

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

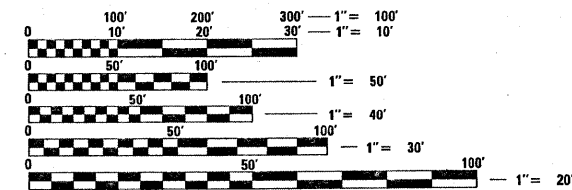
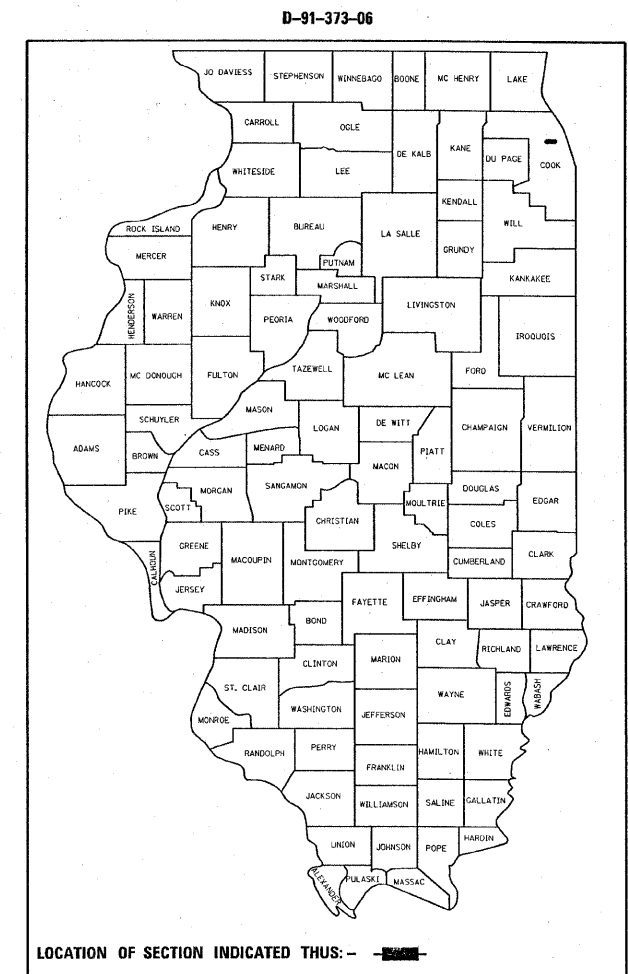
**PROPOSED  
HIGHWAY PLANS**  
F.A.U. 1366 /MONTROSE AVE.  
FOREST PRESERVE RD. TO PULASKI RD.  
RESURFACING (3P)  
SECTION: 0101RS-5

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1366	0101RS-5	COOK	34	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60B60		

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE CITIES OF  
CHICAGO AND HARWOOD HEIGHTS

**COOK COUNTY  
C-91-373-06**

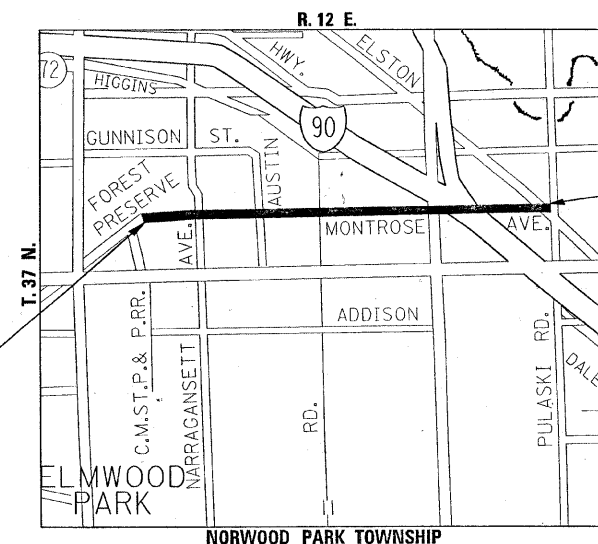


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: JENPAI CHANG (847) 705-4432  
PROJECT MANAGER: KEN ENG (847) 705-4247

CONTRACT NO. 60B60



PROJECT BEGINS  
STA. 14+12

PROJECT ENDS  
STA. 193+50

TRAFFIC DATA:  
2005 ADT - 20300  
SPEED LIMIT - 30 TO 35 MPH

OMISSIONS:  
N. AUSTIN AVE TO N. CENTRAL AVE.  
STA. 59+49 TO STA. 86+96  
N. LA CROSSE AVE. TO N. CICERO AVE.  
STA. 136+26 TO STA. 142+01  
METRRARAIL  
STA. 142+01 TO STA. 143+94  
UNION PACIFIC RAILROAD/NORTHWEST LINE  
STA. 146+86 TO STA. 149+68  
I-90/94 (KENNEDY EXPY.)  
STA. 150+27 TO STA. 155+43

GROSS LENGTH OF PROJECT = 17938 LIN FT = 3.40 MILES  
NET LENGTH OF PROJECT = 13622 LIN FT = 2.58 MILES

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED MARCH 20, 2009

*Diana M. O'Keefe*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 1, 2009  
*Charles J. Ingersoll*  
ENGINEER OF DESIGN AND ENVIRONMENT

May 1, 2009  
*Christine M. Reed*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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STATE STANDARDS

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 442201-03 CLASS C AND D PATCHES
- 604011-04 FRAME AND LIDS, TYPE 1
- 606001-04 CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER
- 701601-06 LANE CLOSURE, MULTILANE, 1-W OR 2-W, WITH NON TRANSVERSABLE MEDIUM
- 701606-06 URBAN LANE CLOSURE, MULTILANE 2-W WITH MOUNTABLE MEDIUM
- 701701-06 LANE CLOSURE, MULTILANE, INTERSECTION, FOR SPEEDS < 45 MPH
- 701901-01 TRAFFIC CONTROL DEVICES
- 780001-02 TYPICAL PAVEMENT MARKINGS
- 781001-03 TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS
- 886001-01 DETECTOR LOOP INSTALLATIONS
- 886006-01 TYPICAL LAYOUT FOR DETECTION LOOPS

GENERAL NOTES - CITY OF CHICAGO

- ALL CATCH BASINS IN THE CITY OF CHICAGO MUST MEET THE DEPARTMENT OF SEWER STANDARDS.
- IN CASE OF DAMAGE TO THE CITY OF CHICAGO SEWER, PRIVATE AND PUBLIC DRAINS, SEWER STRUCTURES AND/OR BENCH MONUMENTS, THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE DEPARTMENT OF SEWERS AT 312-747-7892 OR 312-747-7893.
- BENCH MONUMENT LOCATIONS WITHIN THE LIMITS OF THE IMPROVEMENT CAN BE OBTAINED FROM THE DEPARTMENT OF SEWERS AT 333 SOUTH STATE STREET, SUITE 410, CHICAGO, IL 60604-3971. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF REPLACING ANY BENCH MONUMENT DAMAGED OR DESTROYED DURING CONSTRUCTION.
- ALL BROKEN, CRACKED, WORN OR OTHERWISE DAMAGED OR BICYCLE UNSAFE FRAMES AND GRATES OR LIDS ON SEWER STRUCTURES SHALL BE REPLACED WITH NEW DEPARTMENT OF SEWERS' STANDARD FRAMES AND GRATES OR LIDS. OLD FRAMES AND GRATES OR LIDS SHALL BE DELIVERED TO THE DEPARTMENT OF SEWERS AT 39TH STREET AND ASHLAND AVE.
- CITY OF CHICAGO WATER VALVE VAULTS AND SEWER STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION WITHOUT WRITTEN PERMISSION FROM THE CITY OF CHICAGO DEPARTMENT OF WATER AND/OR DEPARTMENT OF SEWERS.
- CURB AND GUTTER CONSTRUCTION SHALL PROVIDE A MINIMUM CURB HEIGHT OF 75MM (3").
- PAVEMENT REPLACEMENT AROUND FRAMES AND GRATES OR LIDS WHERE DRAINAGE, WATER MAIN OR ELECTRICAL STRUCTURES ARE ADJUSTED OR RECONSTRUCTED SHALL BE WITH CLASS SI CONCRETE.

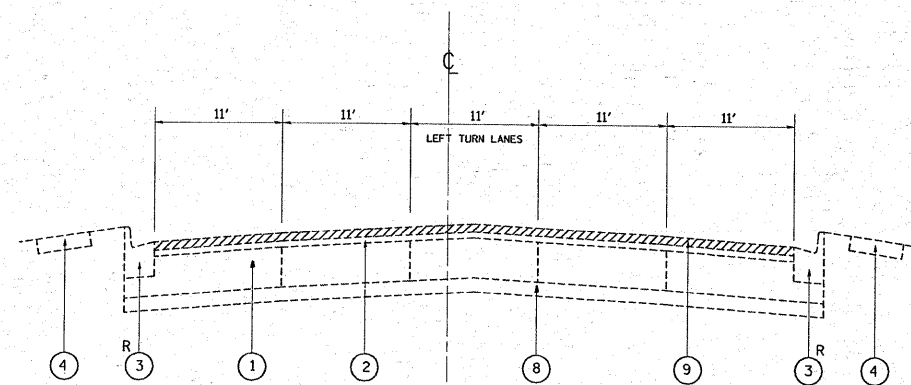
GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).
- 10 FEET (3 METERS) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIUM ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES AND THE CITY OF CHICAGO AND THE CITY OF HARWOOD HEIGHTS.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- THE CONTRACTOR SHALL CONTRACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR CORY JUCCIUS AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO START OF WORK.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H)
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-24)
- RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS" DETAIL.
- THE RESIDENT ENGINEER SHOULD CONTACT MR. WALLY CZARNY, AREA TRAFFIC ENGINEER, AT (773) 685-4342 PRIOR TO PLACING ANY PAVEMENT MARKINGS.

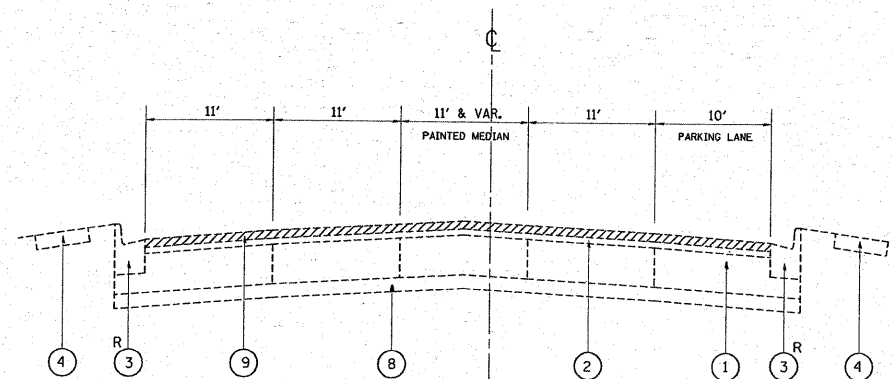
FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES MONTROSE AVE.--FOREST PRESERVE RD. TO PULASKI RD.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pw_work\pwi\dot\steedpa\0100515\0137306\shp\plan.dgn		DRAWN -	REVISED -			1366	0101RS-5	COOK	33	2	
	PLOT SCALE = 50,0000' / 1" IN.	CHECKED -	REVISED -			CONTRACT NO. 60B60					
	PLOT DATE = 3/25/2009	DATE -	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT		TOTAL QUANTITIES						CODE NO	ITEM		UNIT	TOTAL QUANTITIES	1000	1000	1000		
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	30	25	5				70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	1508	1508						
40600300	AGGREGATE (PRIME COAT)	TON	150	124	26				70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	379	379						
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	23	19	4				70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	940	940						
40600895	CONSTRUCTING TEST STRIP	EACH	1	0.5	0.5				70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	10970	10970						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	1250	1250					*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	337	337						
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	6248	5184	1064				*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9053	9053						
42001300	PROTECTIVE COAT	SO YD	2062	1649		413			*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	7572	7572						
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	74380	61715	12665				*78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	1508	1508						
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	5577	4461		1116			*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	379	379						
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SO YD	765	573		192			*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	940	940						
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SO YD	1132	849		283			78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	377	377						
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SO YD	1315	986		329			78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	320	320						
55039700	STORM SEWERS TO BE CLEANED	FOOT	1500	1500					*88600600	DETECTOR LOOP REPLACEMENT	FOOT	1045	1045						
60250200	CATCH BASINS TO BE ADJUSTED	EACH	68	68					X0322256	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4						
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	15	15					X0656100	DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT	SO YD	207	207						
60255500	MANHOLES TO BE ADJUSTED	EACH	3	3					X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2929	2430	499					
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	15	15					<del>44004600</del>	SIDEWALK REMOVAL AND REPLACEMENT	SO FT	8787	8787						
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	40	40					XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SO YD	85	85						
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	129	129					Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	330			330				
60404800	FRAMES AND GRATES, TYPE 11	EACH	1	1															
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	38	38															
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	21	21															
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3															
67100100	MOBILIZATION	L SUM	1	1															
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	0.5		0.5													
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	0.5		0.5													
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.5		0.5													
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	5070	5070															
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	337	337															
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	9053	9053															
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	7572	7572															

\* SPECIALTY ITEMS



EXISTING TYPICAL CROSS SECTION  
STA. 14+12 TO 17+46

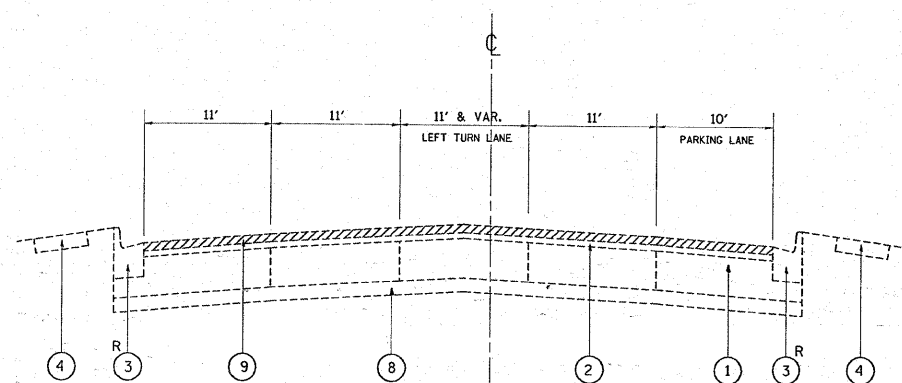


EXISTING TYPICAL CROSS SECTION  
STA. 17+46 TO 23+74  
STA. 24+69 TO 29+32  
STA. 30+11 TO 31+74

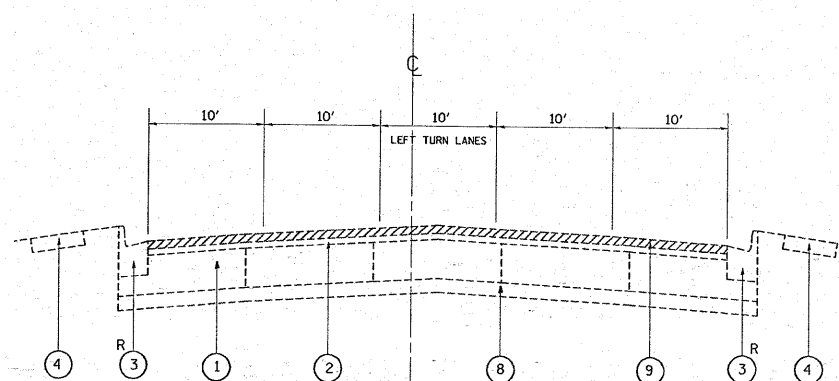
**LEGEND:**

- ① EXIST. P.C.C. PAVEMENT, ±10"
- ② EXIST. HOT-MIX ASPHALT SURFACE, ±3"
- ③ EXIST. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
- ④ EXIST. P.C.C. SIDEWALK
- ⑤ EXIST. P.C.C. SHOULDER, 10'
- ⑥ EXIST. CORRUGATED MEDIAN
- ⑦ EXIST. BARRIER MEDIAN
- ⑧ EXIST. STABILIZED SUB-BASE
- ⑨ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑩ PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50, 3/4"
- ⑪ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

R CURB AND GUTTER REMOVAL AND REPLACEMENT  
(LOCATION AS DIRECTED BY THE ENGINEER)



EXISTING TYPICAL CROSS SECTION  
STA. 23+74 TO 24+69  
STA. 29+32 TO 29+67  
STA. 31+74 TO 32+79



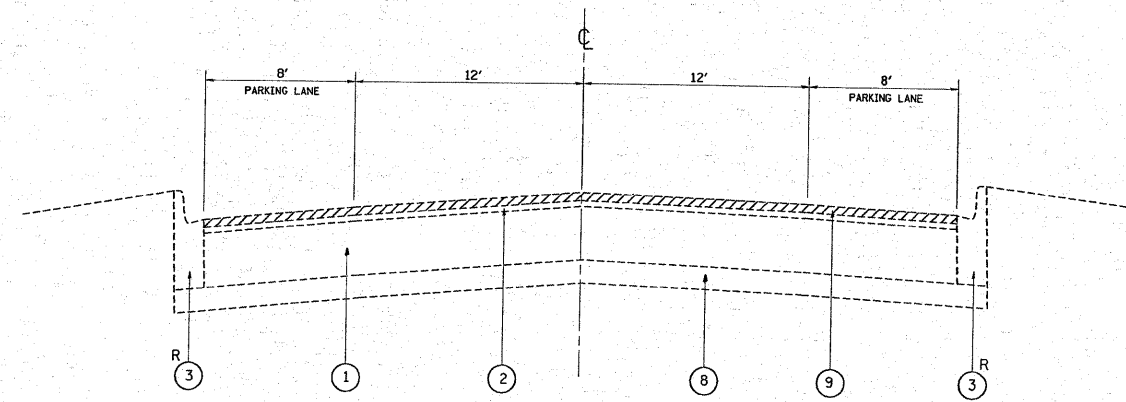
EXISTING TYPICAL CROSS SECTION  
STA. 32+79 TO 35+69  
STA. 57+46 TO 62+39  
STA. 84+67 TO 88+32  
STA. 136+79 TO 142+47  
STA. 191+69 TO 193+50

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		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED TYPICAL SECTIONS			
MONTROSE AVE.--FOREST PRESERVE RD. TO PULASKI RD.			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1366	0101RS-5	COOK	34	4
CONTRACT NO. 60B60				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

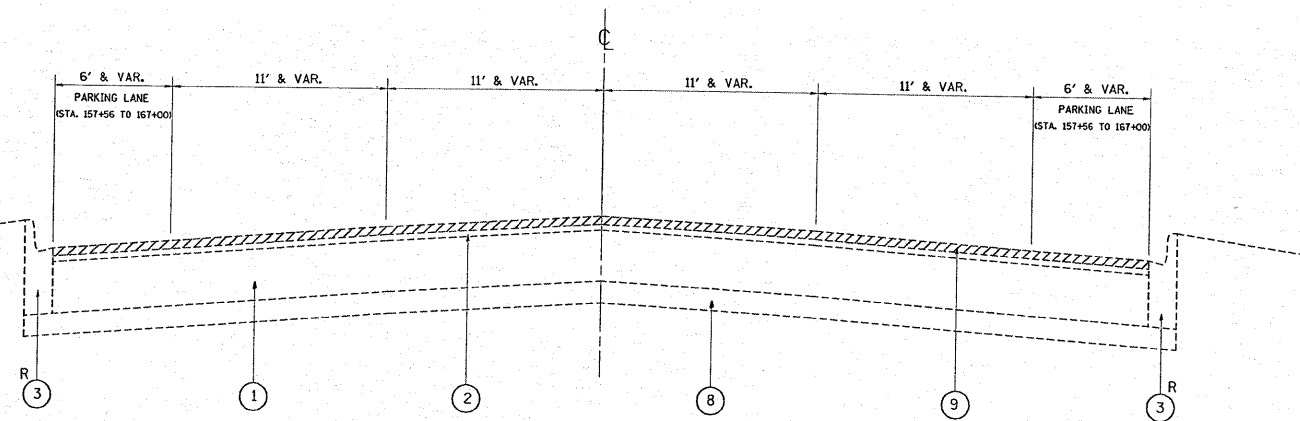


EXISTING TYPICAL CROSS SECTION

STA. 35+69 TO 57+46  
 STA. 62+39 TO 84+67  
 STA. 88+32 TO 136+79  
 STA. 167+00 TO 191+69

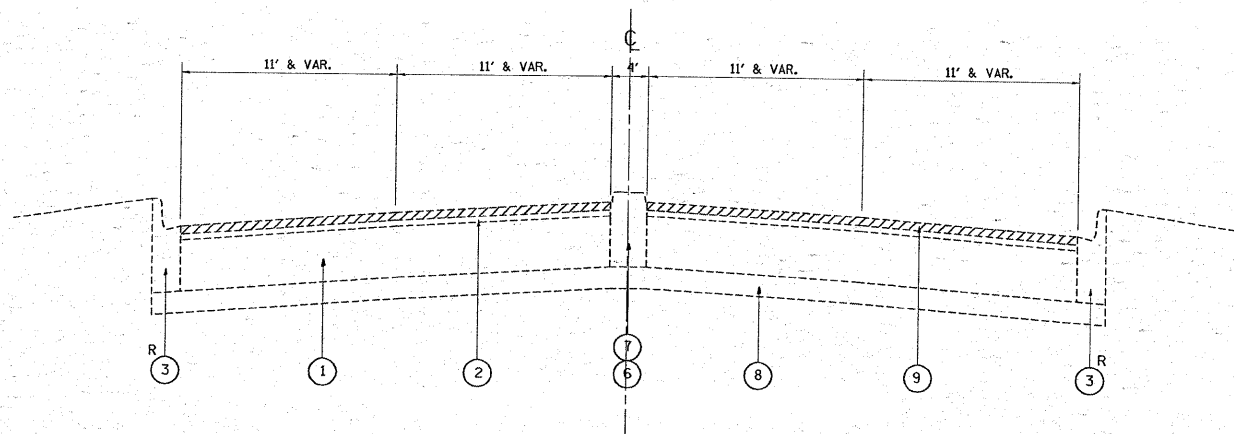
**LEGEND:**

- ① EXIST. P.C.C. PAVEMENT, ±9"
  - ② EXIST. HOT-MIX ASPHALT SURFACE, ±3"
  - ③ EXIST. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
  - ④ EXIST. P.C.C. SIDEWALK
  - ⑤ EXIST. P.C.C. SHOULDER, 10'
  - ⑥ EXIST. CORRUGATED MEDIAN
  - ⑦ EXIST. BARRIER MEDIAN
  - ⑧ EXIST. STABILIZED SUB-BASE
  - ⑨ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
  - ⑩ PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50, 3/4"
  - ⑪ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- R CURB AND GUTTER AND SIDEWALK REMOVAL AND REPLACEMENT  
 (LOCATION AS DIRECTED BY THE ENGINEER)



