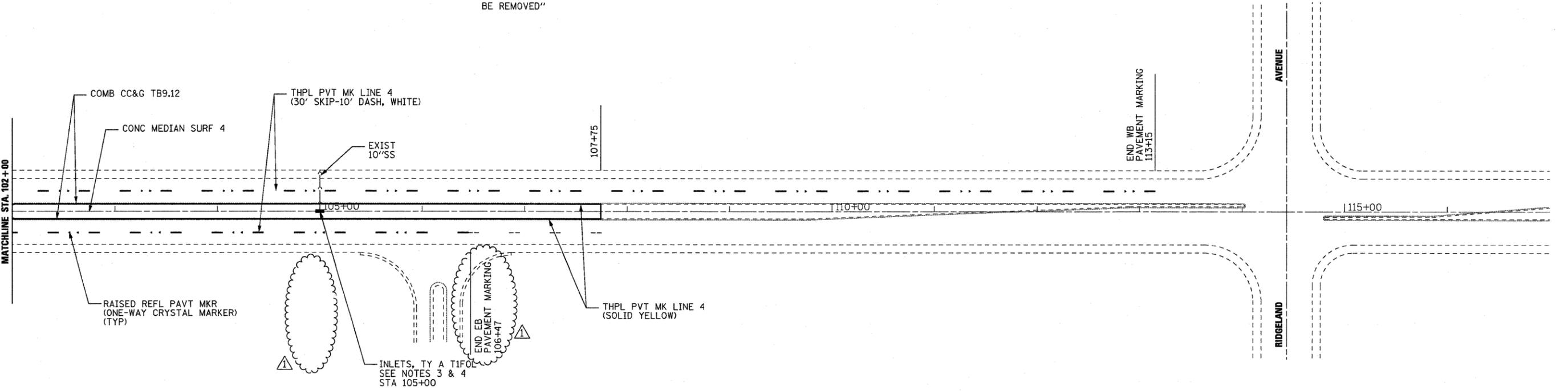


NOTES:

1. REFER TO DISTRICT 1 DETAILS "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" AND "TYPICAL PAVEMENT MARKING" FOR ADDITIONAL INFORMATION.
2. THE PROPOSED PAVEMENT MARKING SHALL BE PLACED IN THE SAME LOCATIONS AS THE EXISTING PAVEMENT MARKING OR AS DIRECTED BY THE ENGINEER
3. EXISTING INLET, TY A DRAINAGE STRUCTURES WILL BE REMOVED TO CONSTRUCT THE TEMPORARY PAVEMENT. THE TOP OF THE FRAME AND INVERT OF THE NEW DRAINAGE STRUCTURE SHALL MATCH THE EXISTING DRAINAGE STRUCTURE OR AS DIRECTED BY THE ENGINEER
4. AFTER THE REMOVAL OF THE EXISTING DRAINAGE STRUCTURE THE CONTRACTOR SHALL PLACE A TEMPORARY PLUG INTO THE EXISTING STORM SEWER IN A METHOD AS APPROVED BY THE ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE PAY ITEM "DRAINAGE STRUCTURE TO BE REMOVED"



FILE NAME =
FILEL



DESIGNED	TWL	REVISED	-	2/3/2010
DRAWN	JLS	REVISED	-	2/9/2010
CHECKED	SF	REVISED	-	2/26/2010
DATE	1/29/2010	REVISED	-	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY & PAVEMENT MARKING PLAN
U.S. RTE 30 (LINCOLN HWY)**

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	23R B-1	COOK	20	5
CONTRACT NO. 60J43				
ILLINOIS FED. AID PROJECT				