

**If you plan to submit a bid directly to the Department of Transportation**

**PREQUALIFICATION**

Any contractor who desires to become pre-qualified to bid on work advertised by IDOT must submit the properly completed pre-qualification forms to the Bureau of Construction no later than 4:30 p.m. prevailing time twenty-one days prior to the letting of interest. This pre-qualification requirement applies to first time contractors, contractors renewing expired ratings, contractors maintaining continuous pre-qualification or contractors requesting revised ratings. To be eligible to bid, existing pre-qualification ratings must be effective through the date of letting.

**REQUESTS FOR AUTHORIZATION TO BID**

Contractors wanting to bid on items included in a particular letting must submit the properly completed "Request for Authorization to Bid/or Not For Bid Status" (BDE 124INT) and the ORIGINAL "Affidavit of Availability" (BC 57) to the proper office no later than 4:30 p.m. prevailing time, three (3) days prior to the letting date.

**WHO CAN BID ?**

Bids will be accepted from only those companies that request and receive written **Authorization to Bid** from IDOT's Central Bureau of Construction.

**WHAT CONSTITUTES WRITTEN AUTHORIZATION TO BID?:** When a prospective prime bidder submits a "Request for Authorization to Bid/or Not For Bid Status"(BDE 124INT) he/she must indicate at that time which items are being requested For Bidding purposes. Only those items requested For Bidding will be analyzed. After the request has been analyzed, the bidder will be issued a **Proposal Denial and/or Authorization Form**, approved by the Central Bureau of Construction, that indicates which items have been approved For Bidding. If **Authorization to Bid** cannot be approved, the **Proposal Denial and/or Authorization Form** will indicate the reason for denial.

**ABOUT AUTHORIZATION TO BID:** Firms that have not received an authorization form within a reasonable time of complete and correct original document submittal should contact the department as to status. This is critical in the week before the letting. These documents must be received three days before the letting date. Firms unsure as to authorization status should call the Prequalification Section of the Bureau of Construction at the number listed at the end of these instructions.

**ADDENDA AND REVISIONS:** It is the contractor's responsibility to determine which, if any, addenda or revisions pertain to any project they may be bidding. Failure to incorporate all relevant addenda or revisions may cause the bid to be declared unacceptable.

Each addendum will be placed with the contract number. Addenda and revisions will also be placed on the Addendum/Revision Checklist and each subscription service subscriber will be notified by e-mail of each addendum and revision issued.

The Internet is the Department's primary way of doing business. The subscription server e-mails are an added courtesy the Department provides. It is suggested that bidders check IDOT's website at <http://www.dot.il.gov/desenv/delett.html> before submitting final bid information.

***IDOT IS NOT RESPONSIBLE FOR ANY E-MAIL FAILURES.***

Addenda Questions may be directed to the Contracts Office at (217)782-7806 or [D&Econtracts@dot.il.gov](mailto:D&Econtracts@dot.il.gov)

Technical Questions about downloading these files may be directed to Tim Garman (217)524-1642 or [Timothy.Garman@illinois.gov](mailto:Timothy.Garman@illinois.gov).

**WHAT MUST BE INCLUDED WHEN BIDS ARE SUBMITTED?:** Bidders need not return the entire proposal when bids are submitted. That portion of the proposal that must be returned includes the following:

1. All documents from the Proposal Cover Sheet through the Proposal Bid Bond
2. Other special documentation and/or information that may be required by the contract special provisions

All proposal documents, including Proposal Guaranty Checks or Proposal Bid Bonds, should be stapled together to prevent loss when bids are processed by IDOT personnel.

**ABOUT SUBMITTING BIDS:** It is recommended that bidders deliver bids in person to insure they arrive at the proper location prior to the time specified for the receipt of bids. Any bid received at the place of letting after the time specified will not be accepted.

**WHO SHOULD BE CALLED IF ASSISTANCE IS NEEDED?**

| <b>Questions Regarding</b>                   | <b>Call</b>  |
|--|--------------|
| Prequalification and/or Authorization to Bid | 217/782-3413 |
| Preparation and submittal of bids            | 217/782-7806 |
| Mailing of plans and proposals               | 217/782-7806 |

**ADDENDUMS AND REVISIONS TO THE PROPOSAL FORMS**

Planholders should verify that they have received and incorporated any addendum and/or revision prior to submitting their bid. Failure by the bidder to include an addendum or revision could result in a bid being rejected as irregular.

RETURN WITH BID

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|                       |
|-----------------------|
| Proposal Submitted By |
| Name                  |
| Address               |
| City                  |

Letting May 15, 2009

BIDDERS NEED NOT RETURN THE ENTIRE PROPOSAL  
(See instructions inside front cover)

**NOTICE TO PROSPECTIVE BIDDERS**

This proposal can be used for bidding purposes by only those companies that request and receive written AUTHORIZATION TO BID from IDOT's Central Bureau of Construction.  
(SEE INSTRUCTIONS ON THE INSIDE OF COVER)

**Notice To Bidders,  
Specifications,  
Proposal, Contract  
and Contract Bond**



**Illinois Department  
of Transportation**

Springfield, Illinois 62764

**Contract No. 78108  
JEFFERSON County  
Section (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
District 9 Construction Funds  
Route FAP 724/FAS 2869**

PLEASE MARK THE APPROPRIATE BOX BELOW:

- A Bid Bond is included.
- A Cashier's Check or a Certified Check is included.

Plans Included  
Herein

|             |   |
|-------------|---|
| Prepared by |   |
| Checked by  | S |

(Printed by authority of the State of Illinois)

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## INSTRUCTIONS

**ABOUT IDOT PROPOSALS:** All proposals issued by IDOT are potential bidding proposals. Each proposal contains all Certifications and Affidavits, a Proposal Signature Sheet and a Proposal Bid Bond required for Prime Contractors to submit a bid after written **Authorization to Bid** has been issued by IDOT's Central Bureau of Construction.

**WHO CAN BID?:** Bids will be accepted from only those companies that request and receive written **Authorization to Bid** from IDOT's Central Bureau of Construction. To request authorization, a potential bidder must complete and submit Part B of the Request for Authorization to Bid/or Not For Bid Status form (BDE 124 INT) and submit an original Affidavit of Availability (BC 57).

**WHAT CONSTITUTES WRITTEN AUTHORIZATION TO BID?:** When a prospective prime bidder submits a "Request for Proposal Forms and Plans" he/she must indicate at that time which items are being requested For Bidding purposes. Only those items requested For Bidding will be analyzed. After the request has been analyzed, the bidder will be issued a **Proposal Denial and/or Authorization Form**, approved by the Central Bureau of Construction, that indicates which items have been approved For Bidding. If **Authorization to Bid** cannot be approved, the **Proposal Denial and/or Authorization Form** will indicate the reason for denial. If a contractor has requested to bid but has not received a **Proposal Denial and/or Authorization Form**, they should contact the Central Bureau of Construction in advance of the letting date.

**WHAT MUST BE INCLUDED WHEN BIDS ARE SUBMITTED?:** Bidders need not return the entire proposal when bids are submitted. That portion of the proposal that must be returned includes the following:

1. All documents from the Proposal Cover Sheet through the Proposal Bid Bond
2. Other special documentation and/or information that may be required by the contract special provisions

All proposal documents, including Proposal Guaranty Checks or Proposal Bid Bonds, should be stapled together to prevent loss when bids are processed by IDOT personnel.

**ABOUT SUBMITTING BIDS:** It is recommended that bidders deliver bids in person to insure they arrive at the proper location prior to the time specified for the receipt of bids. Any bid received at the place of letting after the time specified will not be accepted.

### WHO SHOULD BE CALLED IF ASSISTANCE IS NEEDED?

| Questions Regarding                          | Call         |
|--|--------------|
| Prequalification and/or Authorization to Bid | 217/782-3413 |
| Preparation and submittal of bids            | 217/782-7806 |
| Mailing of CD-ROMS                           | 217/782-7806 |

RETURN WITH BID



PROPOSAL

TO THE DEPARTMENT OF TRANSPORTATION

1. Proposal of \_\_\_\_\_

\_\_\_\_\_

Taxpayer Identification Number (Mandatory) \_\_\_\_\_ a

for the improvement identified and advertised for bids in the Invitation for Bids as:

**Contract No. 78108  
JEFFERSON County  
Section (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
Route FAP 724/FAS 2869  
District 9 Construction Funds**

**11.93 miles of resurfacing on IL 37 from south of IL 142 to south of the Jefferson/Franklin County line.**

2. The undersigned bidder will furnish all labor, material and equipment to complete the above described project in a good and workmanlike manner as provided in the contract documents provided by the Department of Transportation. This proposal will become part of the contract and the terms and conditions contained in the contract documents shall govern performance and payments.

**RETURN WITH BID**

3. **ASSURANCE OF EXAMINATION AND INSPECTION/WAIVER.** The undersigned further declares that he/she has carefully examined the proposal, plans, specifications, form of contract and contract bond, and special provisions, and that he/she has inspected in detail the site of the proposed work, and that he/she has familiarized themselves with all of the local conditions affecting the contract and the detailed requirements of construction, and understands that in making this proposal he/she waives all right to plead any misunderstanding regarding the same.
4. **EXECUTION OF CONTRACT AND CONTRACT BOND.** The undersigned further agrees to execute a contract for this work and present the same to the department within fifteen (15) days after the contract has been mailed to him/her. The undersigned further agrees that he/she and his/her surety will execute and present within fifteen (15) days after the contract has been mailed to him/her contract bond satisfactory to and in the form prescribed by the Department of Transportation, in the penal sum of the full amount of the contract, guaranteeing the faithful performance of the work in accordance with the terms of the contract.
5. **PROPOSAL GUARANTY.** Accompanying this proposal is either a bid bond on the department form, executed by a corporate surety company satisfactory to the department, or a proposal guaranty check consisting of a bank cashier's check or a properly certified check for not less than 5 per cent of the amount bid or for the amount specified in the following schedule:

| <u>Amount of Bid</u> |               | <u>Proposal Guaranty</u> | <u>Amount of Bid</u> |    | <u>Proposal Guaranty</u> |             |
|----------------------|---------------|--------------------------|----------------------|----|--------------------------|-------------|
| Up to                | \$5,000 ..... | \$150                    | \$2,000,000          | to | \$3,000,000 .....        | \$100,000   |
| \$5,000              | to            | \$10,000 .....           | \$3,000,000          | to | \$5,000,000 .....        | \$150,000   |
| \$10,000             | to            | \$50,000 .....           | \$5,000,000          | to | \$7,500,000 .....        | \$250,000   |
| \$50,000             | to            | \$100,000 .....          | \$7,500,000          | to | \$10,000,000 .....       | \$400,000   |
| \$100,000            | to            | \$150,000 .....          | \$10,000,000         | to | \$15,000,000 .....       | \$500,000   |
| \$150,000            | to            | \$250,000 .....          | \$15,000,000         | to | \$20,000,000 .....       | \$600,000   |
| \$250,000            | to            | \$500,000 .....          | \$20,000,000         | to | \$25,000,000 .....       | \$700,000   |
| \$500,000            | to            | \$1,000,000 .....        | \$25,000,000         | to | \$30,000,000 .....       | \$800,000   |
| \$1,000,000          | to            | \$1,500,000 .....        | \$30,000,000         | to | \$35,000,000 .....       | \$900,000   |
| \$1,500,000          | to            | \$2,000,000 .....        | over                 |    | \$35,000,000 .....       | \$1,000,000 |

Bank cashier's checks or properly certified checks accompanying proposals shall be made payable to the Treasurer, State of Illinois, when the state is awarding authority; the county treasurer, when a county is the awarding authority; or the city, village, or town treasurer, when a city, village, or town is the awarding authority.

If a combination bid is submitted, the proposal guaranties which accompany the individual proposals making up the combination will be considered as also covering the combination bid.

The amount of the proposal guaranty check is \_\_\_\_\_ \$( \_\_\_\_\_ ). If this proposal is accepted and the undersigned shall fail to execute a contract bond as required herein, it is hereby agreed that the amount of the proposal guaranty shall become the property of the State of Illinois, and shall be considered as payment of damages due to delay and other causes suffered by the State because of the failure to execute said contract and contract bond; otherwise, the bid bond shall become void or the proposal guaranty check shall be returned to the undersigned.

**Attach Cashier's Check or Certified Check Here**

In the event that one proposal guaranty check is intended to cover two or more proposals, the amount must be equal to the sum of the proposal guaranties which would be required for each individual proposal. If the guaranty check is placed in another proposal, state below where it may be found.

The proposal guaranty check will be found in the proposal for:

Item \_\_\_\_\_

Section No. \_\_\_\_\_

County \_\_\_\_\_

**Mark the proposal cover sheet as to the type of proposal guaranty submitted.**

**RETURN WITH BID**

6. **COMBINATION BIDS.** The undersigned further agrees that if awarded the contract for the sections contained in the following combination, he/she will perform the work in accordance with the requirements of each individual proposal comprising the combination bid specified in the schedule below, and that the combination bid shall be prorated against each section in proportion to the bid submitted for the same. If an error is found to exist in the gross sum bid for one or more of the individual sections included in a combination, the combination bid shall be corrected as provided in the specifications.

**When a combination bid is submitted, the schedule below must be completed in each proposal comprising the combination.**

**If alternate bids are submitted for one or more of the sections comprising the combination, a combination bid must be submitted for each alternate.**

**Schedule of Combination Bids**

| Combination No. | Sections Included in Combination | Combination Bid |       |
|-----------------|----------------------------------|-----------------|-------|
|                 |                                  | Dollars         | Cents |
|                 |                                  |                 |       |
|                 |                                  |                 |       |
|                 |                                  |                 |       |
|                 |                                  |                 |       |

7. **SCHEDULE OF PRICES.** The undersigned bidder submits herewith, in accordance with the rules and instructions, a schedule of prices for the items of work for which bids are sought. The unit prices bid are in U.S. dollars and cents, and all extensions and summations have been made. The bidder understands that the quantities appearing in the bid schedule are approximate and are provided for the purpose of obtaining a gross sum for the comparison of bids. If there is an error in the extension of the unit prices, the unit prices shall govern. Payment to the contractor awarded the contract will be made only for actual quantities of work performed and accepted or materials furnished according to the contract. The scheduled quantities of work to be done and materials to be furnished may be increased, decreased or omitted as provided elsewhere in the contract.

8. **CERTIFICATE OF AUTHORITY.** The undersigned bidder, if a business organized under the laws of another State, assures the Department that it will furnish a copy of its certificate of authority to do business in the State of Illinois with the return of the executed contract and bond. Failure to furnish the certificate within the time provided for execution of an awarded contract may be cause for cancellation of the award and forfeiture of the proposal guaranty to the State.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SCHEDULE OF PRICES  
 CONTRACT  
 NUMBER - 78108

State Job # - C-99-028-09  
 PPS NBR - 9-00273-0000  
 County Name - JEFFERSON- -  
 Code - 81 - -  
 District - 9 - -  
 Section Number - (41-8,4-2)RS-1;(1,4)RS-3;2RS-4

Project Number

Route  
 FAP 724  
 FAS 2869

| Item Number | Pay Item Description  | Unit of Measure | Quantity    | x | Unit Price | = | Total Price |
|-------------|-----------------------|-----------------|-------------|---|------------|---|-------------|
| XX005496    | TRAF BAR TERM T6A SPL | EACH            | 1.000       |   |            |   |             |
| Z0017100    | DOWEL BARS            | EACH            | 320.000     |   |            |   |             |
| Z0030150    | IMPACT ATTEN NRD TL3  | EACH            | 4.000       |   |            |   |             |
| Z0030350    | IMP ATTN REL NRD TL3  | EACH            | 16.000      |   |            |   |             |
| Z0048665    | RR PROT LIABILITY INS | L SUM           | 1.000       |   |            |   |             |
| 40600100    | BIT MATLS PR CT       | GALLON          | 15,979.000  |   |            |   |             |
| 40600300    | AGG PR CT             | TON             | 249.000     |   |            |   |             |
| 40600400    | MIX CR JTS FLANGEWYS  | TON             | 25.000      |   |            |   |             |
| 40600895    | CONSTRUC TEST STRIP   | EACH            | 1.000       |   |            |   |             |
| 40600985    | PCC SURF REM BUTT JT  | SQ YD           | 314.000     |   |            |   |             |
| 40600990    | TEMPORARY RAMP        | SQ YD           | 193.000     |   |            |   |             |
| 40603345    | HMA SC "D" N90        | TON             | 14,096.000  |   |            |   |             |
| 44000155    | HMA SURF REM 1 1/2    | SQ YD           | 176,849.000 |   |            |   |             |
| 44000198    | HMA SURF REM VAR DP   | SQ YD           | 2,706.000   |   |            |   |             |
| 44200176    | PAVT PATCH T1 15      | SQ YD           | 14.000      |   |            |   |             |

ILLINOIS DEPARTMENT OF TRANSPORTATION  
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| Item Number | Pay Item Description  | Unit of Measure | Quantity   | x | Unit Price | = | Total Price |
|-------------|-----------------------|-----------------|------------|---|------------|---|-------------|
| 44200180    | PAVT PATCH T2 15      | SQ YD           | 2,750.000  |   |            |   |             |
| 44200184    | PAVT PATCH T3 15      | SQ YD           | 1,435.000  |   |            |   |             |
| 44200186    | PAVT PATCH T4 15      | SQ YD           | 1,931.000  |   |            |   |             |
| 44200970    | CL B PATCH T2 10      | SQ YD           | 300.000    |   |            |   |             |
| 44213200    | SAW CUTS              | FOOT            | 938.000    |   |            |   |             |
| 44300200    | STRIP REF CR CON TR   | FOOT            | 94,390.000 |   |            |   |             |
| 48101200    | AGGREGATE SHLDS B     | TON             | 1,934.000  |   |            |   |             |
| 48203100    | HMA SHOULDERS         | TON             | 937.000    |   |            |   |             |
| 50104700    | HANDRAIL CONC REM     | FOOT            | 258.000    |   |            |   |             |
| 50900200    | STEEL RAIL TYPE 2399  | FOOT            | 258.000    |   |            |   |             |
| 63000002    | SPBGR TY A 6.75 POSTS | FOOT            | 675.000    |   |            |   |             |
| 63100045    | TRAF BAR TERM T2      | EACH            | 7.000      |   |            |   |             |
| 63100087    | TRAF BAR TERM T6A     | EACH            | 19.000     |   |            |   |             |
| 63100167    | TR BAR TRM T1 SPL TAN | EACH            | 15.000     |   |            |   |             |
| 63200305    | SPBGR REM             | FOOT            | 847.750    |   |            |   |             |

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SCHEDULE OF PRICES  
 CONTRACT  
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State Job # - C-99-028-09  
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Project Number

Route  
 FAP 724  
 FAS 2869

| Item Number | Pay Item Description  | Unit of Measure | Quantity    | x | Unit Price | = | Total Price |
|-------------|-----------------------|-----------------|-------------|---|------------|---|-------------|
| 63300725    | SPBGR (SHORT RADIUS)  | FOOT            | 22.000      |   |            |   |             |
| 67100100    | MOBILIZATION          | L SUM           | 1.000       |   |            |   |             |
| 70100450    | TRAF CONT-PROT 701201 | L SUM           | 1.000       |   |            |   |             |
| 70100460    | TRAF CONT-PROT 701306 | L SUM           | 1.000       |   |            |   |             |
| 70100600    | TRAF CONT-PROT 701336 | L SUM           | 1.000       |   |            |   |             |
| 70101805    | TRAF CONT & PROT SPL  | EACH            | 10.000      |   |            |   |             |
| 70102620    | TR CONT & PROT 701501 | L SUM           | 1.000       |   |            |   |             |
| 70106800    | CHANGEABLE MESSAGE SN | CAL MO          | 8.000       |   |            |   |             |
| 70300100    | SHORT-TERM PAVT MKING | FOOT            | 13,240.000  |   |            |   |             |
| 70300210    | TEMP PVT MK LTR & SYM | SQ FT           | 252.000     |   |            |   |             |
| 70300220    | TEMP PVT MK LINE 4    | FOOT            | 171,950.000 |   |            |   |             |
| 70300260    | TEMP PVT MK LINE 12   | FOOT            | 760.000     |   |            |   |             |
| 70300280    | TEMP PVT MK LINE 24   | FOOT            | 169.000     |   |            |   |             |
| 70301000    | WORK ZONE PAVT MK REM | SQ FT           | 4,413.000   |   |            |   |             |
| 70400100    | TEMP CONC BARRIER     | FOOT            | 275.000     |   |            |   |             |





## RETURN WITH BID

### STATE REQUIRED ETHICAL STANDARDS GOVERNING CONTRACT PROCUREMENT: ASSURANCES, CERTIFICATIONS AND DISCLOSURES

#### I. GENERAL

A. Article 50 of the Illinois Procurement Code establishes the duty of all State chief procurement officers, State purchasing officers, and their designees to maximize the value of the expenditure of public moneys in procuring goods, services, and contracts for the State of Illinois and to act in a manner that maintains the integrity and public trust of State government. In discharging this duty, they are charged by law to use all available information, reasonable efforts, and reasonable actions to protect, safeguard, and maintain the procurement process of the State of Illinois.

B. In order to comply with the provisions of Article 50 and to carry out the duty established therein, all bidders are to adhere to ethical standards established for the procurement process, and to make such assurances, disclosures and certifications required by law. By execution of the Proposal Signature Sheet, the bidder indicates that each of the mandated assurances has been read and understood, that each certification is made and understood, and that each disclosure requirement has been understood and completed.

C. In addition to all other remedies provided by law, failure to comply with any assurance, failure to make any disclosure or the making of a false certification shall be grounds for termination of the contract and the suspension or debarment of the bidder.

#### II. ASSURANCES

A. The assurances hereinafter made by the bidder are each a material representation of fact upon which reliance is placed should the Department enter into the contract with the bidder. The Department may terminate the contract if it is later determined that the bidder rendered a false or erroneous assurance, and the surety providing the performance bond shall be responsible for the completion of the contract.

##### B. Felons

1. The Illinois Procurement Code provides:

Section 50-10. Felons. Unless otherwise provided, no person or business convicted of a felony shall do business with the State of Illinois or any state agency from the date of conviction until 5 years after the date of completion of the sentence for that felony, unless no person held responsible by a prosecutorial office for the facts upon which the conviction was based continues to have any involvement with the business.

2. The bidder assures the Department that the award and execution of the contract would not cause a violation of Section 50-10.

##### C. Conflicts of Interest

1. The Illinois Procurement Code provides in pertinent part:

Section 50-13. Conflicts of Interest.

(a) Prohibition. It is unlawful for any person holding an elective office in this State, holding a seat in the General Assembly, or appointed to or employed in any of the offices or agencies of state government and who receives compensation for such employment in excess of 60% of the salary of the Governor of the State of Illinois, or who is an officer or employee of the Capital Development Board or the Illinois Toll Highway Authority, or who is the spouse or minor child of any such person to have or acquire any contract, or any direct pecuniary interest in any contract therein, whether for stationery, printing, paper, or any services, materials, or supplies, that will be wholly or partially satisfied by the payment of funds appropriated by the General Assembly of the State of Illinois or in any contract of the Capital Development Board or the Illinois Toll Highway authority.

(b) Interests. It is unlawful for any firm, partnership, association or corporation, in which any person listed in subsection (a) is entitled to receive (i) more than 7 1/2% of the total distributable income or (ii) an amount in excess of the salary of the Governor, to have or acquire any such contract or direct pecuniary interest therein.

(c) Combined interests. It is unlawful for any firm, partnership, association, or corporation, in which any person listed in subsection (a) together with his or her spouse or minor children is entitled to receive (i) more than 15%, in the aggregate, of the total distributable income or (ii) an amount in excess of 2 times the salary of the Governor, to have or acquire any such contract or direct pecuniary interest therein.

(d) Securities. Nothing in this Section invalidates the provisions of any bond or other security previously offered or to be offered for sale or sold by or for the State of Illinois.

(e) Prior interests. This Section does not affect the validity of any contract made between the State and an officer or employee of the State or member of the General Assembly, his or her spouse, minor child or any combination of those persons if that contract was in existence before his or her election or employment as an officer, member, or employee. The contract is voidable, however, if it cannot be completed within 365 days after the officer, member, or employee takes office or is employed.

The current salary of the Governor is \$177,412.00. Sixty percent of the salary is \$106,447.20.

## RETURN WITH BID

2. The bidder assures the Department that the award and execution of the contract would not cause a violation of Section 50-13, or that an effective exemption has been issued by the Board of Ethics to any individual subject to the Section 50-13 prohibitions pursuant to the provisions of Section 50-20 of the Code and Executive Order Number 3 (1998). Information concerning the exemption process is available from the Department upon request.

### **D. Negotiations**

1. The Illinois Procurement Code provides in pertinent part:

Section 50-15. Negotiations.

(a) It is unlawful for any person employed in or on a continual contractual relationship with any of the offices or agencies of State government to participate in contract negotiations on behalf of that office or agency with any firm, partnership, association, or corporation with whom that person has a contract for future employment or is negotiating concerning possible future employment.

2. The bidder assures the Department that the award and execution of the contract would not cause a violation of Section 50-15, and that the bidder has no knowledge of any facts relevant to the kinds of acts prohibited therein.

### **E. Inducements**

1. The Illinois Procurement Code provides:

Section 50-25. Inducement. Any person who offers or pays any money or other valuable thing to any person to induce him or her not to bid for a State contract or as recompense for not having bid on a State contract is guilty of a Class 4 felony. Any person who accepts any money or other valuable thing for not bidding for a State contract or who withholds a bid in consideration of the promise for the payment of money or other valuable thing is guilty of a Class 4 felony.

2. The bidder assures the Department that the award and execution of the contract would not cause a violation of Section 50-25, and that the bidder has no knowledge of any facts relevant to the kinds of acts prohibited therein.

### **F. Revolving Door Prohibition**

1. The Illinois Procurement Code provides:

Section 50-30. Revolving door prohibition. Chief procurement officers, associate procurement officers, State purchasing officers, their designees whose principal duties are directly related to State procurement, and executive officers confirmed by the Senate are expressly prohibited for a period of 2 years after terminating an affected position from engaging in any procurement activity relating to the State agency most recently employing them in an affected position for a period of at least 6 months. The prohibition includes, but is not limited to: lobbying the procurement process; specifying; bidding; proposing bid, proposal, or contract documents; on their own behalf or on behalf of any firm, partnership, association, or corporation. This Section applies only to persons who terminate an affected position on or after January 15, 1999.

2. The bidder assures the Department that the award and execution of the contract would not cause a violation of Section 50-30, and that the bidder has no knowledge of any facts relevant to the kinds of acts prohibited therein.

### **G. Reporting Anticompetitive Practices**

1. The Illinois Procurement Code provides:

Section 50-40. Reporting anticompetitive practices. When, for any reason, any vendor, bidder, contractor, chief procurement officer, State purchasing officer, designee, elected official, or State employee suspects collusion or other anticompetitive practice among any bidders, offerors, contractors, proposers, or employees of the State, a notice of the relevant facts shall be transmitted to the Attorney General and the chief procurement officer.

2. The bidder assures the Department that it has not failed to report any relevant facts concerning the practices addressed in Section 50-40 which may involve the contract for which the bid is submitted.

### **H. Confidentiality**

1. The Illinois Procurement Code provides:

Section 50-45. Confidentiality. Any chief procurement officer, State purchasing officer, designee, or executive officer who willfully uses or allows the use of specifications, competitive bid documents, proprietary competitive information, proposals, contracts, or selection information to compromise the fairness or integrity of the procurement, bidding, or contract process shall be subject to immediate dismissal, regardless of the Personnel code, any contract, or any collective bargaining agreement, and may in addition be subject to criminal prosecution.

2. The bidder assures the Department that it has no knowledge of any fact relevant to the practices addressed in Section 50-45 which may involve the contract for which the bid is submitted.

## RETURN WITH BID

### **I. Insider Information**

1. The Illinois Procurement Act provides:

Section 50-50. Insider information. It is unlawful for any current or former elected or appointed State official or State employee to knowingly use confidential information available only by virtue of that office or employment for actual or anticipated gain for themselves or another person.

2. The bidder assures the Department that it has no knowledge of any facts relevant to the practices addressed in Section 50-50 which may involve the contract for which the bid is submitted.

### **III. CERTIFICATIONS**

**A.** The certifications hereinafter made by the bidder are each a material representation of fact upon which reliance is placed should the Department enter into the contract with the bidder. The Department may terminate the contract if it is later determined that the bidder rendered a false or erroneous certification, and the surety providing the performance bond shall be responsible for completion of the contract.

### **B. Bribery**

1. The Illinois Procurement Code provides:

Section 50-5. Bribery.

(a) Prohibition. No person or business shall be awarded a contract or subcontract under this Code who:

(1) has been convicted under the laws of Illinois or any other state of bribery or attempting to bribe an officer or employee of the State of Illinois or any other state in that officer's or employee's official capacity; or

(2) has made an admission of guilt of that conduct that is a matter of record but has not been prosecuted for that conduct.

(b) Businesses. No business shall be barred from contracting with any unit of State or local government as a result of a conviction under this Section of any employee or agent of the business if the employee or agent is no longer employed by the business and:

(1) the business has been finally adjudicated not guilty; or

(2) the business demonstrates to the governmental entity with which it seeks to contract, and that entity finds that the commission of the offense was not authorized, requested, commanded, or performed by a director, officer, or high managerial agent on behalf of the business as provided in paragraph (2) of subsection (a) of Section 5-4 of the Criminal Code of 1961.

(c) Conduct on behalf of business. For purposes of this Section, when an official, agent, or employee of a business committed the bribery or attempted bribery on behalf of the business and in accordance with the direction or authorization of a responsible official of the business, the business shall be chargeable with the conduct.

(d) Certification. Every bid submitted to and contract executed by the State shall contain a certification by the contractor that the contractor is not barred from being awarded a contract or subcontract under this Section. A contractor who makes a false statement, material to the certification, commits a Class 3 felony.

2. The bidder certifies that it is not barred from being awarded a contract under Section 50.5.

### **C. Educational Loan**

1. Section 3 of the Educational Loan Default Act provides:

§ 3. No State agency shall contract with an individual for goods or services if that individual is in default, as defined in Section 2 of this Act, on an educational loan. Any contract used by any State agency shall include a statement certifying that the individual is not in default on an educational loan as provided in this Section.

2. The bidder, if an individual as opposed to a corporation, partnership or other form of business organization, certifies that the bidder is not in default on an educational loan as provided in Section 3 of the Act.

### **D. Bid-Rigging/Bid Rotating**

1. Section 33E-11 of the Criminal Code of 1961 provides:

§ 33E-11. (a) Every bid submitted to and public contract executed pursuant to such bid by the State or a unit of local government shall contain a certification by the prime contractor that the prime contractor is not barred from contracting with any unit of State or local government as a result of a violation of either Section 33E-3 or 33E-4 of this Article. The State and units of local government shall provide the appropriate forms for such certification.

## RETURN WITH BID

(b) A contractor who makes a false statement, material to the certification, commits a Class 3 felony.

A violation of Section 33E-3 would be represented by a conviction of the crime of bid-rigging which, in addition to Class 3 felony sentencing, provides that any person convicted of this offense or any similar offense of any state or the United States which contains the same elements as this offense shall be barred for 5 years from the date of conviction from contracting with any unit of State or local government. No corporation shall be barred from contracting with any unit of State or local government as a result of a conviction under this Section of any employee or agent of such corporation if the employee so convicted is no longer employed by the corporation and: (1) it has been finally adjudicated not guilty or (2) if it demonstrates to the governmental entity with which it seeks to contract and that entity finds that the commission of the offense was neither authorized, requested, commanded, nor performed by a director, officer or a high managerial agent in behalf of the corporation.

A violation of Section 33E-4 would be represented by a conviction of the crime of bid-rotating which, in addition to Class 2 felony sentencing, provides that any person convicted of this offense or any similar offense of any state or the United States which contains the same elements as this offense shall be permanently barred from contracting with any unit of State or local government. No corporation shall be barred from contracting with any unit of State or local government as a result of a conviction under this Section of any employee or agent of such corporation if the employee so convicted is no longer employed by the corporation and: (1) it has been finally adjudicated not guilty or (2) if it demonstrates to the governmental entity with which it seeks to contract and that entity finds that the commission of the offense was neither authorized, requested, commanded, nor performed by a director, officer or a high managerial agent in behalf of the corporation.

2. The bidder certifies that it is not barred from contracting with the Department by reason of a violation of either Section 33E-3 or Section 33E-4.

### **E. International Anti-Boycott**

1. Section 5 of the International Anti-Boycott Certification Act provides:

§ 5. State contracts. Every contract entered into by the State of Illinois for the manufacture, furnishing, or purchasing of supplies, material, or equipment or for the furnishing of work, labor, or services, in an amount exceeding the threshold for small purchases according to the purchasing laws of this State or \$10,000.00, whichever is less, shall contain certification, as a material condition of the contract, by which the contractor agrees that neither the contractor nor any substantially-owned affiliated company is participating or shall participate in an international boycott in violation of the provisions of the U.S. Export Administration Act of 1979 or the regulations of the U.S. Department of Commerce promulgated under that Act.

2. The bidder makes the certification set forth in Section 5 of the Act.

### **F. Drug Free Workplace**

1. The Illinois "Drug Free Workplace Act" applies to this contract and it is necessary to comply with the provisions of the "Act" if the contractor is a corporation, partnership, or other entity (including a sole proprietorship) which has 25 or more employees.

2. The bidder certifies that if awarded a contract in excess of \$5,000 it will provide a drug free workplace by:

(a) Publishing a statement notifying employees that the unlawful manufacture, distribution, dispensation, possession or use of a controlled substance, including cannabis, is prohibited in the contractor's workplace; specifying the actions that will be taken against employees for violations of such prohibition; and notifying the employee that, as a condition of employment on such contract, the employee shall abide by the terms of the statement, and notify the employer of any criminal drug statute conviction for a violation occurring in the workplace no later than five (5) days after such conviction.

(b) Establishing a drug free awareness program to inform employees about the dangers of drug abuse in the workplace; the contractor's policy of maintaining a drug free workplace; any available drug counseling, rehabilitation, and employee assistance programs; and the penalties that may be imposed upon employees for drug violations.

(c) Providing a copy of the statement required by subparagraph (1) to each employee engaged in the performance of the contract and to post the statement in a prominent place in the workplace.

(d) Notifying the Department within ten (10) days after receiving notice from an employee or otherwise receiving actual notice of the conviction of an employee for a violation of any criminal drug statute occurring in the workplace.

(e) Imposing or requiring, within 30 days after receiving notice from an employee of a conviction or actual notice of such a conviction, an appropriate personnel action, up to and including termination, or the satisfactory participation in a drug abuse assistance or rehabilitation program approved by a federal, state or local health, law enforcement or other appropriate agency.

(f) Assisting employees in selecting a course of action in the event drug counseling, treatment, and rehabilitation is required and indicating that a trained referral team is in place.

(g) Making a good faith effort to continue to maintain a drug free workplace through implementation of the actions and efforts stated in this certification.

## RETURN WITH BID

### **G. Debt Delinquency**

1. The Illinois Procurement Code provides:

Section 50-11 and 50-12. Debt Delinquency.

The contractor or bidder certifies that it, or any affiliate, is not barred from being awarded a contract under 30 ILCS 500. Section 50-11 prohibits a person from entering into a contract with a State agency if it knows or should know that it, or any affiliate, is delinquent in the payment of any debt to the State as defined by the Debt Collection Board. Section 50-12 prohibits a person from entering into a contract with a State agency if it, or any affiliate, has failed to collect and remit Illinois Use Tax on all sales of tangible personal property into the State of Illinois in accordance with the provisions of the Illinois Use Tax Act. The contractor further acknowledges that the contracting State agency may declare the contract void if this certification is false or if the contractor, or any affiliate, is determined to be delinquent in the payment of any debt to the State during the term of the contract.

### **H. Sarbanes-Oxley Act of 2002**

1. The Illinois Procurement Code provides:

Section 50-60(c).

The contractor certifies in accordance with 30 ILCS 500/50-10.5 that no officer, director, partner or other managerial agent of the contracting business has been convicted of a felony under the Sarbanes-Oxley Act of 2002 or a Class 3 or Class 2 felony under the Illinois Securities Law of 1953 for a period of five years prior to the date of the bid or contract. The contractor acknowledges that the contracting agency shall declare the contract void if this certification is false.

### **I. Addenda**

The contractor or bidder certifies that all relevant addenda have been incorporated in to this contract. Failure to do so may cause the bid to be declared unacceptable.

### **J. Section 42 of the Environmental Protection Act**

The contractor certifies in accordance with 30 ILCS 500/50-12 that the bidder or contractor is not barred from being awarded a contract under this Section which prohibits the bidding on or entering into contracts with the State of Illinois or a State agency by a person or business found by a court or the Pollution Control Board to have committed a willful or knowing violation of Section 42 of the Environmental Protection Act for a period of five years from the date of the order. The contractor acknowledges that the contracting agency may declare the contract void if this certification is false.

### **K. Apprenticeship and Training Certification (Does not apply to federal aid projects)**

In accordance with the provisions of Section 30-22 (6) of the Illinois Procurement Code, the bidder certifies that it is a participant, either as an individual or as part of a group program, in the approved apprenticeship and training programs applicable to each type of work or craft that the bidder will perform with its own forces. The bidder further certifies for work that will be performed by subcontract that each of its subcontractors submitted for approval either (a) is, at the time of such bid, participating in an approved, applicable apprenticeship and training program; or (b) will, prior to commencement of performance of work pursuant to this contract, begin participation in an approved apprenticeship and training program applicable to the work of the subcontract. The Department, at any time before or after award, may require the production of a copy of each applicable Certificate of Registration issued by the United States Department of Labor evidencing such participation by the contractor and any or all of its subcontractors. Applicable apprenticeship and training programs are those that have been approved and registered with the United States Department of Labor. The bidder shall list in the space below, the official name of the program sponsor holding the Certificate of Registration for all of the types of work or crafts in which the bidder is a participant and that will be performed with the bidder's forces. Types of work or craft work that will be subcontracted shall be included and listed as subcontract work. The list shall also indicate any type of work or craft job category that does not have an applicable apprenticeship or training program. **The bidder is responsible for making a complete report and shall make certain that each type of work or craft job category that will be utilized on the project as reported on the Construction Employee Workforce Projection (Form BC-1256) and returned with the bid is accounted for and listed.**

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The requirements of this certification and disclosure are a material part of the contract, and the contractor shall require this certification provision to be included in all approved subcontracts. In order to fulfill this requirement, it shall not be necessary that an applicable program sponsor be currently taking or that it will take applications for apprenticeship, training or employment during the performance of the work of this contract.

### **L. Executive Order Number 1 (2007) Regarding Lobbying on Government Procurements**

The bidder hereby warrants and certifies that they have complied and will comply with the requirements set forth in this Order. The requirements of this warrant and certification are a material part of the contract, and the contractor shall require this warrant and certification provision to be included in all approved subcontracts.

## RETURN WITH BID

### **M. Disclosure of Business Operations in Iran**

Section 50-36 of the Illinois Procurement Code, 30ILCS 500/50-36 provides that each bid, offer, or proposal submitted for a State contract shall include a disclosure of whether or not the Company acting as the bidder, offeror, or proposing entity, or any of its corporate parents or subsidiaries, within the 24 months before submission of the bid, offer, or proposal had business operations that involved contracts with or provision of supplies or services to the Government of Iran, companies in which the Government of Iran has any direct or indirect equity share, consortiums or projects commissioned by the Government of Iran, or companies involved in consortiums or projects commissioned by the Government of Iran and either of the following conditions apply:

- (1) More than 10% of the Company's revenues produced in or assets located in Iran involve oil-related activities or mineral-extraction activities; less than 75% of the Company's revenues produced in or assets located in Iran involve contracts with or provision of oil-related or mineral-extraction products or services to the Government of Iran or a project or consortium created exclusively by that government; and the Company has failed to take substantial action.
- (2) The Company has, on or after August 5, 1996, made an investment of \$20 million or more, or any combination of investments of at least \$10 million each that in the aggregate equals or exceeds \$20 million in any 12-month period, which directly or significantly contributes to the enhancement of Iran's ability to develop petroleum resources of Iran.

The terms "Business operations", "Company", "Mineral-extraction activities", "Oil-related activities", "Petroleum resources", and "Substantial action" are all defined in the Code.

Failure to make the disclosure required by the Code shall cause the bid, offer or proposal to be considered not responsive. The disclosure will be considered when evaluating the bid, offer, or proposal or awarding the contract. The name of each Company disclosed as doing business or having done business in Iran will be provided to the State Comptroller.

Check the appropriate statement:

Company has no business operations in Iran to disclose.

Company has business operations in Iran as disclosed the attached document.

### **N. Political Contributions and Registration with the State Board of Elections**

Sections 20-160 and 50-37 of the Illinois Procurement Code regulate political contributions from business entities and any affiliated entities or affiliated persons bidding on or contracting with the state. Generally under Section 50-37, any business entity, and any affiliated entity or affiliated person of the business entity, whose current year contracts with all state agencies exceed an awarded value of \$50,000, are prohibited from making any contributions to any political committees established to promote the candidacy of the officeholder responsible for the awarding of the contracts or any other declared candidate for that office for the duration of the term of office of the incumbent officeholder or a period 2 years after the termination of the contract, whichever is longer. Any business entity and affiliated entities or affiliated persons whose state contracts in the current year do not exceed an awarded value of \$50,000, but whose aggregate pending bids and proposals on state contracts exceed \$50,000, either alone or in combination with contracts not exceeding \$50,000, are prohibited from making any political contributions to any political committee established to promote the candidacy of the officeholder responsible for awarding the pending contract during the period beginning on the date the invitation for bids or request for proposals is issued and ending on the day after the date of award or selection if the entity was not awarded or selected. Section 20-160 requires certification of registration of affected business entities in accordance with procedures found in Section 9-35 of The Election Code.

By submission of a bid, the contractor business entity acknowledges and agrees that it has read and understands Sections 20-160 and 50-37 of the Illinois Procurement Code, and that it makes the following certification:

**The undersigned business entity certifies that it has registered as a business with the State Board of Elections and acknowledges a continuing duty to update the registration in accordance with the above referenced statutes. A copy of the certificate of registration shall be submitted with the bid. The bidder is cautioned that the Department will not award a contract without submission of the certificate of registration.**

These requirements and compliance with the above referenced statutory sections are a material part of the contract, and any breach thereof shall be cause to void the contract under Section 50-60 of the Illinois Procurement Code. This provision does not apply to Federal-aid contracts.

**TO BE RETURNED WITH BID**

**IV. DISCLOSURES**

**A.** The disclosures hereinafter made by the bidder are each a material representation of fact upon which reliance is placed should the Department enter into the contract with the bidder. The Department may terminate the contract if it is later determined that the bidder rendered a false or erroneous disclosure, and the surety providing the performance bond shall be responsible for completion of the contract.

**B. Financial Interests and Conflicts of Interest**

1. Section 50-35 of the Illinois Procurement Code provides that all bids of more than \$10,000 shall be accompanied by disclosure of the financial interests of the bidder. This disclosed information for the successful bidder, will be maintained as public information subject to release by request pursuant to the Freedom of Information Act.

The financial interests to be disclosed shall include ownership or distributive income share that is in excess of 5%, or an amount greater than 60% of the annual salary of the Governor, of the bidding entity or its parent entity, whichever is less, unless the contractor or bidder is a publicly traded entity subject to Federal 10K reporting, in which case it may submit its 10K disclosure in place of the prescribed disclosure. If a bidder is a privately held entity that is exempt from Federal 10K reporting, but has more than 400 shareholders, it may submit the information that Federal 10K companies are required to report, and list the names of any person or entity holding any ownership share that is in excess of 5%. The disclosure shall include the names, addresses, and dollar or proportionate share of ownership of each person making the disclosure, their instrument of ownership or beneficial relationship, and notice of any potential conflict of interest resulting from the current ownership or beneficial interest of each person making the disclosure having any of the relationships identified in Section 50-35 and on the disclosure form.

In addition, all disclosures shall indicate any other current or pending contracts, proposals, leases, or other ongoing procurement relationships the bidding entity has with any other unit of state government and shall clearly identify the unit and the contract, proposal, lease, or other relationship.

2. Disclosure Forms. Disclosure Form A is attached for use concerning the individuals meeting the above ownership or distributive share requirements. Subject individuals should be covered each by one form. In addition, a second form (Disclosure Form B) provides for the disclosure of current or pending procurement relationships with other (non-IDOT) state agencies. **The forms must be included with each bid or incorporated by reference.**

**C. Disclosure Form Instructions**

**Form A: For bidders that have previously submitted the information requested in Form A**

The Department has retained the Form A disclosures submitted by all bidders responding to these requirements for the April 24, 1998 or any subsequent letting conducted by the Department. The bidder has the option of submitting the information again or the bidder may check the following certification statement indicating that the information previously submitted by the bidder is, as of the date of submission, current and accurate. Before checking this certification, the bidder should carefully review its prior submissions to ensure the Certification is correct. If the Bidder checks the Certification, the Bidder should proceed to Form B instructions.

**CERTIFICATION STATEMENT**

|   |  |       |
|---|--|-------|
| <p><b>I have determined that the Form A disclosure information previously submitted is current and accurate, and all forms are hereby incorporated by reference in this bid. Any necessary additional forms or amendments to previously submitted forms are attached to this bid.</b></p> |  |       |
| <p>_____ (Bidding Company)</p>  |  |       |
| <input type="checkbox"/>  | _____                                  | _____ |
|   | Signature of Authorized Representative | Date  |

**Form A: For bidders who have NOT previously submitted the information requested in Form A**

If the bidder is a publicly traded entity subject to Federal 10K reporting, the 10K Report may be submitted to meet the requirements of Form A. If a bidder is a privately held entity that is exempt from Federal 10K reporting, but has more than 400 shareholders, it may submit the information that Federal 10K companies are required to report, and list the names of any person or entity holding any ownership share that is in excess of 5%. If a bidder is not subject to Federal 10K reporting, the bidder must determine if any individuals are required by law to complete a financial disclosure form. To do this, the bidder should answer each of the following questions. A "YES" answer indicates Form A must be completed. If the answer to each of the following questions is "NO", then the NOT APPLICABLE STATEMENT on the second page of Form A must be signed and dated by a person that is authorized to execute contracts for the bidding company. Note: These questions are for assistance only and are not required to be completed.

1. Does anyone in your organization have a direct or beneficial ownership share of greater than 5% of the bidding entity or parent entity? YES \_\_\_ NO \_\_\_
2. Does anyone in your organization have a direct or beneficial ownership share of less than 5%, but which has a value greater than \$106,447.20? YES \_\_\_ NO \_\_\_
3. Does anyone in your organization receive more than \$106,447.20 of the bidding entity's or parent entity's distributive income? (Note: Distributive income is, for these purposes, any type of distribution of profits. An annual salary is not distributive income.) YES \_\_\_ NO \_\_\_
4. Does anyone in your organization receive greater than 5% of the bidding entity's or parent entity's total distributive income, but which is less than \$106,447.20? YES \_\_\_ NO \_\_\_

(Note: Only one set of forms needs to be completed per person per bid even if a specific individual would require a yes answer to more than one question.)

A "YES" answer to any of these questions requires the completion of Form A. The bidder must determine each individual in the bidding entity or the bidding entity's parent company that would cause the questions to be answered "Yes". Each form must be signed and dated by a person that is authorized to execute contracts for your organization. **Photocopied or stamped signatures are not acceptable.** The person signing can be, but does not have to be, the person for which the form is being completed. The bidder is responsible for the accuracy of any information provided.

If the answer to each of the above questions is "NO", then the NOT APPLICABLE STATEMENT on page 2 of Form A must be signed and dated by a person that is authorized to execute contracts for your company.

**Form B: Identifying Other Contracts & Procurement Related Information** Disclosure Form B must be completed for each bid submitted by the bidding entity. Note: *Checking the NOT APPLICABLE STATEMENT on Form A does not allow the bidder to ignore Form B. Form B must be completed, checked, and dated or the bidder may be considered nonresponsive and the bid will not be accepted.*

The Bidder shall identify, by checking Yes or No on Form B, whether it has any pending contracts (including leases), bids, proposals, or other ongoing procurement relationship with any other (non-IDOT) State of Illinois agency. If "No" is checked, the bidder only needs to complete the check box on the bottom of Form B. If "Yes" is checked, the bidder must do one of the following:

Option I: If the bidder did not submit an Affidavit of Availability to obtain authorization to bid, the bidder must list all non-IDOT State of Illinois agency pending contracts, leases, bids, proposals, and other ongoing procurement relationships. These items may be listed on Form B or on an attached sheet(s). Do not include IDOT contracts. Contracts with cities, counties, villages, etc. are not considered State of Illinois agency contracts and are not to be included. Contracts with other State of Illinois agencies such as the Department of Natural Resources or the Capital Development Board must be included. Bidders who submit Affidavits of Availability are suggested to use Option II.

Option II: If the bidder is required and has submitted an Affidavit of Availability in order to obtain authorization to bid, the bidder may write or type "See Affidavit of Availability" which indicates that the Affidavit of Availability is incorporated by reference and includes all non-IDOT State of Illinois agency pending contracts, leases, bids, proposals, and other ongoing procurement relationships. For any contracts that are not covered by the Affidavit of Availability, the bidder must identify them on Form B or on an attached sheet(s). These might be such things as leases.

**D. Bidders Submitting More Than One Bid**

Bidders submitting multiple bids may submit one set of forms consisting of all required Form A disclosures and one Form B for use with all bids. Please indicate in the space provided below the bid item that contains the original disclosure forms and the bid items which incorporate the forms by reference.

- The bid submitted for letting item \_\_\_\_\_ contains the Form A disclosures or Certification Statement and the Form B disclosures. The following letting items incorporate the said forms by reference:

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**ILLINOIS DEPARTMENT  
OF TRANSPORTATION**

**Form A  
Financial Information &  
Potential Conflicts of Interest  
Disclosure**

|                  |               |                           |
|------------------|---------------|---------------------------|
| Contractor Name  |               |                           |
| Legal Address    |               |                           |
| City, State, Zip |               |                           |
| Telephone Number | Email Address | Fax Number (if available) |

Disclosure of the information contained in this Form is required by the Section 50-35 of the Illinois Procurement Code (30 ILCS 500). Vendors desiring to enter into a contract with the State of Illinois must disclose the financial information and potential conflict of interest information as specified in this Disclosure Form. This information shall become part of the publicly available contract file. This Form A must be completed for bids in excess of \$10,000, and for all open-ended contracts. **A publicly traded company may submit a 10K disclosure (or equivalent if applicable) in satisfaction of the requirements set forth in Form A. See Disclosure Form Instructions.**

**DISCLOSURE OF FINANCIAL INFORMATION**

**1. Disclosure of Financial Information.** The individual named below has an interest in the BIDDER (or its parent) in terms of ownership or distributive income share in excess of 5%, or an interest which has a value of more than \$106,447.20 (60% of the Governor's salary as of 7/1/07). **(Make copies of this form as necessary and attach a separate Disclosure Form A for each individual meeting these requirements)**

**FOR INDIVIDUAL (type or print information)**

**NAME:** \_\_\_\_\_

**ADDRESS** \_\_\_\_\_

**Type of ownership/distributable income share:**

stock \_\_\_\_\_ sole proprietorship \_\_\_\_\_ Partnership \_\_\_\_\_ other: (explain on separate sheet):  
% or \$ value of ownership/distributable income share: \_\_\_\_\_

**2. Disclosure of Potential Conflicts of Interest.** Check "Yes" or "No" to indicate which, if any, of the following potential conflict of interest relationships apply. If the answer to any question is "Yes", please attach additional pages and describe.

(a) State employment, currently or in the previous 3 years, including contractual employment of services. Yes \_\_\_ No \_\_\_

If your answer is yes, please answer each of the following questions.

1. Are you currently an officer or employee of either the Capitol Development Board or the Illinois Toll Highway Authority? Yes \_\_\_ No \_\_\_

2. Are you currently appointed to or employed by any agency of the State of Illinois? If you are currently appointed to or employed by any agency of the State of Illinois, and your annual salary exceeds \$106,447.20, (60% of the Governor's salary as of 7/1/07) provide the name the State agency for which you are employed and your annual salary. \_\_\_\_\_

**RETURN WITH BID/OFFER**

- 3. If you are currently appointed to or employed by any agency of the State of Illinois, and your annual salary exceeds \$106,447.20, (60% of the Governor's salary as of 7/1/07) are you entitled to receive (i) more than 7 1/2% of the total distributable income of your firm, partnership, association or corporation, or (ii) an amount in excess of the salary of the Governor? Yes \_\_\_ No \_\_\_
  
- 4. If you are currently appointed to or employed by any agency of the State of Illinois, and your annual salary exceeds \$106,447.20, (60% of the Governor's salary as of 7/1/07) are you and your spouse or minor children entitled to receive (i) more than 15 % in the aggregate of the total distributable income of your firm, partnership, association or corporation, or (ii) an amount in excess of 2 times the salary of the Governor? Yes \_\_\_ No \_\_\_

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(b) State employment of spouse, father, mother, son, or daughter, including contractual employment services in the previous 2 years.

Yes \_\_\_ No \_\_\_

If your answer is yes, please answer each of the following questions.

- 1. Is your spouse or any minor children currently an officer or employee of the Capitol Development Board or the Illinois Toll Highway Authority? Yes \_\_\_ No \_\_\_
  
- 2. Is your spouse or any minor children currently appointed to or employed by any agency of the State of Illinois? If your spouse or minor children is/are currently appointed to or employed by any agency of the State of Illinois, and his/her annual salary exceeds \$106,447.20, (60 % of the Governor's salary as of 7/1/07) provide the name of your spouse and/or minor children, the name of the State agency for which he/she is employed and his/her annual salary. \_\_\_\_\_  
\_\_\_\_\_
  
- 3. If your spouse or any minor children is/are currently appointed to or employed by any agency of the State of Illinois, and his/her annual salary exceeds \$106,447.20, (60% of the salary of the Governor as of 7/1/07) are you entitled to receive (i) more then 71/2% of the total distributable income of your firm, partnership, association or corporation, or (ii) an amount in excess of the salary of the Governor? Yes \_\_\_ No \_\_\_
  
- 4. If your spouse or any minor children are currently appointed to or employed by any agency of the State of Illinois, and his/her annual salary exceeds \$106,447.20, (60% of the Governor's salary as of 7/1/07) are you and your spouse or minor children entitled to receive (i) more than 15 % in the aggregate of the total distributable income of your firm, partnership, association or corporation, or (ii) an amount in excess of 2 times the salary of the Governor? Yes \_\_\_ No \_\_\_

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(c) Elective status; the holding of elective office of the State of Illinois, the government of the United States, any unit of local government authorized by the Constitution of the State of Illinois or the statutes of the State of Illinois currently or in the previous 3 years. Yes \_\_\_ No \_\_\_

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(d) Relationship to anyone holding elective office currently or in the previous 2 years; spouse, father, mother, son, or daughter. Yes \_\_\_ No \_\_\_

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(e) Appointive office; the holding of any appointive government office of the State of Illinois, the United States of America, or any unit of local government authorized by the Constitution of the State of Illinois or the statutes of the State of Illinois, which office entitles the holder to compensation in excess of the expenses incurred in the discharge of that office currently or in the previous 3 years. Yes \_\_\_ No \_\_\_

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(f) Relationship to anyone holding appointive office currently or in the previous 2 years; spouse, father, mother, son, or daughter. Yes \_\_\_ No \_\_\_

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(g) Employment, currently or in the previous 3 years, as or by any registered lobbyist of the State government. Yes \_\_\_ No \_\_\_

---

**RETURN WITH BID/OFFER**

(h) Relationship to anyone who is or was a registered lobbyist in the previous 2 years; spouse, father, mother, son, or daughter. Yes \_\_\_ No \_\_\_

(i) Compensated employment, currently or in the previous 3 years, by any registered election or reelection committee registered with the Secretary of State or any county clerk of the State of Illinois, or any political action committee registered with either the Secretary of State or the Federal Board of Elections. Yes \_\_\_ No \_\_\_

(j) Relationship to anyone; spouse, father, mother, son, or daughter; who was a compensated employee in the last 2 years by any registered election or re-election committee registered with the Secretary of State or any county clerk of the State of Illinois, or any political action committee registered with either the Secretary of State or the Federal Board of Elections. Yes \_\_\_ No \_\_\_

**APPLICABLE STATEMENT**

**This Disclosure Form A is submitted on behalf of the INDIVIDUAL named on previous page.**

Completed by:  \_\_\_\_\_ Date \_\_\_\_\_  
Signature of Individual or Authorized Representative

**NOT APPLICABLE STATEMENT**

**I have determined that no individuals associated with this organization meet the criteria that would require the completion of this Form A.**

**This Disclosure Form A is submitted on behalf of the CONTRACTOR listed on the previous page.**

\_\_\_\_\_ Date \_\_\_\_\_  
Signature of Authorized Representative

RETURN WITH BID/OFFER

ILLINOIS DEPARTMENT OF TRANSPORTATION

Form B Other Contracts & Procurement Related Information Disclosure

|                  |               |                           |
|------------------|---------------|---------------------------|
| Contractor Name  |               |                           |
| Legal Address    |               |                           |
| City, State, Zip |               |                           |
| Telephone Number | Email Address | Fax Number (if available) |

Disclosure of the information contained in this Form is required by the Section 50-35 of the Illinois Procurement Act (30 ILCS 500). This information shall become part of the publicly available contract file. This Form B must be completed for bids in excess of \$10,000, and for all open-ended contracts.

DISCLOSURE OF OTHER CONTRACTS AND PROCUREMENT RELATED INFORMATION

1. Identifying Other Contracts & Procurement Related Information. The BIDDER shall identify whether it has any pending contracts (including leases), bids, proposals, or other ongoing procurement relationship with any other State of Illinois agency: Yes \_\_\_ No \_\_\_

If "No" is checked, the bidder only needs to complete the signature box on the bottom of this page.

2. If "Yes" is checked. Identify each such relationship by showing State of Illinois agency name and other descriptive information such as bid or project number (attach additional pages as necessary). SEE DISCLOSURE FORM INSTRUCTIONS:

THE FOLLOWING STATEMENT MUST BE CHECKED

|                          |   |   |
|--------------------------|---|---|
| <input type="checkbox"/> | <hr style="border: none; border-top: 1px solid black;"/> Signature of Authorized Representative | <hr style="border: none; border-top: 1px solid black;"/> Date |
|--------------------------|---|---|

## **RETURN WITH BID**

### **SPECIAL NOTICE TO CONTRACTORS**

The following requirements of the Illinois Department of Human Rights' Rules and Regulations are applicable to bidders on all construction contracts advertised by the Illinois Department of Transportation:

#### **CONSTRUCTION EMPLOYEE UTILIZATION PROJECTION**

- (a) All bidders on construction contracts shall complete and submit, along with and as part of their bids, a Bidder's Employee Utilization Form (Form BC-1256) setting forth a projection and breakdown of the total workforce intended to be hired and/or allocated to such contract work by the bidder including a projection of minority and female employee utilization in all job classifications on the contract project.
- (b) The Department of Transportation shall review the Employee Utilization Form, and workforce projections contained therein, of the contract awardee to determine if such projections reflect an underutilization of minority persons and/or women in any job classification in accordance with the Equal Employment Opportunity Clause and Section 7.2 of the Illinois Department of Human Rights' Rules and Regulations for Public Contracts adopted as amended on September 17, 1980. If it is determined that the contract awardee's projections reflect an underutilization of minority persons and/or women in any job classification, it shall be advised in writing of the manner in which it is underutilizing and such awardee shall be considered to be in breach of the contract unless, prior to commencement of work on the contract project, it submits revised satisfactory projections or an acceptable written affirmative action plan to correct such underutilization including a specific timetable geared to the completion stages of the contract.
- (c) The Department of Transportation shall provide to the Department of Human Rights a copy of the contract awardee's Employee Utilization Form, a copy of any required written affirmative action plan, and any written correspondence related thereto. The Department of Human Rights may review and revise any action taken by the Department of Transportation with respect to these requirements.



**RETURN WITH BID**

**Contract No. 78108  
JEFFERSON County  
Section (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
Route FAP 724/FAS 2869  
District 9 Construction Funds**

**PART II. WORKFORCE PROJECTION - continued**

- B. Included in "Total Employees" under Table A is the total number of **new hires** that would be employed in the event the undersigned bidder is awarded this contract.

The undersigned bidder projects that: (number) \_\_\_\_\_ new hires would be recruited from the area in which the contract project is located; and/or (number) \_\_\_\_\_ new hires would be recruited from the area in which the bidder's principal office or base of operation is located.

- C. Included in "Total Employees" under Table A is a projection of numbers of persons to be employed directly by the undersigned bidder as well as a projection of numbers of persons to be employed by subcontractors.

