF TERMINATION (NOT)" FORM PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY. FORMS FOR THE IEPA SHALL BE MAILED TO THE FOLLOWING ADDRESS"

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL, MAIL CODE #15
ATTN: PERMIT SECTION
1021 NORTH GRAND AVENUE EAST
P.O. BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276

NPDES PERMIT # _____

DATE ISSUED _____

DATE EXPIRED _____

DATE TERMINATED _____

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

1. ALL TREE PROTECTION (COST INCIDENTAL), SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, COOK COUNTY, CHICAGO EXECUTIVE AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING. TEMPORARY EROSION CONTROL MEASURES WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

CONTRACTOR CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PROJECT INFORMATION:

ROUTE: CHICAGO EXECUTIVE AIRPORT
MARKED: EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON
SECTION: 13
PROJECT NUMBER: PWK-4795
COUNTY: COOK
CONTRACT NUMBER: _____

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: _____ DATE: _____
PRINTED NAME: _____ TITLE: _____
NAME OF FIRM: _____
STREET ADDRESS: _____
CITY, STATE, ZIP: _____
PHONE NUMBER: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.

IL. CONTRACT: PA065
IL. LETTING ITEM: 11A
IL. PROJECT: PWK-4795
S.B.G. PROJECT: N/A

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

STORM WATER POLLUTION PREVETION PLAN NOTES AND DETAILS - 1

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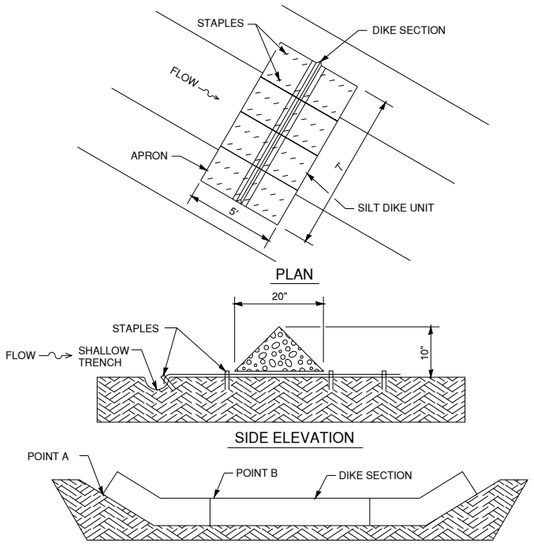
CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

SHEET 8 OF 27 SHEETS

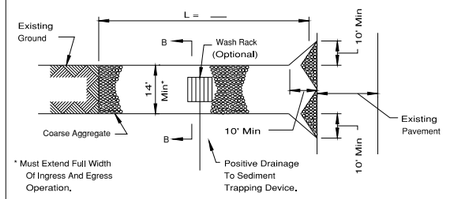
UPDATE BY: Sheue Torng Lee
LAYOUT: Layout1
DATE: Wednesday, March 22, 2023 12:56:14 PM
FILE: K:\ChicagoExec\Ap\21002031-00_ExpandEQuadApron\Draw\Sheets\storm Water Pollution Prevention Plan Notes And Details - 1.dwg



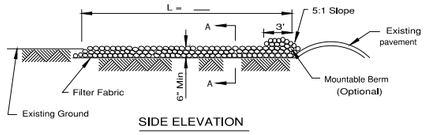
URETHANE FOAM/GEOTEXTILE DITCH CHECK

NOT TO SCALE

STABILIZED CONSTRUCTION ENTRANCE PLAN



PLAN VIEW



SIDE ELEVATION

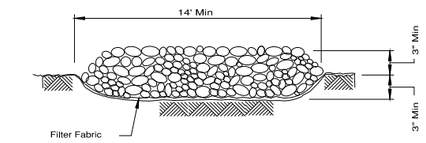
NOTES:

1. Filter fabric shall meet the requirements of material specification 592 GEOTEXTILE, Table I or 2, Class I, II or IV and shall be placed over the cleared area prior to the placing of rock.
2. Rock or reclaimed concrete shall meet one of the following IDOT coarse aggregate gradation, CA-1, CA-2, CA-3 or CA-4 and be placed according to construction specification 25 ROCKFILL using placement Method 1 and Class III compaction.
3. Any drainage facilities required because of washing shall be constructed according to manufacturers specifications.
4. If wash racks are used they shall be installed according to the manufacturer's specifications.

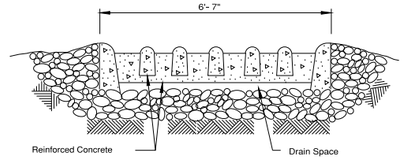
REFERENCE		STANDARD DWG. NO.	
Project			IL-630
Designed	Date		SHEET 1 OF 2
Checked	Date		DATE 8-18-94
Approved	Date		

NRCS Natural Resources Conservation Service

STABILIZED CONSTRUCTION ENTRANCE PLAN



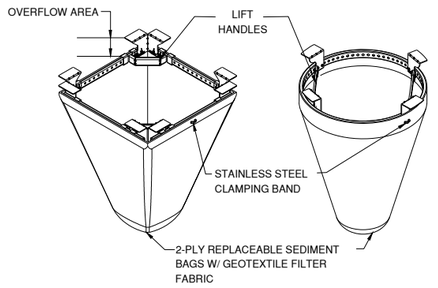
SECTION A-A



SECTION B-B

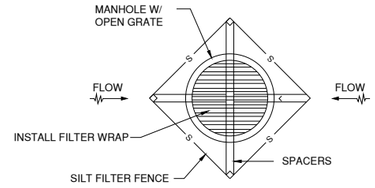
REFERENCE		STANDARD DWG. NO.	
Project			IL-630
Designed	Date		SHEET 2 OF 2
Checked	Date		DATE 8-18-94
Approved	Date		

NRCS Natural Resources Conservation Service



INLET PROTECTION - SILT BASKET (PAVEMENT AND TURF)

NOT TO SCALE
FOR ALL RECTANGULAR AND CIRCULAR INLETS



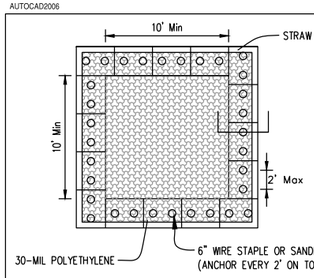
INLET PROTECTION (INLET/MANHOLES - IN TURF)

NOT TO SCALE
IDOT STANDARD 280001-07

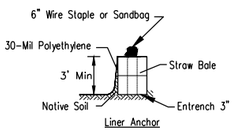
NOTES FOR INLET PROTECTION DETAILS

1. FILTER WRAP TO BE PLACED IN ALL SLOPE BOX INLETS, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
2. FABRIC SHALL BE IN CONFORMANCE WITH ARTICLE 1080.03 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2022.
3. FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
5. FABRIC SHALL REMAIN IN PLACE UNTIL COMPLETION OF PAVEMENT REHABILITATION.
6. COST OF FILTER WRAP AND MAINTENANCE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

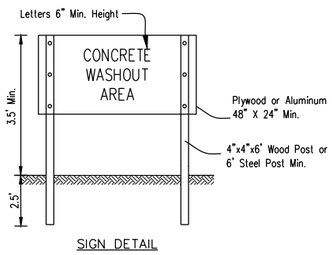
AUTOCAD2006



PLAN VIEW



STRAW BALE ANCHOR SECTIONS



SIGN DETAIL

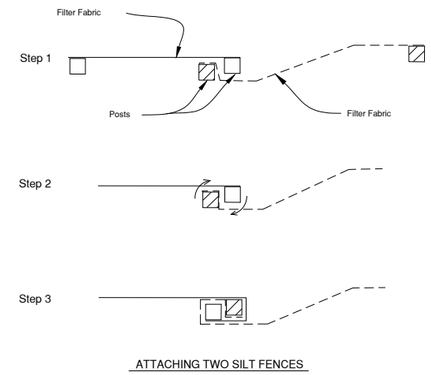
NOTES:

1. Maintaining temporary concrete washout facilities shall include removing and disposing of hardend concrete and/or slurry and returning the facilities to a functional condition.
2. Facility shall be cleaned or reconstructed in a new area once washout becomes two-thirds full.
3. The contractor may propose alternate configurations/materials or a pre-fabricated washout container for approval by the resident engineer at no additional cost to the project.
4. The construction, maintenance, materials, removals, labor and all other incidentals necessary to construct the washout facility will not be measured for payment but will be considered incidental to the contract.

TEMPORARY CONCRETE WASHOUT FACILITY - STRAW BALE

Designed	Date
Drawn	6/08
Checked	Date
Approved	Date

SILT FENCE - SPLICING TWO FENCES

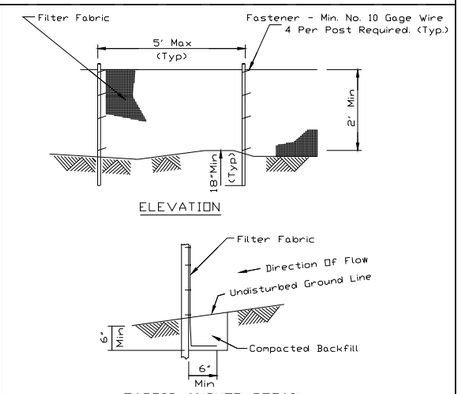


ATTACHING TWO SILT FENCES

1. Place the end post of the second fence inside the end post of the first fence.
2. Rotate both posts at least 180 degrees in a clockwise direction to create a tight seal with the fabric material.
3. Cut the fabric near the bottom of the stakes to accommodate the 6" flap.
4. Drive both posts a minimum of 18 inches into the ground and bury the flap.
5. Compact backfill (particularly at splices) completely to prevent stormwater piping.

REFERENCE		STANDARD DWG. NO.	
Project			IUM-620B(W)
Designed	Date		SHEET 1 OF 1
Checked	Date		DATE 3-16-2012
Approved	Date		

SILT FENCE PLAN



NOTES:

1. Temporary sediment fence shall be installed prior to any grading work in the area to be protected. They shall be maintained throughout the construction period and removed in conjunction with the final grading and site stabilization.
2. Filter fabric shall meet the requirements of material specification 592 Geotextile Table 1 or 2, Class I with equivalent opening size of at least 30 for nonwoven and 40 for oven.
3. Fence posts shall be either ~~standard steel post or~~ wood post with a minimum cross-sectional area of 3.0 sq. in.

SILT FENCE NOTES

1. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.

REFERENCE		STANDARD DWG. NO.	
Project			IUM-620A
Designed	Date		SHEET 1 OF 2
Checked	Date		DATE 3-16-12
Approved	Date		

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

STORM WATER POLLUTION PREVENTION PLAN NOTES AND DETAILS - 2

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CHICAGO EXECUTIVE AIRPORT

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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

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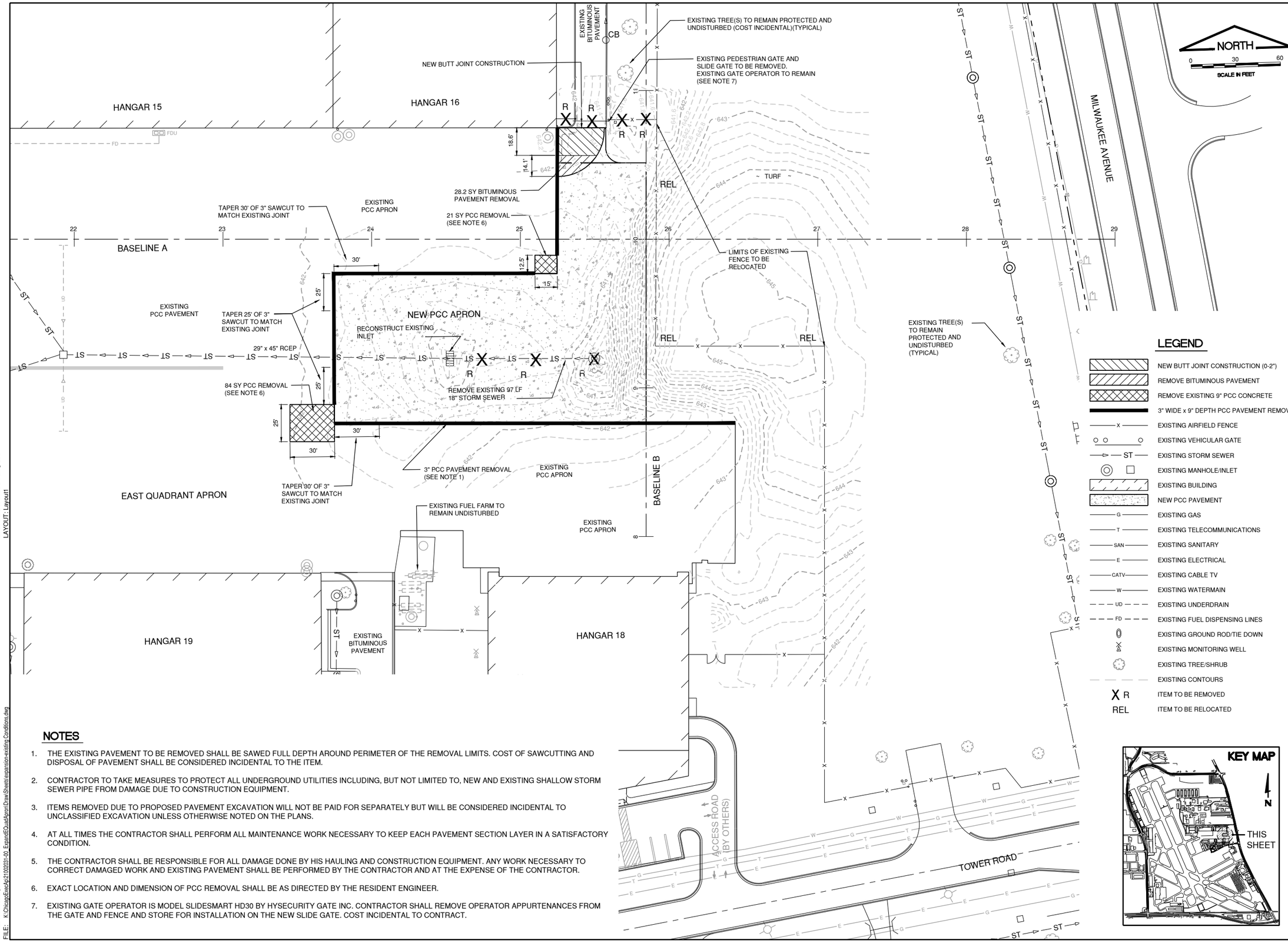
UPDATE BY: Sheue Torng Lee
LAYOUT: Layout1
DATE: Wednesday, March 22, 2023 12:56:19 PM
FILE: K:\ChicagoExecAp\21002031-00_Expand5QuadApron\Drawr\Sheets\storm Water Pollution Prevention Plan Notes And Details - 2.dwg



NOTES

1. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
2. CONTRACTOR TO TAKE MEASURES TO PROTECT ALL UNDERGROUND UTILITIES INCLUDING, BUT NOT LIMITED TO, NEW AND EXISTING SHALLOW STORM SEWER PIPE FROM DAMAGE DUE TO CONSTRUCTION EQUIPMENT.
3. ITEMS REMOVED DUE TO PROPOSED PAVEMENT EXCAVATION WILL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED INCIDENTAL TO UNCLASSIFIED EXCAVATION UNLESS OTHERWISE NOTED ON THE PLANS.
4. AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
6. EXACT LOCATION AND DIMENSION OF PCC REMOVAL SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
7. EXISTING GATE OPERATOR IS MODEL SLIDESMART HD30 BY HYSECURITY GATE INC. CONTRACTOR SHALL REMOVE OPERATOR APPURTENANCES FROM THE GATE AND FENCE AND STORE FOR INSTALLATION ON THE NEW SLIDE GATE. COST INCIDENTAL TO CONTRACT.

LEGEND

- NEW BUTT JOINT CONSTRUCTION (0-2")
- REMOVE BITUMINOUS PAVEMENT
- REMOVE EXISTING 9" PCC CONCRETE
- 3" WIDE x 9" DEPTH PCC PAVEMENT REMOVAL
- EXISTING AIRFIELD FENCE
- EXISTING VEHICULAR GATE
- EXISTING STORM SEWER
- EXISTING MANHOLE/INLET
- EXISTING BUILDING
- NEW PCC PAVEMENT
- EXISTING GAS
- EXISTING TELECOMMUNICATIONS
- EXISTING SANITARY
- EXISTING ELECTRICAL
- EXISTING CABLE TV
- EXISTING WATERMAIN
- EXISTING UNDERDRAIN
- EXISTING FUEL DISPENSING LINES
- EXISTING GROUND ROD/TIE DOWN
- EXISTING MONITORING WELL
- EXISTING TREE/SHRUB
- EXISTING CONTOURS
- X R — ITEM TO BE REMOVED
- REL — ITEM TO BE RELOCATED

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

EXISTING CONDITIONS/PROPOSED REMOVALS

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CHICAGO EXECUTIVE AIRPORT

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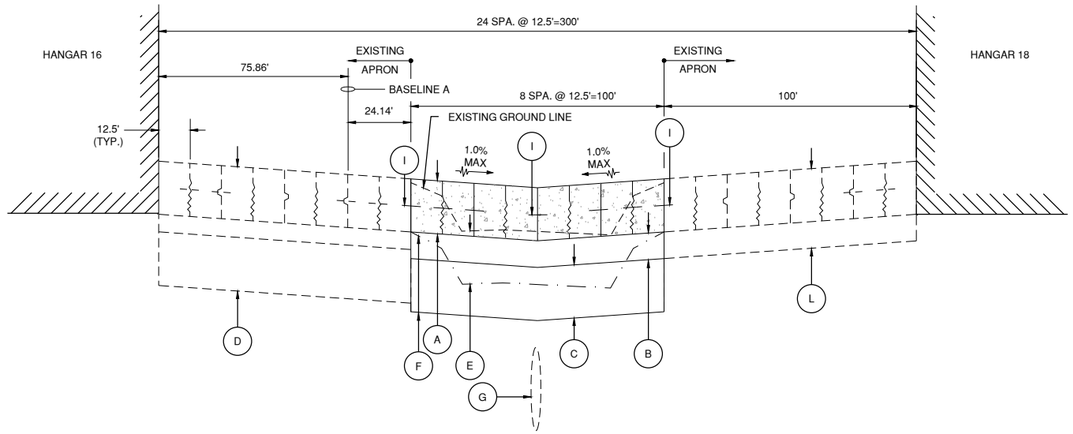
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

SHEET 10 OF 27 SHEETS

UPDATE BY: Sheue Tomg Lee
DATE: Wednesday, March 22, 2023 12:56:29 PM
LAYOUT: Layout1
FILE: K:\ChicagoExecAp\21002031-00_Expand5QuadApron\Drawi\Sheets expansion-existing Conditions.dwg

