

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	23R-B-I	COOK	20	1
FED ROAD DIST No. 1 ILLINOIS		CONTRACT No. 60J43		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

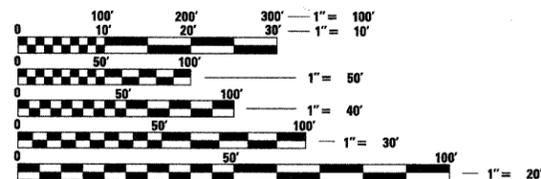
FAP 353 / US 30 AT BUTTERFIELD CREEK
BRIDGE DECK OVERLAY, JOINT REPAIR
SECTION 23R-B-I
PROJECT NO. NHF-0353 (016)
COOK COUNTY
C-91-233-10

FOR INDEX OF SHEETS, SEE SHEET NO. 2

TRAFFIC DATA:

EXISTING ADT US 30 17500 (2007)
POSTED SPEED = 45 MPH

PROJECT IS LOCATED IN THE
VILLAGE OF MATTESON



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

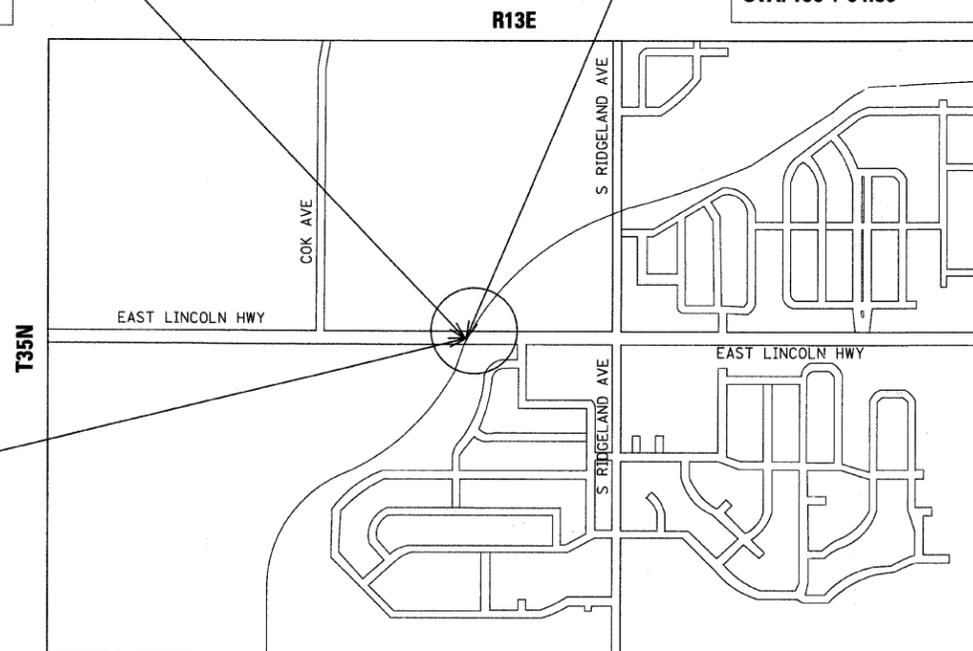
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 OR 811

PROJECT MANAGER RAJENDRA SHAH (847) 705-4555
PROJECT ENGINEER MICHELLE AQUINO (847) 705-4606
CONTRACT NO. 60J43

PROJECT LOCATION
US 30 (LINCOLN HWY)
OVER BUTTERFIELD CREEK
STRUCTURE NO. 016-0286

IMPROVEMENT BEGINS
STA. 99 + 99.14

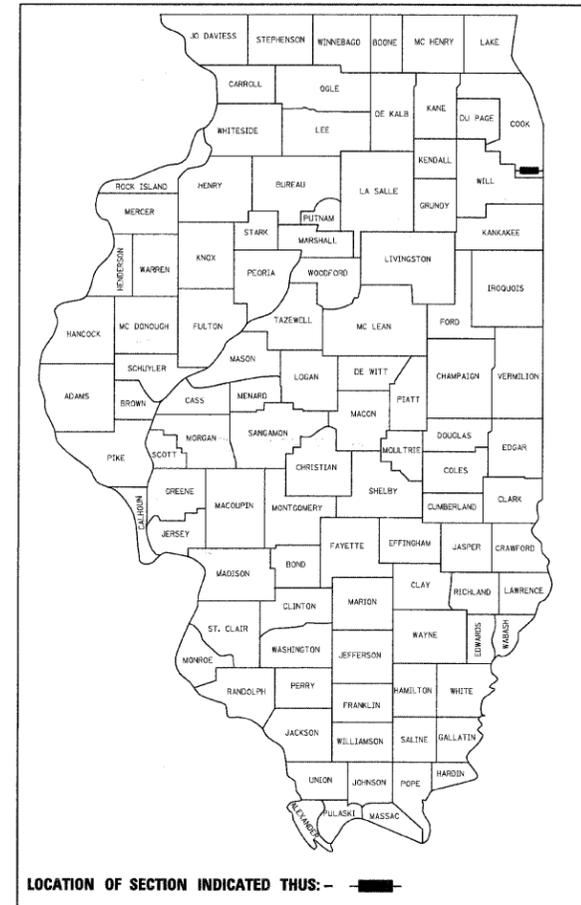
IMPROVEMENT ENDS
STA. 100 + 64.86



RICH TOWNSHIP
LOCATION MAP
NOT TO SCALE

GROSS LENGTH OF PROJECT = 65.72 FEET = 0.012 MILES
NET LENGTH OF PROJECT = 65.72 FEET = 0.012 MILES

D-91-233-10

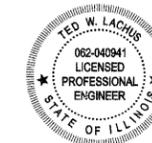


LOCATION OF SECTION INDICATED THUS: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED JANUARY 22 2010
Diane M. O'Keefe *as*
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
March 19 2010
Scott E. Stitt P.E. *as*
ASSISTANT ENGINEER OF DESIGN AND ENVIRONMENT
March 19 2010
Christine M. Reed *as*
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS



Ted W. Lachus
TED W. LACHUS, P.E.
EXPIRES 12-30-2011
DATE 1-29-2010

Primera

100 S. WACKER DRIVE SUITE 700 CHICAGO IL 60606.
P:312-606-0910 F:312-606-0415

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- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001001-02 AREAS OF REINFORCEMENT BARS
- 001006 DECIMAL OF AN INCH AND OF A FOOT
- 631031-08 TRAFFIC BARRIER TERMINAL, TYPE 6
- 701311-03 LANE CLOSURE 2 LANE 2 WAY MOVING OPERATIONS DAY ONLY
- 701606-06 URBAN LANE CLOSURE, MULTILANE 2W WITH MOUNTABLE MEDIAN
- 701901-01 TRAFFIC CONTROL DEVICES

GENERAL NOTES:

1. THESE PLANS HAVE BEEN PREPARED FROM INFORMATION ACQUIRED FROM EXISTING PLANS AND NOTES RECEIVED FROM IDOT FIELD MAINTENANCE ENGINEERS.
2. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO VARIATIONS FOUND IN THE FIELD. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. ANY ADJUSTMENTS PROPOSED BY THE CONTRACTOR MUST BE APPROVED BY THE ENGINEER. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE.
3. QUANTITIES FOR DECK SLAB REPAIR ARE APPROXIMATE. LOCATIONS WILL BE DETERMINED BY THE ENGINEER FOLLOWING REMOVAL OF THE HMA SURFACE COURSE AND HYDRO-SCARIFICATION. ACTUAL REPAIR LOCATIONS SHALL BE SHOWN ON THE AS-BUILT PLANS.
4. FORTY- EIGHT HOURS BEFORE STARTING EXCAVATION, THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) OR 811 FOR LOCATIONS OF THE EXISTING UTILITIES.
5. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
6. SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
7. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
8. THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
9. WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AS WELL AS ADJOINING RESIDENTIAL AREAS.
10. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS, AS REQUIRED, PRIOR TO COMMENCING WITH CONSTRUCTION.
11. THE CONTRACTOR SHALL TAKE ALL NECESSARY MEASURES TO ASSURE THAT NO DEBRIS FALLS INTO THE WATERWAY. THE COST OF THIS WORK SHALL BE INCLUDED IN THE VARIOUS PAY ITEMS.
12. THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
13. THE CONTRACTOR SHALL CONTACT PATRICE HARRIS, THE AREA TRAFFIC FIELD ENGINEER, AT (708) 597-9800 TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
14. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OUTSIDE THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
15. ALL RAISED REFLECTIVE PAVEMENT MARKERS (BRIDGE) SHALL BE LOW PROFILE.
16. ACCESS SHALL BE PROVIDED AT ALL TIMES TO PROPERTIES ABUTTING THE PROPOSED IMPROVEMENT.
17. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LANES AND RAISED REFLECTIVE MARKERS IN ORDER THAT THESE LOCATIONS CAN BE REESTABLISHED FOR STRIPING, EXACT LOCATIONS OF ALL PAVEMENT MARKING SHALL BE AS DIRECTED BY THE ENGINEER.

COMMITMENTS

NONE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS
SURFACE COURSE	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	4% @ 70 Gyr.
BINDER COURSE	
LEVELING BINDER (MACHINE METHOD), IL-9.5mm, N70	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

The "AC Type" for Polymerized HMA Mixes SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" shall be "PG 64 -22" UNLESS modified by District ONE Special Provisions. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

Note: For proposed section see sheet number 4 and S7.

TEMPORARY PAVEMENT	
HOT-MIX ASPHALT BINDER IL-19MM	4% @ 50 GYR
HOT-MIX ASPHALT SURFACE COURSE, "MIX D", N50 (IL-9.5MM)	4% @ 50 GYR

SUMMARY OF QUANTITIES			URBAN 80% FED. 20% STATE	CONSTRUCTION TYPE CODE	
CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	016-0286 SFTY-2A QUANTITY	
40600637	LEVELING BINDER (MACHINE METHOD), IL-9.5 ^{FG} , N70	TON	12	12	
20200100	EARTH EXCAVATION	CU YD	545	545	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	13	13	
42001300	PROTECTIVE COAT	SQ YD	218	218	
44000100	PAVEMENT REMOVAL	SQ YD	2012	2012	
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	144	144	
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	2175	2175	
44000915	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	154	154	
44002020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	13,868	13,868	
50102400	CONCRETE REMOVAL	CU YD	14	14	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	14	14	
50300260	BRIDGE DECK GROOVING	SQ YD	201	201	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2650	2650	
60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	2	2	
60605900	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12	FOOT	2175	2175	
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	13,868	13,868	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3	
67100100	MOBILIZATION	L SUM	1	1	
70101800	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	20	20	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	10	10	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	3052	3052	

SUMMARY OF QUANTITIES			URBAN 80% FED. 20% STATE	CONSTRUCTION TYPE CODE	
CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	016-0286 SFTY-2A QUANTITY	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	280	280	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	210	210	
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3026	3026	
*78006110	PREFORMED THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	14	14	
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	86	86	
*78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	- EACH	4	4	
78300100	PAVEMENT MARKING REMOVAL	SQ FT	1022	1022	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	90	90	
X0320887	POLYMER CONCRETE	CU FT	7	7	
X0322489	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/2 INCHES	SQ YD	213	213	
X0325239	TEMPORARY PAVEMENT 10"	SQ YD	2012	2012	
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51	51	
X0322276	CLEANING AND PAINTING EXPOSED REBAR	L SUM	1	1	
X0325303	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	83	83	
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	54	54	
X0325775	WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH	FOOT	9185	9185	
Z0006204	BRIDGE DECK HYDRO-SCARIFICATION 1/2"	SQ YD	213	213	
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	9	9	
Z0018700	DRAINAGE STRUCTURE TO BE REMOVED	EACH	2	2	
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	

FILE NAME =
FILEL



DESIGNED	VEA	REVISED	-
DRAWN	VEA	REVISED	-
CHECKED	TWL	REVISED	-
DATE	1/29/2010	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

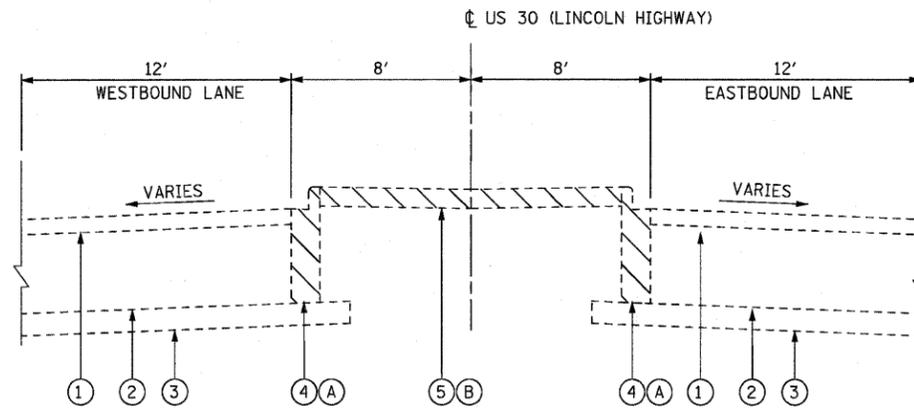
SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