The undersigned bidder estimates that (number) \_\_\_\_\_ persons will be directly employed by the prime contractor and that (number) \_\_\_\_\_ persons will be employed by subcontractors.

**PART III. AFFIRMATIVE ACTION PLAN**

- A. The undersigned bidder understands and agrees that in the event the foregoing minority and female employee utilization projection included under **PART II** is determined to be an underutilization of minority persons or women in any job category, and in the event that the undersigned bidder is awarded this contract, he/she will, prior to commencement of work, develop and submit a written Affirmative Action Plan including a specific timetable (geared to the completion stages of the contract) whereby deficiencies in minority and/or female employee utilization are corrected. Such Affirmative Action Plan will be subject to approval by the contracting agency and the **Department of Human Rights**.
- B. The undersigned bidder understands and agrees that the minority and female employee utilization projection submitted herein, and the goals and timetable included under an Affirmative Action Plan if required, are deemed to be part of the contract specifications.

Company \_\_\_\_\_ Telephone Number \_\_\_\_\_

Address \_\_\_\_\_

**NOTICE REGARDING SIGNATURE**

The Bidder's signature on the Proposal Signature Sheet will constitute the signing of this form. The following signature block needs to be completed if revisions are required.

Signature:  \_\_\_\_\_ Title: \_\_\_\_\_ Date: \_\_\_\_\_

Instructions: All tables must include subcontractor personnel in addition to prime contractor personnel.

Table A - Include both the number of employees that would be hired to perform the contract work and the total number currently employed (Table B) that will be allocated to contract work, and include all apprentices and on-the-job trainees. The "Total Employees" column should include all employees including all minorities, apprentices and on-the-job trainees to be employed on the contract work.

Table B - Include all employees currently employed that will be allocated to the contract work including any apprentices and on-the-job trainees currently employed.

Table C - Indicate the racial breakdown of the total apprentices and on-the-job trainees shown in Table A.

**RETURN WITH BID**

**Contract No. 78108  
JEFFERSON County  
Section (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
Route FAP 724/FAS 2869  
District 9 Construction Funds**

PROPOSAL SIGNATURE SHEET

The undersigned bidder hereby makes and submits this bid on the subject Proposal, thereby assuring the Department that all requirements of the Invitation for Bids and rules of the Department have been met, that there is no misunderstanding of the requirements of paragraph 3 of this Proposal, and that the contract will be executed in accordance with the rules of the Department if an award is made on this bid.

(IF AN INDIVIDUAL)

Firm Name \_\_\_\_\_

Signature of Owner \_\_\_\_\_

Business Address \_\_\_\_\_

\_\_\_\_\_

(IF A CO-PARTNERSHIP)

Firm Name \_\_\_\_\_

By \_\_\_\_\_

Business Address \_\_\_\_\_

\_\_\_\_\_

Name and Address of All Members of the Firm:

\_\_\_\_\_

\_\_\_\_\_

(IF A CORPORATION)

Corporate Name \_\_\_\_\_

By \_\_\_\_\_

Signature of Authorized Representative \_\_\_\_\_

Typed or printed name and title of Authorized Representative \_\_\_\_\_

Attest \_\_\_\_\_

Signature \_\_\_\_\_

(IF A JOINT VENTURE, USE THIS SECTION FOR THE MANAGING PARTY AND THE SECOND PARTY SHOULD SIGN BELOW)

Business Address \_\_\_\_\_

\_\_\_\_\_

(IF A JOINT VENTURE)

Corporate Name \_\_\_\_\_

By \_\_\_\_\_

Signature of Authorized Representative \_\_\_\_\_

Typed or printed name and title of Authorized Representative \_\_\_\_\_

Attest \_\_\_\_\_

Signature \_\_\_\_\_

Business Address \_\_\_\_\_

\_\_\_\_\_

If more than two parties are in the joint venture, please attach an additional signature sheet.



Return with Bid

Division of Highways
Proposal Bid Bond
(Effective November 1, 1992)

Item No. \_\_\_\_\_

Letting Date \_\_\_\_\_

KNOW ALL MEN BY THESE PRESENTS, That We \_\_\_\_\_

as PRINCIPAL, and \_\_\_\_\_

\_\_\_\_\_ as SURETY, are held jointly, severally and firmly bound unto the STATE OF ILLINOIS in the penal sum of 5 percent of the total bid price, or for the amount specified in Article 102.09 of the "Standard Specifications for Road and Bridge Construction" in effect on the date of invitation for bids, whichever is the lesser sum, well and truly to be paid unto said STATE OF ILLINOIS, for the payment of which we bind ourselves, our heirs, executors, administrators, successors and assigns.

THE CONDITION OF THE FOREGOING OBLIGATION IS SUCH, that whereas, the PRINCIPAL has submitted a bid proposal to the STATE OF ILLINOIS, acting through the Department of Transportation, for the improvement designated by the Transportation Bulletin Item Number and Letting Date indicated above.

NOW, THEREFORE, if the Department shall accept the bid proposal of the PRINCIPAL; and if the PRINCIPAL shall, within the time and as specified in the bidding and contract documents, submit a DBE Utilization Plan that is accepted and approved by the Department; and if, after award by the Department, the PRINCIPAL shall enter into a contract in accordance with the terms of the bidding and contract documents including evidence of the required insurance coverages and providing such bond as specified with good and sufficient surety for the faithful performance of such contract and for the prompt payment of labor and material furnished in the prosecution thereof; or if, in the event of the failure of the PRINCIPAL to make the required DBE submission or to enter into such contract and to give the specified bond, the PRINCIPAL pays to the Department the difference not to exceed the penalty hereof between the amount specified in the bid proposal and such larger amount for which the Department may contract with another party to perform the work covered by said bid proposal, then this obligation shall be null and void, otherwise, it shall remain in full force and effect.

IN THE EVENT the Department determines the PRINCIPAL has failed to comply with any requirement as set forth in the preceding paragraph, then Surety shall pay the penal sum to the Department within fifteen (15) days of written demand therefor. If Surety does not make full payment within such period of time, the Department may bring an action to collect the amount owed. Surety is liable to the Department for all its expenses, including attorney's fees, incurred in any litigation in which it prevails either in whole or in part.

In TESTIMONY WHEREOF, the said PRINCIPAL and the said SURETY have caused this instrument to be signed by

their respective officers this \_\_\_\_\_ day of \_\_\_\_\_ A.D., \_\_\_\_\_ .

PRINCIPAL

SURETY

\_\_\_\_\_  
(Company Name)

\_\_\_\_\_  
(Company Name)

By \_\_\_\_\_  
(Signature & Title)

By: \_\_\_\_\_  
(Signature of Attorney-in-Fact)

Notary Certification for Principal and Surety

STATE OF ILLINOIS,  
County of \_\_\_\_\_

I, \_\_\_\_\_, a Notary Public in and for said County, do hereby certify that

\_\_\_\_\_ and \_\_\_\_\_  
(Insert names of individuals signing on behalf of PRINCIPAL & SURETY)

who are each personally known to me to be the same persons whose names are subscribed to the foregoing instrument on behalf of PRINCIPAL and SURETY, appeared before me this day in person and acknowledged respectively, that they signed and delivered said instrument as their free and voluntary act for the uses and purposes therein set forth.

Given under my hand and notarial seal this \_\_\_\_\_ day of \_\_\_\_\_ A.D. \_\_\_\_\_

My commission expires \_\_\_\_\_

Notary Public

In lieu of completing the above section of the Proposal Bid Form, the Principal may file an Electronic Bid Bond. By signing the proposal and marking the check box next to the Signature and Title line below, the Principal is ensuring the identified electronic bid bond has been executed and the Principal and Surety are firmly bound unto the State of Illinois under the conditions of the bid bond as shown above.

Electronic Bid Bond ID# \_\_\_\_\_

Company / Bidder Name \_\_\_\_\_



Signature and Title \_\_\_\_\_

# PROPOSAL ENVELOPE



# PROPOSALS

for construction work advertised for bids by the  
Illinois Department of Transportation

| Item No. | Item No. | Item No. |
|----------|----------|----------|
|          |          |          |
|          |          |          |
|          |          |          |
|          |          |          |

Submitted By:

|           |
|-----------|
| Name:     |
| Address:  |
|           |
|           |
| Phone No. |

Bidders should use an IDOT proposal envelope or affix this form to the front of a 10" x 13" envelope for the submittal of bids. If proposals are mailed, they should be enclosed in a second or outer envelope addressed to:

Engineer of Design and Environment - Room 326  
Illinois Department of Transportation  
2300 South Dirksen Parkway  
Springfield, Illinois 62764

## **NOTICE**

**Individual bids, including Bid Bond and/or supplemental information if required, should be securely stapled.**

# CONTRACTOR OFFICE COPY OF CONTRACT SPECIFICATIONS

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## NOTICE

None of the following material needs to be returned with the bid package unless the special provisions require documentation and/or other information to be submitted.

**Contract No. 78108**  
**JEFFERSON County**  
**Section (41-8,4-2)RS-1;(1,4)RS-3;2RS-4**  
**Route FAP 724/FAS 2869**  
**District 9 Construction Funds**



**Illinois Department of Transportation**



## NOTICE TO BIDDERS

**1. TIME AND PLACE OF OPENING BIDS.** Sealed proposals for the improvement described herein will be received by the Department of Transportation at the Harry R. Hanley Building, 2300 South Dirksen Parkway, in Springfield, Illinois until 10:00 o'clock a.m., May 15, 2009. All bids will be gathered, sorted, publicly opened and read in the auditorium at the Department of Transportation's Harry R. Hanley Building shortly after the 10:00 a.m. cut off time.

**2. DESCRIPTION OF WORK.** The proposed improvement is identified and advertised for bids in the Invitation for Bids as:

**Contract No. 78108  
JEFFERSON County  
Section (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
Route FAP 724/FAS 2869  
District 9 Construction Funds**

**11.93 miles of resurfacing on IL 37 from south of IL 142 to south of the Jefferson/Franklin County line.**

**3. INSTRUCTIONS TO BIDDERS.** (a) This Notice, the invitation for bids, proposal and letter of award shall, together with all other documents in accordance with Article 101.09 of the Standard Specifications for Road and Bridge Construction, become part of the contract. Bidders are cautioned to read and examine carefully all documents, to make all required inspections, and to inquire or seek explanation of the same prior to submission of a bid.

(b) State law, and, if the work is to be paid wholly or in part with Federal-aid funds, Federal law requires the bidder to make various certifications as a part of the proposal and contract. By execution and submission of the proposal, the bidder makes the certification contained therein. A false or fraudulent certification shall, in addition to all other remedies provided by law, be a breach of contract and may result in termination of the contract.

**4. AWARD CRITERIA AND REJECTION OF BIDS.** This contract will be awarded to the lowest responsive and responsible bidder considering conformity with the terms and conditions established by the Department in the rules, Invitation for Bids and contract documents. The issuance of plans and proposal forms for bidding based upon a prequalification rating shall not be the sole determinant of responsibility. The Department reserves the right to determine responsibility at the time of award, to reject any or all proposals, to readvertise the proposed improvement, and to waive technicalities.

By Order of the  
Illinois Department of Transportation

Gary Hannig,  
Acting Secretary

INDEX  
FOR  
SUPPLEMENTAL SPECIFICATIONS  
AND RECURRING SPECIAL PROVISIONS

Adopted January 1, 2009

This index contains a listing of SUPPLEMENTAL SPECIFICATIONS and frequently used RECURRING SPECIAL PROVISIONS.

ERRATA Standard Specifications for Road and Bridge Construction (Adopted 1-1-07) (Revised 1-1-09)

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RECURRING SPECIAL PROVISIONS

The following RECURRING SPECIAL PROVISIONS indicated by an "X" are applicable to this contract and are included by reference:

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**STATE OF ILLINOIS**

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**SPECIAL PROVISIONS**

The following Special Provisions supplement the "Standard Specifications for Road and Bridge Construction," adopted January 1, 2007, the latest edition of the "Manual on Uniform Traffic Control Devices for Streets and Highways," and the "Manual of Test Procedures for Materials" in effect on the date of invitation for bids, and the Supplemental Specifications and Recurring Special Provisions indicated on the Check Sheet included herein which apply to and govern the construction of FAP 724 / FAS 2869 (IL 37), Sections (41-8,4-2)RS-1; (1,4)RS-3; 2RS-4, Jefferson County, Contract No. 78108, and in case of conflict with any part or parts of said Specifications, the said Special Provisions shall take precedence and shall govern.

**LOCATION OF PROJECT**

This project begins approximately 300 ft south of IL 142 and IL 37 intersection in Mt. Vernon and continues south on IL 37 for 12 miles to a point approximately 0.1 miles south of the Jefferson / Franklin County line.

**DESCRIPTION OF PROJECT**

The proposed project consists of pavement and pcc patching, hot-mix asphalt surface removal, hot-mix asphalt resurfacing, aggregate shoulders, pavement markings, and guardrail safety improvements on bridge structures.

**UTILITIES**

Effective 1984

Revised 1/2/97

Add the following after the first paragraph of Article 105.07 of the Standard Specifications:

Underground utilities have been plotted from available surveys and records and, therefore, their locations must be considered approximate only. There also may be utilities for which the locations are unknown. Verification of locations of underground utilities, shown or not shown, will be the responsibility of the Contractor. The following utility companies have facilities within the project limits which will require adjustment:

| Name and Address of Utility   | Type              | Locations          | Estimated Date Adjustment Completed |
|---|-------------------|--------------------|-------------------------------------|
| Frontier Communications<br>117 W Jefferson St.<br>Mount Pulaski, IL 62548 | TV/Internet/Phone | Throughout Project | No adjustments anticipated          |
| Consolidated Utility Services<br>1537 Mill Race Drive<br>Salem, VA 24153  | Locating service  | Throughout Project | No adjustments anticipated          |

|   |                   |                    |                            |
|---|-------------------|--------------------|----------------------------|
| Ameren IP - Electric<br>P.O. Box 868<br>Mt. Vernon, IL 62864                            | Electric          | Throughout Project | No adjustments anticipated |
| Rend Lake Intercity Water<br>11231 Marcum Branch Rd<br>P.O. Box 907<br>Benton, IL 62812 | Water             | Throughout Project | No adjustments anticipated |
| Tri-County Electric<br>3906 W. Broadway<br>Mt. Vernon, IL 62864                         | Electric          | Throughout Project | No adjustments anticipated |
| Village of Bonnie<br>P.O. Box 117<br>Bonnie, IL 62816                                   | Water / Sewer     | Throughout Project | No adjustments anticipated |
| Ameren CIPS<br>P.O. Box 460<br>Marion, IL 62959   | Electric / Gas    | Throughout Project | No adjustments anticipated |
| Ewing-Ina Water Commission<br>104 West Main<br>Whittington, IL 62897                    | Water             | Throughout Project | No adjustments anticipated |
| Village of Ina<br>P.O. Box 147<br>Ina, IL 62846   | Water / Sewer     | Throughout Project | No adjustments anticipated |
| AT&T Distribution<br>210 North Locust<br>Centralia, IL 62801                            | Phone             | Throughout Project | No adjustments anticipated |
| Village of Belle Rive<br>P.O. Box 147<br>Belle Rive, IL 62810                           | Water / Sewer     | Throughout Project | No adjustments anticipated |
| Charter Communications<br>823 E Broadway Street<br>Sparta, IL 62286                     | TV/Internet/Phone | Throughout Project | No adjustments anticipated |
| City of Mt. Vernon<br>P.O. Box 1708<br>Mt. Vernon, IL 62864                             | Water / Sewer     | Throughout Project | No adjustments anticipated |

Additional utility information may be obtained by calling the "Joint Utility Location Information for Excavators" phone number, 800-892-0123. This project is located in the Dodds, Spring Garden, and Ewing Townships.

Add the following after the first paragraph of Article 107.31 of the Standard Specifications:

The Contractor is advised that this project includes areas of highway illumination. These areas have underground cable or conduit throughout which is to remain in service. Before driving any posts or beginning any excavation operations, the Contractor shall locate, uncover by hand and relocate any wiring which conflicts with the proposed work. Any cable or conduit which is damaged as a result of the Contractor's operations shall be replaced by him at his expense. Replacement material and methods shall meet or exceed the original specifications for the wiring. Splicing will be permitted.

## TRAFFIC CONTROL PLAN

Effective 1985

Revised 2/17/99

Traffic control shall be in accordance with the applicable sections of the Standard Specifications for Road and Bridge Construction, the guidelines contained in the National Manual on Uniform Traffic Control Devices for Streets and Highways, the Supplemental Specifications, these Special Provisions, and any special details and highway standards contained herein and in the plans.

Special attention is called to Articles 107.09 and 107.14 of the Standard Specifications for Road and Bridge Construction and the following traffic control related (1) Highway Standards; (2) Supplemental Specifications and Recurring Special Provisions; (3) other Special Provisions; and (4) Plan Details which are included in this contract:

1. Standards: 701001 701006 701011 701201 701301 701306 701311 701336  
701501 701901
2. Supplemental Specifications and Recurring Special Provisions:  
Work Zone Traffic Control and Protection  
Work Zone Traffic Control Devices
3. Special Provisions:  
Flagger at Side Roads and Entrances (BDE)
4. Plan Details: Traffic Control and Protection (Special)

Traffic control standards shall be applied as directed by the Engineer. Suggested applications for each standard are as follows:

- 701001 This standard should be used for off-road operations greater than 15 feet from The edge of pavement.
- 701006 This standard should be used for miscellaneous work which is performed within 15', but not closer than 2' to the edge of the traffic lane.
- 701011 This standard should be used when the Contractor's work is confined to the shoulder.
- 701201 This standard shall be applied during patching operations which require encroachment of any equipment, vehicles, or workers within 2' of the edge of pavement
- 701301 This standard will apply when short time work operations are being performed. Typical such operations are bituminous density testing, application of temporary pavement marking, marking patches, and miscellaneous survey operations.
- 701306 This standard should be used when the Contractor's prime coat, surface course, binder course, and aggregate shoulders are performed under traffic

701311 This standard should be used for pavement marking or other continuous or intermittent moving operations where the average speed is greater than 3 mph.

701336 This standard should be used during pavement patching operations.

701501 This standard shall be applied when patching, HMA surface removal, and resurfacing operations are within the Woodland Drive intersection limits and at the Ina intersection.

During the entire construction period, the road shall be kept open to traffic as follows:

- (a) In accordance with the applicable portions of the Standard Specifications during the widening, patching and resurfacing operations.
- (b) The highway shall be kept open to at least one lane of traffic at all times, and to two lanes of traffic to the greatest extent possible.
- (c) Access to all public roads and private entrances shall be maintained during all stages of the work.
- (d) In accordance with Article 406.04 of the Standard Specifications and by alternating lifts between lanes during construction of the granular or bituminous grade raises.

Prior to allowing traffic on any portion of the roadway that has been cold milled, the Contractor shall have erected "Rough Grooved Surface" and "Uneven Pavement" signs that conform to the details shown in the plans. A minimum of one sign at each end of the improvement will be required. The Contractor shall maintain the "Rough Grooved Surface" signs until the cold milled surface is covered with leveling binder. The Contractor shall maintain the "Uneven Pavement" signs until the resurfacing operations are completed.

If at any time the signs are in place but not applicable, they shall be turned from the view of motorists or covered as directed by the Engineer.

At all locations where the Contractor's equipment is required to cross the traffic lanes, traffic control and protection in accordance with Standard 701306 shall be used. The "One Lane Road Ahead" signs indicated on this standard shall be replaced with "Road Machinery Ahead" signs [W21-3(0)-48].

The cost of furnishing, erecting, maintaining, and removing the required signs shall be incidental to the contract.

### **TRAFFIC CONTROL AND PROTECTION, (SPECIAL)**

Revise the second paragraph of article 701.04 of the Standard Specifications to read:

"The Contractor shall notify the Engineer 14 days in advance as to which lane, northbound or southbound, this traffic control will begin. Once the traffic control has been initiated, all proposed work to the structures adjacent to that lane must be completed prior to setting up

traffic control in the opposite lane of travel. All proposed guardrail and retrofit rail must be in place prior to relocating the temporary concrete barrier to the next sequential structure, unless the Contractor provides flaggers for a one day operation at their expense, or a temporary crashworthy terminal approved by the Engineer. Before any set-up in the opposite lane of travel will be allowed, all permanent guardrail must be in place. The Contractor shall perform work at two structures at a time or as approved by the Engineer. Advance warning signs shall be erected prior to channelizing devices and shall remain until all devices have been removed from the pavement.”

This work shall apply to the work on structure numbers 041-0035, 041-0092, 041-0093, 041-0095, and 041-0031 and shall be performed in accordance with applicable portions of Section 701 of the Standard Specifications and as shown in the details in the plans.

Method of Measurement. Traffic Control and Protection, (Special) will be measured for payment on an each basis, meaning 2 each per structure.

Basis of Payment. Traffic control and protection will be paid for at the contract unit price per each for TRAFFIC CONTROL AND PROTECTION, (SPECIAL) which shall include all signs, barrels, relocation of signs and barrels, and barrier reflectors needed.

## **PORTABLE CHANGEABLE MESSAGE SIGNS**

This work consists of furnishing, placing, and maintaining changeable message sign(s) according to the Standard Specifications and the following:

At project start-up, total of 4 changeable message signs shall be required in this contract: Two at the interchange off-ramps and one each at the north and south ends of the job. Two signs will also be required during retrofit rail operations All signs must be in place and operational for a minimum of 7 calendar days prior to lane and road closures. Each sign shall state the day work will begin and delays are possible. The exact message will be approved by the Engineer. The Contractor may be required to relocate each sign multiple times during the contract at his or her expense. The exact location of the placement of these signs shall be determined in the field by the Engineer.

The furnishing, placing, and maintaining of portable changeable message sign(s) shall be paid for per calendar month as CHANGEABLE MESSAGE SIGN.

## **TEMPORARY CONCRETE BARRIER RELOCATION**

Effective 1988

Revised 7/17/03

This item shall be performed in accordance with the plans, the applicable portions of Section 704 of the Standard Specifications, and as specified herein.

The Contractor shall plan the operations involved in removing and relocating the temporary concrete barrier so as to minimize the period when the work zone will be unprotected; and to

assure that, in the opinion of the Engineer, sufficient barrier will be in place at the end of each work day to shield traffic from all hazards. During the relocation operations, cones, drums or barricades shall be placed at 10 foot centers across all gaps in the barrier.

Prior to the relocation of the temporary concrete barrier from its initial location, the Contractor shall submit and obtain the Engineer's approval of a work plan for relocating the barrier which will meet the above requirements. The Engineer may require the Contractor to first furnish and install separate additional temporary concrete barrier of sufficient length to assure that the remaining needed barrier can be relocated from the initial installation in one daylight period.

Any additional barrier which must be furnished shall be paid for at the contract unit price per foot for RELOCATE TEMPORARY CONCRETE BARRIER. No additional compensation will be allowed on account of the above requirements.

### **TRAFFIC BARRIER TERMINAL, TYPE 6A (SPECIAL)**

**Description.** This work shall be performed in accordance with the plans, the applicable portions of Section 631 of the Standard Specifications, and as specified herein.

The rail elements shall be shop curved to the radii shown on the plans.

**Method of Measurement.** The traffic barrier terminal will be measured for payment, complete in place, in units of each.

**Basis of Payment.** This work will be paid for at the contract unit price per each for TRAFFIC BARRIER TERMINAL, TYPE 6A (SPECIAL).

### **STEEL PLATE BEAM GUARDRAIL (SHORT RADIUS)**

**Description.** This work shall be performed in accordance with the plans, the applicable portions of Sections 630 and 631 of the Standard Specifications, and as specified herein.

The rail elements shall be shortened as shown on the plans. Posts and guardrail attachment shall be as shown on the plans.

**Method of Measurement.** Steel Plate Beam Guardrail (Short Radius) will be measured for payment according to Article 630.07.

**Basis of Payment.** This work will be paid for at the contract unit price per foot for STEEL PLATE BEAM GUARDRAIL (SHORT RADIUS), which price shall include the specified end section.

## **TWO WEEK NOTIFICATION PRIOR TO STARTING WORK**

Effective December 2005

Revise the first sentence of Article 107.09 Public Convenience and Safety to the following "The Contractor shall notify the Engineer at least 14 days in advance of starting any construction work.

This additional notification is required so that the public can be notified of the pending construction.

## **LATE START DUE TO MULTIPLE CONTRACTS AND USE OF IL 37 AS AN ALTERNATE ROUTE DETOUR**

Revise Section 108.03 of the Standard Specifications to read:

"The Department will waive the 10 day start-up requirement due to the use of ILL 37 as an alternate route detour for contract 78112 and also for Contractors who are awarded multiple contracts in this letting or in combination with the two State lettings held on April 3 and April 24, 2009. Contract 78112 (Rubblization and overlay of I-57 from ILL 14 to Jefferson/Franklin County line) will use Illinois 37 as an alternate route detour between the dates of July 16, 2009 and on or about August 28, 2009. No work will be allowed on this contract until the use of Illinois 37 as an alternate route detour is no longer required.

The apparent low bidder shall submit a written request for waiver within 10 days after bid opening to the Project Implementation Engineer in whose District the affected project is located. The request shall include specific reasons for the waiver in a starting date coordination plan and a suggested preliminary progress schedule for each project. Each Project Implementation Engineer will schedule a meeting with the Contractor within 5 working days after receipt of the request for waiver. Procedures and schedules to the prosecution of each contract will be discussed and exact starting dates, as well as dates for preconstruction conferences, for each project will be established. Consideration of waivers will not affect award decisions or the procedures followed to execute awarded contracts.

The intent of this is to allow the Contractor reasonable flexibility to complete each of the affected contracts within the number of working days specified in each contract in the context of the total work to be completed. The working day contract start date shall not be delayed, in the starting date coordination plan, in a manner that leaves insufficient working days to complete all work, except off-pavement and/or cleanup work, by September 1, 2010. By submission of a waiver plan, the Contractor understands and agrees that the granting of waivers in any contract starting date shall not be reason for an extension of time to complete, and that the decision to approve a waiver for any or all contracts shall reside with the Department, which decision shall be final."

In addition, the contractor shall work with the contractor for contract 78112 regarding removal of alternate route detour signs or any other traffic control issues as needed.

## **UNION PACIFIC RAILROAD MINIMUM CONSTRUCTION REQUIREMENTS**

### **1.01 DESCRIPTION**

This project includes construction work within the Right-of-Way and/or properties of the Union Pacific Railroad Company ("UPRR") and adjacent to tracks, wire lines and other facilities. This section describes the special minimum requirements for coordination with UPRR when work by the Contractor will be performed upon, over or under the UPRR Right-of-Way or may impact current or future UPRR operations. The Contractor will coordinate with UPRR while performing the work outlined in this Contract, and shall afford the same cooperation with UPRR as it does with the Agency. All submittals and work shall be completed in accordance with UPRR Guidelines and AREMA recommendations as modified by these minimum special requirements or as directed in writing by the UPRR Designated Representative.

For purposes of this project, the UPRR Designated Representative shall be the person or persons designated by the UPRR Manager of Industry and Public Projects to handle specific tasks related to the project.

### **1.02 DEFINITION OF AGENCY AND CONTRACTOR**

As used in these UPRR requirements, the term "Agency" shall mean the State of Illinois, by and through its Department of Transportation.

As used in these UPRR requirements, the term "Contractor" shall mean the contractor or contractor's hired by the Agency to perform any project work on any portion of UPRR's property and shall also include the contractor's subcontractors and the contractor's and subcontractor's respective officer, agents and employees, and others acting under its or their authority.

### **1.03 UPRR CONTACTS**

The primary UPRR point of contact for this project is:

Dave McKernan  
Manager Industry and Public Projects  
Union Pacific Railroad Company  
100 North Broadway, Suite 1500  
St. Louis, Missouri 63102  
Phone: 314-331-0682  
Fax: 402-501-2520

For UPRR flagging services and track work, contact:

Bill Brendel  
Manager Track Maintenance  
Union Pacific Railroad Company  
3412 Carondelet Avenue  
Dupo, Illinois 62239  
Phone: 618-286-0048  
Fax: 618-286-0481

#### **1.04 REQUEST FOR INFORMATION / CLARIFICATION**

All Requests for Information ("RFI") involving work within any UPRR Right-Of-Way shall be in accordance with the procedures listed elsewhere in these bid documents. All RFI's shall be submitted to the Engineer of Record. The Engineer of Record will submit the RFI to the UPRR Designated Representative for review and approval for corresponding to work within the UPRR Right-Of-Way. The Contractor shall allow four (4) weeks for the review and approval process by UPRR.

#### **1.05 PLANS / SPECIFICATIONS**

The plans and specifications for this project, affecting the UPRR, are subject to the written approval by the UPRR and changes in the plans may be required after award of the Contract. Such changes are subject to the approval of the Agency and the UPRR.

#### **1.06 UTILITIES AND FIBER OPTICS**

All installations shall be constructed in accordance with current AREMA recommendations and UPRR specifications and requirements. UPRR general guidelines and the required application forms for utility installations can be found on the UPRR website at [www.uprr.com](http://www.uprr.com).

#### **1.07 GENERAL**

A. Contractor shall perform all work in compliance with all applicable UPRR and FRA rules and regulations. Contractor shall arrange and conduct all work in such manner and at such times as shall not endanger or interfere with the safe operation of the tracks and property of UPRR and the traffic moving on such tracks, or the wires, signals and other property of UPRR, its tenants or licensees, at or in the vicinity of the work. UPRR shall be reimbursed by Contractor or Agency for train delay costs and lost revenue claims due to any delays or interruption of train operations resulting from Contractor's construction work or other activities.

B. Construction activities will be permitted within 12 feet of the centerline of operational tracks only if absolutely necessary and UPRR's Designated Representative grants approval. Construction activities within 12 feet of the operational track(s) must allow the tracks to stay operational.

C. Track protection is required for all work equipment (including rubber tired equipment) operating within 25 feet from nearest rail.

D. The Contractor is also advised that new railroad facilities within the project may be built by UPRR and that certain Contractor's activities cannot proceed until that work is completed. The Contractor shall be aware of the limits of responsibilities and allow sufficient time in the schedule for that work to be accomplished and shall coordinate its efforts with the UPRR.

## **1.08 RAILROAD OPERATIONS**

A. The Contractor shall be advised that trains and/or equipment are expected on any track, at any time, in either direction. Contractor shall become familiar with the train schedules in this location and structure its bid assuming intermittent track windows in this period, as defined in Paragraph B below.

B. All railroad tracks within and adjacent to the Contract Site are active, and rail traffic over these facilities shall be maintained throughout the Project. Activities may include both through moves and switching moves to local customers. Railroad traffic and operations will occur continuously throughout the day and night on these tracks and shall be maintained at all times as defined herein. The Contractor shall coordinate and schedule the work so that construction activities do not interfere with railroad operations.

C. Work windows for this Contract shall be coordinated with the Agency's and the UPRR's Designated Representatives. Types of work windows include Conditional Work Windows and Absolute Work Windows, as defined below:

1. Conditional Work Window: A Conditional Work Window is a period of time that railroad operations have priority over construction activities. When construction activities may occur on and adjacent to the railroad tracks within 25 feet of the nearest track, a UPRR flag person will be required. At the direction of the UPRR flag person, upon approach of a train, and when trains are present on the tracks, the tracks must be cleared (i.e., no construction equipment, materials or personnel within 25 feet, or as directed by the UPRR Designated Representative, from the tracks). Conditional Work Windows are available for the Project.
2. Absolute Work Window: An Absolute Work Window is a period of time that construction activities are given priority over railroad operations. During this time frame the designated railroad track(s) will be inactive for train movements and may be fouled by the Contractor. At the end of an Absolute Work Window the railroad tracks and/or signals must be completely operational for train operations and all UPRR, Public Utilities Commission (PUC) and Federal Railroad Administration (FRA) requirements, codes and regulations for operational tracks must be complied with. In the situation where the operating tracks and/or signals have been affected, the UPRR will perform inspections of the work prior to placing that track back into service. UPRR flag persons will be required for construction activities requiring an Absolute Work Window. **Absolute Work Windows will not generally be granted. Any request will require a detailed explanation for UPRR review.**

## **1.09 RIGHT OF ENTRY, ADVANCE NOTICE AND WORK STOPPAGES**

A. Prior to beginning any work on or over the property of, or affecting the facilities of, the UPRR, the Contractor shall notify the primary railroad representative at least ten (10) working days in advance of such work and at least ten (10) working days in

advance of proposed performance of any work by contractor in which any person or equipment will be within twenty-five (25) feet of any track or will be near enough to any track that any equipment extension (such as, but not limited to, a crane boom) will reach within twenty-five (25) feet of any track. If the contractor will be on UPRR property outside the limits of the State's easements, Contractor shall enter into an agreement with the UPRR in the form of the "Contractor's Right of Entry Agreement", attached as **EXHIBIT 5**, or latest version thereof provided by the UPRR. There is a fee for processing of the agreement. This cost shall be borne by the Contractor. Contractor shall submit a copy of the executed agreement and the insurance policies, binders, certificates and endorsements set forth therein to the Agency prior to commencing work on UPRR property. The right of entry agreement shall specify working time frames, flagging and inspection requirements, and any other items specified by the UPRR.

B. The Contractor shall give the advance notice to the UPRR as required above before commencing work in connection with construction upon or over UPRR's Right-of-Way and shall observe UPRR's rules and regulations with respect thereto.

C. All work upon UPRR's Right-of-Way shall be done at such times and in such manner so as not to interfere with or endanger the operations of UPRR. Whenever work may affect the operations or safety of trains, the method of doing such work shall first be submitted to UPRR's Designated Representative for approval, but such approval shall not relieve the Contractor from liability. Any work to be performed by the Contractor, which requires flagging and/or inspection service, shall be deferred until the flagging protection required by UPRR is available at the job site. See Section 1.24 for railroad flagging requirements.

D. The Contractor shall make requests in writing for both Absolute and Conditional Work Windows, at least two weeks in advance of any work. The written request must include:

1. Exactly what the work entails.
2. The days and hours that work will be performed.
3. The exact location of work, and proximity to the tracks.
4. The type of window requested and the amount of time requested.
5. The designated contact person.

The Contractor shall provide a written confirmation notice to the UPRR at least 48 hours before commencing work in connection with approved work windows when work will be performed within **25 feet of any track center line**. All work shall be performed in accordance with previously approved work plans.

E. Should a condition arising from, or in connection with the work, require that immediate and unusual provisions be made to protect operations and property of UPRR, the Contractor shall make such provisions. If in the judgment of UPRR's Designated Representative such provisions are insufficient, the UPRR's Designated Representative may require or provide such provisions as deemed necessary. In any event, such provisions shall be at the Contractor's expense and without cost to the UPRR. UPRR or the Agency shall have the right to order Contractor to temporarily cease operations in the event of an emergency or, if in

the opinion of the UPRR's Designated Representative, the Contractor's operations could endanger UPRR's operations. In the event such an order is given, Contractor shall immediately notify the Agency of the order.

#### **1.10 INSURANCE**

Contractor shall not begin work upon or over UPRR's Right-of-Way until UPRR has been furnished the insurance policies, binders, certificates, and endorsements required by the Right-of-Entry Agreement and UPRR's Designated Representative has advised the Agency that such insurance is in accordance with the Agreement. The required insurance shall be kept in full force and effect during the performance of work and thereafter until Contractor removes all tools, equipment, and material from UPRR's property and cleans the premises in a manner reasonably satisfactory to UPRR.

#### **1.11 RAILROAD SAFETY ORIENTATION**

All personnel employed by the Contractor and all subcontractors must complete the UPRR course "Orientation for Contractor's Safety", and be registered prior to working on UPRR property. This orientation is available at [www.contractororientation.com](http://www.contractororientation.com). This course is required to be completed annually.

#### **1.12 COOPERATION**

UPRR will cooperate with Contractor so that work may be conducted in an efficient manner, and will cooperate with Contractor in enabling use of UPRR's right-of-way in performing the work.

#### **1.13 MINIMUM CONSTRUCTION CLEARANCES FOR FALSEWORK AND OTHER TEMPORARY STRUCTURES**

The Contractor shall abide by the following minimum temporary clearances during the course of construction:

- 12' – 0" horizontal from centerline of track
- 21' – 6" vertically above top of rail.

For construction clearance less than listed above, local Operating Unit review and approval is required.

#### **1.14 APPROVAL OF REDUCED CLEARANCES**

A. The minimum track clearances to be maintained by the Contractor during construction are specified in Section 3.07 herein.

B. Any proposed infringement on the specified minimum clearances due to the Contractor's operations shall be submitted to UPRR's Designated Representative through the Agency at least 30 days in advance of the work and shall not be undertaken until approved in writing by the UPRR's Designated Representative.

C. No work shall commence until the Contractor receives in writing assurance from UPRR's Designated Representative that arrangements have been made for flagging service, as may be necessary and receives permission from UPRR's Designated Representative to proceed with the work.

**1.15 CONSTRUCTION AND AS-BUILT SUBMITTALS**

A. Submittals are required for construction materials and procedures as outlined below. The submittals shall include all review comments from the Agency and the Engineer of Record. All design submittals shall be stamped and signed by a Professional Engineer registered in the State of Illinois.

B. The tables below provide UPRR's minimum submittal requirements for the construction items noted. Submittal requirements are in addition to those specified elsewhere in these bid documents. The minimum review times indicated below represent UPRR's requirements only. The Contractor shall allow additional time for the Agency's review time as stated elsewhere in these bid documents.

C. Submittals shall be made by the Agency to the UPRR Manager of Industry and Public Projects unless otherwise directed by the Railroad. Items in Table 1 shall be submitted for both railroad overpass and underpass projects, as applicable. Items in Table 2 shall be submitted for railroad underpass projects only.

**TABLE 1**

| ITEM | DESCRIPTION                     | SETS REQD. | UPRR's Minimum Review Time |
|------|---------------------------------|------------|----------------------------|
| 1    | Shoring design and details      | 4          | 4 weeks                    |
| 2    | Falsework design and details    | 4          | 4 weeks                    |
| 3    | Drainage design provisions      | 4          | 4 weeks                    |
| 4    | Erection diagrams and sequence  | 4          | 4 weeks                    |
| 5    | Demolition diagram and sequence | 4          | 4 weeks                    |

Prior to or during construction of railroad underpass structures, the UPRR requires the review of drawings, reports, test data and material data sheets to determine compliance with the specifications. Product information for items noted in Table 2 be submitted to UPRR's Designated Representative through the Agency for their own review and approval of the material. The signed submittal and the Agency's review comments will be reviewed by UPRR or their consultant. If a consultant performs the reviews, the consultant may reply directly to the Agency or its Designated Representative after consultation with UPRR. Review of the submittals will not be conducted until after review by the Agency or its Designated Representative. Review of the submittal items will require a minimum of four (4) weeks after receipt from the Agency.

**TABLE 2**

| ITEM | DESCRIPTION  | SETS REQD. | NOTES   |
|------|--|------------|---|
| 1    | Shop drawings  | 4          | Steel and Concrete members  |
| 2    | Bearings   | 4          | For entire structures   |
| 3    | Concrete Mix Designs   | 4          | For entire structures   |
| 4    | Rebar & Strand certifications                                    | 4          | For superstructure only   |
| 5    | 28 day concrete strength   | 4          | For superstructure only   |
| 6    | Waterproofing material certifications and installation procedure | 4          | Waterproofing & protective boards   |
| 7    | Structural steel certifications                                  | 4          | All fracture critical members & other members requiring improved notch toughness            |
| 8    | Fabrication and Test reports                                     | 4          | All fracture critical members & other members requiring improved notch toughness            |
| 9    | Welding Procedures and Welder Certification                      | 4          | AWS requirements  |
| 10   | Foundation Construction Reports                                  | 4          | Pile driving, drilled shaft construction, bearing pressure test reports for spread footings |
| 11   | Compaction testing reports for backfill at abutments             | 4          | Must meet 95% maximum dry density, Modified Proctor ASTM D1557                              |

D. As-Built Records shall be submitted to the UPRR within 60 days of completion of the structures. These records shall consist of the following items:

Overpass Projects

1. Electronic files of all structure design drawings with as-constructed modifications shown, in Microstation J or Acrobat .PDF format.
2. Hard copies of all structure design drawings with as-constructed modifications shown.

Underpass Projects

1. Electronic files of all structure design drawings with as-constructed modifications shown, in Microstation J or Acrobat .PDF format.
2. Hard copies of all structure design drawings with as-constructed modifications shown.

3. Final approved copies of shop drawings for concrete and steel members.
4. Foundation Construction Reports
5. Compaction testing reports for backfill at abutments

### **1.16 APPROVAL OF DETAILS**

The details of the construction affecting the UPRR tracks and property not already included in the Contract Plans shall be submitted to UPRR's Designated Representative through the Agency for UPRR's review and written approval before such work is undertaken. Review and approval of these submittals will require a minimum of four (4) weeks in addition to the Agency's review time as stated elsewhere in these bid documents.

### **1.17 MAINTENANCE OF RAILROAD FACILITIES**

A. The Contractor shall be required to maintain all ditches and drainage structures free of silt or other obstructions which may result from Contractor's operations; to promptly repair eroded areas within UPRR's right of way and to repair any other damage to the property of UPRR, or its tenants.

B. All such maintenance and repair of damages due to the Contractor's operations shall be done at the Contractor's expense.

C. The Contractor must submit a proposed method of erosion control and have the method reviewed by the UPRR prior to beginning any grading on the Project Site. Erosion control methods must comply with all applicable local, state, and federal regulations.

### **1.18 SITE INSPECTIONS BY UPRR'S DESIGNATED REPRESENTATIVE**

A. In addition to the office reviews of construction submittals, site inspections may be performed by UPRR's Designated Representative at significant points during construction, including but not limited to the following:

1. Preconstruction meetings.
2. Pile driving, drilling of caissons or drilled shafts.
3. Reinforcement and concrete placement for railroad bridge substructure and/or superstructure.
4. Erection of precast concrete or steel bridge superstructure.
5. Placement of waterproofing (prior to placing ballast on bridge deck).
6. Completion of the bridge structure.

B. Site inspection is not limited to the milestone events listed above. Site visits to check progress of the work may be performed at any time throughout the construction as deemed necessary by UPRR.

C. A detailed construction schedule, including the proposed temporary horizontal and vertical clearances and construction sequence for all work to be performed, shall be provided to the Agency for submittal to UPRR's Designated Representative for review prior to commencement of work. This schedule shall also include the anticipated dates when the above listed events will occur. This schedule shall be updated for the above listed events as necessary, but at least monthly so that site visits may be scheduled.

### **1.19 UPRR REPRESENTATIVES**

A. UPRR representatives, conductors, flag person or watch person will be provided by UPRR at expense of the Agency or Contractor (as stated elsewhere in these bid documents) to protect UPRR facilities, property and movements of its trains or engines. In general, UPRR will furnish such personnel or other protective services as follows:

1. When any part of any equipment is standing or being operated within 25 feet, measured horizontally, from centerline of any track on which trains may operate, or when any object is off the ground and any dimension thereof could extend inside the 25 foot limit, or when any erection or construction activities are in progress within such limits, regardless of elevation above or below track.
2. For any excavation below elevation of track subgrade if, in the opinion of UPRR's Designated Representative, track or other UPRR facilities may be subject to settlement or movement.
3. During any clearing, grubbing, excavation or grading in proximity to UPRR facilities, which, in the opinion of UPRR's Designated Representative, may endanger UPRR facilities or operations.
4. During any contractor's operations when, in the opinion of UPRR's Designated Representative, UPRR facilities, including, but not limited to, tracks, buildings, signals, wire lines, or pipe lines, may be endangered.
5. The Contractor shall arrange with the UPRR Designated Representative to provide the adequate number of flag persons to accomplish the work.

### **1.20 WALKWAYS REQUIRED**

Along the outer side of each exterior track of multiple operated track, and on each side of single operated track, an unobstructed continuous space suitable for trainman's use in walking along trains, extending to a line not less than twelve feet (12') from centerline of track, shall be maintained. Any temporary impediments to walkways and track drainage encroachments or obstructions allowed during work hours while UPRR's flagman service is provided shall be removed before the close of each work day. Walkways with railings shall be constructed by Contractor over open excavation areas when in close proximity of track, and railings shall not be closer than 8' – 6" horizontally from center line of tangent track or 9' – 6" horizontally from centerline of curved track.

### **1.21 COMMUNICATIONS AND SIGNAL LINES**

If required, UPRR will rearrange its communications and signal lines, its grade crossing warning devices, train signals and tracks, and facilities that are in use and maintained by UPRR's forces in connection with its operation at expense of the Agency. This work by UPRR will be done by its own forces and it is not a part of the Work under this Contract.

### **1.22 TRAFFIC CONTROL**

Contractor's operations that control traffic across or around UPRR facilities shall be coordinated with and approved by the UPRR's Designated Representative.

### **1.23 CONSTRUCTION EXCAVATIONS**

A. The Contractor shall be required to take special precaution and care in connection with excavating and shoring. Excavations for construction of footings, piers, columns, walls, or other facilities that require shoring shall comply with requirements of OSHA, AREMA and UPRR "Guidelines for Temporary Shoring".

B. The Contractor shall contact UPRR's "Call Before Your Dig" at least 48 hours prior to commencing work at 1-800-336-9193 during normal business hours (6:30 a.m. to 8:00 p.m. central time, Monday through Friday, except holidays - also a 24 hour, 7 day a week number for emergency calls) to determine location of fiber optics. If a telecommunications system is buried anywhere on or near UPRR property, the Contractor will co-ordinate with UPRR and the Telecommunication Company(ies) to arrange for relocation or other protection of the system prior to beginning any work on or near UPRR property.

### **1.24 RAILROAD FLAGGING**

Performance of any work by the Contractor in which person(s) or equipment will be within twenty-five (25) feet of any track, or will be near enough to any track that any equipment extension (such as, but not limited to, a crane boom) will reach within twenty-five (25) feet of any track, may require railroad flagging services or other protective measures. Contractor shall give the advance notice to the UPRR as required in Section 1.09 above before commencing any such work, so that the UPRR may determine the need for flagging or other protective measures to ensure the safety of the railroad's operations. Contractor must provide Railroad a minimum of five (5) days notice prior to the cessation of the need for a flagman. If five (5) days notice of cessation is not given, Contractor will still be required to pay flagging charges for the five (5) day notice period required by union agreement to be given to the employee, even though flagging is not required for that period. An additional ten (10) days notice must then be given to Railroad if flagging services are needed again after such five day cessation notice has been given to Railroad. Contractor shall comply with all other requirements regarding flagging services as specified by the UPRR. Any costs associated with failure to abide by these requirements will be borne by the Contractor.

The UPRR shall invoice the Agency directly for flagging services as outlined in this agreement.

## **1.25 CLEANING OF RIGHT-OF-WAY**

Contractor shall, upon completion of the work to be performed by Contractor upon the premises, over or beneath the tracks of UPRR, promptly remove from the Right-of-Way of UPRR all of Contractor's tools, implements, and other materials whether brought upon the Right-of-Way by Contractor or any subcontractors, employee or agent of Contractor or of any subcontractor, and leave the Right-of-Way in a clean and presentable condition to satisfaction of UPRR.

## **APPROVAL OF PROPOSED BORROW AREAS, USE AREAS, AND/OR WASTE AREAS INSIDE ILLINOIS STATE BORDERS (BDE)**

Effective: November 1, 2008

Revise the title of Article 107.22 of the Standard Specifications to read:

**"107.22 Approval of Proposed Borrow Areas, Use Areas, and/or Waste Areas Inside Illinois State Borders."**

Add the following sentence to the end of the first paragraph of Article 107.22 of the Standard Specifications:

"Proposed borrow areas, use areas, and/or waste areas outside of Illinois shall comply with Article 107.01."

## **AUTOMATED FLAGGER ASSISTANCE DEVICES (BDE)**

Effective: January 1, 2008

Description. This work shall consist of furnishing and operating automated flagger assistance devices (AFADs) as part of the work zone traffic control and protection for two-lane highways where two-way traffic is maintained over one lane of pavement. Use of these devices shall be at the option of the Contractor.

Equipment. AFADs shall be according to the FHWA memorandum, "MUTCD - Revised Interim Approval for the use of Automated Flagger Assistance Devices in Temporary Traffic Control Zones (IA-4R)", dated January 28, 2005. The devices shall be mounted on a trailer or a moveable cart and shall meet the requirements of NCHRP 350, Category 4.

The AFAD shall be the Stop/Slow type. This device uses remotely controlled "STOP" and "SLOW" signs to alternately control right-of-way.

Signs for the AFAD shall be according to Article 701.03 of the Standard Specifications and the MUTCD. The signs shall be 24 x 24 in. (600 x 600 mm) having an octagon shaped "STOP" sign on one side and a diamond shaped "SLOW" sign on the opposite side. The letters on the signs shall be 8 in. (200 mm) high. If the "STOP" sign has louvers, the full sign face shall be visible at a distance of 50 ft (15 m) and greater.

The signs shall be supplemented with one of the following types of lights.

- (a) Flashing Lights. When flashing lights are used, white or red flashing lights shall be mounted within the "STOP" sign face and white or yellow flashing lights within the "SLOW" sign face.
- (b) Stop and Warning Beacons. When beacons are used, a stop beacon shall be mounted 24 in. (600 mm) or less above the "STOP" sign face and a warning beacon mounted 24 in. (600 mm) or less above, below, or to the side of the "SLOW" sign face. As an option, a Type B warning light may be used in lieu of the warning beacon.

A "WAIT ON STOP" sign shall be placed on the right hand side of the roadway at a point where drivers are expected to stop. The sign shall be 24 x 30 in. (600 x 750 mm) with a black legend and border on a white background. The letters shall be at least 6 in. (150 mm) high.

This device may include a gate arm or mast arm that descends to a horizontal position when the "STOP" sign is displayed and rises to a vertical position when the "SLOW" sign is displayed. When included, the end of the arm shall reach at least to the center of the lane being controlled. The arm shall have alternating red and white retroreflective stripes, on both sides, sloping downward at 45 degrees toward the side on which traffic will pass. The stripes shall be 6 in. (150 mm) in width and at least 2 in. (50 mm) in height.

Flagging Requirements. Flaggers and flagging requirements shall be according to Article 701.13 of the Standard Specifications and the following.

AFADs shall be placed at each end of the traffic control, where a flagger is shown on the plans. The flaggers shall be able to view the face of the AFAD and approaching traffic during operation.

To stop traffic, the "STOP" sign shall be displayed, the corresponding lights/beacon shall flash, and when included, the gate arm shall descend to a horizontal position. To permit traffic to move, the "SLOW" sign shall be displayed, the corresponding lights/beacon shall flash, and when included, the gate arm shall rise to a vertical position.

If used at night, the AFAD location shall be illuminated according to Section 701 of the Standard Specifications.

When not in use, AFADs will be considered nonoperating equipment and shall be stored according to Article 701.11 of the Standard Specifications.

Basis of Payment. This work will not be paid for separately but shall be considered as included in the cost of the various traffic control items included in the contract.

**CEMENT (BDE)**

Effective: January 1, 2007

Revised: April 1, 2009

Revise Section 1001 of the Standard Specifications to read:

**"SECTION 1001. CEMENT**

**1001.01 Cement Types.** Cement shall be according to the following.

- (a) Portland Cement. Acceptance of portland cement shall be according to the current Bureau of Materials and Physical Research's Policy Memorandum, "Portland or Blended Cement Acceptance Procedure for Qualified and Non-Qualified Plants".

Portland cement shall be according to ASTM C 150, and shall meet the standard physical and chemical requirements. Type I or Type II may be used for cast-in-place, precast, and precast prestressed concrete. Type III may be used according to Article 1020.04, or when approved by the Engineer. All other cements referenced in ASTM C 150 may be used when approved by the Engineer.

The total of all organic processing additions shall be a maximum of 1.0 percent by weight (mass) of the cement. The total of all inorganic processing additions shall be a maximum of 4.0 percent by weight (mass) of the cement. However, a cement kiln dust inorganic processing addition shall be limited to a maximum of 1.0 percent. Organic processing additions shall be limited to grinding aids that improve the flowability of cement, reduce pack set, and improve grinding efficiency. Inorganic processing additions shall be limited to granulated blast-furnace slag according to the chemical requirements of AASHTO M 302, Class C fly ash according to the chemical requirements of AASHTO M 295, and cement kiln dust.

- (b) Portland-Pozzolan Cement. Acceptance of portland-pozzolan cement shall be according to the current Bureau of Materials and Physical Research's Policy Memorandum, "Portland or Blended Cement Acceptance Procedure for Qualified and Non-Qualified Plants".

Portland-pozzolan cement shall be according to ASTM C 595 and shall meet the standard physical and chemical requirements. Type IP may be used for cast-in-place, precast, and precast prestressed concrete, except when Class PP concrete is used. The pozzolan constituent for Type IP shall be a maximum of 21 percent of the weight (mass) of the portland-pozzolan cement.

For cast-in-place construction, portland-pozzolan cement shall not be used in concrete mixtures when the air temperature is below 40 °F (4 °C) without permission of the Engineer. If permission is given, the mix design strength requirement may require the Contractor to increase the cement or eliminate the cement factor reduction for a water-reducing or high range water-reducing admixture which is permitted according to Article 1020.05(b).

The total of all organic processing additions shall be a maximum of 1.0 percent by weight (mass) of the cement. Organic processing additions shall be limited to grinding aids as defined in (a) above. Inorganic processing additions shall be limited to cement kiln dust at a maximum of 1.0 percent.

- (c) Portland Blast-Furnace Slag Cement. Acceptance of portland blast-furnace slag cement shall be according to the current Bureau of Materials and Physical Research's Policy Memorandum, "Portland or Blended Cement Acceptance Procedure for Qualified and Non-Qualified Plants".

Portland blast-furnace slag cement shall be according to ASTM C 595 and shall meet the standard physical and chemical requirements. Type IS portland blast-furnace slag cement may be used for cast-in-place, precast, and precast prestressed concrete, except when Class PP concrete is used. The blast-furnace slag constituent for Type IS shall be a maximum of 25 percent of the weight (mass) of the portland blast-furnace slag cement.

For cast-in-place construction, portland blast-furnace slag cement shall not be used in concrete mixtures when the air temperature is below 40 °F (4 °C) without permission of the Engineer. If permission is given, the mix design strength requirement may require the Contractor to increase the cement or eliminate the cement factor reduction for a water-reducing or high range water-reducing admixture which is permitted according to Article 1020.05(b).

The total of all organic processing additions shall be a maximum of 1.0 percent by weight (mass) of the cement. Organic processing additions shall be limited to grinding aids as defined in (a) above. Inorganic processing additions shall be limited to cement kiln dust at a maximum of 1.0 percent.

(d) Rapid Hardening Cement. Rapid hardening cement shall be used according to Article 1020.04 or when approved by the Engineer. The cement shall be on the Department's current "Approved List of Packaged, Dry, Rapid Hardening Cementitious Materials for Concrete Repairs", and shall be according to the following.

(1) The cement shall have a maximum final set of 25 minutes, according to Illinois Modified ASTM C 191.

(2) The cement shall have a minimum compressive strength of 2000 psi (13,800 kPa) at 3.0 hours, 3200 psi (22,100 kPa) at 6.0 hours, and 4000 psi (27,600 kPa) at 24.0 hours, according to Illinois Modified ASTM C 109.

(3) The cement shall have a maximum drying shrinkage of 0.050 percent at seven days, according to Illinois Modified ASTM C 596.

(4) The cement shall have a maximum expansion of 0.020 percent at 14 days, according to Illinois Modified ASTM C 1038.

(5) The cement shall have a minimum 80 percent relative dynamic modulus of elasticity; and shall not have a weight (mass) gain in excess of 0.15 percent or a weight (mass) loss in excess of 1.0 percent, after 100 cycles, according to AASHTO T 161, Procedure B.

(e) Calcium Aluminate Cement. Calcium aluminate cement shall be used only where specified by the Engineer. The cement shall meet the standard physical requirements for Type I cement according to ASTM C 150, except the time of setting shall not apply. The chemical requirements shall be determined according to ASTM C 114 and shall be as follows: minimum 38 percent aluminum oxide ( $Al_2O_3$ ), maximum 42 percent calcium

oxide (CaO), maximum 1 percent magnesium oxide (MgO), maximum 0.4 percent sulfur trioxide (SO<sub>3</sub>), maximum 1 percent loss on ignition, and maximum 3.5 percent insoluble residue.

**1001.02 Uniformity of Color.** Cement contained in single loads or in shipments of several loads to the same project shall not have visible differences in color.

**1001.03 Mixing Brands and Types.** Different brands or different types of cement from the same manufacturing plant, or the same brand or type from different plants shall not be mixed or used alternately in the same item of construction unless approved by the Engineer.

**1001.04 Storage.** Cement shall be stored and protected against damage, such as dampness which may cause partial set or hardened lumps. Different brands or different types of cement from the same manufacturing plant, or the same brand or type from different plants shall be kept separate.”

## **CONCRETE ADMIXTURES (BDE)**

Effective: January 1, 2003

Revised: April 1, 2009

Replace the first paragraph of Article 1020.05(b) of the Standard Specifications to read:

“(b) Admixtures. The use of admixtures to increase the workability or to accelerate the hardening of the concrete will be permitted when approved by the Engineer. Admixture dosages shall result in the mixture meeting the specified plastic and hardened properties. The Department will maintain an Approved List of Corrosion Inhibitors. Corrosion inhibitor dosage rates shall be according to Article 1020.05(b)(12). The Department will also maintain an Approved List of Concrete Admixtures, and an admixture technical representative shall be consulted when determining an admixture dosage from this list. The dosage shall be within the range indicated on the approved list unless the influence by other admixtures, jobsite conditions (such as a very short haul time), or other circumstances warrant a dosage outside the range. The Engineer shall be notified when a dosage is proposed outside the range. To determine an admixture dosage, air temperature, concrete temperature, cement source and quantity, finely divided mineral sources(s) and quantity, influence of other admixtures, haul time, placement conditions, and other factors as appropriate shall be considered. The Engineer may request the Contractor to have a batch of concrete mixed in the lab or field to verify the admixture dosage is correct. An admixture dosage or combination of admixture dosages shall not delay the initial set of concrete by more than one hour. When a retarding admixture is required or appropriate for a bridge deck or bridge deck overlayer pour, the initial set time shall be delayed until the deflections due to the concrete dead load are no longer a concern for inducing cracks in the completed work. However, a retarding admixture shall not be used to further extend the pour time and justify the alteration of a bridge deck pour sequence.

When determining water in admixtures for water/cement ratio, the Contractor shall calculate 70 percent of the admixture dosage as water, except a value of 50 percent shall be used for a latex admixture used in bridge deck latex concrete overlays.”

Revise Section 1021 of the Standard Specifications to read:

**“SECTION 1021. CONCRETE ADMIXTURES**

**1021.01 General.** Admixtures shall be furnished in liquid form ready for use. The admixtures shall be delivered in the manufacturer's original containers, bulk tank trucks or such containers or tanks as are acceptable to the Engineer. Delivery shall be accompanied by a ticket which clearly identifies the manufacturer and trade name of the material. Containers shall be readily identifiable as to manufacturer and trade name of the material they contain.

Corrosion inhibitors will be maintained on the Department's Approved List of Corrosion Inhibitors. All other concrete admixture products will be maintained on the Department's Approved List of Concrete Admixtures. For the admixture submittal, a report prepared by an independent laboratory accredited by the AASHTO Materials Reference Laboratory (AMRL) for Portland Cement Concrete shall be provided. The report shall show the results of physical tests conducted no more than five years prior to the time of submittal, according to applicable specifications. However, for corrosion inhibitors the ASTM G 109 test information specified in ASTM C 1582 is not required to be from an independent lab. All other information in ASTM C 1582 shall be from an independent lab.

Tests shall be conducted using materials and methods specified on a "test" concrete and a "reference" concrete, together with a certification that no changes have been made in the formulation of the material since the performance of the tests. Per the manufacturer's option, the cement content for all required tests shall either be according to applicable specifications or 5.65 cwt/cu yd (335 kg/cu m). Compressive strength test results for six months and one year will not be required.

Prior to the approval of an admixture, the Engineer reserves the right to request a sample for testing. The test and reference concrete mixtures tested by the Engineer will contain a cement content of 5.65 cwt/cu yd (335 kg/cu m). For freeze-thaw testing, the Department will perform the test according to AASHTO T 161, Procedure B. The flexural strength test will be performed according to AASHTO T 177. If the Engineer decides to test the admixture, the manufacturer shall submit AASHTO T 197 water content and set time test results on the standard cement used by the Department. The test and reference concrete mixture shall contain a cement content of 5.65 cwt/cu yd (335 kg/cu m). The manufacturer may select their lab or an independent lab to perform this testing. The laboratory is not required to be accredited by AASHTO.