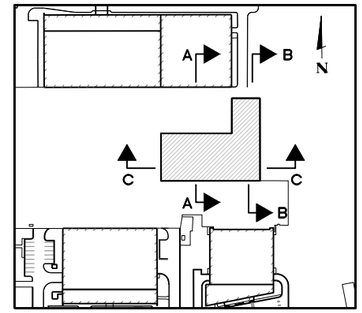


STA 23+74.72 TO STA 25+24.84 BASELINE A SECTION A-A

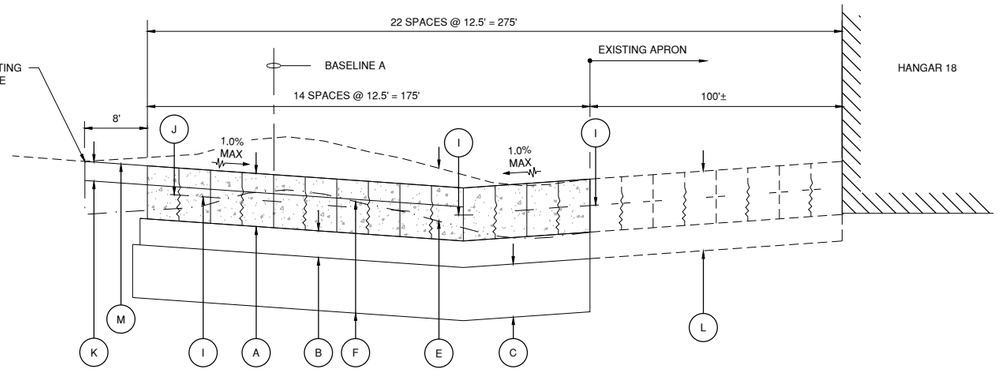
NOT TO SCALE

LEGEND

- (A) NEW 9" PCC PAVEMENT (501)
- (B) NEW 6" CRUSHED AGGREGATE BASE COURSE (209)
- (C) NEW 12" POROUS GRANULAR EMBANKMENT (208) WITH SOIL STABILIZATION FABRIC PLACED AT BOTTOM (152)
- (D) EXISTING PAVEMENT STRUCTURE:
  9" PCC PAVEMENT
  4" ASPHALT TREATED PERMEABLE BASE COURSE
  12" LIME MODIFIED SUBGRADE
- (E) AVERAGE 12" TOPSOIL STRIPPING (152)
- (F) UNCLASSIFIED EXCAVATION (152)
- (G) EXISTING 29"x45" RCEP STORM SEWER
- (H) EXISTING 18" RCP STORM SEWER
- (I) NEW DOWEL BAR (501)
- (J) NEW TIE BAR (501)
- (K) MINIMUM 4" TOPSOIL PLACEMENT (905)
- (L) EXISTING PAVEMENT STRUCTURE
  9" PCC PAVEMENT
  6" CA-6
- (M) NEW SEEDING (800159) AND MULCHING (800162)

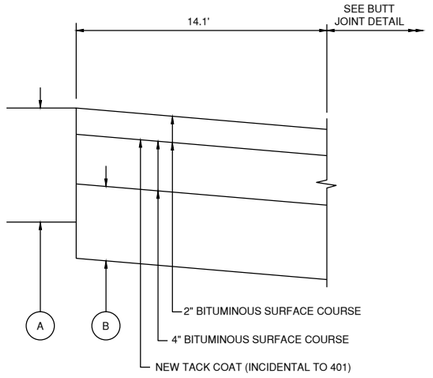


KEY MAP



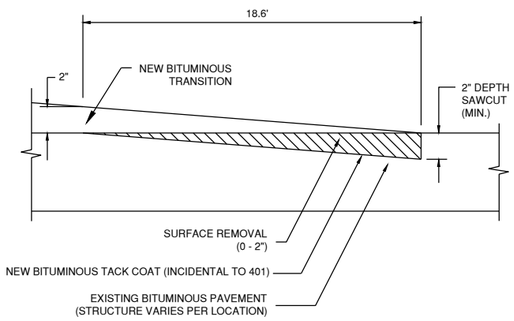
STA 25+24.84 TO STA 25+84.57 BASELINE A SECTION B-B

NOT TO SCALE



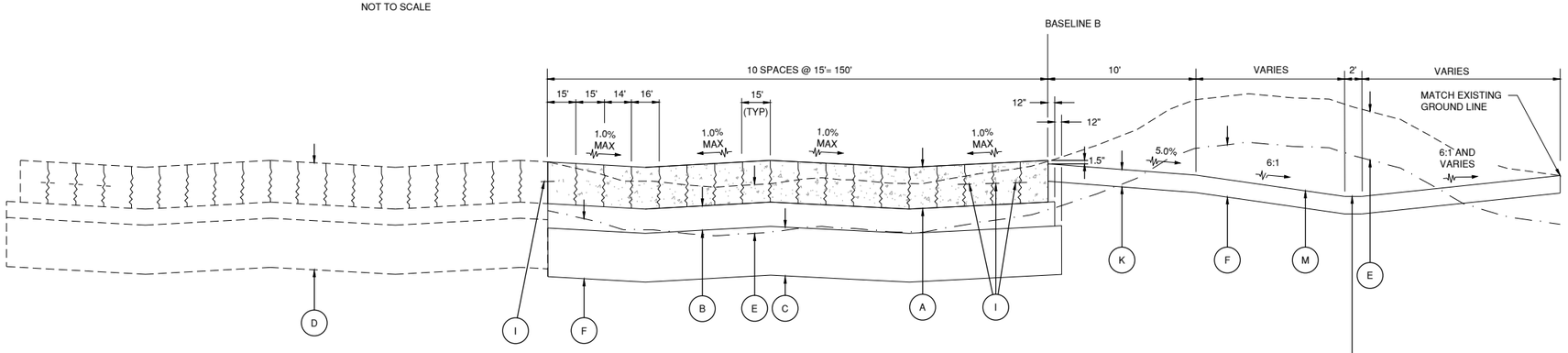
NEW HMA PAVEMENT DETAIL

NOT TO SCALE



BUTT JOINT CONSTRUCTION DETAIL (AR401655)

NOT TO SCALE



STA 8+76.36 TO STA 9+76.36 BASELINE B SECTION C-C

NOT TO SCALE

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

TYPICAL SECTIONS

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CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

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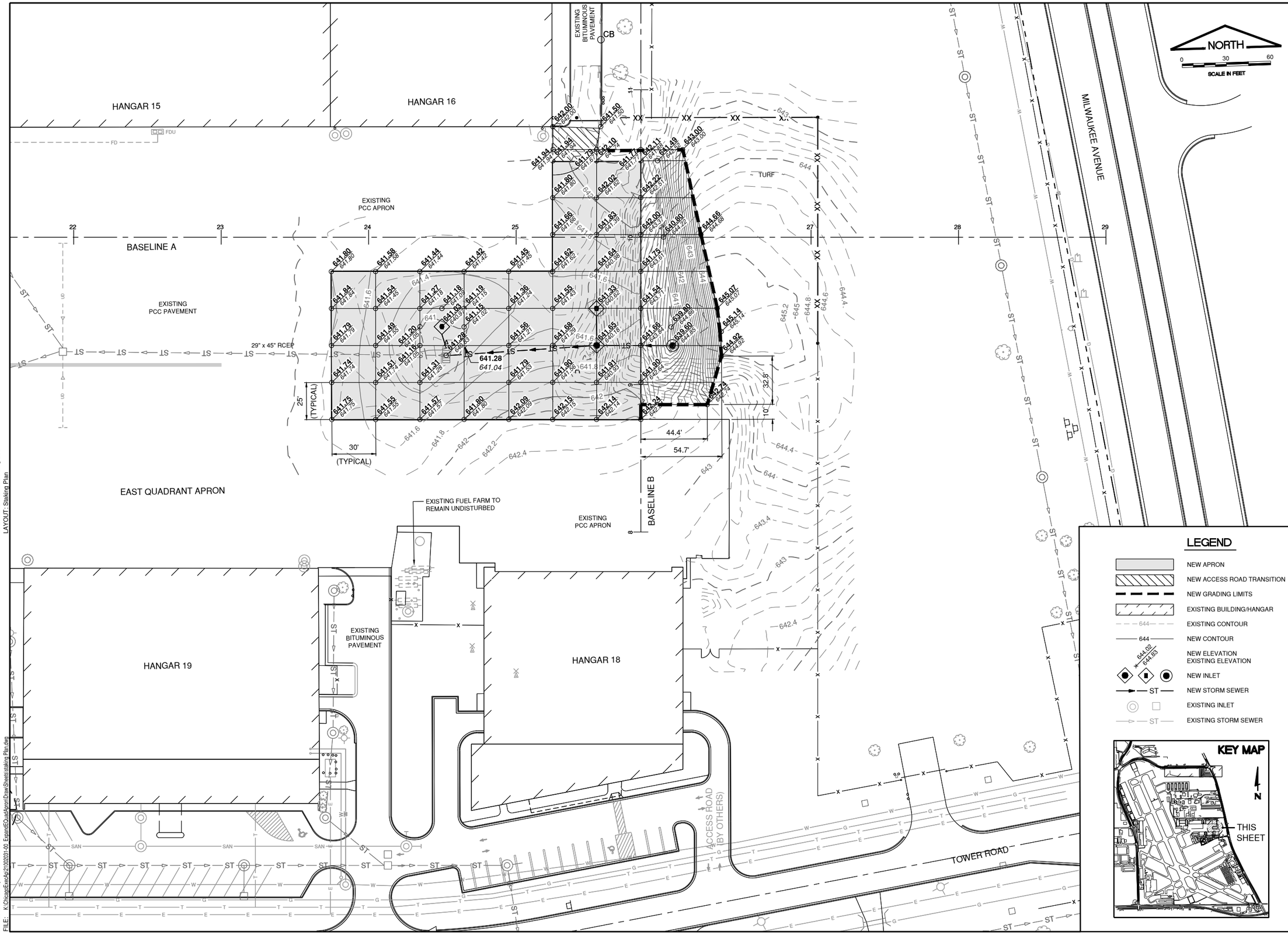
SHEET 11 OF 27 SHEETS

UPDATE BY: Sheue Torng Lee
LAYOUT: Layout1
DATE: Wednesday, March 22, 2023 12:56:36 PM
FILE: K:\ChicagoExecAp\21002031-00_ExpandEQuadApron\Draw\Sheets\typical Sections.dwg



IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

LEGEND

- NEW APRON
- NEW ACCESS ROAD TRANSITION
- NEW GRADING LIMITS
- EXISTING BUILDING/HANGAR
- EXISTING CONTOUR
- NEW CONTOUR
- NEW ELEVATION / EXISTING ELEVATION
- NEW INLET
- NEW STORM SEWER
- EXISTING INLET
- EXISTING STORM SEWER

KEY MAP

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

STAKING PLAN

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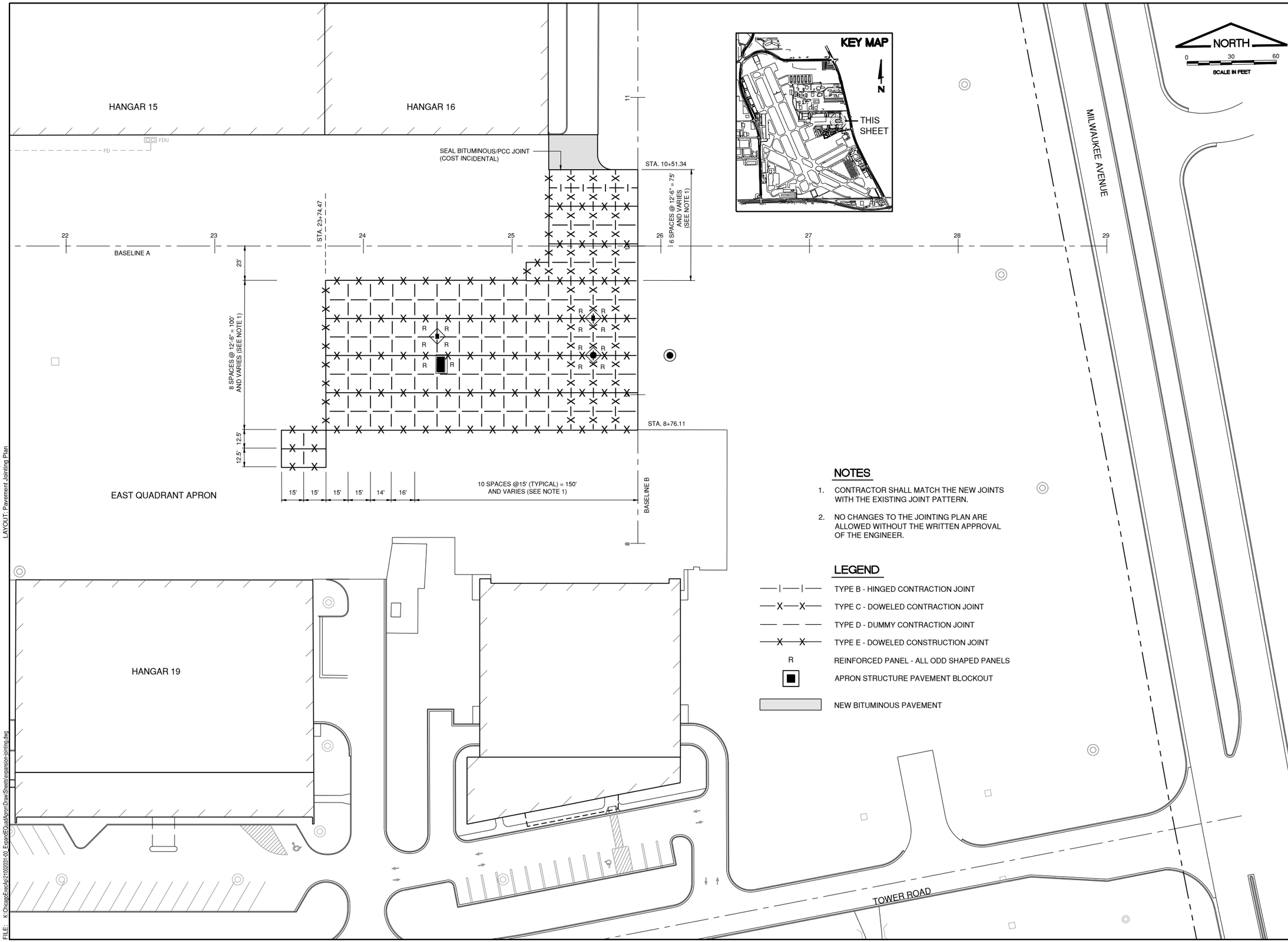
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

SHEET 12 OF 27 SHEETS

UPDATE BY: Sheue Tomg Lee
LAYOUT: Staking Plan
DATE: Wednesday, March 22, 2023 12:56:59 PM
FILE: K:\ChicagoExecAp\21002031-00 ExpandEQuadApron\Draw\Sheets\staking Plan.dwg



NOTES

1. CONTRACTOR SHALL MATCH THE NEW JOINTS WITH THE EXISTING JOINT PATTERN.
2. NO CHANGES TO THE JOINTING PLAN ARE ALLOWED WITHOUT THE WRITTEN APPROVAL OF THE ENGINEER.

LEGEND

- TYPE B - HINGED CONTRACTION JOINT
- TYPE C - DOWELED CONTRACTION JOINT
- TYPE D - DUMMY CONTRACTION JOINT
- TYPE E - DOWELED CONSTRUCTION JOINT
- R — REINFORCED PANEL - ALL ODD SHAPED PANELS
- APRON STRUCTURE PAVEMENT BLOCKOUT
- NEW BITUMINOUS PAVEMENT

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
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CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

PAVEMENT JOINTING PLAN

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DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

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SHEET 13 OF 27 SHEETS

UPDATE BY: Sheue Torng Lee
LAYOUT: Pavement Jointing Plan
DATE: Wednesday, March 22, 2023 12:57:06 PM
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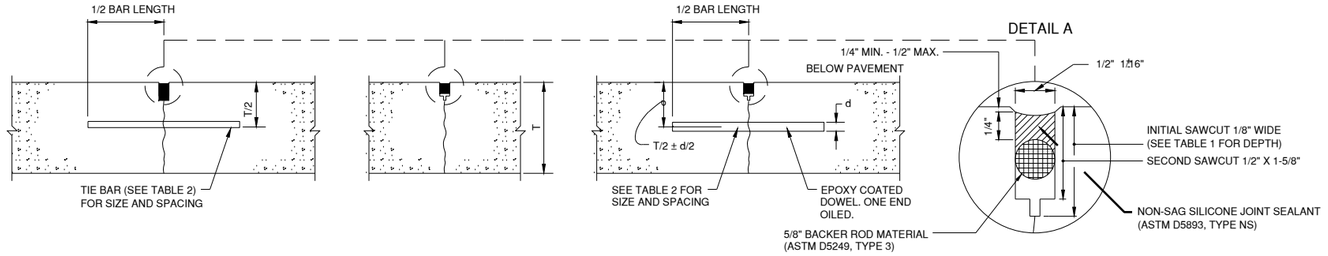


TYPE B HINGED (TIED)

SYMBOL — I — I —

TYPE D DUMMY

SYMBOL — —

TYPE C DOWELED

SYMBOL —X—X—

CONTRACTION JOINTS

NOTES

1. THE CONTRACTOR SHALL VISIT THE PROJECT SITE AND ACQUAINT HIMSELF WITH THE REQUIRED WORK. CONTRACTOR SHALL BE FAMILIAR TO THE SITE AND REQUIRED WORK PRIOR TO BID. SITE VISITS SHALL BE COORDINATED WITH THE AIRPORT MANAGER AT (630) 256-3120.
2. PRIOR TO ORDERING NEW MATERIAL, CONTRACTOR SHALL FIELD MEASURE FOR JOINTING MATERIAL SIZES AND AMOUNT. MEASUREMENTS SHALL ALSO DETERMINE BACKER ROD SIZE AND QUANTITIES. ALSO PER NEW JOINTS DETAILS, DEPTH OF NEW JOINT SEALANT SHALL BE AS RECOMMENDED BY MANUFACTURER. NO EXTRA COMPENSATION WILL BE MADE FOR EXCESS MATERIALS NOT USED.