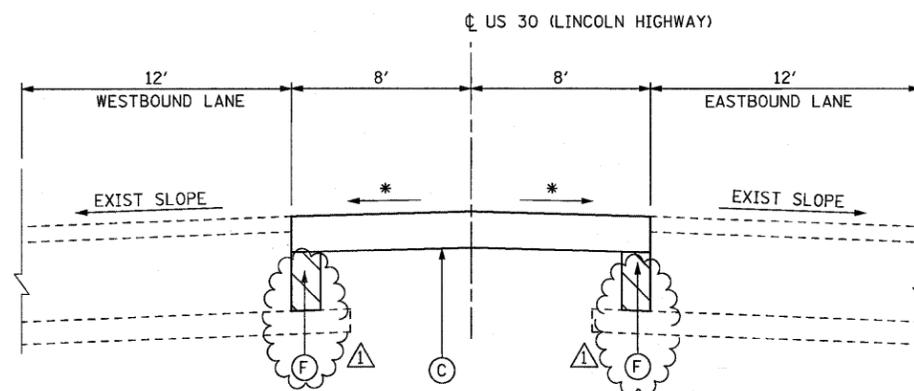
*Specialty Items

Rev.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	23R B-1	COOK	20	3
CONTRACT NO. 60J43				
[ILLINOIS] FED. AID PROJECT				

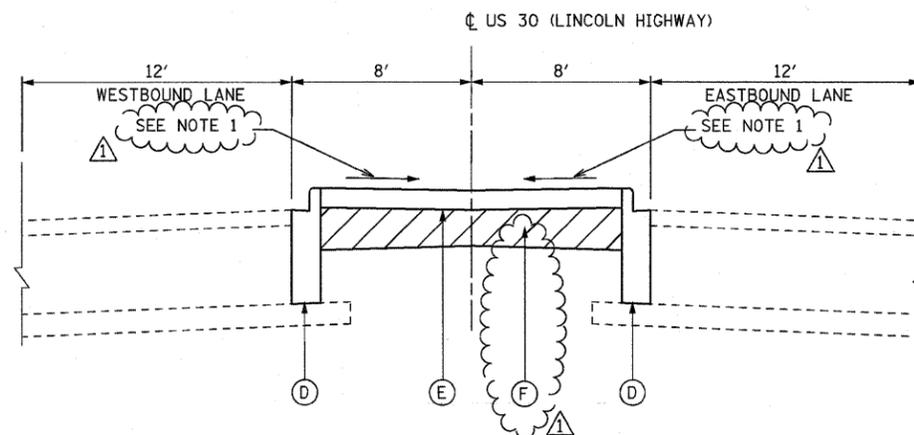


EXISTING TYPICAL SECTION
US 30 (LINCOLN HIGHWAY)



PROPOSED TYPICAL SECTION
TEMPORARY PAVEMENT
FOR MAINTENANCE OF TRAFFIC
US 30 (LINCOLN HIGHWAY)

* MATCH EXISTING CROSS-SLOPE OF ADJACENT
LANE OR AS DIRECTED BY THE ENGINEER



PROPOSED TYPICAL SECTION
US 30 (LINCOLN HIGHWAY)

LEGEND

- ① EXISTING HMA SURFACING 4-1/2" +/-
- ② EXISTING CONCRETE BASE COURSE, 10"
- ③ EXISTING STABILIZED SUB-BASE, 4"
- ④ EXISTING COMB. CONC. C&G TYPE B-9.12
- ⑤ EXISTING CONCRETE MEDIAN, 4"

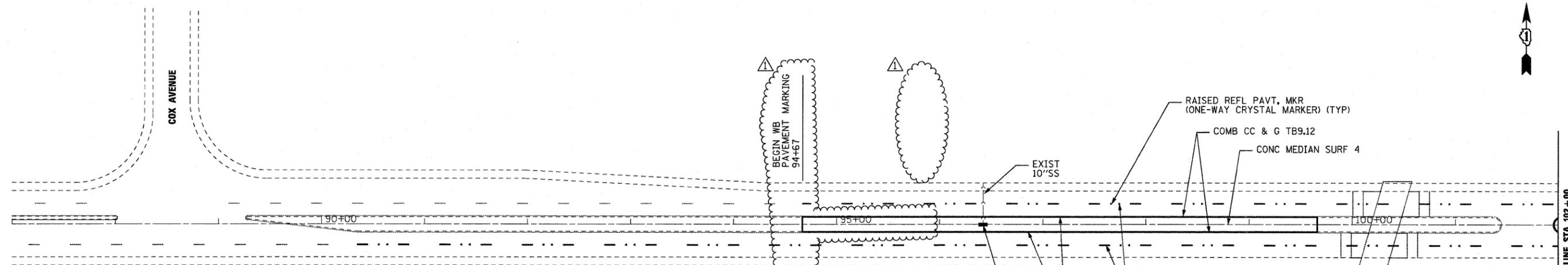
- Ⓐ COMBINATION CURB & GUTTER REMOVAL
- Ⓑ CONCRETE MEDIAN SURFACE REMOVAL
- Ⓒ TEMPORARY PAVEMENT 10" (SEE NOTE 2) ⚠
- Ⓓ COMBINATION CONCRETE CURB & GUTTER, TYPE B-9.12
- Ⓔ CONCRETE MEDIAN SURFACE, 4"
- Ⓕ EARTH EXCAVATION ⚠

NOTES

1. THE CONTRACTOR SHALL SLOPE THE CONCRETE MEDIAN SURFACE BOTH IN CROSS-SECTION AND LONGITUDINALLY IN ORDER TO CREATE POSITIVE FLOW PATTERNS TO THE INLETS IN THE MEDIAN AND TO AVOID PONDING OF WATER IN THE MEDIAN

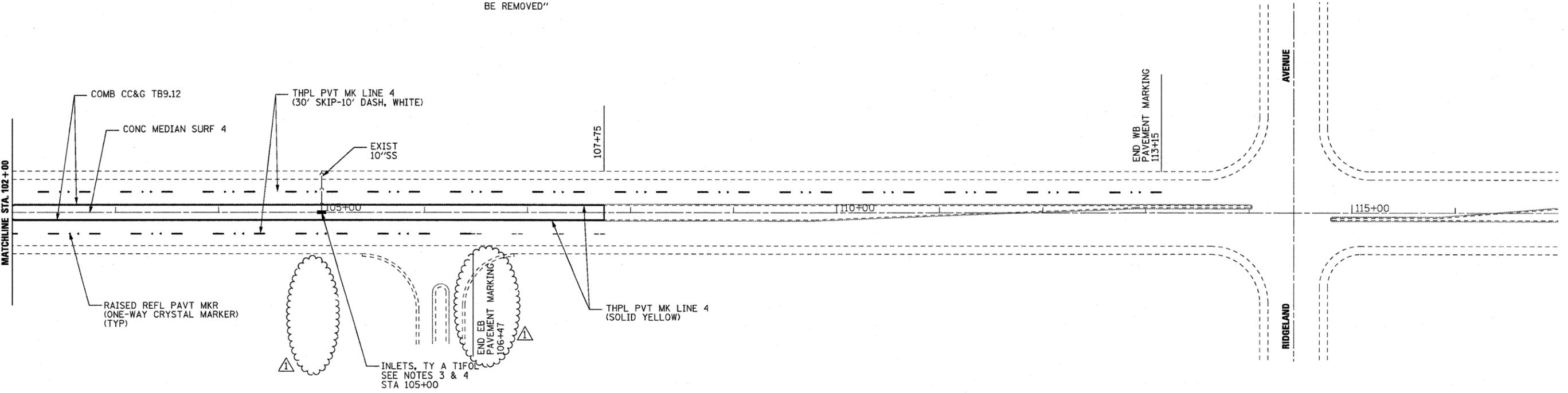
2. THE TEMPORARY PAVEMENT 10" WILL BE COMPRISED OF:
 8 1/2" HOT-MIX ASPHALT BINDER IL-19MM &
 1 1/2" HOT MIX ASPHALT SURFACE COURSE,
 MIX "D", N50 (IL-9.5MM)

FILE NAME = *FILEL*		DESIGNED	TWL	REVISED	-	2/3/2010	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS US RTE 30 (LINCOLN HWY)		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN	JLS	REVISED	-	2/9/2010		353	23R B-1	COOK	20	4		
CHECKED	SF	REVISED	-	2/26/2010	CONTRACT NO. 60J43									
DATE	1/29/2010	REVISED	-		SCALE:	SHEET NO. OF SHEETS		STA. TO STA.	ILLINOIS FED. AID PROJECT					



NOTES:

1. REFER TO DISTRICT 1 DETAILS "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" AND "TYPICAL PAVEMENT MARKING" FOR ADDITIONAL INFORMATION.
2. THE PROPOSED PAVEMENT MARKING SHALL BE PLACED IN THE SAME LOCATIONS AS THE EXISTING PAVEMENT MARKING OR AS DIRECTED BY THE ENGINEER
3. EXISTING INLET, TY A DRAINAGE STRUCTURES WILL BE REMOVED TO CONSTRUCT THE TEMPORARY PAVEMENT. THE TOP OF THE FRAME AND INVERT OF THE NEW DRAINAGE STRUCTURE SHALL MATCH THE EXISTING DRAINAGE STRUCTURE OR AS DIRECTED BY THE ENGINEER
4. AFTER THE REMOVAL OF THE EXISTING DRAINAGE STRUCTURE THE CONTRACTOR SHALL PLACE A TEMPORARY PLUG INTO THE EXISTING STORM SEWER IN A METHOD AS APPROVED BY THE ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE PAY ITEM "DRAINAGE STRUCTURE TO BE REMOVED"



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DESIGNED	TWL	REVISED	-	2/3/2010
DRAWN	JLS	REVISED	-	2/9/2010
CHECKED	SF	REVISED	-	2/26/2010
DATE	1/29/2010	REVISED	-	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY & PAVEMENT MARKING PLAN
U.S. RTE 30 (LINCOLN HWY)**

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	23R B-1	COOK	20	5
CONTRACT NO. 60J43				
ILLINOIS FED. AID PROJECT				

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE MAINTENANCE OF TRAFFIC CONTROL (MOT) PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY MODIFY THE MOT PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE MOT PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
2. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE TO THE MOT PLANS.
3. ALL EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE MAINTENANCE OF TRAFFIC STRIPING SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT, "PAVEMENT MARKING REMOVAL".
4. THE CONTRACTOR SHALL REMOVE ALL TEMPORARY PAVEMENT MARKING TAPE WHICH CONFLICTS WITH THE NEXT STAGE OR FINAL STRIPING. REMOVAL OF TEMPORARY PAVEMENT MARKING TAPE WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT, "WORK ZONE PAVEMENT MARKING REMOVAL".
5. ALL TRAFFIC CONTROL DEVICES USED FOR THE MAINTENANCE OF TRAFFIC, AS DETAILED ON THE PLANS, OR HIGHWAY STANDARD SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS SPECIFIED IN MAINTENANCE OF TRAFFIC SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.
6. ALL DRUMS, VERTICAL PANELS AND BARRICADES ADJACENT TO THE EDGE OF TRAVELED WAY SHALL BE EQUIPPED WITH STEADY-BURNING LIGHTS.
7. ALL EXISTING SIGNS WITHIN THE LIMITS OF MAINTENANCE OF TRAFFIC WHICH ARE OBSCURED BY OR OTHERWISE INTERFERED WITH BY THE CONSTRUCTION OPERATIONS AND MAINTENANCE OF TRAFFIC, SHALL BE COVERED OR REMOVED BY THE CONTRACTOR UNLESS SPECIFIED IN THE PLANS OR WHEN DIRECTED BY THE ENGINEER. THIS WORK SHALL BE IN ACCORDANCE WITH ARTICLE 107.25 OF THE IDOT STANDARD SPECIFICATIONS.
8. TEMPORARY, OFF-PEAK HOUR LANE CLOSURES MUST BE REQUESTED THROUGH THE ENGINEER AND AS SPECIFIED IN THE SPECIAL PROVISIONS, WHEN OFF-PEAK HOUR OR WEEKEND LANE CLOSURES ARE REQUIRED, A PORTABLE CHANGEABLE MESSAGE SIGN SHALL BE INSTALLED ONE WEEK PRIOR TO THE CLOSURE. THE MESSAGE SIGN WORDING AND LOCATION WILL BE DETERMINED BY THE ENGINEER.
9. THE CONTRACTOR SHALL PLACE A CHANGEABLE MESSAGE SIGN AT EACH END OF THE PROJECT AND/OR AS DIRECTED BY THE ENGINEER TO INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE MESSAGE SIGNS WITH THE APPROPRIATE INFORMATION SHALL BE IN PLACED TWO WEEKS BEFORE START OF CONSTRUCTION ACTIVITY. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR MONTH, "CHANGEABLE MESSAGE SIGN".
10. ALL TEMPORARY INFORMATION SIGNS SHALL BE PAID FOR SEPARATELY AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR "TEMPORARY INFORMATION SIGNING".
11. FOR ADDITIONAL BRIDGE CONSTRUCTION STAGING INFORMATION, SEE STRUCTURAL PLANS.