The manufacturer shall include in the submittal the following admixture information: the manufacturing range for specific gravity, the midpoint and manufacturing range for residue by oven drying, and the manufacturing range for pH. The submittal shall also include an infrared spectrophotometer trace no more than five years old.

For air-entraining admixtures according to Article 1021.02, the specific gravity allowable manufacturing range shall be established by the manufacturer and the test method shall be according to ASTM C 494. For residue by oven drying and pH, the allowable manufacturing range and test methods shall be according to ASTM C 260.

For admixtures according to Articles 1021.03, 1021.04, 1021.05, 1021.06, and 1021.07, the pH allowable manufacturing range shall be established by the manufacturer and the test method shall be according to ASTM E 70. For specific gravity and residue by oven drying, the allowable manufacturing range and test methods shall be according to ASTM C 494.

When test results are more than seven years old, the manufacturer shall re-submit the infrared spectrophotometer trace and the report prepared by an independent laboratory accredited by AASHTO.

All admixtures, except chloride-based accelerators, shall contain a maximum of 0.3 percent chloride by weight (mass).

Random field samples may be taken by the Department to verify an admixture meets specification. A split sample will be provided to the manufacturer if requested. Admixtures that do not meet specification requirements or an allowable manufacturing range established by the manufacturer shall be replaced with new material.

**1021.02 Air-Entraining Admixtures.** Air-entraining admixtures shall be according to AASHTO M 154.

**1021.03 Retarding and Water-Reducing Admixtures.** The admixture shall be according to the following.

- (a) The retarding admixture shall be according to AASHTO M 194, Type B (retarding) or Type D (water-reducing and retarding).
- (b) The water-reducing admixture shall be according to AASHTO M 194, Type A.
- (c) The high range water-reducing admixture shall be according to AASHTO M 194, Type F (high range water-reducing) or Type G (high range water-reducing and retarding).

**1021.04 Accelerating Admixtures.** The admixture shall be according to AASHTO M 194, Type C (accelerating) or Type E (water reducing and accelerating).

**1021.05 Self-Consolidating Admixtures.** The self-consolidating admixture system shall consist of either a high range water-reducing admixture only or a high range water-reducing admixture combined with a separate viscosity modifying admixture. The one or two component admixture system shall be capable of producing a concrete mixture that can flow around reinforcement and consolidate under its own weight without additional effort and without segregation.

The high range water-reducing admixture shall be according to AASHTO M 194, Type F.

The viscosity modifying admixture shall be according to ASTM C 494, Type S (specific performance).

**1021.06 Rheology-Controlling Admixture.** The rheology-controlling admixture shall be capable of producing a concrete mixture with a lower yield stress that will consolidate easier for slipform applications used by the Contractor. The rheology-controlling admixture shall be according to ASTM C 494, Type S (specific performance).

**1021.07 Corrosion Inhibitor.** The corrosion inhibitor shall be according to one of the following.

(a) Calcium Nitrite. The corrosion inhibitor shall contain a minimum 30 percent calcium nitrite by weight (mass) of solution, and shall comply with the requirements of AASHTO M 194, Type C (accelerating).

(b) Other Materials. The corrosion inhibitor shall be according to ASTM C 1582.”

### **CONSTRUCTION AIR QUALITY - IDLING RESTRICTIONS (BDE)**

Effective: April 1, 2009

Idling Restrictions. The Contractor shall establish truck-staging areas for all diesel powered vehicles that are waiting to load or unload material at the jobsite. Staging areas shall be located where the diesel emissions from the equipment will have a minimum impact on adjacent sensitive receptors. The Department will review the selection of staging areas, whether within or outside the existing highway right-of-way, to avoid locations near sensitive areas or populations to the extent possible. Sensitive receptors include, but are not limited to, hospitals, schools, residences, motels, hotels, daycare facilities, elderly housing and convalescent facilities. Diesel powered engines shall also be located as far away as possible from fresh air intakes, air conditioners, and windows. The Engineer will approve staging areas before implementation.

Diesel powered vehicle operators may not cause or allow the motor vehicle, when it is not in motion, to idle for more than a total of 10 minutes within any 60 minute period, except under any of the following circumstances:

- 1) The motor vehicle has a gross vehicle weight rating of less than 8000 lb (3630 kg).
- 2) The motor vehicle idles while forced to remain motionless because of on-highway traffic, an official traffic control device or signal, or at the direction of a law enforcement official.
- 3) The motor vehicle idles when operating defrosters, heaters, air conditioners, or other equipment solely to prevent a safety or health emergency.
- 4) A police, fire, ambulance, public safety, other emergency or law enforcement motor vehicle, or any motor vehicle used in an emergency capacity, idles while in an emergency or training mode and not for the convenience of the vehicle operator.
- 5) The primary propulsion engine idles for maintenance, servicing, repairing, or diagnostic purposes if idling is necessary for such activity.
- 6) A motor vehicle idles as part of a government inspection to verify that all equipment is in good working order, provided idling is required as part of the inspection.
- 7) When idling of the motor vehicle is required to operate auxiliary equipment to accomplish the intended use of the vehicle (such as loading, unloading, mixing, or processing cargo; controlling cargo temperature; construction operations, lumbering operations; oil or gas

well servicing; or farming operations), provided that this exemption does not apply when the vehicle is idling solely for cabin comfort or to operate non-essential equipment such as air conditioning, heating, microwave ovens, or televisions.

- 8) When the motor vehicle idles due to mechanical difficulties over which the operator has no control.
- 9) The outdoor temperature is less than 32 °F (0 °C) or greater than 80 °F (26 °C).

When the outdoor temperature is greater than or equal to 32 °F (0 °C) or less than or equal to 80 °F (26 °C), a person who operates a motor vehicle operating on diesel fuel shall not cause or allow the motor vehicle to idle for a period greater than 30 minutes in any 60 minute period while waiting to weigh, load, or unload cargo or freight, unless the vehicle is in a line of vehicles that regularly and periodically moves forward.

The above requirements do not prohibit the operation of an auxiliary power unit or generator set as an alternative to idling the main engine of a motor vehicle operating on diesel fuel.

Environmental Deficiency Deduction. When the Engineer is notified, or determines that an environmental control deficiency exists based on non-compliance with the idling restrictions, he/she will notify the Contractor, and direct the Contractor to correct the deficiency.

If the Contractor fails to correct the deficiency a monetary deduction will be imposed. The monetary deduction will be \$1,000.00 for each deficiency identified.

#### **DETERMINATION OF THICKNESS (BDE)**

Effective: April 1, 2009

Revise Articles 353.12 and 353.13 of the Standard Specifications to Articles 353.13 and 353.14 respectively.

Add the following Article to the Standard Specifications:

**“353.12 Tolerance in Thickness.** The thickness of base course pay items that individually contain at least 1000 sq yd (840 sq m) of contiguous area, except for temporary construction, bike paths, and individual locations less than 500 ft (150 m) long, will be evaluated. Temporary construction is defined as those areas constructed and removed under the same contract. If the base course cannot be cored for thickness prior to placement of the cover layer(s), the Engineer will determine the thickness of the cover layer(s), and subtract them from the measured core thickness to determine the base course thickness.

The procedure described in Article 407.10(b) will be followed, except the option of correcting deficient pavement with additional lift(s) shall not apply.”

Revise Article 354.09 of the Standard Specifications to read:

**“354.09 Tolerance in Thickness.** The thickness of base course widening pay items that individually contain at least 1000 sq yd (840 sq m) of contiguous area, except for temporary construction; bike paths and individual locations less than 3 ft (1 m) wide or 1000 ft (300 m)

long, will be evaluated. Temporary construction is defined as those areas constructed and removed under the same contract. If the base course widening cannot be cored for thickness prior to placement of the cover layer(s), the Engineer will determine the thickness of the cover layer(s), and subtract them from the measured core thickness to determine the base course widening thickness.

The procedure described in Article 407.10(b) will be followed, except:

- (a) The width of a unit shall be the width of the widening along one edge of the pavement.
- (b) The length of the unit shall be 1000 ft (300 m).
- (c) The option of correcting deficient pavement with additional lift(s) shall not apply.”

Revise Article 355.09 of the Standard Specifications to read:

**“355.09 Tolerance in Thickness.** The thickness of HMA base course pay items that individually contain at least 1000 sq yd (840 sq m) of contiguous area, except for temporary construction; bike paths and individual locations less than 500 ft (150 m) long, will be evaluated according to Article 407.10(b). Temporary construction is defined as those areas constructed and removed under the same contract. If the base course cannot be cored for thickness prior to placement of the cover layer(s), the Engineer will determine the thickness of the cover layer(s), and subtract them from the measured core thickness to determine the base course thickness.”

Revise Article 356.07 of the Standard Specifications to read:

**“356.07 Tolerance in Thickness.** The thickness of HMA base course widening pay items that individually contain at least 1000 sq yd (840 sq m) of contiguous area, except for temporary construction; bike paths and individual locations less than 3 ft (1 m) wide or 1000 ft (300 m) long, will be evaluated according to Article 407.10(b) except, the width of a unit shall be the width of the widening along one edge of the pavement and the length of a unit shall be 1000 ft (300 m). Temporary locations are defined as those constructed and removed under the same contract. If the base course widening cannot be cored for thickness prior to placement of the cover layer(s), the Engineer will determine the thickness of the cover layer(s) and subtract them from the measured core thickness to determine the base course widening thickness.”

Revise Article 407.10 of the Standard Specifications to read:

**“407.10 Tolerance in Thickness.** Determination of pavement thickness shall be performed after the pavement surface tests and corrective action have been completed according to Article 407.09. Pay adjustments made for pavement thickness will be in addition to and independent of those made for pavement smoothness. Pavement pay items that individually contain at least 1000 sq yd (840 sq m) of contiguous pavement shall be evaluated with the following exclusions: temporary pavements; variable width pavements; radius returns; short lengths of contiguous pavements less than 500 ft (125 m) in length; and constant width portions of turn lanes less than 500 ft (125 m) in length. Temporary pavements are defined as pavements constructed and removed under the same contract.

The method described in Article 407.10(a), shall be used except for those pavements constructed in areas where access to side streets and entrances necessitates construction in segments less than 1000 ft (300 m). The method described in Article 407.10(b) shall be used in areas where access to side streets and entrances necessitates construction in segments less than 1000 ft (300 m).

(a) Percent Within Limits. The percent within limits (PWL) method shall be as follows.

- (1) Lots and Sublots. The pavement will be divided into approximately equal lots of not more than 5000 ft (1500 m) in length. When the length of a continuous strip of pavement is 500 ft (150 m) or greater but less than 5000 ft (1500 m), these short lengths of pavement, ramps, turn lanes, and other short sections of continuous pavement will be grouped together to form lots approximately 5000 ft (1500 m) in length. Short segments between structures will be measured continuously with the structure segments omitted. Each lot will be subdivided into ten equal sublots. The width of a subplot and lot will be the width from the pavement edge to the adjacent lane line, from one lane line to the next, or between pavement edges for single-lane pavements.
- (2) Cores. Cores 2 in. (50 mm) in diameter shall be taken from the pavement by the Contractor, at locations selected by the Engineer. The exact location for each core will be selected at random, but will result in one core per subplot. Core locations will be specified prior to beginning the coring operations.

The Contractor and the Engineer shall witness the coring operations, as well as the measuring and recording of the core lengths. The cores will be measured with a device supplied by the Department immediately upon removal from the core bit and prior to moving to the next core location. Upon concurrence of the length, the core samples shall be disposed of according to Article 202.03.

Upon completion of each core, all water shall be removed from the hole and the hole then filled with a rapid hardening mortar or concrete. The material shall be mixed in a separate container, placed in the hole, consolidated by rodding, and struck-off flush with the adjacent pavement.

- (3) Deficient Sublot. When the length of the core in a subplot is deficient by more than ten percent of plan thickness, the Contractor may take three additional cores within that subplot at locations selected at random by the Engineer. If the Contractor chooses not to take additional cores, the pavement in that subplot shall be removed and replaced.

When the three additional cores are taken, the length of those cores will be averaged with the original core length. If the average shows the subplot to be deficient by ten percent or less, no additional action is necessary. If the average shows the subplot to be deficient by more than ten percent, the pavement in that subplot shall be removed and replaced; however, when requested in writing by the Contractor, the Engineer may permit in writing such deficient sublots to remain in place. For deficient sublots allowed to remain in place, additional lift(s) may be placed, at no additional cost to

the Department, to bring the deficient pavement to plan thickness when the Engineer determines grade control conditions will permit such lift(s). The area(s) to be overlaid, material to be used, thickness(es) of the lift(s), and method of placement will be approved by the Engineer.

When a deficient subplot is removed and replaced, or additional lifts are placed, the corrected subplot shall be retested for thickness. The length of the new core taken in the subplot will be used in determining the PWL for the lot.

When a deficient subplot is left in place, and no additional lift(s) are placed, no payment will be made for the deficient subplot. The length of the original core taken in the subplot will be used in determining the PWL for the lot.

- (4) Deficient Lot. After addressing deficient sublots, the PWL for each lot will be determined. When the PWL of a lot is 60 percent or less, the pavement in that lot shall be removed and replaced; however, when requested in writing by the Contractor, the Engineer may permit in writing such deficient lots to remain in place. For deficient lots allowed to remain in place, additional lift(s) may be placed, at no additional cost to the Department, to bring the deficient pavement to plan thickness when the Engineer determines grade control conditions will permit such lift(s). The area(s) to be overlaid, material to be used, thickness(es) of the lift(s), and method of placement will be approved by the Engineer.

When a deficient lot is removed and replaced, or additional lifts are placed, the corrected lot shall be retested for thickness. The PWL for the lot will then be recalculated based upon the new cores; however, the pay factor for the lot shall be a maximum of 100 percent.

When a deficient lot is left in place, and no additional lift(s) are placed, the PWL for the lot will not be recalculated.

- (5) Right of Discovery. When the Engineer has reason to believe the random core selection process will not accurately represent the true conditions of the work, he/she may order additional cores. The additional cores shall be taken at specific locations determined by the Engineer. The Engineer will provide notice to the Contractor containing an explanation of the reasons for his/her action. The need for, and location of, additional cores will be determined prior to commencement of coring operations.

When the additional cores show the pavement to be deficient by more than ten percent of plan thickness, more additional cores shall be taken to determine the limits of the deficient pavement and that area shall be removed and replaced; however, when requested in writing by the Contractor, the Engineer may permit in writing such areas of deficient pavement to remain in place. The area of deficient pavement will be defined using the length between two acceptable cores and the full width of the subplot. An acceptable core is a core with a length of at least 90 percent of plan thickness.

For deficient areas allowed to remain in place, additional lift(s) may be placed, at no additional cost to the Department, to bring the deficient pavement to plan thickness when the Engineer determines grade control conditions will permit such lift(s). The area(s) to be overlaid, material to be used, thickness(es) of the lift(s), and method of placement will be approved by the Engineer.

When an area of deficient pavement is removed and replaced, or additional lifts are placed, the corrected pavement shall be retested for thickness.

When an area of deficient pavement is left in place, and no additional lift(s) are placed, no payment will be made for the deficient pavement.

When the additional cores show the pavement to be at least 90 percent of plan thickness, the additional cores will be paid for according to Article 109.04.

- (6) Profile Index Adjustment. After any area of pavement is removed and replaced or any additional lifts are placed, the corrected areas shall be retested for pavement smoothness and any necessary profile index adjustments and/or corrections will be made based on these final profile readings prior to retesting for thickness.
- (7) Determination of PWL. The PWL for each lot will be determined as follows.

Definitions:

- $x_i$  = Individual values (core lengths) under consideration
- $n$  = Number of individual values under consideration (10 per lot)
- $\bar{x}$  = Average of the values under consideration
- LSL = Lower Specification Limit (98% of plan thickness)
- $Q_L$  = Lower Quality Index
- $s$  = Sample Standard Deviation
- PWL = Percent Within Limits

Determine  $\bar{x}$  for the lot to the nearest two decimal places.

Determine  $s$  for the lot to the nearest three decimal places using:

$$s = \sqrt{\frac{\sum(x_i - \bar{x})^2}{n - 1}} \quad \text{where} \quad \sum(x_i - \bar{x})^2 = (x_1 - \bar{x})^2 + (x_2 - \bar{x})^2 + \dots + (x_{10} - \bar{x})^2$$

Determine  $Q_L$  for the lot to the nearest two decimal places using:

$$Q_L = \frac{(\bar{x} - LSL)}{s}$$

Determine PWL for the lot using the  $Q_L$  and the following table. For  $Q_L$  values less than zero the value shown in the table must be subtracted from 100 to obtain PWL.

- (8) Pay Factors. The pay factor (PF) for each lot will be determined, to the nearest two decimal places, using:

$$PF \text{ (in percent)} = 55 + 0.5 (PWL)$$

If  $\bar{x}$  for a lot is less than the plan thickness, the maximum PF for that lot shall be 100 percent.

- (9) Payment. Payment of incentive or disincentive for pay items subject to the PWL method will be calculated using:

$$\text{Payment} = (((TPF/100)-1) \times CUP) \times (TOTPAVT - DEFPAVT)$$

TPF = Total Pay Factor

CUP = Contract Unit Price

TOTPAVT = Area of Pavement Subject to Coring

DEFPAVT = Area of Deficient Pavement

The TPF for the pavement shall be the average of the PF for all the lots; however, the TPF shall not exceed 102 percent.

Area of Deficient pavement (DEFPAVT) is defined as an area of pavement represented by a subplot deficient by more than ten percent which is left in place with no additional thickness added.

Area of Pavement Subject to Coring (TOTPAVT) is defined as those pavement areas included in lots for pavement thickness determination.

| PERCENT WITHIN LIMITS            |                             |                                  |                             |                                  |                             |                                  |                             |
|----------------------------------|-----------------------------|----------------------------------|-----------------------------|----------------------------------|-----------------------------|----------------------------------|-----------------------------|
| Quality Index (Q <sub>L</sub> )* | Percent Within Limits (PWL) | Quality Index (Q <sub>L</sub> )* | Percent Within Limits (PWL) | Quality Index (Q <sub>L</sub> )* | Percent Within Limits (PWL) | Quality Index (Q <sub>L</sub> )* | Percent Within Limits (PWL) |
| 0.00                             | 50.00                       | 0.40                             | 65.07                       | 0.80                             | 78.43                       | 1.20                             | 88.76                       |
| 0.01                             | 50.38                       | 0.41                             | 65.43                       | 0.81                             | 78.72                       | 1.21                             | 88.97                       |
| 0.02                             | 50.77                       | 0.42                             | 65.79                       | 0.82                             | 79.02                       | 1.22                             | 89.17                       |
| 0.03                             | 51.15                       | 0.43                             | 66.15                       | 0.83                             | 79.31                       | 1.23                             | 89.38                       |
| 0.04                             | 51.54                       | 0.44                             | 66.51                       | 0.84                             | 79.61                       | 1.24                             | 89.58                       |
| 0.05                             | 51.92                       | 0.45                             | 66.87                       | 0.85                             | 79.90                       | 1.25                             | 89.79                       |
| 0.06                             | 52.30                       | 0.46                             | 67.22                       | 0.86                             | 80.19                       | 1.26                             | 89.99                       |
| 0.07                             | 52.69                       | 0.47                             | 67.57                       | 0.87                             | 80.47                       | 1.27                             | 90.19                       |
| 0.08                             | 53.07                       | 0.48                             | 67.93                       | 0.88                             | 80.76                       | 1.28                             | 90.38                       |
| 0.09                             | 53.46                       | 0.49                             | 68.28                       | 0.89                             | 81.04                       | 1.29                             | 90.58                       |
| 0.10                             | 53.84                       | 0.50                             | 68.63                       | 0.90                             | 81.33                       | 1.30                             | 90.78                       |
| 0.11                             | 54.22                       | 0.51                             | 68.98                       | 0.91                             | 81.61                       | 1.31                             | 90.96                       |
| 0.12                             | 54.60                       | 0.52                             | 69.32                       | 0.92                             | 81.88                       | 1.32                             | 91.15                       |
| 0.13                             | 54.99                       | 0.53                             | 69.67                       | 0.93                             | 82.16                       | 1.33                             | 91.33                       |
| 0.14                             | 55.37                       | 0.54                             | 70.01                       | 0.94                             | 82.43                       | 1.34                             | 91.52                       |
| 0.15                             | 55.75                       | 0.55                             | 70.36                       | 0.95                             | 82.71                       | 1.35                             | 91.70                       |
| 0.16                             | 56.13                       | 0.56                             | 70.70                       | 0.96                             | 82.97                       | 1.36                             | 91.87                       |
| 0.17                             | 56.51                       | 0.57                             | 71.04                       | 0.97                             | 83.24                       | 1.37                             | 92.04                       |
| 0.18                             | 56.89                       | 0.58                             | 71.38                       | 0.98                             | 83.50                       | 1.38                             | 92.22                       |
| 0.19                             | 57.27                       | 0.59                             | 71.72                       | 0.99                             | 83.77                       | 1.39                             | 92.39                       |
| 0.20                             | 57.65                       | 0.60                             | 72.06                       | 1.00                             | 84.03                       | 1.40                             | 92.56                       |
| 0.21                             | 58.03                       | 0.61                             | 72.39                       | 1.01                             | 84.28                       | 1.41                             | 92.72                       |
| 0.22                             | 58.40                       | 0.62                             | 72.72                       | 1.02                             | 84.53                       | 1.42                             | 92.88                       |
| 0.23                             | 58.78                       | 0.63                             | 73.06                       | 1.03                             | 84.79                       | 1.43                             | 93.05                       |
| 0.24                             | 59.15                       | 0.64                             | 73.39                       | 1.04                             | 85.04                       | 1.44                             | 93.21                       |
| 0.25                             | 59.53                       | 0.65                             | 73.72                       | 1.05                             | 85.29                       | 1.45                             | 93.37                       |
| 0.26                             | 59.90                       | 0.66                             | 74.04                       | 1.06                             | 85.53                       | 1.46                             | 93.52                       |
| 0.27                             | 60.28                       | 0.67                             | 74.36                       | 1.07                             | 85.77                       | 1.47                             | 93.67                       |
| 0.28                             | 60.65                       | 0.68                             | 74.69                       | 1.08                             | 86.02                       | 1.48                             | 93.83                       |
| 0.29                             | 61.03                       | 0.69                             | 75.01                       | 1.09                             | 86.26                       | 1.49                             | 93.98                       |
| 0.30                             | 61.40                       | 0.70                             | 75.33                       | 1.10                             | 86.50                       | 1.50                             | 94.13                       |
| 0.31                             | 61.77                       | 0.71                             | 75.64                       | 1.11                             | 86.73                       | 1.51                             | 94.27                       |
| 0.32                             | 62.14                       | 0.72                             | 75.96                       | 1.12                             | 86.96                       | 1.52                             | 94.41                       |
| 0.33                             | 62.51                       | 0.73                             | 76.27                       | 1.13                             | 87.20                       | 1.53                             | 94.54                       |
| 0.34                             | 62.88                       | 0.74                             | 76.59                       | 1.14                             | 87.43                       | 1.54                             | 94.68                       |
| 0.35                             | 63.25                       | 0.75                             | 76.90                       | 1.15                             | 87.66                       | 1.55                             | 94.82                       |
| 0.36                             | 63.61                       | 0.76                             | 77.21                       | 1.16                             | 87.88                       | 1.56                             | 94.95                       |
| 0.37                             | 63.98                       | 0.77                             | 77.51                       | 1.17                             | 88.10                       | 1.57                             | 95.08                       |
| 0.38                             | 64.34                       | 0.78                             | 77.82                       | 1.18                             | 88.32                       | 1.58                             | 95.20                       |
| 0.39                             | 64.71                       | 0.79                             | 78.12                       | 1.19                             | 88.54                       | 1.59                             | 95.33                       |

\*For Q<sub>L</sub> values less than zero, subtract the table value from 100 to obtain PWL

| PERCENT WITHIN LIMITS (continued) |                             |                                  |                             |                                  |                             |
|-----------------------------------|-----------------------------|----------------------------------|-----------------------------|----------------------------------|-----------------------------|
| Quality Index (Q <sub>L</sub> )*  | Percent Within Limits (PWL) | Quality Index (Q <sub>L</sub> )* | Percent Within Limits (PWL) | Quality Index (Q <sub>L</sub> )* | Percent Within Limits (PWL) |
| 1.60                              | 95.46                       | 2.00                             | 98.83                       | 2.40                             | 99.89                       |
| 1.61                              | 95.58                       | 2.01                             | 98.88                       | 2.41                             | 99.90                       |
| 1.62                              | 95.70                       | 2.02                             | 98.92                       | 2.42                             | 99.91                       |
| 1.63                              | 95.81                       | 2.03                             | 98.97                       | 2.43                             | 99.91                       |
| 1.64                              | 95.93                       | 2.04                             | 99.01                       | 2.44                             | 99.92                       |
| 1.65                              | 96.05                       | 2.05                             | 99.06                       | 2.45                             | 99.93                       |
| 1.66                              | 96.16                       | 2.06                             | 99.10                       | 2.46                             | 99.94                       |
| 1.67                              | 96.27                       | 2.07                             | 99.14                       | 2.47                             | 99.94                       |
| 1.68                              | 96.37                       | 2.08                             | 99.18                       | 2.48                             | 99.95                       |
| 1.69                              | 96.48                       | 2.09                             | 99.22                       | 2.49                             | 99.95                       |
| 1.70                              | 96.59                       | 2.10                             | 99.26                       | 2.50                             | 99.96                       |
| 1.71                              | 96.69                       | 2.11                             | 99.29                       | 2.51                             | 99.96                       |
| 1.72                              | 96.78                       | 2.12                             | 99.32                       | 2.52                             | 99.97                       |
| 1.73                              | 96.88                       | 2.13                             | 99.36                       | 2.53                             | 99.97                       |
| 1.74                              | 96.97                       | 2.14                             | 99.39                       | 2.54                             | 99.98                       |
| 1.75                              | 97.07                       | 2.15                             | 99.42                       | 2.55                             | 99.98                       |
| 1.76                              | 97.16                       | 2.16                             | 99.45                       | 2.56                             | 99.98                       |
| 1.77                              | 97.25                       | 2.17                             | 99.48                       | 2.57                             | 99.98                       |
| 1.78                              | 97.33                       | 2.18                             | 99.50                       | 2.58                             | 99.99                       |
| 1.79                              | 97.42                       | 2.19                             | 99.53                       | 2.59                             | 99.99                       |
| 1.80                              | 97.51                       | 2.20                             | 99.56                       | 2.60                             | 99.99                       |
| 1.81                              | 97.59                       | 2.21                             | 99.58                       | 2.61                             | 99.99                       |
| 1.82                              | 97.67                       | 2.22                             | 99.61                       | 2.62                             | 99.99                       |
| 1.83                              | 97.75                       | 2.23                             | 99.63                       | 2.63                             | 100.00                      |
| 1.84                              | 97.83                       | 2.22                             | 99.66                       | 2.64                             | 100.00                      |
| 1.85                              | 97.91                       | 2.25                             | 99.68                       | ≥ 2.65                           | 100.00                      |
| 1.86                              | 97.98                       | 2.26                             | 99.70                       |                                  |                             |
| 1.87                              | 98.05                       | 2.27                             | 99.72                       |                                  |                             |
| 1.88                              | 98.11                       | 2.28                             | 99.73                       |                                  |                             |
| 1.89                              | 98.18                       | 2.29                             | 99.75                       |                                  |                             |
| 1.90                              | 98.25                       | 2.30                             | 99.77                       |                                  |                             |
| 1.91                              | 98.31                       | 2.31                             | 99.78                       |                                  |                             |
| 1.92                              | 98.37                       | 2.32                             | 99.80                       |                                  |                             |
| 1.93                              | 98.44                       | 2.33                             | 99.81                       |                                  |                             |
| 1.94                              | 98.50                       | 2.34                             | 99.83                       |                                  |                             |
| 1.95                              | 98.56                       | 2.35                             | 99.84                       |                                  |                             |
| 1.96                              | 98.61                       | 2.36                             | 99.85                       |                                  |                             |
| 1.97                              | 98.67                       | 2.37                             | 99.86                       |                                  |                             |
| 1.98                              | 98.72                       | 2.38                             | 99.87                       |                                  |                             |
| 1.99                              | 98.78                       | 2.39                             | 99.88                       |                                  |                             |

\*For Q<sub>L</sub> values less than zero, subtract the table value from 100 to obtain PWL

(b) Minimum Thickness. The minimum thickness method shall be as follows.

- (1) Length of Units. The length of a unit will be a continuous strip of pavement 500 ft (150 m) in length.
- (2) Width of Units. The width of a unit will be the width from the pavement edge to the adjacent lane line, from one lane line to the next, or between pavement edges for single-lane pavements.
- (3) Thickness Measurements. Pavement thickness will be based on 2 in. (50 mm) diameter cores.

Cores shall be taken from the pavement by the Contractor at locations selected by the Engineer. When determining the thickness of a unit, one core shall be taken in each unit.

The Contractor and the Engineer shall witness the coring operations, as well as the measuring and recording of the cores. Core measurements will be determined immediately upon removal from the core bit and prior to moving to the next core location. Upon concurrence of the length, the core samples may be disposed of according to Article 202.03.

Upon completion of each core, all water shall be removed from the hole and the hole then filled with a rapid hardening mortar or concrete. The material shall be mixed in a separate container, placed in the hole, consolidated by rodding, and struck-off flush with the adjacent pavement.

- (4) Unit Deficient in Thickness. In considering any portion of the pavement that is deficient, the entire limits of the unit will be used in computing the deficiency or determining the remedial action required.
- (5) Thickness Equals or Exceeds Specified Thickness. When the thickness of a unit equals or exceeds the specified plan thickness, payment will be made at the contract unit price per square yard (square meter) for the specified thickness.
- (6) Thickness Deficient by Ten Percent or Less. When the thickness of a unit is less than the specified plan thickness by ten percent or less, a deficiency deduction will be assessed against payment for the item involved. The deficiency will be a percentage of the contract unit price as given in the following table.

| Percent Deficiency<br>(of Plan Thickness) | Percent Deduction<br>(of Contract Unit Price) |
|---|---|
| 0.0 to 2.0                                | 0   |
| 2.1 to 3.0                                | 20  |
| 3.1 to 4.0                                | 28  |
| 4.1 to 5.0                                | 32  |
| 5.1 to 7.5                                | 43  |
| 7.6 to 10.0                               | 50  |

- (7) Thickness Deficient by More than Ten Percent. When a core shows the pavement to be deficient by more than ten percent of plan thickness, additional cores shall be taken on each side of the deficient core, at stations selected by the Contractor and offsets selected by the Engineer, to determine the limits of the deficient pavement. No core shall be located within 5 ft (1.5 m) of a previous core obtained for thickness determination. The first acceptable core obtained on each side of a deficient core will be used to determine the length of the deficient pavement. An acceptable core is a core with a thickness of at least 90 percent of plan thickness. The area of deficient pavement will be defined using the length between two acceptable cores and the full width of the unit. The area of deficient pavement shall be removed and replaced; however, when requested in writing by the Contractor, the Engineer may permit in writing such areas of deficient pavement to remain in place. For deficient areas allowed to remain in place, additional lift(s) may be placed, at no additional cost to the Department, to bring the deficient pavement to plan thickness when the Engineer determines grade control conditions will permit such lift(s). The area(s) to be overlaid, material to be used, thickness(es) of the lift(s), and method of placement will be approved by the Engineer.

When an area of deficient pavement is removed and replaced, or additional lifts are placed, the corrected pavement shall be retested for thickness. The thickness of the new core will be used to determine the pay factor for the corrected area.

When an area of deficient pavement is left in place, and no additional lift(s) are placed, no payment will be made for the deficient pavement. In addition, an amount equal to two times the contract cost of the deficient pavement will be deducted from the compensation due the Contractor.

The thickness of the first acceptable core on each side of the core more than ten percent deficient will be used to determine any needed pay adjustments for the remaining areas on each side of the area deficient by more than ten percent. The pay adjustment will be determined according to Article 407.10(b)(6).

- (8) Right of Discovery. When the Engineer has reason to believe any core location does not accurately represent the true conditions of the work, he/she may order additional cores. These additional cores shall be taken at specific locations determined by the Engineer. The Engineer will provide notice to the Contractor containing an explanation of the reasons for his/her action.

When the additional cores show the pavement to be deficient by more than ten percent of plan thickness, the procedures outlined in Article 407.10(b)(7) shall be followed, except the Engineer will determine the additional core locations.

When the additional cores, ordered by the Engineer, show the pavement to be at least 90 percent of plan thickness, the additional cores will be paid for according to Article 109.04.

- (9) Profile Index Adjustment. After any area of pavement is removed and replaced or any additional lifts are added, the corrected areas shall be retested for pavement smoothness and any necessary profile index adjustments and/or corrections will be made based on these final profile readings prior to retesting for thickness.”

Revise Article 482.06 of the Standard Specifications to read:

**“482.06 Tolerance in Thickness.** The shoulder shall be constructed to the thickness shown on the plans. When the contract includes square yards (square meters) as the unit of measurement for HMA shoulder, thickness determinations shall be made according to Article 407.10(b)(3) and the following.

- (a) Length of the Units. The length of a unit shall be a continuous strip of shoulder 2500 ft (750 m) long.
- (b) Width of the Units. The width of the unit shall be the full width of the shoulder.
- (c) Thickness Deficient by More than Ten Percent. When a core shows the shoulder to be deficient by more than ten percent of plan thickness, additional cores shall be taken on each side of the deficient core, at stations selected by the Contractor and offsets selected by the Engineer, to determine the limits of the deficient shoulder. No core shall be located within 5 ft (1.5 m) of a previous core obtained for thickness determination. The first acceptable core obtained on each side of a deficient core will be used to determine the length of the deficient shoulder. An acceptable core is a core with a thickness of at least 90 percent of plan thickness. The area of deficient shoulder will be defined using the length between two acceptable cores and the full width of the unit. The area of deficient shoulder shall be brought to specified thickness by the addition of the applicable mixture, at no additional cost to the Department and subject to the lift thickness requirements of Article 312.05, or by removal and replacement with a new mixture. However, the surface elevation of the completed shoulder shall not exceed by more than 1/8 in. (3 mm) the surface elevation of the adjacent pavement. When requested in writing by the Contractor, the Engineer may permit in writing such thin shoulder to remain in place. When an area of thin shoulder is left in place, and no additional lift(s) are placed, no payment will be made for the thin shoulder. In addition, an amount equal to two times the contract unit price of the shoulder will be deducted from the compensation due the Contractor.

When an area of deficient shoulder is removed and replaced, or additional lifts are placed, the corrected pavement shall be retested for thickness.

- (d) Right of Discovery. When the Engineer has reason to believe any core location does not accurately represent the true conditions of the work, he/she may order additional cores. When the additional cores, ordered by the Engineer, show the shoulder to be at least 90 percent of plan thickness, the additional cores will be paid for according to Article 109.04. When the additional core shows the shoulder to be less than 90 percent of plan thickness, the procedure in (c), above shall be followed.”

Revise Article 483.07 of the Standard Specifications to read:

**“483.07 Tolerance in Thickness.** The shoulder shall be constructed to the thickness shown on the plans. Thickness determinations shall be made according to Article 482.06 except the option of correcting deficient pavement with additional lift(s) shall not apply.”

**DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION (DBE)**

Effective: September 1, 2000

Revised: November 1, 2008

FEDERAL OBLIGATION. The Department of Transportation, as a recipient of federal financial assistance, is required to take all necessary and reasonable steps to ensure nondiscrimination in the award and administration of contracts. Consequently, the federal regulatory provisions of 49 CFR part 26 apply to this contract concerning the utilization of disadvantaged business enterprises. For the purposes of this Special Provision, a disadvantaged business enterprise (DBE) means a business certified by the Department in accordance with the requirements of 49 CFR part 26 and listed in the Illinois Unified Certification Program (IL UCP) DBE Directory or most recent addendum.

STATE OBLIGATION. This Special Provision will also be used by the Department to satisfy the requirements of the Business Enterprise for Minorities, Females, and Persons with Disabilities Act, 30 ILCS 575. When this Special Provision is used to satisfy state law requirements on 100 percent state-funded contracts, the federal government has no involvement in such contracts (not a federal-aid contract) and no responsibility to oversee the implementation of this Special Provision by the Department on those contracts. DBE participation on 100 percent state-funded contracts will not be credited toward fulfilling the Department's annual overall DBE goal required by the US Department of Transportation to comply with the federal DBE program requirements.

CONTRACTOR ASSURANCE. The Contractor makes the following assurance and agrees to include the assurance in each subcontract that the Contractor signs with a subcontractor:

The Contractor, subrecipient, or subcontractor shall not discriminate on the basis of race, color, national origin, or sex in the performance of this contract. The Contractor shall carry out applicable requirements of 49 CFR part 26 in the award and administration of contracts funded in whole or in part with federal or state funds. Failure by the Contractor to carry out these requirements is a material breach of this contract, which may result in the termination of this contract or such other remedy as the recipient deems appropriate.

OVERALL GOAL SET FOR THE DEPARTMENT. As a requirement of compliance with 49 CFR part 26, the Department has set an overall goal for DBE participation in its federally assisted contracts. That goal applies to all federal-aid funds the Department will expend in its federally assisted contracts for the subject reporting fiscal year. The Department is required to make a good faith effort to achieve the overall goal. The dollar amount paid to all approved DBE companies performing work called for in this contract is eligible to be credited toward fulfillment of the Department's overall goal.

CONTRACT GOAL TO BE ACHIEVED BY THE CONTRACTOR. This contract includes a specific DBE utilization goal established by the Department. The goal has been included because the Department has determined that the work of this contract has subcontracting

opportunities that may be suitable for performance by DBE companies. This determination is based on an assessment of the type of work, the location of the work, and the availability of DBE companies to do a part of the work. The assessment indicates that, in the absence of unlawful discrimination, and in an arena of fair and open competition, DBE companies can be expected to perform 5.0% of the work. This percentage is set as the DBE participation goal for this contract. Consequently, in addition to the other award criteria established for this contract, the Department will award this contract to a bidder who makes a good faith effort to meet this goal of DBE participation in the performance of the work. A bidder makes a good faith effort for award consideration if either of the following is done in accordance with the procedures set forth in this Special Provision:

- (a) The bidder documents that firmly committed DBE participation has been obtained to meet the goal; or
- (b) The bidder documents that a good faith effort has been made to meet the goal, even though the effort did not succeed in obtaining enough DBE participation to meet the goal.

DBE LOCATOR REFERENCES. Bidders may consult the IL UCP DBE Directory as a reference source for DBE-certified companies. In addition, the Department maintains a letting and item specific DBE locator information system whereby DBE companies can register their interest in providing quotes on particular bid items advertised for letting. Information concerning DBE companies willing to quote work for particular contracts may be obtained by contacting the Department's Bureau of Small Business Enterprises at telephone number (217)785-4611, or by visiting the Department's web site at [www.dot.il.gov](http://www.dot.il.gov).

BIDDING PROCEDURES. Compliance with the bidding procedures of this Special Provision is required prior to the award of the contract and the failure of the as-read low bidder to comply will render the bid not responsive.

- (a) In order to assure the timely award of the contract, the as-read low bidder shall submit a Disadvantaged Business Utilization Plan on Department form SBE 2026 within seven working days after the date of letting. To meet the seven day requirement, the bidder may send the Plan by certified mail or delivery service within the seven working day period. If a question arises concerning the mailing date of a Plan, the mailing date will be established by the U.S. Postal Service postmark on the original certified mail receipt from the U.S. Postal Service or the receipt issued by a delivery service. It is the responsibility of the bidder to ensure that the postmark or receipt date is affixed within the seven working days if the bidder intends to rely upon mailing or delivery to satisfy the submission day requirement. The Plan is to be submitted to the Department of Transportation, Bureau of Small Business Enterprises, Contract Compliance Section, 2300 South Dirksen Parkway, Room 319, Springfield, Illinois 62764 (Telefax: (217)785-1524). It is the responsibility of the bidder to obtain confirmation of telefax delivery. The Department will not accept a Utilization Plan if it does not meet the seven day submittal requirement and the bid will be declared not responsive. In the event the bid is declared not responsive due to a failure to submit a Plan or failure to comply with the bidding procedures set forth herein, the Department may elect to cause the forfeiture of the penal sum of the bidder's proposal guaranty, and may deny authorization to bid the

project if re-advertised for bids. The Department reserves the right to invite any other bidder to submit a Utilization Plan at any time for award consideration or to extend the time for award.

- (b) The Utilization Plan shall indicate that the bidder either has obtained sufficient DBE participation commitments to meet the contract goal or has not obtained enough DBE participation commitments in spite of a good faith effort to meet the goal. The Utilization Plan shall further provide the name, telephone number, and telefax number of a responsible official of the bidder designated for purposes of notification of plan approval or disapproval under the procedures of this Special Provision.
- (c) The Utilization Plan shall include a DBE Participation Commitment Statement, Department form SBE 2025, for each DBE proposed for the performance of work to achieve the contract goal. The signatures on these forms must be original signatures. All elements of information indicated on the said form shall be provided, including but not limited to the following:
  - (1) The name and address of each DBE to be used;
  - (2) A description, including pay item numbers, of the commercially useful work to be done by each DBE;
  - (3) The price to be paid to each DBE for the identified work specifically stating the quantity, unit price, and total subcontract price for the work to be completed by the DBE. If partial pay items are to be performed by the DBE, indicate the portion of each item, a unit price where appropriate and the subcontract price amount;
  - (4) A commitment statement signed by the bidder and each DBE evidencing availability and intent to perform commercially useful work on the project; and
  - (5) If the bidder is a joint venture comprised of DBE companies and non-DBE companies, the plan must also include a clear identification of the portion of the work to be performed by the DBE partner(s).
- (d) The contract will not be awarded until the Utilization Plan submitted by the bidder is approved. The Utilization Plan will be approved by the Department if the Plan commits sufficient commercially useful DBE work performance to meet the contract goal. The Utilization Plan will not be approved by the Department if the Plan does not commit sufficient DBE performance to meet the contract goal unless the bidder documents that it made a good faith effort to meet the goal. The good faith procedures of Section VIII of this special provision apply. If the Utilization Plan is not approved because it is deficient in a technical matter, unless waived by the Department, the bidder will be notified and will be allowed no less than a five working day period in order to cure the deficiency.

CALCULATING DBE PARTICIPATION. The Utilization Plan values represent work anticipated to be performed and paid for upon satisfactory completion. The Department is only able to count toward the achievement of the overall goal and the contract goal the value of payments made for the work actually performed by DBE companies. In addition, a DBE must perform a

commercially useful function on the contract to be counted. A commercially useful function is generally performed when the DBE is responsible for the work and is carrying out its responsibilities by actually performing, managing, and supervising the work involved. The Department and Contractor are governed by the provisions of 49 CFR part 26.55(c) on questions of commercially useful functions as it affects the work. Specific counting guidelines are provided in 49 CFR part 26.55, the provisions of which govern over the summary contained herein.

- (a) DBE as the Contractor: 100 percent goal credit for that portion of the work performed by the DBE's own forces, including the cost of materials and supplies. Work that a DBE subcontracts to a non-DBE does not count toward the DBE goals.
- (b) DBE as a joint venture Contractor: 100 percent goal credit for that portion of the total dollar value of the contract equal to the distinct, clearly defined portion of the work performed by the DBE's own forces.
- (c) DBE as a subcontractor: 100 percent goal credit for the work of the subcontract performed by the DBE's own forces, including the cost of materials and supplies, excluding the purchase of materials and supplies or the lease of equipment by the DBE subcontractor from the prime Contractor or its affiliates. Work that a DBE subcontractor in turn subcontracts to a non-DBE does not count toward the DBE goal.
- (d) DBE as a trucker: 100 percent goal credit for trucking participation provided the DBE is responsible for the management and supervision of the entire trucking operation for which it is responsible. At least one truck owned, operated, licensed, and insured by the DBE must be used on the contract. Credit will be given for the full value of all such DBE trucks operated using DBE employed drivers. Goal credit will be limited to the value of the reasonable fee or commission received by the DBE if trucks are leased from a non-DBE company.
- (e) DBE as a material supplier:
  - (1) 60 percent goal credit for the cost of the materials or supplies purchased from a DBE regular dealer.
  - (2) 100 percent goal credit for the cost of materials or supplies obtained from a DBE manufacturer.
  - (3) 100 percent credit for the value of reasonable fees and commissions for the procurement of materials and supplies if not a regular dealer or manufacturer.

GOOD FAITH EFFORT PROCEDURES. If the bidder cannot obtain sufficient DBE commitments to meet the contract goal, the bidder must document in the Utilization Plan the good faith efforts made in the attempt to meet the goal. This means that the bidder must show that all necessary and reasonable steps were taken to achieve the contract goal. Necessary and reasonable steps are those which could reasonably be expected to obtain sufficient DBE participation. The Department will consider the quality, quantity, and intensity of the kinds of

efforts that the bidder has made. Mere *pro forma* efforts are not good faith efforts; rather, the bidder is expected to have taken those efforts that would be reasonably expected of a bidder actively and aggressively trying to obtain DBE participation sufficient to meet the contract goal.

- (a) The following is a list of types of action that the Department will consider as part of the evaluation of the bidder's good faith efforts to obtain participation. These listed factors are not intended to be a mandatory checklist and are not intended to be exhaustive. Other factors or efforts brought to the attention of the Department may be relevant in appropriate cases, and will be considered by the Department.
- (1) Soliciting through all reasonable and available means (e.g. attendance at pre-bid meetings, advertising and/or written notices) the interest of all certified DBE companies that have the capability to perform the work of the contract. The bidder must solicit this interest within sufficient time to allow the DBE companies to respond to the solicitation. The bidder must determine with certainty if the DBE companies are interested by taking appropriate steps to follow up initial solicitations.
  - (2) Selecting portions of the work to be performed by DBE companies in order to increase the likelihood that the DBE goals will be achieved. This includes, where appropriate, breaking out contract work items into economically feasible units to facilitate DBE participation, even when the prime Contractor might otherwise prefer to perform these work items with its own forces.
  - (3) Providing interested DBE companies with adequate information about the plans, specifications, and requirements of the contract in a timely manner to assist them in responding to a solicitation.
  - (4) a. Negotiating in good faith with interested DBE companies. It is the bidder's responsibility to make a portion of the work available to DBE subcontractors and suppliers and to select those portions of the work or material needs consistent with the available DBE subcontractors and suppliers, so as to facilitate DBE participation. Evidence of such negotiation includes the names, addresses, and telephone numbers of DBE companies that were considered; a description of the information provided regarding the plans and specifications for the work selected for subcontracting; and evidence as to why additional agreements could not be reached for DBE companies to perform the work.  
  
b. A bidder using good business judgment would consider a number of factors in negotiating with subcontractors, including DBE subcontractors, and would take a firm's price and capabilities as well as contract goals into consideration. However, the fact that there may be some additional costs involved in finding and using DBE companies is not in itself sufficient reason for a bidder's failure to meet the contract DBE goal, as long as such costs are reasonable. Also, the ability or desire of a bidder to perform the work of a contract with its own organization does not relieve the bidder of the responsibility to make good faith efforts. Bidders are not, however, required to accept higher quotes from DBE companies if the price difference is excessive or unreasonable.

- (5) Not rejecting DBE companies as being unqualified without sound reasons based on a thorough investigation of their capabilities. The bidder's standing within its industry, membership in specific groups, organizations, or associations and political or social affiliations (for example union vs. non-union employee status) are not legitimate causes for the rejection or non-solicitation of bids in the bidder's efforts to meet the project goal.
  - (6) Making efforts to assist interested DBE companies in obtaining bonding, lines of credit, or insurance as required by the recipient or Contractor.
  - (7) Making efforts to assist interested DBE companies in obtaining necessary equipment, supplies, materials, or related assistance or services.
  - (8) Effectively using the services of available minority/women community organizations; minority/women contractors' groups; local, state, and federal minority/women business assistance offices; and other organizations as allowed on a case-by-case basis to provide assistance in the recruitment and placement of DBE companies.
- (b) If the Department determines that the bidder has made a good faith effort to secure the work commitment of DBE companies to meet the contract goal, the Department will award the contract provided that it is otherwise eligible for award. If the Department determines that a good faith effort has not been made, the Department will notify the bidder of that preliminary determination by contacting the responsible company official designated in the Utilization Plan. The preliminary determination shall include a statement of reasons why good faith efforts have not been found, and may include additional good faith efforts that the bidder could take. The notification will designate a five working day period during which the bidder shall take additional efforts. The bidder is not limited by a statement of additional efforts, but may take other action beyond any stated additional efforts in order to obtain additional DBE commitments. The bidder shall submit an amended Utilization Plan if additional DBE commitments to meet the contract goal are secured. If additional DBE commitments sufficient to meet the contract goal are not secured, the bidder shall report the final good faith efforts made in the time allotted. All additional efforts taken by the bidder will be considered as part of the bidder's good faith efforts. If the bidder is not able to meet the goal after taking additional efforts, the Department will make a pre-final determination of the good faith efforts of the bidder and will notify the designated responsible company official of the reasons for an adverse determination.
- (c) The bidder may request administrative reconsideration of a pre-final determination adverse to the bidder within the five working days after the notification date of the determination by delivering the request to the Department of Transportation, Bureau of Small Business Enterprises, Contract Compliance Section, 2300 South Dirksen Parkway, Room 319, Springfield, Illinois 62764 (Telefax: (217)785-1524). Deposit of the request in the United States mail on or before the fifth business day shall not be deemed delivery. The pre-final determination shall become final if a request is not made and delivered. A request may provide additional written documentation and/or argument concerning the issue of whether an adequate good faith effort was made to meet the contract goal. In addition, the request shall be considered a consent by the bidder to extend the time for award. The request will be forwarded to the Department's

Reconsideration Officer. The Reconsideration Officer will extend an opportunity to the bidder to meet in person in order to consider all issues of whether the bidder made a good faith effort to meet the goal. After the review by the Reconsideration Officer, the bidder will be sent a written decision within ten working days after receipt of the request for reconsideration, explaining the basis for finding that the bidder did or did not meet the goal or make adequate good faith efforts to do so. A final decision by the Reconsideration Officer that a good faith effort was made shall approve the Utilization Plan submitted by the bidder and shall clear the contract for award. A final decision that a good faith effort was not made shall render the bid not responsive.

CONTRACT COMPLIANCE. Compliance with this Special Provision is an essential part of the contract. The Department is prohibited by federal regulations from crediting the participation of a DBE included in the Utilization Plan toward either the contract goal or the Department's overall goal until the amount to be applied toward the goals has been paid to the DBE. The following administrative procedures and remedies govern the compliance by the Contractor with the contractual obligations established by the Utilization Plan. After approval of the Plan and award of the contract, the Utilization Plan and individual DBE Participation Statements become part of the contract. If the Contractor did not succeed in obtaining enough DBE participation to achieve the advertised contract goal, and the Utilization Plan was approved and contract awarded based upon a determination of good faith, the total dollar value of DBE work calculated in the approved Utilization Plan as a percentage of the awarded contract value shall become the amended contract goal.

- (a) No amendment to the Utilization Plan may be made without prior written approval from the Department's Bureau of Small Business Enterprises. All requests for amendment to the Utilization Plan shall be submitted to the Department of Transportation, Bureau of Small Business Enterprises, Contract Compliance Section, 2300 South Dirksen Parkway, Room 319, Springfield, Illinois 62764. Telephone number (217) 785-4611. Telefax number (217) 785-1524.
- (b) All work indicated for performance by an approved DBE shall be performed, managed, and supervised by the DBE executing the Participation Statement. The Contractor shall not terminate for convenience a DBE listed in the Utilization Plan and then perform the work of the terminated DBE with its own forces, those of an affiliate or those of another subcontractor, whether DBE or not, without first obtaining the written consent of the Bureau of Small Business Enterprises to amend the Utilization Plan. If a DBE listed in the Utilization Plan is terminated for reasons other than convenience, or fails to complete its work on the contract for any reason, the Contractor shall make good faith efforts to find another DBE to substitute for the terminated DBE. The good faith efforts shall be directed at finding another DBE to perform at least the same amount of work under the contract as the DBE that was terminated, but only to the extent needed to meet the contract goal or the amended contract goal. The Contractor shall notify the Bureau of Small Business Enterprises of any termination for reasons other than convenience, and shall obtain approval for inclusion of the substitute DBE in the Utilization Plan. If good faith efforts following a termination of a DBE for cause are not successful, the Contractor shall contact the Bureau of Small Business Enterprises and provide a full accounting of the efforts undertaken to obtain substitute DBE participation. The Bureau of Small

Business Enterprises will evaluate the good faith efforts in light of all circumstances surrounding the performance status of the contract, and determine whether the contract goal should be amended.

- (c) The Contractor shall maintain a record of payments for work performed to the DBE participants. The records shall be made available to the Department for inspection upon request. After the performance of the final item of work or delivery of material by a DBE and final payment therefore to the DBE by the Contractor, but not later than thirty calendar days after payment has been made by the Department to the Contractor for such work or material, the Contractor shall submit a DBE Payment Agreement on Department form SBE 2115 to the Regional Engineer. If full and final payment has not been made to the DBE, the DBE Payment Agreement shall indicate whether a disagreement as to the payment required exists between the Contractor and the DBE or if the Contractor believes that the work has not been satisfactorily completed. If the Contractor does not have the full amount of work indicated in the Utilization Plan performed by the DBE companies indicated in the Plan, the Department will deduct from contract payments to the Contractor the amount of the goal not achieved as liquidated and ascertained damages.
- (d) The Department reserves the right to withhold payment to the Contractor to enforce the provisions of this Special Provision. Final payment shall not be made on the contract until such time as the Contractor submits sufficient documentation demonstrating achievement of the goal in accordance with this Special Provision or after liquidated damages have been determined and collected.
- (e) Notwithstanding any other provision of the contract, including but not limited to Article 109.09 of the Standard Specifications, the Contractor may request administrative reconsideration of a decision to deduct the amount of the goal not achieved as liquidated damages. A request to reconsider shall be delivered to the Contract Compliance Section and shall be handled and considered in the same manner as set forth in paragraph (c) of "Good Faith Effort Procedures" of this Special Provision, except a final decision that a good faith effort was not made during contract performance to achieve the goal agreed to in the Utilization Plan shall be the final administrative decision of the Department.

**DOWEL BARS (BDE)**

Effective: April 1, 2007

Revised: January 1, 2008

Revise the fifth and sixth sentences of Article 1006.11(b) of the Standard Specifications to read:

"The bars shall be epoxy coated according to AASHTO M 284, except the thickness of the epoxy shall be 7 to 12 mils (0.18 to 0.30 mm) and patching of the ends will not be required. The epoxy coating applicator shall be certified according to the current Bureau of Materials and Physical Research Policy Memorandum, "Epoxy Coating Plant Certification Procedure". The Department will maintain an approved list."

## **EQUIPMENT RENTAL RATES (BDE)**

Effective: August 2, 2007

Revised: January 2, 2008

Replace the second and third paragraphs of Article 105.07(b)(4)a. of the Standard Specifications with the following:

“Equipment idled which cannot be used on other work, and which is authorized to standby on the project site by the Engineer, will be paid for according to Article 109.04(b)(4).”

Replace Article 109.04(b)(4) of the Standard Specifications with the following:

“(4) Equipment. Equipment used for extra work shall be authorized by the Engineer. The equipment shall be specifically described, be of suitable size and capacity for the work to be performed, and be in good operating condition. For such equipment, the Contractor will be paid as follows.

- a. Contractor Owned Equipment. Contractor owned equipment will be paid for by the hour using the applicable FHWA hourly rate from the “Equipment Watch Rental Rate Blue Book” (Blue Book) in effect when the force account work begins. The FHWA hourly rate is calculated as follows.

$$\text{FHWA hourly rate} = (\text{monthly rate}/176) \times (\text{model year adj.}) \times (\text{Illinois adj.}) + \text{EOC}$$

Where: EOC = Estimated Operating Costs per hour (from the Blue Book)

The time allowed will be the actual time the equipment is operating on the extra work. For the time required to move the equipment to and from the site of the extra work and any authorized idle (standby) time, payment will be made at the following hourly rate:  $0.5 \times (\text{FHWA hourly rate} - \text{EOC})$ .

All time allowed shall fall within the working hours authorized for the extra work.

The rates above include the cost of fuel, oil, lubrication, supplies, small tools, necessary attachments, repairs, overhaul and maintenance of any kind, depreciation, storage, overhead, profits, insurance, and all incidentals. The rates do not include labor.

The Contractor shall submit to the Engineer sufficient information for each piece of equipment and its attachments to enable the Engineer to determine the proper equipment category. If a rate is not established in the Blue Book for a particular piece of equipment, the Engineer will establish a rate for that piece of equipment that is consistent with its cost and use in the industry.

- b. Rented Equipment. Whenever it is necessary for the Contractor to rent equipment to perform extra work, the rental and transportation costs of the equipment plus five percent for overhead will be paid. In no case shall the rental rates exceed those of established distributors or equipment rental agencies.

All prices shall be agreed to in writing before the equipment is used.”

**FLAGGER AT SIDE ROADS AND ENTRANCES (BDE)**

Effective: April 1, 2009

Revise the second paragraph of Article 701.13(a) of the Standard Specifications to read:

“The Engineer will determine when a side road or entrance shall be closed to traffic. A flagger will be required at each side road or entrance remaining open to traffic within the operation where two-way traffic is maintained on one lane of pavement. The flagger shall be positioned as shown on the plans or as directed by the Engineer.”

Revise the first and second paragraph of Article 701.20(i) of the Standard Specifications to read:

“Signs, barricades, or other traffic control devices required by the Engineer over and above those specified will be paid for according to Article 109.04. All flaggers required at side roads and entrances remaining open to traffic including those that are shown on the Highway Standards and/or additional barricades required by the Engineer to close side roads and entrances will be paid for according to Article 109.04.”

**HOT-MIX ASPHALT - FIELD VOIDS IN THE MINERAL AGGREGATE (BDE)**

Effective: April 1, 2007

Revised: April 1, 2008

Add the following to the table in Article 1030.05(d)(2)a. of the Standard Specifications:

| “Parameter | Frequency of Tests   | Frequency of Tests | Test Method<br>See Manual of Test<br>Procedures for<br>Materials |
|------------|--|--------------------|--|
|            | High ESAL Mixture<br>Low ESAL Mixture  | All Other Mixtures |  |
| VMA        | Day’s production<br>≥ 1200 tons:   | N/A                | Illinois-Modified<br>AASHTO R 35                                 |
| Note 5.    | 1 per half day of production   |                    |  |
|            | Day’s production<br>< 1200 tons:   |                    |  |
|            | 1 per half day of production for<br>first 2 days and 1 per day<br>thereafter (first sample of the day) |                    |  |

Note 5. The  $G_{sb}$  used in the voids in the mineral aggregate (VMA) calculation shall be the same average  $G_{sb}$  value listed in the mix design.”

Add the following to the Control Limits table in Article 1030.05(d)(4) of the Standard Specifications:

| "CONTROL LIMITS |                       |                       |                 |
|-----------------|-----------------------|-----------------------|-----------------|
| Parameter       | High ESAL<br>Low ESAL | High ESAL<br>Low ESAL | All Other       |
|                 | Individual Test       | Moving Avg. of 4      | Individual Test |
| VMA             | -0.7 % <sup>2/</sup>  | -0.5 % <sup>2/</sup>  | N/A             |

2/ Allowable limit below minimum design VMA requirement"

Add the following to the table in Article 1030.05(d)(5) of the Standard Specifications:

| "CONTROL CHART<br>REQUIREMENTS | High ESAL<br>Low ESAL | All Other |
|--------------------------------|-----------------------|-----------|
|                                | VMA"                  |           |

Revise the heading of Article 1030.05(d)(6)a.1. of the Standard Specifications to read:

"1. Voids, VMA, and Asphalt Binder Content."

Revise the first sentence of the first paragraph of Article 1030.05(d)(6)a.1.(a.) of the Standard Specifications to read:

"If the retest for voids, VMA, or asphalt binder content exceeds control limits, HMA production shall cease and immediate corrective action shall be instituted by the Contractor."

Revise the table in Article 1030.05(e) of the Standard Specifications to read:

| "Test Parameter                                     | Acceptable Limits of Precision |
|---|--------------------------------|
| % Passing: <sup>1/</sup>                            |                                |
| 1/2 in. (12.5 mm)                                   | 5.0 %                          |
| No. 4 (4.75 mm)                                     | 5.0 %                          |
| No. 8 (2.36 mm)                                     | 3.0 %                          |
| No. 30 (600 μm)                                     | 2.0 %                          |
| Total Dust Content<br>No. 200 (75 μm) <sup>1/</sup> | 2.2 %                          |
| Asphalt Binder Content                              | 0.3 %                          |
| Maximum Specific Gravity<br>of Mixture              | 0.026                          |
| Bulk Specific Gravity                               | 0.030                          |
| VMA   | 1.4 %                          |
| Density (% Compaction)                              | 1.0 % (Correlated)             |

1/ Based on washed ignition."

