

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	1
		ILLINOIS	CONTRACT NO. 60V71	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

**TRAFFIC DATA**

ADT (2008)  
INTERSTATE 80 = 81,200

ADT (2030):  
INTERSTATE 80 = 115,000

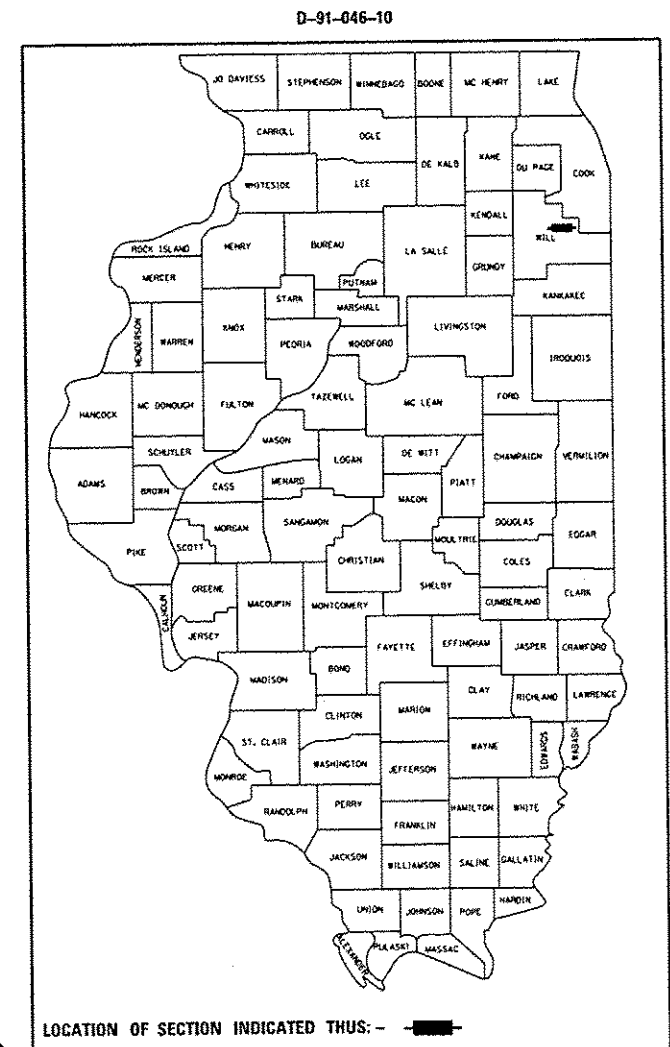
DESIGN SPEED (I-80) = 60 MPH  
POSTED SPEED (I-80) = 55 MPH

**PROPOSED  
HIGHWAY PLANS**

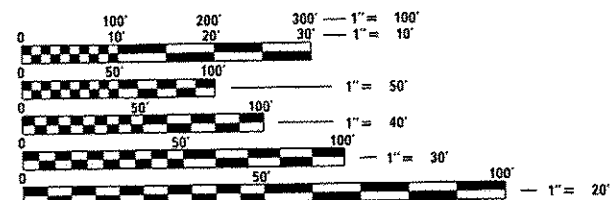
FAI ROUTE 80 (INTERSTATE 80)  
SECTION 99-4-1-SB-B-R  
OLD PLANK ROAD TRAIL  
PEDESTRIAN OVERPASS  
WILL COUNTY

C-91-061-13

THE PROJECT CONSISTS OF THE REPLACEMENT OF S.N. 099-0235 WITH S.N. 099-3404 OVER I-80; RECONSTRUCTION OF OLD PLANK TRAIL FROM STA. 199+91.79 TO STA. 207+71.14.



LOCATION OF SECTION INDICATED THUS: - [shaded box] -



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: SUNG BYUN  
PROJECT MANAGER: KIM HARVEY

CONTRACT NO. 60V71

BEGIN PROJECT  
STA 199+91.79

S.N. 099-3404 STA 203+76.65  
TWO SPAN PREFABRICATED  
PEDESTRIAN TRUSS BRIDGE  
STA 203+79.65

END PROJECT  
STA 207+71.14

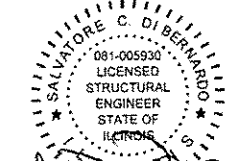


LOCATION MAP  
NOT TO SCALE

GROSS LENGTH (OLD PLANK TRAIL ROAD) = 779 FT. = 0.15 MILE  
NET LENGTH (OLD PLANK TRAIL ROAD) = 779 FT. = 0.15 MILE



*Duane O'Laughlin*  
DATE: 3/14/16  
SEAL EXPIRES: 11/30/2017  
APPLIES TO SHEETS  
1 - 24  
33 - 46



*Salvatore C. DiBernardo*  
DATE: 3/14/16  
SEAL EXPIRES: 11/30/2016  
APPLIES TO SHEETS  
25 - 32

Ciorba Group, Inc.

DESIGN FIRM  
REGISTRATION NUMBER  
184-001016  
CONSULTING ENGINEERS  
SUITE 402, 5507 NORTH CUMBERLAND AVE  
CHICAGO, ILLINOIS 60656 :: (773) 775-4009

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED *March 17, 2016*

*John F. ...*  
REGIONAL ENGINEER

*May 16, 2016*  
*Mahmoud M. Addis P.E.*  
ENGINEER OF DESIGN AND ENVIRONMENT

*May 16, 2016*  
*David ...*  
DIRECTOR OF PROGRAM DEVELOPMENT

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

VOLUME 1

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LIST OF STATE STANDARDS

STD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
515001-03	NAME PLATE FOR BRIDGES
630001-10	STEEL PLATE BEAM GUARDRAIL
630301-06	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631011-09	TRAFFIC BARRIER TERMINAL, TYPE 2
642001-02	SHOULDER RUMBLE STRIPS, 16 in.
643001-02	SAND MODULE IMPACT ATTENUATORS
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24' (600 mm) FROM PAVEMENT EDGE
701400-08	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-09	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
701426-08	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≥ 45 MPH
701428-01	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701446-07	TWO LANE CLOSURE FREEWAY/EXPRESSWAY
701901-05	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
725001	OBJECT AND TERMINAL MARKERS
780001-05	TYPICAL PAVEMENT MARKINGS
782006	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

COMMITMENTS

- THE OLD PLANK ROAD TRAIL BRIDGE OVER I-80 IS PROPOSED FOR RELOCATION EAST OF THE EXISTING STRUCTURE ALMOST ENTIRELY WITHIN THE EXISTING PERMANENT EASEMENT. CLOSURE OF THE EXISTING TRAIL IS NOT ANTICIPATED AS THE EXISTING STRUCTURE AND APPROACHES WILL REMAIN OPERATIONAL AS THE RELOCATED APPROACHES ARE BEING CONSTRUCTED. PER COORDINATION WITH THE FPDWC, CLOSURE OF THE TRAIL DURING CONSTRUCTION WILL BE MINIMIZED TO THE EXTENT POSSIBLE. A SHORT TERM CLOSURE MAY BE REQUIRED FOR THE PLACEMENT OF THE NEW STRUCTURE OVER I-80 AND SHIFTING OF BICYCLE TRAFFIC TO THE NEW BRIDGE.

I:\PROJECTS\10003181\003181\_01\01\Design\Notes\_Sheets\10003181\_01\01\Index\Index.dwg

 ENGINEERING CONSULTANT CONSULTING ENGINEERS 2201 North Cumberland Parkway, Suite 402 Chicago, Illinois 60624 Tel: 773.775.4400 Fax: 773.775.4414 Email: info@ciorbagroup.com	USER NAME: msdeboud DESIGNED - EPS DRAWN - JB CHECKED - DJO DATE - 8/14/2014	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80 INDEX OF SHEETS, STATE STANDARDS, AND COMMITMENTS		F.A.I. RTE.: 80	SECTION: 99-4-1-SB-B-R	COUNTY: WILL	TOTAL SHEETS: 46	SHEET NO.: 2
	PLOT SCALE: 1/2" = 1'-0" PLOT DATE: 3/17/2016	SCALE:		SHEET NO. OF SHEETS:	STA. TO STA.:	CONTRACT NO. 60V71 ILLINOIS FED. AID PROJECT				

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
2. THE LOCATIONS OF EXISTING UTILITIES, AS SHOWN ON THE DRAWINGS, REPRESENT DATA RECEIVED FROM VARIOUS SOURCES. IT IS NOT GUARANTEED TO BE CORRECT OR ALL INCLUSIVE. THE CONTRACTOR SHALL CONDUCT HIS OWN INVESTIGATIONS INTO THE LOCATION, SIZE, DEPTH AND NATURE OF ANY AND ALL EXISTING UTILITIES WHICH MAY INTERFERE WITH THE WORK UNDER THIS CONTRACT. ANY EXISTING UTILITIES WHICH ARE TO REMAIN IN SERVICE SHALL BE FULLY PROTECTED BY THE CONTRACTOR AND ANY DAMAGE CAUSED BY THE CONSTRUCTION OPERATIONS SHALL BE IMMEDIATELY REPAIRED OR REPLACED UNDER THE UTILITY OWNER'S DIRECTION BY THE CONTRACTOR AT HIS EXPENSE.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH AFFECTED UTILITY COMPANIES AND MUNICIPALITIES.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
6. ALL FRAMES AND GRATES, PAVEMENT, FENCES, DELINEATORS AND APPURTENANCES DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION WILL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
7. ANY GUARDRAIL REMOVED AND DEEMED SALVAGEABLE BY THE RESIDENT ENGINEER SHALL BE DELIVERED BY THE CONTRACTOR TO IDOT'S MAINTENANCE FACILITY IN NEW LENOX. TEMPORARY STORAGE AND DELIVERY SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "GUARDRAIL REMOVAL."
8. THE ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4155 A MINIMUM OF 72 HOURS PRIOR TO THE START OF WORK.
9. TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS THE RESIDENT ENGINEER SHALL CONTACT THE EXPRESSWAY AREA TRAFFIC FIELD ENGINEER, AT (847) 705-4153.
10. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS, ELEVATIONS AND EXISTING FIELD CONDITIONS PRIOR TO BIDDING, ORDERING MATERIALS, OR BEGINNING CONSTRUCTION ON THIS PROJECT, SPECIFICALLY AS THEY RELATE TO LUMP SUM ITEMS.
11. THE FOLLOWING PAY ITEMS HAVE BEEN PROVIDED FOR FOREST MANAGEMENT. THE ACTUAL NEED FOR THESE PAY ITEMS WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE RESIDENT ENGINEER. IF THESE PAY ITEMS ARE NOT USED OR ARE NOT USED IN THEIR ENTIRETY, THE REMAINING QUANTITY SHALL BE DEDUCTED FROM THE TOTAL AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
  - SELECTIVE CLEARING
  - WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE
  - TREE PRUNING (1 TO 10 INCH DIAMETER)
  - TREE PRUNING (OVER 10 INCH DIAMETER)
12. THE CONTRACTOR WILL CONTACT KAREN FONTE OF THE WILL COUNTY FOREST PRESERVE DISTRICT AT 815-727-8700 IN ORDER TO OBTAIN A SPECIAL USE PERMIT MINIMALLY 48 HOURS PRIOR TO BRIDGE PLACEMENT OR BRIDGE REMOVAL OF THE OLD PLANK TRAIL BRIDGE OVER I-80.
13. ANY SURPLUS TOPSOIL SHALL REMAIN ON SITE BY INCREASING THE THICKNESS OF THE TOPSOIL PLACED OR AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE INCLUDED IN THE COST OF THE CONTRACT UNIT PRICE FOR TOPSOIL EXCAVATION AND PLACEMENT.
14. PHOSPHORUS FERTILIZER SHALL NOT BE APPLIED TO SEEDDED AREAS.