SUGGESTED CONSTRUCTION SEQUENCING

PRESTAGE

CONSTRUCTION:
REMOVE EXISTING CURB & GUTTER AND MEDIAN SURFACE, CONSTRUCT TEMPORARY BITUMINOUS PAVEMENT.

IMPLEMENT STAGE 1 MOT PAVEMENT MARKING AND TRAFFIC CONTROL

MAINTENANCE OF TRAFFIC:
UTILIZE STANDARDS 701431-05, 701426-03 AND 701601-06. 

STAGE 1

CONSTRUCTION:
WESTBOUND LANES: REMOVE BITUMINOUS OVERLAY, HYDROSCARIFY CONCRETE SLAB SURFACE & FULL DEPTH SLAB REPAIRS. PLACE LATEX CONCRETE OVERLAY AND PERFORM SUBSTRUCTURE REPAIRS.

MAINTENANCE OF TRAFFIC:
UTILIZE MAINTENANCE OF TRAFFIC DETAILS IN THE PLANS AND STANDARD 701431-05. 

STAGE 2

CONSTRUCTION:
EASTBOUND LANES: REMOVE BITUMINOUS OVERLAY, HYDROSCARIFY CONCRETE SLAB SURFACE & FULL DEPTH SLAB REPAIRS. PLACE LATEX CONCRETE OVERLAY. REMOVE CONCRETE FOR ELIMINATION OF EXPANSION JOINT AND PERFORM SUBSTRUCTURE REPAIRS.

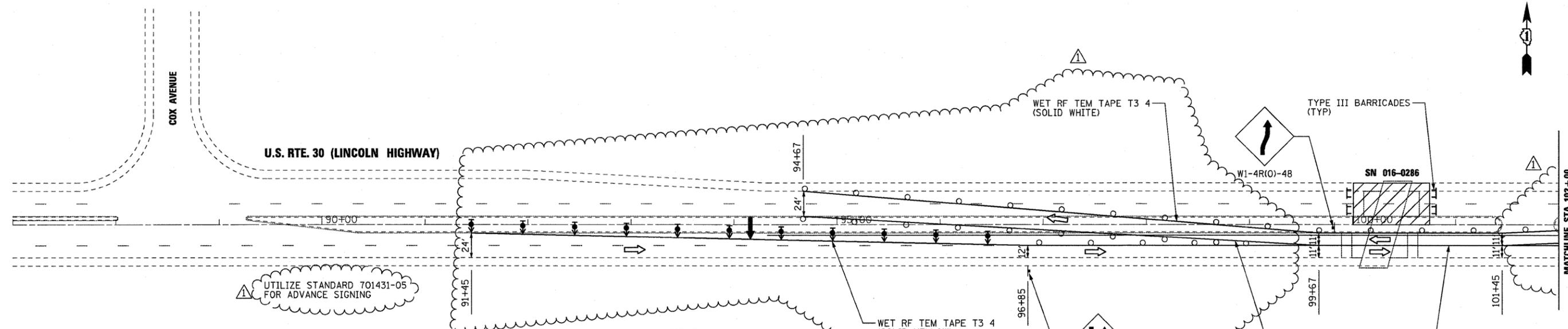
MAINTENANCE OF TRAFFIC:
UTILIZE MAINTENANCE OF TRAFFIC DETAILS IN THE PLANS AND STANDARD 701431-05. 

STAGE 3

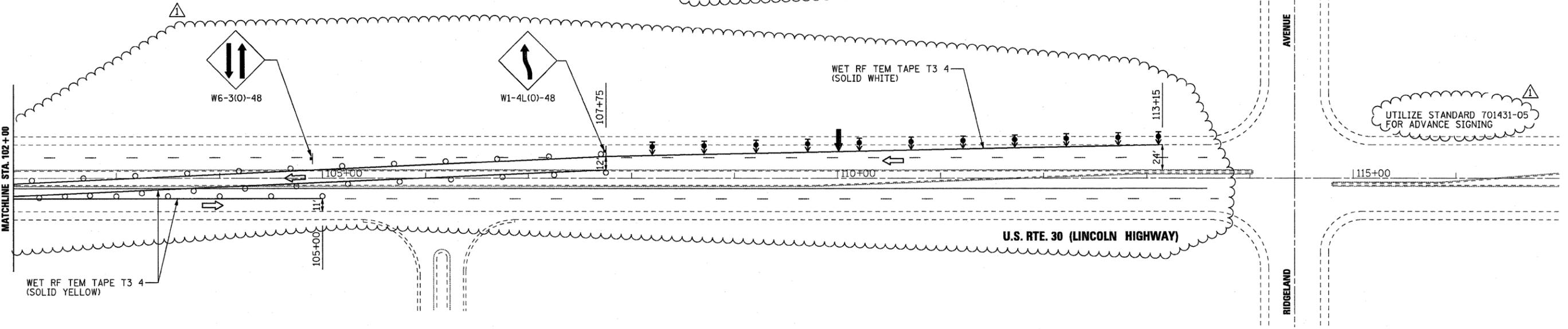
CONSTRUCTION:
CONSTRUCT NEW CURB & GUTTER, MEDIAN SURFACE AND DRAINAGE INLETS. PLACE FINAL PAVEMENT MARKINGS AND RAISED REFLECTIVE MARKERS.

MAINTENANCE OF TRAFFIC:
UTILIZE STANDARDS 701431-05 AND 701426-03. 

FILE NAME =		DESIGNED TWL	REVISED - 2/3/2010	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MOT GENERAL NOTES AND SUGGESTED CONSTRUCTION SEQUENCING			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN VEA	REVISED - 2/9/2010					353	23R B-1	COOK	20	6
		CHECKED SF	REVISED - 2/26/2010					CONTRACT NO. 60J43				
		DATE 1/29/2010	REVISED -					ILLINOIS FED. AID PROJECT				
					SCALE:	SHEET NO. OF SHEETS		STA. TO STA.				



- LEGEND:**
- WORK AREA
 - TEMPORARY CONCRETE BARRIER
 - ARROW BOARD
 - SIGN
 - TYPE II BARRICADE OR DRUM WITH STEADY BURNING LIGHTS @ 50 FT CENTERS ALONG ROADWAY (TANGENT) AND 20 FT CENTERS ALONG TAPERS
 - TYPE III BARRICADE
 - IMPACT ATTENUATOR, TEMPORARY NON-REDIRECTIVE
 - DIRECTION OF TRAFFIC
 - DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT



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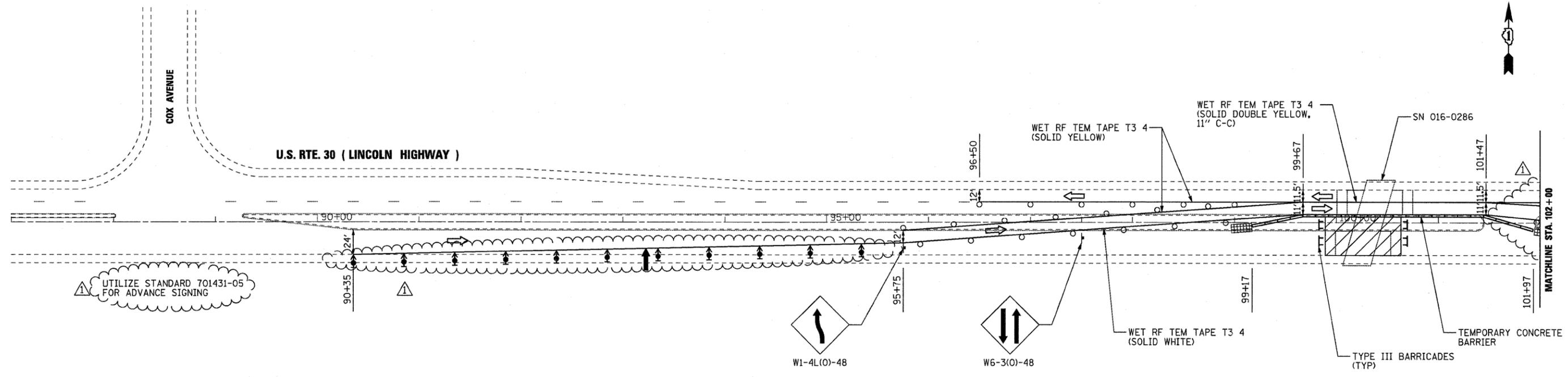
DESIGNED	TWL	REVISED	-	2/3/2010
DRAWN	JLS	REVISED	-	2/9/2010
CHECKED	SF	REVISED	-	2/26/2010
DATE	1/29/2010	REVISED	-	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGE OF CONSTRUCTION & TRAFFIC CONTROL
STAGE 1
U.S. RTE 30 (LINCOLN HIGHWAY)**

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. TO STA.

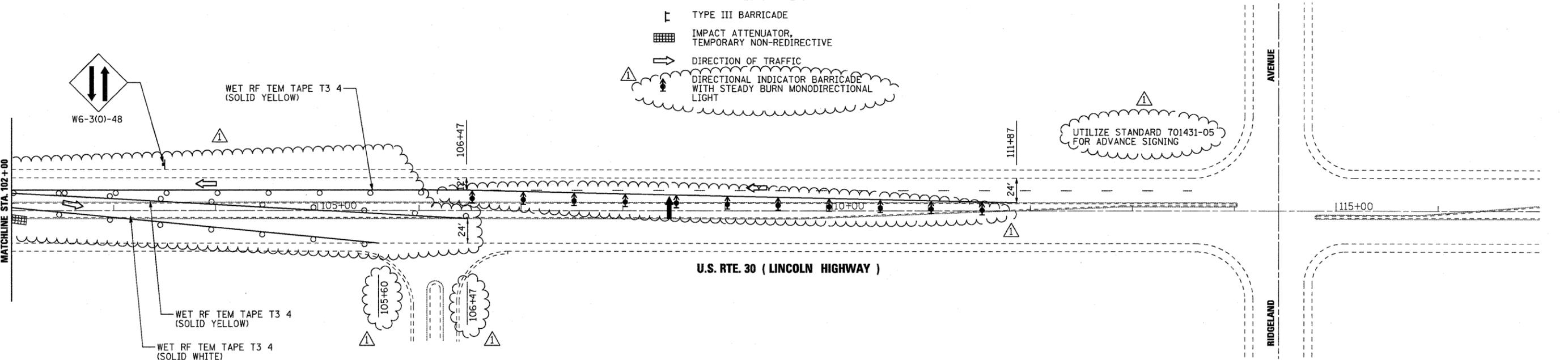
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	23R B-1	COOK	20	7
CONTRACT NO. 60J43				
ILLINOIS FED. AID PROJECT				



UTILIZE STANDARD 701431-05 FOR ADVANCE SIGNING

LEGEND:

- WORK AREA
- TEMPORARY CONCRETE BARRIER
- ARROW BOARD
- SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURNING LIGHTS @ 50 FT CENTERS ALONG ROADWAY (TANGENT) AND 20 FT CENTERS ALONG TAPERS
- TYPE III BARRICADE
- IMPACT ATTENUATOR, TEMPORARY NON-REDIRECTIVE
- DIRECTION OF TRAFFIC
- DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT



UTILIZE STANDARD 701431-05 FOR ADVANCE SIGNING

FILE NAME =
FILEL



DESIGNED	TWL	REVISED	-	2/3/2010
DRAWN	JLS	REVISED	-	2/9/2010
CHECKED	SF	REVISED	-	2/26/2010
DATE	1/29/2010	REVISED	-	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

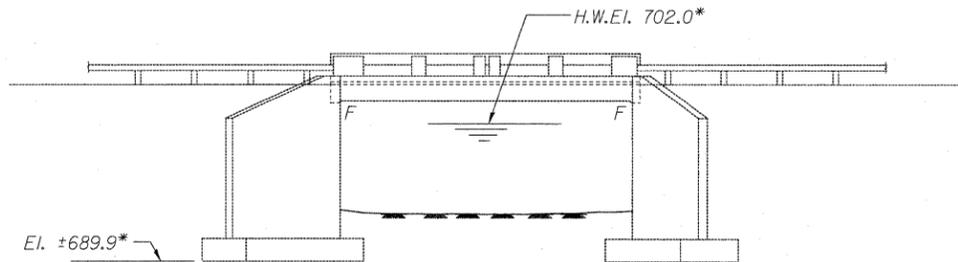
SUGGESTED STAGE OF CONSTRUCTION & TRAFFIC CONTROL
STAGE 2
U.S. RTE 30 (LINCOLN HIGHWAY)

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. TO STA.

