

INDEX OF SHEETS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE CITY OF CHICAGO

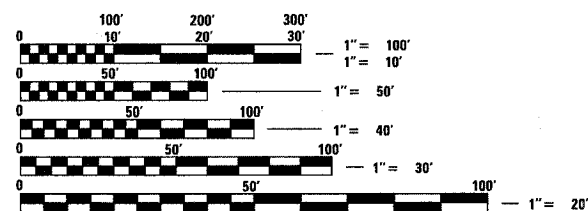
DAN RYAN EXPRESSWAY:	ADT (2010)	DESIGN SPEED	POSTED SPEED												
NB I-94 (DAN RYAN)	151,800	60 MPH	55 MPH												
NB I-94 (BISHOP FORD)	94,200	60 MPH	55 MPH												
NB I-57	58,000	60 MPH	55 MPH												
NB HALSTED STREET ENTRANCE RAMP	22,200	45 MPH	40 MPH												
NB MICHIGAN AVENUE ENTRANCE RAMP	9,000	45 MPH	40 MPH												
NB 95TH STREET ENTRANCE RAMP	10,600	45 MPH	40 MPH												
NB 87TH STREET EXIT RAMP	11,600	45 MPH	40 MPH												
NB 87TH STREET ENTRANCE RAMP	14,300	45 MPH	40 MPH												
NB 83RD STREET C-D ENTRANCE RAMP	8,100	45 MPH	40 MPH												
NB 79TH STREET C-D EXIT RAMP	14,000	45 MPH	40 MPH												
NB 79TH STREET C-D ENTRANCE RAMP	9,200	45 MPH	40 MPH												
NB 76TH STREET C-D EXIT RAMP	7,200	45 MPH	40 MPH												
NB 75TH STREET C-D ENTRANCE RAMP	11,300	45 MPH	40 MPH </tr <tr> <td>NB 71ST STREET C-D EXIT RAMP</td> <td>6,600</td> <td>45 MPH</td> <td>40 MPH</td> </tr> <tr> <td>EB I-57 TO I-94 CROSS-CONNECTOR</td> <td>22,800</td> <td>50 MPH</td> <td>50 MPH</td> </tr> <tr> <td>WB I-94 TO I-57 CROSS-CONNECTOR</td> <td>24,900</td> <td>50 MPH</td> <td>50 MPH</td> </tr>	NB 71ST STREET C-D EXIT RAMP	6,600	45 MPH	40 MPH	EB I-57 TO I-94 CROSS-CONNECTOR	22,800	50 MPH	50 MPH	WB I-94 TO I-57 CROSS-CONNECTOR	24,900	50 MPH	50 MPH
NB 71ST STREET C-D EXIT RAMP	6,600	45 MPH	40 MPH												
EB I-57 TO I-94 CROSS-CONNECTOR	22,800	50 MPH	50 MPH												
WB I-94 TO I-57 CROSS-CONNECTOR	24,900	50 MPH	50 MPH												

PROJECT DESCRIPTION

THE PROPOSED IMPROVEMENT CONSISTS OF PAVEMENT AND CTA BARRIER WALL RECONSTRUCTION, WIDENING, RAMP TERMINALS, SHOULDERS, AND DRAINAGE MODIFICATIONS ALONG NB I-94 (DAN RYAN & BISHOP FORD) AND NB I-57; AND PAVEMENT RESURFACING ALONG NB I-94 (BISHOP FORD) AND WB I-94 TO I-57 CONNECTOR.

S.N. 016-2852: NORTHBOUND I-57 BRIDGE OVER WESTBOUND I-94 TO I-57 CONNECTOR - RECONSTRUCTION

S.N. 016-0073: NORTHBOUND I-57 OVER SOUTHBOUND BISHOP FORD & CTA TUNNELS - ROOF REPAIRS AND WATERPROOFING



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES; REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED

DIGGER:
CHICAGO UTILITY ALERT NETWORK
(312) 744-7000

CTA CONTACT:
MARVIN A. WATSON,
GENERAL MANAGER, CONSTRUCTION
(312) 681-3860

CONTRACT NO. 62304

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PLANS

F.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)

SECTION (1516.1, 1717, & 1818) R-4

PROJECT NO.: IM-94-3(402)061

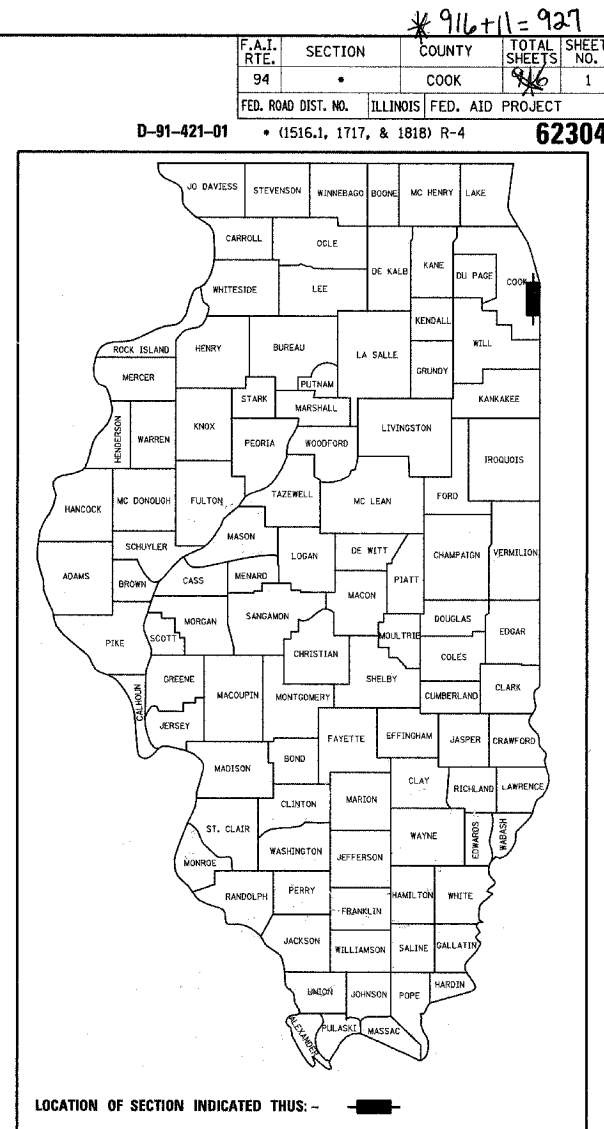
I-57 TO 79TH STREET

NB LANES 1 - 5, SHOULDERS & CTA BARRIER WALL

COOK COUNTY

C-91-421-01

VOL. 1 OF 2



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 8 2006

Diane M. O'Keefe / cm
DISTRICT ENGINEER

20
ENGINEER OF DESIGN AND ENVIRONMENT

20
DIRECTOR, DIVISION OF HIGHWAYS

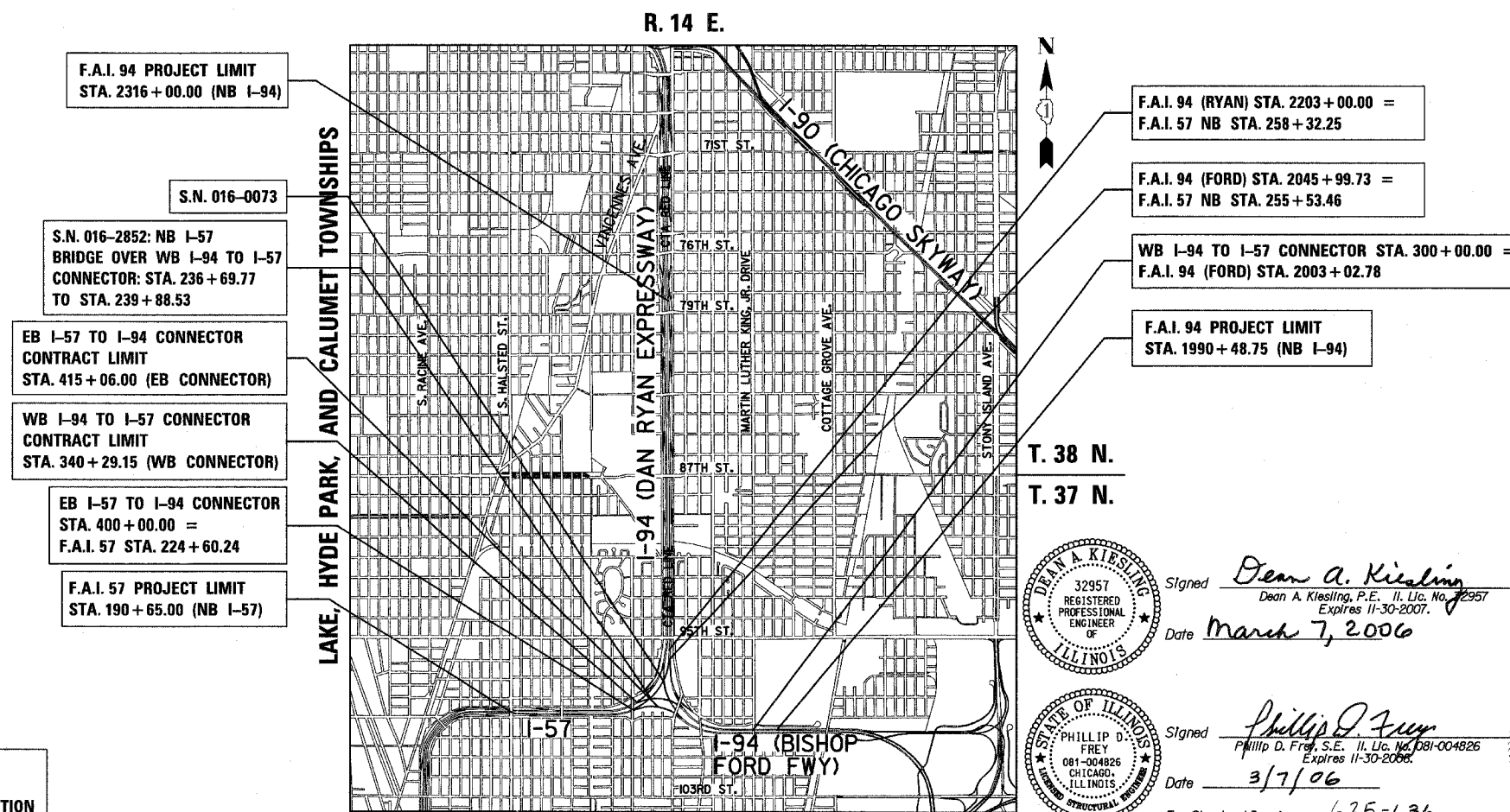
CONTRACT 20

Signed Dean A. Kieseling
Dean A. Kieseling, P.E., Ill. Lic. No. 72957
Expires 11-30-2007.
Date March 7, 2006

Signed Thomas Mikolajewski
Thomas Mikolajewski, P.E., Ill. Lic. No. 062-056151
Expires 11-30-2007.
Date March 7, 2006
For Electrical Drawings 478-524

Signed Phillip D. Frey
Phillip D. Frey, S.E., Ill. Lic. No. 081-004826
Expires 11-30-2006.
Date 3/7/06
For Structural Drawings 625-636

Signed Bryan T. Danley
Bryan T. Danley, Ill. Lic. No. 081-004732
Date 3/7/06
For Structural Drawings 562-624



LOCATION MAP

GROSS LENGTH OF PROJECT = 29153.4 FT. = 5.521 MI.
NET LENGTH OF PROJECT = 29153.4 FT. = 5.521 MI.

DISTRICT ONE DESIGN / PROJECT MANAGER:
BRIAN KUTTAB (847) 705 - 4431

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	3
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
STA. 1990+48 (NB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				

62304

GENERAL NOTES:

- UTILITY LOCATIONS SHOWN ON THESE PLANS MAY NOT BE CORRECT OR COMPLETE. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE RESPECTIVE UTILITIES OF THE CITY OF CHICAGO. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL THE CHICAGO UTILITY ALERT NETWORK AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, CABLE, AND GAS FACILITIES (48 HOURS NOTIFICATION IS REQUIRED). CONTACT THE CHICAGO DEPARTMENT OF WATER MANAGEMENT PERMIT SECTION AT (312) 747-7893 FOR WATER AND SEWER LOCATIONS.
 - WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKS AND MONUMENTS. THE ENGINEER OR AN AUTHORIZED SURVEYOR AGENT WILL WITNESS OR OTHERWISE REFERENCE AND RESET MONUMENTS AS NECESSARY. ALL PROPERTY CORNERS EXCEPT THOSE WITHIN AREAS WHERE THE SCHEDULE, IF PROVIDED, SHOWS PLACEMENT OF R.O.W. MARKERS SHALL REMAIN UNDISTURBED.
 - THE CONTRACTOR SHALL NOT SET UP A YARD OR FIELD OFFICE ON IDOT PROPERTY WITHOUT WRITTEN PERMISSION FROM IDOT.
 - THE CONTRACTOR SHALL TAKE ALL NECESSARY SAFETY PRECAUTIONS TO PROTECT AND PROVIDE ACCESS TO ADJUTING PROPERTY, UTILITIES, PEDESTRIANS, AND VEHICULAR TRAFFIC.
 - NIGHT OPERATIONS: WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTION IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AS WELL AS THE ADJOINING RESIDENTIAL AREAS.
 - ALL ELEVATIONS SHOWN ARE BASED ON THE CHICAGO CITY DATUM OF 0.00, WHICH IS 579.19 FEET ABOVE MEAN TIDE NEW YORK. (NAVD 88)
7. THE CONTRACTOR SHALL MAINTAIN THE SURFACE DRAINAGE OF ALL ROADWAYS DURING CONSTRUCTION OF THIS PROJECT. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS, INLETS AND CATCH BASINS. HE SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE SAME. HE SHALL PROVIDE AND MAINTAIN A TEMPORARY OUTLET, AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL INSTALLATION IS COMPLETE INCLUDING PAVEMENT. THIS WORK SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT. COORDINATION WITH IDOT CONTRACT 62593 REQUIRED.
- ON STATE STANDARD 483001, SUB-BASE GRANULAR MATERIAL, TYPE B 24" SHALL BE USED AS THE IMPROVED SUBGRADE.
 - ALL STORM SEWER CONNECTIONS WITH PIPES 27 INCH DIAMETER AND SMALLER SHALL BE MADE WITH PRECAST "TEE" OR "WYE" PIPES. FOR PROPOSED STORM SEWER PIPES LARGER THAN 27 INCH DIAMETER, OPENINGS OF THE SPECIFIED DIAMETER SHALL BE MADE IN THE PIPE AT THE TIME IT IS MANUFACTURED. PRECAST "TEE" AND "WYE" PIPE CONNECTIONS FOR STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST FOR THE STORM SEWERS.
 - 10' TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB & GUTTER AND MEDIAN ITEMS TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24" UNLESS OTHERWISE SHOWN.
 - NO PAYMENT WILL BE MADE FOR RESTORATION BEYOND THE LIMITS SHOWN ON THE PLANS.
 - THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
 - WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 V:H.
 - SPECIAL ATTENTION IS CALLED TO ARTICLE 107.12 AND SPECIAL PROVISIONS ENTITLED "RAILROAD PROTECTIVE LIABILITY INSURANCE" AND "CTA COORDINATION" REGARDING FLAGGERS AND WORK PERFORMED ADJACENT TO THE CTA PASSENGER TRAIN RAIL FACILITIES.
 - A PAINT STRIPE SHALL BE APPLIED TO THE FACE OF BARRIER WALL ADJACENT TO ALL DRAINAGE STRUCTURES. THE STRIPE SHALL BE 4" WIDE BY 12" LONG, AND EXTEND VERTICALLY AT 90 DEGREES FROM THE TOP EDGE OF THE BARRIER. THE PAINT WILL BE SPECIFIED AS ORANGE PAINT AND SHALL BE ZINC OXIDE. COST OF THE STRIPE IS TO BE INCLUDED IN THE COST OF THE CONCRETE BARRIER.
 - STORM SEWERS TO BE REMOVED SHALL NOT BE SALVAGED.
 - THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS SURFACE MIXTURES IS 112 LB/SQ YD/IN.
 - ANY WASTE GENERATED AS A SPECIAL WASTE OR A WASTE NOT CERTIFIED AS A NON-SPECIAL WASTE FROM THIS PROJECT SHOULD BE MANIFESTED OFF-SITE USING THE GENERATOR NUMBER ASSOCIATED WITH COOK COUNTY, WHICH IS 0318995023.

GENERAL NOTES (CONT.):

- THE CONTRACTOR SHALL BE AWARE THAT MANY CITY OF CHICAGO SEWERS ARE LOCATED IN THE PROPOSED SUB-BASE OR A SHORT DISTANCE BELOW THE SUBGRADE. THE CONTRACTOR SHALL PROTECT THESE FACILITIES FROM DAMAGE DURING CONSTRUCTION OPERATIONS AND SHALL BE RESPONSIBLE FOR ANY DAMAGE AND REPAIR DURING CONSTRUCTION. GAS, ELECTRIC AND TELEPHONE FACILITIES ARE ALSO LOCATED BELOW THE AREA OF PROPOSED CONSTRUCTION. DURING CONSTRUCTION, THE CONTRACTOR SHALL COORDINATE WITH THE PRIVATE UTILITIES (GAS, ELECTRIC AND TELEPHONE) SO THAT THESE UTILITIES MAY PROVIDE APPROPRIATE PROTECTION FOR THEIR FACILITIES. ANY DAMAGE DONE TO EXISTING UTILITIES BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
- THE LOCATIONS OF VARIOUS ITEMS SUCH AS PAVEMENT, BARRIER WALLS AND DRAINAGE STRUCTURES BUILT IN CONTRACTS UNDER CONSTRUCTION DURING THE PREPARATION OF THESE PLANS ARE BASED ON THE PUBLISHED CONTRACT PLAN DRAWINGS AVAILABLE DURING DESIGN. THE CONTRACTOR MUST FIELD VERIFY LIMITS, LOCATIONS AND ELEVATIONS OF THESE PREVIOUSLY CONSTRUCTED ITEMS.
- LOCATIONS OF ACCESS CONTROL FENCING AS SHOWN ON THE PLANS MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER TO BETTER FIT FIELD CONDITIONS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT NO GAP REMAINS BETWEEN PROPOSED FENCING OR WHERE PROPOSED FENCING TERMINATES AND EXISTING FENCE REMAINS IN PLACE.
- TEMPORARY CONCRETE BARRIER: THE BARRIER UNIT AT EACH END OF THE INSTALLATIONS SHALL BE SECURED TO THE PAVEMENT OR SHOULDER USING THREE (3) ANCHORING PINS FOR F SHAPE OR THREE (3) DOWEL BARS FOR NEW JERSEY SHAPE.
- HAMMER DRIVING OF PILES WILL NOT BE ALLOWED.
- CRUSHING PLANT AND CONCRETE PLANT LOCATIONS REQUIRE CITY OF CHICAGO APPROVAL.
- ALL PROPOSED HIGH MAST LIGHT TOWER FOUNDATIONS CONSTRUCTED BY OTHERS ARE SHOWN AS EXISTING IN THIS CONTRACT. HOWEVER, DUE TO THE VARYING PROJECT SCHEDULES ALL HIGH MAST LIGHT TOWER FOUNDATIONS MAY NOT BE CONSTRUCTED AT THE START OF THIS CONTRACT.
- ALL PAVEMENT SHALL BE CONSTRUCTED USING THE SPECIAL PROVISION "EXTENDED LIFE CONCRETE PAVEMENT (30 YEAR)".

GENERAL NOTES - LANDSCAPE REQUIREMENTS:

- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD.
- AREAS TO BE SEEDED BETWEEN NOVEMBER 1 AND APRIL 1 SHALL REQUIRE DORMANT SEEDING, WHICH SHALL BE INCLUDED IN THE COST OF SEEDING, CLASS 2A.

GENERAL NOTES - SEDIMENT AND EROSION CONTROL REQUIREMENTS:

- EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES, WHICH WILL POTENTIALLY CREATE ERODABLE CONDITIONS.
- THE EROSION CONTROL MEASURE SHOWN ARE BUT A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURE. DEVIATIONS FROM THIS PLAN ARE TO BE EXPECTED PENDING A JOBSITE INSPECTION BETWEEN THE CONTRACTOR AND THE DEPARTMENT.
- THE CONTRACTOR WILL BE REQUIRED TO IMPLEMENT AND MAINTAIN SEDIMENT CONTROL MEASURE PRIOR TO STRIPPING EXISTING VEGETATION.

PROJECT COMMITMENTS:

IDOT HAS MADE THE FOLLOWING COMMITMENTS FOR THE PROJECT:

IDOT HAS COMMITTED TO ADDRESS CONSTRUCTION RELATED AIR QUALITY CONCERNS. THESE STRATEGIES INCLUDE REQUIRING DETAILED DUST CONTROL PLANS, REQUIRING THE USE OF CLEANER BURNING DIESEL FUELS ON CERTAIN DIESEL POWERED CONSTRUCTION EQUIPMENT AND/OR THE INSTALLATION OF EXHAUST EMISSION SCRUBBERS, AND THE REDUCTION OF CONSTRUCTION EQUIPMENT IDLING TIMES. THESE STRATEGIES ARE ADDRESSED IN SPECIAL PROVISIONS INCLUDED IN THIS CONTRACT AND DEVELOPED FOR THE DAN RYAN RECONSTRUCTION PROJECT.

IDOT HAS COMMITTED TO REDUCE TIRE-PAVEMENT HIGHWAY TRAFFIC NOISE FOR THE FINISHED PROJECT BY INCLUDING A SPECIAL PROVISION TO INCORPORATE VARIABLE WIDTH AND SKEWED "TINING" OF THE NEW CONCRETE PAVEMENT.

GENERAL NOTES - CITY OF CHICAGO:

- ALL CATCH BASINS IN THE CITY OF CHICAGO MUST MEET THE DEPARTMENT OF WATER MANAGEMENT'S STANDARDS.
- SEWER SIZES 21" DIAMETER OR SMALLER MUST BE EXTRA STRENGTH VITRIFIED CLAY PIPE C-700 OR DUCTILE IRON PIPE WITH PUSH-ON OR MECHANICAL JOINTS. SEWER SIZES 24" DIAMETER OR LARGER MUST BE REINFORCED CONCRETE PIPE TYPE C-76, CLASS III, WALL "B" WITH "O-RING" JOINTS.
- PERMITS FROM THE DEPARTMENT OF WATER MANAGEMENT ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY AND COMBINED SEWER SYSTEM CONSTRUCTION, AND FOR ALL WORK INVOLVING ADJUSTMENT OF SEWER STRUCTURES. THE DEPARTMENT OF WATER MANAGEMENT'S PERMIT MUST BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF CONSTRUCTION. THE LICENSED SEWER CONTRACTOR/ SUBCONTRACTOR MUST SUBMIT TWO SETS OF PLANS APPROVED BY THE DEPARTMENT OF WATER MANAGEMENT FOR THE ISSUANCE OF THE SEWER PERMIT TO BUREAU OF ENGINEERING SERVICES-SEWER SECTION, JARDINE PURIFICATION PLANT, EL+51, ROOM 313, 1000 E. OHIO ST., CHICAGO, IL 60611. INSPECTION WILL BE PROVIDED BY THE DEPARTMENT OF WATER MANAGEMENT.
- IF THE SEWER PIPE COVER IS REDUCED TO LESS THAN 3 FT., CONCRETE ENCASEMENT OF THE SEWER OR REPLACEMENT OF THE SEWER WITH CLASS 52 DUCTILE IRON PIPE WILL BE REQUIRED.
- IN CASE OF DAMAGE TO CITY OF CHICAGO SEWERS, PRIVATE AND PUBLIC DRAINS, SEWER STRUCTURES AND/OR BENCH MONUMENTS, THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE DEPARTMENT OF WATER MANAGEMENT AT (312) 744-0409 OR (312) 744-0408.
- PERFORATED LIDS SHALL BE PLACED ON ALL SEWER MANHOLES AND CATCH BASINS.
- SIDEWALK ACCESSIBILITY RAMPS SHALL NOT BE CONSTRUCTED DIRECTLY OVER EXISTING OR PROPOSED DRAINAGE STRUCTURES.
- CITY OF CHICAGO WATER VALVE VAULTS AND SEWER STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION WITHOUT WRITTEN PERMISSION FROM THE CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT.
- CURB AND GUTTER CONSTRUCTION SHALL PROVIDE A MINIMUM CURB HEIGHT OF 3".
- BACKFILL MATERIAL UNDER SIDEWALKS SHALL BE FA-2.
- PAVEMENT REPLACEMENT AROUND FRAMES AND GRATES OR LIDS WHERE DRAINAGE, WATER MAIN OR ELECTRICAL STRUCTURES ARE ADJUSTED OR RECONSTRUCTED, SHALL BE WITH CLASS SI CONCRETE.
- ALL PAVEMENT PATCHING ALONG FRONTAGE ROADS SHALL BE CLASS C.
- PRE-CONSTRUCTION VIDEO TAPED INSPECTION REQUIRED PRIOR TO ISSUANCE OF SEWER PERMIT. POST-CONSTRUCTION VIDEO TAPED INSPECTION REQUIRED PRIOR TO ACCEPTANCE OF SEWER BY THE DEPARTMENT OF WATER MANAGEMENT.
- THE CONTRACTOR IS RESPONSIBLE FOR THE ADEQUATE PROTECTION OF THE EXISTING SEWERS, DRAIN CONNECTIONS, SEWER STRUCTURES AND BENCH MONUMENTS DURING CONSTRUCTION OPERATIONS AND USE OF HEAVY EQUIPMENT IN THE LIMITS OF THE PROJECT.
- THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT MUST BE NOTIFIED AT LEAST 48 HOURS IN ADVANCE OF CONSTRUCTION, WITH THE NAME AND TELEPHONE NUMBER OF THE RESIDENT ENGINEER WHO COULD BE CONTACTED FOR ANY SEWER EMERGENCY.
- MANHOLES, CATCH BASINS AND INLETS MUST BE PROTECTED FROM THE ENTRY OF ASPHALT/DEBRIS INTO THE SEWER SYSTEM DURING CONSTRUCTION. THE CONTRACTOR MUST MARK LOCATIONS OF ALL SEWER STRUCTURES ON THE SIDEWALK BEFORE STARTING PAVEMENT REMOVAL/REPLACEMENT. ADJUSTMENT OF FRAMES AND LIDS OF SEWER STRUCTURES MUST BE COMPLETED PRIOR TO STREET RESURFACING.
- THE CONTRACTOR MUST LOCATE AND PROMPTLY CONNECT TO THE NEW SEWERS ALL LIVE HOUSE DRAINS, CATCH BASIN DRAINS AND OTHER EXISTING LATERALS, DRAINS AND SEWERS, OF WHATEVER NATURE, WHICH ARE CONNECTED TO THE EXISTING SEWERS BEING REPAIRED OR REPLACED.
- EXISTING CATCH BASIN LATERALS TO BE REUSED MUST BE RODDED AND FLUSHED IN THE PRESENCE OF THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT INSPECTOR. A NEW CONNECTION TO THE MAIN SEWER IS REQUIRED IF THE EXISTING CATCH BASIN LATERAL IS NOT APPROVED BY THE SEWER INSPECTOR.
- THE FRAMES AND LIDS OF SEWER STRUCTURES TO BE ABANDONED, REMOVED, OR FILLED MUST BE SALVAGED AND THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT NOTIFIED FOR PICKUP.
- WHEN A SEWER STRUCTURE IS ABANDONED, ALL PIPE OPENINGS MUST BE PLUGGED, STRUCTURES FILLED WITH TRENCH BACKFILL, LIDS AND FRAMES REMOVED AND SURFACE RESTORED AS PER THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT STANDARDS AND SPECIFICATIONS.

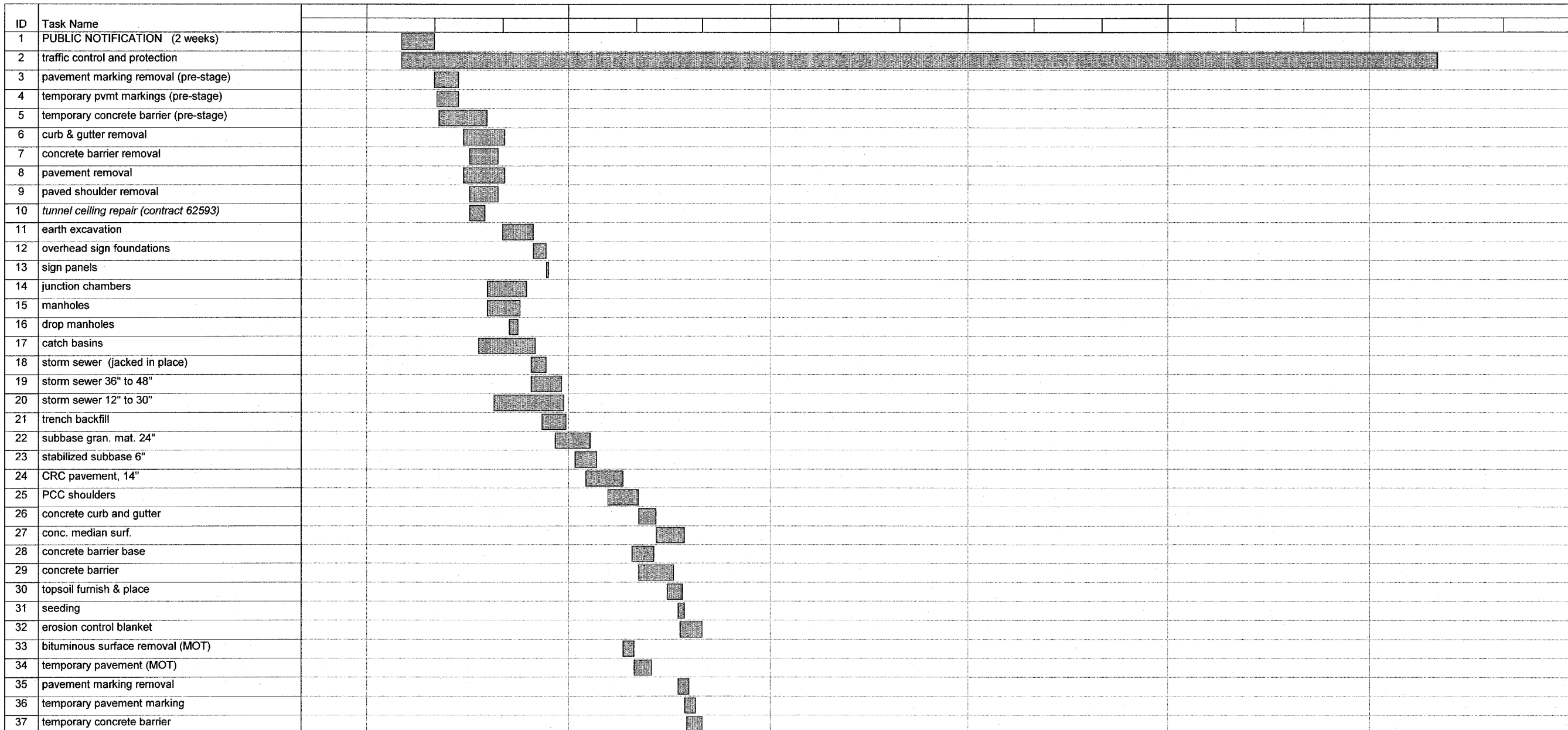
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

GENERAL NOTES & COMMITMENTS

SCALE: NONE DRAWN BY: RTM
DATE: MARCH 7, 2006 CHECKED BY: MPG



Suggested Sequence of Work
Date: Thu 3/2/06

Task		Milestone		Rolled Up Task		Rolled Up Progress		External Tasks		Group By Summary	
Progress		Summary		Rolled Up Milestone		Split		Project Summary			



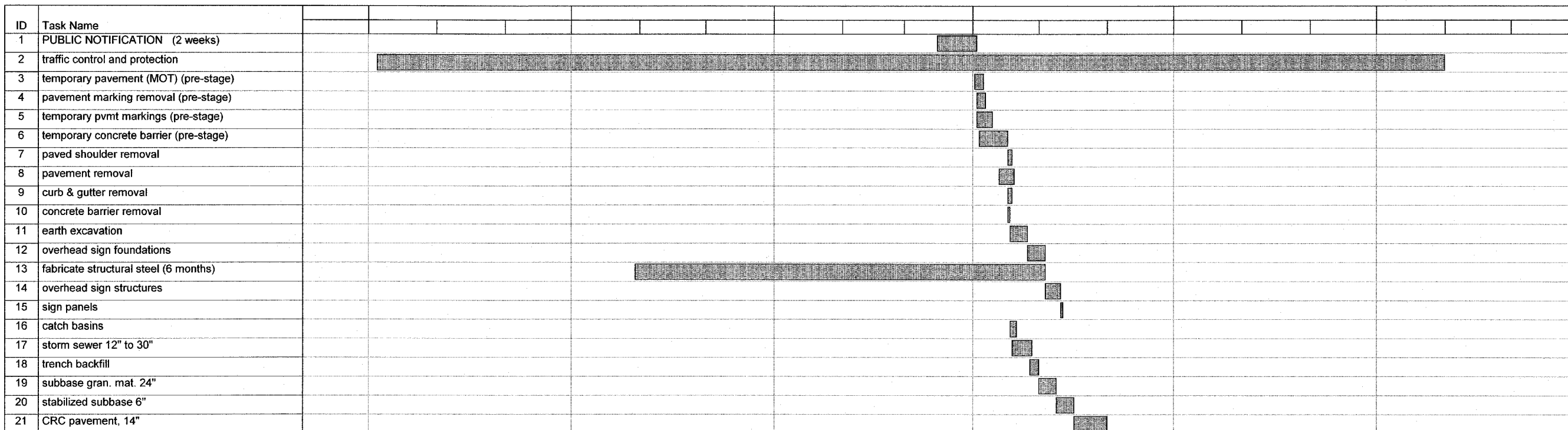
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUGGESTED SEQUENCE OF WORK
 STAGE 1

SCALE: NONE
 DATE: MARCH 7, 2006

DRAWN BY: JJS
 CHECKED BY: TGB



Suggested Sequence of Work
Date: Thu 3/2/06

Task		Milestone		Rolled Up Task		Rolled Up Progress		External Tasks		Group By Summary	
Progress		Summary		Rolled Up Milestone		Split		Project Summary			

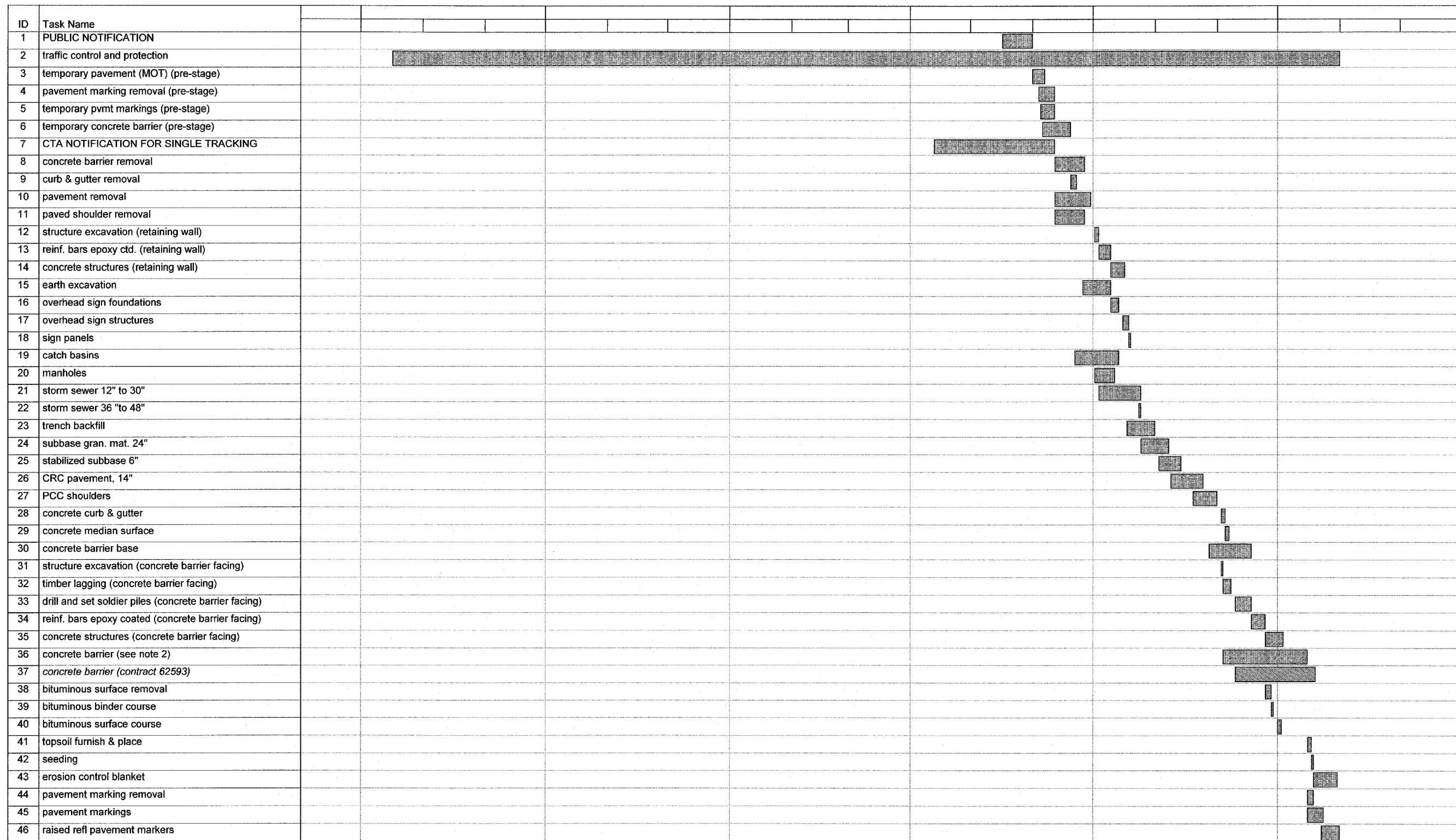


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUGGESTED SEQUENCE OF WORK
 STAGE 2

SCALE: NONE DRAWN BY: JJS
 DATE: MARCH 7, 2006 CHECKED BY: TGB



Suggested Sequence of Work
Date: Thu 3/2/06

Task: [shaded bar] Milestone: [diamond] Rolled Up Task: [thick bar] Rolled Up Progress: [dotted bar] External Tasks: [thin bar] Group By Summary: [arrow]

Progress: [dotted bar] Summary: [thick bar] Rolled Up Milestone: [diamond] Split: [dotted bar] Project Summary: [thin bar]



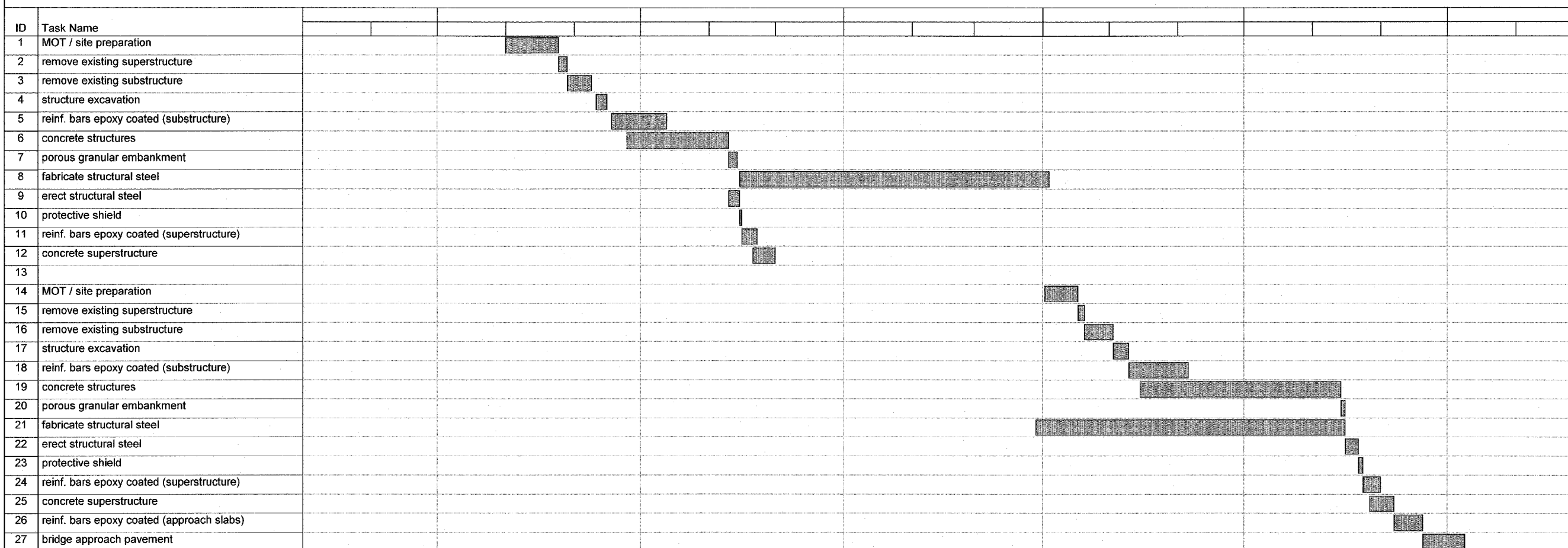
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUGGESTED SEQUENCE OF WORK
 STAGE 3

SCALE: NONE DRAWN BY: JJS
 DATE: MARCH 7, 2006 CHECKED BY: TGB

CONTRACT 62304



Suggested Sequence of Work
Date: Thu 3/2/06

Task		Milestone		Rolled Up Task		Rolled Up Progress		External Tasks		Group By Summary	
Progress		Summary		Rolled Up Milestone		Split		Project Summary			

* BRIDGE STAGING DOESN'T CORRELATE TO MAINLINE STAGING

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUGGESTED SEQUENCE OF WORK
NB I-57 BRIDGE

SCALE: NONE DRAWN BY: JJS
DATE: MARCH 7, 2006 CHECKED BY: TGB

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE								
				DAN RYAN J000-2A	DAN RYAN I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL			
									Y007 WALL C	X771-2A SNO16-2852	SFTY-2A SNO16-0073	
20100110	TREE REMOVAL (6-15 UNIT DIAMETER)	UNIT	1208	1208								
20100210	TREE REMOVAL (>15 UNIT DIAMETER)	UNIT	366	366								
20101000	TEMPORARY FENCE	FOOT	4456	4456								
20200100	EARTH EXCAVATION	CU YD	150634	150634								
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	13305	13305								
20200410	EARTH EXCAVATION (SPECIAL)	CU YD	725	725								
20700220	POROUS GRANULAR EMBANKMENT	CU YD	195	184				11				
20700400	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	1463							1463		
20700420	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	2899	2899								
20800150	TRENCH BACKFILL	CU YD	10328	9979	349							
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	178227	178227								
* 21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	56134	49380	6754							
* 21101630	TOPSOIL FURNISH AND PLACE, 8"	SQ YD	4837	4837								
* 21101645	TOPSOIL FURNISH AND PLACE, 12"	SQ YD	36178	30531	5647							
* 21101825	COMPOST FURNISH AND PLACE, 6"	SQ YD	38417	32770	5647							
* 25000210	SEEDING, CLASS 2A	ACRE	13.00	11.50	1.50							
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1234	1099	135							
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	1228	1093	135							
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	1225	1090	135							
* 25000750	MOWING	ACRE	43.25	38.25	5.00							
* 25001800	SEEDING, CLASS 4 (MODIFIED)	ACRE	4.25	3.00	1.25							
* 25001820	SEEDING, CLASS 5 (MODIFIED)	ACRE	3.25	3.25								
* 25002014	SEEDING, CLASS 4A (MODIFIED)	ACRE	3.25	3.25								
* 25100630	EROSION CONTROL BLANKET	SQ YD	96333	83932	12401							
* 25200200	SUPPLEMENTAL WATERING	UNIT	4981	4335	646							
* 28000200	EARTH EXCAVATION FOR EROSION CONTROL	CU YD	45	30	15							
* 28000250	TEMPORARY EROSION CONTROL SEEDINGS	POUND	1911	1752	159							
* 28000300	TEMPORARY DITCH CHECKS	EACH	61	55	6							
* 28000510	INLET FILTERS	EACH	29	29								
28001000	AGGREGATE (EROSION CONTROL)	TONS	2	1	1							
31101400	SUB-BASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	1974		1974							
31101810	SUB-BASE GRANULAR MATERIAL, TYPE B 12"	SQ YD	2986		2986							
31101860	SUB-BASE GRANULAR MATERIAL, TYPE B 24"	SQ YD	179673		179673							

- * - SPECIALTY ITEM
- - IDOT PAY CODE SFTY-3N
- ⊙ - NON-PARTICIPATING

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 1 OF 9

SCALE: NONE DRAWN BY: RTM
DATE: MARCH 7, 2006 CHECKED BY: MPG

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE							
				DAN RYAN J000-2A	DAN RYAN I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL		
									Y007 WALL C	X071-2A SNO16-2852	S071-2A SNO16-0073
50300225	CONCRETE STRUCTURES	CU YD	2505	265				10	85	2145	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	219							219	
50300260	BRIDGE DECK GROOVING	SQ YD	716							716	
50300300	PROTECTIVE COAT	SQ YD	4822	1477				24	241	3080	
50300440	ERECTING ELASTOMERIC BEARING ASSEMBLY, TYPE 1	EACH	8							8	
50301245	FORMED CONCRETE REPAIR (DEPTH EQUAL TO OR LESS THAN 5")	SQ FT	10						10		
50500305	ERECTING STRUCTURAL STEEL	L SUM	1							1	
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	300							300	
50500505	STUD SHEER CONNECTORS	EACH	3558							3558	
* 50700209	UNTREATED TIMBER LAGGING	SQ FT	2980	2980							
50700211	FURNISHING SOLDIER PILES (HP SECTION)	FOOT	3816	3816							
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	285850	23240				2210	16800	243600	
50900805	PEDESTRIAN RAILING	FOOT	477								477
51500100	NAME PLATES	EACH	1							1	
550A0340	STORM SEWERS, CLASS A, TYPE 2, 12"	FOOT	7755	6578	1177						
550A0360	STORM SEWERS, CLASS A, TYPE 2, 15"	FOOT	5715	5495	220						
550A0380	STORM SEWERS, CLASS A, TYPE 2, 18"	FOOT	933	677	256						
550A0400	STORM SEWERS, CLASS A, TYPE 2, 21"	FOOT	63	63							
550A0410	STORM SEWERS, CLASS A, TYPE 2, 24"	FOOT	985	503	482						
550A0420	STORM SEWERS, CLASS A, TYPE 2, 27"	FOOT	317	136	181						
550A0430	STORM SEWERS, CLASS A, TYPE 2, 30"	FOOT	709	583	126						
550A0450	STORM SEWERS, CLASS A, TYPE 2, 36"	FOOT	376	376							
550A0470	STORM SEWERS, CLASS A, TYPE 2, 42"	FOOT	47	47							
550A0480	STORM SEWERS, CLASS A, TYPE 2, 48"	FOOT	96	96							
550A0660	STORM SEWERS, CLASS A, TYPE 3, 15"	FOOT	409	409							
550A0680	STORM SEWERS, CLASS A, TYPE 3, 18"	FOOT	595	595							
550A0700	STORM SEWERS, CLASS A, TYPE 3, 21"	FOOT	312	312							
550A0710	STORM SEWERS, CLASS A, TYPE 3, 24"	FOOT	1187	1187							
550A0720	STORM SEWERS, CLASS A, TYPE 3, 27"	FOOT	285	285							
550A0730	STORM SEWERS, CLASS A, TYPE 3, 30"	FOOT	165		165						
550A0750	STORM SEWERS, CLASS A, TYPE 3, 36"	FOOT	238	238							
550A0780	STORM SEWERS, CLASS A, TYPE 3, 48"	FOOT	367	367							

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ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 3 OF 9

SCALE: NONE DRAWN BY: RTM
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SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE									
				DAN RYAN J000-2A	DAN RYAN I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL				
									Y007 WALL C	X771-2A SNO16-2852	SFTY-2A SNO16-0073		
550A0940	STORM SEWERS, CLASS A, TYPE 4, 12"	FOOT	45	45									
550A0980	STORM SEWERS, CLASS A, TYPE 4, 18"	FOOT	183	183									
550A1010	STORM SEWERS, CLASS A, TYPE 4, 24"	FOOT	126	126									
550A1030	STORM SEWERS, CLASS A, TYPE 4, 30"	FOOT	443	443									
550A1080	STORM SEWERS, CLASS A, TYPE 4, 48"	FOOT	317	317									
550A1350	STORM SEWERS, CLASS A, TYPE 5, 48"	FOOT	252	252									
550A1860	STORM SEWERS, CLASS A, TYPE 7, 18"	FOOT	147	147									
55035800	STORM SEWERS, TYPE 2, ELLIPTICAL, SPAN 53, RISE 34	FOOT	509	509									
55100300	STORM SEWER REMOVAL 8"	FOOT	3	3									
55100400	STORM SEWER REMOVAL 10"	FOOT	3653	3565	88								
55100500	STORM SEWER REMOVAL 12"	FOOT	6764	6598	166								
55100700	STORM SEWER REMOVAL 15"	FOOT	1437	1437									
55100900	STORM SEWER REMOVAL 18"	FOOT	1931	1931									
55101100	STORM SEWER REMOVAL 21"	FOOT	27	27									
55101200	STORM SEWER REMOVAL 24"	FOOT	1161	1161									
55101400	STORM SEWER REMOVAL 30"	FOOT	16	16									
55101600	STORM SEWER REMOVAL 36"	FOOT	56	56									
552A0900	STORM SEWERS JACKED IN PLACE, CLASS A 24"	FOOT	45	45									
58000110	MEMBRANE WATERPROOFING (SPECIAL)	SQ FT	32000									32000	
58300100	PORTLAND CEMENT MORTAR FAIRING COURSE	FOOT	500									500	
58700200	BRIDGE SEAT SEALER	SQ FT	643									643	
59000100	EPOXY CRACK SEALING	FOOT	100							100			
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	1163									1163	
60107700	PIPE UNDERDRAINS 6"	FOOT	43160	43160									
60108200	PIPE UNDERDRAINS 6" (SPECIAL)	FOOT	1026	1026									
60109582	PIPE UNDERDRAINS FOR STRUCTURES 6"	FOOT	596									596	
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	4	4									
60201310	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	328	303	25								
60206120	CATCH BASINS, TYPE A, SPECIAL, 5'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	1	1									
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	39	36	3								
60208210	CATCH BASINS, TYPE C, TYPE 20 FRAME AND GRATE	EACH	2	2									
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	16	14	2								

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REVISIONS	
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ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 4 OF 9

SCALE: NONE DRAWN BY: RTM
DATE: MARCH 7, 2006 CHECKED BY: MPG

3/8/2006 2:01:30 PM

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE								
				DAN RYAN J000-2A	DAN RYAN I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL			
									Y007 WALL C	X771-2A SNO16-2852	S003-2A SNO16-0073	
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	42	33	9							
60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	13	12	1							
60226730	MANHOLES, DROP TYPE, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2								
60247800	JUNCTION CHAMBERS	EACH	3	3								
60237420	INLETS, TYPE A, TYPE 20 FRAME & GRATE	EACH	11	10	1							
60250200	CATCH BASINS TO BE ADJUSTED	EACH	41	41								
60255500	MANHOLES TO BE ADJUSTED	EACH	96	88	8							
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	9	8	1							
60500040	REMOVING MANHOLES	EACH	98	94	4							
60500050	REMOVING CATCH BASINS	EACH	206	190	16							
60500060	REMOVING INLETS	EACH	74	64	10							
60500105	FILLING MANHOLES	EACH	8	8								
60500205	FILLING CATCH BASINS	EACH	7	7								
60608521	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24	FOOT	369.0	156.5	212.5							
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	27281	21271	6010							
60618324	CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)	SQ FT	9143	7932	1211							
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	14	11	3							
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (TANGENT)	EACH	14	11	3							
63700400	CONCRETE BARRIER, DOUBLE FACE (SPECIAL)	FOOT	225	225								
63700805	CONCRETE BARRIER TRANSITION	FOOT	571	526	45							
64200105	SHOULDER RUMBLE STRIPS	FOOT	44757	36326	8431							
* 66400560	CHAIN LINK FENCE, 6' (SPECIAL)	FOOT	8889	8861				28				
* 66402900	CHAIN LINK GATE, 6' x 6' SINGLE	EACH	11	11								
66410300	CHAIN LINK FENCE REMOVAL	FOOT	378	258					120			
* 66900200	NON-SPECIAL WASTE DISPOSAL	CY YD	31360	31360								
* 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1								
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2								
67000600	ENGINEER'S FIELD LABORATORY	CAL MO	15	15								
67100100	MOBILIZATION	L SUM	1	0.8	0.2							
* 70102550	TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR	EACH	2					2				
* 70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	65920	55270	10650							
* 70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	94700	94700								

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ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
 SHEET 5 OF 9

SCALE: NONE
 DATE: MARCH 7, 2006

DRAWN BY: RTM
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SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE									
				DAN RYAN J000-2A	DAN RYAN I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL				
									Y007 WALL C	X771-2A SNO16-2852	SFTY-2A SNO16-0073		
* 70300530	PAVEMENT MARKING TAPE, TYPE III 5"	FOOT	8161	8161									
* 70300550	PAVEMENT MARKING TAPE, TYPE III 8"	FOOT	26663	26663									
* 70300560	PAVEMENT MARKING TAPE, TYPE III 12"	FOOT	4609	4609									
* 70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	126419	108622	17797								
70400100	TEMPORARY CONCRETE BARRIER	FOOT	33180	27660	5520								
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	60230	51710	8520								
* 72000100	SIGN PANEL - TYPE 1	SQ FT	63					63					
* 72000200	SIGN PANEL - TYPE 2	SQ FT	175					175					
* 72000300	SIGN PANEL - TYPE 3	SQ FT	790					790					
* 72300100	INSTALL EXISTING SIGN PANEL	SQ FT	242					242					
* 72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	12					12					
* 72400330	REMOVE SIGN PANEL - TYPE 3	SQ FT	28					28					
* 72400720	RELOCATE SIGN PANEL - TYPE 2	SQ FT	12					12					
* 72400730	RELOCATE SIGN PANEL - TYPE 3	SQ FT	300					300					
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	99					99					
* 73000100	WOOD SIGN SUPPORT	FOOT	500					500					
* 73300100	OVERHEAD SIGN SUPPORT STRUCTURE - SPAN, TYPE I-A (4'-0" X 4'-6")	FOOT	181					181					
* 73301000	OVERHEAD SIGN STRUCTURE - SPAN (SPECIAL)	FOOT	96					96					
* 73305000	OVERHEAD SIGN STRUCTURE WALKWAY	FOOT	86					86					
* 73400200	DRILLED SHAFT CONCRETE FOUNDATIONS	CU YD	45					45					
* 73600100	REMOVE OVERHEAD SIGN STRUCTURE - SPAN	EACH	3					3					
* 73700300	REMOVE CONCRETE FOUNDATION - OVERHEAD	EACH	9					9					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	110		110								
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	12356	1416	10940								
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	1682		1682								
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	726	32	694								
* 78003120	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 5"	FOOT	1128	354	774								
* 78003140	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 8"	FOOT	201		201								
* 78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	122314	83145	39169								
* 78005120	EPOXY PAVEMENT MARKING - LINE 5"	FOOT	15924	14802	1122								
* 78005140	EPOXY PAVEMENT MARKING - LINE 8"	FOOT	25335	25335									
* 78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	4278	4278									

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ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 6 OF 9

SCALE: NONE
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SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE								
				DAN RYAN J000-2A	DAN RYAN I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL			
									Y007 WALL C	X771-2A SNO16-2552	SFTY-2A SNO16-0073	
* 78008210	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4"	FOOT	36368	36368								
* 78008220	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 5"	FOOT	16123	16123								
* 78008240	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 8"	FOOT	15580	15580								
* 78008250	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 12"	FOOT	3022	2981	41							
* 78008270	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 24"	FOOT	16	16								
* 78100100	RAISED REFLECTIVE PAVEMENT MARKERS	EACH	1862	1746	116							
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	16	16								
* 78200100	MONODIRECTIONAL PRISMATIC BARRIER REFLECTOR	EACH	5318	4108	1210							
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	56	44	12							
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	287	257	30							
* 78201000	TERMINAL MARKERS, DIRECT APPLIED	EACH	14	11	3							
* 78300100	PAVEMENT MARKING REMOVAL	SQ FT	25500	21058	4442							
* 80700140	GROUND ROD, 5/8" DIA. X 10 FT.	EACH	15			15						
* 81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	1227			1227						
* 81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	250			250						
* 81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	272			272						
* 81023750	CONDUIT ENCASED IN CONCRETE, 3" DIA., PVC	FOOT	663				663					
* 81400200	HEAVY-DUTY HANDHOLE	EACH	25				25					
* 81400205	HEAVY-DUTY HANDHOLE, SPECIAL	EACH	2				2					
* 81500200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	3043			3043						
* 83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	135			135						
* 84200705	LIGHTING FOUNDATION REMOVAL, PARTIAL	EACH	130			130						
* 87900200	DRILL EXISTING HANDHOLE	EACH	8			8						
* 89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	2				2					
* X6643310	CHAIN LINK FENCE TO BE REMOVED AND RE-ERECTED (CTA)	FOOT	23					23				
* X0325315	CONDUIT ENCASED, REINFORCED CONCRETE, 6 - 4" DIA., CNC AND 2 - 2" DIA., CNC	FOOT	140			140						
* X0325316	CONDUIT IN TRENCH, 2" DIA., CNC	FOOT	92			92						
* X0325318	LIGHTWEIGHT CELLULAR CONCRETE FILL	CU YD	987					987				
* X0325314	LUG SYSTEM COMPLETE 38'	EACH	2	2								
* X2500322	SEEDING, CLASS 5A (MODIFIED)	ACRE	4.00	2.75	1.25							
* X0325317	TEMPORARY SOIL RETENTION SYSTEM (BRIDGE)	SQ FT	13038							13038		
* C2C05818	SHRUB, RHUS AROMATICA GRO-LOW (GRO-LOW FRAGRANT SUMAC), 18" WIDTH, CONTAINER	EACH	105	105								

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ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

**SUMMARY OF QUANTITIES
SHEET 7 OF 9**

SCALE: NONE DRAWN BY: RTM
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SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	15
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				
				62304

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE																
				DAN RYAN J000-2A	DAN RYAN I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL											
									Y007 WALL C	X771-2A SNO16-2852	S771-2A SNO16-0073									
* E20200G1	VINE-PARTHENOCISSUS QUINQUEFOLIA (VIRGINIA CREEPER), 1-GALLON POT	EACH	712	712																
* E20220G1	VINE-PARTHENOCISSUS TRICUSPIDATA (BOSTON IVY), 1-GALLON POT	EACH	516	516																
* K0030400	PERENNIAL PLANTS, DAYLILIES	UNIT	11	11																
X0300057	MANHOLES, TYPE A, 6"-DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE	EACH	11	7	4															
X0301424	SILICONE JOINT SEALER	FOOT	1110																1110	
⊙ X0320333	ROADWAY CLEANING (SPECIAL)	EACH	28	28																
X0320870	BRACED EXCAVATION	CU YD	8							8										
X0321027	DRILLING GROUT HOLES	FOOT	960	960																
X0321430	BRIDGE APPROACH PAVEMENT CONNECTOR (PCC), SPECIAL	SQ YD	849																849	
* X0321866	REMOVE, STORE, AND RE-ERECT SIGN PANEL	SQ FT	866							866										
* X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	1580	64						1516										
X0322434	LIGHT TOWER SERVICE PAD, SPECIAL	EACH	1	1																
X0322671	STABILIZED CONSTRUCTION ENTRANCE	SQ YD	3360	3360																
* X0322859	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	17	17																
X0322934	POLYMER CONCRETE	CU YD	3																3	
* ⊙ X0323426	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	58	58																
X0323830	DRAINAGE SCUPPERS, DS-11	EACH	1																1	
X0323907	COMMUNICATIONS VAULT	EACH	1						1											
* X0323973	SEDIMENT CONTROL, SILT FENCE	FOOT	18060	15745	2315															
* X0323974	SEDIMENT CONTROL, SILT FENCE MAINTENANCE	FOOT	4516	3937	579															
X0323988	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	12056	12056																
X0324112	BARRIER BASE	FOOT	22725	21663	1062															
X0324455	DRILLING AND SETTING SOLDIER PILES (IN SOIL)	CU FT	10237	10237																
* X0324469	CONDUIT ENCASED, REINFORCED CONCRETE, 2 - 4" DIA., CNC	FOOT	124								124									
* X0324471	CONDUIT ENCASED, REINFORCED CONCRETE, 4 - 4" DIA., CNC	FOOT	373								373									
* X0324646	CONDUIT ENCASED, REINFORCED CONCRETE, 6 - 4" DIA., CNC	FOOT	927								927									
X0324697	SOIL STABILIZERS	POUND	170000	170000																
X0324698	APPLYING DUST SUPPRESSION AGENT	UNIT	147	147																
* X0324980	CONDUIT ENCASED, REINFORCED CONCRETE, 3 - 4" DIA., CNC AND 1 - 2" DIA., CNC 2 WIDE X 2 HIGH	FOOT	65								65									
X0325082	CTA BARRIER REMOVAL	FOOT	8152	8152																
* X0325083	CTA FENCE	FOOT	8295	8295																
* X0325084	CTA GATES	EACH	11	11																
X0325088	PLACEMENT OF CEMENT GROUT	CU FT	2400	2400																
X0325089	CONNECTION TO GROUT HOLE	EACH	64	64																
* X0325095	MAIN DRAIN CLEANING	FOOT	4809	4809																
X0325132	SHAPING AND GRADING AT HIGH MAST LIGHT TOWER	SQ YD	23	23																
X0712400	TEMPORARY PAVEMENT	SQ YD	9634	9634																

- * - SPECIALTY ITEM
- - IDOT PAY CODE SFTY-3N
- ⊙ - NON-PARTICIPATING

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
 SHEET 8 OF 9

SCALE: NONE
 DATE: MARCH 7, 2006

DRAWN BY: RTM
 CHECKED BY: MPG

TYLIN INTERNATIONAL

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	16
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE										
				DAN RYAN J000-2A	DAN RYAN I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL					
									Y007 WALL C	X771-2A SNO16-2852	SFTY-2A SNO16-0073			
X3540580	PORTLAND CEMENT IN GROUT	CU FT	800	800										
X4066426	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70	TON	261	13	248									
X4066550	POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105	TON	1248	38	1210									
X4066660	POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105	TON	892		892.0									
X4409400	BITUMINOUS SURFACE REMOVAL 1 3/4"	SQ YD	5249	391	4858									
X4834090	PORTLAND CEMENT CONCRETE SHOULDERS 14"	SQ YD	35370	35370										
X6061001	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48	FOOT	2896.0	2896.0										
X6063600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	FOOT	12017.0	9791.0	2226.0									
X6370910	CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT	FOOT	13677	12898	779									
X6370912	CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT, SPECIAL	FOOT	238		238									
X6370925	CONCRETE BARRIER, SINGLE FACE, 42" (SPECIAL)	FOOT	7994	7994										
X6370935	CONCRETE BARRIER, SINGLE FACE, 32" (MODIFIED)	FOOT	882	882										
X6370940	CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT	FOOT	20	20										
X6640210	TEMPORARY CHAIN LINK FENCE (PORTABLE)	FOOT	1885	1885										
X6700410	ENGINEER'S FIELD OFFICE, TYPE A (SPECIAL)	CAL MO	24	24										
* X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1										
* X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	457	457										
* X7015000	CHANGEABLE MESSAGE SIGN	CAL MO	160	160										
XX001854	STABILIZED SUB-BASE, 6"	SQ YD	175338	175338										
XX004200	PORTLAND CEMENT CONCRETE PAVEMENT 14" (JOINTED)	SQ YD	1239	1239										
XX004201	PAVEMENT REINFORCEMENT, 14"	SQ YD	127554	127554										
Z0002400	BALLAST	TON	1890	1890										
Z0002600	BAR SPLICERS	EACH	1028								1028			
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	0.8	0.2									
Z0013825	CONTROLLED LOW STRENGTH MATERIAL	CU YD	503	327	176									
Z0018800	DRAINAGE SYSTEM	L SUM	1								1			
Z0022800	FENCE REMOVAL	FOOT	477										477	
Z0029300	GROUT REPAIR	FOOT	515										515	
Z0032700	KEYWAY REPAIR	FOOT	500										500	
* <input type="checkbox"/> Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	12	11	1									
* <input type="checkbox"/> Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	36	30	6									
Z0047300	PROTECTIVE SHIELD	SQ YD	445								445			
* Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	0.8	0.2									
<input type="checkbox"/> Z0056220	SAND MODULE IMPACT ATTENUATOR TO BE REMOVED	EACH	11	11										
* Z0068400	STEEL CASINGS, 42"	FOOT	45	45										

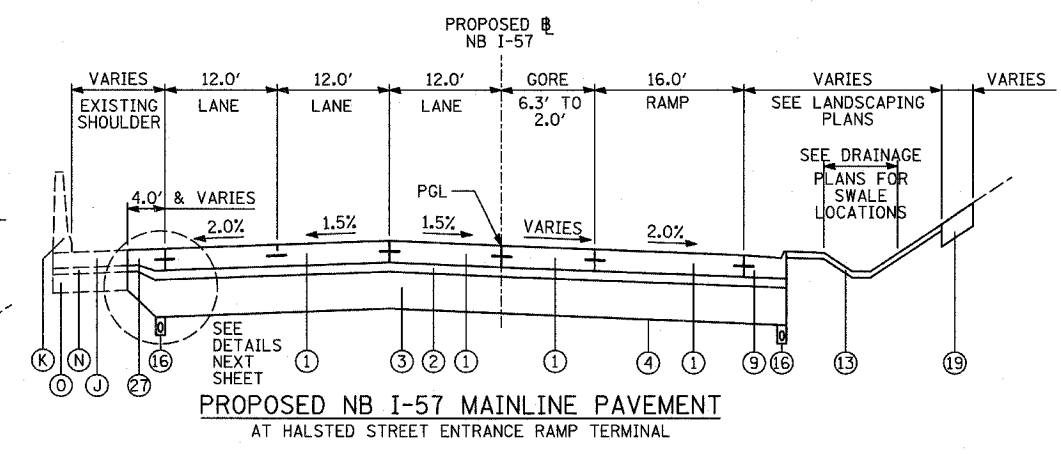
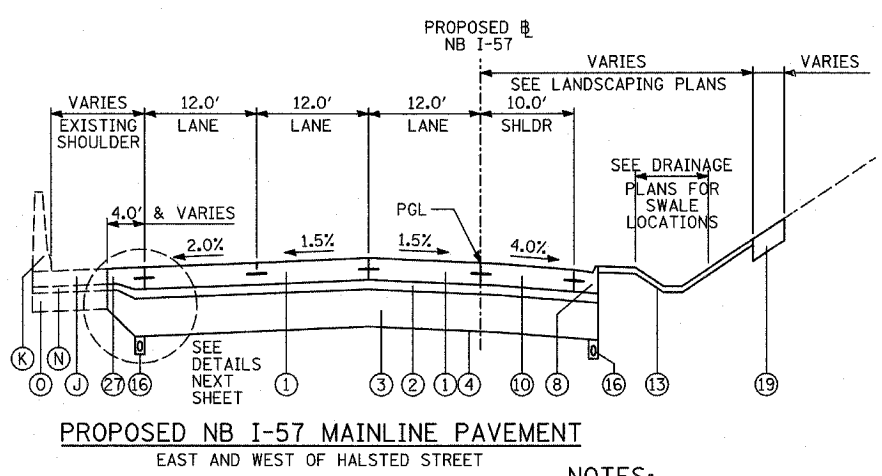
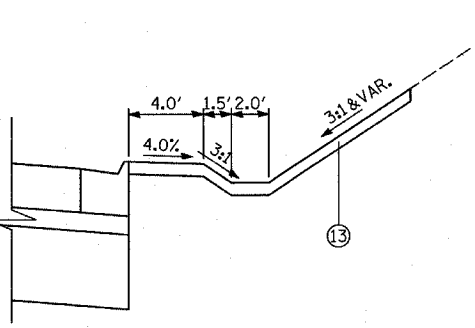
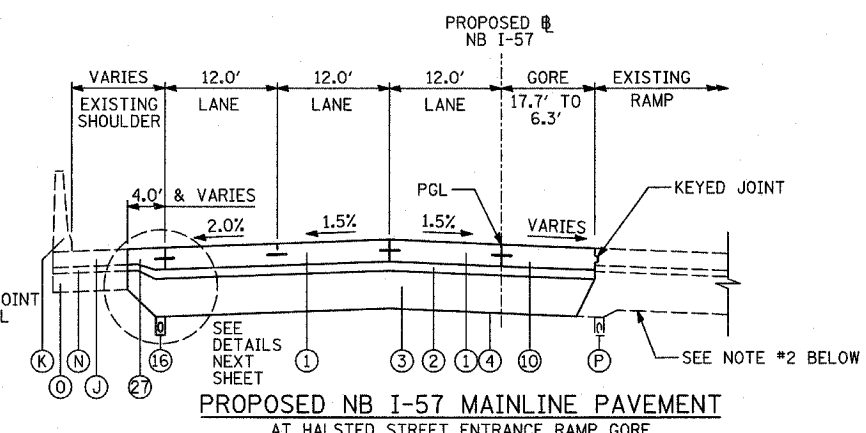
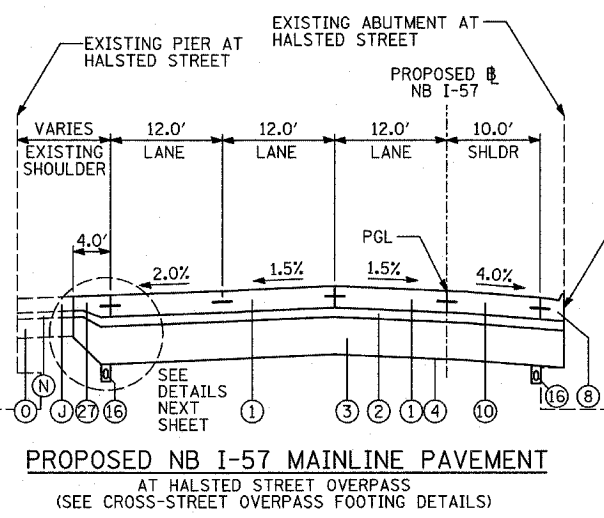
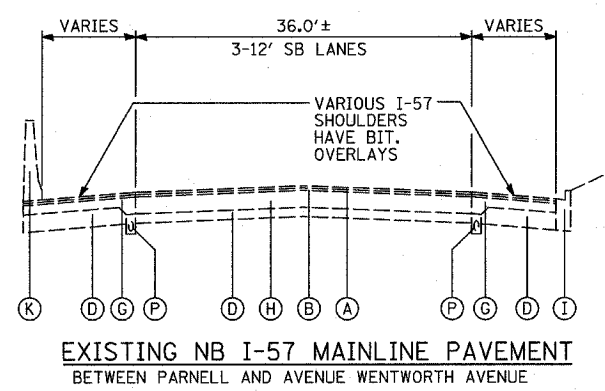
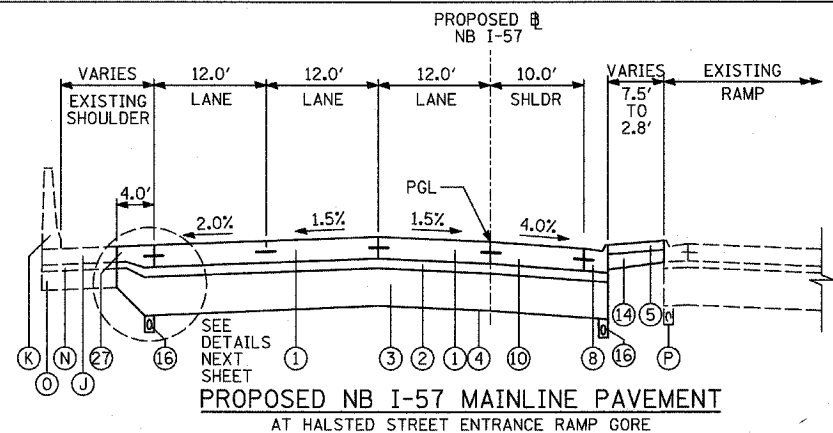
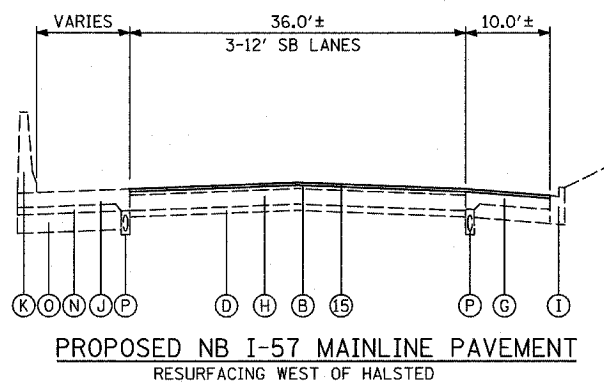
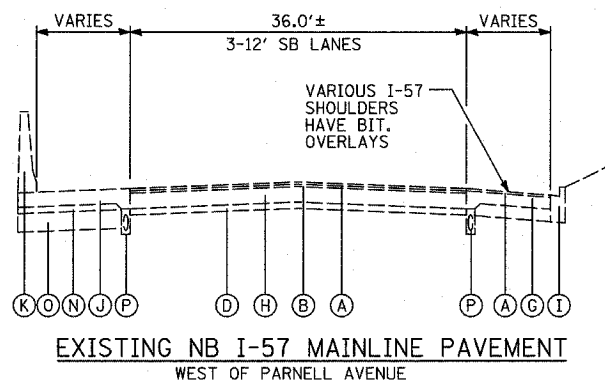
- * - SPECIALTY ITEM
- IDOT PAY CODE SFTY-3N
- ⊙ - NON-PARTICIPATING

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 9 OF 9

SCALE: NONE DRAWN BY: RTM
DATE: MARCH 7, 2006 CHECKED BY: MPG



- ### PROPOSED LEGEND
- CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"; & PAVEMENT REINFORCEMENT, 14"
 - STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
 - SUB-BASE GRANULAR MATERIAL, TYPE B 24"
 - GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
 - CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
 - PORTLAND CEMENT CONCRETE SHOULDERS 14"
 - CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - TRAFFIC BARRIER TERMINAL, TYPE VARIES
 - TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
 - AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
 - POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, MIX "F", N105, 1 1/4"
 - PIPE UNDERDRAIN, 6" (SEE DETAILS)
 - POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
 - ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
 - TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE 6" EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
 - CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
 - SUB-BASE GRANULAR MATERIAL, TYPE B 12"
 - SUB-BASE GRANULAR MATERIAL, TYPE B 6"
 - PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX "D", N70, 1 1/2"
 - PORTLAND CEMENT CONCRETE BASE COURSE 13"
 - PORTLAND CEMENT CONCRETE PAVEMENT 14" (JOINTED)

- ### EXISTING LEGEND
- ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- BIT CONC SURFACE COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 4 3/4"±
 - SUB-BASE GRANULAR MATERIAL, 4"±
 - SUB-BASE GRANULAR MATERIAL, 6"±
 - CRUSHED STONE, 5"±
 - PCC SHOULDERS, 9"±
 - PCC BASE COURSE, 9"±
 - COMB CONC CURB & GUTTER
 - PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - CONCRETE BARRIER WALL
 - CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - BITUMINOUS SURFACE, 7"±
 - STABILIZED SUB-BASE, 4"±
 - SUB-BASE GRANULAR MATERIAL, 12"±
 - EXISTING PIPE UNDERDRAIN
 - EXISTING FIBER OPTIC DUCT

NOTES:

- REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.
- ANY REQUIRED REGRADING OF EXISTING ADJACENT SUB-BASE GRANULAR MATERIAL SHALL BE INCLUDED IN THE COST OF "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

STRUCTURAL PAVEMENT DESIGN INFORMATION

STRUCTURAL DESIGN TRAFFIC: YEAR = 2020	PV = 322,920	SU = 7,020	MJ = 21,060
ROAD/STREET CLASSIFICATION: CLASS 1	PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: P = 8% S = 37% M = 37%		
TRAFFIC FACTOR: ACTUAL TF = 116.08	AC TYPE = N/A		
MINIMUM TF = 8.26	AC GRADE: BINDER = - SURFACE = -		
SUBGRADE SUPPORT RATING: SSR = 2.00	ILLINOIS BEARING RATIO: IBR = 3.00		

BITUMINOUS MIXTURE REQUIREMENT

ITEM	AC TYPE	VOIDS	RAP %
SHOULDER RESURFACING			
1 1/2" DEPTH: BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70	PG 64-22	4%±70 Gyr	10
MAINLINE RESURFACING			
1 3/4" DEPTH: POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105	SBS/SBR 76-22	4%±105 Gyr	0
4" DEPTH: POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105	SBS/SBR 76-22	4%±105 Gyr	0
POLY. BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19, N105	SBS/SBR 76-22	4%±105 Gyr	0
TEMPORARY PAVEMENT			
POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105	SBS/SBR 76-22	4%±105 Gyr	0
POLY. BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19, N105	SBS/SBR 76-22	4%±105 Gyr	0
STABILIZED SUBBASE, 6"	PG 58-22	3%±50 Gyr	25
BITUMINOUS REPLACEMENT OVER PATCHES	PG 64-22	4%±70 Gyr	15

THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS SURFACE MIXTURES IS 112 LB/SQ YD/IN.

