

SCOPE OF WORK

THE PROPOSED IMPROVEMENTS INCLUDE THE CONSTRUCTION OF A NEW ELECTRICAL VAULT, REMOVAL OF THE EXISTING ELECTRICAL VAULT, GROUNDING UPGRADES TO AND REFURBISHMENT OF AIRFIELD EQUIPMENT, REMOVAL AND REPLACEMENT OF PRIMARY WIND CONE, AND MODIFICATIONS TO THE ROTATING BEACON.

CONTRACTOR'S RESPONSIBILITIES

GENERAL :
THE CONTRACTOR MUST FOLLOW PROCEDURES ON THIS SHEET THAT ASSURES SAFE OPERATING CONDITIONS FOR AIRCRAFT AS WELL AS HIS PERSONNEL AND EQUIPMENT. THE AIRPORT MANAGER WILL AT ALL TIMES HAVE JURISDICTION OVER THE SAFETY OF AIR TRAFFIC DURING CONSTRUCTION.

IDENTIFICATION - THE CONTRACTOR IS REQUIRED TO MARK ALL VEHICLES AND EQUIPMENT USED FOR CONSTRUCTION WITH 3 FT. SQUARE, INTERNATIONAL ORANGE AND WHITE CHECKERED FLAGS ANYTIME THEY ARE ON AIRPORT PROPERTY.

ALL CONTRACTOR PERSONNEL SHALL HAVE IDENTIFICATION MAKING IT OBVIOUS THAT THEY ARE A PART OF THE CONSTRUCTION CREW.

THE CONTRACTOR, HIS EMPLOYEES, AND EQUIPMENT SHALL BE RESTRICTED TO THE PROJECT WORK AREA.

RADIO CONTROL - THE CONTRACTOR IS REQUIRED TO BE IN TWO-WAY RADIO CONTACT WITH THE KEWANE MUNCIPAL AIRPORT UNICOM (122.80 MHZ) WHENEVER HIS PERSONNEL ARE ON THE AIRPORT PROPERTY.

EQUIPMENT PARKING AND STORAGE - THE CONTRACTOR'S EQUIPMENT PARKING, MATERIAL STORAGE, AND EMPLOYEE PARKING WILL BE AT THE LOCATION SHOWN ON THIS DRAWING. ONLY VEHICLES AND EQUIPMENT NECESSARY FOR CONSTRUCTION WILL BE PERMITTED TO LEAVE THESE AREAS.

THE CONTRACTOR IS REQUIRED TO LIMIT THE USE OF CONSTRUCTION EQUIPMENT ON THE EXISTING PAVEMENTS. ONLY EQUIPMENT NEEDED TO COMPLETE THE SPECIFIC WORK ON THE EXISTING PAVEMENT WILL BE PERMITTED. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE TO EXISTING PAVEMENTS CAUSED BY HIS PERSONNEL OR EQUIPMENT.

NO OPEN HOLES OR TRENCHES WILL BE ALLOWED WITHIN 200' OF AN ACTIVE RUNWAY, WITHIN 40' OF AN ACTIVE TAXIWAY OR TAXILANE, NOR WILL EITHER HOLES OR OPEN TRENCHES BE ALLOWED TO REMAIN EXPOSED OR OPEN OVER NIGHT.

TRAFFIC CONES

TRAFFIC CONES WILL BE USED ON A CLOSED RUNWAY FOR TRAFFIC CONTROL IN LIEU OF BARRICADES.