EXISTING TYPICAL CROSS SECTION

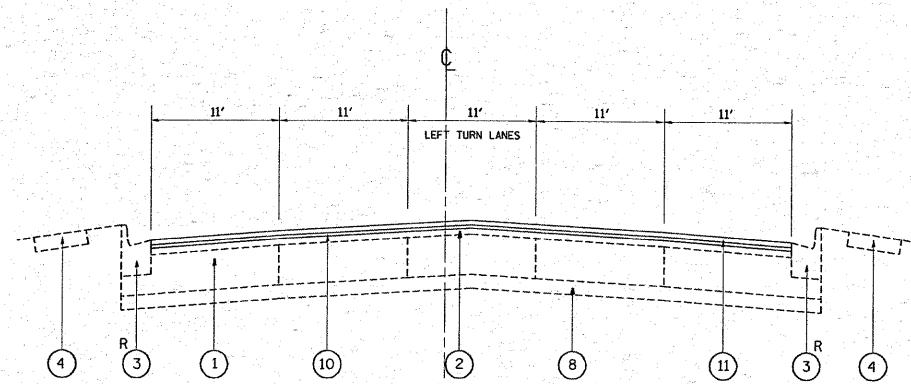
STA. 142+47 TO 146+25  
 STA. 157+56 TO 167+00



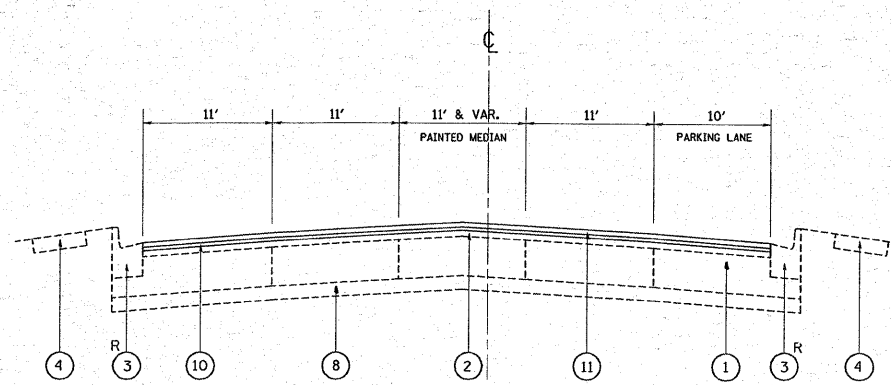
EXISTING TYPICAL CROSS SECTION

STA. 146+25 TO 157+56

FILE NAME =	USER NAME = steepps	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	EXISTING AND PROPOSED TYPICAL SECTIONS MONTROSE AVE.--FOREST PRESERVE RD. TO PULASKI RD.		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
oz:\pwork\pndot\steepps\d0100515\0137306-sh-t-plen.dgn	PLOT SCALE = 50,0000' / IN.	DRAWN -	REVISED -				1366	0101RS-5	COOK	34	5
	PLOT DATE = 3/25/2009	CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60B60		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
		DATE -	REVISED -								



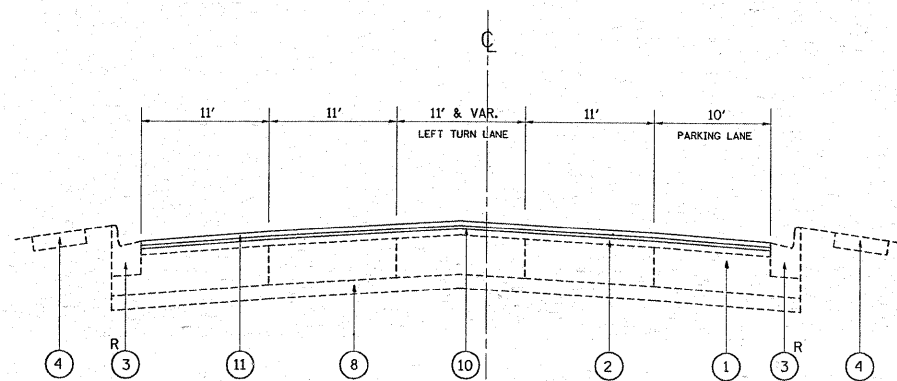
PROPOSED TYPICAL CROSS SECTION  
STA. 14+12 TO 17+46



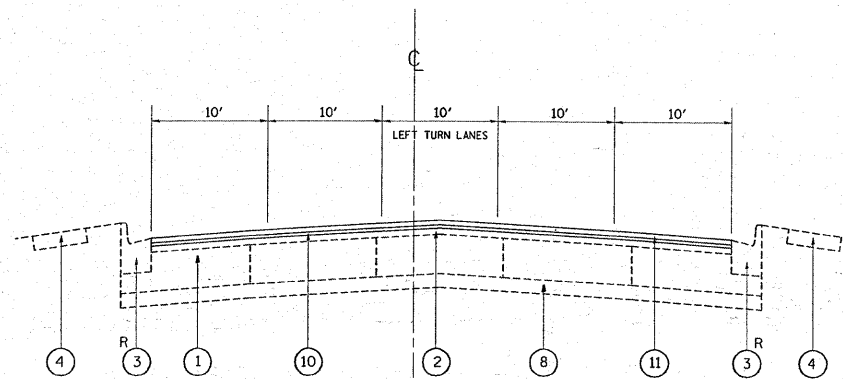
PROPOSED TYPICAL CROSS SECTION  
STA. 17+46 TO 23+74  
STA. 24+69 TO 29+32  
STA. 30+11 TO 31+74

**LEGEND:**

- ① EXIST. P.C.C. PAVEMENT, ±9"
  - ② EXIST. HOT-MIX ASPHALT SURFACE, ±3"
  - ③ EXIST. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
  - ④ EXIST. P.C.C. SIDEWALK
  - ⑤ EXIST. P.C.C. SHOULDER, 10'
  - ⑥ EXIST. CORRUGATED MEDIAN
  - ⑦ EXIST. BARRIER MEDIAN
  - ⑧ EXIST. STABILIZED SUB-BASE
  - ⑨ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
  - ⑩ PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50, 3/4"
  - ⑪ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- R CURB AND GUTTER AND SIDEWALK REMOVAL AND REPLACEMENT  
(LOCATION AS DIRECTED BY THE ENGINEER)



PROPOSED TYPICAL CROSS SECTION  
STA. 23+74 TO 24+69  
STA. 29+32 TO 29+67  
STA. 31+74 TO 32+79



PROPOSED TYPICAL CROSS SECTION  
STA. 32+79 TO 35+69  
STA. 57+46 TO 62+39  
STA. 84+67 TO 88+32  
STA. 136+79 TO 142+47  
STA. 191+69 TO 193+50

**MIXTURE REQUIREMENTS**

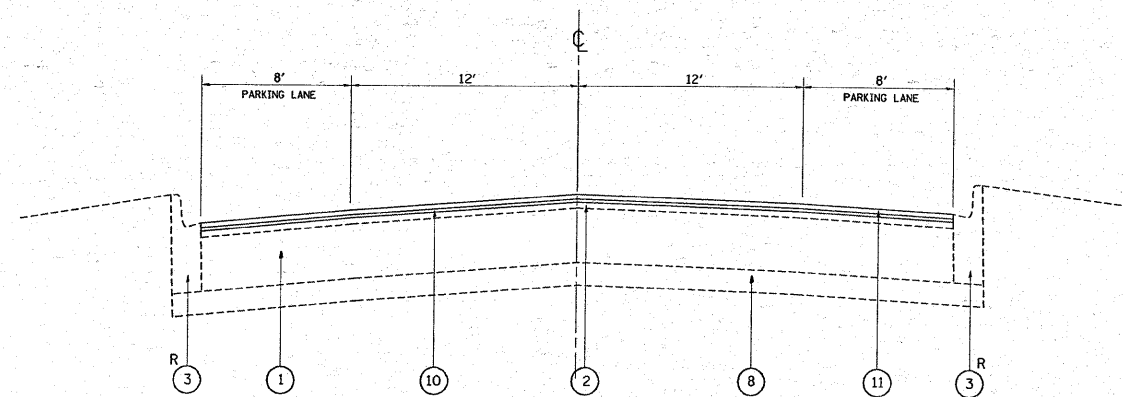
MIXTURE USE	AC/PG	DESIGN AIR VOIDS
CLASS "D" PATCHES, 13" HMA BINDER COURSE, IL-19MM *	PG 64 -22*	4% @ 70
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	SBS/SBR PG 76 -28/ -22	4% @ 50
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, IL-9.5MM	PG 64 -22	4% @ 70

**NOTE:**

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE COURSE QUANTITIES IS 112 LBS./SQ. YD./ IN.

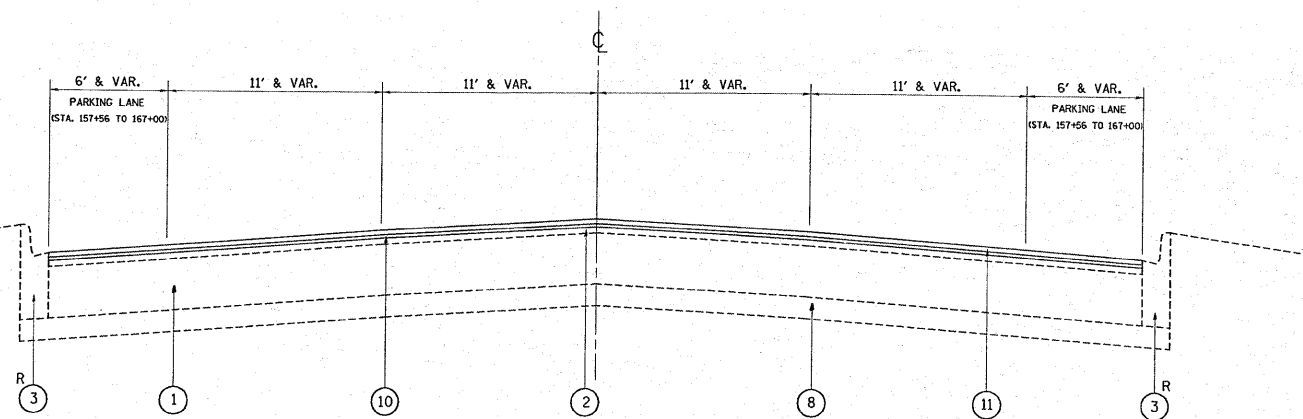
THE MILLING SHALL BE DONE PRIOR TO PATCHING

\* WHEN RAP EXCEEDS 20% THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22



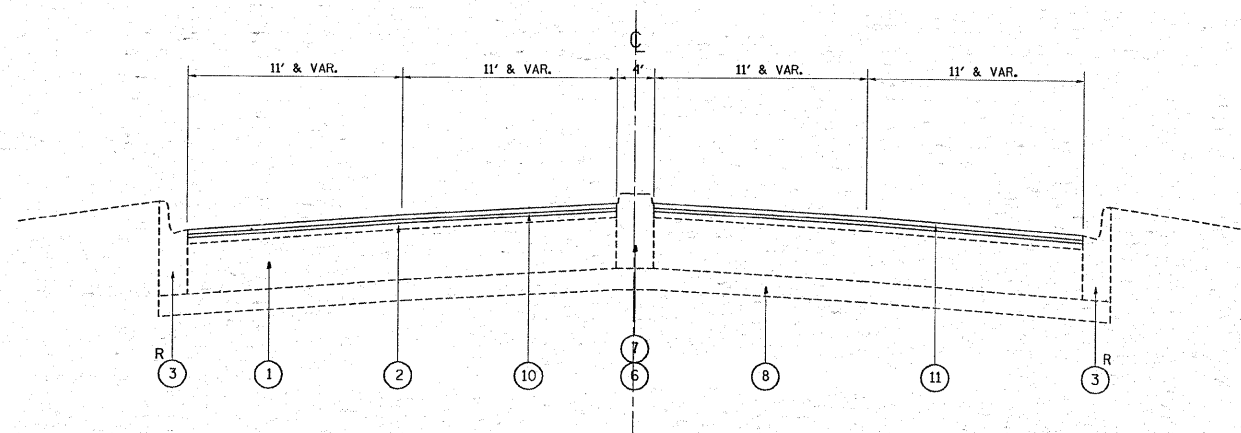
PROPOSED TYPICAL CROSS SECTION

STA. 35+69 TO 57+46  
 STA. 62+39 TO 84+67  
 STA. 88+32 TO 136+79  
 STA. 167+00 TO 191+69



PROPOSED TYPICAL CROSS SECTION

STA. 142+47 TO 146+25  
 STA. 157+56 TO 167+00



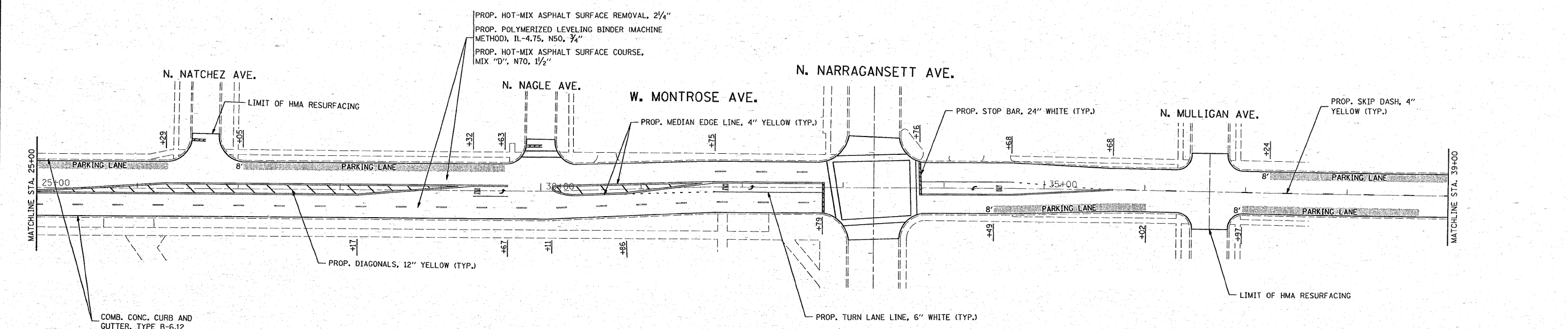
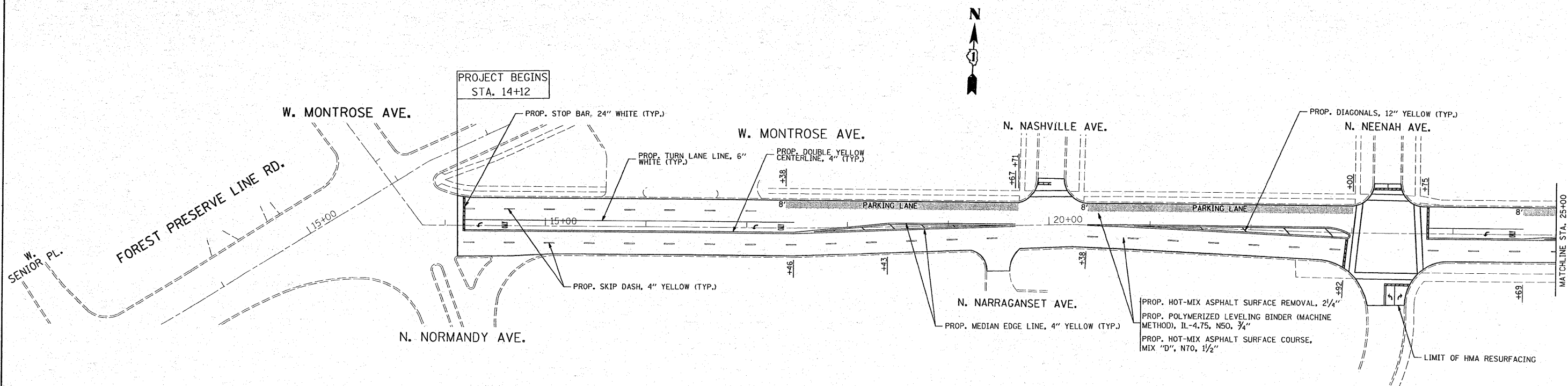
PROPOSED TYPICAL CROSS SECTION

STA. 146+25 TO 157+56

**LEGEND:**

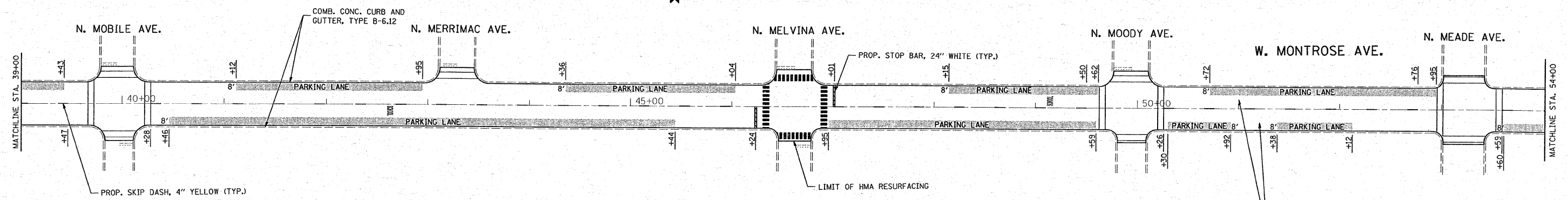
- ① EXIST. P.C.C. PAVEMENT, ±9"
  - ② EXIST. HOT-MIX ASPHALT SURFACE, ±3"
  - ③ EXIST. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
  - ④ EXIST. P.C.C. SIDEWALK
  - ⑤ EXIST. P.C.C. SHOULDER, 10'
  - ⑥ EXIST. CORRUGATED MEDIAN
  - ⑦ EXIST. BARRIER MEDIAN
  - ⑧ EXIST. STABILIZED SUB-BASE
  - ⑨ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
  - ⑩ PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50, 3/4"
  - ⑪ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- R CURB AND GUTTER AND SIDEWALK REMOVAL AND REPLACEMENT  
 (LOCATION AS DIRECTED BY THE ENGINEER)

FILE NAME =	USER NAME = s teedpa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	EXISTING AND PROPOSED TYPICAL SECTIONS MONTROSE AVE.--FOREST PRESERVE RD. TO PULASKI RD.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\stedpa\d0100515\0137306-sh1-plan.dgn	DRAWN -	REVISED -	1366			0101RS-5	COOK	34	7	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60B60							
PLOT DATE = 3/25/2009	DATE -	REVISED -	FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT							
					SCALE:	SHEET NO. OF SHEETS STA. TO STA.				