JOINT NOTES

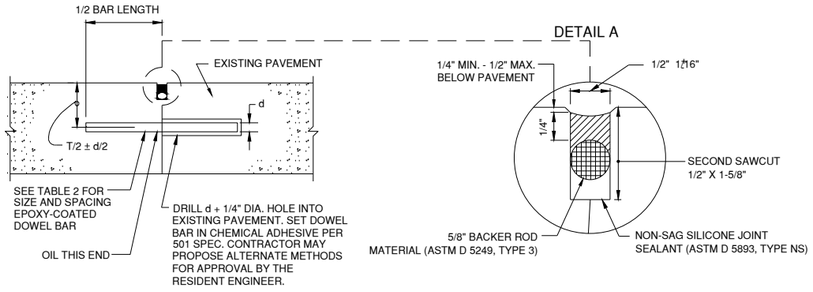
1. ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
2. THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
3. ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY, WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
4. ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
5. TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RESTRAIGHTEND DURING CONSTRUCTION. TIE BARS DESIGNATED AS GRADE 40 IN ASTM A615 CAN BE USED FOR CONSTRUCTION REGARDING BENT BARS.
6. THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
7. JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
8. COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
9. SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
10. DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
11. CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS PER THE DETAIL ON THIS SHEET AT NO ADDITIONAL COST.

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

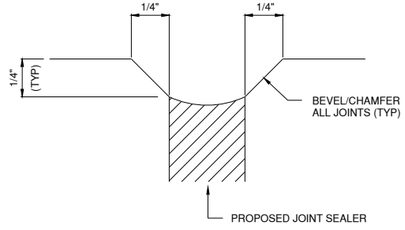
0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



TYPE E DOWELED

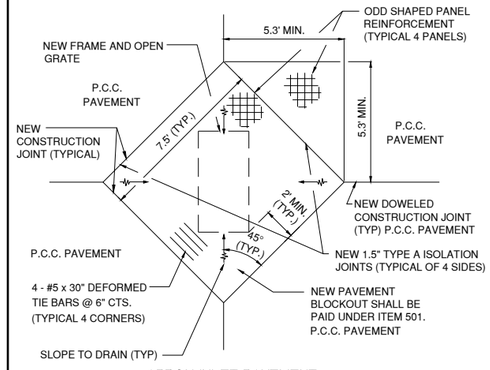
SYMBOL —X—X—X—

CONSTRUCTION JOINTS



CHAMFER/BEVEL JOINT DETAIL

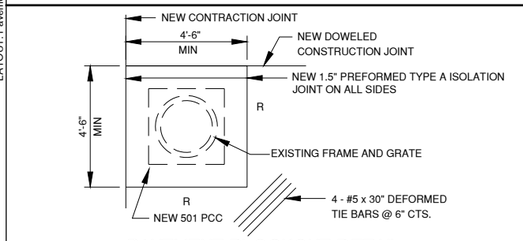
NOT TO SCALE



APRON INLET PAVEMENT BLOCKOUT DETAIL

N.T.S.

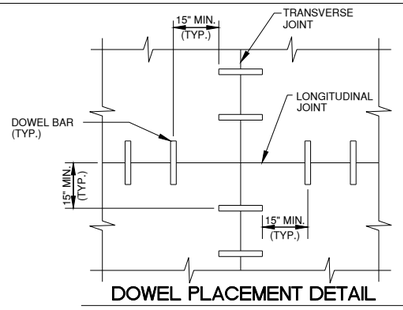
NOTE: ABOVE BLOCKOUT SHALL BE CONSTRUCTED FOR ALL NEW APRON INLETS



PAVEMENT BLOCKOUT DETAIL FOR EXISTING MANHOLES/INLETS

NOT TO SCALE

1. ABOVE BLOCKOUT SHALL BE CONSTRUCTED FOR EXISTING MANHOLE/ INLET CONSTRUCTION. DIMENSION OF BOXOUT MAY VARY TO FIT FIELD CONDITIONS.
2. R - DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WIRE FABRIC AS SHOWN ON THIS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)
3. ALL CONCRETE AND OTHER MATERIALS SHALL BE IN CONFORMANCE WITH THE SPECIFICATION FOR ITEMS 501 AND 605.



DOWEL PLACEMENT DETAIL

NOT TO SCALE

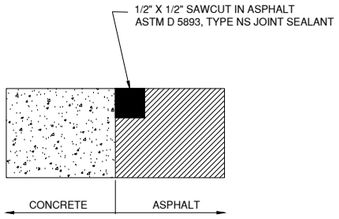
DIMENSION TABLES

TABLE 1

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES T=(T/4) ±1/4"
9	2.25"

TABLE 2

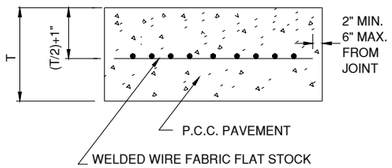
PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA. (d)	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
9	1"	19"	12"	#5	30"	30"



JOINT SEALING AT CONCRETE ASPHALT INTERFACE

N.T.S.

NOTE: JOINT DIMENSIONS SHALL MEET MANUFACTURER'S RECOMMENDATION



ODD SHAPED PANEL REINFORCEMENT

SYMBOL R

NOTE: REINFORCEMENT SHALL NOT CROSS ANY JOINT

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

PAVEMENT JOINTING DETAILS

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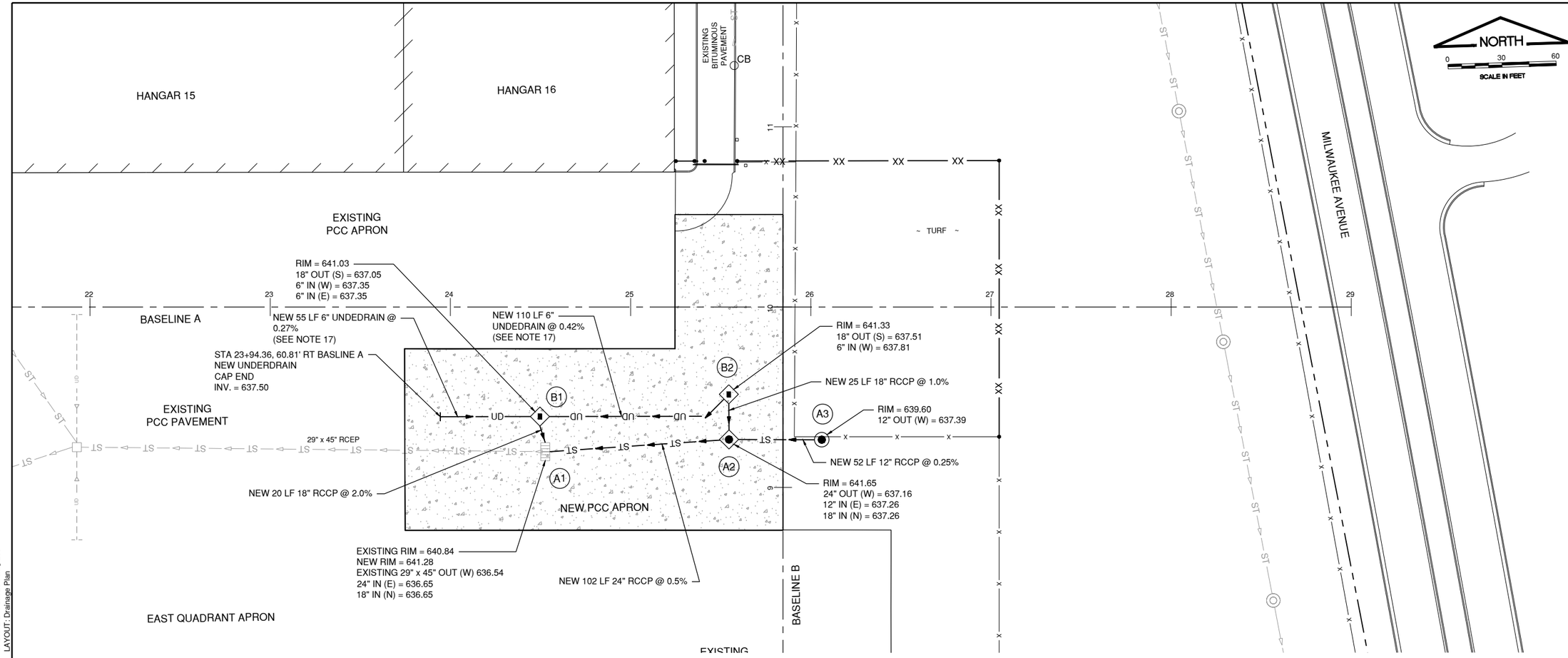
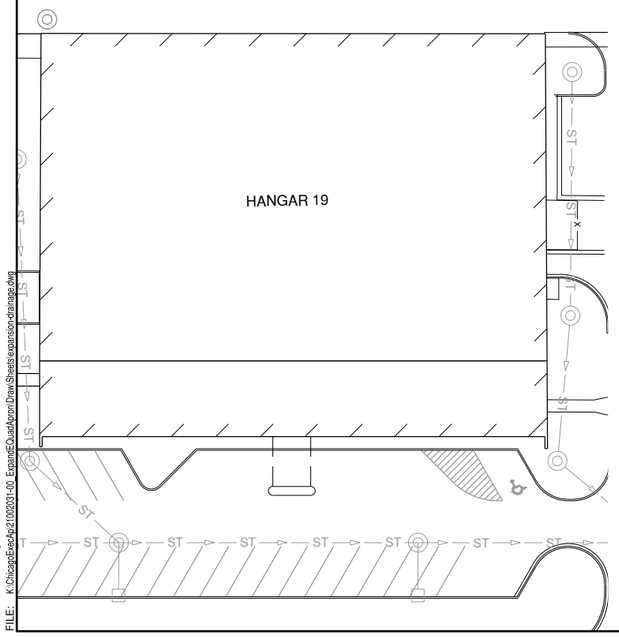
CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

UPDATE BY: Sheue Torng Lee
LAYOUT: Pavement Jointing Details
DATE: Wednesday, March 22, 2023 12:57:14 PM
FILE: K:\ChicagoExecApt\21002031-00_ExpandEQuadApron\Draw\Sheets\expansion-jointing Details.dwg



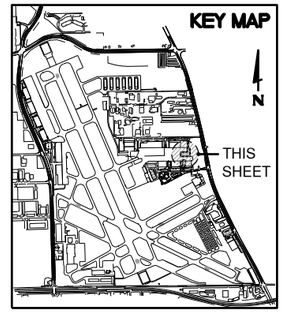


DRAINAGE NOTES

1. DURING CONSTRUCTION OPERATIONS THE CONTRACTOR SHALL ENSURE POSITIVE SITE DRAINAGE AT THE CONCLUSION OF EACH DAY. SITE DRAINAGE MAY BE ACHIEVED BY DITCHING, PUMPING OR ANY OTHER METHOD ACCEPTABLE TO THE ENGINEER.
2. FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE DRAINAGE STRUCTURE COST.
3. CEMENT BRICKS AND NON-SHRINK MORTAR SHALL BE USED IN ALL STORM STRUCTURES FOR PIPE CONNECTIONS.
4. BEFORE ORDERING PIPE CULVERTS, STORM SEWER, INLETS OR MANHOLES, THE CONTRACTOR SHALL VERIFY THE DEPTH OF EXISTING DOWNSTREAM STRUCTURES OR PIPES FOR CONNECTION.
5. DURING CONSTRUCTION OPERATIONS, WHEN ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR DRAINAGE STRUCTURES IMPEDING THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY.
6. THE COST OF CONNECTING EXISTING STORM SEWERS AND REMOVAL OF EXISTING CONCRETE "BENCHES" TO THE PROPOSED DRAINAGE SYSTEM SHALL BE INCIDENTAL TO THE CONTRACT.
7. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS, OR CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS. CONTRACTOR SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL PERMANENT CONNECTIONS WITH THE SEWERS ARE BUILT AND IN SERVICE. DISCHARGED WATER MUST BE FILTERED TO THE SATISFACTION OF THE ENGINEER AND SHALL BE VISIBLY FREE OF SEDIMENT. THIS WORK SHALL NOT BE PAID FOR DIRECTLY, BUT WILL BE INCLUDED IN THE COST OF STORM SEWER ITEMS BEING INSTALLED.
8. DRAINAGE STRUCTURE GRADES SHALL BE VERIFIED BY THE CONTRACTOR IN THE FIELD PRIOR TO INSTALLATION OF DRAINAGE ITEMS. GRADES OF EXISTING SEWER LINES WERE DETERMINED FROM AVAILABLE PLANS AND SURVEY. THE INVERTS OF THE PROPOSED DRAINAGE MAY REQUIRE REVISIONS TO MEET THE EXISTING FIELD CONDITIONS. ANY ADJUSTMENTS SHALL BE DIRECTED BY THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PRICE OF THE PROPOSED STORM SEWER AND STRUCTURES.
9. BEFORE FINAL ACCEPTANCE OF THE PROJECT, ALL PROPOSED AND EXISTING STORM SEWER LINES AND STRUCTURES AFFECTED BY PROJECT LIMIT SHALL BE CLEANED AS DIRECTED BY THE ENGINEER. CLEANING OF THE PROPOSED AND EXISTING STORM SEWER LINES AND STRUCTURES IS CONSIDERED TO BE INCLUDED IN THE COST OF THE DRAINAGE ITEM.
10. WHEN STORM SEWER IS CONSTRUCTED NEAR WATER MAIN, SEPARATION REQUIREMENTS SHALL BE MET IN ACCORDANCE WITH THE ILLINOIS STANDARD SPECIFICATIONS FOR WATER AND SEWER CONSTRUCTION.
11. THE CONTRACTOR SHALL VERIFY ALL ELEVATIONS AND FLOWLINES PRIOR TO ORDERING NEW MANHOLE AND INLET STRUCTURES.
12. IF DURING MANHOLE ADJUSTMENTS, THE CONTRACTOR DAMAGES EITHER THE FRAME OR LID, THEY SHALL FURNISH A NEW FRAME AND LID, SAME OR EQUAL, AT NO ADDITIONAL COST. ALL NEW MANHOLES AND INLETS AND THOSE TO BE RECONSTRUCTED SHALL BE CONSTRUCTED WITH NEW FRAMES AND GRATES.
13. NEW MANHOLE AND INLET LIDS ON THIS PROJECT SHALL HAVE THE WORD "STORM" ON THE LID. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN THE APPROPRIATE TYPE OF LID. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK. .
14. THE CONTRACTOR SHALL CALL JULIE AT 811 OR 1-800-892-0123 48 HOURS PRIOR TO CONSTRUCTION FOR CONFIRMATION OF CURRENT UTILITY LOCATIONS AND FOR ALL NON-EMERGENCY WORK. THESE ARE THE KNOWN UTILITIES LOCATED WITHIN THE PROJECT LIMITS OR IMMEDIATELY ADJACENT TO THE PROJECT CONSTRUCTION LIMITS. UTILITIES WHICH ARE NOT MEMBERS OF JULIE SHOULD BE NOTIFIED INDIVIDUALLY BY THE CONTRACTOR.
15. ALL TESTING, FITTINGS, BEDDING AND GRANULAR CRADLE WHERE NECESSARY, SHALL BE INCLUDED IN THE INSTALLATION OF UNDERGROUND FACILITIES. TRENCH BACKFILL IS REQUIRED WHEREVER UNDERGROUND PIPING AND UTILITIES PASS BENEATH OR WITHIN 2 FEET OF THE PAVEMENT, SIDEWALK OR CURB.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRS TO ANY UTILITY LINES AND EXISTING IMPROVEMENTS TO REMAIN THAT ARE DAMAGED AS A RESULT OF THE WORK.
17. THE CONTRACTOR SHALL PROTECT SHALLOW UNDERDRAIN DURING CONSTRUCTION. ANY UNDERDRAIN DAMAGED DURING CONSTRUCTION SHALL BE REMOVED AND REPLACED AT NO ADDITIONAL COST.

LEGEND

- NEW PCC LIMITS
- EXISTING MANHOLE/INLET
- NEW STORM SEWER STRUCTURE
- UD — EXISTING UNDERDRAIN
- ST — EXISTING STORM SEWER
- ST — NEW STORM SEWER
- UD — NEW UNDERDRAIN



IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DRAINAGE PLAN

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

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DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

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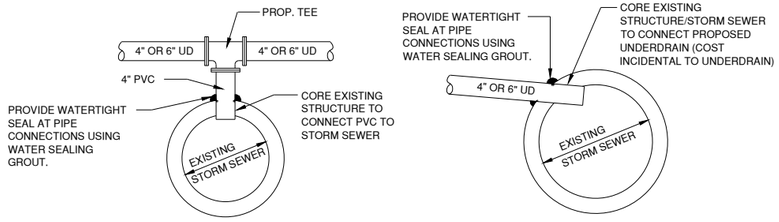
SHEET 15 OF 27 SHEETS

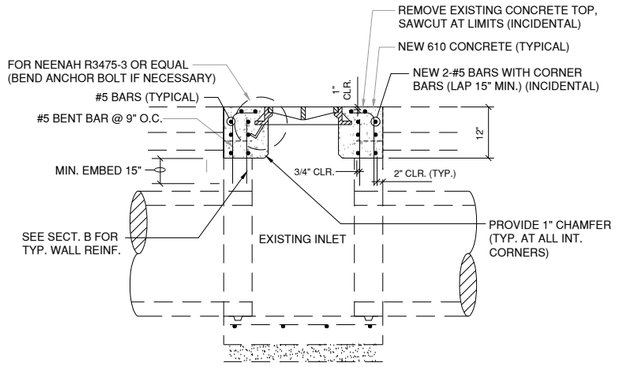
UPDATE BY: Sheue Tomg Lee
LAYOUT: Drainage Plan
DATE: Wednesday, March 22, 2023 12:57:20 PM
FILE: K:\ChicagoExec\Ag\21002031-00 Expand\EQuadApron\Draw\Sheets\expansion-drainage.dwg



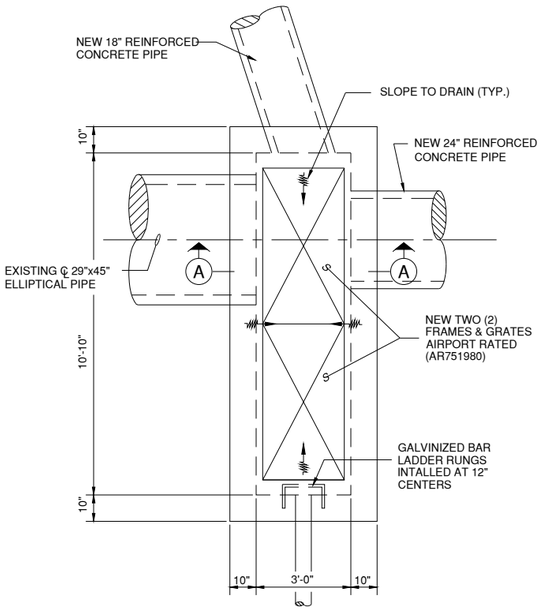
UNDERDRAIN CONNECTION DETAILS

NOT TO SCALE

UNDERDRAIN CONNECTIONS AND FITTINGS, TEES AND ELBOWS USED FOR CONNECTIONS TO PROPOSED STRUCTURES AND STORM SEWERS / EXISTING STRUCTURES AND STORM SEWERS, SHALL BE CONSIDERED INCIDENTAL TO THE PROPOSED UNDERDRAIN.