AIRCRAFT OPERATION LINE

THE CONTRACTOR WILL ESTABLISH THE AIRCRAFT OPERATIONAL AREA FOR EACH OF THE AIRPORT RUNWAYS, IN THE PROPOSED WORK AREAS, BY PLACING A ROW OF LATH, 200 FT. FROM EACH OF THE CENTERLINES. NO PERSONNEL OR EQUIPMENT WILL CROSS THE LATH ROW WITHOUT CLOSING THE RESPECTIVE RUNWAY. THE COST OF PLACING THE LATH WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

HORIZONTAL AND VERTICAL DATA				
NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
1	KEWPORT AZ (NGS) MARKER, ALUM. ROD	1,652,491.649	2,349,792.846	850.62
2	CHISELED "□" ON INSPECTION HOLE	-	-	848.87
3	KEWPORT (NGS) MARKER, ALUM. ROD	1,652,431.715	2,352,728.103	854.13
4	CHISELED "□" ON EAST END OF CONC. SIGN BASE	-	-	854.76
5	CHISELED "□" ON WEST END OF CONC. SIGN BASE	-	-	851.21

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

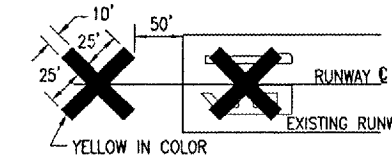
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.

150-ENGINEER'S FIELD OFFICE NOTES

AN ENGINEER'S FIELD OFFICE WILL NOT BE REQUIRED FOR THIS PROJECT.

TEMPORARY RUNWAY CLOSURE NOTE

COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE RESIDENT ENGINEER. THE CROSSES WILL BE PLACED AS SHOWN ON THIS SHEET AND SECURED IN A MANNER APPROVED BY THE RESIDENT ENGINEER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION TO THE CONTRACT WILL BE ALLOWED.



CROSSES MAY BE PLACED OVER THE NUMERALS OR OFF THE RUNWAY END AS DETAILED.

DETAIL OF CROSS FOR CLOSED RUNWAY

"NOT TO SCALE"

RUNWAY CLOSURE PROCEDURES:

- * CONTACT THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
- * ISSUANCE OF NOTAM BY THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
- * PLACEMENT OF CROSSES (SEE DETAIL THIS SHEET).
- * PLACEMENT OF LIGHTED BARRICADES. ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS WITHIN 200 FT. OF THE AFFECTED RUNWAY CENTERLINE BEGIN.

RUNWAY RE-OPENING PROCEDURES:

- * REMOVE CROSSES.
- * REMOVE LIGHTED BARRICADES.
- * NOTIFY THE AIRPORT MANAGER OR HIS REPRESENTATIVE TO CANCEL THE NOTAM.
- * CANCELLATION OF THE NOTAM. A CLOSED RUNWAY WILL NOT BE RE-OPENED UNTIL ALL EQUIPMENT AND WORK ARE FURTHER THAN 200 FT. FROM THE AFFECTED RUNWAY CENTERLINE.

RUNWAY AND TAXIWAY CLOSURE NOTE:

RUNWAY 9-27 AND 1-19 WILL BE CLOSED ANY TIME CONSTRUCTION ACTIVITIES ARE REQUIRED WITHIN 200' OF THE RESPECTIVE RUNWAY CENTERLINE, IN ACCORDANCE WITH THE PROCEDURES SHOWN ON THIS PROPOSED SAFETY PLAN. HOWEVER, ONE RUNWAY WILL REMAIN OPEN AT ALL TIMES. TAXIWAY "B" AND "C", AS WELL AS THE PORTION OF THE RAMP AFFECTED BY THE PROJECT, WILL BE CLOSED ANY TIME CONSTRUCTION ACTIVITIES ARE WITHIN 40' OF THE TAXILANE OR TAXIWAY CENTERLINE.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FT. THE TALLEST EQUIPMENT IS EXPECTED TO BE A CRANE OR BUCKET TRUCK USED TO WORK ON THE AIRPORT PRIMARY WIND CONE.

HAUL ROUTE AND EQUIPMENT PARKING

THE CONTRACTOR IS REQUIRED TO CONSTRUCT A TEMPORARY HAUL ROUTE AND EQUIPMENT PARKING AREA IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND AT THE LOCATION SHOWN ON THIS SHEET. THE HAUL ROUTE AND EQUIPMENT PARKING AREA SHALL BE ESTABLISHED AS FAR SOUTH AND EAST OF RUNWAY 1-19 AS IS PRACTICAL.