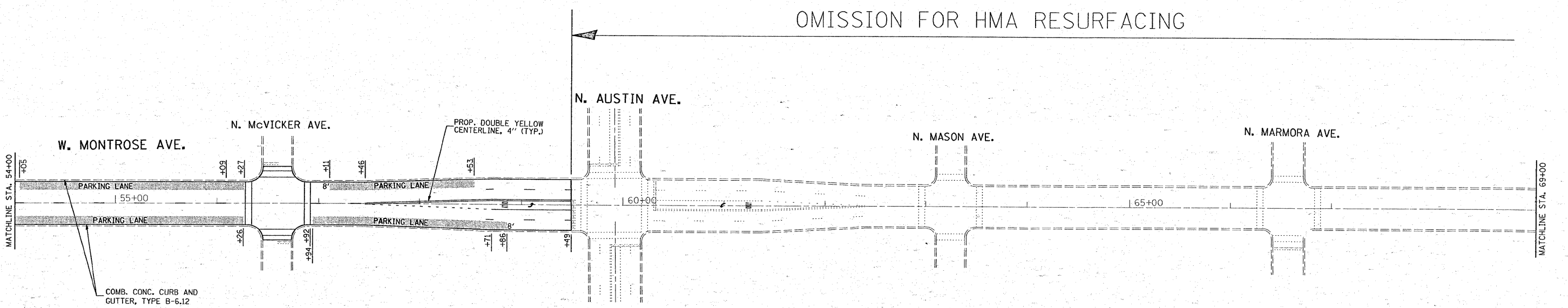
FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	ROADWAY AND PAVEMENT MARKING PLAN MONTROSE AVE.--FOREST PRESERVE RD. TO PULASKI RD.		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw\work\p\dot\steedpa\d\0100515\0137306-sh-t-plan.dgn	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -		SCALE: 1"=50'	SHEET NO. OF SHEETS	STA. TO STA.	1366	0101 RS-5	COOK	34	8
	PLOT DATE = 3/26/2009	CHECKED -	REVISED -					CONTRACT NO. 60B60				
		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							





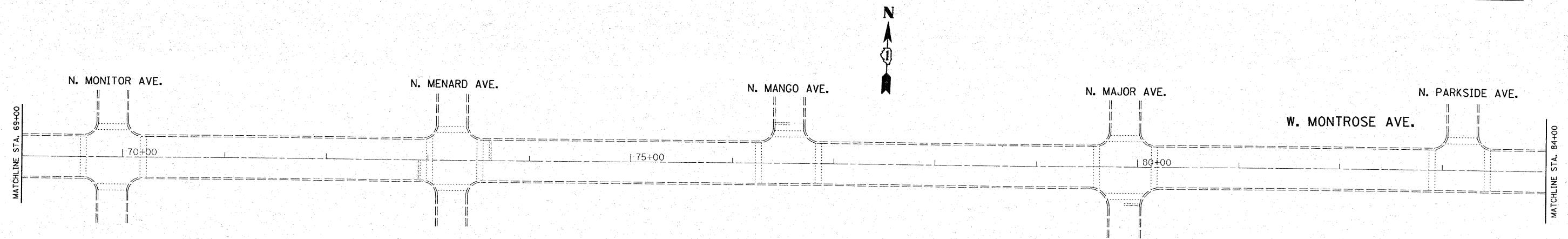
PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

OMISSION FOR HMA RESURFACING

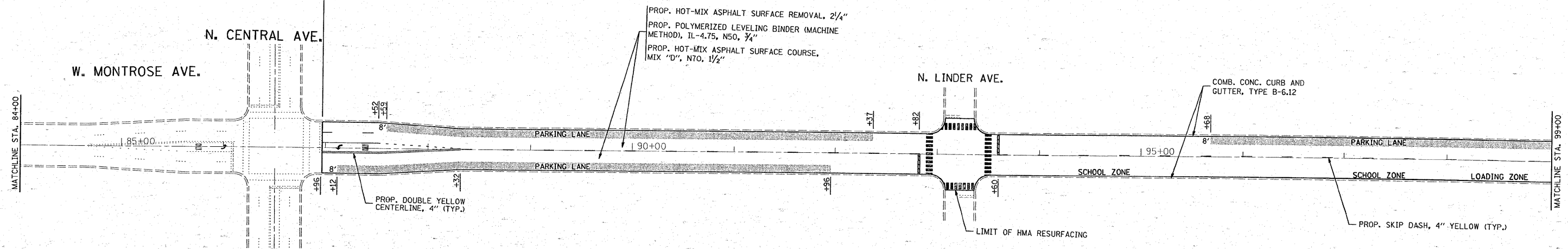


FILE NAME =	USER NAME = s teedpa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	ROADWAY AND PAVEMENT MARKING PLAN MONTROSE AVE.--FOREST PRESERVE RD. TO PULASKI RD.			F.A.I. RTE. 1366	SECTION 0101 RS-5	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 9
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		DATE -	REVISED -									

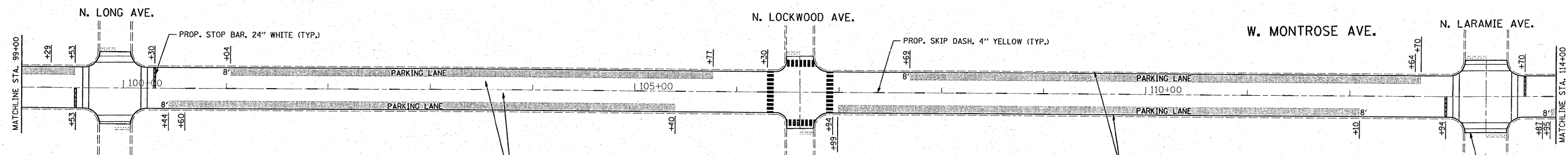
# OMISSION FOR HMA RESURFACING



## OMISSION FOR HMA RESURFACING



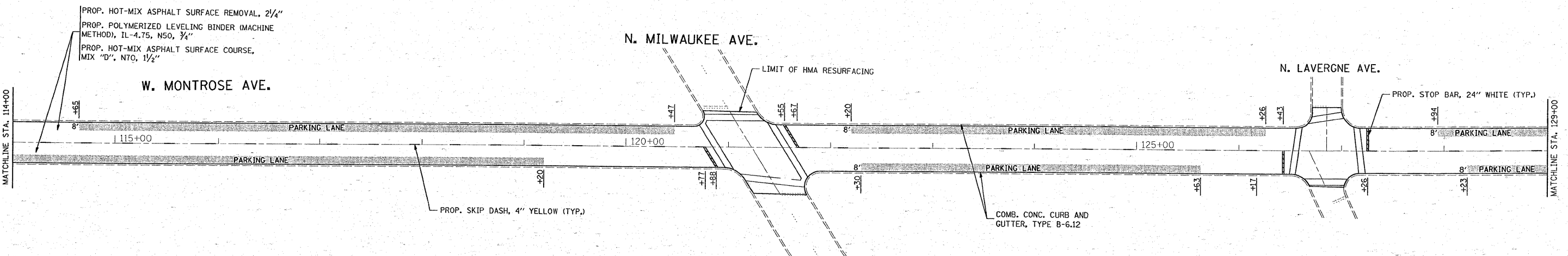
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PLOT SCALE = 50,0000 "/ IN.		CHECKED -	REVISED -								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
PLOT DATE = 3/26/2009		DATE -	REVISED -										



PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

COMB. CONC. CURB AND GUTTER, TYPE B-6.12

LIMIT OF HMA RESURFACING

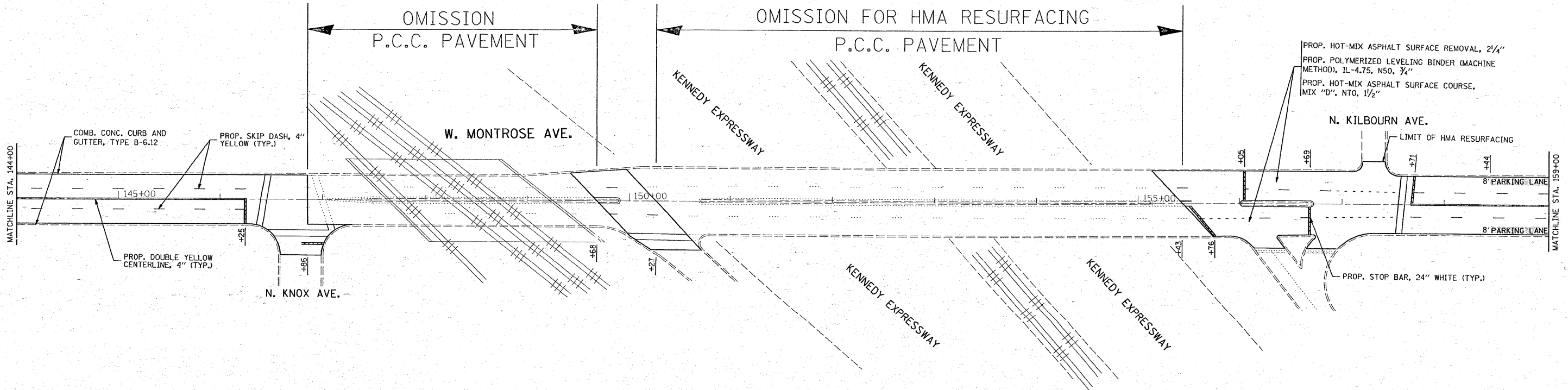
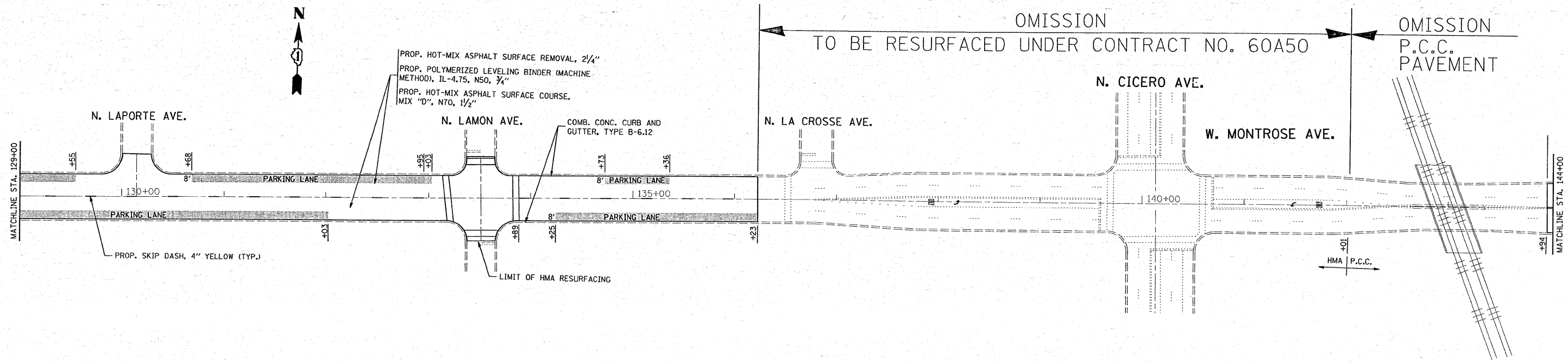


PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

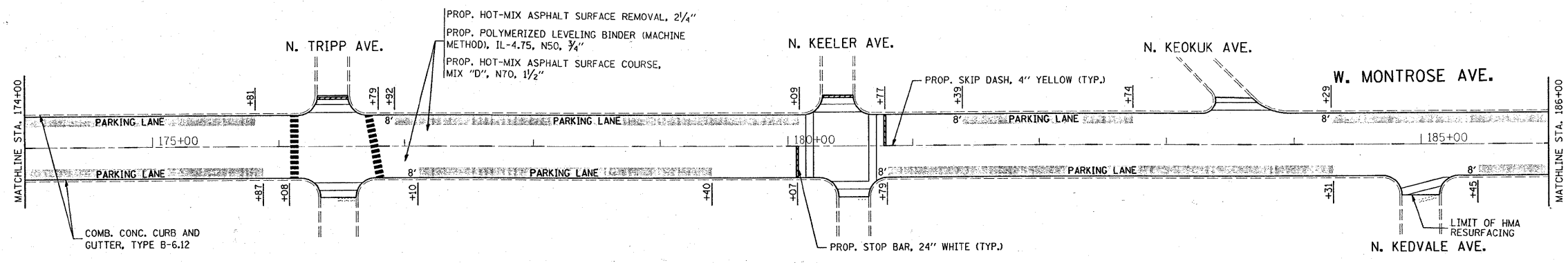
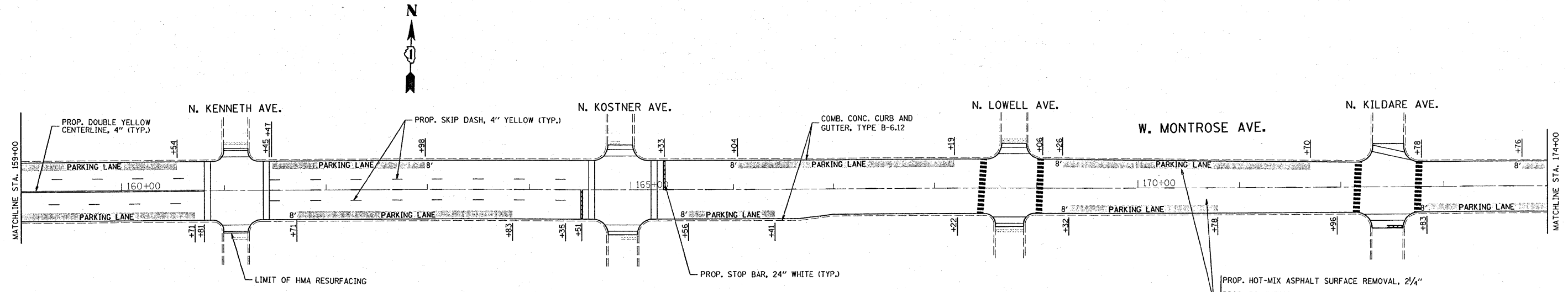
PROP. SKIP DASH, 4" YELLOW (TYP.)

COMB. CONC. CURB AND GUTTER, TYPE B-6.12

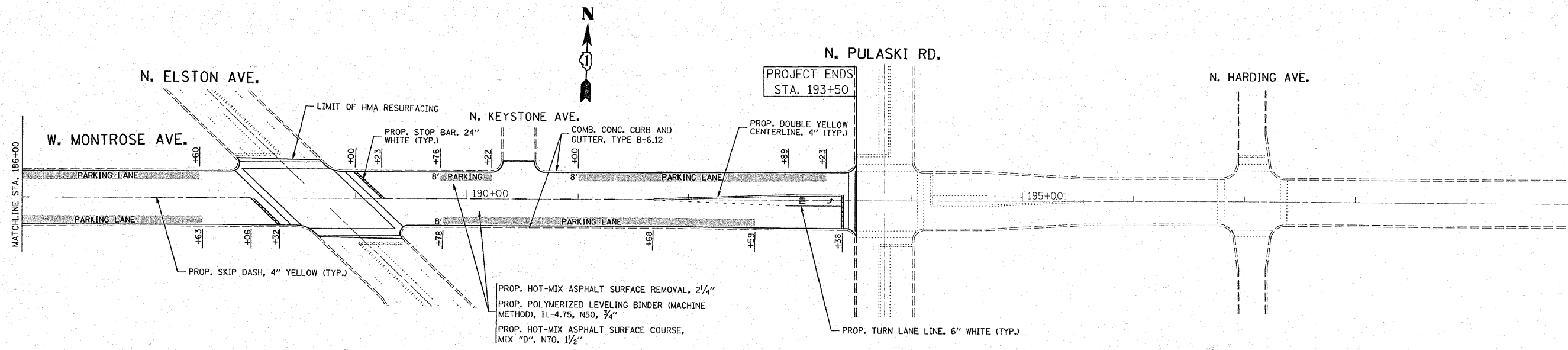
FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	ROADWAY AND PAVEMENT MARKING PLAN MONTROSE AVE.--FOREST PRESERVE RD. TO PULASKI RD.		F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 3/26/2009	DATE -	CHECKED -	REVISED -		SCALE: 1"=50'	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
		DATE -	REVISED -								



FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	ROADWAY AND PAVEMENT MARKING PLAN MONTROSE AVE.--FOREST PRESERVE RD. TO PULASKI RD.			F.A.U. RTE. 1366	SECTION 0101 RS-5	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 12
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		CHECKED -	REVISED -								FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT	
		DATE -	REVISED -									



FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	ROADWAY AND PAVEMENT MARKING PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 3/26/2009	DATE -	CHECKED -	REVISED -		SCALE: 1"=50'	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 60B60		
		DATE -	REVISED -							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



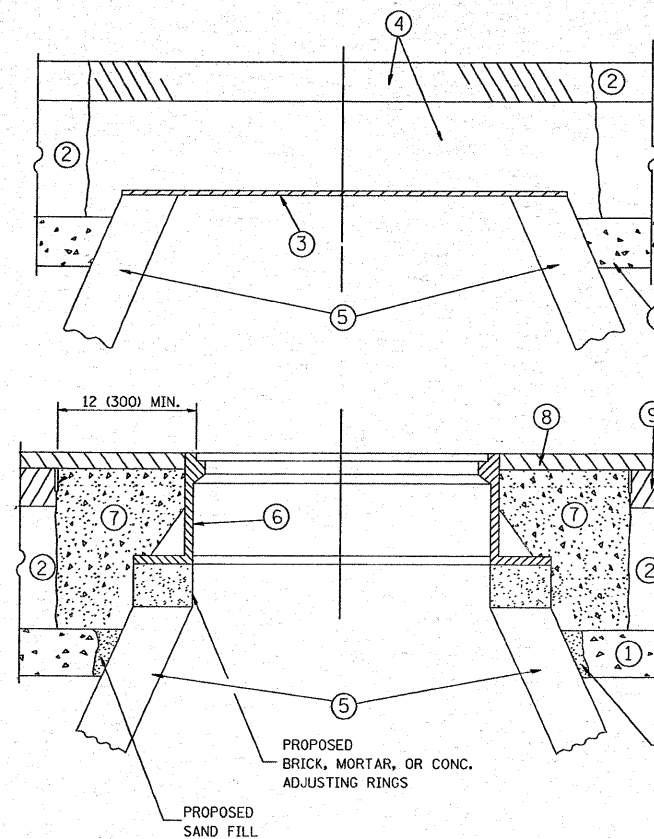
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PLOT SCALE = 5/8"=20'00" / IN.		CHECKED -	REVISED -
PLOT DATE = 3/26/2009		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

ROADWAY AND PAVEMENT MARKING PLAN  
MONTROSE AVE.--FOREST PRESERVE RD. TO PULASKI RD.

SCALE: 1"=50'    SHEET NO.    OF    SHEETS    STA.    TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1366	0101 RS-5	COOK	34	14
CONTRACT NO. 60B60				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

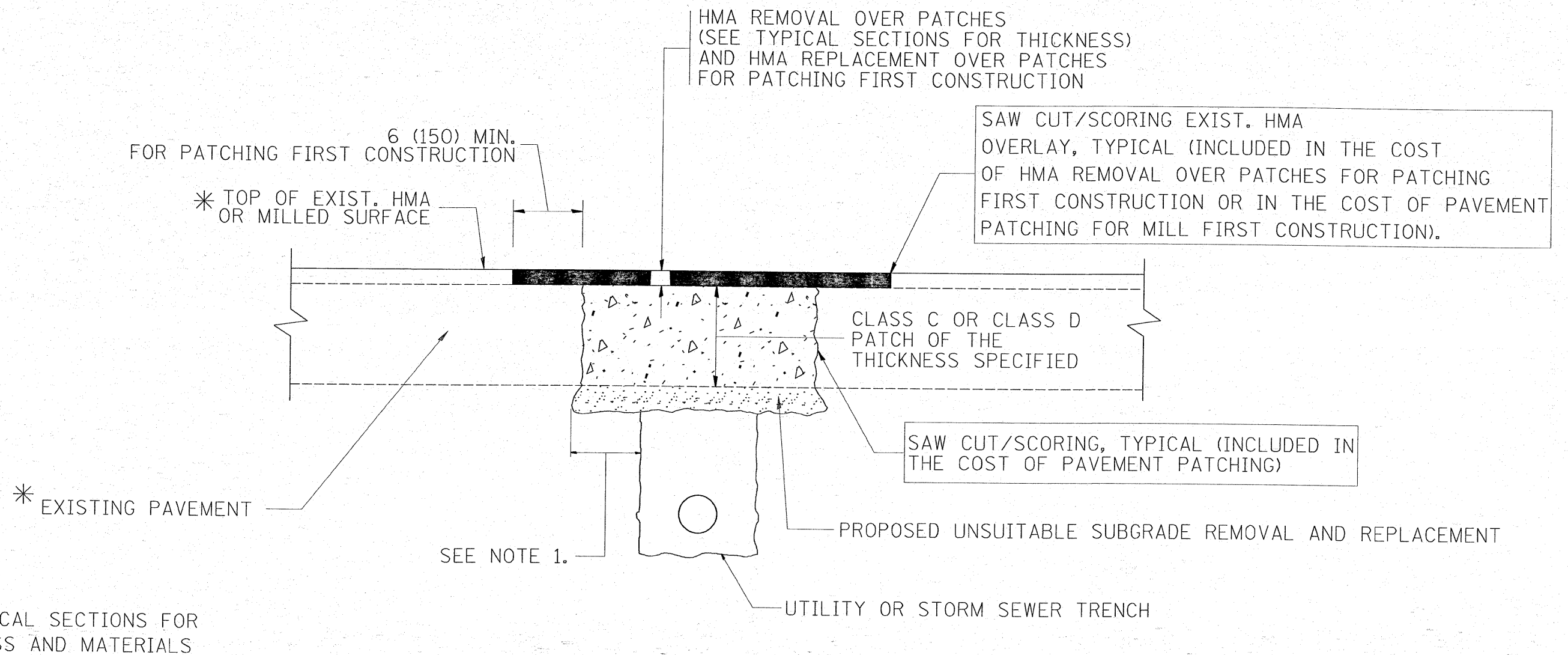
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = steedpa	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
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	PLOT DATE = 3/25/2009	DATE - 10-25-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1366	010IRS-5	COOK	34	15
<b>BD600-03 (BD-8)</b>			CONTRACT NO. 60B60	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = steedpa	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. BORD 01-01-07					1366	0101RS-5	COOK	34	16
		CHECKED -	REVISED - R. BORD 09-04-07		<b>BD400-04 (BD-22)</b>			CONTRACT NO. 60B60				
		DATE - 10-25-94	REVISED - R. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

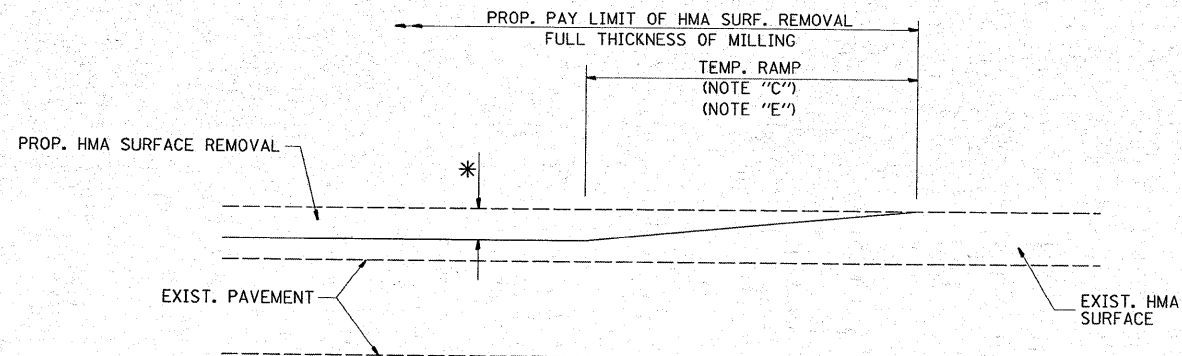
**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