GENERAL NOTES (CONT'D.)

15. THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION II.G.1 AND 2 OF THE SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

EROSION AND SEDIMENT CONTROL NOTES

1. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.
2. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED, TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
3. SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA, WETLAND AREAS OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES.
4. THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DEPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING, OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO ANY DEVELOPMENT SITE, CHANNEL, WATER OF THE U.S..
5. THE CONTRACTOR SHALL SURROUND ALL EARTH STOCKPILES WITH SILT FENCE. THIS SHALL BE PAID FOR AS PERIMETER CONTROL EROSION BARRIER.
6. STOCKPILES OF SOIL AND OTHER MATERIALS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS SHALL BE FURNISHED WITH EROSION AND SEDIMENT CONTROL MEASURES. STOCKPILES TO REMAIN IN PLACE FOR 14 DAYS OR MORE SHALL RECEIVE TEMPORARY SEEDING WITHIN 1 DAY.
7. ALL ESC MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION AND IDOT'S BEST MANAGEMENT PRACTICES - MAINTENANCE GUIDE:  
(<http://www.idot.illinois.gov/transportation-system/environment/erosion-and-sediment-control>).
8. THE CONTRACTOR WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DURING CONSTRUCTION.
9. THE CONTRACTOR SHALL CHECK ALL ESC MEASURES WEEKLY AND AFTER EACH RAINFALL, 0.5 INCHES OR GREATER IN A 24 HOUR PERIOD, OR EQUIVALENT SNOWFALL. ADDITIONALLY DURING WINTER MONTHS, ALL MEASURES SHOULD BE CHECK BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT.
10. THE CONTRACTOR SHOULD PROVIDE TO THE RE A PLAN TO ENSURE THAT A STABILIZED FLOW LINE WILL BE PROVIDED DURING STORM SEWER CONSTRUCTION. THE USE OF A STABILIZED FLOW LINE BETWEEN INSTALLED STORM SEWER AND OPEN DISTURBANCE WILL REDUCE THE POTENTIAL FOR THE OFFSITE DISCHARGE OF SEDIMENT-BEARING WATERS, ESPECIALLY WHEN RAIN IS FORECASTED, SO THAT FLOW WILL NOT ERODE. LACK OF APPROVED PLAN OR FAILURE TO COMPLY WILL RESULT IN AN ESC DEFICIENCY DEDUCTION.


EROSION AND SEDIMENT CONTROL NOTES (CONT'D.)

11. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.
12. TEMPORARY OR PERMANENT STABILIZATION SHALL BE INITIATED IMMEDIATELY UPON COMPLETION OF DISTURBANCE OR IF THE WORK AREA IS TO BE LEFT UNDISTURBED FOR 14 DAYS OR MORE.
13. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDDED AT ONE TIME.
14. EROSION CONTROL ITEMS ARE CONSIDERED TO BE A HIGH PRIORITY ON THIS CONTRACT. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE RE.

DRAINAGE NOTES

1. INVERT ELEVATIONS FOR EXISTING PIPES HAVE BEEN SHOWN ON THE PLANS WHEN SURVEY INFORMATION WAS AVAILABLE. THE CONTRACTOR SHALL VERIFY THE INVERT ELEVATIONS FOR EXISTING PIPES IN THE FIELD AT THE TIME OF CONSTRUCTION AND SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
2. TOP OF GRATE ELEVATIONS FOR EXISTING STRUCTURES SHOWN ON THE PLANS WERE SURVEYED. THE RESIDENT ENGINEER MAY REVISE THE PROPOSED TOP OF FRAME ELEVATIONS AT THE TIME OF CONSTRUCTION BASED ON FIELD CONDITIONS.
3. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN DRAINAGE FLOWS AT ALL TIMES DURING THE PERFORMANCE OF THE WORK. METHODS USED BY THE CONTRACTOR SHALL BE SUBJECT TO APPROVAL OF THE ENGINEER. THE COST OF MAINTAINING DRAINAGE FLOWS SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
4. WHERE REQUIRED BY RESTRICTIVE DEPTHS, PRECAST REINFORCED CONCRETE FLAT SLAB TOPS SHALL BE USED FOR MANHOLES AND CATCH BASINS.
5. THERE ARE SEVERAL LOCATIONS WHERE PROPOSED STORM SEWER/PIPE CULVERTS WILL BE PLACED ADJACENT TO THE PIERS AND ABUTMENT WALLS OF EXISTING OVERHEAD BRIDGES. THE ORIGINAL BRIDGE PLANS HAVE BEEN REVIEWED TO REDUCE THE LIKELIHOOD OF ANY CONFLICT BETWEEN THE PROPOSED STORM SEWER/PIPE CULVERTS AND THE OVERHEAD BRIDGE SUBSTRUCTURE(S), HOWEVER SUCH CONFLICTS MAY EXIST. THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN EXCAVATING OR JACKING PIPE CULVERTS ADJACENT TO THE OVERHEAD BRIDGE PIERS AND ABUTMENT WALLS.
6. FOR STORM SEWER CONSTRUCTED UNDER THE ROADWAY, BACKFILLING METHODS TWO AND THREE AUTHORIZED UNDER THE PROVISIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED.

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	USER NAME : ahern	DESIGNED - EPS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80 GENERAL NOTES	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 2,0000 1" = 10'	DRAWN - JB	REVISED -			80	99-4-1-SB-B-R	WILL	46	3
	PLOT DATE = 3/16/2016	CHECKED - DJO	REVISED -	SCALE: 1" = 50'		SHEET NO. OF SHEETS		STA. TO STA.		CONTRACT NO. 60V71
ILLINOIS FED. AID PROJECT										

CONSTRUCTION CODE  
URBAN  
100% STATE  
FACILITIES  
0028

CODE NO.	ITEM	UNIT	TOTAL	CONSTRUCTION CODE
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	507	507
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	108	108
20101100	TREE TRUNK PROTECTION	EACH	55	55
* 20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	25	25
* 20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	25	25
20200100	EARTH EXCAVATION	CU YD	371	371
* 20200200	ROCK EXCAVATION	CU YD	1,399	1,399
20400800	FURNISHED EXCAVATION	CU YD	506	506
20800150	TRENCH BACKFILL	CU YD	69	69
* 21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	456	456
* 21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	92	92
* 25000115	SEEDING, CLASS 1B	ACRE	0.25	0.25
* 25000210	SEEDING, CLASS 2A	ACRE	1.00	1.00
* 25000310	SEEDING, CLASS 4	ACRE	1.00	1.00
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	205	205
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	205	205
* 25100105	MULCH, METHOD 1	ACRE	0.75	0.75
* 25100115	MULCH, METHOD 2	ACRE	0.25	0.25

\* SPECIALTY ITEMS


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	USER NAME = ebern	DESIGNED - EPS	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80</b> <b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE. 80	SECTION 99-4-1-SB-B-R	COUNTY WILL	TOTAL SHEETS 46	SHEET NO. 4
	PLOT SCALE = 2,000' / in.	CHECKED - DJO	REVISED -			SCALE: 1" = 50'	SHEET NO. OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT	
	PLOT DATE = 3/16/2016	DATE = 8/14/2014	REVISED -							

CODE NO.	ITEM	UNIT	TOTAL	CONSTRUCTION CODE
				URBAN 100% STATE FACILITIES 0028
* 25100630	EROSION CONTROL BLANKET	SQ YD	13,309	13,309
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	183	183
28000305	TEMPORARY DITCH CHECKS	FOOT	160	160
28000400	PERIMETER EROSION BARRIER	FOOT	1,020	1,020
28000510	INLET FILTERS	EACH	8	8
28100107	STONE RIPRAP, CLASS A4	SQ YD	76	76
28200200	FILTER FABRIC	SQ YD	76	76
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	45	45
31101400	SUBBASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	625	625
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	1,774	1,774
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	69	69
44000100	PAVEMENT REMOVAL	SQ YD	710	710
44004250	PAVED SHOULDER REMOVAL	SQ YD	45	45
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	20	20
48203013	HOT-MIX ASPHALT SHOULDERS, 4"	SQ YD	45	45
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1	1
50102400	CONCRETE REMOVAL	CU YD	26.1	26.1
50157300	PROTECTIVE SHIELD	SQ YD	414	414

• SPECIALTY ITEMS

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 <b>ENGINEERING CONSULTANT</b> CONSULTING ENGINEERS 2101 NORTH CANTON STREET, SUITE 100, CHICAGO, ILLINOIS 60647 TEL: (773) 330-1100 FAX: (773) 330-1104 WWW.CIORBAGROUP.COM	USER NAME : epslme	DESIGNED - EPS	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>OLD PLANK ROAD TRAIL OVER F.A.J. ROUTE 80</b> <b>SUMMARY OF QUANTITIES</b>	F.A.J. RTE. 80	SECTION 99-4-1-SB-B-R	COUNTY WILL	TOTAL SHEETS 46	SHEET NO. 5
	PLOT SCALE : 2.0000 "/>									
	PLOT DATE : 3/16/2016	CHECKED - DJD	REVISED -			DATE - 8/14/2014	REVISED -	SCALE: 1" = 50'	SHEET NO. OF SHEETS	STA. TO STA.