THE DESIGNATED HAUL ROUTE SHALL BE THE ONLY ACCESS USED BY THE CONTRACTOR OR HIS EMPLOYEES.

THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS TO USE STATE, CITY, COUNTY, OR TOWNSHIP ROADS.

THE CONTRACTOR AND HIS PERSONNEL SHALL ACCESS THE AIRPORT THROUGH AN EXISTING GATE LOCATED OFF MIDLAND ROAD. THE CONTRACTOR WILL ENSURE THE GATE IS CLOSED AND LOCKED AT THE END OF EACH DAY.

THE INSTALLATION OF THE PROPOSED LIGHTING HOMERUN CABLES THROUGH OR ACROSS THE HAUL ROUTE WILL REQUIRE COORDINATION BETWEEN CONTRACTORS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR LABOR OR MATERIAL ASSOCIATED WITH THE COORDINATION OR INSTALLATION FOR WORK BEYOND THAT IS SPECIFIED.

ALL WORK ASSOCIATED WITH THE PROPOSED HAUL ROUTE SHALL BE CONSIDERED INCIDENTAL TO AR150540 - HAUL ROUTE.

EROSION CONTROL

THE PROPOSED CONSTRUCTION WILL DISTURB LESS THAN 1 ACRES OF LAND, THEREFORE AN EROSION CONTROL PLAN AND AN NPDES PERMIT ARE NOT REQUIRED.

OPERATIONAL LIGHTING

ALL EXISTING AIRFIELD CIRCUITS SHALL BE OPERABLE DURING NIGHTFALL. CONTRACTOR SHALL PROVIDE ALL TEMPORARY WORK, AS NECESSARY, TO MAINTAIN OPERATION OF THE AIRFIELD LIGHTING SYSTEMS AT NIGHTFALL. ALL WORK SHALL BE COORDINATED WITH THE AIRPORT MANAGER AND SHALL BE COORDINATED TO MINIMIZE DOWN TIME TO THE RESPECTIVE LIGHTING SYSTEMS.