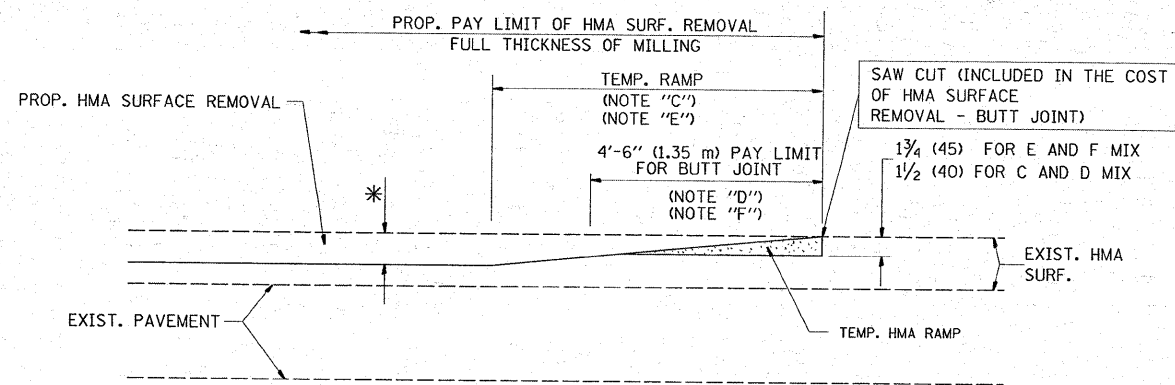
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = steedpa	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw_work\pwidot\steedpa\d0100515\0137306-shr-plan.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			1366	0101RS-5	COOK	34	17	
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PLOT DATE = 3/25/2009		DATE - 03-11-94	REVISED - R. BORO 01-01-07			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
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MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

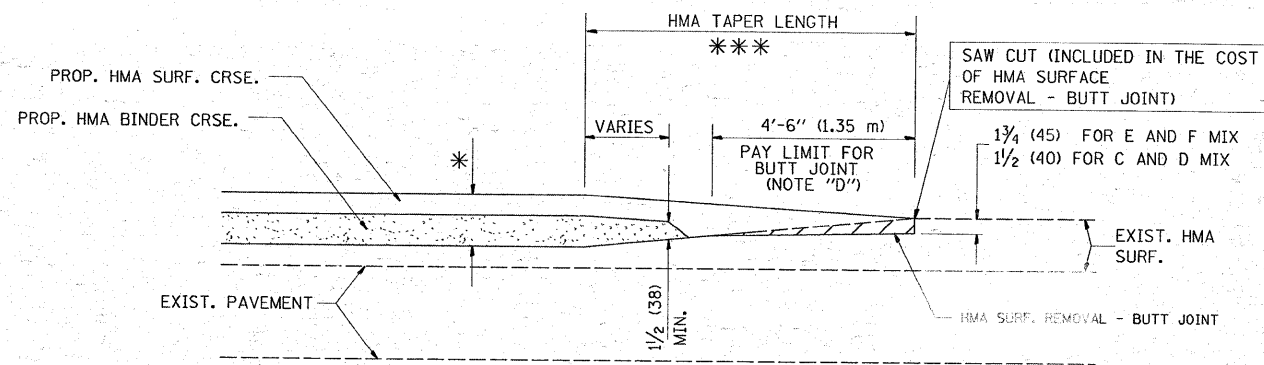
**OPTION 1**



HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

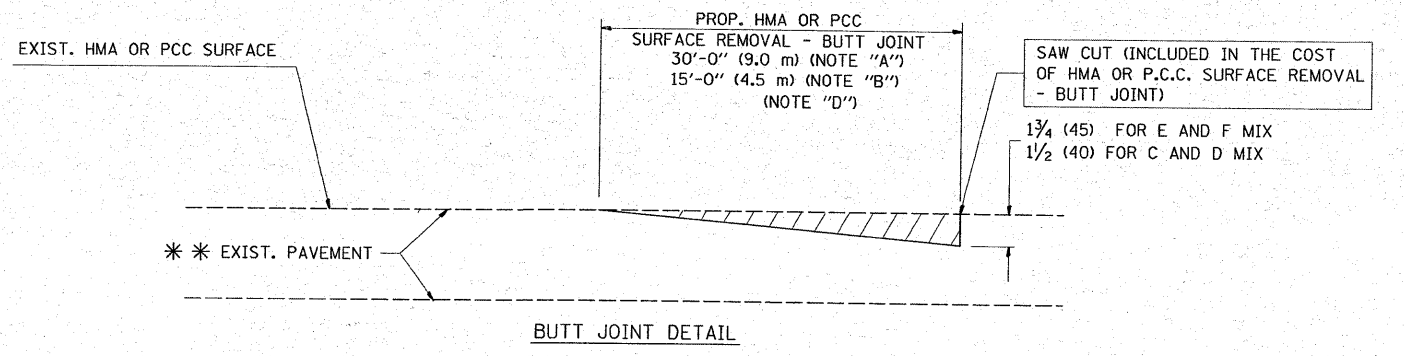
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

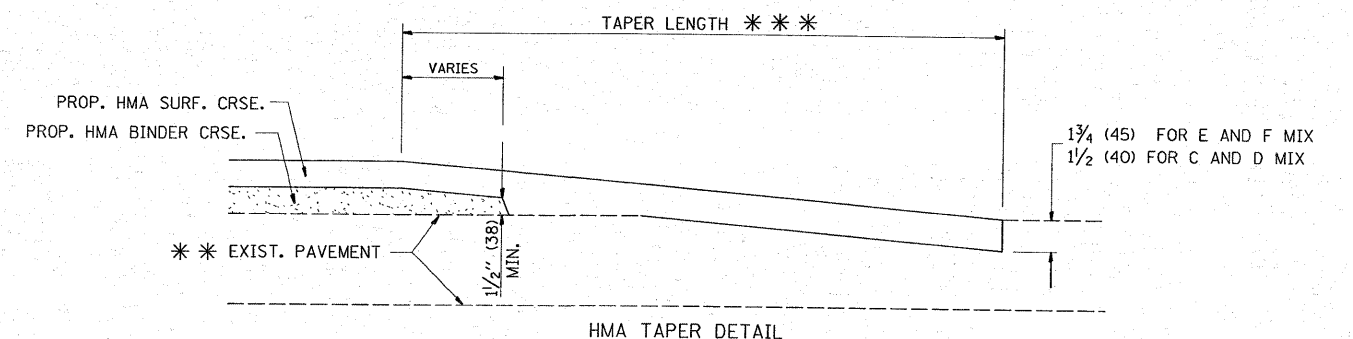


BUTT JOINT AND  
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

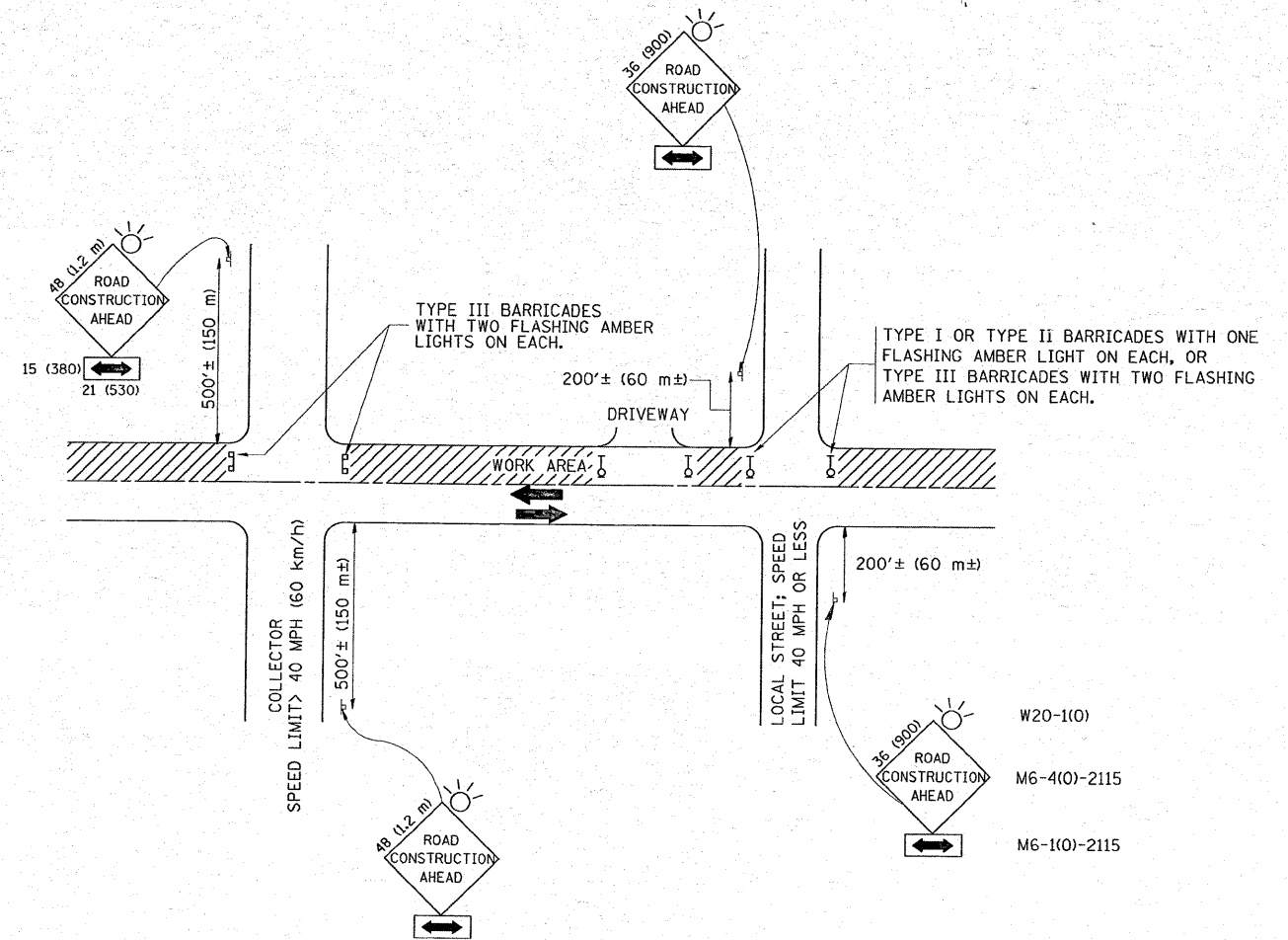
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = steedpa	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
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PLOT DATE = 3/25/2009		DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1366	010IRS-5	COOK	34	18
BD400-05 BD32			CONTRACT NO. 60B60	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

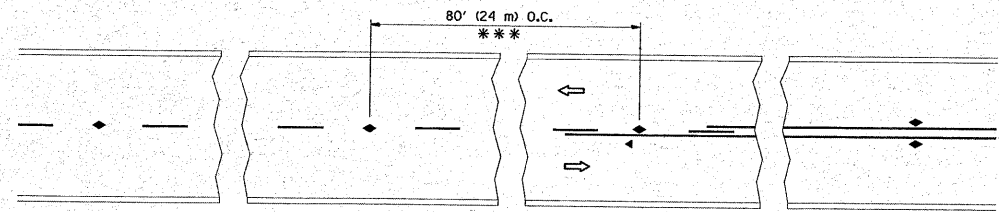
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PLOT DATE = 3/25/2009		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

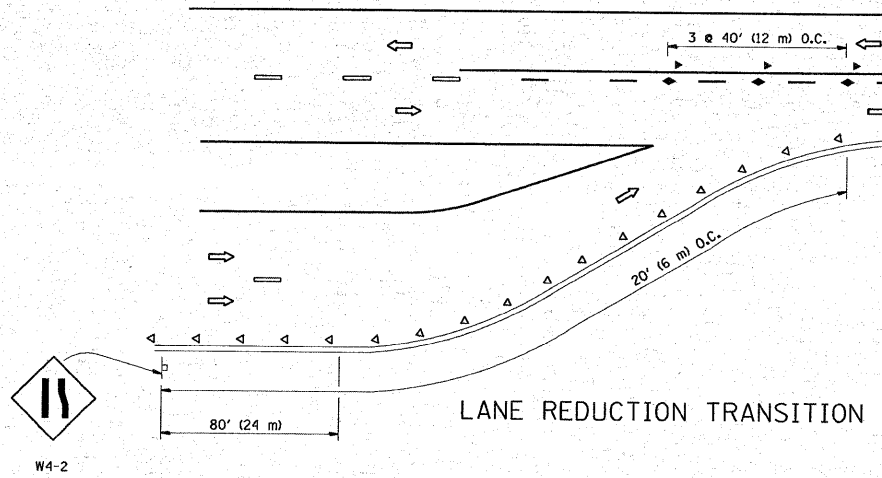
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TC-10			CONTRACT NO. 60B60	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

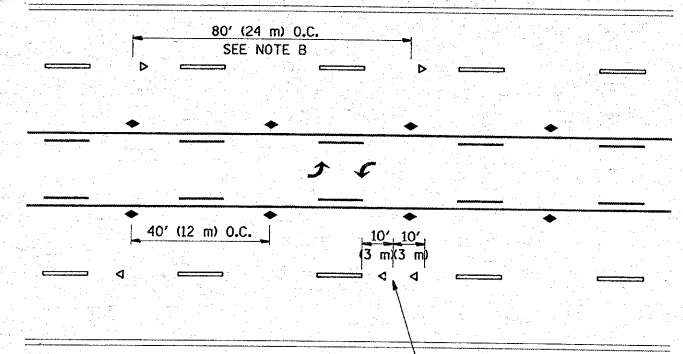


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

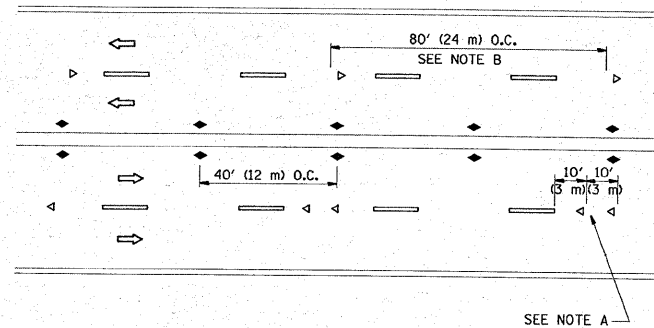
TWO-LANE/TWO-WAY



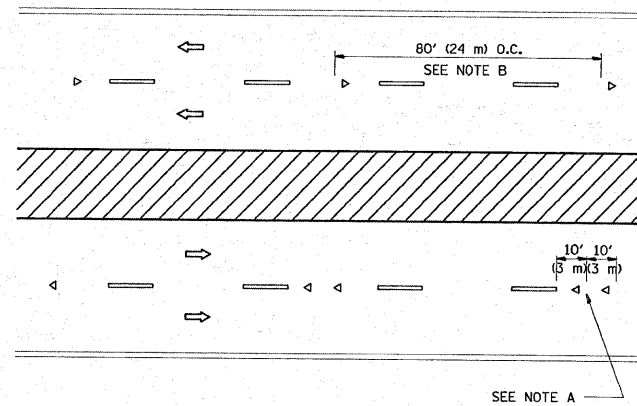
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

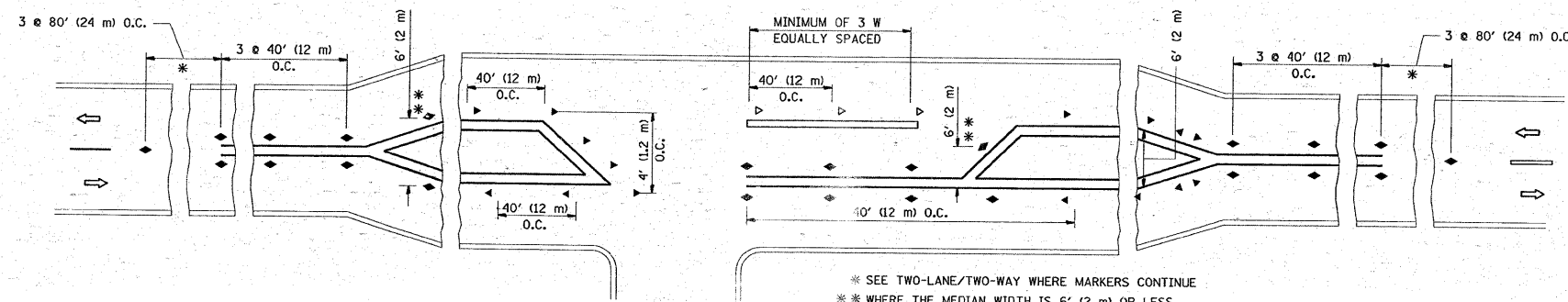
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

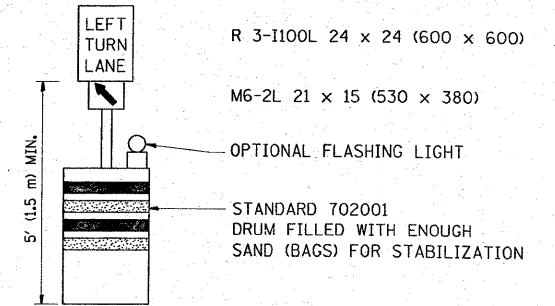
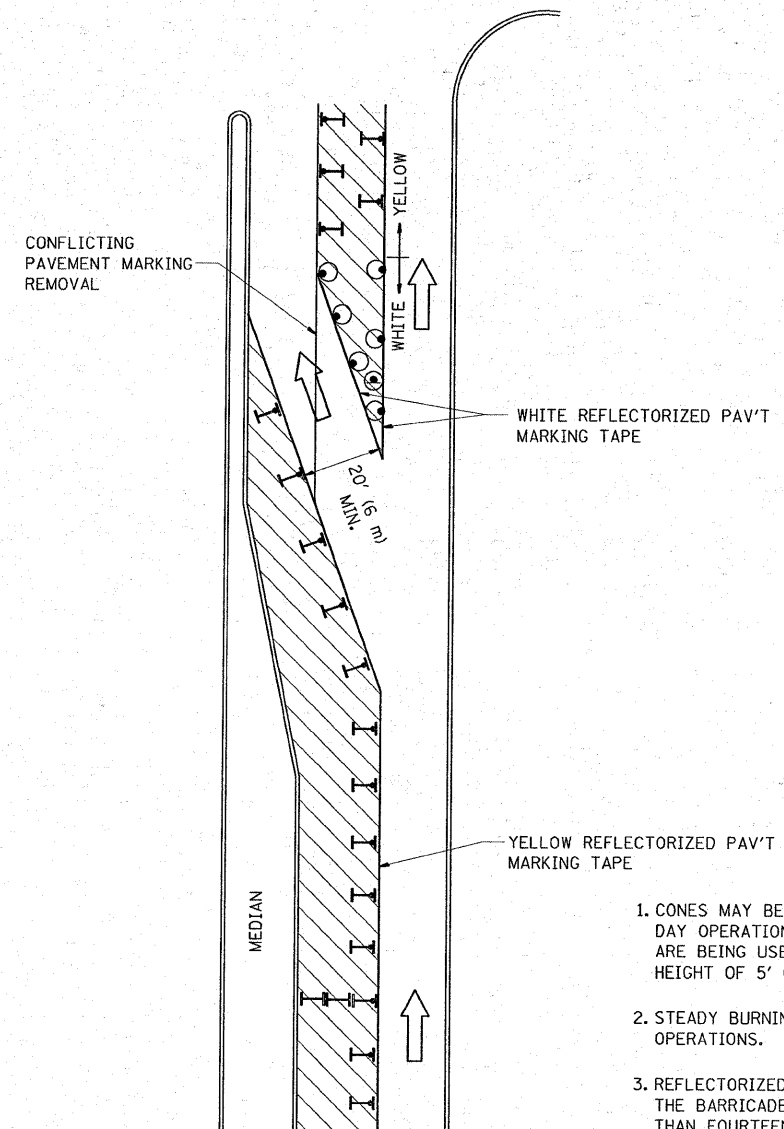
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	PLOT DATE = 3/25/2009	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.