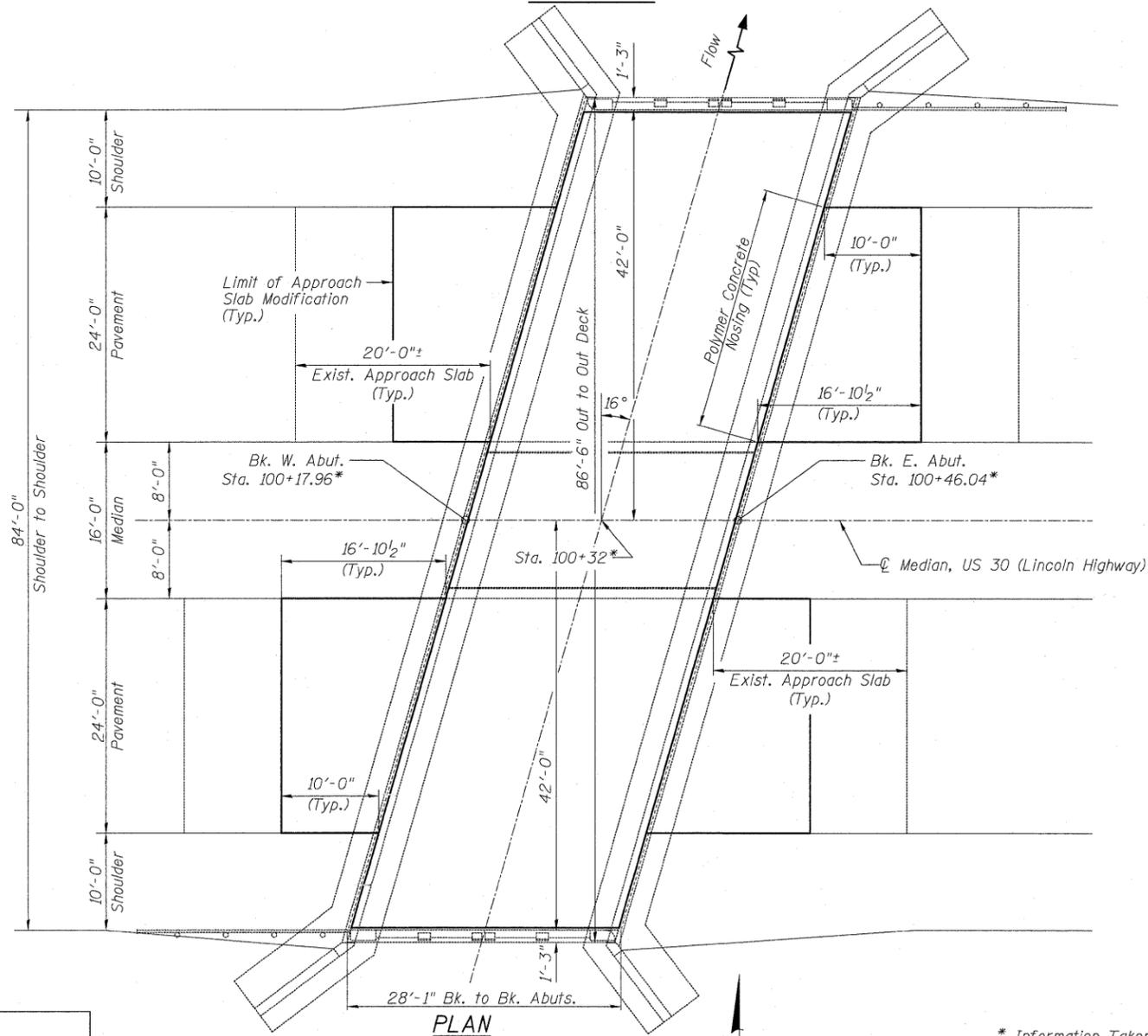
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	23R B-1	COOK	20	8
CONTRACT NO. 60J43			ILLINOIS FED. AID PROJECT	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Existing Structure: SN 016-0286, built in 1954 as FA Rte. 23, Section 22, 23-R-1 and widened in 1968 as a single span Reinforced Concrete slab. The structure measures 86'-6" Out to Out Deck and 28'-1" Bk. to Bk. Abutments. The substructure consists of Reinforced Concrete closed abutments. Traffic is to be maintained utilizing stage construction.



ELEVATION



PLAN

SCOPE OF WORK

1. Remove HMA Overlay, 2"±.
2. Hydro-scarify 1/2 inch slab surface.
3. Structural Repairs of Concrete to parapets and substructures.
4. Full and Partial Depth Slab repairs.
5. Eliminate longitudinal joint in median.
6. 2 1/2" inch latex concrete overlay.
7. Overlay approaches to account for change in deck elevation.

INDEX OF SHEETS

- S1 - General Plan & Elevation
- S2 - Stage Construction Details
- S3 - Temporary Concrete Barrier
- S4 - Bridge Deck Patching Plan
- S5 - Parapet Repair Details
- S6 - Abutments Repairs
- S7 - Approach Slab Modification & Median Joint Elimination Details

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications, 17th Edition.

DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi

LAST DELAMINATION SURVEY

October 2009

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	154	-	154
Hot-Mix Asphalt Surface Removal, 2"	Sq. Yd.	144	-	144
Concrete Removal	Cu. Yd.	14	-	14
Concrete Superstructure	Cu. Yd.	14	-	14
Reinforcement Bars, Epoxy Coated	Pound	2,650	-	2,650
Bridge Deck Hydro-scarification 1/2"	Sq. Yd.	213	-	213
Bridge Deck Grooving	Sq. Yd.	201	-	201
Protective Coat	Sq. Yd.	218	-	218
Bridge Deck Latex Concrete Overlay, 2 1/2"	Sq. Yd.	213	-	213
Structural Repair of Concrete (Depth equal to or less 5 inches)	Sq. Ft.	40	14	54
Structural Repair of Concrete (Depth greater than 5 inches)	Sq. Ft.	-	83	83
Deck Slab Repair (Full Depth Type II)	Sq. Yd.	9	-	9
Hot-Mix Asphalt Surface Course, Mix "D", N-70, 1"	Tons	13	-	13
Leveling Binder (Machine Method), IL9.5, N-70, 1"	Tons	12	-	12
Polymer Concrete	Cu. Ft.	7	-	7



Theodore P. George 3-26-10
Theodore P. George Date
Licensed Structural Engineer
State of Illinois 081-4609
Expires 11/30/2010

GENERAL PLAN & ELEVATION
FAP 353/US 30 (LINCOLN HIGHWAY)
OVER BUTTERFIELD CREEK
COOK COUNTY
STATION 100+32.00
STRUCTURE NO. 016-0286

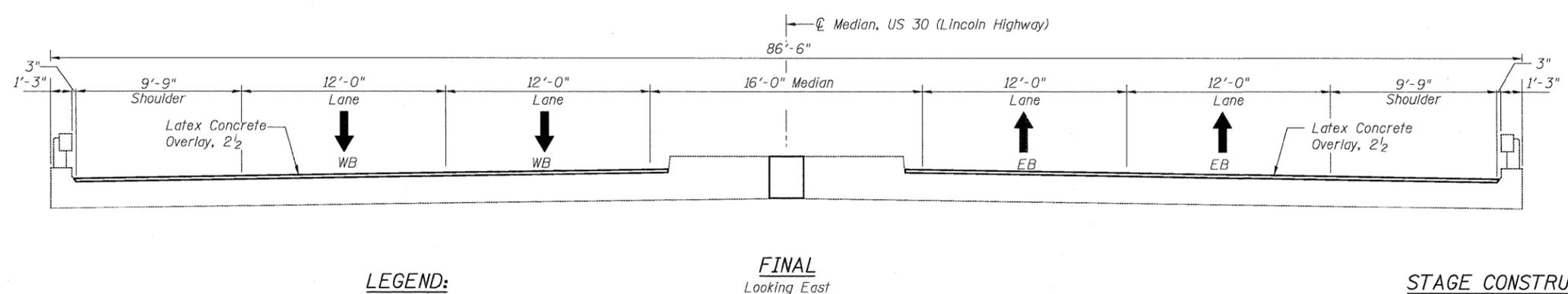
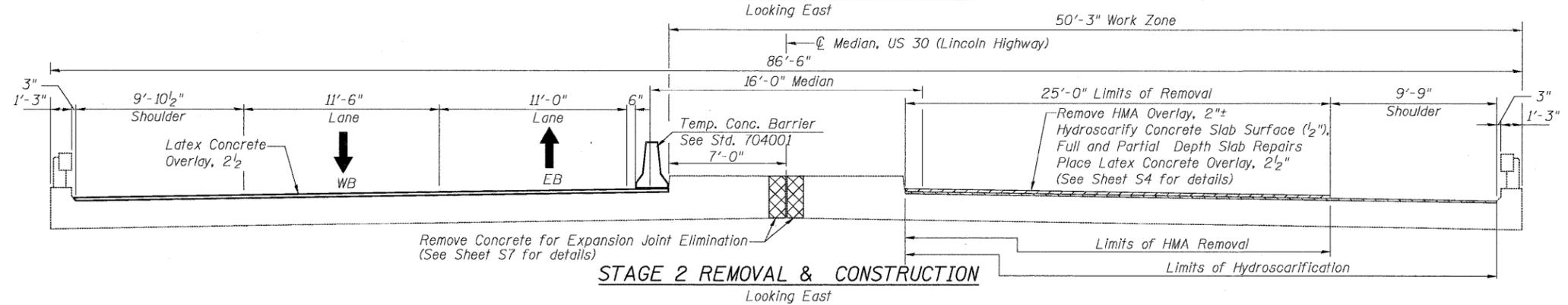
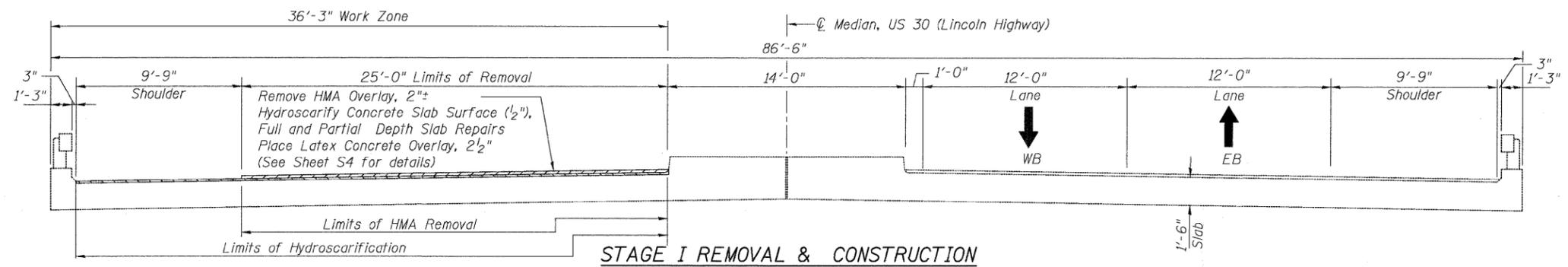
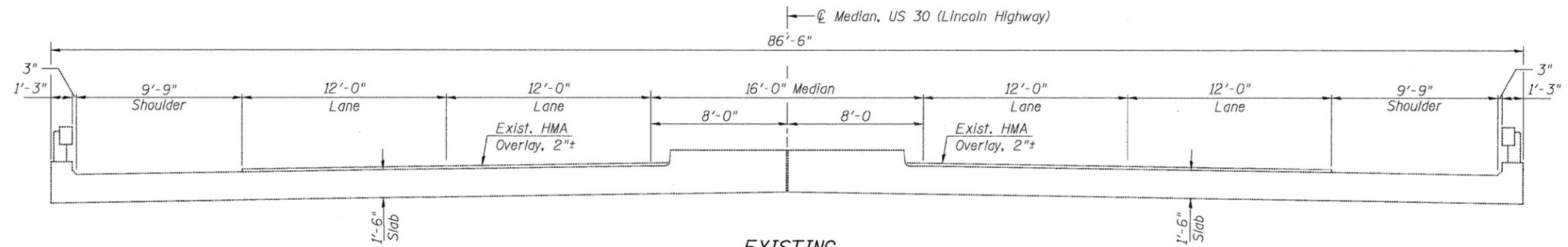
* Information Taken from 1967 Exist. Plans.

DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG

Primera
100 S. WACKER DRIVE SUITE 700, CHICAGO IL 60606 . P:312-686-8910 F:312-686-8415

SHEET NO. S1	F.A.P. RTE. 353	SECTION 23 R-B-I	COUNTY COOK	TOTAL SHEETS 20	SHEET NO. 9
S7 SHEETS					
CONTRACT NO. 60J43					
ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



LEGEND:

-  HMA Overlay Removal
-  Bridge Deck Hydro-scarification 1/2"
-  Concrete Removal

STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 016-0286

DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG

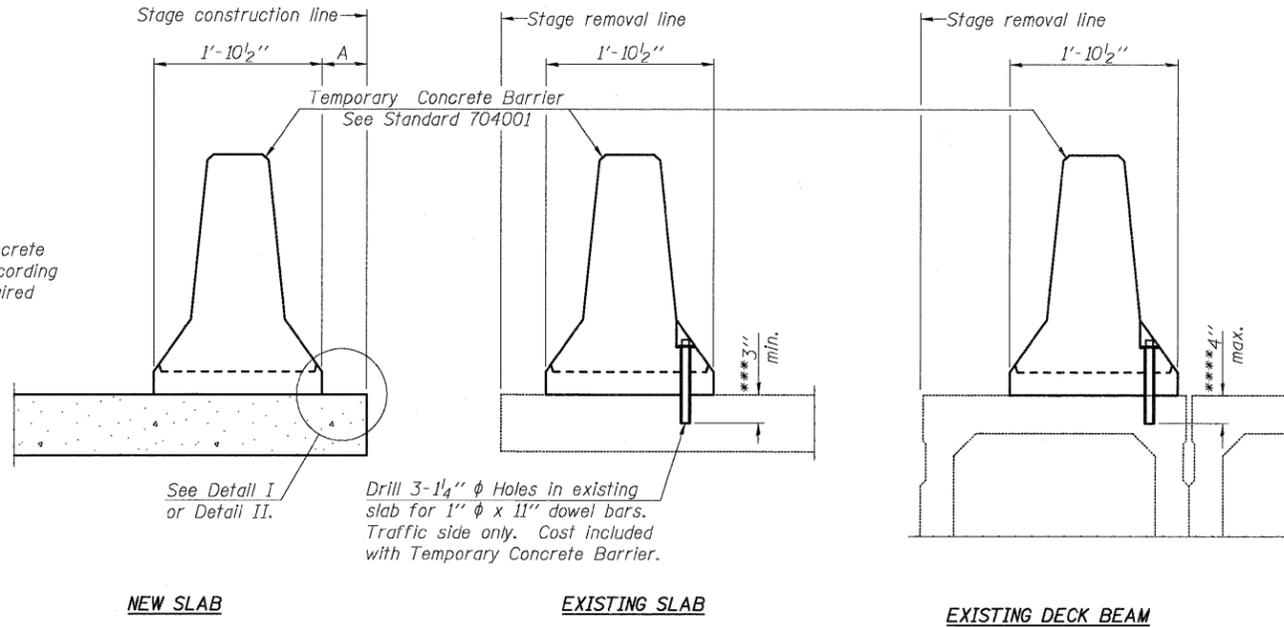


100 S. WACKER DRIVE SUITE 700, CHICAGO IL 60606 . P:312-606-0910 F:312-606-0415

SHEET NO. S2	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	353	23 R-B-I	COOK	20	10
S7 SHEETS		CONTRACT NO. 60J43			
ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



Drill 3-1/4" ϕ Holes in existing slab for 1" ϕ x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

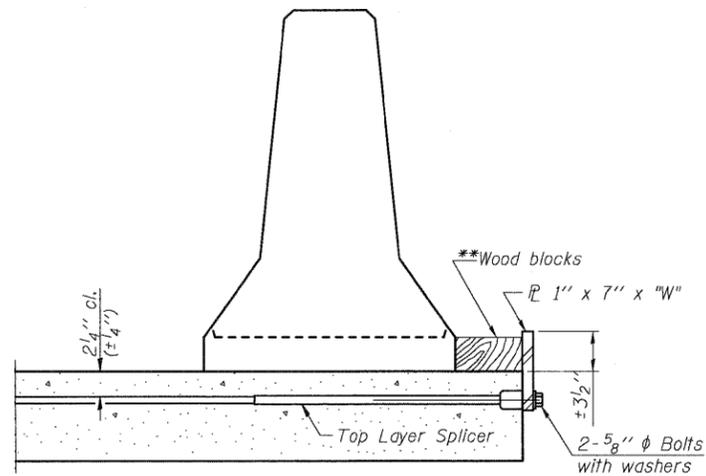
NOTES

- Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel PL to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate ϕ of each barrier panel.
- Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel PL to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate ϕ of each barrier panel.
- Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

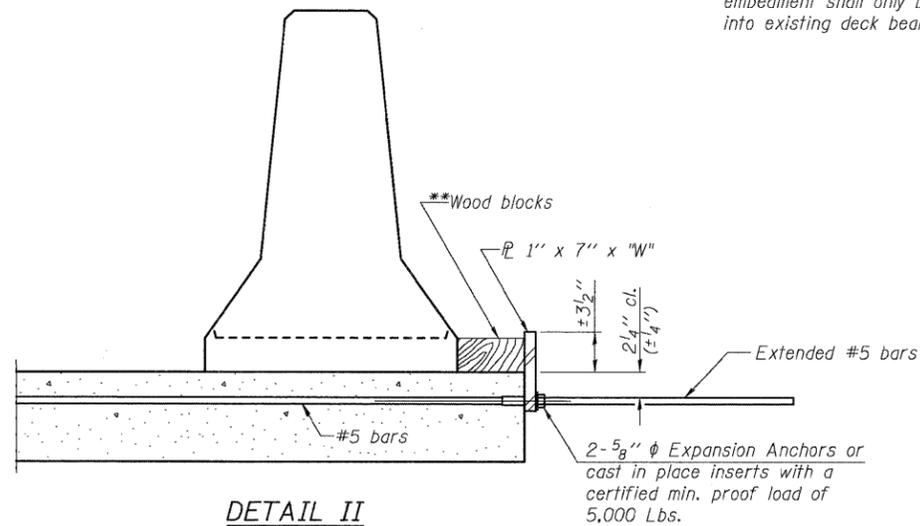
SECTIONS THRU SLAB OR DECK BEAM

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

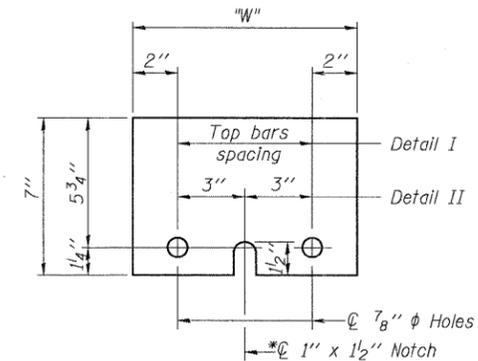
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER PL 1" x 7" x 10"

* Required only with Detail II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"

DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG

Primer

100 S. WACKER DRIVE SUITE 700 . CHICAGO IL 60606 . P.312-606-0910 F.312-606-0415

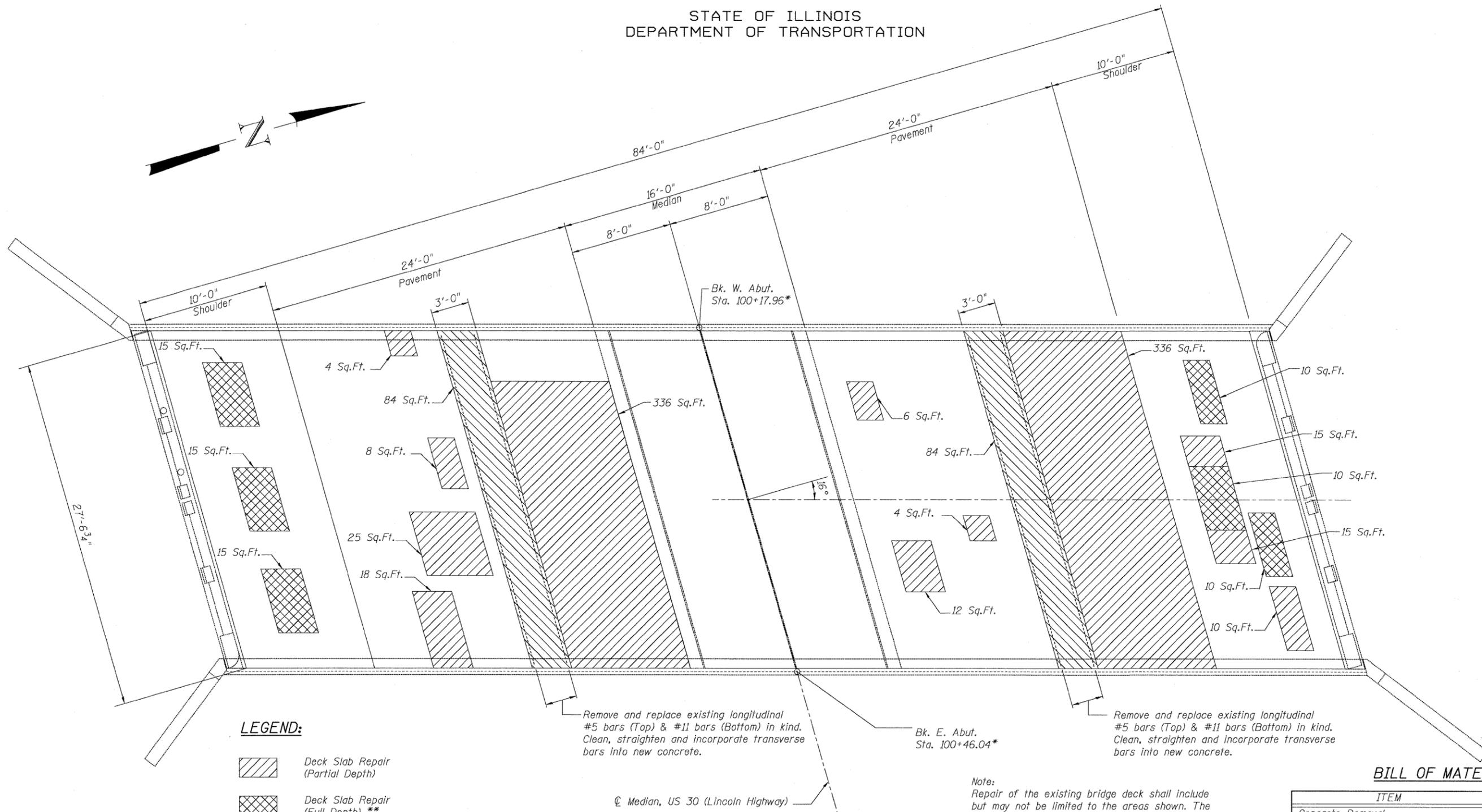
R-27

11-1-09

TEMPORARY CONCRETE BARRIER
S.N. 016-0286

SHEET NO. S3	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	353	23 R-B-I	COOK	20	11
S7 SHEETS			CONTRACT NO. 60J43		
ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



LEGEND:

-  Deck Slab Repair (Partial Depth)
-  Deck Slab Repair (Full Depth) **
-  Concrete Removal

Remove and replace existing longitudinal #5 bars (Top) & #11 bars (Bottom) in kind. Clean, straighten and incorporate transverse bars into new concrete.

Bk. E. Abut. Sta. 100+46.04*

Remove and replace existing longitudinal #5 bars (Top) & #11 bars (Bottom) in kind. Clean, straighten and incorporate transverse bars into new concrete.

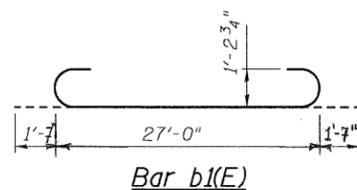
⊙ Median, US 30 (Lincoln Highway)

Note:
Repair of the existing bridge deck shall include but may not be limited to the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction.

* Information taken from 1967 existing plans.

BAR LIST

Bar	No.	Size	Length	Shape
b1(E)	8	#11	30'-2"	
b2(E)	8	#5	27'-3"	



BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	9.2
Reinforcement Bars, Epoxy Coated	Pound	1,510
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	9.0
Bridge Deck Hydro-scarification, 1/2"	Sq. Yd.	213
Bridge Deck Latex Concrete Overlay, 2 1/2"	Sq. Yd.	213
Concrete Superstructure	Cu. Yd.	9.2

**BRIDGE DECK PATCHING PLAN
STRUCTURE NO. 016-0286**

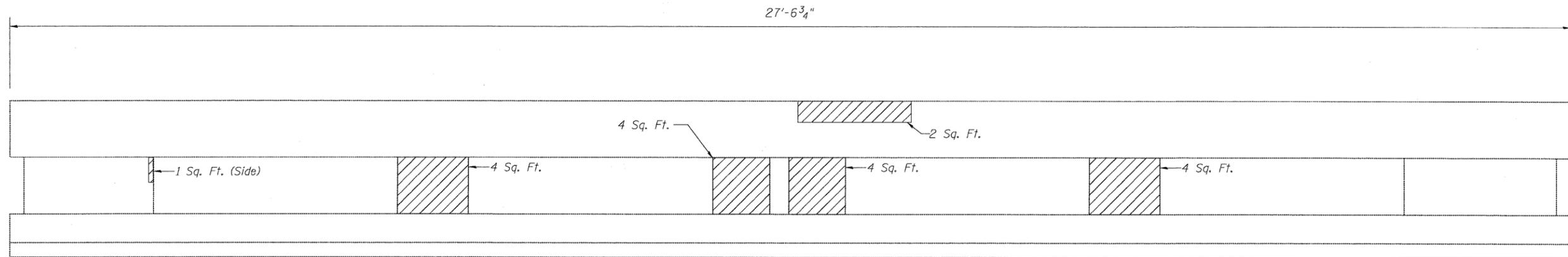
DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG



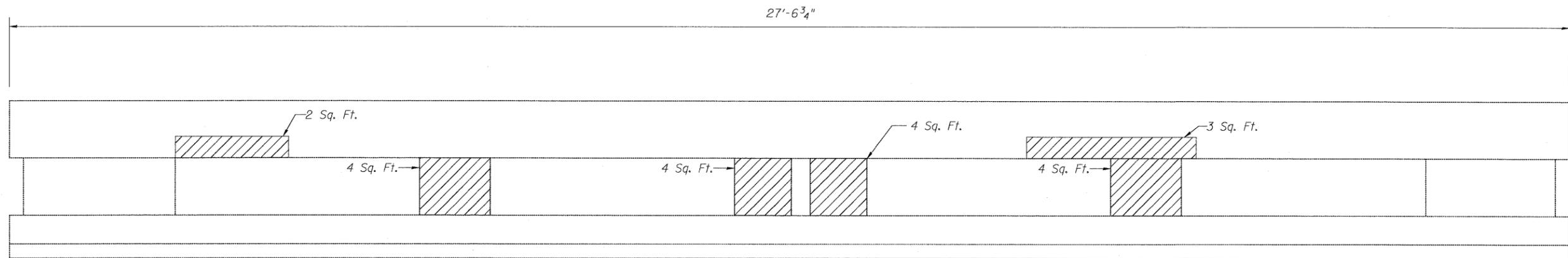
180 S. WACKER DRIVE SUITE 700, CHICAGO IL 60606 . P.312-606-0910 F.312-606-0415

SHEET NO. 54	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	353	23 R-B-1	COOK	20	12
S6 SHEETS	CONTRACT NO. 60J43			ILLINOIS FED. AID PROJECT	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



NORTH RAIL INSIDE ELEVATION



SOUTH RAIL INSIDE ELEVATION

Note:
Repair of the existing parapet shall include but may not be limited to the areas shown. The actual areas to be repaired will be determined by the Engineer at the time of construction.