**HOT-MIX ASPHALT – PLANT TEST FREQUENCY (BDE)**

Effective: April 1, 2008

Revise the table in Article 1030.05(d)(2)a. of the Standard Specifications to read:

| "Parameter  | Frequency of Tests  | Frequency of Tests  | Test Method                                    |
|---|---|---|--|
|   | High ESAL Mixture<br>Low ESAL Mixture   | All Other Mixtures  | See Manual of Test<br>Procedures for Materials |
| Aggregate Gradation<br><br>Hot bins for batch and continuous plants.<br><br>Individual cold-feed or combined belt-feed for drier drum plants.<br><br>% passing sieves:<br>1/2 in. (12.5 mm),<br>No. 4 (4.75 mm),<br>No. 8 (2.36 mm),<br>No. 30 (600 μm)<br>No. 200 (75 μm)<br><br>Note 1. | 1 dry gradation per day of production (either morning or afternoon sample).<br>and<br>1 washed ignition oven test on the mix per day of production (conduct in the afternoon if dry gradation is conducted in the morning or vice versa).<br><br>Note 3.<br><br>Note 4. | 1 gradation per day of production.<br><br>The first day of production shall be a washed ignition oven test on the mix. Thereafter, the testing shall alternate between dry gradation and washed ignition oven test on the mix.<br><br>Note 4. | Illinois Procedure                             |
| Asphalt Binder Content by Ignition Oven<br><br>Note 2.  | 1 per half day of production  | 1 per day   | Illinois-Modified AASHTO T 308                 |
| Air Voids<br><br>Bulk Specific Gravity of Gyrotory Sample   | Day's production ≥ 1200 tons:<br><br>1 per half day of production<br><br>Day's production < 1200 tons:<br><br>1 per half day of production for first 2 days and 1 per day thereafter (first sample of the day)  | 1 per day   | Illinois-Modified AASHTO T 312                 |

|                                     |   |           |                                 |
|-------------------------------------|---|-----------|---------------------------------|
| Maximum Specific Gravity of Mixture | Day's production $\geq$ 1200 tons:<br><br>1 per half day of production  | 1 per day | Illinois-Modified AASHTO T 209" |
|                                     | Day's production < 1200 tons:<br><br>1 per half day of production for first 2 days and 1 per day thereafter (first sample of the day) |           |                                 |

**HOT-MIX ASPHALT – TRANSPORTATION (BDE)**

Effective: April 1, 2008

Revise Article 1030.08 of the Standard Specifications to read:

**“1030.08 Transportation.** Vehicles used in transporting HMA shall have clean and tight beds. The beds shall be sprayed with asphalt release agents from the Department’s approved list. In lieu of a release agent, the Contractor may use a light spray of water with a light scatter of manufactured sand (FA 20 or FA 21) evenly distributed over the bed of the vehicle. After spraying, the bed of the vehicle shall be in a completely raised position and it shall remain in this position until all excess asphalt release agent or water has been drained.

When the air temperature is below 60 °F (15 °C), the bed, including the end, endgate, sides and bottom shall be insulated with fiberboard, plywood or other approved insulating material and shall have a thickness of not less than 3/4 in (20 mm). When the insulation is placed inside the bed, the insulation shall be covered with sheet steel approved by the Engineer. Each vehicle shall be equipped with a cover of canvas or other suitable material meeting the approval of the Engineer which shall be used if any one of the following conditions is present.

- (a) Ambient air temperature is below 60 °F (15 °C).
- (b) The weather is inclement.
- (c) The temperature of the HMA immediately behind the paver screed is below 250 °F (120 °C).

The cover shall extend down over the sides and ends of the bed for a distance of approximately 12 in. (300 mm) and shall be fastened securely. The covering shall be rolled back before the load is dumped into the finishing machine.”

**IMPACT ATTENUATORS, TEMPORARY (BDE)**

Effective: November 1, 2003

Revised: January 1, 2007

Description. This work shall consist of furnishing, installing, maintaining, and removing temporary impact attenuators of the category and test level specified.

Materials. Materials shall meet the requirements of the impact attenuator manufacturer and the following:

| Item   | Article/Section           |
|--|---------------------------|
| (a) Fine Aggregate (Note 1)                              | 003.01                    |
| (b) Steel Posts, Structural Shapes, and Plates           | 1006.04                   |
| (c) Rail Elements, End Section Plates, and Splice Plates | 1006.25                   |
| (d) Bolts, Nuts, Washers and Hardware                    | 1006.25                   |
| (e) Hollow Structural Tubing                             | 1006.27(b)                |
| (f) Wood Posts and Wood Blockouts                        | 1007.01, 1007.02, 1007.06 |
| (g) Preservative Treatment                               | 1007.12                   |
| (h) Packaged Rapid Hardening Mortar                      | 1018.01                   |

Note 1. Fine aggregate shall be FA 1 or FA 2, Class A quality. The sand shall be unbagged and shall have a maximum moisture content of five percent.

CONSTRUCTION REQUIREMENTS

General. Impact Attenuators shall meet the testing criteria contained in National Cooperative Highway Research Program (NCHRP) Report 350 for the test level specified and shall be on the Department's approved list.

Installation. Regrading of slopes or approaches for the installation shall be as shown on the plans.

Attenuator bases, when required by the manufacturer, shall be constructed on a prepared subgrade according to the manufacturer's specifications. The surface of the base shall be slightly sloped or crowned to facilitate drainage.

Impact attenuators shall be installed according to the manufacturer's specifications and include all necessary transitions between the impact attenuator and the item to which it is attached.

When water filled attenuators are used between November 1 and April 15, they shall contain anti-freeze according to the manufacturer's recommendations.

Markings. Sand module impact attenuators shall be striped with alternating reflectorized Type AA or Type AP fluorescent orange and reflectorized white horizontal, circumferential stripes. There shall be at least two of each stripe on each module.

Other types of impact attenuators shall have a terminal marker applied to their nose and reflectors along their sides.

Maintenance. All maintenance of the impact attenuators shall be the responsibility of the Contractor until removal is directed by the Engineer.

Relocate. When relocation of temporary impact attenuators is specified, they shall be removed, relocated and reinstalled at the new location. The reinstallation requirements shall be the same as those for a new installation.

Removal. When the Engineer determines the temporary impact attenuators are no longer required, the installation shall be dismantled with all hardware becoming the property of the Contractor.

Surplus material shall be disposed of according to Article 202.03. Anti-freeze, when present, shall be disposed of/recycled according to local ordinances.

When impact attenuators have been anchored to the pavement, the anchor holes shall be repaired with rapid set mortar. Only enough water to permit placement and consolidation by rodding shall be used and the material shall be struck-off flush.

Method of Measurement. This work will be measured for payment as each, where each is defined as one complete installation.

Basis of Payment. This work will be paid for at the contract unit price per each for IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW); IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, WIDE); IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, RESETTABLE); IMPACT ATTENUATORS, TEMPORARY (SEVERE USE, NARROW); IMPACT ATTENUATORS, TEMPORARY (SEVERE USE, WIDE); or IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE) of the test level specified.

Relocation of the devices will be paid for at the contract unit price per each for IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE); IMPACT ATTENUATORS, RELOCATE (SEVERE USE); or IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE); of the test level specified.

Regrading of slopes or approaches will be paid for according to Section 202 and/or Section 204 of the Standard Specifications.

### **LIQUIDATED DAMAGES (BDE)**

Effective: April 1, 2009

Revise the table in Article 108.09 of the Standard Specifications to read:

| "Schedule of Deductions for Each Day of Overrun in Contract Time |                  |               |          |
|--|------------------|---------------|----------|
| Original Contract Amount   |                  | Daily Charges |          |
| From More Than   | To and Including | Calendar Day  | Work Day |
| \$ 0   | \$ 100,000       | \$ 375        | \$ 500   |
| 100,000  | 500,000          | 625           | 875      |
| 500,000  | 1,000,000        | 1,025         | 1,425    |
| 1,000,000  | 3,000,000        | 1,125         | 1,550    |
| 3,000,000  | 5,000,000        | 1,425         | 1,950    |
| 5,000,000  | 10,000,000       | 1,700         | 2,350    |
| 10,000,000   | And over         | 3,325         | 4,650"   |

**MULTILANE PAVEMENT PATCHING (BDE)**

Effective: November 1, 2002

Pavement broken and holes opened for patching shall be completed prior to weekend or holiday periods. Should delays of any type or for any reason prevent the completion of the work, temporary patches shall be constructed. Material able to support the average daily traffic and meeting the approval of the Engineer shall be used for the temporary patches. The cost of furnishing, placing, maintaining, removing and disposing of the temporary work, including traffic control, shall be the responsibility of the Contractor.

**NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM / EROSION AND SEDIMENT CONTROL DEFICIENCY DEDUCTION (BDE)**

Effective: April 1, 2007

Revised: November 1, 2008

Revise Article 105.03(a) of the Standard Specifications to read:

"(a) National Pollutant Discharge Elimination System (NPDES) / Erosion and Sediment Control Deficiency Deduction. When the Engineer is notified or determines an erosion and/or sediment control deficiency(s) exists, or the Contractor's activities represents a violation of the Department's NPDES permits, the Engineer will notify and direct the Contractor to correct the deficiency within a specified time. The specified time, which begins upon notification to the Contractor, will be from 1/2 hour to 1 week based on the urgency of the situation and the nature of the work effort required. The Engineer will be the sole judge.

A deficiency may be any lack of repair, maintenance, or implementation of erosion and/or sediment control devices included in the contract, or any failure to comply with the conditions of the Department's NPDES permits. A deficiency may also be applied to situations where corrective action is not an option such as the failure to participate in a

jobsite inspection of the project, failure to install required measures prior to initiating earth moving operations, disregard of concrete washout requirements, or other disregard of the NPDES permit.

If the Contractor fails to correct a deficiency within the specified time, a daily monetary deduction will be imposed for each calendar day or fraction thereof the deficiency exists. The calendar day(s) will begin with notification to the Contractor and end with the Engineer's acceptance of the correction. The daily monetary deduction will be either \$1000.00 or 0.05 percent of the awarded contract value, whichever is greater. For those deficiencies where corrective action was not an option, the monetary deduction will be immediate and will be valued at one calendar day."

### **PAYMENTS TO SUBCONTRACTORS (BDE)**

Effective: June 1, 2000

Revised: January 1, 2006

Federal regulations found at 49 CFR §26.29 mandate the Department to establish a contract clause to require Contractors to pay subcontractors for satisfactory performance of their subcontracts and to set the time for such payments.

State law also addresses the timing of payments to be made to subcontractors and material suppliers. Section 7 of the Prompt Payment Act, 30 ILCS 540/7, requires that when a Contractor receives any payment from the Department, the Contractor shall make corresponding, proportional payments to each subcontractor and material supplier performing work or supplying material within 15 calendar days after receipt of the Department payment. Section 7 of the Act further provides that interest in the amount of two percent per month, in addition to the payment due, shall be paid to any subcontractor or material supplier by the Contractor if the payment required by the Act is withheld or delayed without reasonable cause. The Act also provides that the time for payment required and the calculation of any interest due applies to transactions between subcontractors and lower-tier subcontractors and material suppliers throughout the contracting chain.

This Special Provision establishes the required federal contract clause, and adopts the 15 calendar day requirement of the State Prompt Payment Act for purposes of compliance with the federal regulation regarding payments to subcontractors. This contract is subject to the following payment obligations.

When progress payments are made to the Contractor according to Article 109.07 of the Standard Specifications, the Contractor shall make a corresponding payment to each subcontractor and material supplier in proportion to the work satisfactorily completed by each subcontractor and for the material supplied to perform any work of the contract. The proportionate amount of partial payment due to each subcontractor and material supplier throughout the contracting chain shall be determined by the quantities measured or otherwise determined as eligible for payment by the Department and included in the progress payment to the Contractor. Subcontractors and material suppliers shall be paid by the Contractor within 15 calendar days after the receipt of payment from the Department. The Contractor shall not hold retainage from the subcontractors. These obligations shall also apply to any payments made by subcontractors and material suppliers to their subcontractors and material suppliers; and to all

payments made to lower tier subcontractors and material suppliers throughout the contracting chain. Any payment or portion of a payment subject to this provision may only be withheld from the subcontractor or material supplier to whom it is due for reasonable cause.

This Special Provision does not create any rights in favor of any subcontractor or material supplier against the State or authorize any cause of action against the State on account of any payment, nonpayment, delayed payment, or interest claimed by application of the State Prompt Payment Act. The Department will not approve any delay or postponement of the 15 day requirement except for reasonable cause shown after notice and hearing pursuant to Section 7(b) of the State Prompt Payment Act. State law creates other and additional remedies available to any subcontractor or material supplier, regardless of tier, who has not been paid for work properly performed or material furnished. These remedies are a lien against public funds set forth in Section 23(c) of the Mechanics Lien Act, 770 ILCS 60/23(c), and a recovery on the Contractor's payment bond according to the Public Construction Bond Act, 30 ILCS 550.

### **PAYROLLS AND PAYROLL RECORDS (BDE)**

Effective: March 1, 2009

FEDERAL AID CONTRACTS. Revise the following section of Check Sheet #1 of the Recurring Special Provisions to read:

#### **"STATEMENTS AND PAYROLLS**

The payroll records shall include each worker's name, address, telephone number, social security number, classification, rate of pay, number of hours worked each day, starting and ending times of work each day, total hours worked each week, itemized deductions made, and actual wages paid.

The Contractor and each subcontractor shall submit payroll records to the Engineer each week from the start to the completion of their respective work, except that full social security numbers and home addresses shall not be included on weekly transmittals. Instead the payrolls shall include an identification number for each employee (e.g., the last four digits of the employee's social security number.). The submittals shall be on the Department's form SBE 48, or an approved facsimile. When there has been no activity during a work week, a payroll record shall still be submitted with the appropriate box ("No Work", "Suspended", or "Complete") checked on the form."

STATE CONTRACTS. Revise Section IV of Check Sheet #5 of the Recurring Special Provisions to read:

#### **"IV.COMPLIANCE WITH THE PREVAILING WAGE ACT**

1. Prevailing Wages. All wages paid by the Contractor and each subcontractor shall be in compliance with The Prevailing Wage Act (820 ILCS 130), as amended, except where a prevailing wage violates a federal law, order, or ruling, the rate conforming to the federal law, order, or ruling shall govern. The Contractor shall be responsible to notify each

subcontractor of the wage rates set forth in this contract and any revisions thereto. If the Department of Labor revises the wage rates, the Contractor will not be allowed additional compensation on account of said revisions.

2. Payroll Records. The Contractor and each subcontractor shall make and keep, for a period of three years from the date of completion of this contract, records of the wages paid to his/her workers. The payroll records shall include each worker's name, address, telephone number, social security number, classification, rate of pay, number of hours worked each day, starting and ending times of work each day, total hours worked each week, itemized deductions made, and actual wages paid. Upon two business days' notice, these records shall be available, at all reasonable hours at a location within the State, for inspection by the Department or the Department of Labor.
3. Submission of Payroll Records. The Contractor and each subcontractor shall submit payroll records to the Engineer each week from the start to the completion of their respective work, except that full social security numbers and home addresses shall not be included on weekly transmittals. Instead the payrolls shall include an identification number for each employee (e.g., the last four digits of the employee's social security number). The submittals shall be on the Department's form SBE 48, or an approved facsimile. When there has been no activity during a work week, a payroll record shall still be submitted with the appropriate box ("No Work", "Suspended", or "Complete") checked on the form.

Each submittal shall be accompanied by a statement signed by the Contractor or subcontractor which avers that: (i) such records are true and accurate; (ii) the hourly rate paid to each worker is not less than the general prevailing rate of hourly wages required by the Act; and (iii) the Contractor or subcontractor is aware that filing a payroll record that he/she knows to be false is a Class B misdemeanor.

4. Employee Interviews. The Contractor and each subcontractor shall permit his/her employees to be interviewed on the job, during working hours, by compliance investigators of the Department or the Department of Labor."

#### **PERSONAL PROTECTIVE EQUIPMENT (BDE)**

Effective: November 1, 2008

Revise the first sentence of Article 701.12 of the Standard Specifications to read:

"All personnel on foot, excluding flaggers, within the highway right-of-way shall wear a fluorescent orange, fluorescent yellow/green, or a combination of fluorescent orange and fluorescent yellow/green vest meeting the requirements of ANSI/ISEA 107-2004 for Conspicuity Class 2 garments."

#### **PLASTIC BLOCKOUTS FOR GUARDRAIL (BDE)**

Effective: November 1, 2004

Revised: January 1, 2007

Add the following to Article 630.02 of the Standard Specifications:

“(g) Plastic Blockouts (Note 1.)

Note 1. Plastic blockouts may be used in lieu of wood blockouts for steel plate beam guardrail. The plastic blockouts shall be the minimum dimensions shown on the plans and shall be on the Department’s approved list.”

**RAILROAD PROTECTIVE LIABILITY INSURANCE (5 AND 10) (BDE)**

Effective: January 1, 2006

Description. Railroad Protective Liability and Property Damage Liability Insurance shall be carried according to Article 107.11 of the Standard Specifications, except the limits shall be a minimum of \$5,000,000 combined single limit per occurrence for bodily injury liability and property damage liability with an aggregate limit of \$10,000,000 over the life of the policy. A separate policy is required for each railroad unless otherwise noted.

| NAMED INSURED & ADDRESS  | NUMBER & SPEED OF PASSENGER TRAINS               | NUMBER & SPEED OF FREIGHT TRAINS |
|--|--|----------------------------------|
| Union Pacific Railroad Company<br>1416 Dodge Street<br>Omaha, NE 68179                                       | N/A  | 1 at up to 20 MPH                |
| DOT/AAR No.: 167709G<br>RR Division: St. Louis   | RR Mile Post: 0.9<br>RR Sub-Division: JSW IND LD |                                  |
| For Freight/Passenger Information Contact: Dave McKernan<br>For Insurance Information Contact: Jim Busenbark | Phone: (314) 331-0682<br>Phone: (402) 280-6188   |                                  |

Approval of Insurance. The original and one certified copy of each required policy shall be submitted to the following address for approval:

Illinois Department of Transportation  
 Bureau of Design and Environment  
 2300 South Dirksen Parkway, Room 326  
 Springfield, Illinois 62764

The Contractor will be advised when the Department has received approval of the insurance from the railroad(s). Before any work begins on railroad right-of-way, the Contractor shall submit to the Engineer evidence that the required insurance has been approved by the railroad(s). The Contractor shall also provide the Engineer with the expiration date of each required policy.

Basis of Payment. Providing Railroad Protective Liability and Property Damage Liability Insurance will be paid for at the contract unit price per Lump Sum for RAILROAD PROTECTIVE LIABILITY INSURANCE.

## **RECLAIMED ASPHALT PAVEMENT (RAP) (BDE)**

Effective: January 1, 2007

Revised: April 1, 2009

In Article 1030.02(g), delete the last sentence of the first paragraph in (Note 2).

Revise Section 1031 of the Standard Specifications to read:

### **“SECTION 1031. RECLAIMED ASPHALT PAVEMENT**

**1031.01 Description.** Reclaimed asphalt pavement (RAP) is reclaimed asphalt pavement resulting from cold milling or crushing of an existing dense graded hot-mix asphalt (HMA) pavement. The Contractor shall supply written documentation that the RAP originated from routes or airfields under federal, state, or local agency jurisdiction.

**1031.02 Stockpiles.** The Contractor shall construct individual, sealed RAP stockpiles meeting one of the following definitions. No additional RAP shall be added to the pile after the pile has been sealed. Stockpiles shall be sufficiently separated to prevent intermingling at the base. Stockpiles shall be identified by signs indicating the type as listed below (i.e. “Homogeneous Surface”).

Prior to milling, the Contractor shall request the District to provide verification of the quality of the RAP to clarify appropriate stockpile.

- (a) Homogeneous. Homogeneous RAP stockpiles shall consist of RAP from Class I, Superpave (High ESAL), HMA (High ESAL), or equivalent mixtures and represent:  
1) the same aggregate quality, but shall be at least C quality; 2) the same type of crushed aggregate (either crushed natural aggregate, ACBF slag, or steel slag); 3) similar gradation; and 4) similar asphalt binder content. If approved by the Engineer, combined single pass surface/binder millings may be considered “homogenous” with a quality rating dictated by the lowest coarse aggregate quality present in the mixture.
- (b) Conglomerate. Conglomerate RAP stockpiles shall consist of RAP from Class I, Superpave (High ESAL), HMA (High ESAL), or equivalent mixtures. The coarse aggregate in this RAP shall be crushed aggregate and may represent more than one aggregate type and/or quality but shall be at least C quality. This RAP may have an inconsistent gradation and/or asphalt binder content prior to processing. All conglomerate RAP shall be processed prior to testing by crushing to where all RAP shall pass the 5/8 in. (16 mm) or smaller screen. Conglomerate RAP stockpiles shall not contain steel slag or other expansive material as determined by the Department.
- (c) Conglomerate “D” Quality (DQ). Conglomerate DQ RAP stockpiles shall consist of RAP from Class I, Superpave (High or Low ESAL), HMA (High or Low ESAL), or equivalent mixtures. The coarse aggregate in this RAP may be crushed or round but shall be at least D quality. This RAP may have an inconsistent gradation and/or asphalt binder content. Conglomerate DQ RAP stockpiles shall not contain steel slag or other expansive material as determined by the Department.
- (d) Non-Quality. RAP stockpiles that do not meet the requirements of the stockpile categories listed above shall be classified as “Non-Quality”.

RAP containing contaminants, such as earth, brick, sand, concrete, sheet asphalt, bituminous surface treatment (i.e. chip seal), pavement fabric, joint sealants, etc., will be unacceptable unless the contaminants are removed to the satisfaction of the Engineer. Sheet asphalt shall be stockpiled separately.

**1031.03 Testing.** When used in HMA, the RAP shall be sampled and tested either during or after stockpiling.

For testing during stockpiling, washed extraction samples shall be run at the minimum frequency of one sample per 500 tons (450 metric tons) for the first 2000 tons (1800 metric tons) and one sample per 2000 tons (1800 metric tons) thereafter. A minimum of five tests shall be required for stockpiles less than 4000 tons (3600 metric tons).

For testing after stockpiling, the Contractor shall submit a plan for approval to the District proposing a satisfactory method of sampling and testing the RAP pile either in-situ or by restocking. The sampling plan shall meet the minimum frequency required above and detail the procedure used to obtain representative samples throughout the pile for testing.

Before extraction, each field sample shall be split to obtain two samples of test sample size. One of the two test samples from the final split shall be labeled and stored for Department use. The Contractor shall extract the other test sample according to Department procedure. The Engineer reserves the right to test any sample (split or Department-taken) to verify Contractor test results.

Evaluation of Test Results. All of the extraction results shall be compiled and averaged for asphalt binder content and gradation and, when applicable  $G_{mm}$ . Individual extraction test results, when compared to the averages, will be accepted if within the tolerances listed below.

| Parameter         | Homogeneous / Conglomerate | Conglomerate "D" Quality |
|-------------------|----------------------------|--------------------------|
| 1 in. (25 mm)     |                            | ± 5 %                    |
| 1/2 in. (12.5 mm) | ± 8 %                      | ± 15 %                   |
| No. 4 (4.75 mm)   | ± 6 %                      | ± 13 %                   |
| No. 8 (2.36 mm)   | ± 5 %                      |                          |
| No. 16 (1.18 mm)  |                            | ± 15 %                   |
| No. 30 (600 μm)   | ± 5 %                      |                          |
| No. 200 (75 μm)   | ± 2.0 %                    | ± 4.0 %                  |
| Asphalt Binder    | ± 0.4 % <sup>1/</sup>      | ± 0.5 %                  |

1/ The tolerance for fractionated reclaimed asphalt pavement (FRAP) shall be ± 0.3 %.

If more than 20 percent of the individual sieves are out of the gradation tolerances, or if more than 20 percent of the asphalt binder content test results fall outside the appropriate tolerances, the RAP shall not be used in HMA unless the RAP representing the failing tests is removed from the stockpile. All test data and acceptance ranges shall be sent to the District for evaluation.

With the approval of the Engineer, the ignition oven may be substituted for extractions according to the Illinois Test Procedure, "Calibration of the Ignition Oven for the Purpose of Characterizing Reclaimed Asphalt Pavement (RAP)".

**1031.04 Quality Designation of Aggregate in RAP.** The quality of the RAP shall be set by the lowest quality of coarse aggregate in the RAP stockpile and are designated as follows.

- (a) RAP from Class I, Superpave (High ESAL), or HMA (High ESAL) surface mixtures are designated as containing Class B quality coarse aggregate.
- (b) RAP from Superpave (Low ESAL)/HMA (Low ESAL) IL-19.0L binder and IL-9.5L surface mixtures are designated as Class D quality coarse aggregate.
- (c) RAP from Class I, Superpave (High ESAL), or HMA (High ESAL) binder mixtures, bituminous base course mixtures, and bituminous base course widening mixtures are designated as containing Class C quality coarse aggregate.
- (d) RAP from bituminous stabilized subbase and BAM shoulders are designated as containing Class D quality coarse aggregate.

**1031.05 Use of RAP in HMA.** The use of RAP shall be a Contractor's option when constructing HMA in all contracts. The use of RAP in HMA shall be as follows.

- (a) Coarse Aggregate Size. The coarse aggregate in all RAP shall be equal to or less than the nominal maximum size requirement for the HMA mixture to be produced.
- (b) Steel Slag Stockpiles. RAP stockpiles containing steel slag or other expansive material, as determined by the Department, shall be homogeneous and will be approved for use in HMA (High ESAL and Low ESAL) surface mixtures only.
- (c) Use in HMA Surface Mixtures (High and Low ESAL). RAP stockpiles for use in HMA surface mixtures (High and Low ESAL) shall be homogeneous in which the coarse aggregate is Class B quality or better.
- (d) Use in HMA Binder Mixtures (High and Low ESAL), HMA Base Course, and HMA Base Course Widening. RAP stockpiles for use in HMA binder mixtures (High and Low ESAL), HMA base course, and HMA base course widening shall be homogeneous, or conglomerate, in which the coarse aggregate is Class C quality or better.
- (e) Use in Shoulders and Subbase. RAP stockpiles for use in HMA shoulders and stabilized subbase (HMA) shall be homogeneous, conglomerate, or conglomerate DQ.
- (f) When the Contractor chooses the RAP option, the percentage of RAP shall not exceed the amounts indicated in the table below for a given N Design.

Max RAP Percentage

| HMA Mixtures <sup>1/, 3/</sup><br>Ndesign | Maximum % RAP             |                       |                     |
|---|---------------------------|-----------------------|---------------------|
|   | Binder/Leveling<br>Binder | Surface               | Polymer<br>Modified |
| 30  | 30                        | 30                    | 10                  |
| 50  | 25                        | 15                    | 10                  |
| 70  | 15 / 25 <sup>2/</sup>     | 10 / 15 <sup>2/</sup> | 10                  |
| 90  | 10                        | 10                    | 10                  |
| 105                                       | 10                        | 10                    | 10                  |

- 1/ For HMA shoulder and stabilized subbase (HMA) N-30, the amount of RAP shall not exceed 50% of the mixture.
- 2/ Value of Max % RAP if homogeneous RAP stockpile of IL-9.5 RAP is utilized.
- 3/ When RAP exceeds 20 percent, the high and low virgin asphalt binder grades shall each be reduced by one grade (i.e. 25 percent RAP would require a virgin asphalt binder grade of PG64-22 to be reduced to a PG58-28). If warm mix asphalt (WMA) technology is utilized, and production temperatures do not exceed 275°F (135 °C) the grades shall be reduced as follows:

Overlays:

When WMA contains between 20 and 30 percent RAP the high temperature shall be reduced by one grade (i.e. 25 percent RAP would require a virgin asphalt binder grade of PG64-22 to be reduced to a PG58-22). When WMA contains 30 percent or more RAP the high and low temperature grades shall each be reduced by one grade (i.e. 35 percent RAP would require a virgin asphalt binder grade of PG64-22 to be reduced to a PG58-28).

Full Depth:

When WMA contains between 20 and 30 percent RAP, the low temperature shall be reduced by one grade (i.e. 25 percent RAP would require a virgin asphalt binder grade of PG64-22 to be reduced to a PG64-28). When the WMA contains 30 percent or more RAP the high and low temperature grades shall each be reduced by one grade (i.e. 35 percent RAP would require a virgin asphalt binder grade of PG64-22 to be reduced to a PG58-28).

- (g) When the Contractor chooses the FRAP option, the percentage of FRAP shall not exceed the amounts indicated in the table below for a given N Design.

Max FRAP Percentage<sup>1/</sup>

| HMA Mixtures <sup>2/, 3/</sup><br>Ndesign | Maximum % FRAP            |         |                     |
|---|---------------------------|---------|---------------------|
|   | Binder/Leveling<br>Binder | Surface | Polymer<br>Modified |
| 30  | 35                        | 35      | 10                  |
| 50  | 30                        | 25      | 10                  |
| 70  | 25                        | 20      | 10                  |
| 90  | 20                        | 15      | 10                  |
| 105                                       | 10                        | 10      | 10                  |

- 1/ Minimum of two fractions for surface and binder applications.
- 2/ For HMA shoulder and stabilized subbase (HMA) N30, the amount of RAP shall not exceed 50 percent of the mixture.
- 3/ When FRAP exceeds 20 percent, the high and low virgin asphalt binder grades shall each be reduced by one grade (i.e. 25 percent RAP would require a virgin asphalt binder grade of PG64-22 to be reduced to a PG58-28). If warm mix asphalt (WMA) technology is utilized, and production temperatures do not exceed 275°F (135 °C) the grades shall be reduced as follows:

Overlays:

When WMA contains between 20 and 30 percent FRAP the high temperature shall be reduced by one grade (i.e. 25 percent FRAP would require a virgin asphalt binder grade of PG64-22 to be reduced to a PG58-22). When WMA contains 30 percent or more FRAP the high and low temperature grades shall each be reduced by one grade (i.e. 35 percent FRAP would require a virgin asphalt binder grade of PG64-22 to be reduced to a PG58-28).

Full Depth:

When WMA contains between 20 and 30 percent FRAP, the low temperature shall be reduced by one grade (i.e. 25 percent FRAP would require a virgin asphalt binder grade of PG64-22 to be reduced to a PG64-28). When the WMA contains 30 percent or more FRAP the high and low temperature grades shall each be reduced by one grade (i.e. 35 percent FRAP would require a virgin asphalt binder grade of PG64-22 to be reduced to a PG58-28).

**1031.06 HMA Mix Designs.** At the Contractor’s option, HMA mixtures may be constructed utilizing RAP material meeting the above detailed requirements.

RAP designs shall be submitted for volumetric verification. If additional RAP stockpiles are tested and found that no more than 20 percent of the results, as defined under “Testing” herein, are outside of the control tolerances set for the original RAP stockpile and HMA mix design, and meets all of the requirements herein, the additional RAP stockpiles may be used in the original mix design at the percent previously verified.

**1031.07 HMA Production.** The coarse aggregate in all RAP used shall be equal to or less than the nominal maximum size requirement for the HMA mixture being produced.

To remove or reduce agglomerated material, a scalping screen, crushing unit, or comparable sizing device approved by the Engineer shall be used in the RAP feed system to remove or reduce oversized material. If material passing the sizing device adversely affects the mix production or quality of the mix, the sizing device shall be set at a size specified by the Engineer.

If the RAP control tolerances or QC/QA test results require corrective action, the Contractor shall cease production of the mixture containing RAP and either switch to the virgin aggregate design or submit a new RAP design.

HMA plants utilizing RAP shall be capable of automatically recording and printing the following information.

(a) Dryer Drum Plants.

- (1) Date, month, year, and time to the nearest minute for each print.
- (2) HMA mix number assigned by the Department.
- (3) Accumulated weight of dry aggregate (combined or individual) in tons (metric tons) to the nearest 0.1 ton (0.1 metric ton).
- (4) Accumulated dry weight of RAP in tons (metric tons) to the nearest 0.1 ton (0.1 metric ton).
- (5) Accumulated mineral filler in revolutions, tons (metric tons), etc. to the nearest 0.1 unit.
- (6) Accumulated asphalt binder in gallons (liters), tons (metric tons), etc. to the nearest 0.1 unit.
- (7) Residual asphalt binder in the RAP material as a percent of the total mix to the nearest 0.1 percent.
- (8) Aggregate and RAP moisture compensators in percent as set on the control panel. (Required when accumulated or individual aggregate and RAP are printed in wet condition.)

(b) Batch Plants.

- (1) Date, month, year, and time to the nearest minute for each print.
- (2) HMA mix number assigned by the Department.
- (3) Individual virgin aggregate hot bin batch weights to the nearest pound (kilogram).

- (4) Mineral filler weight to the nearest pound (kilogram).
- (5) RAP weight to the nearest pound (kilogram).
- (6) Virgin asphalt binder weight to the nearest pound (kilogram).
- (7) Residual asphalt binder in the RAP material as a percent of the total mix to the nearest 0.1 percent.

The printouts shall be maintained in a file at the plant for a minimum of one year or as directed by the Engineer and shall be made available upon request. The printing system will be inspected by the Engineer prior to production and verified at the beginning of each construction season thereafter.

**1031.08 RAP in Aggregate Surface Course and Aggregate Shoulders.** The use of RAP in aggregate surface course and aggregate shoulders shall be as follows.

- (a) Stockpiles and Testing. RAP stockpiles may be any of those listed in Article 1031.02, except "Other". The testing requirements of Article 1031.03 shall not apply.
- (b) Gradation. One hundred percent of the RAP material shall pass the 1 1/2 in. (37.5 mm) sieve. The RAP material shall be reasonably well graded from coarse to fine. RAP material that is gap-graded or single sized will not be accepted."

**REFLECTIVE SHEETING ON CHANNELIZING DEVICES (BDE)**

Effective: April 1, 2007

Revised: November 1, 2008

Revise the seventh paragraph of Article 1106.02 of the Standard Specifications to read:

"At the time of manufacturing, the retroreflective prismatic sheeting used on channelizing devices shall meet or exceed the initial minimum coefficient of retroreflection as specified in the following table. Measurements shall be conducted according to ASTM E 810, without averaging. Sheeting used on cones, drums and flexible delineators shall be reboundable as tested according to ASTM D 4956. Prestriped sheeting for rigid substrates on barricades shall be white and orange. *The sheeting shall be uniform in color and devoid of streaks throughout the length of each roll. The color shall conform to the latest appropriate standard color tolerance chart issued by the U.S. Department of Transportation, Federal Highway Administration, and to the daytime and nighttime color requirements of ASTM D 4956.*

| Initial Minimum Coefficient of Retroreflection<br>candelas/foot candle/sq ft (candelas/lux/sq m) of material |                       |       |        |                    |
|--|-----------------------|-------|--------|--------------------|
| Observation Angle (deg.)   | Entrance Angle (deg.) | White | Orange | Fluorescent Orange |
| 0.2  | -4                    | 365   | 160    | 150                |
| 0.2  | +30                   | 175   | 80     | 70                 |
| 0.5  | -4                    | 245   | 100    | 95                 |
| 0.5  | +30                   | 100   | 50     | 40"                |

Revise the first sentence of the first paragraph of Article 1106.02(c) of the Standard Specifications to read:

“Barricades and vertical panels shall have alternating white and orange stripes sloping downward at 45 degrees toward the side on which traffic will pass.”

Revise the third sentence of the first paragraph of Article 1106.02(d) of the Standard Specifications to read:

“The bottom panels shall be 8 x 24 in. (200 x 600 mm) with alternating white and orange stripes sloping downward at 45 degrees toward the side on which traffic will pass.”

### **STEEL PLATE BEAM GUARDRAIL (BDE)**

Effective: November 1, 2005

Revised: August 1, 2007

Revise the first paragraph of Article 1006.25 of the Standard Specifications to read:

**"1006.25 Steel Plate Beam Guardrail.** Steel plate beam guardrail, including bolts, nuts, and washers, shall be according to AASHTO M 180. The guardrail shall be Class A, with a Type II galvanized coating; except the weight (mass) of the coating for each side of the guardrail shall be at least 2.00 oz/sq ft (610 g/sq m). The coating will be determined for each side of the guardrail using the average of at least three non-destructive test readings taken on that side of the guardrail. The minimum average thickness for each side shall be 3.4 mils (86 µm).”

### **SUBCONTRACTOR MOBILIZATION PAYMENTS (BDE)**

Effective: April 2, 2005

To account for the preparatory work and operations necessary for the movement of subcontractor personnel, equipment, supplies, and incidentals to the project site and for all other work or operations that must be performed or costs incurred when beginning work approved for subcontracting in accordance with Article 108.01 of the Standard Specifications, the Contractor shall make a mobilization payment to each subcontractor.

This mobilization payment shall be made at least 14 days prior to the subcontractor starting work. The amount paid shall be equal to 3 percent of the amount of the subcontract reported on form BC 260A submitted for the approval of the subcontractor’s work.

This provision shall be incorporated directly or by reference into each subcontract approved by the Department.

### **WORKING DAYS (BDE)**

Effective: January 1, 2002

The Contractor shall complete the work within **80** working days.

## BITUMINOUS MATERIALS COST ADJUSTMENTS (BDE) (RETURN FORM WITH BID)

Effective: November 2, 2006

Revised: April 1, 2009

Description. Bituminous material cost adjustments will be made to provide additional compensation to the Contractor, or credit to the Department, for fluctuations in the cost of bituminous materials when optioned by the Contractor. The adjustments shall apply to permanent and temporary hot-mix asphalt (HMA) mixtures, bituminous surface treatments (cover and seal coats), and pavement preservation type surface treatments. The adjustments shall not apply to bituminous prime coats, tack coats, crack filling/sealing, or joint filling/sealing.

The bidder shall indicate on the attached form whether or not this special provision will be part of the contract and submit the completed form with his/her bid. Failure to submit the form, or failure to fill out the form completely, shall make this contract exempt of bituminous materials cost adjustments.

Method of Adjustment. Bituminous materials cost adjustments will be computed as follows.

$$CA = (BPI_P - BPI_L) \times (\%AC_V / 100) \times Q$$

- Where: CA = Cost Adjustment, \$.  
BPI<sub>P</sub> = Bituminous Price Index, as published by the Department for the month the work is performed, \$/ton (\$/metric ton).  
BPI<sub>L</sub> = Bituminous Price Index, as published by the Department for the month prior to the letting, \$/ton (\$/metric ton).  
%AC<sub>V</sub> = Percent of virgin Asphalt Cement in the Quantity being adjusted. For HMA mixtures, the % AC<sub>V</sub> will be determined from the adjusted job mix formula. For bituminous materials applied, a performance graded or cutback asphalt will be considered to be 100% AC<sub>V</sub> and undiluted emulsified asphalt will be considered to be 65% AC<sub>V</sub>.  
Q = Authorized construction Quantity, tons (metric tons) (see below).

For HMA mixtures measured in square yards:  $Q, \text{ tons} = A \times D \times (G_{mb} \times 46.8) / 2000$ . For HMA mixtures measured in square meters:  $Q, \text{ metric tons} = A \times D \times (G_{mb} \times 24.99) / 1000$ . When computing adjustments for full-depth HMA pavement, separate calculations will be made for the binder and surface courses to account for their different  $G_{mb}$  and % AC<sub>V</sub>.

For bituminous materials measured in gallons:  $Q, \text{ tons} = V \times 8.33 \text{ lb/gal} \times SG / 2000$

For bituminous materials measured in liters:  $Q, \text{ metric tons} = V \times 1.0 \text{ kg/L} \times SG / 1000$

- Where: A = Area of the HMA mixture, sq yd (sq m).  
D = Depth of the HMA mixture, in. (mm).  
G<sub>mb</sub> = Average bulk specific gravity of the mixture, from the approved mix design.  
V = Volume of the bituminous material, gal (L).  
SG = Specific Gravity of bituminous material as shown on the bill of lading.

Basis of Payment. Bituminous materials cost adjustments may be positive or negative but will only be made when there is a difference between the BPI<sub>L</sub> and BPI<sub>P</sub> in excess of five percent, as calculated by:

$$\text{Percent Difference} = \{(BPI_L - BPI_P) \div BPI_L\} \times 100$$

Bituminous materials cost adjustments will be calculated for each calendar month in which applicable bituminous material is placed; and will be paid or deducted when all other contract requirements for the work placed during the month are satisfied. The adjustments shall not apply during contract time subject to liquidated damages for completion of the entire contract.

## RETURN WITH BID

### ILLINOIS DEPARTMENT OF TRANSPORTATION

### OPTION FOR BITUMINOUS MATERIALS COST ADJUSTMENTS

The bidder shall submit this completed form with his/her bid. Failure to submit the form, or failure to fill out the form completely, shall make this contract exempt of bituminous materials cost adjustments. After award, this form, when submitted, shall become part of the contract.

**Contract No.:** \_\_\_\_\_

**Company Name:** \_\_\_\_\_

**Contractor's Option:**

Is your company opting to include this special provision as part of the contract?

Yes  No

**Signature:** \_\_\_\_\_ **Date:** \_\_\_\_\_

## **FUEL COST ADJUSTMENT (BDE) (RETURN FORM WITH BID)**

Effective: April 1, 2009

Description. Fuel cost adjustments will be made to provide additional compensation to the Contractor, or a credit to the Department, for fluctuations in fuel prices when optioned by the Contractor. The bidder shall indicate on the attached form whether or not this special provision will be part of the contract and submit the completed form with his/her bid. Failure to submit the form or failure to indicate contract number, company name and sign and date the form shall make this contract exempt of fuel cost adjustments for all categories of work. Failure to indicate "Yes" for any category of work will make that category of work exempt from fuel cost adjustment.

General. The fuel cost adjustment shall apply to contract pay items as grouped by category. The adjustment shall only apply to those categories of work checked "Yes", and only when the cumulative plan quantities for a category exceed the required threshold. Adjustments to work items in a category, either up or down, and work added by adjusted unit price will be subject to fuel cost adjustment only when the category representing the added work was subject to the fuel cost adjustment. Added work paid for by time and materials will not be subject to fuel cost adjustment. Category descriptions and thresholds for application and the fuel usage factors which are applicable to each are as follows:

(a) Categories of Work.

- (1) Category A: Earthwork. Contract pay items performed under Sections 202, 204, and 206 including any modified standard or nonstandard items where the character of the work to be performed is considered earthwork. The cumulative total of all applicable item plan quantities shall exceed 25,000 cu yd (20,000 cu m). Included in the fuel usage factor is a weighted average 0.10 gal/cu yd (0.50 liters/cu m) factor for trucking.
- (2) Category B: Subbases and Aggregate Base Courses. Contract pay items constructed under Sections 311, 312 and 351 including any modified standard or nonstandard items where the character of the work to be performed is considered construction of a subbase or aggregate, stabilized or modified base course. The cumulative total of all applicable item plan quantities shall exceed 5000 tons (4500 metric tons). Included in the fuel usage factor is a 0.60 gal/ton (2.50 liters/metric ton) factor for trucking.
- (3) Category C: Hot-Mix Asphalt (HMA) Bases, Pavements and Shoulders. Contract pay items constructed under Sections 355, 406, 407 and 482 including any modified standard or nonstandard items where the character of the work to be performed is considered HMA bases, pavements and shoulders. The cumulative total of all applicable item plan quantities shall exceed 5000 tons (4500 metric tons). Included in the fuel usage factor is 0.60 gal/ton (2.50 liters/metric ton) factor for trucking.
- (4) Category D: Portland Cement Concrete (PCC) Bases, Pavements and Shoulders. Contract pay items constructed under Sections 353, 420, 421 and 483 including any modified standard or nonstandard items where the character of the work to be

performed is considered PCC base, pavement or shoulder. The cumulative total of all applicable item plan quantities shall exceed 7500 sq yd (6000 sq m). Included in the fuel usage factor is 1.20 gal/cu yd (5.94 liters/cu m) factor for trucking.

- (5) Category E: Structures. Structure items having a cumulative bid price that exceeds \$250,000 for pay items constructed under Sections 502, 503, 504, 505, 512, 516 and 540 including any modified standard or nonstandard items where the character of the work to be performed is considered structure work when similar to that performed under these sections and not included in categories A through D.

(b) Fuel Usage Factors.

| English Units                          |        |              |
|--|--------|--------------|
| Category                               | Factor | Units        |
| A - Earthwork                          | 0.34   | gal / cu yd  |
| B – Subbase and Aggregate Base courses | 0.62   | gal / ton    |
| C – HMA Bases, Pavements and Shoulders | 1.05   | gal / ton    |
| D – PCC Bases, Pavements and Shoulders | 2.53   | gal / cu yd  |
| E – Structures                         | 8.00   | gal / \$1000 |

| Metric Units                           |        |                     |
|--|--------|---------------------|
| Category                               | Factor | Units               |
| A - Earthwork                          | 1.68   | liters / cu m       |
| B – Subbase and Aggregate Base courses | 2.58   | liters / metric ton |
| C – HMA Bases, Pavements and Shoulders | 4.37   | liters / metric ton |
| D – PCC Bases, Pavements and Shoulders | 12.52  | liters / cu m       |
| E – Structures                         | 30.28  | liters / \$1000     |

(c) Quantity Conversion Factors.

| Category | Conversion         | Factor                               |
|----------|--------------------|--------------------------------------|
| B        | sq yd to ton       | 0.057 ton / sq yd / in depth         |
|          | sq m to metric ton | 0.00243 metric ton / sq m / mm depth |
| C        | sq yd to ton       | 0.056 ton / sq yd / in depth         |
|          | sq m to metric ton | 0.00239 m ton / sq m / mm depth      |
| D        | sq yd to cu yd     | 0.028 cu yd / sq yd / in depth       |
|          | sq m to cu m       | 0.001 cu m / sq m / mm depth         |

Method of Adjustment. Fuel cost adjustments will be computed as follows.

$$CA = (FPI_P - FPI_L) \times (FUF / 100) \times Q$$

Where: CA = Cost Adjustment, \$  
 FPI<sub>P</sub> = Fuel Price Index, as published by the Department for the month the work is performed, \$/gal (\$/liter)

- FPI<sub>L</sub> = Fuel Price Index, as published by the Department for the month prior to the letting, \$/gal (\$/liter)
- FUF = Fuel Usage Factor in the pay item(s) being adjusted
- Q = Authorized construction Quantity, tons (metric tons) or cu yd (cu m)

The entire FUF indicated in paragraph (b) will be used regardless of use of trucking to perform the work.

Progress Payments. Fuel cost adjustments will be calculated for each calendar month in which applicable work is performed; and will be paid or deducted when all other contract requirements for the items of work are satisfied. The adjustments shall not apply during contract time subject to liquidated damages for completion of the entire contract.

Final Quantities. Upon completion of the work and determination of final pay quantities, an adjustment will be prepared to reconcile any differences between estimated quantities previously paid and the final quantities. The value for the balancing adjustment will be based on a weighted average of FPI<sub>P</sub> and Q only for those months requiring the cost adjustment. The cost adjustment will be applicable to the final measured quantities of all applicable pay items.

Basis of Payment. Fuel cost adjustments may be positive or negative but will only be made when there is a difference between the FPI<sub>L</sub> and FPI<sub>P</sub> in excess of five percent, as calculated by:

$$\text{Percent Difference} = \{(FPI_L - FPI_P) \div FPI_L\} \times 100$$

## RETURN WITH BID

### ILLINOIS DEPARTMENT OF TRANSPORTATION

### OPTION FOR FUEL COST ADJUSTMENT

The bidder shall submit this completed form with his/her bid. Failure to submit the form or properly complete contract number, company name, and sign and date the form shall make this contract exempt of fuel cost adjustments in all categories. Failure to indicate "Yes" for any category of work at the time of bid will make that category of work exempt from fuel cost adjustment. After award, this form, when submitted shall become part of the contract.

**Contract No.:** \_\_\_\_\_

**Company Name:** \_\_\_\_\_

#### **Contractor's Option:**

Is your company opting to include this special provision as part of the contract plans for the following categories of work?

- |  |     |                          |
|--|-----|--------------------------|
| Category A Earthwork.                          | Yes | <input type="checkbox"/> |
| Category B Subbases and Aggregate Base Courses | Yes | <input type="checkbox"/> |
| Category C HMA Bases, Pavements and Shoulders  | Yes | <input type="checkbox"/> |
| Category D PCC Bases, Pavements and Shoulders  | Yes | <input type="checkbox"/> |
| Category E Structures                          | Yes | <input type="checkbox"/> |

**Signature:** \_\_\_\_\_ **Date:** \_\_\_\_\_

**STEEL COST ADJUSTMENT (BDE) (RETURN FORM WITH BID)**

Effective: April 2, 2004

Revised: April 1, 2009

Description. Steel cost adjustments will be made to provide additional compensation to the Contractor, or a credit to the Department, for fluctuations in steel prices when optioned by the Contractor. The bidder shall indicate on the attached form whether or not this special provision will be part of the contract and submit the completed form with his/her bid. Failure to submit the form or failure to indicate contract number, company name, and sign and date the form shall make this contract exempt of steel cost adjustments for all items of steel. Failure to indicate "Yes" for any item of work will make that item of steel exempt from steel cost adjustment.

Types of Steel Products. An adjustment will be made for fluctuations in the cost of steel used in the manufacture of the following items:

- Metal Piling (excluding temporary sheet piling)
- Structural Steel
- Reinforcing Steel

Other steel materials such as dowel bars, tie bars, mesh reinforcement, guardrail, steel traffic signal and light poles, towers and mast arms, metal railings (excluding wire fence), and frames and grates will be subject to a steel cost adjustment when the pay items they are used in has a contract value of \$10,000 or greater.

Documentation. Sufficient documentation shall be furnished to the Engineer to verify the following:

- (a) The dates and quantity of steel, in lb (kg), shipped from the mill to the fabricator.
- (b) The quantity of steel, in lb (kg), incorporated into the various items of work covered by this special provision. The Department reserves the right to verify submitted quantities.

Method of Adjustment. Steel cost adjustments will be computed as follows:

$$SCA = Q \times D$$

- Where: SCA = steel cost adjustment, in dollars  
Q = quantity of steel incorporated into the work, in lb (kg)  
D = price factor, in dollars per lb (kg)

$$D = MPI_M - MPI_L$$

Where:  $MPI_M$  = The Materials Cost Index for steel as published by the Engineering News-Record for the month the steel is shipped from the mill. The indices will be converted from dollars per 100 lb to dollars per lb (kg).

$MPI_L$  = The Materials Cost Index for steel as published by the Engineering News-Record for the month prior to the letting. The indices will be converted from dollars per 100 lb to dollars per lb (kg).

The unit weights (masses) of steel that will be used to calculate the steel cost adjustment for the various items are shown in the attached table.

No steel cost adjustment will be made for any products manufactured from steel having a mill shipping date prior to the letting date.

If the Contractor fails to provide the required documentation, the method of adjustment will be calculated as described above; however, the  $MPI_M$  will be based on the date the steel arrives at the job site. In this case, an adjustment will only be made when there is a decrease in steel costs.

Basis of Payment. Steel cost adjustments may be positive or negative but will only be made when there is a difference between the  $MPI_L$  and  $MPI_M$  in excess of five percent, as calculated by:

$$\text{Percent Difference} = \{(MPI_L - MPI_M) \div MPI_L\} \times 100$$

Steel cost adjustments will be calculated by the Engineer and will be paid or deducted when all other contract requirements for the items of work are satisfied. Adjustments will only be made for fluctuations in the cost of the steel as described herein. No adjustment will be made for changes in the cost of manufacturing, fabrication, shipping, storage, etc.

The adjustments shall not apply during contract time subject to liquidated damages for completion of the entire contract.

**Attachment**

| Item  | Unit Mass (Weight)             |
|---|--------------------------------|
| Metal Piling (excluding temporary sheet piling)                                   |                                |
| Furnishing Metal Pile Shells 12 in. (305 mm), 0.179 in. (3.80 mm) wall thickness) | 23 lb/ft (34 kg/m)             |
| Furnishing Metal Pile Shells 12 in. (305 mm), 0.250 in. (6.35 mm) wall thickness) | 32 lb/ft (48 kg/m)             |
| Furnishing Metal Pile Shells 14 in. (356 mm), 0.250 in. (6.35 mm) wall thickness) | 37 lb/ft (55 kg/m)             |
| Other piling  | See plans                      |
| Structural Steel  | See plans for weights (masses) |
| Reinforcing Steel   | See plans for weights (masses) |
| Dowel Bars and Tie Bars   | 6 lb (3 kg) each               |
| Mesh Reinforcement  | 63 lb/100 sq ft (310 kg/sq m)  |
| Guardrail   |                                |
| Steel Plate Beam Guardrail, Type A w/steel posts                                  | 20 lb/ft (30 kg/m)             |
| Steel Plate Beam Guardrail, Type B w/steel posts                                  | 30 lb/ft (45 kg/m)             |
| Steel Plate Beam Guardrail, Types A and B w/wood posts                            | 8 lb/ft (12 kg/m)              |
| Steel Plate Beam Guardrail, Type 2  | 305 lb (140 kg) each           |
| Steel Plate Beam Guardrail, Type 6  | 1260 lb (570 kg) each          |
| Traffic Barrier Terminal, Type 1 Special (Tangent)                                | 730 lb (330 kg) each           |
| Traffic Barrier Terminal, Type 1 Special (Flared)                                 | 410 lb (185 kg) each           |
| Steel Traffic Signal and Light Poles, Towers and Mast Arms                        |                                |
| Traffic Signal Post   | 11 lb/ft (16 kg/m)             |
| Light Pole, Tenon Mount and Twin Mount, 30 - 40 ft (9 - 12 m)                     | 14 lb/ft (21 kg/m)             |
| Light Pole, Tenon Mount and Twin Mount, 45 - 55 ft (13.5 - 16.5 m)                | 21 lb/ft (31 kg/m)             |
| Light Pole w/Mast Arm, 30 - 50 ft (9 - 15.2 m)                                    | 13 lb/ft (19 kg/m)             |
| Light Pole w/Mast Arm, 55 - 60 ft (16.5 - 18 m)                                   | 19 lb/ft (28 kg/m)             |
| Light Tower w/Luminaire Mount, 80 - 110 ft (24 - 33.5 m)                          | 31 lb/ft (46 kg/m)             |
| Light Tower w/Luminaire Mount, 120 - 140 ft (36.5 - 42.5 m)                       | 65 lb/ft (97 kg/m)             |
| Light Tower w/Luminaire Mount, 150 - 160 ft (45.5 - 48.5 m)                       | 80 lb/ft (119 kg/m)            |
| Metal Railings (excluding wire fence)   |                                |
| Steel Railing, Type SM  | 64 lb/ft (95 kg/m)             |
| Steel Railing, Type S-1   | 39 lb/ft (58 kg/m)             |
| Steel Railing, Type T-1   | 53 lb/ft (79 kg/m)             |
| Steel Bridge Rail   | 52 lb/ft (77 kg/m)             |
| Frames and Grates   |                                |
| Frame   | 250 lb (115 kg)                |
| Lids and Grates   | 150 lb (70 kg)                 |

## RETURN WITH BID

### ILLINOIS DEPARTMENT OF TRANSPORTATION

### OPTION FOR STEEL COST ADJUSTMENT

The bidder shall submit this completed form with his/her bid. Failure to submit the form or properly complete contract number, company name, and sign and date the form shall make this contract exempt of steel cost adjustments for all items of steel. Failure to indicate "Yes" for any item of work will make that item of steel exempt from steel cost adjustment. After award, this form, when submitted shall become part of the contract.

**Contract No.:** \_\_\_\_\_

**Company Name:** \_\_\_\_\_

**Contractor's Option:**

Is your company opting to include this special provision as part of the contract plans for the following items of work?

- |  |     |                          |
|--|-----|--------------------------|
| Metal Piling   | Yes | <input type="checkbox"/> |
| Structural Steel   | Yes | <input type="checkbox"/> |
| Reinforcing Steel  | Yes | <input type="checkbox"/> |
| Dowel Bars, Tie Bars and Mesh Reinforcement                | Yes | <input type="checkbox"/> |
| Guardrail  | Yes | <input type="checkbox"/> |
| Steel Traffic Signal and Light Poles, Towers and Mast Arms | Yes | <input type="checkbox"/> |
| Metal Railings (excluding wire fence)                      | Yes | <input type="checkbox"/> |
| Frames and Grates  | Yes | <input type="checkbox"/> |

**Signature:** \_\_\_\_\_ **Date:** \_\_\_\_\_

## **HANDRAIL CONCRETE REMOVAL**

Effective: 03/08/2005

### Description.

This work consists of the removal and disposal of the existing concrete rail and concrete posts to the limits indicated in the contract plans. It also includes the cutting off flush off the existing vertical reinforcement extending from the safety walk into the concrete post and coating it with an approved epoxy.

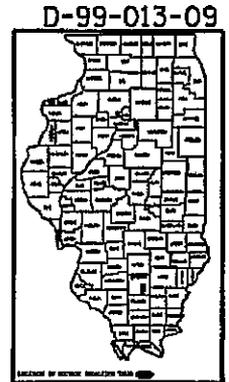
### Basis of Payment.

This work will be paid for at the contract unit price per foot for HANDRAIL CONCRETE REMOVAL; which price shall include all labor and materials to satisfactorily complete the work.

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS  
**PLANS FOR PROPOSED  
 HIGHWAY**  
 DISTRICT 9

IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 1 OF 72

IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 JEFFERSON COUNTY  
 C-99-028-09

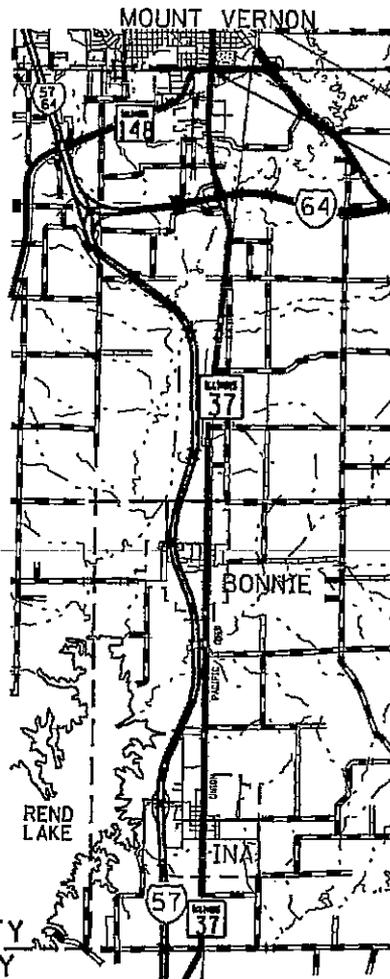


**EQUATION STATIONS:**

STA 35+00 BK = STA 120+65.50 AH  
 STA 136+43.29 BK = STA 137+39.62 AH  
 STA 362+76.86 BK = STA 362+64.47 AH

**OMISSIONS:**

STA 121+07.84 TO STA 124+92.16  
 STA 197+29.57 TO STA 198+44.43  
 STA 205+56.11 TO STA 206+91.07



**PROJECT BEGINS**  
 STA 140+15.00

YEAR 2007  
 TRAFFIC DATE:  
 ADT = 8600  
 TRUCKS = 9%

**TOWNSHIP:**  
 DODDS  
 SPRING GARDENS  
 EWING

PROJECT LEADER: DENNIS HILLEBRENNER 618-549-2171 CENTREX 782-4554  
 SQUAD LEADER: RICHARD BRASEL 618-549-2171

STA 648+33.80 **JEFFERSON COUNTY**  
**FRANKLIN COUNTY**

**PROJECT ENDS**  
 STA 652+50

MAP IS NOT TO SCALE

GROSS LENGTH OF PROJECT = 63,615.56 FT (12.05 MILES)

OMISSION LENGTH = 634.14 FT (0.12 MILE)

NET LENGTH OF PROJECT = 62,981.42 FT (11.93 MILES)

FUNCTIONAL CLASS: MAJOR COLLECTOR (NON URBAN)

FOR SIGNATURE SHEET, SEE SHEET NO. 2  
 FOR INDEX OF SHEETS, SEE SHEET NO. 3  
 FOR SUMMARY OF QUANTITIES, SEE SHEET NO. 7-8

JULIE  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123 www.julie1call.com

**CONTRACT NO. 78108**



Prepared By: Dennis W. Hillebrun  
DISTRICT STUDIES & PLANS ENGINEER

Examined By: James Provo Emer  
DISTRICT LAND ACQUISITION ENGINEER

Examined By: Conni Nelson  
DISTRICT PROGRAM DEVELOPMENT ENGINEER

Examined By: Keith Wiley  
DISTRICT OPERATIONS ENGINEER

Examined By: Jim Smother  
DISTRICT CONSTRUCTION ENGINEER

Examined By: Bruce W. Peoples  
DISTRICT MATERIALS ENGINEER

Examined By: Jim Smother  
DISTRICT PROJECT IMPLEMENTATION ENGINEER

Examined By: Danish Clayton  
ASSISTANT REGIONAL ENGINEER

Approved By: Mary C. Lammie  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 5 ENGINEER

April 9 2009  
DATE

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## STANDARDS

|           |           |           |
|-----------|-----------|-----------|
| 000001-05 | 701011-02 | 701501-05 |
| 442201-03 | 701201-03 | 701901-01 |
| 515001-03 | 701301-03 | 704001-05 |
| 630001-08 | 701306-02 | 780001-02 |
| 631011-05 | 701311-03 | 781001-03 |
| 631031-07 | 701336-05 |           |
| 631032-04 |           |           |
| 701001-02 |           |           |
| 701006-03 |           |           |

## GENERAL NOTES

THE HORIZONTAL ALIGNMENT DIAGRAM PROVIDED ON SHEETS 27-41 IS FOR REFERENCE AND INFORMATIONAL PURPOSE ONLY. THIS INFORMATION IS INTENDED TO CALCULATE PLAN QUANTITIES AND PROVIDE THE ENGINEER DATA TO RESTAMP THE PAVEMENT AS NOTED.