F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1366	010IRS-5	COOK	34	20
TC-11		CONTRACT NO. 60B60		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

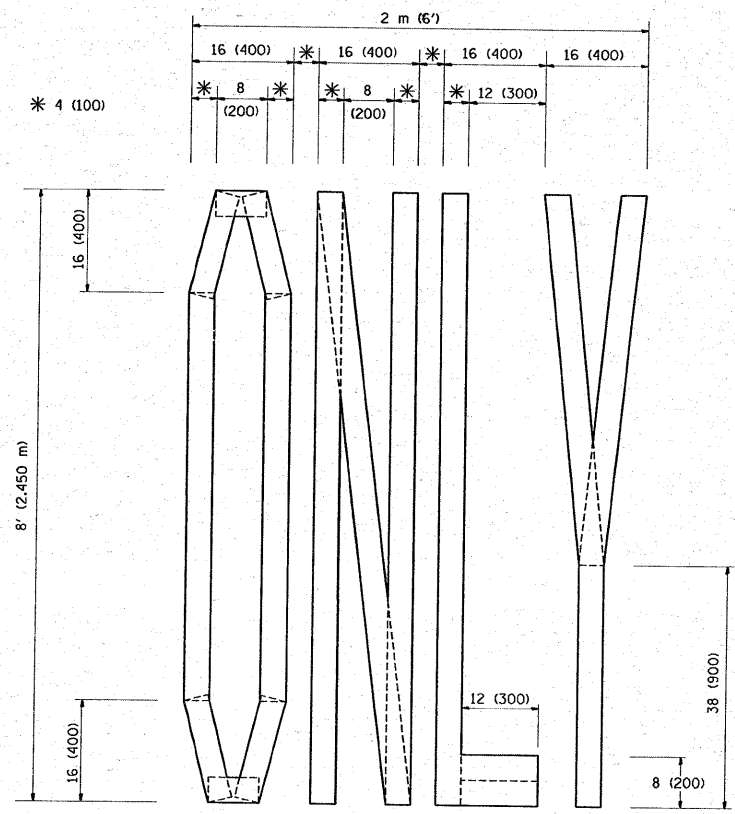
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		DATE -	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

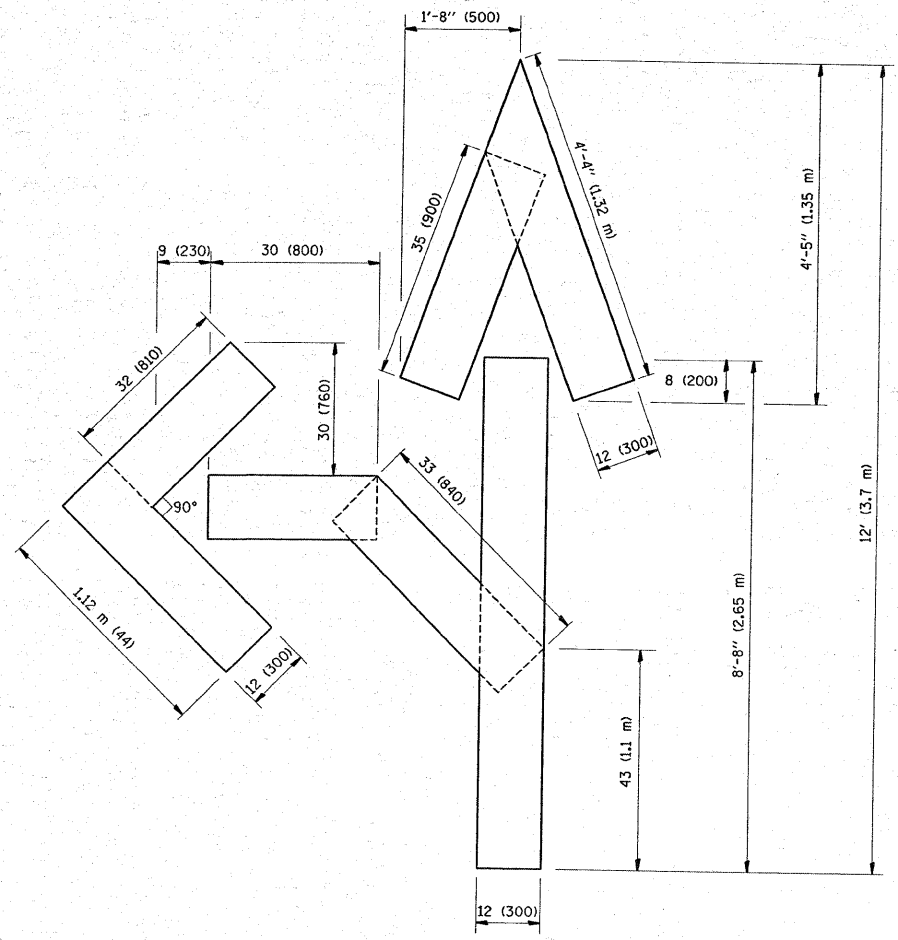
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

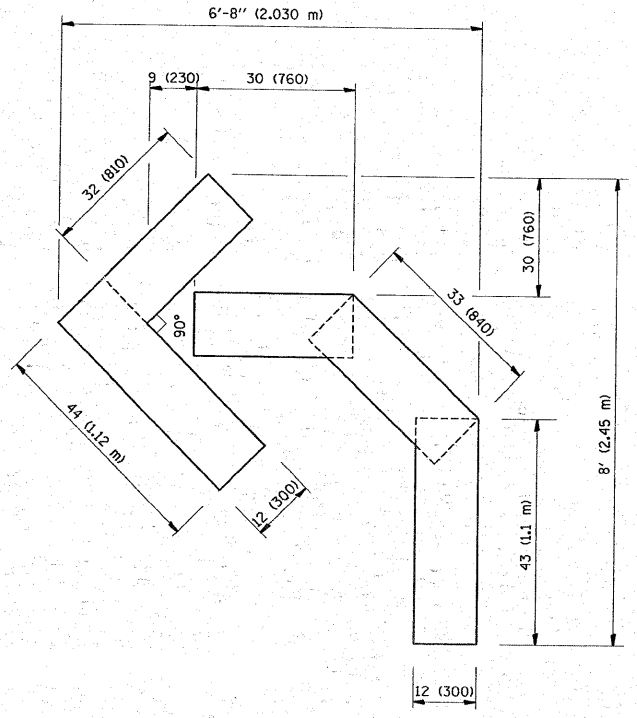
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1366	010IRS-5	COOK	34	21
TC-14			CONTRACT NO. 60B60	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

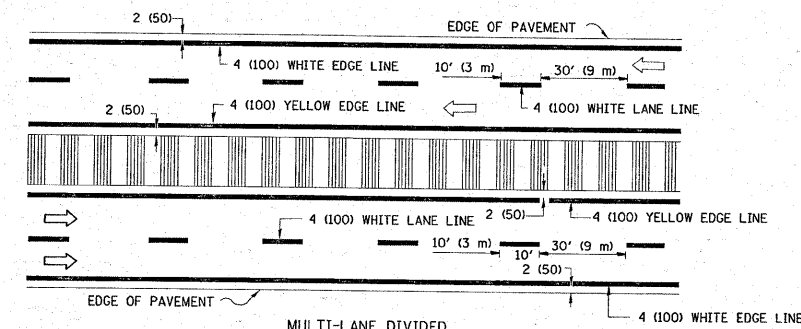
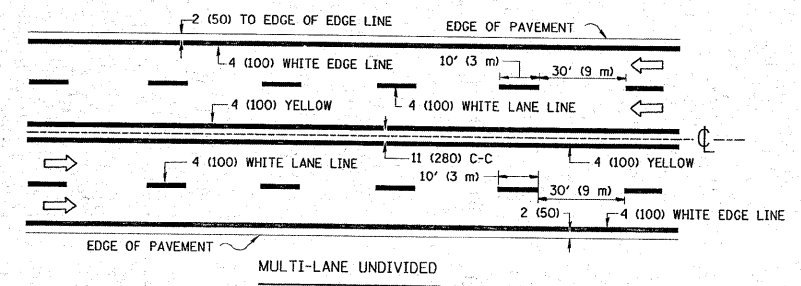
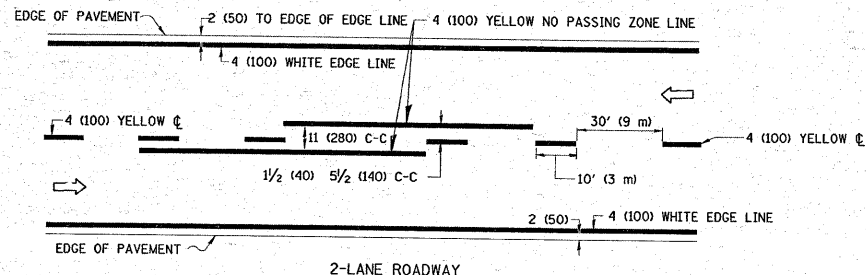
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...	...	CHECKED -	REVISED - T. RAMMACHER 03-02-98
...	...	DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING**

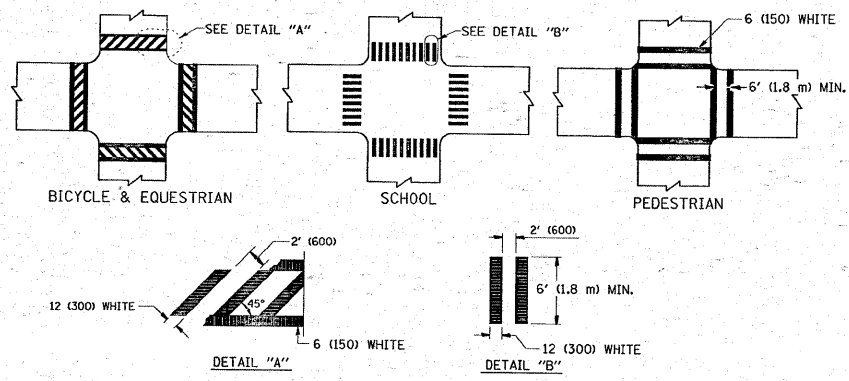
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1366	0101RS-5	COOK	34	22
<b>TC-16</b>			<b>CONTRACT NO. 60B60</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

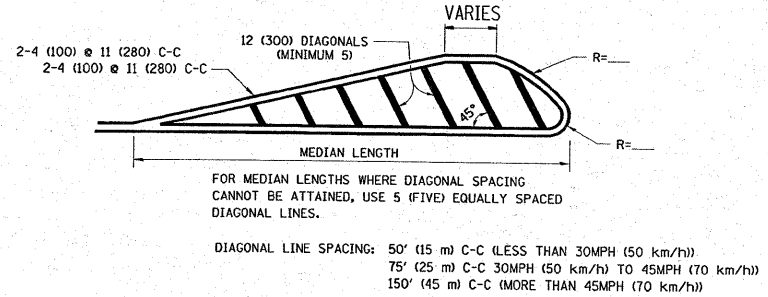
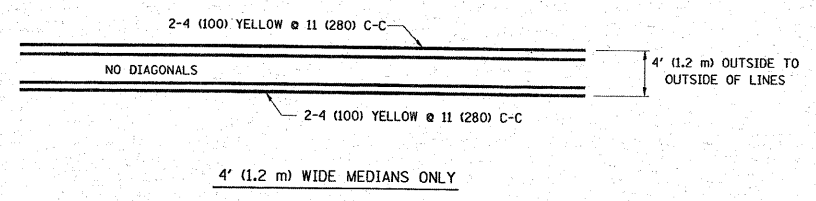


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

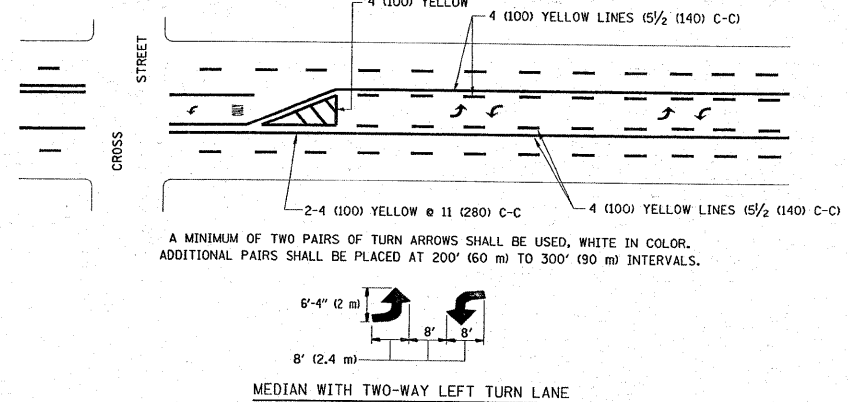
TYPICAL LANE AND EDGE LINE MARKING



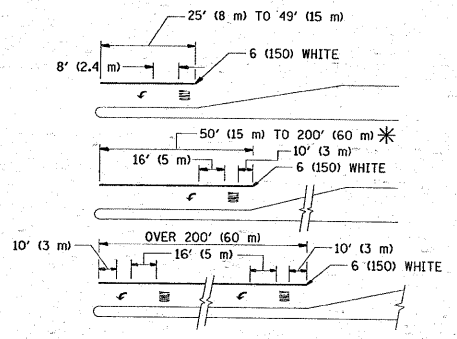
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING

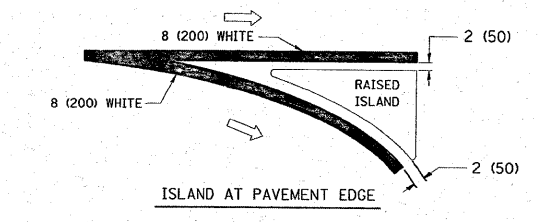
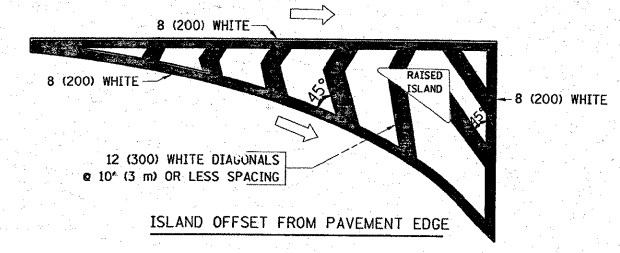


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
LANE LINES	5 (125) ON FREEWAYS	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
TWO WAY LEFT TURN MARKING	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	2' (600) APART
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

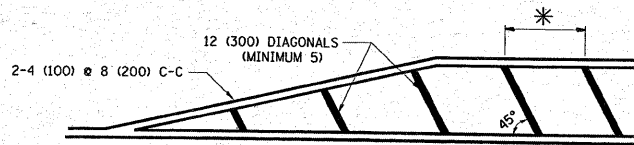
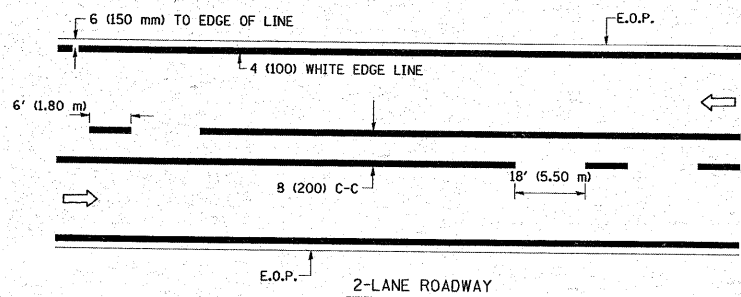
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	PLOT DATE = 3/25/2009	DATE - 03-19-90	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

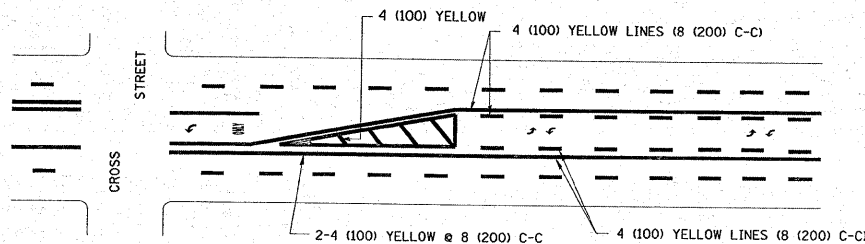
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

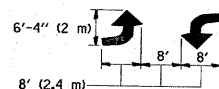


\* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
\* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

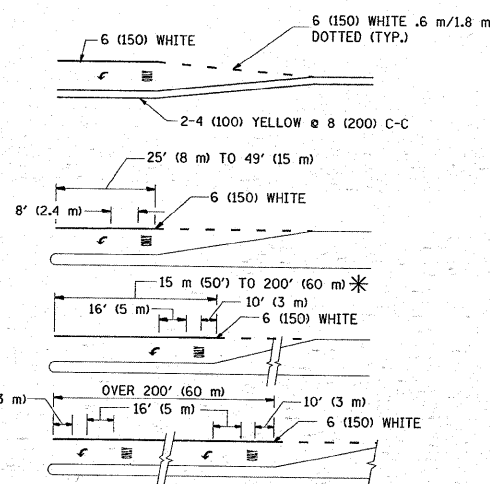


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

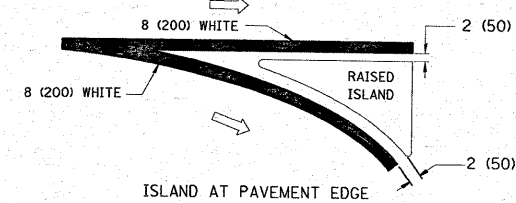
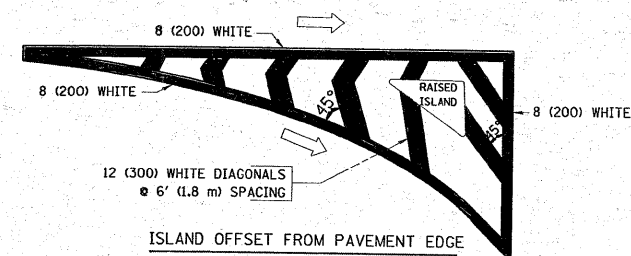


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.8 SQ. FT. (1.47 m<sup>2</sup>) ONLY AREA = 22.9 SQ. FT. (2.13 m<sup>2</sup>)

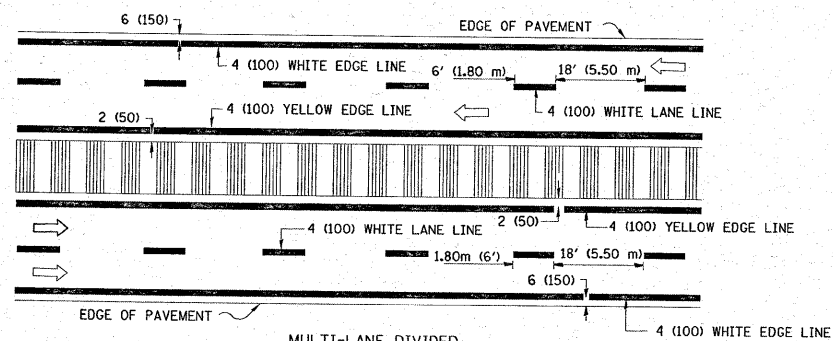
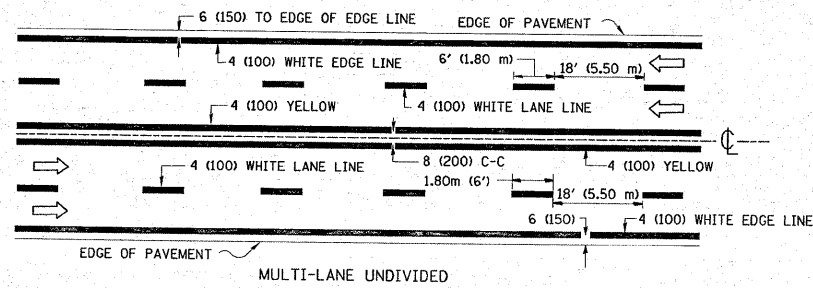
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

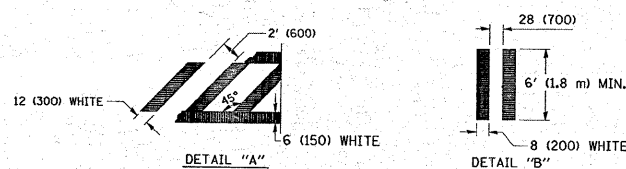
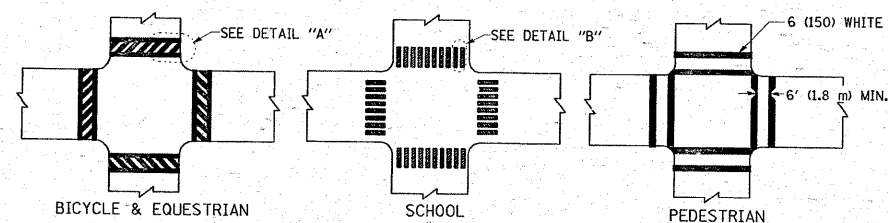


TYPICAL ISLAND MARKING



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 8 (200) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2'-4" (700) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PLACE TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "R"=3.6 SQ. FT. (0.33m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
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	PLOT DATE = 3/25/2009	DATE -	REVISED -

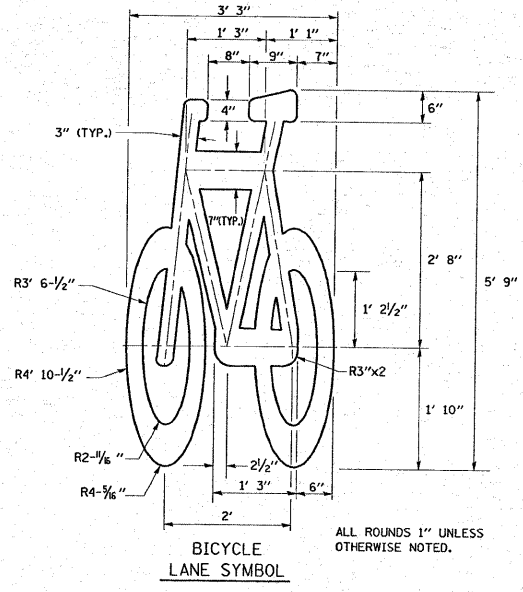
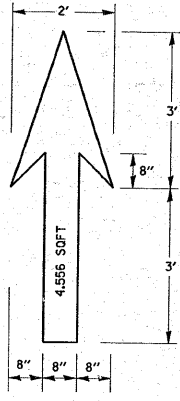
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO  
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 2 SHEETS STA. TO STA.