POTENTIAL UNDERCUT

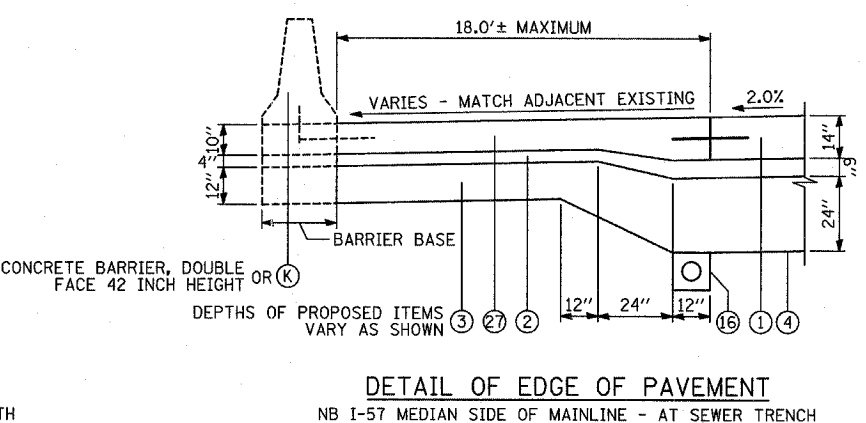
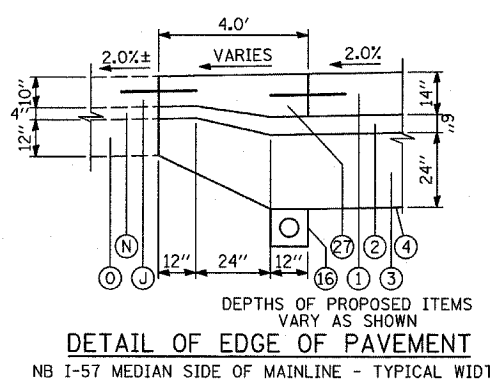
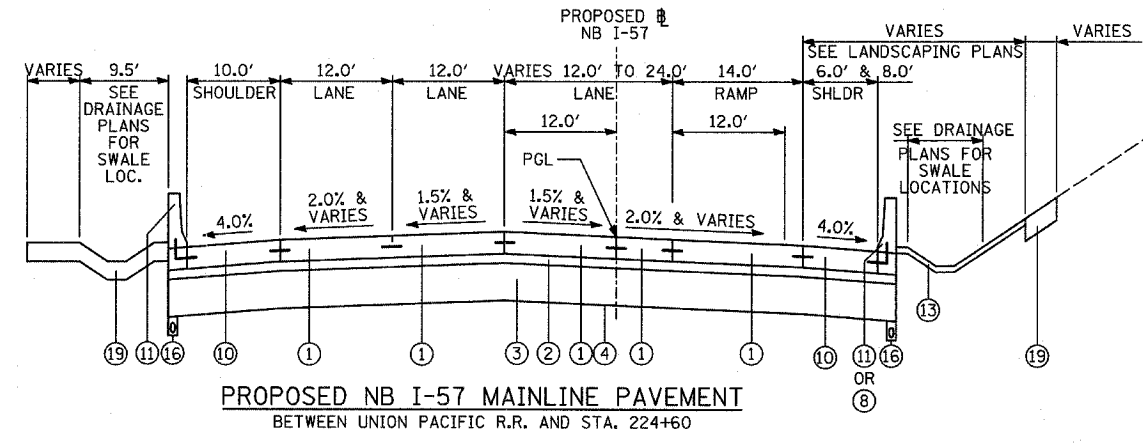
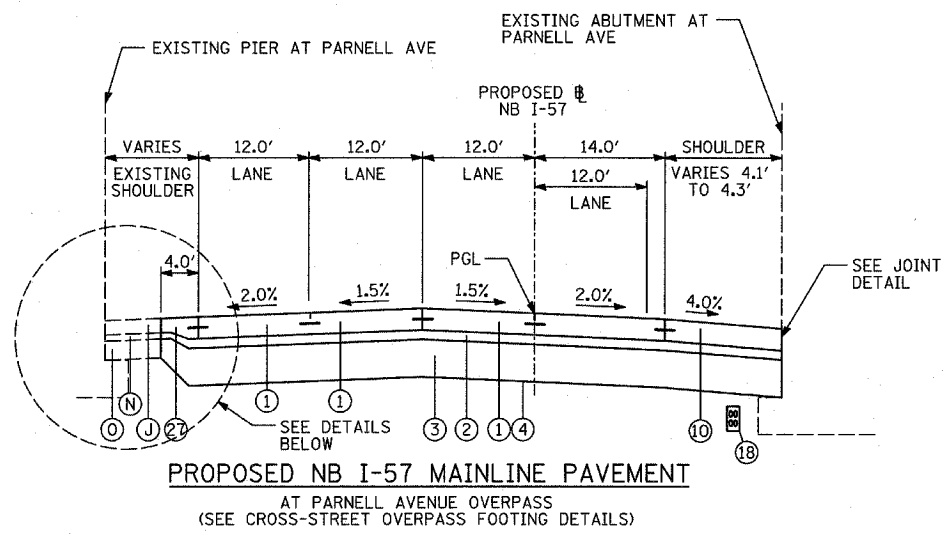
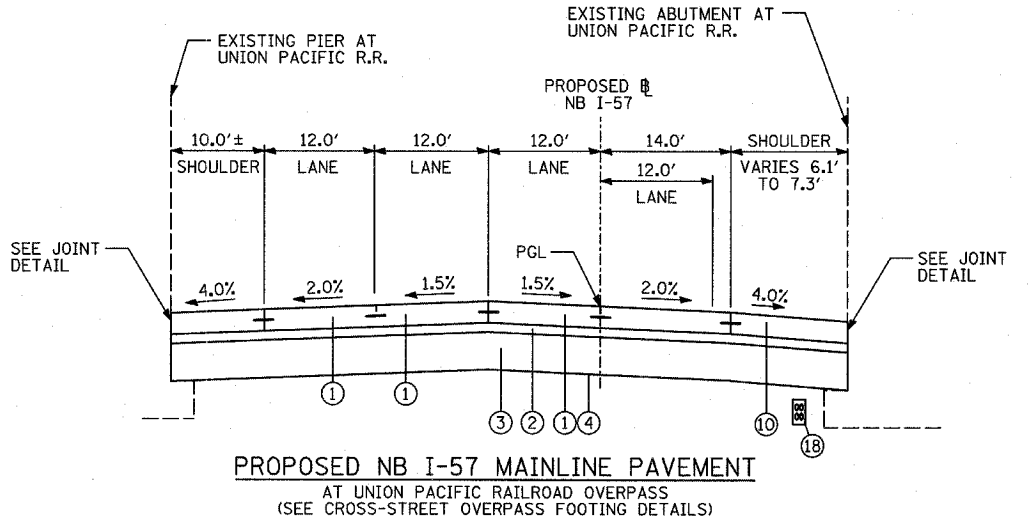
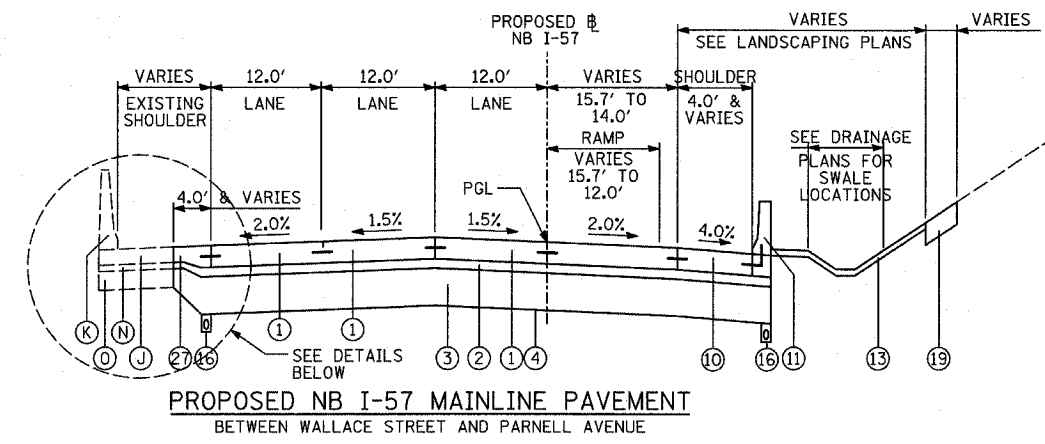
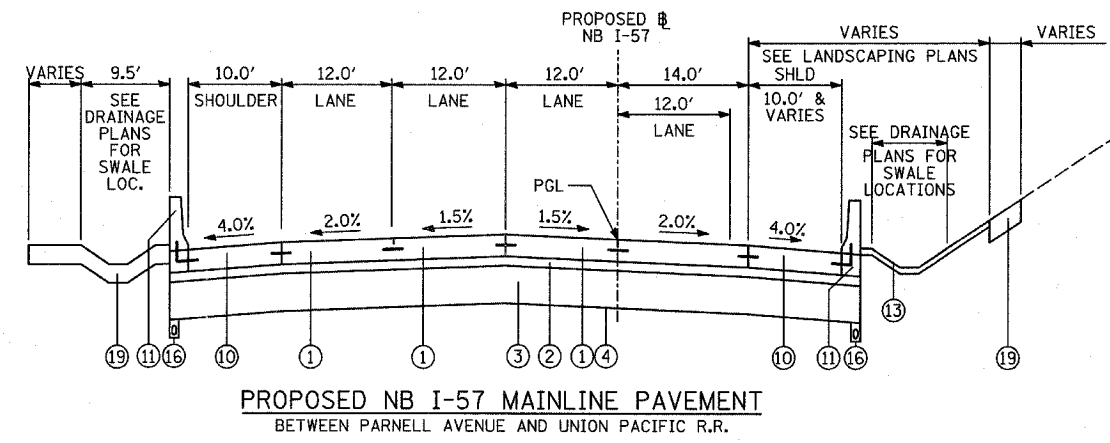
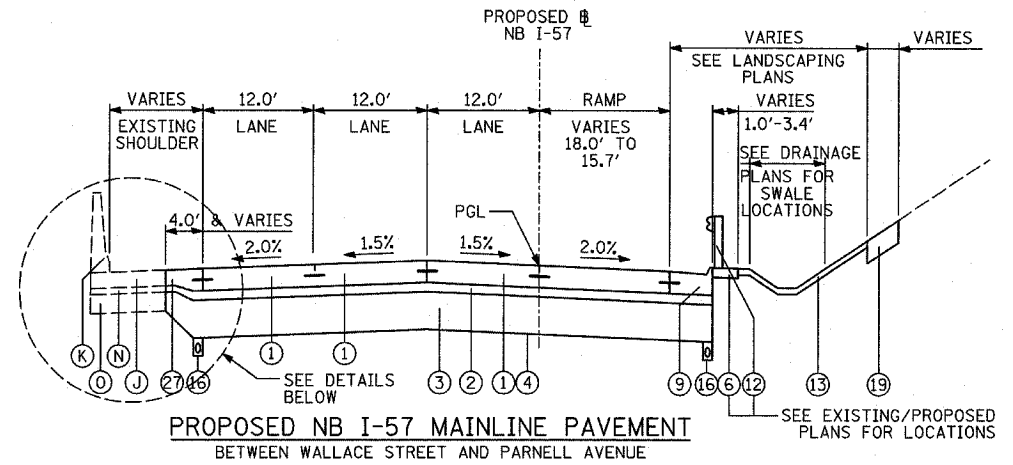
"POROUS GRANULAR EMBANKMENT, SUBGRADE" (PGES) HAS BEEN RECOMMENDED FOR USE AT LOCATIONS WITH SOILS THAT TEND TO BE UNSTABLE OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 303.13 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIALS ARE ENCOUNTERED, THE SOIL SHALL BE REMOVED AND REPLACED WITH PGES AS DETERMINED BY THE GEOTECHNICAL ENGINEER. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE THE CONTRACTOR.

(SEE SCHEDULE OF QUANTITIES FOR LOCATIONS)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING & PROPOSED TYPICAL SECTIONS
NB I-57
(SHEET 1 OF 8)

SCALE: NONE DRAWN BY: RTM
DATE: MARCH 7, 2006 CHECKED BY: MPG



- PROPOSED LEGEND**
- CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14" & PAVEMENT REINFORCEMENT, 14"
 - STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
 - SUB-BASE GRANULAR MATERIAL, TYPE B 24"
 - GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
 - CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
 - PORTLAND CEMENT CONCRETE SHOULDERS 14"
 - CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - TRAFFIC BARRIER TERMINAL, TYPE VARIES
 - TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
 - AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
 - POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, MIX "F", N105, 1 1/4"
 - PIPE UNDERDRAIN, 6" (SEE DETAILS)
 - POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
 - ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
 - TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE 6"; EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
 - CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
 - SUB-BASE GRANULAR MATERIAL, TYPE B 12"
 - SUB-BASE GRANULAR MATERIAL, TYPE B 6"
 - PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX "D", N70, 1 1/2"
 - PORTLAND CEMENT CONCRETE BASE COURSE 13"
 - PORTLAND CEMENT CONCRETE PAVEMENT 14" (JOINTED)

- EXISTING LEGEND**
- ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- BIT CONC SURFACE COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 4 3/4"±
 - SUB-BASE GRANULAR MATERIAL, 4"±
 - SUB-BASE GRANULAR MATERIAL, 6"±
 - CRUSHED STONE, 5"±
 - PCC SHOULDERS, 9"±
 - PCC BASE COURSE, 9"±
 - COMB CONC CURB & GUTTER
 - PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - CONCRETE BARRIER WALL
 - CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - BITUMINOUS SURFACE, 7"±
 - STABILIZED SUB-BASE, 4"±
 - SUB-BASE GRANULAR MATERIAL, 12"±
 - EXISTING PIPE UNDERDRAIN
 - EXISTING FIBER OPTIC DUCT

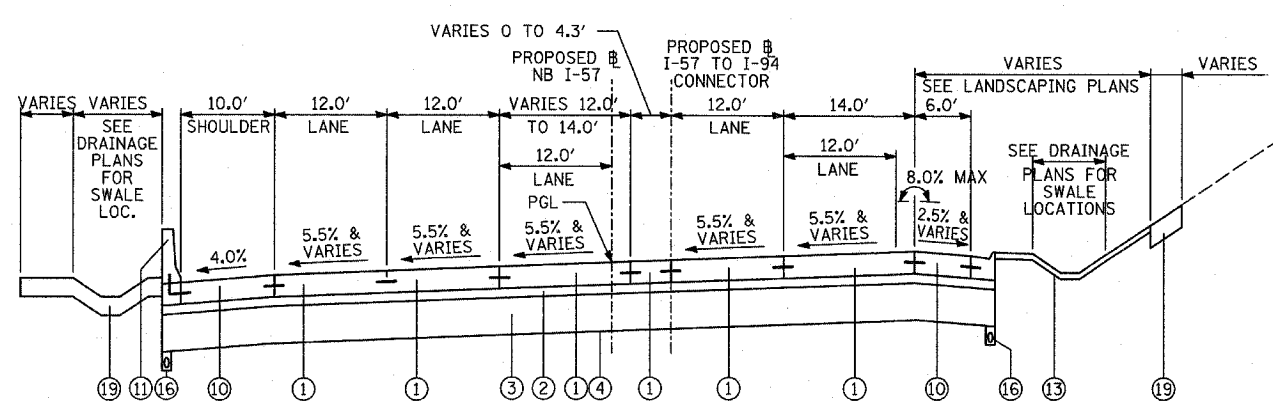
NOTES:

- REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.

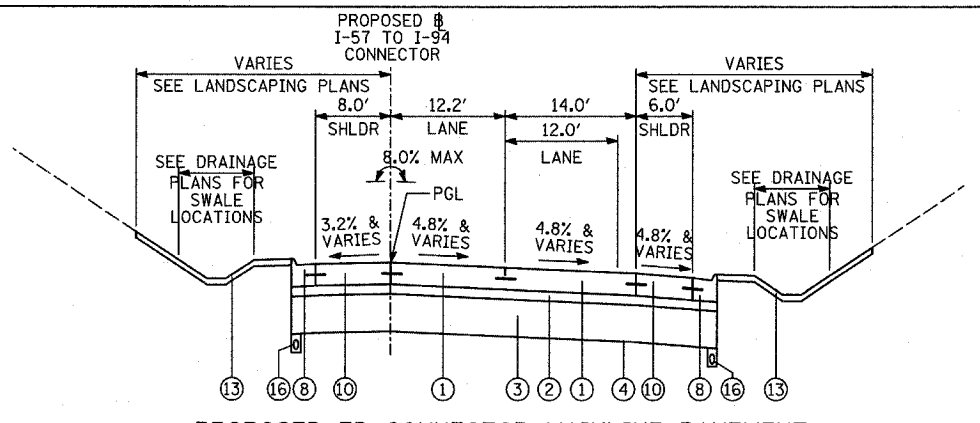
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING & PROPOSED TYPICAL SECTIONS
NB I-57
(SHEET 2 OF 8)

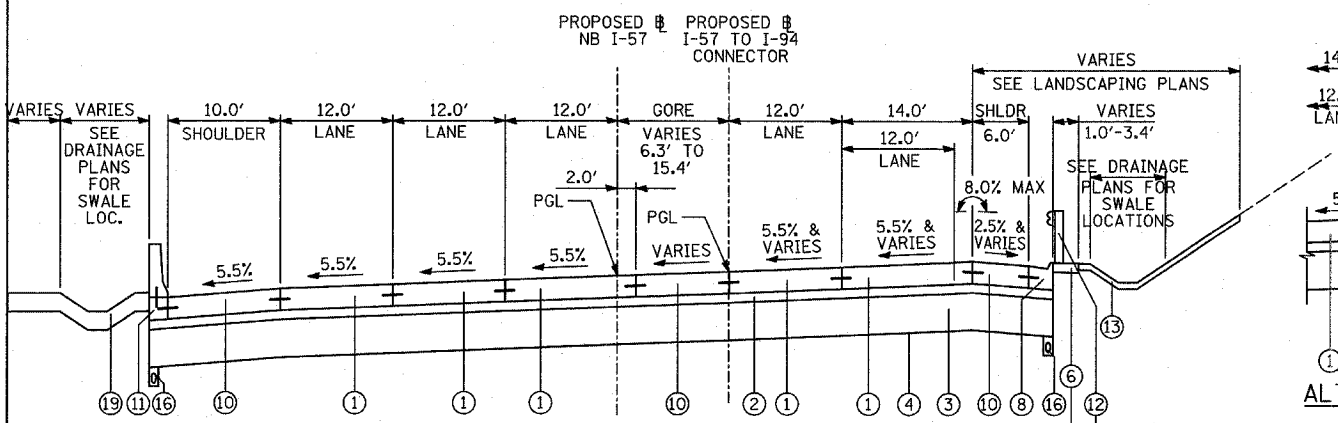
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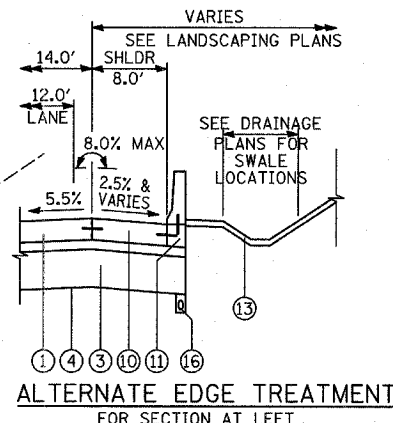
PROPOSED NB I-57 MAINLINE PAVEMENT
BETWEEN STA. 224+60 AND STA. 227+77



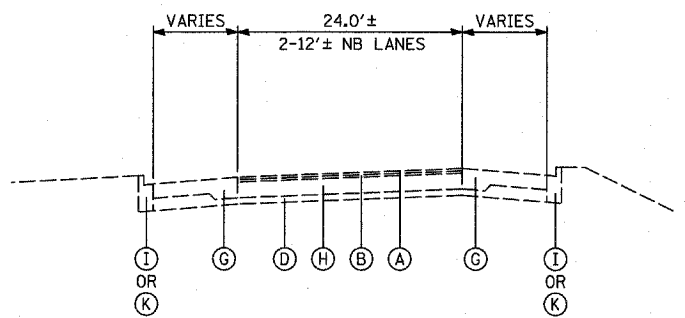
PROPOSED EB CONNECTOR MAINLINE PAVEMENT
BETWEEN NB I-57/EB CONNECTOR SPLIT AND CONTRACT LIMIT AT STA. 415+06



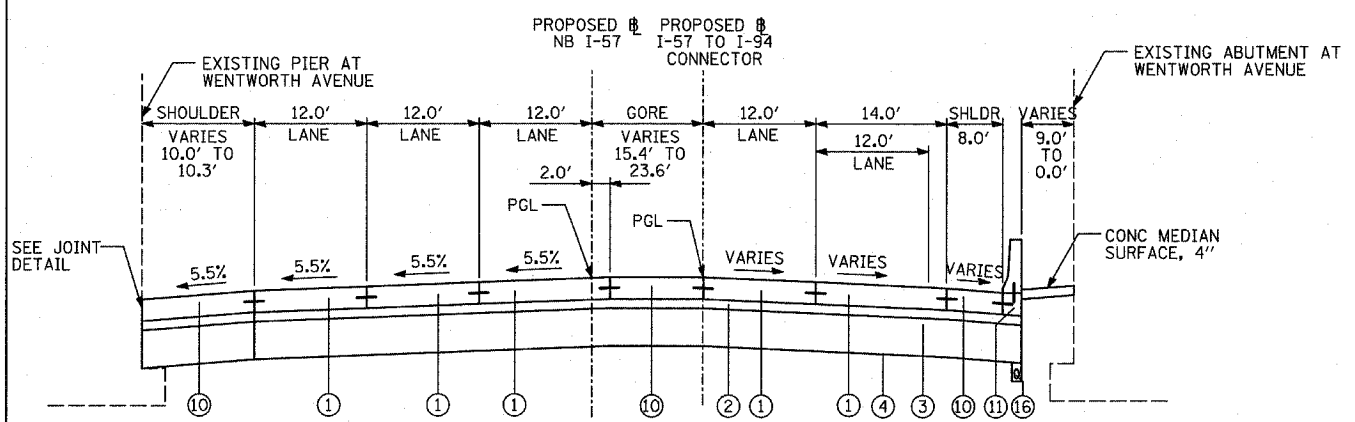
PROPOSED NB I-57 / EB I-57 TO I-94 CONNECTOR PAVEMENT
LOCATIONS VARY BETWEEN STA. 227+77 AND WENTWORTH AVENUE



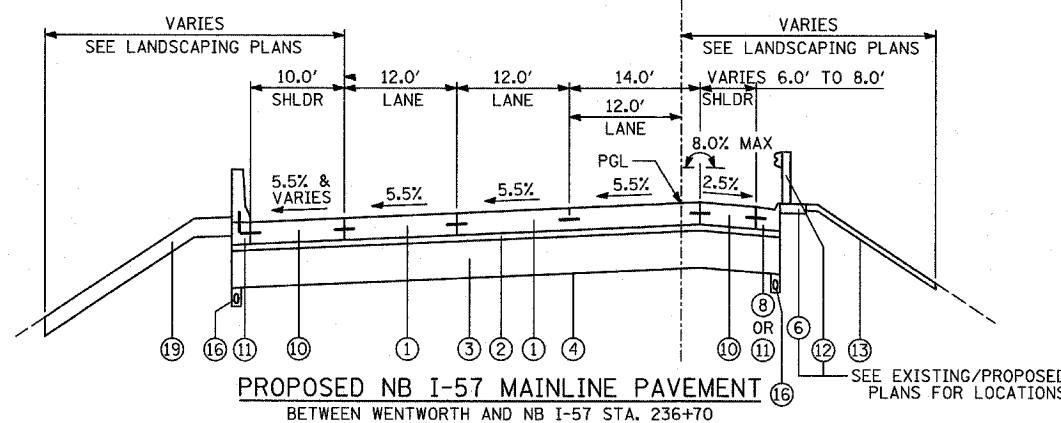
ALTERNATE EDGE TREATMENT
FOR SECTION AT LEFT



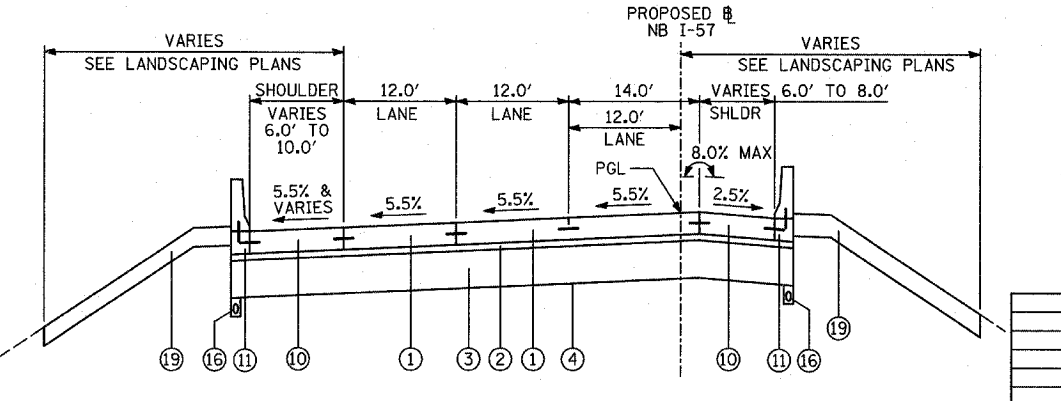
EXISTING NB I-57 MAINLINE PAVEMENT
BETWEEN NB I-57/EB CONNECTOR SPLIT AND NB I-57/I-94 MERGE



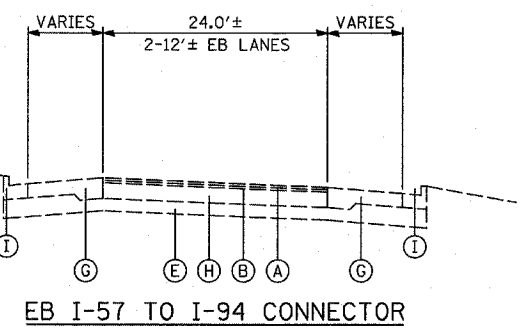
PROPOSED NB I-57 / EB I-57 TO I-94 CONNECTOR PAVEMENT
AT WENTWORTH AVENUE OVERPASS
(SEE CROSS-STREET OVERPASS FOOTING DETAILS)



PROPOSED NB I-57 MAINLINE PAVEMENT
BETWEEN WENTWORTH AND NB I-57 STA. 236+70



PROPOSED NB I-57 MAINLINE PAVEMENT
BETWEEN NB I-57 STA. 239+89 AND I-57 / I-94 MERGE



EB I-57 TO I-94 CONNECTOR

PROPOSED LEGEND

- 1 CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14" & PAVEMENT REINFORCEMENT, 14"
- 2 STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
- 3 SUB-BASE GRANULAR MATERIAL, TYPE B 24"
- 4 GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- 5 CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
- 6 CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
- 7 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
- 8 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- 9 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
- 10 PORTLAND CEMENT CONCRETE SHOULDERS 14"
- 11 CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- 12 TRAFFIC BARRIER TERMINAL, TYPE VARIES
- 13 TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
- 14 AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
- 15 POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, MIX "F", N105, 1 1/4"
- 16 PIPE UNDERDRAIN, 6" (SEE DETAILS)
- 17 POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
- 18 ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
- 19 TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE 6" EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
- 20 CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- 21 PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- 22 SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- 23 SUB-BASE GRANULAR MATERIAL, TYPE B 6"
- 24 PORTLAND CEMENT CONCRETE SHOULDERS 9"
- 25 BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX "D", N70, 1 1/2"
- 26 PORTLAND CEMENT CONCRETE BASE COURSE 13"
- 27 PORTLAND CEMENT CONCRETE PAVEMENT 14" (JOINTED)

EXISTING LEGEND

- ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- A BIT CONC SURFACE COURSE, 1 1/2"±
 - B BIT CONC BINDER COURSE, 1 1/2"±
 - C BIT CONC BINDER COURSE, 4 3/4"±
 - D SUB-BASE GRANULAR MATERIAL, 4"±
 - E SUB-BASE GRANULAR MATERIAL, 6"±
 - F CRUSHED STONE, 5"±
 - G PCC SHOULDERS, 9"±
 - H PCC BASE COURSE, 9"±
 - I COMB CONC CURB & GUTTER
 - J PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - K CONCRETE BARRIER WALL
 - L CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADE IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - M BITUMINOUS SURFACE, 7"±
 - N STABILIZED SUB-BASE, 4"±
 - O SUB-BASE GRANULAR MATERIAL, 12"±
 - P EXISTING PIPE UNDERDRAIN
 - Q EXISTING FIBER OPTIC DUCT

NOTES:
1. REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.

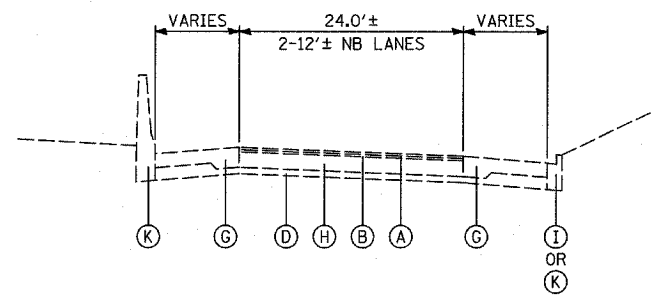
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REVISIONS	
NAME	DATE

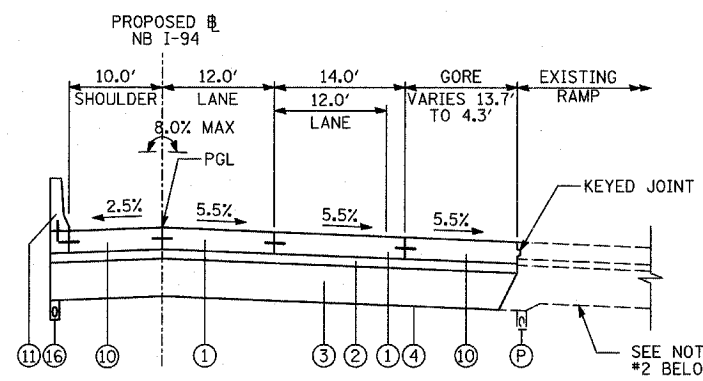
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING & PROPOSED TYPICAL SECTIONS
NB I-57 / EB I-57 TO I-94 CONNECTOR
(SHEET 3 OF 8)

SCALE: NONE
DATE: MARCH 7, 2006
DRAWN BY: RTM
CHECKED BY: MPG

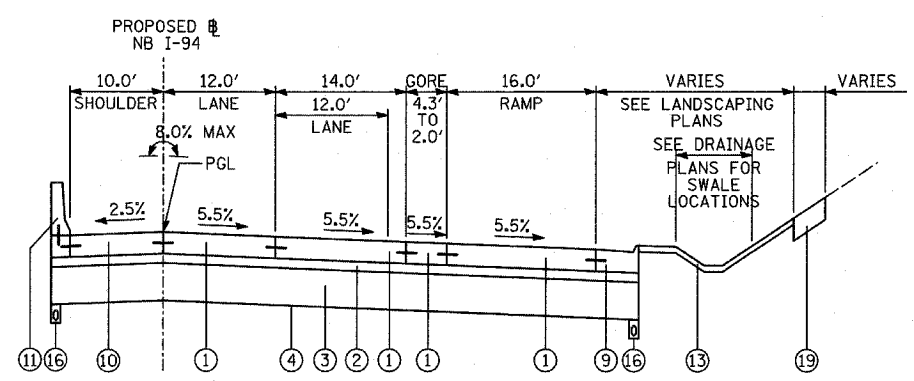
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	20
STA. 190+45 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4 62304				



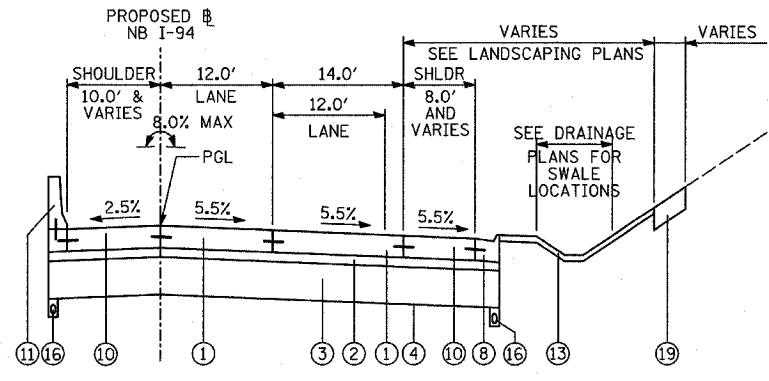
EXISTING NB I-94 (FORD) MAINLINE PAVEMENT BETWEEN WB CONNECTOR SPLIT AND MERGE



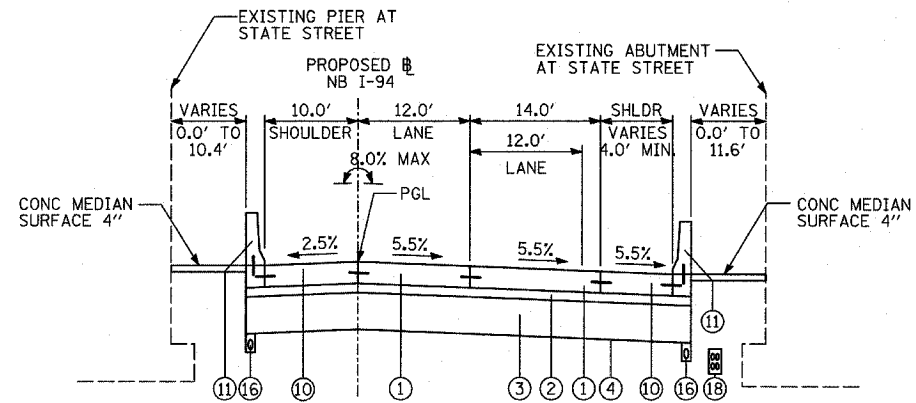
PROPOSED NB I-94 (FORD) MAINLINE PAVEMENT AT MICHIGAN AVE ENTRANCE RAMP GORE



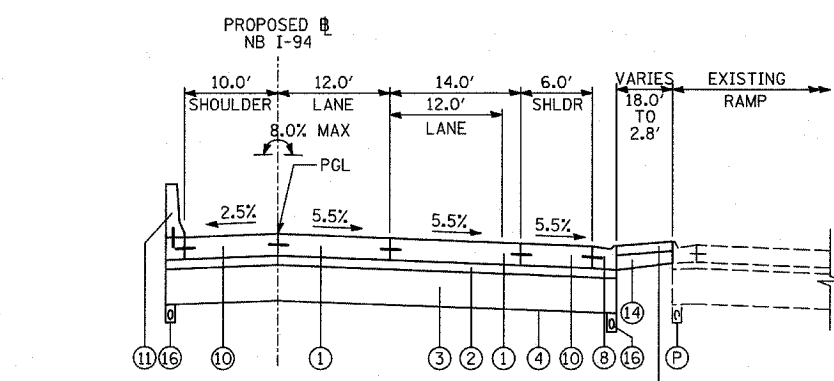
PROPOSED NB I-94 (FORD) MAINLINE PAVEMENT AT MICHIGAN AVE ENTRANCE RAMP TERMINAL



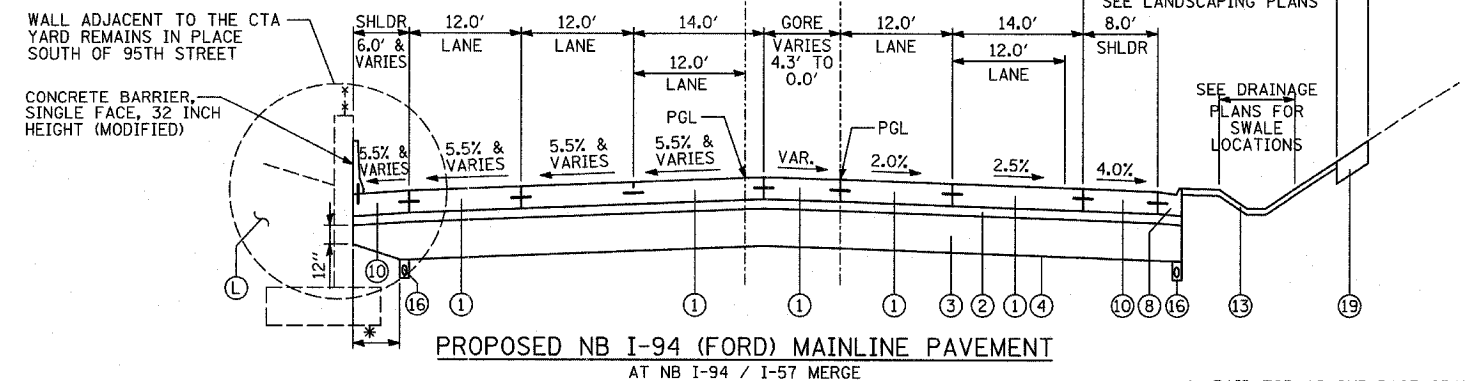
PROPOSED NB I-94 (FORD) MAINLINE PAVEMENT SOUTH OF NB MICHIGAN AVE ENTRANCE RAMP (SEE ALTERNATE EDGE TREATMENT DETAILS)



PROPOSED NB I-94 (FORD) MAINLINE PAVEMENT SOUTH OF NB MICHIGAN AVE ENTRANCE RAMP (SEE ALTERNATE EDGE TREATMENT DETAILS)



PROPOSED NB I-94 MAINLINE PAVEMENT AT MICHIGAN AVE ENTRANCE RAMP GORE



PROPOSED NB I-94 (FORD) MAINLINE PAVEMENT AT NB I-94 / I-57 MERGE

- NOTES:**
- REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.
 - ANY REQUIRED REGRADING OF EXISTING ADJACENT SUB-BASE GRANULAR MATERIAL SHALL BE INCLUDED IN THE COST OF "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

- PROPOSED LEGEND**
- CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"; & PAVEMENT REINFORCEMENT, 14"
 - STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
 - SUB-BASE GRANULAR MATERIAL, TYPE B 24"
 - GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
 - CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
 - PORTLAND CEMENT CONCRETE SHOULDERS 14"
 - CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - TRAFFIC BARRIER TERMINAL, TYPE VARIES
 - TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
 - AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
 - POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, MIX "F", N105, 1 1/4"
 - PIPE UNDERDRAIN, 6" (SEE DETAILS)
 - POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
 - ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
 - TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE 6" EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
 - CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
 - SUB-BASE GRANULAR MATERIAL, TYPE B 12"
 - SUB-BASE GRANULAR MATERIAL, TYPE B 6"
 - PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX "D", N10, 1 1/2"
 - PORTLAND CEMENT CONCRETE BASE COURSE 13"
 - PORTLAND CEMENT CONCRETE PAVEMENT 14" (JOINTED)

- EXISTING LEGEND**
- ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- BIT CONC SURFACE COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 4 3/4"±
 - SUB-BASE GRANULAR MATERIAL, 4"±
 - SUB-BASE GRANULAR MATERIAL, 6"±
 - CRUSHED STONE, 5"±
 - PCC SHOULDERS, 9"±
 - PCC BASE COURSE, 9"±
 - COMB CONC CURB & GUTTER
 - PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - CONCRETE BARRIER WALL
 - CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - BITUMINOUS SURFACE, 7"±
 - STABILIZED SUB-BASE, 4"±
 - SUB-BASE GRANULAR MATERIAL, 12"±
 - EXISTING PIPE UNDERDRAIN
 - EXISTING FIBER OPTIC DUCT

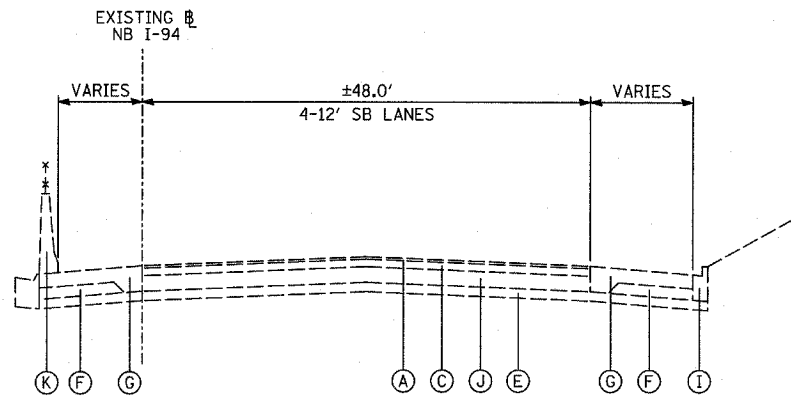
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING & PROPOSED TYPICAL SECTIONS
 NB I-94 (BISHOP FORD FREEWAY)
 (SHEET 4 OF 8)

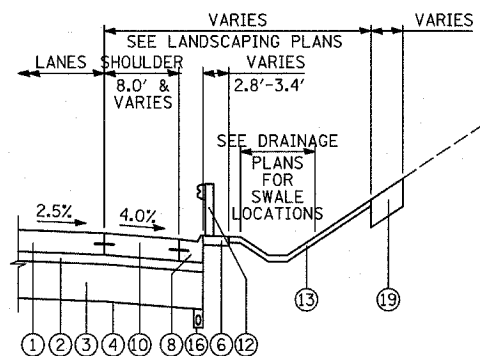
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TYLIN INTERNATIONAL

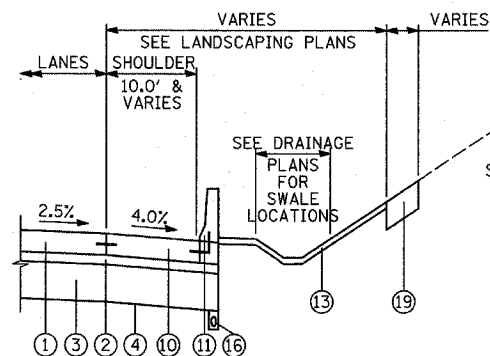
* PAID FOR AS SUB-BASE GRANULAR MATERIAL, TYPE B 24"



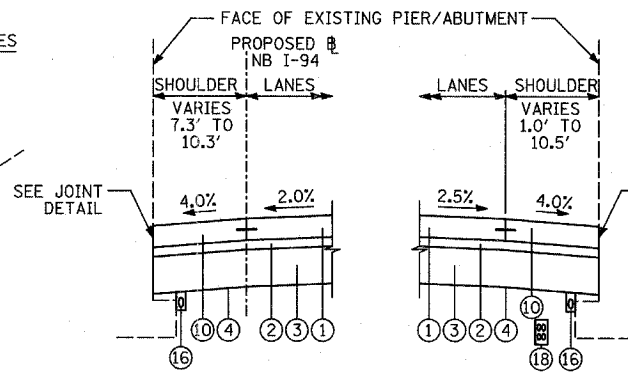
EXISTING NB I-94 MAINLINE PAVEMENT



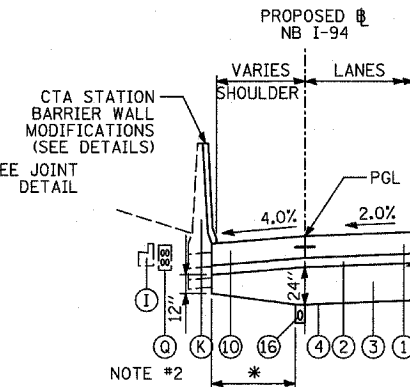
PROPOSED NB I-94 (DAN RYAN EXPWAY)
ALTERNATE EAST PAVEMENT EDGE
TREATMENTS: GUARDRAIL
LOCATIONS VARY - SEE EXISTING/PROPOSED PLANS



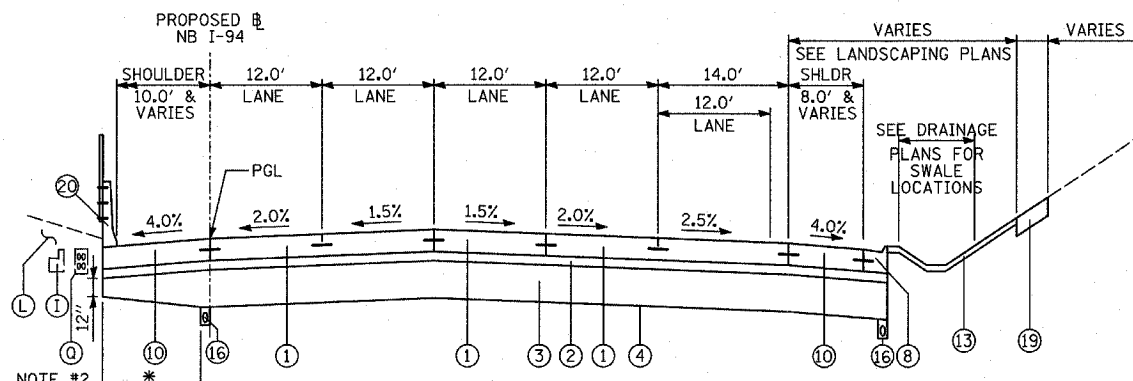
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ALTERNATE EAST PAVEMENT EDGE
TREATMENTS: CONCRETE BARRIER
LOCATIONS VARY - SEE EXISTING/PROPOSED PLANS



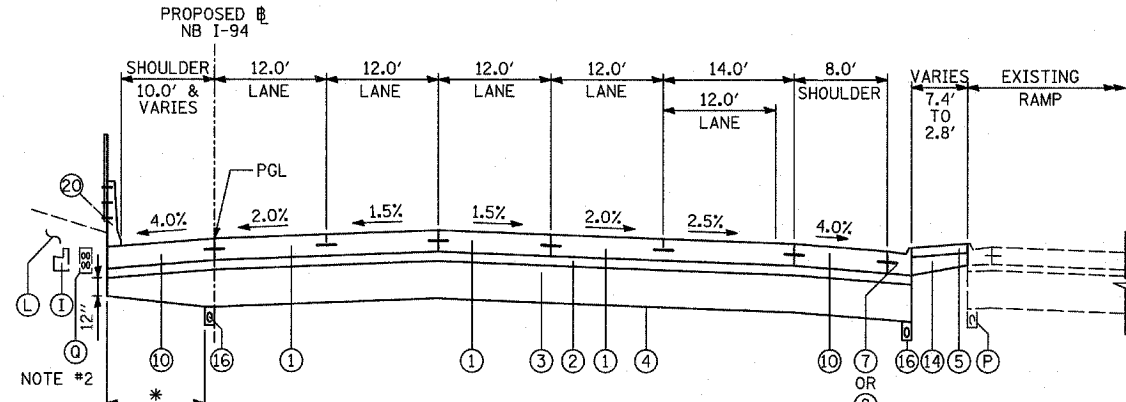
PROPOSED NB I-94 (DAN RYAN EXPWAY)
ALTERNATE PAVEMENT EDGE
TREATMENTS: EXISTING PIER/ABUTMENT
LOCATIONS VARY AT BRIDGES - SEE EXISTING/PROPOSED PLANS FOR LOCATIONS AND CROSS-STREET OVERPASS FOOTING DETAILS FOR ADDITIONAL DETAIL



PROPOSED NB I-94 (DAN RYAN EXPWAY)
ALTERNATE WEST PAVEMENT EDGE
TREATMENTS: CTA BARRIER MODIFICATIONS
ADJACENT TO CTA STATIONS - SEE EXISTING/PROPOSED PLANS



PROPOSED NB I-94 MAINLINE PAVEMENT
LOCATIONS VARY BETWEEN 95TH STREET & 83rd TO 79th STREET C-D RAMPS



PROPOSED NB I-94 MAINLINE PAVEMENT
LOCATIONS VARY (95TH ST. ENTRANCE, 87TH ST. EXIT/ENTRANCE RAMP GORES)

- NOTES:**
- REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.
 - EXACT LOCATION OF EXISTING FIBER OPTIC DUCT IS UNKNOWN. CONTRACTOR MUST NOTIFY THE CTA TO LOCATE THE DUCT PRIOR TO THE START OF WORK.

* PAID FOR AS SUB-BASE GRANULAR MATERIAL, TYPE B 24"

PROPOSED LEGEND

- CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"; & PAVEMENT REINFORCEMENT, 14"
- STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
- SUB-BASE GRANULAR MATERIAL, TYPE B 24"
- GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
- CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
- COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
- COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
- PORTLAND CEMENT CONCRETE SHOULDERS 14"
- CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- TRAFFIC BARRIER TERMINAL, TYPE VARIES
- TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
- AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
- POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, MIX "F", N105, 1 1/4"
- PIPE UNDERDRAIN, 6" (SEE DETAILS)
- POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
- ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
- TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE 6" EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
- CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- SUB-BASE GRANULAR MATERIAL, TYPE B 6"
- PORTLAND CEMENT CONCRETE SHOULDERS 9"
- BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX "D", N70, 1 1/2"
- PORTLAND CEMENT CONCRETE BASE COURSE 13"
- PORTLAND CEMENT CONCRETE PAVEMENT 14" (JOINTED)

EXISTING LEGEND

- ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- BIT CONC SURFACE COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 4 3/4"±
 - SUB-BASE GRANULAR MATERIAL, 4"±
 - SUB-BASE GRANULAR MATERIAL, 6"±
 - CRUSHED STONE, 5"±
 - PCC SHOULDERS, 9"±
 - PCC BASE COURSE, 9"±
 - COMB CONC CURB & GUTTER
 - PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - CONCRETE BARRIER WALL
 - CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - BITUMINOUS SURFACE, 7"±
 - STABILIZED SUB-BASE, 4"±
 - SUB-BASE GRANULAR MATERIAL, 12"±
 - EXISTING PIPE UNDERDRAIN
 - EXISTING FIBER OPTIC DUCT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING & PROPOSED TYPICAL SECTIONS
NB I-94 (DAN RYAN EXPRESSWAY)
(SHEET 5 OF 8)

SCALE: NONE
DATE: MARCH 7, 2006
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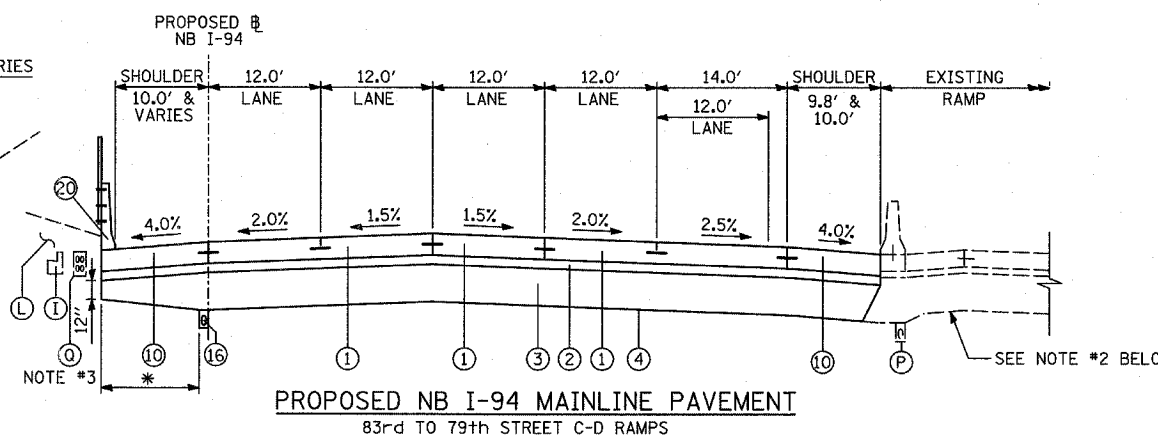
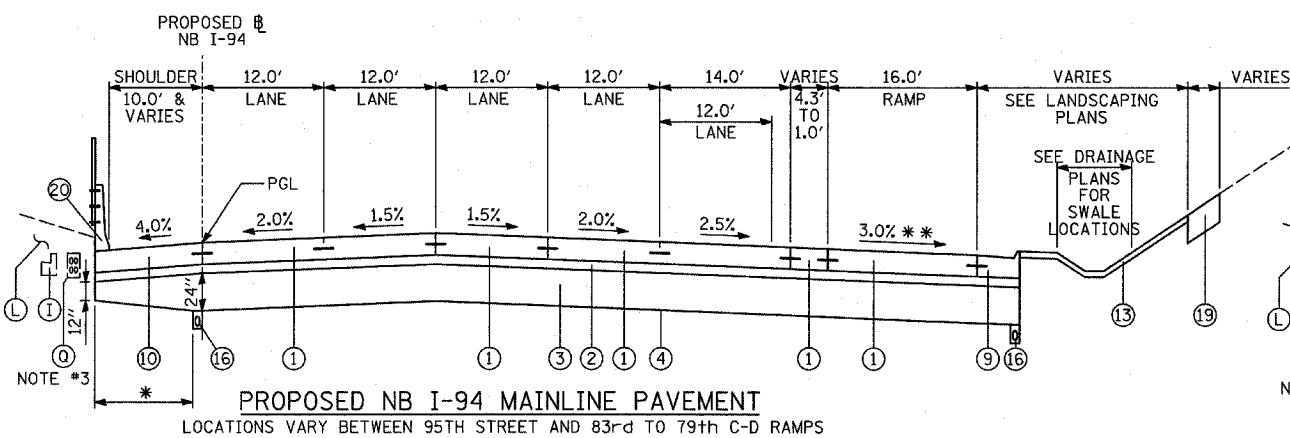
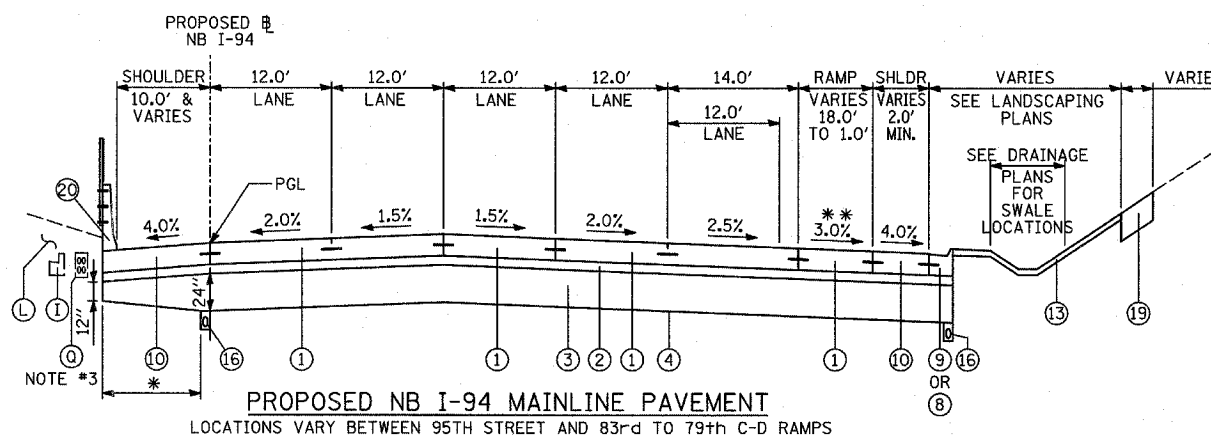
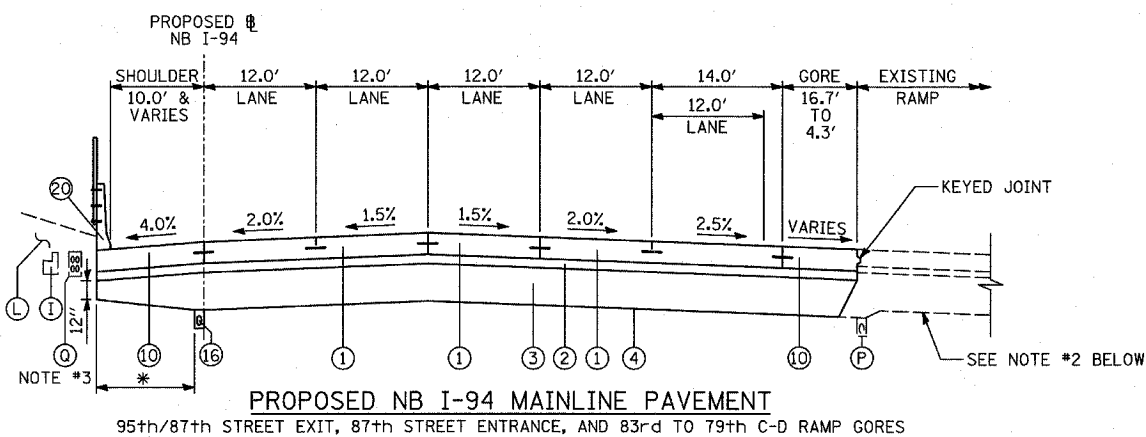


PROPOSED LEGEND

- ① CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"; & PAVEMENT REINFORCEMENT, 14"
- ② STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
- ③ SUB-BASE GRANULAR MATERIAL, TYPE B 24"
- ④ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ⑤ CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
- ⑥ CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
- ⑦ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
- ⑧ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- ⑨ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
- ⑩ PORTLAND CEMENT CONCRETE SHOULDERS 14"
- ⑪ CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- ⑫ TRAFFIC BARRIER TERMINAL, TYPE VARIES
- ⑬ TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
- ⑭ AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
- ⑮ POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, MIX "F", N105, 1 3/4"
- ⑯ PIPE UNDERDRAIN, 6" (SEE DETAILS)
- ⑰ POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
- ⑱ ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
- ⑲ TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE 6" EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
- ⑳ CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- ㉑ PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- ㉒ SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- ㉓ SUB-BASE GRANULAR MATERIAL, TYPE B 6"
- ㉔ PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ㉕ BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX "D", N70, 1 1/2"
- ㉖ PORTLAND CEMENT CONCRETE BASE COURSE 13"
- ㉗ PORTLAND CEMENT CONCRETE PAVEMENT 14" (JOINTED)

EXISTING LEGEND

- ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- (A) BIT CONC SURFACE COURSE, 1 1/2"±
 - (B) BIT CONC BINDER COURSE, 1 1/2"±
 - (C) BIT CONC BINDER COURSE, 4 3/4"±
 - (D) SUB-BASE GRANULAR MATERIAL, 4"±
 - (E) SUB-BASE GRANULAR MATERIAL, 6"±
 - (F) CRUSHED STONE, 5"±
 - (G) PCC SHOULDERS, 9"±
 - (H) PCC BASE COURSE, 9"±
 - (I) COMB CONC CURB & GUTTER
 - (J) PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - (K) CONCRETE BARRIER WALL
 - (L) CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - (M) BITUMINOUS SURFACE, 7"±
 - (N) STABILIZED SUB-BASE, 4"±
 - (O) SUB-BASE GRANULAR MATERIAL, 12"±
 - (P) EXISTING PIPE UNDERDRAIN
 - (Q) EXISTING FIBER OPTIC DUCT



NOTES:

- REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.
- ANY REQUIRED REGRADING OF EXISTING ADJACENT SUB-BASE GRANULAR MATERIAL SHALL BE INCLUDED IN THE COST OF "SUB-BASE GRANULAR MATERIAL, TYPE B 24"."
- EXACT LOCATION OF EXISTING FIBER OPTIC DUCT IS UNKNOWN. CONTRACTOR MUST NOTIFY THE CTA TO LOCATE THE DUCT PRIOR TO THE START OF WORK.

* - PAID FOR AS SUB-BASE GRANULAR MATERIAL, TYPE B 24"
** - CROSS-SLOPES VARY AT 87TH STREET ENTRANCE RAMP

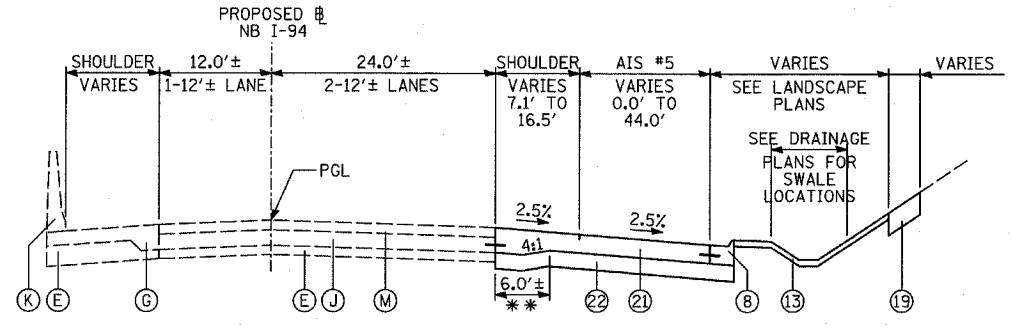
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REVISIONS	
NAME	DATE

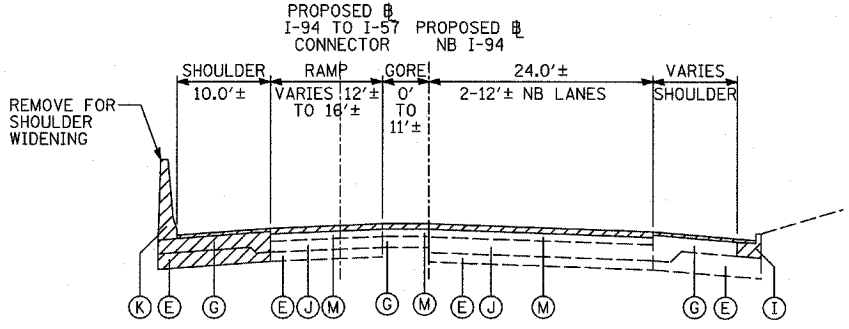
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING & PROPOSED TYPICAL SECTIONS
NB I-94 (DAN RYAN EXPRESSWAY)
(SHEET 6 OF 8)

SCALE: NONE
DATE: MARCH 7, 2006
DRAWN BY: RTM
CHECKED BY: MPG

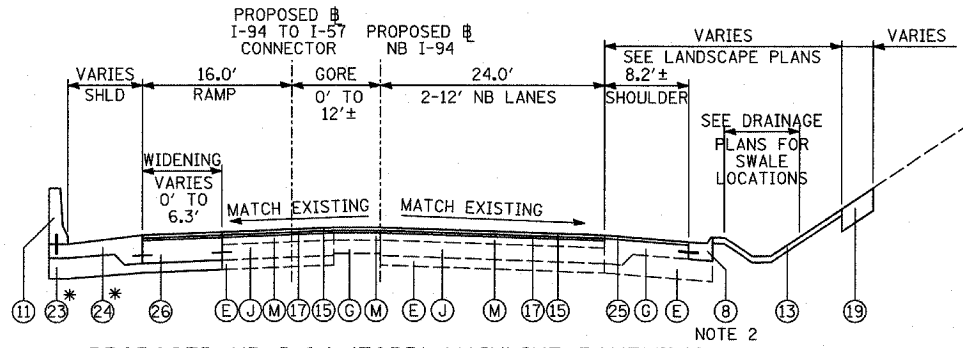
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	23
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				



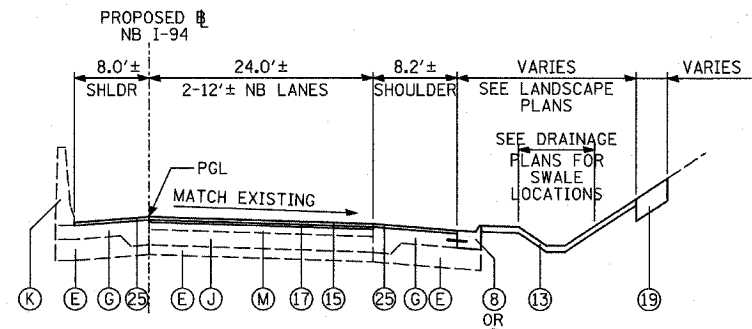
PROPOSED NB I-94 (FORD) MAINLINE PAVEMENT
AIS #5 - EAST OF S. M.L.K. JR. DR.



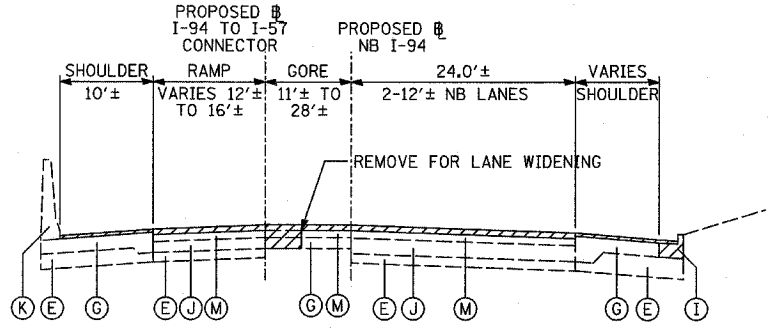
EXISTING NB I-94 (FORD) MAINLINE PAVEMENT
EAST RESURFACING LIMIT TO STA. 2006+61



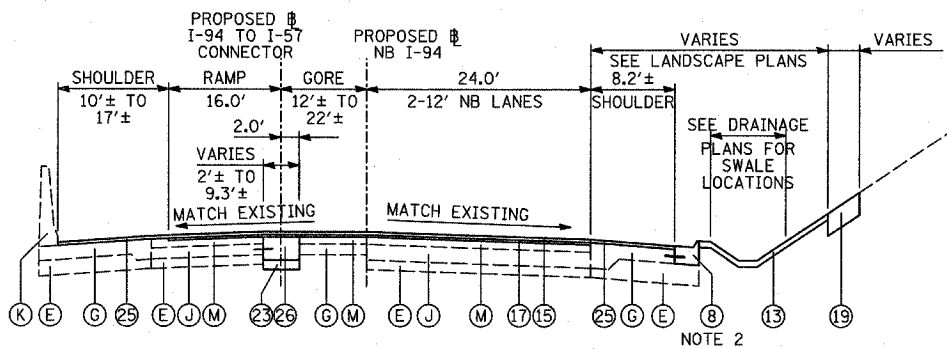
PROPOSED NB I-94 (FORD) MAINLINE PAVEMENT
EAST RESURFACING LIMIT TO STA. 2006+61



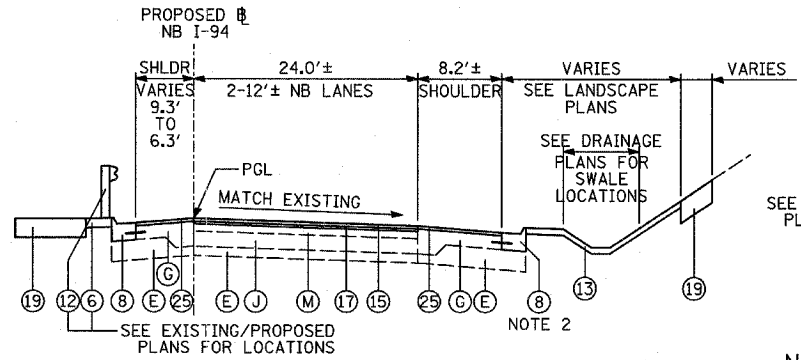
PROPOSED NB I-94 (FORD) MAINLINE PAVEMENT
LOCATIONS VARY BETWEEN SPLIT AND RECONSTRUCTION LIMIT



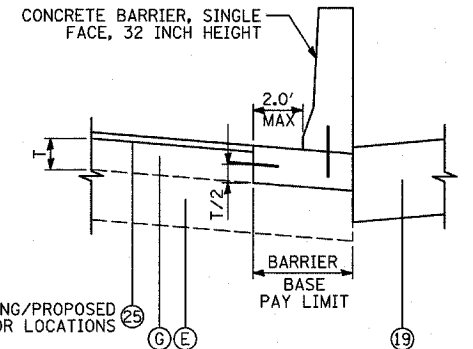
EXISTING NB I-94 (FORD) MAINLINE PAVEMENT
STA. 2006+61 TO WB CONNECTOR SPLIT



PROPOSED NB I-94 (FORD) MAINLINE PAVEMENT
STA. 2006+61 TO WB CONNECTOR SPLIT



PROPOSED NB I-94 (FORD) MAINLINE PAVEMENT
LOCATIONS VARY BETWEEN SPLIT AND RECONSTRUCTION LIMIT



NOTES:

- REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.
- PROPOSED CURB OR BARRIER BASE SHALL BE PLACED ON EXISTING SUBGRADE WITH THICKNESS EQUAL TO EXISTING ADJACENT PCC SHOULDERS. RECOMPACTING OF EXISTING SUBGRADE OR PLACEMENT OF ADDITIONAL AGGREGATE SHALL BE INCLUDED IN THE COST OF "COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24, OR "CONCRETE BARRIER, SINGLE FACE", OF THE TYPE SPECIFIED.

PROPOSED LEGEND

- CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"; & PAVEMENT REINFORCEMENT, 14"
- STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
- SUB-BASE GRANULAR MATERIAL, TYPE B 24"
- GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
- CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
- COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
- COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
- PORTLAND CEMENT CONCRETE SHOULDERS 14"
- CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- TRAFFIC BARRIER TERMINAL, TYPE VARIES
- TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
- AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL) (MATCH DEPTH TO ADJACENT CURB & GUTTER)
- POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, MIX "F", N105, 1 1/4"
- PIPE UNDERDRAIN, 6" (SEE DETAILS)
- POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
- ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
- TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE 6"; EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
- CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- SUB-BASE GRANULAR MATERIAL, TYPE B 6"
- PORTLAND CEMENT CONCRETE SHOULDERS 9"
- BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX "D", N70, 1 1/2"
- PORTLAND CEMENT CONCRETE BASE COURSE 13"
- PORTLAND CEMENT CONCRETE PAVEMENT 14" (JOINTED)

EXISTING LEGEND

- ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- BIT CONC SURFACE COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 4 3/4"±
 - SUB-BASE GRANULAR MATERIAL, 4"±
 - SUB-BASE GRANULAR MATERIAL, 6"±
 - CRUSHED STONE, 5"±
 - PCC SHOULDERS, 9"±
 - PCC BASE COURSE, 9"±
 - COMB CONC CURB & GUTTER
 - PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - CONCRETE BARRIER WALL
 - CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - BITUMINOUS SURFACE, 7"±
 - STABILIZED SUB-BASE, 4"±
 - SUB-BASE GRANULAR MATERIAL, 12"±
 - EXISTING PIPE UNDERDRAIN
 - EXISTING FIBER OPTIC DUCT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING & PROPOSED TYPICAL SECTIONS
AIS #5 / RESURFACING ALONG
NB I-94 (BISHOP FORD FREEWAY)
(SHEET 7 OF 8)

SCALE: NONE
DATE: MARCH 7, 2006
DRAWN BY: RTM
CHECKED BY: MPG



* - ADDITIONAL THICKNESS OF SUB-BASE GRANULAR MATERIAL SHALL BE INCLUDED IN THE COST OF SUB-BASE GRANULAR MATERIAL, TYPE B 6".
ADDITIONAL THICKNESS OF PORTLAND CEMENT CONCRETE PAVEMENT SHALL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE SHOULDERS 9".

** - ADDITIONAL THICKNESS OF SUB-BASE GRANULAR MATERIAL SHALL BE INCLUDED IN THE COST OF SUB-BASE GRANULAR MATERIAL, TYPE B 12".
ADDITIONAL THICKNESS OF PORTLAND CEMENT CONCRETE PAVEMENT SHALL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE PAVEMENT, 10" (JOINTED).

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	24
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516, 1, 1717, & 1818) R-4				

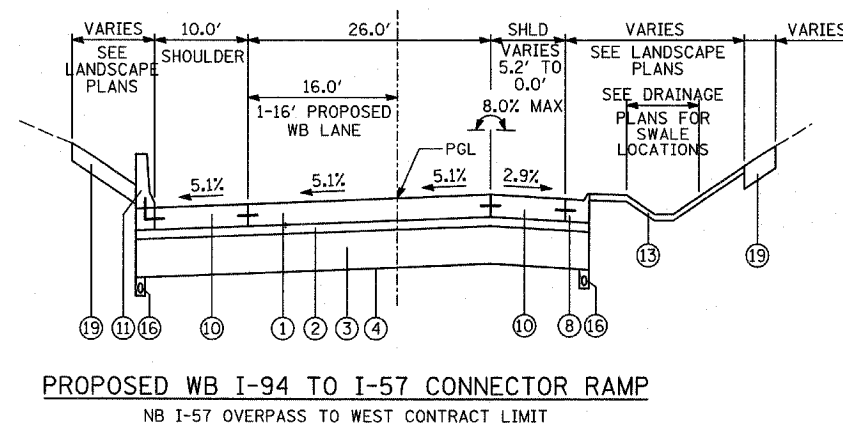
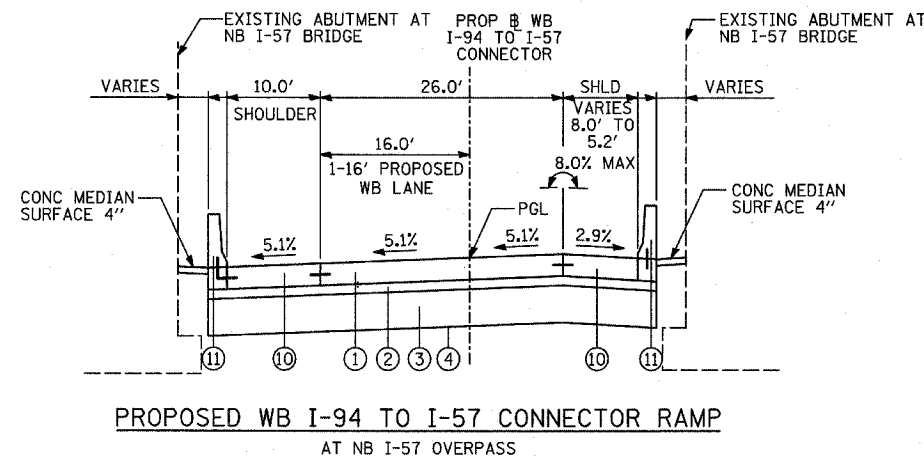
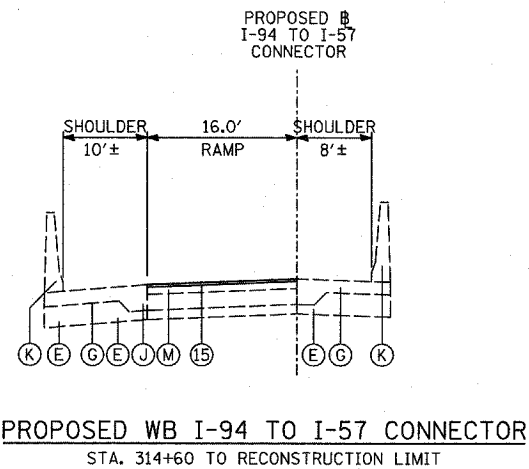
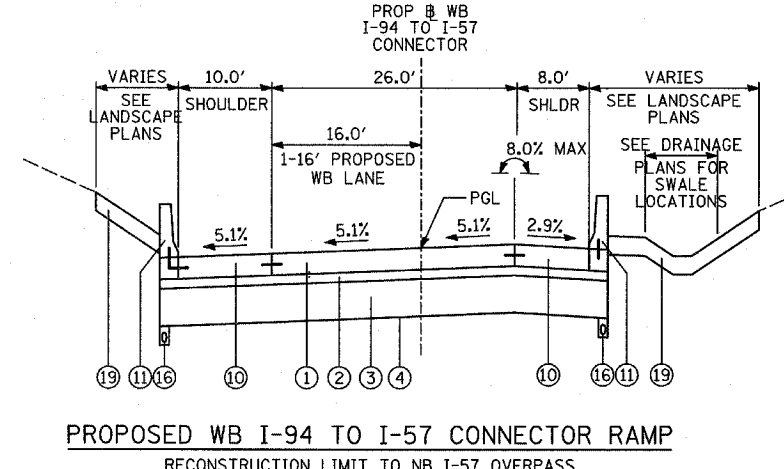
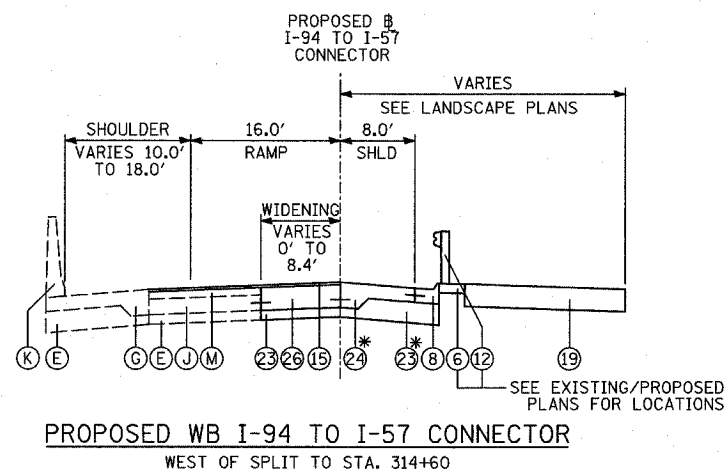
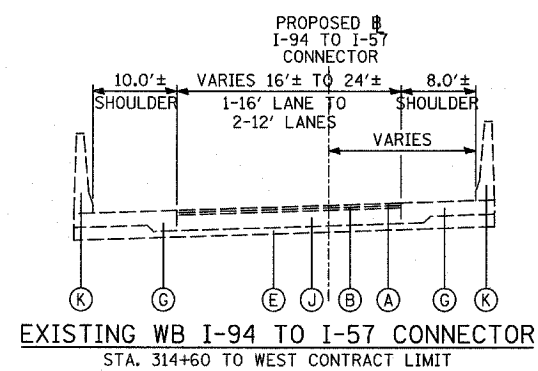
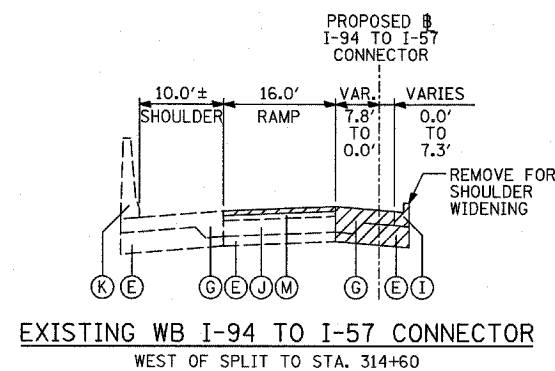
PROPOSED LEGEND 62304

- 1 CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"; & PAVEMENT REINFORCEMENT, 14"
- 2 STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
- 3 SUB-BASE GRANULAR MATERIAL, TYPE B 24"
- 4 GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- 5 CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
- 6 CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
- 7 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
- 8 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- 9 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
- 10 PORTLAND CEMENT CONCRETE SHOULDERS 14"
- 11 CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- 12 TRAFFIC BARRIER TERMINAL, TYPE VARIES
- 13 TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
- 14 AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
- 15 POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, MIX "F", N105, 1 3/4"
- 16 PIPE UNDERDRAIN, 6" (SEE DETAILS)
- 17 POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
- 18 ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
- 19 TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE 6" EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
- 20 CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- 21 PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- 22 SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- 23 SUB-BASE GRANULAR MATERIAL, TYPE B 6"
- 24 PORTLAND CEMENT CONCRETE SHOULDERS 9"
- 25 BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX "D", N70, 1 1/2"
- 26 PORTLAND CEMENT CONCRETE BASE COURSE 13"
- 27 PORTLAND CEMENT CONCRETE PAVEMENT 14" (JOINTED)

EXISTING LEGEND

ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE

- A BIT CONC SURFACE COURSE, 1 1/2"±
- B BIT CONC BINDER COURSE, 1 1/2"±
- C BIT CONC BINDER COURSE, 4 3/4"±
- D SUB-BASE GRANULAR MATERIAL, 4"±
- E SUB-BASE GRANULAR MATERIAL, 6"±
- F CRUSHED STONE, 5"±
- G PCC SHOULDERS, 9"±
- H PCC BASE COURSE, 9"±
- I COMB CONC CURB & GUTTER
- J PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
- K CONCRETE BARRIER WALL
- L CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
- M BITUMINOUS SURFACE, 7"±
- N STABILIZED SUB-BASE, 4"±
- O SUB-BASE GRANULAR MATERIAL, 12"±
- P EXISTING PIPE UNDERDRAIN
- Q EXISTING FIBER OPTIC DUCT



NOTES:

1. REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.

* - ADDITIONAL THICKNESS OF SUB-BASE GRANULAR MATERIAL SHALL BE INCLUDED IN THE COST OF SUB-BASE GRANULAR MATERIAL, TYPE B 6".
 ADDITIONAL THICKNESS OF OF PORTLAND CEMENT CONCRETE PAVEMENT SHALL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE SHOULDERS 9".