THE CONTRACTOR SHALL STAMP STATIONING IN THE HMA SURFACE AT 300 FT. INTERVALS ON THE ALTERNATING SIDES OF PAVEMENT AND AS DIRECTED BY THE ENGINEER. THE STATION SYMBOL STAMPS USED SHALL BE FURNISHED BY THE CONTRACTOR. THEY SHALL BE 5 1/2" TALL OF A DESIGN APPROVED BY THE ENGINEER, AND SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

THE THICKNESS OF HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OF BASE ON WHICH THE HMA MIXTURE IS PLACED.

FACTORS USED FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:

|                         |                     |
|-------------------------|---------------------|
| ALL HMA CONCRETE:       | 2.016 TONS/CU. YD.  |
| ALL AGGREGATE:          | 2.05 TONS/CU. YD.   |
| HMA MATERIALS:          |                     |
| ON PAVEMENT:            | 0.09 GALS./SQ. YD.  |
| AGGREGATE (PRIME COAT): | 0.0015 TONS/SQ. YD. |

ALL PAVEMENT MARKINGS SHALL BE LOCATED AND RECORDED BEFORE HOT-MIX ASPHALT SURFACE REMOVAL AND REPLACE ACCORDINGLY.

PRIOR TO PLACEMENT OF THE FINAL PAVEMENT MARKINGS, THE RESIDENT ENGINEER SHOULD CONTACT THE BUREAU OF OPERATIONS AND ARRANGE FOR THE INSPECTION AND APPROVAL OF THE PAVEMENT MARKING LAYOUT.

THE QUANTITY SHOWN FOR MIXTURES FOR CRACKS, JOINTS, AND FLANGEWAYS IS AN ESTIMATE ONLY. THE ACTUAL AMOUNT USED SHALL BE DETERMINED BY THE ENGINEER.

SAWCUTS REQUIRED FOR HMA JOINTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE HMA SURFACE REMOVAL 1 1/2 ".

QUANTITIES SHOWN IN THE PLANS FOR PATCHING ARE ESTIMATES. THE ACTUAL AMOUNT OF PATCHING REQUIRED SHALL BE DETERMINED BY THE ENGINEER. PATCHING SURVEY DATE: (12/16/2008)

THE QUANTITY OF SHORT-TERM PAVEMENT MARKING SHOWN IN THE PLANS IS BASED ON ONE APPLICATION EACH FOR THE HMA SURFACED REMOVED AND THE HMA SURFACE COURSE.

## GENERAL NOTES CON' T

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, HMA RESURFACING SHALL BE PLACED IN A SEQUENCE THAT WILL MINIMIZE THE TIME THAT THE CENTERLINE EDGE IS EXPOSED TO TRAFFIC. WHEN AT THE END OF THE DAY'S OPERATION, THE EXPOSED CENTERLINE EDGE IS GREATER THAN 2000 FT, THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE ADJACENT LANE ON THE FOLLOWING DAY. PRIOR TO WINTER SHUTDOWN, RESURFACING ON ADJACENT LANES IS TO BE BROUGHT UP TO THE SAME ELEVATION.

AFTER A LIFT OF HMA CONCRETE HAS BEEN PLACED ON A LANE, THAT LANE SHALL REMAIN CLOSED TO TRAFFIC UNTIL THE NEW MAT HAS COOLED TO 150° F

IN ADDITION TO THE REQUIREMENTS OF ARTICLE 107.16, THE CONTRACTOR SHALL PROTECT THE SURFACE OF ALL BRIDGE DECKS AND BRIDGE APPROACH PAVEMENTS IN A MANNER SATISFACTORY TO THE ENGINEER BEFORE ANY EQUIPMENT IS ALLOWED TO CROSS THE STRUCTURE. PROTECTION SHALL BE PROVIDED FOR ALL EQUIPMENT AS DEFINED IN ARTICLE 101.16 REGARDLESS IF TRACK MOUNTED OR WHEELED.

RECLAIMED ASPHALT PAVEMENT (RAP) WILL NOT BE ALLOWED FOR USE AS AGGREGATE IN AGGREGATE SHOULDERS, TYPE B.

ANY MIXING OR PLACEMENT OF HMA MIXTURES OCCURRING PRIOR TO THE TEST STRIP EVALUATION IS AT THE CONTRACTOR'S OWN RISK.

NO MATERIAL TRANSFER DEVICE IS ALLOWED ON THIS PROJECT.

DO NOT DISTURB ANY CONTROL POINTS IN THIS REGION.

### STRUCTURES WITHIN PROJECT LIMITS:

| STRUCTURE NO. | OPERATING RATING | INVENTORY RATING | POSTING         |
|---------------|------------------|------------------|-----------------|
| 041-0031      | 20.0             | 11.7             | NONE            |
| 041-0032      | 42.8             | 25.6             | NONE            |
| 041-7051      | 00.0             | 00.0             | NONE            |
| 041-0105      | 54.5             | 32.7             | NONE            |
| 041-0035      | 28.4             | 15.5             | NONE            |
| 041-0036      | 17.3             | 10.4             | LEGAL LOAD ONLY |
| 041-0093      | 19.1             | 11.4             | LEGAL LOAD ONLY |
| 041-0097      | 33.9             | 20.0             | NONE            |
| 041-0092      | 16.3             | 9.8              | LEGAL LOAD ONLY |
| 041-0095      | 32.2             | 20.6             | NONE            |

PAVEMENT MARKINGS ARE TO BE PLACED THROUGH ALL RESURFACING OMISSIONS.

COMMITMENTS: NONE

SHEET 2 OF 2

## MIXTURE REQUIREMENTS

|   |  |
|---|--|
| LOCATION(S):                                | HOT-MIX ASPHALT SURFACE COURSE             |
| MIXTURE USE(S):                             | HOT-MIX ASPHALT SURFACE COURSE, MIX D, N90 |
| AC/PG                                       | PG64-22                                    |
| RAP % (MAX)                                 | 10   |
| DESIGN AIR VOIDS                            | 4.0%, 90 GYRATION DESIGN                   |
| MIXTURE COMPOSITION:<br>(GRADATION MIXTURE) | IL-9.5 mm OR IL 12.5 mm                    |
| FRICTION AGGREGATE:                         | D SURFACE                                  |
|   |  |

|   |                           |
|---|---------------------------|
| LOCATION(S):                                | HOT-MIX ASPHALT SHOULDERS |
| MIXTURE USE(S):                             | HOT-MIX ASPHALT SHOULDERS |
| AC/PG                                       | PG58-22                   |
| RAP % (MAX)                                 | 50                        |
| DESIGN AIR VOIDS                            | 2.0%, 30 GYRATION DESIGN  |
| MIXTURE COMPOSITION:<br>(GRADATION MIXTURE) | HMA SHOULDERS             |
| FRICTION AGGREGATE:                         | NONE                      |
|   |                           |

|   |   |
|---|---|
| LOCATION(S):                                | PAVEMENT PATCHING, 12''-16''                |
| MIXTURE USE(S):                             | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 |
| AC/PG                                       | PG64-22                                     |
| RAP % (MAX)                                 | 10  |
| DESIGN AIR VOIDS                            | 4.0%, 90 GYRATION DESIGN                    |
| MIXTURE COMPOSITION:<br>(GRADATION MIXTURE) | IL-19.0 mm                                  |
| FRICTION AGGREGATE:                         | NONE  |
|   |   |

SUMMARY OF QUANTITIES

| CODE NUMBER | ITEM DESCRIPTION                                    | UNIT   | FUNDING:       |                   |                   |                   | 100% STATE |             |        |                   |        |
|-------------|---|--------|----------------|-------------------|-------------------|-------------------|------------|-------------|--------|-------------------|--------|
|             |   |        | TOTAL QUANTITY | LOCATION QUANTITY | LOCATION QUANTITY | LOCATION QUANTITY | JEFFERSON  | URBAN/RURAL | RURAL  | LOCATION QUANTITY |        |
| 40600100    | BITUMINOUS MATERIALS (PRIME COAT)                   | GALLON | 15,979         | 4,161             |                   |                   |            |             | 11,818 |                   |        |
| 40600300    | AGGREGATE (PRIME COAT)                              | TON    | 249            | 52                |                   |                   |            | 197         |        |                   |        |
| 40600400    | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS          | TON    | 25             | .5                |                   |                   |            | 20          |        |                   |        |
| 40600895    | CONSTRUCTING TEST STRIP                             | EACH   | 1              | 0.5               |                   |                   |            | 0.5         |        |                   |        |
| 40600990    | TEMPORARY RAMP                                      | SO YD  | 193            | 106               |                   |                   |            | 87          |        |                   |        |
| 40600985    | PORTLAND CEMENT CONCRETE SURFACE REMOVAL-BUTT JOINT | SO YD  | 314            | 314               |                   |                   |            |             |        |                   |        |
| 40603345    | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90        | TON    | 14,096         | 3,013             |                   |                   |            | 11,083      |        |                   |        |
| 44000155    | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"             | SO YD  | 176,849        | 45,558            |                   |                   |            | 131,291     |        |                   |        |
| 44000198    | HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH     | SO YD  | 2,706          | 2,706             |                   |                   |            |             |        |                   |        |
| 44200170    | PAVEMENT PATCHING, TYPE I, .75 INCH                 | SO YD  | 14             | 7                 |                   |                   |            | 7           |        |                   |        |
| 44200180    | PAVEMENT PATCHING, TYPE II, .75 INCH                | SO YD  | 2,750          | 1,375             |                   |                   |            | 1,375       |        |                   |        |
| 44200184    | PAVEMENT PATCHING, TYPE III, .75 INCH               | SO YD  | 1,435          | 718               |                   |                   |            | 717         |        |                   |        |
| 44200186    | PAVEMENT PATCHING, TYPE IV, .75 INCH                | SO YD  | 1,931          | 966               |                   |                   |            | 965         |        |                   |        |
| 44200970    | CLASS B PATCHING, TYPE IV, 10 INCH                  | SO YD  | 300            | 300               |                   |                   |            |             |        |                   |        |
| 44213200    | SAW CUTS  | FOOT   | 938            | 674               |                   |                   | 44         |             |        |                   | 220    |
| 44300200    | STRIP REFLECTIVE CRACK CONTROL TREATMENT            | FOOT   | 94,390         | 37,226            |                   |                   |            | 57,164      |        |                   |        |
| 48101200    | AGGREGATE SHOULDERS, TYPE B                         | TON    | 1,934          | 70                |                   |                   |            | 1,864       |        |                   |        |
| 48203100    | HOT-MIX ASPHALT SHOULDERS                           | TON    | 937            | 929               |                   |                   |            | 8           |        |                   |        |
| 50104700    | HANDRAIL CONCRETE REMOVAL                           | FOOT   | 258            |                   |                   |                   | 62         |             |        |                   | 196    |
| 50900200    | STEEL RAILING, TYPE 2399                            | FOOT   | 258            |                   |                   |                   | 62         |             |        |                   | 196    |
| 63000002    | STEEL PLATE BEAM GUARD RAIL, TYPE A, 6.75 POSTS     | FOOT   | 675            |                   |                   |                   |            |             |        |                   | 675    |
| 63100087    | TRAFFIC BARRIER TERMINAL, TYPE 6A                   | EACH   | 19             |                   |                   |                   | 3          |             |        |                   | 16     |
| 63100045    | TRAFFIC BARRIER TERMINAL, TYPE 2                    | EACH   | 7              |                   |                   |                   |            |             |        |                   | 7      |
| 63100167    | TRAFFIC BARRIER TERMINAL TYPE 1(SPECIAL)TANGENT     | EACH   | 15             |                   |                   |                   | 3          |             |        |                   | 12     |
| 63200305    | STEEL PLATE BEAM GUARD RAIL REMOVAL                 | FOOT   | 847.75         |                   |                   |                   | 390.25     |             |        |                   | 457.50 |
| 63300725    | STEEL PLATE BEAM GUARD RAIL (SHORT RADIUS)          | FOOT   | 22             |                   |                   |                   | 22         |             |        |                   |        |
| 67100100    | MOBILIZATION  | L SUM  | 1              | 0.5               |                   |                   |            | 0.5         |        |                   |        |
| 70100450    | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201     | L SUM  | 1              | 0.5               |                   |                   |            |             |        |                   | 0.5    |
| 70100460    | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306     | L SUM  | 1              | 0.5               |                   |                   |            |             |        |                   | 0.5    |
| 70100600    | TRAFFIC CONTROL AND PROTECTION, STANDARD 701336     | L SUM  | 1              | 0.5               |                   |                   |            |             |        |                   | 0.5    |
| 70101805    | TRAFFIC CONTROL AND PROTECTION, (SPECIAL)           | EACH   | 10             |                   |                   |                   | 2          |             |        |                   | 8      |

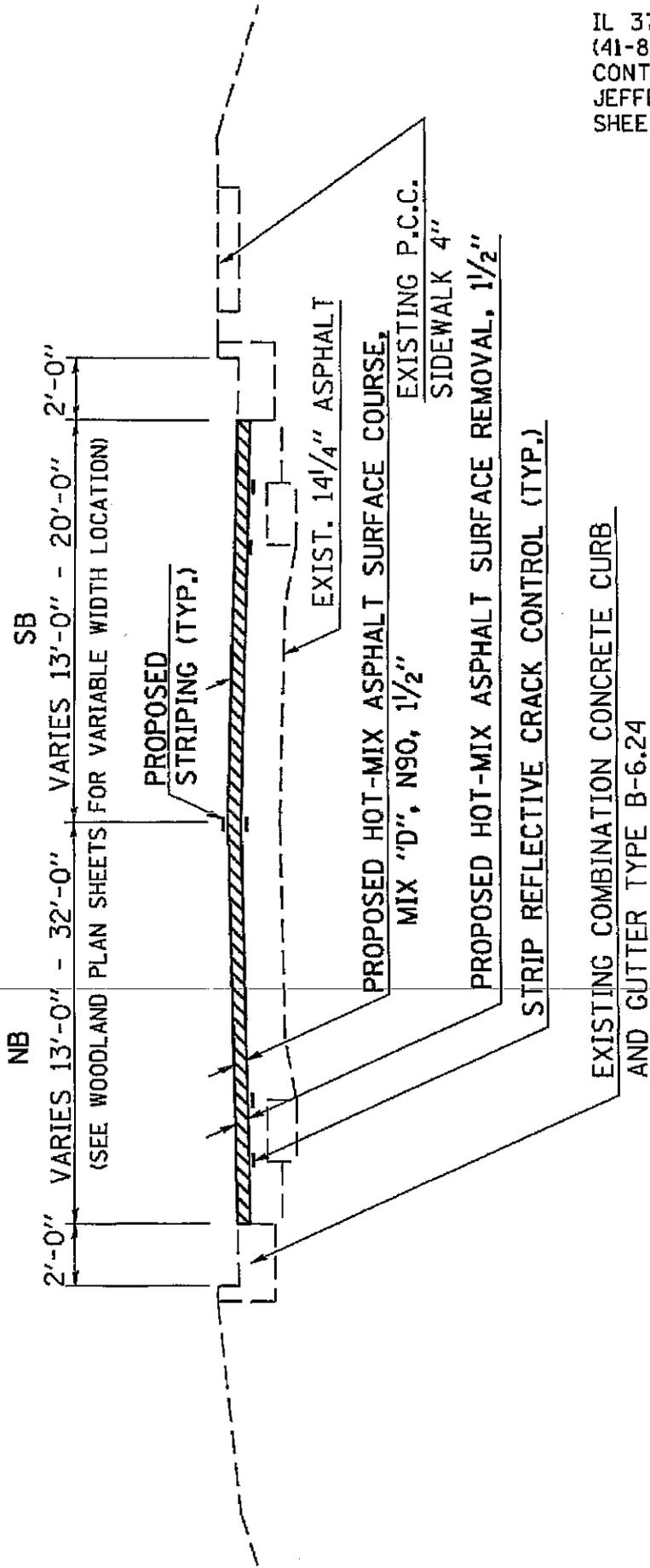
\*Specialty Items

\* \* \* \* \*

SUMMARY OF QUANTITIES

| CODE NUMBER | ITEM DESCRIPTION   | UNIT   | TOTAL QUANTITY    |                   | URBAN/RURAL |       | 100% STATE |         |
|-------------|--|--------|-------------------|-------------------|-------------|-------|------------|---------|
|             |  |        | LOCATION QUANTITY | LOCATION QUANTITY | JEFFERSON   | RURAL | JEFFERSON  | RURAL   |
| 70102620    | TRAFFIC CONTROL AND PROTECTION, STANDARD 701501              | L SUM  | 1                 | 0.5               |             |       |            | 0.5     |
| 70106800    | CHANGEABLE MESSAGE SIGN                                      | CAL MO | 8                 | 3                 | 1           |       |            | 1       |
| 70300100    | SHORT-TERM PAVEMENT MARKING                                  | FOOT   | 13,240            | 2,736             |             |       |            | 10,504  |
| 70300210    | TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS             | SO FT  | 252               | 193               |             |       |            | 59      |
| 70300220    | TEMPORARY PAVEMENT MARKING - LINE 4"                         | FOOT   | 171,950           | 27,928            |             |       |            | 144,022 |
| 70300260    | TEMPORARY PAVEMENT MARKING - LINE 12"                        | FOOT   | 760               | 350               |             |       |            | 410     |
| 70300280    | TEMPORARY PAVEMENT MARKING - LINE 24"                        | FOOT   | 169               | 169               |             |       |            |         |
| 70301000    | WORK ZONE PAVEMENT MARKING REMOVAL                           | SO FT  | 4,413             | 912               |             |       |            | 3,501   |
| 70400100    | TEMPORARY CONCRETE BARRIER                                   | FOOT   | 275               | 137.5             |             |       |            | 137.5   |
| 70400200    | RELOCATE TEMPORARY CONCRETE BARRIER                          | FOOT   | 1,100             | 137.5             |             |       |            | 962.5   |
| 78001100    | PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS                 | SO FT  | 252               | 193               |             |       |            | 59      |
| 78001110    | PAINT PAVEMENT MARKING - LINE 4"                             | FOOT   | 171,950           | 27,928            |             |       |            | 144,022 |
| 78001150    | PAINT PAVEMENT MARKING - LINE 12"                            | FOOT   | 760               | 350               |             |       |            | 410     |
| 78001180    | PAINT PAVEMENT MARKING - LINE 24"                            | FOOT   | 169               | 169               |             |       |            |         |
| 78100100    | RAISED REFLECTIVE PAVEMENT MARKER                            | EACH   | 1,006             | 241               |             |       |            | 765     |
| 78200420    | GUARDRAIL MARKERS, TYPE B                                    | EACH   | 12                | 1                 |             |       |            | 11      |
| 78201000    | TERMINAL MARKER - DIRECT APPLIED                             | EACH   | 15                | 3                 |             |       |            | 12      |
| 78300200    | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL                    | EACH   | 1,006             | 241               |             |       |            | 765     |
| XX005496    | TRAFFIC BARRIER TERMINAL, TYPE 6 (SPECIAL)                   | EACH   | 1                 | 1                 |             |       |            |         |
| Z0030150    | IMPACT ATTENUATORS, NON-REDIRECTIVE, TEST LEVEL 3            | EACH   | 4                 | 2                 |             |       |            | 2       |
| Z0030350    | IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3 | EACH   | 16                | 2                 |             |       |            | 14      |
| Z0017100    | DOWEL BARS   | EACH   | 320               | 320               |             |       |            |         |
| Z0048665    | RAILROAD PROTECTIVE LIABILITY INSURANCE                      | L SUM  | 1                 |                   |             |       |            | 1       |

**TYPICAL SECTION**  
 IL 37 (FAP 724/FAS 2869)  
 NOT TO SCALE



IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 9 OF 72

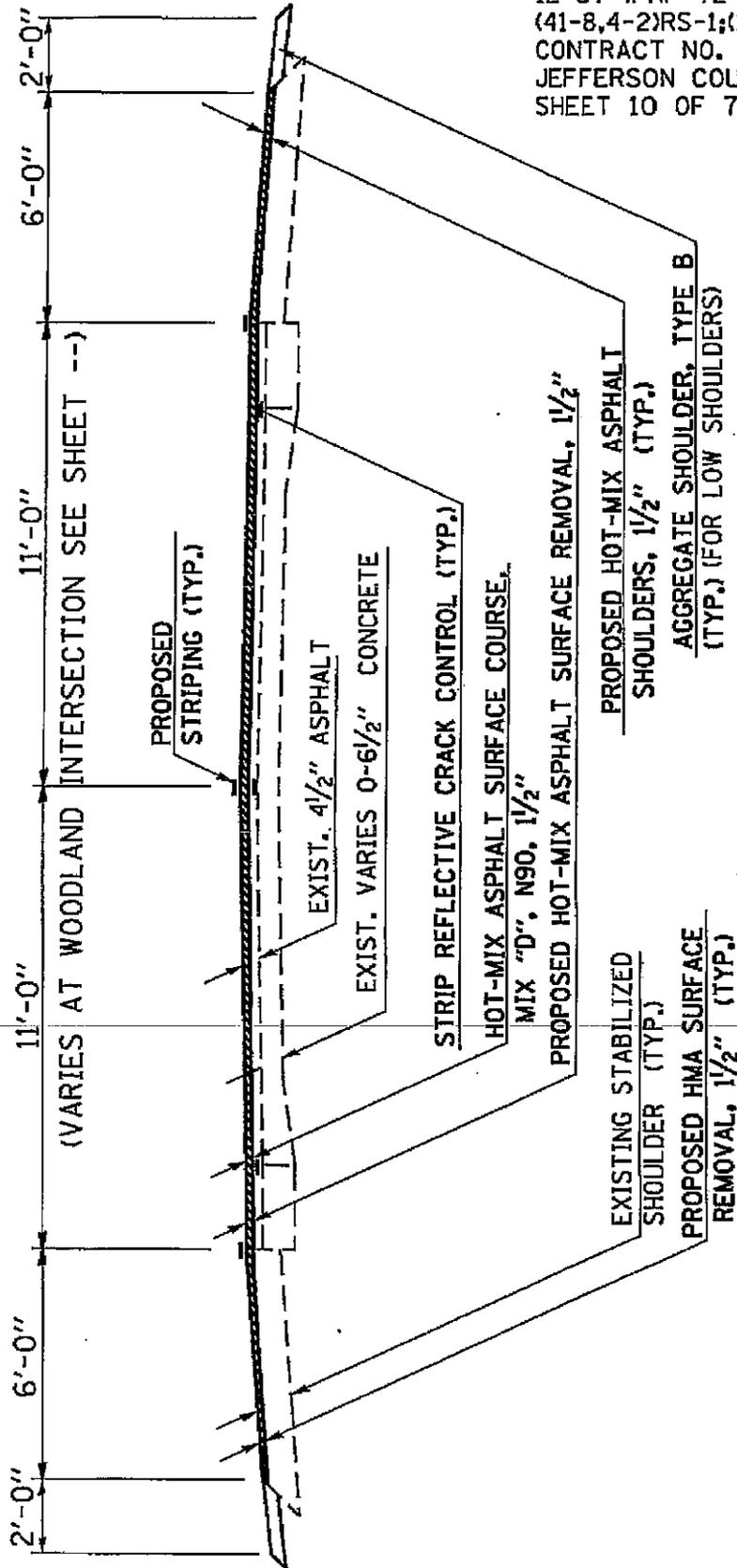
**TO BE USED:**

STA. 140+15 TO STA. 91+80  
 (VARIABLE WIDTH FROM STA. 140+15 TO STA. 140+04)  
 AND STA. 100+85 TO STA. 91+80

# TYPICAL SECTION

IL 37 (FAP 724/FAS 2869)

NOT TO SCALE



IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 10 OF 72

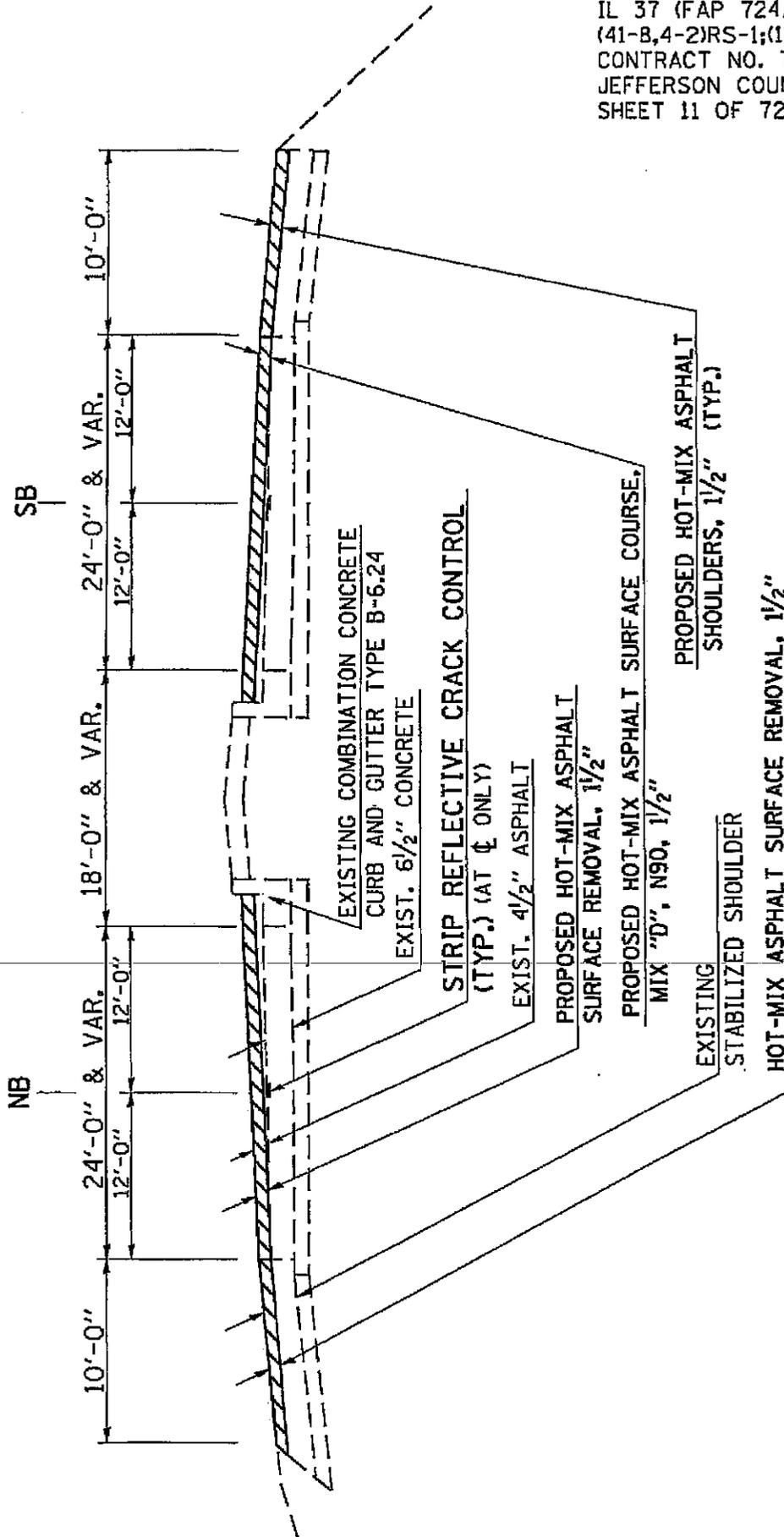
**TO BE USED:**

STA. 91+80 TO STA. 67+73  
 STA. 120+65.50 AH TO STA. 121+07.84\*

\* 10' PAVED SHOULDER THIS REGION

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**TYPICAL SECTION**  
 IL 37 (FAP 724/FAS 2869)  
 NOT TO SCALE

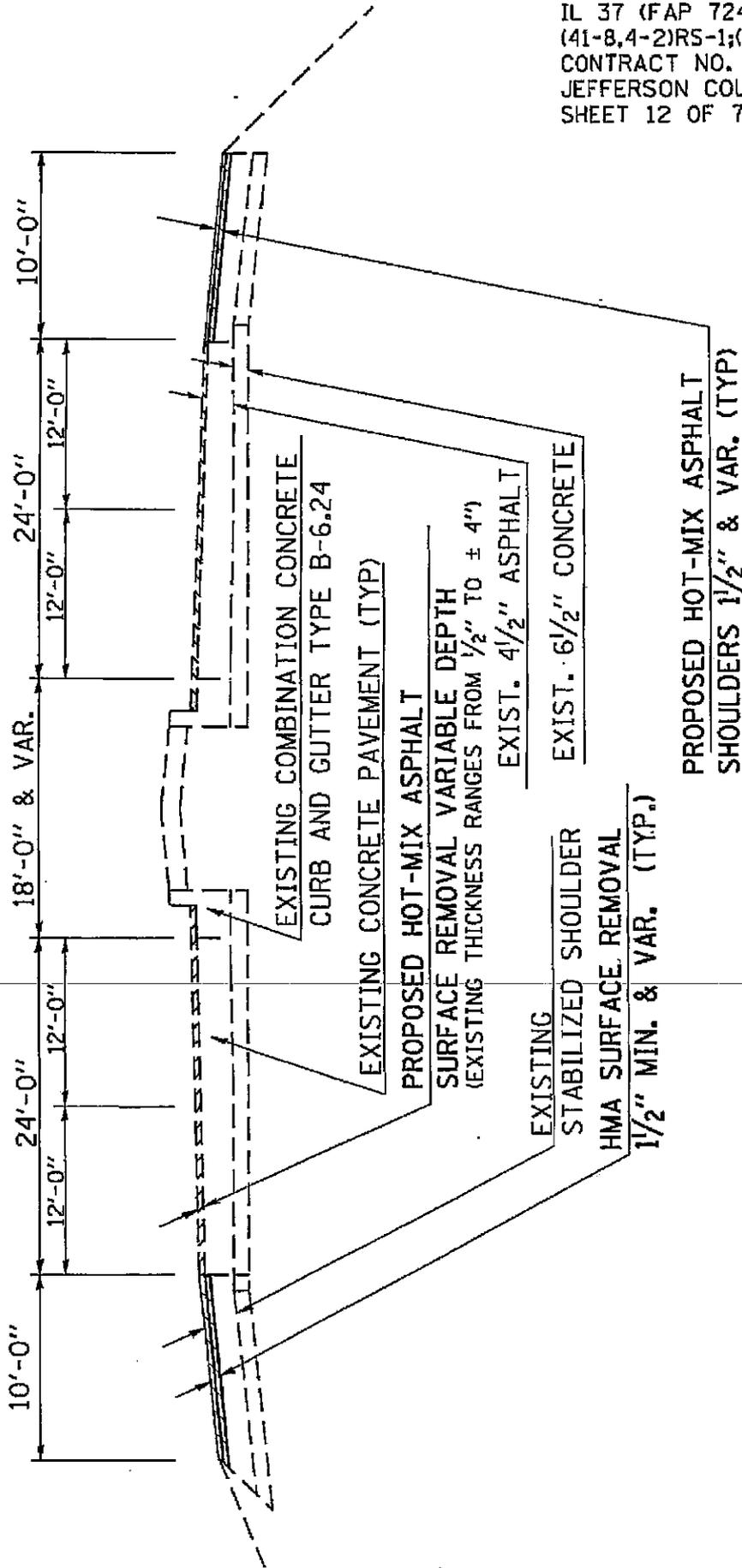


**TO BE USED:**  
 STA. 67+73 TO STA. 51+90  
 STA. 48+32 TO STA. 35+00 AH

# TYPICAL SECTION

IL 37 (FAP 724/FAS 2869)

NOT TO SCALE

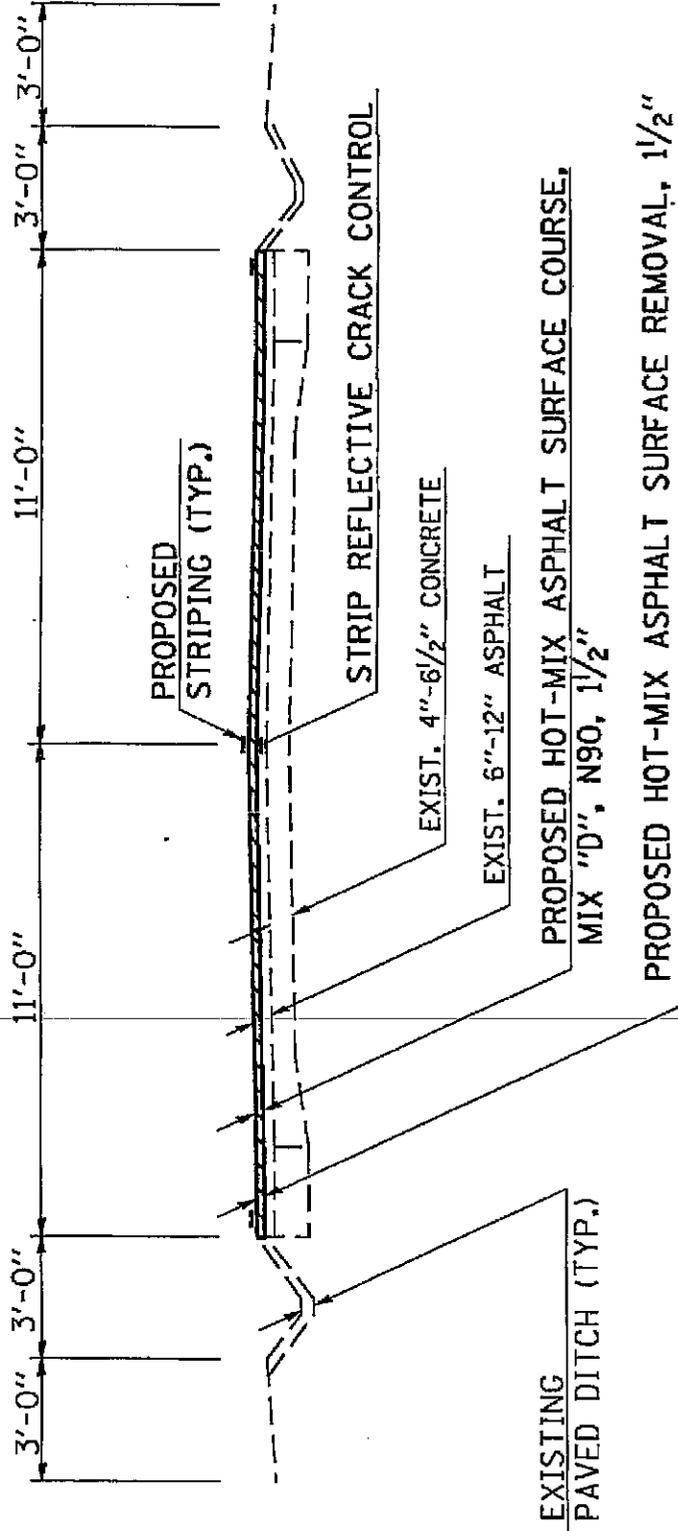


**TO BE USED:**

STA. 51+90 TO STA. 48+32  
SEE DETAIL SHEET FOR MORE INFORMATION

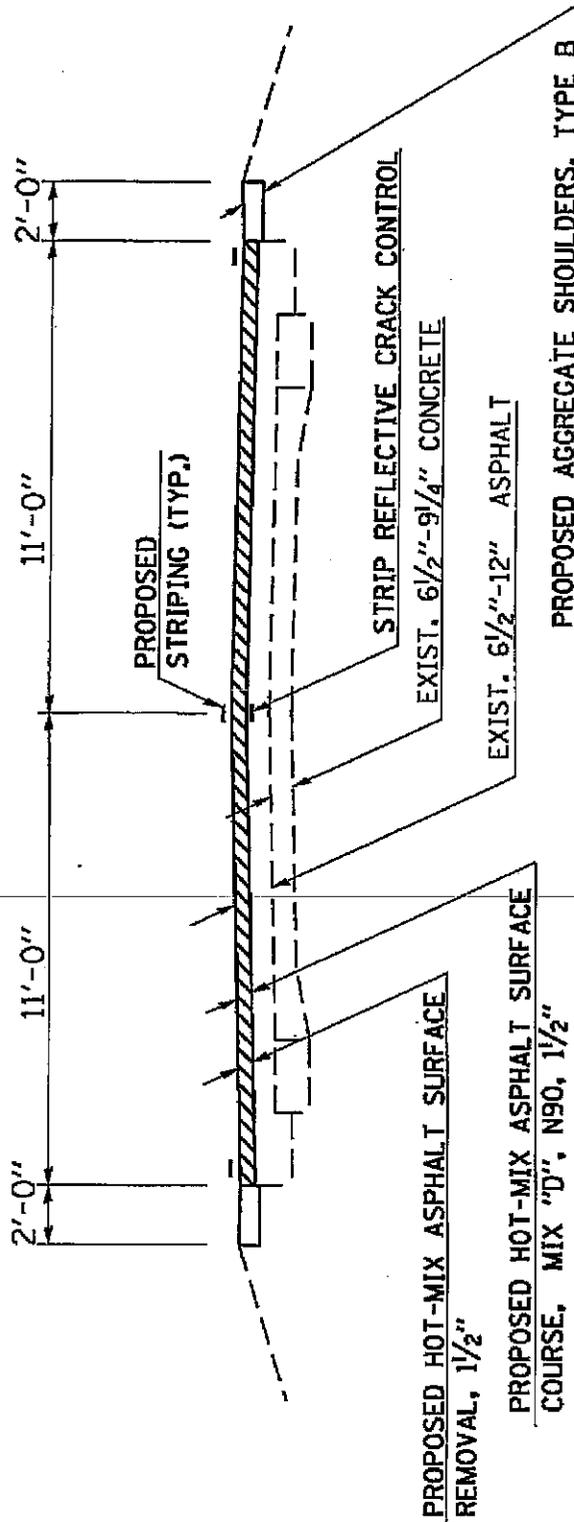
IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 12 OF 72

**TYPICAL SECTION**  
 IL 37 (FAP 724/FAS 2869)  
 NOT TO SCALE



**TO BE USED:**  
 STA. 372+37 TO STA. 379+57  
 STA. 542+50 TO STA. 568+94

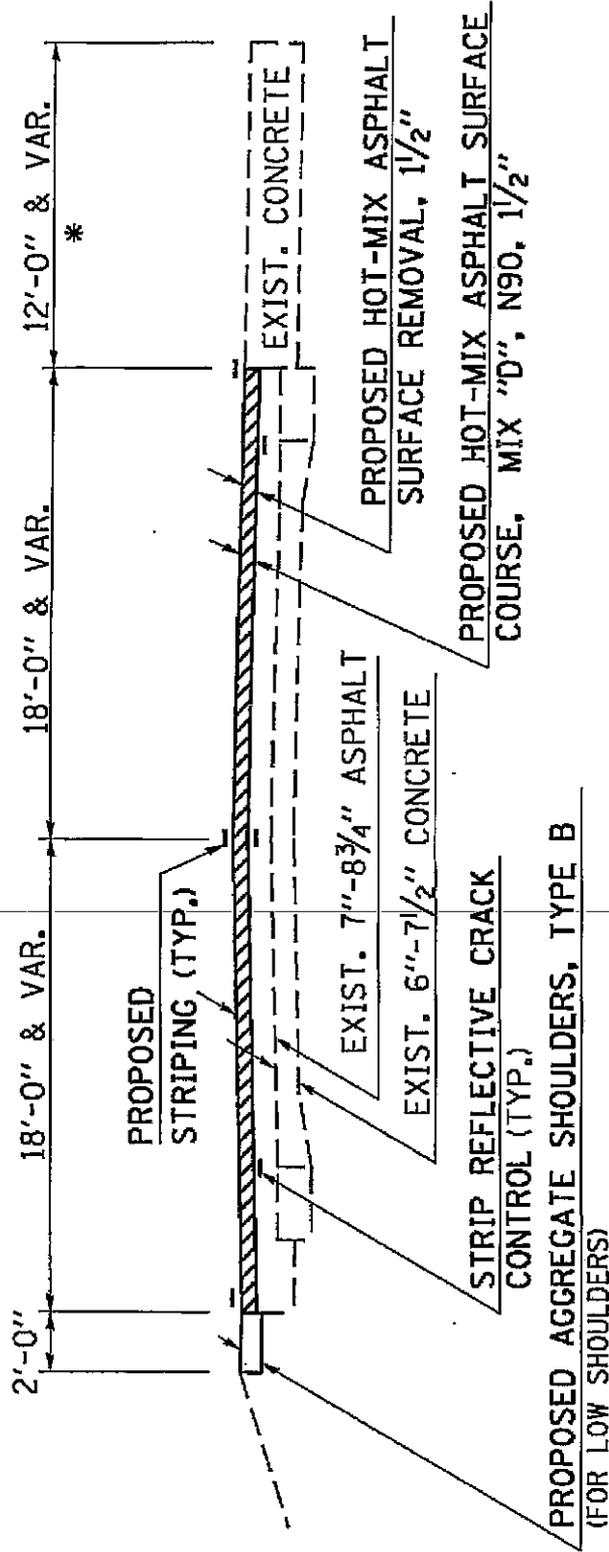
**TYPICAL SECTION**  
 IL 37 (FAP 724/FAS 2869)  
 NOT TO SCALE



**TO BE USED:**

- STA. 124+92.16 TO 136+43.29 BK
- STA. 137+39.62 AH TO STA. 197+30
- STA. 198+44 TO STA. 205+56
- STA. 206+91 TO 362+76.86 BK
- STA. 362+64.47 AH TO STA. 372+37
- STA. 379+57 TO STA. 542+50
- STA. 568+94 TO STA. 619+05
- STA. 642+54 TO STA. 652+50

**TYPICAL SECTION**  
 IL 37 (FAP 724/FAS 2869)  
 NOT TO SCALE



IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 15 OF 72

**TO BE USED:**

STA. 619+05 TO STA. 630+05  
 \* STA. 630+05 TO STA. 637+13 (LIMITS OF THE CONCRETE TURN LANE)  
 STA. 637+13 TO STA. 642+54

PLT DATE = 4/9/2009  
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 USER NAME = haddon

## SURFACE REMOVAL

| LOCATION<br>STATION TO STATION    | HMA SURFACE<br>REMOVAL,<br>1.5" | HMA SURFACE<br>REMOVAL<br>VAR. DEPTH | TEMPORARY<br>RAMP | REMARKS                    |
|-----------------------------------|---------------------------------|--------------------------------------|-------------------|----------------------------|
|                                   | SQ YD                           | SQ YD                                | SQ YD             |                            |
| <b>FAS 724 (IL 37)</b>            |                                 |                                      |                   |                            |
| STA 140+15 TO STA 130+04          | 2921                            |                                      | 15                | TEMP RAMP AT BEGIN         |
| STA 130+04 TO STA 100+82          | 8441                            |                                      | 29                | 2 TEMP RAMPS AT RR         |
| STA 100+82 TO STA 97+50           | 1199                            |                                      |                   |                            |
| STA 97+50 TO STA 94+00            | 1517                            |                                      |                   | SEE NOTE 2**               |
| STA 94+00 TO STA 91+80            | 1271                            |                                      |                   | SEE NOTE 2**               |
| * STA 91+80 TO STA 89+00          | 1556                            |                                      |                   | SEE NOTE 2**               |
| * STA 89+00 TO STA 89+00          | 7556                            |                                      |                   |                            |
| * STA 89+00 TO STA 87+73          | 494                             |                                      |                   |                            |
| * SB STA 67+73 TO STA 58+75       | 2794                            |                                      |                   | LANE WIDTH TRANSITION      |
| * SB STA 58+75 TO STA 51+90       | 2588                            |                                      | 14                |                            |
| * SB STA 51+90 TO STA 48+32       |                                 | 1353                                 |                   | BENEATH I-64, 2 TEMP RAMPS |
| * SB STA 48+32 TO STA 44+00       | 1632                            |                                      | 14                |                            |
| * SB STA 44+00 TO STA 35+00.00 BK | 2750                            |                                      |                   | LANE WIDTH TRANSITION      |
| * NB STA 67+73 TO STA 58+75       | 2794                            |                                      |                   | LANE WIDTH TRANSITION      |
| * NB STA 58+75 TO STA 51+90       | 2588                            |                                      | 20                |                            |
| * NB STA 51+90 TO STA 48+32       |                                 | 1353                                 |                   | BENEATH I-64, 2 TEMP RAMPS |
| * NB STA 48+32 TO STA 44+00       | 1632                            |                                      | 14                |                            |
| * NB STA 44+00 TO STA 35+00.00 BK | 2750                            |                                      |                   | LANE WIDTH TRANSITION      |
| NB STA 57+09 TO STA 54+00         | 412                             |                                      |                   | TURN LANE TO WB I-64       |
| NB STA 54+00 TO STA 52+50         | 100                             |                                      |                   | TURN LANE TO WB I-64       |
| STA 57+89 TO STA 57+09            | 281                             |                                      |                   | INTERSECTION               |
| STA 44+90 TO STA 44+10            | 282                             |                                      |                   | INTERSECTION               |
| <b>LOCATION TOTAL</b>             | <b>45558</b>                    | <b>2706</b>                          | <b>106</b>        |                            |
| <b>FAP 2869 (IL 37)</b>           |                                 |                                      |                   |                            |
| STA 120+65.50 AH TO STA 121+08    | 198                             |                                      |                   |                            |
| SN 041-0032                       |                                 |                                      | 25                | 2 TEMP RAMPS AT STRUCT     |
| STA 124+92 TO STA 136+43.29 BK    | 2814                            |                                      |                   |                            |
| STA 137+39.62 AH TO STA 197+30    | 14643                           |                                      |                   |                            |
| SN 041-0105                       |                                 |                                      | 25                | 2 TEMP RAMPS AT STRUCT     |
| STA 198+44 TO STA 205+56          | 1740                            |                                      |                   |                            |
| SN 041-0097                       |                                 |                                      | 25                | 2 TEMP RAMPS AT STRUCT     |
| STA 208+91 TO STA 382+76.88 BK    | 38099                           |                                      |                   |                            |
| STA 382+64.47 AH TO STA 372+37    | 2377                            |                                      |                   |                            |
| STA 372+37 TO STA 379+57          | 1760                            |                                      |                   |                            |
| STA 379+57 TO STA 542+50          | 39827                           |                                      |                   |                            |
| STA 542+50 TO STA 568+94          | 6483                            |                                      |                   |                            |
| STA 568+94 TO STA 619+05          | 12249                           |                                      |                   |                            |
| STA 619+05 TO STA 622+75          | 1192                            |                                      |                   | SEE NOTE 3***              |
| STA 622+75 TO STA 637+13          | 5752                            |                                      |                   | SEE NOTE 3***              |
| STA 637+13 TO STA 642+53          | 1740                            |                                      |                   | SEE NOTE 3***              |
| STA 642+53 TO STA 648+34          | 1420                            |                                      |                   |                            |
| STA 648+34 TO STA 652+50          | 1017                            |                                      | 12                |                            |
| <b>LOCATION TOTAL</b>             | <b>131291</b>                   | <b>0</b>                             | <b>87</b>         |                            |
| <b>PROJECT TOTAL</b>              | <b>176849</b>                   | <b>2706</b>                          | <b>193</b>        |                            |

\*NOTE: STA 91+50 TO STA 35+00 QUANTITIES INCLUDE HMA SHOULDER REMOVAL AND REMOVAL FOR RAMP TRANSITIONS.  
 \*\*NOTE: SEE PLAN SHEET 60 FOR PAVEMENT WIDTHS AT WOODLAND ROAD.  
 \*\*\*NOTE: SEE PLAN SHEET 64 FOR PAVEMENT WIDTHS AT PRISON ROAD.

## BUTT JOINT REMOVAL

| LOCATION<br>STATION TO STATION | PCC SURFACE<br>REMOVAL,<br>BUTT JOINT<br>SQ YD |
|--------------------------------|--|
| <b>FAP 724 (IL 37)</b>         |  |
| STA 51+90                      | 107  |
| STA 48+32                      | 107  |
| <b>PROJECT TOTAL</b>           | <b>314</b>                                     |

**RESURFACING SCHEDULE**

| LOCATION<br>STATION TO STATION      | HOT - MIX<br>ASPHALT<br>SURFACE COURSE<br>MIX "D", N90, 1.5" |              | BITUMINOUS<br>MATERIAL<br>(PRIMECOAT) |            | AGGREGATE<br>(PRIMECOAT) |              | STRIP<br>REFLECTIVE<br>CRACK<br>CONTROL<br>FOOT | REMARKS                     |
|-------------------------------------|--|--------------|---------------------------------------|------------|--------------------------|--------------|---|-----------------------------|
|                                     | TON  | GALLON       | GALLON                                | TON        | TON                      | TON          |   |                             |
| <b>FAS 724 (IL 37)</b>              |  |              |                                       |            |                          |              |   |                             |
| STA 140+15 TO STA 140+04            | 3  |              | 3                                     |            | 0.1                      |              | 55  |                             |
| STA 140+04 TO STA 91+80             | 1287   | 1,378        | 1,378                                 | 23.0       | 24120                    | 24120        | 24120   | SEE WOODLAND RD PLAN SHT 50 |
| STA 91+80 TO STA 89+00              | 100  | 106          | 106                                   | 1.8        | 840                      | 840          | 840   | SEE WOODLAND RD PLAN SHT 50 |
| STA 89+00 TO STA 89+00              | 413  | 440          | 440                                   | 7.3        | 6000                     | 6000         |   |                             |
| STA 89+00 TO STA 67+73              | 26   | 29           | 29                                    | 0.5        | 381                      | 381          |   |                             |
| <b>SB STA 67+73 TO STA 58+75</b>    |  |              |                                       |            |                          |              |   |                             |
| SB STA 58+75 TO STA 51+80           | 152  | 162          | 162                                   | 2.7        | 898                      | 898          |   | SEE L-64 PLAN SHEETS        |
| SB STA 48+32 TO STA 35+00.00 BK     | 187  | 189          | 189                                   | 2.7        | 685                      | 685          |   | SEE L-64 PLAN SHEETS        |
| NB STA 67+73 TO STA 58+75           | 251  | 268          | 268                                   | 3.9        | 1332                     | 1332         |   | SEE L-64 PLAN SHEETS        |
| NB STA 58+75 TO STA 51+90           | 152  | 162          | 162                                   | 2.7        | 898                      | 898          |   | SEE L-64 PLAN SHEETS        |
| NB STA 51+90 TO STA 35+00.00        | 154  | 184          | 184                                   | 2.7        | 685                      | 685          |   | SEE L-64 PLAN SHEETS        |
| NB STA 48+32 TO STA 35+00.00        | 219  | 233          | 233                                   | 3.9        | 1332                     | 1332         |   | SEE L-64 PLAN SHEETS        |
| <b>NB STA 57+09 TO STA 54+00</b>    |  |              |                                       |            |                          |              |   |                             |
| NB STA 54+00 TO STA 52+50           | 35   | 37           | 37                                    | 0.6        |                          |              |   | TURN LANE                   |
| NB STA 57+89 TO STA 57+09           | 8  | 9            | 9                                     | 0.2        |                          |              |   | TURN LANE                   |
| NB STA 44+80 TO STA 44+10           | 13   | 14           | 14                                    | 0.2        |                          |              |   | WB RAMPS INTERSECTION       |
| NB STA 44+80 TO STA 44+10           | 13   | 14           | 14                                    | 0.2        |                          |              |   | EB RAMPS INTERSECTION       |
| <b>TOTAL FROM SHOULDER SCHEDULE</b> |  |              |                                       |            |                          |              |   |                             |
| LOCATION TOTAL                      | 3013   | 4161         | 4161                                  | 52         | 37226                    | 37226        |   |                             |
| <b>FAP 2869 (IL 37)</b>             |  |              |                                       |            |                          |              |   |                             |
| STA 120+65.50 AH TO STA 121+08      | 9  | 9            | 9                                     | 0.2        |                          |              | 43  |                             |
| STA 124+92 TO STA 136+43.29 BK      | 238  | 253          | 253                                   | 4.2        | 1151                     | 1151         |   |                             |
| STA 137+39.82 AH TO STA 197+30      | 1237   | 1,318        | 1,318                                 | 22.0       | 5990                     | 5990         |   |                             |
| STA 198+44 TO STA 205+56            | 147  | 157          | 157                                   | 2.6        | 712                      | 712          |   |                             |
| STA 208+91 TO STA 362+76.86 BK      | 3218   | 3,429        | 3,429                                 | 57.2       | 15586                    | 15586        |   |                             |
| STA 619+05 TO STA 619+05            | 5295   | 5,641        | 5,641                                 | 94.0       | 25641                    | 25641        |   |                             |
| STA 619+05 TO STA 622+75            | 101  | 107          | 107                                   | 1.8        | 1110                     | 1110         |   | SEE PRISON RD PLAN SHEET 54 |
| STA 622+75 TO STA 637+13            | 485  | 518          | 518                                   | 8.6        | 4314                     | 4314         |   | SEE PRISON RD PLAN SHEET 54 |
| STA 637+13 TO STA 642+53            | 147  | 157          | 157                                   | 2.6        | 1820                     | 1820         |   | SEE PRISON RD PLAN SHEET 54 |
| STA 642+53 TO STA 648+34            | 120  | 128          | 128                                   | 2.1        | 581                      | 581          |   |                             |
| STA 648+34 TO STA 652+50            | 86   | 92           | 92                                    | 1.5        | 416                      | 416          |   |                             |
| <b>TOTAL FROM SHOULDER SCHEDULE</b> |  |              |                                       |            |                          |              |   |                             |
| LOCATION TOTAL                      | 11083  | 11818        | 11818                                 | 197        | 57164                    | 57164        |   |                             |
| <b>PROJECT TOTAL</b>                | <b>14096</b>   | <b>15979</b> | <b>15979</b>                          | <b>249</b> | <b>94330</b>             | <b>94330</b> |   |                             |

\*SURFACE COURSE AND BITUMINOUS PRIMECOAT INCLUDES QUANTITY FOR RAMP TRANSITIONS

# SHOULDERS

| LOCATION<br>STATION TO STATION |                     | AGGREGATE<br>SHOULDERS,<br>TYPE B<br>TON | HOT-MIX<br>ASPHALT<br>SHOULDERS<br>TON | BITUMINOUS<br>MATERIALS<br>(PRIMECOAT)<br>GALLON | REMARKS                       |
|--------------------------------|---------------------|--|--|--|-------------------------------|
| <b>FAS 724 (IL 37)</b>         |                     |  |  |  |                               |
| STA 91+80                      | TO STA 89+00        | 11                                       | 32                                     | 34   |                               |
| STA 89+00                      | TO STA 73+43        | 59                                       | 178                                    | 187  |                               |
| STA 73+43                      | TO STA 67+73        |  | 108                                    | 114  | 10' SHLDRS BEGIN AT BETHEL RD |
| SB STA 67+73                   | TO STA 58+60        |  | 87                                     | 91   |                               |
| SB STA 58+60                   | TO STA 51+90        |  | 40                                     | 43   |                               |
| SB STA 51+90                   | TO STA 48+32        |  | 51                                     | 36   | VARIABLE DEPTH SHOULDERS      |
| SB STA 48+32                   | TO STA 45+40        |  | 28                                     | 29   |                               |
| SB STA 43+00                   | TO STA 35+00        |  | 76                                     | 80   |                               |
| NB STA 67+73                   | TO STA 58+75        |  | 86                                     | 90   |                               |
| NB STA 58+75                   | TO STA 51+90        |  | 65                                     | 69   |                               |
| NB STA 51+90                   | TO STA 48+32        |  | 51                                     | 36   | VARIABLE DEPTH SHOULDERS      |
| NB STA 48+32                   | TO STA 43+00        |  | 51                                     | 53   |                               |
| NB STA 43+00                   | TO STA 35+00.00 BK  |  | 76                                     | 80   |                               |
| LOCATION TOTAL                 |                     | 70                                       | 929                                    | 942  |                               |
| <b>FAP 2869 (IL 37)</b>        |                     |  |  |  |                               |
| STA 120+65.50 AH               | TO STA 121+06       |  | 8                                      | 9  |                               |
| SN 041-0032                    |                     |  |  |  |                               |
| STA 124+92                     | TO STA 136+43.29 BK | 44                                       |  |  |                               |
| STA 137+39.62 AH               | TO STA 197+30       | 227                                      |  |  |                               |
| SN 041-0105                    |                     |  |  |  |                               |
| STA 198+44                     | TO STA 205+56       | 27                                       |  |  |                               |
| SN 041-0097                    |                     |  |  |  |                               |
| STA 206+91                     | TO STA 362+76.86 BK | 592                                      |  |  |                               |
| STA 362+64.47 AH               | TO STA 372+37       | 37                                       |  |  |                               |
| STA 379+57                     | TO STA 542+50       | 619                                      |  |  |                               |
| STA 568+94                     | TO STA 619+05       | 180                                      |  |  |                               |
| STA 619+05                     | TO STA 622+75       | 14                                       |  |  |                               |
| STA 622+75                     | TO STA 637+13       | 55                                       |  |  |                               |
| STA 637+13                     | TO STA 642+53       | 21                                       |  |  |                               |
| STA 642+53                     | TO STA 648+34       | 22                                       |  |  |                               |
| STA 648+34                     | TO STA 652+50       | 16                                       |  |  |                               |
| LOCATION TOTAL                 |                     | 1864                                     | 8                                      | 9  |                               |
| PROJECT TOTAL                  |                     | 1934                                     | 937                                    | 951  |                               |

**GUARDRAIL**

| LOCATION<br>STATION TO STATION  | TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL TANGENT |      | TRAFFIC BARRIER TERMINAL, TYPE 6A |      | TRAFFIC BARRIER TERMINAL, TYPE 6A, SPECIAL |      | TRAFFIC BARRIER TERMINAL, TYPE 2 |        | STEEL PLATE BEAM GUARDRAIL (SHORT RADIUS) |      | STEEL PLATE BEAM GUARDRAIL TYPE A, 67.5 POST |      | STEEL RAILING TYPE 2389 |      | HANDRAIL CONCRETE REMOVAL* |                            | REMARKS |
|---------------------------------|--|------|-----------------------------------|------|--|------|----------------------------------|--------|---|------|--|------|-------------------------|------|----------------------------|----------------------------|---------|
|                                 | EACH   | EACH | EACH                              | EACH | EACH                                       | EACH | EACH                             | FOOT   | FOOT                                      | FOOT | FOOT   | FOOT | FOOT                    | FOOT | FOOT                       |                            |         |
| FAS 724 (IL 37)<br>SN 041-0091  | 3  | 3    | 3                                 | 1    |  |      |                                  | 390.25 | 22  |      |  |      | 62                      |      | 62                         | ALL TYPE 1 FOR REPLACEMENT |         |
| LOCATION TOTAL                  | 3  | 3    | 3                                 | 1    | 0  | 0    | 390.25                           | 22     |   |      |  |      | 62                      |      | 62                         |                            |         |
| FAP 2869 (IL 37)<br>SN 041-0092 | 1  |      |                                   |      |  |      | 50.00                            |        |   |      |  |      |                         |      |                            | ALL TYPE 1 FOR REPLACEMENT |         |
| SN 041-0097                     | 4  |      |                                   |      |  |      | 200.00                           |        |   |      |  |      |                         |      |                            | ALL TYPE 1 FOR REPLACEMENT |         |
| SN 041-0095                     | 2  | 4    | 4                                 |      | 2  |      |                                  |        |   |      | 100  |      | 62                      |      | 62                         |                            |         |
| SN 041-0092                     | 2  | 4    | 4                                 |      | 2  |      | 50.00                            |        |   |      | 350  |      | 50                      |      | 50                         |                            |         |
| SN 041-0095                     | 2  | 4    | 4                                 |      | 1  |      | 167.50                           |        |   |      | 75   |      | 40                      |      | 40                         | 1 TYPE 1 FOR REPLACEMENT   |         |
| SN 041-0096                     | 1  |      |                                   |      |  |      |                                  |        |   |      |  |      |                         |      |                            | ALL TYPE 1 FOR REPLACEMENT |         |
| SN 041-0093                     |  | 4    | 4                                 |      | 2  |      |                                  |        |   |      | 150  |      | 44                      |      | 44                         |                            |         |
| LOCATION TOTAL                  | 12   | 16   | 16                                | 0    | 7  | 0    | 457.50                           | 0      | 0   |      |  |      | 196                     |      | 196                        |                            |         |
| PROJECT TOTAL                   | 43   | 19   | 19                                | 4    | 7  | 4    | 847.75                           | 22     | 22  |      |  |      | 673                     |      | 673                        | 258                        |         |

\*NAME PLATES NEED TO BE REMOVED AND REPLACED AT EACH STRUCTURE WITH HANDRAIL CONCRETE REMOVAL.