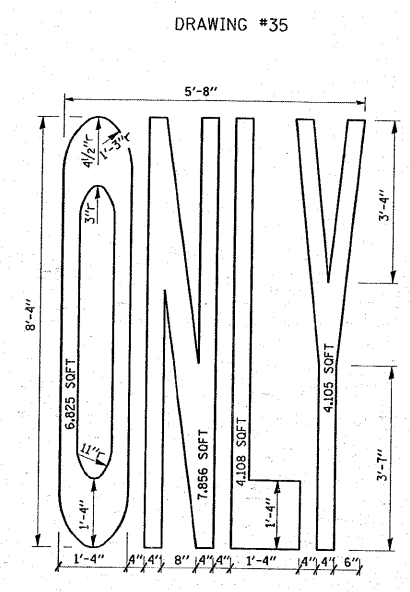
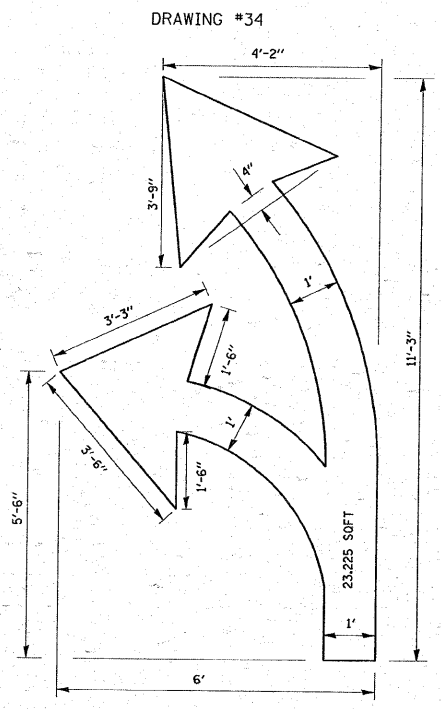
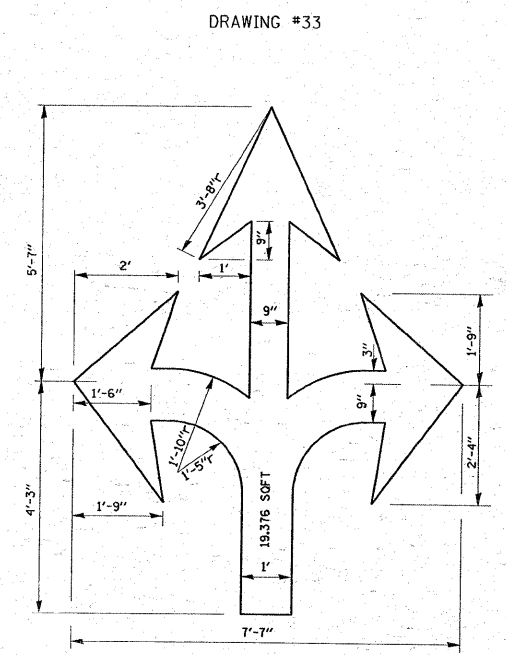
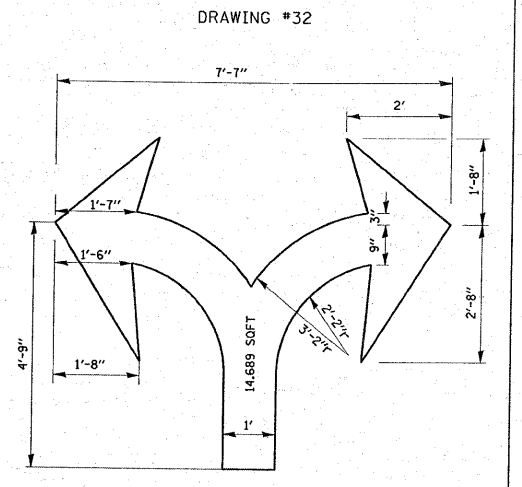
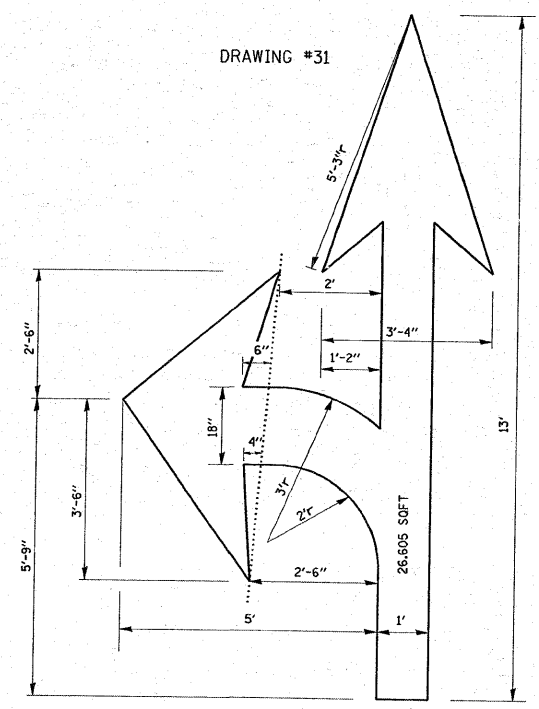
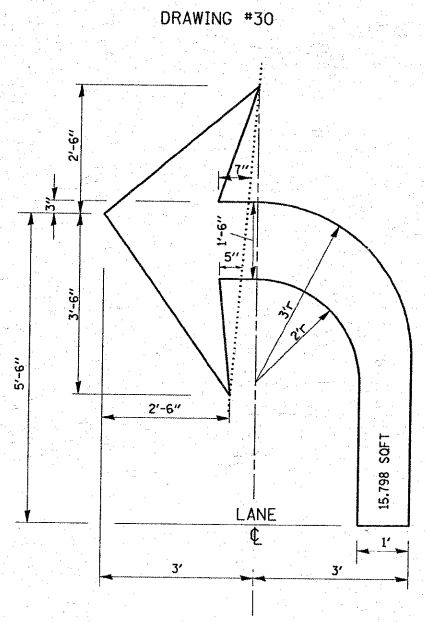
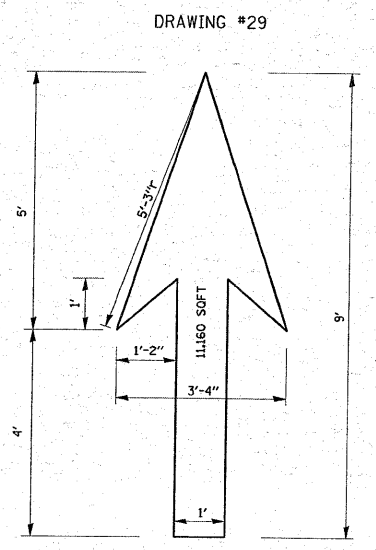
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1366	0101RS-5	COOK	34	24
TC-24		CONTRACT NO. 60B60		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				





**NOTE:**  
 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.  
 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

**TYPICAL BIKE LANE SYMBOLS**  
DRAWING #28



**NOTE:**  
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO**  
**TYPICAL PAVEMENT MARKINGS**

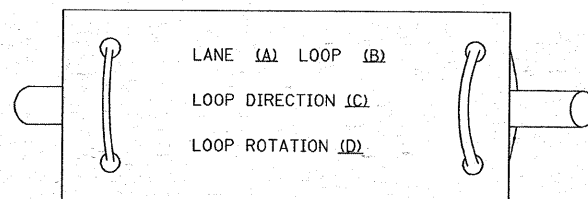
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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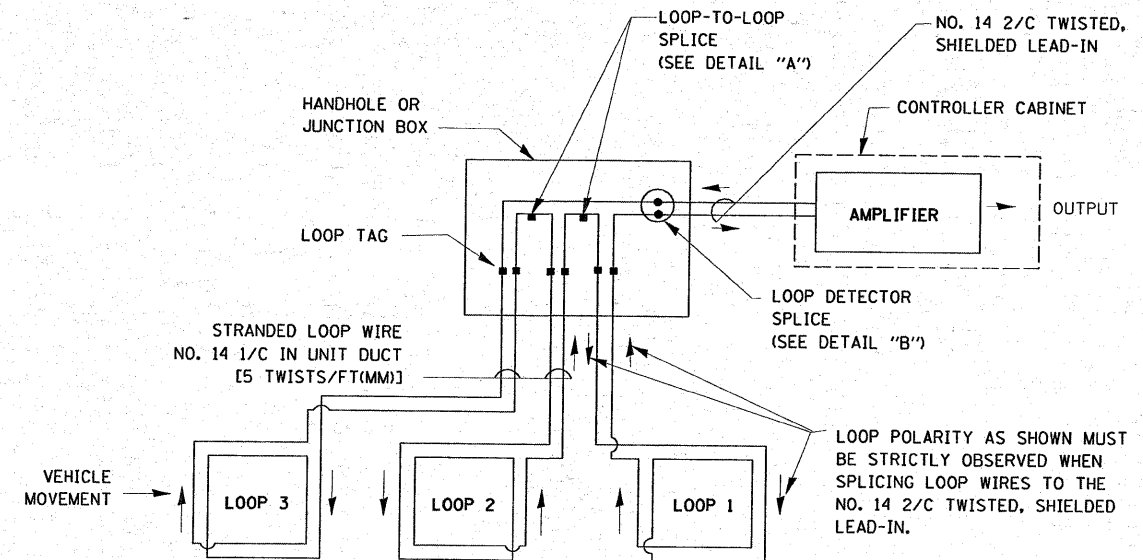
## LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

### LOOP LEAD-IN CABLE TAG

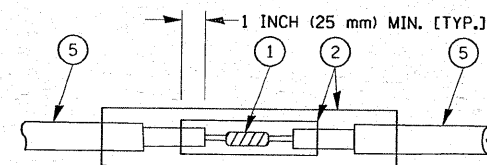


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

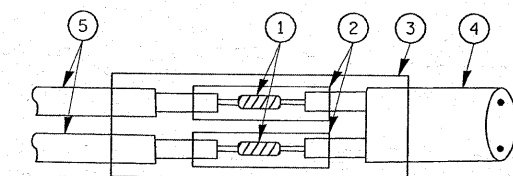


### DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

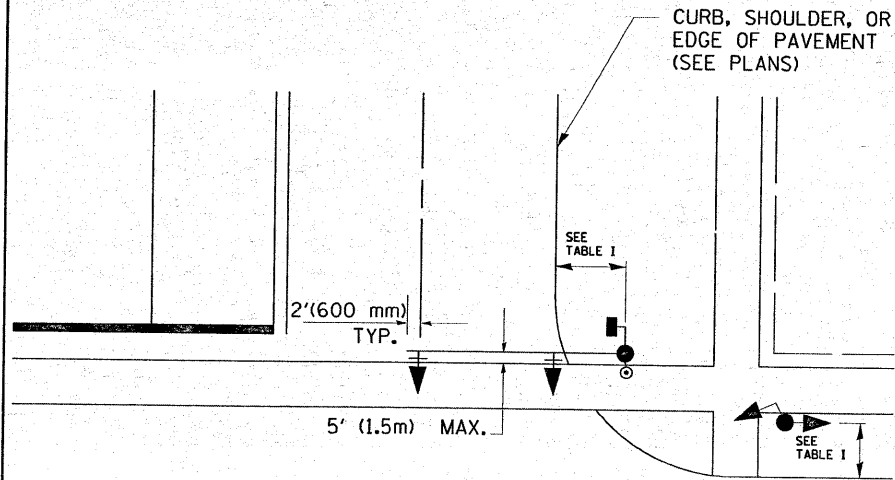
### LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

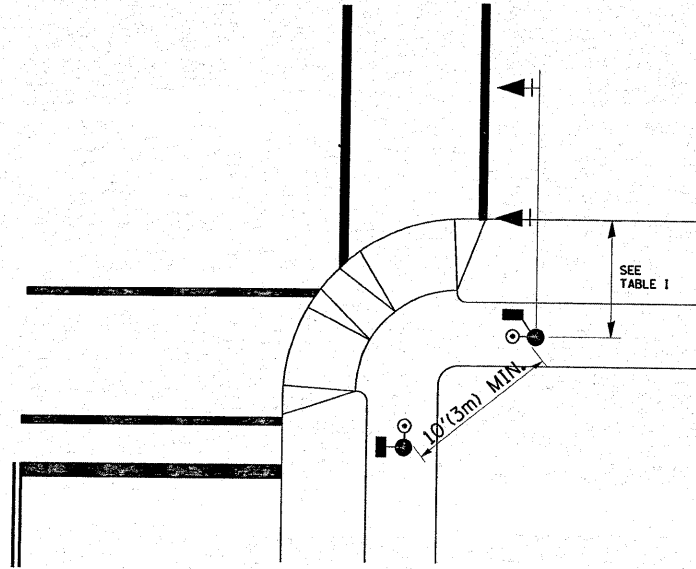
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PLOT DATE = 3/25/2009	DATE - 05-30-00	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
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**TRAFFIC SIGNAL MAST ARM AND POST**

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



**PEDESTRIAN SIGNAL PUSHBUTTON**



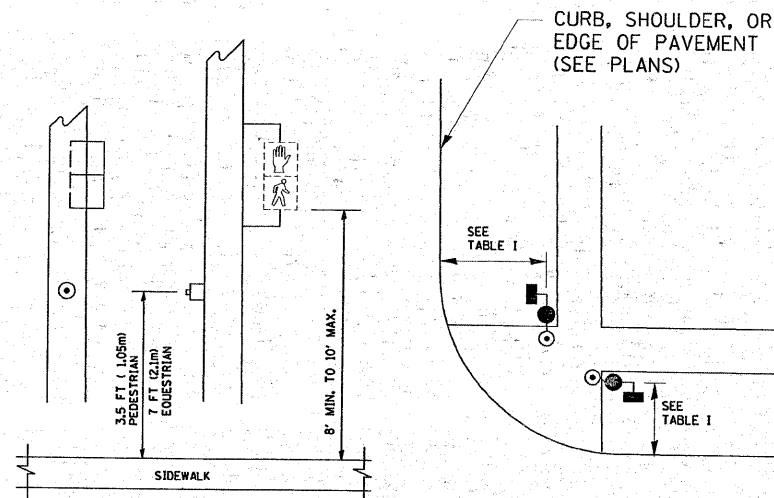
RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

**NOTES:**

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.  
 AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.  
 PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:  
 A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL-WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.  
 B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.  
 C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.  
 D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).  
 E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

**PEDESTRIAN SIGNAL POST**

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION



**TABLE I**

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

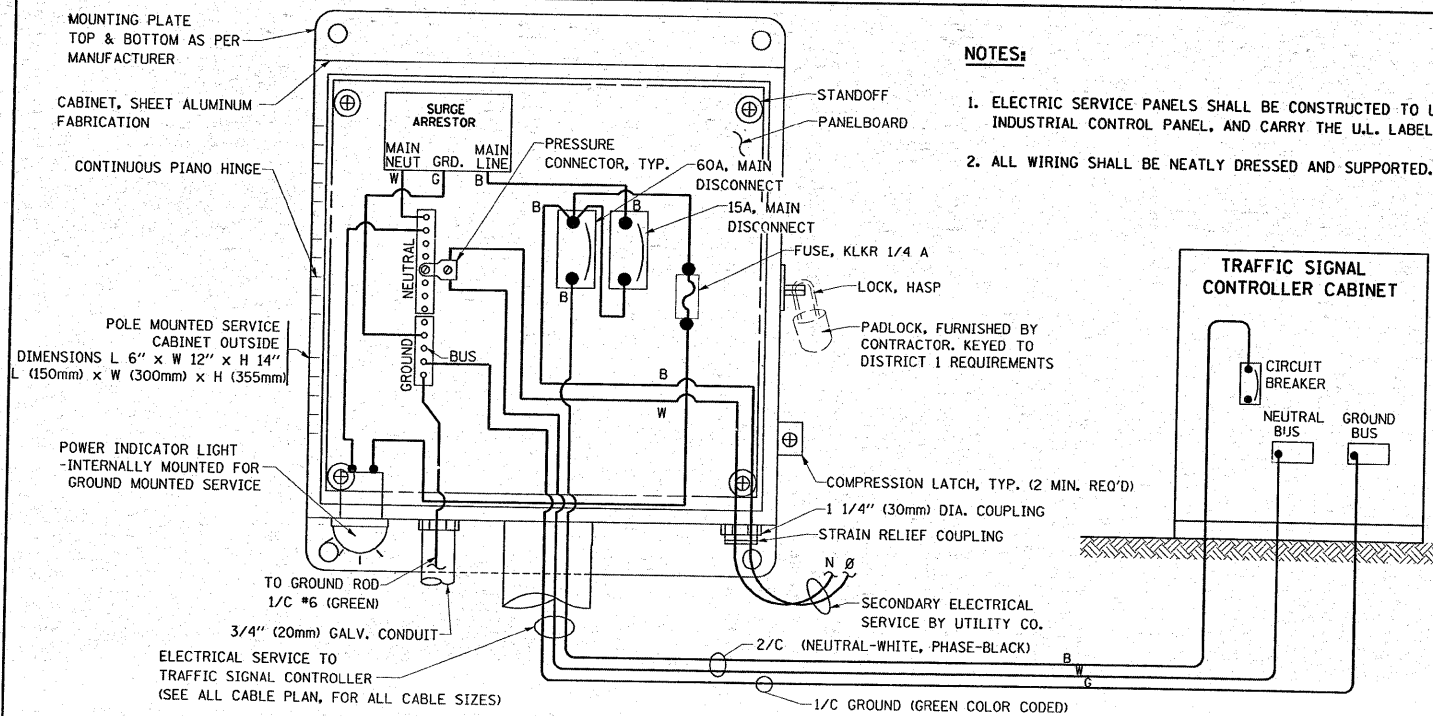
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PLOT DATE = 3/25/2009		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

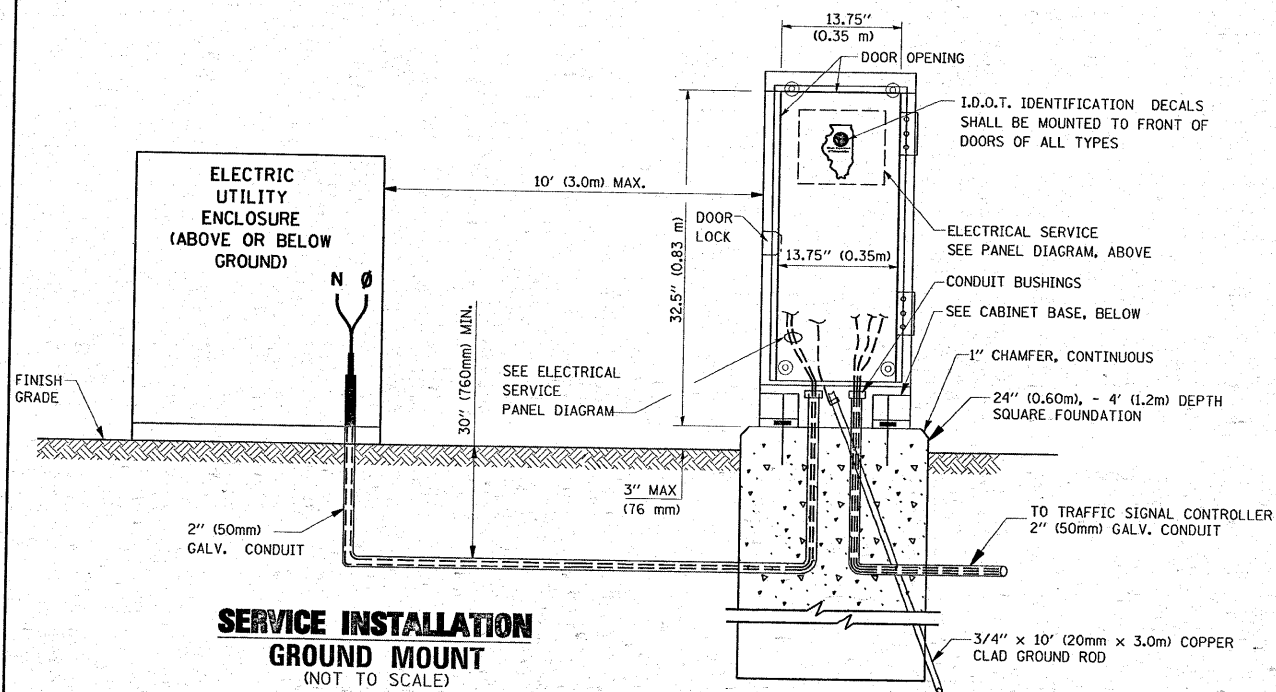
**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

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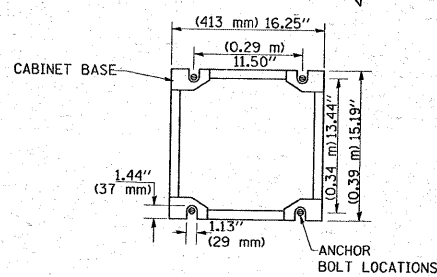
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TS-05			CONTRACT NO. 60B60	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



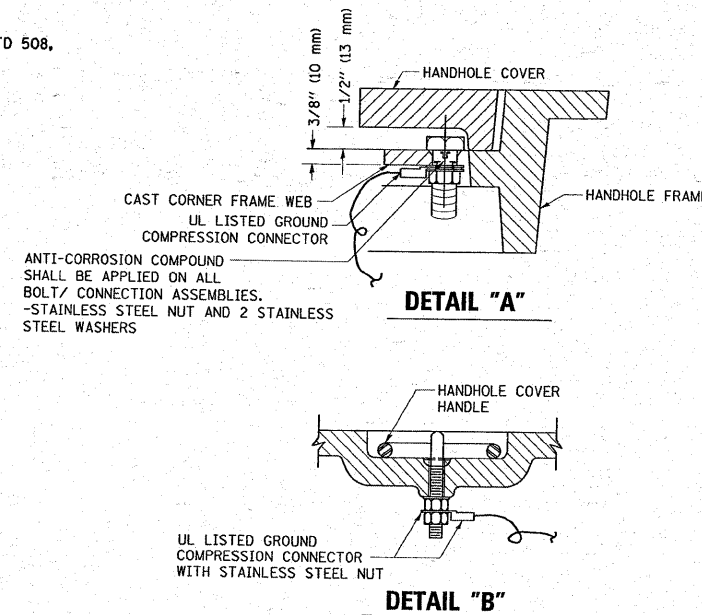
**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)**  
**SERVICE INSTALLATION POLE MOUNT (SHOWN)**  
 (NOT TO SCALE)