LEGEND:

 Structural Repair of Concrete
(Depth equal to or less than 5 inches)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	40

PARAPET REPAIR DETAILS
STRUCTURE NO. 016-0286

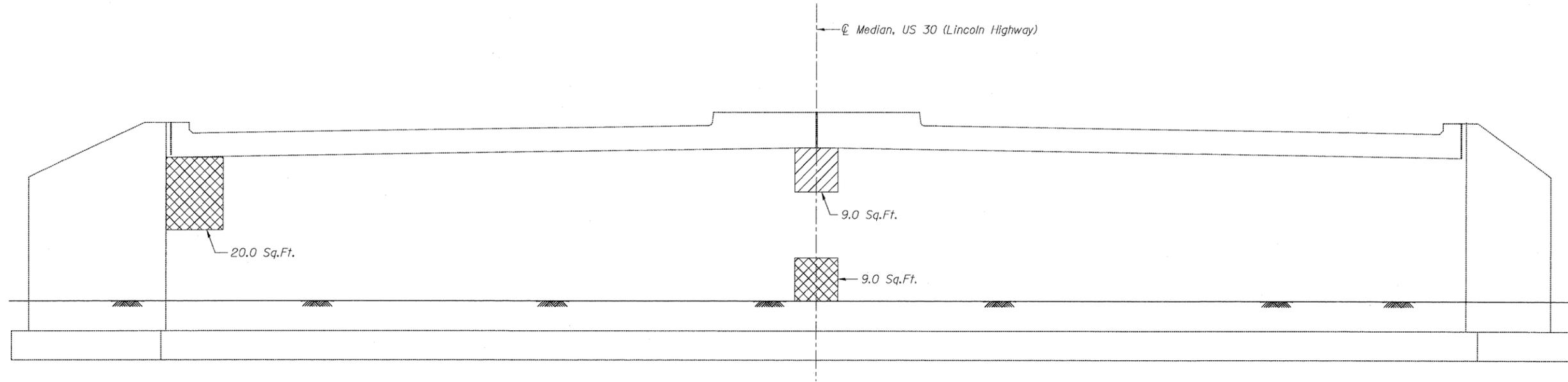
DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG



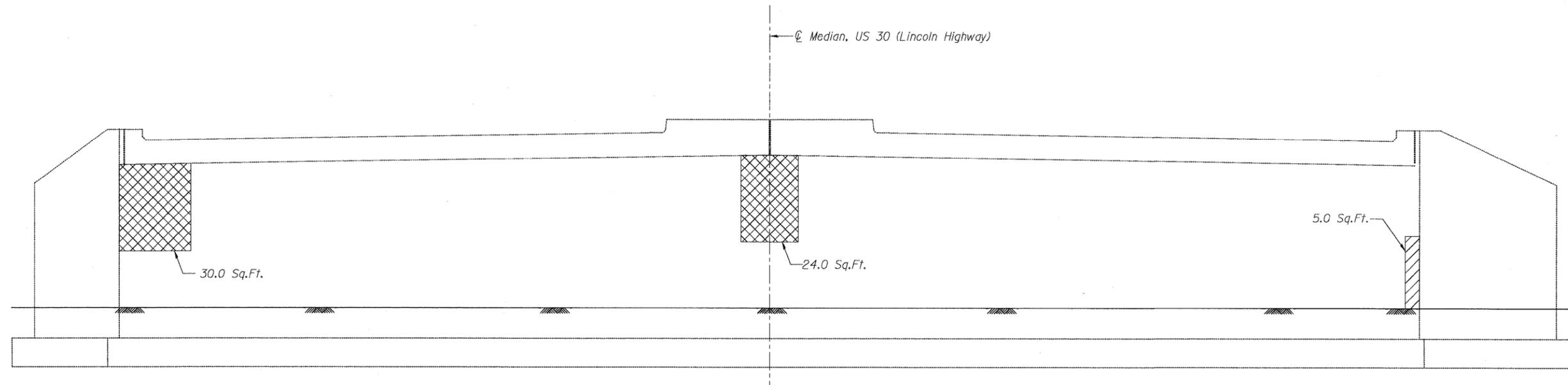
100 S. WACKER DRIVE SUITE 700, CHICAGO IL 60606 . P:312-686-0910 F:312-686-0415

SHEET NO. 55	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	353	23 R-B-I	COOK	20	13
S7 SHEETS			CONTRACT NO. 60J43		
ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



WEST ABUTMENT ELEVATION



EAST ABUTMENT ELEVATION

Note:
Repair of the existing abutment shall include
but may not be limited to the areas shown. The
actual areas to be repaired will be determined
by the Engineer at the time of construction.

ABUTMENT REPAIRS
STRUCTURE NO. 016-0286

DESIGNED	JPM
CHECKED	TG
DRAWN	MPS
CHECKED	JPM, TG

Primer
100 S. WACKER DRIVE SUITE 700 . CHICAGO IL 60606 . P.312-606-0910 F.312-606-0415

LEGEND:

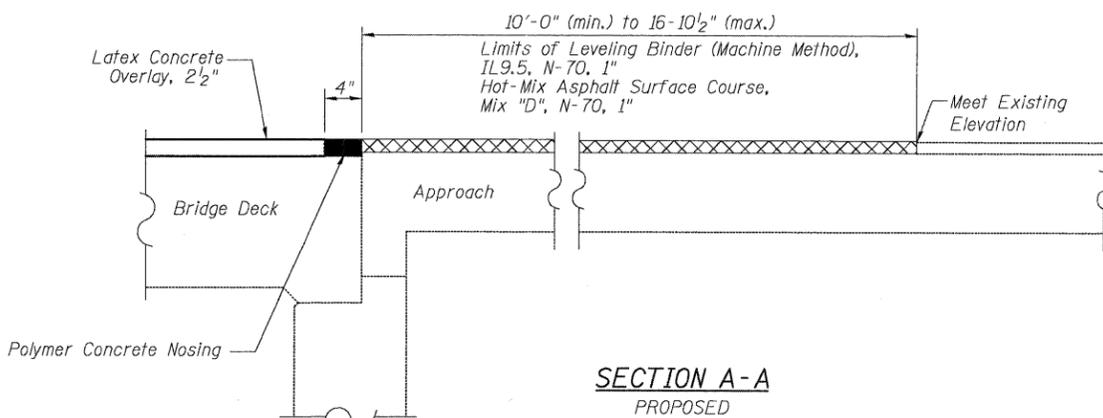
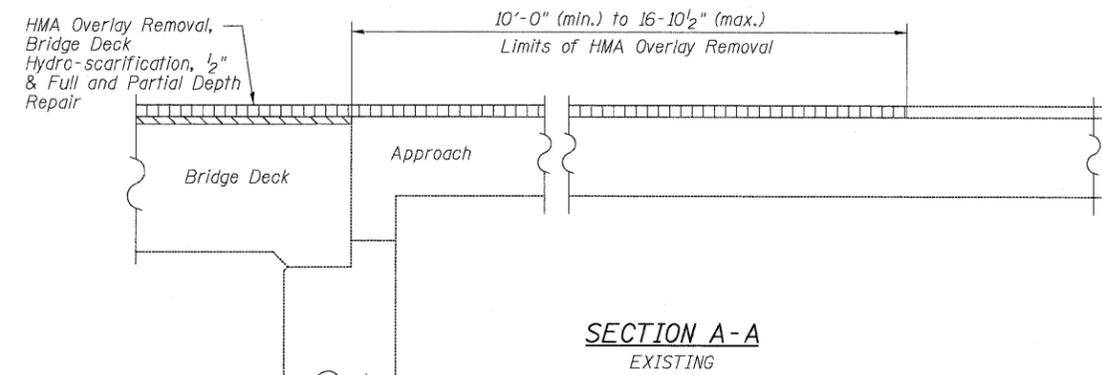
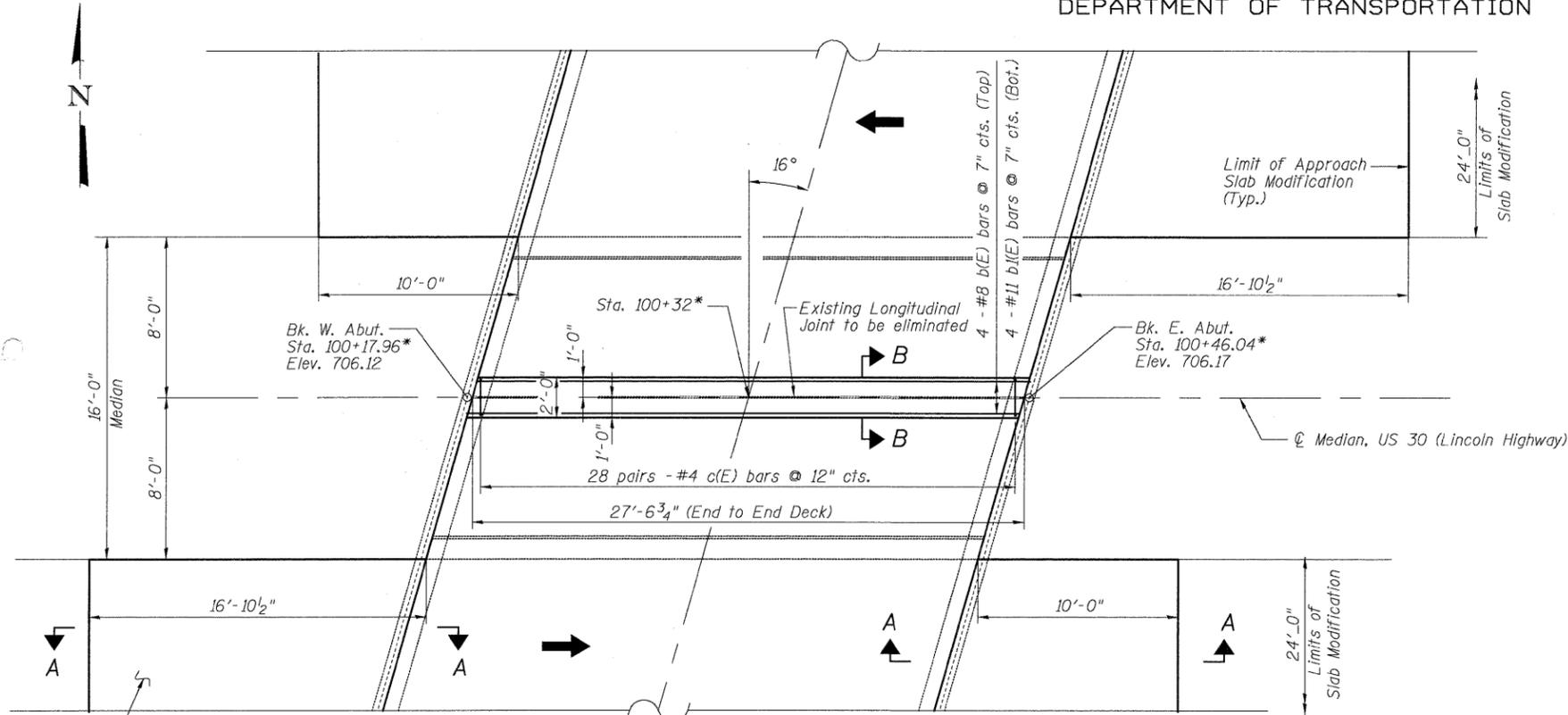
-  Structural Repair of Concrete
(Depth equal to or less than 5 inches)
-  Structural Repair of Concrete
(Depth greater than 5 inches)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	14
Structural Repair of Concrete (Depth greater than 5 inches)	Sq. Ft.	83