CODE NO.	ITEM	UNIT	TOTAL	CONSTRUCTION CODE
				URBAN 100% STATE FACILITIES 0028
50200100	STRUCTURE EXCAVATION	CU YD	82	82
* 50200400	ROCK EXCAVATION FOR STRUCTURES	CU YD	20	20
50301350	CONCRETE STRUCTURES (APPROACH SLAB)	CU YD	112.4	112.4
50300255	CONCRETE SUPERSTRUCTURE	CU YD	11.1	11.1
50300300	PROTECTIVE COAT	SQ YD	50	50
50800105	REINFORCEMENT BARS	POUND	890	890
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	19,810	19,810
51500100	NAME PLATES	EACH	2	2
51603000	DRILLED SHAFT IN SOIL	CU YD	4.1	4.1
51604000	DRILLED SHAFT IN ROCK	CU YD	4.8	4.8
54210192	PIPE ELBOW, 36"	EACH	2	2
54213459	END SECTIONS 24"	EACH	4	4
5421D024	PIPE CULVERTS, CLASS D, TYPE 1 24" (TEMPORARY)	FOOT	200	200
55080160	STORM SEWERS, CLASS B, TYPE 1 36"	FOOT	110	110
58700300	CONCRETE SEALER	SO FT	1,181	1,181
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	19	19
60224445	MANHOLES, TYPE A, 7' -DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	2	2
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	437.5	437.5

\* SPECIALTY ITEMS

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 <b>ENGINEERING CONSULTANT</b> Clorba Group, Inc. CONSULTING ENGINEERS <small>500 North LaSalle Street, Suite 1012          Chicago, Illinois 60610          Tel: 312.777.8800 Fax: 312.777.8804          www.clorbagroup.com</small>	USER NAME : shorn	DESIGNED - EPS	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80</b> <b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE : 2,0000 1/ in.	CHECKED - DJO	REVISED -			80	99-4-1-SB-B-R	WILL	46	6
PLOT DATE : 3/16/2016	DATE : 8/14/2014	REVISED -		SCALE: 1" = 50'	SHEET NO. OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT			
						CONTRACT NO. 60V71				

CONSTRUCTION CODE  
URBAN  
100% STATE  
FACILITIES  
0028

CODE NO.	ITEM	UNIT	TOTAL	CONSTRUCTION CODE
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1	1
63200310	GUARDRAIL REMOVAL	FOOT	470	470
* 63301210	REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A	FOOT	100	100
64300450	IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2
64301090	ATTENUATOR BASE	SD YD	33	33
66400305	CHAIN LINK FENCE, 6'	FOOT	336	336
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12
67100100	MOBILIZATION	L SUM	1	1
<del>70100420</del>	<del>TRAFFIC CONTROL AND PROTECTION, STANDARD 701411</del>	<del>EACH</del>	<del>4</del>	<del>4</del>
<del>70100800</del>	<del>TRAFFIC CONTROL AND PROTECTION, STANDARD 701401</del>	<del>L SUM</del>	<del>1</del>	<del>1</del>
<del>70100815</del>	<del>TRAFFIC CONTROL AND PROTECTION, STANDARD 701446</del>	<del>L SUM</del>	<del>1</del>	<del>1</del>
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	12	12
70400100	TEMPORARY CONCRETE BARRIER	FOOT	2,100	2,100
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	4	4
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1,780	1,780
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	730	730
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	6	6
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	168	168

\* SPECIALTY ITEMS


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 <p><b>ENGINEERING CONSULTANT</b> Ciorba Group, Inc. CONSULTING ENGINEERS 8501 North Cicero Avenue, Suite 102 Chicago, Illinois 60631 TEL: 773.750.1100 FAX: 773.750.1114 E-MAIL: CIO@CGI.COM</p>	USER NAME : dhamr	DESIGNED - EPS	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80</b> <b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE. 80	SECTION 99-4-1-58-B-R	COUNTY WILL	TOTAL SHEETS 46	SHEET NO. 7		
	PLOT SCALE * 2.0000 ' / in.	CHECKED - DJD	REVISED -			SCALE: 1" = 50'	SHEET NO. OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT			
	PLOT DATE * 3/16/2016	DATE - 8/14/2014	REVISED -			<div style="text-align: right;">Rev.</div>						

CODE NO.	ITEM	UNIT	TOTAL	CONSTRUCTION CODE
				URBAN 100% STATE FACILITIES 0028
78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	1	1
78300100	PAVEMENT MARKING REMOVAL	SQ FT	830	830
A2002920	TREE, CELTIS OCCIDENTALIS (COMMON HACKBERRY), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	4	4
A2007116	TREE, QUERCUS RUBRA (RED OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	5	5
B0001720	TREE, AMELANCHIER X GRANDIFLORA (APPLE SERVICEBERRY), 12' HEIGHT, SHRUB FORM, BALLED AND BURLAPPED	EACH	9	9
C2002048	SHRUB, CORYLUS AMERICANA (AMERICAN FILBERT), 4' HEIGHT, BALLED AND BURLAPPED	EACH	20	20
D2001984	EVERGREEN, PICEA GLAUCA DENSATA (BLACK HILLS SPRUCE), 7' HEIGHT, BALLED AND BURLAPPED	EACH	8	8
K0029634	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	38	38
X0322508	PEDESTRIAN TRUSS SUPERSTRUCTURE	SQ FT	3,372	3,372
X5860110	GRANULAR BACKFILL FOR STRUCTURES	CU YD	30	30
X6431120	REMOVE IMPACT ATTENUATOR SAND MODULE	EACH	2	2
<del>X7011015</del>	<del>TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)</del>	<del>L SUM</del>	<del>1</del>	<del>1</del>
<b>X7011015</b>	<b>TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)</b>	<b>L SUM</b>	<b>1</b>	<b>1</b>
<del>X7013820</del>	<del>TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS</del>	<del>CAL DA</del>	<del>30</del>	<del>30</del>
X7040125	PINNING TEMPORARY CONCRETE BARRIER	EACH	532	532
X7240600	REMOVE AND RE-ERECT EXISTING SIGN	EACH	1	1
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1
Z0022800	FENCE REMOVAL	FOOT	472	472
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	52

• SPECIALTY ITEMS

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	USER NAME : aham	DESIGNED - EPS	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80</b> <b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE • 2.0000" = 1' in.	DRAWN - JB	REVISIONS -			80	99-4-1-SB-B-R	WILL	46	3
PLOT DATE • 3/15/2016	CHECKED - OJO	DATE - 8/14/2014	REVISED -	SCALE: 1" = 50'		SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60V71		
ILLINOIS FED. AID PROJECT										


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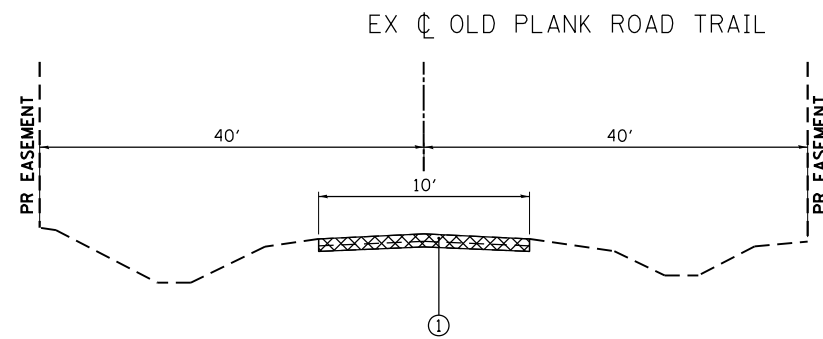
CODE NO.	ITEM	UNIT	TOTAL	CONSTRUCTION CODE
				URBAN 100% STATE FACILITIES 0028
Z0046304	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	83	83
<del>Z0074404</del>	<del>TRAINEES-TRAINING PROGRAM GRADUATE</del>	<del>HOUR</del>	<del>1000</del>	<del>1000</del>
Z0064800	SELECTIVE CLEARING	UNIT	2.5	2.5

• SPECIALTY ITEMS  
~~0042~~

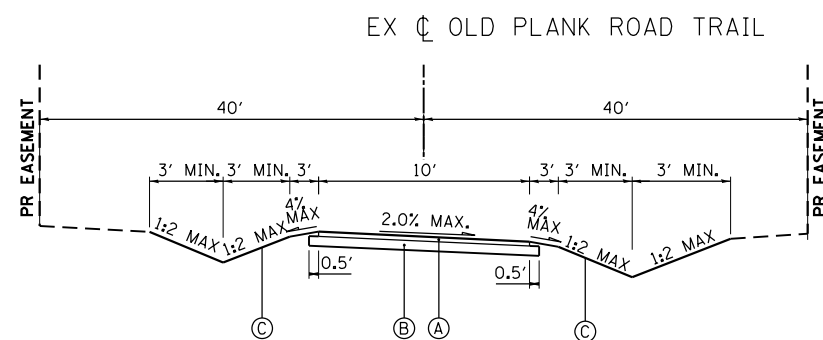
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 <b>Clorba Group, Inc.</b> CONSULTING ENGINEERS <small>200 North Corporate Avenue, Suite 100          Chicago, Illinois 60610          Tel: 773.775.1000 Fax: 773.775.4014</small>	USER NAME = shann	DESIGNED - EPS	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80</b> <b>SUMMARY OF QUANTITIES</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 2.0000' / 1" =	CHECKED - DJO	REVISED -			80	99-4-1-SB-8-R	WILL	46	9
PLOT DATE = 3/16/2016	DATE - 8/14/2014	REVISED -	CONTRACT NO. 60V71							
			ILLINOIS FED. AID PROJECT							
					SCALE: 1" = 50'	SHEET NO. OF SHEETS		STA. TO STA.		

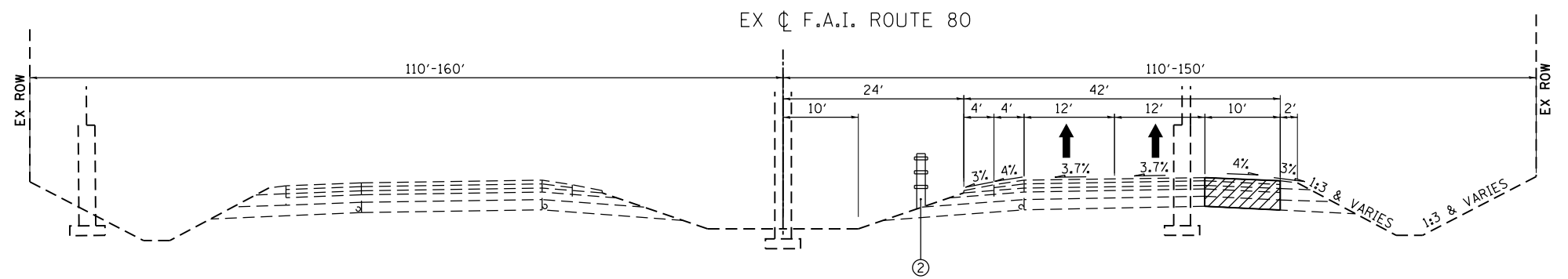
*Rev.*



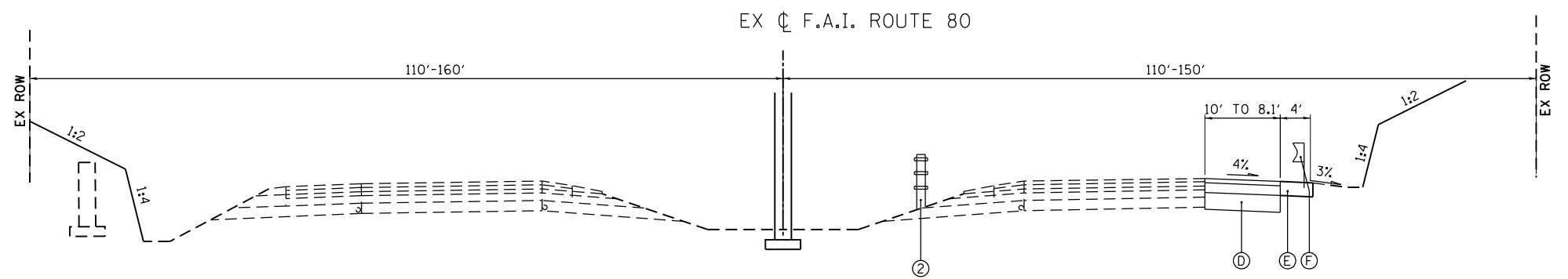
**EXISTING TYPICAL CROSS SECTION**  
 STA. 106+00.38 TO STA. 108+79.86\*  
 STA. 110+49.30\* TO STA. 113+75.89  
 \*S.N. 099-0235 (STA. 108+79.86 TO STA. 110+49.30)