**SERVICE INSTALLATION GROUND MOUNT**  
 (NOT TO SCALE)

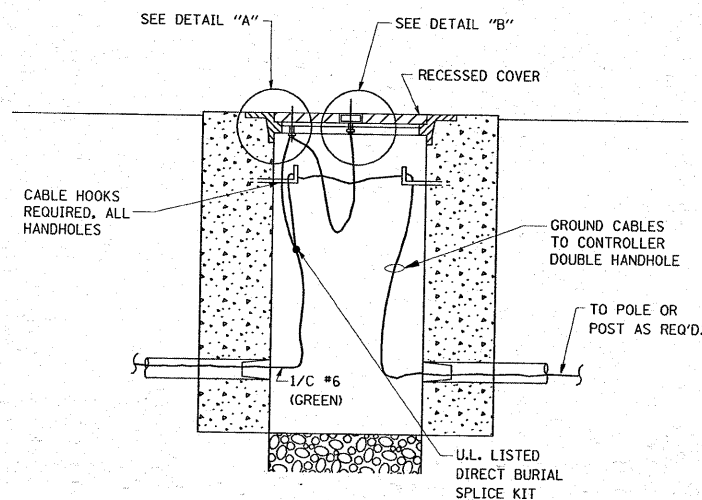


**CABINET - BASE BOLT PATTERN**  
 (NOT TO SCALE)

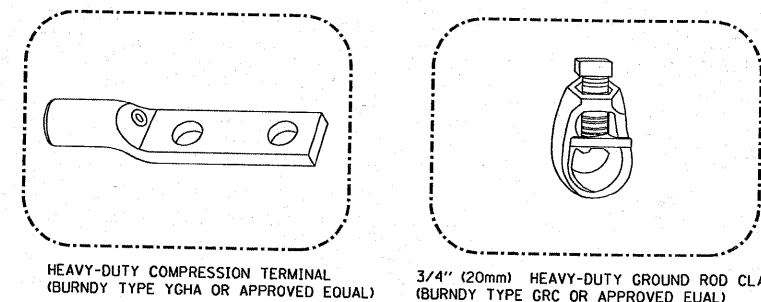


**NOTES:**  
**GROUNDING SYSTEM**

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

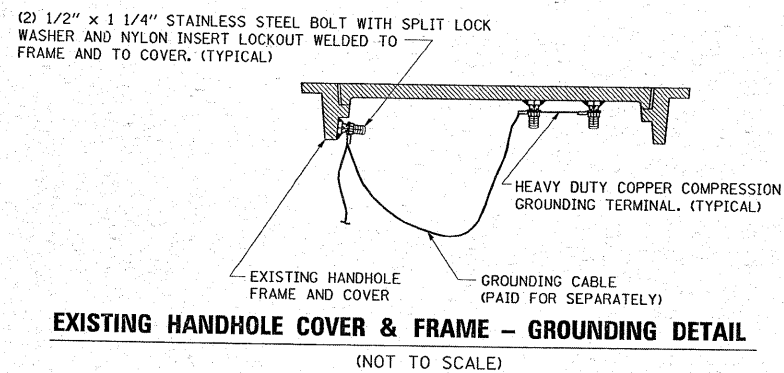


**HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
 (NOT TO SCALE)

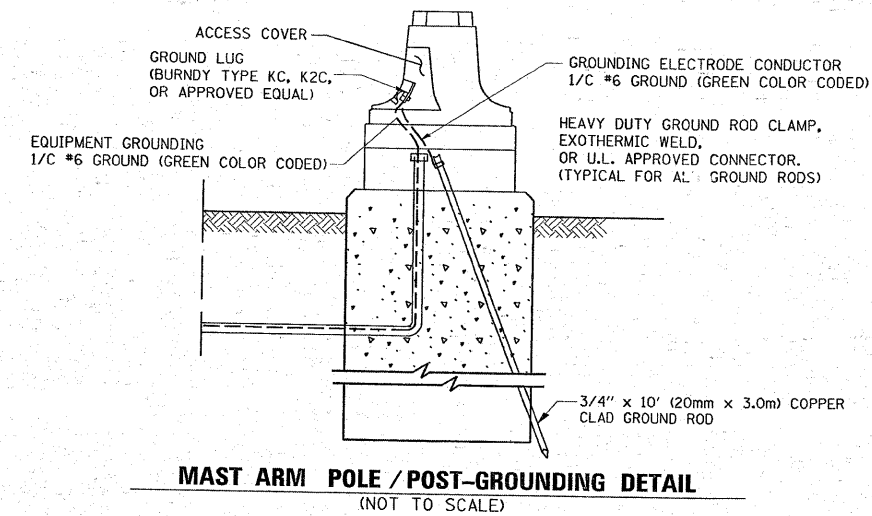


**NOTES:**

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
- 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
- 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
- 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



**EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
 (NOT TO SCALE)



**MAST ARM POLE / POST-GROUNDING DETAIL**  
 (NOT TO SCALE)

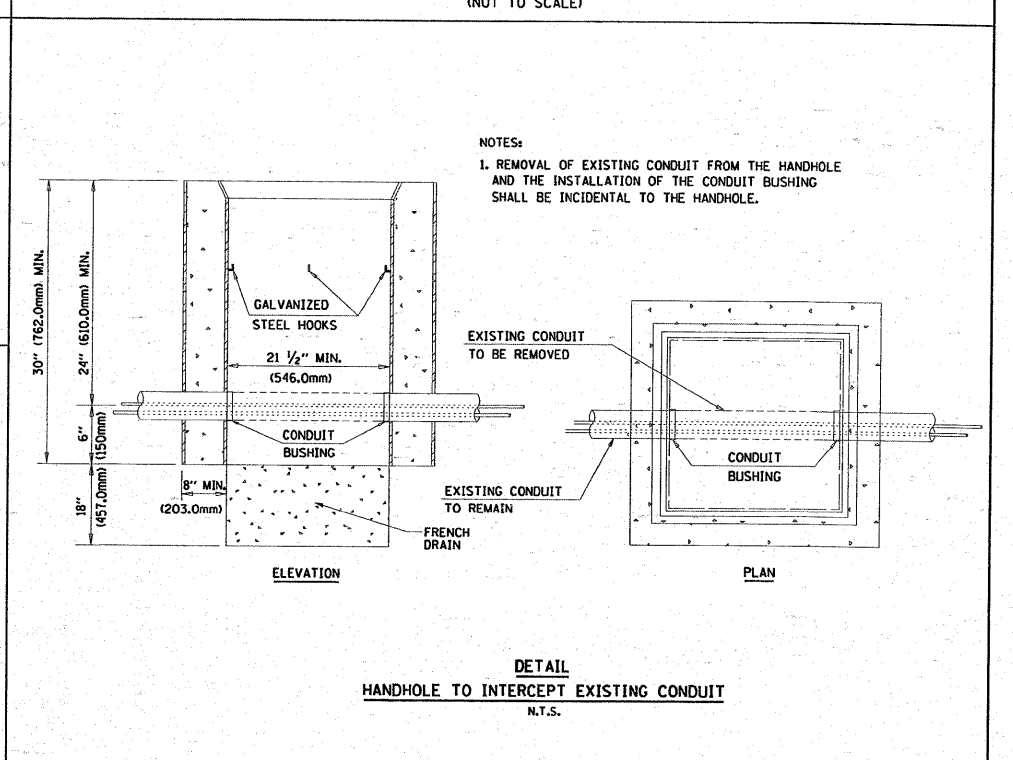
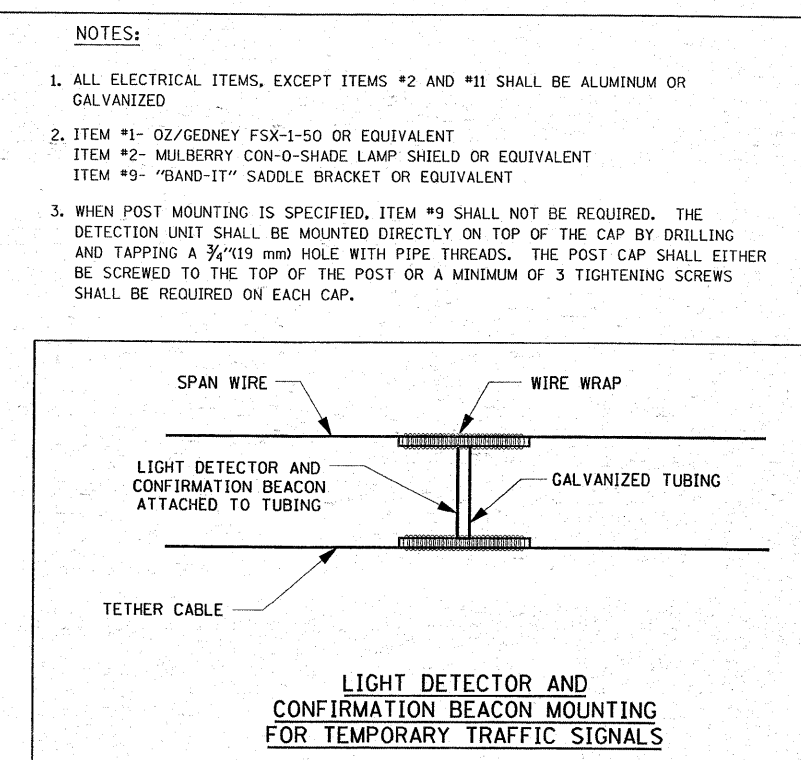
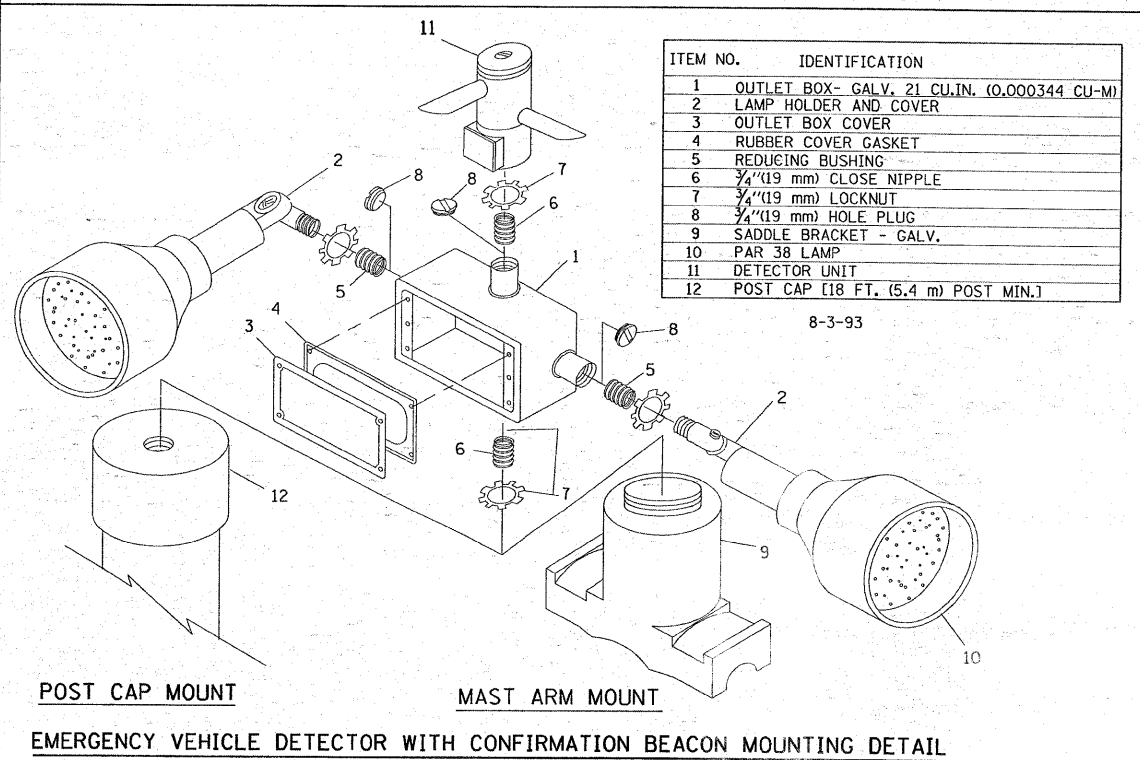
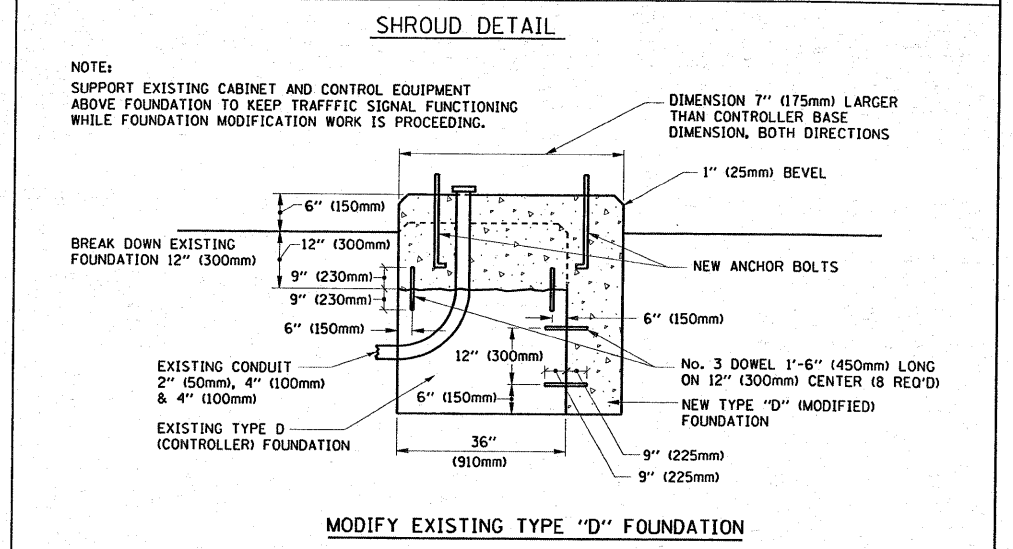
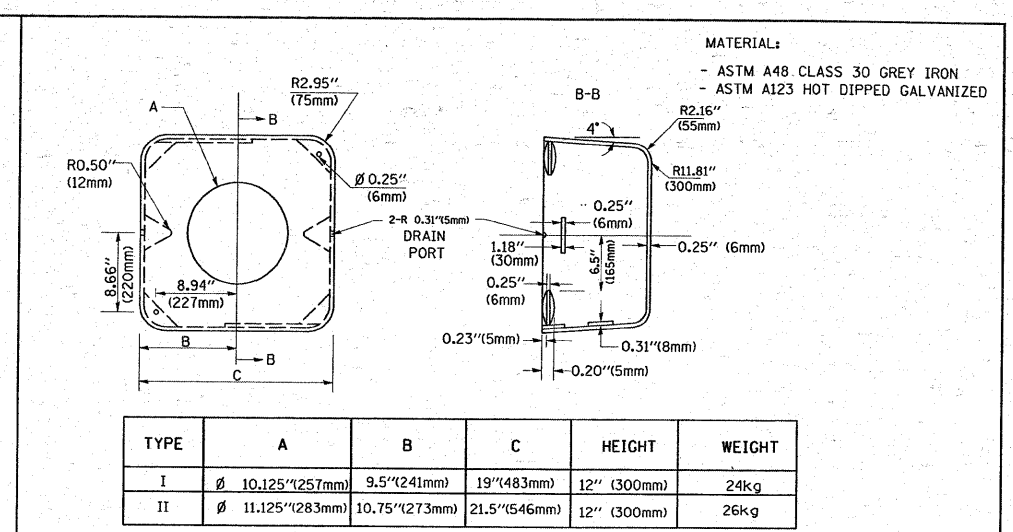
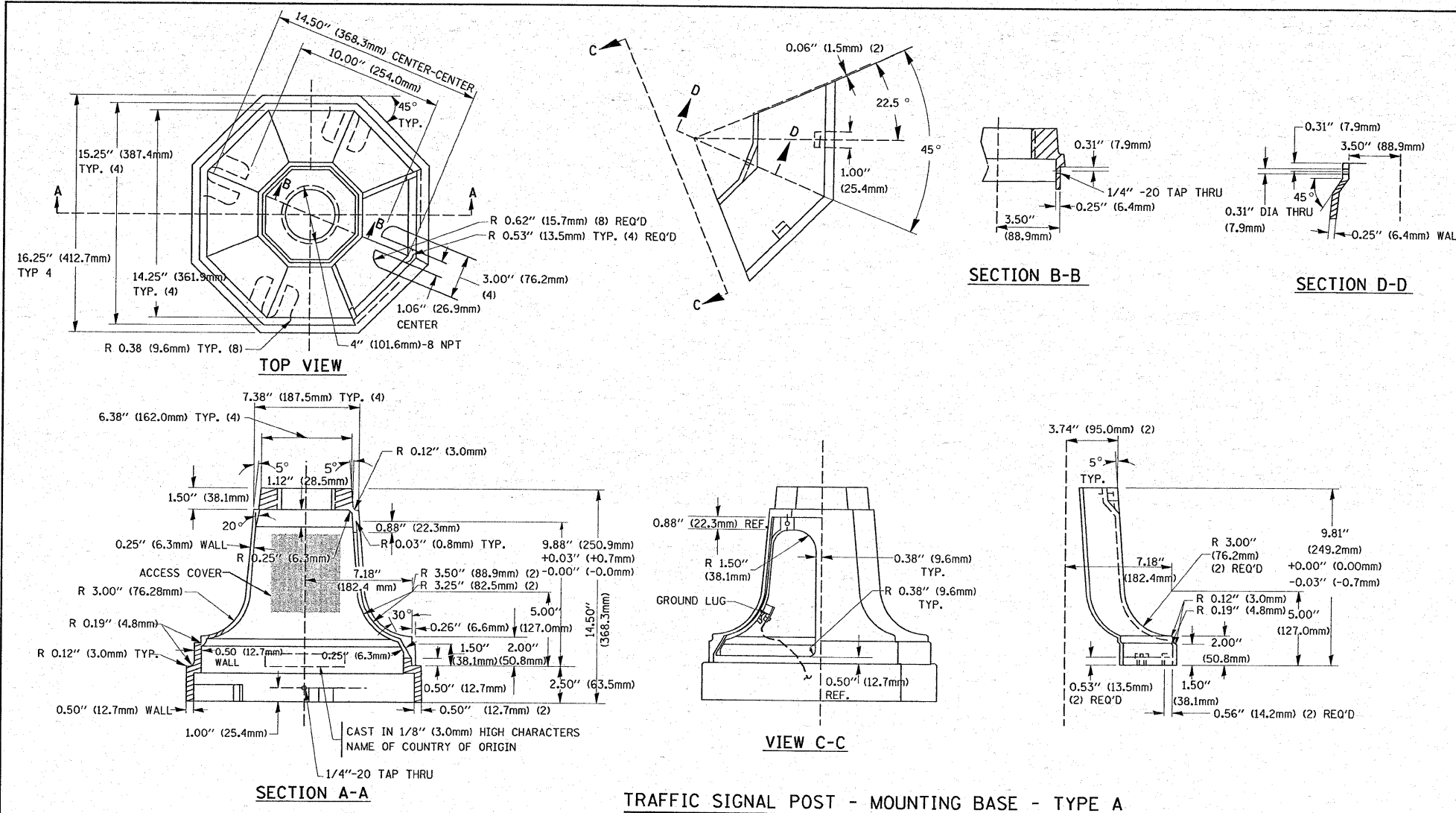
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PLOT SCALE = 50.0000' / IN.		CHECKED - D.A.Z.	REVISED -
PLOT DATE = 3/25/2009		DATE - 05-30-00	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE**  
**STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

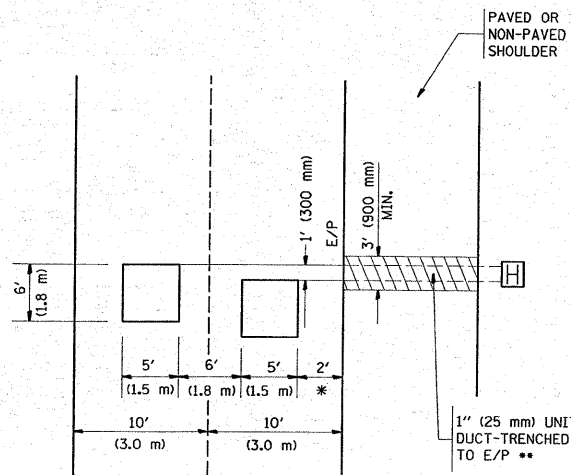
SCALE: NONE SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1366	0101RS-5	COOK	34	28
TS-05			CONTRACT NO. 60B60	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



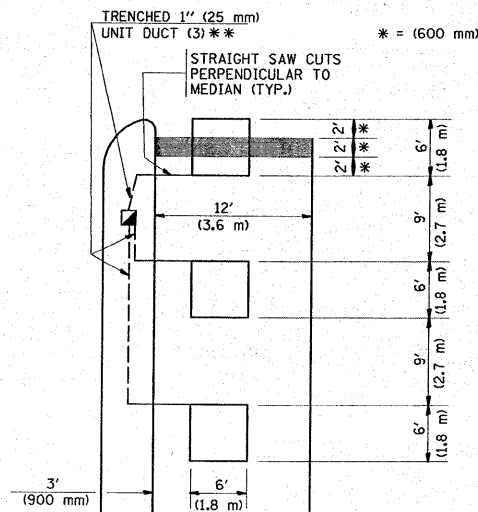
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

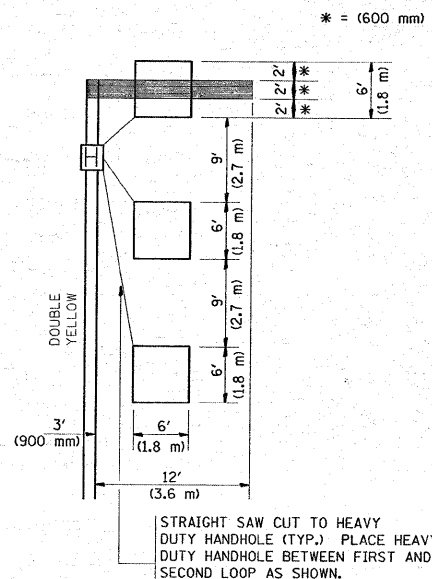


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

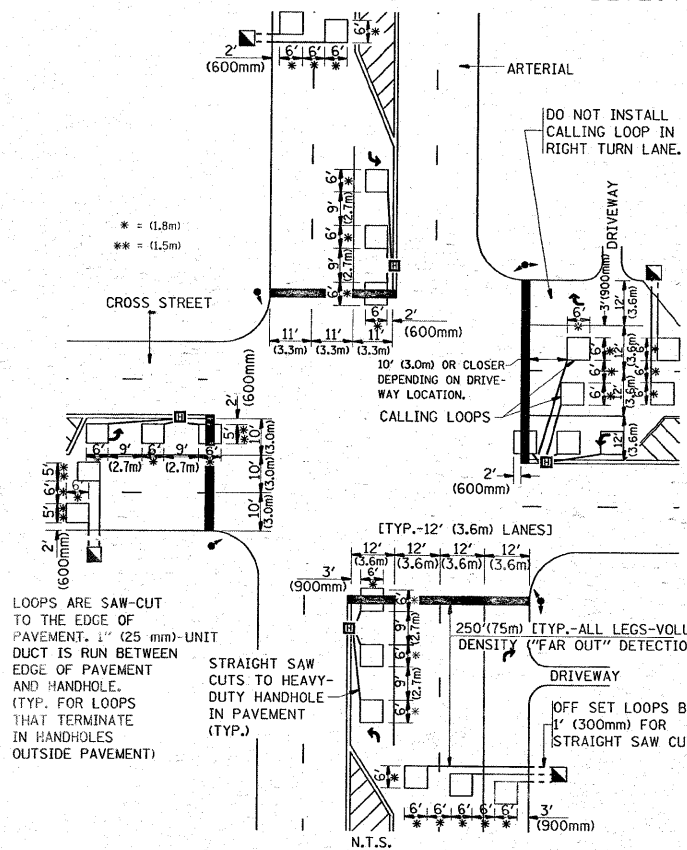
**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



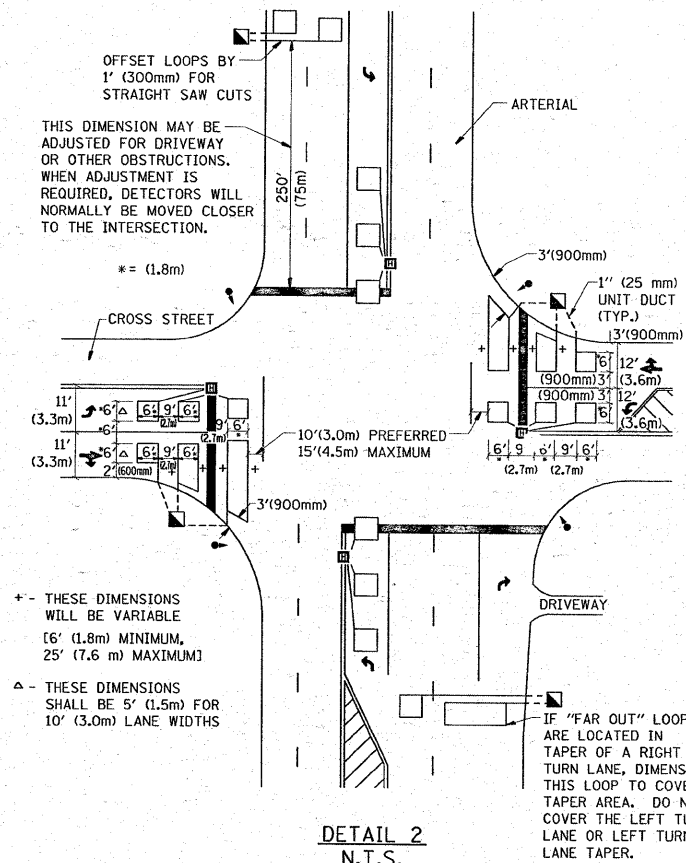
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2  
N.T.S.