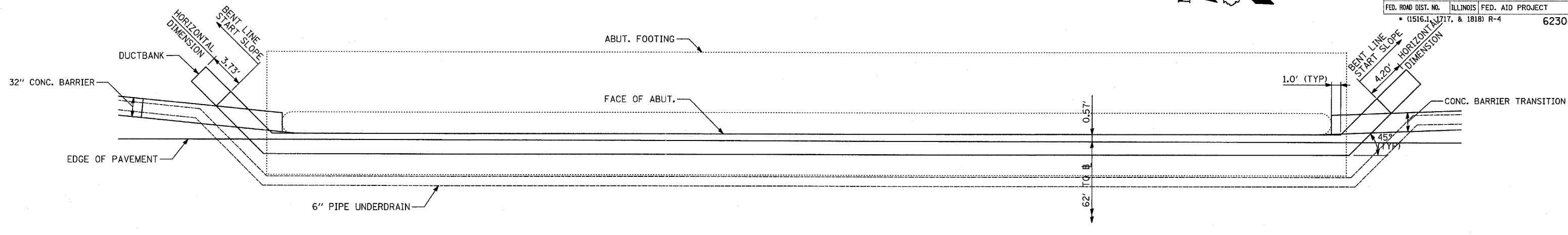
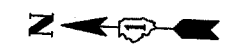
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REVISIONS	
NAME	DATE

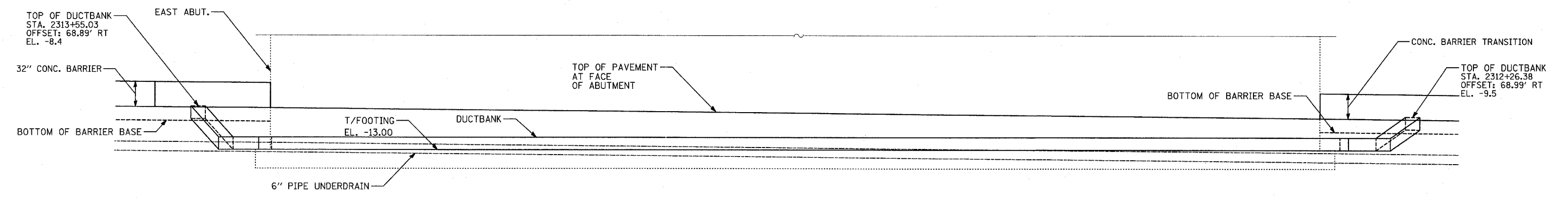
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING & PROPOSED TYPICAL SECTIONS
 WB I-57 TO I-94 CONNECTOR
 (SHEET 8 OF 8)

SCALE: NONE DRAWN BY: RTM
 DATE: MARCH 7, 2006 CHECKED BY: MPG

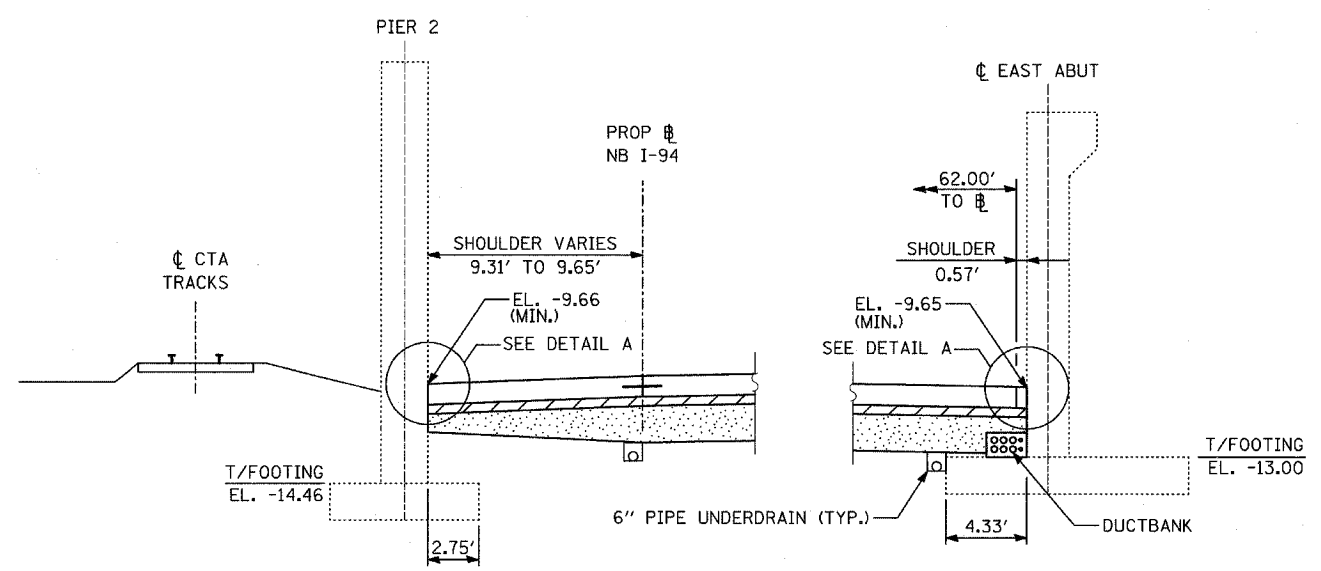
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	24A
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				
				62304



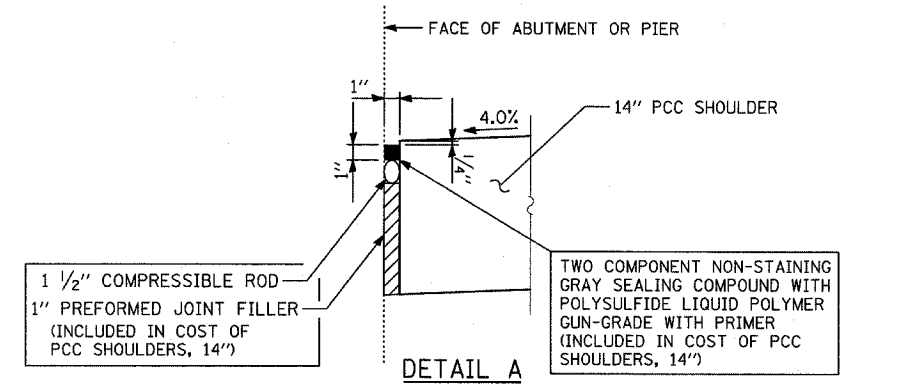
PLAN



ELEVATION



SHOULDER DETAIL AT 79th STREET



DETAIL A
SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"

NOTES

- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
- ALL VIEWS LOOKING UPSTATION.
- CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
- ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
- THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
- REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
- MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

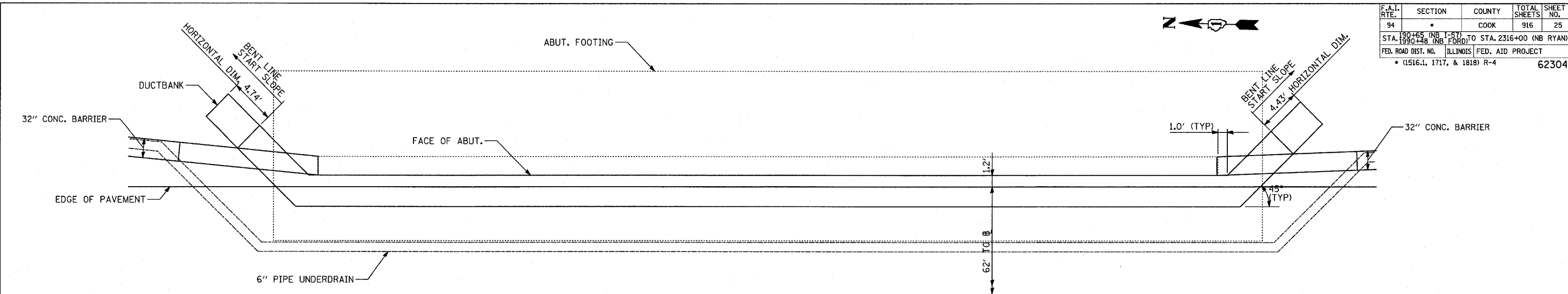
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
TYPICAL SECTIONS AND DETAILS:
79TH. STREET OVERPASS FOOTINGS,
DUCTBANK & UNDERDRAIN DETAILS

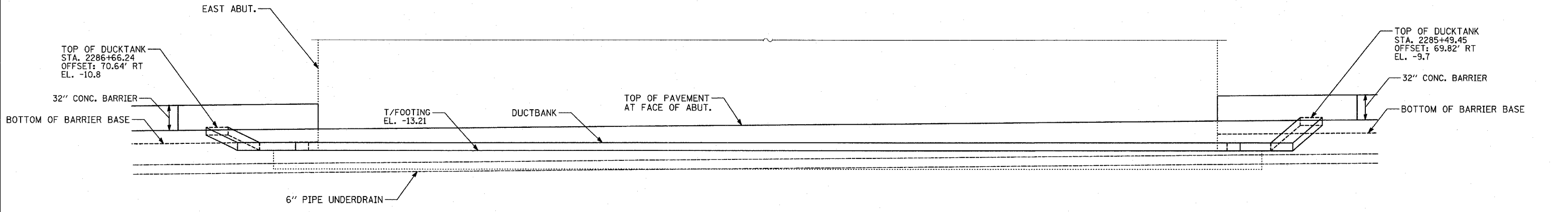
SCALE: NONE
DATE: MARCH 7, 2006
DRAWN BY: TB
CHECKED BY: TGB



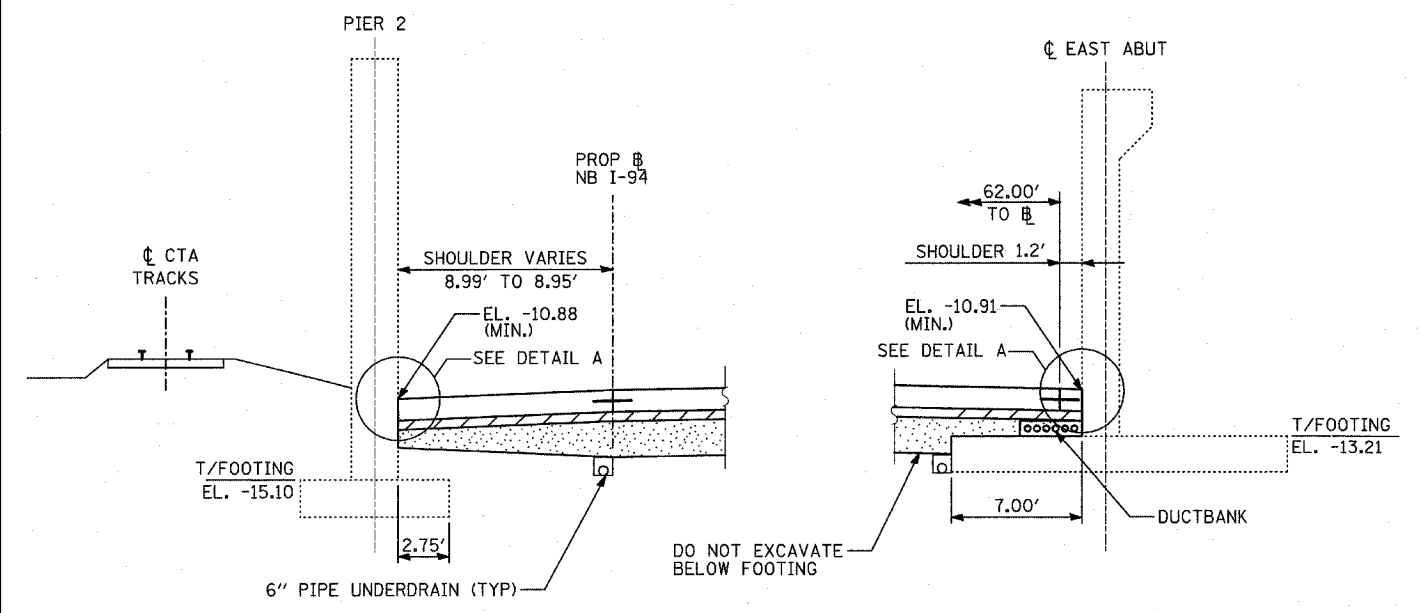
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	25
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				



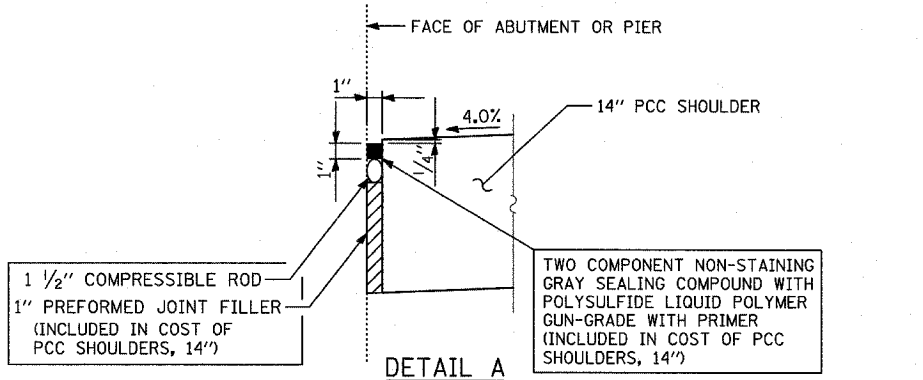
PLAN



ELEVATION



SHOULDER DETAIL AT 83rd STREET



DETAIL A

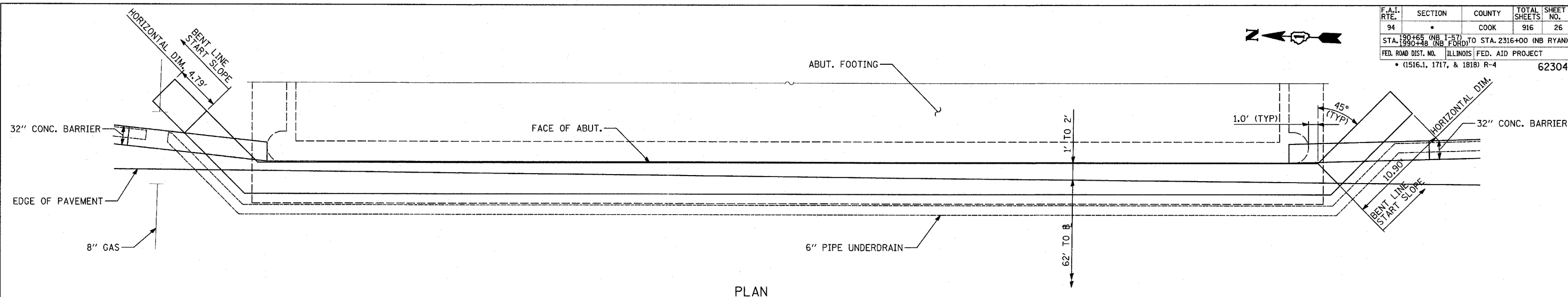
NOTES

1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
6. REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
7. MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

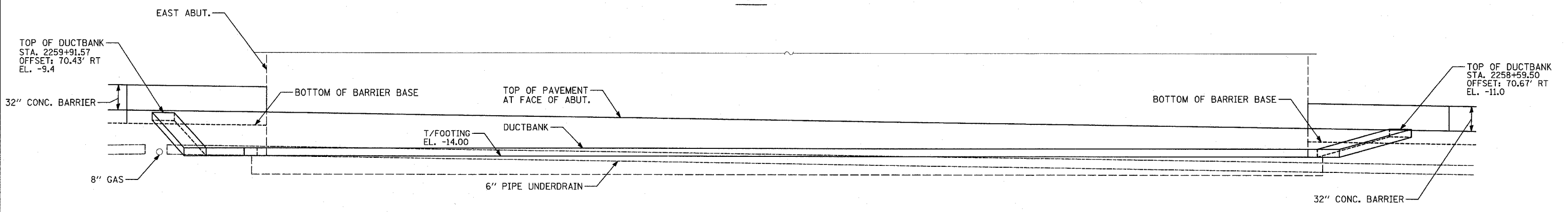
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 83RD STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS

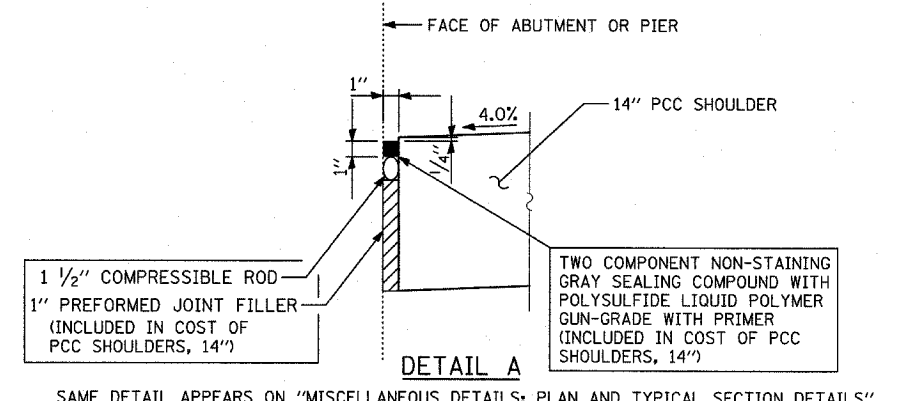
SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: TB
 CHECKED BY: TGB



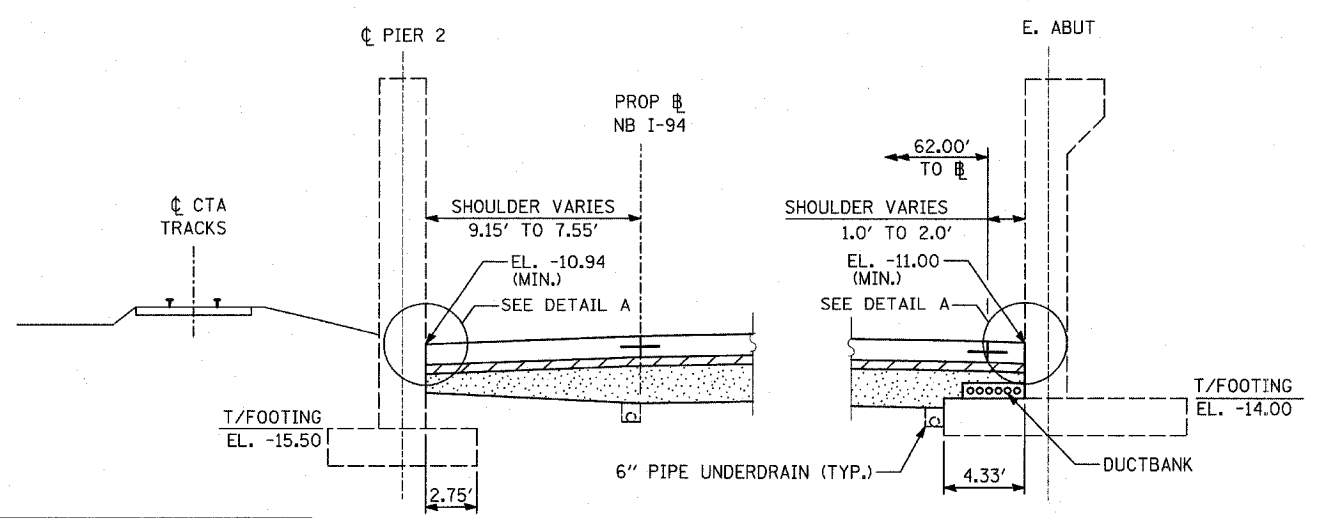
PLAN



ELEVATION



SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"



SHOULDER DETAIL AT 87th STREET

NOTES

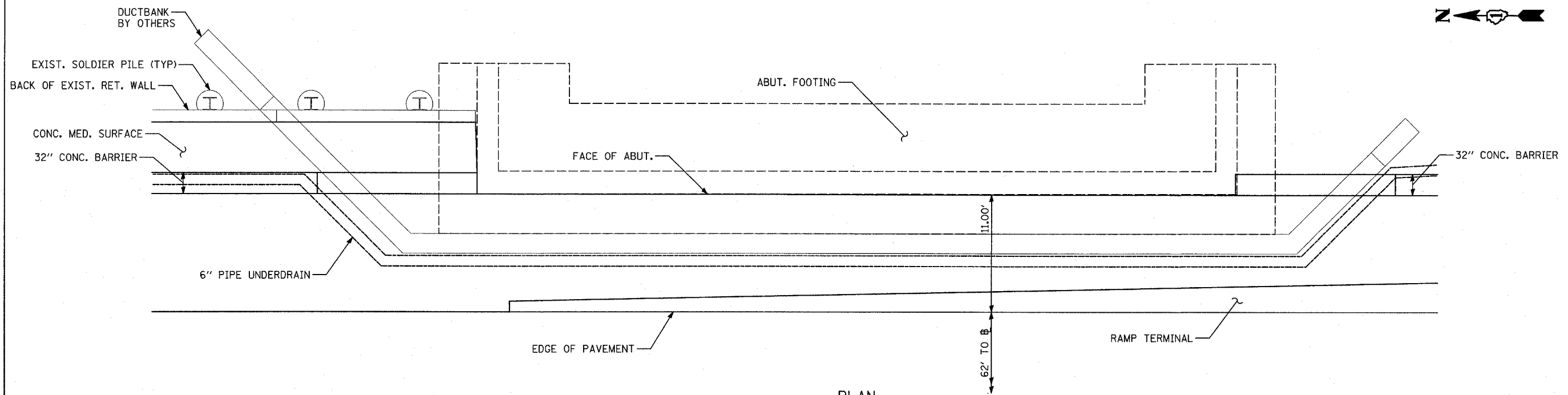
1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
6. REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
7. MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

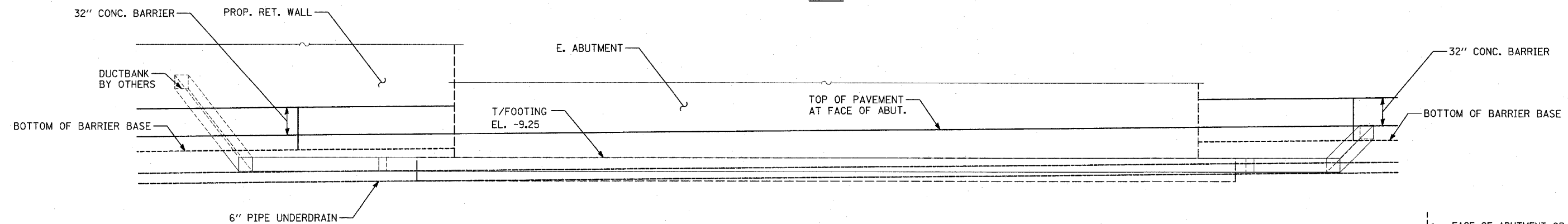
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 87TH STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS

SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: TB
 CHECKED BY: TCB

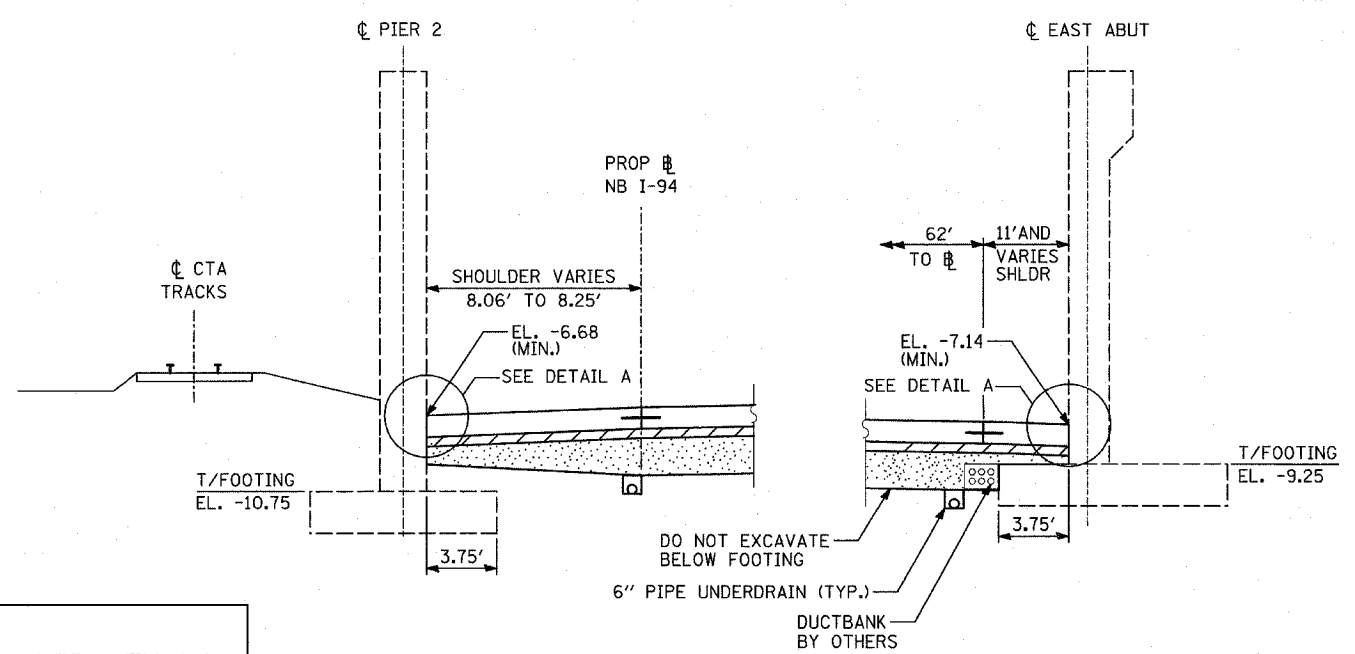




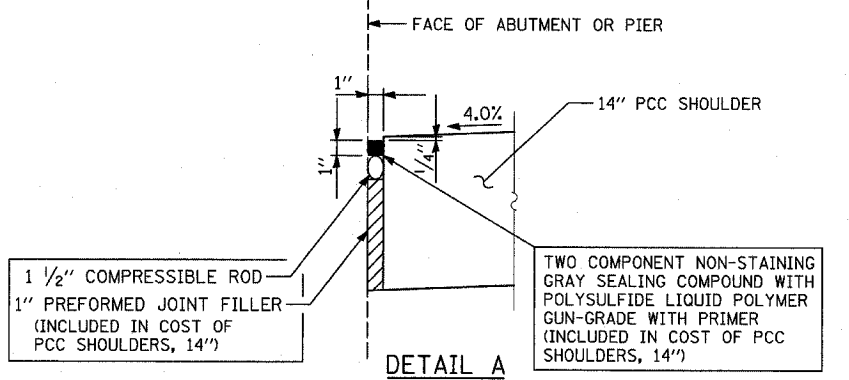
PLAN



ELEVATION



SHOULDER DETAIL AT 91st STREET



SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"

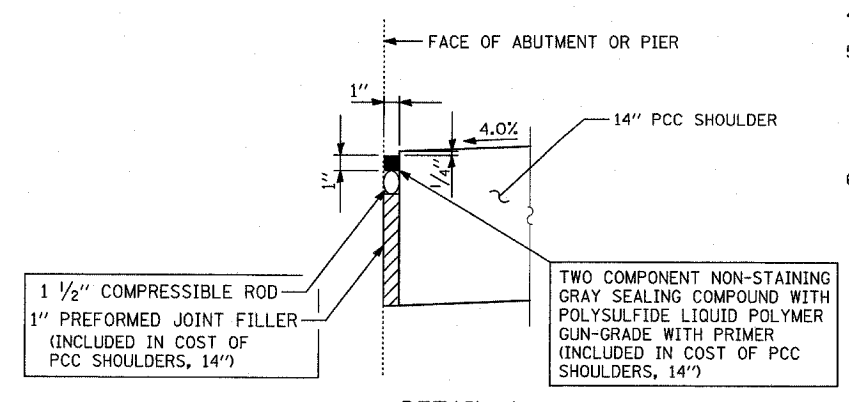
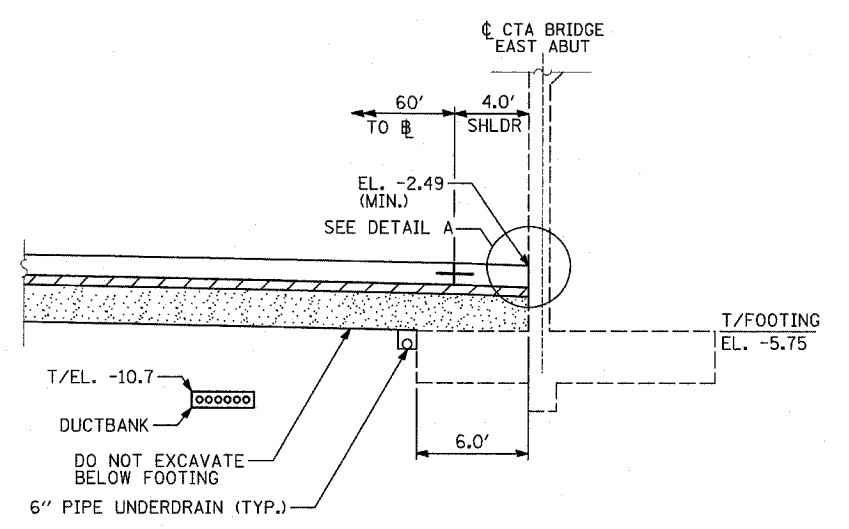
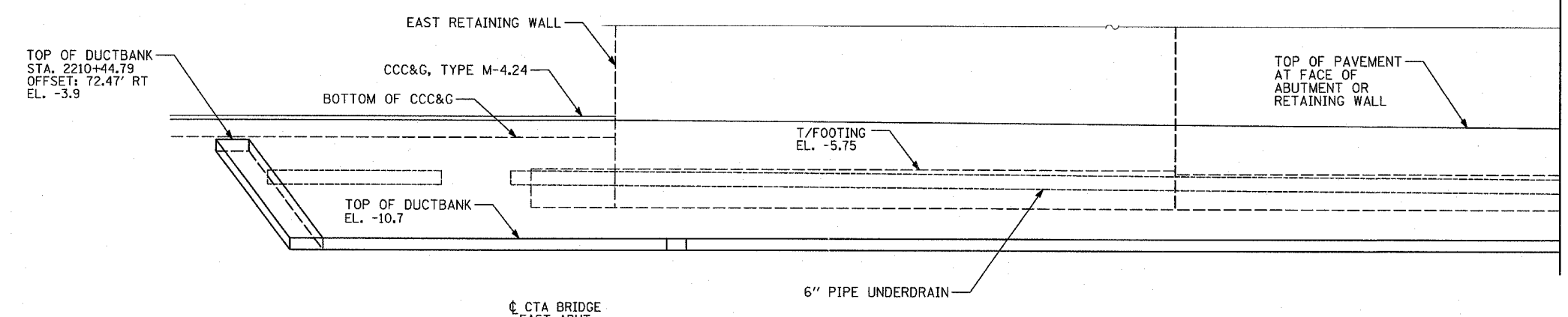
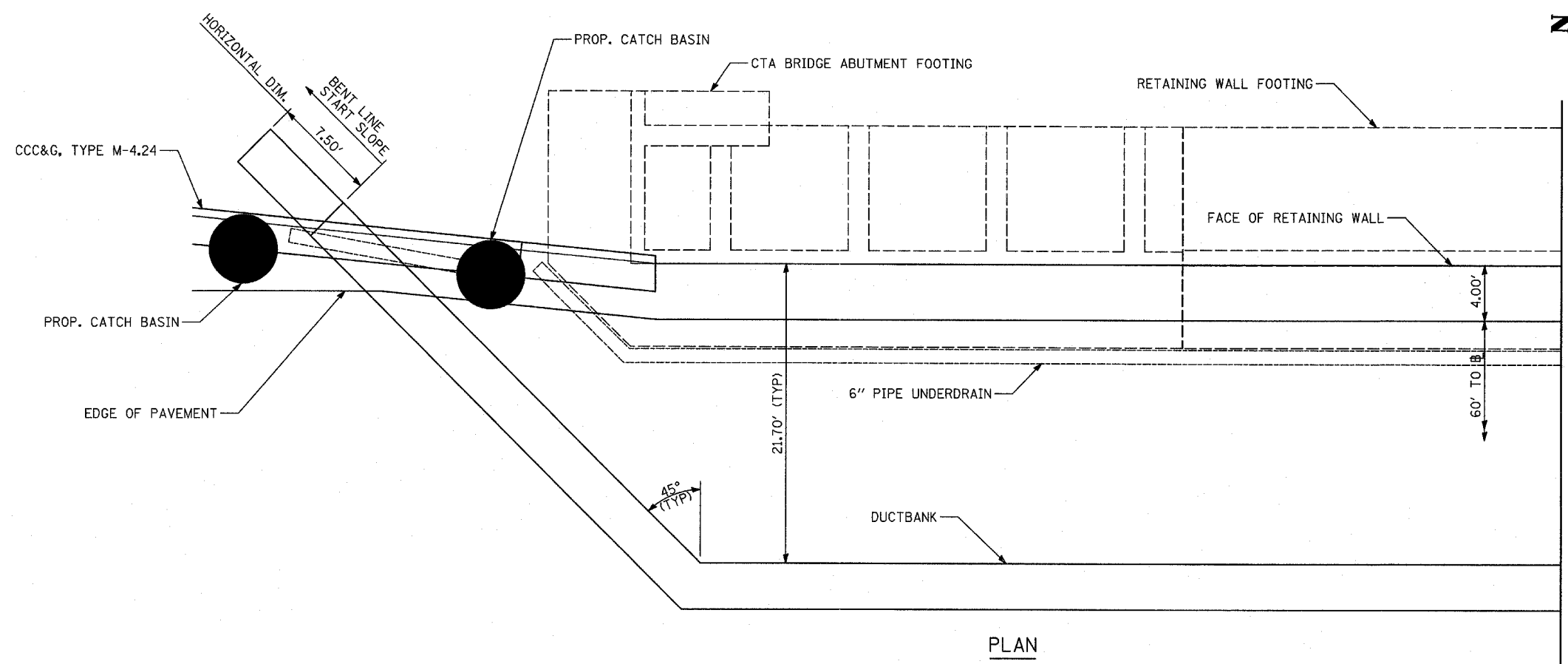
NOTES

1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDER, 14"
6. REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
7. MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 91ST STREET OVERPASS FOOTINGS
 & UNDERDRAIN DETAILS

SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: TB
 CHECKED BY: TGB



MATCHLINE STA. 2209+50

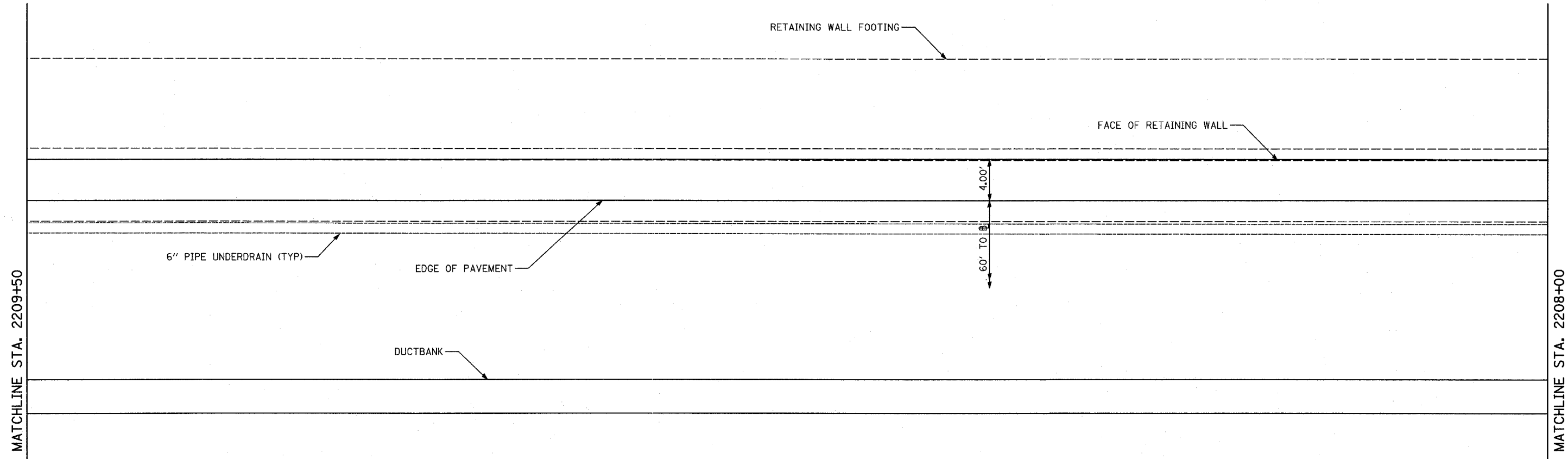
- NOTES**
- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
 - ALL VIEWS LOOKING UPSTATION.
 - CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
 - ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
 - THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
 - REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.

REVISIONS	
NAME	DATE

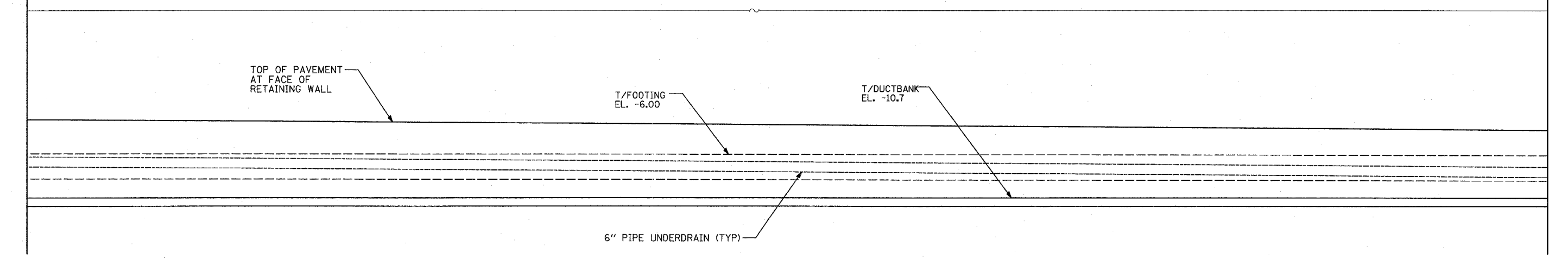
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 95TH STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS
 SHEET 1 OF 4

SCALE: NONE
 DATE: MARCH 7, 2006

DRAWN BY: TB, PL
 CHECKED BY: TGB



PLAN



ELEVATION

MATCHLINE STA. 2209+50

MATCHLINE STA. 2208+00

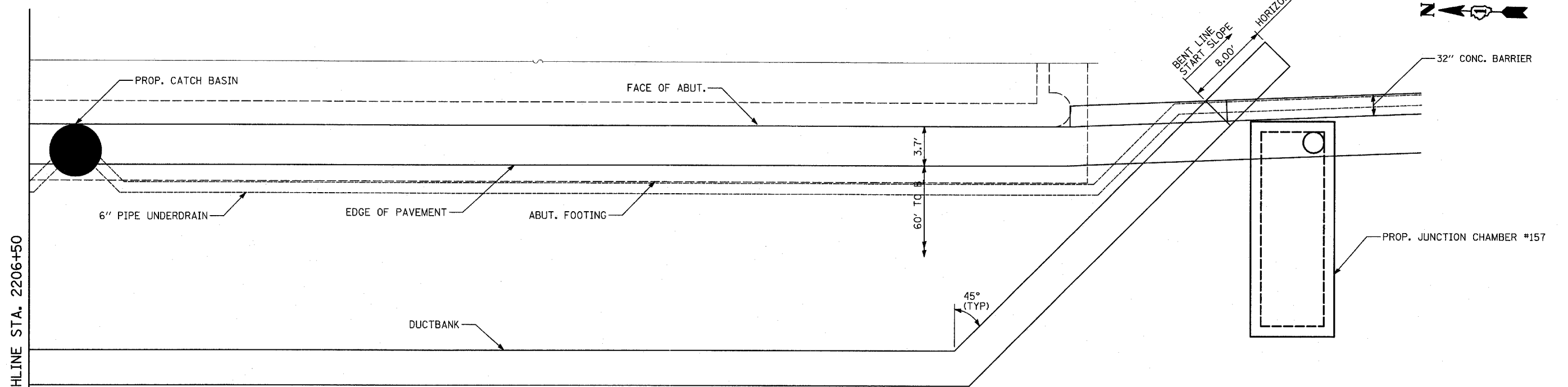
TYLININTERNATIONAL

REVISIONS	
NAME	DATE

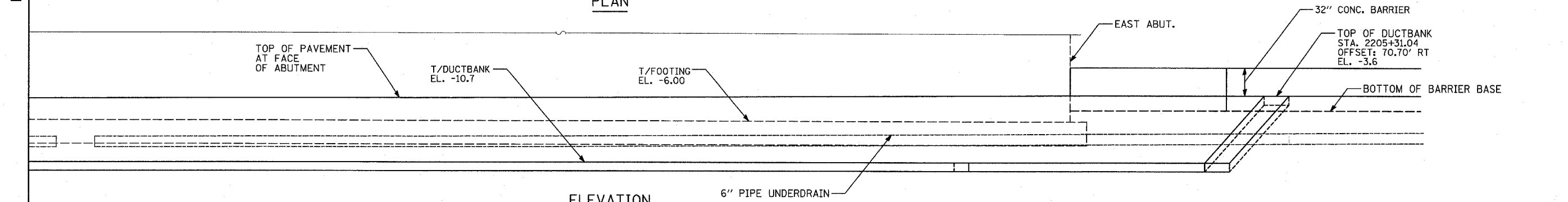
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 95TH STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS
 SHEET 2 OF 4

SCALE: NONE
 DATE: MARCH 7, 2006

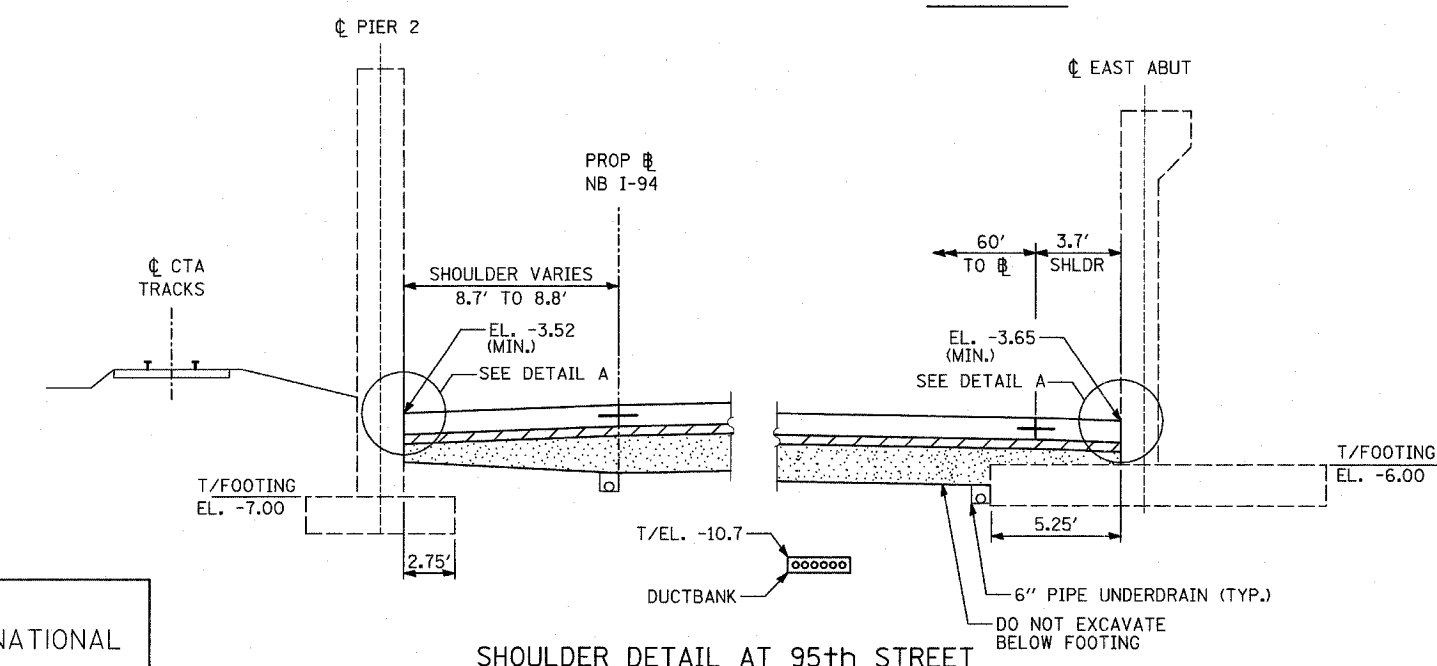
DRAWN BY: TB, PL
 CHECKED BY: TGB



PLAN



ELEVATION

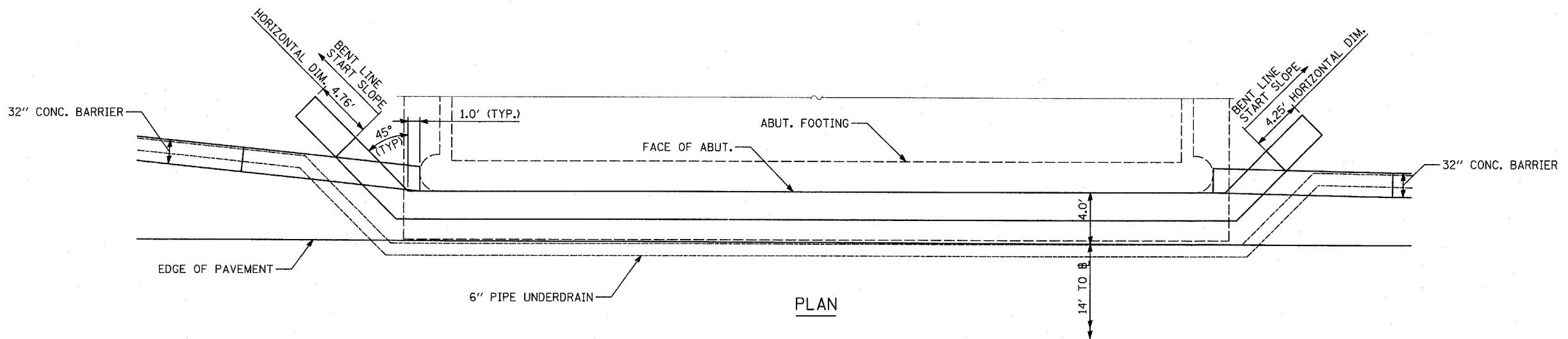


SHOULDER DETAIL AT 95th STREET

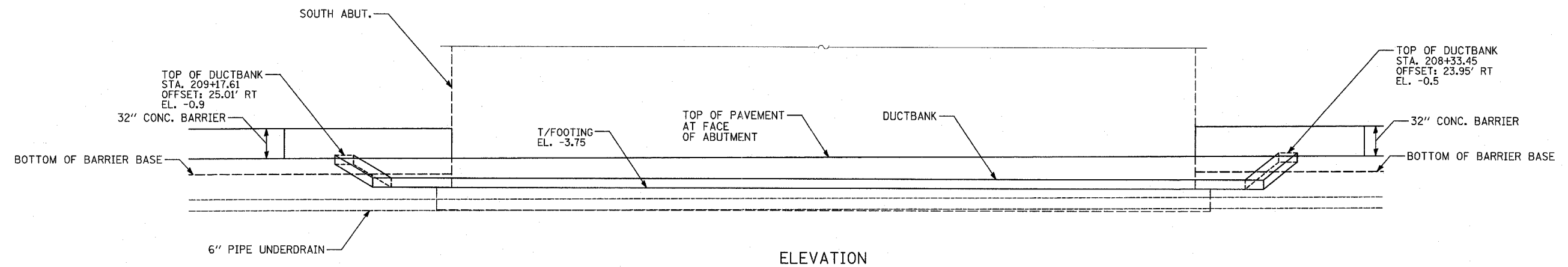
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 95TH STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS
 SHEET 4 OF 4
 SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: TB, PL
 CHECKED BY: TGB

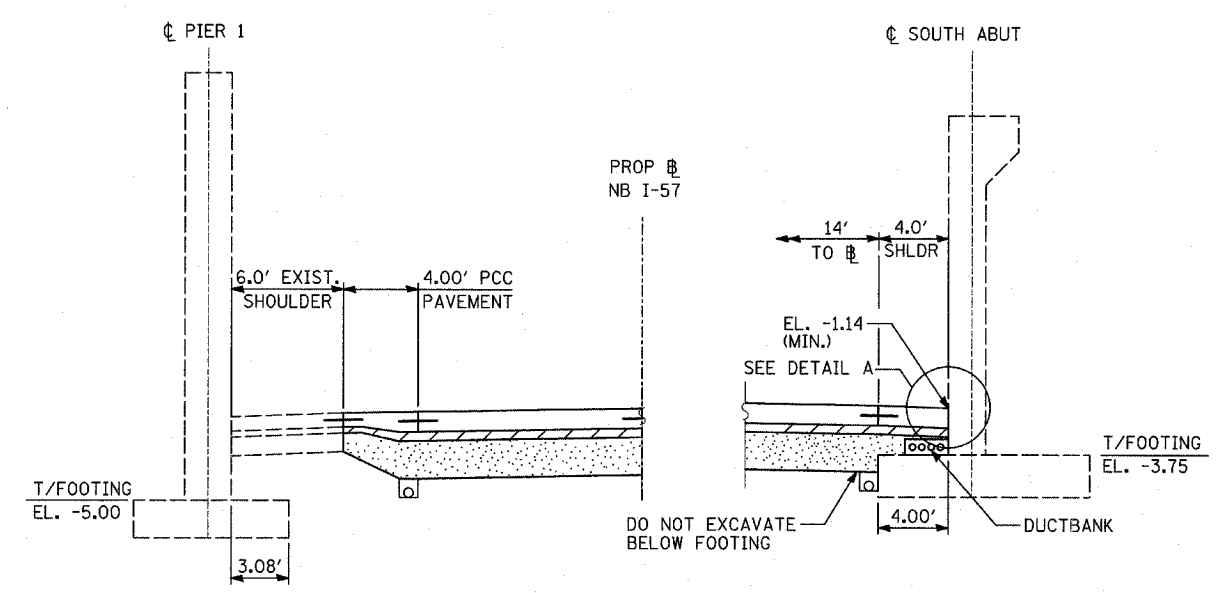
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	32
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				



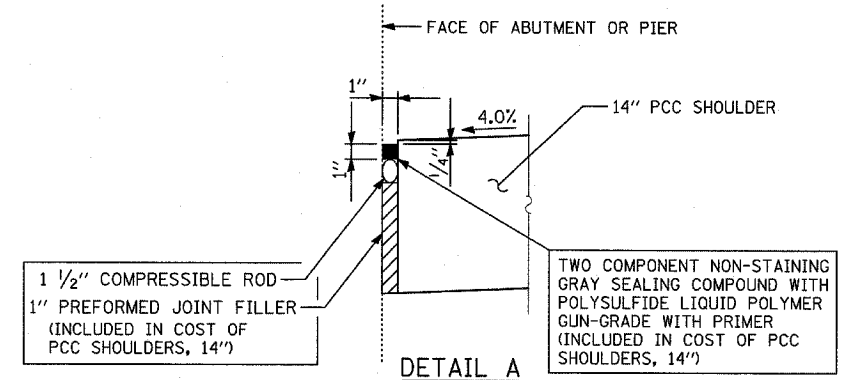
PLAN



ELEVATION



SHOULDER DETAIL AT S. PARNELL AVENUE



DETAIL A

SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"

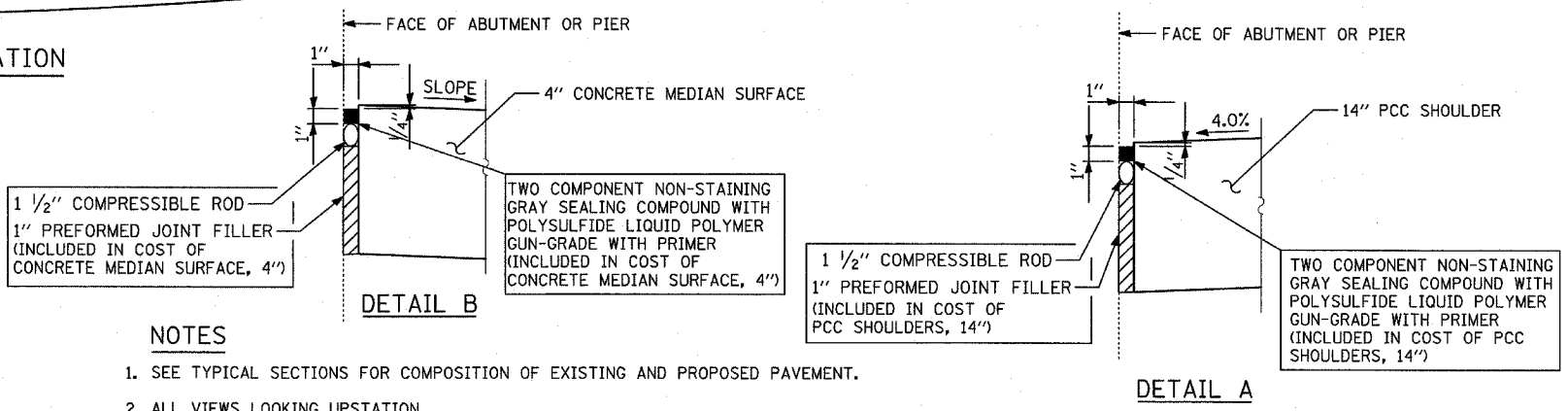
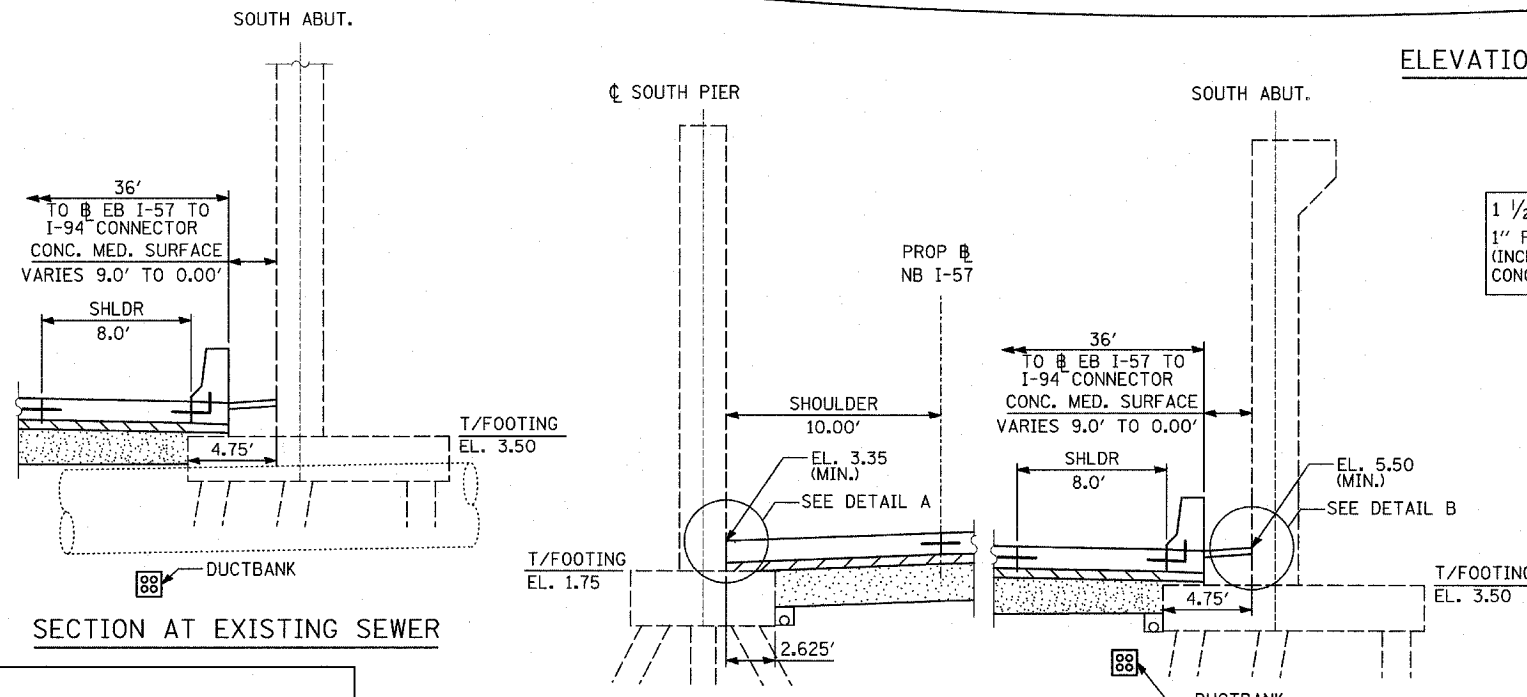
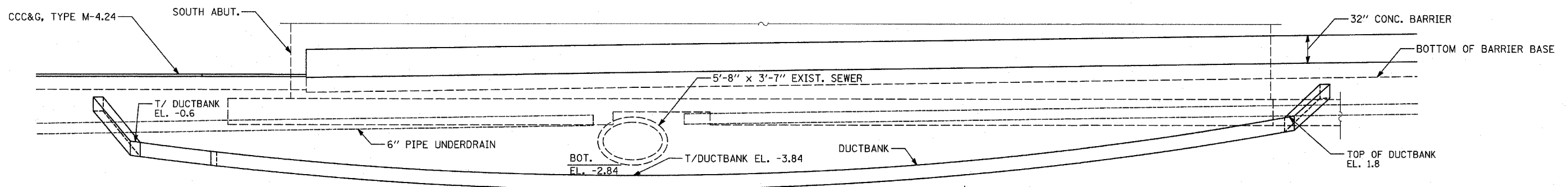
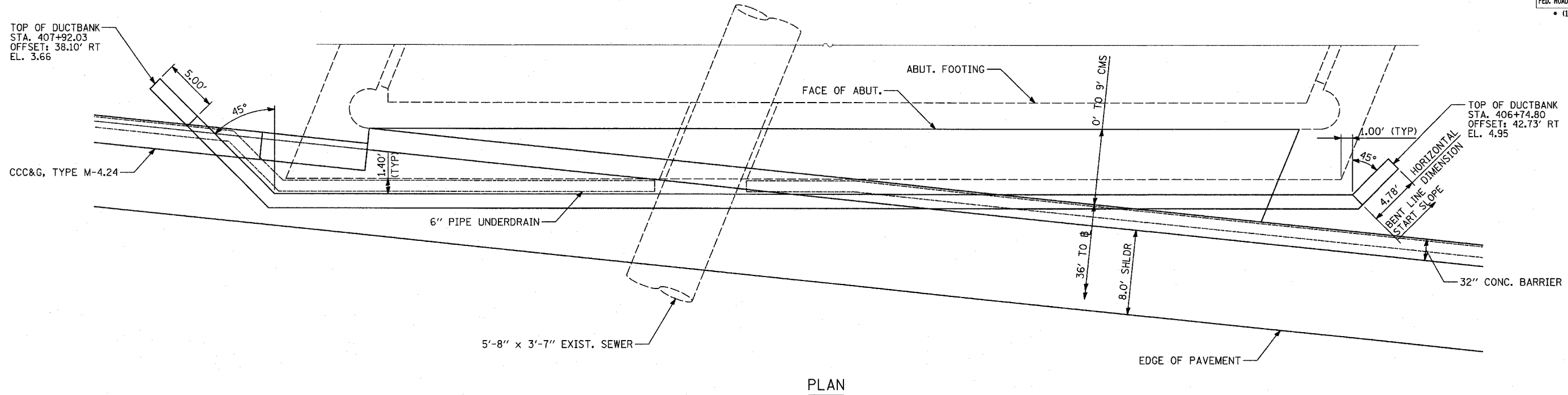
NOTES

- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
- ALL VIEWS LOOKING UPSTATION.
- CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
- ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
- THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
- REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
- MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 PARNELL AVENUE OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS

SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: TB
 CHECKED BY: TGB

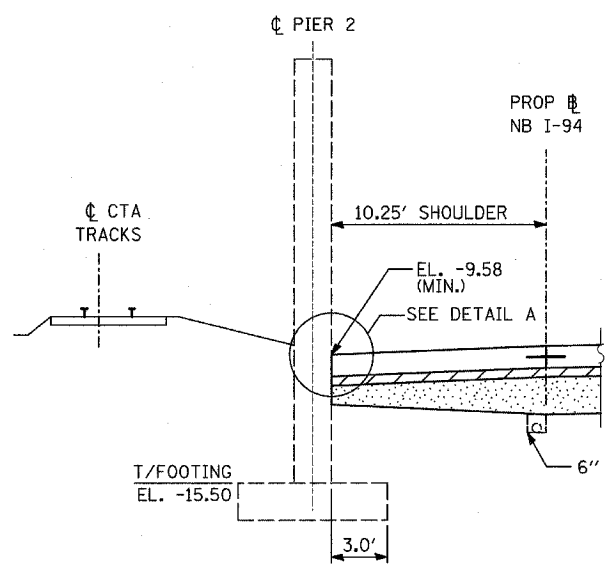


- NOTES**
- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
 - ALL VIEWS LOOKING UPSTATION.
 - CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
 - ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
 - THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14\"
 - REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
 - CONTRACTOR SHALL SUPPORT EXISTING SEWER WHEN EXCAVATING FOR DUCTBANK.
 - MAINTAIN 3\"

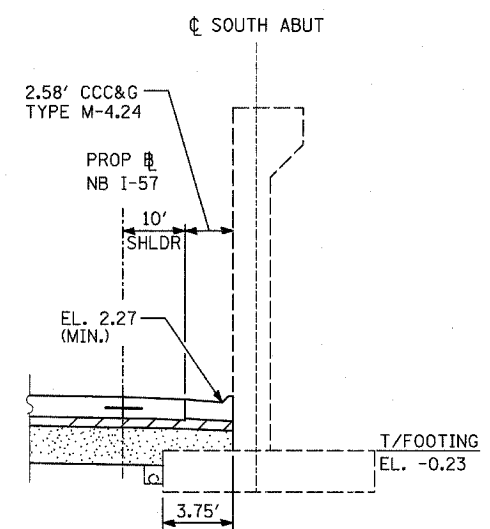
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
TYPICAL SECTIONS AND DETAILS:
WENTWORTH AVE. OVERPASS FOOTINGS,
DUCTBANK & UNDERDRAIN DETAILS

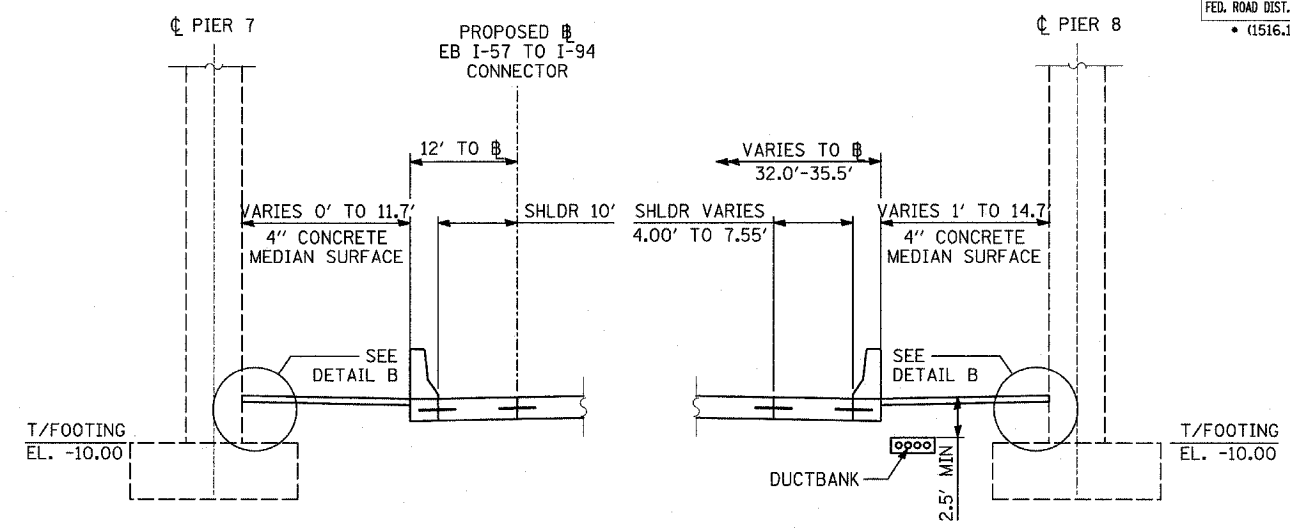
SCALE: NONE
DATE: MARCH 7, 2006
DRAWN BY: TB
CHECKED BY: TGB



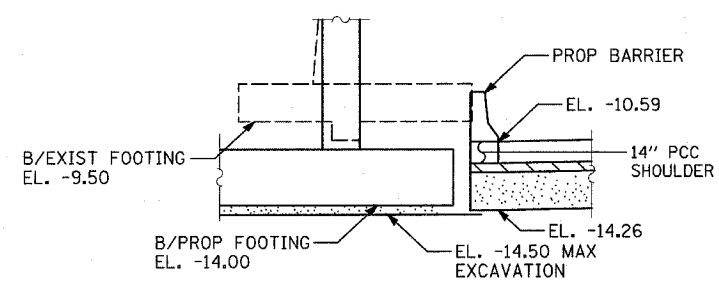
SHOULDER DETAIL AT 76TH STREET



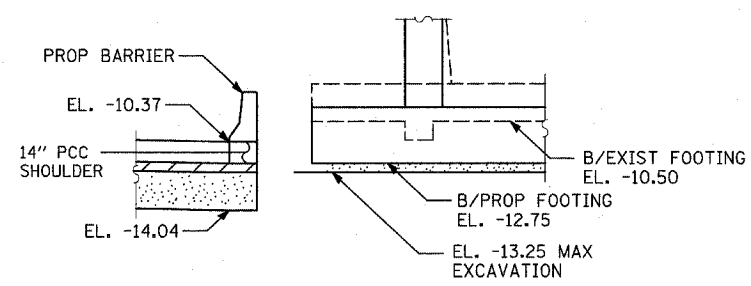
SHOULDER DETAIL AT S. HALSTED STREET



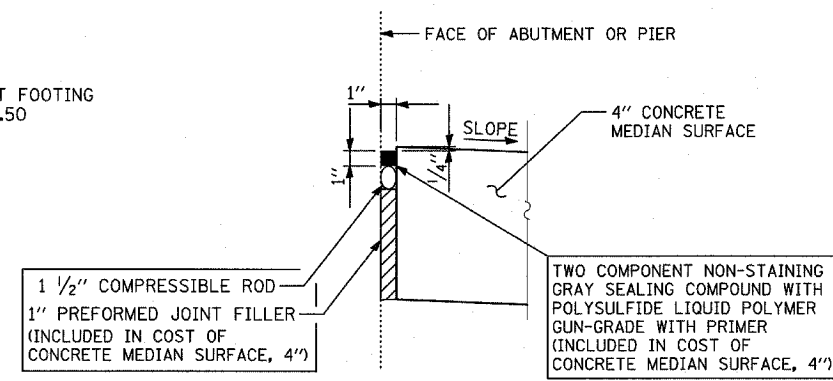
SHOULDER DETAIL AT STATE STREET



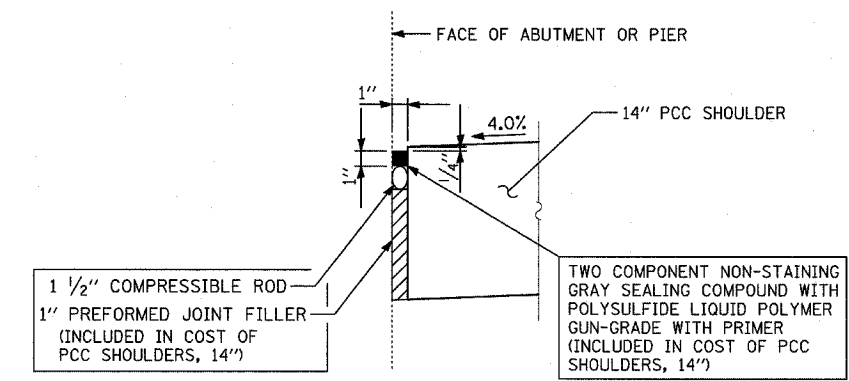
NB 57 OVER WB CONNECTOR
S. ABUTMENT EAST END



NB 57 OVER WB CONNECTOR
N. ABUTMENT EAST END

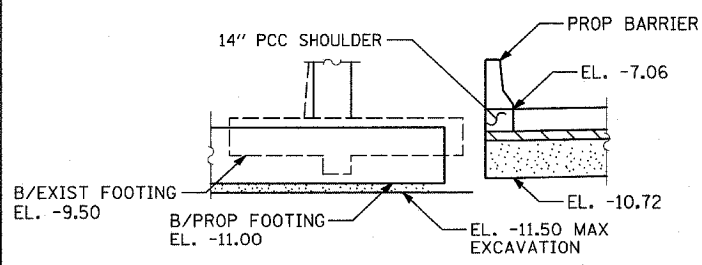


DETAIL B

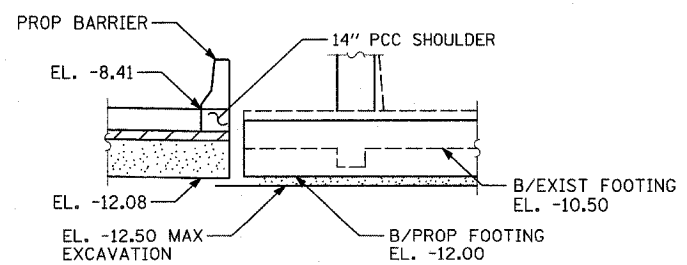


DETAIL A

SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"



NB 57 OVER WB CONNECTOR
S. ABUTMENT WEST END



NB 57 OVER WB CONNECTOR
N. ABUTMENT WEST END

NOTES

- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
- ALL VIEWS LOOKING UPSTATION.
- CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
- ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
- THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
- REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
- MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

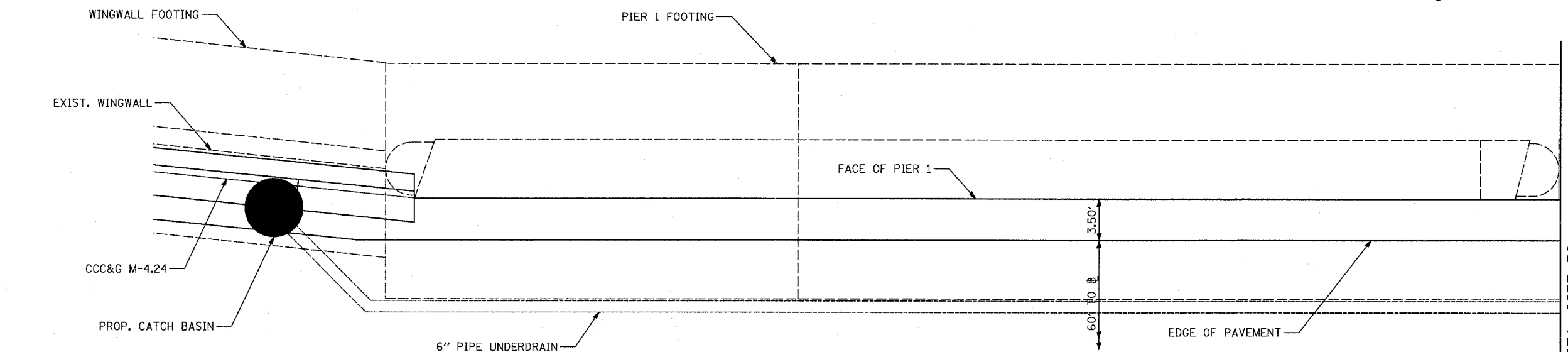
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

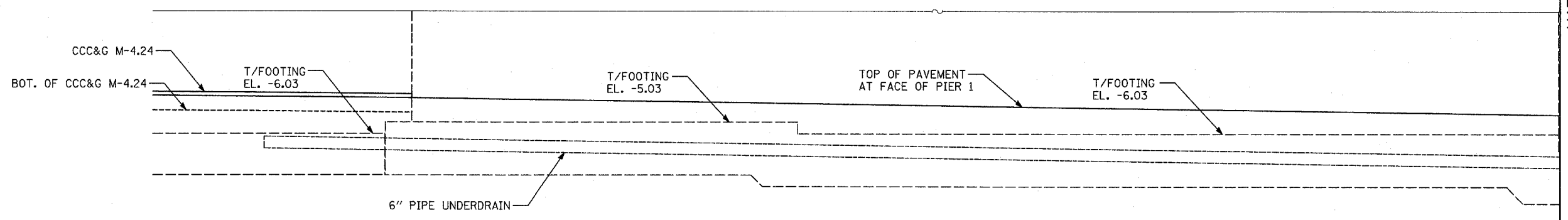
TYPICAL SECTIONS AND DETAILS:
OVERPASS FOOTINGS

SCALE: NONE
DATE: MARCH 7, 2006

DRAWN BY: KA
CHECKED BY: TGB

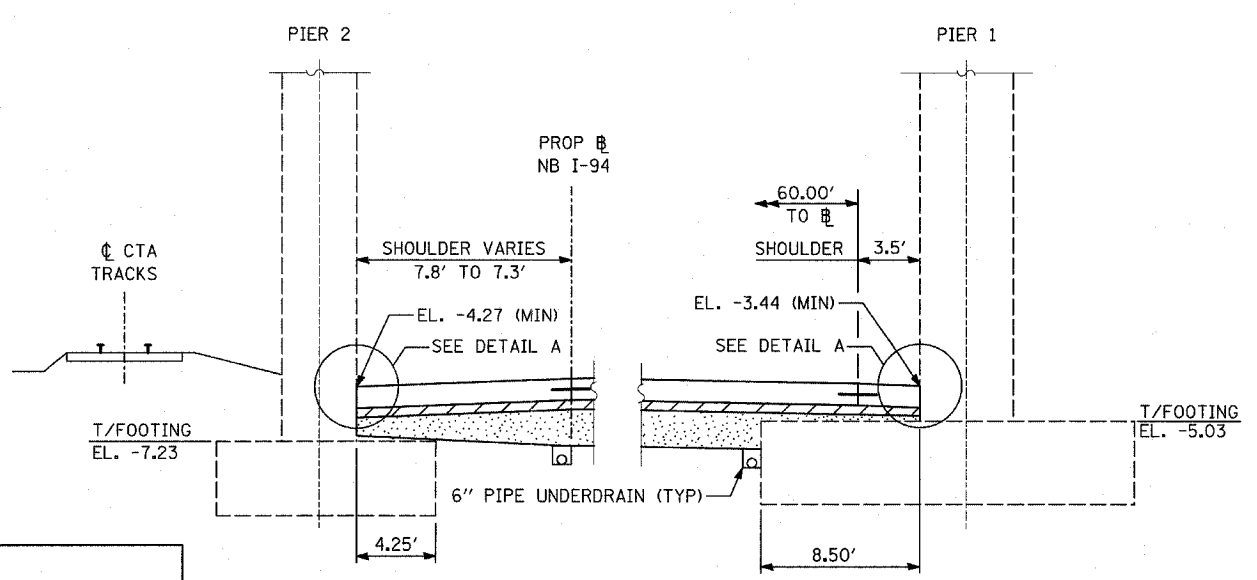


PLAN

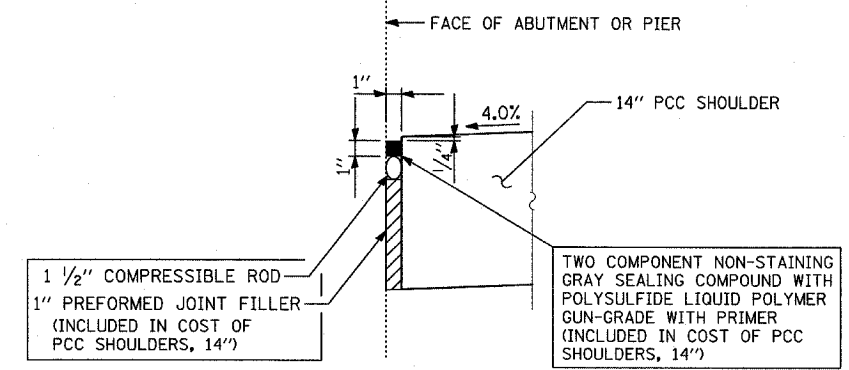


ELEVATION

MATCHLINE STA. 2237+50



SHOULDER DETAIL AT BELT RAILWAY



DETAIL A

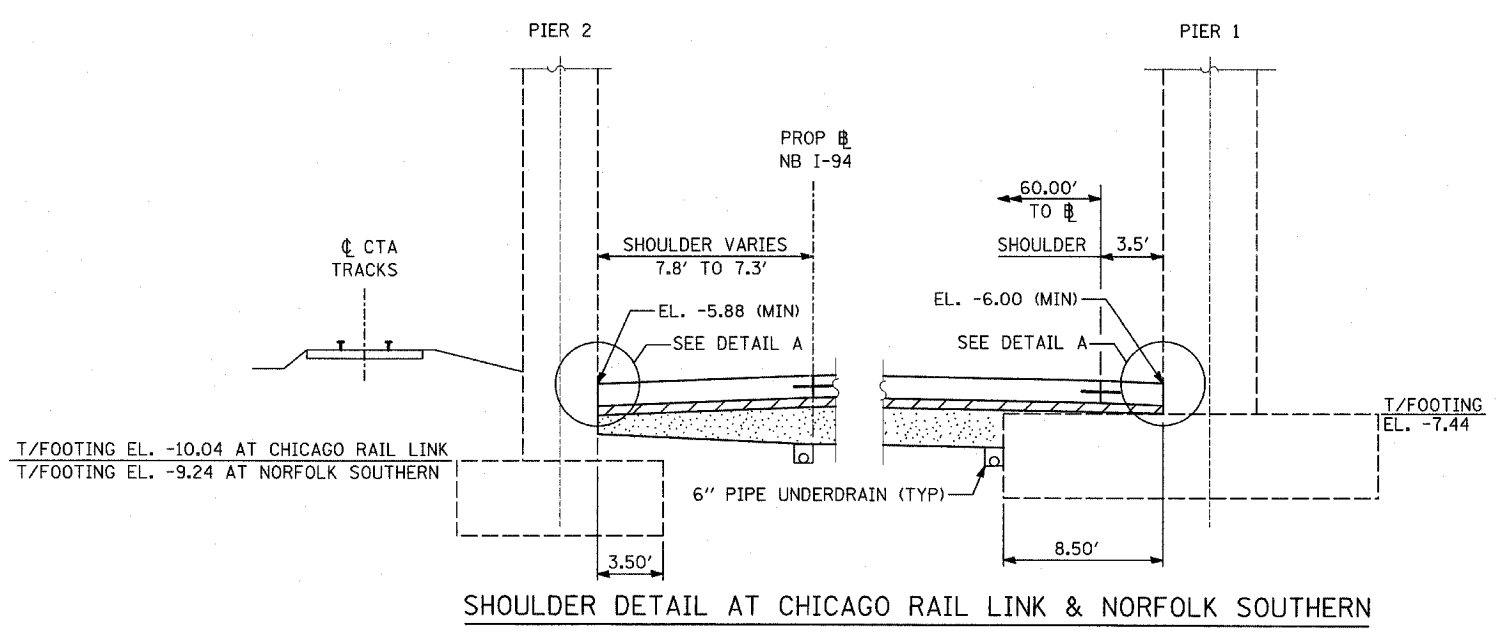
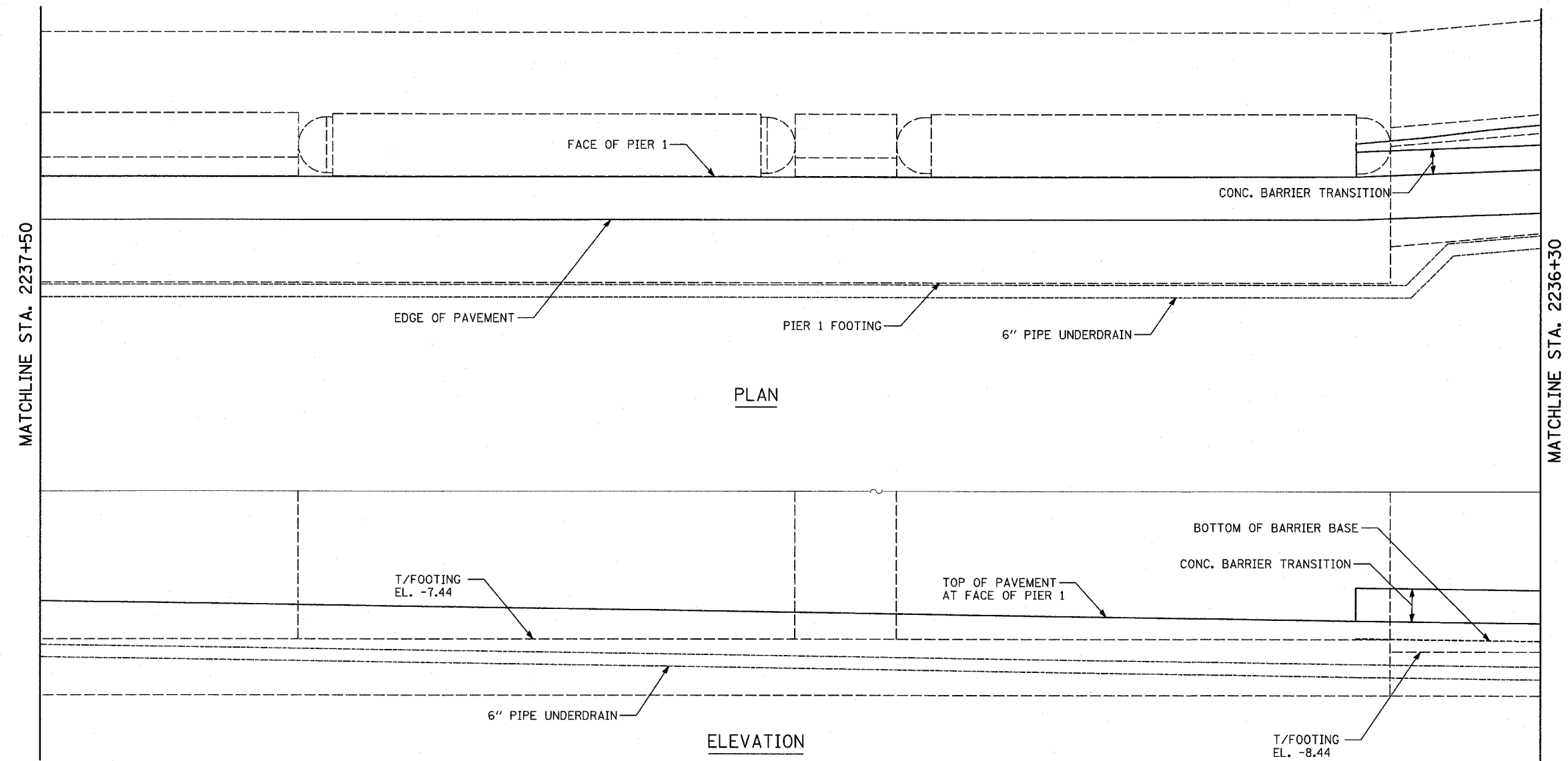
NOTES

1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 CHICAGO RAIL LINK, NORFOLK SOUTHERN AND
 BELT RAILWAY OVERPASS FOOTINGS &
 UNDERDRAIN DETAILS SHEET 1 OF 3

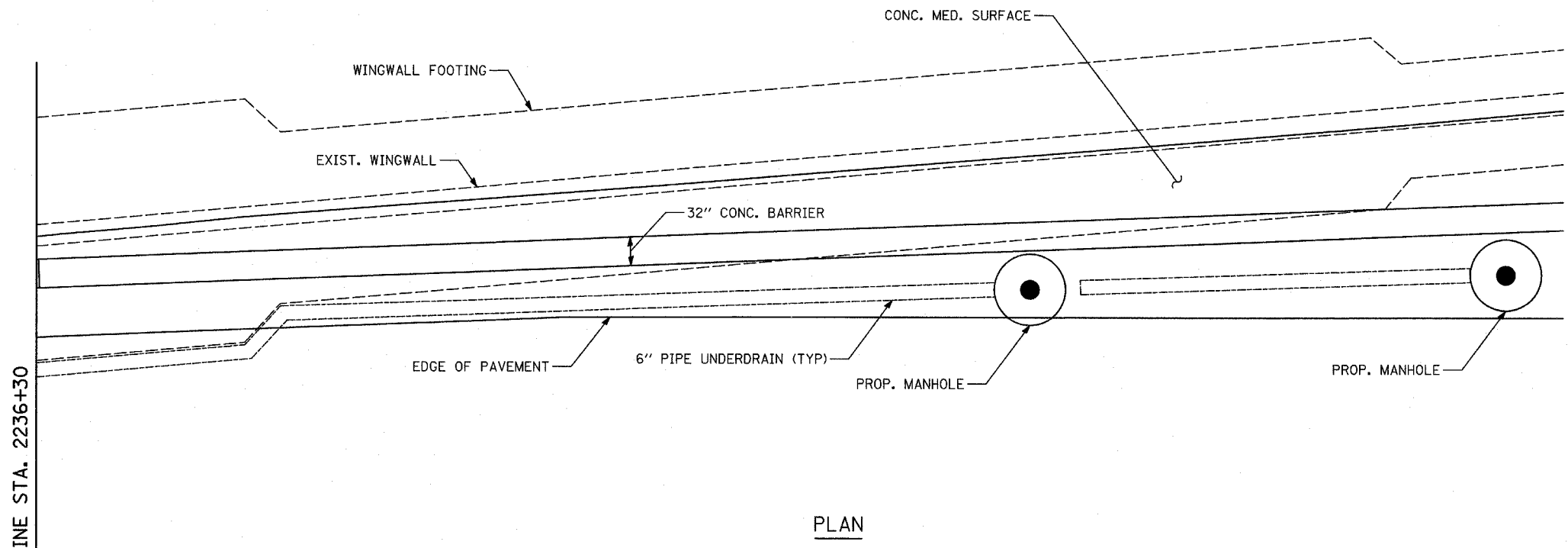
SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: TB, KA
 CHECKED BY: TGB



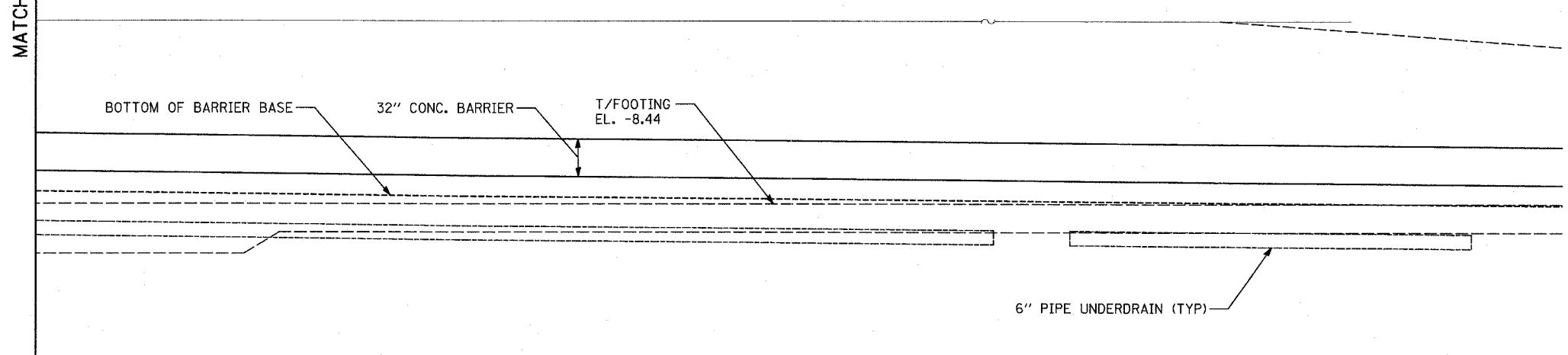
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 CHICAGO RAIL LINK, NORFOLK SOUTHERN AND
 BELT RAILWAY OVERPASS FOOTINGS &
 UNDERDRAIN DETAILS SHEET 2 OF 3

SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: TB, KA
 CHECKED BY: TGB



PLAN



ELEVATION

MATCHLINE STA. 2236+30

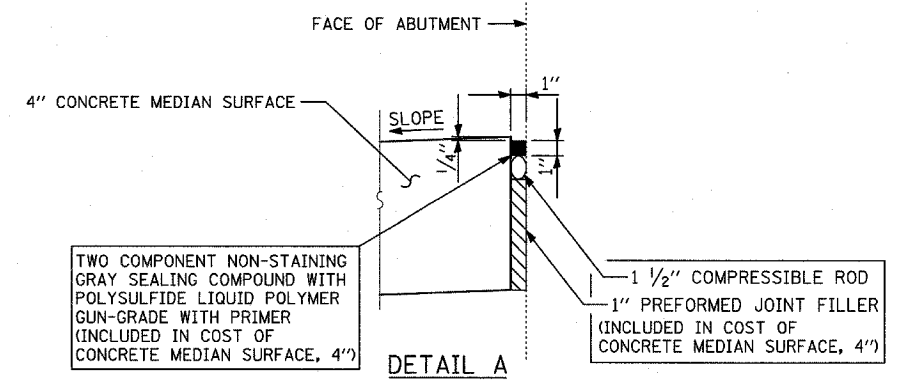
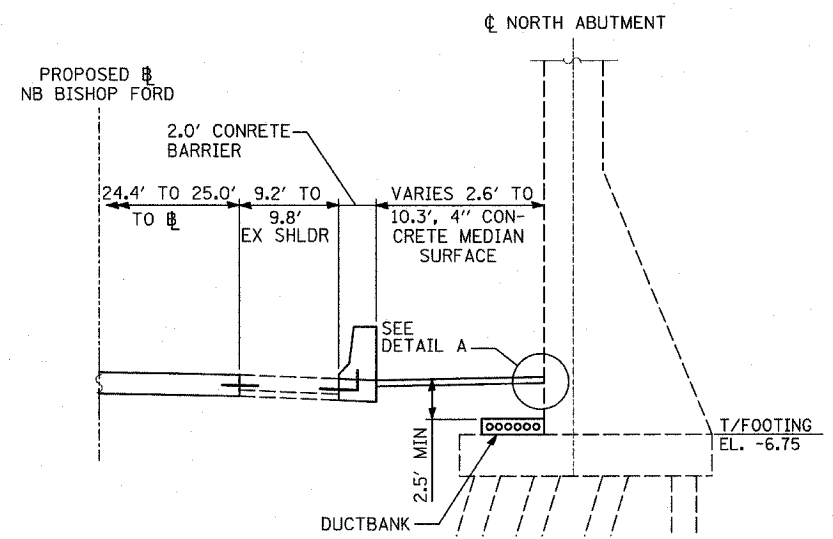
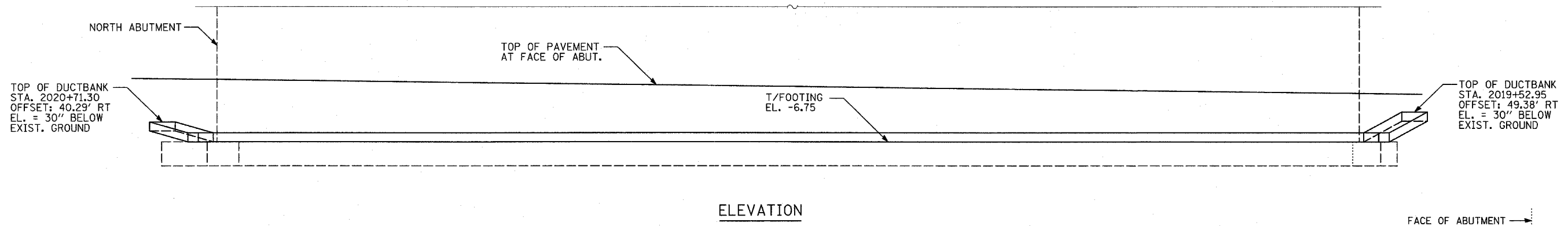
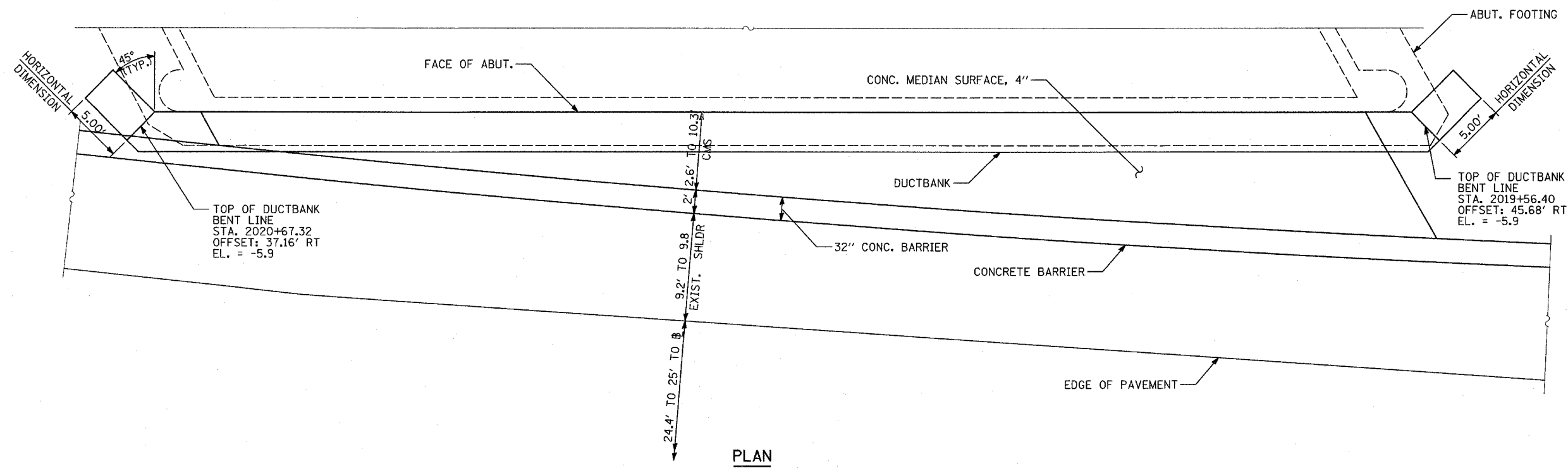
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 CHICAGO RAIL LINK, NORFOLK SOUTHERN AND
 BELT RAILWAY OVERPASS FOOTINGS &
 UNDERDRAIN DETAILS SHEET 3 OF 3

SCALE: NONE
 DATE: MARCH 7, 2006

DRAWN BY: TB, KA
 CHECKED BY: TGB



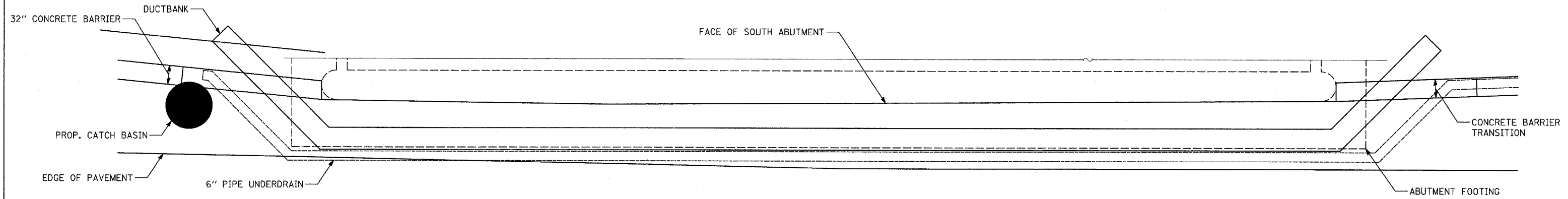
NOTES

1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
6. REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
TYPICAL SECTIONS AND DETAILS:
S. MICHIGAN AVENUE OVERPASS FOOTING,
& DUCTBANK DETAILS

SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: TB, PL
 CHECKED BY: TGB



NOTES

1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
6. REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
7. NO INFORMATION WAS AVAILABLE FOR THE EXISTING BRIDGE, THEREFORE FOOTING SIZE AND ELEVATION ARE UNKNOWN. CONTRACTOR SHALL ADJUST DUCTBANK LOCATION AND ELEVATION AS NECESSARY TO AVOID CONFLICT WITH FOOTING. CONTRACTOR SHALL ADJUST UNDERDRAIN LOCATION AND ELEVATION AS NECESSARY.
8. MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 UNION PACIFIC RAILROAD
 DUCTBANK DETAILS

SCALE: NONE
 DATE: MARCH 7, 2006
 DRAWN BY: TB
 CHECKED BY: TGB



PAVEMENT REMOVAL

DESCRIPTION OF EXISTING LOCATION	FROM		TO		PAVEMENT REM (SQ YD)	PAVT REMOVAL SPL (SQ YD)
	ALIGNMENT	STATION	ALIGNMENT	STATION		
EB CONNECTOR MAINLINE	EB CONN	407+22.9	EB CONN	407+61.1		84
EB CONNECTOR MAINLINE	EB CONN	407+53.9	EB CONN	415+06.0	1980	
NB I-57 MAINLINE	NB I-57	191+41.6	NB I-57	192+81.4	615	
NB I-57 MAINLINE	NB I-57	192+81.4	NB I-57	193+11.4		133
NB I-57 MAINLINE	NB I-57	193+11.4	NB I-57	211+00.0	9481	
NB I-57 MAINLINE	NB I-57	211+00.0	NB I-57	221+00.0	4043	
NB I-57 MAINLINE	NB I-57	221+00.0	NB I-57	232+05.9	5238	
NB I-57 MAINLINE	NB I-57	231+95.7	NB I-57	232+38.8		93
NB I-57 MAINLINE	NB I-57	232+28.0	NB I-57	238+05.3	1476	
NB I-57 MAINLINE	NB I-57	238+82.9	NB I-57	245+47.4	1744	
NB I-57 MAINLINE (OVER SB I-94)	NB I-57	245+47.4	NB I-57	248+44.1		795
NB I-57 MAINLINE	NB I-57	248+44.1	NB I-57	253+93.4	1467	
NB I-57/NB I-94 (RYAN) MAINLINE	NB I-57	253+93.4	NB I-94(RYAN)	2206+65.9	4457	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2206+65.9	NB I-94(RYAN)	2207+01.7		230
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2207+01.1	NB I-94(RYAN)	2208+04.1	642	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2208+04.1	NB I-94(RYAN)	2208+34.1		185
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2208+34.1	NB I-94(RYAN)	2226+83.5	12308	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2226+83.4	NB I-94(RYAN)	2227+13.5		174
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2226+83.3	NB I-94(RYAN)	2227+13.4		20
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2227+13.3	NB I-94(RYAN)	2229+29.4	108	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2227+13.4	NB I-94(RYAN)	2245+63.1	11526	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2245+63.0	NB I-94(RYAN)	2245+93.1		163
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2245+93.0	NB I-94(RYAN)	2258+31.0	7136	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2258+31.0	NB I-94(RYAN)	2258+81.0		311
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2258+81.0	NB I-94(RYAN)	2259+82.5	538	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2259+81.3	NB I-94(RYAN)	2260+12.5		160
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2260+11.4	NB I-94(RYAN)	2265+90.4	3095	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2265+90.4	NB I-94(RYAN)	2266+32.4		225
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2266+32.4	NB I-94(RYAN)	2279+24.0	8898	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2279+24.0	NB I-94(RYAN)	2279+74.1		266
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2279+74.1	NB I-94(RYAN)	2298+56.0	12057	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2298+56.0	NB I-94(RYAN)	2299+00.0		345
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2299+00.0	NB I-94(RYAN)	2313+45.4	10415	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2313+44.6	NB I-94(RYAN)	2314+14.4		368
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2314+14.2	NB I-94(RYAN)	2316+00.0	986	
NB I-94 (DAN RYAN) MAINLINE	NB I-94(RYAN)	2315+21.4	NB I-94(RYAN)	2316+00.0	58	
NB I-94 (BISHOP FORD) MAINLINE	NB I-94(FORD)	2021+00.0	NB I-94(FORD)	2027+71.8	1855	
NB I-94 (BISHOP FORD) MAINLINE	NB I-94(FORD)	2027+71.8	NB I-94(FORD)	2029+03.4		345
NB I-94 (BISHOP FORD) MAINLINE	NB I-94(FORD)	2029+03.4	NB I-94(FORD)	2030+02.6	222	
NB I-94 (BISHOP FORD) MAINLINE	NB I-94(FORD)	2029+83.7	NB I-94(FORD)	2030+42.9		98
NB I-94 (BISHOP FORD) MAINLINE	NB I-94(FORD)	2030+23.1	NB I-94(FORD)	2033+13.4	880	
NB I-94 (BISHOP FORD) MAINLINE	NB I-94(FORD)	2033+13.1	NB I-94(FORD)	2033+43.6		124
NB I-94 (BISHOP FORD) MAINLINE	NB I-94(FORD)	2033+43.3	NB I-94(FORD)	2038+43.1	2699	
NB I-94 (BISHOP FORD) MAINLINE	NB I-94(FORD)	2038+42.5	NB I-94(FORD)	2038+93.5		253
NB I-94 (BISHOP FORD) MAINLINE	NB I-94(FORD)	2038+92.9	NB I-94(FORD)	2044+40.5	1629	
WB CONNECTOR MAINLINE	WB CONN	330+65.5	WB CONN	340+29.2	2576	
REMOVAL OF TEMPORARY PAVEMENT					9634	
I000-2A TOTAL						
J000-2A TOTAL					117763	4372
TOTAL					117763	4372

COMBINATION CONCRETE CURB AND GUTTER REMOVAL

DESCRIPTION OF EXISTING LOCATION	FROM			TO			COMB CURB GUTTER REM (FOOT)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
NB I-57	NB I-57	191+41.6	9.9 RT	NB I-57	191+71.2	9.9 RT	30
NB I-57	NB I-57	192+79.6	10.0 RT	NB I-57	200+58.0	16.0 RT	792
NB I-57	NB I-57	203+53.1	26.3 RT	NB I-57	206+78.7	21.9 RT	329
NB I-57	NB I-57	209+07.4	18.3 RT	NB I-57	213+65.7	9.7 RT	459
NB I-57	NB I-57	218+51.9	9.7 RT	NB I-57	225+80.4	16.3 RT	730
NB I-57	NB I-57	231+75.3	39.9 LT	NB I-57	232+84.3	40.4 LT	108
NB I-57	NB I-57	232+38.5	9.0 RT	NB I-57	235+24.8	5.5 RT	290
WB CONNECTOR	WB CONN	306+71.4	8.5 RT	WB CONN	314+60.2	8.3 RT	789
WB CONNECTOR	WB CONN	330+11.8	22.0 RT	WB CONN	332+17.6	17.0 RT	209
WB CONNECTOR	WB CONN	335+71.5	19.8 RT	WB CONN	340+29.5	16.7 RT	464
EB CONNECTOR	EB CONN	406+88.1	31.1 RT	EB CONN	415+06.0	33.6 RT	805
EB CONNECTOR	EB CONN	407+80.6	13.5 LT	EB CONN	415+06.0	9.6 LT	731
NB I-94 (BISHOP FORD FREEWAY)	NB I-94(FORD)	1990+48.7	34.7 RT	NB I-94(FORD)	1998+45.2	34.6 RT	802
NB I-94 (BISHOP FORD FREEWAY)	NB I-94(FORD)	2002+55.0	33.9 RT	NB I-94(FORD)	2018+40.4	34.5 RT	1571
NB I-94 (BISHOP FORD FREEWAY)	NB I-94(FORD)	2009+74.1	13.9 LT	NB I-94(FORD)	2010+30.4	9.9 LT	58
NB I-94 (BISHOP FORD FREEWAY)	NB I-94(FORD)	2019+67.7	34.7 RT	NB I-94(FORD)	2021+00.0	33.9 RT	129
NB I-94 (BISHOP FORD FREEWAY)	NB I-94(FORD)	2021+00.0	33.9 RT	NB I-94(FORD)	2028+04.1	31.8 RT	693
NB I-94 (BISHOP FORD FREEWAY)	NB I-94(FORD)	2029+18.8	30.8 RT	NB I-94(FORD)	2032+75.6	22.1 RT	354
NB I-94 (BISHOP FORD FREEWAY)	NB I-94(FORD)	2031+88.8	38.0 RT	NB I-94(FORD)	2032+75.6	22.1 RT	94
NB I-94 (RYAN/FORD)	NB I-94(FORD)	2034+13.6	50.3 RT	NB I-94(RYAN)	2204+00.1	59.5 RT	1553
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2210+15.1	63.8 RT	NB I-94(RYAN)	2219+11.7	76.0 RT	921
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2223+48.8	86.3 RT	NB I-94(RYAN)	2229+94.4	61.6 RT	647
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2233+18.1	63.6 RT	NB I-94(RYAN)	2235+03.4	61.5 RT	186
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2238+49.7	65.2 RT	NB I-94(RYAN)	2242+45.8	82.8 RT	397
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2244+95.7	76.0 RT	NB I-94(RYAN)	2256+81.1	62.1 RT	1193
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2259+80.5	62.8 RT	NB I-94(RYAN)	2269+48.1	76.0 RT	992
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2272+62.0	86.3 RT	NB I-94(RYAN)	2282+84.1	61.6 RT	1023
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2286+55.0	62.6 RT	NB I-94(RYAN)	2292+40.4	86.3 RT	587
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2303+64.6	86.3 RT	NB I-94(RYAN)	2310+53.6	61.0 RT	690
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2313+43.7	60.7 RT	NB I-94(RYAN)	2316+00.0	66.0 RT	258
I000-2A TOTAL							2547
J000-2A TOTAL							15337
TOTAL							17884

TYLIN INTERNATIONAL

NOTE:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
REMOVAL QUANTITIES - SHEET 1

SCALE: NONE
DATE: MARCH 7, 2006
DRAWN BY: RTM
CHECKED BY: MPG

PAVED SHOULDER REMOVAL

DESCRIPTION OF EXISTING LOCATION	OFFSET DIRECTION	FROM		TO		PAVED SHLD REMOVAL (SQ YD)	PAVED SHLD REM SPL (SQ YD)
		ALIGNMENT	STATION	ALIGNMENT	STATION		
NB I-57 SHOULDER	RT	NB I-57	191+41.6	NB I-57	192+81.4	159	
NB I-57 SHOULDER	RT	NB I-57	193+11.4	NB I-57	193+11.4		35
NB I-57 SHOULDER	RT	NB I-57	193+11.4	NB I-57	199+44.9	712	
NB I-57/EB CONNECTOR SHOULDER	RT	NB I-57	204+47.1	NB I-57	231+80.0	2938	
NB I-57 SHOULDER	LT	NB I-57	209+07.4	NB I-57	232+09.8	2220	
NB I-57 SHOULDER	RT	NB I-57	218+30.0	NB I-57	219+17.5	91	
NB I-57/EB CONNECTOR GORE	-	NB I-57	228+74.2	NB I-57	231+95.7	386	
NB I-57/EB CONNECTOR GORE	-	NB I-57	231+87.4	NB I-57	232+28.0		83
NB I-57 SHOULDER	LT	NB I-57	231+75.3	NB I-57	232+12.4	23	
NB I-57 SHOULDER	LT	NB I-57	232+05.9	NB I-57	232+42.7		33
NB I-57 SHOULDER	LT	NB I-57	232+09.8	NB I-57	232+45.6		23
NB I-57 SHOULDER	RT	NB I-57	232+22.4	NB I-57	238+13.8	533	
NB I-57 SHOULDER	LT	NB I-57	232+38.9	NB I-57	237+61.4	433	
NB I-57 SHOULDER	LT	NB I-57	232+43.0	NB I-57	232+83.5	23	
NB I-57 SHOULDER	LT	NB I-57	238+73.8	NB I-57	245+47.4	574	
NB I-57 SHOULDER	RT	NB I-57	239+23.1	NB I-57	245+47.4	512	
NB I-57 SHOULDER (OVER SB I-94)	LT	NB I-57	245+47.4	NB I-57	248+44.1		283
NB I-57 SHOULDER (OVER SB I-94)	RT	NB I-57	245+47.4	NB I-57	248+44.1		293
NB I-57 SHOULDER	LT	NB I-57	248+44.1	NB I-57	252+25.9	363	
NB I-57 SHOULDER	RT	NB I-57	248+44.1	NB I-57	252+25.9	367	
NB I-57/NB I-94 (FORD) GORE	-	NB I-57	252+25.9	NB I-57	253+93.8	240	
NB I-57/NB I-94 (RYAN) SHOULDER	LT	NB I-57	252+25.9	NB I-94(RYAN)	2205+51.8	1098	
WB CONNECTOR SHOULDER	LT	WB CONN	309+57.9	WB CONN	312+19.7	585	
WB CONNECTOR SHOULDER	RT	WB CONN	330+65.5	WB CONN	340+29.5	1002	
WB CONNECTOR SHOULDER	LT	WB CONN	330+65.5	WB CONN	340+29.2	922	
EB CONNECTOR SHOULDER	RT	EB CONN	407+17.1	EB CONN	407+51.1		27
EB CONNECTOR SHOULDER	RT	EB CONN	407+20.0	EB CONN	407+53.9		34
EB CONNECTOR SHOULDER	RT	EB CONN	407+49.4	EB CONN	407+72.8	10	
EB CONNECTOR SHOULDER	RT	EB CONN	407+51.1	EB CONN	415+06.0	805	
EB CONNECTOR SHOULDER	LT	EB CONN	407+61.1	EB CONN	415+06.0	799	
NB I-94 (BISHOP FORD) SHOULDER	RT	NB I-94(FORD)	1990+48.7	NB I-94(FORD)	1998+33.5	971	
NB I-94 (BISHOP FORD) SHOULDER	LT	NB I-94(FORD)	2002+55.0	NB I-94(FORD)	2006+61.4	467	
NB I-94 (FORD)/WB CONN MEDIAN	-	NB I-94(FORD)	2006+59.2	NB I-94(FORD)	2017+46.3	897	
NB I-94 (FORD)/WB CONN MEDIAN	-	NB I-94(FORD)	2009+74.1	NB I-94(FORD)	2010+31.1	49	
NB I-94 (FORD)/WB CONN MEDIAN	-	NB I-94(FORD)	2019+08.4	NB I-94(FORD)	2024+25.2	934	
NB I-94 (BISHOP FORD) SHOULDER	RT	NB I-94(FORD)	2019+53.7	NB I-94(FORD)	2020+63.9	124	
NB I-94 (BISHOP FORD) SHOULDER	LT	NB I-94(FORD)	2021+00.0	NB I-94(FORD)	2027+71.8	593	
NB I-94 (BISHOP FORD) SHOULDER	RT	NB I-94(FORD)	2021+00.0	NB I-94(FORD)	2027+71.8	707	
UNDER STATE ST BRIDGE	-	NB I-94(FORD)	2026+41.1	NB I-94(FORD)	2029+66.2	914	
NB I-94 (BISHOP FORD) SHOULDER	RT	NB I-94(FORD)	2027+71.8	NB I-94(FORD)	2029+03.4		141
NB I-94 (BISHOP FORD) SHOULDER	LT	NB I-94(FORD)	2027+71.8	NB I-94(FORD)	2029+03.4		125
UNDER STATE ST BRIDGE	-	NB I-94(FORD)	2028+63.4	NB I-94(FORD)	2029+39.2	102	
NB I-94 (BISHOP FORD) SHOULDER	RT	NB I-94(FORD)	2029+03.4	NB I-94(FORD)	2030+06.6	86	
NB I-94 (BISHOP FORD) SHOULDER	LT	NB I-94(FORD)	2029+03.4	NB I-94(FORD)	2029+83.7	137	
NB MICHIGAN AVE ENTRANCE RAMP	RT	NB I-94(FORD)	2029+19.3	NB I-94(FORD)	2032+74.7	594	
NB I-94 (BISHOP FORD) SHOULDER	LT	NB I-94(FORD)	2029+66.3	NB I-94(FORD)	2030+23.1		77
NB I-94 (BISHOP FORD) SHOULDER	RT	NB I-94(FORD)	2030+02.6	NB I-94(FORD)	2030+33.3		10
NB I-94 (BISHOP FORD) SHOULDER	LT	NB I-94(FORD)	2030+11.3	NB I-94(FORD)	2033+13.5	297	
NB I-94 (BISHOP FORD) SHOULDER	LT	NB I-94(FORD)	2033+13.4	NB I-94(FORD)	2033+43.3		30
NB I-94 (BISHOP FORD) SHOULDER	LT	NB I-94(FORD)	2033+43.3	NB I-94(FORD)	2038+43.2	499	
NB I-94 (BISHOP FORD) SHOULDER	LT	NB I-94(FORD)	2038+43.1	NB I-94(FORD)	2038+92.9		50
NB I-94 (BISHOP FORD) SHOULDER	LT	NB I-94(FORD)	2038+92.8	NB I-94(FORD)	2042+72.7	363	
NB I-94 (BISHOP FORD) SHOULDER	RT	NB I-94(FORD)	2039+89.3	NB I-94(FORD)	2042+72.7	312	
NB I-94 (BISHOP FORD) SHOULDER	RT	NB I-94(FORD)	2042+72.7	NB I-94(RYAN)	2206+65.9	1100	
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2206+65.9	NB I-94(RYAN)	2207+01.9		53
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2207+01.7	NB I-94(RYAN)	2208+04.1	147	

(CONT. ABOVE)

PAVED SHOULDER REMOVAL (CONT. FROM BELOW)

DESCRIPTION OF EXISTING LOCATION	OFFSET DIRECTION	FROM		TO		PAVED SHLD REMOVAL (SQ YD)	PAVED SHLD REM SPL (SQ YD)	
		ALIGNMENT	STATION	ALIGNMENT	STATION			
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2208+04.1	NB I-94(RYAN)	2208+34.1		43	
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2208+34.1	NB I-94(RYAN)	2216+00.5	1040		
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2213+32.0	NB I-94(RYAN)	2226+83.6	1297		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2224+99.6	NB I-94(RYAN)	2226+83.4	235		
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2226+83.5	NB I-94(RYAN)	2227+13.6		29	
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2226+83.4	NB I-94(RYAN)	2227+13.4		40	
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2227+13.5	NB I-94(RYAN)	2235+65.3	840		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2227+13.4	NB I-94(RYAN)	2239+34.1	1593		
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2235+78.3	NB I-94(RYAN)	2245+63.2	960		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2244+95.5	NB I-94(RYAN)	2245+17.3	17		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2245+11.6	NB I-94(RYAN)	2245+63.0	61		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2245+63.0	NB I-94(RYAN)	2245+93.0		40	
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2245+63.1	NB I-94(RYAN)	2245+93.2		29	
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2245+93.1	NB I-94(RYAN)	2253+20.5	708		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2245+93.0	NB I-94(RYAN)	2258+31.0	1661		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2258+31.0	NB I-94(RYAN)	2258+81.0		74	
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2258+72.2	NB I-94(RYAN)	2258+81.0		10	
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2258+81.0	NB I-94(RYAN)	2259+81.3	120		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2258+81.0	NB I-94(RYAN)	2259+82.8	152		
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2259+81.1	NB I-94(RYAN)	2260+11.4		33	
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2259+82.5	NB I-94(RYAN)	2260+12.8		42	
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2260+11.1	NB I-94(RYAN)	2265+90.4	552		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2260+12.5	NB I-94(RYAN)	2265+90.5	771		
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2265+90.4	NB I-94(RYAN)	2266+32.4		40	
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2265+90.4	NB I-94(RYAN)	2266+32.5		56	
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2266+32.4	NB I-94(RYAN)	2279+24.0	1240		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2266+32.4	NB I-94(RYAN)	2267+01.9	93		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2273+83.9	NB I-94(RYAN)	2279+24.0	503		
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2279+24.0	NB I-94(RYAN)	2279+74.1		51	
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2279+24.0	NB I-94(RYAN)	2279+74.1		58	
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2279+74.1	NB I-94(RYAN)	2298+56.0	1845		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2279+74.1	NB I-94(RYAN)	2294+17.7	1736		
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2298+56.0	NB I-94(RYAN)	2299+02.3		40	
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2299+00.0	NB I-94(RYAN)	2313+44.6	1490		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2308+19.1	NB I-94(RYAN)	2313+45.7	721		
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2313+44.4	NB I-94(RYAN)	2314+14.4		77	
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2313+43.7	NB I-94(RYAN)	2314+14.2		85	
NB I-94 (DAN RYAN) SHOULDER	LT	NB I-94(RYAN)	2314+14.4	NB I-94(RYAN)	2316+00.0	202		
NB I-94 (DAN RYAN) SHOULDER	RT	NB I-94(RYAN)	2314+14.1	NB I-94(RYAN)	2316+00.0	183		
INTERCHANGE SEWER TRENCHES	-	-	-	-	-	356		
						I000-2A TOTAL	2478	
						J000-2A TOTAL	39986	2044
						TOTAL	42464	2044

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	41
STA. 190+48 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				
				6230

TYLIN INTERNATIONAL

NOTE:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
REMOVAL QUANTITIES - SHEET 2

SCALE: NONE DRAWN BY: RTM
DATE: MARCH 7, 2006 CHECKED BY: MPG

CTA BARRIER REMOVAL

DESCRIPTION OF EXISTING LOCATION	FROM			TO			CTA BARRIER REMOV (FOOT)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2213+52.0	8.0 LT	NB I-94(RYAN)	2232+44.8	8.9 LT	1893
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2233+18.3	9.0 LT	NB I-94(RYAN)	2236+46.1	7.5 LT	328
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2336+78.5	7.7 LT	NB I-94(RYAN)	2236+97.7	7.9 LT	20
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2237+31.0	8.1 LT	NB I-94(RYAN)	2237+72.7	8.0 LT	42
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2238+71.8	8.0 LT	NB I-94(RYAN)	2242+50.0	7.1 LT	379
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2243+00.0	7.0 LT	NB I-94(RYAN)	2253+20.5	9.4 LT	1021
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2259+79.5	10.1 LT	NB I-94(RYAN)	2263+98.6	7.2 LT	420
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2264+59.4	7.0 LT	NB I-94(RYAN)	2266+39.0	7.2 LT	180
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2267+02.7	7.3 LT	NB I-94(RYAN)	2282+22.0	8.7 LT	1520
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2282+72.0	8.6 LT	NB I-94(RYAN)	2285+60.7	8.8 LT	289
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2286+54.5	9.2 LT	NB I-94(RYAN)	2290+70.0	8.0 LT	420
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2291+20.0	8.3 LT	NB I-94(RYAN)	2306+97.2	10.2 LT	1578
NB I-94 (DAN RYAN EXPRESSWAY)	NB I-94(RYAN)	2315+38.2	10.3 LT	NB I-94(RYAN)	2316+00.0	8.8 LT	62
I000-2A TOTAL							
J000-2A TOTAL							8152
TOTAL							8152

CONCRETE BARRIER REMOVAL

DESCRIPTION OF EXISTING LOCATION	BARRIER TYPE	FROM			TO			CONC BARRIER REMOV (FOOT)
		ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
NB I-57		NB I-57	206+78.8	22.5 RT	NB I-57	208+43.5	20.7 RT	165
NB I-57		NB I-57	209+07.4	46.8 LT	NB I-57	215+18.7	45.4 LT	612
NB I-57		NB I-57	213+65.8	10.3 RT	NB I-57	218+51.9	10.4 RT	487
NB I-57		NB I-57	215+18.7	46.1 LT	NB I-57	215+38.6	47.0 LT	20
NB I-57		NB I-57	215+49.5	47.0 LT	NB I-57	215+79.5	45.8 LT	30
NB I-57		NB I-57	215+79.4	45.5 LT	NB I-57	218+39.1	45.5 LT	260
NB I-57		NB I-57	218+86.7	45.7 LT	NB I-57	227+08.5	43.9 LT	817
NB I-57		NB I-57	225+80.4	16.6 RT	NB I-57	231+43.8	46.4 RT	573
NB I-57		NB I-57	227+08.5	44.3 LT	NB I-57	227+59.7	43.8 LT	50
NB I-57		NB I-57	227+59.7	43.6 LT	NB I-57	231+75.3	39.9 LT	407
NB I-57		NB I-57	232+68.8	46.4 LT	NB I-57	237+50.9	31.2 LT	476
NB I-57		NB I-57	235+24.8	5.5 RT	NB I-57	238+13.8	3.0 RT	290
NB I-57		NB I-57	238+73.9	30.7 LT	NB I-57	253+18.0	42.8 LT	1419
NB I-57		NB I-57	239+30.5	3.6 RT	NB I-57	252+25.8	6.4 RT	1302
WB CONNECTOR		WB CONN	315+03.7	8.0 RT	WBCONN	315+28.9	8.0 RT	25
WB CONNECTOR		WB CONN	321+47.6	8.9 RT	WBCONN	323+84.8	10.5 RT	238
WB CONNECTOR		WB CONN	330+65.5	25.0 LT	WBCONN	340+29.2	25.4 LT	949
WB CONNECTOR		WB CONN	332+17.6	17.0 RT	WBCONN	335+71.5	19.8 RT	359
NB I-94 (BISHOP FORD FREEWAY)		NB I-94(FORD)	1998+45.2	34.6 RT	NB I-94(FORD)	1999+42.3	34.2 RT	98
NB I-94 (BISHOP FORD FREEWAY)		NB I-94(FORD)	2002+55.0	22.9 LT	NB I-94(FORD)	2006+61.0	39.0 LT	407
NB I-94 (BISHOP FORD FREEWAY)		NB I-94(FORD)	2010+30.7	12.6 LT	NB I-94(FORD)	2011+54.2	8.5 LT	125
NB I-94 (BISHOP FORD FREEWAY)		NB I-94(FORD)	2018+40.4	34.5 RT	NB I-94(FORD)	2019+67.7	33.8 RT	125
NB I-94 (BISHOP FORD FREEWAY)		NB I-94(FORD)	2021+00.0	8.9 LT	NB I-94(FORD)	2024+25.3	9.4 LT	327
NB I-94 (BISHOP FORD FREEWAY)		NB I-94(FORD)	2024+25.4	10.4 LT	NB I-94(FORD)	2026+54.1	10.8 LT	230
NB I-94 (BISHOP FORD FREEWAY)		NB I-94(FORD)	2026+54.9	9.8 LT	NB I-94(FORD)	2028+87.9	10.8 LT	235
NB I-94 (BISHOP FORD FREEWAY)		NB I-94(FORD)	2028+04.1	31.8 RT	NB I-94(FORD)	2029+19.3	31.4 RT	114
NB I-94 (BISHOP FORD FREEWAY)		NB I-94(FORD)	2029+68.3	22.4 LT	NB I-94(FORD)	2042+72.7	21.5 LT	1316
NB I-94 (DAN RYAN EXPRESSWAY)		NB I-94(RYAN)	2204+00.1	60.3 RT	NB I-94(RYAN)	2205+49.8	64.4 RT	151
NB I-94 (DAN RYAN EXPRESSWAY)		NB I-94(RYAN)	2229+94.5	62.4 RT	NB I-94(RYAN)	2232+46.9	66.3 RT	253
NB I-94 (DAN RYAN EXPRESSWAY)		NB I-94(RYAN)	2235+03.4	62.5 RT	NB I-94(RYAN)	2236+45.4	64.4 RT	143
NB I-94 (DAN RYAN EXPRESSWAY)		NB I-94(RYAN)	2242+50.0	7.4 LT	NB I-94(RYAN)	2243+00.0	7.4 LT	50
NB I-94 (DAN RYAN EXPRESSWAY)		NB I-94(RYAN)	2256+81.3	62.4 RT	NB I-94(RYAN)	2258+72.2	64.6 RT	191
NB I-94 (DAN RYAN EXPRESSWAY)		NB I-94(RYAN)	2263+98.7	8.3 LT	NB I-94(RYAN)	2264+28.6	7.9 LT	30
NB I-94 (DAN RYAN EXPRESSWAY)		NB I-94(RYAN)	2264+39.4	7.9 LT	NB I-94(RYAN)	2264+59.4	8.2 LT	20
NB I-94 (DAN RYAN EXPRESSWAY)		NB I-94(RYAN)	2282+22.0	8.8 LT	NB I-94(RYAN)	2282+72.0	8.9 LT	50
NB I-94 (DAN RYAN EXPRESSWAY)		NB I-94(RYAN)	2282+83.7	62.1 RT	NB I-94(RYAN)	2285+57.9	64.1 RT	275
NB I-94 (DAN RYAN EXPRESSWAY)		NB I-94(RYAN)	2290+70.0	7.5 LT	NB I-94(RYAN)	2291+20.0	7.7 LT	50
NB I-94 (DAN RYAN EXPRESSWAY)		NB I-94(RYAN)	2310+53.6	61.6 RT	NB I-94(RYAN)	2312+37.6	63.6 RT	185
NB I-57 SEWER TRENCHES		-	-	-	-	-	-	20
INTERCHANGE SEWER TRENCHES		-	-	-	-	-	-	238
I000-2A TOTAL							920	
J000-2A TOTAL							12192	
TOTAL							13112	

BITUMINOUS SURFACE REMOVAL

DESCRIPTION OF EXISTING LOCATION	OFFSET DIRECTION	FROM		TO		BIT SURF REM 1 1/2 (SQ YD)	BIT SURF REM 1 3/4 (SQ YD)	BIT SURF REM 4 (SQ YD)
		ALIGNMENT	STATION	ALIGNMENT	STATION			
NB I-57 MAINLINE	-	NB I-57	190+65.0	NB I-57	191+41.6		391	
WB CONNECTOR SHOULDER	LT	NB I-94(FORD)	2006+61.0	WB CONN	306+70.3	479		
WB CONNECTOR MAINLINE	-	WB CONN	306+70.3	WB CONN	320+00.0		2382	
WB CONNECTOR MAINLINE	-	WB CONN	320+00.0	WB CONN	330+65.5		2476	
WB CONN./NB I-94 (FORD) GORE	-	NB I-94(FORD)	2002+55.0	NB I-94(FORD)	2009+73.1			3672
NB I-94 (BISHOP FORD) MAINLINE	-	NB I-94(FORD)	2009+73.1	NB I-94(FORD)	2021+00.0			3061
NB I-94 (BISHOP FORD) SHOULDER	RT	NB I-94(FORD)	2003+03.2	NB I-94(FORD)	2018+86.6	1441		
NB I-94 (BISHOP FORD) SHOULDER	LT	NB I-94(FORD)	2010+30.1	NB I-94(FORD)	2021+00.0	1040		
INSIDE SHOULDER PATCHES	-	-	-	-	-	147		
I000-2A TOTAL						2960	4858	6733
J000-2A TOTAL						147	391	
TOTAL						3107	5249	6733

TYLIN INTERNATIONAL

NOTE:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
REMOVAL QUANTITIES - SHEET 3

SCALE: NONE
DATE: MARCH 7, 2006
DRAWN BY: RTM
CHECKED BY: MPG

TREE REMOVAL (6 TO 15 UNITS)

ALIGNMENT	LOCATION				TREE REMOV 6-15 (UNIT)
	STATION	OFFSET	NORTHING	EASTING	
NB I-57	209+23.8	73.6 RT	1839269.9	1174421.3	12
NB I-57	209+29.1	49.1 RT	1839294.5	1174426.0	11
NB I-57	209+55.4	53.5 RT	1839290.8	1174452.4	14
NB I-57	211+30.3	49.6 RT	1839299.0	1174627.2	15
NB I-57	211+42.1	76.2 RT	1839272.7	1174639.7	7
NB I-57	211+45.4	76.1 RT	1839272.9	1174642.9	7
NB I-57	211+56.3	76.2 RT	1839273.0	1174653.8	6
NB I-57	211+59.6	76.3 RT	1839273.1	1174657.2	6
NB I-57	211+93.8	56.7 RT	1839293.4	1174690.8	13
NB I-57	211+93.8	56.7 RT	1839293.4	1174690.8	6
NB I-57	212+06.0	72.1 RT	1839278.4	1174703.4	6
NB I-57	212+15.2	73.2 RT	1839277.5	1174712.7	7
NB I-57	212+24.9	58.4 RT	1839292.5	1174721.9	11
NB I-57	213+21.3	76.0 RT	1839277.3	1174818.8	6
NB I-57	213+72.5	75.1 RT	1839279.5	1174869.9	10
NB I-57	213+77.8	75.3 RT	1839279.4	1174875.2	10
NB I-57	214+25.1	55.8 RT	1839300.0	1174922.0	13
NB I-57	214+82.1	39.7 RT	1839317.6	1174978.6	6
NB I-57	214+82.1	39.7 RT	1839317.6	1174978.6	7
NB I-57	218+95.3	60.4 RT	1839307.1	1175392.2	10
NB I-57	219+01.1	49.5 RT	1839318.1	1175397.7	12
NB I-57	219+54.6	74.4 RT	1839294.6	1175451.8	10
NB I-57	221+26.3	74.7 RT	1839298.6	1175623.4	6
NB I-57	221+32.9	74.6 RT	1839298.8	1175630.1	8
NB I-57	221+54.0	74.5 RT	1839299.4	1175651.2	7
NB I-57	223+30.7	50.0 RT	1839328.3	1175827.2	6
NB I-57	224+06.4	63.6 RT	1839316.6	1175903.3	6
NB I-57	226+08.7	54.1 RT	1839335.1	1176108.5	15
NB I-57	237+32.3	35.6 LT	1839825.6	1177111.5	8
NB I-57	237+43.7	41.5 LT	1839837.2	1177116.6	12
NB I-57	237+89.3	24.8 RT	1839814.6	1177193.7	6
NB I-57	240+18.7	19.7 RT	1839977.5	1177358.9	11
NB I-57	240+26.5	6.6 RT	1839992.1	1177354.6	7
NB I-57	240+26.5	6.6 RT	1839992.1	1177354.6	8
NB I-57	240+97.1	8.5 RT	1840043.5	1177403.5	14
NB I-57	241+08.6	49.9 RT	1840025.3	1177442.5	10
NB I-57	241+10.2	9.0 RT	1840053.2	1177412.5	9
NB I-57	241+48.7	12.7 RT	1840080.5	1177440.2	10
NB I-57	242+04.9	10.4 RT	1840126.1	1177473.7	8
NB I-57	242+16.8	10.7 RT	1840135.4	1177481.2	6
NB I-57	244+02.6	10.3 RT	1840289.2	1177587.2	12
NB I-57	244+80.1	10.0 RT	1840356.3	1177626.8	6
NB I-57	244+81.1	10.0 RT	1840357.1	1177627.2	8
NB I-57	245+35.4	15.8 RT	1840402.2	1177658.6	10
NB I-57	245+47.7	10.1 RT	1840415.7	1177659.4	6
NB I-57	245+52.0	14.8 RT	1840417.4	1177665.7	9
NB I-57	245+62.9	10.2 RT	1840429.3	1177666.6	12
NB I-57	245+63.2	19.3 RT	1840425.4	1177674.8	6
NB I-57	245+68.6	37.0 RT	1840422.2	1177693.1	8
NB I-57	245+86.1	10.2 RT	1840450.1	1177677.2	6
NB I-57	245+95.7	31.9 RT	1840449.1	1177700.9	15
NB I-57	246+03.3	22.3 RT	1840460.3	1177695.7	6
NB I-57	246+10.5	25.1 RT	1840465.6	1177701.4	8
NB I-57	246+19.1	20.0 RT	1840475.7	1177700.6	8
NB I-57	246+22.8	9.7 RT	1840483.5	1177692.9	6
NB I-57	246+43.8	16.5 RT	1840499.7	1177708.0	6
NB I-57	246+45.9	13.2 RT	1840503.0	1177705.9	6
NB I-57	246+53.6	14.6 RT	1840509.4	1177710.5	15
NB I-57	247+81.7	66.4 RT	1840609.9	1177808.7	10

(CONT. ABOVE)

TREE REMOVAL (6 TO 15 UNITS) (CONT. FROM BELOW)

ALIGNMENT	LOCATION				TREE REMOV 6-15 (UNIT)
	STATION	OFFSET	NORTHING	EASTING	
NB I-57	249+22.8	59.6 LT	1840784.3	1177735.1	9
NB I-57	249+27.5	38.7 LT	1840782.7	1177756.5	8
NB I-57	249+29.7	60.3 LT	1840790.9	1177736.3	6
NB I-57	249+38.7	39.1 LT	1840793.4	1177759.2	6
NB I-57	249+77.9	58.7 LT	1840835.5	1177750.7	6
NB I-57	250+03.8	58.8 LT	1840859.8	1177757.0	8
NB I-57	250+11.1	57.3 LT	1840866.2	1177760.2	7
NB I-57	250+11.1	57.3 LT	1840866.2	1177760.2	7
NB I-57	250+11.1	57.3 LT	1840866.2	1177760.2	7
NB I-57	250+11.1	57.3 LT	1840866.2	1177760.2	7
NB I-57	250+17.4	57.9 LT	1840872.4	1177761.1	8
NB I-57	250+65.3	42.6 LT	1840914.3	1177786.8	7
NB I-57	250+65.3	42.6 LT	1840914.3	1177786.8	9
NB I-57	250+67.3	40.1 LT	1840915.7	1177789.6	6
NB I-57	250+67.3	40.1 LT	1840915.7	1177789.6	7
NB I-57	251+36.4	41.5 LT	1840982.2	1177801.8	10
NB I-57	252+21.8	54.9 LT	1841066.5	1177801.9	10
NB I-57	252+21.8	54.9 LT	1841066.5	1177801.9	9
NB I-57	252+25.4	58.1 LT	1841070.5	1177799.3	12
NB I-57	252+25.4	58.1 LT	1841070.5	1177799.3	6
NB I-57	252+43.7	41.3 LT	1841086.0	1177818.3	8
WB CONN	330+11.6	27.1 LT	1839823.3	1177821.3	10
WB CONN	333+64.7	27.5 LT	1839875.7	1177479.9	8
NB I-94(FORD)	2033+21.4	18.1 LT	1840123.7	1178019.2	10
NB I-94(FORD)	2033+45.3	18.5 LT	1840145.9	1178009.7	10
NB I-94(FORD)	2033+67.0	22.5 LT	1840164.9	1177997.8	10
NB I-94(FORD)	2033+73.3	21.0 LT	1840171.4	1177997.0	10
NB I-94(FORD)	2033+80.5	24.2 LT	1840177.0	1177991.4	10
NB I-94(FORD)	2033+82.2	29.5 LT	1840176.8	1177985.8	10
NB I-94(FORD)	2033+86.6	17.2 LT	1840185.3	1177995.8	10
NB I-94(FORD)	2034+71.4	28.7 LT	1840262.5	1177956.6	10
NB I-94(FORD)	2034+91.4	30.4 LT	1840281.3	1177948.8	10
NB I-94(FORD)	2035+18.2	19.9 LT	1840310.3	1177950.9	10
NB I-94(FORD)	2035+37.5	20.6 LT	1840328.8	1177944.8	10
NB I-94(FORD)	2035+56.4	28.2 LT	1840345.2	1177932.2	10
NB I-94(FORD)	2035+59.5	21.8 LT	1840350.0	1177937.6	10
NB I-94(FORD)	2035+70.4	19.5 LT	1840361.2	1177936.9	10
NB I-94(FORD)	2035+90.3	20.7 LT	1840380.4	1177930.6	10
NB I-94(FORD)	2036+26.8	22.5 LT	1840415.8	1177920.0	10
NB I-94(FORD)	2036+35.5	21.5 LT	1840424.5	1177919.0	10
NB I-94(FORD)	2036+49.0	22.7 LT	1840437.6	1177914.8	10
NB I-94(FORD)	2037+50.0	31.7 LT	1840536.4	1177886.2	10
NB I-94(FORD)	2037+50.2	24.0 LT	1840537.8	1177893.7	10
NB I-94(FORD)	2037+68.2	37.5 LT	1840553.8	1177877.5	10
NB I-94(FORD)	2037+73.9	25.7 LT	1840561.2	1177888.2	10
NB I-94(FORD)	2037+80.8	38.7 LT	1840566.2	1177874.3	10
NB I-94(FORD)	2037+87.9	25.2 LT	1840575.3	1177886.5	10
NB I-94(FORD)	2035+81.4	67.9 LT	1840359.7	1177887.2	10
NB I-94(FORD)	2036+44.3	76.9 LT	1840420.9	1177863.0	10
NB I-94(FORD)	2036+55.0	87.2 LT	1840429.6	1177850.5	10
NB I-94(FORD)	2036+57.1	98.5 LT	1840429.3	1177839.0	10
NB I-94(FORD)	2036+63.3	107.4 LT	1840433.8	1177828.9	10
NB I-94(RYAN)	2203+04.7	90.7 RT	1841674.7	1177921.4	10
NB I-94(RYAN)	2203+06.4	110.5 RT	1841676.9	1177941.1	10
NB I-94(RYAN)	2203+13.1	108.4 RT	1841683.5	1177938.8	10
NB I-94(RYAN)	2203+37.1	105.4 RT	1841707.4	1177935.1	10
NB I-94(RYAN)	2203+53.7	109.6 RT	1841724.1	1177938.9	10
NB I-94(RYAN)	2203+74.2	102.0 RT	1841744.3	1177930.6	10
NB I-94(RYAN)	2203+95.0	112.6 RT	1841765.5	1177940.7	10

(CONT. ABOVE)

TREE REMOVAL (6 TO 15 UNITS) (CONT. FROM BELOW)

ALIGNMENT	LOCATION				TREE REMOV 6-15 (UNIT)
	STATION	OFFSET	NORTHING	EASTING	
NB I-94(RYAN)	2204+02.2	103.6 RT	1841772.3	1177931.5	10
NB I-94(RYAN)	2204+41.4	95.9 RT	1841791.8	1177922.5	10
NB I-94(RYAN)	2279+19.8	113.4 RT	1849287.1	1177726.1	15
NB I-94(RYAN)	2281+44.4	108.4 RT	1849511.5	1177714.6	14
NB I-94(RYAN)	2283+96.7	112.6 RT	1849763.9	1177711.5	7
NB I-94(RYAN)	2283+99.0	115.3 RT	1849766.3	1177714.3	7
NB I-94(RYAN)	2284+07.9	112.4 RT	1849775.1	1177711.0	6
NB I-94(RYAN)	2284+51.4	103.6 RT	1849818.3	1177701.0	6
NB I-94(RYAN)	2284+53.9	116.8 RT	1849821.2	1177714.1	6
NB I-94(RYAN)	2284+81.5	110.9 RT	1849848.6	1177707.4	15
NB I-94(RYAN)	2285+07.7	105.2 RT	1849874.7	1177701.0	14
NB I-94(RYAN)	2285+07.7	105.2 RT	1849874.7	1177701.0	12
NB I-94(RYAN)	2285+43.2	112.3 RT	1849910.3	1177706.0	10
NB I-94(RYAN)	2285+51.8	100.2 RT	1849918.6	1177694.7	10
TOTAL (6-15 UNITS)					1208

TREE REMOVAL (OVER 15 UNITS)

ALIGNMENT	LOCATION				TREE REMOV 6-15 (UNIT)
	STATION	OFFSET	NORTHING	EASTING	
NB I-57	209+12.3	70.7 RT	1839272.5	1174409.8	20
NB I-57	211+80.2	73.8 RT	1839276.1	1174677.6	16
NB I-57	212+97.0	76.2 RT	1839276.5	1174794.5	18
NB I-57	213+38.2	75.9 RT	1839277.8	1174835.6	17
NB I-57	214+01.7	72.9 RT	1839282.4	1174899.0	20
NB I-57	214+82.1	39.7 RT	1839317.6	1174978.6	16
NB I-57	218+99.8	76.5 RT	1839291.1	1175397.1	18
NB I-57	222+64.2	74.8 RT	1839301.8	1175761.3	17
NB I-57	241+35.1	11.3 RT	1840070.8	1177430.4	18
NB I-57	246+35.3	124.2 LT	1840551.7	1177577.1	20
NB I-57	248+94.9	53.1 LT	1840756.6	1177733.3	24
NB I-57	249+21.7	50.4 LT	1840780.7	1177743.6	18
NB I-57	249+39.7	58.3 LT	1840799.7	1177741.0	18
NB I-57	249+85.9	38.9 LT	1840837.8	1177771.8	22
NB I-57	250+29.7	42.4 LT	1840880.3	1177779.1	34
NB I-57	250+80.8	41.1 LT	1840928.8	1177791.5	17
NB I-57	251+85.7	45.0 LT	1841030.3	1177806.5	18
NB I-57	252+66.9	42.7 LT	1841108.6	1177819.8	19
NB I-94(RYAN)	2281+92.9	103.8 RT	1849559.9	1177708.6	16
TOTAL (>15 UNITS)					366

PROPOSED MAINLINE PAVEMENT RECONSTRUCTION

LOCATION DESCRIPTION	FROM		TO		PCC PVT 10 JOINTD (SQ YD)	PCC PVT 14 JOINTD (SQ YD)	CONT REINF PCC PVT 14 (SQ YD)	PAVT REINFORCEMENT 14 (SQ YD)	SUB GRAN MAT B 12 (SQ YD)	SUB GRAN MAT B 24 (SQ YD)	GEOTECH FAB F/GR STAB (SQ YD)	STAB SUB-BASE 6	PROTECTIVE COAT (SQ YD)
	ALIGNMENT	STATION	ALIGNMENT	STATION									
NB I-57 (LEFT SHOULDER)	NB I-57	191+41.6	NB I-57	209+07.5		1107				1107	1107	1107	1107
NB I-57	NB I-57	191+41.6	NB I-57	200+00.0			3434	3434		4637	4637	4419	3434
NB I-57	NB I-57	200+00.0	NB I-57	220+00.0			10778	10778		14175	14175	14317	10778
NB I-57	NB I-57	220+00.0	NB I-57	232+48.1			8571	8571		12016	12016	12016	8571
NB I-57	NB I-57	232+48.1	NB I-57	236+69.8			1765	1765		2751	2751	2521	1765
NB I-57	NB I-57	239+88.5	NB I-57	252+42.3			5248	5248		7849	7849	7618	5248
EB I-57 TO I-94 CONNECTOR	EB CON	407+90.5	EB CON	414+90.7			2009	2009		3491	3491	3334	2009
EB I-57 TO I-94 CONNECTOR	EB CON	414+90.7	EB CON	415+06.0		44				76	76	76	44
WB I-94 TO I-57 CONNECTOR	WB CON	330+65.5	WB CON	330+80.6		44				78	78	78	44
WB I-94 TO I-57 CONNECTOR	WB CON	330+80.6	WB CON	340+29.2			2735	2735		4716	4716	4558	2735
NB I-94 (BISHOP FORD)	NB I-94(FORD)	1990+48.7	NB I-94(FORD)	1998+33.5	2757				2986		2986		2757
NB I-94 (BISHOP FORD)	NB I-94(FORD)	2021+00.0	NB I-94(FORD)	2021+15.2		44				80	80	80	44
NB I-94 (BISHOP FORD)	NB I-94(FORD)	2021+15.2	NB I-94(FORD)	2042+88.0			7312	7312		12015	12015	11967	7312
NB I-94 (DAN RYAN)	NB I-57	252+42.3	NB I-94(RYAN)	2210+00.0			8857	8857		11353	11353	11353	8857
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2210+00.0	NB I-94(RYAN)	2230+00.0			14773	14773		18251	18251	18457	14773
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2230+00.0	NB I-94(RYAN)	2250+00.0			14244	14244		18358	18358	18476	14244
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2250+00.0	NB I-94(RYAN)	2270+00.0			13778	13778		17813	17813	17836	13778
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2270+00.0	NB I-94(RYAN)	2290+00.0			14976	14976		18879	18879	19006	14976
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2290+00.0	NB I-94(RYAN)	2310+00.0			14922	14922		18517	18517	19040	14922
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2310+00.0	NB I-94(RYAN)	2316+00.0			4152	4152		5213	5213	5213	4152
ADDITIONAL QUANTITY FOR STAGING OVERDIG										8298	3866	3866	
I000-2A TOTAL													
J000-2A TOTAL					2757	1239	127554	127554	2986	179673	178227	175338	131550
TOTAL					2757	1239	127554	127554	2986	179673	178227	175338	131550

PROPOSED INTERCHANGE PAVEMENT PATCHING/RESURFACING

LOCATION DESCRIPTION	OFFSET DIRECTION	FROM		TO		P BCSC SUPER 1/2" N105 1 3/4" (TON)	P BCSC SUP 1 1/2" N105 2 1/4" (TON)	BC SC SUPER 1/2" N70 1 1/2" (TON)	SUB GRAN MAT B 6 (SQ YD)	PCC BSE CSE 13 (SQ YD)	PCC SHOULDERS 9 (SQ YD)	PROTECTIVE COAT (SQ YD)
		ALIGNMENT	STATION	ALIGNMENT	STATION							
NB I-57 MAINLINE	-	NB I-57	190+65.0	NB I-57	191+41.6	38						
NB FORD SHOULDER & WIDENING	LT	NB I-94(FORD)	2002+55.0	NB I-94(FORD)	2006+61.4	15	19		610	153	370	370
WB CONNECTOR SHOULDER	LT	WB CON	303+59.3	WB CON	306+70.3			40				
NB FORD MAINLINE	-	NB I-94(FORD)	2002+55.0	NB I-94(FORD)	2009+73.1	379	487					
NB FORD SHOULDER	RT	NB I-94(FORD)	2003+03.2	NB I-94(FORD)	2009+73.1			52				
NB FORD SHOULDER	RT	NB I-94(FORD)	2009+73.1	NB I-94(FORD)	2018+86.6			72				
NB FORD SHOULDER	LT	NB I-94(FORD)	2010+30.1	NB I-94(FORD)	2021+00.0			85				
NB FORD MAINLINE	-	NB I-94(FORD)	2009+73.1	NB I-94(FORD)	2021+00.0	300	386					
WB CON SHOULDER & WIDENING	RT	WB CON	303+56.6	WB CON	306+70.3				198	198		
WB CON SHOULDER & WIDENING	RT	WB CON	306+70.3	WB CON	314+60.2	39			1166	403	542	542
WB CONNECTOR MAINLINE	-	WB CON	306+70.3	WB CON	320+00.0	234						
WB CONNECTOR MAINLINE	-	WB CON	320+00.0	WB CON	330+65.5	243						
UTILITY TRENCH PATCHES	-	-	-	-	-						325	325
INSIDE SHOULDER PATCHES	-	-	-	-	-			13				
I000-2A TOTAL						1210	892	248	1974	754	1237	1237
J000-2A TOTAL						38		13				
TOTAL						1248	892	261	1974	754	1237	1237

TYLININTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 1

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 7, 2006 CHECKED BY: RTM

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24

LOCATION DESCRIPTION	FROM			TO			COMB CC&G TM2.24 (FOOT)	PROTECTIVE COAT (SQ YD)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET		
NB I-57	NB I-57	232+50.1	11.9 RT	NB I-57	232+97.8	10.0 RT	49.0	14
EB CONNECTOR	EB CON	407+92.5	11.9 LT	EB CON	408+40.9	10.0 LT	49.5	14
WB CONNECTOR	WB CON	306+92.7	9.9 RT	WB CON	307+43.4	8.0 RT	52.0	15
NB I-94 (BISHOP FORD)	NB I-94(FORD)	2009+95.4	13.2 LT	NB I-94(FORD)	2011+54.2	8.3 LT	160.5	46
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2244+95.7	76.0 RT	NB I-94(RYAN)	2245+47.6	72.0 RT	58.0	17
							I000-2A TOTAL	61
							J000-2A TOTAL	45
							TOTAL	105

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48

LOCATION DESCRIPTION	FROM			TO			COMB CC&G TM4.48 (FOOT)	PROTECTIVE COAT (SQ YD)	
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET			
NB I-57	NB I-57	203+53.1	26.3 RT	NB I-57	205+45.8	22.5 RT	193.0	101	
NB I-94 (BISHOP FORD)	NB I-94(FORD)	2034+13.6	50.3 RT	NB I-94(FORD)	2041+11.9	36.0 RT	684.5	359	
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2223+48.8	86.3 RT	NB I-94(RYAN)	2227+54.8	78.2 RT	406.5	213	
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2240+70.0	72.0 RT	NB I-94(RYAN)	2242+45.8	82.8 RT	176.5	93	
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2272+62.0	86.3 RT	NB I-94(RYAN)	2279+28.6	73.0 RT	667.0	350	
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2289+78.0	72.0 RT	NB I-94(RYAN)	2292+40.4	86.3 RT	263.0	138	
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2303+64.6	86.3 RT	NB I-94(RYAN)	2308+69.6	72.0 RT	505.5	265	
							I000-2A TOTAL		
							J000-2A TOTAL	2896.0	1520
							TOTAL	2896.0	1520

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24

LOCATION DESCRIPTION	FROM			TO			COMB CC&G TM4.24 (FOOT)	PROTECTIVE COAT (SQ YD)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET		
NB I-57	NB I-57	191+41.6	13.1 RT	NB I-57	200+58.0	16.0 RT	922.5	279
NB I-57	NB I-57	223+60.0	32.0 RT	NB I-57	229+83.3	44.3 RT	634.0	192
NB I-57	NB I-57	232+97.8	10.0 RT	NB I-57	234+50.0	10.0 RT	153.0	46
EB CONNECTOR	EB CON	407+71.5	34.0 RT	EB CON	415+06.0	34.0 RT	722.0	219
EB CONNECTOR	EB CON	408+40.9	10.0 LT	EB CON	415+06.0	10.0 LT	669.0	203
WB CONNECTOR	WB CON	307+43.4	8.0 RT	WB CON	314+29.0	8.0 RT	745.0	226
WB CONNECTOR	WB CON	330+11.8	22.0 RT	WB CON	332+36.4	18.0 RT	228.0	69
WB CONNECTOR	WB CON	336+74.8	14.5 RT	WB CON	340+29.2	12.0 RT	357.5	108
NB I-94 (BISHOP FORD)	NB I-94(FORD)	1990+48.7	34.1 RT	NB I-94(FORD)	1999+42.3	33.7 RT	915.0	277
NB I-94 (BISHOP FORD)	NB I-94(FORD)	2002+55.0	33.9 RT	NB I-94(FORD)	2017+48.7	34.2 RT	1481.0	448
NB I-94 (BISHOP FORD)	NB I-94(FORD)	2021+00.0	34.2 RT	NB I-94(FORD)	2027+43.1	36.0 RT	631.5	191
NB I-94 (BISHOP FORD)	NB I-94(FORD)	2030+49.7	34.0 RT	NB I-94(FORD)	2031+88.8	38.0 RT	142.5	43
NB I-94 (FORD/RYAN)	NB I-94(FORD)	2041+11.9	36.0 RT	NB I-94(RYAN)	2203+35.4	72.0 RT	802.5	243
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2210+15.7	64.0 RT	NB I-94(RYAN)	2219+11.7	76.0 RT	902.5	273
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2238+47.7	63.5 RT	NB I-94(RYAN)	2240+70.0	72.0 RT	223.0	68
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2245+47.6	72.0 RT	NB I-94(RYAN)	2256+64.7	72.0 RT	1117.5	338
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2261+00.0	72.0 RT	NB I-94(RYAN)	2269+48.1	76.0 RT	854.5	259
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2287+52.9	72.0 RT	NB I-94(RYAN)	2289+78.0	72.0 RT	225.5	68
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2308+69.6	72.0 RT	NB I-94(RYAN)	2309+97.6	72.0 RT	128.0	39
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2314+38.5	72.0 RT	NB I-94(RYAN)	2316+00.0	72.0 RT	162.5	49
							I000-2A TOTAL	674
							J000-2A TOTAL	2964
							TOTAL	3638

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 2

SCALE: NONE
DATE: MARCH 7, 2006
DRAWN BY: MPG
CHECKED BY: RTM

CONCRETE MEDIAN SURFACE, 6" (SPECIAL)

LOCATION DESCRIPTION	OFFSET DIRECTION	FROM		TO		CONC MEDIAN SURF 6" SP (SQ FT)
		ALIGNMENT	STATION	ALIGNMENT	STATION	
NB I-57 - HALSTED RAMP GORE	RT	NB I-57	200+00.0	NB I-57	200+57.9	303
NB I-57 - GUARDRAIL	RT	NB I-57	204+40.9	NB I-57	205+45.9	305
NB I-57 - GUARDRAIL	RT	NB I-57	228+76.6	NB I-57	229+83.2	334
NB I-57/EB CON SPLIT - GORE	RT	NB I-57	232+51.9	NB I-57	232+97.8	263
NB I-57 - GUARDRAIL	RT	NB I-57	233+55.4	NB I-57	234+50.0	272
NB I-94 (DAN RYAN) - GUARDRAIL	RT	NB I-57	257+59.1	NB I-94(RYAN)	2203+35.4	338
WB CONNECTOR - GUARDRAIL	RT	WB CON	313+51.5	WB CON	314+60.3	344
WB CONNECTOR - GUARDRAIL	RT	WB CON	331+32.9	WB CON	332+36.4	297
NB I-94 (FORD) - GUARDRAIL	RT	NB I-94(FORD)	1998+37.3	NB I-94(FORD)	1999+42.3	305
NB I-94 (FORD) - GUARDRAIL	RT	NB I-94(FORD)	2009+97.4	NB I-94(FORD)	2011+54.2	557
NB I-94 (FORD) - GUARDRAIL	RT	NB I-94(FORD)	2016+41.7	NB I-94(FORD)	2017+48.7	310
NB I-94 (FORD) - GUARDRAIL	RT	NB I-94(FORD)	2026+32.8	NB I-94(FORD)	2027+43.1	343
NB I-94 (FORD) - MICHIGAN RAMP GORE	RT	NB I-94(FORD)	2031+37.7	NB I-94(FORD)	2031+88.7	258
NB I-94 (RYAN) - 95TH RAMP GORE	RT	NB I-94(RYAN)	2217+08.8	NB I-94(RYAN)	2219+11.6	1305
NB I-94 (RYAN) - GUARDRAIL	RT	NB I-94(RYAN)	2226+46.2	NB I-94(RYAN)	2227+54.9	338
NB I-94 (RYAN) - 91ST ST AND RR BRIDGES	RT	NB I-94(RYAN)	2233+18.3	NB I-94(RYAN)	2236+44.8	1667
NB I-94 (RYAN) - RR BRIDGE TO 87TH EXIT	RT	NB I-94(RYAN)	2238+47.7	NB I-94(RYAN)	2239+36.4	130
NB I-94 (RYAN) - 87TH EXIT RAMP GORE	RT	NB I-94(RYAN)	2244+95.8	NB I-94(RYAN)	2245+47.6	232
NB I-94 (RYAN) - GUARDRAIL	RT	NB I-94(RYAN)	2255+56.1	NB I-94(RYAN)	2256+64.7	338
NB I-94 (RYAN) - 87TH ENT RAMP GORE	RT	NB I-94(RYAN)	2268+89.9	NB I-94(RYAN)	2269+48.1	260
NB I-94 (RYAN) - GUARDRAIL	RT	NB I-94(RYAN)	2278+23.7	NB I-94(RYAN)	2279+28.7	306
NB I-94 (RYAN) - GUARDRAIL	RT	NB I-94(RYAN)	2308+89.0	NB I-94(RYAN)	2309+97.7	338
				I000-2A TOTAL		1211
				J000-2A TOTAL		7932
				TOTAL		9143

PORTLAND CEMENT CONCRETE SHOULDERS 14"

LOCATION DESCRIPTION	OFFSET DIRECTION	FROM		TO		PCC SHOULDERS 14" (SQ YD)	PROTECTIVE COAT (SQ YD)
		ALIGNMENT	STATION	ALIGNMENT	STATION		
NB I-57 - WEST OF HALSTED RAMP	RT	NB I-57	191+41.6	NB I-57	203+52.7	1361	1361
NB I-57 - HALSTED RAMP TO EB CON SPLIT	RT	NB I-57	205+45.8	NB I-57	232+44.4	2260	2260
NB I-57 / EB CON SPLIT - GORE	-	NB I-57	227+77.4	NB I-57	232+48.1	519	519
EB CONNECTOR	RT	EB CON	407+90.9	EB CON	415+06.0	470	470
EB CONNECTOR	LT	EB CON	407+90.9	EB CON	415+06.0	637	637
NB I-57 - EB CON GORE TO WB CON BRIDGE	RT	NB I-57	232+48.1	NB I-57	236+69.8	331	331
NB I-57 - WB CON BRIDGE AND I-94 MERGE	RT	NB I-57	239+88.5	NB I-57	244+28.1	394	394
NB I-57 - WB CON BRIDGE AND I-94 MERGE	RT	NB I-57	245+47.4	NB I-57	252+44.0	529	529
NB I-57 MEDIAN	LT	NB I-57	209+07.5	NB I-57	232+48.1	2587	2587
NB I-57 MEDIAN	LT	NB I-57	232+48.1	NB I-57	236+69.8	460	460
NB I-57 MEDIAN	LT	NB I-57	239+88.5	NB I-57	252+44.0	1161	1161
NB I-94 (DAN RYAN) MEDIAN	LT	NB I-94(RYAN)	252+44.0	NB I-94(RYAN)	2210+00.0	1155	1155
WB CONNECTOR	LT	WB CON	330+65.5	WB CON	340+29.2	1055	1055
WB CONNECTOR	RT	WB CON	330+65.5	WB CON	337+12.2	498	498
NB I-57 / NB I-94 MERGE - GORE	-	NB I-57	252+44.0	NB I-57	253+96.0	210	210
NB I-94 (BISHOP FORD) MEDIAN	LT	NB I-94(FORD)	2021+00.0	NB I-94(FORD)	2042+90.0	2435	2435
NB I-94 (FORD) - LIMIT TO MICHIGAN RAMP	RT	NB I-94(FORD)	2021+00.0	NB I-94(FORD)	2034+13.2	1112	1112
NB I-94 (FORD/RYAN) - MICH RAMP TO 95TH ST	RT	NB I-94(FORD)	2041+11.8	NB I-94(RYAN)	2210+15.7	965	965
NB I-94 (RYAN) - 95TH ST TO 95TH RAMP	RT	NB I-94(RYAN)	2210+15.1	NB I-94(RYAN)	2223+48.4	1216	1216
NB I-94 (RYAN) - 95TH RAMP TO 87TH EXIT	RT	NB I-94(RYAN)	2227+54.7	NB I-94(RYAN)	2240+70.3	833	833
NB I-94 (RYAN) - 87TH EXIT TO 87TH ENT	RT	NB I-94(RYAN)	2243+05.0	NB I-94(RYAN)	2272+61.6	2557	2557
NB I-94 (RYAN) - 87TH ENT TO 79TH EXIT	RT	NB I-94(RYAN)	2279+28.6	NB I-94(RYAN)	2289+78.3	702	702
NB I-94 (RYAN) - 79TH EX TO 83RD ENT	RT	NB I-94(RYAN)	2292+41.5	NB I-94(RYAN)	2303+63.9	1252	1252
NB I-94 (RYAN) - 83RD ENT TO 79TH ST	RT	NB I-94(RYAN)	2308+69.5	NB I-94(RYAN)	2316+00.0	389	389
NB I-94 (DAN RYAN) MEDIAN	LT	NB I-94(RYAN)	2210+00.0	NB I-94(RYAN)	2230+00.0	1559	1559
NB I-94 (DAN RYAN) MEDIAN	LT	NB I-94(RYAN)	2230+00.0	NB I-94(RYAN)	2250+00.0	2126	2126
NB I-94 (DAN RYAN) MEDIAN	LT	NB I-94(RYAN)	2250+00.0	NB I-94(RYAN)	2270+00.0	1665	1665
NB I-94 (DAN RYAN) MEDIAN	LT	NB I-94(RYAN)	2270+00.0	NB I-94(RYAN)	2290+00.0	2209	2209
NB I-94 (DAN RYAN) MEDIAN	LT	NB I-94(RYAN)	2290+00.0	NB I-94(RYAN)	2310+00.0	2121	2121
NB I-94 (DAN RYAN) MEDIAN	LT	NB I-94(RYAN)	2310+00.0	NB I-94(RYAN)	2316+00.0	602	602
				I000-2A TOTAL			
				J000-2A TOTAL		35370	35370
				TOTAL		35370	35370

CONCRETE MEDIAN SURFACE, 4"

LOCATION DESCRIPTION	OFFSET DIRECTION	FROM		TO		CONC MEDIAN SURF 4" (SQ FT)	PROTECTIVE COAT (SQ YD)
		ALIGNMENT	STATION	ALIGNMENT	STATION		
NB I-57 - UNDER WENTWORTH AV	RT	NB I-57	231+39.2	NB I-57	232+25.1	384	43
WB CONNECTOR - UNDER NB I-57	RT	WB CON	335+44.8	WB CON	336+68.5	623	69
WB CONNECTOR - UNDER NB I-57	LT	WB CON	336+26.0	WB CON	337+58.4	473	53
NB I-94 (FORD) - BISHOP FORD MEDIAN	LT	NB I-94(FORD)	2012+54.1	NB I-94(FORD)	2015+11.3	5263	585
NB I-94 (FORD) - UNDER MICHIGAN AV	LT	NB I-94(FORD)	2019+08.4	NB I-94(FORD)	2024+17.9	8204	912
NB I-94 (FORD) - UNDER MICHIGAN AV	RT	NB I-94(FORD)	2019+55.0	NB I-94(FORD)	2020+62.3	714	79
NB I-94 (FORD) - UNDER STATE ST	LT	NB I-94(FORD)	2026+41.1	NB I-94(FORD)	2029+66.2	8221	913
NB I-94 (FORD) - UNDER STATE ST	LT	NB I-94(FORD)	2028+63.4	NB I-94(FORD)	2029+39.2	915	102
NB I-94 (FORD) - UNDER STATE ST	LT	NB I-94(FORD)	2028+95.9	NB I-94(FORD)	2029+76.6	410	46
NB I-94 (FORD) - MICHIGAN RAMP GORE	RT	NB I-94(FORD)	2029+22.7	NB I-94(FORD)	2031+37.7	2041	227
UTILITY TRENCH PATCHES	-	-	-	-	-	33	4
				I000-2A TOTAL		6010	668
				J000-2A TOTAL		21271	2363
				TOTAL		27281	3031

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 3

SCALE: NONE
DATE: MARCH 7, 2006
DRAWN BY: MPG
CHECKED BY: RTM

CTA FENCE

LOCATION DESCRIPTION	FROM			TO			CTA FENCE (FOOT)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2213+52.0	8.3 LT	NB I-94(RYAN)	2215+74.7	6.8 LT	223
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2215+80.7	6.8 LT	NB I-94(RYAN)	2223+64.7	8.3 LT	784
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2223+70.7	8.3 LT	NB I-94(RYAN)	2232+45.2	10.0 LT	875
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2233+20.3	9.8 LT	NB I-94(RYAN)	2233+71.2	11.8 LT	51
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2233+77.2	11.8 LT	NB I-94(RYAN)	2236+46.1	9.6 LT	269
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2236+78.5	9.2 LT	NB I-94(RYAN)	2236+97.7	9.1 LT	20
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2237+31.6	9.2 LT	NB I-94(RYAN)	2237+71.8	9.4 LT	41
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2238+72.3	9.4 LT	NB I-94(RYAN)	2244+10.2	11.8 LT	539
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2244+16.2	11.8 LT	NB I-94(RYAN)	2252+85.3	8.2 LT	870
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2252+91.3	8.2 LT	NB I-94(RYAN)	2253+20.5	9.5 LT	30
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2259+79.1	10.9 LT	NB I-94(RYAN)	2263+20.7	8.3 LT	342
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2263+26.7	8.3 LT	NB I-94(RYAN)	2266+39.0	9.0 LT	313
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2267+02.7	9.0 LT	NB I-94(RYAN)	2268+96.2	11.8 LT	194
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2269+02.2	11.8 LT	NB I-94(RYAN)	2279+45.5	11.8 LT	1044
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2279+51.5	11.8 LT	NB I-94(RYAN)	2285+58.4	10.7 LT	608
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2286+56.5	10.7 LT	NB I-94(RYAN)	2289+94.7	11.8 LT	339
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2290+00.7	11.8 LT	NB I-94(RYAN)	2300+95.4	11.8 LT	1096
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2301+01.4	11.8 LT	NB I-94(RYAN)	2305+62.7	9.8 LT	462
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2305+68.7	9.8 LT	NB I-94(RYAN)	2307+01.8	10.5 LT	134
NB I-94 (DAN RYAN) - MEDIAN	NB I-94(RYAN)	2315+38.2	11.9 LT	NB I-94(RYAN)	2315+98.4	9.8 LT	61
I000-2A TOTAL							
J000-2A TOTAL							8295
TOTAL							8295

EARTHWORK SCHEDULE

RAMP/WALL	STATION		EARTH EXCAVATION (CU YD)	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL (CU YD)	TOPSOIL (CU YD)	EMBANKMENT (CU YD)
	FROM	TO				
HALSTED TO PARNELL	191+50	208+50	7525	550	715	25
PARNELL TO I-57 BRIDGE	209+50	236+50	23210	3475	4785	275
I-57 BRIDGE TO I-94 NB	237+00	258+00	11450	1765	2320	6650
AIS 5 to MICHIGAN BRIDGE	1990+00	2020+50	4505	2035	2655	40
MICHIGAN BRIDGE TO I-57	2021+00	2042+50	7840	905	980	915
I-57 TO 95TH CTA STATION	2203+00	2209+50	5735	295	465	0
95TH CTA STATION TO BELT RAILWAY	2210+00	2237+50	22565	610	1150	20
BELT RAILWAY TO 87TH	2238+00	2259+50	14580	505	525	5
87TH TO 83RD	2260+00	2285+50	20825	1420	1565	30
83RD TO 79TH	2286+00	2312+00	17930	400	400	25
79TH TO 78TH	2312+50	2316+00	2915	135	270	5
I-57 NB to I-94 SB CONNECTOR	408+00	415+06	2610	365	380	120
I-94 NB to I-57 SB CONNECTOR	325+00	340+29	6045	845	1230	500
12" PGES - SEE SEPARATE SCHEDULE			2899			
TOTAL			150634	13305	17440	8610

PROPOSED GUARDRAIL

LOCATION DESCRIPTION	ALIGNMENT	END OF TYPE 1 TERMINAL		JOINT BETWEEN TERMINALS		END OF TYPE 6 TERMINAL		TR BAR TRM T1 SPL TAN (EACH)	TRAF BAR TRM T6 (EACH)	GUARDRAIL MKR TYPE A (EACH)	TERMINAL MARKER - DA (EACH)
		STATION	OFFSET	STATION	OFFSET	STATION	OFFSET				
NB I-57	NB I-57	204+65.2	25.1 RT	205+15.2	23.1 RT	205+45.8	22.5 RT	1	1	4	1
NB I-57	NB I-57	229+04.4	44.4 RT	229+53.3	44.3 RT	229+83.2	44.9 RT	1	1	4	1
NB I-57	NB I-57	233+69.8	11.0 RT	234+19.5	10.0 RT	234+50.0	10.0 RT	1	1	4	1
NB I-94 (BISHOP FORD)	NB I-94(FORD)	1998+61.7	34.7 RT	1999+11.7	33.7 RT	1999+42.3	33.7 RT	1	1	4	1
NB I-94 (BISHOP FORD)	NB I-94(FORD)	2010+73.9	9.8 LT	2011+23.7	8.4 LT	2011+54.2	8.3 LT	1	1	4	1
NB I-94 (BISHOP FORD)	NB I-94(FORD)	2016+66.6	35.2 RT	2017+17.5	34.2 RT	2017+17.5	34.2 RT	1	1	4	1
WB CONNECTOR	WB CON	313+79.1	8.3 RT	314+29.4	8.0 RT	314+60.2	7.7 RT	1	1	4	1
WB CONNECTOR	WB CON	331+56.8	19.0 RT	332+06.2	18.0 RT	332+06.2	18.0 RT	1	1	4	1
NB I-94 (BISHOP FORD)	NB I-94(FORD)	2026+60.8	37.0 RT	2027+11.8	36.0 RT	2027+43.1	36.0 RT	1	1	4	1
NB I-94 (DAN RYAN)	NB I-57	257+87.0	37.0 RT	2203+04.8	72.0 RT	2203+35.4	72.0 RT	1	1	4	1
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2226+74.2	80.8 RT	2227+24.2	78.8 RT	2227+54.8	78.2 RT	1	1	4	1
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2255+84.0	73.0 RT	2256+34.0	72.0 RT	2256+64.7	72.0 RT	1	1	4	1
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2278+48.0	75.6 RT	2278+98.0	73.6 RT	2279+28.6	73.0 RT	1	1	4	1
NB I-94 (DAN RYAN)	NB I-94(RYAN)	2309+17.0	73.0 RT	2309+67.0	72.0 RT	2309+97.6	72.0 RT	1	1	4	1
I000-2A TOTAL								3	3	12	3
J000-2A TOTAL								11	11	44	11
TOTAL								14	14	56	14

CTA GATES

LOCATION DESCRIPTION	LOCATION			CTA GATES (EACH)
	ALIGNMENT	STATION	OFFSET	
NB I-94 (DAN RYAN) MEDIAN	NB I-94(RYAN)	2215+80.7	6.8 LT	1
NB I-94 (DAN RYAN) MEDIAN	NB I-94(RYAN)	2223+70.7	8.3 LT	1
NB I-94 (DAN RYAN) MEDIAN	NB I-94(RYAN)	2233+77.2	11.8 LT	1
NB I-94 (DAN RYAN) MEDIAN	NB I-94(RYAN)	2244+16.2	11.8 LT	1
NB I-94 (DAN RYAN) MEDIAN	NB I-94(RYAN)	2252+91.3	8.2 LT	1
NB I-94 (DAN RYAN) MEDIAN	NB I-94(RYAN)	2263+26.7	8.3 LT	1
NB I-94 (DAN RYAN) MEDIAN	NB I-94(RYAN)	2269+02.2	11.8 LT	1
NB I-94 (DAN RYAN) MEDIAN	NB I-94(RYAN)	2279+51.5	11.8 LT	1
NB I-94 (DAN RYAN) MEDIAN	NB I-94(RYAN)	2290+00.7	11.8 LT	1
NB I-94 (DAN RYAN) MEDIAN	NB I-94(RYAN)	2301+01.4	11.8 LT	1
NB I-94 (DAN RYAN) MEDIAN	NB I-94(RYAN)	2305+68.7	9.8 LT	1
I000-2A TOTAL				
J000-2A TOTAL				11
TOTAL				11

POROUS GRANULAR EMBANKMENT, SUBGRADE

ALIGNMENT	DESCRIPTION	STATION		LENGTH (FOOT)	WIDTH (FOOT)	DEPTH (INCH)	VOLUME (CU YD)
		FROM	TO				
NB I-94 (FORD)	MAINLINE BISHOP FORD	2034+50.0	2038+25.0	375	58	12	806
NB I-94 (FORD)	MAINLINE BISHOP FORD	2041+25.0	2044+25.0	300	70	12	778
NB I-94 (RYAN)	MAINLINE DAN RYAN	2277+50.0	2278+25.0	75	88	12	244
NB I-94 (RYAN)	MAINLINE DAN RYAN	2295+75.0	2298+50.0	275	80	12	815
NB I-57	SOFT SOIL AT NW RET WALL	239+05.0	239+41.0	36	-	-	256
TOTAL (CU YD)							2899