**PROPOSED TYPICAL CROSS SECTION**  
 STA. 199+91.79 TO STA. 202+49.10\*  
 STA. 205+17.20\* TO STA. 207+71.14  
 \*S.N. 099-3404 (STA. 202+49.10 TO STA. 205+17.20)



**EXISTING TYPICAL CROSS SECTION**  
 F.A.I. ROUTE 80  
 STA. 658+25.00 TO STA. 658+65.00



**PROPOSED TYPICAL CROSS SECTION**  
 F.A.I. ROUTE 80  
 STA. 658+25.00 TO STA. 658+65.00

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

OPERATION	MIXTURE TYPE	AIR VOIDS @ NDES	QMP
BIKEWAY	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" (IL-9.5mm)	4% @ 70 GYR.	QC/OA
SHOULDER RECON	HMA SHOULDER, 4" (HMA BINDER IL-19mm)	4% @ 70 GYR.	QC/OA
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", 2" N70 (IL-9.5mm)	4% @ 70 GYR.	QC/OA

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA)

**NOTE:**

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SOYD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

**EXISTING LEGEND**

- ① BIKE PATH
- ② CABLE ROAD GUARD
- ▨ PAVEMENT REMOVAL
- ▨ PAVED SHOULDER REMOVAL

**PROPOSED LEGEND**

- Ⓐ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- Ⓑ SUBBASE GRANULAR MATERIAL, TYPE B 6"
- Ⓒ TOPSOIL EXCAVATION AND PLACEMENT
- Ⓓ HOT-MIX ASPHALT SHOULDERS, 4"  
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"  
AGGREGATE SUBGRADE IMPROVEMENT 12"
- Ⓔ AGGREGATE SHOULDERS, TYPE B 6"
- Ⓕ STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS

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STATION			20200100		21101505			20200200	20400800
			EARTH EXCAVATION	EARTH EXCAVATION VOLUME USED (15% SHRINKAGE)	TOPSOIL EXCAVATION AND PLACEMENT	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)	ROCK EXCAVATION	FURNISHED EXCAVATION
			(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)
<b>I-80</b>									
657+00.00	TO	658+00.00	79.7	67.8	21.9	0.0	67.8	41.5	
658+00.00	TO	658+50.00	79.7	67.8	21.9	0.0	67.8	588.7	
658+50.00	TO	659+00.00	29.5	25.1	15.6	0.0	25.1	658.0	
659+00.00	TO	660+00.00	29.5	25.1	15.6	0.0	25.1	110.7	
<b>OLD PLANK ROAD TRAIL</b>									
199+91.79	TO	200+00.00	1.4	1.2	3.0	2.5	-1.3	0.0	
200+00.00	TO	201+00.00	9.1	7.7	61.5	154.1	-146.4	0.0	
201+00.00	TO	202+00.00	2.6	2.2	75.7	236.5	-234.3	0.0	
202+00.00	TO	202+48.37	1.4	1.2	43.3	131.2	-130.0	0.0	
202+48.37	TO	202+64.11	0.5	0.4	16.5	87.5	-87.1	0.0	
202+64.11	TO	202+79.57	0.2	0.2	9.0	61.0	-60.8	0.0	
<b>BRIDGE</b>									
204+82.47	TO	205+02.18	0.3	0.2	10.6	31.5	-31.3	0.0	
205+02.18	TO	205+17.71	1.2	1.0	14.7	34.7	-33.7	0.0	
205+17.71	TO	206+00.00	52.6	44.7	66.9	58.1	-13.4	0.0	
206+00.00	TO	207+00.00	67.2	57.1	58.9	14.4	42.7	0.0	
207+00.00	TO	207+71.14	12.9	11.0	18.2	7.5	3.5	0.0	
207+71.14	TO	208+00.00	2.6	2.2	2.1	1.0	1.2	0.0	
<b>TOTALS</b>			<b>371.0</b>	<b>315.0</b>	<b>456.0</b>	<b>821.0</b>	<b>-506.0</b>	<b>1399.0</b>	<b>506.0</b>

TOPSOIL EXCAVATION AND PLACEMENT - EXCAVATION WILL BE APPROXIMATELY 8". PLACEMENT WILL BE AT MINIMUM 4". SEE GENERAL NOTE 13.

TREE REMOVAL (OVER 15 UNITS DIAMETER) (20100210)		
LOCATION		UNIT
STATION	L/R	
200+12	RT	24
200+47	LT	16
200+65	LT	16
202+07	LT	28
205+92	LT	24
<b>TOTAL</b>		<b>108</b>

TREE TRUNK PROTECTION (20101100)		
LOCATION		EACH
STATION	L/R	
199+47	LT	1
199+56	RT	1
199+65	LT	1
199+77	RT	1
199+80	LT	1
199+80	RT	1
199+87	LT	1
199+97	LT	1
200+07	LT	1
200+08	LT	1
200+20	LT	1
200+20	LT	1
200+36	LT	1
200+39	LT	1
200+39	LT	1
200+45	LT	1
200+55	LT	1
200+91	LT	1
200+91	LT	1
200+97	LT	1
201+24	LT	1
201+29	LT	1
201+96	LT	1
201+99	LT	1
202+32	LT	1
202+43	LT	1
206+36	LT	1
206+43	LT	1
206+53	LT	1
206+74	LT	1
206+95	LT	1
207+14	LT	1
207+14	LT	1
207+18	LT	1
207+32	RT	1
207+33	LT	1
207+35	RT	1
207+43	LT	1
207+43	LT	1
207+49	LT	1
207+49	LT	1
207+58	LT	1
207+61	LT	1
207+67	RT	1
207+69	LT	1
207+69	RT	1
207+74	LT	1
207+78	LT	1
207+90	LT	1
207+99	LT	1
208+05	LT	1
208+07	LT	1
208+10	RT	1
208+11	RT	1
208+13	RT	1
<b>TOTAL</b>		<b>55</b>

TREE REMOVAL (6 TO 15 UNITS DIAMETER) (20100110)		
LOCATION		UNIT
STATION	L/R	
200+07	LT	6
200+20	LT	6
200+20	LT	6
200+35	LT	6
200+56	LT	10
200+61	LT	8
200+61	LT	8
200+69	LT	6
200+94	LT	6
201+01	LT	6
201+01	LT	6
201+03	LT	8
201+03	LT	8
201+10	LT	6
201+10	LT	6
201+17	RT	6
201+18	LT	8
201+38	LT	6
201+38	LT	6
201+42	LT	12
201+54	LT	10
201+54	LT	6
201+56	RT	8
201+63	RT	12
201+65	LT	6
201+66	LT	12
201+78	LT	6
201+91	LT	6
201+93	LT	8
202+07	RT	10
<b>SUBTOTAL</b>		<b>224</b>

TREE REMOVAL (6 TO 15 UNITS DIAMETER) (20100110)		
LOCATION		UNIT
STATION	L/R	
202+16	LT	6
202+16	LT	6
202+16	LT	6
202+28	RT	6
202+31	LT	6
202+34	LT	6
202+34	LT	6
202+36	LT	8
202+56	RT	8
205+26	LT	6
205+31	RT	15
205+46	LT	6
205+67	LT	10
205+68	LT	8
205+75	LT	10
205+76	RT	10
205+84	LT	8
205+89	RT	6
205+89	RT	6
205+89	RT	6
205+89	RT	8
205+91	RT	8
205+91	RT	8
206+01	LT	8
206+11	LT	12
206+11	RT	8
206+16	RT	6
206+19	LT	6
206+19	RT	8
206+20	RT	6
206+28	RT	6
<b>SUBTOTAL</b>		<b>233</b>

TREE REMOVAL (6 TO 15 UNITS DIAMETER) (20100110)		
LOCATION		UNIT
STATION	L/R	
206+35	LT	6
206+35	LT	8
206+38	LT	8
206+43	LT	10
206+64	LT	12
206+88	LT	6
<b>SUBTOTAL</b>		<b>50</b>
<b>TOTAL</b>		<b>507</b>