# TEMPORARY TRAFFIC BARRIERS

| LOCATION<br>STATION TO STATION | IMPACT<br>ATTENUATORS<br>(NON-REDIRECT)<br>TEST LEVEL 3<br>EACH | IMPACT ATTEN,<br>RELOCATE,<br>(NON-REDIRECT)<br>TEST LEVEL 3<br>EACH | RELOCATE<br>TEMPORARY<br>CONCRETE<br>BARRIER |              | TEMPORARY<br>CONCRETE<br>BARRIER |              | REMARKS                                      |
|--------------------------------|---|--|--|--------------|----------------------------------|--------------|--|
|                                |   |  | FOOT   | FOOT         | FOOT                             | FOOT         |  |
| FAS 724 (IL 37)                |   |  |  |              |                                  |              |  |
| SN 041-0031                    | 2   | 2  | 137.5  | 137.5        | 137.5                            | 137.5        | USE AT 2 LOCATIONS AT SAME TIME IN SAME LANE |
| <b>LOCATION TOTAL</b>          | <b>2</b>  | <b>2</b>   | <b>137.5</b>                                 | <b>137.5</b> | <b>137.5</b>                     | <b>137.5</b> |  |
| FAP 2889 (IL 37)               |   |  |  |              |                                  |              |  |
| SN 041-0085                    | 2   | 2  | 137.5  | 137.5        | 137.5                            | 137.5        | USE AT 2 LOCATIONS AT SAME TIME IN SAME LANE |
| SN 041-0092                    |   | 4  |  | 275          |                                  |              |  |
| SN 041-0095                    |   | 4  |  | 275          |                                  |              |  |
| SN 041-0093                    |   | 4  |  | 275          |                                  |              |  |
| <b>LOCATION TOTAL</b>          | <b>2</b>  | <b>14</b>  | <b>962.5</b>                                 | <b>1100</b>  | <b>137.5</b>                     | <b>275</b>   |  |
| <b>PROJECT TOTAL</b>           | <b>4</b>  | <b>18</b>  | <b>1100</b>                                  | <b>1100</b>  | <b>275</b>                       | <b>275</b>   |  |

IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 20 OF 72

# PAVEMENT MARKING SCHEDULE

| LOCATION<br>STATION TO STATION | PAINT PAVEMENT MARKINGS |                    |                     |                 |                |               |               |      |                           |        |  | REMARKS          |
|--------------------------------|-------------------------|--------------------|---------------------|-----------------|----------------|---------------|---------------|------|---------------------------|--------|--|------------------|
|                                | LINE 4"                 |                    |                     | LINE 12"        |                |               | LINE 24"      |      | LETTERS<br>AND<br>SYMBOLS | SQ. YD |  |                  |
|                                | WHITE<br>SKIP-DASH      | WHITE<br>SKIP-DASH | YELLOW<br>SKIP-DASH | YELLOW<br>SOLID | YELLOW<br>FOOT | WHITE<br>FOOT | WHITE<br>FOOT |      |                           |        |  |                  |
|                                | FOOT                    | FOOT               | FOOT                | FOOT            | FOOT           | FOOT          | FOOT          |      |                           |        |  |                  |
| FAS 724 (IL 37)                |                         |                    |                     |                 |                |               |               |      |                           |        |  |                  |
| STA 140+15.50 TO STA 139+22    |                         |                    | 20                  |                 | 94             |               |               |      |                           |        |  |                  |
| STA 139+22 TO STA 133+54       |                         |                    | 140                 |                 |                |               |               |      |                           |        |  |                  |
| STA 133+54 TO STA 132+84       |                         |                    | 20                  |                 | 70             |               |               |      |                           |        |  |                  |
| STA 132+84 TO STA 130+04       |                         |                    | 70                  |                 | 280            |               |               |      |                           |        |  |                  |
| STA 130+04 TO STA 127+24       |                         |                    | 70                  |                 | 280            |               |               |      |                           |        |  |                  |
| STA 127+24 TO STA 126+54       |                         |                    | 20                  |                 | 70             |               |               |      |                           |        |  |                  |
| STA 126+54 TO STA 105+31       |                         |                    | 530                 |                 |                |               |               |      |                           |        |  |                  |
| STA 105+31 TO STA 100+82       |                         |                    | 110                 |                 | 449            |               |               |      |                           |        |  |                  |
| STA 100+82 TO STA 95+55        |                         |                    |                     |                 | 2108           |               |               | 180  |                           |        |  |                  |
| STA 95+55 TO STA 93+75         |                         |                    |                     |                 | 360            |               |               |      |                           |        |  |                  |
| STA 94+95 TO STA 93+75         | 120                     |                    |                     |                 |                |               |               |      |                           |        |  | TURN LANE        |
| STA 93+75 TO STA 93+00         |                         |                    |                     |                 |                |               |               |      |                           |        |  | OMISSION         |
| STA 93+00 TO STA 89+88         |                         |                    |                     |                 |                |               |               | 1248 |                           | 170    |  |                  |
| STA 93+00 TO STA 91+80         | 120                     |                    |                     |                 |                |               |               |      |                           |        |  | TURN LANE        |
| STA 89+88 TO STA 84+65         | 1046                    |                    |                     |                 |                |               |               | 1046 |                           |        |  |                  |
| STA 84+65 TO STA 81+22         | 686                     |                    | 90                  |                 | 343            |               |               |      |                           |        |  |                  |
| STA 81+22 TO STA 78+63         | 518                     |                    | 60                  |                 |                |               |               |      |                           |        |  |                  |
| STA 78+63 TO STA 72+61         | 1204                    |                    | 150                 |                 | 602            |               |               |      |                           |        |  |                  |
| STA 72+61 TO STA 68+81         | 760                     |                    | 100                 |                 |                |               |               |      |                           |        |  |                  |
| STA 68+81 TO STA 64+75         | 812                     |                    |                     |                 | 812            |               |               |      |                           |        |  |                  |
| STA 64+75 TO STA 63+85         | 180                     |                    |                     |                 | 360            |               |               |      |                           |        |  |                  |
| STA 63+85 TO STA 57+85         | 1200                    |                    | 300                 |                 | 1200           |               |               |      |                           |        |  | I-64 INTERCHANGE |
| STA 57+85 TO STA 57+15         |                         |                    | 40                  |                 |                |               |               |      |                           |        |  | I-64 INTERCHANGE |
| STA 57+15 TO STA 54+55         | 260                     |                    |                     |                 |                |               |               |      |                           |        |  | TURN LANE        |
| STA 57+15 TO STA 52+00         | 1030                    |                    | 260                 |                 | 1030           |               |               |      |                           |        |  | I-64 INTERCHANGE |
| STA 52+00 TO STA 44+75         | 1450                    |                    | 360                 |                 | 1450           |               |               |      |                           |        |  | I-64 INTERCHANGE |
| STA 44+75 TO STA 44+10         |                         |                    | 40                  |                 |                |               |               |      |                           |        |  | I-64 INTERCHANGE |
| STA 44+10 TO STA 39+31         | 958                     |                    | 240                 |                 | 958            |               |               |      |                           |        |  | I-64 INTERCHANGE |
| STA 39+31 TO STA 38+91         | 480                     |                    |                     |                 | 960            |               |               |      |                           |        |  | I-64 INTERCHANGE |
| STA 36+81 TO STA 35+00 BK      | 382                     |                    |                     |                 | 382            |               |               |      |                           |        |  | I-64 INTERCHANGE |

# PAVEMENT MARKING SCHEDULE

| LOCATION<br>STATION TO STATION | PAINT PAVEMENT MARKINGS |                            |                             |                         |                         |                         |                         |                         |                         |                         | REMARKS |                           |                                    |                  |
|--------------------------------|-------------------------|----------------------------|-----------------------------|-------------------------|-------------------------|-------------------------|-------------------------|-------------------------|-------------------------|-------------------------|---------|---------------------------|------------------------------------|------------------|
|                                | LINE 4"                 |                            |                             |                         |                         | LINE 12"                |                         |                         |                         |                         |         | LINE 24"<br>WHITE<br>FOOT | LETTERS<br>AND<br>SYMBOLS<br>SQ YD |                  |
|                                | WHITE<br>SOLID<br>FOOT  | WHITE<br>SKIP-DASH<br>FOOT | YELLOW<br>SKIP-DASH<br>FOOT | YELLOW<br>SOLID<br>FOOT |         |                           |                                    |                  |
|                                | FOOT                    | FOOT                       | FOOT                        | FOOT                    | FOOT                    | FOOT                    | FOOT                    | FOOT                    | FOOT                    | FOOT                    |         |                           |                                    |                  |
| STOP BAR                       |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           |                                    |                  |
| STA 93+38                      |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         | 27      |                           |                                    | WOODLAND RD      |
| STA 57+20                      |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         | 16      |                           |                                    | I-64 WB OFF RAMP |
| STA 56+90                      |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         | 16      |                           |                                    | I-64 WB OFF RAMP |
| STA 44+20                      |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         | 16      |                           |                                    | I-64 EB OFF RAMP |
| STA 43+90                      |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         | 16      |                           |                                    | I-64 EB OFF RAMP |
| RAILROAD MARKINGS              |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           |                                    |                  |
| STA 133+44 SB                  |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         | 13      |                           |                                    |                  |
| STA 132+94 SB                  |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         | 13      |                           |                                    |                  |
| STA 130+14 SB                  |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         | 13      |                           |                                    |                  |
| STA 129+94 NB                  |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         | 13      |                           |                                    |                  |
| STA 127+14 NB                  |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         | 13      |                           |                                    |                  |
| STA 126+64 NB                  |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         | 13      |                           |                                    |                  |
| RXR                            |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           |                                    |                  |
| STA 133+19 SB                  |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           | 61.2                               |                  |
| STA 126+89 NB                  |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           | 61.2                               |                  |
| ONLY                           |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           |                                    |                  |
| STA 94+75 SB                   |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           | 11.8                               |                  |
| STA 92+00 NB                   |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           | 11.8                               |                  |
| STA 56+55 NB                   |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           | 11.8                               |                  |
| TURN ARROW                     |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           |                                    |                  |
| STA 94+45 SB                   |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           | 8.8                                |                  |
| STA 92+30 NB                   |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           | 8.8                                |                  |
| STA 56+85 NB                   |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           | 8.8                                |                  |
| STA 56+05 NB                   |                         |                            |                             |                         |                         |                         |                         |                         |                         |                         |         |                           | 8.8                                |                  |
| LOCATION TOTAL                 | 11206                   | 1240                       | 1380                        | 14102                   | 350                     | 169                     | 193                     |                         |                         |                         |         |                           |                                    |                  |





# TEMPORARY PAVEMENT MARKING SCHEDULE

| LOCATION<br>STATION TO STATION | PAINT PAVEMENT MARKINGS |                    |                     |                 |                |                |                |               |               |                           |       | REMARKS |                  |
|--------------------------------|-------------------------|--------------------|---------------------|-----------------|----------------|----------------|----------------|---------------|---------------|---------------------------|-------|---------|------------------|
|                                | LINE 4"                 |                    |                     |                 | LINE 12"       |                |                | LINE 24"      |               | LETTERS<br>AND<br>SYMBOLS | SQ YD |         |                  |
|                                | WHITE<br>SOLID          | WHITE<br>SKIP-DASH | YELLOW<br>SKIP-DASH | YELLOW<br>SOLID | YELLOW<br>FOOT | YELLOW<br>FOOT | YELLOW<br>FOOT | WHITE<br>FOOT | WHITE<br>FOOT |                           |       |         |                  |
|                                | FOOT                    | FOOT               | FOOT                | FOOT            | FOOT           | FOOT           | FOOT           | FOOT          | FOOT          |                           |       |         |                  |
| FAS 724 (IL 37)                |                         |                    |                     |                 |                |                |                |               |               |                           |       |         |                  |
| STA 140+15.50 TO STA 139+22    |                         |                    |                     |                 | 20             |                |                |               | 94            |                           |       |         |                  |
| STA 139+22 TO STA 133+54       |                         |                    |                     | 140             |                |                |                |               |               |                           |       |         |                  |
| STA 133+54 TO STA 132+84       |                         |                    |                     | 20              |                |                |                |               | 70            |                           |       |         |                  |
| STA 132+84 TO STA 130+04       |                         |                    |                     | 70              |                |                |                |               | 280           |                           |       |         |                  |
| STA 130+04 TO STA 127+24       |                         |                    |                     | 70              |                |                |                |               | 280           |                           |       |         |                  |
| STA 127+24 TO STA 126+54       |                         |                    |                     | 20              |                |                |                |               | 70            |                           |       |         |                  |
| STA 126+54 TO STA 105+31       |                         |                    |                     | 530             |                |                |                |               |               |                           |       |         |                  |
| STA 105+31 TO STA 100+82       |                         |                    |                     | 110             |                |                |                |               | 449           |                           |       |         |                  |
| STA 100+82 TO STA 95+55        |                         |                    |                     |                 |                |                |                |               | 2108          |                           | 180   |         |                  |
| STA 95+55 TO STA 93+75         |                         |                    |                     |                 |                |                |                |               | 360           |                           |       |         |                  |
| STA 94+95 TO STA 93+75         |                         |                    |                     | 120             |                |                |                |               |               |                           |       |         | TURN LANE        |
| STA 93+75 TO STA 93+00         |                         |                    |                     |                 |                |                |                |               |               |                           |       |         | OMISSION         |
| STA 93+00 TO STA 89+88         |                         |                    |                     |                 |                |                |                |               |               | 1248                      | 170   |         |                  |
| STA 93+00 TO STA 91+80         |                         |                    |                     | 120             |                |                |                |               |               |                           |       |         | TURN LANE        |
| STA 89+88 TO STA 84+65         |                         |                    |                     | 1046            |                |                |                |               |               |                           |       |         |                  |
| STA 84+65 TO STA 81+22         |                         |                    |                     | 686             |                |                |                |               | 90            | 1046                      |       |         |                  |
| STA 81+22 TO STA 78+63         |                         |                    |                     | 518             |                |                |                |               | 60            |                           |       |         |                  |
| STA 78+63 TO STA 72+61         |                         |                    |                     | 1204            |                |                |                |               | 150           | 602                       |       |         |                  |
| STA 72+61 TO STA 68+81         |                         |                    |                     | 760             |                |                |                |               | 100           |                           |       |         |                  |
| STA 68+81 TO STA 64+75         |                         |                    |                     | 812             |                |                |                |               |               | 812                       |       |         |                  |
| STA 64+75 TO STA 63+85         |                         |                    |                     | 180             |                |                |                |               |               | 360                       |       |         |                  |
| STA 63+85 TO STA 57+85         |                         |                    |                     | 1200            |                |                |                |               | 300           | 1200                      |       |         | I-64 INTERCHANGE |
| STA 57+85 TO STA 57+15         |                         |                    |                     |                 |                |                |                |               | 40            |                           |       |         | I-64 INTERCHANGE |
| STA 57+15 TO STA 54+55         |                         |                    |                     | 260             |                |                |                |               |               |                           |       |         | TURN LANE        |
| STA 57+15 TO STA 52+00         |                         |                    |                     | 1030            |                |                |                |               | 260           |                           |       |         | I-64 INTERCHANGE |
| STA 52+00 TO STA 44+75         |                         |                    |                     | 1450            |                |                |                |               | 360           | 1450                      |       |         | I-64 INTERCHANGE |
| STA 44+75 TO STA 44+10         |                         |                    |                     |                 |                |                |                |               | 40            |                           |       |         | I-64 INTERCHANGE |
| STA 44+10 TO STA 39+31         |                         |                    |                     | 958             |                |                |                |               | 240           | 958                       |       |         | I-64 INTERCHANGE |
| STA 39+31 TO STA 36+91         |                         |                    |                     | 480             |                |                |                |               |               | 960                       |       |         | I-64 INTERCHANGE |
| STA 36+91 TO STA 35+00 BK      |                         |                    |                     | 382             |                |                |                |               |               | 382                       |       |         | I-64 INTERCHANGE |

# TEMPORARY PAVEMENT MARKING SCHEDULE

| LOCATION<br>STATION TO STATION | PAINT PAVEMENT MARKINGS |                            |                             |                         |                |               |          |  |                           |       |      | REMARKS          |
|--------------------------------|-------------------------|----------------------------|-----------------------------|-------------------------|----------------|---------------|----------|--|---------------------------|-------|------|------------------|
|                                | LINE 4"                 |                            |                             | LINE 12"                |                |               | LINE 24" |  | LETTERS<br>AND<br>SYMBOLS | SQ YD |      |                  |
|                                | WHITE<br>SOLID<br>FOOT  | WHITE<br>SKIP-DASH<br>FOOT | YELLOW<br>SKIP-DASH<br>FOOT | YELLOW<br>SOLID<br>FOOT | YELLOW<br>FOOT | WHITE<br>FOOT |          |  |                           |       |      |                  |
|                                | FOOT                    | FOOT                       | FOOT                        | FOOT                    | FOOT           | FOOT          |          |  |                           |       |      |                  |
| STOP BAR                       |                         |                            |                             |                         |                |               |          |  |                           |       |      |                  |
| STA 93+38                      |                         |                            |                             |                         |                |               |          |  |                           |       |      |                  |
| STA 57+20                      |                         |                            |                             |                         |                |               |          |  |                           |       | 27   | WOODLAND RD      |
| STA 56+90                      |                         |                            |                             |                         |                |               |          |  |                           |       | 16   | I-64 WB OFF RAMP |
| STA 44+20                      |                         |                            |                             |                         |                |               |          |  |                           |       | 16   | I-64 WB OFF RAMP |
| STA 43+90                      |                         |                            |                             |                         |                |               |          |  |                           |       | 16   | I-64 EB OFF RAMP |
| RAILROAD MARKINGS              |                         |                            |                             |                         |                |               |          |  |                           |       |      |                  |
| STA 133+44 SB                  |                         |                            |                             |                         |                |               |          |  |                           |       | 13   |                  |
| STA 132+94 SB                  |                         |                            |                             |                         |                |               |          |  |                           |       | 13   |                  |
| STA 130+14 SB                  |                         |                            |                             |                         |                |               |          |  |                           |       | 13   |                  |
| STA 129+94 NB                  |                         |                            |                             |                         |                |               |          |  |                           |       | 13   |                  |
| STA 127+14 NB                  |                         |                            |                             |                         |                |               |          |  |                           |       | 13   |                  |
| STA 126+64 NB                  |                         |                            |                             |                         |                |               |          |  |                           |       | 13   |                  |
| RXR                            |                         |                            |                             |                         |                |               |          |  |                           |       |      |                  |
| STA 133+19 SB                  |                         |                            |                             |                         |                |               |          |  |                           |       | 61.2 |                  |
| STA 126+89 NB                  |                         |                            |                             |                         |                |               |          |  |                           |       | 61.2 |                  |
| ONLY                           |                         |                            |                             |                         |                |               |          |  |                           |       |      |                  |
| STA 94+76 SB                   |                         |                            |                             |                         |                |               |          |  |                           |       | 11.8 |                  |
| STA 92+00 NB                   |                         |                            |                             |                         |                |               |          |  |                           |       | 11.8 |                  |
| STA 56+55 NB                   |                         |                            |                             |                         |                |               |          |  |                           |       | 11.8 |                  |
| TURN ARROW                     |                         |                            |                             |                         |                |               |          |  |                           |       |      |                  |
| STA 94+45 SB                   |                         |                            |                             |                         |                |               |          |  |                           |       | 8.8  |                  |
| STA 92+30 NB                   |                         |                            |                             |                         |                |               |          |  |                           |       | 8.8  |                  |
| STA 56+85 NB                   |                         |                            |                             |                         |                |               |          |  |                           |       | 8.8  |                  |
| STA 56+05 NB                   |                         |                            |                             |                         |                |               |          |  |                           |       | 8.8  |                  |
| LOCATION TOTAL                 | 11206                   | 1240                       | 1380                        | 14102                   | 350            | 169           |          |  |                           |       | 193  |                  |





# SHORT-TERM PAVEMENT MARKING

| LOCATION<br>STATION TO STATION | SHORT-TERM<br>PAVEMENT<br>MARKING |             | REMARKS          |
|--------------------------------|-----------------------------------|-------------|------------------|
|                                | FOOT*                             | SQ. FT.     |                  |
| <b>FAS 724 (IL 37)</b>         |                                   |             |                  |
| STA 140+15 TO STA 130+04       | 200                               | 67          |                  |
| STA 130+04 TO STA 100+82       | 584                               | 195         |                  |
| STA 100+82 TO STA 97+50        | 84                                | 21          |                  |
| STA 97+50 TO STA 94+00         | 72                                | 24          |                  |
| STA 94+00 TO STA 91+80         | 48                                | 16          |                  |
| STA 91+80 TO STA 89+00         | 48                                | 16          |                  |
| STA 89+00 TO STA 69+00         | 400                               | 133         |                  |
| STA 69+00 TO STA 67+73         | 24                                | 8           |                  |
| STA 67+73 TO STA 58+75         | 352                               | 117         | I-64 INTERCHANGE |
| STA 58+75 TO STA 43+00         | 624                               | 208         | I-64 INTERCHANGE |
| STA 43+00 TO STA 35+00.00 BK   | 320                               | 107         | I-64 INTERCHANGE |
| <b>LOCATION TOTAL</b>          | <b>2736</b>                       | <b>912</b>  |                  |
| <b>FAP 2869 (IL 37)</b>        |                                   |             |                  |
| STA 120+65.50 AH TO STA 121+08 | 8                                 | 3           |                  |
| SN 041-0032                    |                                   |             |                  |
| STA 124+92 TO STA 136+43.29 BK | 232                               | 77          |                  |
| STA 137+39.62 AH TO STA 197+30 | 1200                              | 400         |                  |
| SN 041-0105                    |                                   |             |                  |
| STA 198+44 TO STA 205+56       | 144                               | 48          |                  |
| SN 041-0097                    |                                   |             |                  |
| STA 206+91 TO STA 362+76.86 BK | 3120                              | 1040        |                  |
| STA 362+64.47 AH TO STA 619+05 | 5128                              | 1709        |                  |
| STA 619+05 TO STA 622+75       | 72                                | 24          |                  |
| STA 622+75 TO STA 637+13       | 288                               | 96          |                  |
| STA 637+13 TO STA 642+53       | 112                               | 37          |                  |
| STA 642+53 TO STA 648+34       | 120                               | 40          |                  |
| STA 648+34 TO STA 652+50       | 80                                | 27          |                  |
| <b>LOCATION TOTAL</b>          | <b>10504</b>                      | <b>3501</b> |                  |
| <b>PROJECT TOTAL</b>           | <b>13240</b>                      | <b>4413</b> |                  |

\*DENOTES TWO APPLICATIONS

# RAISED REFLECTIVE PAVEMENT MARKERS

| LOCATION<br>STATION TO STATION | RAISED REFLECTIVE<br>PAVEMENT MARKERS |                          |                          |                          | RAISED REFLECTIVE<br>PAVEMENT<br>MARKER REMOVAL | REMARKS |
|--------------------------------|---------------------------------------|--------------------------|--------------------------|--------------------------|---|---------|
|                                | 2-WAY<br>AMBER<br>EACH                |                          | 1-WAY<br>CRYSTAL<br>EACH |                          |   |         |
|                                | 2-WAY<br>AMBER<br>EACH                | 1-WAY<br>CRYSTAL<br>EACH | 2-WAY<br>AMBER<br>EACH   | 1-WAY<br>CRYSTAL<br>EACH |   |         |
| <b>FAS 724 (IL 37)</b>         |                                       |                          |                          |                          |   |         |
| STA 140+15 TO STA 100+82       | 53                                    |                          |                          |                          | 53  |         |
| STA 100+82 TO STA 95+55        |                                       | 24                       |                          |                          | 24  |         |
| STA 95+55 TO STA 93+75         | 8                                     |                          | 4                        |                          | 12  |         |
| STA 93+00 TO STA 89+88         |                                       |                          | 16                       | 4                        | 20  |         |
| STA 89+88 TO STA 84+75         | 40                                    |                          |                          |                          | 40  |         |
| STA 84+75 TO STA 83+85         |                                       | 4                        |                          |                          | 4   |         |
| STA 83+85 TO STA 57+85         |                                       |                          | 16                       |                          | 16  |         |
| STA 57+85 TO STA 39+31         |                                       |                          | 53                       |                          | 53  |         |
| STA 39+31 TO STA 36+91         |                                       | 12                       |                          |                          | 12  |         |
| STA 36+91 TO STA 35+00 BK      | 7                                     |                          |                          |                          | 7   |         |
| <b>LOCATION TOTAL</b>          | <b>108</b>                            |                          | <b>56</b>                | <b>77</b>                | <b>241</b>                                      |         |
| <b>FAP 2869 (IL 37)</b>        |                                       |                          |                          |                          |   |         |
| STA 120+66 AH TO STA 136+43 BK | 20                                    |                          |                          |                          | 20  |         |
| STA 137+40 AH TO STA 362+77 BK | 282                                   |                          |                          |                          | 282   |         |
| STA 362+64 AH TO STA 611+55    | 315                                   |                          |                          |                          | 315   |         |
| STA 611+55 TO STA 617+83       |                                       | 32                       |                          |                          | 32  |         |
| STA 618+58 TO STA 620+16       | 8                                     |                          |                          | 4                        | 12  |         |
| STA 620+16 TO STA 624+91       |                                       | 24                       |                          | 4                        | 28  |         |
| STA 625+44 TO STA 627+29       | 8                                     |                          |                          | 4                        | 12  |         |
| STA 627+29 TO STA 634+84       |                                       | 38                       |                          |                          | 38  |         |
| STA 634+84 TO STA 648+34       | 21                                    |                          |                          |                          | 21  |         |
| STA 648+34 TO STA 652+50       | 5                                     |                          |                          |                          | 5   |         |
| <b>LOCATION TOTAL</b>          | <b>659</b>                            |                          | <b>94</b>                | <b>12</b>                | <b>765</b>                                      |         |
| <b>PROJECT TOTALS</b>          |                                       |                          | <b>1006</b>              |                          | <b>1006</b>                                     |         |

# IL 37 EXISTING ALIGNMENT

IL 37 (FAP 724/FAS 2869)  
(41-8.4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 27 OF 72

PI Sta 145+16.97

PI Sta 143+99.94

PC Sta 142+82.91

EXIST. CURVE N3  
PI STA. = 143+99.94  
 $\Delta = 0^\circ 37' 27''$  (RT)  
D = 0° 16' 00"  
R = 21,485.69'  
T = 117.03'  
L = 234.06'  
E = 0.32'  
P.C. STA. = 142+82.91  
P.T. STA. = 145+16.97

PROJECT BEGINS  
STA 140+15.00

140

BLED SOE ST.  
STA 139+08.25

PAULINE ST.  
STA 133+58.25

UNION PACIFIC RR

130

MAXEY AVE.  
STA 128+07

--- STA 130+04.55

OLIN LN.  
STA 124+42.32

DOGWOOD DR.  
STA 121+62.30

120

NOTE:  
SIDE ROAD STATIONS  
ARE APPROXIMATE.

110

FOUNTAIN PL.  
STA 110+89.87

MOCKINGBIRD LA.  
STA 107+63.21



IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 28 OF 72

EXIST. CURVE N2  
PI STA. = 96+36.11  
 $\Delta = 6^\circ 15' 10''$  (RT)  
D =  $0^\circ 47' 37''$   
R = 7,219.27'  
T = 394.32'  
L = 787.85'  
E = 10.76'  
P.C. STA. = 92+41.79  
P.T. STA. = 100+29.64

PT Sta 100+29.64

WHITE OAK ST.

STA 100+76

PI Sta 96+36.11

WOODLAND DR.

STA 93+41.94

PC Sta 92+41.79

STR. 041-0031  
STA 85+67.85

**NOTE:**  
SIDE ROAD STATIONS  
ARE APPROXIMATE.

BETHEL  
CEMETARY

W BETHEL RD.

E BETHEL RD.

STA 73+43.45

EXIST. CURVE N1  
PI STA. = 58+39.87  
 $\Delta = 14^\circ 01' 43''$  (RT)  
D =  $1^\circ 30' 00''$   
R = 3,819.76'  
T = 469.98'  
L = 935.25'  
E = 28.80'  
P.C. STA. = 53+69.89  
P.T. STA. = 63+05.14

PT Sta 63+05.14



SHEET 2 OF 15

IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 29 OF 72

IL 37 TO 164 EB  
 STA 57+83.40  
 PI Sta 58+39.87  
 164 WB TO IL 37  
 STA 57+09.40  
 PC Sta 53+69.89

**NOTE:**  
 SIDE ROAD STATIONS  
 ARE APPROXIMATE.

IL 37 TO 164 WB  
 STA 44+75  
 164 EB TO IL 37  
 STA 44+00

E. BUNNY FARM RD.  
 STA 38+85

POT Sta 35+00.00  
 POT Sta 120+65.50  
 STR. 041-0032  
 STA 123+00

PC Sta 126+07.12

EXIST. CURVE 4  
 PI STA. = 131+34.72  
 $\Delta = 26^\circ 36' 34''$  (RT)  
 $D = 2^\circ 34' 05''$   
 $R = 2,231.08'$   
 $T = 527.60'$   
 $L = 1,036.17'$   
 $E = 61.53'$   
 S.E. RUN = 0.05%  
 P.C. STA. = 126+07.12  
 P.T. STA. = 136+43.29

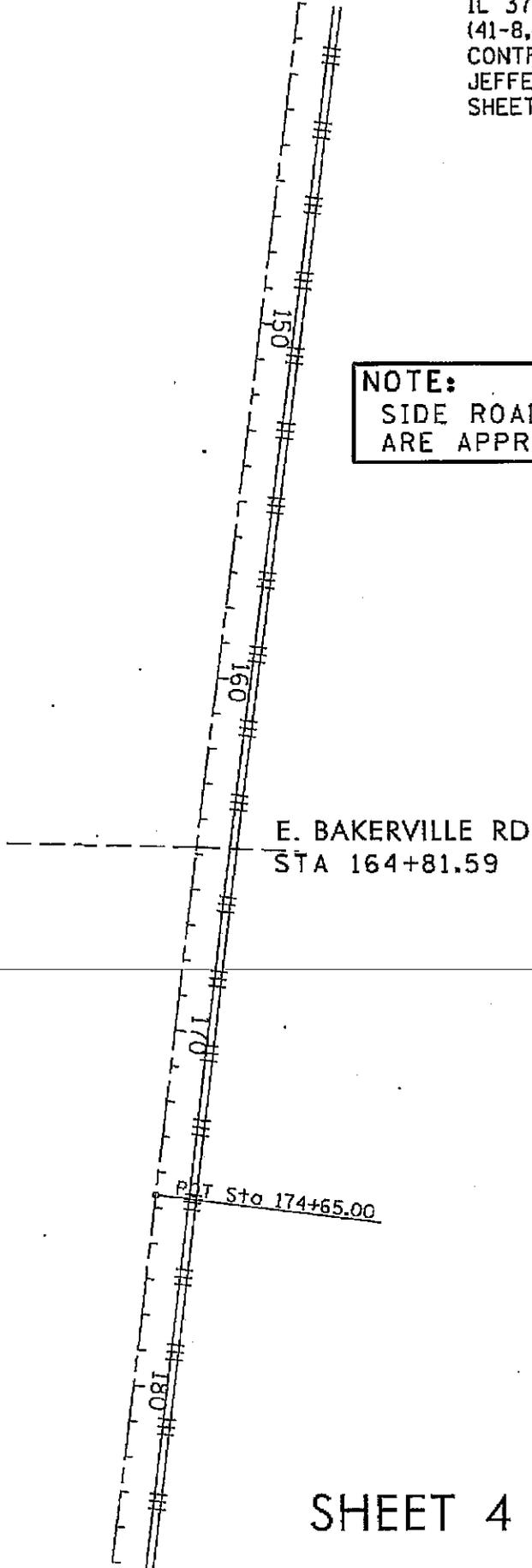
EQUATION:  
 $\text{Sta } 136+43.29 \text{ BK} = \text{PT Sta } 136+43.29$   
 $\text{Sta } 137+39.62 \text{ AH}$

VAN DOREN LA  
 STA 138+00



IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 30 OF 72

**NOTE:**  
SIDE ROAD STATIONS  
ARE APPROXIMATE.



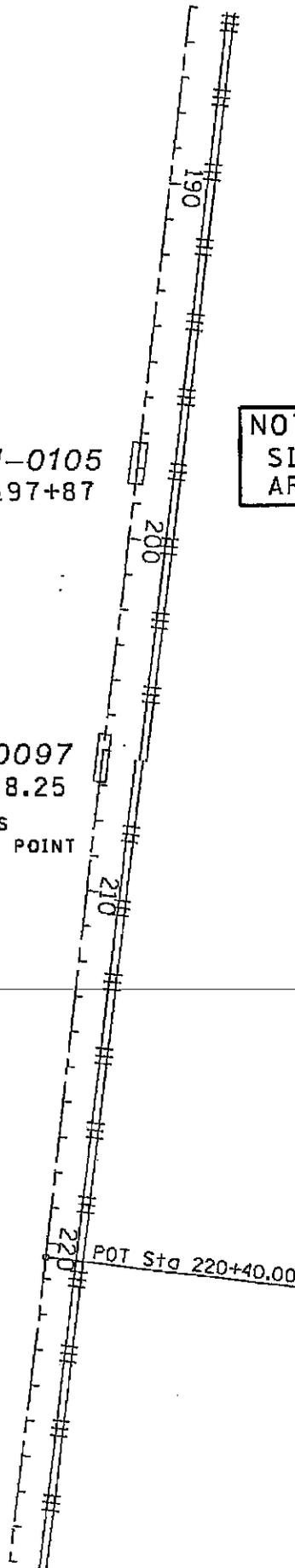
SHEET 4 OF 15

IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 31 OF 72

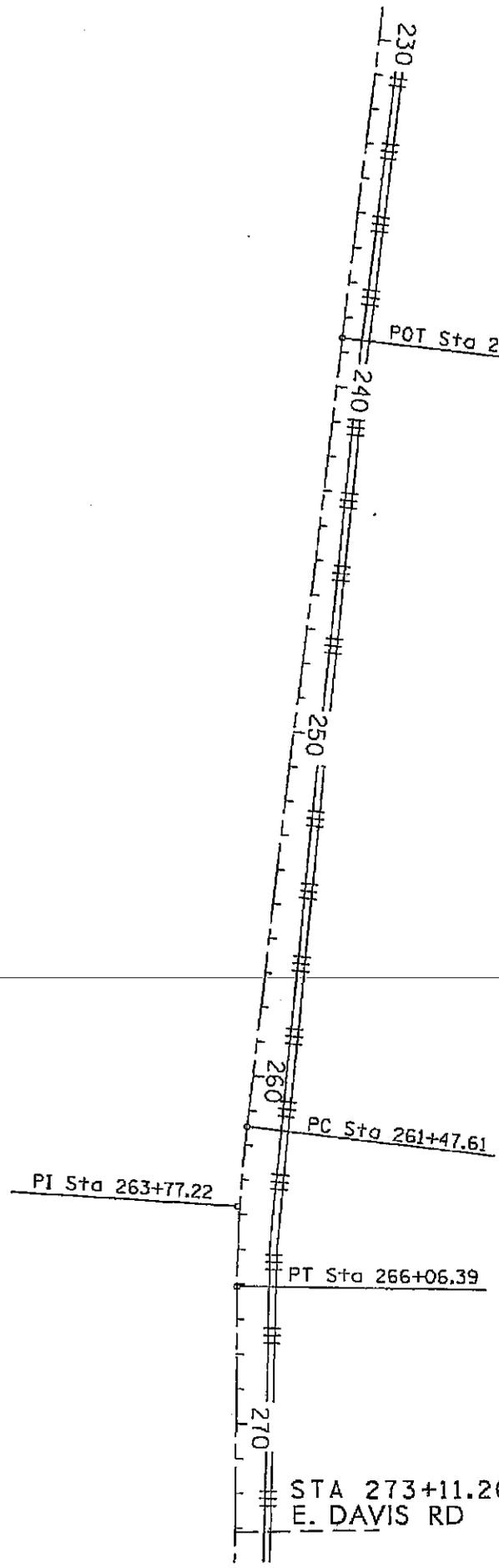
STR. 041-0105  
STA 197+87

**NOTE:**  
SIDE ROAD STATIONS  
ARE APPROXIMATE.

STR. 041-0097  
\* STA 206+48.25  
\* DO NOT USE THIS  
STRUCTURE AS A POINT  
OF REFERENCE



IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 32 OF 72



**NOTE:**  
SIDE ROAD STATIONS  
ARE APPROXIMATE.

EXIST. CURVE 5  
PI STA. = 263+77.22  
 $\Delta = 6^\circ 08' 52''$  (LT)  
 $D = 1^\circ 20' 24''$   
 $R = 4,275.69'$   
 $T = 229.61'$   
 $L = 458.78'$   
 $E = 6.16'$   
S.E. RUN = 0.03%  
P.C. STA. = 261+47.61  
P.T. STA. = 266+06.39



STA 273+11.26  
E. DAVIS RD

SHEET 6 OF 15

IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 33 OF 72

STR. 041-0035  
STA 286+61

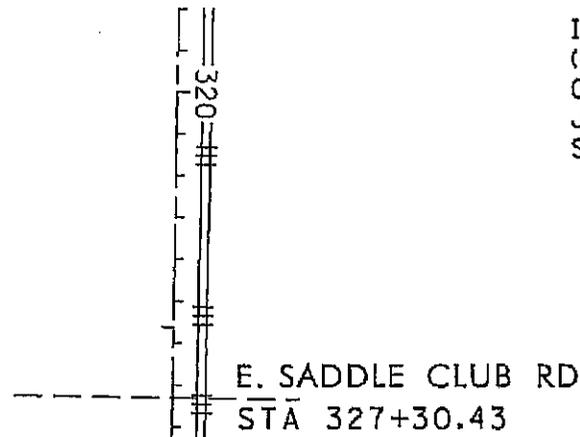
POT Sta 292+00.00

**NOTE:**  
SIDE ROAD STATIONS  
ARE APPROXIMATE.

E. WEBER RD  
STA 300+33.40

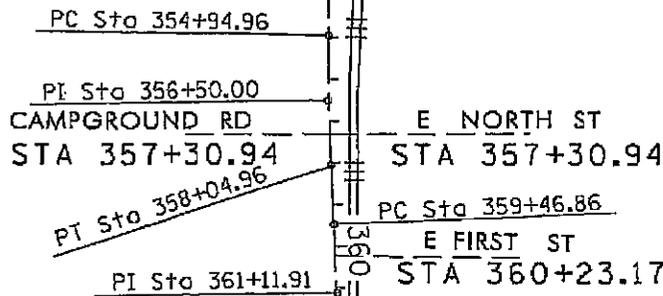


SHEET 7 OF 15



**NOTE:**  
 SIDE ROAD STATIONS  
 ARE APPROXIMATE.

EXIST. CURVE 6  
 PI STA. = 356+50.00  
 $\Delta = 3^\circ 27' 02''$  (LT)  
 $D = 1^\circ 06' 47''$   
 $R = 5,147.46'$   
 $T = 155.05'$   
 $L = 310.00'$   
 $E = 2.33'$   
 S.E. RUN = 0.04%  
 P.C. STA. = 354+94.96  
 P.T. STA. = 358+04.96



EXIST. CURVE 7  
 PI STA. = 361+11.91  
 $\Delta = 3^\circ 30' 15''$  (RT)  
 $D = 1^\circ 03' 43''$   
 $R = 5,395.94'$   
 $T = 165.05'$   
 $L = 330.00'$   
 $E = 2.52'$   
 S.E. RUN = 0.04%  
 P.C. STA. = 359+46.86  
 P.T. STA. = 362+76.86



SHORT ST. ---  
STA 362+20.58

PT Sta 362+76.86

IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 35 OF 72

EQUATION:  
Sta 362+76.86 BK =  
Sta 362+64.47 AH

E SECOND ST  
STA 363+85.57

W THIRD ST  
STA 367+84.42

E THIRD ST

W FOURTH ST  
STA 371+44.47

E FOURTH

W FIFTH ST  
STA 375+06.91

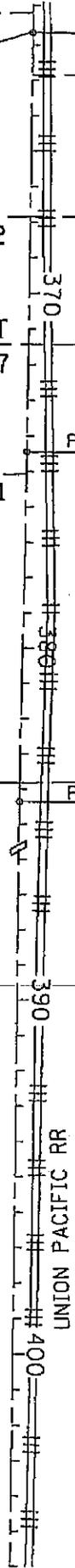
POT Sta 374+44.48

NOTE:  
SIDE ROAD STATIONS  
ARE APPROXIMATE.

W PALMER LN  
STA 383+83.44

E LANG LN  
POT Sta 384+35.68

STR. 041-0092  
STA 385+67



UNION PACIFIC RR



IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1:(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY.  
SHEET 36 OF 72

STR. 041-0095  
STA 408+48.4

POT Sta 411+00.02



NOTE:  
SIDE ROAD STATIONS  
ARE APPROXIMATE.

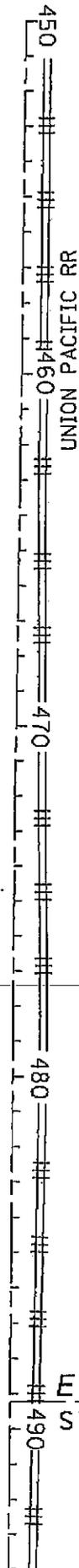
STR. 041-0036  
STA 429+51.3

E ADAMS ST  
STA 436+39.65

POT Sta 439+37.92



IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 37 OF 72



**NOTE:**  
SIDE ROAD STATIONS  
ARE APPROXIMATE.

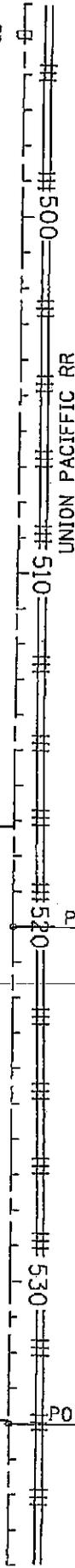
E MIDNIGHT RD  
STA 489+20.89



SHEET 11 OF 15

STR. 041-0093  
STA 494+87.4

IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1:(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 38 OF 72



**NOTE:**  
SIDE ROAD STATIONS  
ARE APPROXIMATE.

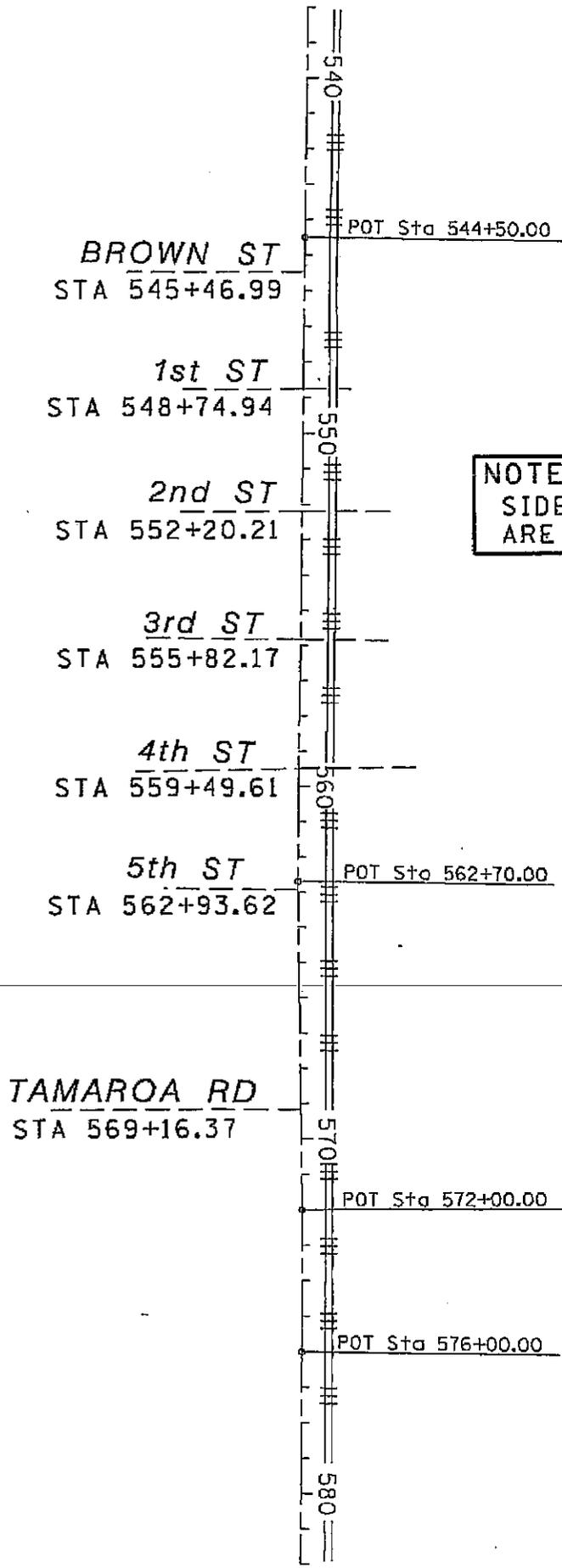
EVERGREEN DR  
STA 517+16.37

POT Sta 520+00.00

HAYSE  
STA 533+90.93

POT Sta 534+00.00



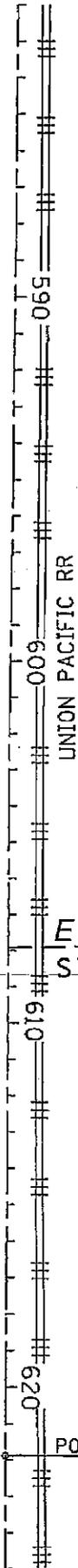


**NOTE:**  
SIDE ROAD STATIONS  
ARE APPROXIMATE.



IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 40 OF 72

NOTE:  
SIDE ROAD STATIONS  
ARE APPROXIMATE.



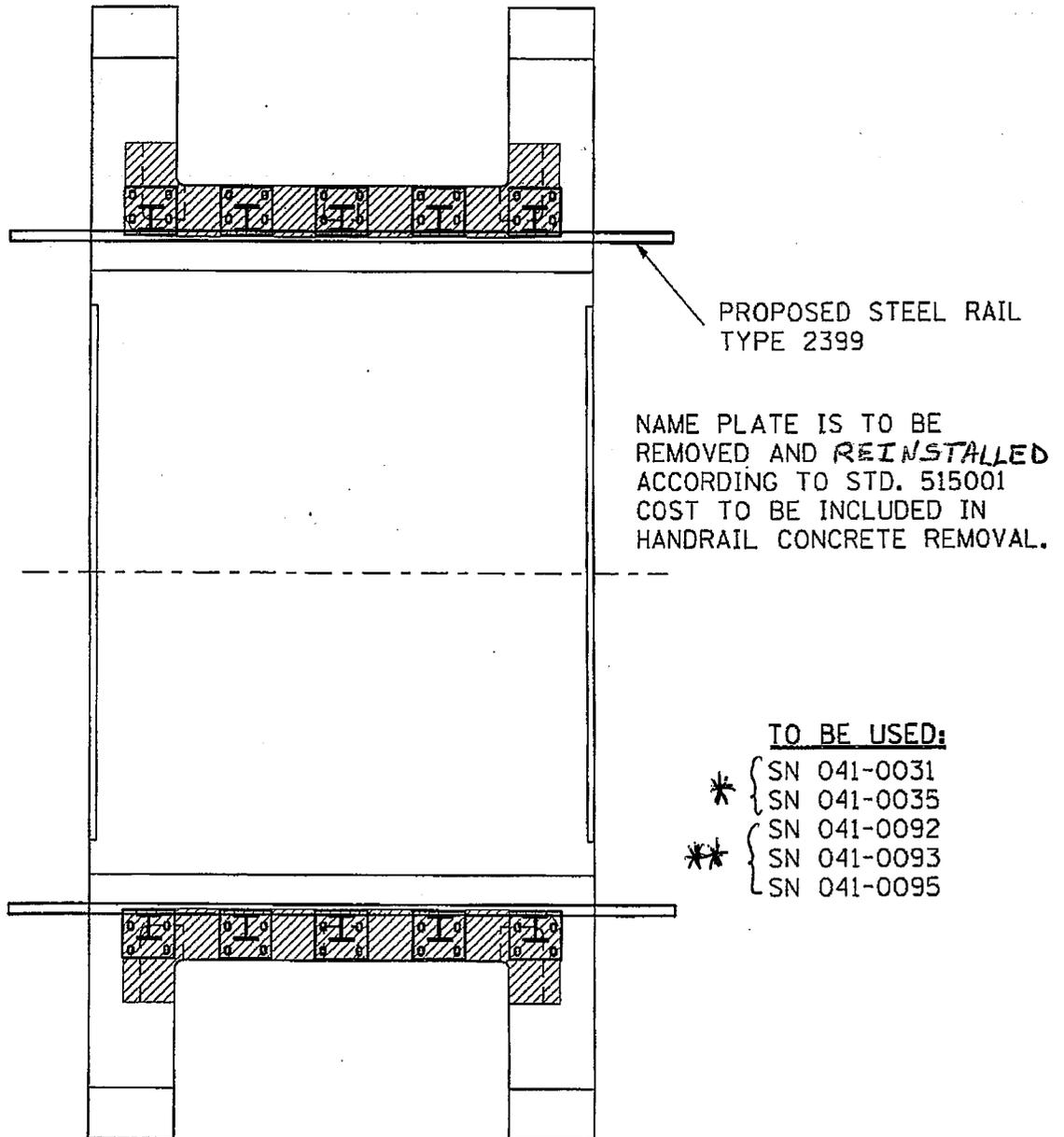
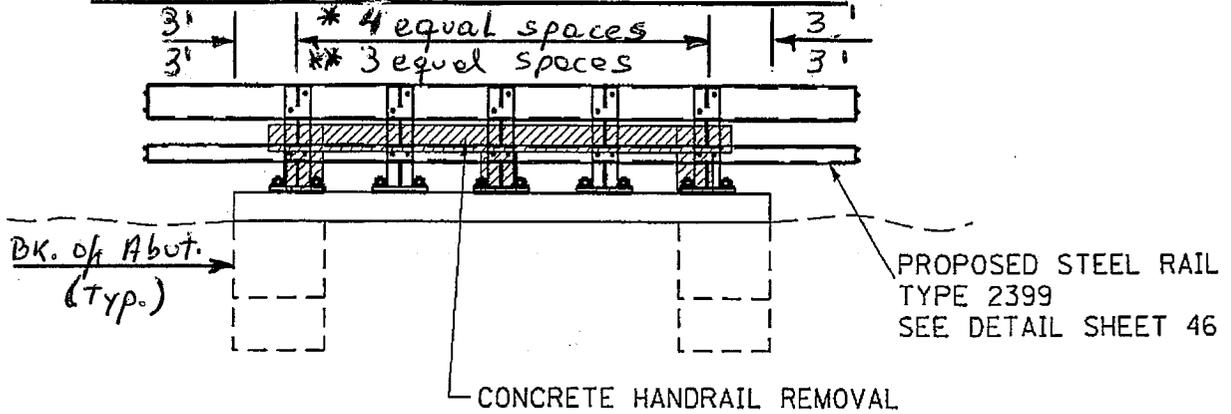
E. WHEATON RD.  
STA 607+91.62

POT Sta 621+87.80



SHEET 14 OF 15

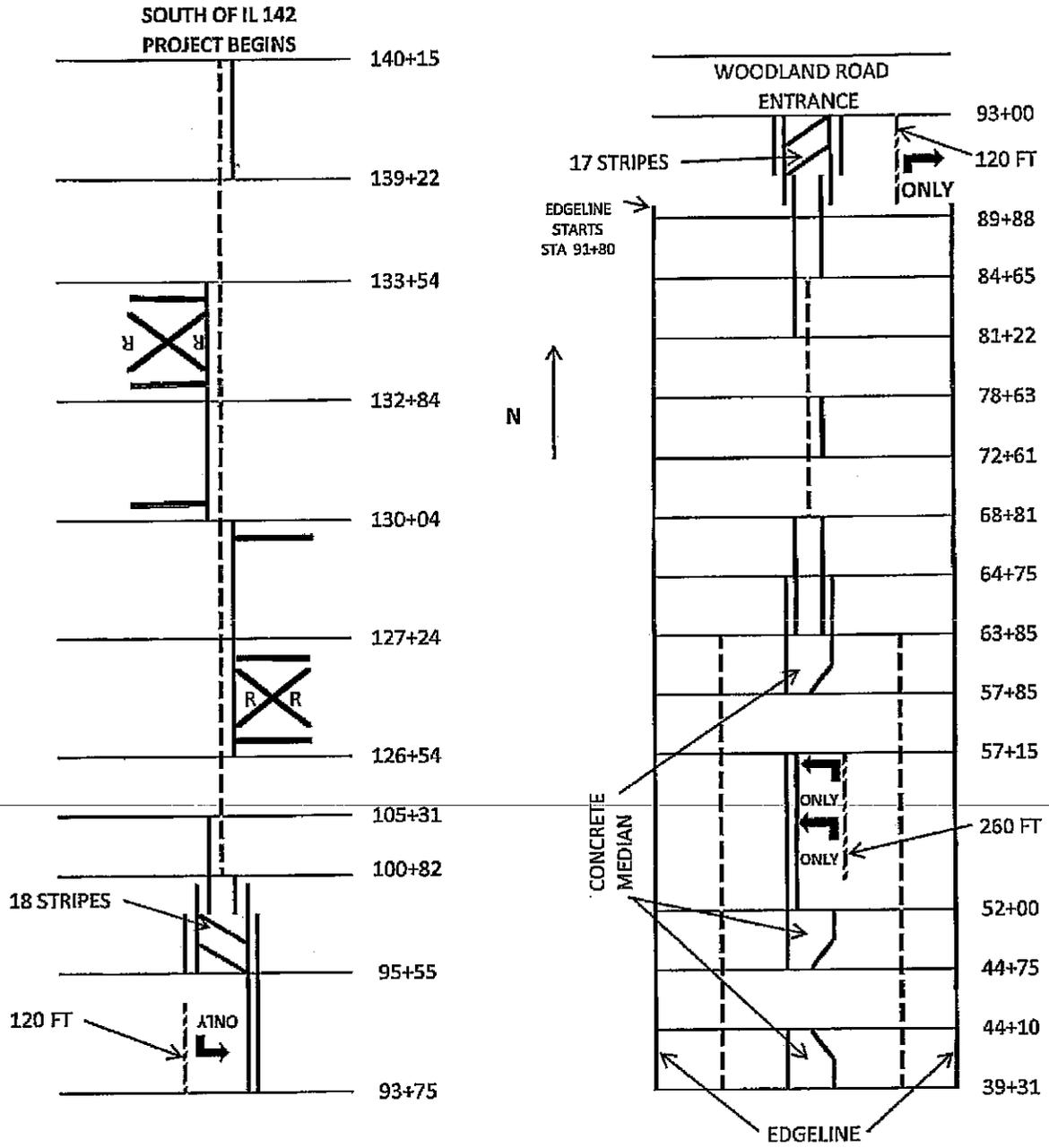
**EXISTING CONCRETE RAIL RETROFIT**



**TO BE USED:**

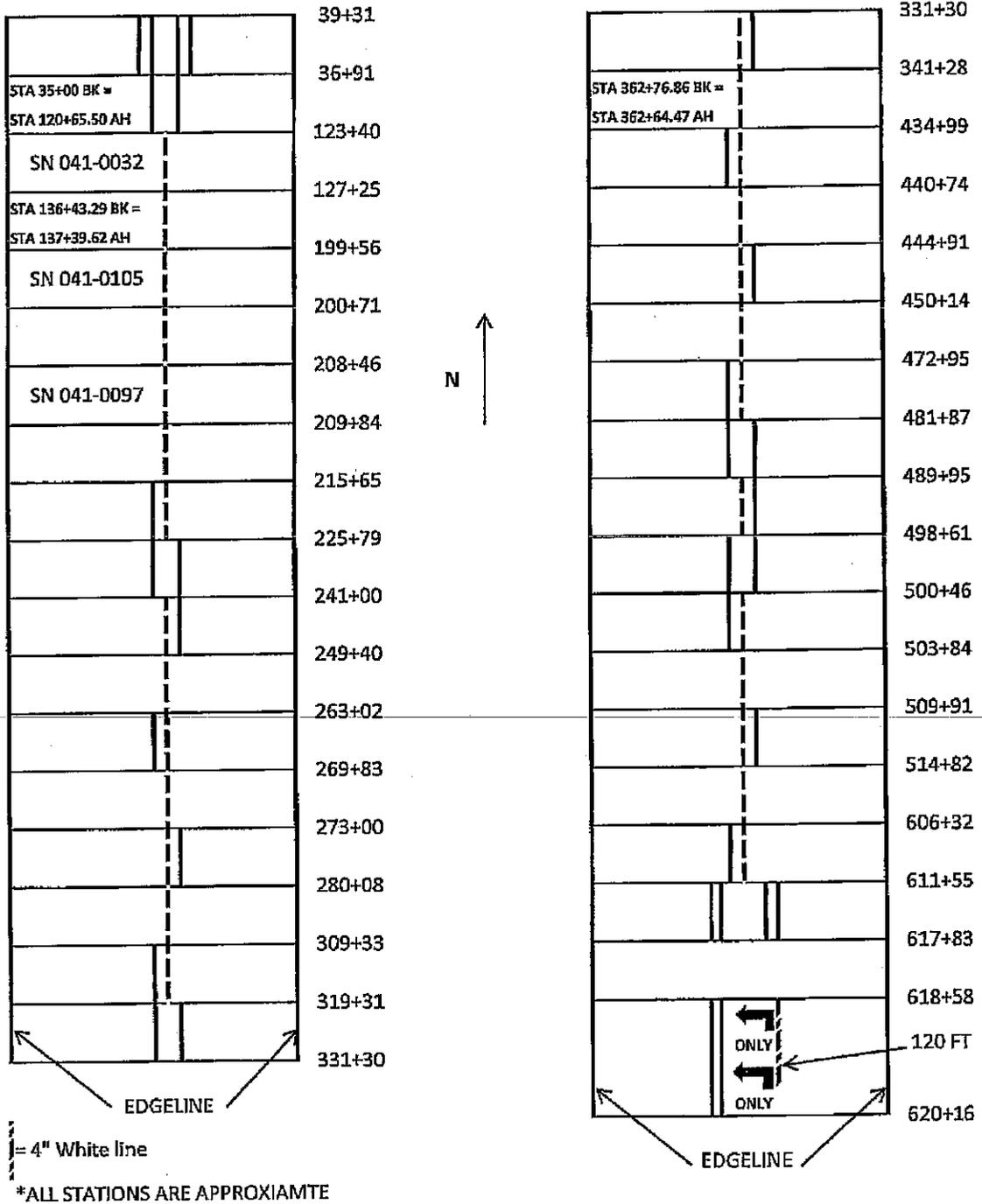
- \* { SN 041-0031
- { SN 041-0035
- \*\* { SN 041-0092
- { SN 041-0093
- { SN 041-0095

# STRIPING DIAGRAM

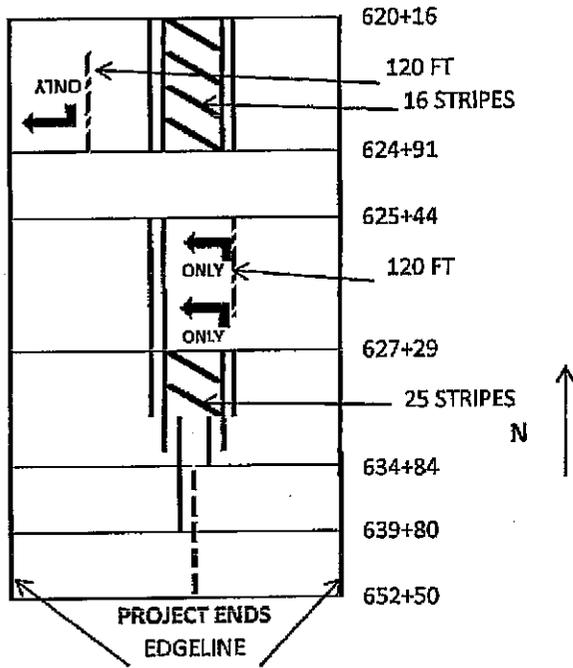


— = 4" White line  
 \*ALL STATIONS ARE APPROXIMATE

# STRIPING DIAGRAM



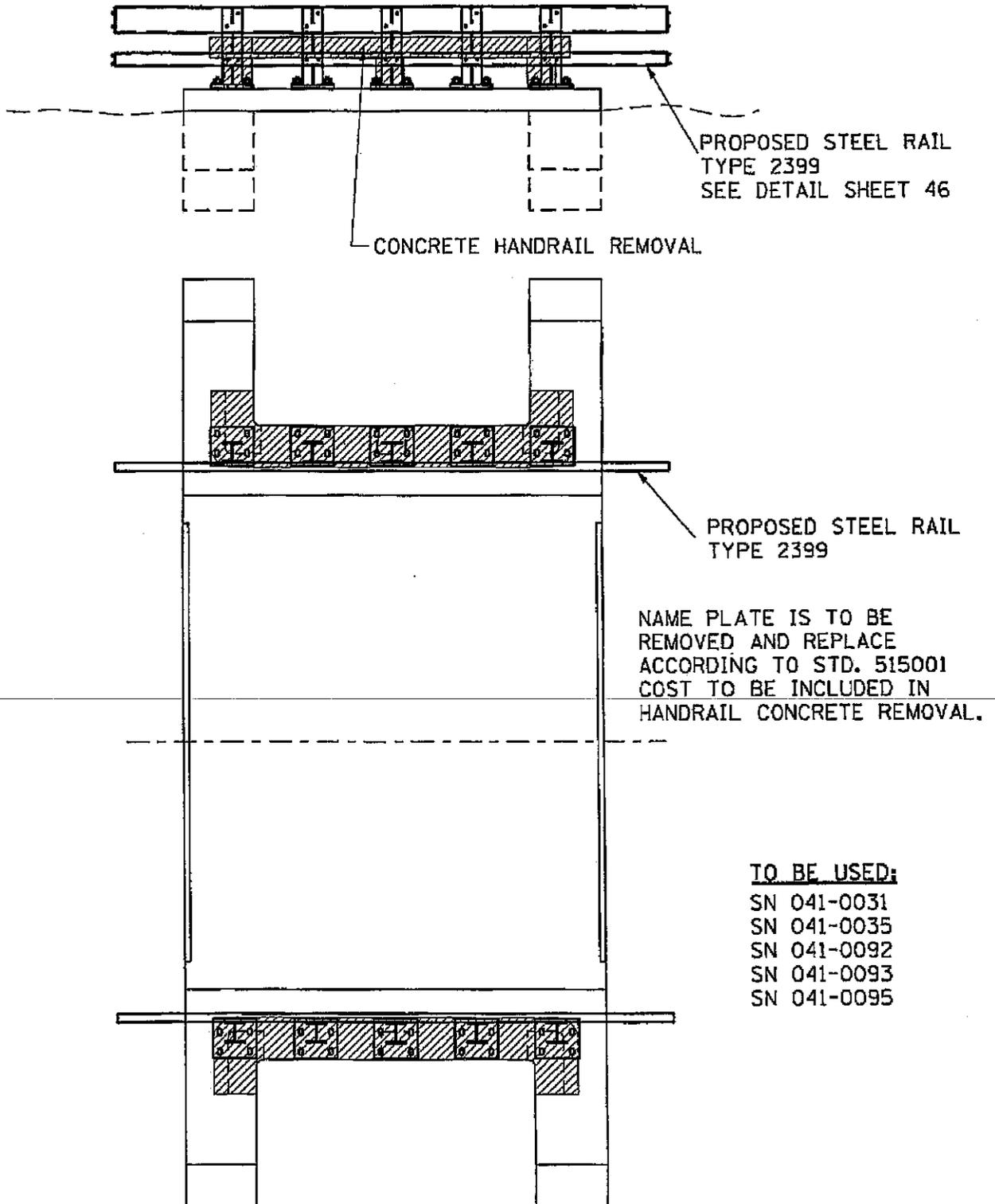
# STRIPING DIAGRAM



--- = 4" White line

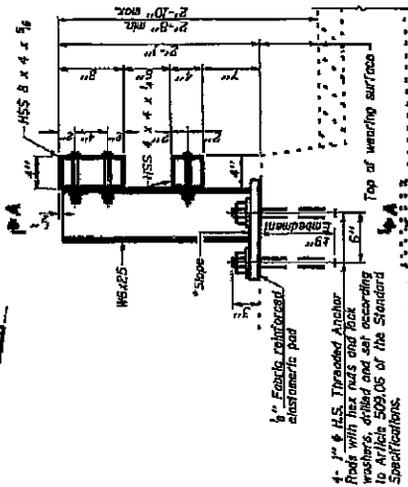
\*ALL STATIONS ARE APPROXIMATE

## EXISTING CONCRETE RAIL RETROFIT



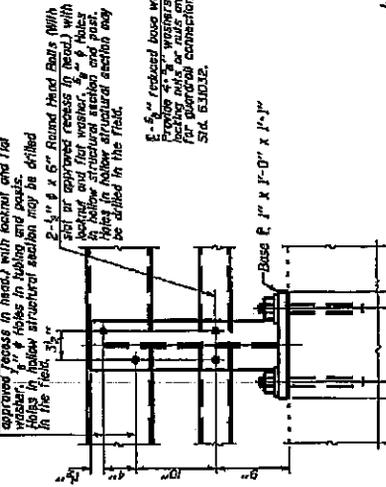
**STEEL RAILING, TYPE 2399  
 STRUCTURE NO.**  
 (AS REFERENCED ON DETAIL SHEET 45)

\*Cut bottom end of post to curb slope



4- 1/2"  $\phi$  H.S. Threaded Anchor Posts with hex nuts and flat washers, drilled and set according to Article 508.05 of the Standard Specifications.