SHEET NO. S6	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	353	23 R-B-I	COOK	20	14
S7 SHEETS	CONTRACT NO. 60J43			ILLINOIS FED. AID PROJECT	

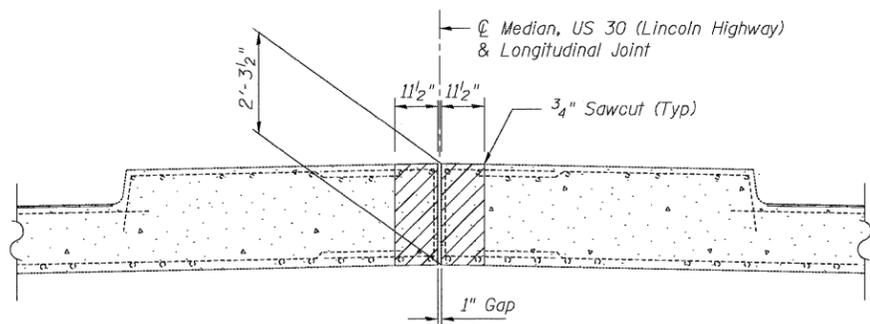
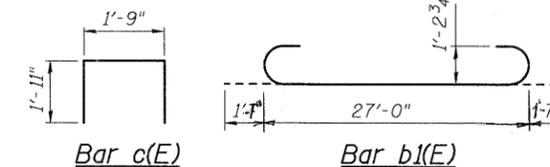
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



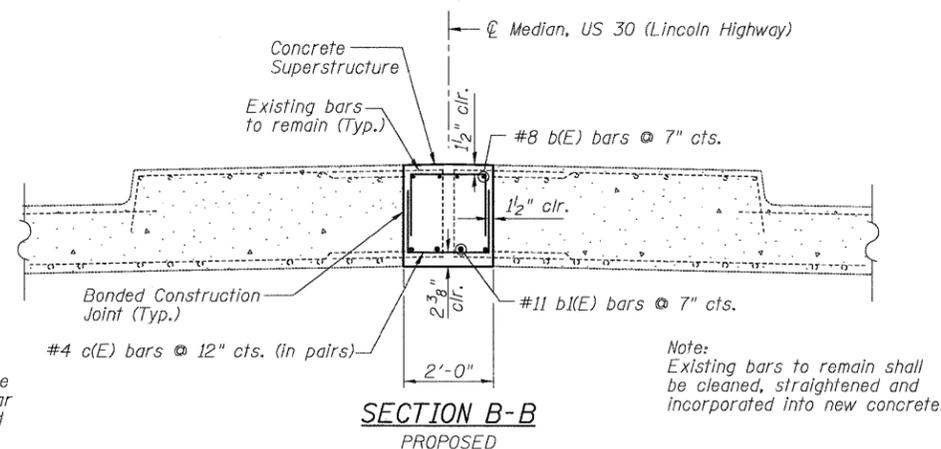
* Information Taken from 1967 Exist. Plans.

BAR LIST

Bar	No.	Size	Length	Shape
b(E)	4	#8	27'-3"	—
b ₁ (E)	4	#11	30'-2"	⌋
c(E)	56	#4	5'-7"	⌋



Note:
Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".



LEGEND:

- Indicates Concrete Removal
- Indicates Leveling Binder (Machine Method), IL9.5, N-70, 1"
Hot-Mix Asphalt Surface Course, Mix "D", N-70, 1 1/2"
- Indicates Hydro-scarification 1/2"
- Indicates Bituminous Concrete Removal

BILL OF MATERIAL

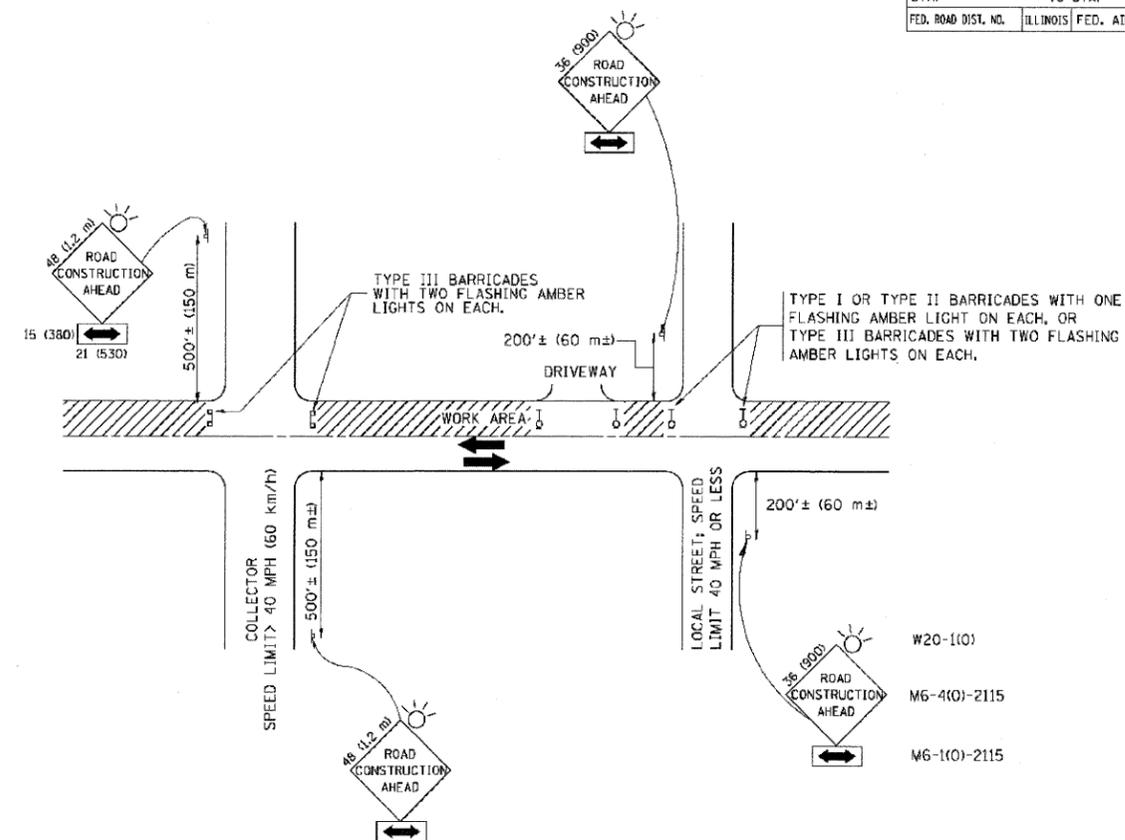
ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	4.7
Reinforcement Bars, Epoxy Coated	Pound	1,140
Concrete Superstructure	Cu. Yd.	4.7
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	154
Hot-Mix Asphalt Surface Removal, 2"	Sq. Yd.	144
Leveling Binder (Machine Method), IL9.5, N-70, 1"	Tons	12
Hot-Mix Asphalt Surface Course, Mix "D", N-70, 1"	Tons	13

APPROACH SLAB MODIFICATION &
MEDIAN JOINT ELIMINATION DETAILS
STRUCTURE NO. 016-0286

DESIGNED JPM
CHECKED TG
DRAWN MPS
CHECKED JPM, TG

Primera
100 S. WACKER DRIVE SUITE 700, CHICAGO IL 60606 P:312-606-0910 F:312-606-0415

SHEET NO. S7	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	353	23 R-B-I	COOK	20	15
S7 SHEETS			CONTRACT NO. 60J43		
ILLINOIS FED. AID PROJECT					



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

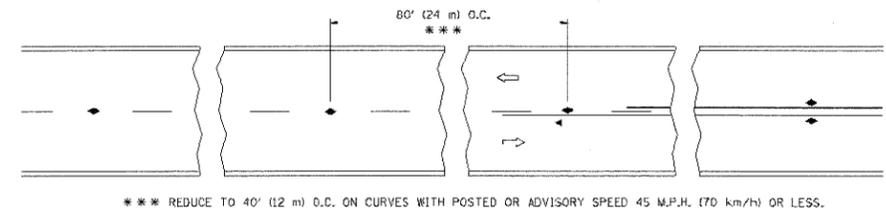
REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL AND PROTECTION
FOR
SIDE ROADS, INTERSECTIONS, AND
DRIVEWAYS

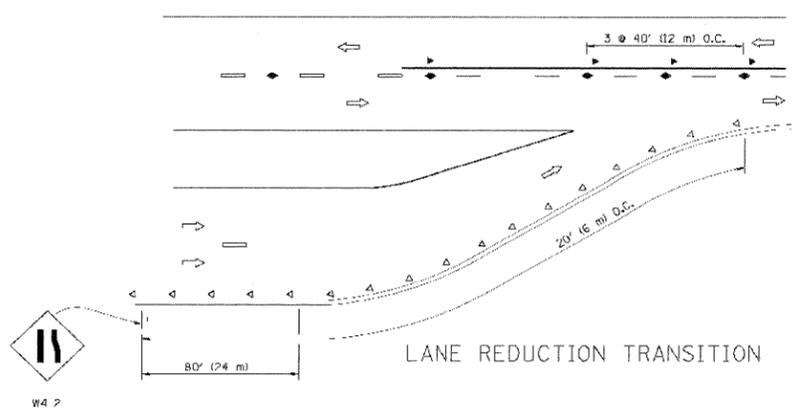
SCALE: NONE

DRAWN BY
CHECKED BY
TC-10

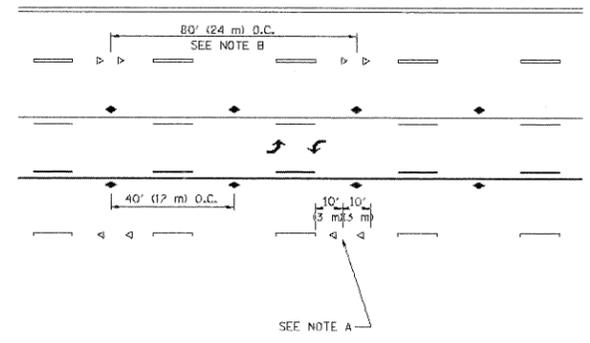
CONTRACT NO.				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SIA.	ID SIA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



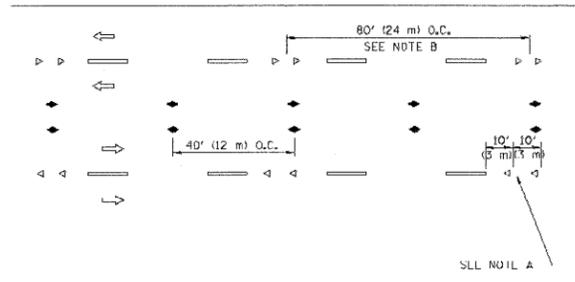
TWO-LANE/TWO-WAY



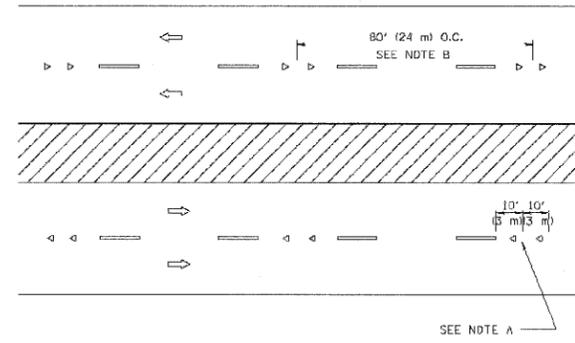
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

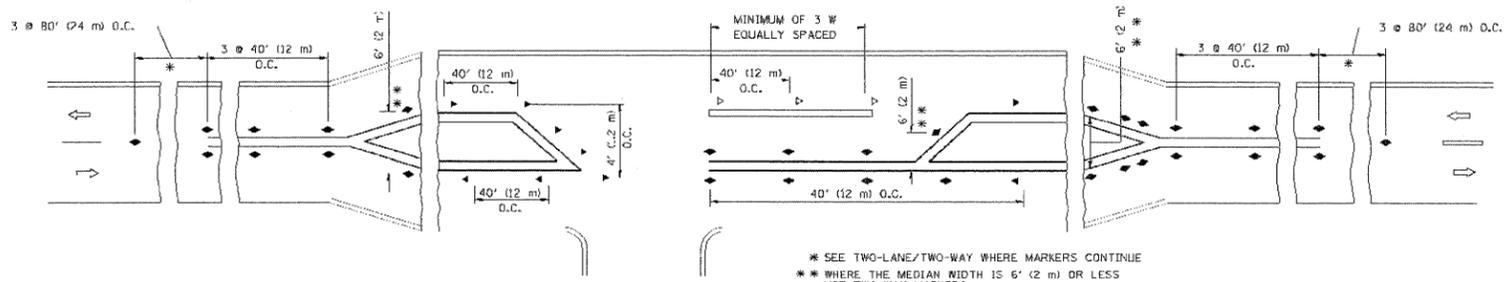
- YELLOW STRIPE
- WHITE STRIP
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND RAMPWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