SECTION A



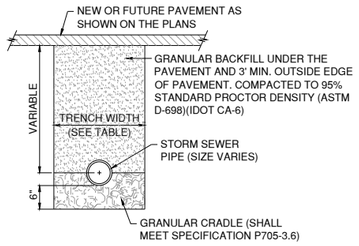
PLAN VIEW

RECONSTRUCT INLET - (AR751980)

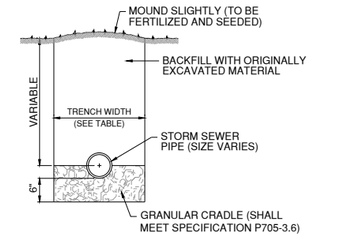
NOT TO SCALE

SEE EXISTING CONDITIONS/PROPOSED REMOVALS FOR LOCATIONS

INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH
6	3'-7"
8	3'-9"
12	4'-2"
15	4'-6"
18	4'-9"
21	5'-0"
24	5'-4"
27	5'-7"
30	5'-11"
36	6'-6"
42	7'-1"
48	7'-8"
54	8'-3"
60	8'-10"
66	9'-5"
72	10'-0"
78	10'-7"
84	11'-2"
90	11'-9"
96	12'-4"
102	12'-11"
108	13'-6"



ALL PAVED AREAS



NON-PAVED AREAS

TRENCH DETAILS STORM SEWER

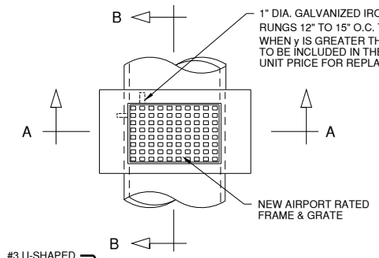
N.T.S.

STORM SEWER/UNDERDRAIN NOTES

1. CONTRACTOR SHALL FIELD VERIFY EXISTING STORM SEWER/UNDERDRAIN INVERTS BEFORE INSTALLING PROPOSED PIPE, CONNECTIONS AND ORDERING MATERIALS.
2. ALL UNDERDRAIN CONNECTIONS, CORING INTO STRUCTURES, CAPS, TEES, BENDS, STORM SEWER ETC. SHALL BE CONSIDERED INCLUDED IN THE COST OF THE UNDERDRAIN.
3. UNDERDRAIN SLOPES FOLLOW EDGE OF PAVEMENT SLOPE UNLESS OTHERWISE NOTED.
4. INSTALL PROPOSED ELECTRICAL DUCTS/CONDUITS TO BE CLEAR OF UNDERDRAIN, COSTS INCLUDED.
5. UNDERDRAIN CONFLICTS WITH EXISTING CONDITIONS SHALL BE RESOLVED AND COST SHALL BE INCIDENTAL TO UNDERDRAIN.
6. CORING OF DRAINAGE STRUCTURE AND REMOVAL OF EXISTING STORM SEWER MANHOLE/INLET CONCRETE BENCHES TO FACILITATE CONNECTIONS OF PROPOSED STORM SEWER AND UNDERDRAIN PIPE SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PIPE.

STRUCTURE SCHEDULE

STRUCTURE	TYPE	RIM	INVERT	STATION (OFFSET)
A1	RECONSTRUCT EXISTING TYPE 1-C INLET	641.28	EXISTING 29"X45"OUT (W) = 636.54 24" IN (E) = 636.65 18" IN (N) = 636.65	STA. 24+52+64, 80.22' RT. BASELINE A
A2	NEW 5' MANHOLE WITH TYPE 1 FRAME AND OPEN LID	641.65	24" OUT (W) = 637.16 12" IN (E) = 637.26 18" IN (N) = 637.26	STA. 25+54.86, 73.92' RT. BASELINE A
A3	NEW TYPE A INLET WITH TYPE 8 GRATE	639.60	12" OUT (W) = 637.39	STA. 26+6.24, 73.38' RT. BASELINE A
B1	NEW TYPE 1 INLET	641.03	18" OUT (S) = 637.05 6" IN (W) = 637.35 6" IN (E) = 637.35	STA. 24+49.66, 60.73' RT. BASELINE A
B2	NEW TYPE 1 INLET	641.33	18" OUT (S) = 637.51 6" IN (W) = 637.81	STA. 25+54.86, 48.33' RT. BASELINE A



SECTION A-A

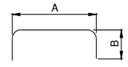
TYPE 1 INLET (ITEM AR751416)

N.T.S.

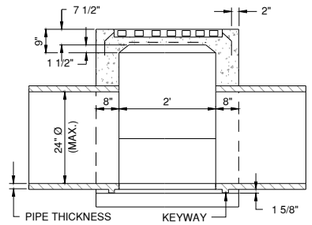
REINFORCING BAR SCHEDULE

TYPE	PER INLET	DIMENSIONS A	DIMENSIONS B	SIZE	APPROX.WT.OF BARS IN INLET
A	2	3'4"	2'4"	#5	16.7

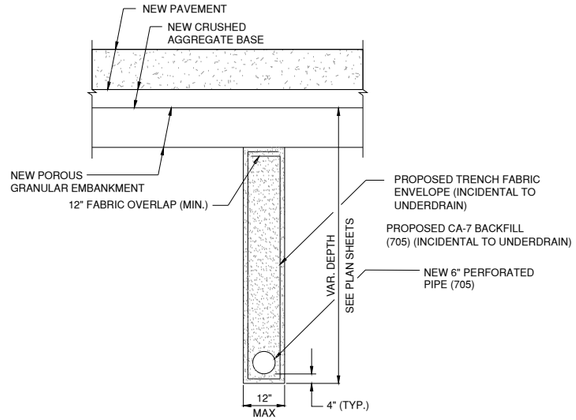
REINFORCING STEEL BARS TYPES



REINFORCING BAR TYPE "A"



SECTION B-B



UNDERDRAIN DETAIL - PAVED AREAS

NOT TO SCALE

NOTES

1. THE 6" UNDERDRAIN SHALL BE INSTALLED AFTER THE SUBGRADE IS COMPACTED.
2. CONTRACTOR SHALL PROTECT SHALLOW DEPTH UNDERDRAIN DURING CONSTRUCTION. ANY DAMAGED UNDERDRAIN DURING CONSTRUCTION SHALL BE REMOVED AND REPLACED AT NO ADDITIONAL COST.

STRUCTURE SCHEDULE NOTES:

1. THE STATION AND OFFSET IS MEASURED TO THE CENTER OF THE STRUCTURE.
2. ALL ELEVATIONS ARE IN 1929 DATUM.
3. D/S: DOWNSTREAM.
4. U/S: UPSTREAM.
5. LENGTH OF PIPE FOR MANHOLE TO MANHOLE IS FROM CENTER OF STRUCTURE.
6. RCCP: REINFORCED CONCRETE CIRCULAR PIPE, CLASS IV.
7. PVC: POLYVINYL CHLORIDE PIPE, SDR 26.
8. MANHOLES SHALL BE IDOT STANDARD 602401-07.
9. CONTRACTOR SHALL VERIFY RIM AND INVERT ELEVATIONS ON EXISTING DRAINAGE STRUCTURES THAT ARE TO BE CONNECTED TO, ADJUSTED OR TO RECONSTRUCTED BEFORE ORDERING MATERIAL (INCIDENTAL TO CONTRACT).
10. FRAME AND LIDS SHALL BE IDOT STANDARD 604001-05.

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

DRAINAGE AND MISCELLANEOUS DETAILS

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CHICAGO EXECUTIVE AIRPORT

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DATE:	03/03/2023
JOB No:	21002031.00

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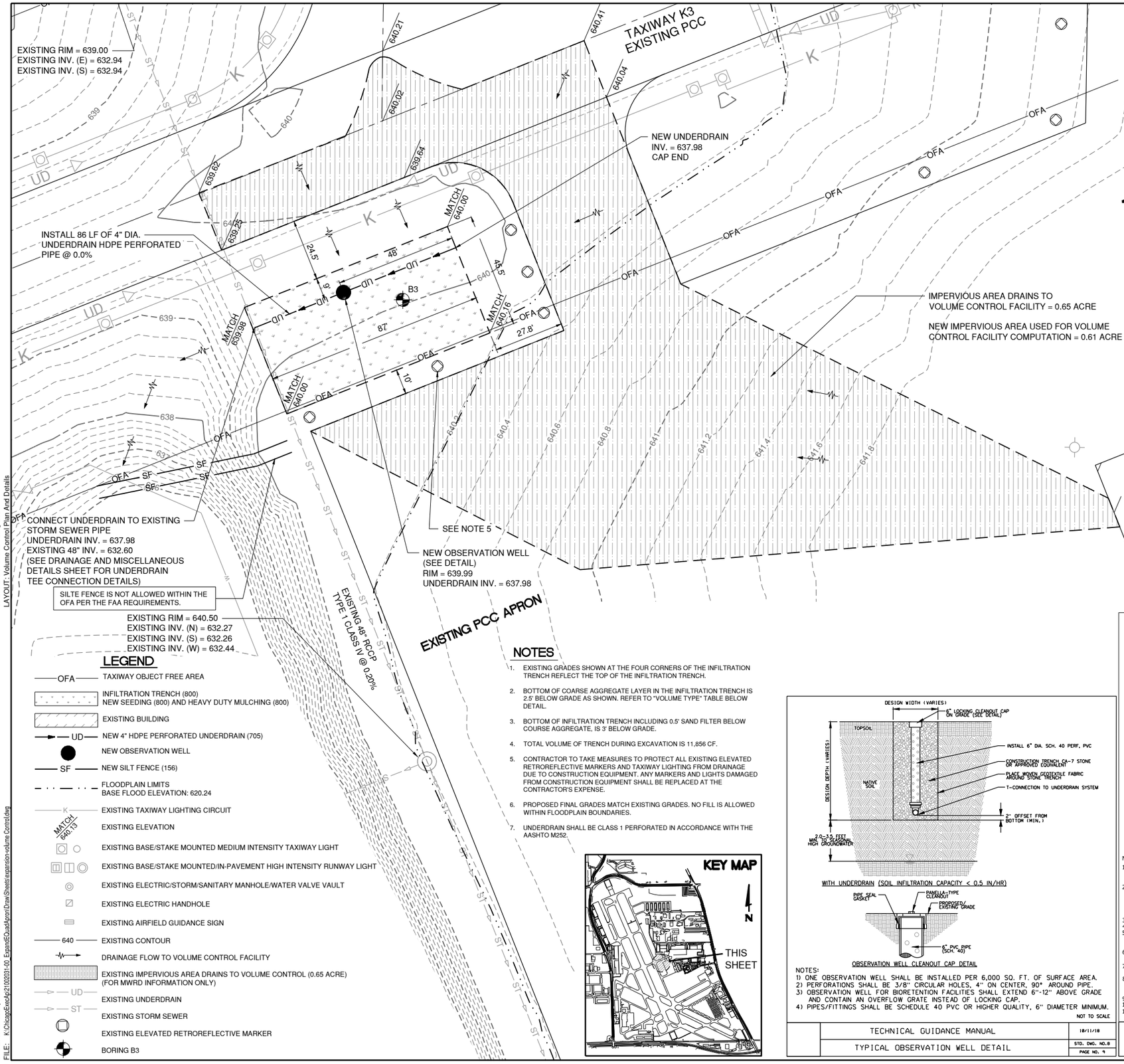
SHEET 16 OF 27 SHEETS

UPDATE BY: Sheue Torng Lee
LAYOUT: Drainage And Miscellaneous Details
DATE: Wednesday, March 22, 2023 12:57:29 PM
FILE: K:\ChicagoExecApt\21002031-00_ExpandEQuadApron\Draw\Sheets\expansion-drainage Details.dwg



EXCAVATION NOTES:

1. ANY EXCESS MATERIAL INCLUDING CLAY, UNSUITABLE MATERIAL AND TOPSOIL FROM THE EXCAVATION AT THE VOLUME CONTROL FACILITY SHALL BE HAULED OFF AIRPORT PROPERTY AND DISPOSED OF BY THE CONTRACTOR.
2. ENVIRONMENTAL REPORT DATED MARCH 20, 2023 CONTAINS FILLED OUT IEPA LLC 663 FORM FOR BORING B3.

LEGEND

- OFA — TAXIWAY OBJECT FREE AREA
- INFILTRATION TRENCH (800) / NEW SEEDING (800) AND HEAVY DUTY MULCHING (800)
- EXISTING BUILDING
- UD — NEW 4" HDPE PERFORATED UNDERDRAIN (705)
- NEW OBSERVATION WELL
- SF — NEW SILT FENCE (156)
- FLOODPLAIN LIMITS / BASE FLOOD ELEVATION: 620.24
- K — EXISTING TAXIWAY LIGHTING CIRCUIT
- MATCH 640.13 — EXISTING ELEVATION
- EXISTING BASE/STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING BASE/STAKE MOUNTED/IN-PAVEMENT HIGH INTENSITY RUNWAY LIGHT
- EXISTING ELECTRIC/STORM/SANITARY MANHOLE/WATER VALVE VAULT
- EXISTING ELECTRIC HANDHOLE
- EXISTING AIRFIELD GUIDANCE SIGN
- 640 — EXISTING CONTOUR
- DRAINAGE FLOW TO VOLUME CONTROL FACILITY
- EXISTING IMPERVIOUS AREA DRAINS TO VOLUME CONTROL (0.65 ACRE) (FOR MWRD INFORMATION ONLY)
- UD — EXISTING UNDERDRAIN
- ST — EXISTING STORM SEWER
- EXISTING ELEVATED RETROREFLECTIVE MARKER
- BORING B3

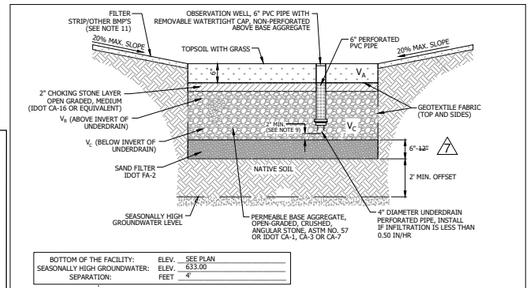
NOTES

1. EXISTING GRADES SHOWN AT THE FOUR CORNERS OF THE INFILTRATION TRENCH REFLECT THE TOP OF THE INFILTRATION TRENCH.
2. BOTTOM OF COARSE AGGREGATE LAYER IN THE INFILTRATION TRENCH IS 2.5' BELOW GRADE AS SHOWN. REFER TO "VOLUME TYPE" TABLE BELOW DETAIL.
3. BOTTOM OF INFILTRATION TRENCH INCLUDING 0.5' SAND FILTER BELOW COURSE AGGREGATE, IS 3' BELOW GRADE.
4. TOTAL VOLUME OF TRENCH DURING EXCAVATION IS 11,856 CF.
5. CONTRACTOR TO TAKE MEASURES TO PROTECT ALL EXISTING ELEVATED RETROREFLECTIVE MARKERS AND TAXIWAY LIGHTING FROM DRAINAGE DUE TO CONSTRUCTION EQUIPMENT. ANY MARKERS AND LIGHTS DAMAGED FROM CONSTRUCTION EQUIPMENT SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
6. PROPOSED FINAL GRADES MATCH EXISTING GRADES. NO FILL IS ALLOWED WITHIN FLOODPLAIN BOUNDARIES.
7. UNDERDRAIN SHALL BE CLASS 1 PERFORATED IN ACCORDANCE WITH THE AASHTO M252.

NOTES:
1) ONE OBSERVATION WELL SHALL BE INSTALLED PER 6,000 SQ. FT. OF SURFACE AREA.
2) PERFORATIONS SHALL BE 3/8" CIRCULAR HOLES, 4" ON CENTER, 90° AROUND PIPE.
3) OBSERVATION WELL FOR BIORETENTION FACILITIES SHALL EXTEND 6"-12" ABOVE GRADE AND CONTAIN AN OVERFLOW GRATE INSTEAD OF LOCKING CAP.
4) PIPES/FITTINGS SHALL BE SCHEDULE 40 PVC OR HIGHER QUALITY, 6" DIAMETER MINIMUM.

NOT TO SCALE

TECHNICAL GUIDANCE MANUAL	10/11/18
TYPICAL OBSERVATION WELL DETAIL	STD. DWG. NO.8
	PAGE NO. 9



BOTTOM OF THE FACILITY:	ELEV.	SEE PLAN
SEASONALLY HIGH GROUNDWATER:	ELEV.	633.00
SEPARATION:	FEET	4'

VOLUME TYPE	SURFACE AREA	DEPTH	POROSITY	STORAGE VOLUME	VOLUME PROVIDED
V_A : TURF	3,952	0.5	0.25	0.50 X 0.25 X V_A	247
V_B : COARSE AGGREGATE (ABOVE INVERT)	3,952	1.0	0.36	0.50 X 0.36 X V_B	711
V_C : COARSE AGGREGATE (BELOW INVERT)	3,952	1.0	0.36	0.36 X V_C	1,422
					2,380

NOTES:
1. THE PERIMETER OF THE VOLUME CONTROL FACILITY SHALL MAINTAIN THE MINIMUM HORIZONTAL SEPARATION DISTANCE OF: 10-FEET FROM FOUNDATIONS, UNLESS WATERPROOFED; 20-FEET FROM ROADWAY GRAVEL SHOULDER; AND 100-FEET FROM POTABLE WATER WELLS, SEPTIC TANKS/FIELDS, OR OTHER UNDERGROUND TANKS.
2. SANITARY OR COMBINED SEWERS SHALL NOT BE LOCATED WITHIN THE VOLUME CONTROL FACILITY. SANITARY OR COMBINED SEWERS SHALL NOT BE LOCATED BELOW THE FOOTPRINT OF THE VOLUME CONTROL FACILITY. WHEN LOCAL CONDITIONS PREVENT THE SEWER FROM BEING LOCATED OUTSIDE THE FOOTPRINT OF THE FACILITY THE SEWER SHALL BE CONSTRUCTED TO WATER MAIN QUALITY STANDARDS, OR IT SHALL BE ENCASED WITH A WATER MAIN QUALITY CARRIER PIPE WITH THE ENDS SEALED.
3. AVOID INSTALLATION ON SLOPES GREATER THAN 3.00%.
4. AVOID COMPACTING NATIVE SOILS. SCARIFY COMPACTED FILL.
5. GEOTEXTILE FABRIC SHALL MEET REQUIREMENTS OF IUM MATERIAL SPECIFICATION 592. FOR WOVEN: APPARENT OPENING SIZE OF 0.50 MM (TABLE 1, CLASS I). FOR NON WOVEN: APPARENT OPENING SIZE OF 0.30 MM (TABLE 2, CLASS 11).
6. STONE STORAGE OPTIONS ARE IDOT CA-1, IDOT CA-7, DISTRICT VULCAN MIX, OR APPROVED ALTERNATE. NO RECYCLED MATERIALS.
7. MINIMUM DISTANCE OF 2 FEET (3.5 FEET IN COMBINED SEWER AREAS) BETWEEN BOTTOM OF BMP AND SEASONALLY HIGH GROUNDWATER LEVEL.
8. UNDERDRAINS ARE REQUIRED IN TYPICAL CLAYEY SOILS WHERE INFILTRATION RATES ARE LESS THAN 0.5 INCH/HOUR. MAXIMUM OF 1 UNDERDRAIN PER 30 FEET. PROVIDE A SOIL REPORT DOCUMENTING NATIVE INFILTRATION RATE TO FOREGO UNDERDRAINS.
9. MINIMUM UNDERDRAIN BEDDING OF TWO INCHES, MAXIMUM OF 12 INCHES.
10. ONE OBSERVATION WELL REQUIRED PER 6,000 SQUARE FEET OF SURFACE AREA.
11. FOLLOW THE REQUIRED PRETREATMENT MEASURES LISTED ON THE VOLUME CONTROL PRETREATMENT MEASURES DETAIL.