**SECTION AT RAIL POST**



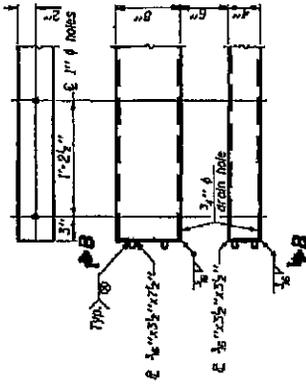
**SECTION A-A**

2- 1/2"  $\phi$  x 6" Round Head Bolts with split or cap washers, 1/2"  $\phi$  Holes in flange and post. Holes in hollow structural section may be drilled in the field.

2- 1/2"  $\phi$  x 6" Round Head Bolts (with Locknut and Flat Washer, 1/2"  $\phi$  Holes in hollow structural section and post. Holes in hollow structural section may be drilled in the field.

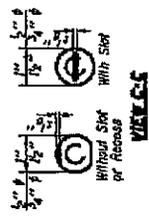
Base 1/2" x 1'-0" x 1'-1/2"

1/2"  $\phi$  reduced base webbed studs Locknut with flat washer and low nuts for general connection shown on STD. 533012.

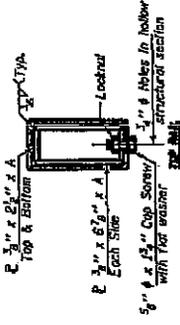


**END OF RAIL DETAILS**

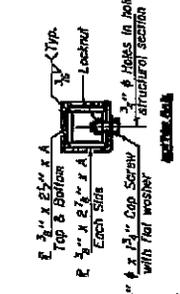
**Notes:**  
 All field drilled holes shall be covered with an approved zinc rich paint before erection.  
 Posts shall not be located closer than 1'-3" to an existing bridge expansion joint or end of bridge.  
 Steel Bridge Rail expansion joint shall be provided between any two (2) posts which span a bridge expansion joint. Balls located at expansion joint shall be provided with locknuts and shall be tightened only to a point that will allow rolling movement.  
 Provide one 1/2" and two 1/4" steel shims for 25% of the posts.  
 Shims shall be similar to base plates in size and holes.  
 All steel rail elements shall be galvanized according to Article 508.05 of the Standard Specifications.



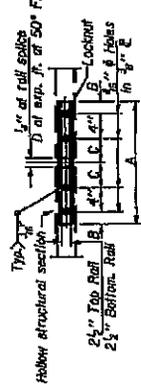
**DETAIL OF 1/2"  $\phi$  & 3/4"  $\phi$  ROUND HEAD BOLTS**



**SECTIONS AT RAIL SPLICE**



**BASE PLATE DETAIL**



**RAIL SPLICE CONNECTION AT EXPANSION JOINT**

**PLAN-BOTTOM SPLICE R TYPICAL**

**SPLICE DIMENSIONS**

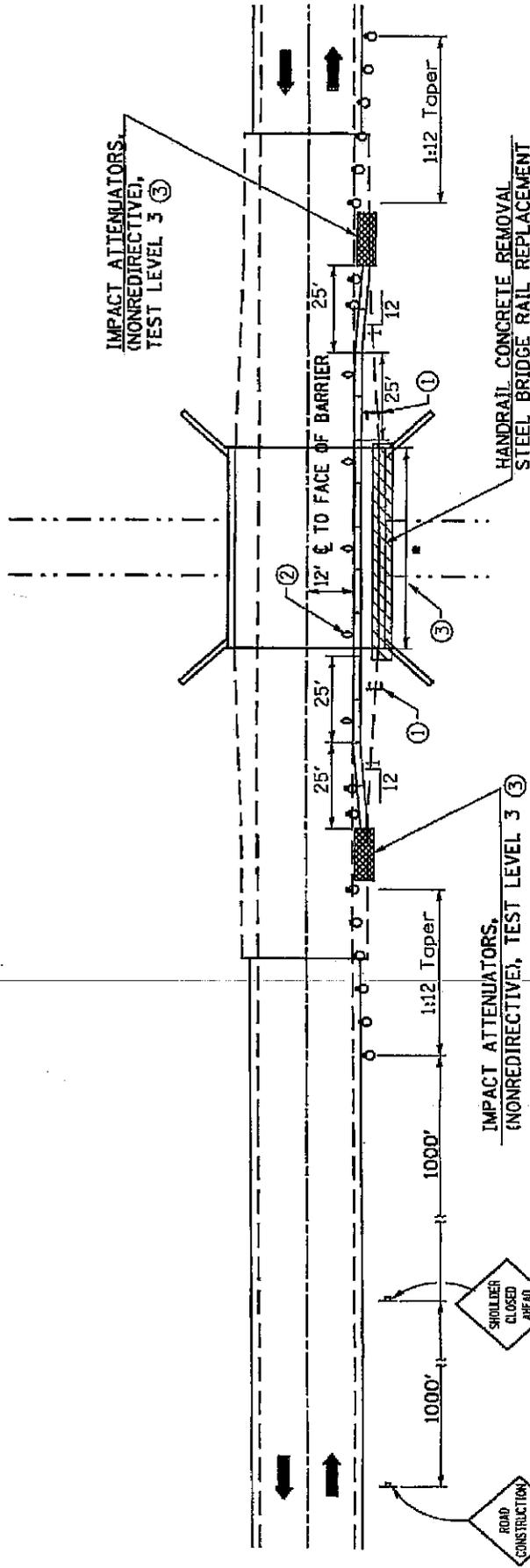
|     | D      | A     | B        | C        | E      |
|-----|--------|-------|----------|----------|--------|
| 1   | 2 1/2" | 1'-8" | 2"       | 4"       | 2 1/2" |
| 2   | 2 1/2" | 2'-0" | 2 1/2"   | 5 1/2"   | 3 1/2" |
| 3   | 2 1/2" | 2'-0" | 3 1/2"   | 6 1/2"   | 9"     |
| 4   | 2 1/2" | 2'-0" | 4 1/2"   | 7 1/2"   | 11"    |
| 5   | 2 1/2" | 2'-0" | 5 1/2"   | 8 1/2"   | 13"    |
| 6   | 2 1/2" | 2'-0" | 6 1/2"   | 9 1/2"   | 15"    |
| 7   | 2 1/2" | 2'-0" | 7 1/2"   | 10 1/2"  | 17"    |
| 8   | 2 1/2" | 2'-0" | 8 1/2"   | 11 1/2"  | 19"    |
| 9   | 2 1/2" | 2'-0" | 9 1/2"   | 12 1/2"  | 21"    |
| 10  | 2 1/2" | 2'-0" | 10 1/2"  | 13 1/2"  | 23"    |
| 11  | 2 1/2" | 2'-0" | 11 1/2"  | 14 1/2"  | 25"    |
| 12  | 2 1/2" | 2'-0" | 12 1/2"  | 15 1/2"  | 27"    |
| 13  | 2 1/2" | 2'-0" | 13 1/2"  | 16 1/2"  | 29"    |
| 14  | 2 1/2" | 2'-0" | 14 1/2"  | 17 1/2"  | 31"    |
| 15  | 2 1/2" | 2'-0" | 15 1/2"  | 18 1/2"  | 33"    |
| 16  | 2 1/2" | 2'-0" | 16 1/2"  | 19 1/2"  | 35"    |
| 17  | 2 1/2" | 2'-0" | 17 1/2"  | 20 1/2"  | 37"    |
| 18  | 2 1/2" | 2'-0" | 18 1/2"  | 21 1/2"  | 39"    |
| 19  | 2 1/2" | 2'-0" | 19 1/2"  | 22 1/2"  | 41"    |
| 20  | 2 1/2" | 2'-0" | 20 1/2"  | 23 1/2"  | 43"    |
| 21  | 2 1/2" | 2'-0" | 21 1/2"  | 24 1/2"  | 45"    |
| 22  | 2 1/2" | 2'-0" | 22 1/2"  | 25 1/2"  | 47"    |
| 23  | 2 1/2" | 2'-0" | 23 1/2"  | 26 1/2"  | 49"    |
| 24  | 2 1/2" | 2'-0" | 24 1/2"  | 27 1/2"  | 51"    |
| 25  | 2 1/2" | 2'-0" | 25 1/2"  | 28 1/2"  | 53"    |
| 26  | 2 1/2" | 2'-0" | 26 1/2"  | 29 1/2"  | 55"    |
| 27  | 2 1/2" | 2'-0" | 27 1/2"  | 30 1/2"  | 57"    |
| 28  | 2 1/2" | 2'-0" | 28 1/2"  | 31 1/2"  | 59"    |
| 29  | 2 1/2" | 2'-0" | 29 1/2"  | 32 1/2"  | 61"    |
| 30  | 2 1/2" | 2'-0" | 30 1/2"  | 33 1/2"  | 63"    |
| 31  | 2 1/2" | 2'-0" | 31 1/2"  | 34 1/2"  | 65"    |
| 32  | 2 1/2" | 2'-0" | 32 1/2"  | 35 1/2"  | 67"    |
| 33  | 2 1/2" | 2'-0" | 33 1/2"  | 36 1/2"  | 69"    |
| 34  | 2 1/2" | 2'-0" | 34 1/2"  | 37 1/2"  | 71"    |
| 35  | 2 1/2" | 2'-0" | 35 1/2"  | 38 1/2"  | 73"    |
| 36  | 2 1/2" | 2'-0" | 36 1/2"  | 39 1/2"  | 75"    |
| 37  | 2 1/2" | 2'-0" | 37 1/2"  | 40 1/2"  | 77"    |
| 38  | 2 1/2" | 2'-0" | 38 1/2"  | 41 1/2"  | 79"    |
| 39  | 2 1/2" | 2'-0" | 39 1/2"  | 42 1/2"  | 81"    |
| 40  | 2 1/2" | 2'-0" | 40 1/2"  | 43 1/2"  | 83"    |
| 41  | 2 1/2" | 2'-0" | 41 1/2"  | 44 1/2"  | 85"    |
| 42  | 2 1/2" | 2'-0" | 42 1/2"  | 45 1/2"  | 87"    |
| 43  | 2 1/2" | 2'-0" | 43 1/2"  | 46 1/2"  | 89"    |
| 44  | 2 1/2" | 2'-0" | 44 1/2"  | 47 1/2"  | 91"    |
| 45  | 2 1/2" | 2'-0" | 45 1/2"  | 48 1/2"  | 93"    |
| 46  | 2 1/2" | 2'-0" | 46 1/2"  | 49 1/2"  | 95"    |
| 47  | 2 1/2" | 2'-0" | 47 1/2"  | 50 1/2"  | 97"    |
| 48  | 2 1/2" | 2'-0" | 48 1/2"  | 51 1/2"  | 99"    |
| 49  | 2 1/2" | 2'-0" | 49 1/2"  | 52 1/2"  | 101"   |
| 50  | 2 1/2" | 2'-0" | 50 1/2"  | 53 1/2"  | 103"   |
| 51  | 2 1/2" | 2'-0" | 51 1/2"  | 54 1/2"  | 105"   |
| 52  | 2 1/2" | 2'-0" | 52 1/2"  | 55 1/2"  | 107"   |
| 53  | 2 1/2" | 2'-0" | 53 1/2"  | 56 1/2"  | 109"   |
| 54  | 2 1/2" | 2'-0" | 54 1/2"  | 57 1/2"  | 111"   |
| 55  | 2 1/2" | 2'-0" | 55 1/2"  | 58 1/2"  | 113"   |
| 56  | 2 1/2" | 2'-0" | 56 1/2"  | 59 1/2"  | 115"   |
| 57  | 2 1/2" | 2'-0" | 57 1/2"  | 60 1/2"  | 117"   |
| 58  | 2 1/2" | 2'-0" | 58 1/2"  | 61 1/2"  | 119"   |
| 59  | 2 1/2" | 2'-0" | 59 1/2"  | 62 1/2"  | 121"   |
| 60  | 2 1/2" | 2'-0" | 60 1/2"  | 63 1/2"  | 123"   |
| 61  | 2 1/2" | 2'-0" | 61 1/2"  | 64 1/2"  | 125"   |
| 62  | 2 1/2" | 2'-0" | 62 1/2"  | 65 1/2"  | 127"   |
| 63  | 2 1/2" | 2'-0" | 63 1/2"  | 66 1/2"  | 129"   |
| 64  | 2 1/2" | 2'-0" | 64 1/2"  | 67 1/2"  | 131"   |
| 65  | 2 1/2" | 2'-0" | 65 1/2"  | 68 1/2"  | 133"   |
| 66  | 2 1/2" | 2'-0" | 66 1/2"  | 69 1/2"  | 135"   |
| 67  | 2 1/2" | 2'-0" | 67 1/2"  | 70 1/2"  | 137"   |
| 68  | 2 1/2" | 2'-0" | 68 1/2"  | 71 1/2"  | 139"   |
| 69  | 2 1/2" | 2'-0" | 69 1/2"  | 72 1/2"  | 141"   |
| 70  | 2 1/2" | 2'-0" | 70 1/2"  | 73 1/2"  | 143"   |
| 71  | 2 1/2" | 2'-0" | 71 1/2"  | 74 1/2"  | 145"   |
| 72  | 2 1/2" | 2'-0" | 72 1/2"  | 75 1/2"  | 147"   |
| 73  | 2 1/2" | 2'-0" | 73 1/2"  | 76 1/2"  | 149"   |
| 74  | 2 1/2" | 2'-0" | 74 1/2"  | 77 1/2"  | 151"   |
| 75  | 2 1/2" | 2'-0" | 75 1/2"  | 78 1/2"  | 153"   |
| 76  | 2 1/2" | 2'-0" | 76 1/2"  | 79 1/2"  | 155"   |
| 77  | 2 1/2" | 2'-0" | 77 1/2"  | 80 1/2"  | 157"   |
| 78  | 2 1/2" | 2'-0" | 78 1/2"  | 81 1/2"  | 159"   |
| 79  | 2 1/2" | 2'-0" | 79 1/2"  | 82 1/2"  | 161"   |
| 80  | 2 1/2" | 2'-0" | 80 1/2"  | 83 1/2"  | 163"   |
| 81  | 2 1/2" | 2'-0" | 81 1/2"  | 84 1/2"  | 165"   |
| 82  | 2 1/2" | 2'-0" | 82 1/2"  | 85 1/2"  | 167"   |
| 83  | 2 1/2" | 2'-0" | 83 1/2"  | 86 1/2"  | 169"   |
| 84  | 2 1/2" | 2'-0" | 84 1/2"  | 87 1/2"  | 171"   |
| 85  | 2 1/2" | 2'-0" | 85 1/2"  | 88 1/2"  | 173"   |
| 86  | 2 1/2" | 2'-0" | 86 1/2"  | 89 1/2"  | 175"   |
| 87  | 2 1/2" | 2'-0" | 87 1/2"  | 90 1/2"  | 177"   |
| 88  | 2 1/2" | 2'-0" | 88 1/2"  | 91 1/2"  | 179"   |
| 89  | 2 1/2" | 2'-0" | 89 1/2"  | 92 1/2"  | 181"   |
| 90  | 2 1/2" | 2'-0" | 90 1/2"  | 93 1/2"  | 183"   |
| 91  | 2 1/2" | 2'-0" | 91 1/2"  | 94 1/2"  | 185"   |
| 92  | 2 1/2" | 2'-0" | 92 1/2"  | 95 1/2"  | 187"   |
| 93  | 2 1/2" | 2'-0" | 93 1/2"  | 96 1/2"  | 189"   |
| 94  | 2 1/2" | 2'-0" | 94 1/2"  | 97 1/2"  | 191"   |
| 95  | 2 1/2" | 2'-0" | 95 1/2"  | 98 1/2"  | 193"   |
| 96  | 2 1/2" | 2'-0" | 96 1/2"  | 99 1/2"  | 195"   |
| 97  | 2 1/2" | 2'-0" | 97 1/2"  | 100 1/2" | 197"   |
| 98  | 2 1/2" | 2'-0" | 98 1/2"  | 101 1/2" | 199"   |
| 99  | 2 1/2" | 2'-0" | 99 1/2"  | 102 1/2" | 201"   |
| 100 | 2 1/2" | 2'-0" | 100 1/2" | 103 1/2" | 203"   |

Total movement at expansion joint as shown on the design plans.

**BILL OF MATERIAL**

| Item                     | Unit | Quantity |
|--------------------------|------|----------|
| Steel Railing, Type 2399 | Foot |          |

# TRAFFIC CONTROL AND PROTECTION, (SPECIAL)



**TO BE USED:**

- SN 041-0035
- SN 041-0095
- SN 041-0093
- SN 041-0031
- SN 041-0092

- W20-4(0)-48
- 40 MPH
- W13-1(0)-2424

- W20-1(0)-48

**NOTES**

Guardrail shall be completed at each structure prior to relocation of Concrete Barrier.

All work adjacent to the NB Lane must be completed prior to moving to the SB lane or vice versa.

- ① Type III barricade to be placed when no work is being performed.
- ② Barrier wall/guardrail markers at 25' (7.6m) cts. See Standards 704001 & 635011.
- ③ For structure length and pay item, see Guardrail Schedule.

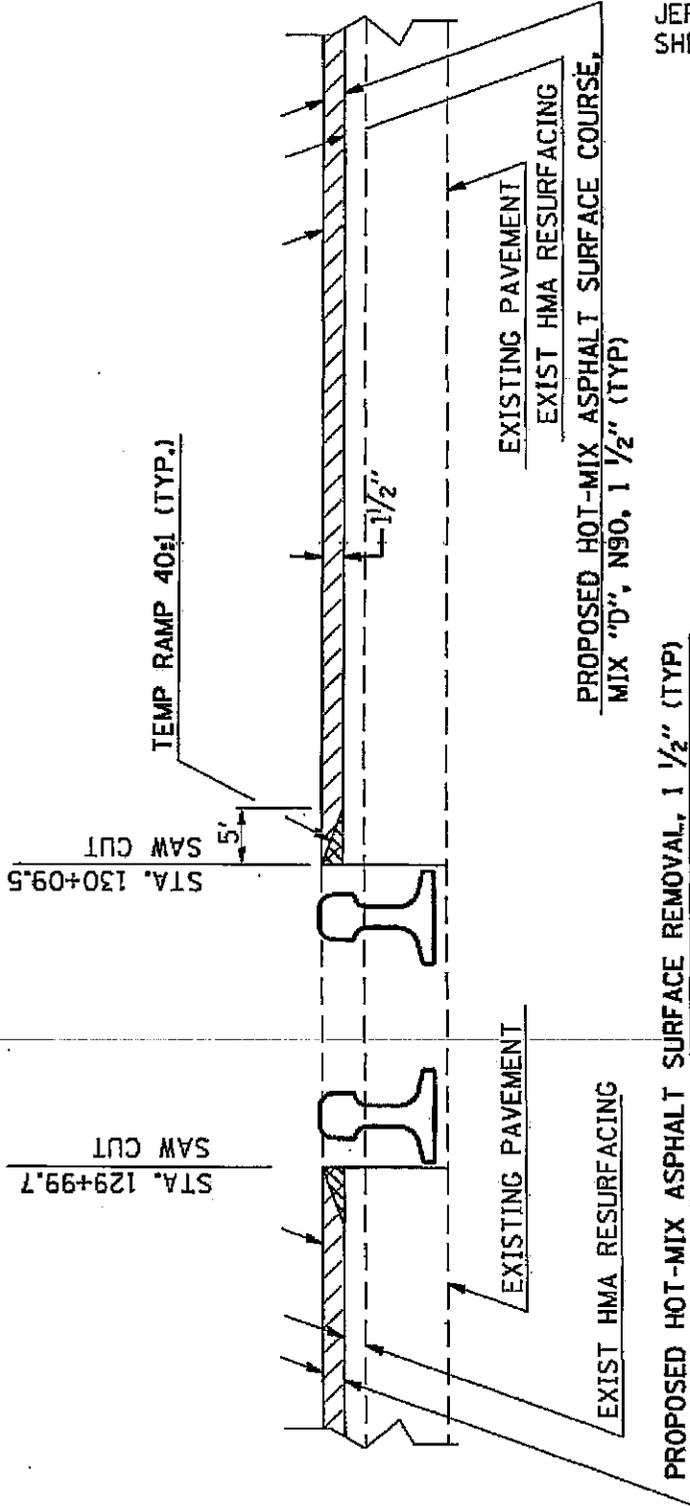
**SYMBOLS**

- Work area
- Sign
- Type III barricade
- Drum with steady burning light
- Temporary concrete barrier
- Crystal, bidirectional barrier wall/guardrail marker

IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 47 OF 72

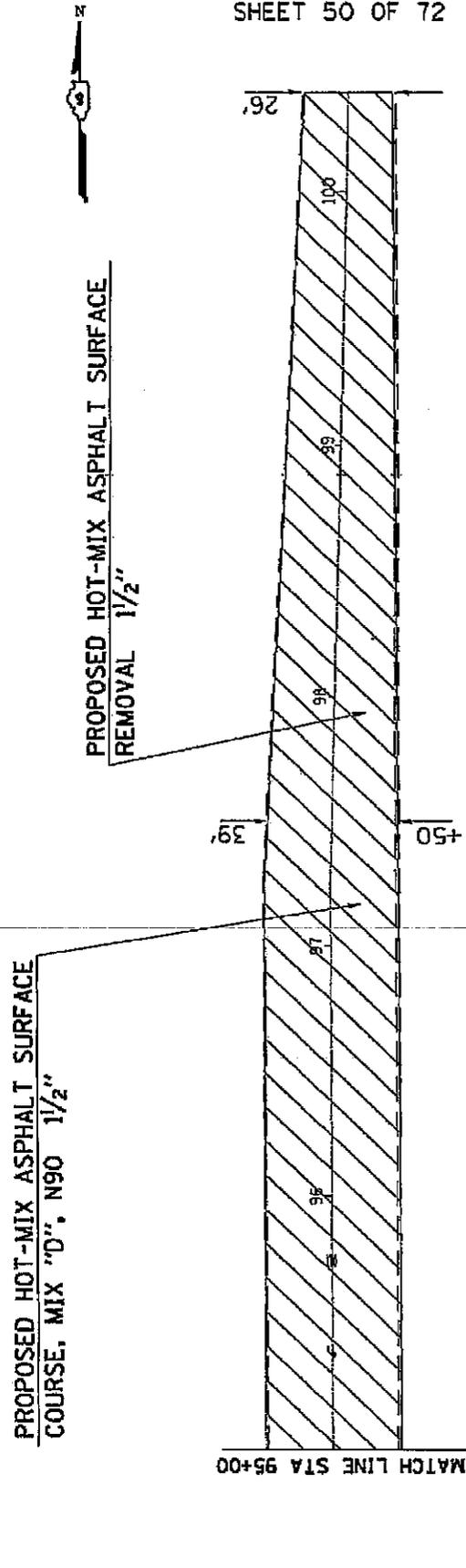
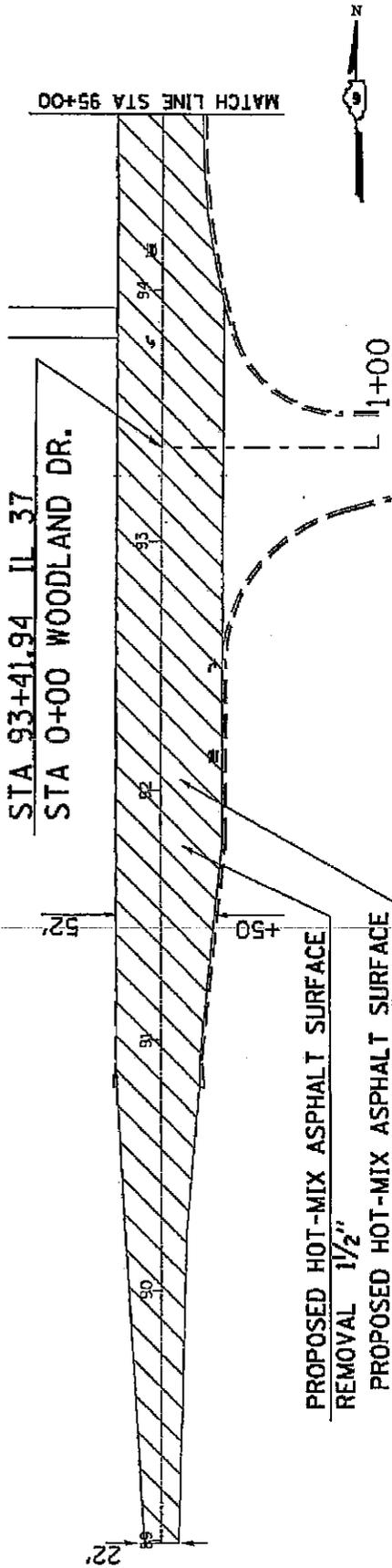


**MILLING AND RESURFACING AT RAILROAD DETAIL**

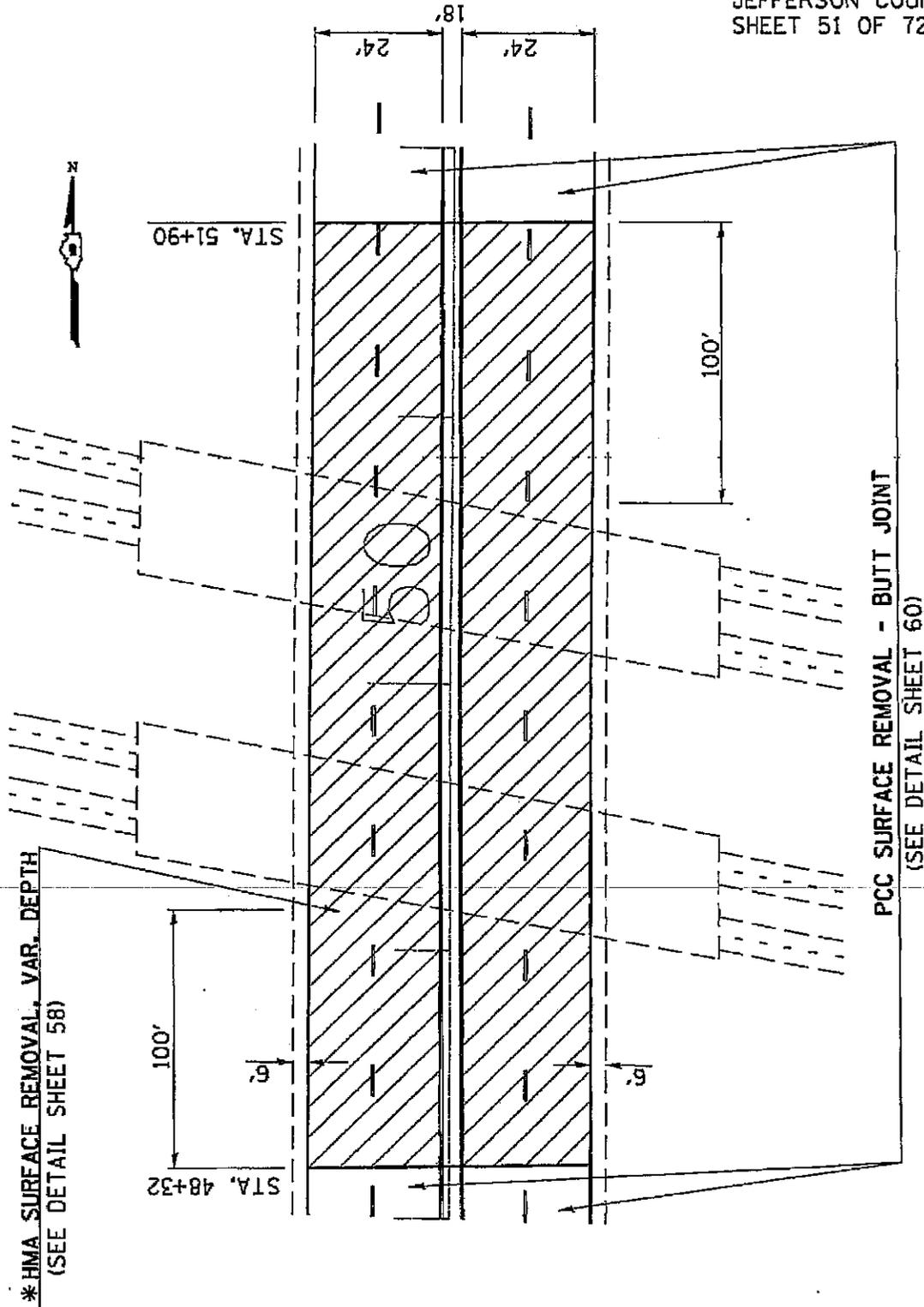


72IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 49 OF 72

**TO BE USED:**  
 SEE STATIONS ABOVE  
 JEFFERSON & SW RAILROAD



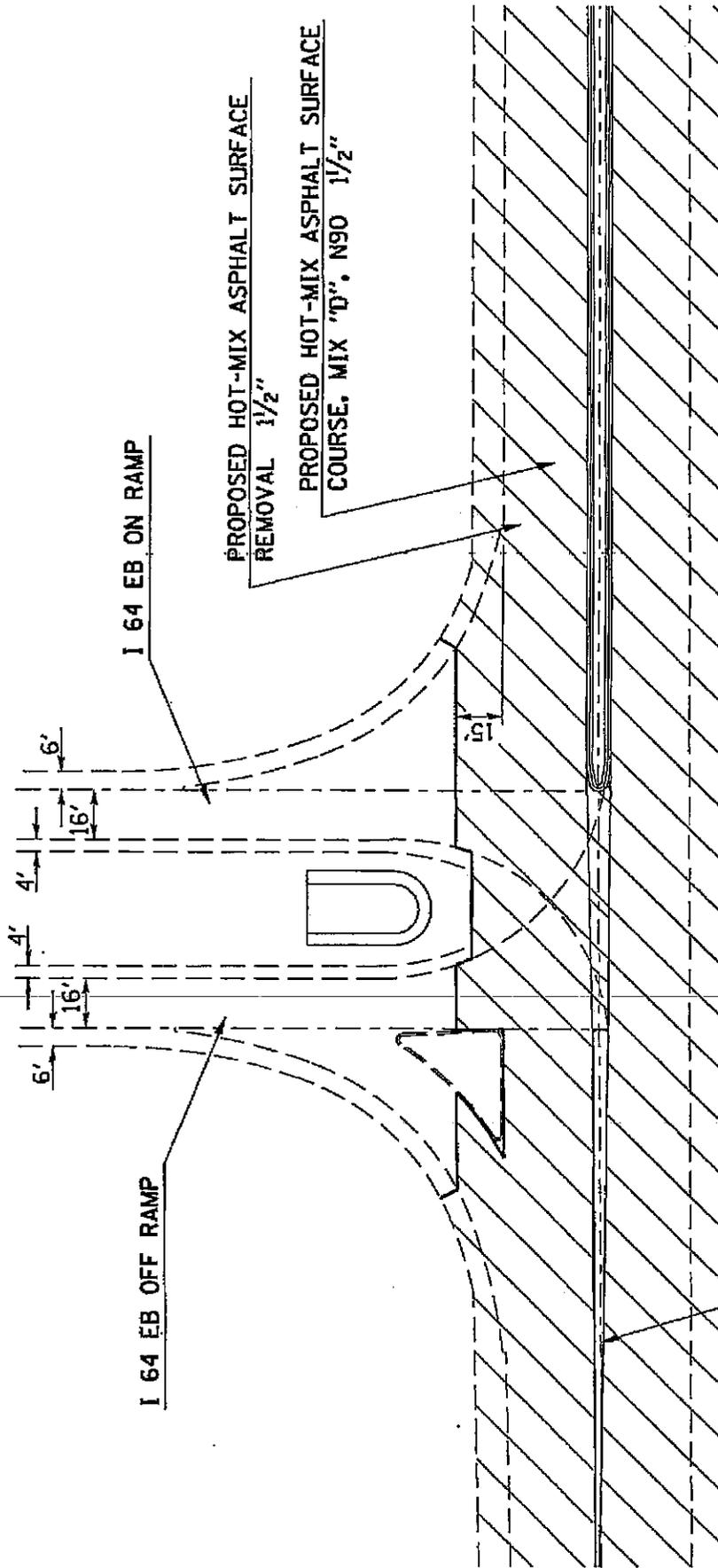
PLAN DETAIL OF WOODLAND DR. INTERSECTION  
WITH FAP 724/FAS 2869 (IL 37)



PLAN DETAILS OF I-64 OVERPASS  
 WITH FAP 724/FAS 2869 (IL 37)  
 (NOT TO SCALE)

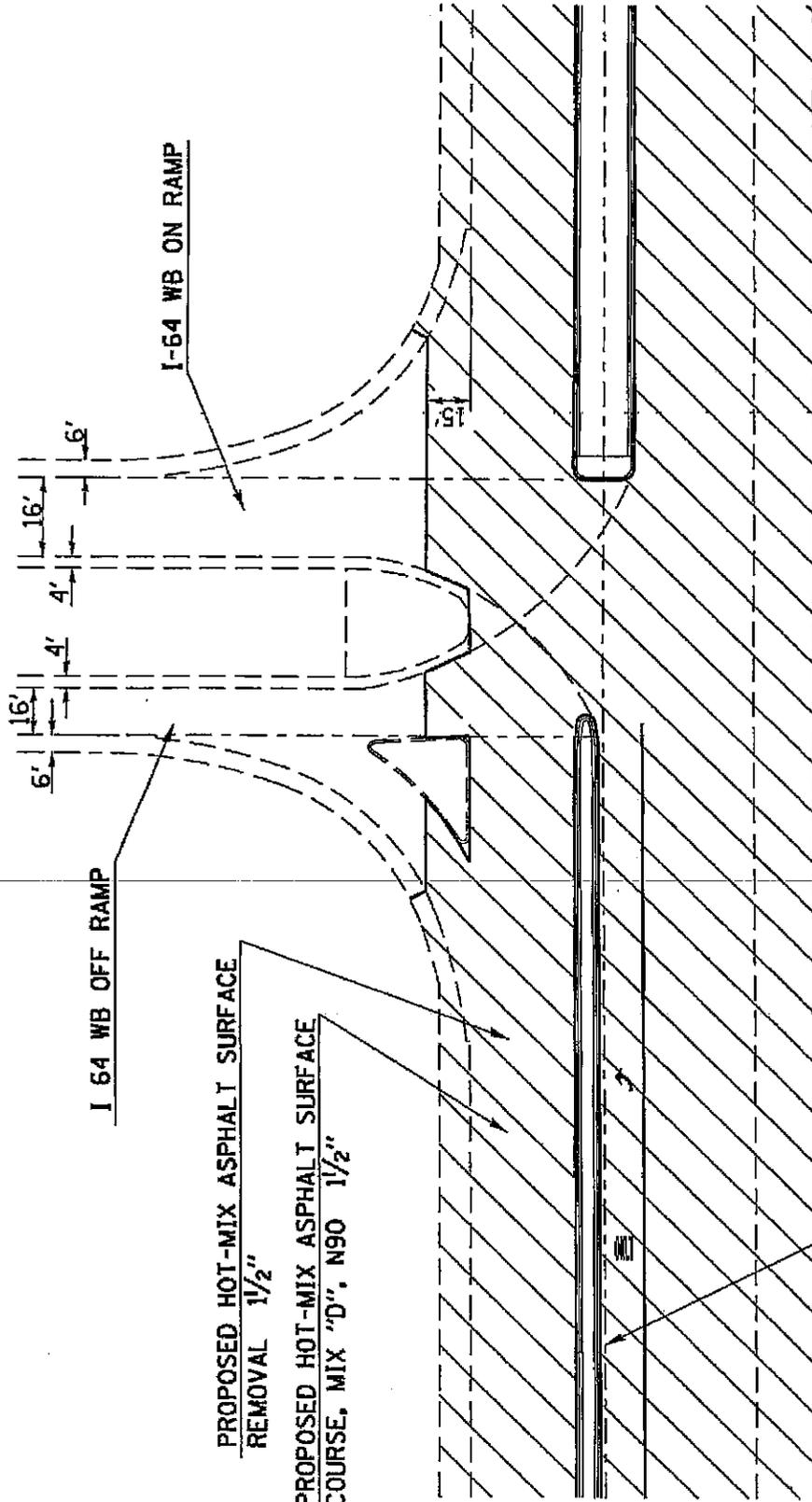
\* NO PAVING IN THIS REGION.

IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 52 OF 72

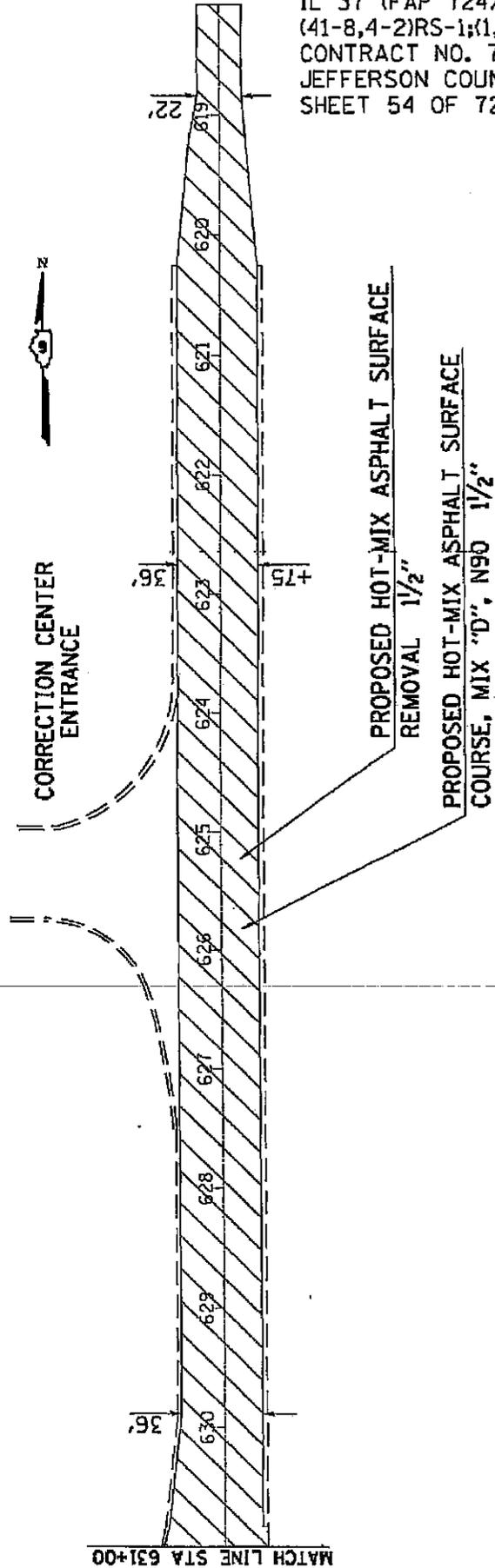
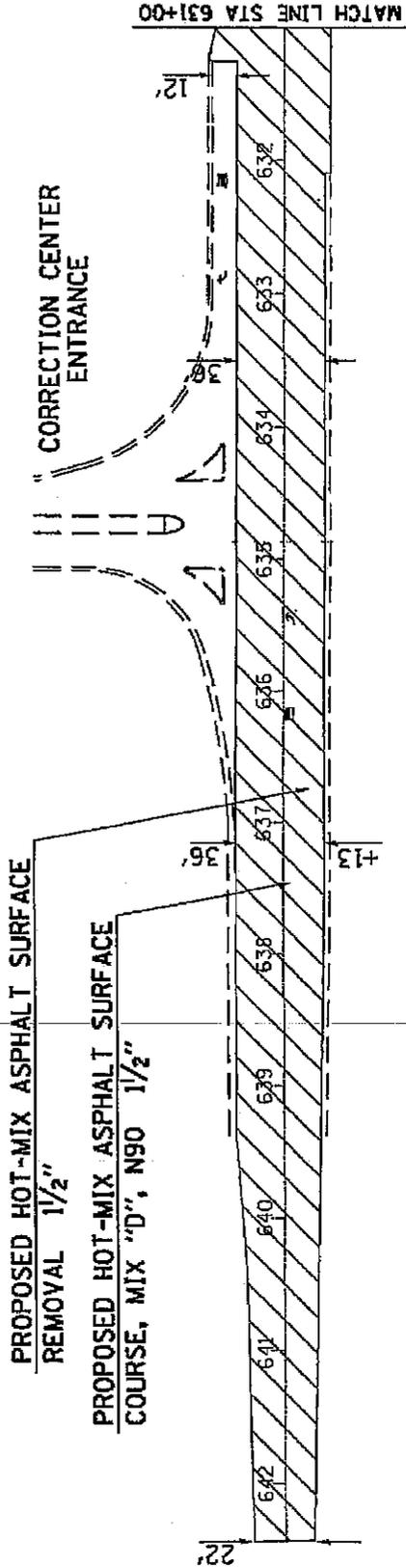


FAP 724/FAS 2869  
 (IL 37)

PLAN DETAIL OF I-64 INTERCHANGE  
EB RAMPS  
WITH FAP 724/FAS 2869 (IL 37)

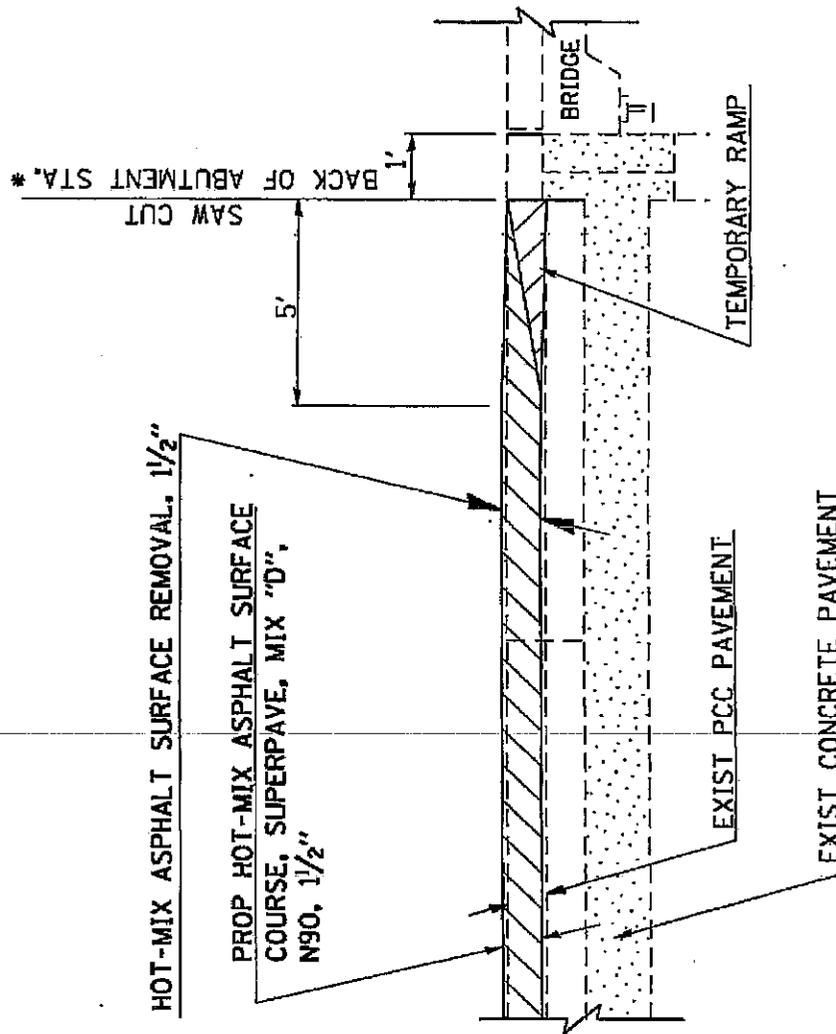


PLAN DETAIL OF I-64 INTERCHANGE  
WB RAMP  
WITH FAP 724/FAS 2869 (IL 37)



PLAN DETAIL OF CORRECTIONAL CENTER INTERSECTION  
WITH FAP 724/FAS 2869 (IL 37)

# TEMPORARY RAMP AT BRIDGE ABUTMENT



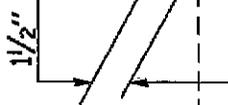
TO BE USED: \*  
SN 041-0032  
SN 041-0105  
SN 041-0097

# TEMPORARY RAMPS

PROP HOT-MIX ASPHALT SURFACE COURSE, MIX D, N90, 1/2"

HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"

SAW CUT



\* STA

IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 56 OF 72

EXIST PAVEMENT  
 IL 37

TO BE USED:\*

| <u>IL 37</u> | <u>I-64 RAMPS</u> |
|--------------|-------------------|
| STA. 140+15  | STA. 57+83.4      |
| STA. 652+50  | STA. 58+09.4      |
|              | STA. 44+75        |
|              | STA. 44+00        |

# TEMPORARY RAMPS

(AT WOODLAND RD. INTERSECTION)

PROP HOT-MIX ASPHALT SURFACE COURSE, MIX D. N90, 1/2"

EXISTING WOODLAND RD. PAVEMENT

\* STA  
SAW CUT

1/2"

HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"

IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 57 OF 72

EXIST PAVEMENT  
IL 37

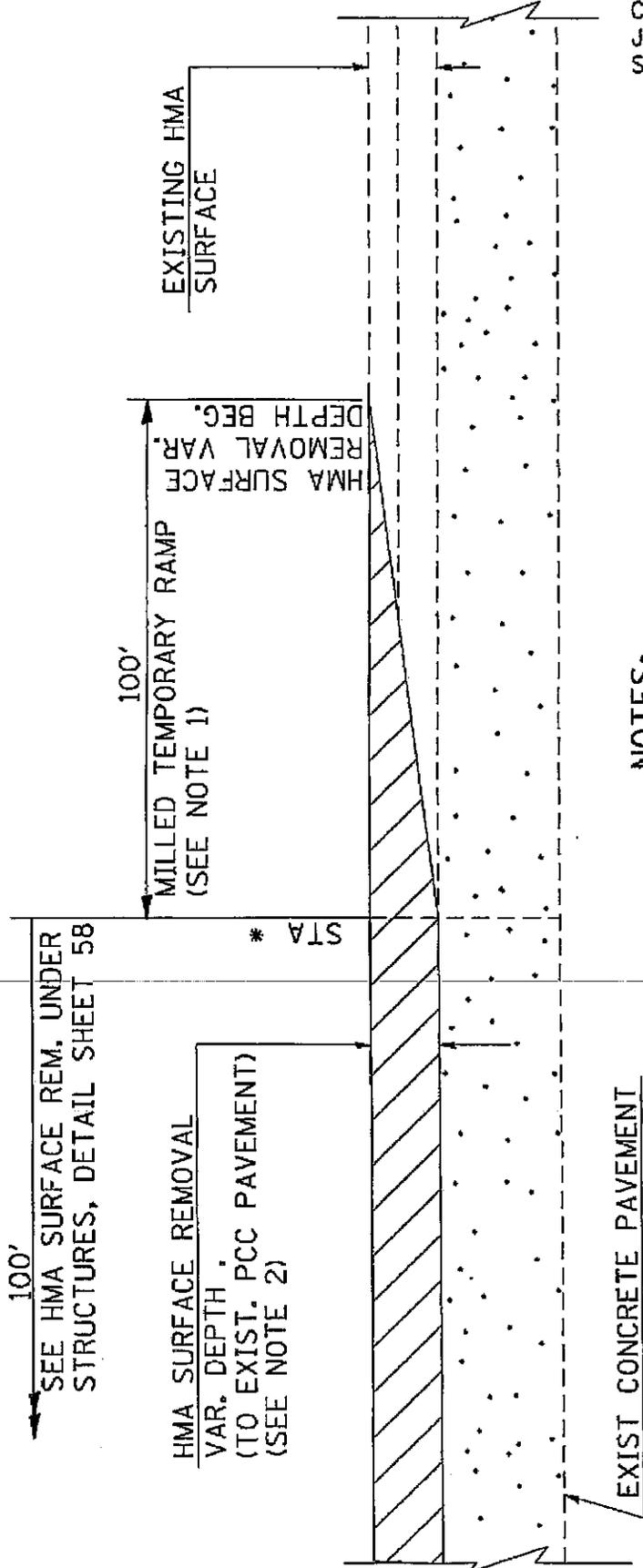
TO BE USED:\*  
STA. 93+41.94

PLT DATE = 4/9/2009  
FILE NAME = c:\pl-work\p\100188\HEADCN\0100188\4978108.sht.misc.dgn  
PLT SCALE = 58.0000' / IN.  
USER NAME = hseiden



# HMA THICKNESS TRANSITION

IL 37 AT I-64 INTERCHANGE



IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 59 OF 72

**NOTES:**

1. THE MILLED TEMPORARY RAMP FOR PAVEMENT REPAIR COST IS TO BE INCLUDED WITH THE HMA SURFACE REMOVAL VAR. DEPTH COST.
2. PATCH THE EXISTING PCC PAVEMENT WITH CLASS B PATCHING AS DIRECTED BY THE ENGINEER.

**\* TO BE USED:**

STA. 51+90 SBL AND NBL  
 STA. 48+32 SBL AND NBL

PLOT DATE = 4/9/2009  
 FILE NAME = c:\p...nor-k\PHOTO\HEADON\0100158\c970108\_sht.msc.dgn  
 PLOT SCALE = 50.0000' / IN.  
 USER NAME =

# PCC SURFACE REMOVAL - BUTT-JOINT

IL 37 AT I-64 INTERCHANGE

PROP HOT-MIX ASPHALT SURFACE COURSE, MIX D, N90, 1/2"

HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"

100'  
@ 240:1

HMA SURFACE  
REMOVAL 1/2"  
DEPTH ENDS

20'  
PCC  
BUTT JOINT

STA \*  
SAW CUT

EXIST CONCRETE PAVEMENT  
EXISTING HMA SURFACE

TEMPORARY RAMP  
@ 80:1

PCC SURFACE REMOVAL  
BUTT JOINT

IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 60 OF 72

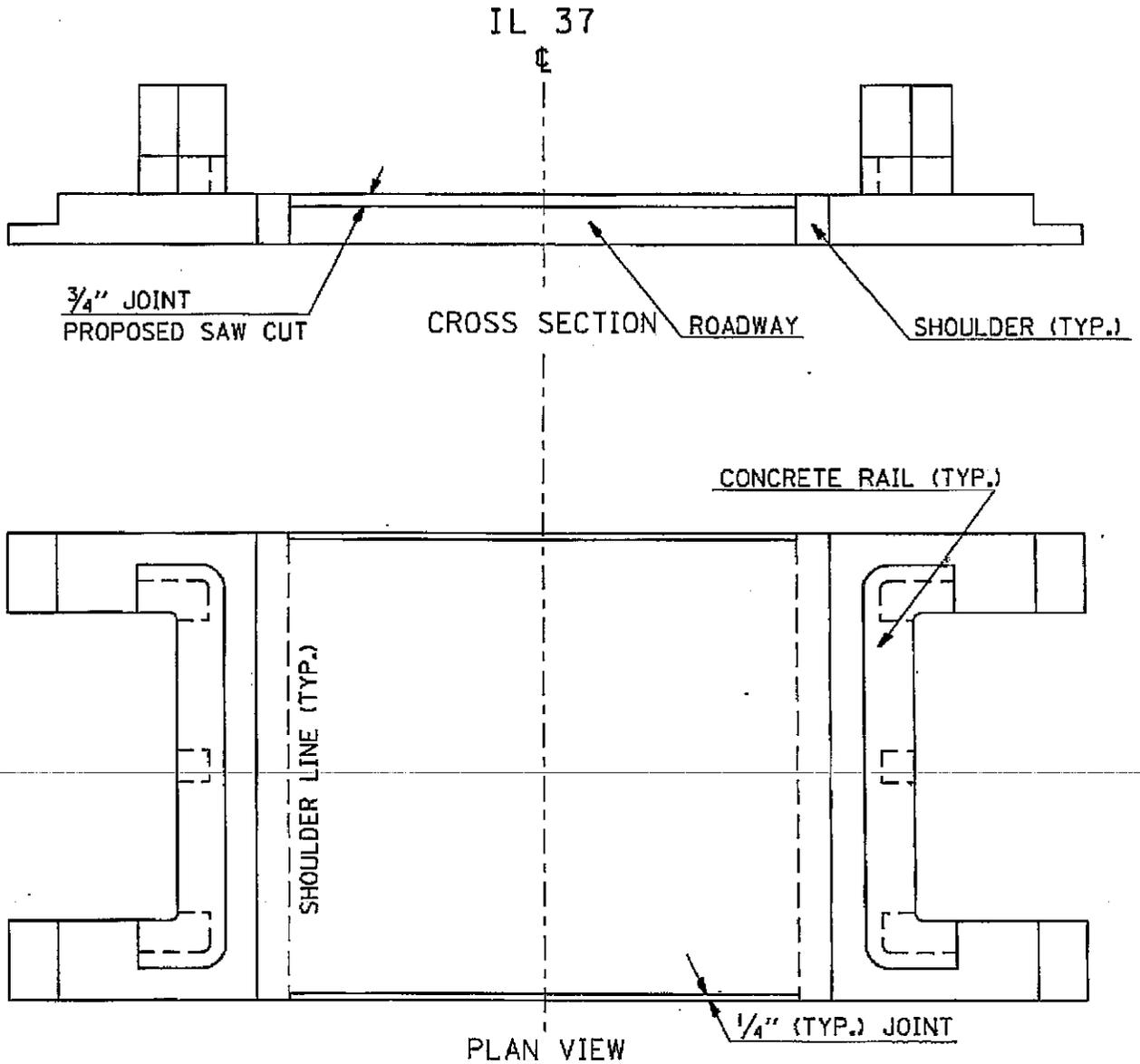
**\* TO BE USED:**

STA. 51+90 SBL AND NBL

STA. 48+32 SBL AND NBL

PLOT DATE = 4/10/2009  
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USER NAME = headcn

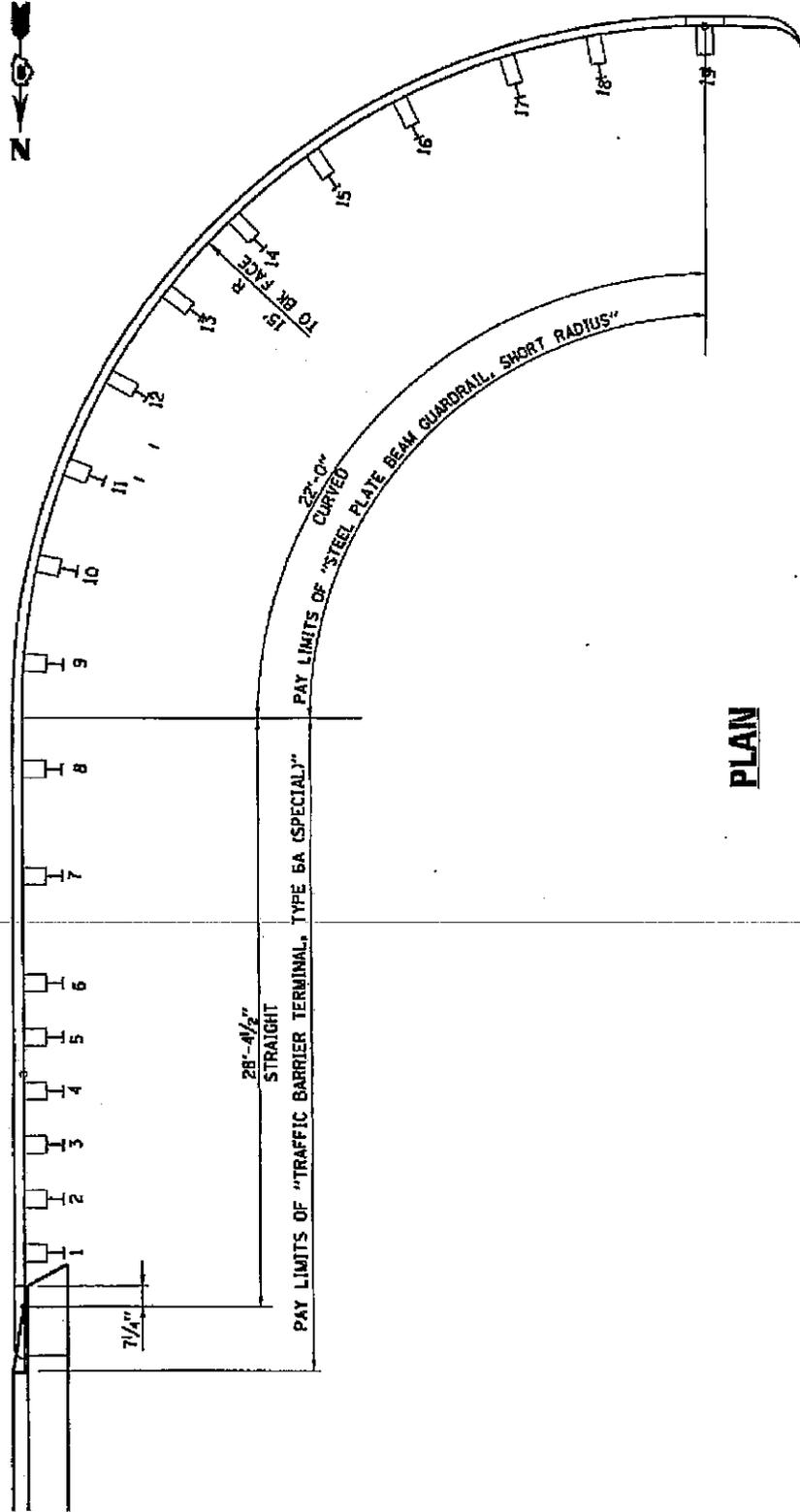
# BRIDGE JOINT DETAIL



FILL JOINT WITH SEALER, COST TO BE INCLUDED WITH PROPOSED SAW CUT.