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

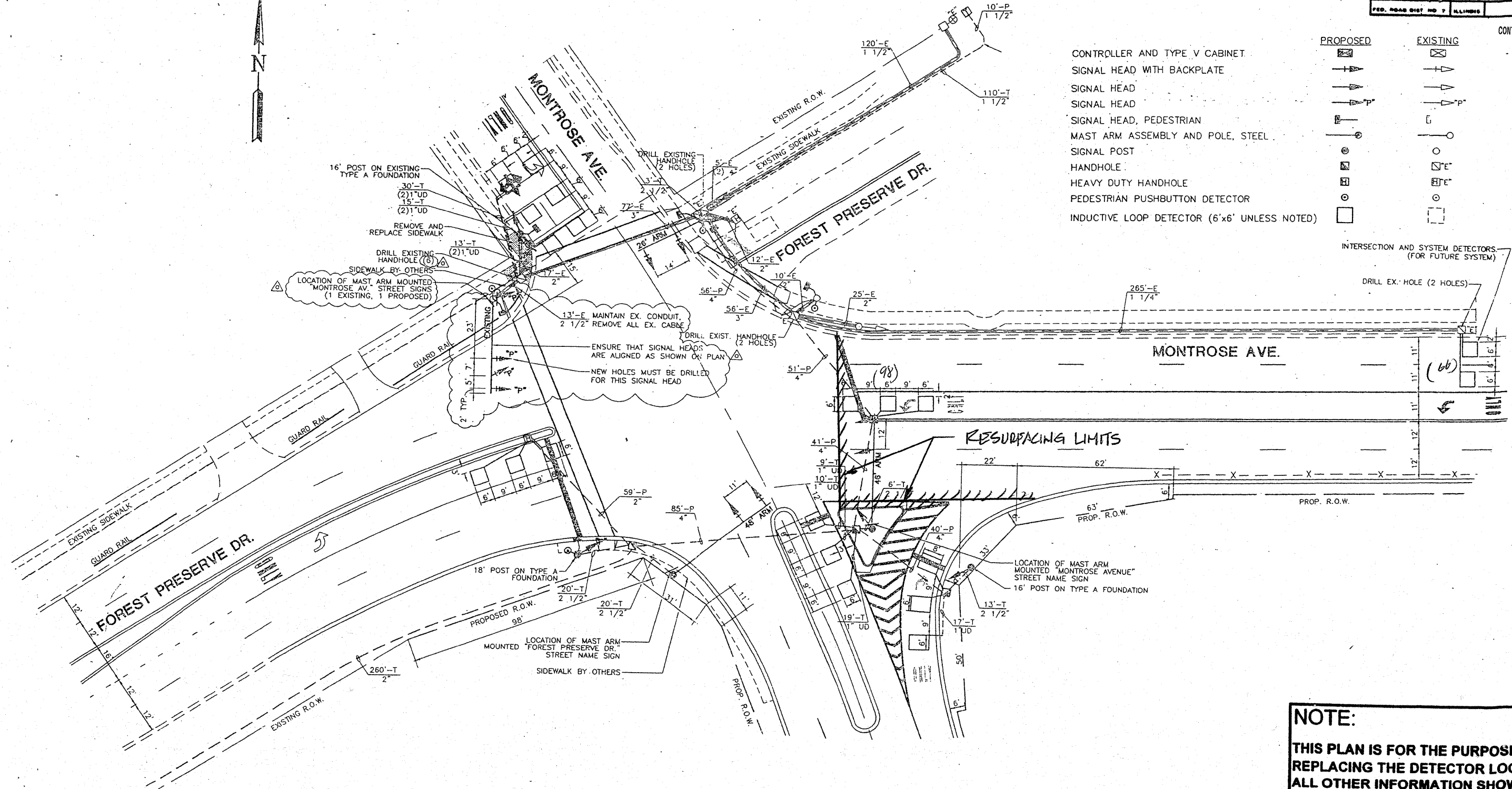
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>		F.A.U. RTE. 1366	SECTION 0101RS-5	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 30
CD:\pwork\pudot\stedpa\401025\1113725\1113725.dwg	PLD SCALE = 32.0000 1/4 IN.	DRAWN	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	<b>TS-07</b>		CONTRACT NO. 60B60	
	PLD DATE = 3/25/2009	CHECKED R.K.F.	REVISED -				TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
		DATE	REVISED -								

P.A. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1366	0101 RS-5	COOK	34	31
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

CONTRACT NO: 60860



- CONTROLLER AND TYPE V CABINET  
 SIGNAL HEAD WITH BACKPLATE  
 SIGNAL HEAD  
 SIGNAL HEAD  
 SIGNAL HEAD, PEDESTRIAN  
 MAST ARM ASSEMBLY AND POLE, STEEL  
 SIGNAL POST  
 HANDHOLE  
 HEAVY DUTY HANDHOLE  
 PEDESTRIAN PUSHBUTTON DETECTOR  
 INDUCTIVE LOOP DETECTOR (6'x6' UNLESS NOTED)
- PROPOSED**
- EXISTING**
- INTERSECTION AND SYSTEM DETECTORS (FOR FUTURE SYSTEM)

**REPLACE ALL DETECTOR LOOPS AS SHOWN**  
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
80600600	164	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

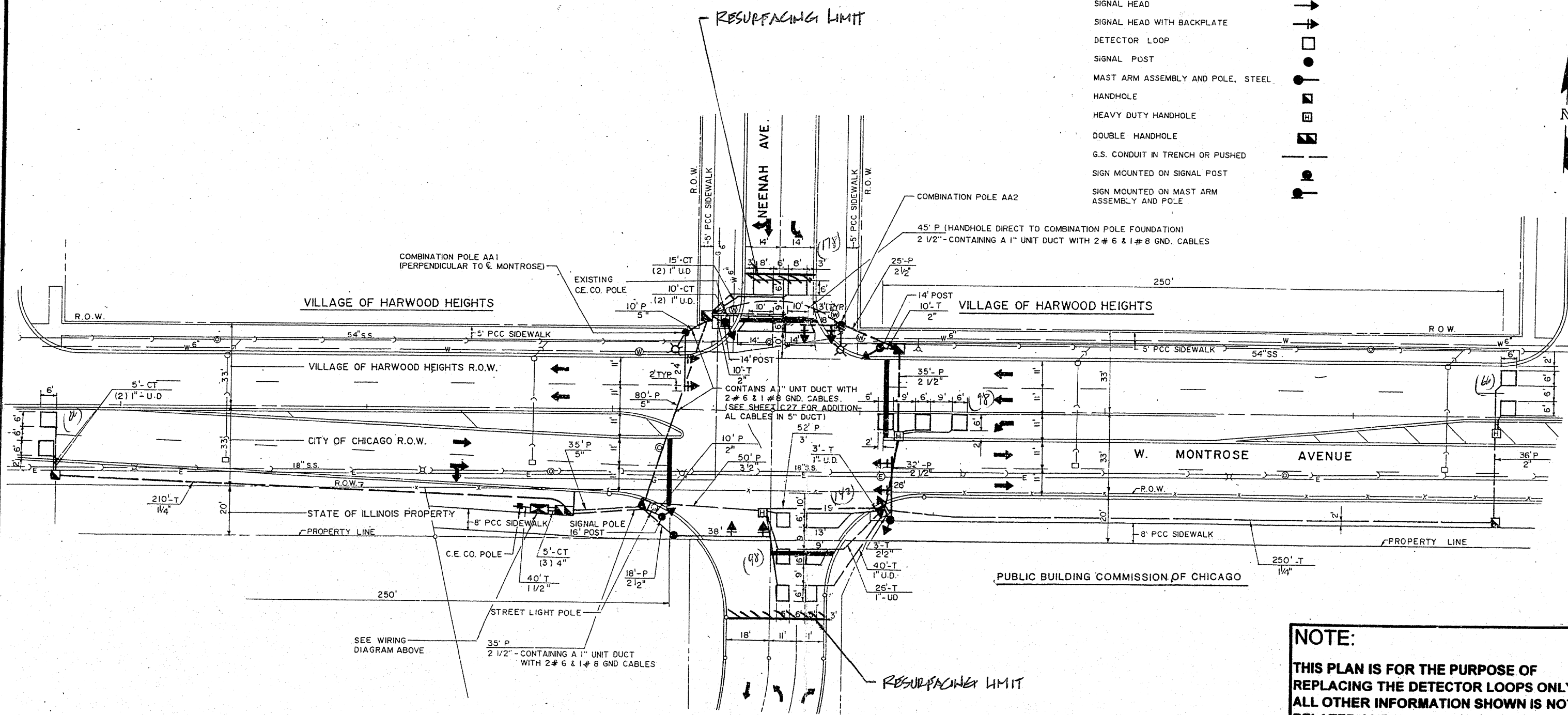
**NOTE:**  
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETECTOR LOOP REPLACEMENT**  
 MONTROSE AV. @ FOREST PRESERVE  
 SCALE: NONE  
 DATE: FEB. 2009  
 DRAWN BY: JHE  
 DESIGNED BY: JHE  
 CHECKED BY: DAD

F.P. SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1366	0101 RS-5	Cook	34	32
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

CONTRACT NO.: 60B60

- PROPOSED
- CONTROLLER
  - SERVICE INSTALLATION
  - SIGNAL HEAD
  - SIGNAL HEAD WITH BACKPLATE
  - DETECTOR LOOP
  - SIGNAL POST
  - MAST ARM ASSEMBLY AND POLE, STEEL
  - HANDHOLE
  - HEAVY DUTY HANDHOLE
  - DOUBLE HANDHOLE
  - G.S. CONDUIT IN TRENCH OR PUSHED
  - SIGN MOUNTED ON SIGNAL POST
  - SIGN MOUNTED ON MAST ARM ASSEMBLY AND POLE



**NOTE:**  
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

**REPLACE ALL DETECTOR LOOPS AS SHOWN**  
 (WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
80600600	648	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETECTOR LOOP REPLACEMENT**  
 MONTROSE AV. @ NEENAH AV.  
 SCALE: NONE  
 DATE: FEB. 2009  
 DRAWN BY: JHE  
 DESIGNED BY: JHE  
 CHECKED BY: DAD



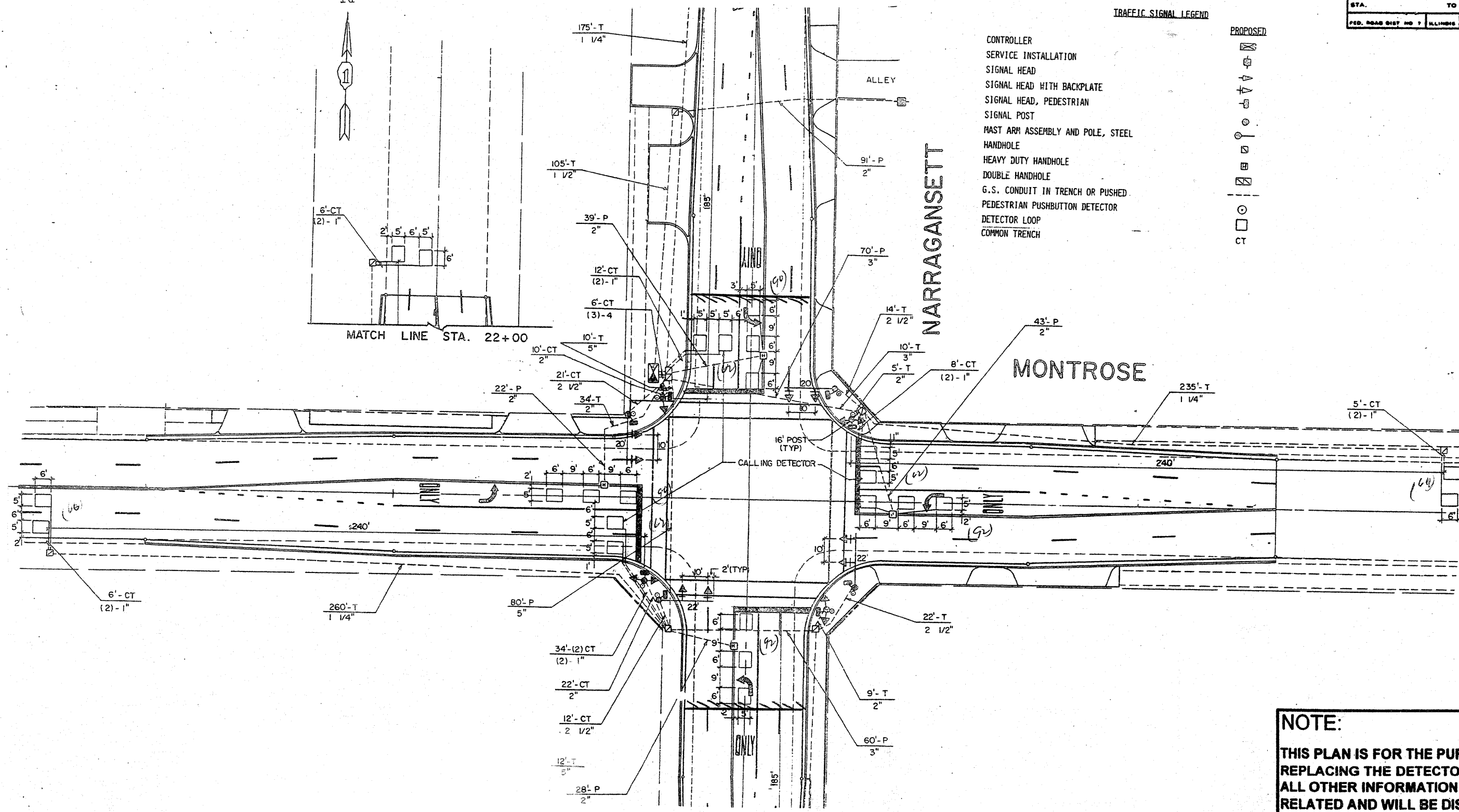
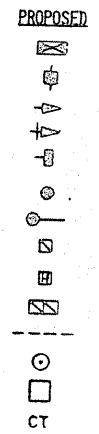
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0101 RS-5	COOK	34	33
STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS	
		FED. AID PROJECT	

CONTRACT NO: 60B60



TRAFFIC SIGNAL LEGEND

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- G.S. CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSHBUTTON DETECTOR
- DETECTOR LOOP
- COMMON TRENCH



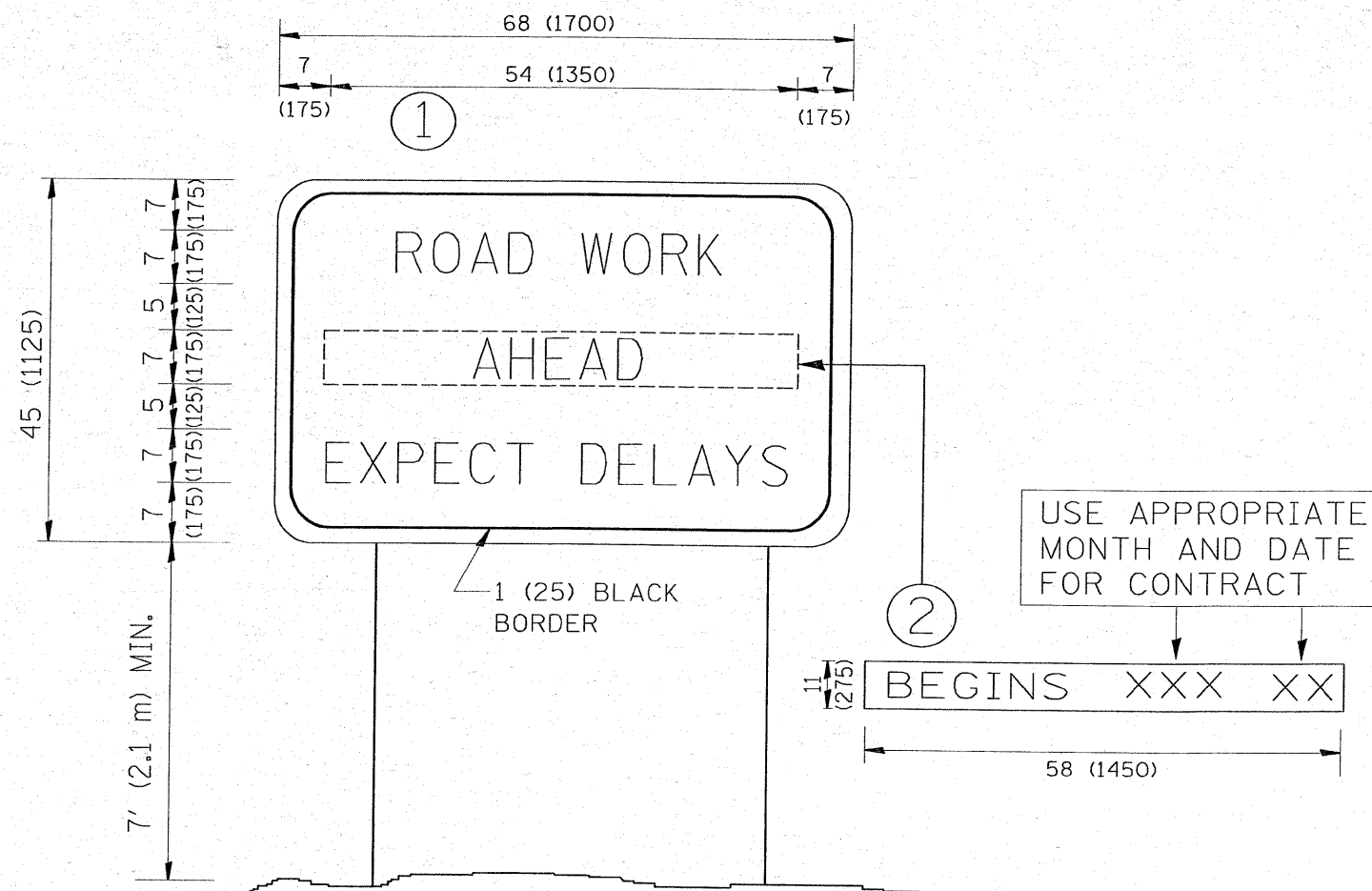
**REPLACE ALL DETECTOR LOOPS AS SHOWN**  
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	662	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

**NOTE:**  
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETECTOR LOOP REPLACEMENT**  
MONTROSE AV. @ NARRAGANSETT  
SCALE: NONE  
DATE: FEB, 2009  
DRAWN BY: JHE  
DESIGNED BY: JHE  
CHECKED BY: DAD



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = steedpa	DESIGNED -	REVISED - R. MIRS 09-15-97.
c:\pw_work\pwsdot\stendpa\d0100515\0137336-sh-t-plan.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
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PLOT DATE = 3/25/2009		DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 1366	SECTION 010IRS-5	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 34
TC-22			CONTRACT NO. 60B60	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				