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 PLOT DATE = 3/16/2016

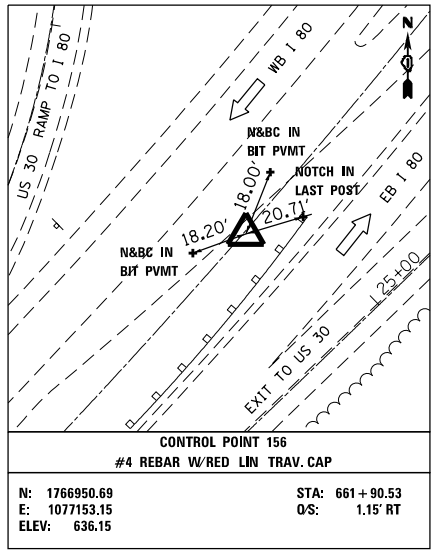
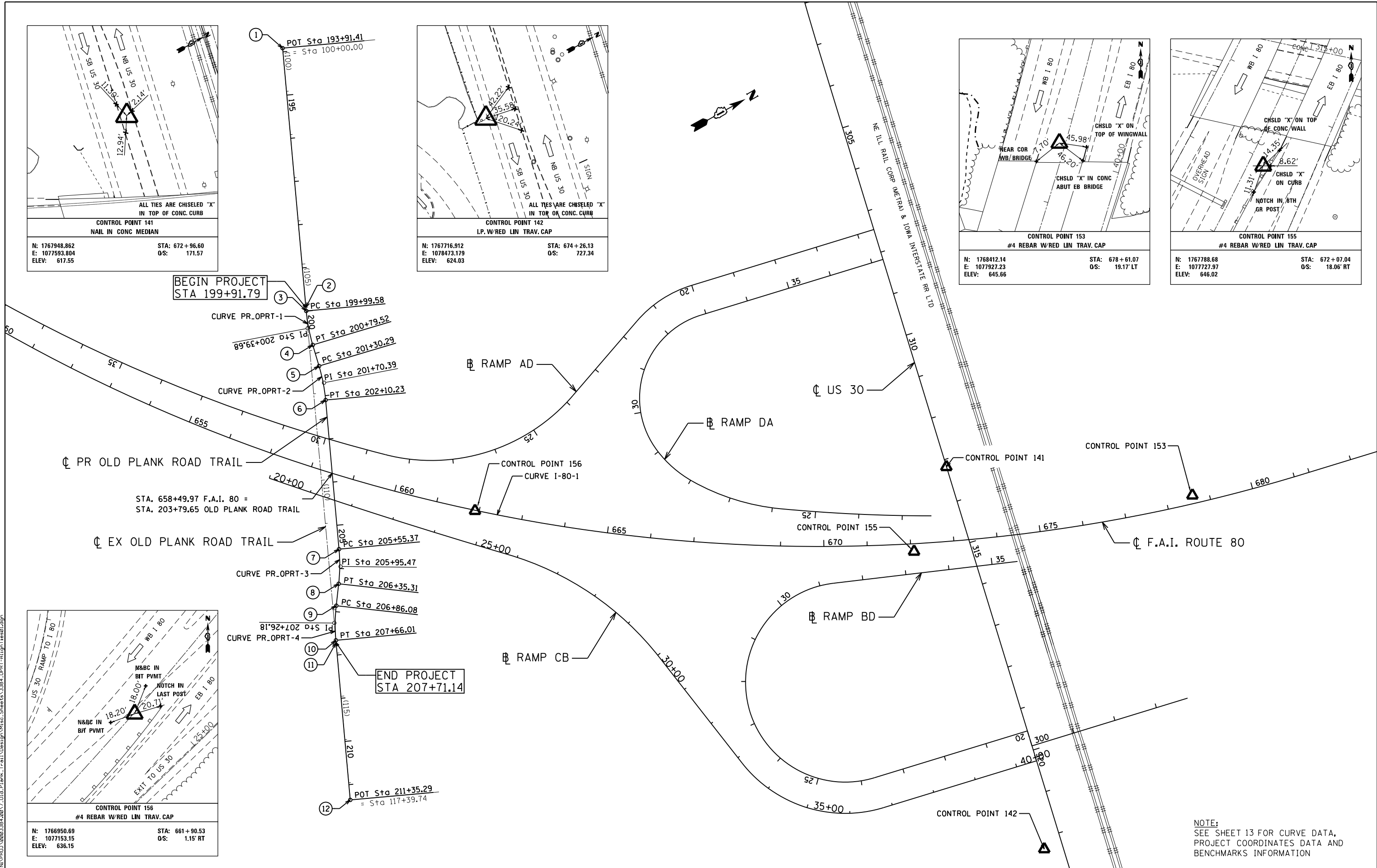
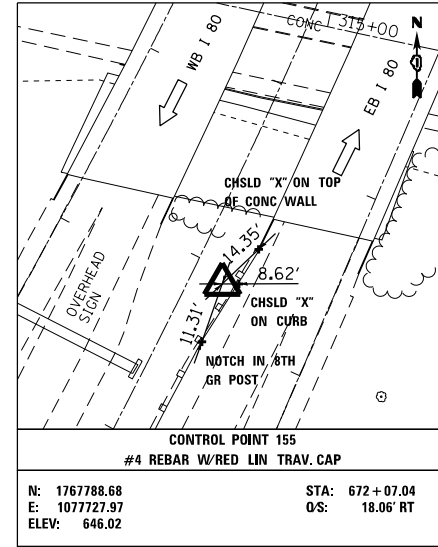
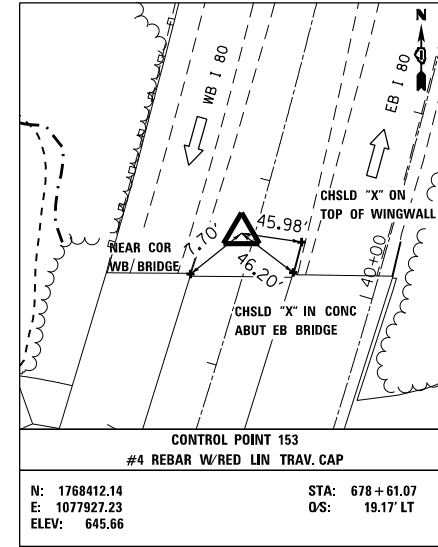
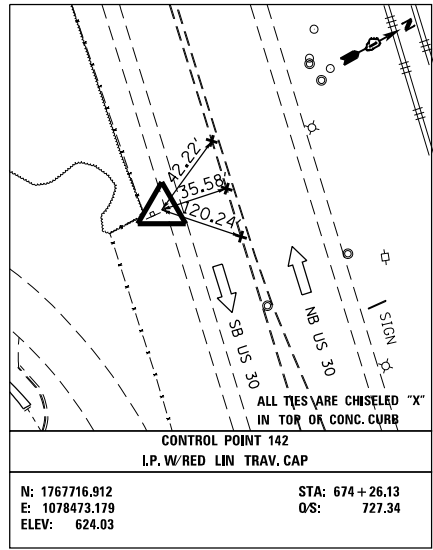
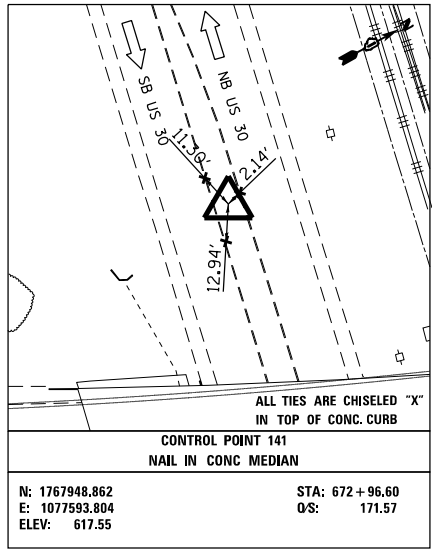
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 DRAWN - JB  
 CHECKED - DJO  
 DATE - 8/14/2014

REVISED -  
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 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80  
 ROADWAY SCHEDULES  
 SCALE: 1" = 50' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	11
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60V71	



NOTE:  
SEE SHEET 13 FOR CURVE DATA,  
PROJECT COORDINATES DATA AND  
BENCHMARKS INFORMATION

<p>ENGINEERING CONSULTANT</p> <p>Clorba Group, Inc.</p> <p>CONSULTING ENGINEERS</p> <p>8007 North Cumberland Avenue, Suite 402</p> <p>Chicago, Illinois 60625</p> <p>Tel. 773.775.4009 Fax 773.775.4014</p> <p>Email: cll@clorba.com</p>	USER NAME = ahern	DESIGNED - EPS	REVISED -	<p>STATE OF ILLINOIS</p> <p>DEPARTMENT OF TRANSPORTATION</p>	<p>OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80</p> <p>ALIGNMENTS, TIES, AND BENCHMARKS</p>		F.A.I. RTE. = 80	SECTION = 99-4-1-SB-B-R	COUNTY = WILL	TOTAL SHEETS = 46	SHEET NO. = 12
	PLOT SCALE = 200.0000 "/>										
PLOT DATE = 3/16/2016	CHECKED - DJO	REVISIED -	DATE = 8/14/2014	SCALE: 1" = 50'	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 60V71				
							ILLINOIS FED. AID PROJECT				



**CURVE DATA - F.A.I. ROUTE 80**

PROP. CURVE I-80-1  
 PI STA. = 660+41.68  
 $\Delta = 76^\circ 06' 07''$  (LT)  
 D = 1° 30' 00"  
 R = 3,819.53'  
 T = 2,989.62'  
 L = 5,073.21'  
 E = 1,030.89'  
 P.C. STA = 630+52.06  
 P.T. STA = 681+25.27

F.A.I. ROUTE 80	P.C. STATION	630+52.06	1,765,646.051	1,074,394.757
F.A.I. ROUTE 80	P.T. STATION	681+25.27	1,768,662.740	1,078,009.844

**CURVE DATA - OLD PLANK ROAD TRAIL**

PROP. CURVE PR_OPRT-1 PI STA. = 200+39.68 $\Delta = 11^\circ 27' 00''$ (LT) D = 14° 19' 26" R = 400.00' T = 40.10' L = 79.94' E = 2.01' P.C. STA = 199+99.58 P.T. STA = 200+79.52	PROP. CURVE PR_OPRT-2 PI STA. = 201+70.39 $\Delta = 11^\circ 27' 00''$ (RT) D = 14° 19' 26" R = 400.00' T = 40.10' L = 79.94' E = 2.01' P.C. STA = 201+30.29 P.T. STA = 202+10.23	PROP. CURVE PR_OPRT-3 PI STA. = 205+95.47 $\Delta = 11^\circ 27' 00''$ (RT) D = 14° 19' 26" R = 400.00' T = 40.10' L = 79.94' E = 2.01' P.C. STA = 205+55.37 P.T. STA = 206+35.31	PROP. CURVE PR_OPRT-4 PI STA. = 207+26.18 $\Delta = 11^\circ 27' 00''$ (LT) D = 14° 19' 26" R = 400.00' T = 40.10' L = 79.94' E = 2.01' P.C. STA = 206+86.08 P.T. STA = 207+66.01
--	--	--	--

1	OLD PLANK RD TRAIL	P.O.T. STATION	193+91.41	1,767,082.606	1,076,005.674
2	OLD PLANK RD TRAIL	BEGIN PROJECT	199+91.79	1,766,838.416	1,076,554.152
3	OLD PLANK RD TRAIL	P.C. STATION	199+99.58	1,766,835.246	1,076,561.271
4	OLD PLANK RD TRAIL	P.T. STATION	200+79.52	1,766,810.222	1,076,637.049
5	OLD PLANK RD TRAIL	P.C. STATION	201+30.29	1,766,799.191	1,076,686.607
6	OLD PLANK RD TRAIL	P.T. STATION	202+10.23	1,766,774.167	1,076,762.385
7	OLD PLANK RD TRAIL	P.C. STATION	205+55.37	1,766,633.788	1,077,077.692
8	OLD PLANK RD TRAIL	P.T. STATION	206+35.31	1,766,594.219	1,077,146.995
9	OLD PLANK RD TRAIL	P.C. STATION	206+86.08	1,766,564.773	1,077,188.354
10	OLD PLANK RD TRAIL	P.T. STATION	207+66.01	1,766,525.204	1,077,257.657
11	OLD PLANK RD TRAIL	END PROJECT	207+71.14	1,766,523.119	1,077,262.341
12	OLD PLANK RD TRAIL	P.O.T. STATION	211+35.29	1,766,375.009	1,077,595.011