NOT TO SCALE

TECHNICAL GUIDANCE MANUAL	10/4/18
DETAIL - TRENCH SECTION	STD. DWG. NO.6
	PAGE NO. 7

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

VOLUME CONTROL PLAN AND DETAILS

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

SHEET 17 OF 27 SHEETS

UPDATE BY: Sheue Tomg Lee
LAYOUT: Volume Control Plan And Details
DATE: Wednesday, March 22, 2023 12:58:28 PM
FILE: K:\ChicagoExecAp\21002031-00_ExpandEQuadApron\Drawsheets\expansion-volume Control.dwg

HANGAR 15

HANGAR 16

EXISTING BITUMINOUS PAVEMENT

NEW 18' ELECTRIC GATE (SEE NOTE 1)

INSTALL NEW GATE POST IN BITUMINOUS PAVEMENT (COST INCIDENTAL)

NEW 4' PEDESTRIAN GATE

NEW GATE ELECTRIC CONNECT TO GATE OPERATOR SO GATE IS IN OPERATIONAL CONDITION

TIE EXISTING 4' FENCE TO NEW 6' FENCE AT NEW END POST (COST INCIDENTAL)

266 LF RELOCATED FENCE

STA. 10+81.07, 120' RT. BASELINE B

265 LF RELOCATED FENCE

50 LF NEW 6' CLASS E FENCE

NEW BITUMINOUS TRANSITION

NEW DETECTOR LOOP (AIRSIDE OPEN AND SAFETY)

EXISTING PCC APRON

41.25' (TYPICAL)

~ TURF ~

23 24 25 26 27 28

BASELINE A

EXISTING PCC PAVEMENT

STA. 23+00.00, 86.6' RT. BASELINE A START

NEW PCC APRON

EXISTING PAVEMENT MARKING

NEW 226' TAXILANE CENTERLINE MARKING

10' (TYPICAL)

10'

STA. 25+25.85, 86.6' RT. BASELINE A END

BASELINE B

EAST QUADRANT APRON

TIE RELOCATED FENCE INTO EXISTING FENCE INSTALL A NEW TERMINAL POST

NORTH

0 20 40

SCALE IN FEET

LEGEND

- NEW AIRFIELD FENCE
- NEW VEHICULAR GATE
- EXISTING AIRFIELD FENCE
- EXISTING VEHICULAR GATE
- EXISTING PAVEMENT MARKING
- NEW PAVEMENT MARKING
- EXISTING BUILDING
- NEW PAVEMENT
- NEW ELEVATED RETROREFLECTIVE MARKER

KEY MAP

N

THIS SHEET

6" BLACK BORDER

12" YELLOW, REFLECTIVE CENTERLINE STRIPE

6" BLACK BORDER

TAXIWAY / TAXILANE CENTERLINE DETAIL

NO SCALE

NOTES

1. THE CONTRACTOR SHALL MAINTAIN A CONTINUOUS PERIMETER FENCE AT THE ACCESS DRIVE TO THE APRON AND WITHIN LIMITS ON CONSTRUCTION. THE USE OF TEMPORARY FENCE OR PHASED FENCE/GATE INSTALLATION MAY BE REQUIRED. ALL MEASURES IMPLEMENTED BY THE CONTRACTOR SHALL BE TO THE SATISFACTION OF THE AIRPORT AND WILL NOT BE PAID SEPARATE BUT CONSIDERED INCIDENTAL

IL. CONTRACT: **PA065**

IL. LETTING ITEM: **11A**

IL. PROJECT: **PWK-4795**

S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

FENCING AND PAVEMENT MARKING PLAN

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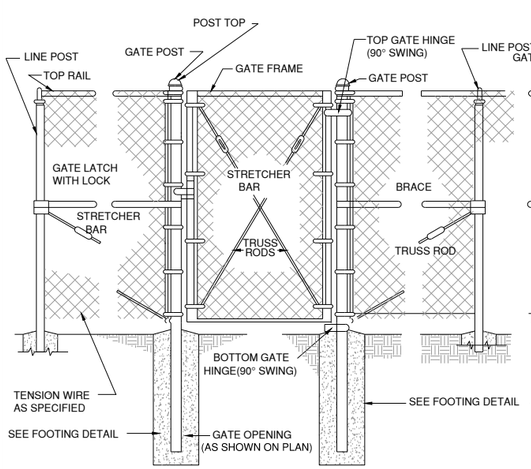
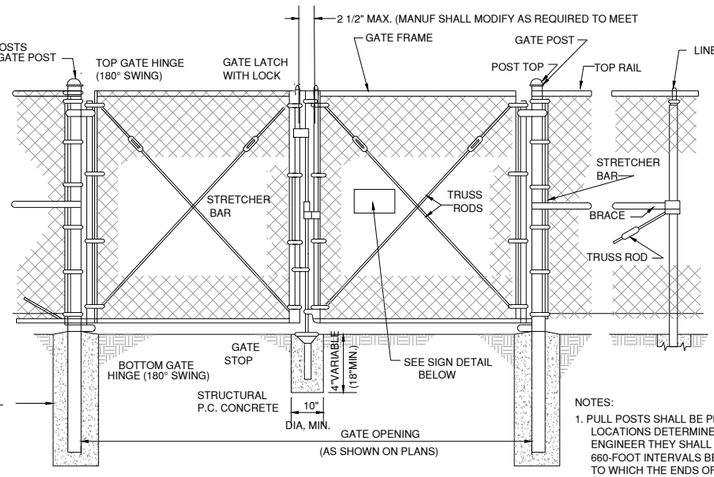
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

UPDATE BY: Sheue Tomg Lee
LAYOUT: Fencing And Pavement Marking Plan
DATE: Wednesday, March 22, 2023 12:58:37 PM
FILE: K:\ChicagoExecApt\21002031-00_Expand\EQuadApron\Draw\Sheets expansion-fencing And Marking.dwg

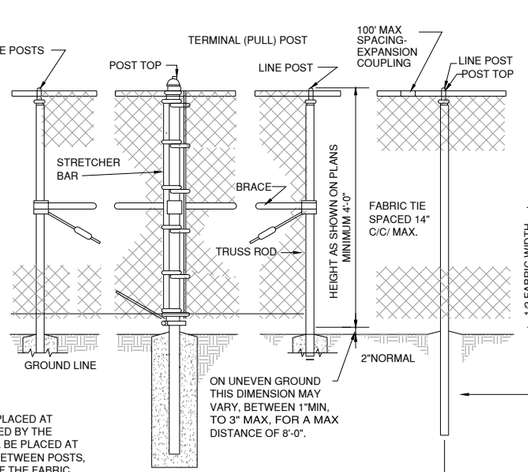


PEDESTRIAN GATE ARRANGEMENT

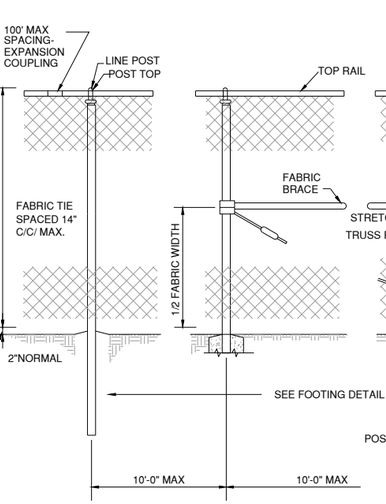


VEHICLE GATE ARRANGEMENT

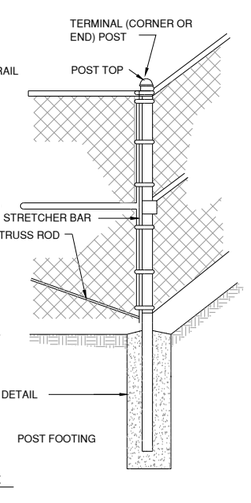
NOTES:
1. PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER THEY SHALL BE PLACED AT 660-FOOT INTERVALS BETWEEN POSTS, TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1320' AND GREATER THAN 660'
2. WHERE FENCE HAS A CHANGE IN DIRECTION OF 15° OR MORE, A TERMINAL POST SHALL BE PLACED AS SHOWN ABOVE.



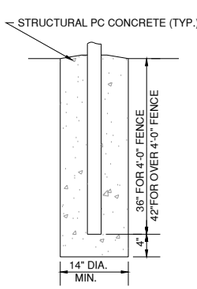
PULL POST ARRANGEMENT



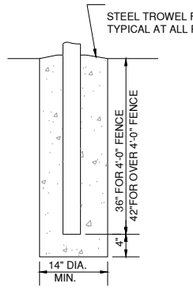
LINE POST ARRANGEMENT



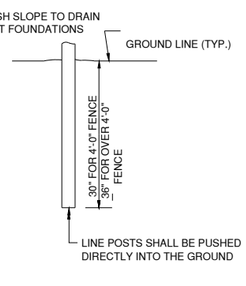
CORNER OF END POST ARRANGEMENT



FOOTING FOR TERMINAL POST



FOOTING FOR GATE POST



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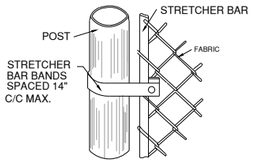
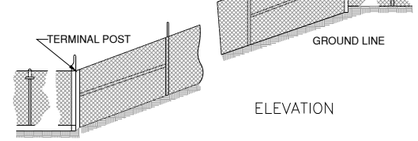
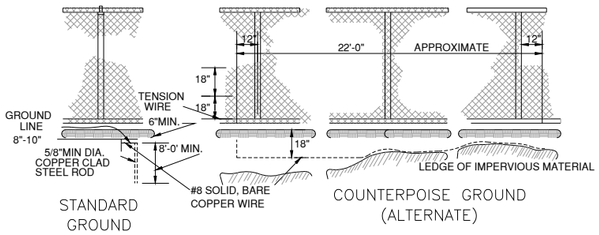


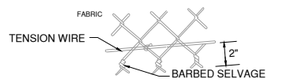
METHOD OF FASTENING STRETCHER BAR TO POST



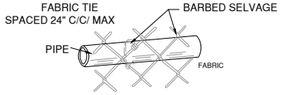
FENCE INSTALLATION ON SLOPES



PROTECTIVE ELECTRICAL GROUND



METHOD OF TYING FABRIC TO TENSION WIRE



METHOD OF TYING FABRIC TO PIPE

NOTES:
1. CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 1000' EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM A GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
2. FENCE UNDER POWER LINE SHALL BE GROUNDED BY THREE GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSING.
3. THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF AN IMPERVIOUS EARTH STRUCTURES.
4. THE GROUND WIRE SHALL BE CONNECTED TO FABRIC, TENSION WIRE, AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS.

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

FENCING DETAILS - 1

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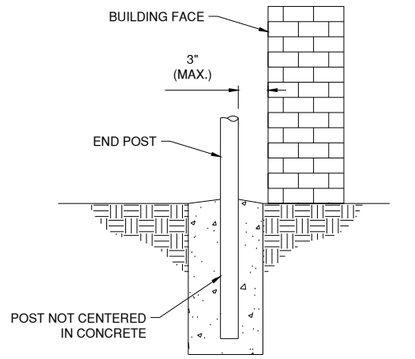
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

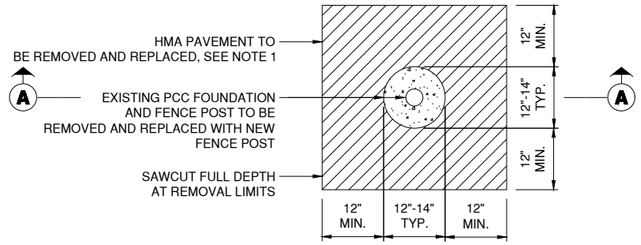
FINAL

UPDATE BY: Sheue Torng Lee
LAYOUT: Fencing Details - 1
DATE: Wednesday, March 22, 2023 12:58:44 PM
FILE: K:\ChicagoExecApt\21002031-00_ExpandEQuadApron\Draw\Sheets\fencing Details - 1.dwg

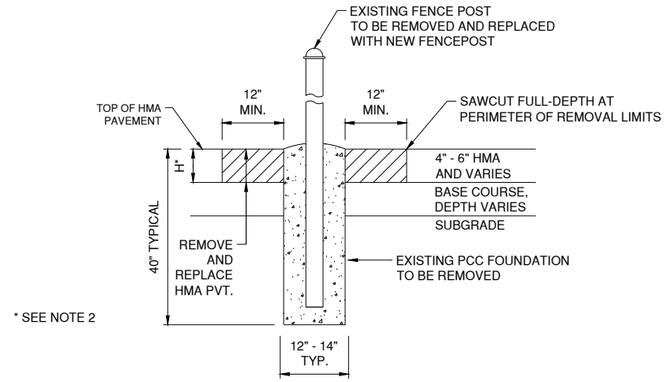


FENCE INSTALLATION AT BUILDING FACE

N.T.S.



PLAN



SECTION A-A

FENCE POST REMOVAL AND REPLACEMENT IN HMA PAVEMENT

NOTES:

1. HMA PAVEMENT TO BE REMOVED FULL DEPTH
2. BASE COURSE AND/OR SUBGRADE TO BE REMOVED SUCH THAT THE DEPTH "H" IS A MINIMUM OF 6 INCHES.
3. NEW FENCE SHALL REUSE EXISTING POST HOLES AS FIELD CONDITIONS ALLOW. SEE DETAIL FOR NEW FENCE POST FOUNDATION IN EXISTING HMA PAVEMENT.
4. COSTS OF PAVEMENT AND FOUNDATION REMOVAL, NEW PAVEMENT, SAW CUTTING, CLEANUP AND DISPOSAL, SHALL BE CONSIDERED INCIDENTAL TO AR162900, REMOVE CLASS E FENCE.
5. AT THE CONTRACTOR'S OPTION AND AT NO ADDITIONAL COST TO THE CONTRACT, THE CONTRACTOR MAY REMOVE A "STRIP" OF ASPHALT PAVEMENT ALONG THE FENCELINE, TO BE REPLACED IN KIND AFTER FENCELINE REMOVAL AND REPLACEMENT.
6. SAME DETAIL IS TO BE USED IF NEW POST HOLES ARE REQUIRED IN EXISTING HMA PAVEMENT, NOT INCLUDING THE REMOVAL PORTION (COST INCIDENTAL TO CONTRACT)

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

FENCING DETAILS - 2

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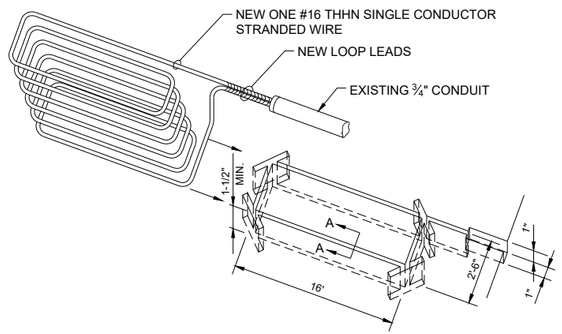
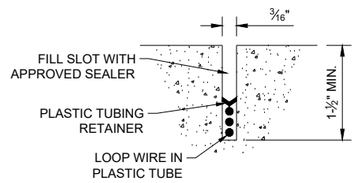
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

UPDATE BY: Sheue Torng Lee
LAYOUT: Fencing Details - 2
DATE: Wednesday, March 22, 2023 12:58:49 PM
FILE: K:\Chicago\Exec\Ap\21002031-00_ExpandEQuadApron\Draw\Sheets expansion-fencing Details - 2.dwg



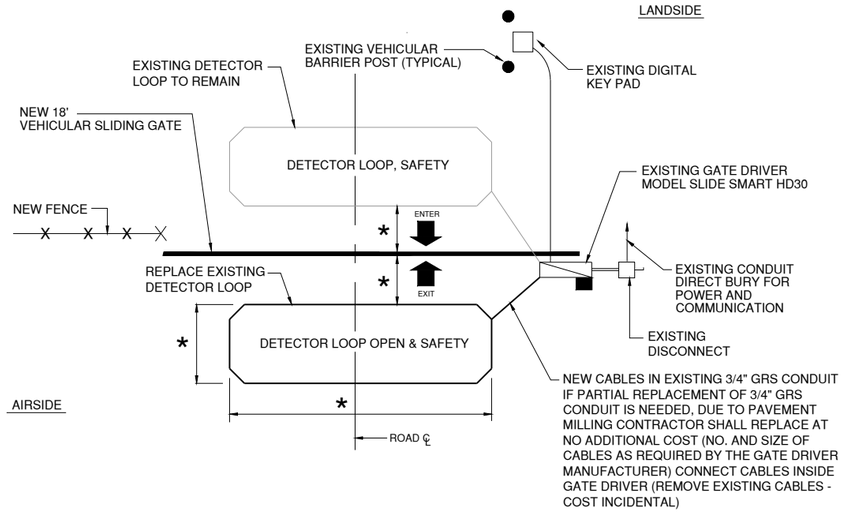
PERSPECTIVE VIEW OF DETECTOR LOOP SAW SLOT



SECTION A-A

DETECTOR LOOP DETAILS

NOT TO SCALE



REPLACE EXISTING DETECTOR LOOP LAYOUT

NOT TO SCALE

* PER MANUFACTURERS RECOMENDATION CONTRACTOR SHALL COORDINATE THIS WORK WITH ENGINEEER.