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 6

SCALE: NONE
DATE: MARCH 7, 2006
DRAWN BY: MPG
CHECKED BY: RTM

IMPACT ATTENUATORS

LOCATION		IMPACT ATTENUATORS REQUIRED (EACH)	STAGE #	REMAINS THROUGH STAGE #	TEMPORARY IMPACT ATTENUATOR (EACH)†	RELOCATE IMPACT ATTENUATOR (EACH)†	ATTENUATOR REMAIN FROM STAGE-STAGE (EACH)	ATTENUATOR RELOCATED @ END STAGE (EACH)	ATTENUATOR REMOVED @ END STAGE (EACH)
ALIGNMENT	STATION								
NB I-57	189+33	1	1	1	1			1	
NB I-57	207+55	1	1	1	1				1
NB I-57	231+68	1	1	1	1				1
NB I-94(FORD)	1990+08	1	1	1	1			1	
WB CON	328+08	1	1	1	1			1	
WB CON	330+11	1	1	1	1			1	
NB I-94(RYAN)	2233+46	1	1	1	1			1	
NB I-94(RYAN)	2243+66	1	1	1	1			1	
NB I-94(RYAN)	2276+91	1	1	1C	1		1		
NB I-94(RYAN)	2289+90	1	1	1C	1		1		
NB I-94(RYAN)	2290+51	1	1	W	1		1		
NB I-94(RYAN)	2299+18	1	1	W	1		1		
NB I-57	188+56	1	1A	1A		1		1	
NB I-94(FORD)	2001+26	1	1A	1A		1		1	
WB CON	305+85	1	1A	1A		1		1	
WB CON	329+77	1	1A	1C		1	1		
NB I-57	250+97	1	1A	1A		1		1	
NB I-94(RYAN)	2251+59	1	1A	1A		1		1	
NB I-94(RYAN)	2276+91	1	1A	1C			1		
NB I-94(RYAN)	2289+90	1	1A	1C			1		
NB I-94(RYAN)	2290+51	1	1A	W			1		
NB I-94(RYAN)	2299+18	1	1A	W			1		
NB I-57	189+38	1	1B	1B		1		1	
WB CON	305+84	1	1B	1C		1	1		
WB CON	329+77	1	1B	1C			1		
NB I-94(RYAN)	2204+31	1	1B	1C		1	1		
NB I-94(RYAN)	2233+45	1	1B	W		1	1		
NB I-94(RYAN)	2251+59	1	1B	W		1	1		
NB I-94(RYAN)	2276+91	1	1B	1C			1		
NB I-94(RYAN)	2289+90	1	1B	1C			1		
NB I-94(RYAN)	2290+51	1	1B	W			1		
NB I-94(RYAN)	2299+18	1	1B	W			1		
NB I-57	206+44	1	1C	1C		1		1	
WB CON	305+84	1	1C	1C				1	
WB CON	329+77	1	1C	1C				1	
NB I-94(RYAN)	2233+45	1	1C	W			1		
NB I-94(RYAN)	2251+59	1	1C	W			1		
NB I-94(RYAN)	2251+59	1	1C	1C				1	
NB I-94(RYAN)	2276+91	1	1C	1C				1	
NB I-94(RYAN)	2289+90	1	1C	1C				1	
NB I-94(RYAN)	2290+51	1	1C	W			1		
NB I-94(RYAN)	2299+18	1	1C	W			1		
NB I-57	231+68	1	W	W		1		1	
WB CON	305+88	1	W	W		1		1	
WB CON	329+92	1	W	2		1	1		
NB I-94(RYAN)	2204+31	1	W	W				1	
NB I-94(RYAN)	2233+45	1	W	W				1	
NB I-94(RYAN)	2243+59	1	W	W		1		1	
NB I-94(RYAN)	2276+92	1	W	W		1		1	
NB I-94(RYAN)	2289+91	1	W	W		1		1	
NB I-94(RYAN)	2290+51	1	W	W				1	
NB I-94(RYAN)	2299+18	1	W	W					1
NB I-57	217+06	1	2	3		1	1		
NB I-57	217+45	1	2	3		1	1		

† - DENOTES IDOT PAY ITEMS

TYLIN INTERNATIONAL

IMPACT ATTENUATORS

LOCATION		IMPACT ATTENUATORS REQUIRED (EACH)	STAGE #	REMAINS THROUGH STAGE #	TEMPORARY IMPACT ATTENUATOR (EACH)†	RELOCATE IMPACT ATTENUATOR (EACH)†	ATTENUATOR REMAIN FROM STAGE-STAGE (EACH)	ATTENUATOR RELOCATED @ END STAGE (EACH)	ATTENUATOR REMOVED @ END STAGE (EACH)
ALIGNMENT	STATION								
NB I-94(FORD)	2001+69	1	2	2		1		1	
NB I-94(FORD)	2004+45	1	2	2		1		1	
NB I-94(FORD)	2041+49	1	2	3		1	1		
WB CON	329+92	1	2	2				1	
NB I-94(RYAN)	2250+75	1	2	2		1		1	
STAGE 2 WORK ZONES		2	2	2		2		2	
NB I-57	217+06	1	3	3				1	
NB I-57	217+45	1	3	3				1	
WB CON	329+89	1	3	3		1			1
WB CON	329+87	1	3	3		1		1	
NB I-94(FORD)	2041+49	1	3	3					1
NB I-94(RYAN)	2204+23	1	3	3		1		1	
STAGE 3 WORK ZONES		3	3	3A		3	3		
NB I-57	217+60	1	3A	3A		1			1
NB I-57	235+41	1	3A	3A		1			1
NB I-57	247+03	1	3A	3A		1			1
WB CON	329+38	1	3A	3A		1			1
STAGE 3 WORK ZONES		3	3A	3A					3
STAGE 1 TOTALS:		12			12		4	6	2
STAGE 1A TOTALS:		10				6	5	5	
STAGE 1B TOTALS:		10				5	9	1	
STAGE 1C TOTALS:		10				1	4	6	
WINTER TOTALS:		10				6	1	8	1
STAGE 2 TOTALS:		9				8	3	6	
STAGE 3 TOTALS:		9				6	3	4	2
STAGE 3A TOTALS:		7				4			7
FINAL TOTALS:		77			12	36	29	36	12

† - DENOTES IDOT PAY ITEMS

NOTE:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
TEMPORARY IMPACT ATTENUATOR
QUANTITIES - SHEET 1

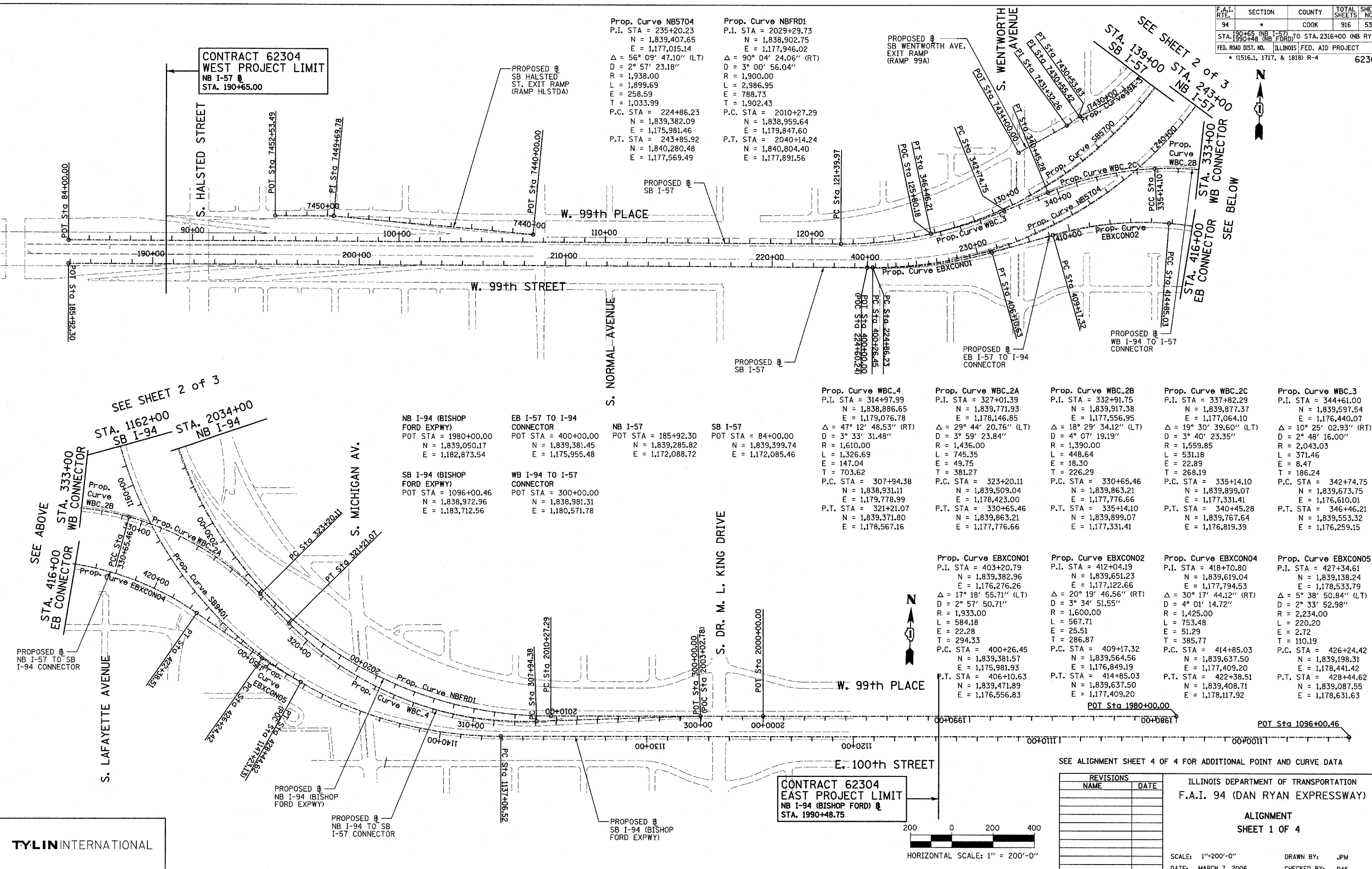
SCALE: NONE DRAWN BY: RTM
DATE: MARCH 7, 2006 CHECKED BY: MPG

Prop. Curve NB5704
 P.I. STA = 235+20.23
 N = 1,839,407.65
 E = 1,177,015.14
 $\Delta = 56^\circ 09' 47.10''$ (LT)
 D = 2° 57' 23.18"
 R = 1,938.00
 L = 1,899.69
 E = 258.59
 T = 1,033.99
 P.C. STA = 224+86.23
 N = 1,839,382.09
 E = 1,175,981.46
 P.T. STA = 243+85.92
 N = 1,840,280.48
 E = 1,177,569.49

Prop. Curve NBFDR1
 P.I. STA = 2029+29.73
 N = 1,838,902.75
 E = 1,177,946.02
 $\Delta = 90^\circ 04' 24.06''$ (RT)
 D = 3° 00' 56.04"
 R = 1,900.00
 L = 2,986.95
 E = 788.73
 T = 1,902.43
 P.C. STA = 2010+27.29
 N = 1,838,959.64
 E = 1,179,847.60
 P.T. STA = 2040+14.24
 N = 1,840,804.40
 E = 1,177,891.56

CONTRACT 62304
WEST PROJECT LIMIT
 NB I-57 @
 STA. 190+65.00

CONTRACT 62304
EAST PROJECT LIMIT
 NB I-94 (BISHOP FORD) @
 STA. 1990+48.75



NB I-94 (BISHOP FORD EXPWY)
 POT STA = 1980+00.00
 N = 1,839,050.17
 E = 1,182,873.54

EB I-57 TO I-94 CONNECTOR
 POT STA = 400+00.00
 N = 1,839,381.45
 E = 1,175,955.48

NB I-57
 POT STA = 185+92.30
 N = 1,839,285.82
 E = 1,172,088.72

SB I-57
 POT STA = 84+00.00
 N = 1,839,399.74
 E = 1,172,085.46

SB I-94 (BISHOP FORD EXPWY)
 POT STA = 1096+00.46
 N = 1,838,972.96
 E = 1,183,712.56

WB I-94 TO I-57 CONNECTOR
 POT STA = 300+00.00
 N = 1,838,981.31
 E = 1,180,571.78

Prop. Curve WBC_4
 P.I. STA = 314+97.99
 N = 1,838,886.65
 E = 1,179,076.78
 $\Delta = 47^\circ 12' 48.53''$ (RT)
 D = 3° 33' 31.48"
 R = 1,610.00
 L = 1,326.69
 E = 147.04
 T = 703.62
 P.C. STA = 307+94.38
 N = 1,838,931.11
 E = 1,179,778.99
 P.T. STA = 321+21.07
 N = 1,839,371.80
 E = 1,178,567.16

Prop. Curve WBC_2A
 P.I. STA = 327+01.39
 N = 1,839,771.93
 E = 1,178,146.85
 $\Delta = 29^\circ 44' 20.76''$ (LT)
 D = 3° 59' 23.84"
 R = 1,436.00
 L = 745.35
 E = 49.75
 T = 381.27
 P.C. STA = 323+20.11
 N = 1,839,509.04
 E = 1,177,776.66
 P.T. STA = 330+65.46
 N = 1,839,863.21
 E = 1,177,776.66

Prop. Curve WBC_2B
 P.I. STA = 332+91.75
 N = 1,839,917.38
 E = 1,177,556.95
 $\Delta = 18^\circ 29' 34.12''$ (LT)
 D = 4° 07' 19.19"
 R = 1,390.00
 L = 448.64
 E = 18.30
 T = 226.29
 P.C. STA = 330+65.46
 N = 1,839,863.21
 E = 1,177,776.66
 P.T. STA = 335+14.10
 N = 1,839,899.07
 E = 1,177,331.41

Prop. Curve WBC_2C
 P.I. STA = 337+82.29
 N = 1,839,877.37
 E = 1,177,064.10
 $\Delta = 19^\circ 30' 39.60''$ (LT)
 D = 3° 40' 23.35"
 R = 1,559.85
 L = 531.18
 E = 22.89
 T = 268.19
 P.C. STA = 335+14.10
 N = 1,839,899.07
 E = 1,177,331.41
 P.T. STA = 340+45.28
 N = 1,839,767.64
 E = 1,176,819.39

Prop. Curve WBC_3
 P.I. STA = 344+61.00
 N = 1,839,597.54
 E = 1,176,440.07
 $\Delta = 10^\circ 25' 02.93''$ (RT)
 D = 2° 48' 16.00"
 R = 2,043.03
 L = 371.46
 E = 8.47
 T = 186.24
 P.C. STA = 342+74.75
 N = 1,839,673.75
 E = 1,176,610.01
 P.T. STA = 346+46.21
 N = 1,839,553.32
 E = 1,176,259.15

Prop. Curve EBXCNO1
 P.I. STA = 403+20.79
 N = 1,839,382.96
 E = 1,176,276.26
 $\Delta = 17^\circ 18' 55.71''$ (LT)
 D = 2° 57' 50.71"
 R = 1,933.00
 L = 584.18
 E = 22.28
 T = 294.33
 P.C. STA = 400+26.45
 N = 1,839,381.57
 E = 1,175,981.93
 P.T. STA = 406+10.63
 N = 1,839,471.89
 E = 1,176,556.83

Prop. Curve EBXCNO2
 P.I. STA = 412+04.19
 N = 1,839,651.23
 E = 1,177,122.66
 $\Delta = 20^\circ 19' 46.56''$ (RT)
 D = 3° 34' 51.55"
 R = 1,600.00
 L = 567.71
 E = 25.51
 T = 286.87
 P.C. STA = 409+17.32
 N = 1,839,564.56
 E = 1,176,849.19
 P.T. STA = 414+85.03
 N = 1,839,637.50
 E = 1,177,409.20

Prop. Curve EBXCNO4
 P.I. STA = 418+70.80
 N = 1,839,619.04
 E = 1,177,944.53
 $\Delta = 30^\circ 17' 44.12''$ (RT)
 D = 4° 01' 14.72"
 R = 1,425.00
 L = 753.48
 E = 51.29
 T = 385.77
 P.C. STA = 414+85.03
 N = 1,839,637.50
 E = 1,177,409.20
 P.T. STA = 422+38.51
 N = 1,839,408.71
 E = 1,178,117.92

Prop. Curve EBXCNO5
 P.I. STA = 427+34.61
 N = 1,839,138.24
 E = 1,175,533.79
 $\Delta = 5^\circ 38' 50.84''$ (LT)
 D = 2° 33' 52.98"
 R = 2,234.00
 L = 220.20
 E = 2.72
 T = 110.19
 P.C. STA = 426+24.42
 N = 1,839,198.31
 E = 1,178,441.42
 P.T. STA = 428+44.62
 N = 1,839,087.55
 E = 1,178,631.63

SEE ALIGNMENT SHEET 4 OF 4 FOR ADDITIONAL POINT AND CURVE DATA

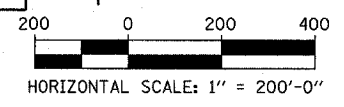
REVISIONS	
NAME	DATE

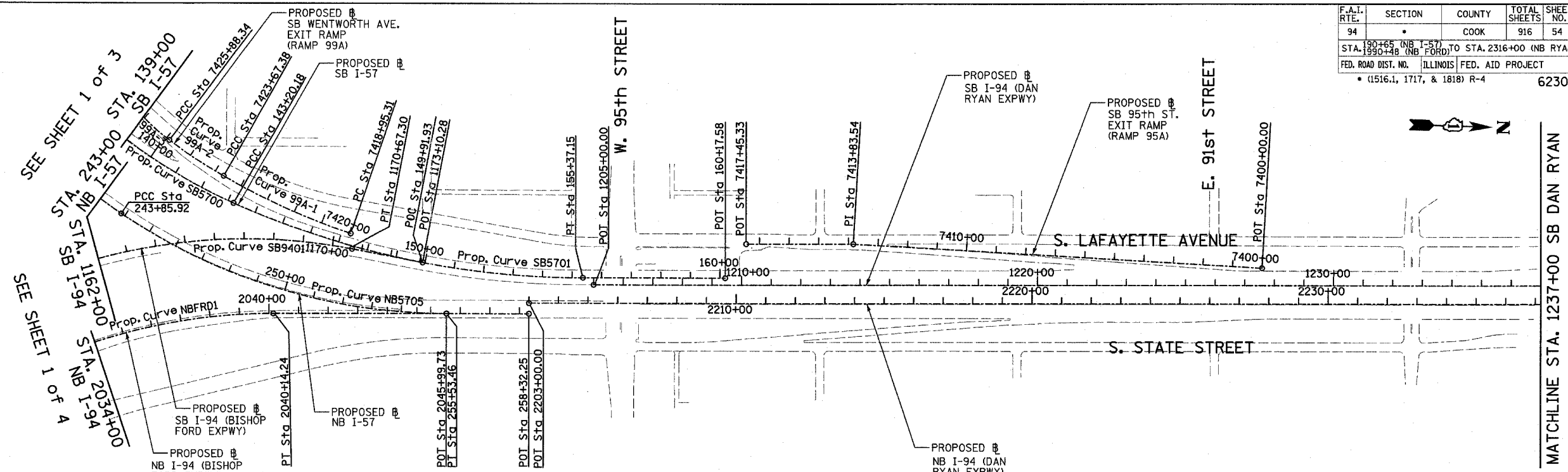
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

ALIGNMENT SHEET 1 OF 4

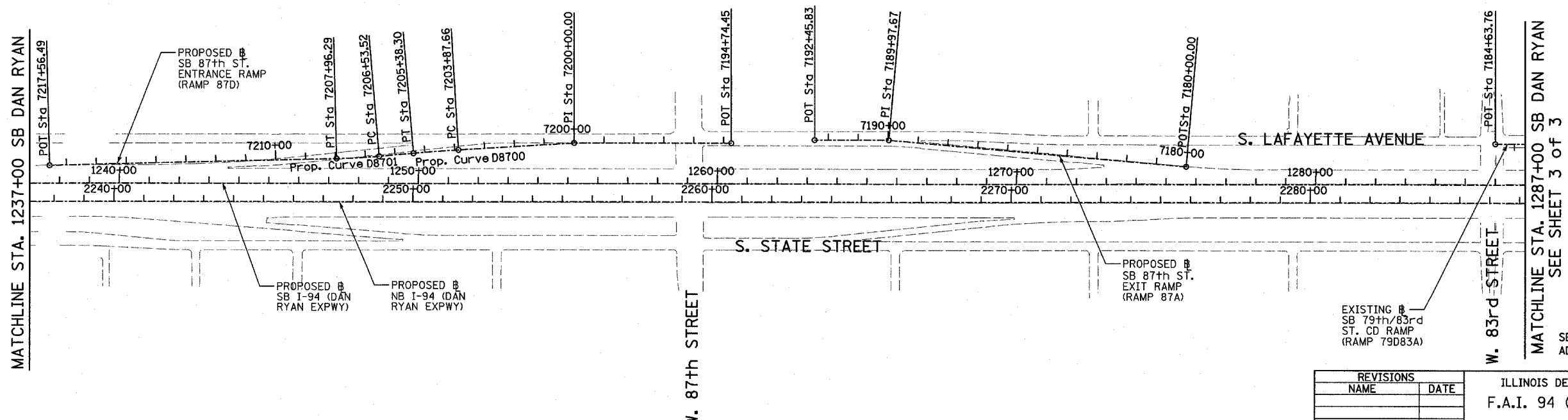
SCALE: 1"=200'-0"
 DATE: MARCH 7, 2006

DRAWN BY: JPM
 CHECKED BY: DAK

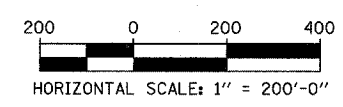




Prop. Curve SB5700 P.I. STA = 133+70.06 N = 1,839,521.70 E = 1,177,054.03 $\Delta = 66^\circ 11' 55.15''$ (LT) $D = 3^\circ 02' 10.83''$ R = 1,887.00 L = 2,180.21 E = 365.53 T = 1,230.09 P.C. STA = 121+39.97 N = 1,839,491.52 E = 1,175,824.31 P.T. STA = 143+20.18 N = 1,840,659.02 E = 1,177,522.68	Prop. Curve SB5701 P.I. STA = 149+37.75 N = 1,841,230.00 E = 1,177,757.97 $\Delta = 24^\circ 02' 07.49''$ (LT) $D = 1^\circ 58' 30.13''$ R = 2,901.00 L = 1,216.96 E = 65.01 T = 617.56 P.C. STA = 143+20.18 N = 1,840,659.02 E = 1,177,522.68 P.T. STA = 155+37.15 N = 1,841,847.31 E = 1,177,740.29	Prop. Curve NB5705 P.I. STA = 249+87.51 N = 1,840,788.31 E = 1,177,892.02 $\Delta = 34^\circ 03' 37.98''$ (LT) $D = 2^\circ 55' 02.28''$ R = 1,964.00 L = 1,167.54 E = 90.07 T = 601.59 P.C. STA = 243+85.92 N = 1,840,280.48 E = 1,177,569.49 P.T. STA = 255+53.46 N = 1,841,389.65 E = 1,177,874.80	Prop. Curve SB9401 P.I. STA = 1160+15.97 N = 1,838,783.95 E = 1,177,299.83 $\Delta = 100^\circ 48' 58.07''$ (RT) $D = 2^\circ 59' 59.20''$ R = 1,910.00 L = 3,360.79 E = 1,086.95 T = 2,309.46 P.C. STA = 1137+06.52 N = 1,838,851.99 E = 1,179,608.28 P.T. STA = 1170+67.30 N = 1,841,064.16 E = 1,177,666.20	SB I-94 (DAN RYAN EXPWY) POT STA = 1205+00.00 N = 1,841,884.04 E = 1,177,763.25	SB I-57 POT STA = 160+17.58 N = 1,842,327.55 E = 1,177,726.54	SB I-94 (BISHOP FORD EXPWY) POT STA = 1173+10.28 N = 1,841,304.06 E = 1,177,704.74
	NB I-57 POT STA = 258+32.25 N = 1,841,668.33 E = 1,177,866.82	NB I-94 (BISHOP FORD EXPWY) POT STA = 2045+99.73 N = 1,841,389.65 E = 1,177,874.80	NB I-94 (DAN RYAN EXPWY) POT STA = 2203+00.00 N = 1,841,667.31 E = 1,177,830.83			



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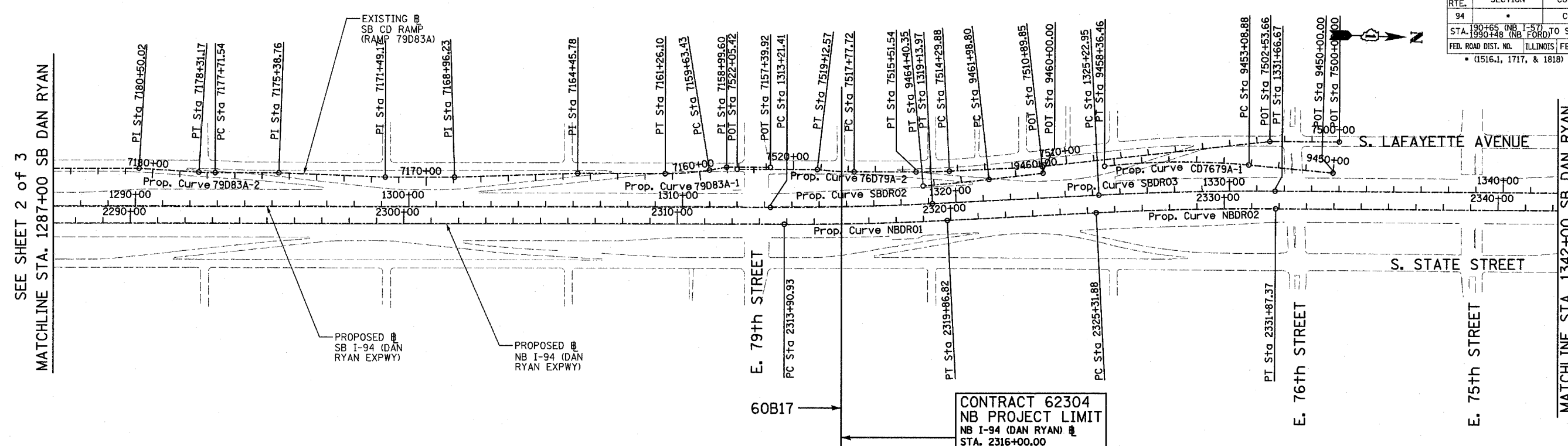


REVISIONS	
NAME	DATE

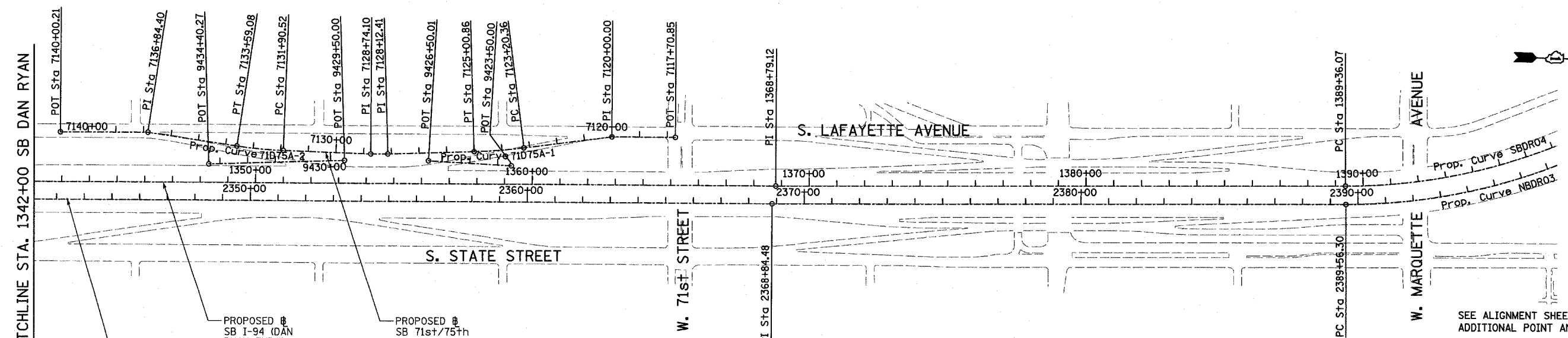
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 ALIGNMENT
 SHEET 2 OF 4

SCALE: 1"=200'
 DATE: MARCH 7, 2006

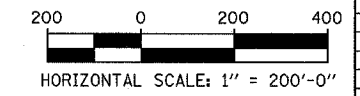
DRAWN BY: JPM
 CHECKED BY: DAK



Prop. Curve NBDR01 P.I. STA = 2316+88.94 N = 1,853,051.58 E = 1,177,504.84 $\Delta = 2^\circ 58' 38.60''$ (LT) $D = 0^\circ 29' 58.77''$ R = 11,467.00 L = 595.89 E = 3.87 T = 298.01 P.C. STA = 2313+90.93 N = 1,852,753.69 E = 1,177,513.37 P.T. STA = 2319+86.82 N = 1,853,348.62 E = 1,177,480.85	Prop. Curve NBDR02 P.I. STA = 2328+59.71 N = 1,854,218.69 E = 1,177,410.58 $\Delta = 3^\circ 16' 48.34''$ (RT) $D = 0^\circ 30' 01.44''$ R = 11,450.00 L = 655.49 E = 4.69 T = 327.84 P.C. STA = 2325+31.88 N = 1,853,891.91 E = 1,177,436.97 P.T. STA = 2331+87.37 N = 1,854,546.43 E = 1,177,402.92	Prop. Curve NBDR03 P.I. STA = 2398+13.58 N = 1,861,170.38 E = 1,177,231.19 $\Delta = 44^\circ 36' 17.12''$ (LT) $D = 2^\circ 44' 29.13''$ R = 2,090.00 L = 1,627.06 E = 168.99 T = 857.27 P.C. STA = 2389+56.30 N = 1,860,313.48 E = 1,177,256.19 P.T. STA = 2405+83.37 N = 1,861,762.92 E = 1,176,611.65	Prop. Curve SBDR02 P.I. STA = 1316+17.76 N = 1,852,997.25 E = 1,177,445.02 $\Delta = 2^\circ 58' 38.60''$ (LT) $D = 0^\circ 30' 08.86''$ R = 11,403.00 L = 592.56 E = 3.85 T = 296.35 P.C. STA = 1313+21.41 N = 1,852,701.02 E = 1,177,453.50 P.T. STA = 1319+13.97 N = 1,853,292.63 E = 1,177,421.16	Prop. Curve SBDR03 P.I. STA = 1328+44.90 N = 1,854,220.54 E = 1,177,346.22 $\Delta = 3^\circ 16' 11.04''$ (RT) $D = 0^\circ 30' 28.59''$ R = 11,280.00 L = 643.72 E = 4.59 T = 321.95 P.C. STA = 1325+22.95 N = 1,853,899.63 E = 1,177,372.14 P.T. STA = 1331+66.67 N = 1,854,542.40 E = 1,177,338.64	Prop. Curve SBDR04 P.I. STA = 1397+75.78 N = 1,861,149.16 E = 1,177,163.97 $\Delta = 44^\circ 32' 58.56''$ (LT) $D = 2^\circ 47' 41.70''$ R = 2,050.00 L = 1,593.95 E = 165.31 T = 839.71 P.C. STA = 1389+36.07 N = 1,860,309.83 E = 1,177,189.29 P.T. STA = 1405+30.02 N = 1,861,729.54 E = 1,176,557.11
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
ALIGNMENT
 SHEET 3 OF 4

SCALE: 1"=200'
 DATE: MARCH 7, 2006
 DRAWN BY: JPM
 CHECKED BY: DAK

SEE ALIGNMENT SHEET 4 OF 4 FOR ADDITIONAL POINT AND CURVE DATA

SB HALSTED ST.
EXIT RAMP
(RAMP HLSTDA)
POT STA = 7440+00.00
N = 1,839,490.94
E = 1,174,333.70
P.I. STA = 7449+69.78
N = 1,839,551.73
E = 1,173,365.83
POT STA = 7452+53.49
N = 1,839,544.76
E = 1,173,082.20

SB WENTWORTH AVE.
EXIT RAMP
(RAMP 99A)
POT STA = 7430+55.42
N = 1,840,141.17
E = 1,176,961.02
POT STA = 7431+32.26
N = 1,840,094.48
E = 1,176,899.99
POT STA = 7434+00.00
N = 1,839,955.89
E = 1,176,670.92

SB 95th ST. EXIT RAMP
(RAMP 95A)
POT STA = 7400+00.00
N = 1,844,139.73
E = 1,177,638.63
P.I. STA = 7413+83.54
N = 1,842,756.77
E = 1,177,598.60
POT STA = 7417+45.33
N = 1,842,395.13
E = 1,177,608.95

SB 87th ST.
ENTRANCE RAMP
(RAMP 87D)
POT STA = 7194+74.45
N = 1,847,424.06
E = 1,177,468.58
P.I. STA = 7200+00.00
N = 1,846,898.74
E = 1,177,484.09
POT STA = 7217+56.49
N = 1,845,147.17
E = 1,177,609.78

SB 87th ST. EXIT RAMP
(RAMP 87A)
POT STA = 7180+00.00
N = 1,848,946.30
E = 1,177,501.00
P.I. STA = 7189+97.67
N = 1,847,950.35
E = 1,177,442.53
POT STA = 7192+45.83
N = 1,847,702.29
E = 1,177,449.63

Prop. Curve 99A-1
P.I. STA = 7421+32.05
N = 1,840,832.05
E = 1,177,544.75
 $\Delta = 10^\circ 49' 08.53''$ (RT)
D = $2^\circ 17' 30.59''$
R = 2,500.00
L = 472.07
E = 11.18
T = 236.74
P.C. STA = 7418+95.31
N = 1,841,058.12
E = 1,177,615.03
P.T. STA = 7423+67.38
N = 1,840,623.20
E = 1,177,433.29

Prop. Curve 99A-2
P.I. STA = 7424+78.02
N = 1,840,525.59
E = 1,177,381.20
 $\Delta = 7^\circ 26' 49.68''$ (RT)
D = $3^\circ 22' 13.22''$
R = 1,700.00
L = 220.96
E = 3.60
T = 110.64
P.C. STA = 7423+67.38
N = 1,840,623.20
E = 1,177,433.29
P.T. STA = 7425+88.34
N = 1,840,435.56
E = 1,177,316.90

Prop. Curve 99A-3
P.I. STA = 7428+26.42
N = 1,840,241.82
E = 1,177,178.53
 $\Delta = 29^\circ 38' 02.27''$ (RT)
D = $6^\circ 21' 58.31''$
R = 900.00
L = 465.49
E = 30.96
T = 238.08
P.C. STA = 7425+88.34
N = 1,840,435.56
E = 1,177,316.90
P.T. STA = 7430+53.83
N = 1,840,141.84
E = 1,176,962.46

Prop. Curve D8700
P.I. STA = 7204+62.99
N = 1,846,437.54
E = 1,177,524.69
 $\Delta = 1^\circ 30' 03.72''$ (LT)
D = $0^\circ 59' 47.21''$
R = 5,750.00
L = 150.64
E = 0.49
T = 75.32
P.C. STA = 7203+87.66
N = 1,846,512.58
E = 1,177,518.09
P.T. STA = 7205+38.30
N = 1,846,362.71
E = 1,177,533.26

Prop. Curve D8701
P.I. STA = 7207+24.93
N = 1,846,177.29
E = 1,177,554.49
 $\Delta = 3^\circ 27' 36.43''$ (RT)
D = $2^\circ 25' 25.25''$
R = 2,364.00
L = 142.76
E = 1.08
T = 71.40
P.C. STA = 7206+53.52
N = 1,846,248.23
E = 1,177,546.37
P.T. STA = 7207+96.29
N = 1,846,105.99
E = 1,177,558.32

Prop. Curve 79D83A-1
P.I. STA = 7160+44.94
N = 1,852,394.55
E = 1,177,340.32
 $\Delta = 9^\circ 19' 12.90''$ (RT)
D = $5^\circ 43' 46.48''$
R = 1,000.00
L = 162.67
E = 3.32
T = 81.51
P.C. STA = 7159+63.43
N = 1,852,474.58
E = 1,177,324.82
P.T. STA = 7161+26.10
N = 1,852,313.07
E = 1,177,342.65

SB 76th/79th ST.
CD RAMP
(RAMP 76D79A)
POT STA = 7500+00.00
N = 1,854,771.42
E = 1,177,153.21
P.I. STA = 7502+53.66
N = 1,854,517.84
E = 1,177,159.17
P.I. STA = 7510+89.85
N = 1,853,689.73
E = 1,177,275.17
POT STA = 7522+05.42
N = 1,852,575.71
E = 1,177,319.03

SB 71st/75th ST.
CD RAMP
(RAMP 71D75A)
POT STA = 7117+70.85
N = 1,857,886.40
E = 1,177,086.26
P.I. STA = 7120+00.00
N = 1,857,657.31
E = 1,177,091.65
P.I. STA = 7128+12.41
N = 1,856,850.29
E = 1,177,177.64
P.I. STA = 7128+74.10
N = 1,856,788.62
E = 1,177,179.10
P.I. STA = 7136+84.40
N = 1,855,981.27
E = 1,177,124.75
POT = 7140+00.21
N = 1,855,665.56
E = 1,177,132.18

Prop. Curve 79D83A-2
P.I. STA = 7178+01.36
N = 1,850,638.94
E = 1,177,390.59
 $\Delta = 3^\circ 24' 57.69''$ (RT)
D = $5^\circ 43' 46.48''$
R = 1,000.00
L = 59.62
E = 0.44
T = 29.82
P.C. STA = 7177+71.54
N = 1,850,668.74
E = 1,177,389.73
P.T. STA = 7178+31.17
N = 1,850,609.13
E = 1,177,389.66

Prop. Curve 76D79A-1
P.I. STA = 7514+90.72
N = 1,853,290.06
E = 1,177,306.25
 $\Delta = 2^\circ 48' 22.72''$ (RT)
D = $2^\circ 18' 23.74''$
R = 2,484.00
L = 121.66
E = 0.75
T = 60.84
P.C. STA = 7514+29.88
N = 1,853,350.72
E = 1,177,301.54
P.T. STA = 7515+51.54
N = 1,853,229.24
E = 1,177,307.99

Prop. Curve 71D75A-1
P.I. STA = 7124+10.68
N = 1,857,251.52
E = 1,177,154.81
 $\Delta = 5^\circ 35' 23.99''$ (RT)
D = $3^\circ 05' 49.45''$
R = 1,850.00
L = 180.49
E = 2.20
T = 90.32
P.C. STA = 7123+20.36
N = 1,857,340.76
E = 1,177,140.92
P.T. STA = 7125+00.86
N = 1,857,161.34
E = 1,177,159.94

SB C-D RAMP
(RAMP 79D83A)
POT STA = 7157+39.92
N = 1,852,696.86
E = 1,177,308.11
P.I. STA = 7158+99.60
N = 1,852,537.24
E = 1,177,312.68
P.I. STA = 7164+45.78
N = 1,851,993.52
E = 1,177,351.80
P.I. STA = 7168+96.23
N = 1,851,543.85
E = 1,177,378.34
P.I. STA = 7171+49.17
N = 1,851,291.02
E = 1,177,385.58
P.I. STA = 7175+38.76
N = 1,850,901.43
E = 1,177,383.07
P.I. STA = 7180+50.02
N = 1,850,390.38
E = 1,177,382.88
POT STA = 7184+63.76
N = 1,849,976.81
E = 1,177,394.72

Prop. Curve 76D79A-2
P.I. STA = 7518+45.22
N = 1,852,935.69
E = 1,177,316.40
 $\Delta = 6^\circ 31' 33.29''$ (RT)
D = $4^\circ 50' 21.01''$
R = 1,184.00
L = 134.86
E = 1.92
T = 67.50
P.C. STA = 7517+77.72
N = 1,853,003.16
E = 1,177,314.47
P.T. STA = 7519+12.57
N = 1,852,868.44
E = 1,177,310.65

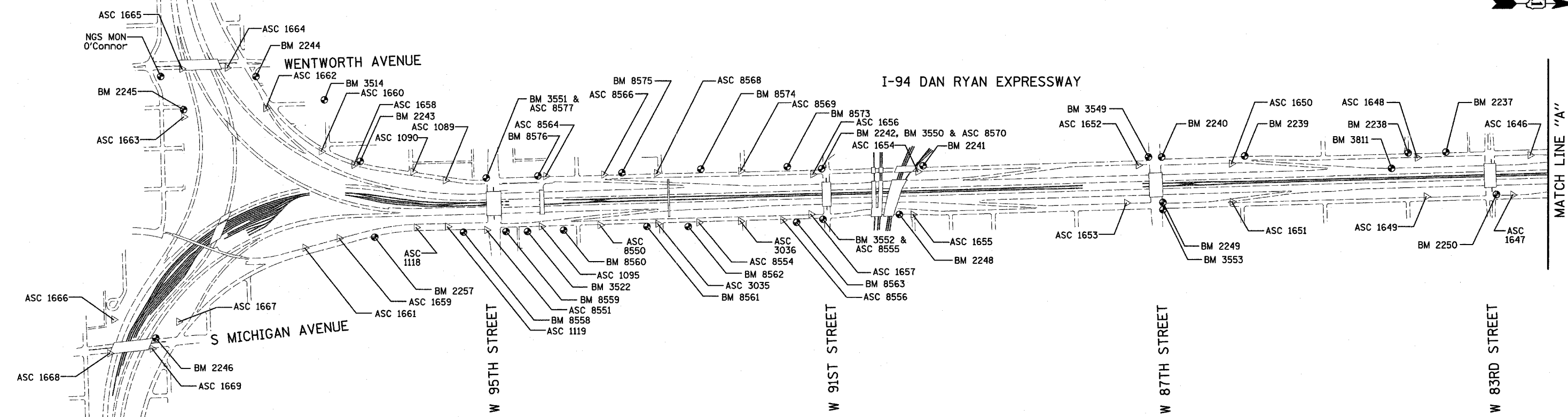
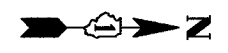
SB 76th/79th ST.
CD ENTRANCE CONNECTOR
(CD7679A)
POT STA = 9450+00.00
N = 1,854,751.70
E = 1,177,267.37
Prop. Curve CD7679A-1
P.I. STA = 9455+73.58
N = 1,854,179.28
E = 1,177,230.83
 $\Delta = 11^\circ 37' 34.33''$ (LT)
D = $2^\circ 12' 13.26''$
R = 2,600.00
L = 527.58
E = 13.44
T = 264.70
P.C. STA = 9453+08.88
N = 1,854,443.44
E = 1,177,247.69
P.T. STA = 9458+36.46
N = 1,853,917.14
E = 1,177,267.55

SB 76th/79th ST.
CD ENTRANCE CONNECTOR
(CD7679B)
POT STA = 9460+00.00
N = 1,853,693.06
E = 1,177,298.94
Prop. Curve CD7679B-1
P.I. STA = 9463+19.58
N = 1,853,376.57
E = 1,177,343.27
 $\Delta = 1^\circ 13' 13.54''$ (RT)
D = $0^\circ 30' 18.91''$
R = 11,340.00
L = 241.55
E = 0.64
T = 120.78
P.C. STA = 9461+98.80
N = 1,853,496.18
E = 1,177,326.52
P.T. STA = 9464+40.35
N = 1,853,256.63
E = 1,177,357.48

SB 71st/75th ST.
CD ENTRANCE CONNECTOR
(RAMP CD7175A)
POT STA = 9423+50.00
N = 1,857,297.2866
E = 1,177,207.4713
POT STA = 9426+50.01
N = 1,856,997.4225
E = 1,177,198.1891
CD ENTRANCE CONNECTOR
(RAMP CD7175B)
POT STA = 9429+50.00
N = 1,856,694.2083
E = 1,177,205.3235
POT STA = 9434+40.27
N = 1,856,204.7281
E = 1,177,233.1786

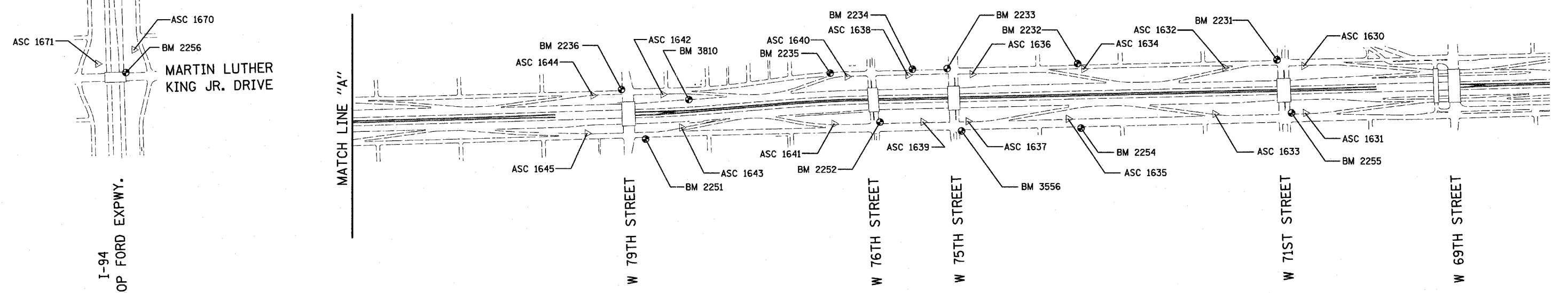
REVISIONS	
NAME	DATE

I-57 EXPWY
SEE SHEET 2 OF 7
MATCH LINE "B"



MATCH LINE "A"

I-94 DAN RYAN EXPRESSWAY



MATCH LINE "A"

I-94 BISHOP FORD EXPWY.

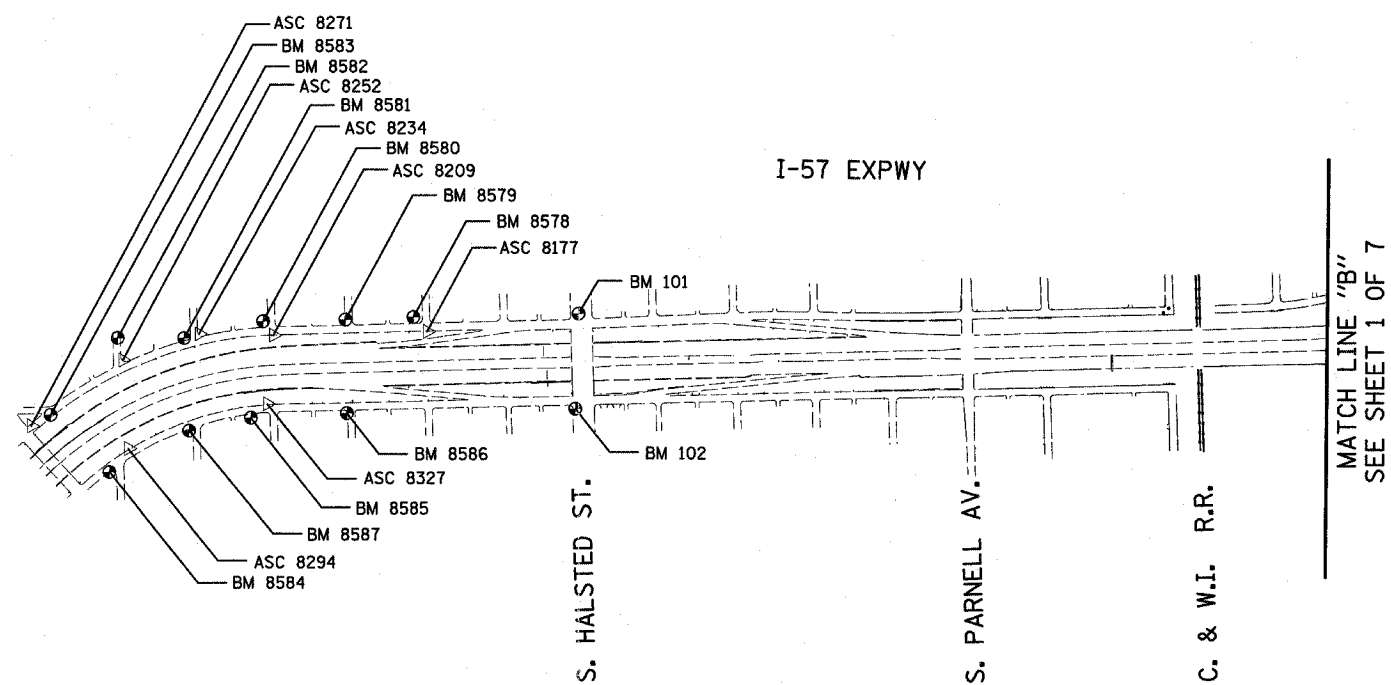
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
SURVEY TIES FOR CONTROL POINTS
SHEET 1 OF 7

SCALE: 1"=400'
DATE: MARCH 7, 2006
DRAWN BY: GSP
CHECKED BY: MMW

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	58
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				

62304

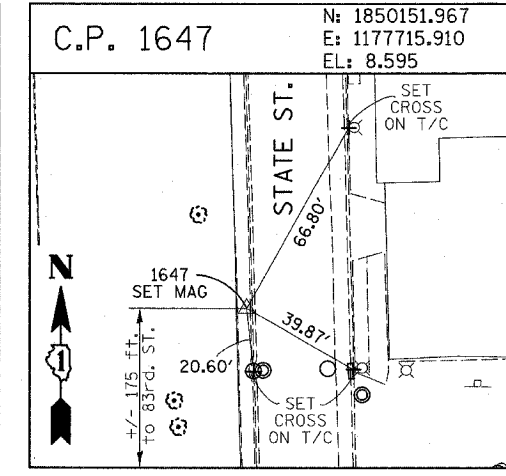
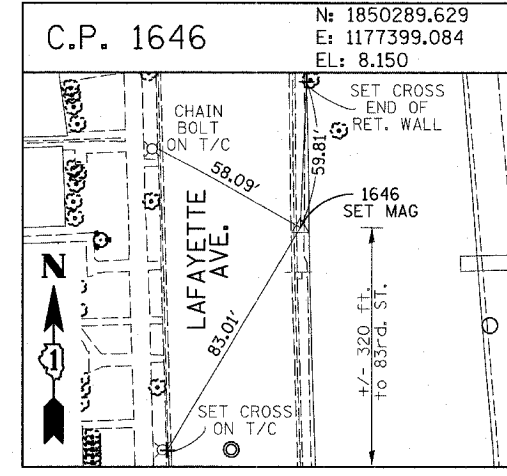
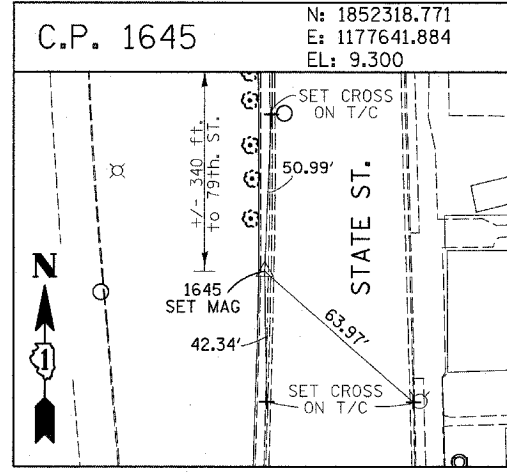
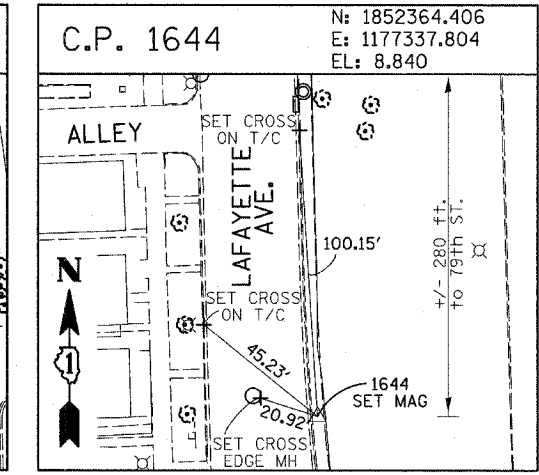
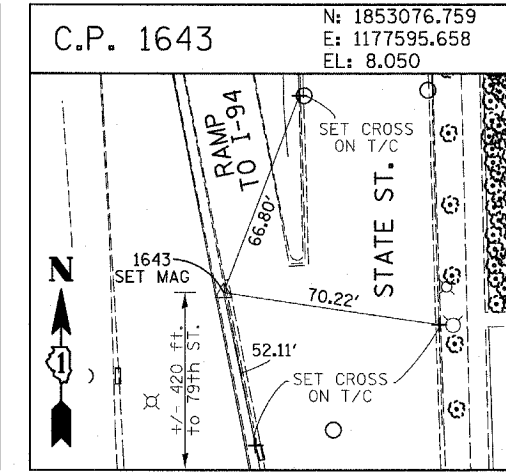
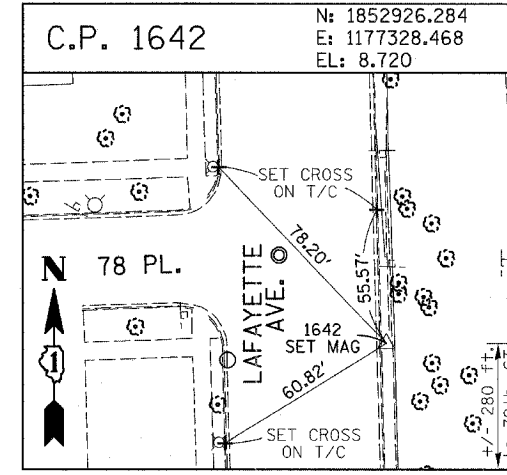
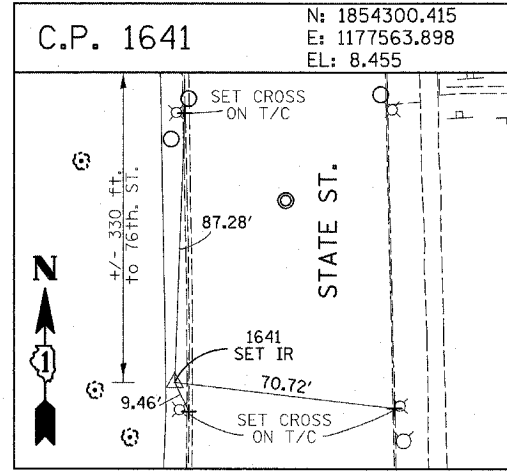
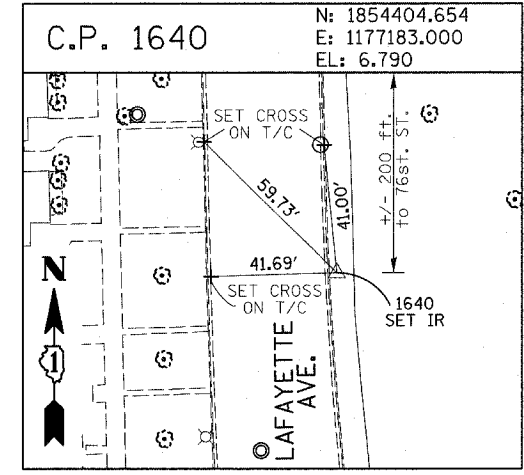
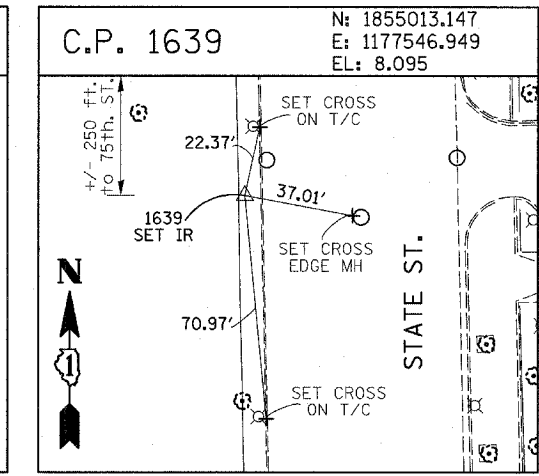
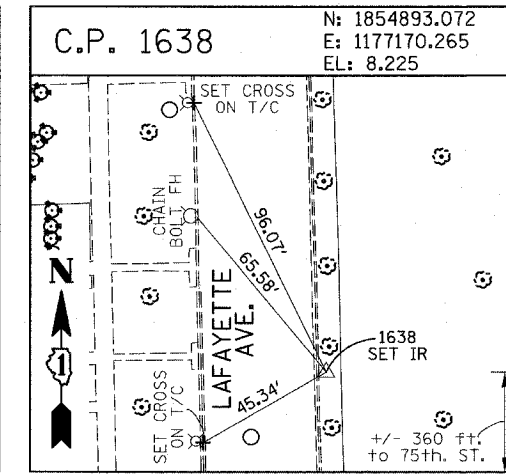
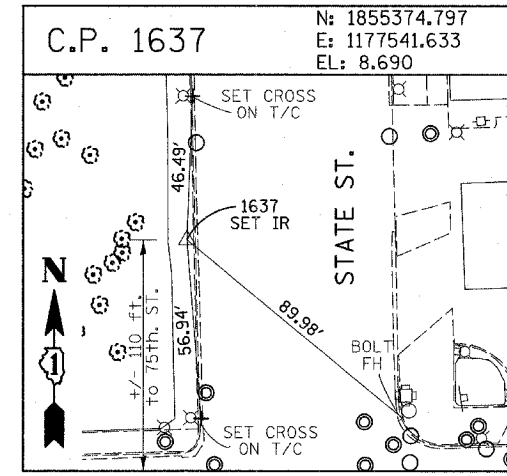
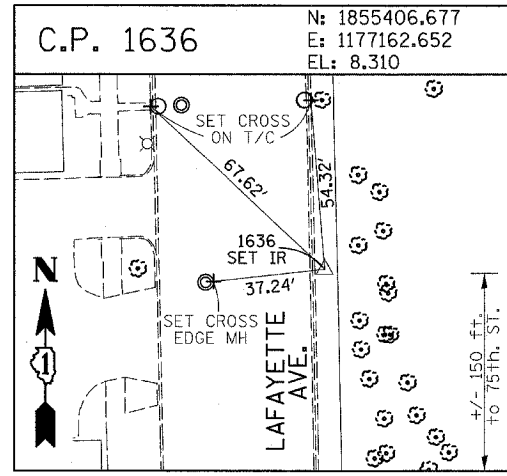
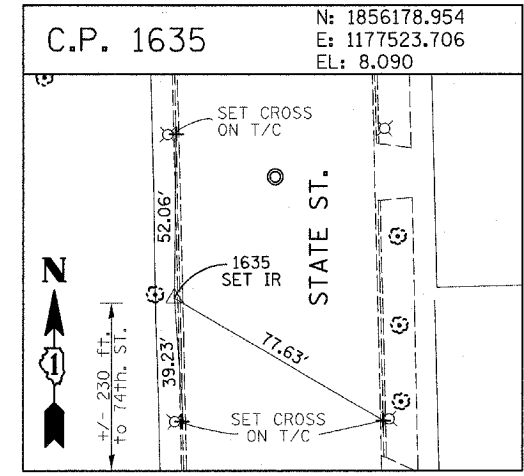
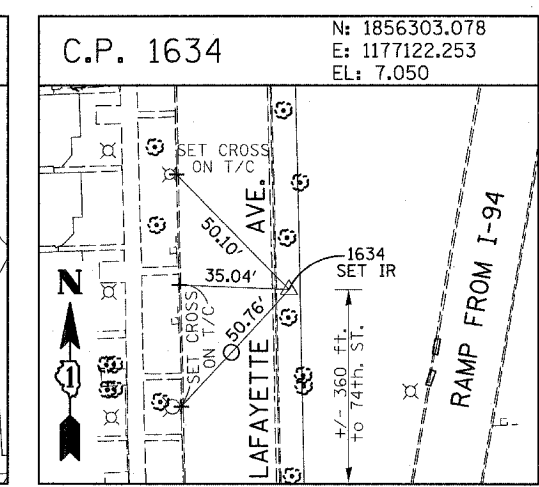
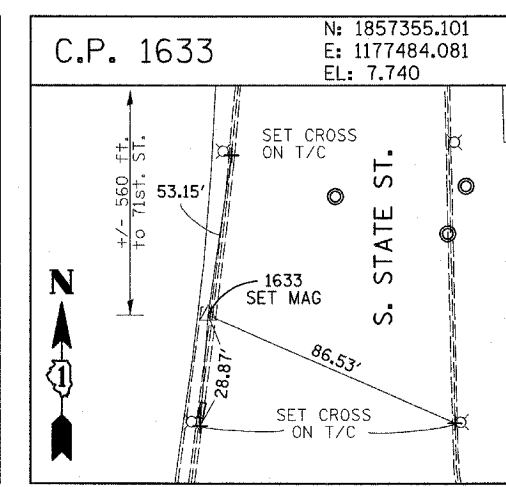
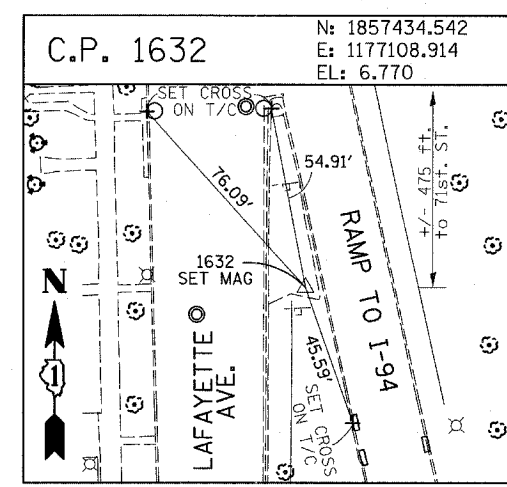
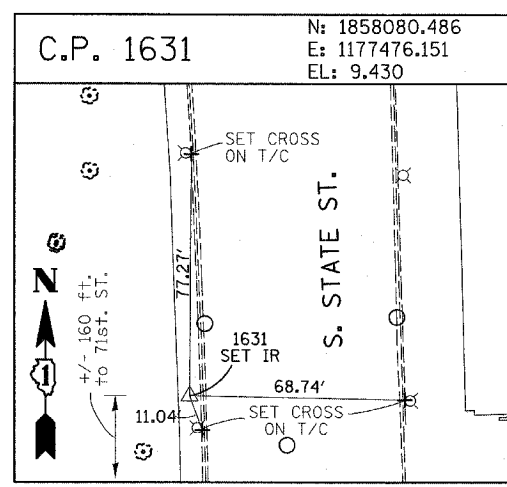
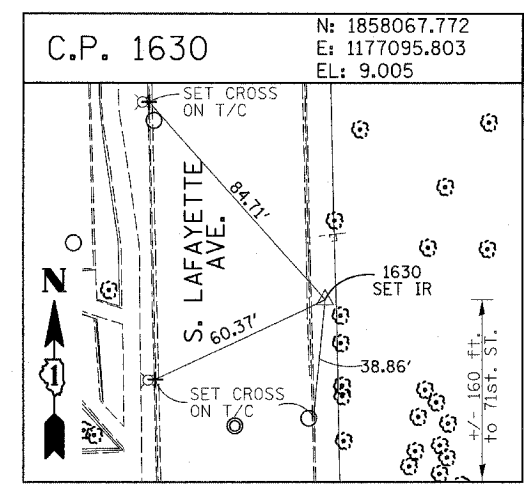


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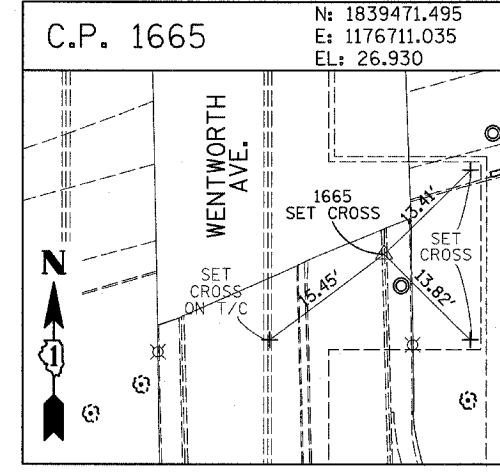
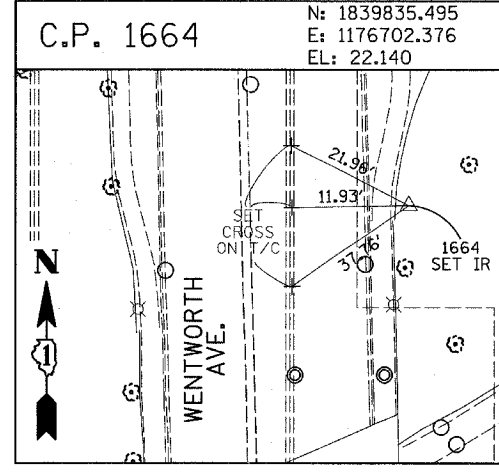
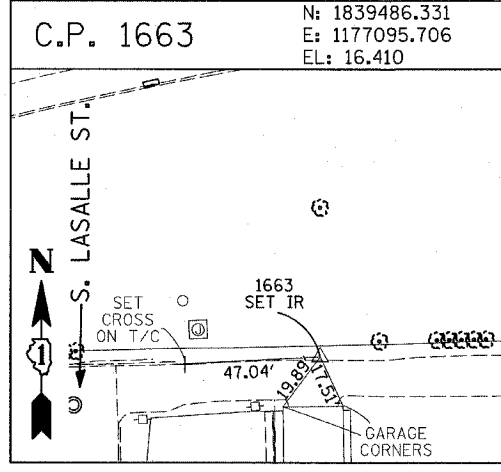
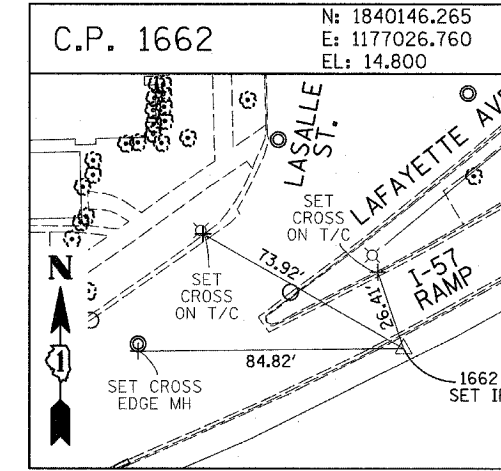
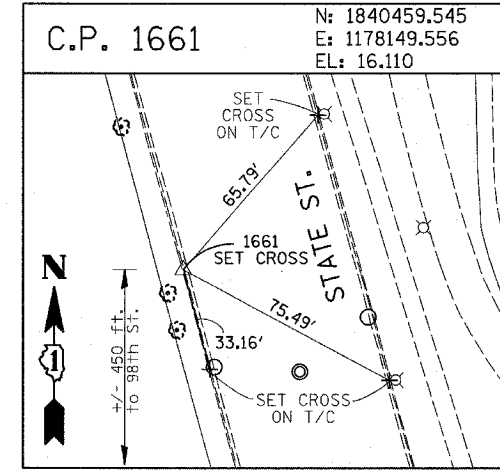
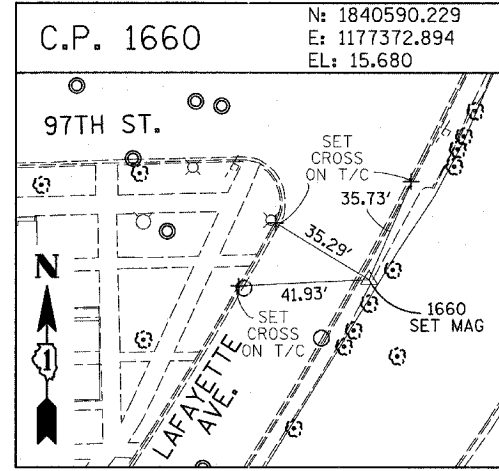
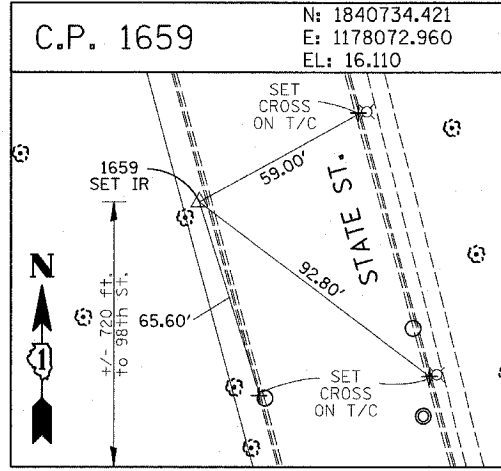
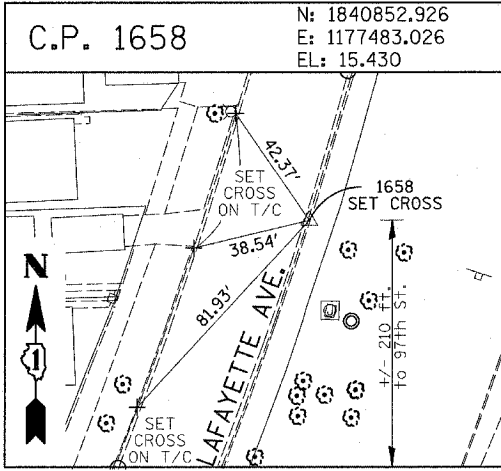
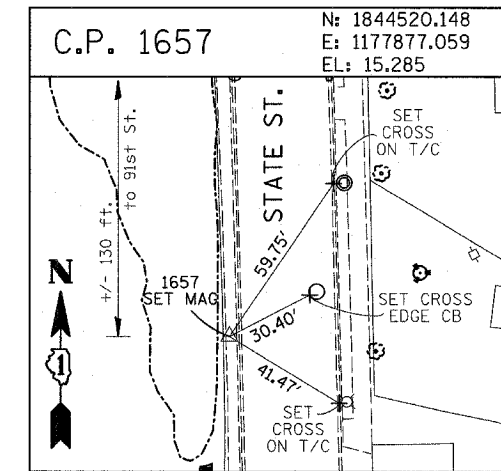
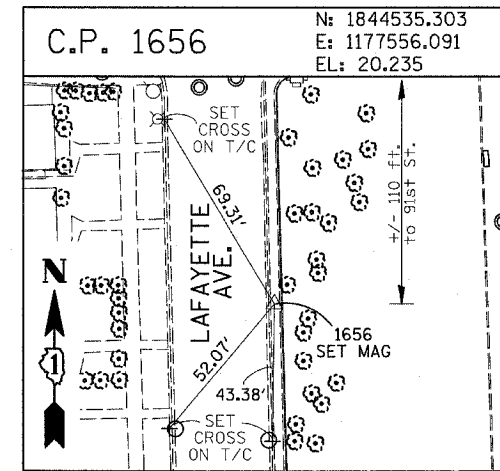
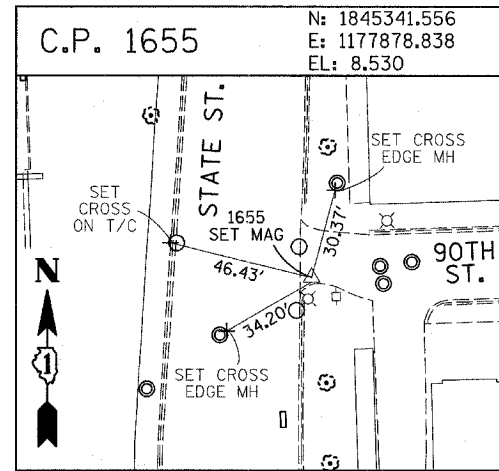
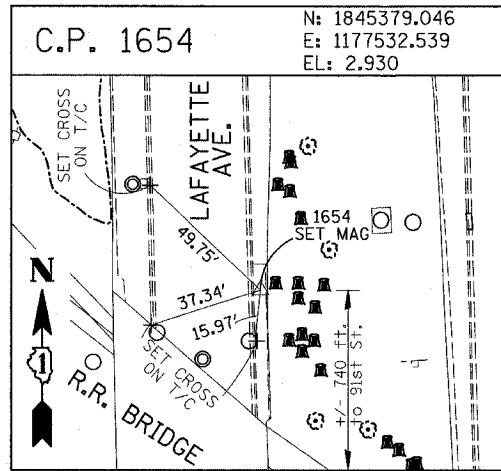
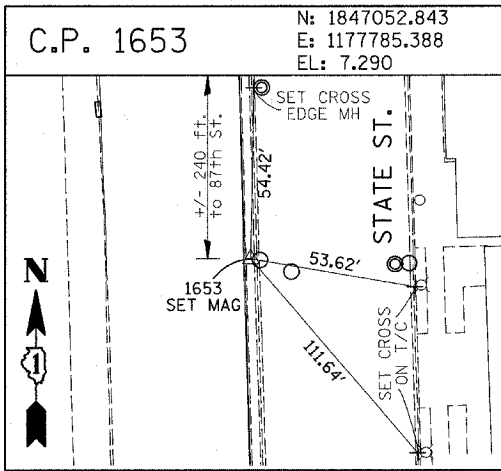
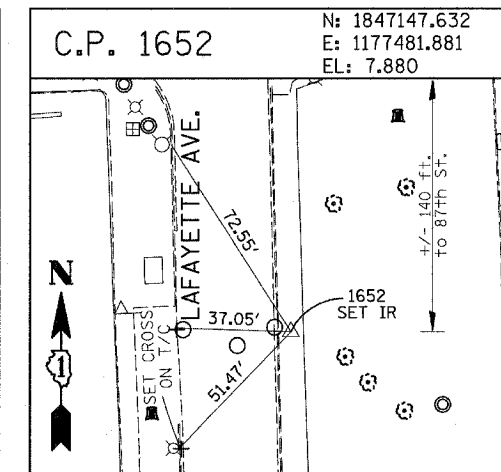
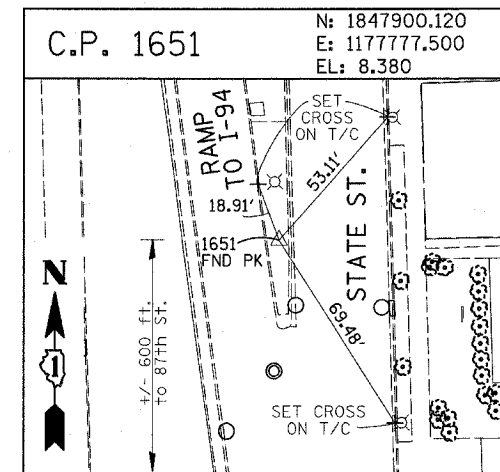
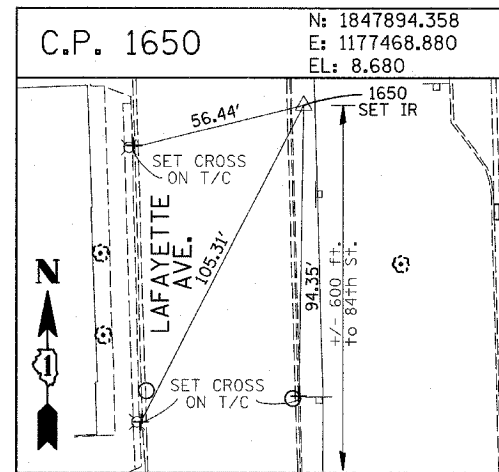
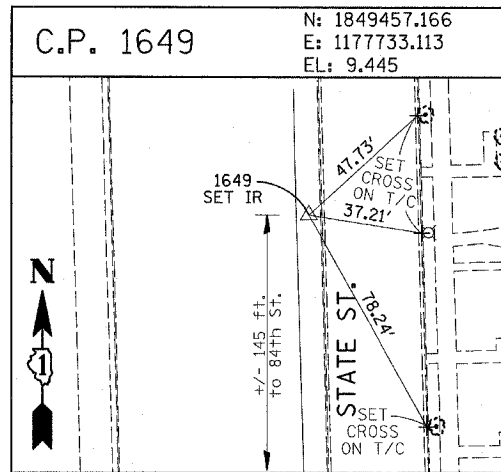
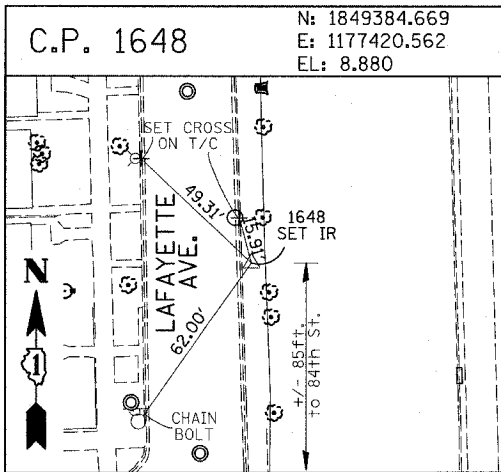
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

SURVEY TIES FOR CONTROL POINTS
 SHEET 2 OF 7

SCALE: 1"=400'
 DATE: MARCH 7, 2006
 DRAWN BY: GSP
 CHECKED BY: MMW

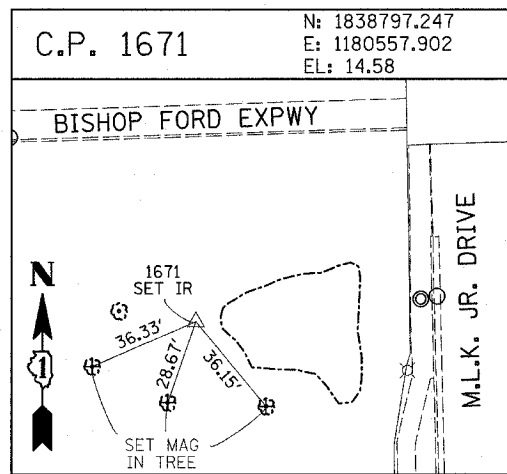
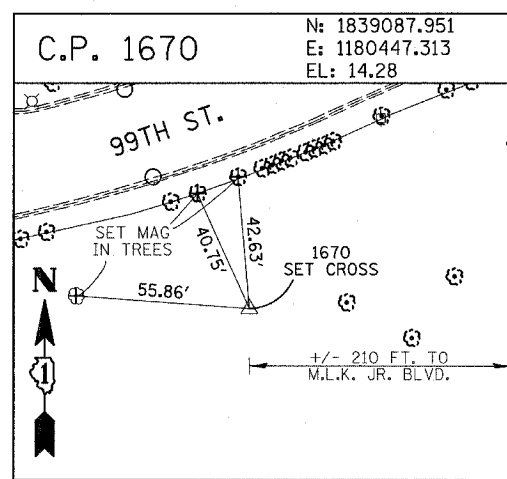
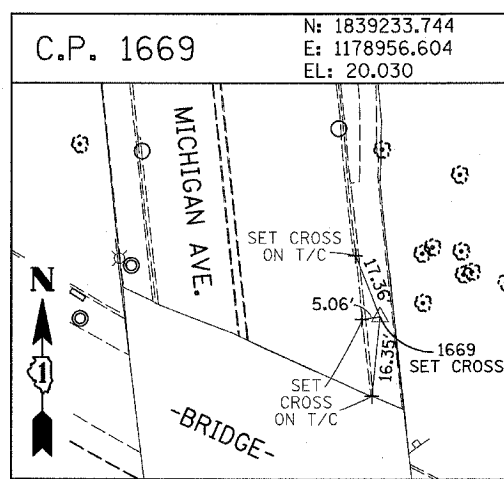
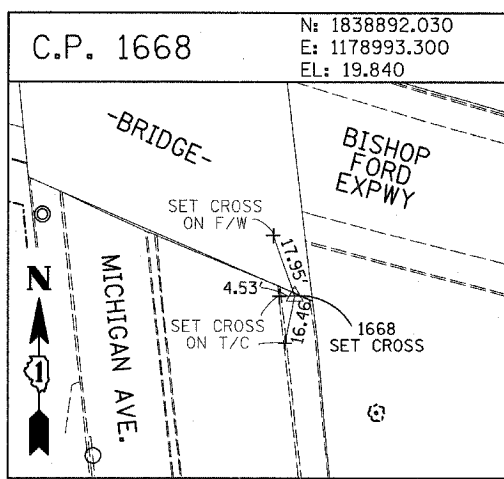
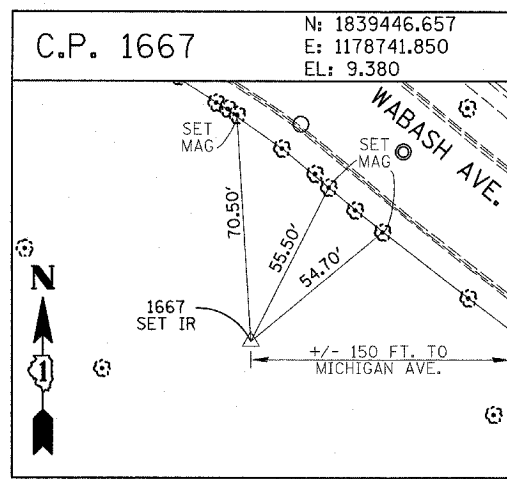
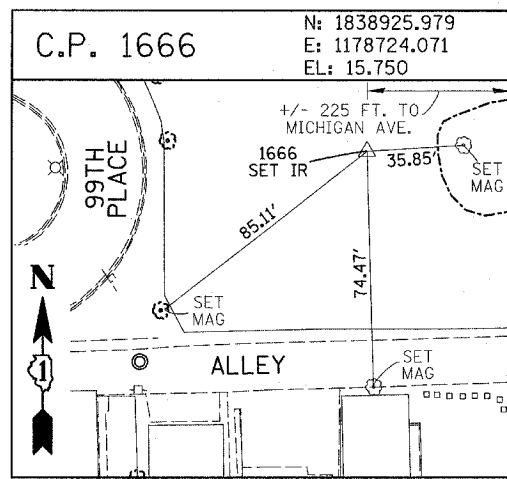


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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				