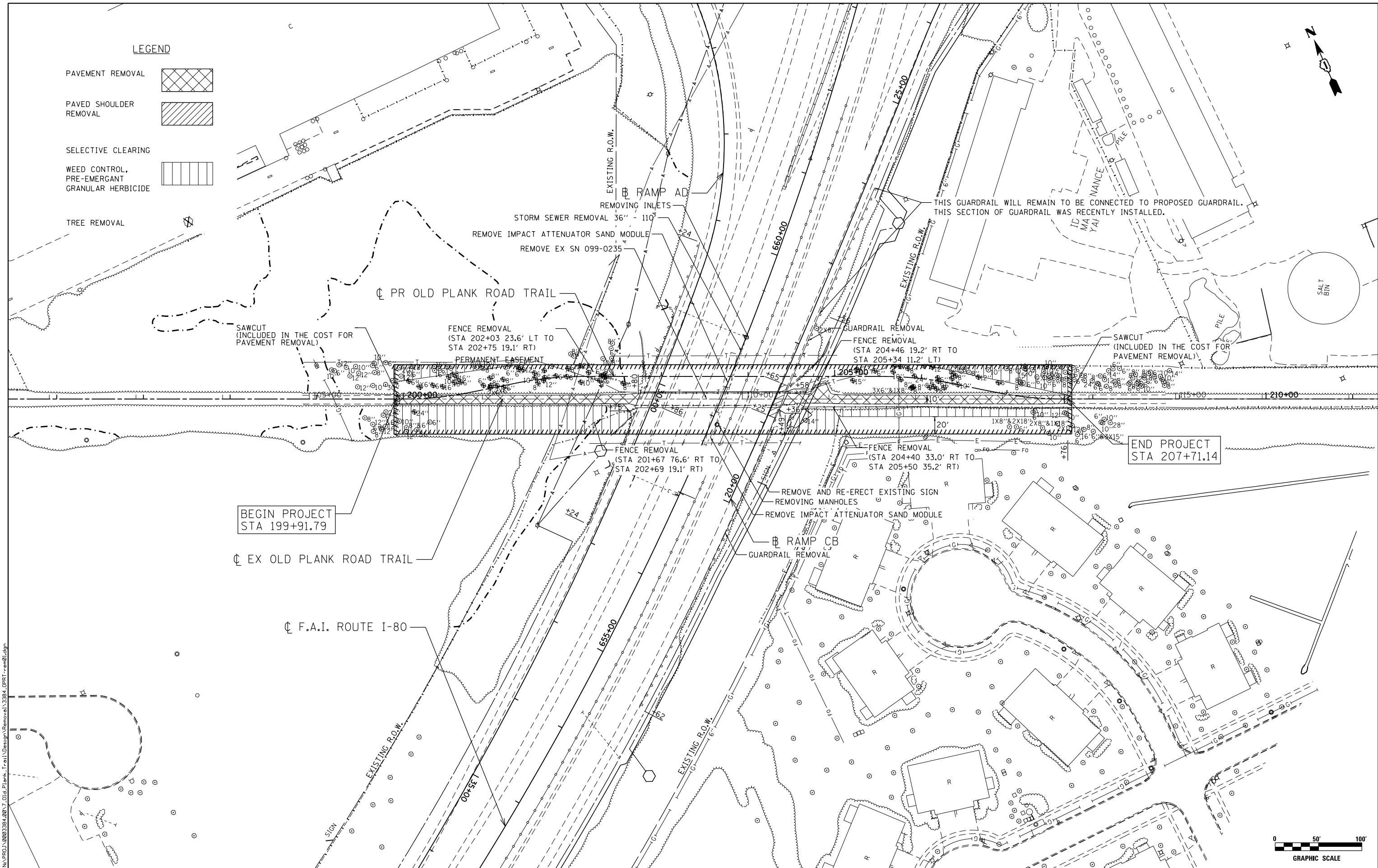
<b>BM#</b>	<b>DESCRIPTION</b>	<b>ELEVATION</b>
BM52	CHISELED "X" ON NE CORNER OF CONCRETE SLAB AROUND MEDIAN PIER FOR OLD PLANK RD TRAIL BRIDGE OVER I 80	632.38
BM51	CHISELED SQUARE ON TOP OF CONCRETE FOUNDATION IN MEDIAN TO OVERHEAD SIGN TRUSS (APPROX STA. 671+50)	646.92

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<b>ENGINEERING CONSULTANT</b> Clorba Group, Inc. CONSULTING ENGINEERS <small>8007 North Cumberland Avenue, Suite 402          Chicago, Illinois 60656          Tel. 773.775.4009 Fax 773.775.4014          Email: clorbagp@clorba.com</small>	USER NAME = ehern	DESIGNED - EPS	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80</b> <b>ALIGNMENTS, TIES, AND BENCHMARKS</b>	F.A.I. RTE. = 80	SECTION = 99-4-1-SB-B-R	COUNTY = WILL	TOTAL SHEETS = 46	SHEET NO. = 13
	PLOT SCALE = 200.0115' / in.	CHECKED - DJO	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 60V71	
	PLOT DATE = 3/16/2016	DATE = 8/14/2014	REVISED -			ILLINOIS FED. AID PROJECT				

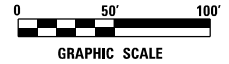
**LEGEND**

- PAVEMENT REMOVAL
- PAVED SHOULDER REMOVAL
- SELECTIVE CLEARING
- WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE
- TREE REMOVAL



BEGIN PROJECT  
STA 199+91.79

END PROJECT  
STA 207+71.14



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 Email: cti@clorba.com

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 PLOT SCALE = 100.0000 "/math>
 <math>1/1000</math> in.  
 PLOT DATE = 3/16/2016

DESIGNED - EPS  
 DRAWN - JB  
 CHECKED - DJJ  
 DATE - 8/14/2014

REVISED -  
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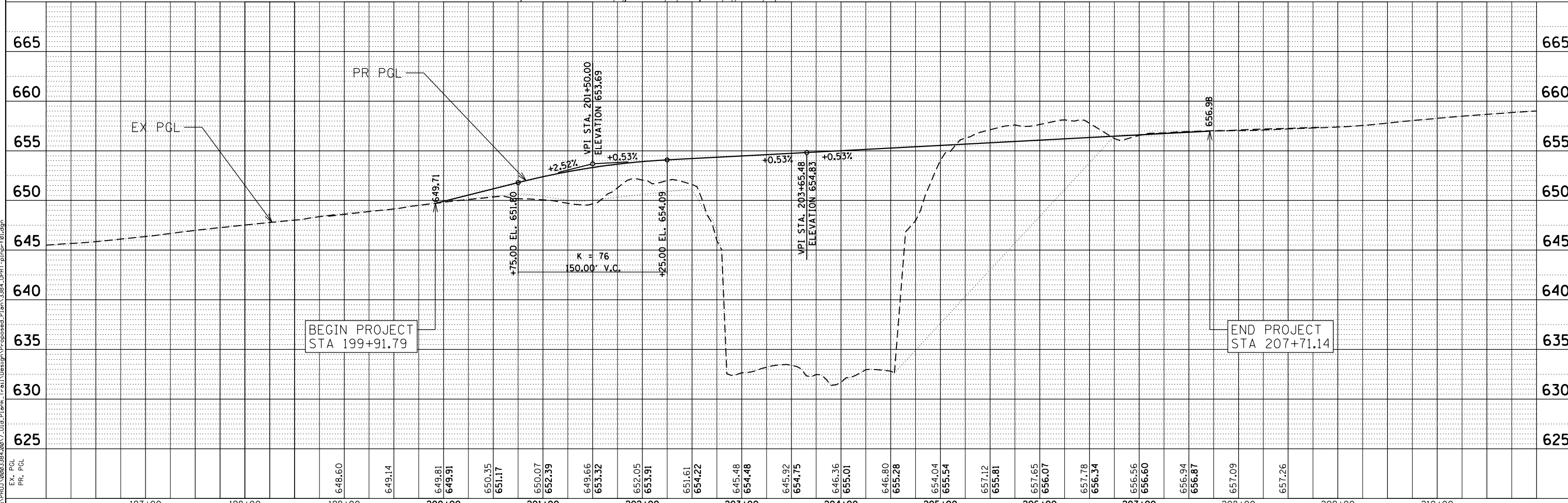
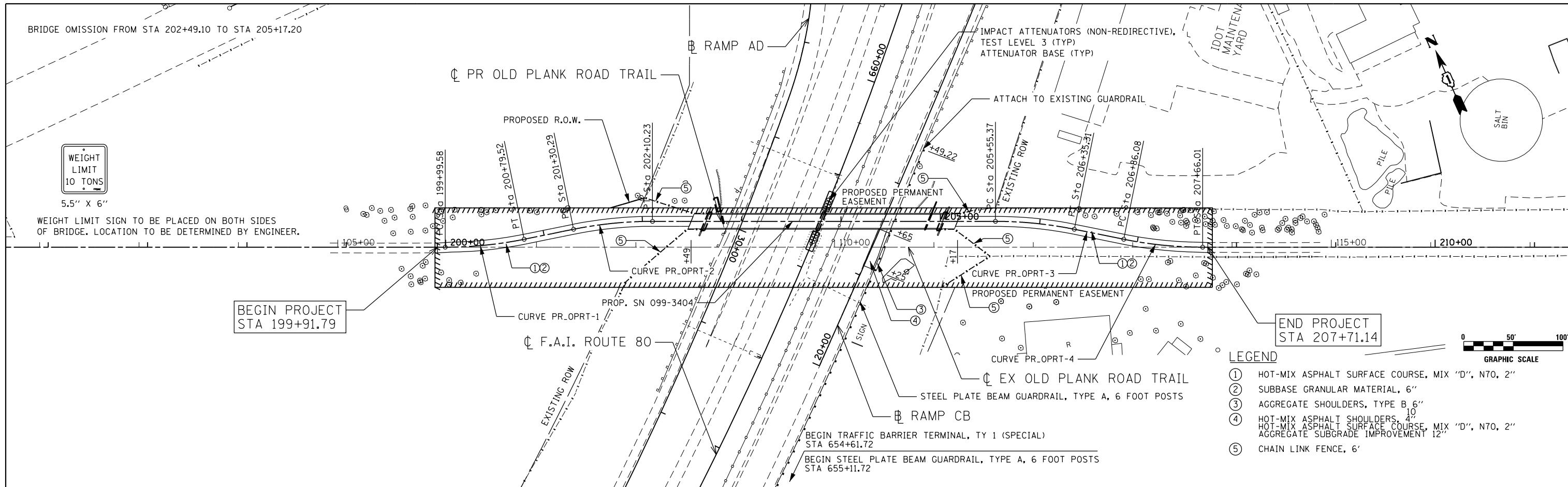
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80  
 REMOVAL PLAN**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	14
CONTRACT NO. 60V71				
ILLINOIS FED. AID PROJECT				

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197+00	198+00	199+00	200+00	201+00	202+00	203+00	204+00	205+00	206+00	207+00	208+00	209+00	210+00									
			648.60	649.14	649.81 649.91	650.35 651.17	650.07 652.39	649.66 653.32	652.05 653.91	651.61 654.22	645.48 654.48	645.92 654.75	646.36 655.01	646.80 655.28	654.04 655.54	657.12 655.81	657.65 656.07	657.78 656.34	656.56 656.60	656.94 656.81	657.09	657.26