NOTES:

1. THE LOCATION OF THE NEW DETECTOR LOOPS ARE FOR INFORMATION ONLY AND SHALL BE FIELD ADJUSTED PER THE MANUFACTURER RECOMMENDATION.

NOTES:

1. LOOP LEADS ARE LIMITED TO 100 FEET.
2. LOOP LEADS MUST HAVE FOUR (4) TWISTS PER FOOT.
3. LOOP AND LOOP LEADS MUST BE LOCATED, AT LEAST, 18" FROM ANY ELECTRICAL POWER SERVICE OR RUN, OR STEEL REINFORCEMENT.
4. LOOP LEADS MUST BE IN SEPARATE CONDUIT BETWEEN LOOP AND DETECTOR. THEY MUST NOT SHARE CONDUIT WITH OTHER WIRING OR LEADS FROM OTHER LOOPS.
5. WIRE SHALL BE #16 THHN SINGLE CONDUCTOR STRANDED WIRE.
6. ALL WIRE SHALL BE CONTINUOUS WITHOUT SPLICING.

CAUTION:

- DO NOT SPLICE WIRE.
- DO NOT FRACTURE WIRE INSULATION. LOOPS SHORTED TO GROUND WILL CAUSE DETECTOR MALFUNCTION. WHEN PLACING WIRE IN THE SLOT, DO NOT USE SCREWDRIVER OR OTHER SHARP TOOLS.

TYPICAL LAYOUT FOR LOOP:

- SAW SLOT 3/16" WIDE x 1-1/2" DEEP. MAKE RECTANGULAR SHAPE TO SPECIFIED LOOP DIMENSIONS PLUS SLOT FOR LEAD CONDUIT.

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

ELECTRIC GATE DETAILS

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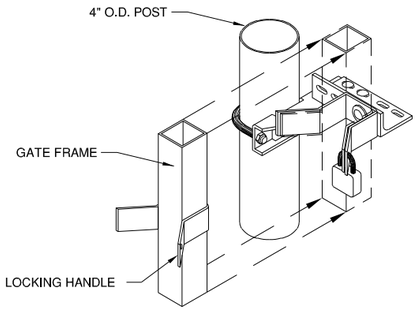
CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

UPDATE BY: Sheue Torng Lee
LAYOUT: Electric Gate Details
DATE: Wednesday, March 22, 2023 12:58:54 PM
FILE: K:\ChicagoExecApt\21002031-00_ExpandEQuadApron\Draw\Sheets\expansion-gate Details.dwg



CATCH ASSEMBLY DETAIL

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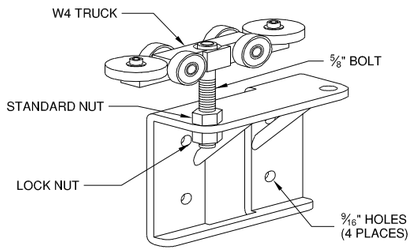

GATE HANGAR ASSEMBLY

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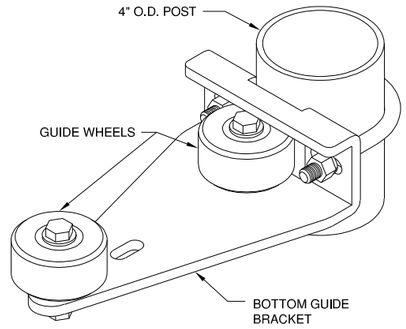

STANDARD BOTTOM GUIDE ASSEMBLY

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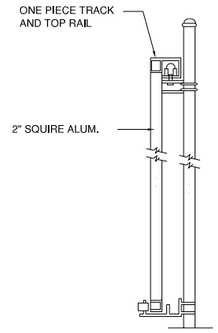

GATE FRAME SECTION

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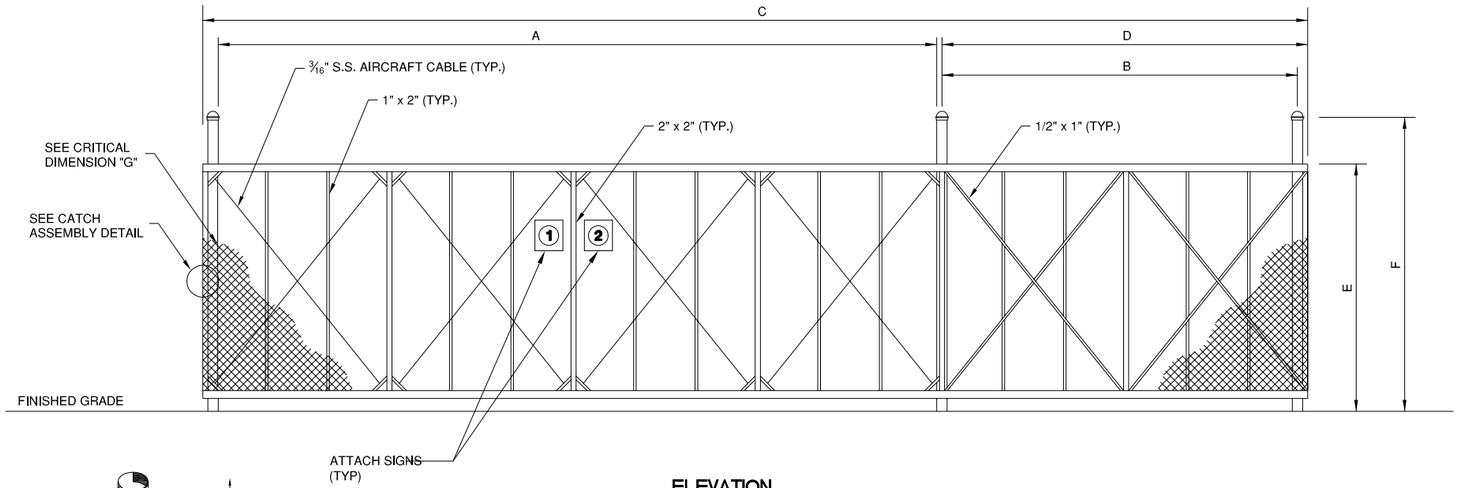
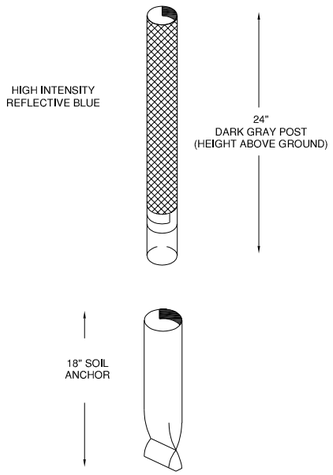


ELEVATION

CANTILEVER SLIDE GATE

N.T.S.

CRITICAL DIMENSIONS		
A	CLEAR OPENING	18'-0"
B	COUNTER BALANCE POST SPACING, CENTER TO CENTER	9'-1"
C	OVERALL GATE LENGTH	27'-0"
D	COUNTERBALANCE LENGTH	9'-0"
E	NOMINAL GATE HEIGHT	6'-0"
F	POST HEIGHT	7'-6"
G	FABRIC HEIGHT	5'-0"



ELEVATED RETROREFLECTIVE MARKER DETAIL

NOT TO SCALE

NOTE: RETROREFLECTIVE MARKER SHALL MET FAA AC 150/5345-39 (LATEST EDITION) AIRFIELD LIGHTING EQUIPMENT CERTIFICATION PROGRAM

UL 235 COMPLIANCE NOTES

GATE INSTALLATION SHALL COMPLY WITH ALL REQUIREMENTS OF UL 235, INCLUDING, BUT NOT LIMITED TO:

1. ALL OPENINGS OF THE SLIDE GATE ARE GUARDED OR SCREENED FROM THE BOTTOM OF THE GATE TO A MINIMUM OF 4 FEET ABOVE GROUND TO PREVENT A 2-1/4" DIAMETER SPHERE FROM PASSING THROUGH THE OPENINGS ANYWHERE IN THE GATE, AND IN THAT PORTION OF THE ADJACENT FENCE THAT THE GATE COVERS IN THE OPEN POSITION.
2. ALL EXPOSED PINCH POINTS ARE ELIMINATED OR GUARDED AND GUARDING IS SUPPLIED FOR ALL EXPOSED ROLLERS.
3. FOR ADDITIONAL UL 235 REQUIREMENTS FOR THIS GATE INSTALLATION, SEE ELECTRIC GATE DETAILS SHEET.

GENERAL NOTES:

1. CANTILEVERED GATE SHALL BE SUFFICIENTLY RIGID TO WITHSTAND FLEXING OR BENDING DURING WINDY CONDITIONS. CONTRACTOR SHALL PROVIDE STIFFENERS, STRUCTURAL SHAPES IN EXCESS OF THE MINIMUM SPECIFIED DIMENSIONS OR ADDITIONAL ROLLERS AND POSTS SUFFICIENT TO PREVENT DISPLACEMENT OF THE GATE BY WIND OR BY UNAUTHORIZED PERSONNEL.
2. CONTRACTOR SHALL PROVIDE AND INSTALL GATE AS A COMPLETE WORKING UNIT. THE GATE WORK SHALL INCLUDE, THE GATE, AND ALL CONNECTIONS, LABOR AND MATERIALS NECESSARY TO COMPLETE OPERATION.
3. THE FABRIC TYPE AND FINISH OF THE GATE SHALL MATCH WITH THE PROPOSED FENCE OR AS DIRECTED BY THE ENGINEER.
4. ALL PROPOSED SLIDING VEHICULAR GATES SHALL HAVE ENCLOSED TRUCK ASSEMBLIES, TYMETAL OR EQUAL.
5. CONTRACTOR SHALL VERIFY TYPE OF ELECTRIC GATE OPERATOR TO BE COMPATIBLE WITH NEW SLIDE GATE (COST INCIDENTAL).
6. NEW GATE NEEDS TO BE CONNECTED TO GATE OPERATOR AND GATE SHALL BE IN ELECTRIC OPERATION (COST INCIDENTAL).

LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET ARE GENERIC. EQUIPMENT LOCATIONS SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

CANTILEVER GATE AND MARKER DETAILS

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CHICAGO EXECUTIVE AIRPORT

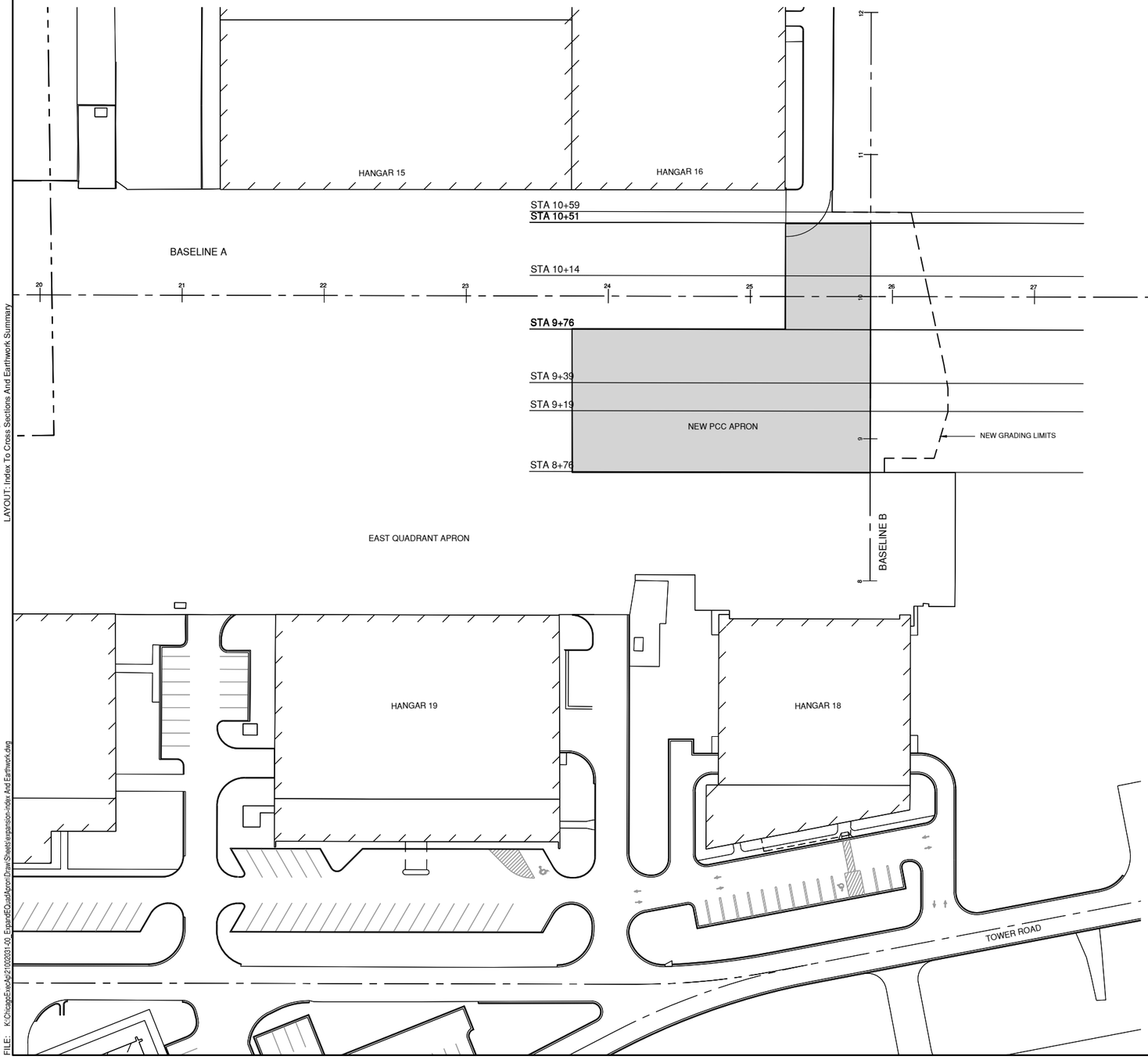
DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

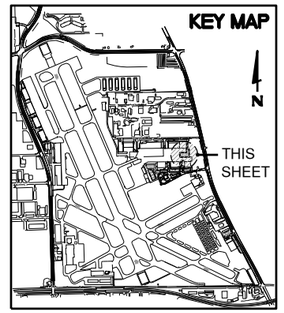
FINAL

DATE: Wednesday, March 22, 2023 1:34:11 PM
FILE: K:\Chicago\Exec\Ap\21002031-00_ExpandEQuadApron\Draw\Sheets\expansion-cantilever Gate Details.dwg
UPDATE BY: Sheue Torng Lee
LAYOUT: Cantilever Gate Details

EARTHWORK SUMMARY TABLE

LOCATION	TOPSOIL STRIPPING INITIAL POSITION (CUBIC YARD)	TOPSOIL PLACEMENT FINAL POSITION (CUBIC YARD)	SHOULDER FILL FINAL POSITION (CUBIC YARD)	UNCLASSIFIED EXCAVATION INITIAL POSITION (CUBIC YARD)	EMBANKMENT FILL FINAL POSITION (CUBIC YARD)	CLEAN CONSTRUCTION DEMOLITION DEBRIS STANDARDS
EAST QUAD APRON EXPANSION	1,295	100	0	1,605	35	STOCKPILE ONSITE (ADD. ALT. NO. 1) DISPOSE OF AT SUBTITLE D LANDFILL (ADD. ALT. NO. 2)
VOLUME CONTROL FACILITY	55	80	0	410	0	IEPA LLC 663 FORM INCLUDED IN ENVIRONMENTAL REPORT SEE SPECIAL PROVISIONS
TOTAL	1,350	180	0	2,015	35	





GENERAL EARTHWORK NOTES:

1. ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
2. AREAS OF UNSUITABLE MATERIAL (UNCLASSIFIED EXCAVATION) SHALL BE AS DESIGNATED BY THE ENGINEER. THE QUANTITY OF UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT FILL MATERIAL, UNLESS AUTHORIZED BY THE ENGINEER.
3. PAYMENT FOR UNCLASSIFIED EXCAVATION IS THE SUM OF TOPSOIL STRIPPING AND UNCLASSIFIED EXCAVATION AREAS.
4. THE CONTRACTOR SHALL ENSURE THAT 4 INCHES OF TOPSOIL CAN BE SPREAD OVER THE LIMITS OF THE GRADED AREA. IN SOME CASES, CONTRACTOR MAY BE REQUIRED TO OVER-EXCAVATE TO PROVIDE THE REQUIRED 4-INCH TOPSOIL LAYER. THE EARTHWORK QUANTITIES SHOWN INCLUDE THE REQUIRED OVER-EXCAVATION AND ARE SHOWN IN THE CROSS SECTIONS.
5. TOPSOIL PLACEMENT, EMBANKMENT FILL AND SHOULDER FILL SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE FOR UNCLASSIFIED EXCAVATION. NO SEPARATE PAYMENT WILL BE MADE FOR TOPSOIL PLACEMENT, EMBANKMENT FILL AND SHOULDER FILL.
6. ALL HAUL ROADS TO BE CONSTRUCTED FOR THE PROJECT WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
7. CONTRACTOR'S HAUL ROADS TO THE SITE SHALL BE RESTORED WITH 4" MINIMUM OF TOPSOIL PLACED. ALL HAUL ROAD RESTORATION SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
8. ANY EXCESS MATERIAL INCLUDING CLAY, UNSUITABLE MATERIAL AND TOPSOIL FROM THE EXCAVATION AT THE VOLUME CONTROL FACILITY SHALL BE HAULED OFF AIRPORT PROPERTY AND DISPOSED OF BY THE CONTRACTOR.
9. IF ANY ADDITIONAL CCDD AND ENVIRONMENTAL TESTING AND ANY TESTING AND HANDLING REQUIREMENT BY THE CONTRACTOR AND/OR CONTRACTOR'S DISPOSAL FACILITY(S) FOR ALL HAULED OFF MATERIALS, SHALL BE COMPLETED AT THE CONTRACTORS EXPENSE. ENVIRONMENTAL REPORT DATED MARCH 20, 2023 IS ATTACHED IN THE SPECIAL PROVISIONS.