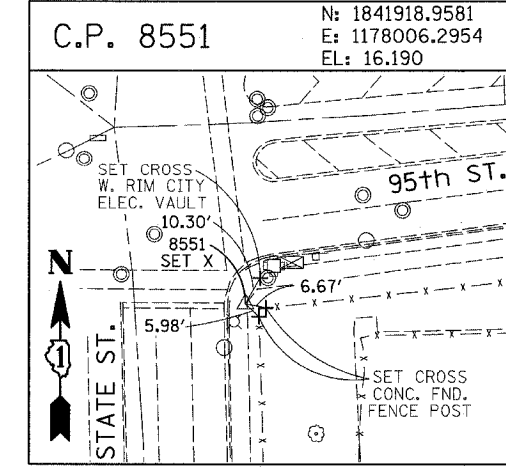
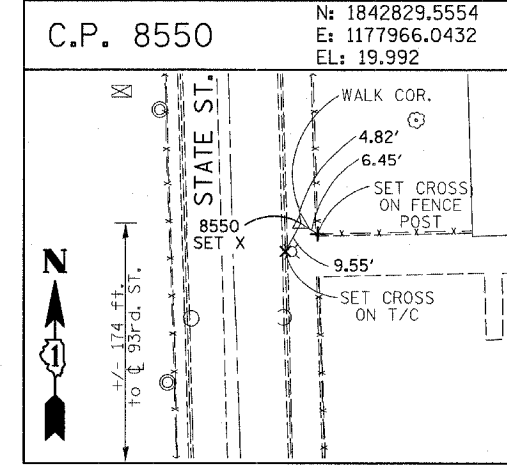
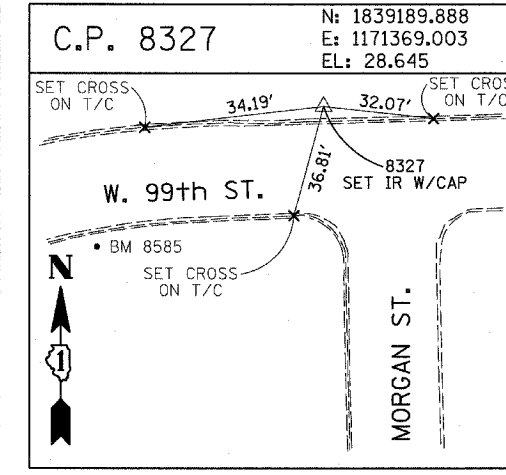
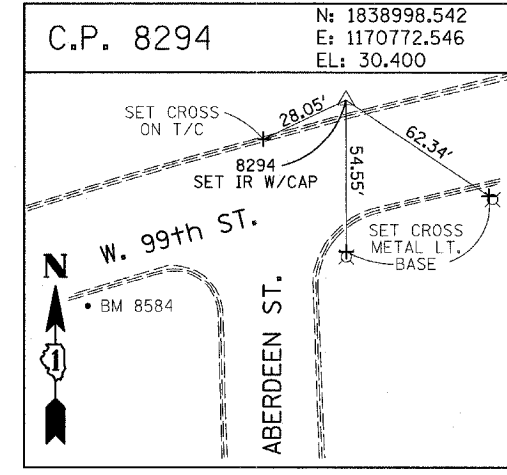
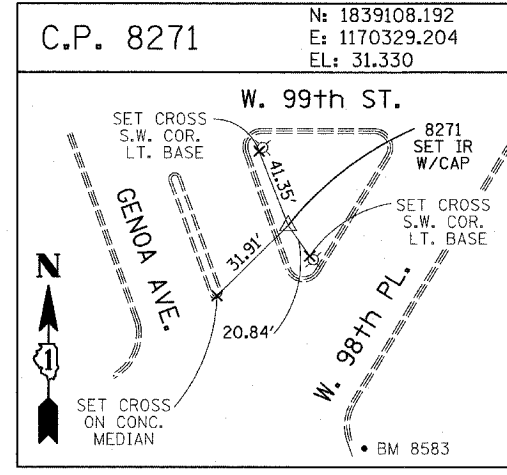
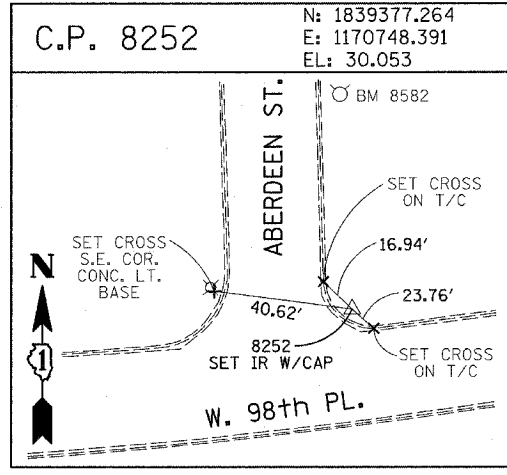
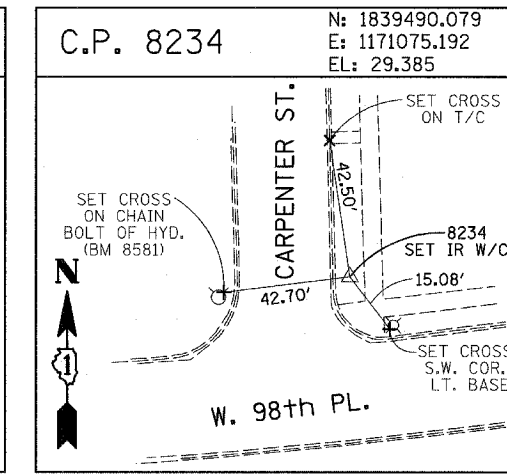
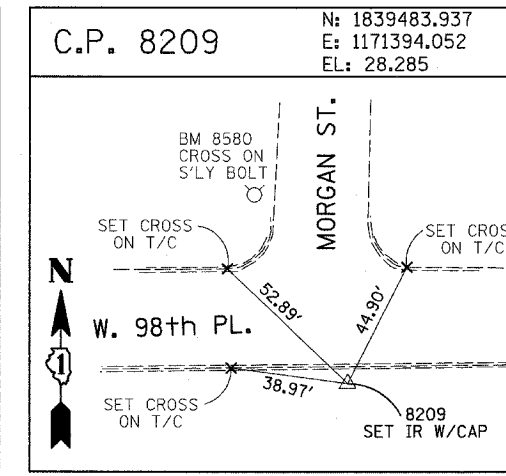
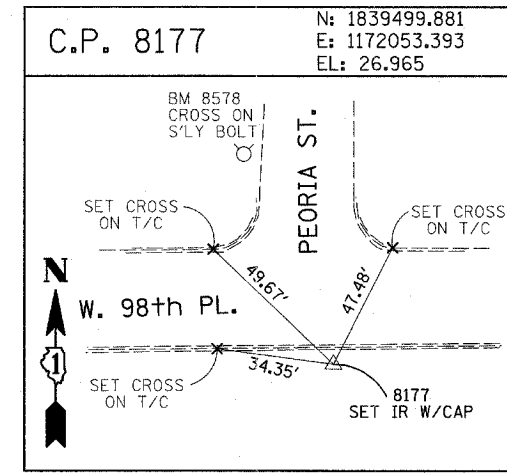
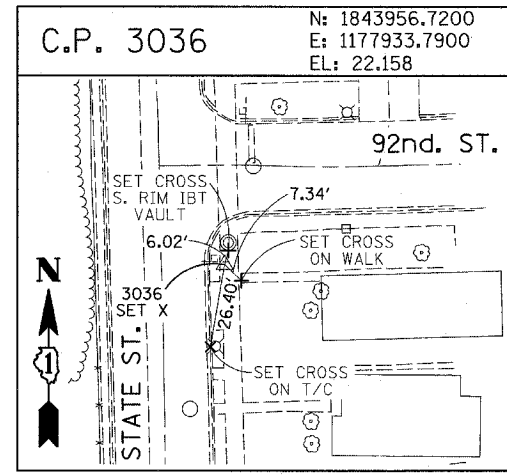
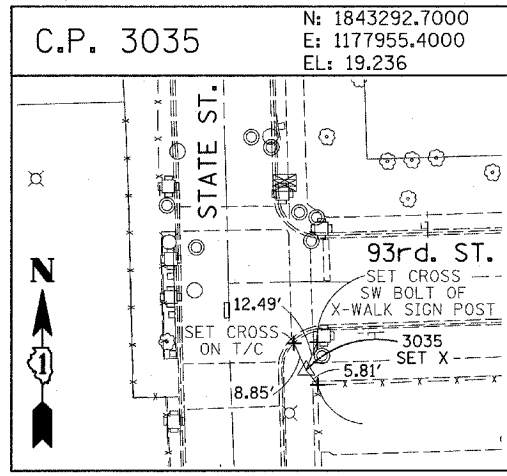
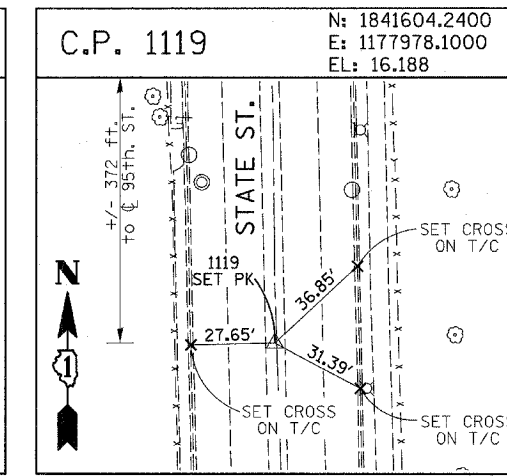
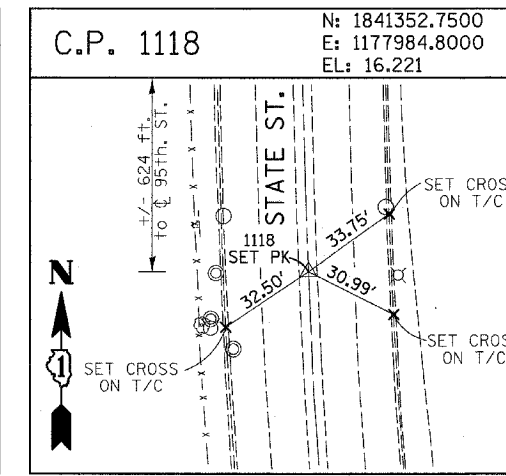
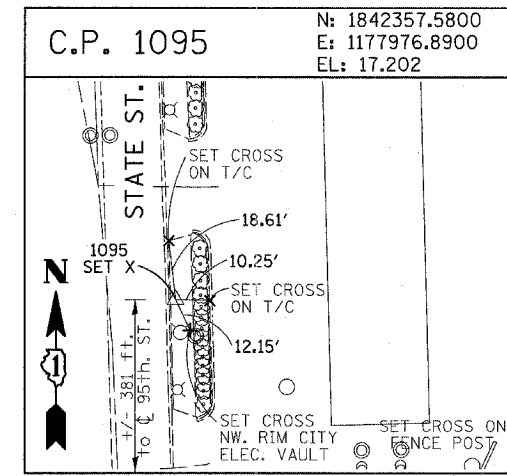
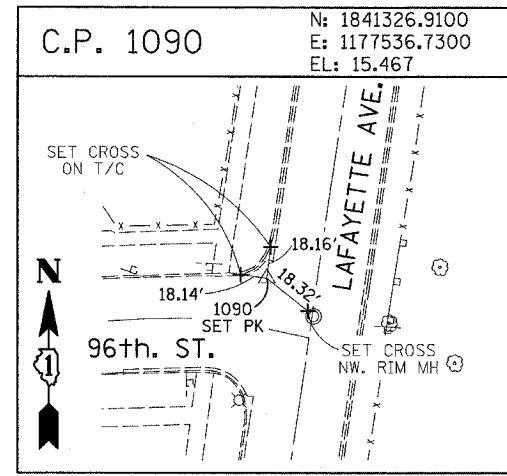
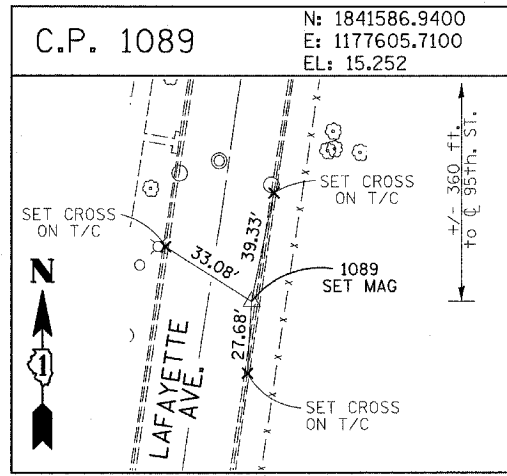


PROJECT BENCHMARKS

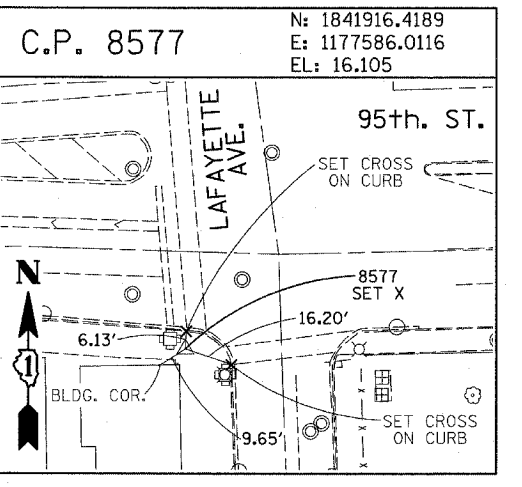
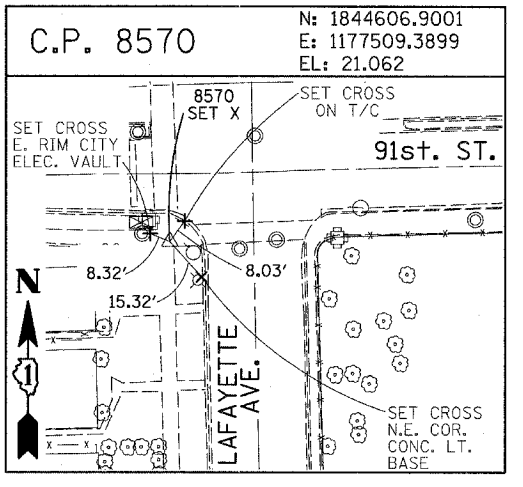
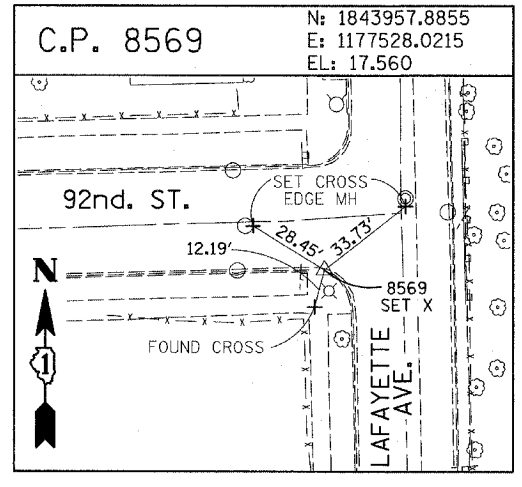
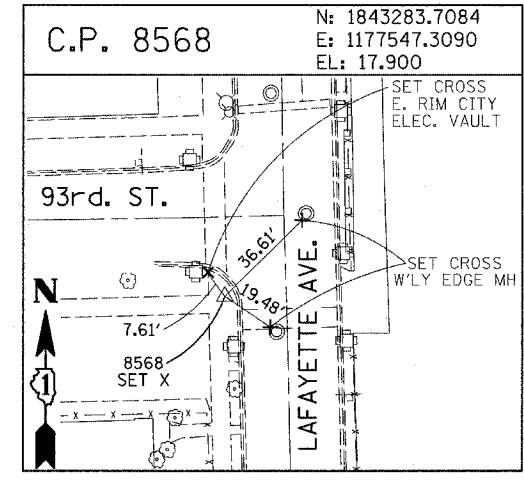
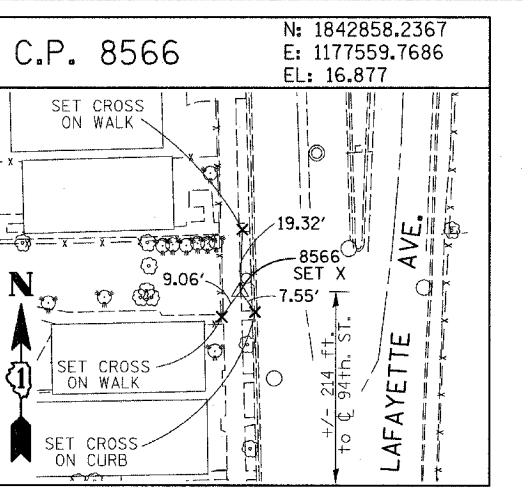
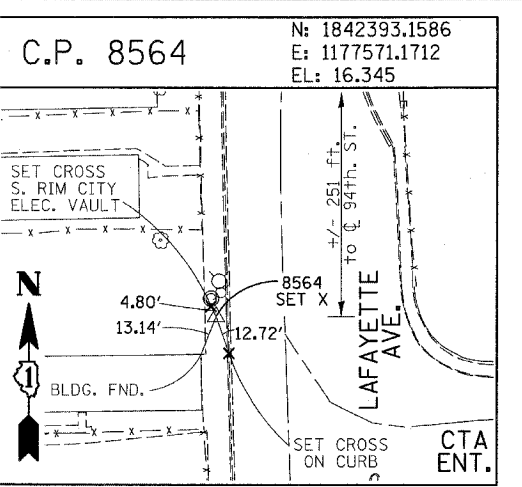
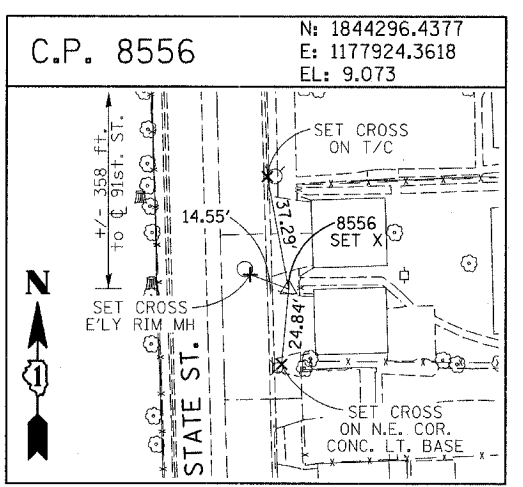
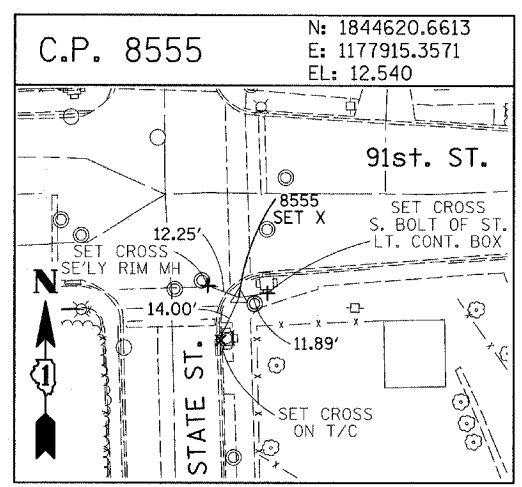
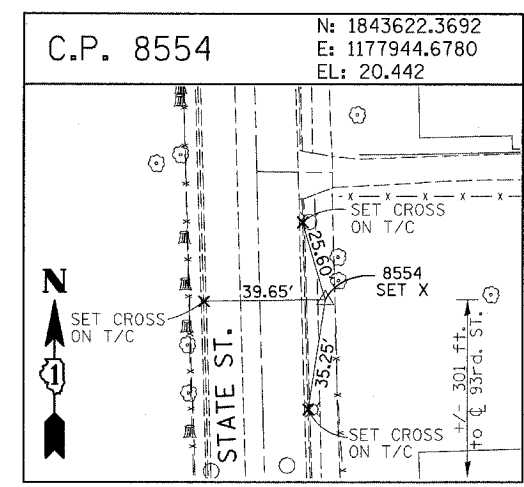
- BM 2231 ELEVATION= 10.24 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 71ST STREET AND LAFAYETTE AVENUE.
- BM 2232 ELEVATION= 8.46 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 73RD STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 330 FEET SOUTH OF 73RD STREET.
- BM 2233 ELEVATION= 9.90 FEET
SET CROSS ON NORTHWEST BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 75TH STREET AND LAFAYETTE AVENUE.
- BM 2234 ELEVATION= 9.56 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 75TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 320 FEET SOUTH OF 75TH STREET.
- BM 2235 ELEVATION= 7.50 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 76TH STREET ON THE WEST SIDE OF LAFAYETTE. APPROXIMATELY 330 FEET SOUTH OF 76TH STREET.
- BM 2236 ELEVATION= 10.41 FEET
SET CROSS ON WEST BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 79TH STREET AND LAFAYETTE AVENUE.
- BM 2237 ELEVATION= 9.20 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF 83RD STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 350 FEET NORTH OF 83RD STREET.
- BM 2238 ELEVATION= 10.04 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT OF THE NORTHWEST CORNER OF 4TH STREET AND LAFAYETTE AVENUE.
- BM 2239 ELEVATION= 9.59 FEET
SET CROSS ON CHAIN BOLT OF SECOND FIRE HYDRANT NORTH OF 87TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 720 FEET OF NORTH OF 87TH STREET.
- BM 2240 ELEVATION= 9.49 FEET
SET CROSS ON THE NORTHWEST BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 78TH STREET AND LAFAYETTE AVENUE.
- BM 2241 ELEVATION= 3.63 FEET
SET SQUARE CUT ON TOP OF CURB NEXT TO LIGHT POLE ON THE NORTHWEST CORNER OF LAFAYETTE AVENUE AND RAILROAD BRIDGE. APPROXIMATELY 770 FEET NORTH OF 91ST STREET.
- BM 2242 ELEVATION= 22.82 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 91ST STREET AND LAFAYETTE AVENUE.
- BM 2243 ELEVATION= 15.68 FEET
SET SQUARE CUT ON THE TOP OF CURB NEXT TO THIRD LIGHT POLE NORTH OF 97TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 260 FEET NORTH OF 97TH STREET.
- BM 2244 ELEVATION= 15.94 FEET
SET SQUARE CUT ON TOP OF CURB NEXT TO LIGHT POLE ON THE NORTH SIDE OF 98TH STREET. APPROXIMATELY 120 FEET EAST OF WENTWORTH AVENUE.

- BM 2245 ELEVATION= 16.35 FEET
SET SQUARE CUT ON THE EAST END OF CURB AT THE NORTHEAST CORNER OF FIRST ALLEY NORTH OF 99TH STREET. APPROXIMATELY 35 FEET EAST OF LASALLE STREET.
- BM 2246 ELEVATION= 20.00 FEET
SET SQUARE CUT WITH CROSS ON TOP OF ABUTMENT WALL ON THE NORTHWEST CORNER OF STATE STREET BRIDGE OVER THE BISHOP FORD EXPRESSWAY. APPROXIMATELY 20 FEET NORTH OF THE NORTH EDGE OF THE BRIDGE.
- BM 2248 ELEVATION= 9.77 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF RAILROAD BRIDGE ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 110 FEET SOUTH OF 90TH STREET.
- BM 2249 ELEVATION= 9.67 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 87TH AND STATE STREET.
- BM 2250 ELEVATION= 10.03 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 83RD ST. AND STATE STREET.
- BM 2251 ELEVATION= 10.95 FEET
FOUND CROSS ON EAST BOLT OF FIRE HYDRANT ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 135 FEET NORTH OF 79TH STREET.
- BM 2252 ELEVATION= 10.02 FEET
SET CROSS ON WEST BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF STATE STREET AND 76TH STREET.
- BM 2254 ELEVATION= 9.17 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 73RD STREET ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 330 FEET SOUTH OF 73RD STREET.
- BM 2255 ELEVATION= 11.25 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 71ST STREET AND STATE STREET.
- BM 2256 ELEVATION= 16.36 FEET
SET SQUARE CUT ON TOP OF ABUTMENT WALL ON THE NORTHWEST CORNER OF MARTIN LUTHER KING DRIVE BRIDGE OVER THE BISHOP FORD EXPRESSWAY. APPROXIMATELY 1 FOOT NORTH OF THE NORTH END OF THE BRIDGE.
- BM 2257 ELEVATION= 18.25 FEET
SET CROSS ON NORTHEAST BOLT OF LIGHT POLE BASE ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 970 FEET SOUTH OF 95TH STREET.
- BM 2258 ELEVATION= 10.53 FEET
SET CROSS ON NORTHWEST BOLT OF LIGHT BASE ON NORTHWEST CORNER OF 71ST STREET AND LAFAYETTE AVENUE.

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- BM 101 ELEVATION= 24.89 FEET
SET SQUARE CUT ON SOUTHERLY CURB OF WEST 98TH PLACE. APPROXIMATELY 70 FEET EAST OF THE CENTERLINE OF GREEN STREET.
- BM 102 ELEVATION= 27.36 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF WEST 99TH STREET AND GREEN STREET.
- BM 8558 ELEVATION= 17.73 FEET
SET SQUARE CUT ON SOUTHEAST CORNER OF FOURTH LIGHT BASE SOUTH OF 95TH STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8559 ELEVATION= 17.10 FEET
SET SQUARE CUT ON SOUTHWEST CORNER OF CONCRETE SIGN BASE AT THE NORTHWEST CORNER OF THE MOBIL GAS STATION PROPERTY AT THE NORTHEAST CORNER OF 95TH STREET AND STATE STREET.
- BM 8560 ELEVATION= 17.50 FEET
SET SQUARE CUT ON SOUTHEAST CORNER OF CONCRETE BUS SLAB ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 50 FEET SOUTH OF THE CENTERLINE OF 94TH STREET.
- BM 8561 ELEVATION= 19.77 FEET
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT SOUTH OF 93RD STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8562 ELEVATION= 22.29 FEET
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT NORTH OF 93RD STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8563 ELEVATION= 20.47 FEET
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT SOUTH OF 91ST STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8573 ELEVATION= 19.89 FEET
SET SQUARE CUT ON EAST SIDE OF SIDEWALK OPPOSITE TO THE SECOND FIRE HYDRANT SOUTH OF 91ST STREET, ON THE WEST SIDE OF LAFAYETTE AVENUE.
- BM 8574 ELEVATION= 18.90 FEET
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT APPROXIMATELY AT 9224 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.
- BM 8575 ELEVATION= 17.77 FEET
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT APPROXIMATELY AT 9326 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.

- BM 8576 ELEVATION= 17.43 FEET
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT AT THE VACANT LOT BETWEEN 9416 AND 9422 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.
- BM 8578 ELEVATION= 27.92 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND PEORIA STREET.
- BM 8579 ELEVATION= 28.59 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND SANGAMON STREET.
- BM 8580 ELEVATION= 29.24 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND MORGAN STREET.
- BM 8581 ELEVATION= 30.82 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF CARPENTER STREET AND WEST 98TH PLACE.
- BM 8582 ELEVATION= 30.69 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF WEST 98TH PLACE, ON THE EAST SIDE OF ABERDEEN STREET.
- BM 8583 ELEVATION= 33.14 FEET
SET SQUARE CUT ON CONCRETE BRIDGE WALL AT THE SOUTHEASTERLY CORNER OF GENOA AVENUE AND 99TH STREET.
- BM 8584 ELEVATION= 32.36 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT WEST OF ABERDEEN STREET, ON THE SOUTHERLY SIDE OF 99TH STREET.
- BM 8585 ELEVATION= 30.28 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRST FIRE HYDRANT WEST OF MORGAN STREET, ON THE SOUTH SIDE OF 99TH STREET.
- BM 8586 ELEVATION= 28.75 FEET
SET CROSS ON NORTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF 99TH STREET AND SANGAMON STREET.
- BM 8587 ELEVATION= 31.19 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF 99TH STREET AND CARPENTER STREET.

- BM 3514 ELEVATION= 17.56 FEET
SET CROSS ON CHAIN BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF LASALLE ST. AND 97TH ST.
- BM 3522 ELEVATION= 18.06 FEET
SET CROSS ON SOUTH SOUTHEAST FLANGE BOLT OF HYDRANT ± HALFWAY BETWEEN NORTH AND SOUTH ENTRANCE TO CITGO, EAST SIDE OF STATE ST. AND NORTH OF 95TH ST.
- BM 3549 ELEVATION= 9.49 FEET
SET CROSS WESTERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 87TH ST. AND LAFAYETTE AVE.
- BM 3550 ELEVATION= 22.88 FEET
SET CROSS WESTERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 91ST ST. AND LAFAYETTE AVE.
- BM 3551 ELEVATION= 17.50 FEET
SET CROSS NORTHERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 95TH ST. AND LAFAYETTE AVE.
- BM 3552 ELEVATION= 14.81 FEET
SET CROSS NORTHEAST BOLT OF TRAFFIC SIGNAL/LIGHT BASE AT SOUTHEAST INTERSECTION OF 91ST ST. AND STATE ST.
- BM 3553 ELEVATION= 8.47 FEET
SET CROSS NORTHWEST BOLT OF TRAFFIC SIGNAL/LIGHT BASE AT NORTHEAST INTERSECTION OF 87TH ST. AND STATE ST.
- BM 3556 ELEVATION= 9.06 FEET
SET CROSS EASTERLY FLANGE BOLT OF HYDRANT AT NORTHEAST CORNER OF 75TH ST. AND STATE ST.
- BM 3558 ELEVATION= 18.15 FEET
SET CROSS SOUTHWESTERLY FLANGE BOLT OF HYDRANT AT NORTHWEST INTERSECTION OF NORMAL ST. AND 98TH PLACE.
- BM 3810 ELEVATION= 0.29 FEET
CUT SQUARE ON SOUTHERLY CORNER OF METAL BASE FOR OVERHEAD SIGN, ON THE WESTERLY SIDE OF SOUTHBOUND DAN RYAN, OPPOSITE OF WEST 77TH PLACE, APPROXIMATELY 1000 FEET NORTH OF 79TH STREET BRIDGE.
- BM 3811 ELEVATION= 1.95 FEET
CUT SQUARE ON SOUTHEASTERLY CORNER OF METAL BASE FOR OVERHEAD SIGN, ON THE WESTERLY SIDE OF SOUTHBOUND DAN RYAN APPROXIMATELY 840 FEET SOUTHERLY OF 83RD STREET.

REVISIONS	
NAME	DATE

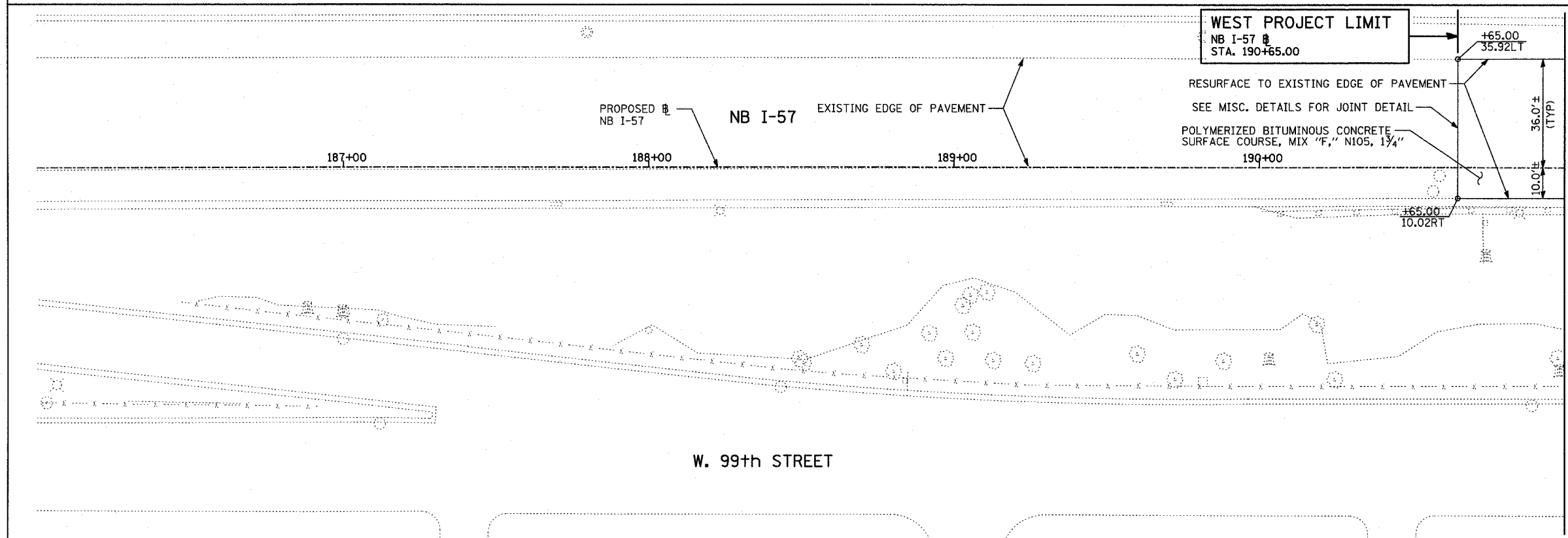
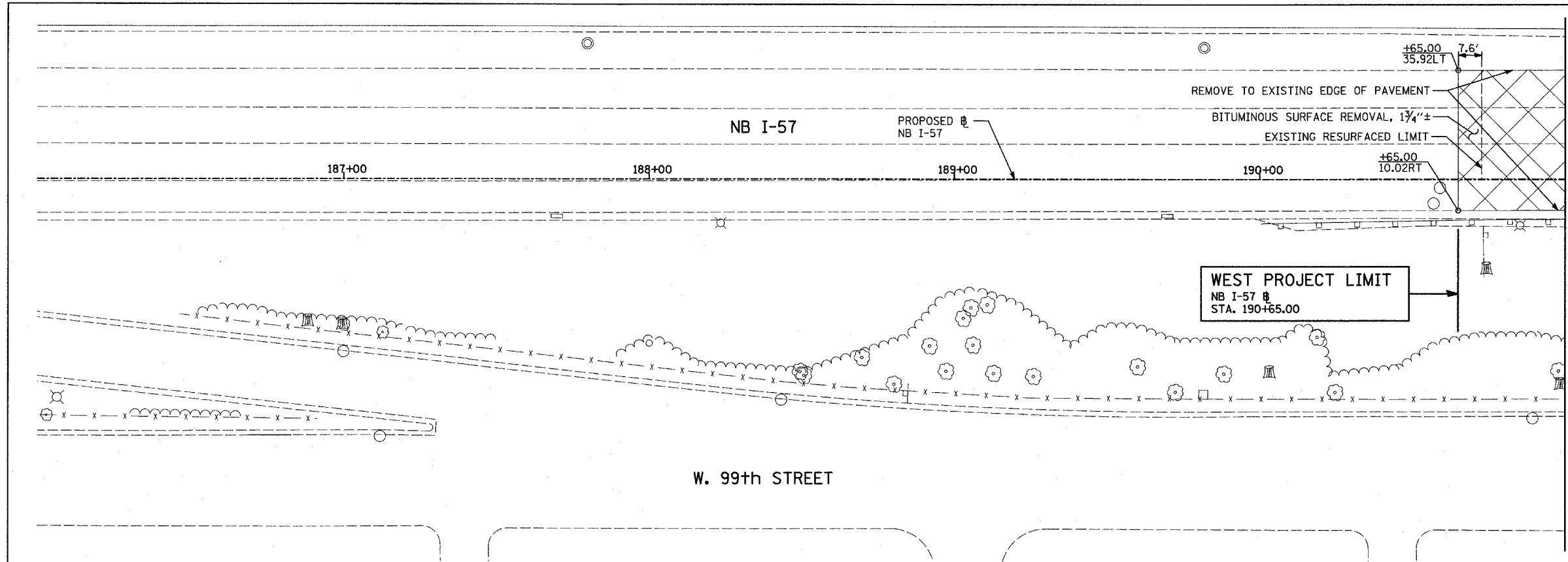
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

**SURVEY TIES FOR CONTROL POINTS
 SHEET 7 OF 7**

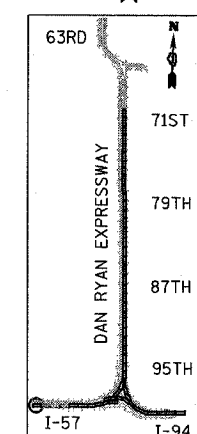
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 DATE: MARCH 7, 2006

DRAWN BY: GSP
 CHECKED BY: MMW

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	64
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4 62304				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP



LEGEND:

- #### COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Hatched Box] PAVED SHOULDER REMOVAL
- [Hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE Ⓢ
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

TYLIN INTERNATIONAL

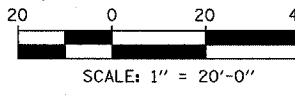
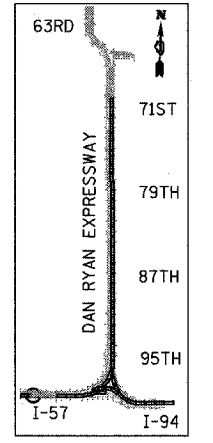
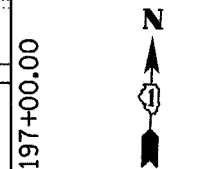
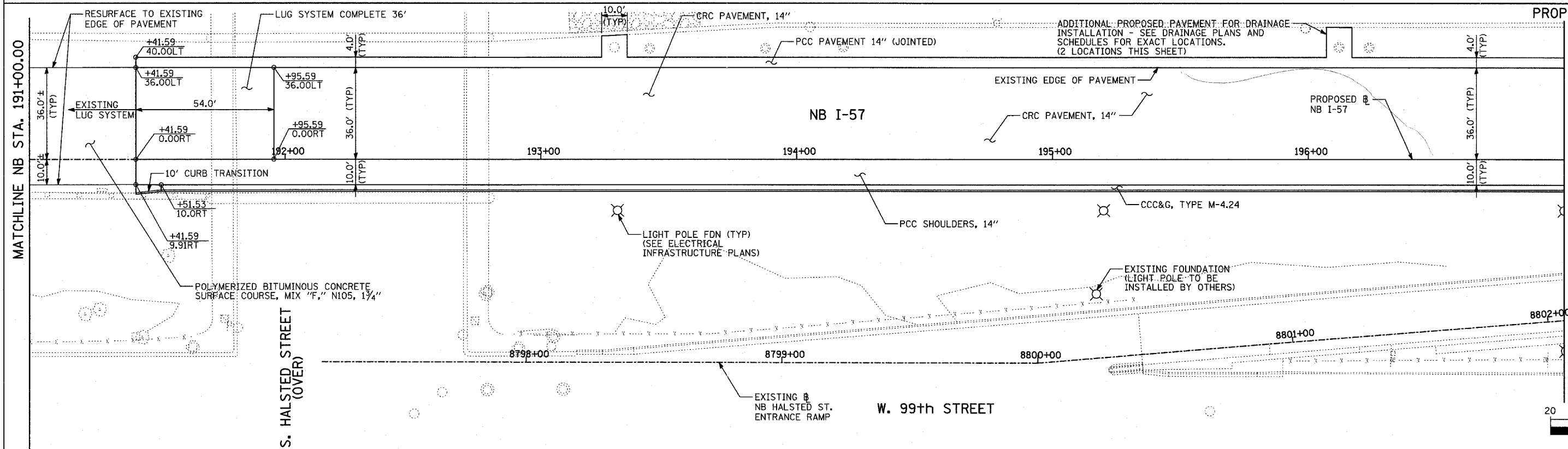
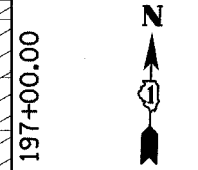
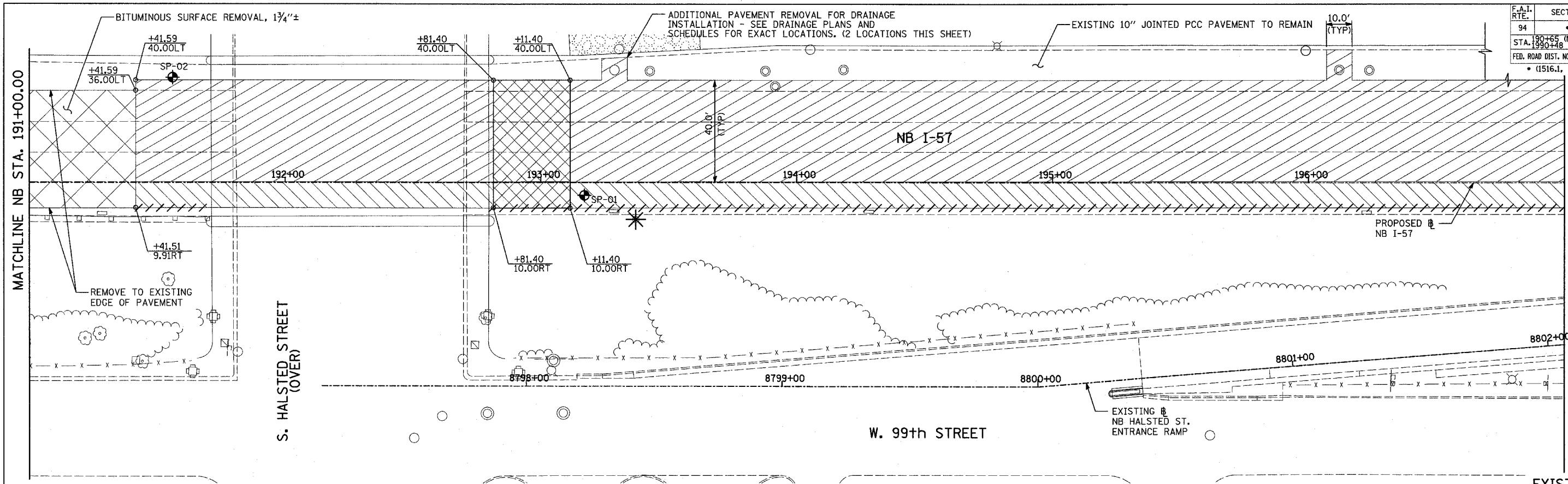
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
NB I-57
NB I-57 STA. 190+65.00 TO 191+00.00

SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: RTM
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	65
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN EXP)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 6230				



LEGEND:

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Diagonal Hatched Box] PAVED SHOULDER REMOVAL
- [Cross-hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Horizontal Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Vertical Hatched Box] BITUMINOUS SURFACE REMOVAL, 1/2" OR 3/4" (SEE CALLOUT)

- ⊗ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE Ⓡ STATION/OFFSET
- XX.XXRT

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

TYLIN INTERNATIONAL

SHEET 2 OF 47

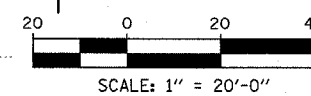
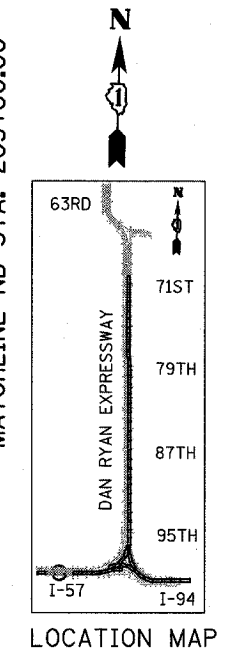
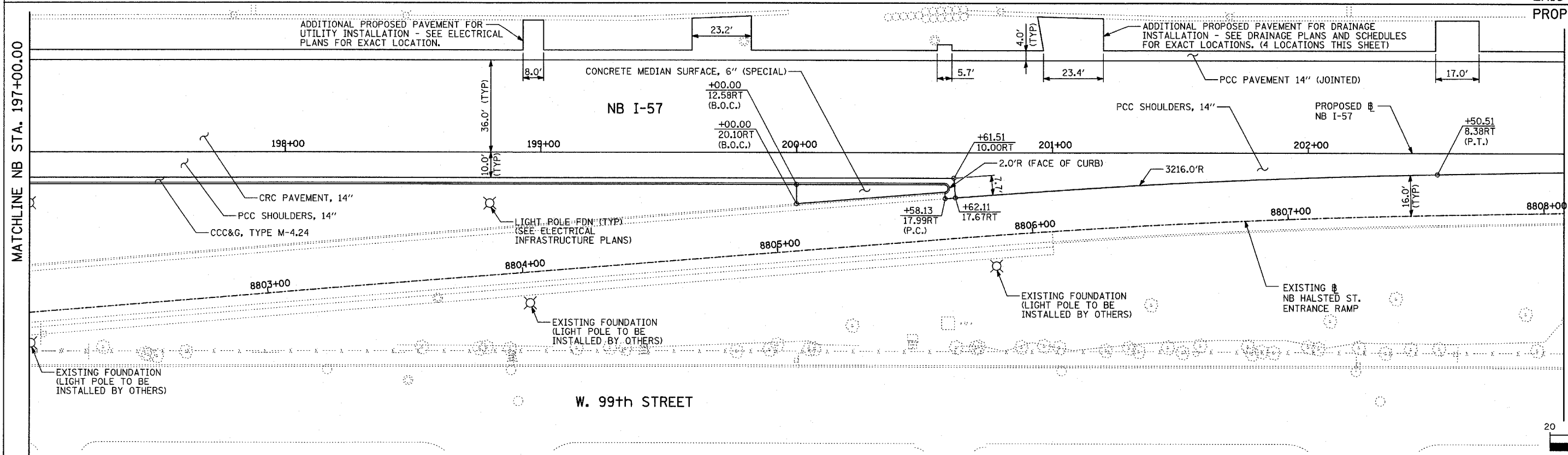
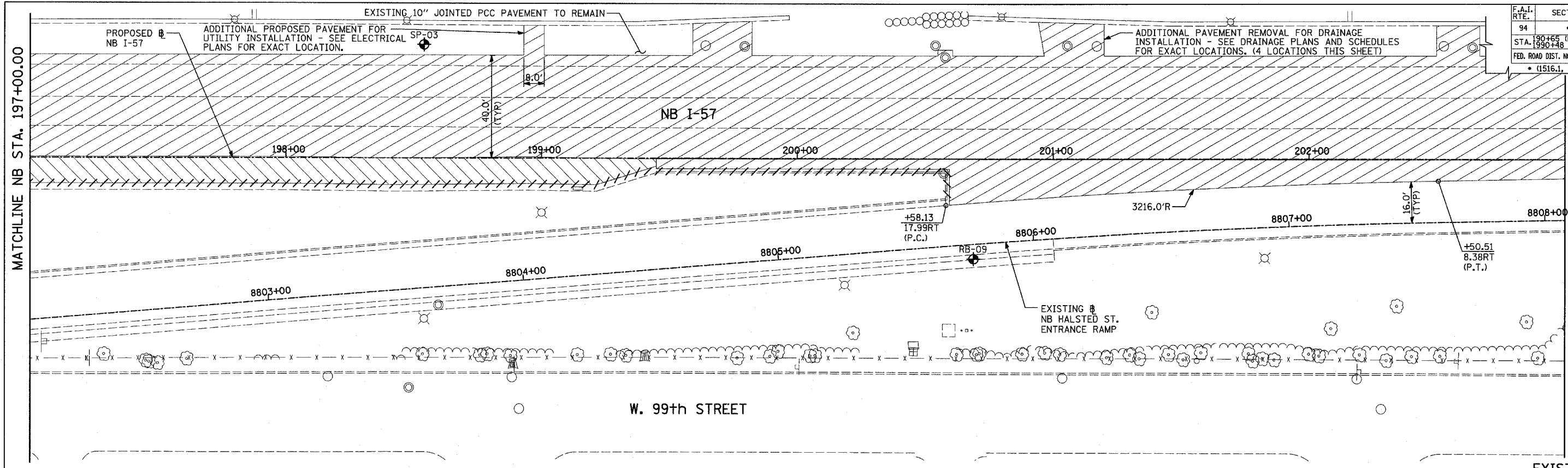
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
 NB I-57
 NB I-57 STA. 191+00.00 TO 197+00.00

SCALE: 1"=20'
 DATE: MARCH 7, 2006

DRAWN BY: RTM
 CHECKED BY: MPG

F.A.I. RTE. 94	SECTION	COUNTY COOK	TOTAL SHEETS 916	SHEET NO. 66
STA. 190+45 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516, 1717, & 1818) R-4				



LEGEND:

####	COMB CONC CURB & GUTTER REMOVAL	[Hatched Box]	PAVEMENT REMOVAL
~~~~~	CHAIN LINK FENCE REMOVAL	[Diagonal Hatched Box]	PAVED SHOULDER REMOVAL
XXXXXX	CONCRETE BARRIER/GUARDRAIL REMOVAL	[Cross-hatched Box]	PAVEMENT/SHOULDER REMOVAL, SPECIAL
X10	TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)	[Horizontal Hatched Box]	BITUMINOUS SURFACE REMOVAL, 4"
◆	SOIL BORING LOCATIONS	[Vertical Hatched Box]	BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

**PLAN NOTES:**

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 NB I-57  
 NB I-57 STA. 197+00.00 TO 203+00.00

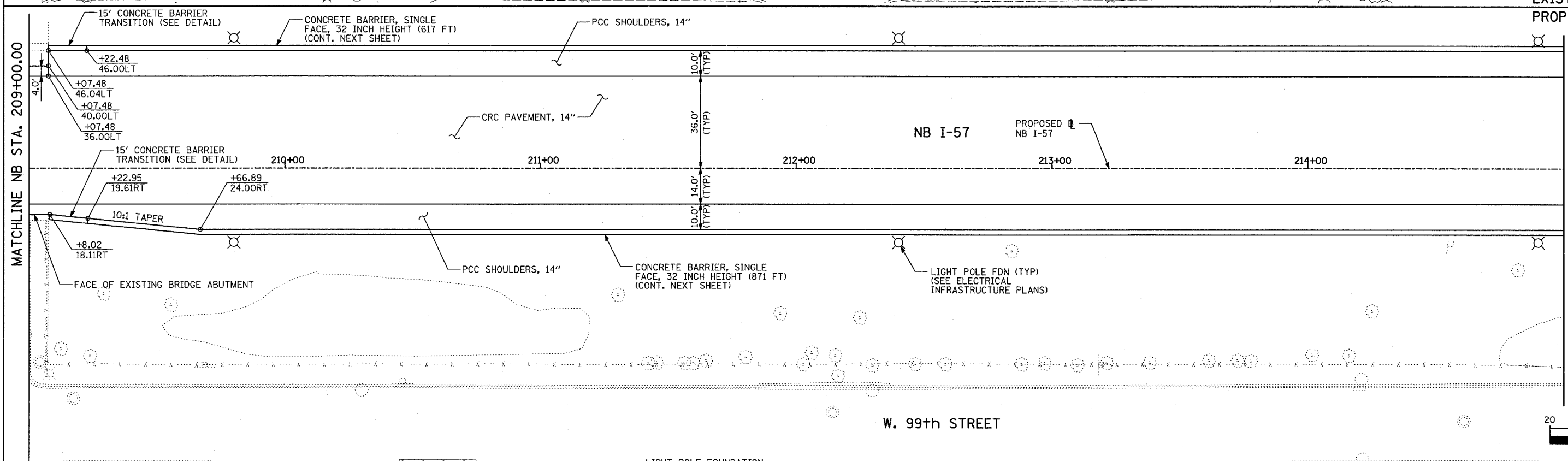
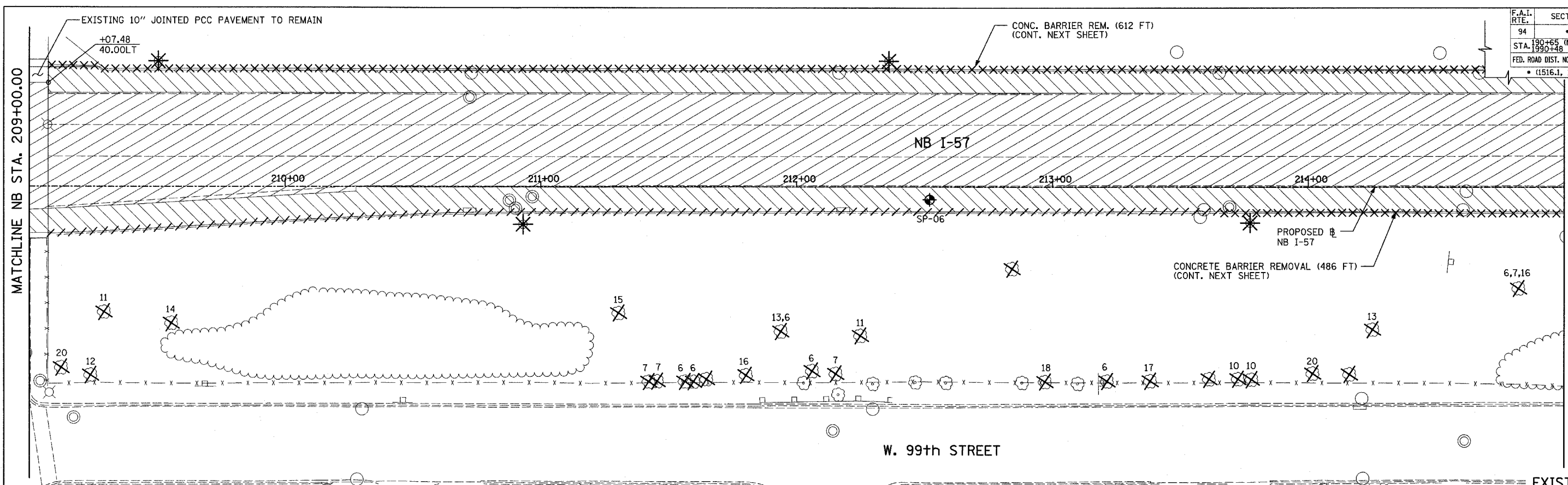
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 DATE: MARCH 7, 2006

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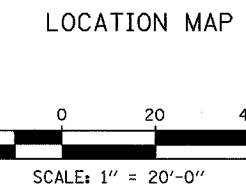
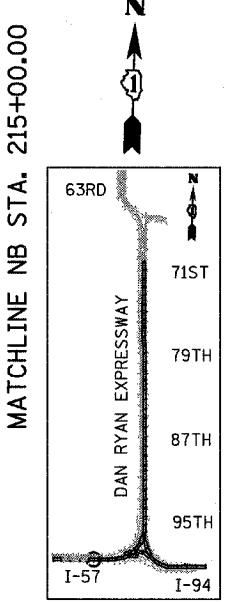
**TYLIN INTERNATIONAL**



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	68
STA. 190+65 (NB I-57) TO STA. 199+48 (NB FORD)		TO STA. 2316+00 (NB RYAN)		
FED. ROAD DIST. NO.		ILLINOIS		FED. AID PROJECT
				(1516.1, 1717, & 1818) R-4



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



**LEGEND:**

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Hatched Box] PAVED SHOULDER REMOVAL
- [Hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE Ⓟ
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN INTERNATIONAL**

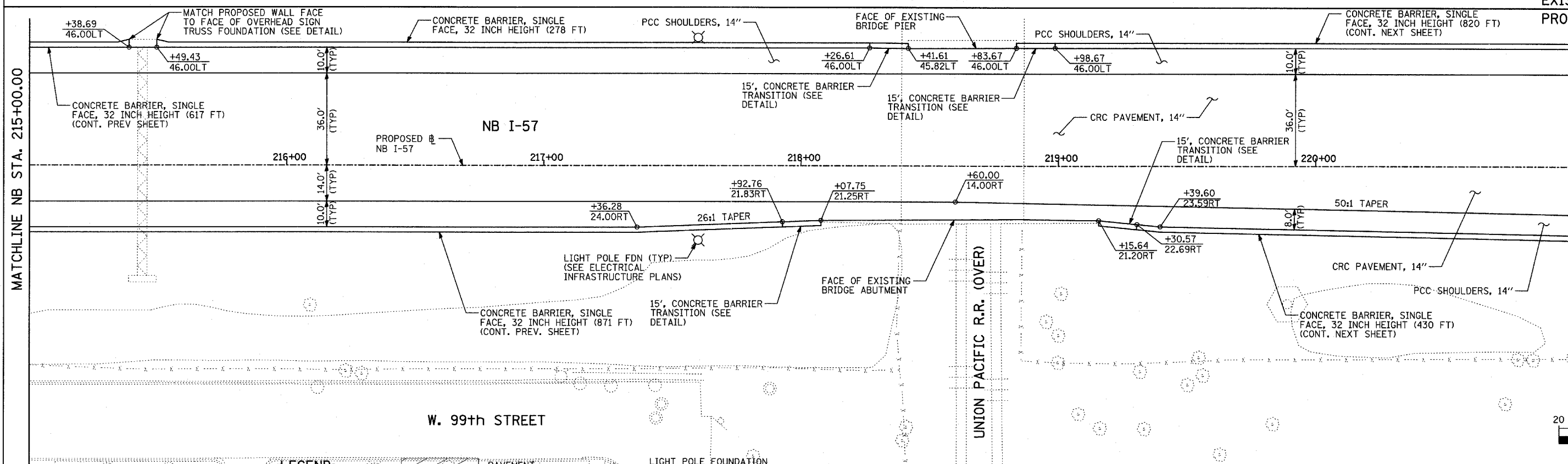
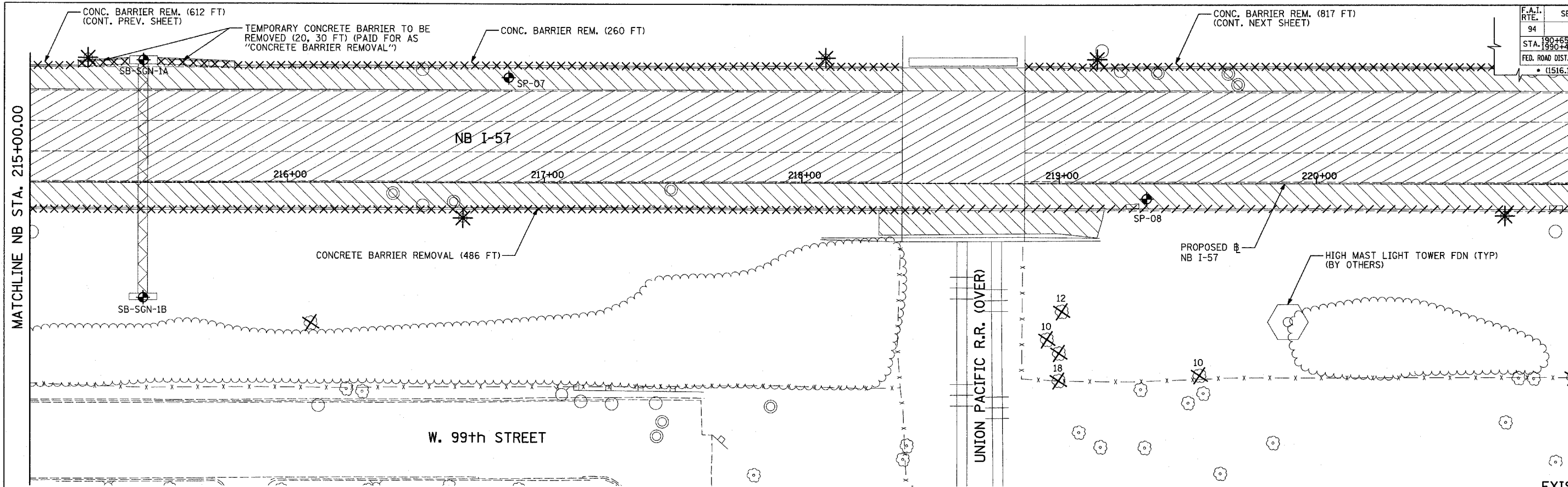
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-57  
NB I-57 STA. 209+00.00 TO 215+00.00

SCALE: 1"=20'  
DATE: MARCH 7, 2006

DRAWN BY: RTM  
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	69
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		
		• (1516.1, 1717, & 1818) R-4		

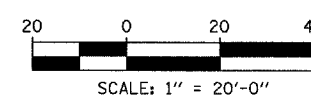


MATCHLINE NB STA. 215+00.00

EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS

MATCHLINE NB STA. 215+00.00

LOCATION MAP



<b>LEGEND:</b>		PAVEMENT REMOVAL		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
		PAVED SHOULDER REMOVAL		+XX.XX MAINLINE #
		PAVEMENT/SHOULDER REMOVAL, SPECIAL		XX.XXRT STATION/OFFSET
		BITUMINOUS SURFACE REMOVAL, 4"	<b>PLAN NOTES:</b>	
		BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)	- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.	
		COMB CONC CURB & GUTTER REMOVAL	- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED	
		CHAIN LINK FENCE REMOVAL		
		CONCRETE BARRIER/GUARDRAIL REMOVAL		
		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)		
		SOIL BORING LOCATIONS		

REVISIONS	
NAME	DATE

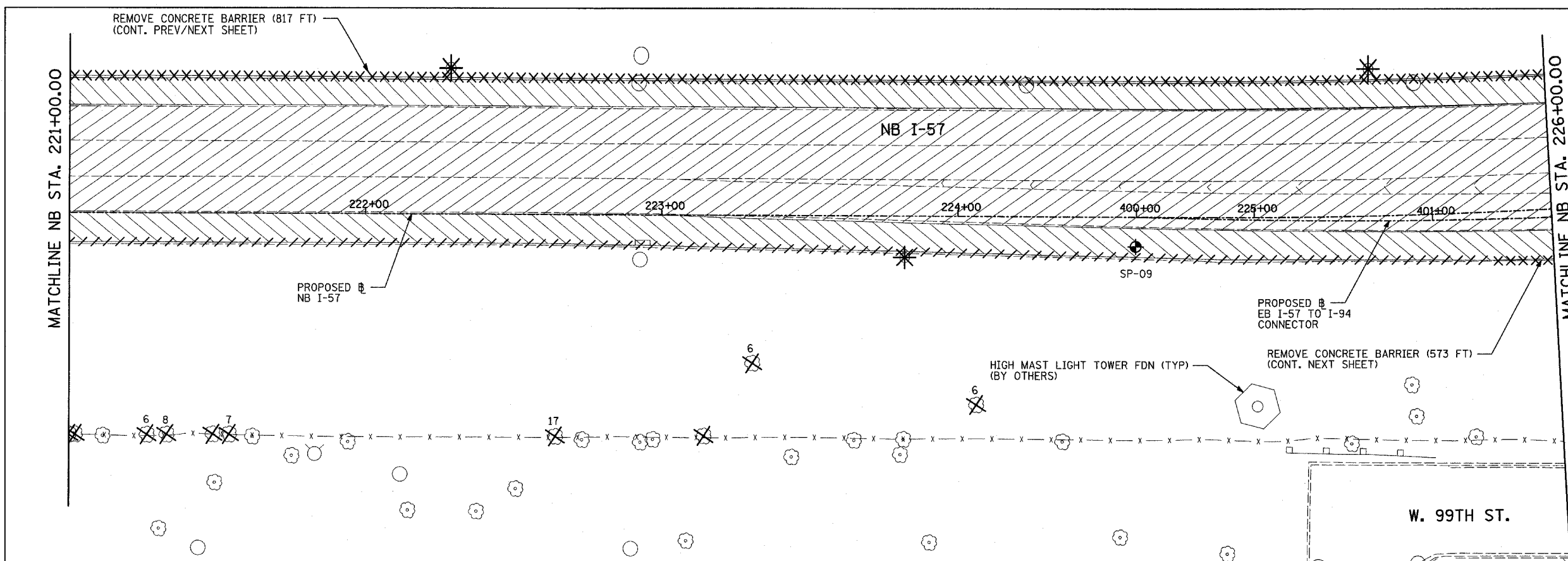
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-57  
NB I-57 STA. 215+00.00 TO 221+00.00

SCALE: 1"=20'  
DATE: MARCH 7, 2006

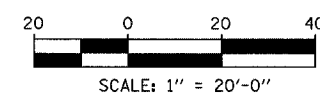
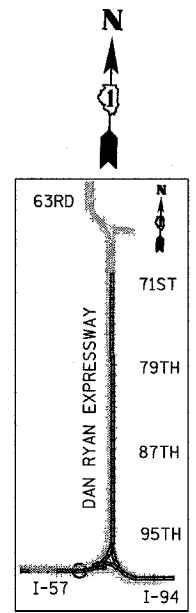
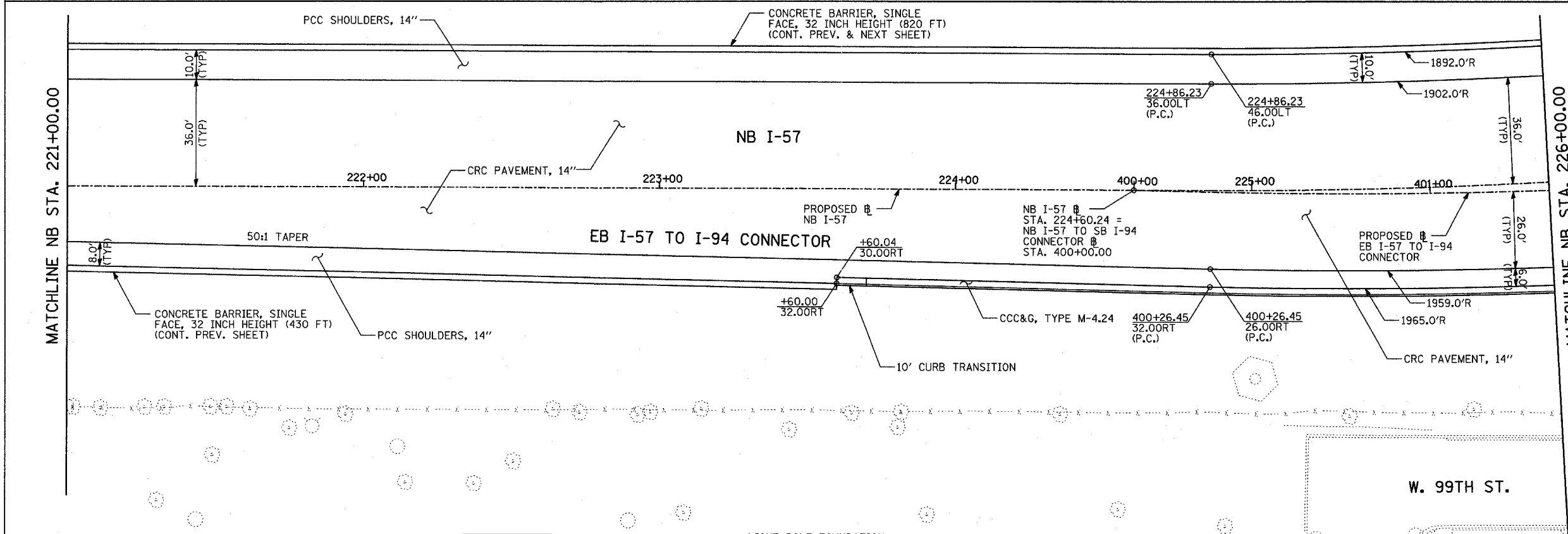
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CHECKED BY: MPG

**TYLIN** INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	916	70
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN EXP. FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4 6230				



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



**LEGEND:**

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- X10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ◆ SOIL BORING LOCATIONS
- ////// PAVEMENT REMOVAL
- ////// PAVED SHOULDER REMOVAL
- XXXXXX PAVEMENT/SHOULDER REMOVAL, SPECIAL
- ////// BITUMINOUS SURFACE REMOVAL, 4"
- XXXXXX BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN** INTERNATIONAL

SHEET 7 OF 47

REVISIONS	
NAME	DATE

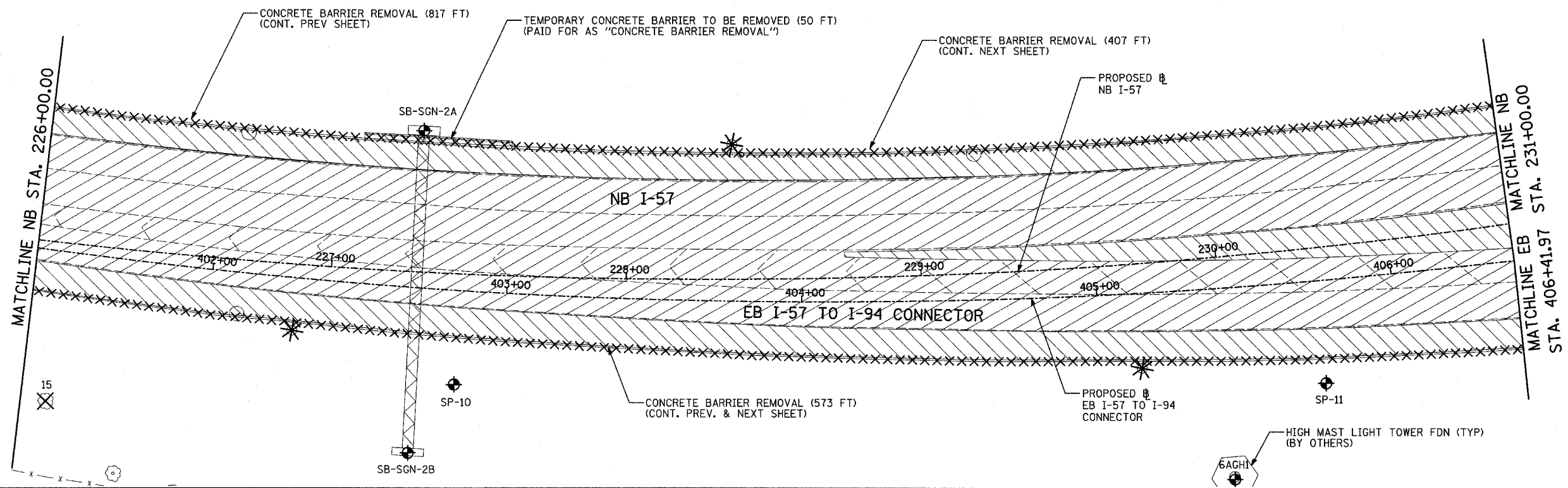
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-57  
NB I-57 STA. 221+00.00 TO 226+00.00

SCALE: 1"=20'  
DATE: MARCH 7, 2006

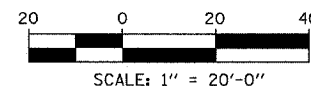
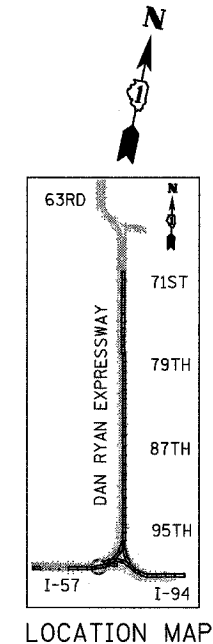
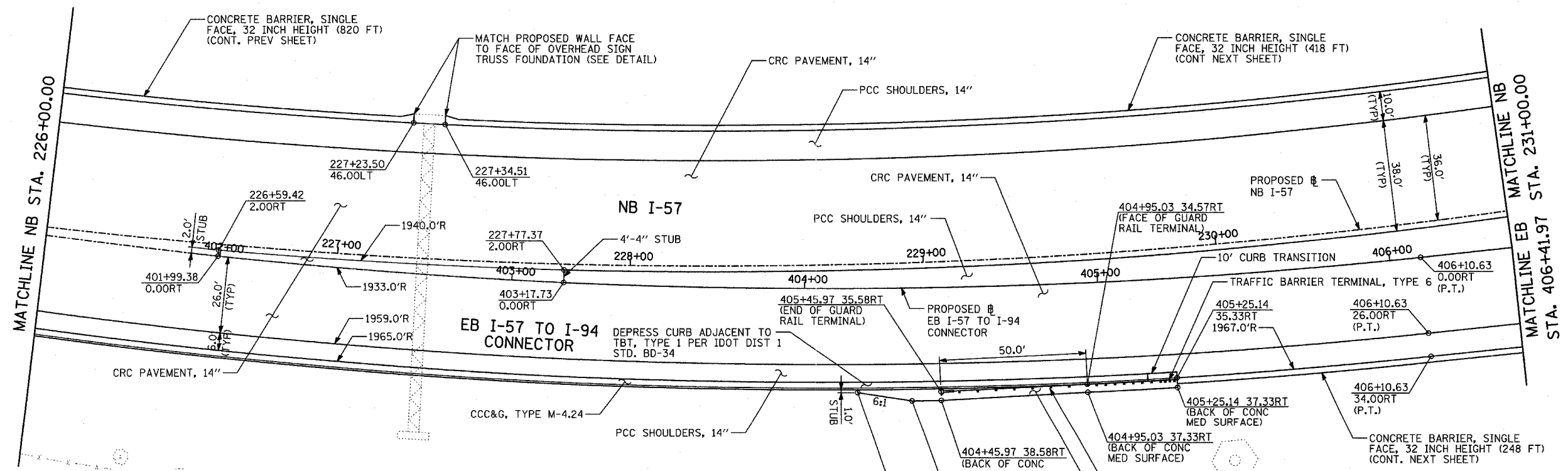
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CHECKED BY: MPG



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	71
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN EXPRESSWAY)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



**LEGEND:**

- ===== COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS
- ▨ PAVEMENT REMOVAL
- ▧ PAVED SHOULDER REMOVAL
- ▩ PAVEMENT/SHOULDER REMOVAL, SPECIAL
- ▦ BITUMINOUS SURFACE REMOVAL, 4"
- ▤ BITUMINOUS SURFACE REMOVAL, 1/2" OR 1 3/4" (SEE CALLOUT)

- ⊗ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
  - +XX.XX MAINLINE #
  - XX.XXRT STATION/OFFSET
- PLAN NOTES:**
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
  - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

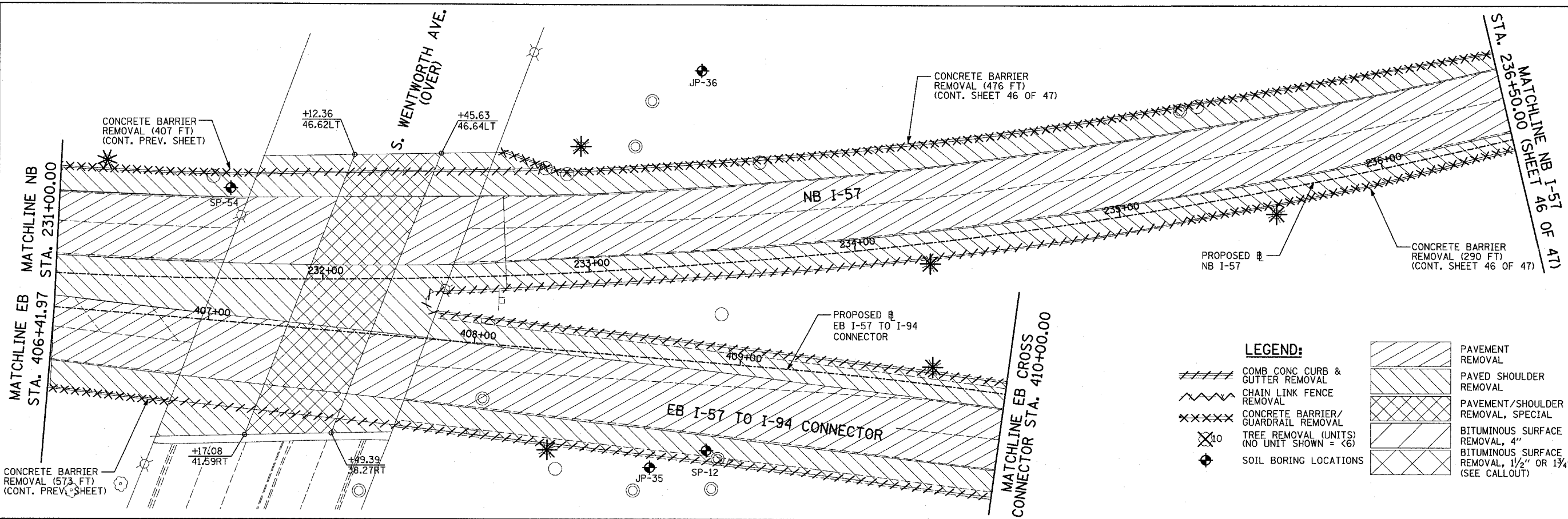
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-57 AND EB I-57 TO I-94 CONNECTOR  
NB I-57 STA. 226+00.00 TO 231+00.00  
AND EB STA. 406+41.97

SCALE: 1"=20'  
DATE: MARCH 7, 2006

DRAWN BY: RTM  
CHECKED BY: MPG

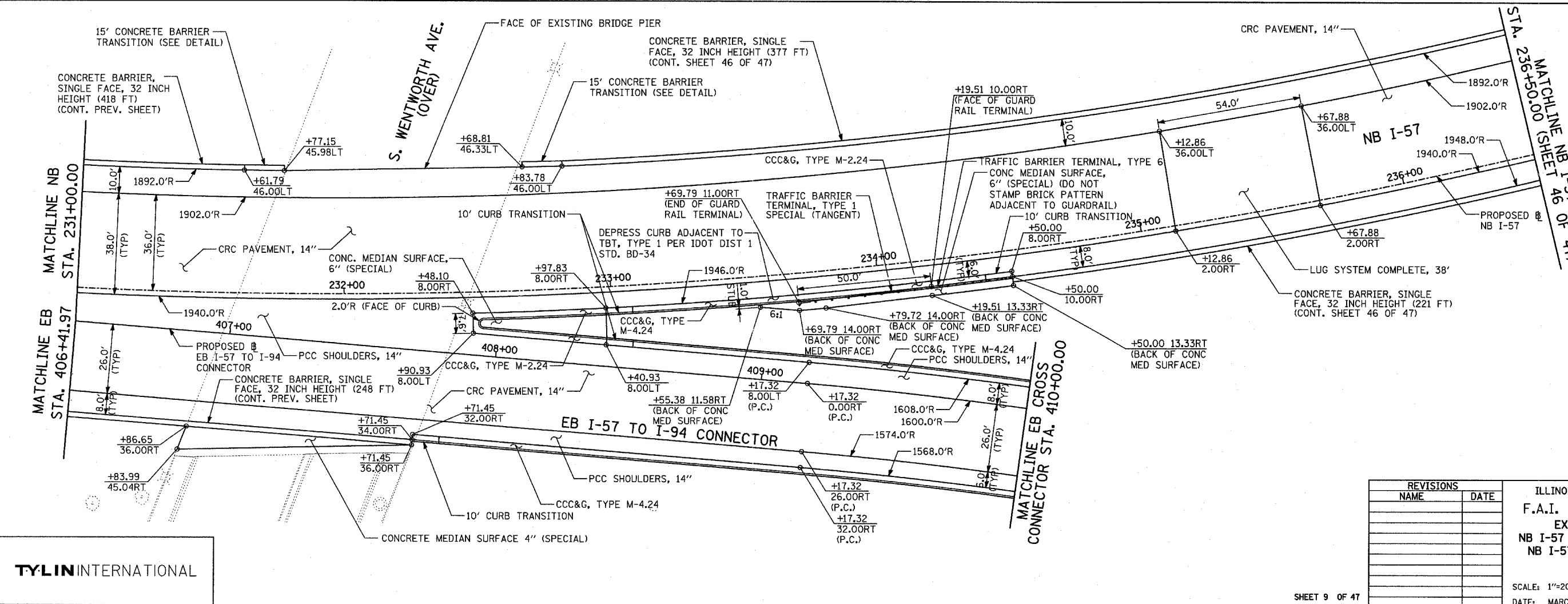


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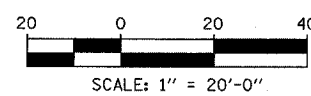
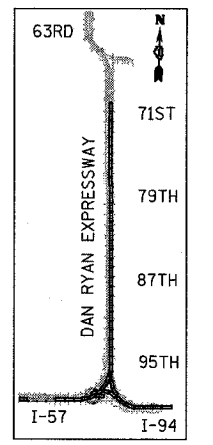
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	CHAIN LINK FENCE REMOVAL		PAVED SHOULDER REMOVAL
	CONCRETE BARRIER/GUARDRAIL REMOVAL		PAVEMENT/SHOULDER REMOVAL, SPECIAL
	TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)		BITUMINOUS SURFACE REMOVAL, 4"
	SOIL BORING LOCATIONS		BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



**EXISTING CONDITIONS**  
**PROPOSED IMPROVEMENTS**



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 NB I-57 AND EB I-57 TO I-94 CONNECTOR  
 NB I-57 STA. 231+00.00 TO 236+50.00  
 AND EB STA. 410+00.00

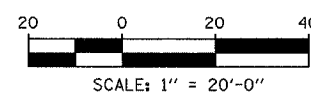
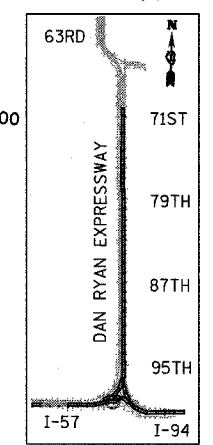
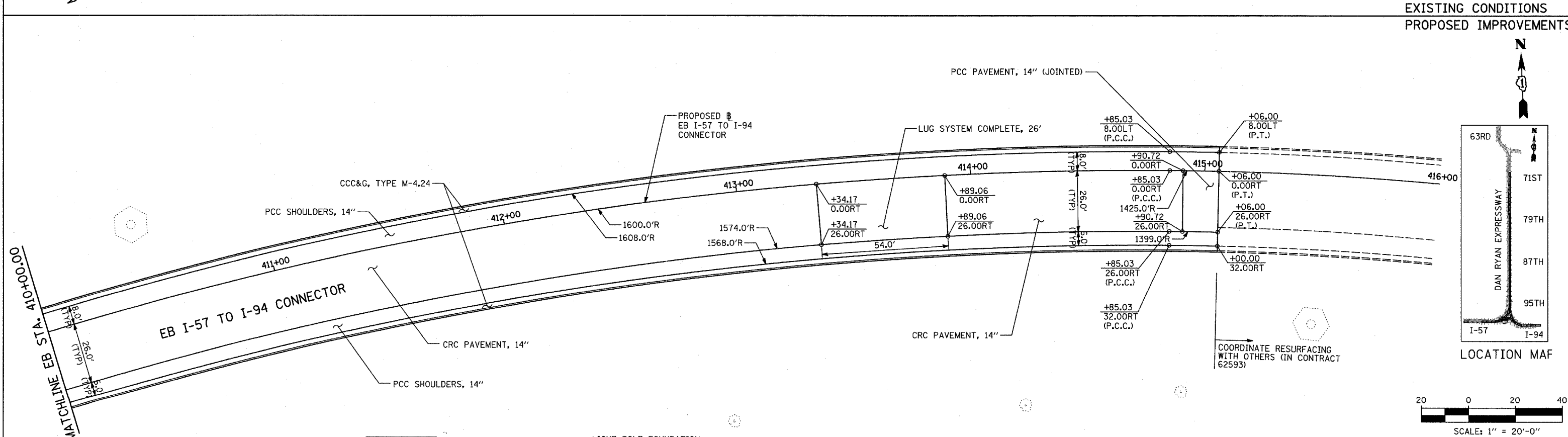
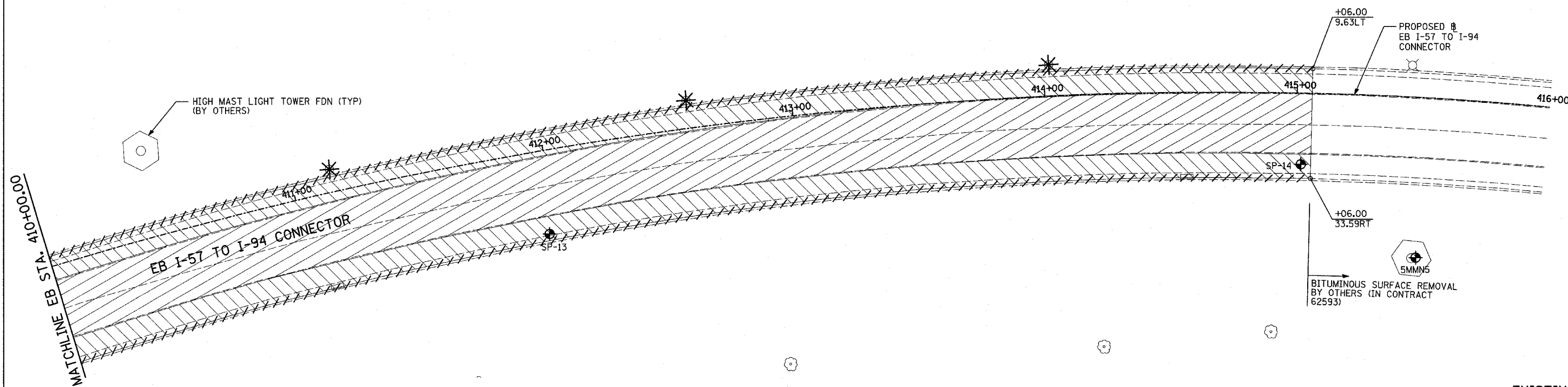
SCALE: 1"=20'  
 DATE: MARCH 7, 2006

DRAWN BY: RTM  
 CHECKED BY: MPG

TYLIN INTERNATIONAL



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	73
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



**LEGEND:**

- ===== COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = 6)
- ⊕ SOIL BORING LOCATIONS
- ////// PAVEMENT REMOVAL
- ==== PAVED SHOULDER REMOVAL
- XXXX PAVEMENT/SHOULDER REMOVAL, SPECIAL
- XXXX BITUMINOUS SURFACE REMOVAL, 4"
- XXXX BITUMINOUS SURFACE REMOVAL, 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 EB I-57 TO I-94 CONNECTOR  
 EB CONNECTOR STA. 410+00.00 TO 415+06.00

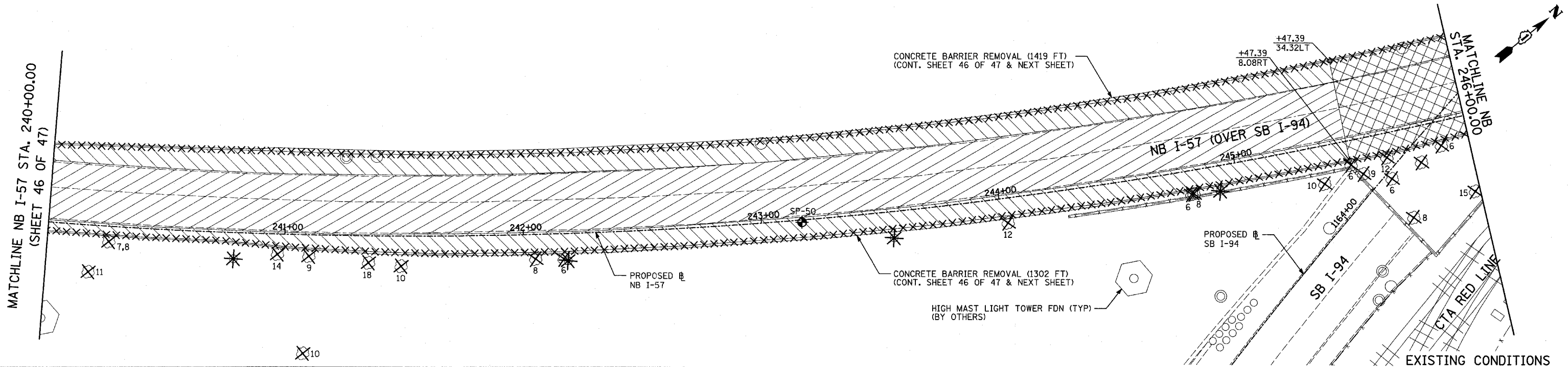
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 DATE: MARCH 7, 2006

DRAWN BY: RTM  
 CHECKED BY: MPG

TYLIN INTERNATIONAL

MATCHLINE NB I-57 STA. 240+00.00  
(SHEET 46 OF 47)

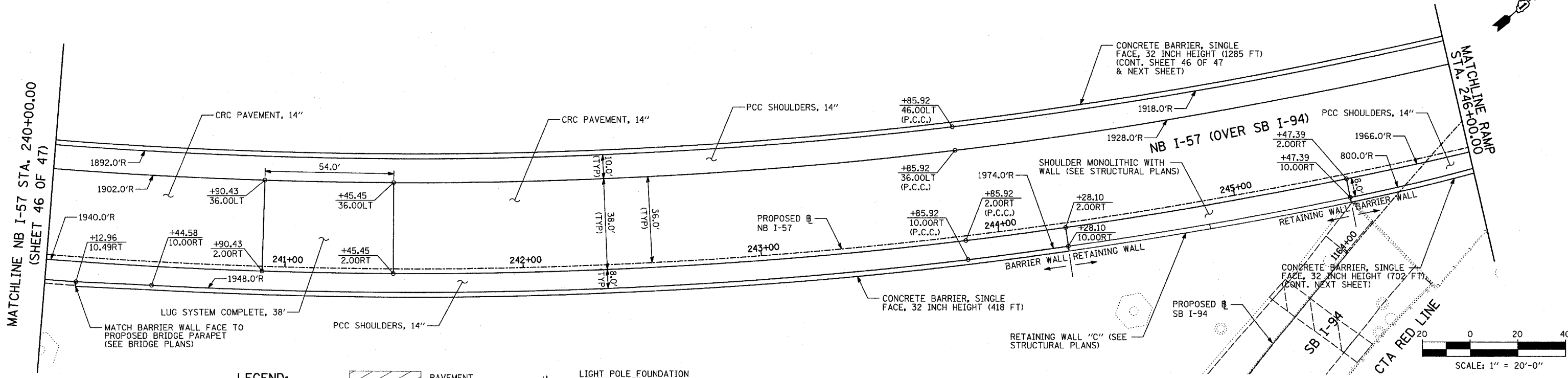
MATCHLINE NB STA. 246+00.00



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS

MATCHLINE NB I-57 STA. 240+00.00  
(SHEET 46 OF 47)

MATCHLINE NB STA. 246+00.00



**LEGEND:**

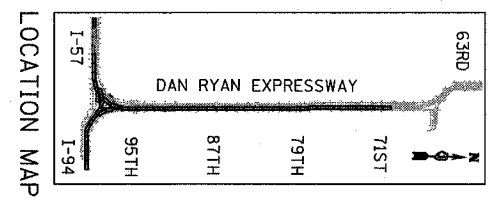
- ===== COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Hatched Box] PAVED SHOULDER REMOVAL
- [Hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE B
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



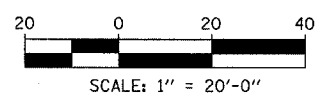
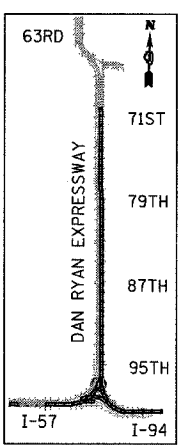
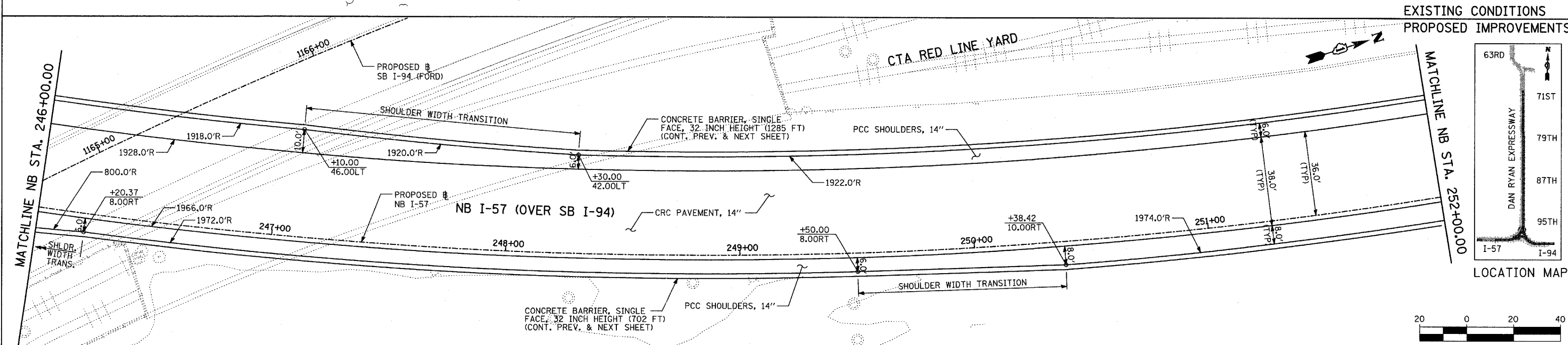
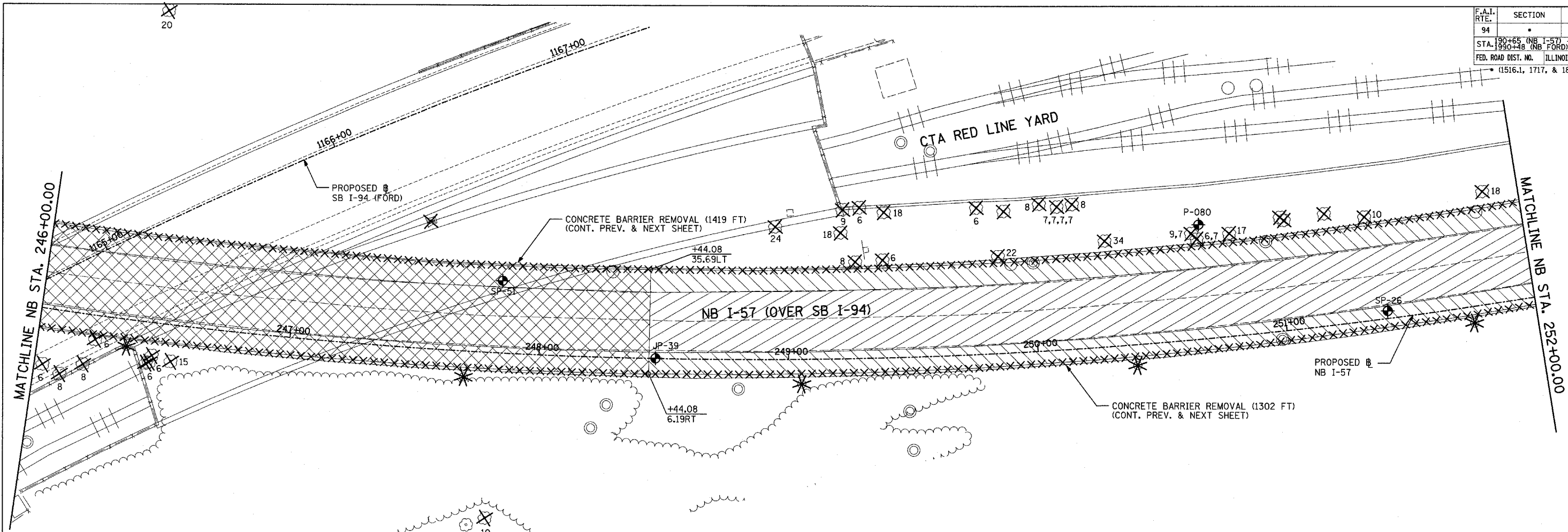
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-57  
NB STA. 240+00.00 TO 246+00.00

SCALE: 1"=20'  
DATE: MARCH 7, 2006

DRAWN BY: RTM  
CHECKED BY: MPG

TYLIN INTERNATIONAL



**LEGEND:**

- ===== COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS
- ▨ PAVEMENT REMOVAL
- ▧ PAVED SHOULDER REMOVAL
- ▩ PAVEMENT/SHOULDER REMOVAL, SPECIAL
- ▤ BITUMINOUS SURFACE REMOVAL, 4"
- ▥ BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN INTERNATIONAL**

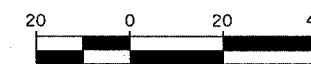
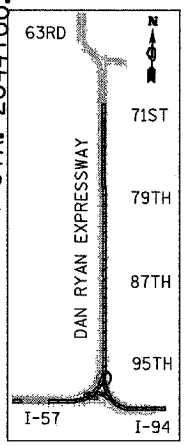
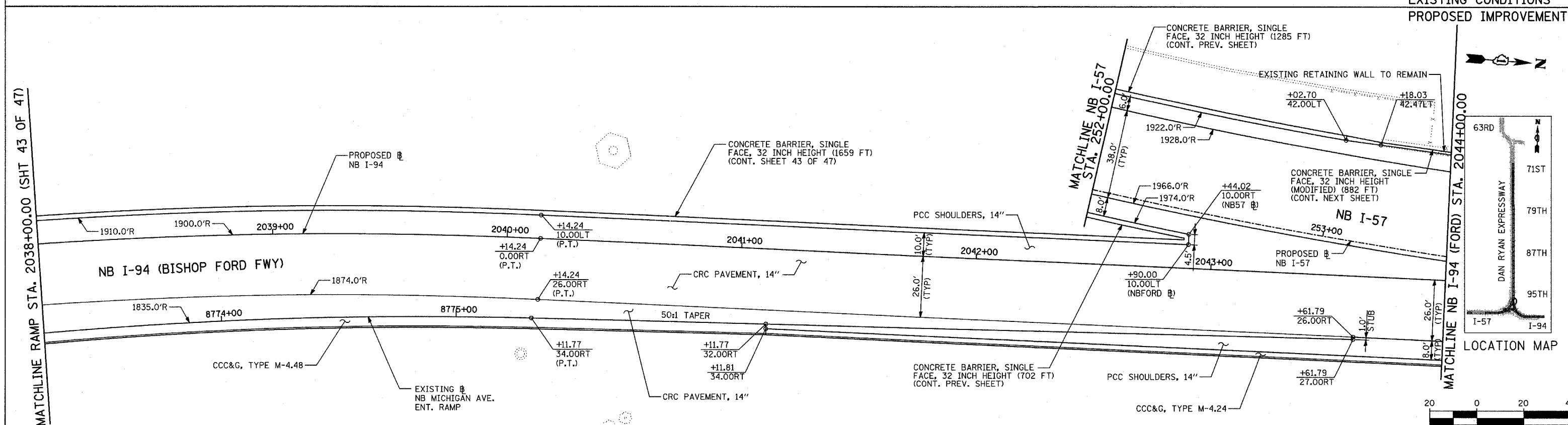
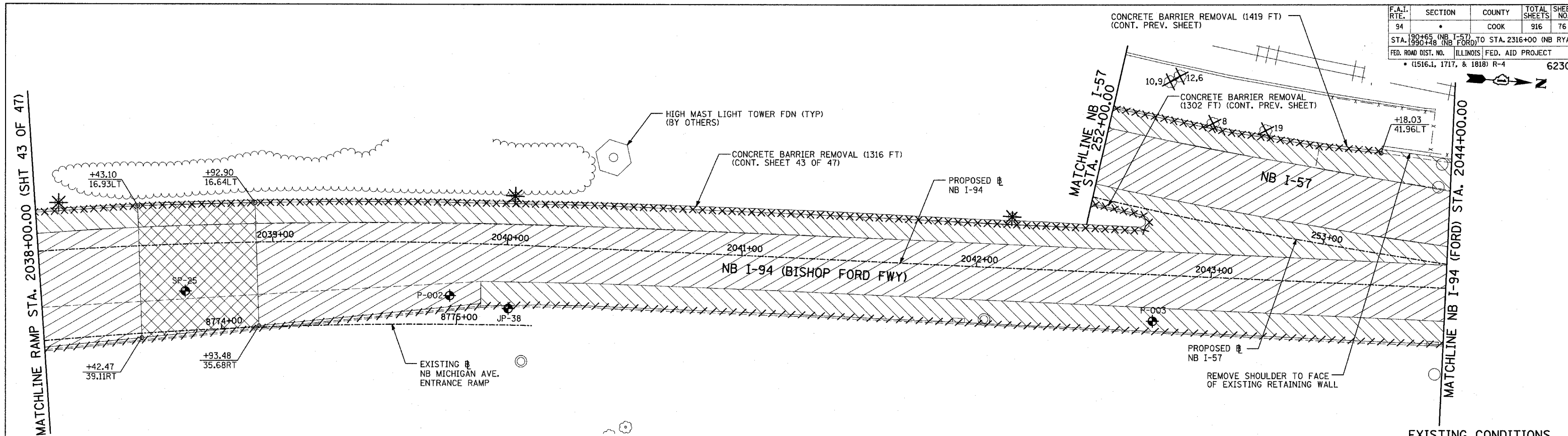
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 NB I-57  
 STA. 246+00.00 TO 252+00.00

SCALE: 1"=20'  
 DATE: MARCH 7, 2006

DRAWN BY: RTM  
 CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	76
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



**LEGEND:**

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- X10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS
- ▨ PAVEMENT REMOVAL
- ▧ PAVED SHOULDER REMOVAL
- ▩ PAVEMENT/SHOULDER REMOVAL, SPECIAL
- ▦ BITUMINOUS SURFACE REMOVAL, 4"
- ▤ BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)
- ⊙ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE STATION/OFFSET
- XX.XXRT

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

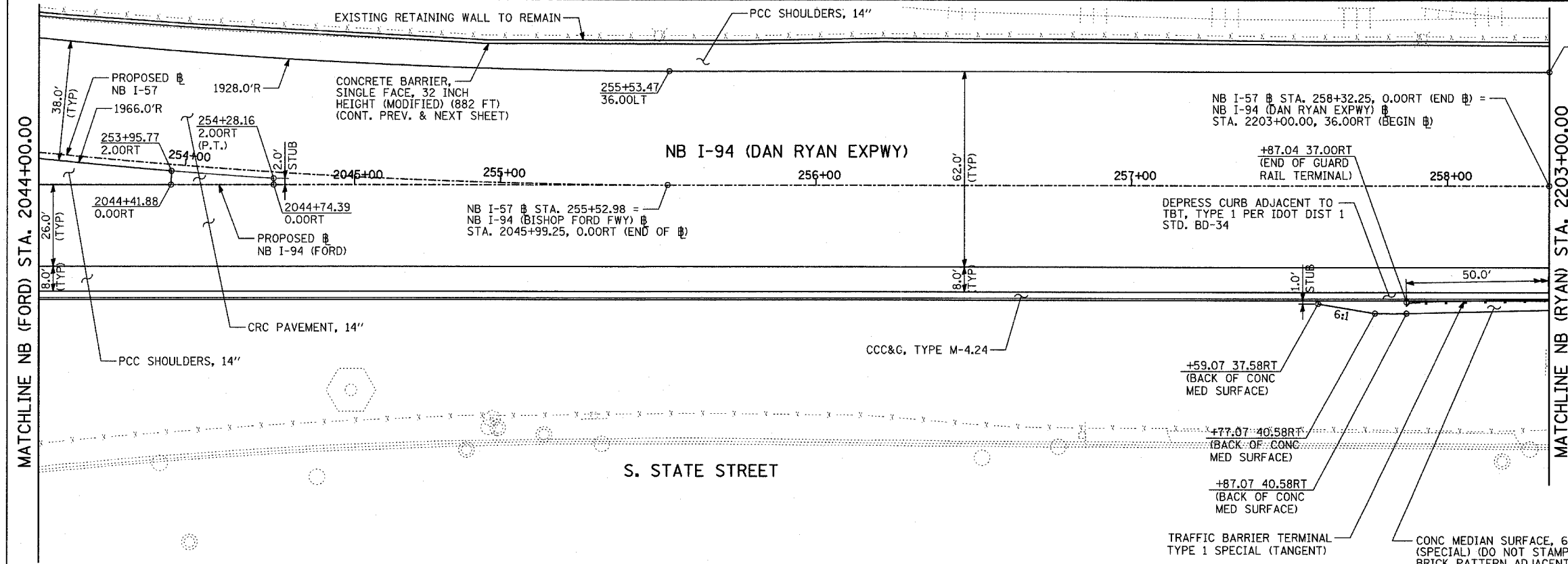
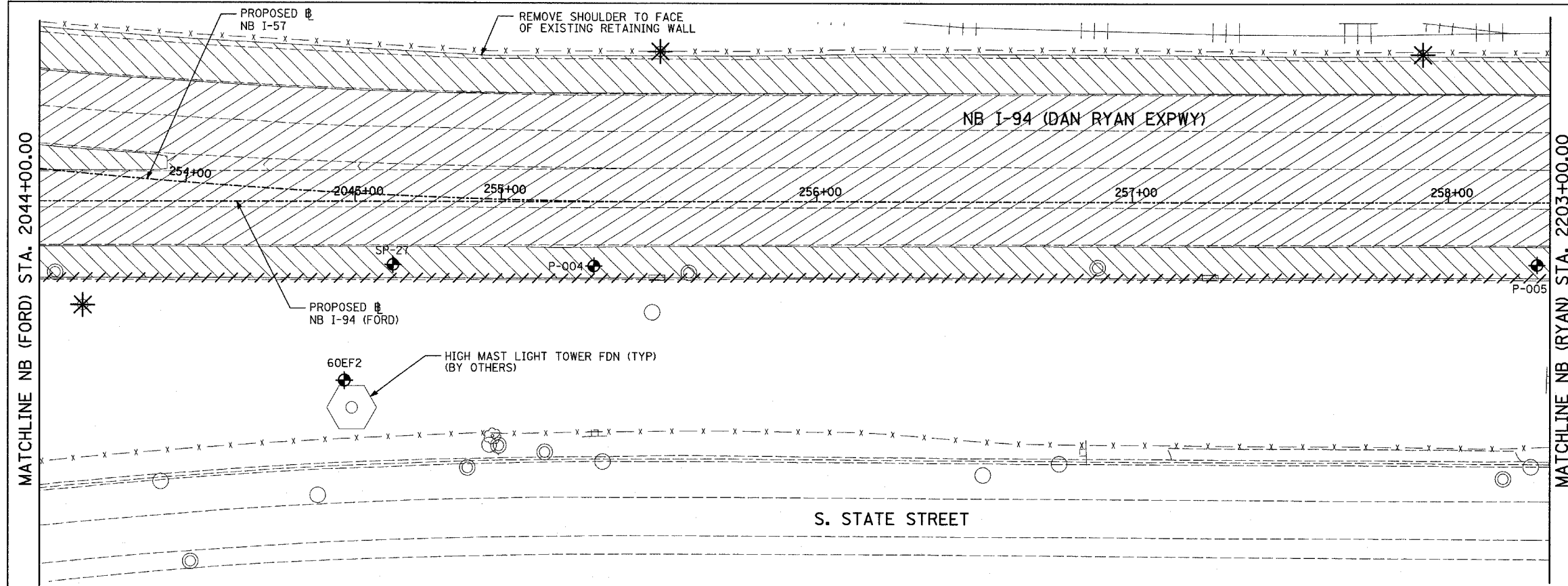
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 NB I-94 (BISHOP FORD FREEWAY)  
 NB STA. 2038+00.00 TO 2044+00.00

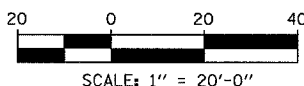
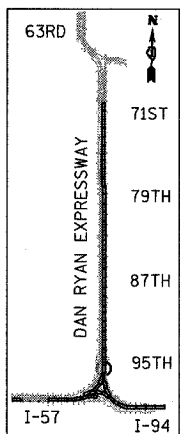
SCALE: 1"=20'  
 DATE: MARCH 7, 2006

DRAWN BY: RTM  
 CHECKED BY: MPG

**TYLIN** INTERNATIONAL



EXISTING CONDITIONS  
 PROPOSED IMPROVEMENTS



**LEGEND:**

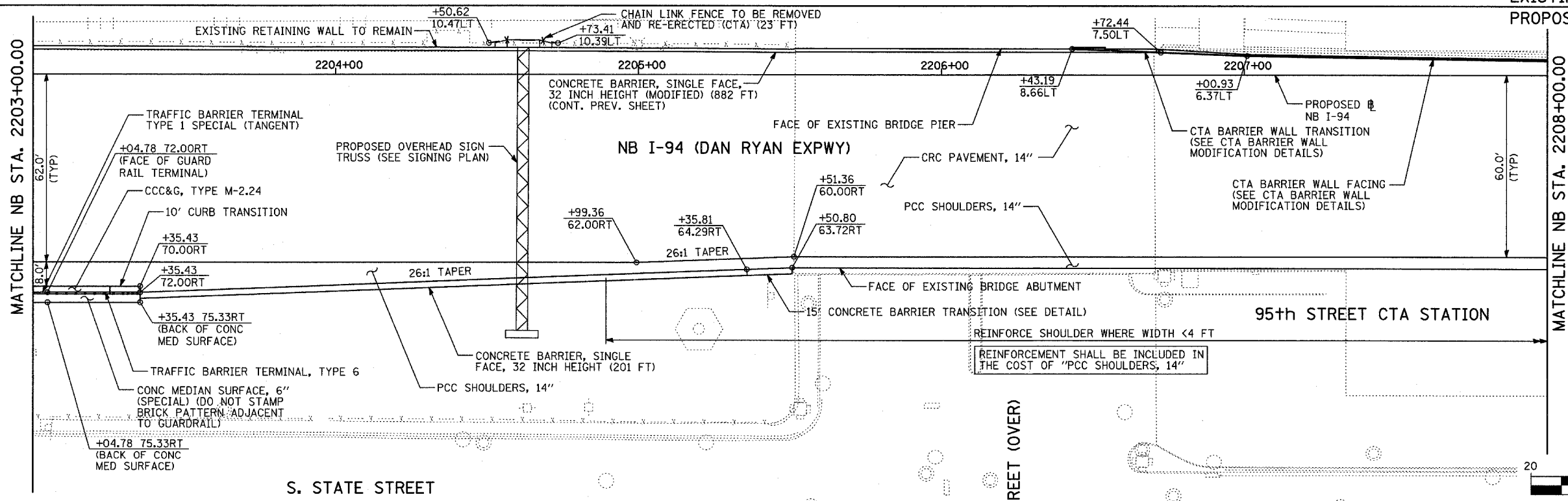
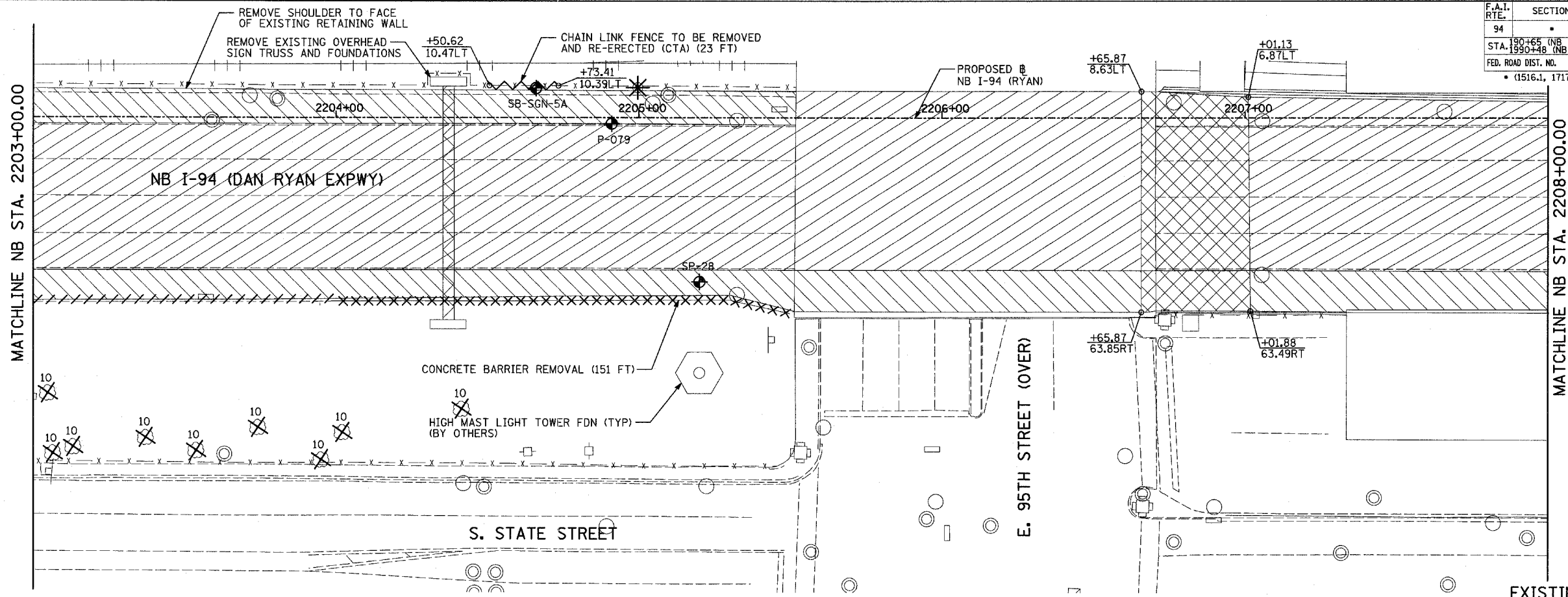
	COMB CONC CURB & GUTTER REMOVAL		PAVEMENT REMOVAL		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	CHAIN LINK FENCE REMOVAL		PAVED SHOULDER REMOVAL		MAINLINE # STATION/OFFSET
	CONCRETE BARRIER/GUARDRAIL REMOVAL		PAVEMENT/SHOULDER REMOVAL, SPECIAL	<b>PLAN NOTES:</b>	
	TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)		BITUMINOUS SURFACE REMOVAL, 4"	- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.	
	SOIL BORING LOCATIONS		BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)	- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED	

**TYLIN INTERNATIONAL**

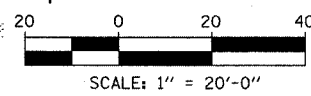
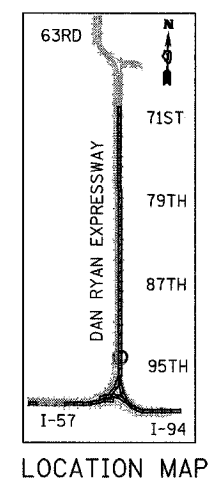
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 NB I-94 (BISHOP FORD FREEWAY) AND  
 I-94 (DAN RYAN EXPRESSWAY) AND NB I-57  
 NB (FORD) STA. 2044+00.00 TO  
 NB (RYAN) STA. 2203+00.00  
 SCALE: 1"=20'  
 DATE: MARCH 7, 2006  
 DRAWN BY: RTM  
 CHECKED BY: MPG





EXISTING CONDITIONS  
 PROPOSED IMPROVEMENTS



**LEGEND:**

- |        |                                    |        |                                                            |                                                                                           |                                                                           |
|--------|------------------------------------|--------|------------------------------------------------------------|-------------------------------------------------------------------------------------------|---------------------------------------------------------------------------|
| ////// | COMB CONC CURB & GUTTER REMOVAL    | *****  | CONCRETE BARRIER/GUARDRAIL REMOVAL                         | ✱                                                                                         | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |
| ~~~~~  | CHAIN LINK FENCE REMOVAL           | XXXXXX | CONCRETE BARRIER/GUARDRAIL REMOVAL                         | +XX.XX                                                                                    | MAINLINE B                                                                |
| XXXXXX | CONCRETE BARRIER/GUARDRAIL REMOVAL | ✱      | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)                  | XX.XXRT                                                                                   | STATION/OFFSET                                                            |
| ✱      | SOIL BORING LOCATIONS              | XXXXXX | BITUMINOUS SURFACE REMOVAL, 4"                             | <b>PLAN NOTES:</b>                                                                        |                                                                           |
|        |                                    | XXXXXX | BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT) | - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |                                                                           |
|        |                                    |        |                                                            | - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED                        |                                                                           |

**TYLIN INTERNATIONAL**

REVISIONS	
NAME	DATE

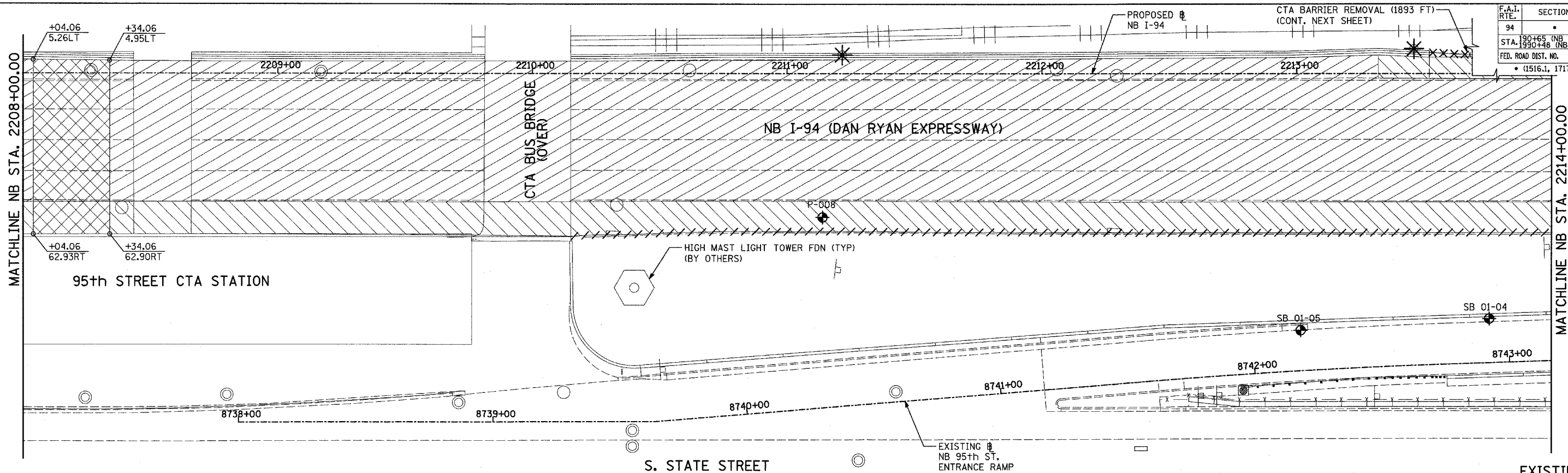
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 NB I-57 AND I-94 (DAN RYAN EXPRESSWAY)  
 NB I-94 (RYAN) STA. 2203+00.00 TO  
 NB I-94 (RYAN) STA. 2208+00.00

SCALE: 1"=20'  
 DATE: MARCH 7, 2006

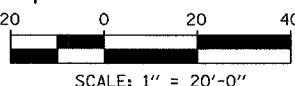
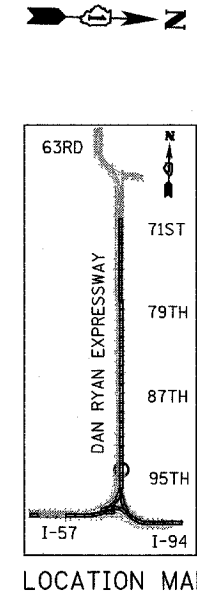
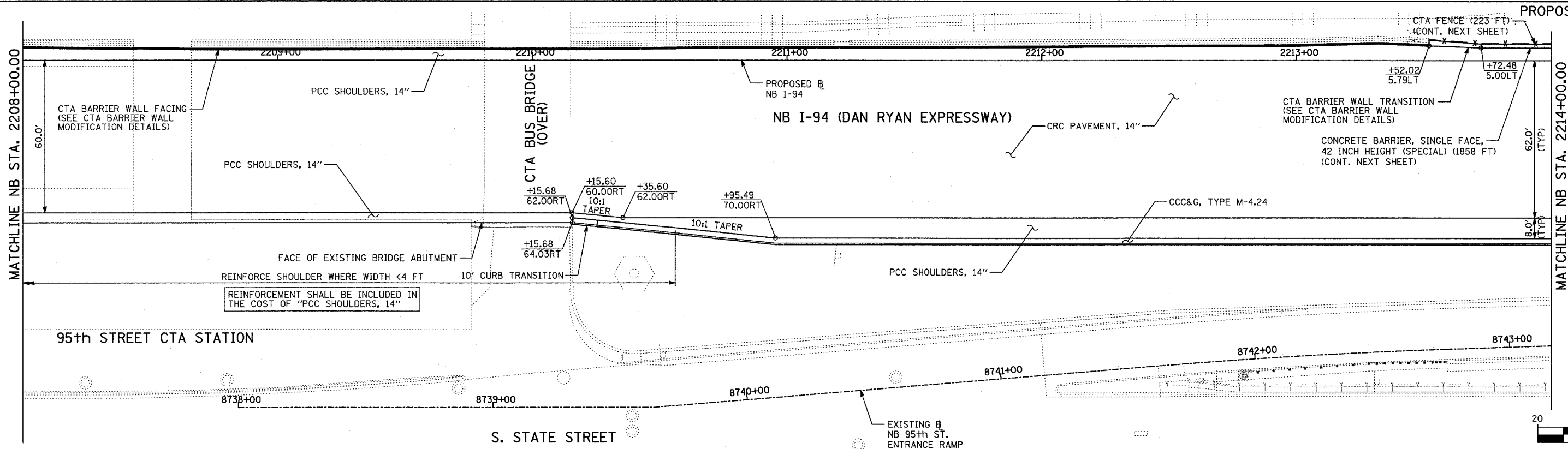
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 CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	79
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				

62304



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



**LEGEND:**

- ===== COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = 6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched] PAVEMENT REMOVAL
- [Diagonal] PAVED SHOULDER REMOVAL
- [Cross-hatched] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Dotted] BITUMINOUS SURFACE REMOVAL, 4"
- [Dotted] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE Ⓢ
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN** INTERNATIONAL

REVISIONS	
NAME	DATE

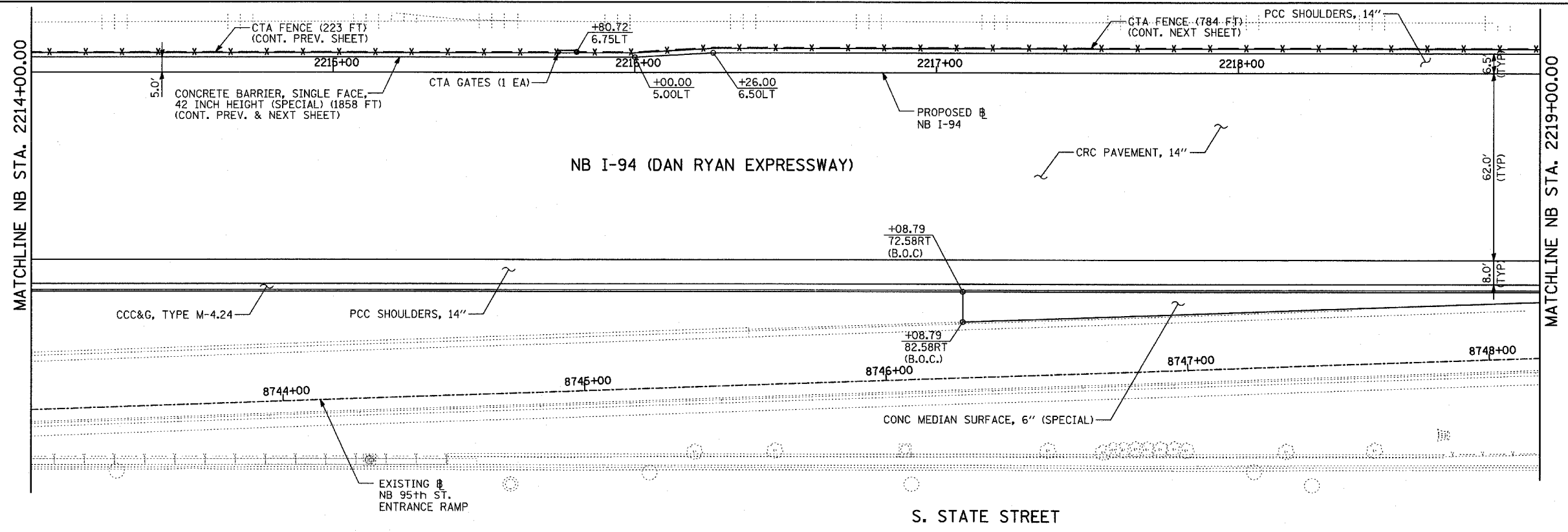
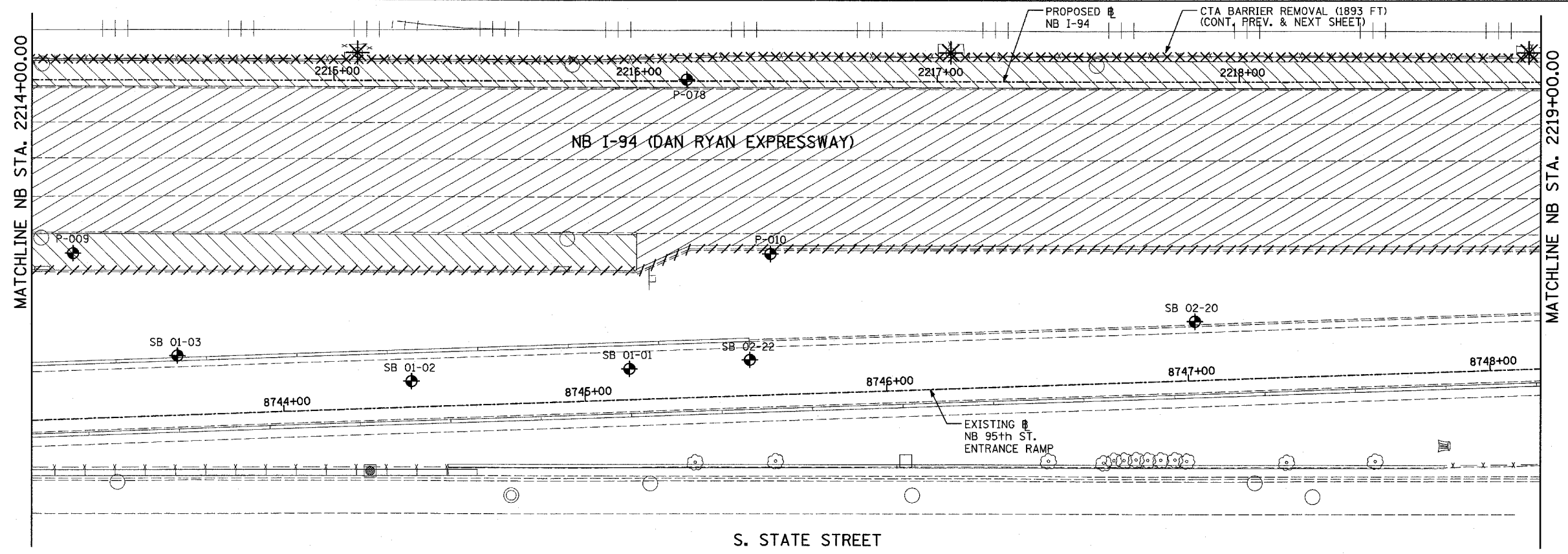
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
 EXISTING AND PROPOSED PLAN  
 NB I-94 (DAN RYAN EXPRESSWAY)  
 NB I-94 STA. 2208+00.00 TO 2214+00.00

SCALE: 1"=20'  
 DATE: MARCH 7, 2006

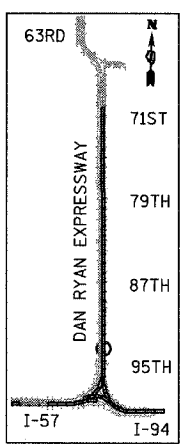
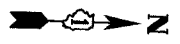
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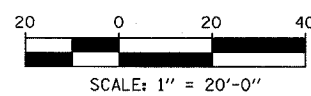
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	80
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



LOCATION MAP



**LEGEND:**

- ##### COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS
- [Diagonal Hatching] PAVEMENT REMOVAL
- [Diagonal Back Hatching] PAVED SHOULDER REMOVAL
- [Cross Hatching] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Horizontal Dashed] BITUMINOUS SURFACE REMOVAL, 4"
- [Vertical Dashed] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE B
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

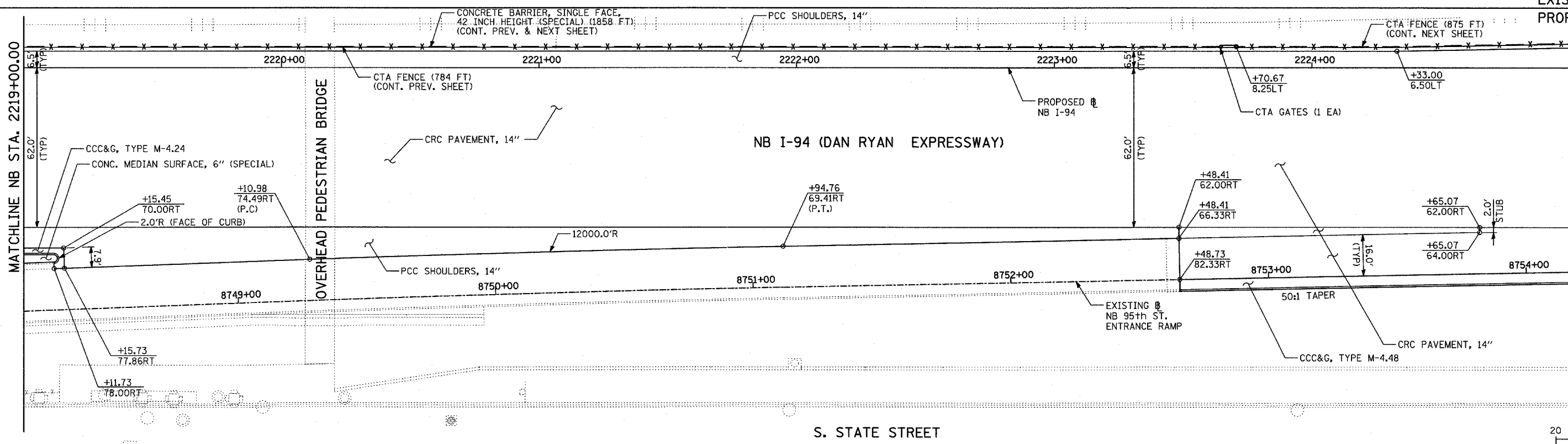
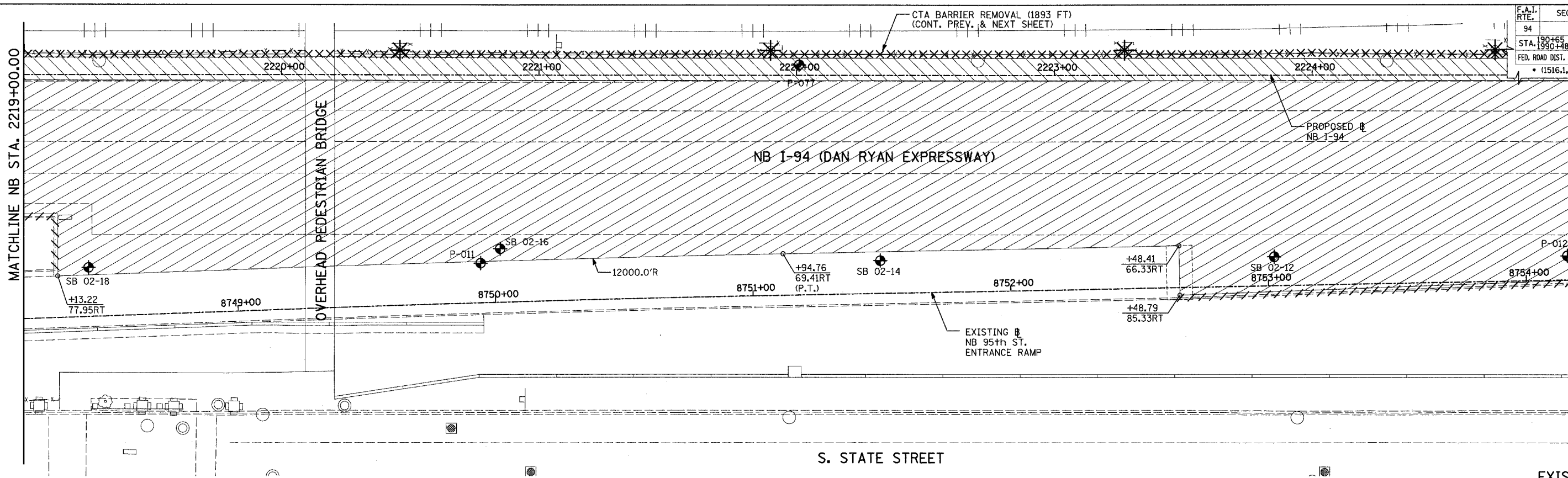
**TYLIN INTERNATIONAL**

REVISIONS	
NAME	DATE

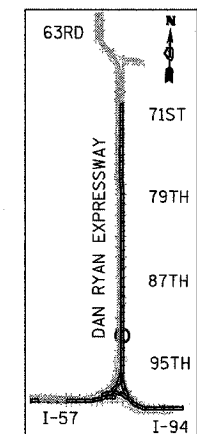
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-94 (DAN RYAN EXPRESSWAY)  
NB I-94 STA. 2214+00.00 TO 2219+00.00

SCALE: 1"=20'  
DATE: MARCH 7, 2006

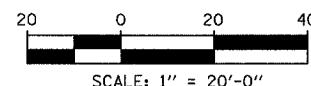
DRAWN BY: RTM  
CHECKED BY: MPG



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



LOCATION MAP



**LEGEND:**

- ===== COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Diagonal Hatched Box] PAVED SHOULDER REMOVAL
- [Cross-hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Horizontal Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Vertical Hatched Box] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- ⊛ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE Ⓢ
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

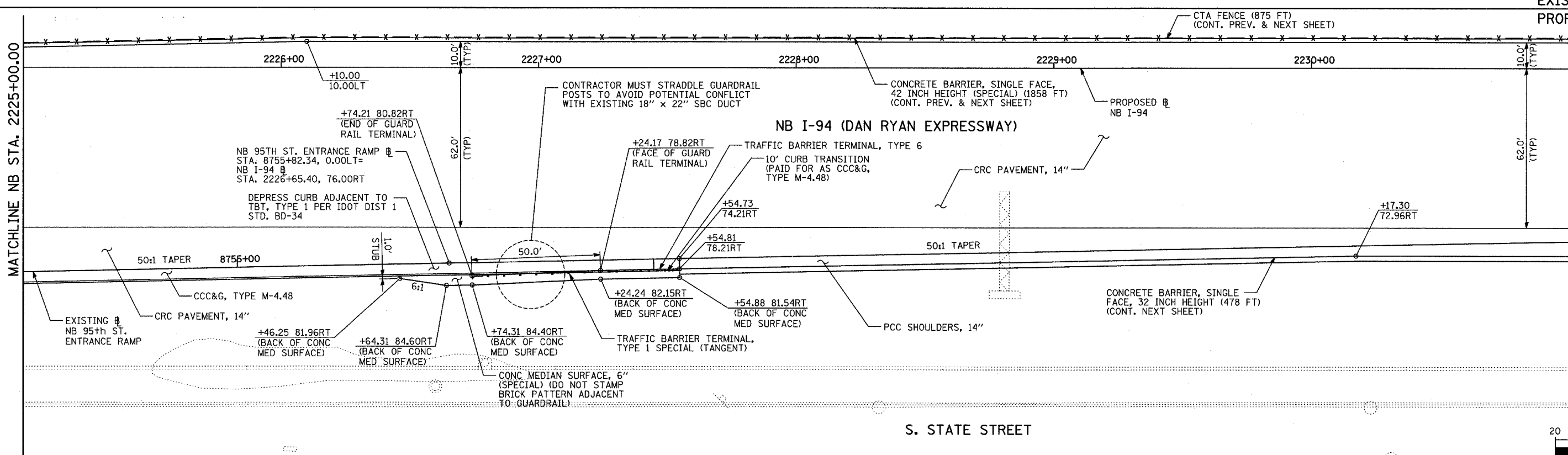
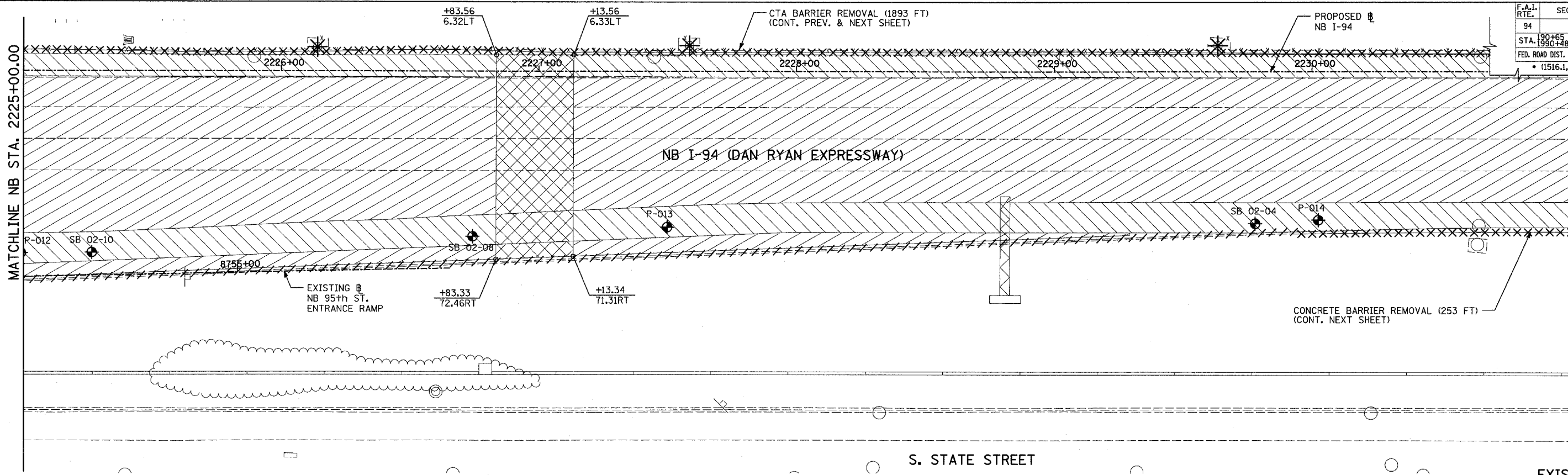
**TYLIN INTERNATIONAL**

REVISIONS	
NAME	DATE

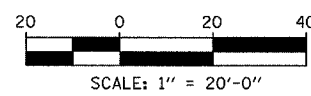
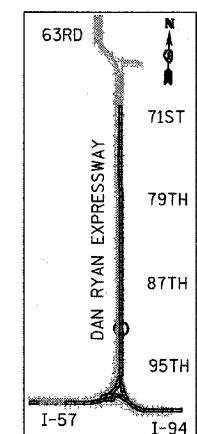
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-94 (DAN RYAN EXPRESSWAY)  
NB I-94 STA. 2219+00.00 TO 2225+00.00

SCALE: 1"=20'  
DATE: MARCH 7, 2006  
DRAWN BY: RTM  
CHECKED BY: MPG

F.A.I. RTE. 94	SECTION	COUNTY COOK	TOTAL SHEETS 916	SHEET NO. 82
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-4 62304				



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



**LEGEND:**

- ==== COMB CONC CURB & GUTTER REMOVAL
- ~~~~ CHAIN LINK FENCE REMOVAL
- XXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = 6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched] PAVEMENT REMOVAL
- [Diagonal] PAVED SHOULDER REMOVAL
- [Cross-hatched] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Dotted] BITUMINOUS SURFACE REMOVAL, 4"
- [Dotted] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- ⊗ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
  - +XX.XX MAINLINE #
  - XX.XXRT STATION/OFFSET
- PLAN NOTES:**
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
  - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN** INTERNATIONAL

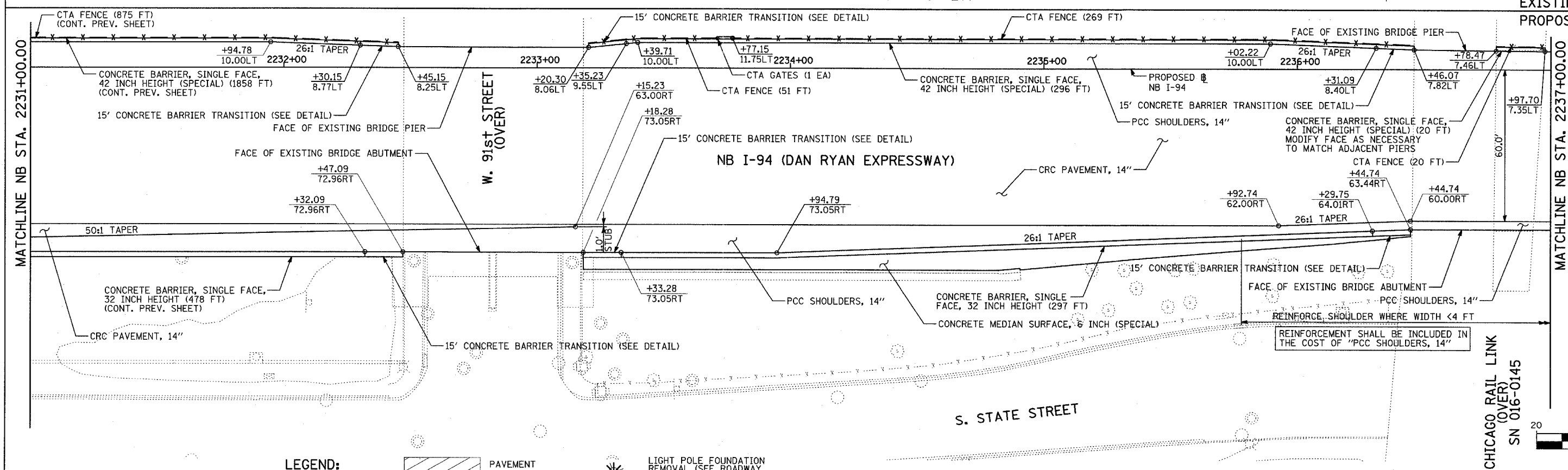
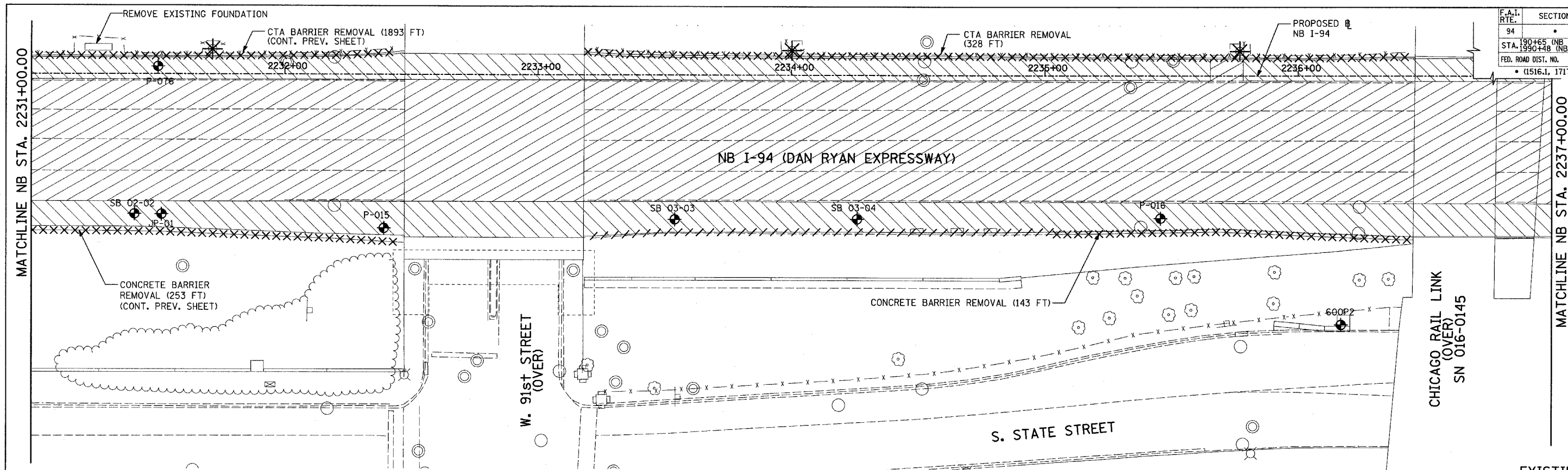
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-94 (DAN RYAN EXPRESSWAY)  
NB I-94 STA. 2225+00.00 TO 2231+00.00

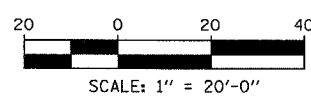
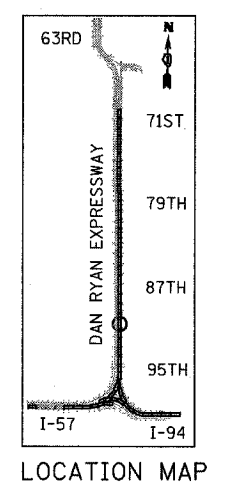
SCALE: 1"=20'  
DATE: MARCH 7, 2006

DRAWN BY: RTM  
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	83
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-4				



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



**LEGEND:**

=====	COMB CONC CURB & GUTTER REMOVAL	[Hatched Box]	PAVEMENT REMOVAL
~~~~~	CHAIN LINK FENCE REMOVAL	[Diagonal Hatched Box]	PAVED SHOULDER REMOVAL
XXXXXX	CONCRETE BARRIER/GUARDRAIL REMOVAL	[Cross-hatched Box]	PAVEMENT/SHOULDER REMOVAL, SPECIAL
⊗10	TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)	[Diagonal Hatched Box]	BITUMINOUS SURFACE REMOVAL, 4"
⊕	SOIL BORING LOCATIONS	[Diagonal Hatched Box]	BITUMINOUS SURFACE REMOVAL, 1/2" OR 1/4" (SEE CALLOUT)

PLAN NOTES:

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

REVISIONS	
NAME	DATE

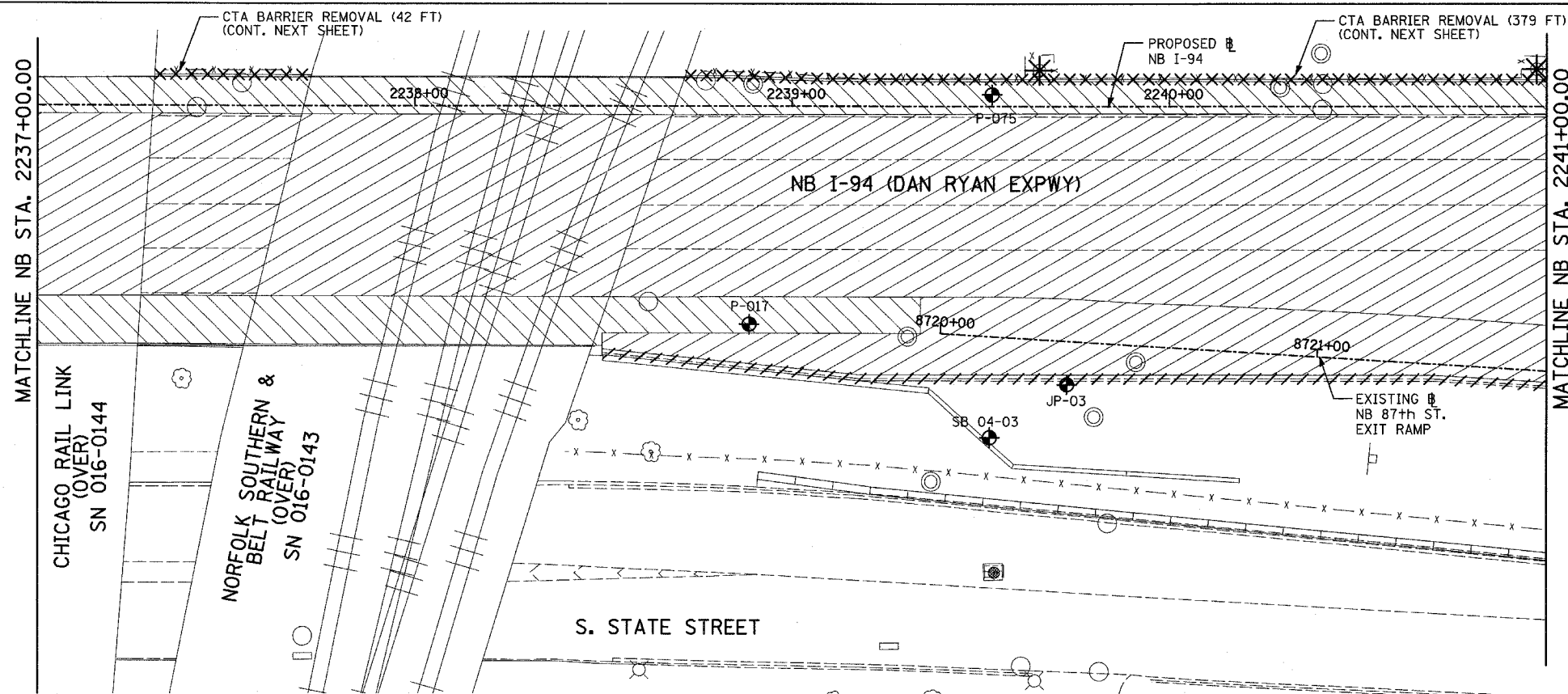
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
NB I-94 STA. 2231+00.00 TO 2237+00.00

SCALE: 1"=20'
DATE: MARCH 7, 2006

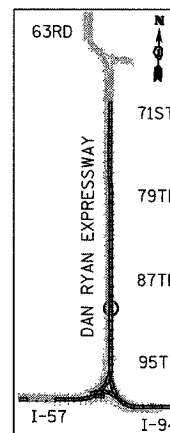
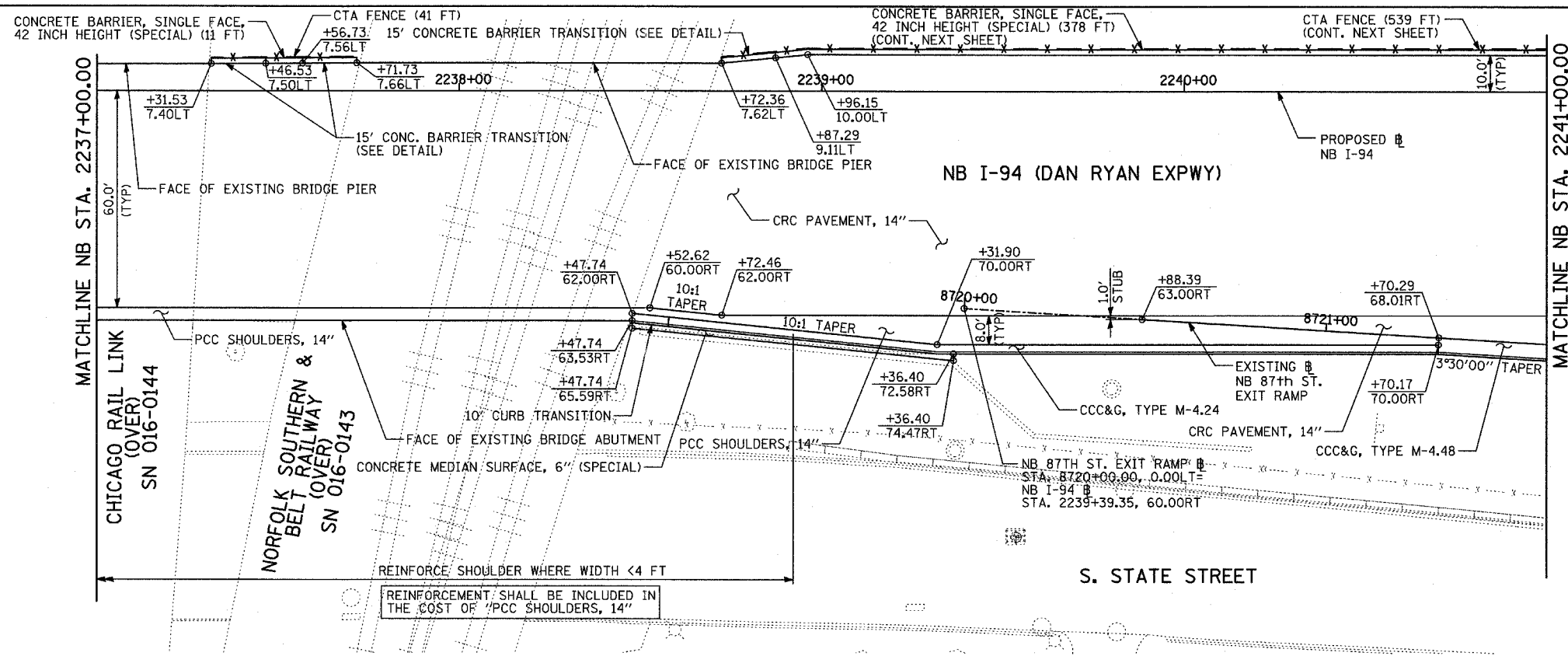
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CHECKED BY: MPG

TYLIN INTERNATIONAL

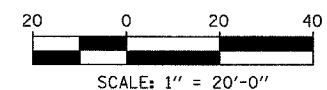
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	84
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP



LEGEND:

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- X10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Hatched Box] PAVED SHOULDER REMOVAL
- [Hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- [Star Symbol] LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE B
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

TYLIN INTERNATIONAL

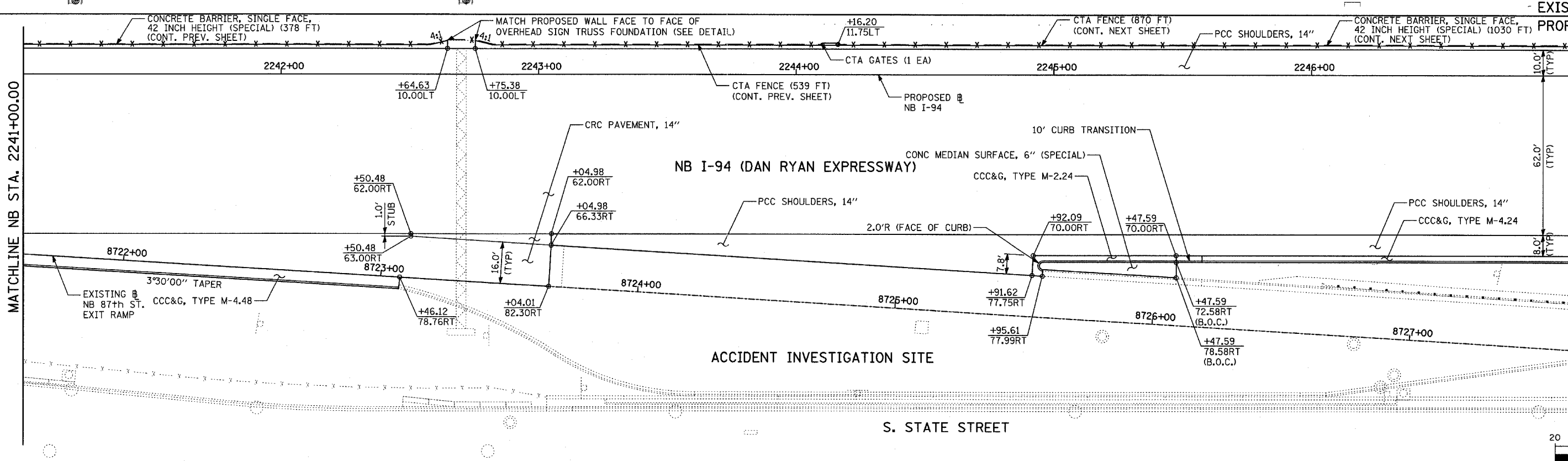
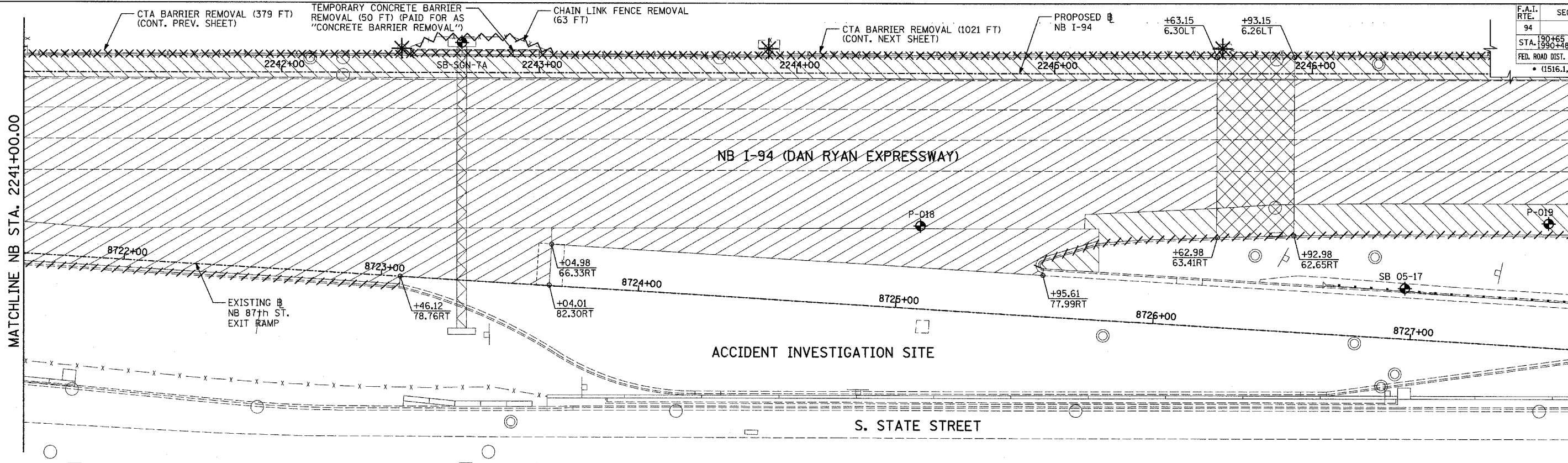
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
NB I-94 STA. 2237+00.00 TO 2241+00.00

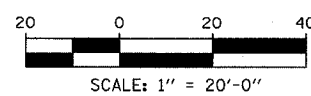
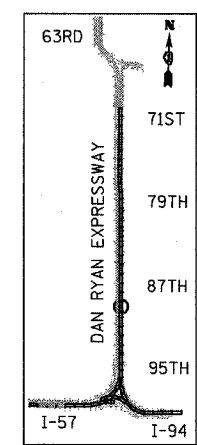
SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: RTM
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	85
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LEGEND:

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- X10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ◆ SOIL BORING LOCATIONS
- PAVEMENT REMOVAL
- PAVED SHOULDER REMOVAL
- PAVEMENT/SHOULDER REMOVAL, SPECIAL
- BITUMINOUS SURFACE REMOVAL, 4"
- BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)
- ★ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

TYLIN INTERNATIONAL

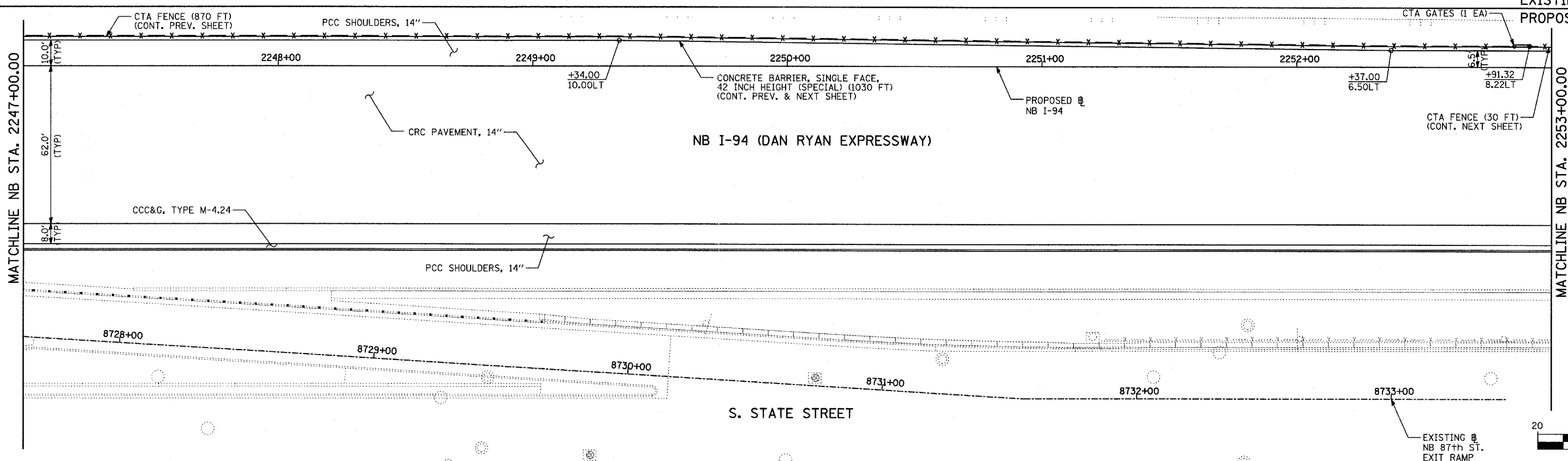
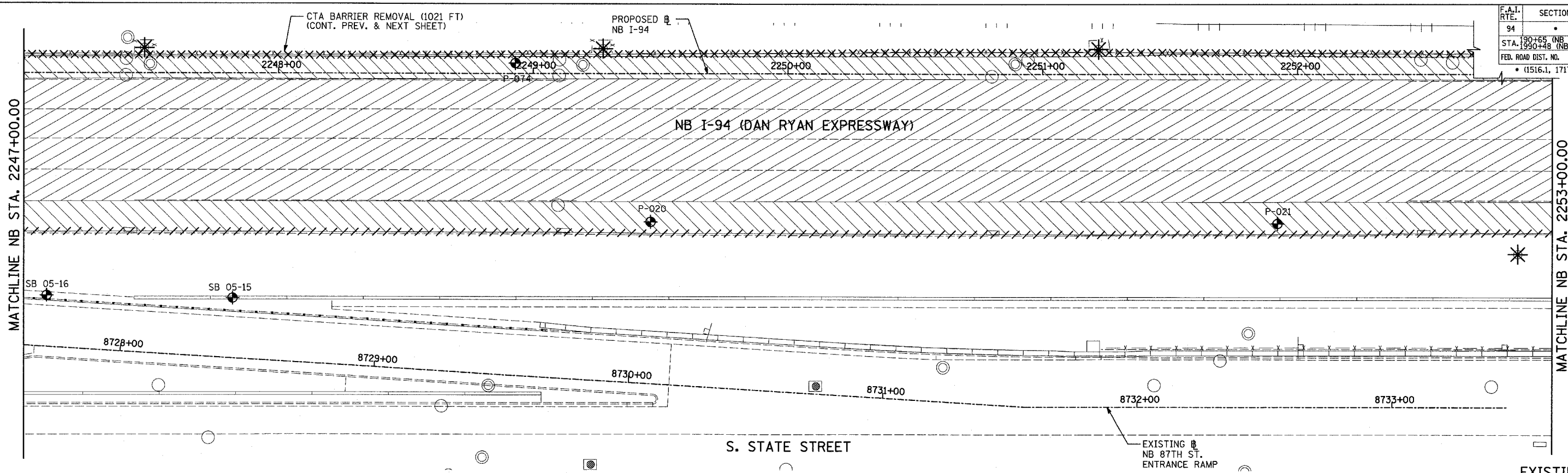
SHEET 22 OF 47

REVISIONS	
NAME	DATE

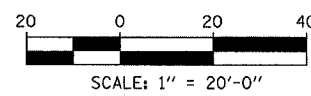
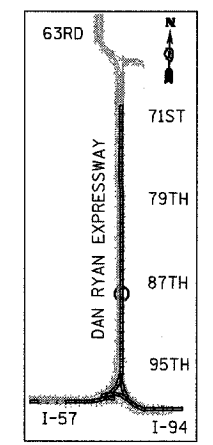
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
NB I-94 STA. 2241+00.00 TO 2247+00.00

SCALE: 1"=20'
DATE: MARCH 7, 2006
DRAWN BY: RTM
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	86
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LEGEND:

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Diagonal Hatched Box] PAVED SHOULDER REMOVAL
- [Cross-hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Diagonal Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Diagonal Hatched Box] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- [Star Symbol] LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

TYLIN INTERNATIONAL

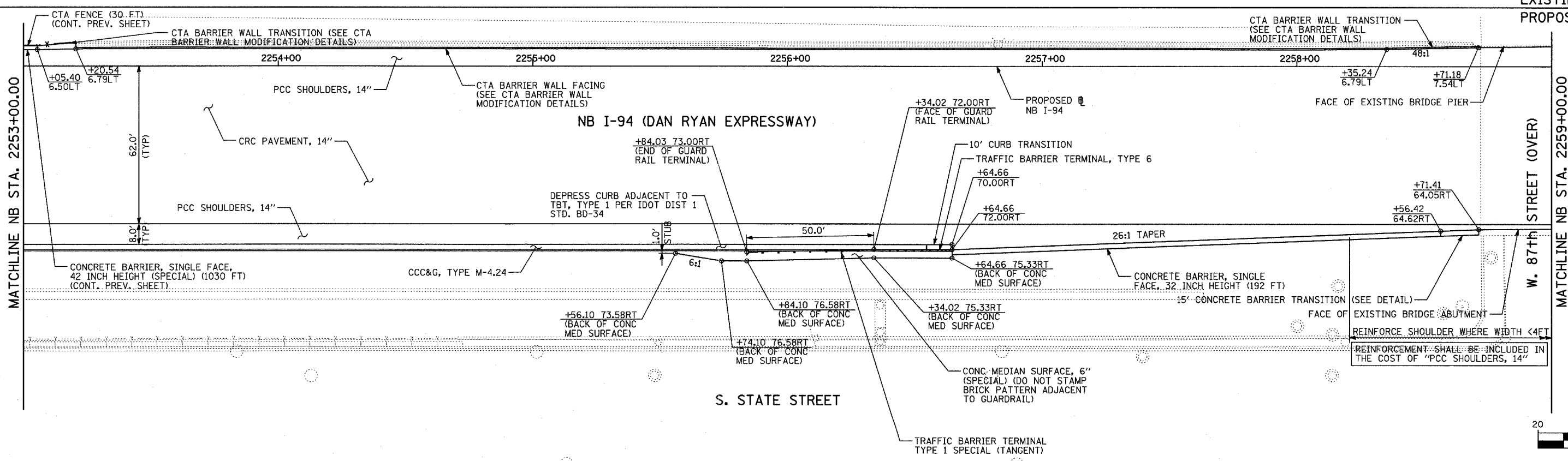
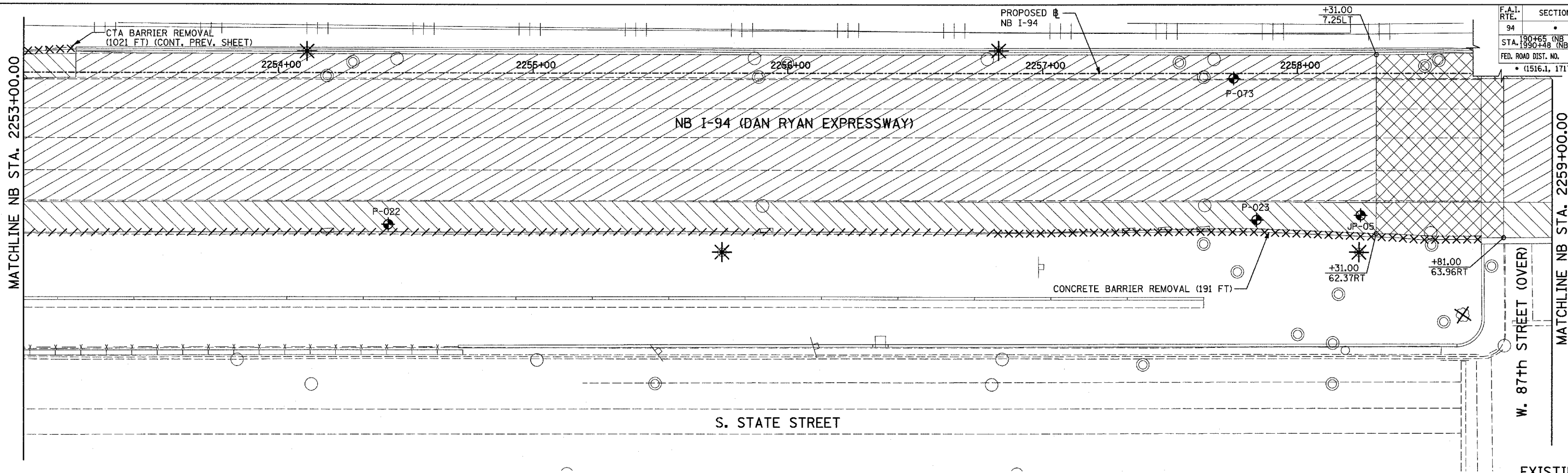
SHEET 23 OF 47

REVISIONS	
NAME	DATE

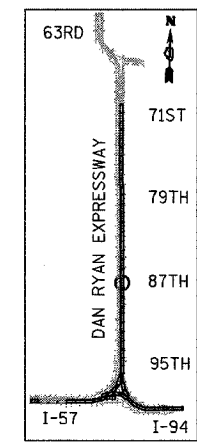
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
NB I-94 STA. 2247+00.00 TO 2253+00.00

SCALE: 1"=20'
DATE: MARCH 7, 2006

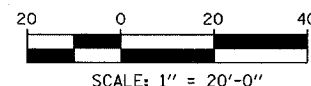
DRAWN BY: RTM
CHECKED BY: MPG



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP



LEGEND:

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ◆ SOIL BORING LOCATIONS
- ▨ PAVEMENT REMOVAL
- ▧ PAVED SHOULDER REMOVAL
- ▩ PAVEMENT/SHOULDER REMOVAL, SPECIAL
- ▦ BITUMINOUS SURFACE REMOVAL, 4"
- ▤ BITUMINOUS SURFACE REMOVAL, 1/2" OR 3/4" (SEE CALLOUT)

- ★ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE B
- XX.XXRT STATION/OFFSET

PLAN NOTES:

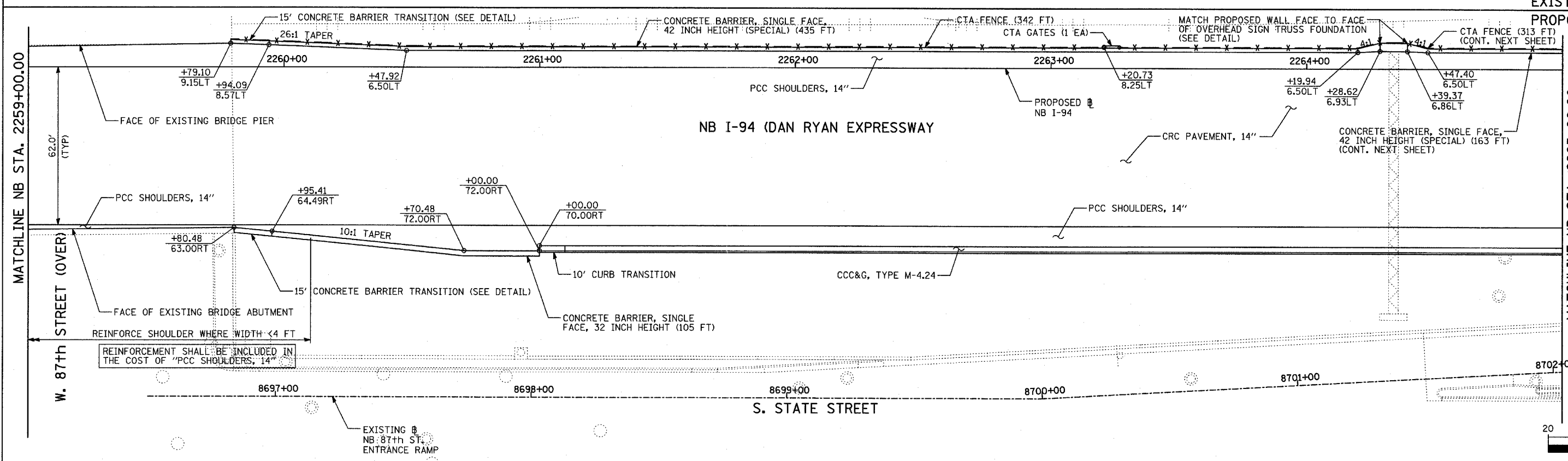
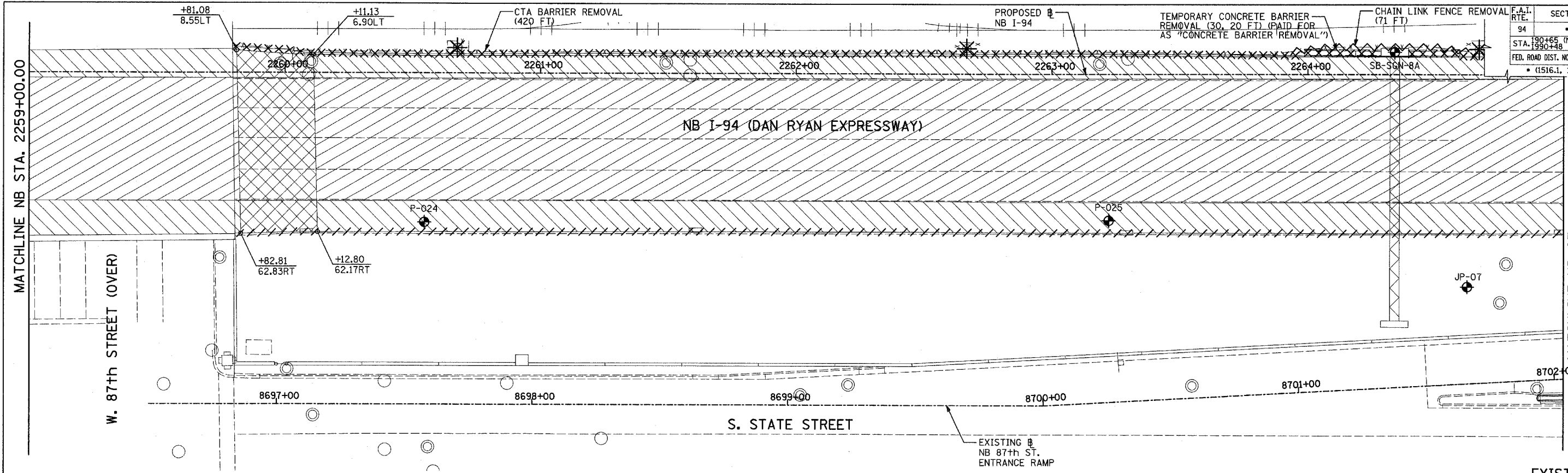
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

REVISIONS	
NAME	DATE

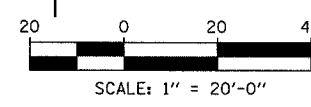
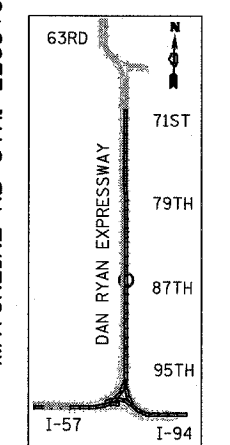
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
NB I-94 STA. 2253+00.00 TO 2259+00.00

SCALE: 1"=20'
DATE: MARCH 7, 2006
DRAWN BY: RTM
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	88
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 6230				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LEGEND:

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- X10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ◆ SOIL BORING LOCATIONS

- PAVEMENT REMOVAL
- PAVED SHOULDER REMOVAL
- PAVEMENT/SHOULDER REMOVAL, SPECIAL
- BITUMINOUS SURFACE REMOVAL, 4"
- BITUMINOUS SURFACE REMOVAL, 1/2" OR 3/4" (SEE CALLOUT)
- LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

TYLIN INTERNATIONAL

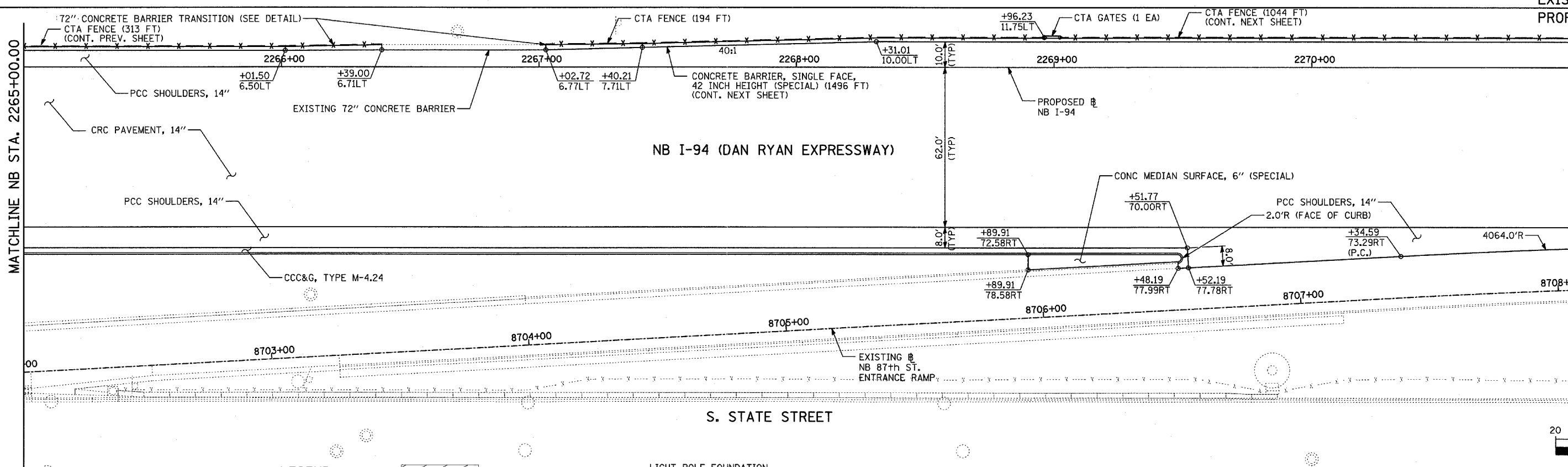
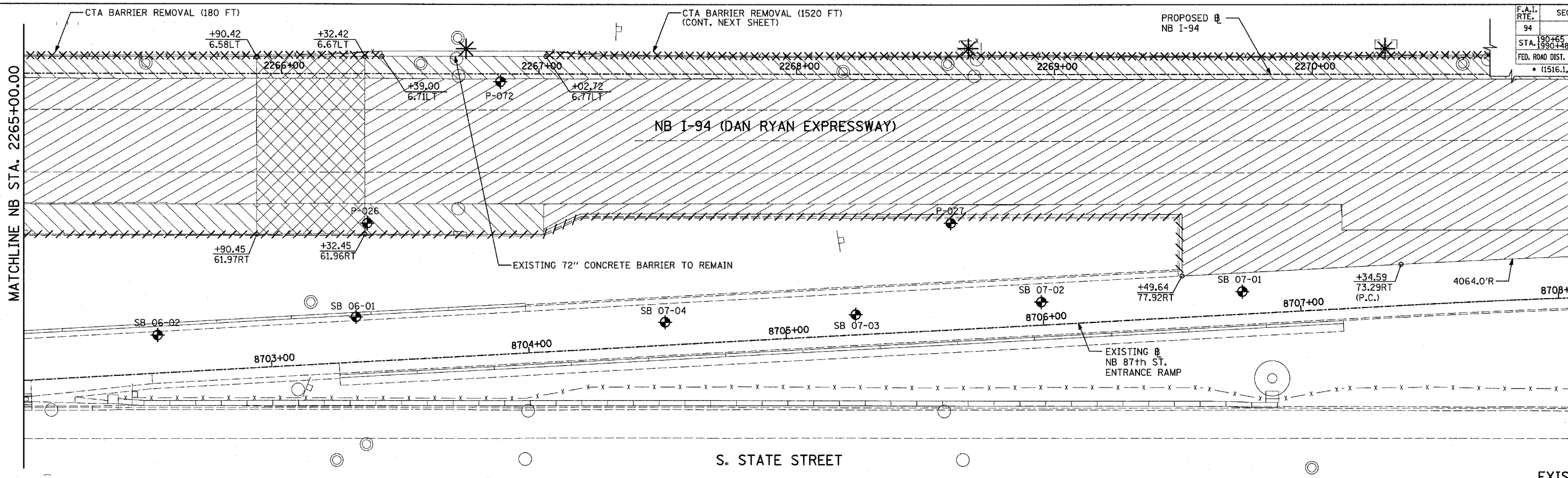
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
NB I-94 (DAN RYAN EXPRESSWAY)
NB I-94 STA. 2259+00.00 TO 2265+00.00

SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: RTM
CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	89
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



MATCHLINE NB STA. 2271+00.00

MATCHLINE NB STA. 2271+00.00

MATCHLINE NB STA. 2265+00.00

LEGEND:

=====	COMB CONC CURB & GUTTER REMOVAL		PAVEMENT REMOVAL
~~~~~	CHAIN LINK FENCE REMOVAL		PAVED SHOULDER REMOVAL
XXXXXX	CONCRETE BARRIER/GUARDRAIL REMOVAL		PAVEMENT/SHOULDER REMOVAL, SPECIAL
	TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)		BITUMINOUS SURFACE REMOVAL, 4"
	SOIL BORING LOCATIONS		BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