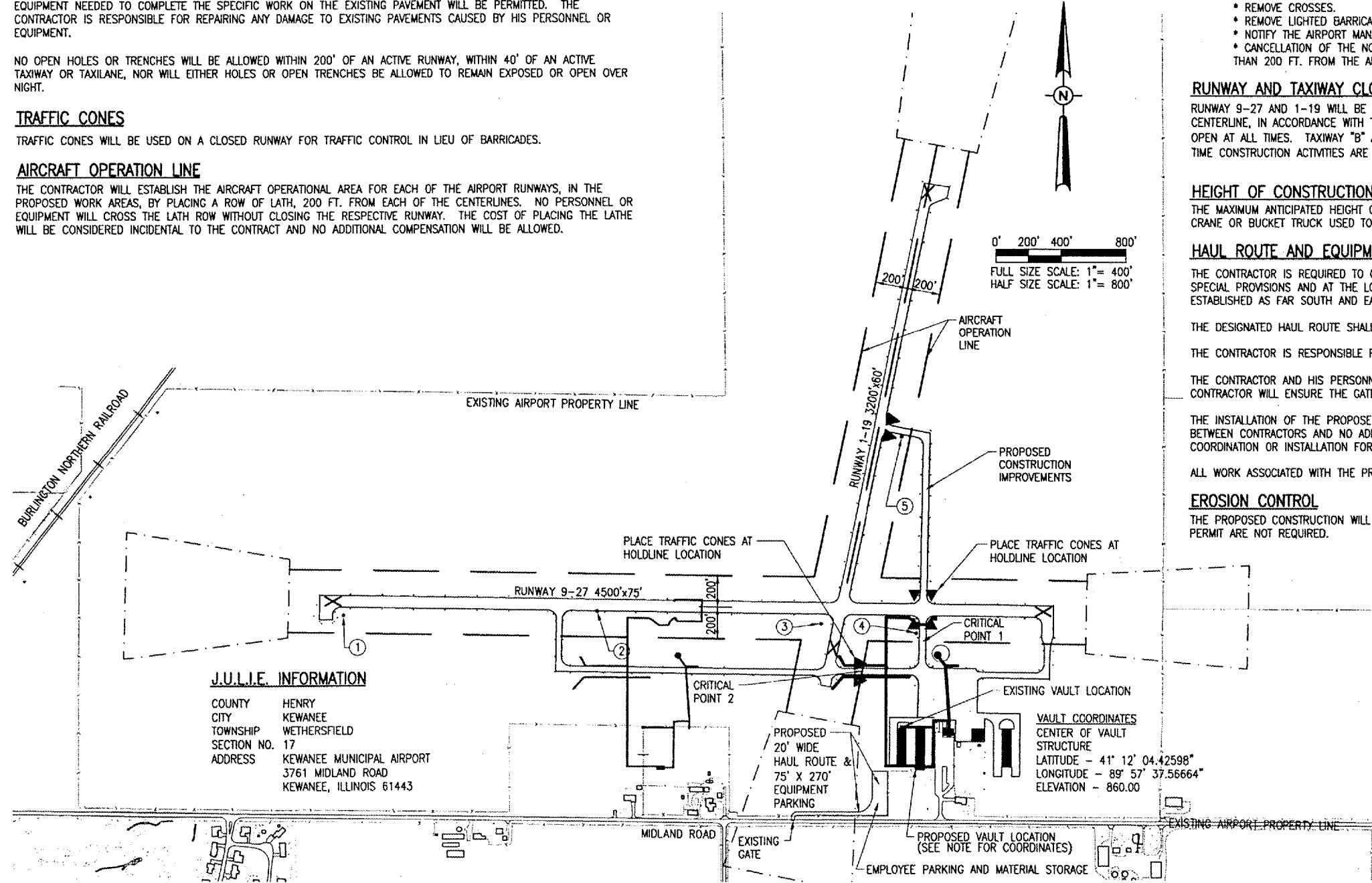
LEGEND

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED EQUIPMENT/VEHICLE PARKING AREA & HAUL ROUTE
- EXISTING AIRPORT PROPERTY LINE
- LIGHTED BARRICADES
- HORIZONTAL/VERTICAL CONTROL

CRITICAL POINT DATA

CRITICAL POINT 1
 LATITUDE - 41° 12' 12.08227"
 LONGITUDE - 89° 57' 37.36973"
 ELEVATION - 854.90

CRITICAL POINT 2
 LATITUDE - 41° 12' 10.29760"
 LONGITUDE - 89° 57' 42.21939"
 ELEVATION - 855.10



J.U.L.I.E. INFORMATION

COUNTY HENRY
 CITY KEWANE
 TOWNSHIP WETHERSFIELD
 SECTION NO. 17
 ADDRESS KEWANE MUNICIPAL AIRPORT
 3761 MIDLAND ROAD
 KEWANE, ILLINOIS 61443

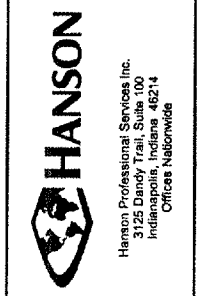
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 I:\AIRPORTS\KEWANE\810-06VLTD\ELECTRICAL\810-06VLTD - Safety Plan

KE008

DATE	REVISION	BY

KEWANE MUNICIPAL AIRPORT
 KEWANE, HENRY COUNTY, ILLINOIS
 I.L. PROJ.: E71-3586 A.I.P. PROJ.: 3-17-0058-B12

PROJECT NO.	DATE	BY	REVIEWED
810-06VLTD	04/01/08	RA.W.	08/01/05
R-101SAF.DWG	04/01/08	BA.K.	08/01/05
Scale 1"=400'	04/01/08	C.A.H.	04/27/06



CONSTRUCT VAULT
 PROPOSED SAFETY PLAN
 3
 3 of 27 sheets