STAGING AND TRAFFIC CONTROL GENERAL NOTES

1. THE STAGING AND TRAFFIC CONTROL PLANS SHALL SERVE AS A GUIDE FOR SAFE MOVEMENT OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY IMPROVE OR MODIFY THE STAGING AND TRAFFIC CONTROL PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE MOT PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
2. THE ENGINEER SHALL BE INFORMED 21 DAYS IN ADVANCE OF ANY CHANGE TO THE STAGING AND TRAFFIC CONTROL PLANS.
3. TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATIONS SHOWN ON THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND WITHIN 30 MINUTES FROM THE TIME OF NOTIFICATION BY THE ENGINEER TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION, IMPROVEMENT, OR MODIFICATION OF THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
4. ALL TRAFFIC CONTROL DEVICES USED FOR TRAFFIC CONTROL AND PROTECTION, AS DETAILED ON THE PLANS, SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS SPECIFIED IN TRAFFIC CONTROL AND PROTECTION SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.
5. THE CONTRACTOR SHALL COVER OR REMOVE ALL CONFLICTING EXISTING SPEED LIMIT SIGNS, GUIDE SIGNS, OR ANY OTHER CONFLICTING SIGNS FOR THE DURATION OF THE CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT THE METHOD IN WHICH THE SIGNS WILL BE COVERED TO THE ENGINEER FOR APPROVAL.
6. LOCATIONS OF CHANGEABLE MESSAGE SIGNS SHALL BE DETERMINED BY THE ENGINEER.
7. THE EXISTING DRAINAGE SYSTEM SHALL BE UTILIZED THROUGHOUT THE MAINTENANCE OF TRAFFIC CONSTRUCTION STAGING.
8. PRIOR TO REMOVING TRAFFIC CONTROL AND OPENING CLOSED PAVEMENT AREAS TO TRAFFIC, THE CONTRACTOR SHALL SWEEP THE PAVEMENT SURFACE CLEAN. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
9. THE CONTRACTOR SHALL CONTACT THE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT (847)705-4155 A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK OR PLACING ANY SIGNS OR TRAFFIC CONTROL DEVICES ON I-80.
10. THE OLD PLANK ROAD TRAIL BRIDGE OVER I-80 IS PROPOSED FOR RELOCATION EAST OF THE EXISTING STRUCTURE ALMOST ENTIRELY WITHIN THE EXISTING PERMANENT EASEMENT. CLOSURE OF THE EXISTING TRAIL IS NOT ANTICIPATED AS THE EXISTING STRUCTURE AND APPROACHES WILL REMAIN OPERATIONAL AS THE RELOCATED APPROACHES ARE BEING CONSTRUCTED. PER COORDINATION WITH THE FPDWC, CLOSURE OF THE TRAIL DURING CONSTRUCTION WILL BE MINIMIZED TO THE EXTENT POSSIBLE. A SHORT TERM CLOSURE MAY BE REQUIRED FOR THE PLACEMENT OF THE NEW STRUCTURE OVER I-80 AND SHIFTING OF BICYCLE TRAFFIC TO THE NEW BRIDGE.
11. EXISTING HIGH TENSION CABLE MEDIAN BARRIER TO REAMIN IN FUNCTION DURING CONSTRUCTION. ANY DAMAGE TO THE CABLE BARRIER DUE TO CONSTRUCTION ACTIVITIES SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST.
12. A 3.5 FOOT CLEAR ZONE SHALL BE MAINTAINED FROM THE BACK OF THE TEMPORARY CONCRETE BARRIER WALL TO ANY OBSTRUCTION OR DROP OFF. IF THE 3.5 FOOT CLEAR ZONE CANNOT BE MAINTAINED, THE TEMPORARY CONCRETE BARRIER SHALL BE ANCHORED TO THE PAVEMENT THROUGH THE 3 ANCHORING HOLES ON THE TRAFFIC SIDE OF THE TEMPORARY CONCRETE BARRIER WALL USING CONNECTING PINS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE PAY ITEM TEMPORARY CONCRETE BARRIER WALL.

STAGING AND TRAFFIC CONTROL DESCRIPTION

STAGE I

**CONSTRUCTION:**

**F&I-80**

CONSTRUCT PIER AND ABUTMENTS FOR OLD PLANK TRAIL STRUCTURE OVER I-80. THE WORK ZONE FOR THE PIER AND ABUTMENTS SHALL NOT BE ACTIVE AT THE SAME TIME.

PLACE OLD PLANK ROAD TRAIL SUPER STRUCTURE OVER I-80.

CONSTRUCT OLD PLANK TRAIL AS SHOWN IN THE ROADWAY PLAN.

**TRAFFIC CONTROL:**

**F&I-80**

ERECT TEMPORARY CONCRETE BARRIER USING STANDARD 701400 AND 701401 DURING ALLOWABLE NIGHT TIME HOURS SPECIFIED IN THE KEEPING THE EXPRESSWAY OPEN TO TRAFFIC SPECIAL PROVISION.

TEMPORARY CONCRETE BARRIER SHALL BE UTILIZED TO PROVIDE POSITIVE SEPARATION BETWEEN THE WORK ZONE AS SHOWN IN THE TRAFFIC CONTROL PLANS.

SHOULDER CLOSURE AS SHOWN IN THE TRAFFIC CONTROL PLANS. MAINTAIN TRAFFIC IN EXISTING LANES.

WHEN REQUIRED TRAFFIC WILL BE STOPPED USING A 15-MINUTE MAXIMUM FULL STOP PROCEDURE LISTED IN THE KEEPING THE EXPRESSWAY OPEN TO TRAFFIC SPECIAL PROVISION.

STAGE II

**CONSTRUCTION:**

**F&I-80**

REMOVE EXISTING OLD PLANK ROAD TRAIL SUPERSTRUCTURE, PIERS AND ABUTMENTS AS SHOWN IN THE STRUCTURAL PLANS. THE WORK ZONE FOR THE PIER AND ABUTMENTS SHALL NOT BE ACTIVE AT THE SAME TIME.

CONSTRUCT HMA SHOULDER, AGGREGATE SHOULDER, AND GUARDRAIL AS SHOWN IN THE ROADWAY PLAN.


REMOVE EXISTING TRAIL AS SHOWN IN THE REMOVAL PLAN.

**TRAFFIC CONTROL:**

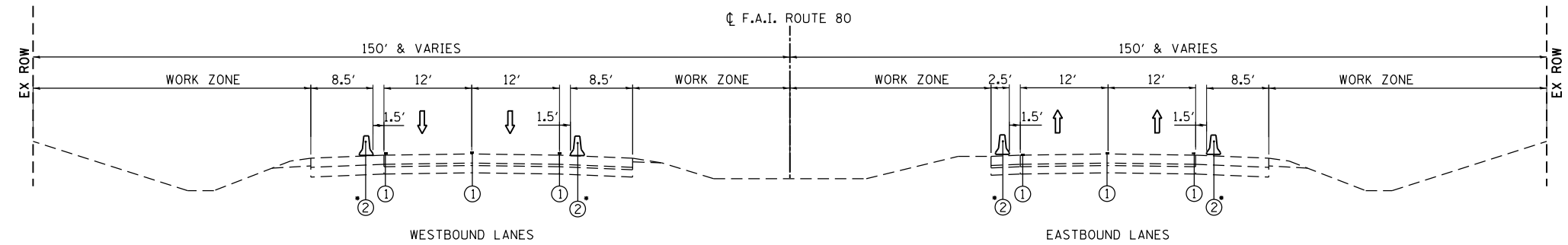
**F&I-80**

MAINTAIN TRAFFIC AS SHOWN IN STAGE I.

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	PLOT SCALE = 2.0000' / in.	CHECKED - DJO	REVISED -			CONTRACT NO. 60V71				
PLOT DATE = 3/16/2016	DATE = 8/14/2014	REVISED -	SCALE: 1" = 50'	SHEET NO. OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				





STAGE I AND II SUGGESTED TYPICAL SECTION  
(LOOKING EASTWARD)  
STA 656+00.00 TO 660+00.00

- TEMPORARY CONCRETE BARRIER LOCATIONS
- WB OUTSIDE - STA 655+73 TO STA 660+07
- WB INSIDE - STA 655+77 TO STA 660+80
- EB INSIDE - STA 655+26 TO 660+24
- EB OUTSIDE - STA 655+90 TO 660+32


**LEGEND**

- ① EXISTING PAVEMENT MARKING
- ② TEMPORARY CONCRETE BARRIER WALL

**NOTES:**

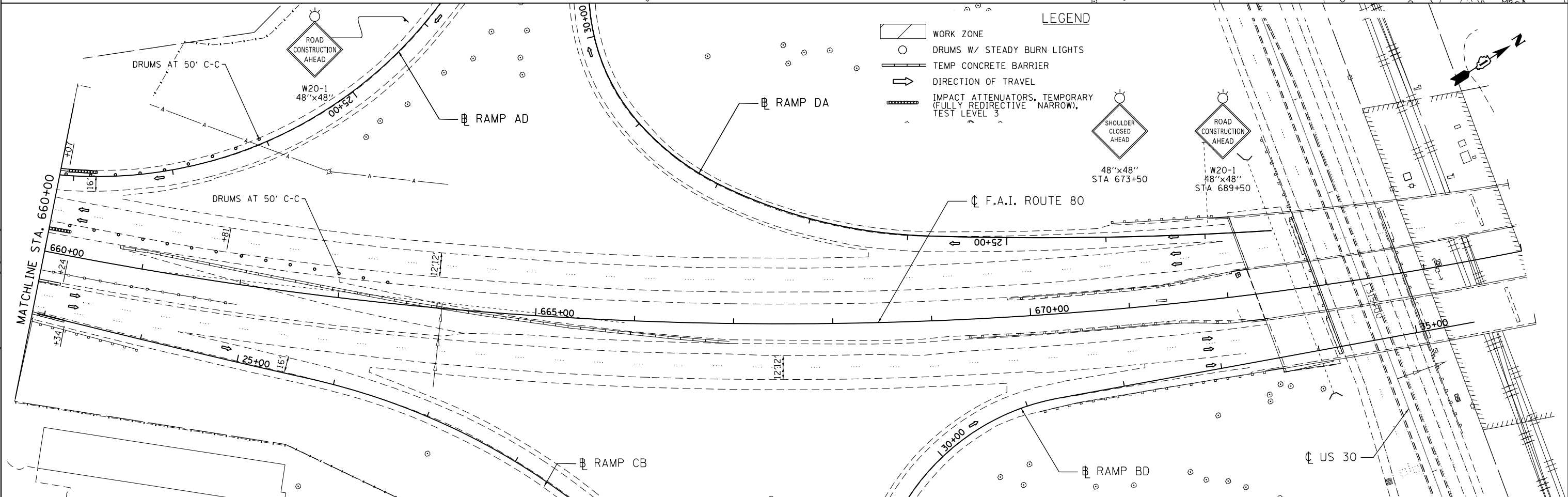
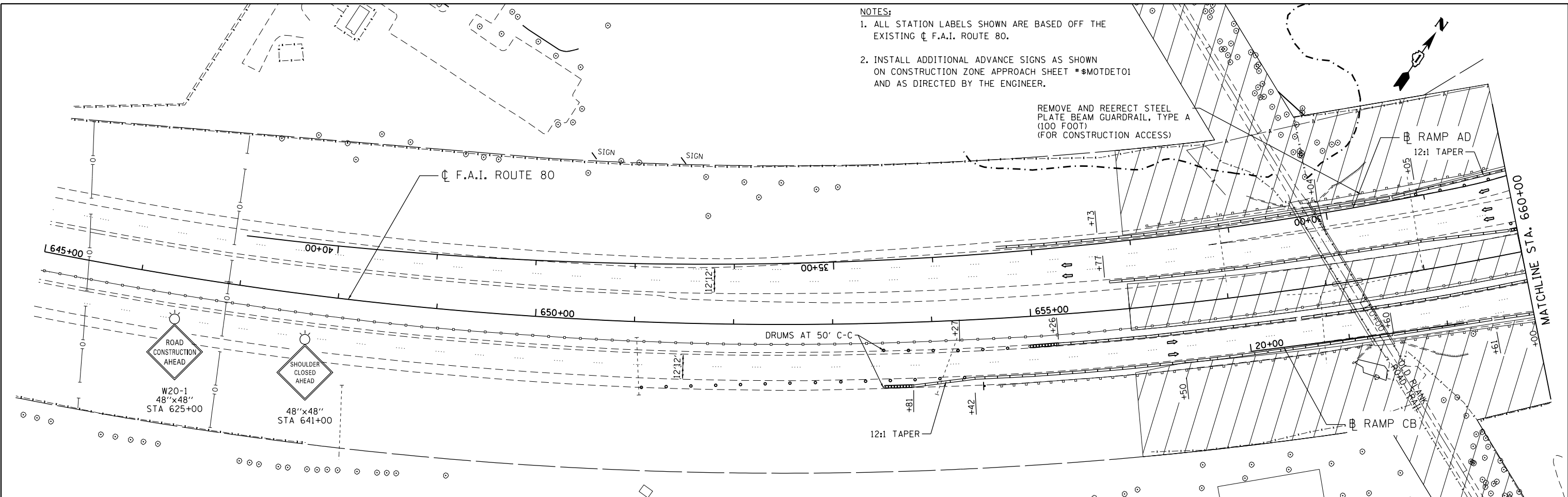
A MINIMUM DISTANCE OF 1.5 FOOT SHALL BE MAINTAINED BETWEEN THE LANE LINE AND THE TEMPORARY CONCRETE BARRIER THROUGHOUT THE PROJECT.