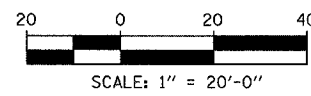
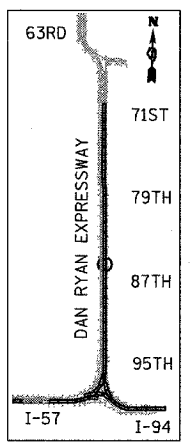
**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**PLAN NOTES:**

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



REVISIONS	
NAME	DATE

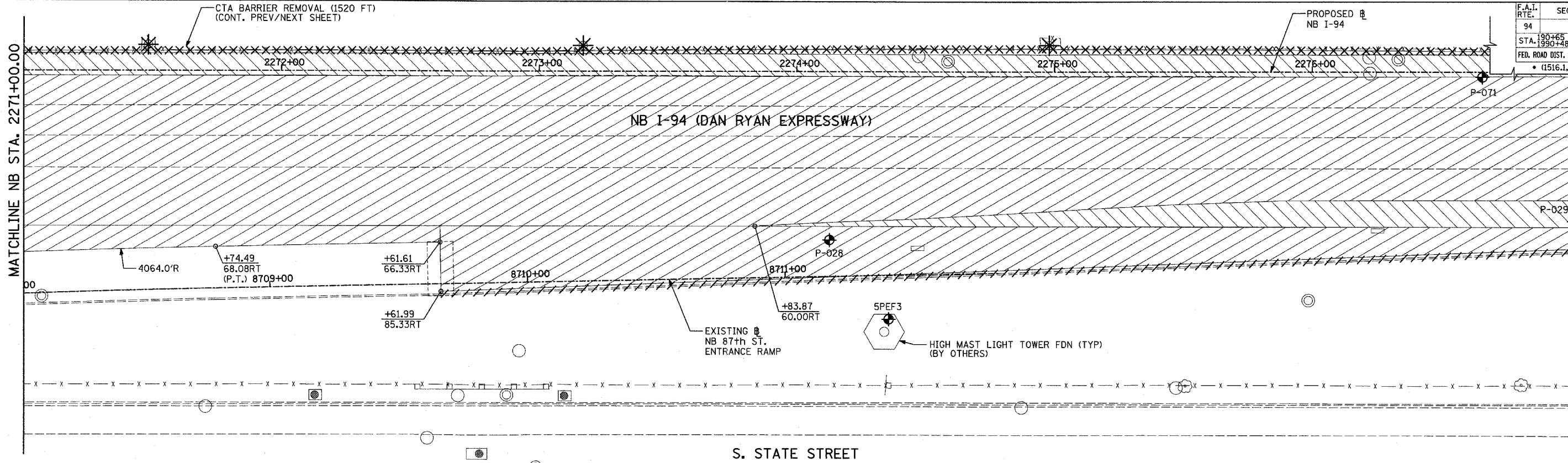
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-94 (DAN RYAN EXPRESSWAY)  
NB I-94 STA. 2265+00.00 TO 2271+00.00

SCALE: 1"=20'  
DATE: MARCH 7, 2006

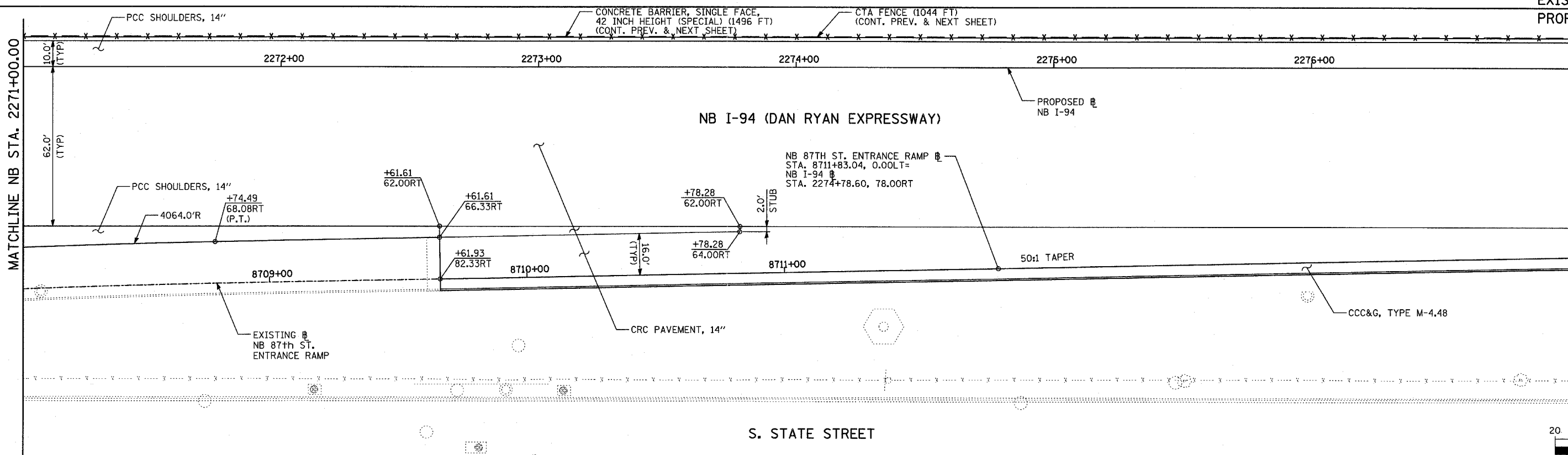
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CHECKED BY: MPG

TYLIN INTERNATIONAL

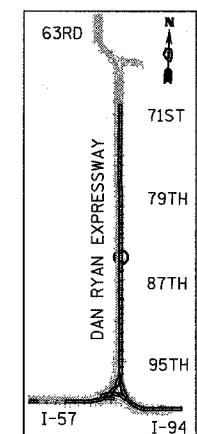
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	90
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 62304				



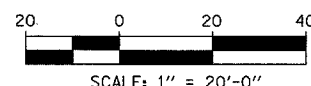
ADDITIONAL LOCATION AT STA. 2276+91.53, OFF. 10' LT



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



LOCATION MAP



**LEGEND:**

- ==== COMB CONC CURB & GUTTER REMOVAL
- ~~~~ CHAIN LINK FENCE REMOVAL
- XXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS
- ▨ PAVEMENT REMOVAL
- ▧ PAVED SHOULDER REMOVAL
- ▩ PAVEMENT/SHOULDER REMOVAL, SPECIAL
- BITUMINOUS SURFACE REMOVAL, 4"
- BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
  - +XX.XX MAINLINE STATION/OFFSET
  - XX.XXRT
- PLAN NOTES:**
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
  - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN INTERNATIONAL**

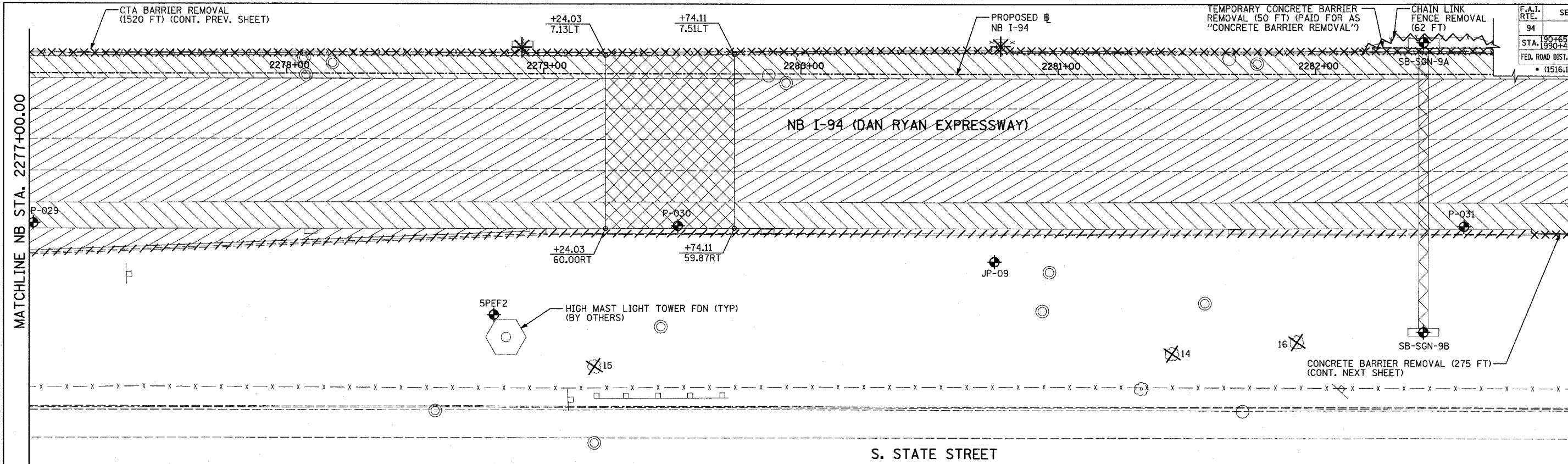
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-94 (DAN RYAN EXPRESSWAY)  
NB I-94 STA. 2271+00.00 TO 2277+00.00

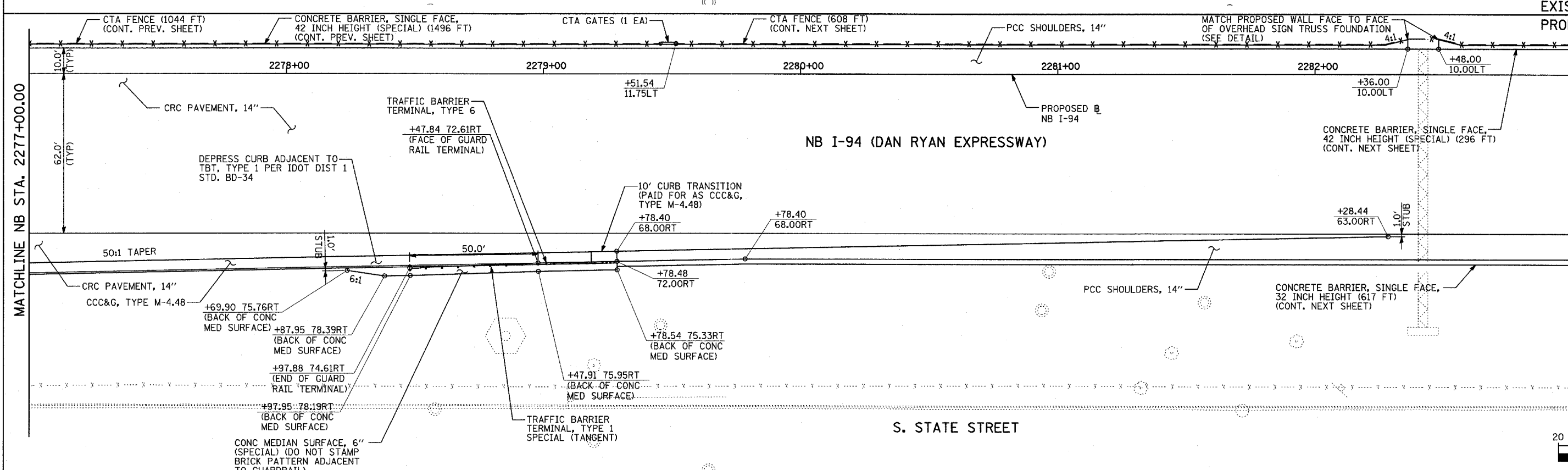
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DATE: MARCH 7, 2006

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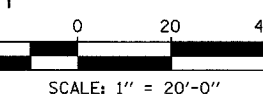
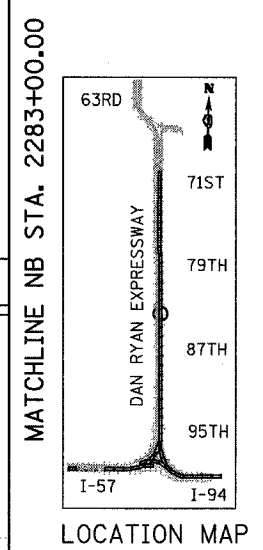
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	91
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 6230				



ADDITIONAL LOCATION AT STA. 2282+83.63, OFF. 10' LT



EXISTING CONDITIONS PROPOSED IMPROVEMENTS



- LEGEND:**
- ////// COMB CONC CURB & GUTTER REMOVAL
  - ~~~~~ CHAIN LINK FENCE REMOVAL
  - ***** CONCRETE BARRIER/GUARDRAIL REMOVAL
  - ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
  - ⊕ SOIL BORING LOCATIONS
  - [Hatched Box] PAVEMENT REMOVAL
  - [Diagonal Hatched Box] PAVED SHOULDER REMOVAL
  - [Cross-hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
  - [Horizontal Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
  - [Vertical Hatched Box] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)
  - ⊗ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
  - +xx.xx MAINLINE #
  - xx.xxRT STATION/OFFSET
- PLAN NOTES:**
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
  - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

REVISIONS	
NAME	DATE

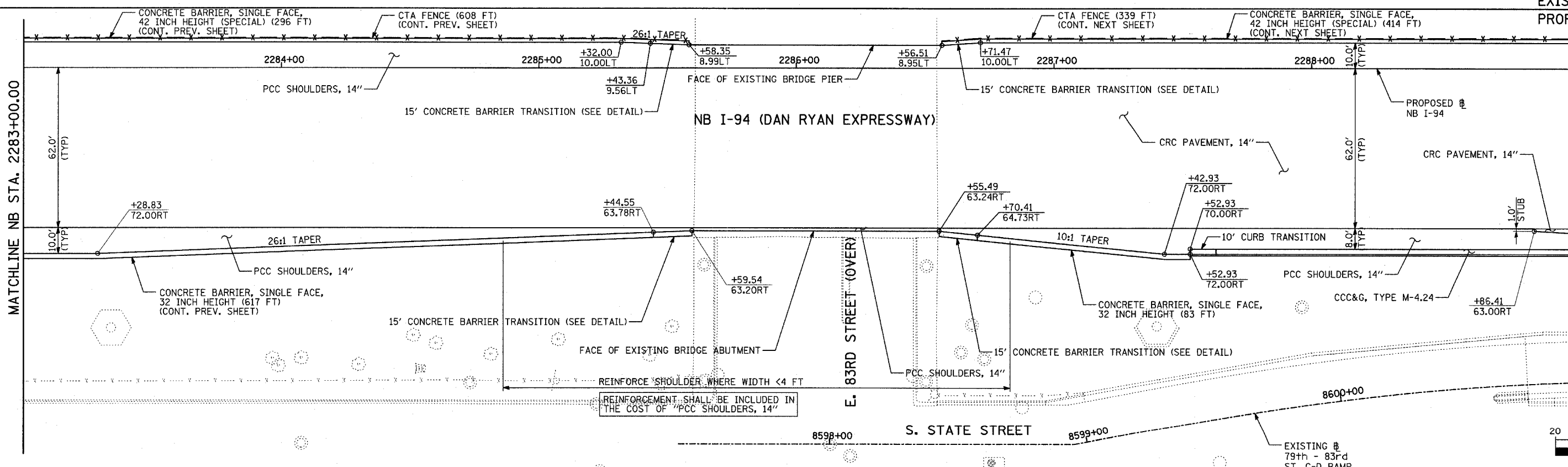
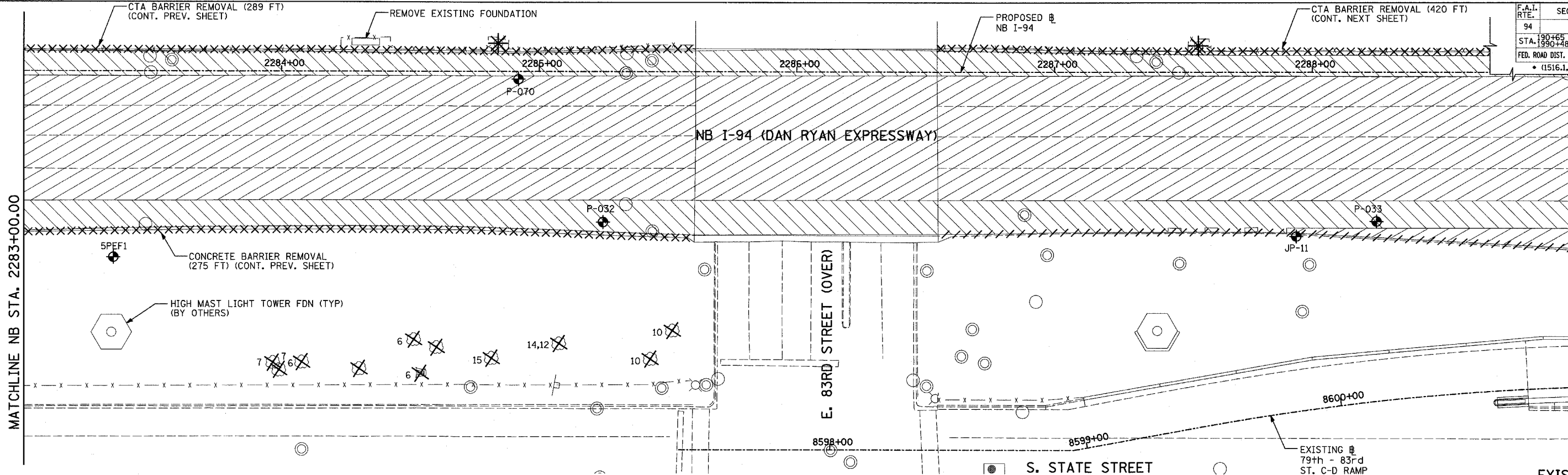
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 NB I-94 (DAN RYAN EXPRESSWAY)  
 NB I-94 STA. 2277+00.00 TO 2283+00.00

SCALE: 1"=20'  
 DATE: MARCH 7, 2006

DRAWN BY: RTM  
 CHECKED BY: MPG

TYLIN INTERNATIONAL



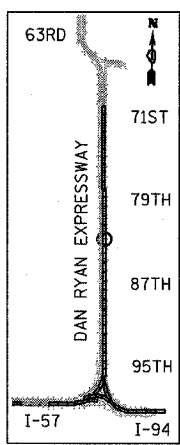


MATCHLINE NB STA. 2289+00.00

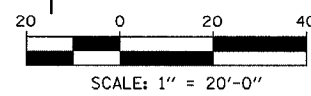
MATCHLINE NB STA. 2289+00.00

MATCHLINE NB STA. 2283+00.00

MATCHLINE NB STA. 2283+00.00



LOCATION MAP



**LEGEND:**

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS
- [Diagonal Hatching] PAVEMENT REMOVAL
- [Diagonal V Hatching] PAVED SHOULDER REMOVAL
- [Cross Hatching] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Horizontal Hatching] BITUMINOUS SURFACE REMOVAL, 4"
- [Vertical Hatching] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- ⊗ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

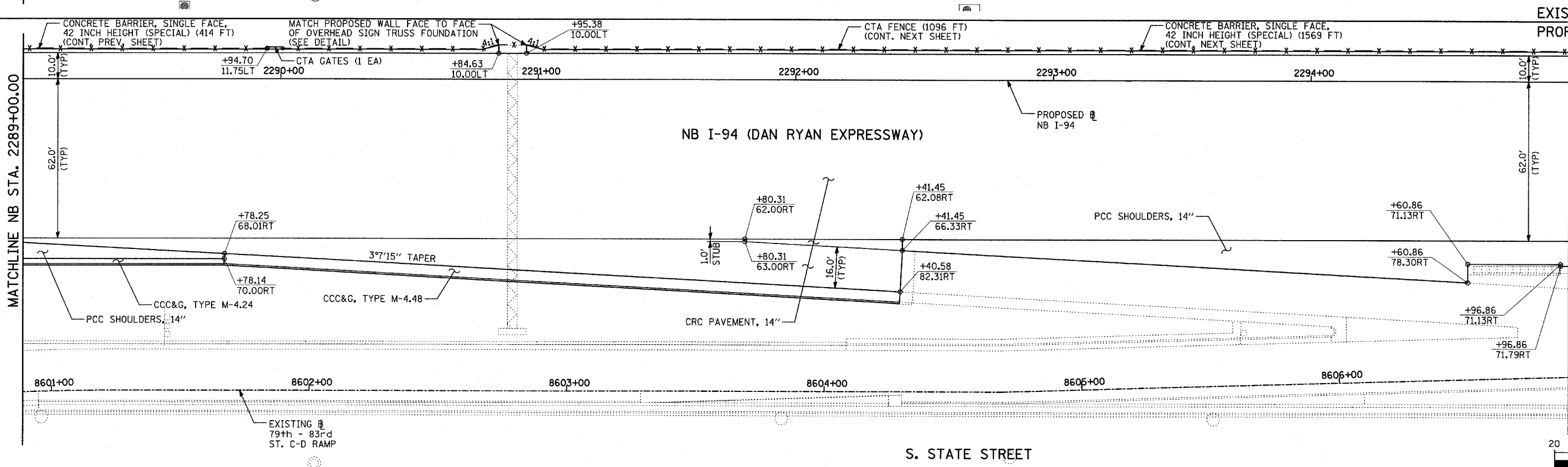
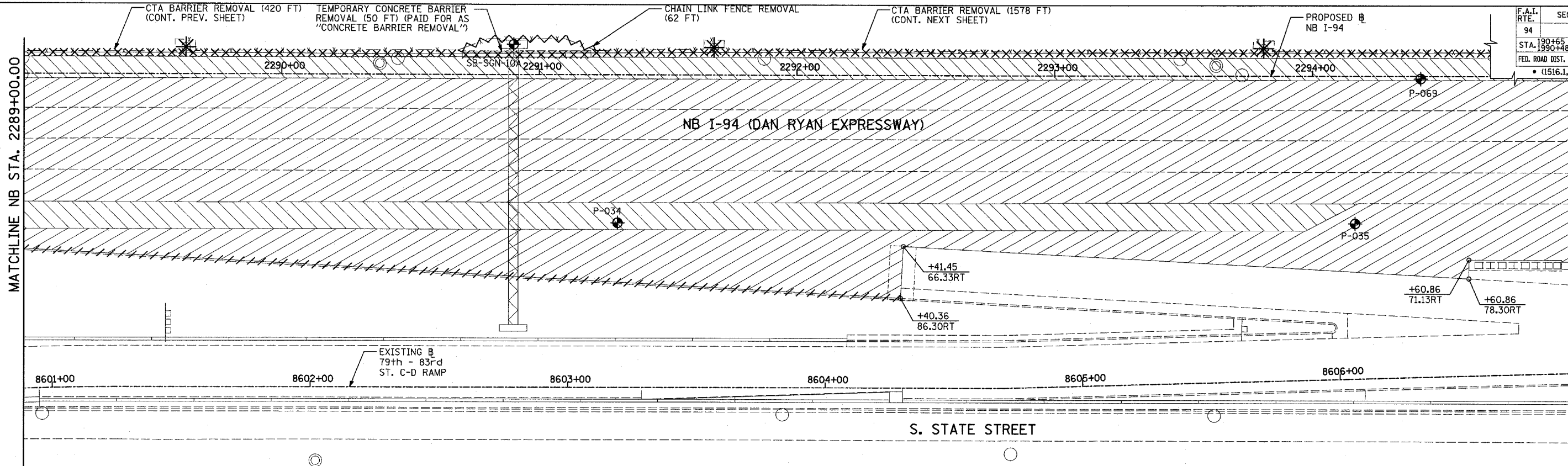
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

REVISIONS	
NAME	DATE

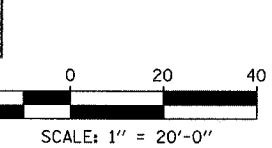
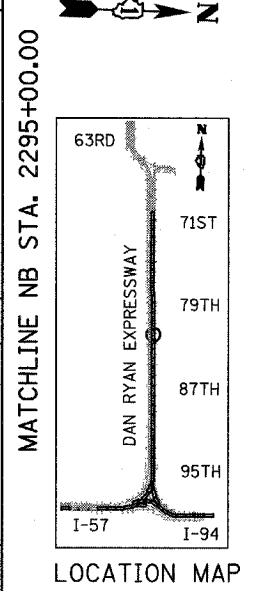
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 NB I-94 (DAN RYAN EXPRESSWAY)  
 NB I-94 STA. 2283+00.00 TO 2289+00.00

SCALE: 1"=20'  
 DATE: MARCH 7, 2006  
 DRAWN BY: RTM  
 CHECKED BY: MPG

**TYLIN INTERNATIONAL**



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



**LEGEND:**

	COMB CONC CURB & GUTTER REMOVAL		PAVEMENT REMOVAL
	CHAIN LINK FENCE REMOVAL		PAVED SHOULDER REMOVAL
	CONCRETE BARRIER/GUARDRAIL REMOVAL		PAVEMENT/SHOULDER REMOVAL, SPECIAL
	TREE REMOVAL (UNITS) (NO UNIT SHOWN = 6)		BITUMINOUS SURFACE REMOVAL, 4"
	SOIL BORING LOCATIONS		BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

**PLAN NOTES:**

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)**

+XX.XX MAINLINE #  
XX.XXRT STATION/OFFSET

**TYLIN INTERNATIONAL**

REVISIONS	
NAME	DATE

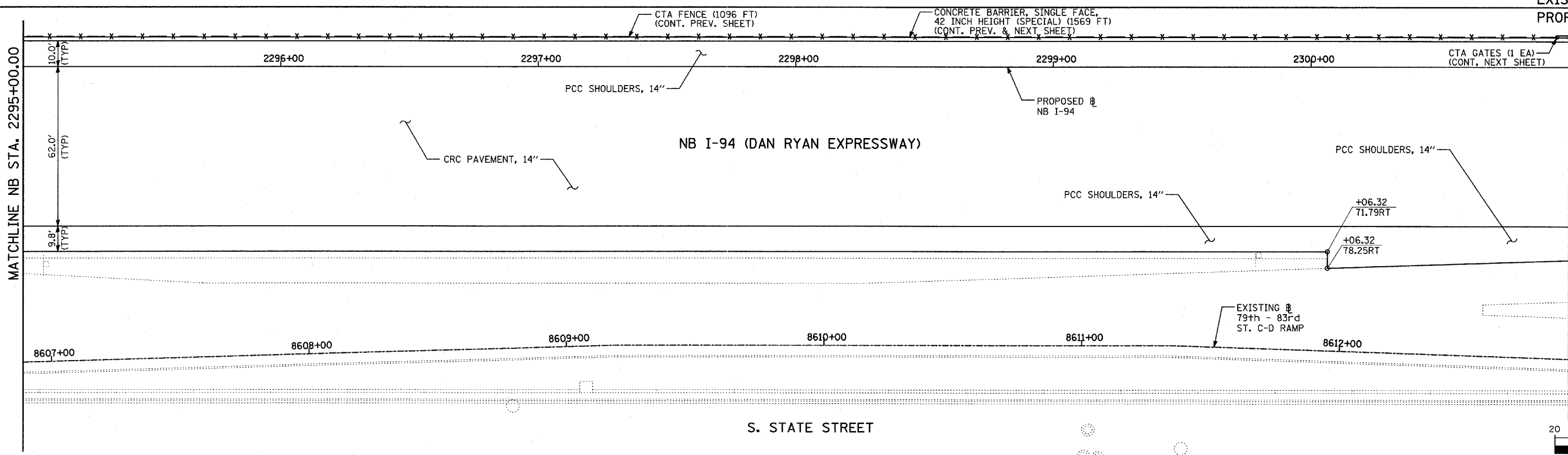
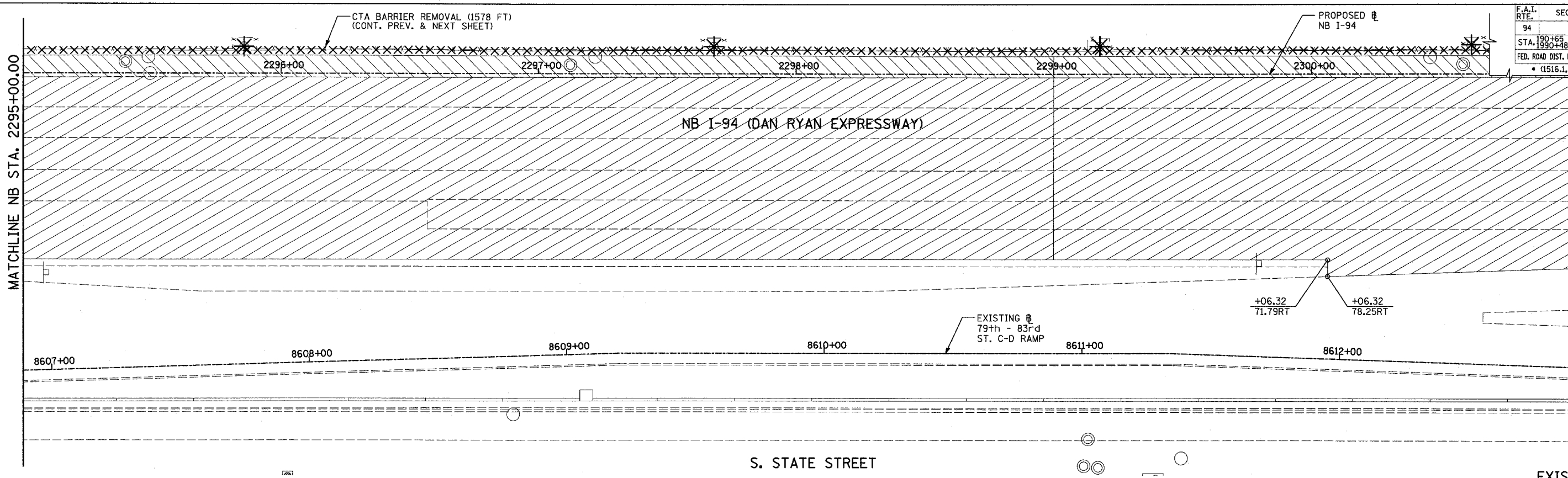
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-94 MAINLINE  
NB I-94 STA. 2289+00.00 TO 2295+00.00

SCALE: 1"=20'  
DATE: MARCH 7, 2006

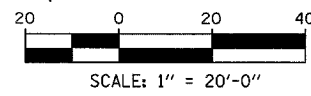
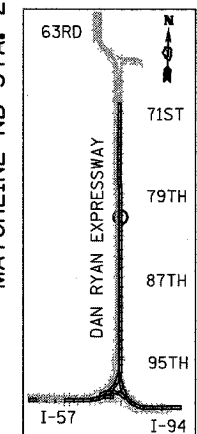
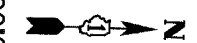
DRAWN BY: RTM  
CHECKED BY: MPG



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	94
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-4				



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



**LEGEND:**

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Diagonal Hatched Box] PAVED SHOULDER REMOVAL
- [Cross-hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Horizontal Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Vertical Hatched Box] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- [Star Symbol] LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
  - +XX.XX MAINLINE #
  - XX.XXRT STATION/OFFSET
- PLAN NOTES:**
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
  - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN INTERNATIONAL**

SHEET 31 OF 47

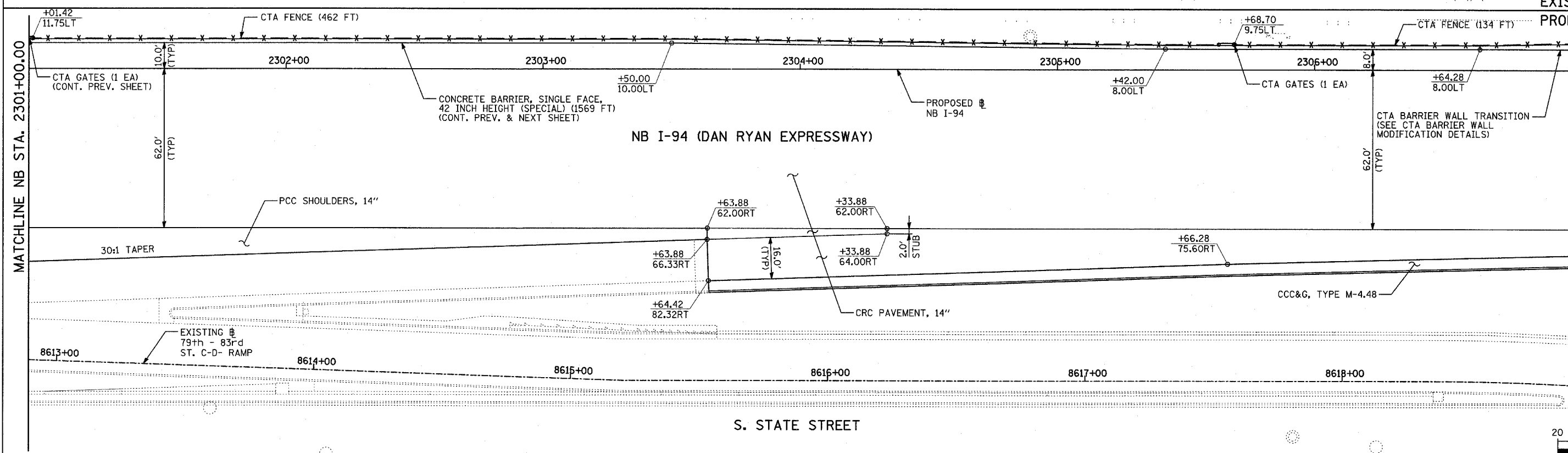
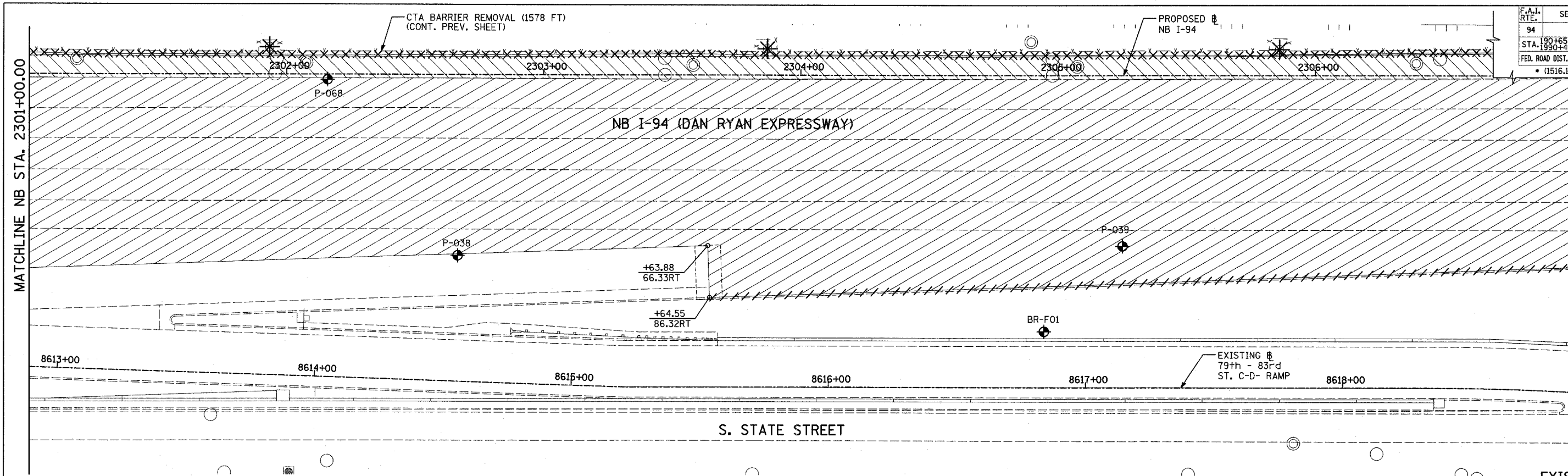
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-94 (DAN RYAN EXPRESSWAY)  
NB I-94 STA. 2295+00.00 TO 2301+00.00

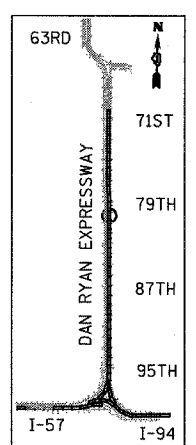
SCALE: 1"=20'  
DATE: MARCH 7, 2006

DRAWN BY: RTM  
CHECKED BY: MPG

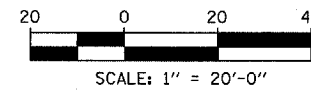
F.A.I. RTE. 94	SECTION	COUNTY COOK	TOTAL SHEETS 916	SHEET NO. 95
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4 6230				



EXISTING CONDITIONS  
PROPOSED IMPROVEMENTS



LOCATION MAP



**LEGEND:**

- #### COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = 6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched Box] PAVEMENT REMOVAL
- [Hatched Box] PAVED SHOULDER REMOVAL
- [Hatched Box] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 4"
- [Hatched Box] BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

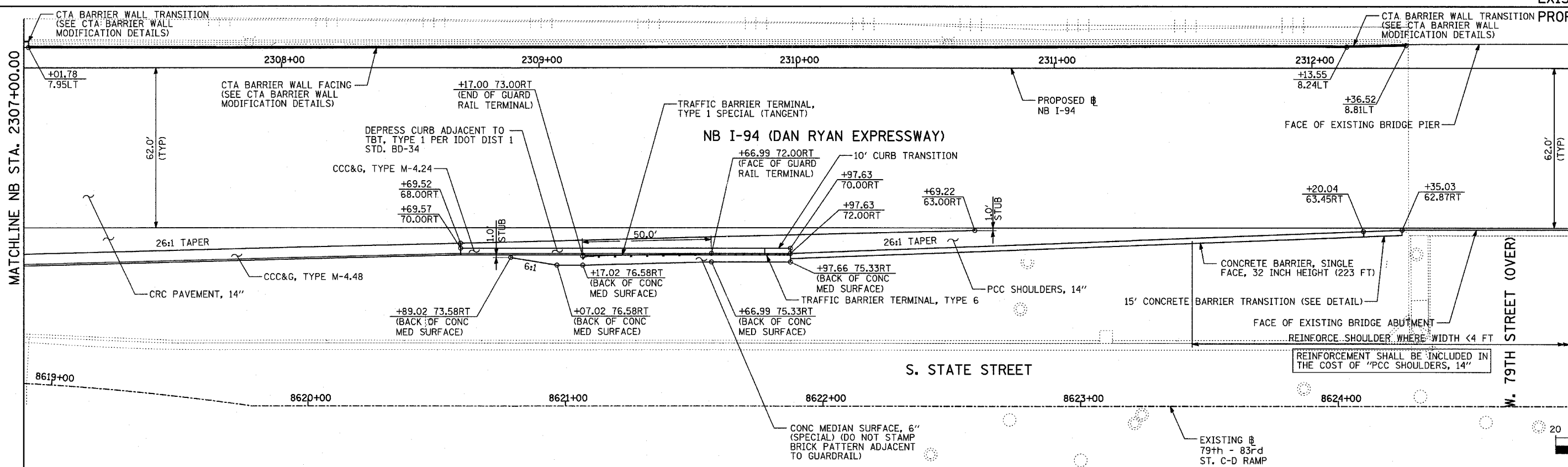
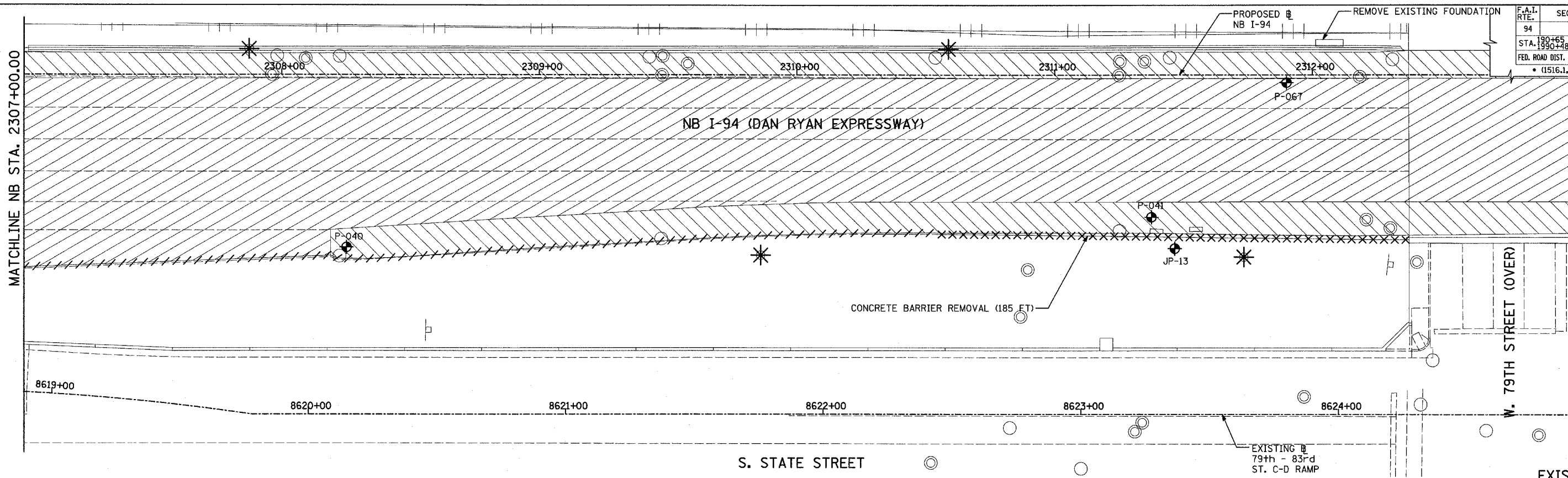
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN INTERNATIONAL**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED PLAN  
NB I-94 (DAN RYAN EXPRESSWAY)  
NB I-94 STA. 2301+00.00 TO 2307+00.00

SCALE: 1"=20'  
DATE: MARCH 7, 2006  
DRAWN BY: RTM  
CHECKED BY: MPG



**LEGEND:**

	PAVEMENT REMOVAL		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	COMB CONC CURB & GUTTER REMOVAL		MAINLINE @ STATION/OFFSET
	CHAIN LINK FENCE REMOVAL		PLAN NOTES:
	CONCRETE BARRIER/GUARDRAIL REMOVAL		- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
	TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)		- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED
	SOIL BORING LOCATIONS		
	PAVED SHOULDER REMOVAL		
	PAVEMENT/SHOULDER REMOVAL, SPECIAL		
	BITUMINOUS SURFACE REMOVAL, 4"		
	BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)		

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 NB I-94 (DAN RYAN EXPRESSWAY)  
 NB I-94 STA. 2307+00.00 TO 2313+00.00

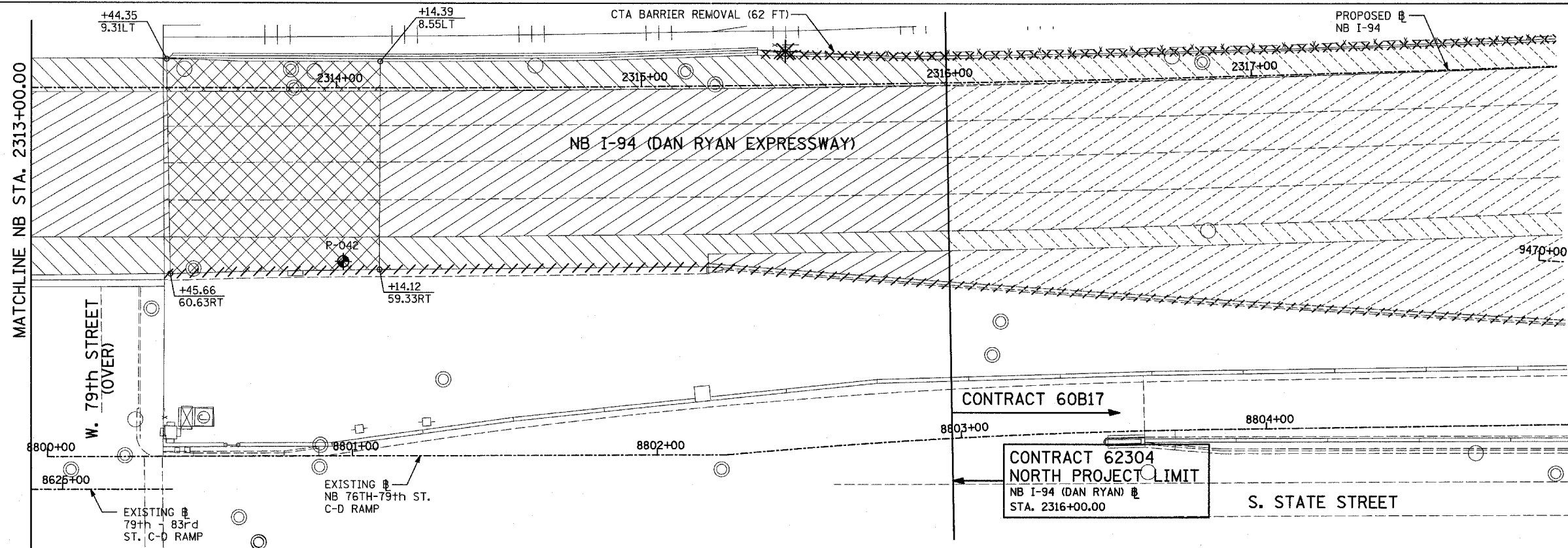
SCALE: 1"=20'  
 DATE: MARCH 7, 2006

DRAWN BY: RTM  
 CHECKED BY: MPG

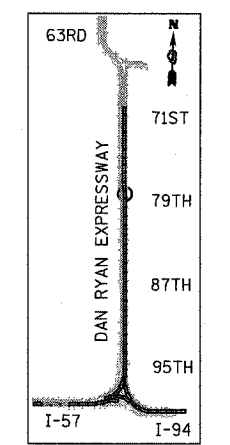
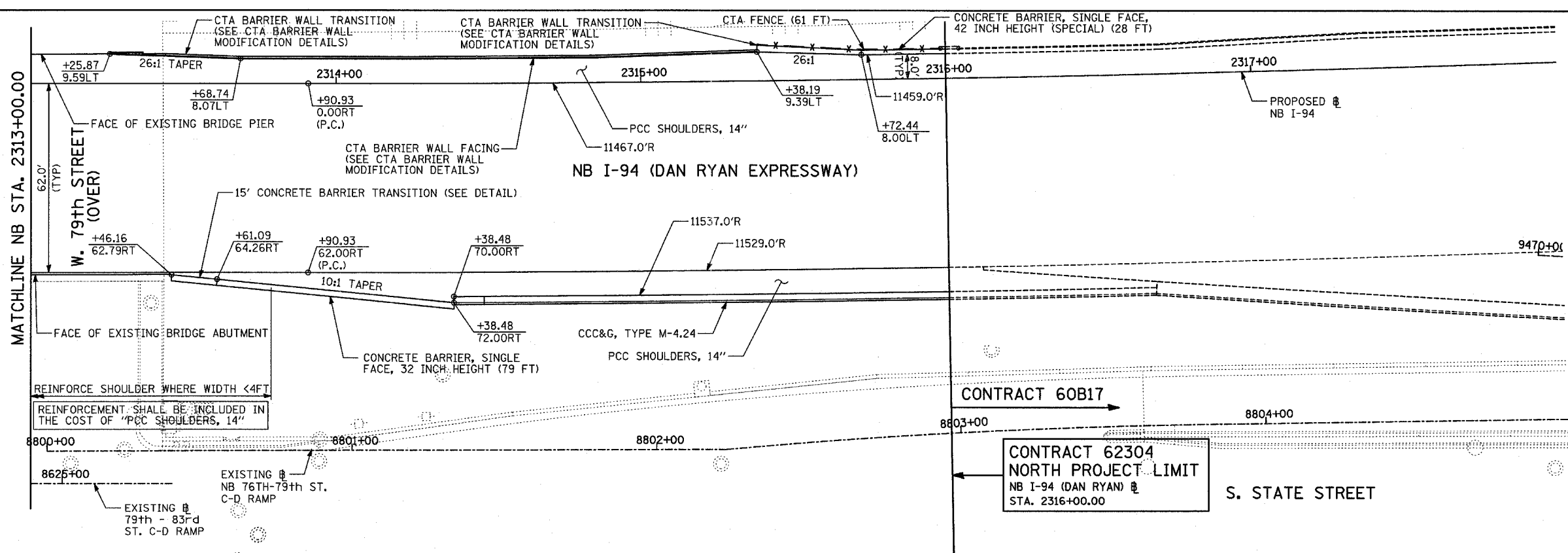
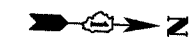
TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	97

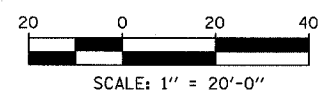
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)  
 STA. 190+48 (NB FORD)  
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT  
 (1516.1, 1717, & 1818) R-4 62304



EXISTING CONDITIONS  
 PROPOSED IMPROVEMENTS



LOCATION MAP



**LEGEND:**

- ////// COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS
- ▨ PAVEMENT REMOVAL
- ▧ PAVED SHOULDER REMOVAL
- ▩ PAVEMENT/SHOULDER REMOVAL, SPECIAL
- ▭ BITUMINOUS SURFACE REMOVAL, 4"
- ▮ BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

- * LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

**PLAN NOTES:**

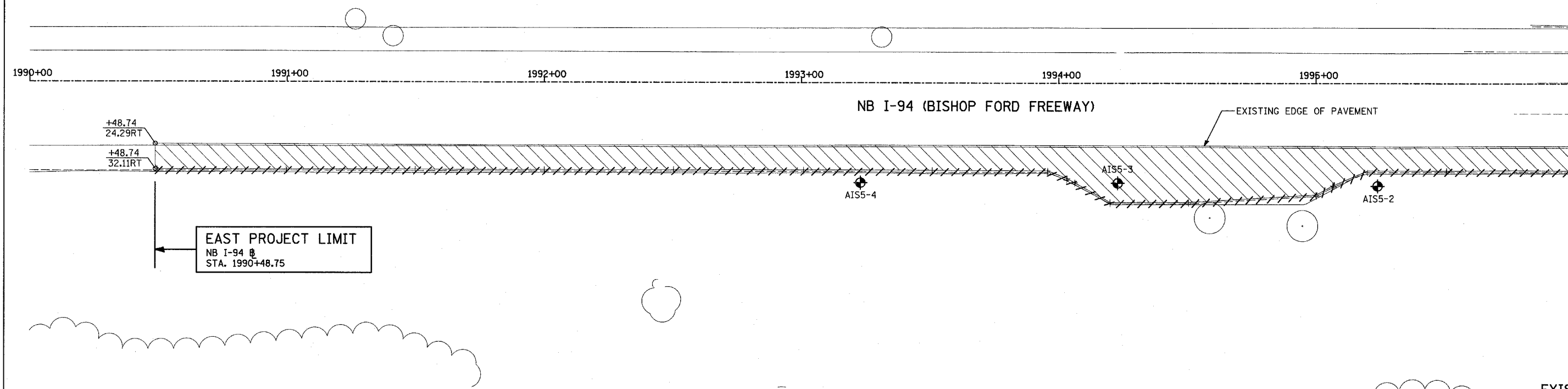
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

**TYLIN INTERNATIONAL**

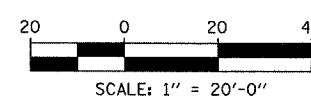
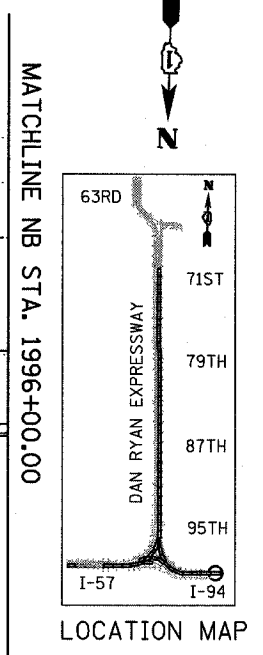
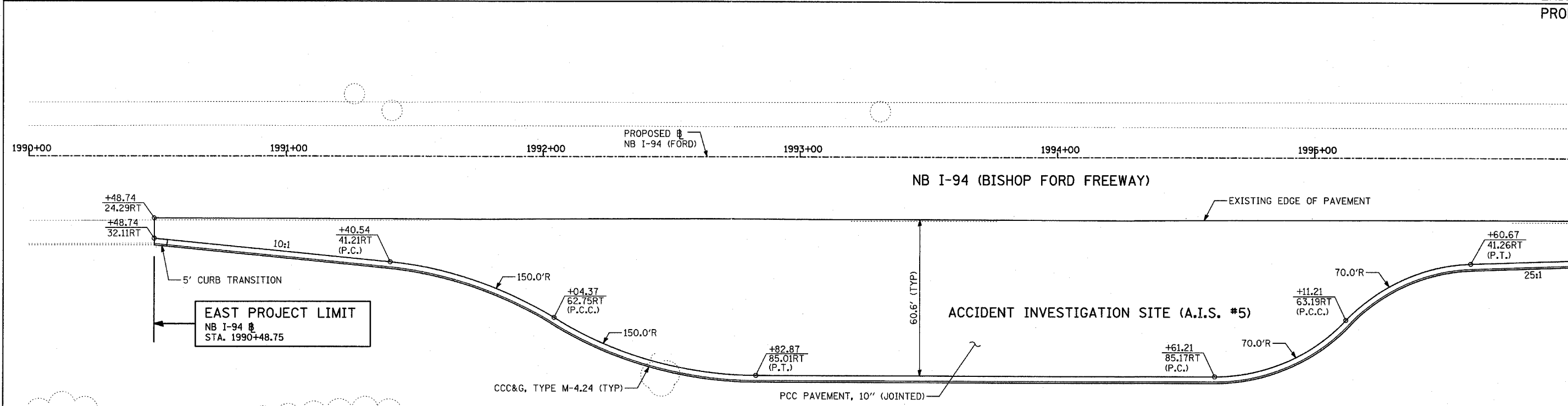
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 EXISTING AND PROPOSED PLAN  
 NB I-94 (DAN RYAN EXPRESSWAY)  
 NB I-94 STA. 2313+00.00 TO 2318+00.00

SCALE: 1"=20'  
 DATE: MARCH 7, 2006  
 DRAWN BY: RTM  
 CHECKED BY: MPG



EXISTING CONDITIONS  
 PROPOSED IMPROVEMENTS



**LEGEND:**

####	COMB CONC CURB & GUTTER REMOVAL	[Hatched Box]	PAVEMENT REMOVAL
~~~~~	CHAIN LINK FENCE REMOVAL	[Diagonal Hatched Box]	PAVED SHOULDER REMOVAL
XXXXXX	CONCRETE BARRIER/GUARDRAIL REMOVAL	[Cross-hatched Box]	PAVEMENT/SHOULDER REMOVAL, SPECIAL
X10	TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)	[Horizontal Hatched Box]	BITUMINOUS SURFACE REMOVAL, 4"
⊕	SOIL BORING LOCATIONS	[Vertical Hatched Box]	BITUMINOUS SURFACE REMOVAL, 1/2" OR 1 3/4" (SEE CALLOUT)

★ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
 +XX.XX MAINLINE #
 XX.XXRT STATION/OFFSET

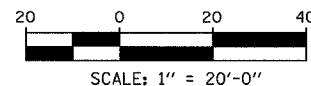
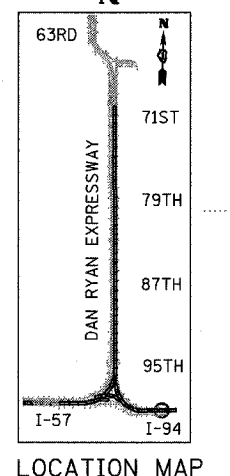
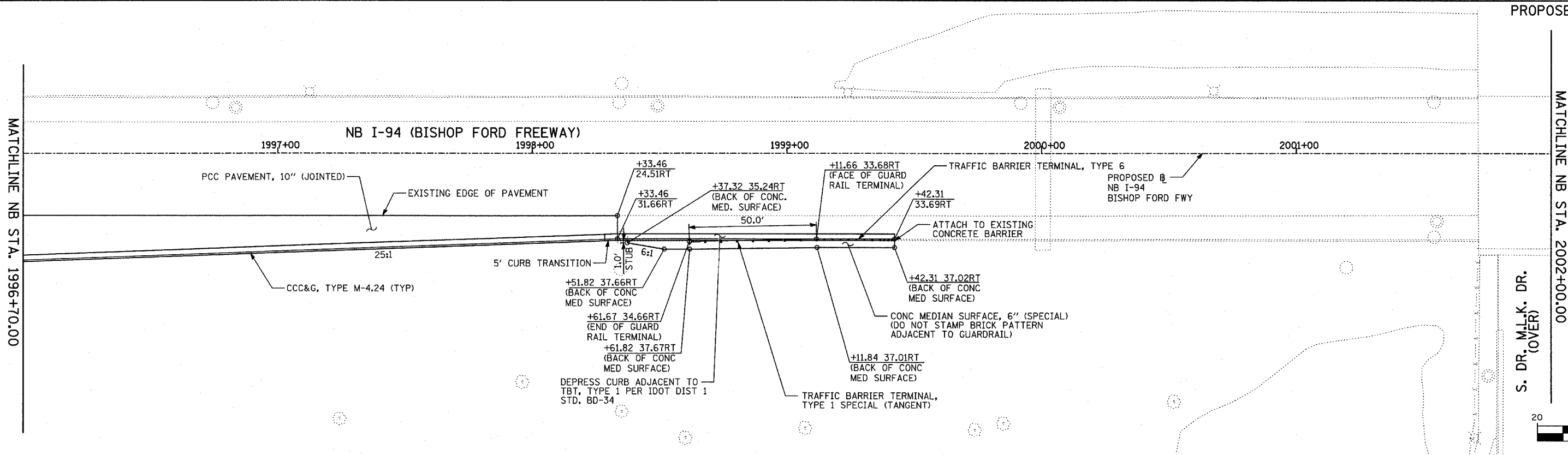
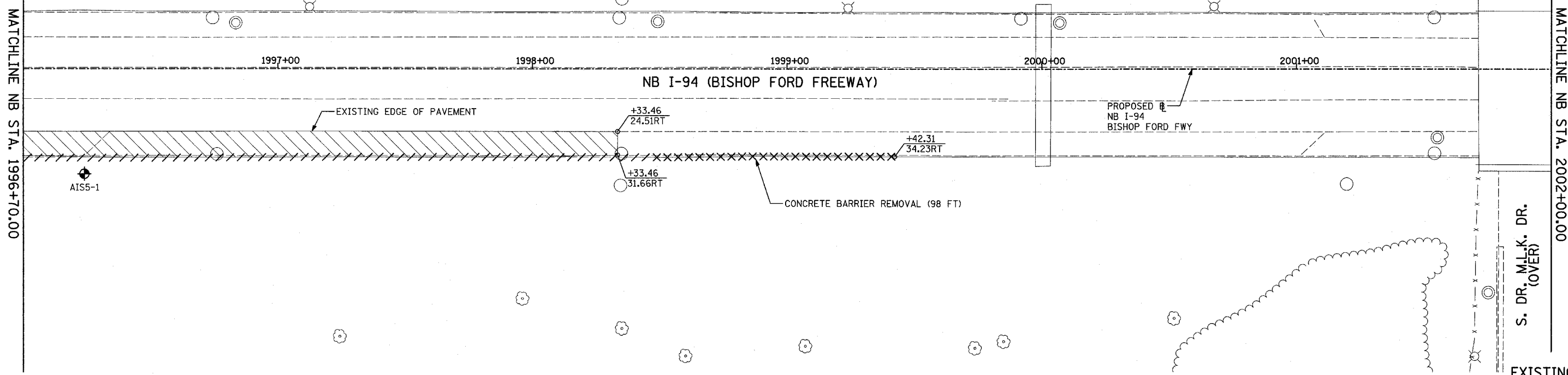
PLAN NOTES:
 - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
 - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 NB I-94
 A.I.S. #5
 NB STA 1990+48.74 TO 1996+00.00
 SCALE: 1"=20'
 DATE: MARCH 7, 2006
 DRAWN BY: RTM
 CHECKED BY: MPG

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	916	99
STA. 190+65 (NB I-57) TO STA. 2316+00 (NB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-4				



LEGEND:

	COMB CONC CURB & GUTTER REMOVAL		PAVEMENT REMOVAL
	CHAIN LINK FENCE REMOVAL		PAVED SHOULDER REMOVAL
	CONCRETE BARRIER/GUARDRAIL REMOVAL		PAVEMENT/SHOULDER REMOVAL, SPECIAL
	TREE REMOVAL (UNITS) (NO UNIT SHOWN = 6)		BITUMINOUS SURFACE REMOVAL, 4"
	SOIL BORING LOCATIONS		BITUMINOUS SURFACE REMOVAL, 1 1/2" OR 1 3/4" (SEE CALLOUT)

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

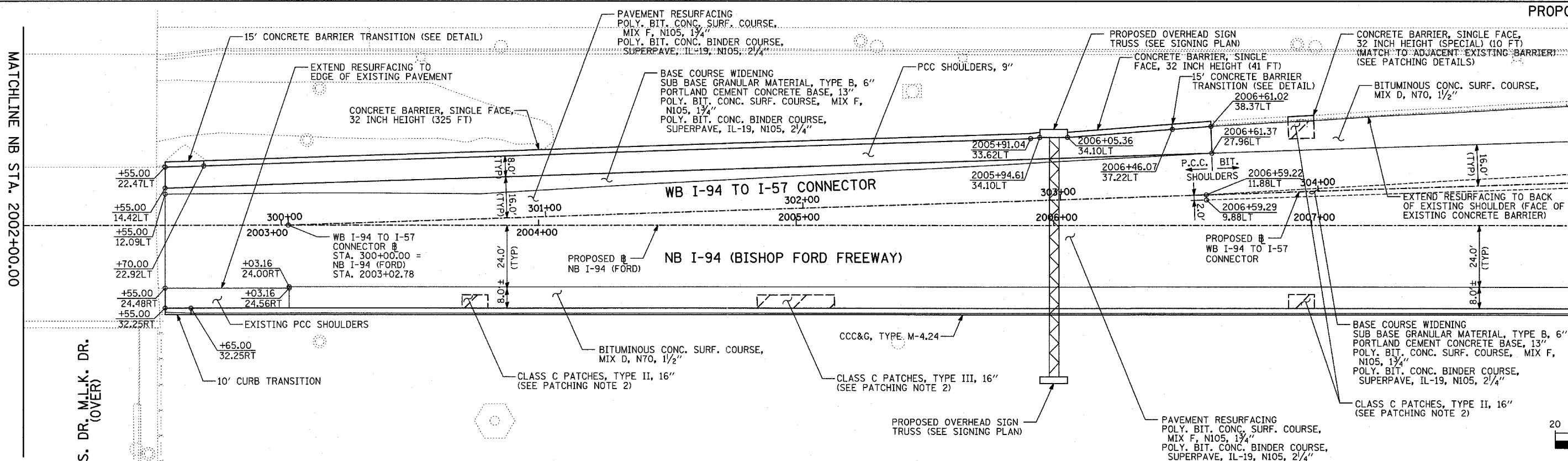
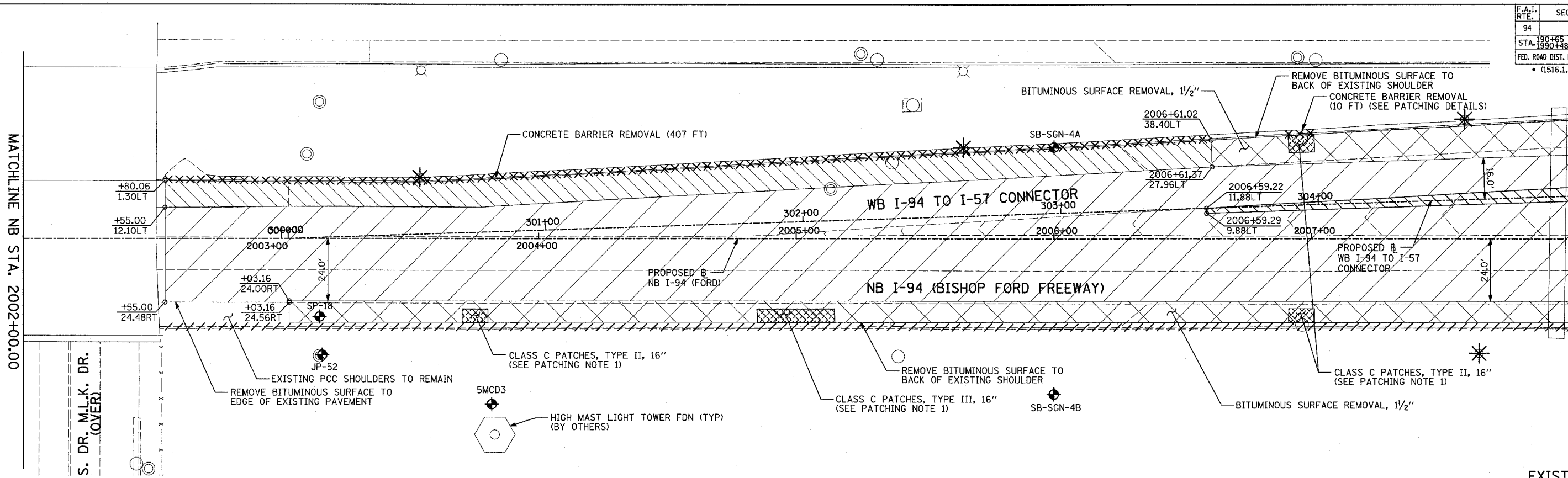
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
NB I-94
A.I.S. #5
NB STA 1996+00.00 TO 2002+00.00

SCALE: 1"=20'
DATE: MARCH 7, 2006

DRAWN BY: RTM
CHECKED BY: MPG

TYLIN INTERNATIONAL



LEGEND:

- ===== COMB CONC CURB & GUTTER REMOVAL
- ~~~~~ CHAIN LINK FENCE REMOVAL
- XXXXXX CONCRETE BARRIER/GUARDRAIL REMOVAL
- ⊗10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- ⊕ SOIL BORING LOCATIONS

- [Hatched] PAVEMENT REMOVAL
- [Diagonal] PAVED SHOULDER REMOVAL
- [Cross-hatched] PAVEMENT/SHOULDER REMOVAL, SPECIAL
- [Dotted] BITUMINOUS SURFACE REMOVAL, 4"
- [Dotted] BITUMINOUS SURFACE REMOVAL, 1/2" OR 1 3/4" (SEE CALLOUT)

- ⊗ LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- +XX.XX MAINLINE Ⓢ
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

PATCHING NOTES:

- SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REMOVAL OVER PATCHES 1/2", (SEE MISCELLANEOUS DETAILS)
- SEWER TRENCH: CLASS C PATCHES, TYPE VARIES, 16"; BITUMINOUS REPLACEMENT OVER PATCHES, (SEE MISCELLANEOUS DETAILS)

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 NB I-94 (BISHOP FORD FREEWAY)
 NB STA. 2002+00.00 TO 2008+00.00

SCALE: 1"=20'
 DATE: MARCH 7, 2006
 DRAWN BY: RTM
 CHECKED BY: MPG