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

INDEX TO CROSS SECTIONS AND EARTHWORK SUMMARY

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

UPDATE BY: Sheue Torng Lee
LAYOUT: Index To Cross Sections And Earthwork Summary
DATE: Wednesday, March 22, 2023 12:59:14 PM
FILE: K:\Chicago\Exec\Ap\21002031-00_ExpandEQuadApron\Draw\Sheets\expansion-Index And Earthwork.dwg

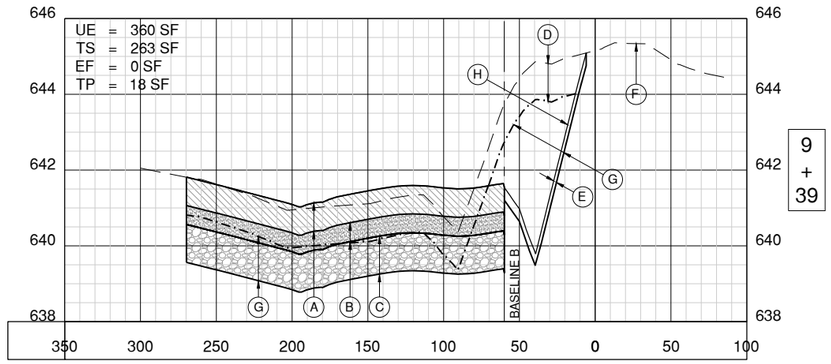
LEGEND

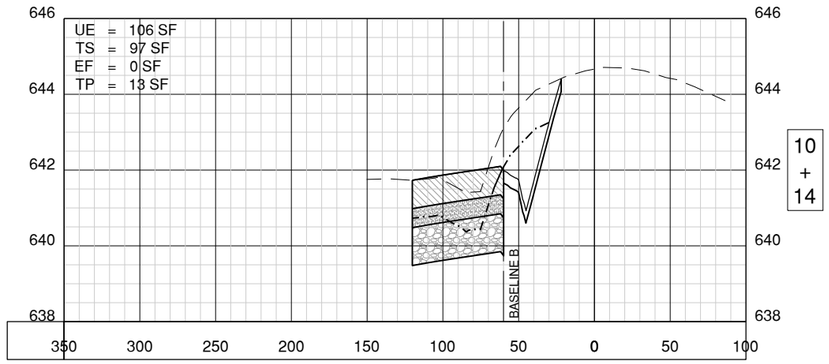
- (A) NEW 9" PCC PAVEMENT (501)
- (B) NEW 6" CRUSHED AGGREGATE BASE COURSE (209)
- (C) NEW 12" POROUS GRANULAR EMABNKMENT (208)
- (D) AVERAGE 12" TOPSOIL STRIPPING (152)
- (E) MINIMUM 4" TOPSOIL PLACEMENT (905)
- (F) EXISTING GROUND LINE
- (G) UNCLASSIFIED EXCAVATION (152)
- (H) NEW GROUND LINE

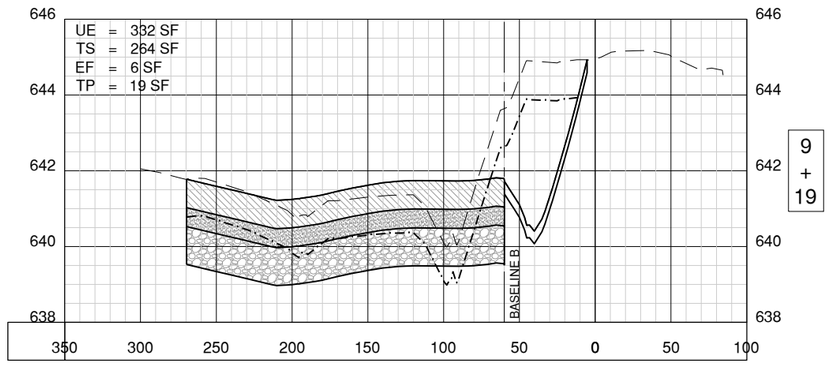
- UE UNCLASSIFIED EXCAVATION
- TS TOPSOIL STRIPPING
- EF EMBANKMENT FILL
- TP TOPSOIL/SHOULDER PLACEMENT

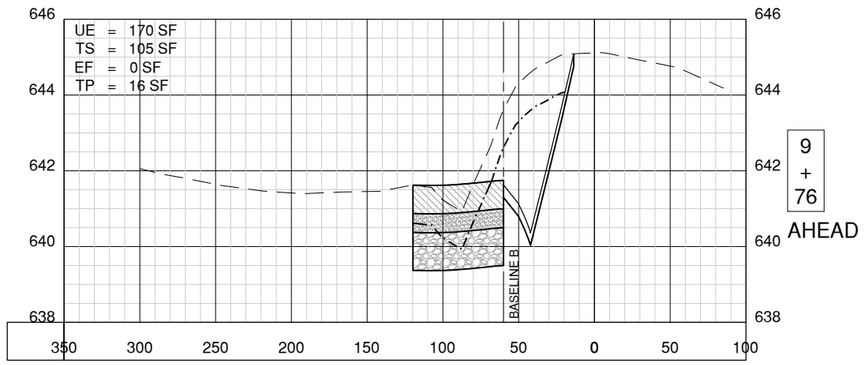
NOTES

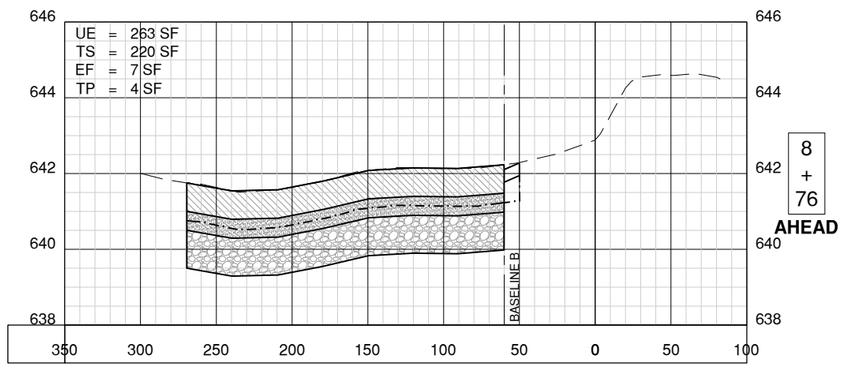
1. SUBTRACT 0.24 FEET FROM GRADES SHOWN IN PLANS (1929 DATUM) TO OBTAIN 1988 NAVD.
2. SEE GRADING PLAN FOR ELEVATIONS.
3. EXISTING AND NEW UTILITIES ARE NOT SHOWN FOR CLARITY. SEE EXISTING CONDITIONS AND REMOVALS FOR APPROXIMATE UTILITY LOCATIONS.

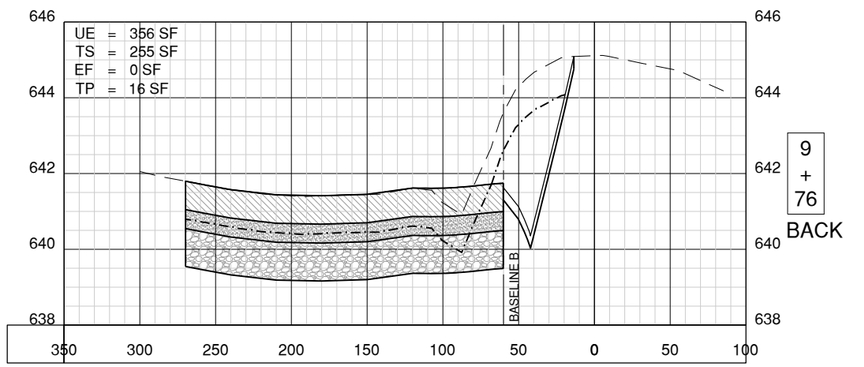












IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

CROSS SECTIONS - 1

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CHICAGO EXECUTIVE AIRPORT

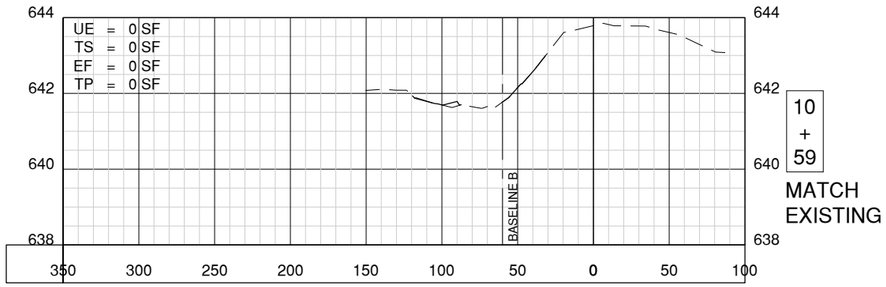
DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

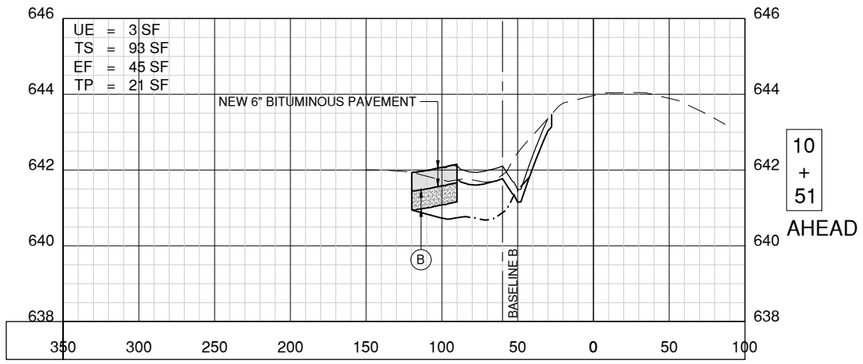
FINAL

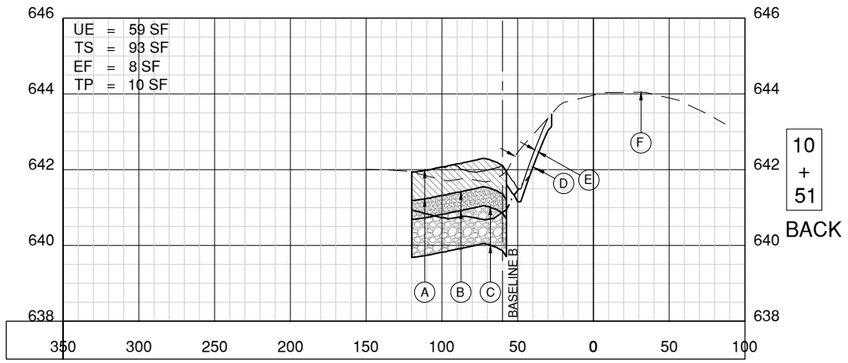
SHEET 24 OF 27 SHEETS

UPDATE BY: Sheue Torng Lee
LAYOUT: Cross Sections - 1

DATE: Wednesday, March 22, 2023 12:59:29 PM
FILE: K:\ChicagoExecApt\21002031-00_ExpandEQuadApron\Draw\Sheets\expansion-cross Sections.dwg







UPDATE BY: Sheue Torng Lee
LAYOUT: Cross Sections - 2

DATE: Wednesday, March 22, 2023 12:59:36 PM
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IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

CROSS SECTIONS - 2

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CHICAGO EXECUTIVE AIRPORT

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JOB No:	21002031.00

FINAL

IL. CONTRACT: PA065
IL. LETTING ITEM: 11A
IL. PROJECT: PWK-4795
S.B.G. PROJECT: N/A

SOIL BORING LOG

G Geo Services, Inc.
eotechnical, Environmental & Civil Engineering
805 Amherst Court, Suite 204
Naperville, Illinois 60565
(630) 355-2838

GEO Job No. 22011
Page 1 of 1
Date 9/14/22

PROJECT Lansing Municipal Airport
LOCATION Lansing Municipal Airport, Lansing, Il.
COUNTY Cook DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

CLIENT Crawford, Murphy & Tilly, Inc.
BORING NO. B-01
Northing 1985790
Easting 1102773
Ground Surface Elev. 640.6 ft

Surface Water Elev. - ft
Stream Bed Elev. - ft
Groundwater Elev.:
First Encounter Dry ft
Upon Completion - ft
After - Hrs. - ft

Description	Elev.	DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIS (%)	DRY DENSITY (pcf)
TOPSOIL with Gravel-black (fill)	639.6				16	
SANDY LEAN CLAY-brown & gray-medium stiff to stiff (CL)			4 / 3 / 3	1.00 P	21	
	635.1	-5	8 / 10 / 4	2.00 P	10	
SILTY CLAY LOAM-gray-stiff (CL)	632.6		3 / 5 / 6	1.30 B	14	
CLAYEY SAND with Gravel-gray-medium dense (SC)	630.6	-10	9 / 10 / 9		11	
End Of Boring @ -10.0'. Boring backfilled with cuttings		-15, -20				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206), GP-Geoprobe Hand Auger
BBS, from 137 (Rev. 8-99)

Z:\PROJECTS\2022\22088 CMT, CHICAGO EXECUTIVE AIRPORT EAST QUAD APRON EXPANSION\22088 BORING LOGS\22088_LOG.GPJ 9/30/22

SOIL BORING LOG

G Geo Services, Inc.
eotechnical, Environmental & Civil Engineering
805 Amherst Court, Suite 204
Naperville, Illinois 60565
(630) 355-2838

GEO Job No. 22011
Page 1 of 1
Date 9/14/22

PROJECT Lansing Municipal Airport
LOCATION Lansing Municipal Airport, Lansing, Il.
COUNTY Cook DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

CLIENT Crawford, Murphy & Tilly, Inc.
BORING NO. B-02
Northing 1985860
Easting 1102925
Ground Surface Elev. 643.9 ft

Surface Water Elev. - ft
Stream Bed Elev. - ft
Groundwater Elev.:
First Encounter Dry ft
Upon Completion - ft
After - Hrs. - ft

Description	Elev.	DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIS (%)	DRY DENSITY (pcf)
TOPSOIL-black	642.9				20	
SANDY LEAN CLAY-brown & gray-very stiff (CL)			3 / 5 / 2	3.50 P	18	
	638.4	-5	8 / 7 / 4	2.50 B	18	
CLAYEY SAND with Gravel-brown & gray-medium dense (SC)			3 / 6 / 9		22	
	633.9	-10	13 / 8 / 5		14	
End Of Boring @ -10.0'. Boring backfilled with cuttings		-15, -20				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206), GP-Geoprobe Hand Auger
BBS, from 137 (Rev. 8-99)

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SOIL BORING LOG

GGeo Services, Inc.
eotechnical, Environmental & Civil Engineering
805 Amherst Court, Suite 204
Naperville, Illinois 60565
(630) 355-2838

GEO Job No. 22011
Page 1 of 1
Date 1/24/23

PROJECT Chicago Executive Airport
LOCATION Chicago Executive Airport, Wheeling, IL
COUNTY Cook DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

CLIENT CMT
BORING NO. B-03
Northing 1985540
Easting 1101514
Ground Surface Elev. 640.0 ft

Surface Water Elev. - ft
Stream Bed Elev. - ft
Groundwater Elev.:
First Encounter Dry ft
Upon Completion - ft
After - Hrs. - ft

Description	Elev.	DEPTH (ft)	BLOWS (/6")	UCS Qu (tsf)	MOIS (%)	DRY DENSITY (pcf)
CLAYEY TOPSOIL-black-hard	639.3				31	
LEAN CLAY-brown & gray-very stiff to hard (CL)			5 / 8 / 9	3.50 P	15	
		-5	7 / 9 / 13	4.50 P	14	121
			10 / 15 / 13	4.50 P	12	123
becoming gray @ -9.0'		-10	5 / 5 / 6	4.50 P	14	116
			8 / 12 / 14	4.00 P	12	119
	625.0	-15	5 / 6 / 6	2.50 P	14	121
End Of Boring @ -15.0'. Boring backfilled with cuttings		-20				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206), GP-Geoprobe Hand Auger
BBS, from 137 (Rev. 8-99)

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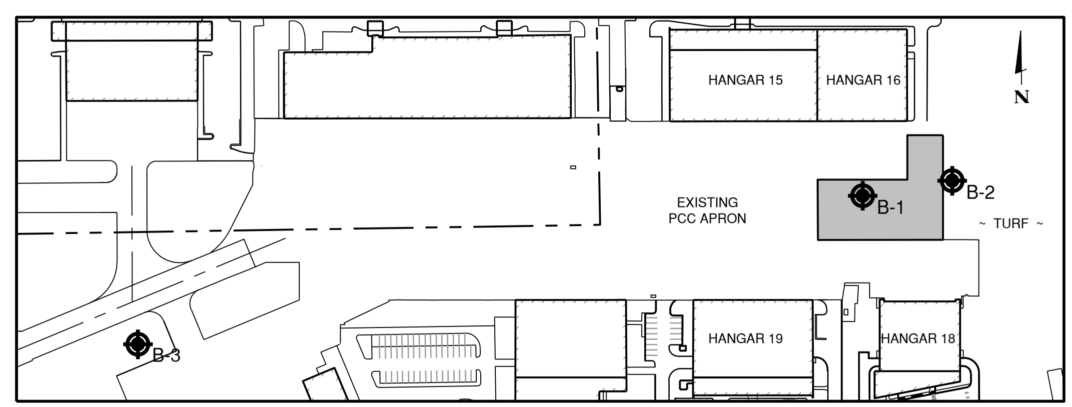
LEGEND

NEW BITUMINOUS PAVEMENT
EXISTING BUILDING/HANGAR
B-2 CORING/BORING LOCATION

THE GEOTECHNICAL INFORMATION (OR ANY PORTIONS THEREOF) ARE PROVIDED ONLY AS AVAILABLE INFORMATION. THE CONTRACTOR MAY DRAW HIS OWN CONCLUSIONS FROM THE DATA SHOWN. THE SOILS INFORMATION IS NOT REPRESENTATIVE OF ALL SOIL WHICH MIGHT BE ENCOUNTERED WITHIN THE LIMITS OF THE PROJECT. THE CONTRACTOR SHALL BY HIS OWN MEANS, SATISFY HIMSELF AS TO THE EXISTING SITE AND GEOTECHNICAL CONDITIONS FOR DETERMINING COST, MEANS, METHODS, TECHNIQUES AND SEQUENCES OF CONSTRUCTION.



KEY MAP

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
EXPAND AIRPORT EAST QUADRANT GENERAL AVIATION AIRCRAFT PARKING APRON

GEOTECHNICAL ENGINEERING INFORMATION

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/03/2023
JOB No:	21002031.00

FINAL

UPDATE BY: Sheue Torng Lee
LAYOUT: Geotechnical Engineering Information

DATE: Wednesday, March 22, 2023 12:59:43 PM
FILE: K:\ChicagoExecAp\21002031-00_ExpandEQuadApron\Draw\Sheets\expansion-geotechnical Info.dwg

A. REFERENCED SPECIFICATIONS

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE FOLLOWING, EXCEPT AS MODIFIED HEREIN OR ON THE PLANS:

* ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, DATED APRIL 1, 2012

* STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (LATEST EDITION), BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT SS) FOR ALL IMPROVEMENTS EXCEPT SANITARY SEWER AND WATER MAIN CONSTRUCTION;
* STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, LATEST EDITION (SSWS) FOR SANITARY SEWER AND WATER MAIN CONSTRUCTION;
* VILLAGE OF WHEELING MUNICIPAL CODE;
* THE METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO (MWRD) WATERSHED MANAGEMENT ORDINANCE AND TECHNICAL GUIDANCE MANUAL;
* IN CASE OF CONFLICT BETWEEN THE APPLICABLE ORDINANCES NOTED, THE MORE STRINGENT SHALL TAKE PRECEDENCE AND SHALL CONTROL ALL CONSTRUCTION.