## TO BE USED: SAW CUTS (FT)

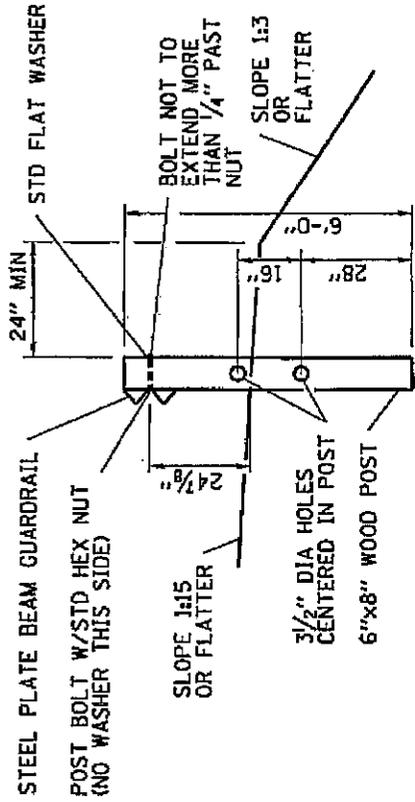
|               |    |
|---------------|----|
| STR. 041-0035 | 44 |
| STR. 041-0092 | 44 |
| STR. 041-0093 | 44 |
| STR. 041-0095 | 44 |
| STR. 041-0031 | 44 |
| STR. 041-0036 | 44 |



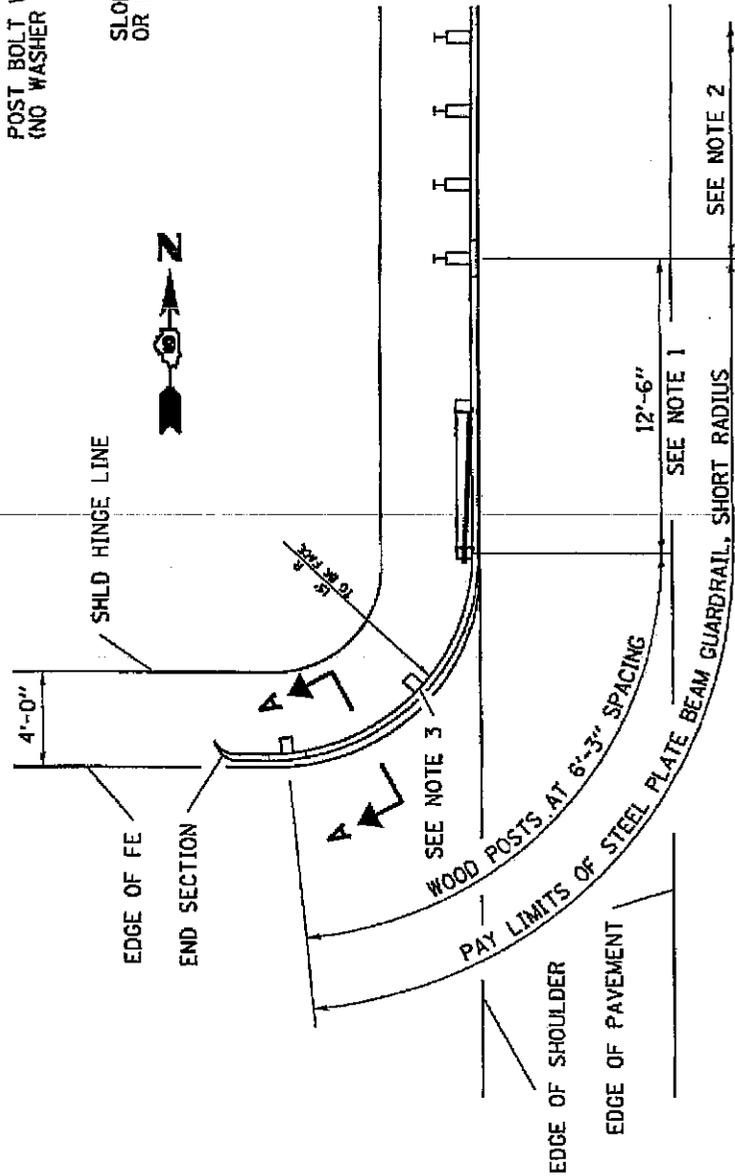
PLAN

SN. 041-0031-SOUTHWEST CORNER

# SHORT RADIUS GUARDRAIL DETAILS



**SECTION A-A**



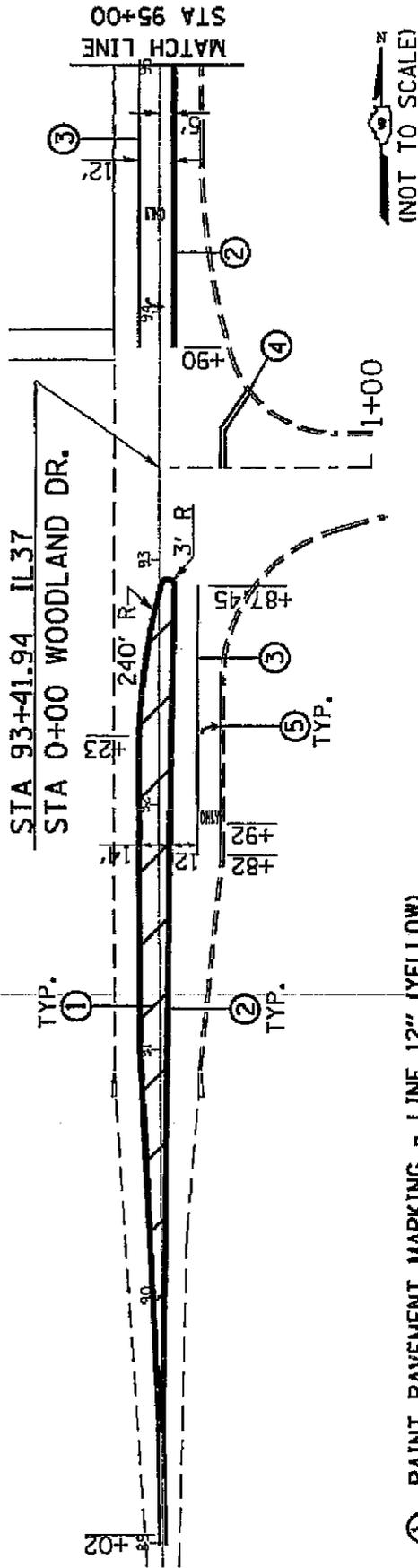
**PLAN**

## NOTES

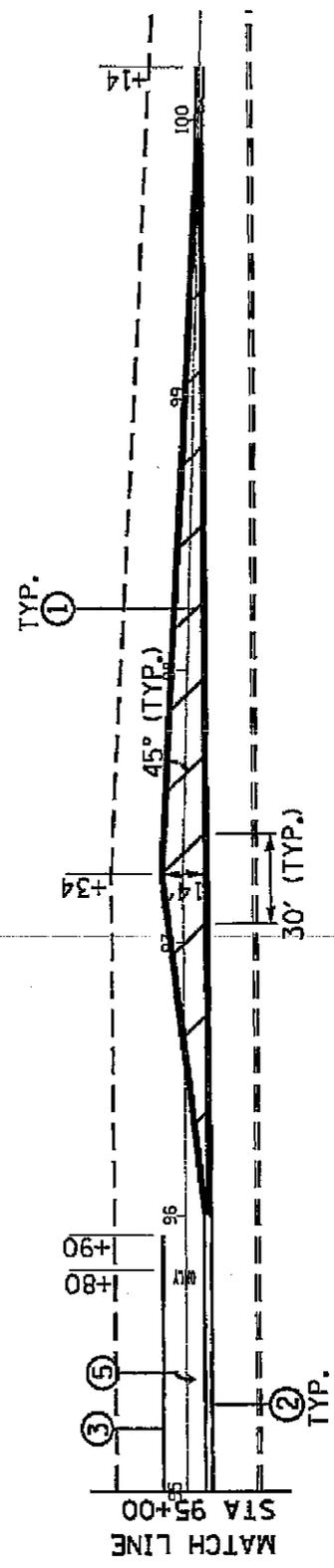
1. CONSTRUCT ACCORDING TO STANDARD 631011 FOR TRAFFIC BARRIER TERMINAL TYPE 6A. EXCEPT DELETE END SECTION AND SPLICE INTO RADIUS GUARDRAIL.
2. STEEL PLATE BEAM GUARDRAIL TYPE A, TYPE B, OR TRAFFIC BARRIER TERMINAL AS SPECIFIED.
3. THE RAIL IS NOT BOLTED TO THE POST LOCATED AT THE MIDPOINT OF THE CURVE. SEE PLAN SHEET 62.

**SIN. 041-0031-SOUTHWEST CORNER**

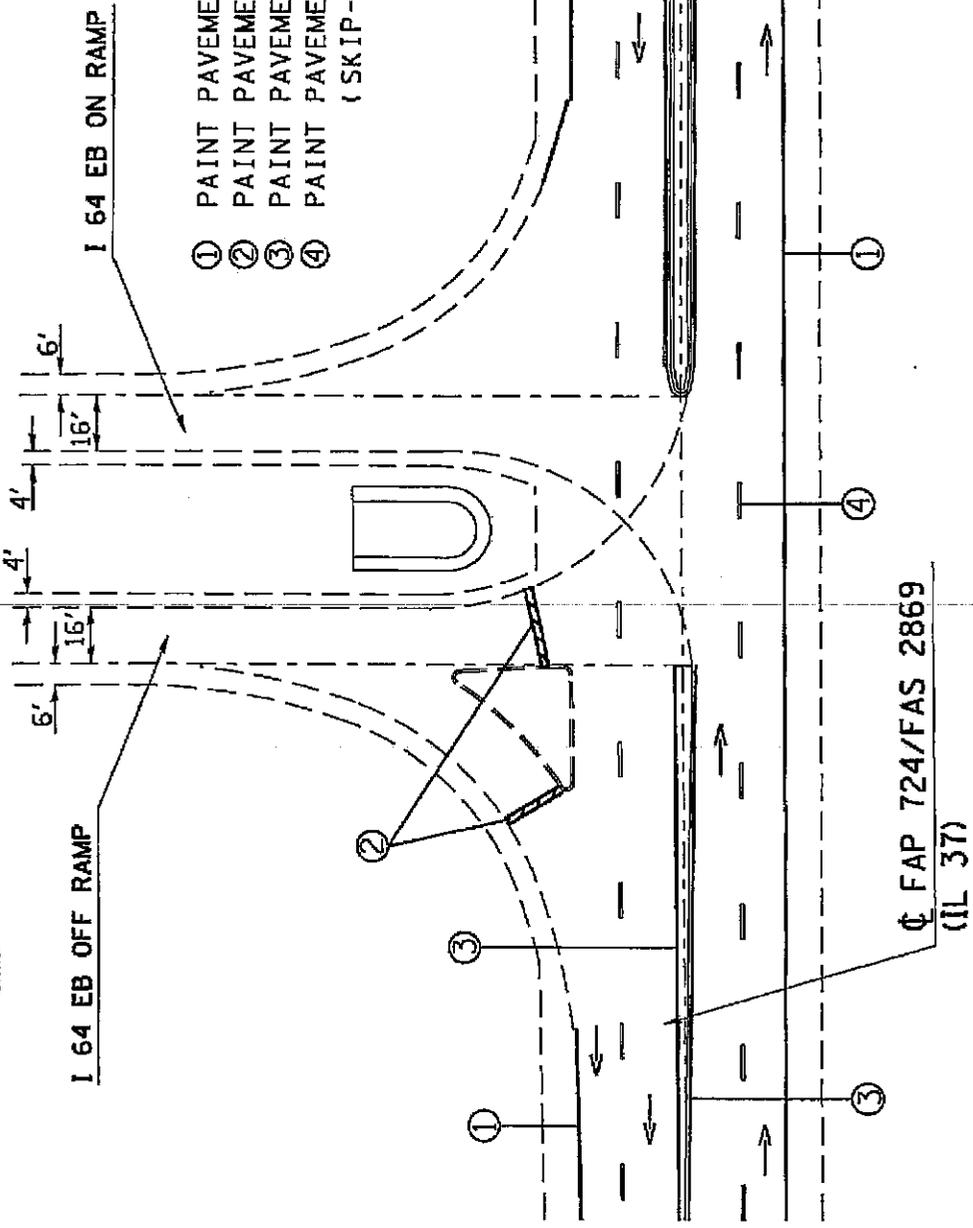
IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 63 OF 72



- ① PAINT PAVEMENT MARKING - LINE 12" (YELLOW)
- ② PAINT PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW)
- ③ PAINT PAVEMENT MARKING - LINE 4" (WHITE)
- ④ PAINT PAVEMENT MARKING - 24" (STOP BAR)
- ⑤ PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS



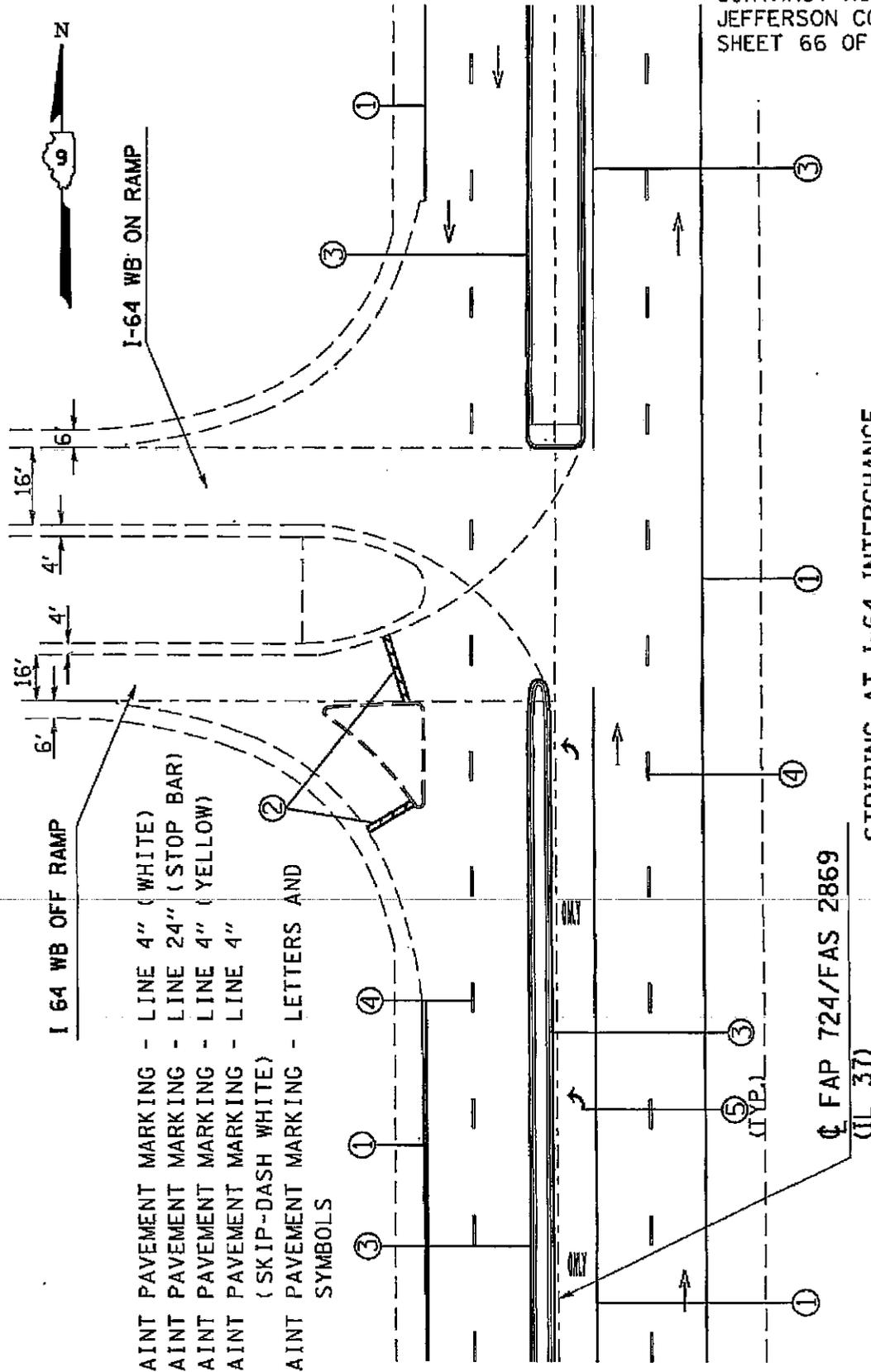
**STRIPING OF WOODLAND DR. INTERSECTION  
 WITH FAP 724/FAS 2869 (IL 37)**



- ① PAINT PAVEMENT MARKING - LINE 4" (WHITE)
- ② PAINT PAVEMENT MARKING - LINE 24" (STOP BAR)
- ③ PAINT PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ PAINT PAVEMENT MARKING - LINE 4" (SKIP-DASH WHITE)

STRIPING AT I-64 INTERCHANGE  
EB RAMPS  
WITH FAP 724/FAS 2869 (IL 37)

☐ FAP 724/FAS 2869  
 (IL 37)



I 64 WB OFF RAMP

I-64 WB ON RAMP

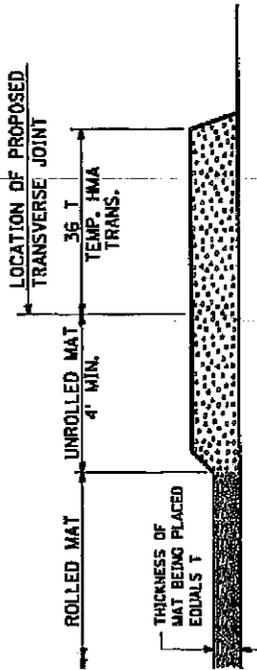
- ① PAINT PAVEMENT MARKING - LINE 4" (WHITE)
- ② PAINT PAVEMENT MARKING - LINE 24" (STOP BAR)
- ③ PAINT PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ PAINT PAVEMENT MARKING - LINE 4" (SKIP-DASH WHITE)
- ⑤ PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS

☐ FAP 724/FAS 2869  
 (IL 37)

STRIPING AT I-64 INTERCHANGE  
WB RAMPS  
WITH FAP 724/FAS 2869 (IL 37)

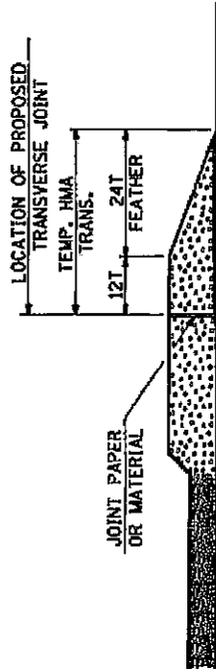


# TEMPORARY HOT-MIX ASPHALT TRANSITIONS



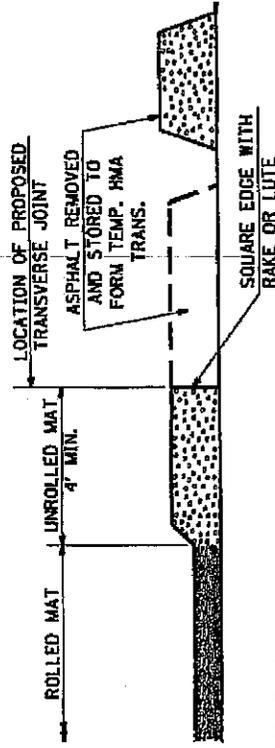
## STEP I

1. PLACE HOT-MIX ASPHALT MAT, LENGTH 36 TIMES THE THICKNESS OF THE MAT BEING PLACED PAST THE PROPOSED TRANSVERSE JOINT LOCATION USING NORMAL OPERATING PROCEDURES.
2. EXTREME CARE SHOULD BE TAKEN TO MAINTAIN ENOUGH MATERIAL IN FRONT OF THE SCREED TO MAINTAIN REQUIRED PAVING DEPTH.



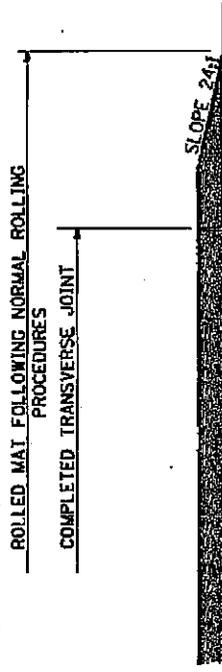
## STEP III

1. JOINT PAPER OR OTHER PRESELECTED JOINT MATERIAL IS THEN PLACED IN THE CLEARED AREA AND THE EXCESS ASPHALT USED TO HAND FORM A TRANSITION TO THE DIMENSIONS SHOWN ABOVE.
2. NOTE THAT IN CONSTRUCTING THE TRANSITION, THE MAT DEPTH IS CONTINUED AS PART OF THE TRANSITION BEFORE FORMING THE FEATHER.



## STEP II

1. MOVE THE PAYER OUT OF THE WAY AND REMOVE THE ASPHALT FROM THE AREA OF THE PROPOSED TEMPORARY HOT-MIX ASPHALT TRANSITION.
2. SQUARE UP THE END OF THE MAT WITH A RAKE OR LUTE.
3. NOTE THAT THE MAT WITHIN 4' OF THE END OF JOINT IS NOT TO BE ROLLED AT THIS TIME.



## STEP IV

1. COMPLETE TEMPORARY TRANSITION BY ROLLING.
2. TO RESUME PAVING, AT THE JOINT, REMOVE TEMPORARY TRANSITION AND DISPOSE OF THE MATERIAL ACCORDING TO ART. 202.03 OF THE STD. SPECS. (COST INCLUDED IN THE CONTRACT).
3. CONSTRUCTING THE TEMPORARY TRANSITIONS WILL NOT BE PAID FOR SEPARATELY IN ACCORDANCE WITH ARTICLE 406.14 OF THE STANDARD SPECIFICATIONS.

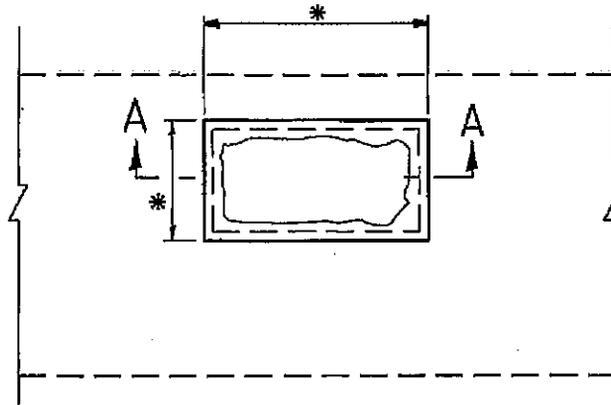
IL 37 (FAP 724/FAS 2869,  
 (41-B,4-2)RS-1;(1,4)RS-3;2RS-1  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 68 OF 72

REVISIONS  
 REVISION 2 12-18-89  
 REVISION 1 12-18-89  
 REVISION 0 12-18-89

PLOT DATE = 4/8/2009  
 FILE NAME = c:\pva\new-k\pva\DOT\HENDON\08100160\08100160.dwg  
 PLOT SCALE = 1/8"=1'-0"  
 150.0 x 110.0 in.

# DETAILS OF P.C.C. PATCHING-PARTIAL DEPTH

IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 69 OF 72

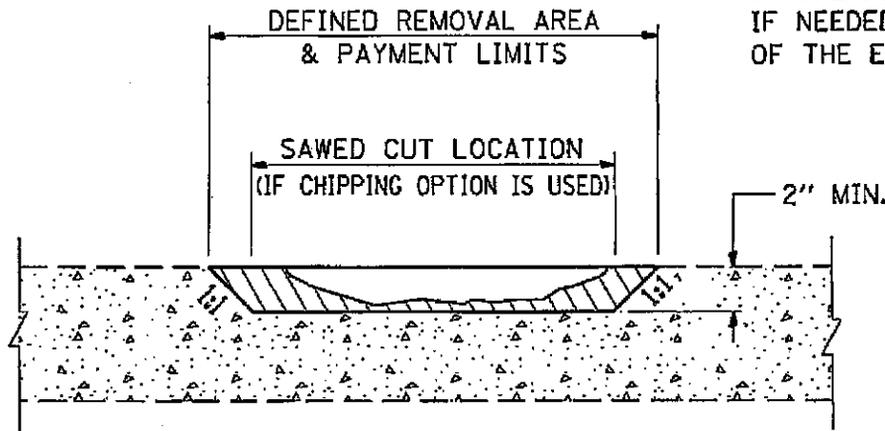


PLAN

\* LIMITS OF PARTIAL DEPTH PATCHING AS DIRECTED BY THE ENGINEER. IF REQUIRED AFTER THE HMA SURFACE REMOVAL BETWEEN STA 46+73 TO STA 53+27.

THE PATCHING MATERIAL SHALL BE PORTLAND CEMENT CONCRETE AT LOCATIONS DIRECTED BY THE ENGINEER

NO QUANTITY HAS BEEN PROVIDED IN THE PLANS BUT TO BE ADDED IF NEEDED AT THE DISCRETION OF THE ENGINEER.



SECTION A-A

## NOTES:

THE CONTRACTOR HAS THE OPTION OF SAWING THE PAVEMENT AND CHIPPING THE EDGES OR BY MILLING THE DEFINED AREA OF THE PATCH.

THE PAVEMENT SHALL BE SAWED A MINIMUM OF 2 INCH DEPTH IF THE SAWING OPTION IS USED.

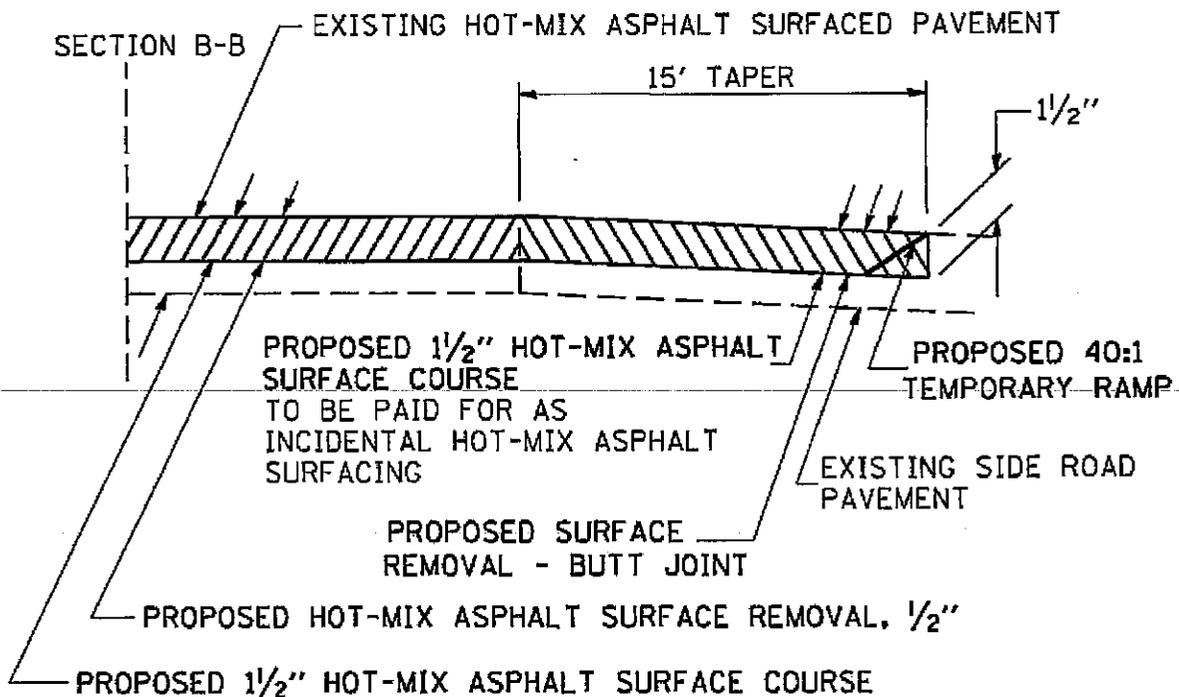
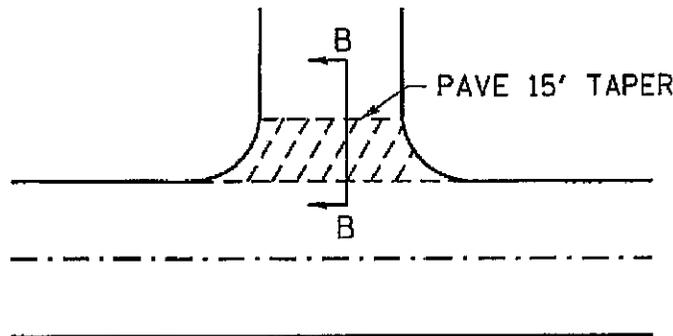
A HAND VIBRATOR OR VIBRATING SCREED SHALL BE USED DURING THE PATCHING OPERATIONS.  
 BURLAP CURING BLANKETS ARE NOT REQUIRED.  
 CURING SHALL BE WITH CURING COMPOUNDS MEETING THE APPROVAL OF THE ENGINEER.

FOR ADDITIONAL REQUIREMENTS AND BASIS OF PAYMENT, SEE THE SPECIAL PROVISIONS.

MODIFIED STD. 9-37

| REVISIONS |         |
|-----------|---------|
| DRAWN     | 2-15-89 |
| REVISED   | 3-26-08 |
| REVISED   |         |
| REVISED   |         |

**I-64 RAMPS**  
**DETAIL OF HOT-MIX ASPHALT OR PCC**  
**SIDE ROADS WITH BUTT-JOINT**

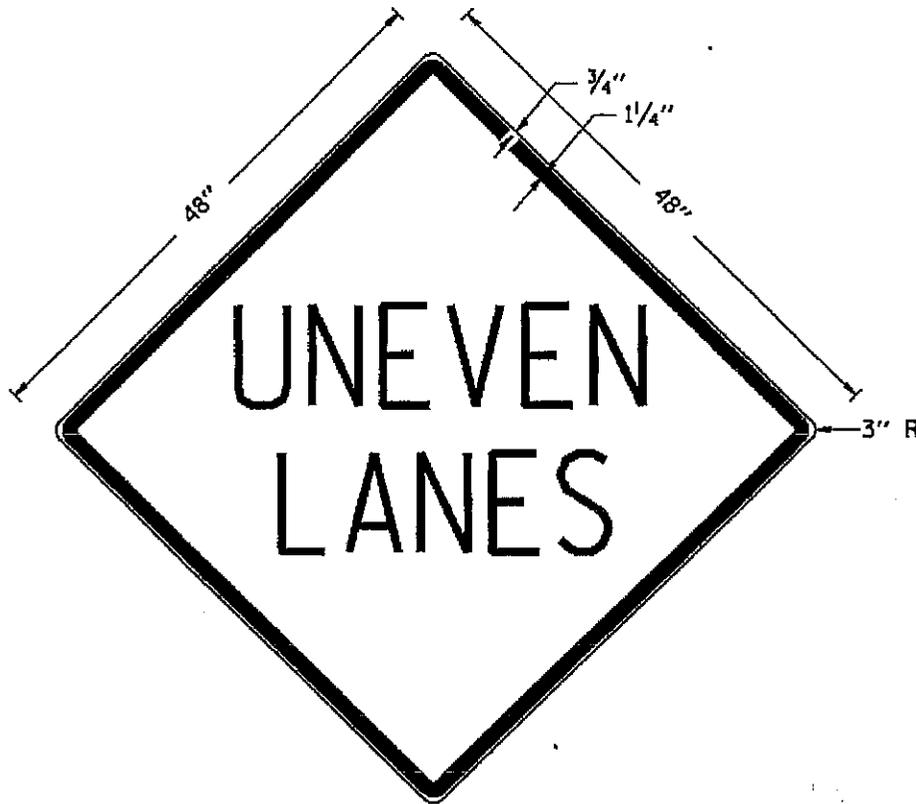


IF EXISTING SUB-BASE IS INADEQUATE, AS DETERMINED BY THE ENGINEER, THE SIDEROADS SHALL BE CORED OUT AND AGGREGATE SUB-BASE, TYPE B SHALL BE PLACED FOR BASE. THE COST OF CORING OUT THE SIDEROAD SHALL BE INCLUDED IN THE COST OF THE AGGREGATE BASE COURSE. IF EXISTING SUB-BASE IS DETERMINED TO BE ADEQUATE, THE AGGREGATE BASE COURSE SHALL BE DELETED AND THE PREPARATION OF THE BASE SHALL BE CONSTRUCTED ACCORDING TO ARTICLE 406.19.

# UNEVEN LANES SIGN

WB-11 (48" x 48")

IL 37 (FAP 724/FAS 2869)  
(41-8,4-2)RS-1;(1,4)RS-3;2RS--  
CONTRACT NO. 78108  
JEFFERSON COUNTY  
SHEET 71 OF 72



## COLORS:

LEGEND AND BORDER - BLACK NON-REFLECTORIZED  
BACKGROUND - ORANGE REFLECTORIZED

**NOTE:** PRIOR TO ALLOWING TRAFFIC ON ANY PORTION OF THE ROADWAY THAT HAS BEEN COLDMILLED OR BEFORE RESURFACING OPERATIONS BEGIN, THE CONTRACTOR SHALL HAVE ERECTED "UNEVEN PAVEMENT" SIGNS THAT CONFORM TO THE ABOVE DETAILS. A MINIMUM OF ONE SIGN AT EACH END OF THE IMPROVEMENT WILL BE REQUIRED. THE CONTRACTOR SHALL MAINTAIN THE "UNEVEN PAVEMENT" SIGNS UNTIL THE RESURFACING OPERATIONS ARE COMPLETED.

IF AT ANY TIME THE SIGNS ARE IN PLACE BUT NOT APPLICABLE, THEY SHALL BE TURNED FROM THE VIEW OF MOTORISTS OR COVERED AS DIRECTED BY THE ENGINEER.

THE COST OF FURNISHING, ERECTING, MAINTAINING, AND REMOVING THE REQUIRED SIGNS SHALL BE INCLUDED IN THE CONTRACT.

PLOT DATE = 4/8/2009  
FILE NAME = c:\pw\_work\PHIDOT\HEADCN\d0100160\d978108.sht\_misc.dgn  
PLOT SCALE = 50.0000 ' / IN.

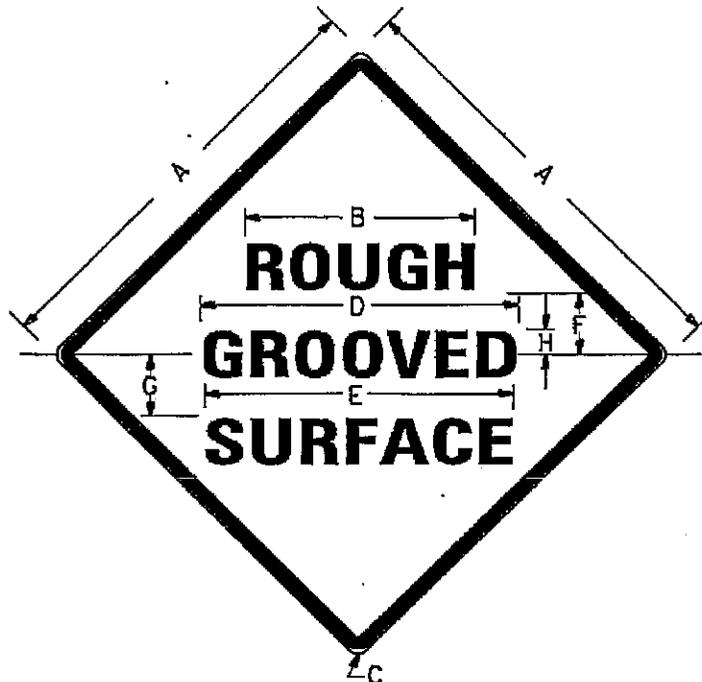
STD. 9-41

| REVISIONS  |         |
|------------|---------|
| DRAWN      | 2-15-89 |
| REVISED    | 4-6-93  |
| REDESIGNED | 7-23-04 |
| REVISED    |         |

# ILLINOIS STANDARD

W8-1106

IL 37 (FAP 724/FAS 2869)  
 (41-8,4-2)RS-1;(1,4)RS-3;2RS-4  
 CONTRACT NO. 78108  
 JEFFERSON COUNTY  
 SHEET 72 OF 72



## COLORS:

LEGEND AND BORDER- BLACK NON-REFLECTORIZED  
 BACKGROUND- ORANGE REFLECTORIZED

| SIGN SIZE | DIMENSIONS |      |     |      |      |     |      |     |
|-----------|------------|------|-----|------|------|-----|------|-----|
|           | A          | B    | C   | D    | E    | F   | G    | H   |
| 48X48     | 48.0       | 24.1 | 3.0 | 34.0 | 33.0 | 6.0 | 13.0 | 3.5 |

| SIGN SIZE | SERIES LINES |    |    | MARGIN | BORDER | BLANK STD. |
|-----------|--------------|----|----|--------|--------|------------|
|           | 1            | 2  | 3  |        |        |            |
| 48X48     | 7C           | 7C | 7C | 0.8    | 1.2    | B4-48D     |

ALL DIMENSIONS IN INCHES

## NOTES:

PRIOR TO ALLOWING TRAFFIC ON ANY PORTION OF THE ROADWAY THAT HAS BEEN COLDMILLED, THE CONTRACTOR SHALL HAVE ERECTED "ROUGH GROOVED SURFACE" SIGNS THAT CONFORM TO THE ABOVE DETAILS. A MINIMUM OF ONE SIGN AT EACH END OF THE IMPROVEMENT WILL BE REQUIRED. THE CONTRACTOR SHALL MAINTAIN THE "ROUGH GROOVED SURFACE" SIGNS UNTIL THE COLDMILLED SURFACE IS COVERED WITH LEVELING BINDER OR SURFACE COURSE.

IF AT ANY TIME THE SIGNS ARE IN PLACE BUT NOT APPLICABLE, THEY SHALL BE TURNED FROM THE VIEW OF MOTORISTS OR COVERED AS DIRECTED BY THE ENGINEER.

THE COST OF FURNISHING, ERECTING, MAINTAINING, AND REMOVING THE REQUIRED SIGNS SHALL BE INCLUDED IN THE CONTRACT.

PLOT DATE = 4/8/2009  
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| REVISIONS |         |
|-----------|---------|
| REDRAWN   | 2-15-89 |
| REVISED   | 4-6-93  |
| REVISED   |         |
| REVISED   |         |

STD. 9-39

## ILLINOIS DEPARTMENT OF LABOR

### PREVAILING WAGES FOR JEFFERSON COUNTY EFFECTIVE MAY 2009

The Prevailing rates of wages are included in the Contract proposals which are subject to Check Sheet #5 of the Supplemental Specifications and Recurring Special Provisions. The rates have been ascertained and certified by the Illinois Department of Labor for the locality in which the work is to be performed and for each craft or type of work or mechanic needed to execute the work of the Contract. As required by Prevailing Wage Act (820 ILCS 130/0.01, et seq.) and Check Sheet #5 of the Contract, not less than the rates of wages ascertained by the Illinois Department of Labor and as revised during the performance of a Contract shall be paid to all laborers, workers and mechanics performing work under the Contract. Post the scale of wages in a prominent and easily accessible place at the site of work.

If the Illinois Department of Labor revises the prevailing rates of wages to be paid as listed in the specification of rates, the contractor shall post the revised rates of wages and shall pay not less than the revised rates of wages. Current wage rate information shall be obtained by visiting the Illinois Department of Labor web site at <http://www.state.il.us/agency/idol/> or by calling 312-793-2814. It is the responsibility of the contractor to review the rates applicable to the work of the contract at regular intervals in order to insure the timely payment of current rates. Provision of this information to the contractor by means of the Illinois Department of Labor web site satisfies the notification of revisions by the Department to the contractor pursuant to the Act, and the contractor agrees that no additional notice is required. The contractor shall notify each of its subcontractors of the revised rates of wages.

# Jefferson County Prevailing Wage for May 2009

| Trade Name          | RG | TYP | C | Base   | FRMAN  | *M-F>8 | OSA | OSH | H/W   | Pensn | Vac   | Trng  |
|---------------------|----|-----|---|--------|--------|--------|-----|-----|-------|-------|-------|-------|
| =====               | == | === | = | =====  | =====  | =====  | === | === | ===== | ===== | ===== | ===== |
| ASBESTOS ABT-GEN    |    | ALL |   | 22.550 | 23.000 | 1.5    | 1.5 | 2.0 | 5.450 | 7.800 | 0.000 | 0.900 |
| ASBESTOS ABT-MEC    |    | BLD |   | 26.610 | 27.610 | 1.5    | 1.5 | 2.0 | 5.250 | 2.500 | 0.000 | 0.250 |
| BOILERMAKER         |    | BLD |   | 31.500 | 34.000 | 1.5    | 1.5 | 2.0 | 6.820 | 11.43 | 1.500 | 0.350 |
| BRICK MASON         |    | BLD |   | 26.260 | 27.760 | 1.5    | 1.5 | 2.0 | 6.900 | 6.200 | 0.000 | 0.430 |
| CARPENTER           |    | BLD |   | 29.530 | 31.030 | 1.5    | 1.5 | 2.0 | 5.550 | 4.250 | 0.000 | 0.350 |
| CARPENTER           |    | HWY |   | 29.480 | 31.230 | 1.5    | 1.5 | 2.0 | 5.550 | 4.250 | 0.000 | 0.350 |
| CEMENT MASON        |    | BLD |   | 25.750 | 27.250 | 1.5    | 1.5 | 2.0 | 5.500 | 4.450 | 0.000 | 0.300 |
| CEMENT MASON        |    | HWY |   | 24.570 | 26.070 | 1.5    | 1.5 | 2.0 | 5.500 | 5.000 | 0.000 | 0.300 |
| CERAMIC TILE FNSHER |    | BLD |   | 24.990 | 0.000  | 1.5    | 1.5 | 2.0 | 6.900 | 6.200 | 0.000 | 0.430 |
| ELECTRICIAN         |    | ALL |   | 34.460 | 36.710 | 1.5    | 1.5 | 2.0 | 5.150 | 7.920 | 0.000 | 0.520 |
| ELECTRONIC SYS TECH |    | BLD |   | 26.740 | 28.240 | 1.5    | 1.5 | 2.0 | 5.150 | 3.470 | 0.000 | 0.250 |
| FLOOR LAYER         |    | BLD |   | 27.680 | 27.980 | 1.5    | 1.5 | 2.0 | 5.550 | 4.250 | 1.000 | 0.350 |
| GLAZIER             |    | BLD |   | 25.180 | 26.180 | 1.5    | 1.5 | 2.0 | 4.700 | 4.000 | 0.000 | 0.250 |
| HT/FROST INSULATOR  |    | BLD |   | 32.910 | 33.910 | 1.5    | 1.5 | 2.0 | 5.600 | 9.360 | 0.000 | 0.500 |
| IRON WORKER         |    | ALL |   | 28.350 | 29.850 | 1.5    | 1.5 | 2.0 | 6.360 | 10.05 | 0.000 | 0.420 |
| LABORER             |    | BLD |   | 22.550 | 23.000 | 1.5    | 1.5 | 2.0 | 5.450 | 7.800 | 0.000 | 0.800 |
| LABORER             |    | HWY |   | 22.550 | 23.000 | 1.5    | 1.5 | 2.0 | 5.450 | 7.800 | 0.000 | 0.800 |
| MACHINIST           |    | BLD |   | 40.530 | 42.530 | 1.5    | 1.5 | 2.0 | 7.000 | 7.670 | 0.650 | 0.000 |
| MARBLE FINISHERS    |    | BLD |   | 24.990 | 0.000  | 1.5    | 1.5 | 2.0 | 6.900 | 6.200 | 0.000 | 0.430 |
| MARBLE MASON        |    | BLD |   | 26.260 | 27.760 | 1.5    | 1.5 | 2.0 | 6.900 | 6.200 | 0.000 | 0.430 |
| MILLWRIGHT          |    | BLD |   | 29.530 | 31.030 | 1.5    | 1.5 | 2.0 | 5.550 | 4.250 | 0.000 | 0.350 |
| MILLWRIGHT          |    | HWY |   | 29.980 | 31.730 | 1.5    | 1.5 | 2.0 | 5.550 | 4.250 | 0.000 | 0.350 |
| OPERATING ENGINEER  |    | ALL | 1 | 28.500 | 31.500 | 1.5    | 1.5 | 2.0 | 7.300 | 12.75 | 0.000 | 1.000 |
| OPERATING ENGINEER  |    | ALL | 2 | 27.370 | 31.500 | 1.5    | 1.5 | 2.0 | 7.300 | 12.75 | 0.000 | 1.000 |
| OPERATING ENGINEER  |    | ALL | 3 | 22.890 | 31.500 | 1.5    | 1.5 | 2.0 | 7.300 | 12.75 | 0.000 | 1.000 |
| OPERATING ENGINEER  |    | ALL | 4 | 22.950 | 31.500 | 1.5    | 1.5 | 2.0 | 7.300 | 12.75 | 0.000 | 1.000 |
| OPERATING ENGINEER  |    | ALL | 5 | 22.620 | 31.500 | 1.5    | 1.5 | 2.0 | 7.300 | 12.75 | 0.000 | 1.000 |
| OPERATING ENGINEER  |    | ALL | 6 | 29.050 | 31.500 | 1.5    | 1.5 | 2.0 | 7.300 | 12.75 | 0.000 | 1.000 |
| OPERATING ENGINEER  |    | ALL | 7 | 29.350 | 31.500 | 1.5    | 1.5 | 2.0 | 7.300 | 12.75 | 0.000 | 1.000 |
| OPERATING ENGINEER  |    | ALL | 8 | 29.630 | 31.500 | 1.5    | 1.5 | 2.0 | 7.300 | 12.75 | 0.000 | 1.000 |
| PAINTER             |    | ALL |   | 22.000 | 22.500 | 1.5    | 1.5 | 2.0 | 4.750 | 5.170 | 0.000 | 0.330 |
| PAINTER OVER 30FT   |    | ALL |   | 25.100 | 25.600 | 1.5    | 1.5 | 2.0 | 4.750 | 5.170 | 0.000 | 0.330 |
| PAINTER PWR EQMT    |    | ALL |   | 25.100 | 25.600 | 1.5    | 1.5 | 2.0 | 4.750 | 5.170 | 0.000 | 0.330 |
| PILEDRIVER          |    | BLD |   | 29.530 | 31.030 | 1.5    | 1.5 | 2.0 | 5.550 | 4.250 | 0.000 | 0.350 |
| PILEDRIVER          |    | HWY |   | 29.980 | 31.730 | 1.5    | 1.5 | 2.0 | 5.550 | 4.250 | 0.000 | 0.350 |
| PIPEFITTER          |    | BLD |   | 31.000 | 34.100 | 1.5    | 1.5 | 2.0 | 8.000 | 8.000 | 0.000 | 0.900 |
| PLASTERER           |    | BLD |   | 23.500 | 24.500 | 1.5    | 1.5 | 2.0 | 5.200 | 3.550 | 0.000 | 0.100 |
| PLUMBER             |    | BLD |   | 31.000 | 34.100 | 1.5    | 1.5 | 2.0 | 8.000 | 8.000 | 0.000 | 0.900 |
| ROOFER              |    | BLD |   | 21.200 | 22.200 | 1.5    | 1.5 | 2.0 | 6.700 | 3.800 | 0.000 | 0.000 |
| SHEETMETAL WORKER   |    | ALL |   | 28.080 | 29.580 | 1.5    | 1.5 | 2.0 | 6.350 | 5.650 | 1.690 | 0.260 |
| SPRINKLER FITTER    |    | BLD |   | 36.140 | 38.890 | 1.5    | 1.5 | 2.0 | 8.200 | 6.550 | 0.000 | 0.250 |
| STONE MASON         |    | BLD |   | 26.260 | 27.760 | 1.5    | 1.5 | 2.0 | 6.900 | 6.200 | 0.000 | 0.430 |
| TERRAZZO FINISHER   |    | BLD |   | 24.990 | 0.000  | 1.5    | 1.5 | 2.0 | 6.900 | 6.200 | 0.000 | 0.430 |
| TERRAZZO MASON      |    | BLD |   | 28.500 | 28.800 | 1.5    | 1.5 | 2.0 | 6.400 | 5.700 | 0.000 | 0.430 |
| TRUCK DRIVER        |    | ALL | 1 | 27.580 | 0.000  | 1.5    | 1.5 | 2.0 | 8.600 | 3.925 | 0.000 | 0.000 |
| TRUCK DRIVER        |    | ALL | 2 | 27.980 | 0.000  | 1.5    | 1.5 | 2.0 | 8.600 | 3.925 | 0.000 | 0.000 |
| TRUCK DRIVER        |    | ALL | 3 | 28.180 | 0.000  | 1.5    | 1.5 | 2.0 | 8.600 | 3.925 | 0.000 | 0.000 |
| TRUCK DRIVER        |    | ALL | 4 | 28.430 | 0.000  | 1.5    | 1.5 | 2.0 | 8.600 | 3.925 | 0.000 | 0.000 |
| TRUCK DRIVER        |    | ALL | 5 | 29.180 | 0.000  | 1.5    | 1.5 | 2.0 | 8.600 | 3.925 | 0.000 | 0.000 |
| TRUCK DRIVER        |    | O&C | 1 | 22.060 | 0.000  | 1.5    | 1.5 | 2.0 | 8.600 | 3.925 | 0.000 | 0.000 |
| TRUCK DRIVER        |    | O&C | 2 | 22.380 | 0.000  | 1.5    | 1.5 | 2.0 | 8.600 | 3.925 | 0.000 | 0.000 |
| TRUCK DRIVER        |    | O&C | 3 | 22.540 | 0.000  | 1.5    | 1.5 | 2.0 | 8.600 | 3.925 | 0.000 | 0.000 |
| TRUCK DRIVER        |    | O&C | 4 | 22.740 | 0.000  | 1.5    | 1.5 | 2.0 | 8.600 | 3.925 | 0.000 | 0.000 |
| TRUCK DRIVER        |    | O&C | 5 | 23.340 | 0.000  | 1.5    | 1.5 | 2.0 | 8.600 | 3.925 | 0.000 | 0.000 |
| TUCKPOINTER         |    | BLD |   | 26.260 | 27.760 | 1.5    | 1.5 | 2.0 | 6.900 | 6.200 | 0.000 | 0.430 |

Legend:

M-F>8 (Overtime is required for any hour greater than 8 worked each day, Monday through Friday.)

OSA (Overtime is required for every hour worked on Saturday)

OSH (Overtime is required for every hour worked on Sunday and Holidays)

H/W (Health & Welfare Insurance)

Pensn (Pension)

Vac (Vacation)

Trng (Training)

## Explanations

### JEFFERSON COUNTY

The following list is considered as those days for which holiday rates of wages for work performed apply: New Years Day, Memorial/Decoration Day, Fourth of July, Labor Day, Veterans Day, Thanksgiving Day, Christmas Day. Generally, any of these holidays which fall on a Sunday is celebrated on the following Monday. This then makes work performed on that Monday payable at the appropriate overtime rate for holiday pay. Common practice in a given local may alter certain days of celebration such as the day after Thanksgiving for Veterans Day. If in doubt, please check with IDOL.

Oil and chip resealing (O&C) means the application of road oils and liquid asphalt to coat an existing road surface, followed by application of aggregate chips or gravel to coated surface, and subsequent rolling of material to seal the surface.

### EXPLANATION OF CLASSES

ASBESTOS - GENERAL - removal of asbestos material/mold and hazardous materials from any place in a building, including mechanical systems where those mechanical systems are to be removed. This includes the removal of asbestos materials/mold and hazardous materials from ductwork or pipes in a building when the building is to be demolished at the time or at some close future date.

ASBESTOS - MECHANICAL - removal of asbestos material from mechanical systems, such as pipes, ducts, and boilers, where the mechanical systems are to remain.

### CERAMIC TILE FINISHER, MARBLE FINISHER, TERRAZZO FINISHER

Assisting, helping or supporting the tile, marble and terrazzo mechanic by performing their historic and traditional work assignments required to complete the proper installation of the work covered by said crafts. The term "Ceramic" is used for naming the classification only, and is in no a limitation of the product handled. Ceramic takes into consideration most hard tiles.

### ELECTRONIC SYSTEMS TECHNICIAN

Installation, service and maintenance of low-voltage systems which utilizes the transmission and/or transference of voice, sound, vision,

or digital for commercial, education, security and entertainment purposes for the following: TV monitoring and surveillance, background/foreground music, intercom and telephone interconnect, field programming, inventory control systems, microwave transmission, multi-media, multiplex, radio page, school, intercom and sound burglar alarms and low voltage master clock systems.

Excluded from this classification are energy management systems, life safety systems, supervisory controls and data acquisition systems not intrinsic with the above listed systems, fire alarm systems, nurse call systems and raceways exceeding fifteen feet in length.

#### TRUCK DRIVER - BUILDING, HEAVY AND HIGHWAY CONSTRUCTION

Class 1. Drivers on 2 axle trucks hauling less than 9 ton. Air compressor and welding machines and brooms, including those pulled by separate units, truck driver helpers, warehouse employees, mechanic helpers, greasers and tiremen, pickup trucks when hauling materials, tools, or workers to and from and on-the-job site, and fork lifts up to 6,000 lb. capacity.

Class 2. Two or three axle trucks hauling more than 9 ton but hauling less than 16 ton. A-frame winch trucks, hydrolift trucks, vector trucks or similar equipment when used for transportation purposes. Fork lifts over 6,000 lb. capacity, winch trucks, four axle combination units, and ticket writers.

Class 3. Two, three or four axle trucks hauling 16 ton or more. Drivers on water pulls, articulated dump trucks, mechanics and working forepersons, and dispatchers. Five axle or more combination units.

Class 4. Low Boy and Oil Distributors.

Class 5. Drivers who require special protective clothing while employed on hazardous waste work.

#### TRUCK DRIVER - OIL AND CHIP RESEALING ONLY.

This shall encompass laborers, workers and mechanics who drive contractor or subcontractor owned, leased, or hired pickup, dump, service, or oil distributor trucks. The work includes transporting materials and equipment (including but not limited to, oils, aggregate supplies, parts, machinery and tools) to or from the job site; distributing oil or liquid asphalt and aggregate; stock piling material when in connection with the actual oil and chip contract. The Truck Driver (Oil & Chip Resealing) wage classification does not include supplier delivered materials.

#### OPERATING ENGINEERS

GROUP I. Cranes, Dragline, Shovels, Skimmer Scoops, Clamshells or Derrick Boats, Pile Drivers, Crane-Type Backhoes, Asphalt Plant Operators, Concrete Plant Operators, Dredges, Asphalt Spreading Machines, All Locomotives, Cable Ways, or Tower Machines, Hoists, Hydraulic Backhoes, Ditching Machines or Backfiller, Cherrypickers, Overhead Cranes, Roller, Steam or Gas, Concrete Pavers, Excavators, Concrete Breakers, Concrete Pumps, Bulk Cement Plants, Cement Pumps, Derrick-Type Drills, Boat Operators, Motor Graders or Pushcats, Scoops or Tournapulls, Bulldozers, Endloaders or Fork Lifts, Power Blade or Elevating Graders, Winch Cats, Boom or Winch Trucks or Boom Tractors, Pipe Wrapping or Painting Machines, Asphalt Plant Engineer, Journeyman Lubricating Engineer, Drills (other than Derrick Type), Mud Jacks, or Well Drilling Machines, Boring Machines or Track Jacks, Mixers, Conveyors (Two), Air Compressors (Two), Water Pumps regardless of size (Two), Welding Machines (Two), Siphons or Jets

(Two), Winch Heads or Apparatuses (Two), Light Plants (Two), Waterblasters (two), All Tractors regardless of size (straight tractor only), Fireman on Stationary Boilers, Automatic Elevators, Form Grading Machines, Finishing Machines, Power Sub-Grader or Ribbon Machines, Longitudinal Floats, Distributor Operators on Trucks, Winch Heads or Apparatuses (One), Mobil Track air and heaters (two to five), Heavy Equipment Greaser, Relief Operator, Assistant Master Mechanic and Heavy Duty Mechanic, all Operators (except those listed below).

GROUP II. Assistant Operators.

GROUP III. Air Compressors (One), Water Pumps, regardless of Size (One), Waterblasters (one), Welding Machine (One), Mixers (One Bag), Conveyor (One), Siphon or Jet (One), Light Plant (One), Heater (One), Immobile Track Air (One), and Self Propelled Walk-Behind Rollers.

GROUP IV. Asphalt Spreader Oilers, Fireman on Whirlies and Heavy Equipment Oilers, Truck Cranes, Dredges, Monigans, Large Cranes - (Over 65-ton rated capacity) Concrete Plant Oiler, Blacktop Plant Oiler, and Creter Crane Oiler (when required).

GROUP V. Oiler.

GROUP VI. Master Mechanics, Operators on equipment with Booms, including jibs, 100 feet and over, and less than 150 feet long.

GROUP VII. Operators on equipment with Booms, including jibs, 150 feet and over, and less than 200 feet long.

GROUP VIII. Operators on Equipment with Booms, including jibs, 200 feet and over; Tower Cranes; Whirlie Cranes; and Operator Foreman.

Other Classifications of Work:

For definitions of classifications not otherwise set out, the Department generally has on file such definitions which are available. If a task to be performed is not subject to one of the classification of pay set out, the Department will upon being contacted state which neighboring county has such a classification and provide such rate, such rate being deemed to exist by reference in this document. If no neighboring county rate applies to the task, the Department shall undertake a special determination, such special determination being then deemed to have existed under this determination. If a project requires these, or any classification not listed, please contact IDOL at 618/993-7271 for wage rates or clarifications.

#### LANDSCAPING

Landscaping work falls under the existing classifications for laborer, operating engineer and truck driver. The work performed by landscape plantsman and landscape laborer is covered by the existing classification of laborer. The work performed by landscape operators (regardless of equipment used or its size) is covered by the classifications of operating engineer. The work performed by landscape truck drivers (regardless of size of truck driven) is covered by the classifications of truck driver.