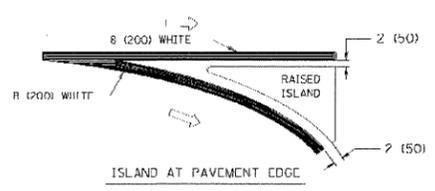
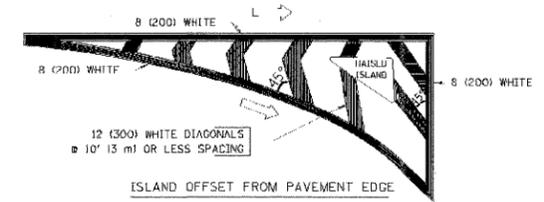
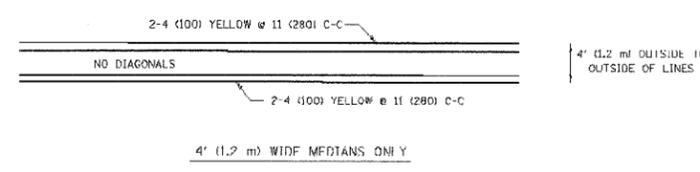
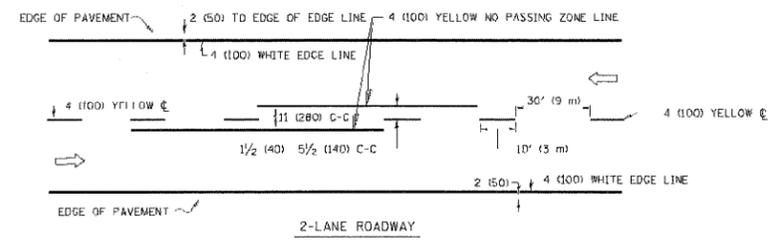
REVISIONS	NAME	DATE
T. RAMMAGHER		09-19-94
T. RAMMAGHER		03-12-99
T. RAMMAGHER		01-06-00
C. JUCIUS		09-09-09

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT
 MARKERS (SNOW-PLOW RESISTANT)

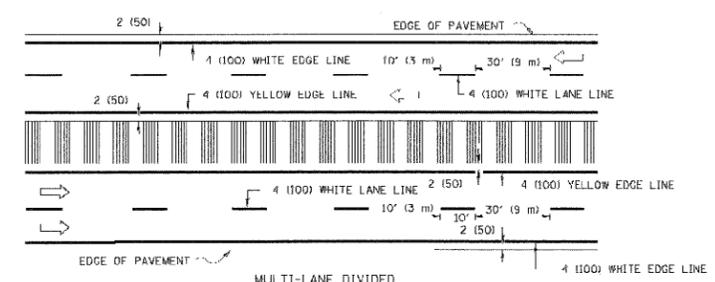
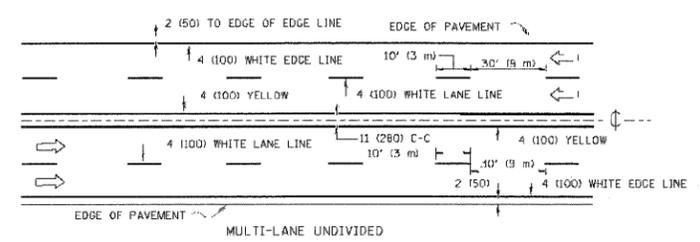
SCALE: NONE

DRAWN BY CAUD
 CHECKED BY
 TC-11

USER MAKE = DRIVEMARKER

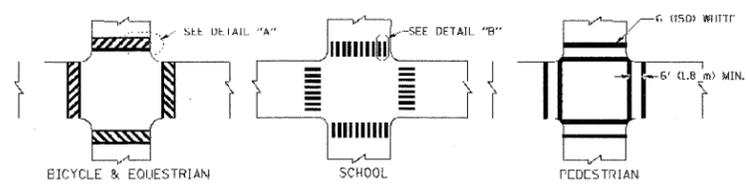


TYPICAL ISLAND MARKING

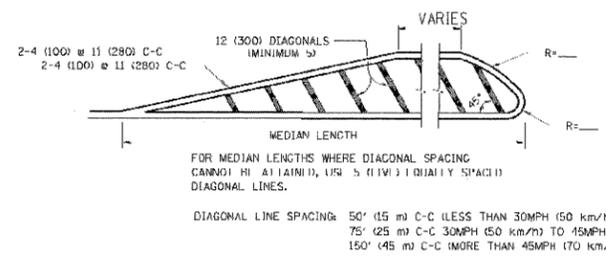


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE.

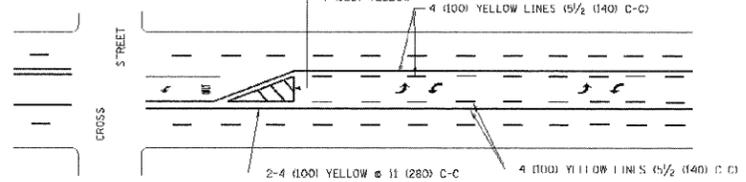
TYPICAL LANE AND EDGE LINE MARKING



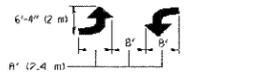
TYPICAL CROSSWALK MARKING



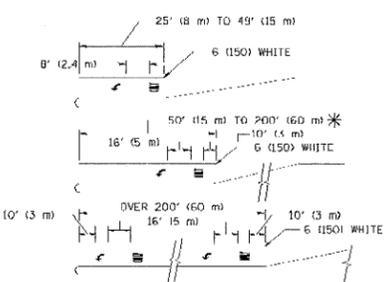
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * AREA = 15.6 SQ. FT. (4.5 m²) ONLY AREA = 20.8 SQ. FT. (4.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB.
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL))	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	2 @ 6 (150)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 15 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "RR" - 3.6 SQ. FT. (0.33 m ²) EACH "X" - 54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

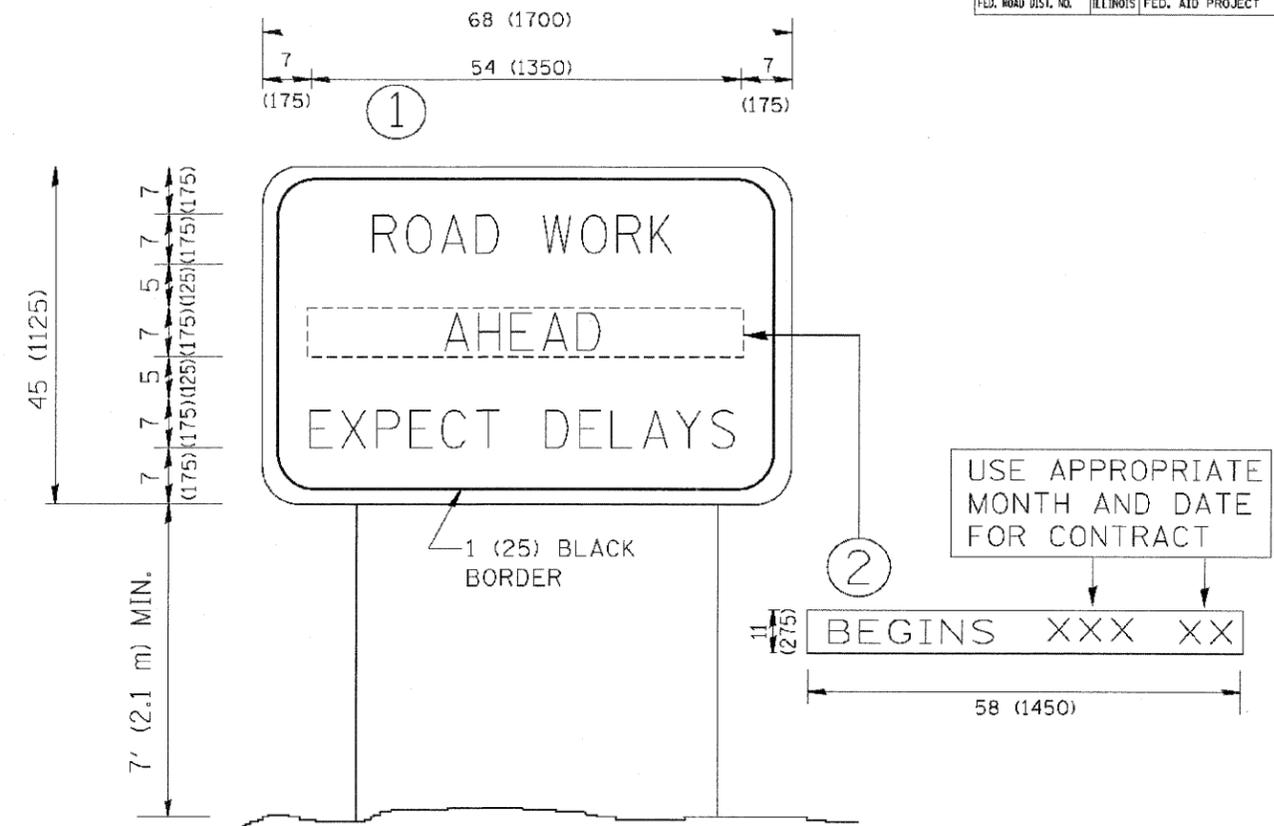
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS NAME	DATE
EVERS	03-19-90
T. HAMMACHEK	10-27-94
C. JUICIUS	09-09-09

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT ONE TYPICAL PAVEMENT MARKINGS
 SCALE: NONE
 DRAWN BY CADD
 CHECKED BY
 TC-13

CONTRACT NO.				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



NOTES:

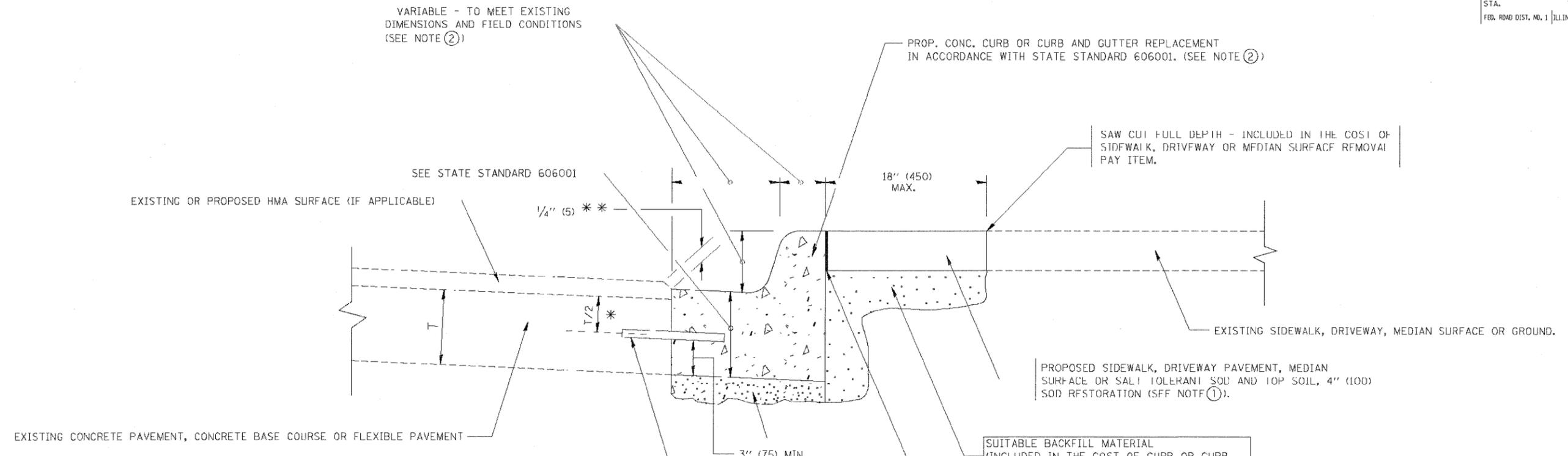
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS		DATE	ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME			
R. WIRS		9-15-97	ARTERIAL ROAD INFORMATION SIGN
R. WIRS		12-11-97	
T. RAMMACHER		2-2-99	
C. JUCIUS		1-31-07	

SCALE: NONE

DRAWN BY DESIGN
CHECKED BY
TC22



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
H. SHAH	09/12/96
R. SHAH	09/19/96
H. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	12/15/09

ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: VERT. NONE
HORIZ. NONE

DRAWN BY
CHECKED BY
BD600-06 (BD-24)

FILE NAME =
#FILEL#



DESIGNED	VEA	REVISED	-
DRAWN	VEA	REVISED	-
CHECKED	TWL	REVISED	-
DATE	1/29/2010	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE STANDARD DETAILS				
BD-24				
SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	23R B-1	COOK	20	20
CONTRACT NO. 60J43				
ILLINOIS FED. AID PROJECT				