B. NOTIFICATIONS

1. THE MWRD LOCAL SEWER SYSTEMS SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF ANY WORK (CALL 708-588-4055 OR SEND EMAIL NOTIFICATION WITH PROJECT NAME, LOCATION AND PERMIT NUMBER TO WMOJOBSTART@MWRD.ORG).

2. THE VILLAGE OF WHEELING ENGINEERING DEPARTMENT AND PUBLIC MUST BE NOTIFIED AT LEAST 24 HOURS PRIOR TO THE START OF CONSTRUCTION AND PRIOR TO EACH PHASE OF WORK. CONTRACTOR SHALL DETERMINE ITEMS REQUIRING INSPECTION PRIOR TO START OF CONSTRUCTION OR EACH WORK PHASE.

3. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES PRIOR TO BEGINNING CONSTRUCTION FOR THE EXACT LOCATIONS OF UTILITIES AND FOR THEIR PROTECTION DURING CONSTRUCTION. IF EXISTING UTILITIES ARE ENCOUNTERED THAT CONFLICT IN LOCATION WITH NEW CONSTRUCTION, IMMEDIATELY NOTIFY THE ENGINEER SO THAT THE CONFLICT CAN BE RESOLVED. CALL J.U.L.I.E. AT 1-800-892-0123.

C. GENERAL NOTES

1. ALL ELEVATIONS SHOWN ON PLANS REFERENCE THE NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD 29). SUBTRACT 0.24 FEET FROM 1929 DATUM TO OBTAIN 1988 DATUM (NAVD88).

2. MWRD, THE MUNICIPALITY AND THE OWNER OR OWNER'S REPRESENTATIVE SHALL HAVE THE AUTHORITY TO INSPECT, APPROVE, AND REJECT THE CONSTRUCTION IMPROVEMENTS.

3. THE CONTRACTOR(S) SHALL INDEMNIFY THE OWNER, ENGINEER, MUNICIPALITY, MWRD, AND THEIR AGENTS, ETC., FROM ALL LIABILITY INVOLVED WITH THE CONSTRUCTION, INSTALLATION, OR TESTING OF THIS WORK ON THE PROJECT.

4. THE PROPOSED IMPROVEMENTS MUST BE CONSTRUCTED IN ACCORDANCE WITH THE ENGINEERING PLANS AS APPROVED BY MWRD AND THE MUNICIPALITY UNLESS CHANGES ARE APPROVED BY MWRD, THE MUNICIPALITY, OR AUTHORIZED AGENT. THE CONSTRUCTION DETAILS, AS PRESENTED ON THE PLANS, MUST BE FOLLOWED. PROPER CONSTRUCTION TECHNIQUES MUST BE FOLLOWED ON THE IMPROVEMENTS INDICATED ON THE PLANS.

5. THE LOCATION OF VARIOUS UNDERGROUND UTILITIES WHICH ARE SHOWN ON THE PLANS ARE FOR INFORMATION ONLY AND REPRESENT THE BEST KNOWLEDGE OF THE ENGINEER. VERIFY LOCATIONS AND ELEVATIONS PRIOR TO BEGINNING THE CONSTRUCTION OPERATIONS.

6. ANY EXISTING PAVEMENT, SIDEWALK, DRIVEWAY, ETC., DAMAGED DURING CONSTRUCTION OPERATIONS AND NOT CALLED FOR TO BE REMOVED SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR.

7. MATERIAL AND COMPACTION TESTING SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE MUNICIPALITY, MWRD, AND OWNER.

8. THE UNDERGROUND CONTRACTOR SHALL MAKE ALL NECESSARY ARRANGEMENTS TO NOTIFY ALL INSPECTION AGENCIES.

9. ALL NEW AND EXISTING UTILITY STRUCTURES ON SITE AND IN AREAS DISTURBED DURING CONSTRUCTION SHALL BE ADJUSTED TO FINISH GRADE PRIOR TO FINAL INSPECTION.

10. RECORD DRAWINGS SHALL BE KEPT BY THE CONTRACTOR AND SUBMITTED TO THE ENGINEER AS SOON AS UNDERGROUND IMPROVEMENTS ARE COMPLETED. FINAL PAYMENTS TO THE CONTRACTOR SHALL BE HELD UNTIL THEY ARE RECEIVED. ANY CHANGES IN LENGTH, LOCATION OR ALIGNMENT SHALL BE SHOWN IN RED. ALL WYES OR BENDS SHALL BE LOCATED FROM THE DOWNSTREAM MANHOLE. ALL VALVES, B-BOXES, TEES OR BENDS SHALL BE TIED TO A FIRE HYDRANT.

D. SANITARY SEWER

1. THE CONTRACTOR SHALL TAKE MEASURES TO PREVENT ANY POLLUTED WATER, SUCH AS GROUND AND SURFACE WATER, FROM ENTERING THE EXISTING SANITARY SEWERS.

2. A WATER-TIGHT PLUG SHALL BE INSTALLED IN THE DOWNSTREAM SEWER PIPE AT THE POINT OF SEWER CONNECTION PRIOR TO COMMENCING ANY SEWER CONSTRUCTION. THE PLUG SHALL REMAIN IN PLACE UNTIL REMOVAL IS AUTHORIZED BY THE MUNICIPALITY AND/OR MWRD AFTER THE SEWERS HAVE BEEN TESTED AND ACCEPTED.

3. DISCHARGING ANY UNPOLLUTED WATER INTO THE SANITARY SEWER SYSTEM FOR THE PURPOSE OF SEWER FLUSHING OF LINES FOR THE DEFLECTION TEST SHALL BE PROHIBITED WITHOUT PRIOR APPROVAL FROM THE MUNICIPALITY OR MWRD.

4. ALL SANITARY SEWER CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS (LATEST EDITION).

5. ALL FLOOR DRAINS SHALL DISCHARGE TO THE SANITARY SEWER SYSTEM.

6. ALL DOWNSPOUTS AND FOOTING DRAINS SHALL DISCHARGE TO THE STORM SEWER SYSTEM.

7. ALL SANITARY SEWER PIPE MATERIALS AND JOINTS (AND STORM SEWER PIPE MATERIALS AND JOINTS IN A COMBINED SEWER AREA) SHALL CONFORM TO THE FOLLOWING:

PIPE MATERIAL	PIPE SPECIFICATIONS	JOINT SPECIFICATIONS
VITRIFIED CLAY PIPE	ASTM C-700	ASTM C-425
REINFORCED CONCRETE SEWER PIPE	ASTM C-76	ASTM C-443
CAST IRON SOIL PIPE	ASTM A-74	ASTM C-564
DUCTILE IRON PIPE	ANSI A21.51	ANSI A21.11
POLYVINYL CHLORIDE (PVC) PIPE		
6-INCH TO 15-INCH DIAMETER SDR 26	ASTM D-3034	ASTM D-3212
18-INCH TO 27-INCH DIAMETER F/DY=46	ASTM F-679	ASTM D-3212
HIGH DENSITY POLYETHYLENE (HDPE)	ASTM D-3350 ASTM D-3035	ASTM D-3261,F-2620 (HEAT FUSION) ASTM D-3212,F-477 (GASKETED)
WATER MAIN QUALITY PVC		
4-INCH TO 36-INCH	ASTM D-2241	ASTM D-3139
4-INCH TO 12-INCH	AWWA C900	ASTM D-3139
14-INCH TO 48-INCH	AWWA C905	ASTM D-3139

THE FOLLOWING MATERIALS ARE ALLOWED ON A QUALIFIED BASIS SUBJECT TO DISTRICT REVIEW AND APPROVAL PRIOR TO PERMIT ISSUANCE. A SPECIAL CONDITION WILL BE ADDED TO THE PERMIT WHEN THE PIPE MATERIAL BELOW IS USED FOR SEWER CONSTRUCTION OR A CONNECTION IS MADE.

PIPE MATERIAL	PIPE SPECIFICATIONS	JOINT SPECIFICATIONS
POLYPROPYLENE (PP) PIPE		
12-INCH TO 24-INCH DOUBLE WALL	ASTM F-2736	D-3212, F-477
30-INCH TO 60-INCH TRIPLE WALL	ASTM F-2764	D3212, F-477

8. ALL SANITARY SEWER CONSTRUCTION (AND STORM SEWER CONSTRUCTION IN COMBINED SEWER AREAS), REQUIRES STONE BEDDING WITH STONE ¼ " TO 1" IN SIZE, WITH MINIMUM BEDDING THICKNESS EQUAL TO ¼ THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR (4) INCHES NOR MORE THAN EIGHT (8) INCHES. MATERIAL SHALL BE CA-7, CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 12" ABOVE THE TOP OF THE PIPE WHEN USING PVC.

9. NON-SHEAR FLEXIBLE-TYPE COUPLINGS SHALL BE USED IN THE CONNECTION OF SEWER PIPES OF DISSIMILAR PIPE MATERIALS.

10. ALL MANHOLES SHALL BE PROVIDED WITH BOLTED, WATERTIGHT COVERS. SANITARY LIDS SHALL BE CONSTRUCTED WITH A CONCEALED PICKHOLE AND WATERTIGHT GASKET WITH THE WORD "SANITARY" CAST INTO THE LID.

11. WHEN CONNECTING TO AN EXISTING SEWER MAIN BY MEANS OTHER THAN AN EXISTING WYE, TEE, OR AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:
    a) A CIRCULAR SAW-CUT OF SEWER MAIN BY PROPER TOOLS ("SHEWER-TAP" MACHINE OR SIMILAR) AND PROPER INSTALLATION OF HUBWYE SADDLE OR HUB-TEE SADDLE.
    b) REMOVE AN ENTIRE SECTION OF PIPE (BREAKING ONLY THE TOP OF ONE BELL) AND REPLACE WITH A WYE OR TEE BRANCH SECTION.
    c) WITH PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING, USING "BAND SEAL" OR SIMILAR COUPLINGS TO HOLD IT FIRMLY IN PLACE.

12. WHENEVER A SANITARY/COMBINED SEWER CROSSES UNDER A WATERMAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATERMAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY/COMBINED SEWERS AND WATERMAINS SHALL BE MAINTAINED UNLESS: THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18" VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATERMAIN LOCATED AT THE OPPOSITE SIDE ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18" VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED CANNOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATER MAIN, THE SEWER SHALL BE CONSTRUCTED TO WATER MAIN STANDARDS OR IT SHALL BE ENCASED WITH A WATER MAIN QUALITY CARRIER PIPE WITH THE ENDS SEALED.

13. ALL EXISTING SEPTIC SYSTEMS SHALL BE ABANDONED. ABANDONED TANKS SHALL BE FILLED WITH GRANULAR MATERIAL OR REMOVED.

14. ALL SANITARY MANHOLES, (AND STORM MANHOLES IN COMBINED SEWER AREAS), SHALL HAVE A MINIMUM INSIDE DIAMETER OF 48 INCHES, AND SHALL BE CAST IN PLACE OR PRE-CAST REINFORCED CONCRETE.

15. ALL SANITARY MANHOLES, (AND STORM MANHOLES IN COMBINED SEWER AREAS), SHALL HAVE PRECAST "RUBBER BOOTS" THAT CONFORM TO ASTM C-923 FOR ALL PIPE CONNECTIONS. PRECAST SECTIONS SHALL CONSIST OF MODIFIED GROOVE TONGUE AND RUBBER GASKET TYPE JOINTS.

16. ALL ABANDONED SANITARY SEWERS SHALL BE PLUGGED AT BOTH ENDS WITH AT LEAST 2 FEET LONG NON-SHRINK CONCRETE OR MORTAR PLUG.

17. EXCEPT FOR FOUNDATION/FOOTING DRAINS PROVIDED TO PROTECT BUILDINGS, OR PERFORATED PIPES ASSOCIATED WITH VOLUME CONTROL FACILITIES, DRAIN TILES/FIELD TILES/UNDERDRAINS/PERFORATED PIPES ARE NOT ALLOWED TO BE CONNECTED TO OR TRIBUTARY TO COMBINED SEWERS, SANITARY SEWERS, OR STORM SEWERS TRIBUTARY TO COMBINED SEWERS IN COMBINED SEWER AREAS. CONSTRUCTION OF NEW FACILITIES OF THIS TYPE IS PROHIBITED; AND ALL EXISTING DRAIN TILES AND PERFORATED PIPES ENCOUNTERED WITHIN THE PROJECT AREA SHALL BE PLUGGED OR REMOVED, AND SHALL NOT BE CONNECTED TO COMBINED SEWERS, SANITARY SEWERS, OR STORM SEWERS TRIBUTARY TO COMBINED SEWERS.

18. A BACKFLOW PREVENTER IS REQUIRED FOR ALL DETENTION BASINS TRIBUTARY TO COMBINED SEWERS. REQUIRED BACKFLOW PREVENTERS SHALL BE INSPECTED AND EXERCISED ANNUALLY BY THE PROPERTY OWNER TO ENSURE PROPER OPERATION, AND ANY NECESSARY MAINTENANCES SHALL BE PERFORMED TO ENSURE FUNCTIONALITY. IN THE EVENT OF A SEWER SURCHARGE INTO AN OPEN DETENTION BASIN TRIBUTARY TO COMBINED SEWERS, THE PERMITTEE SHALL ENSURE THAT CLEAN UP AND WASH OUT OF SEWAGE TAKES PLACE WITHIN 48 HOURS OF THE STORM EVENT.

E. EROSION AND SEDIMENT CONTROL

1. THE CONTRACTOR SHALL INSTALL THE EROSION AND SEDIMENT CONTROL DEVICES AS SHOWN ON THE APPROVED EROSION AND SEDIMENT CONTROL PLAN.

2. EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE FUNCTIONAL PRIOR TO HYDROLOGIC DISTURBANCE OF THE SITE.

3. ALL DESIGN CRITERIA, SPECIFICATIONS, AND INSTALLATION OF EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL.

4. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.

5. INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM:
    a) UPON COMPLETION OF INITIAL EROSION AND SEDIMENT CONTROL MEASURES, PRIOR TO ANY SOIL DISTURBANCE.
    b) ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.

6. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE CO-PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.

7. A STABILIZED MAT OF CRUSHED STONE MEETING THE STANDARDS OF THE ILLINOIS URBAN MANUAL SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.

8. CONCRETE WASHOUT FACILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL AND SHALL BE INSTALLED PRIOR TO ANY ON SITE CONSTRUCTION ACTIVITIES INVOLVING CONCRETE.

9. MORTAR WASHOUT FACILITIES SHALL BE CONSTRUCTED IN ADDITION TO CONCRETE WASHOUT FACILITIES FOR ANY BRICK AND MORTAR BUILDING ENVELOPE CONSTRUCTION ACTIVITIES.

10. TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN. VOLUME CONTROL FACILITIES SHALL NOT BE USED AS TEMPORARY SEDIMENT BASINS.

11. DISTURBED AREAS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) DAYS.

12. ALL FLOOD PROTECTION AREAS AND VOLUME CONTROL FACILITIES SHALL, AT A MINIMUM, BE PROTECTED WITH A DOUBLE-ROW OF SILT FENCE (OR EQUIVALENT).

13. VOLUME CONTROL FACILITIES SHALL NOT BE CONSTRUCTED UNTIL ALL OF THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED.

14. SOIL STOCKPILES SHALL, AT A MINIMUM, BE PROTECTED WITH PERIMETER SEDIMENT CONTROLS. SOIL STOCKPILES SHALL NOT BE PLACED IN FLOOD PROTECTION AREAS OR THEIR BUFFERS.

15. EARTHEN EMBANKMENT SIDE SLOPES SHALL BE STABILIZED WITH APPROPRIATE EROSION CONTROL BLANKET.

16. STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY APPROPRIATE SEDIMENT CONTROL MEASURES.

17. THE CONTRACTOR SHALL EITHER REMOVE OR REPLACE ANY EXISTING DRAIN TILES AND INCORPORATE THEM INTO THE DRAINAGE PLAN FOR THE DEVELOPMENT. DRAIN TILES CANNOT BE TRIBUTARY TO A SANITARY OR COMBINED SEWER. DRAIN TILES ALLOWED IN COMBINED SEWER AREA FOR GREEN INFRASTRUCTURE PRACTICES.

18. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE SITE INSPECTOR MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.

19. THE CONTRCTOR SHALL BE RESPONSIBLE FOR TRENCH DEWATERING AND EXCAVATION FOR THE INSTALLATION OF SANITARY SEWERS, STORM SEWERS, WATERMAINS AS WELL AS THEIR SERVICES AND OTHER APPURTENANCES. ANY TRENCH DEWATERING, WHICH CONTAINS SEDIMENT SHALL PASS THROUGH A SEDIMENT SETTLING POND OR EQUALLY EFFECTIVE SEDIMENT CONTROL DEVICE. ALTERNATIVES MAY INCLUDE DEWATERING INTO A SUMP PIT, FILTER BAG OR EXISTING VEGETATED UPSLOPE AREA. SEDIMENT LADEN WATERS SHALL NOT BE DISCHARGE TO WATERWAYS, FLOOD PROTECTION AREAS OR THE COMBINED SEWER SYSTEM.

20. ALL PERMANENT EROSION CONTROL PRACTICES SHALL BE INITIATED WITHIN SEVEN (7) DAYS FOLLOWING THE COMPLETION OF SOIL DISTURBING ACTIVITIES.

21. ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED AND REPAIRED AS NEEDED ON A YEAR-ROUND BASIS DURING CONSTRUCTION AND ANY PERIODS OF CONSTRUCTION SHUTDOWN UNTIL PERMANENT STABILIZATION IS ACHIEVED.

22. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER PERMANENT SITE STABILIZATION.

23. THE EROSION AND SEDIMENT CONTROL MEASURES SHOWN ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, SITE INSPECTOR, OR MWRD.

IL. CONTRACT: **PA065**
IL. LETTING ITEM: **11A**
IL. PROJECT: **PWK-4795**
S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

0 1 2
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CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
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MWRD GENERAL NOTES

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SHEET 27 OF 27 SHEETS

UPDATE BY: Sheue Torng Lee
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DATE: Wednesday, March 22, 2023 12:59:49 PM
FILE: K:\ChicagoExecAvp\21002031-00_ExpandEQuadApron\Draw\Sheets expansion-mwrd General Notes.dwg