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	PLOT SCALE = 20.0000' / 1" = PLOT DATE = 3/16/2016	SCALE:      SHEET NO.      OF      SHEETS      STA.      TO STA.				

- NOTES:**
1. ALL STATION LABELS SHOWN ARE BASED OFF THE EXISTING C.F.A.I. ROUTE 80.
  2. INSTALL ADDITIONAL ADVANCE SIGNS AS SHOWN ON CONSTRUCTION ZONE APPROACH SHEET #SMDTETO1 AND AS DIRECTED BY THE ENGINEER.

REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A (100 FOOT) (FOR CONSTRUCTION ACCESS)



- LEGEND**
- WORK ZONE
  - DRUMS W/ STEADY BURN LIGHTS
  - TEMP CONCRETE BARRIER
  - DIRECTION OF TRAVEL
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE NARROW), TEST LEVEL 3

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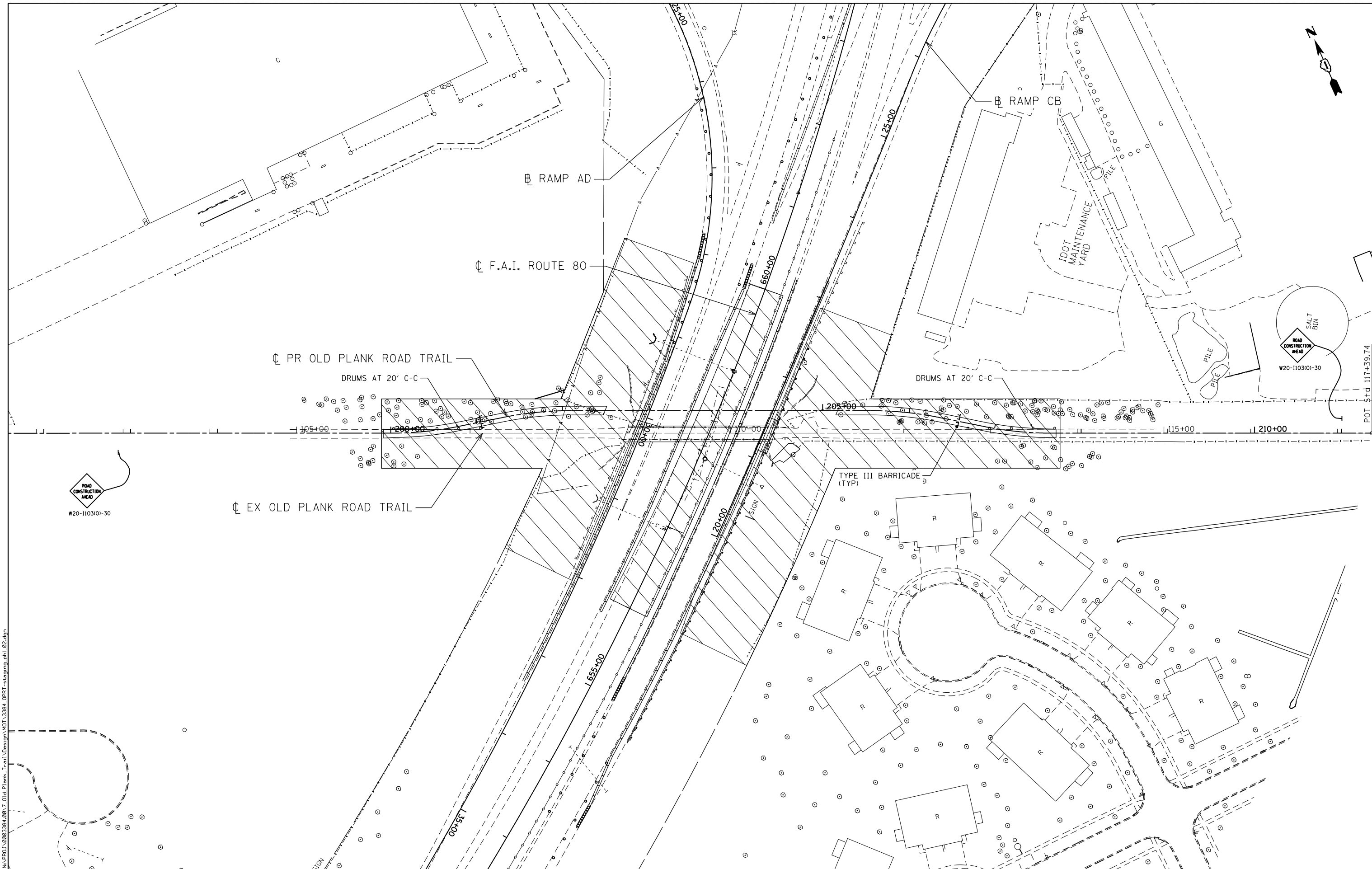
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PLOT SCALE = 100.0000' / in.	DRAWN - JB	REVISED -
PLOT DATE = 3/16/2016	CHECKED - DJO	REVISED -
	DATE - 8/14/2014	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80**  
**STAGING AND TRAFFIC CONTROL - STAGE I & II**

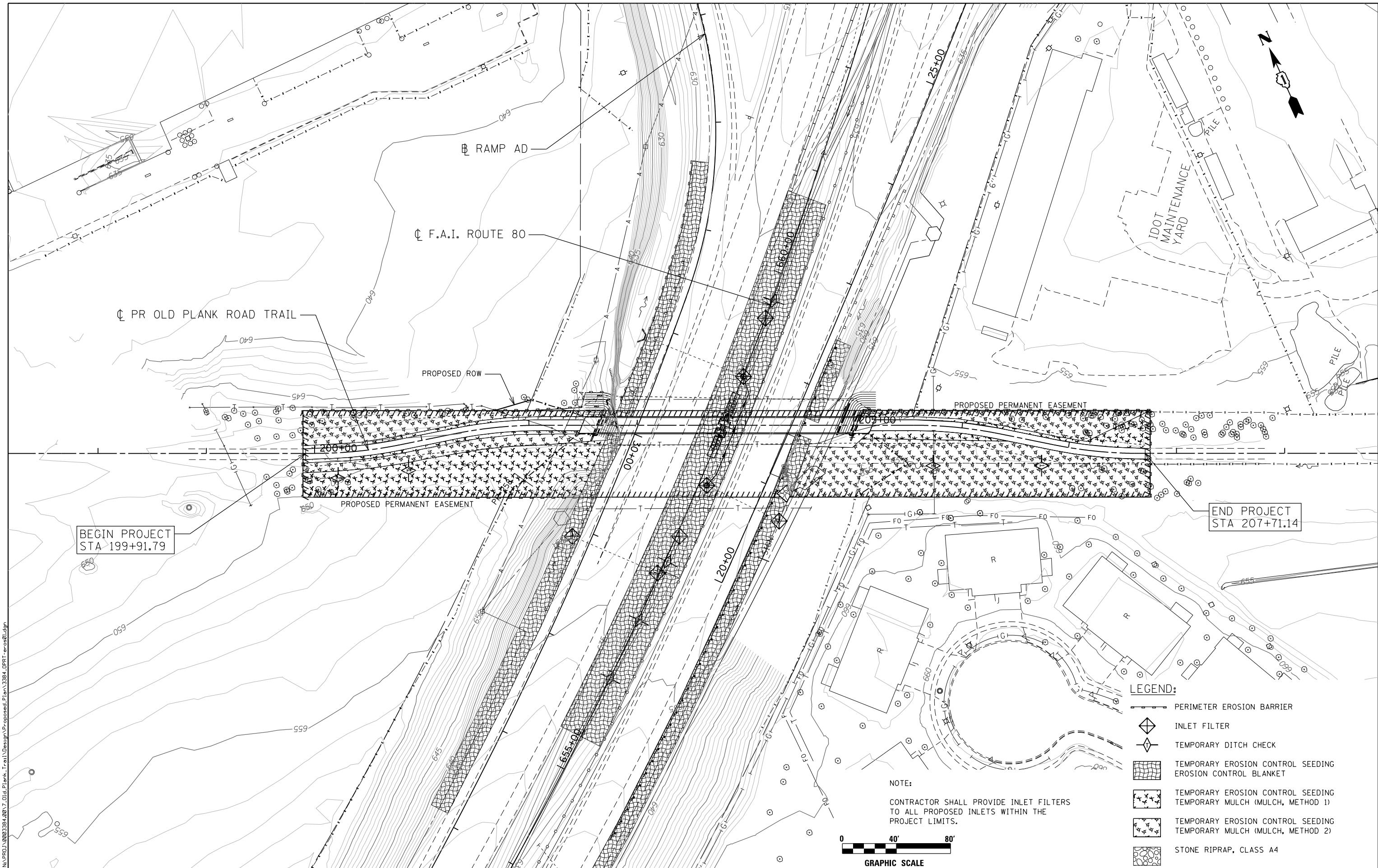
SCALE: 1" = 50' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	18
CONTRACT NO. 60V71				
ILLINOIS FED. AID PROJECT				



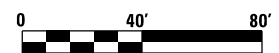
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 <b>Ciorba Group, Inc.</b> CONSULTING ENGINEERS 8007 North Cumberland Avenue, Suite 402 Chicago, Illinois 60655 Tel. 773.775.4009 Fax 773.775.4014 Email: cgr@cginc.com	USER NAME = espino	DESIGNED - DMM DRAWN - DMM	REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80</b> <b>STAGING AND TRAFFIC CONTROL - STAGE I &amp; II</b>	F.A.I. RTE. 80	SECTION 99-4-1-SB-B-R	COUNTY WILL	TOTAL SHEETS 46	SHEET NO. 19
	PLOT SCALE = 100.0000' / in.	CHECKED - TW DATE - 8/14/2014	SCALE: 1" = 50' SHEET NO. OF SHEETS STA. TO STA.			<b>CONTRACT NO. 60V71</b>				
	PLOT DATE = 4/6/2016	ILLINOIS FED. AID PROJECT								



- LEGEND:**
- PERIMETER EROSION BARRIER
  - ◇ INLET FILTER
  - ◇ TEMPORARY DITCH CHECK
  - ▨ TEMPORARY EROSION CONTROL SEEDING
  - ▨ EROSION CONTROL BLANKET
  - ★ TEMPORARY EROSION CONTROL SEEDING
  - ★ TEMPORARY MULCH (MULCH, METHOD 1)
  - TEMPORARY EROSION CONTROL SEEDING
  - TEMPORARY MULCH (MULCH, METHOD 2)
  - STONE RIPRAP, CLASS A4

**NOTE:**  
 CONTRACTOR SHALL PROVIDE INLET FILTERS TO ALL PROPOSED INLETS WITHIN THE PROJECT LIMITS.



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USER NAME = ahern	DESIGNED -	REVISED -
PLOT SCALE = 80.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 3/16/2016	CHECKED -	REVISED -
	DATE = 8/14/2014	REVISED -

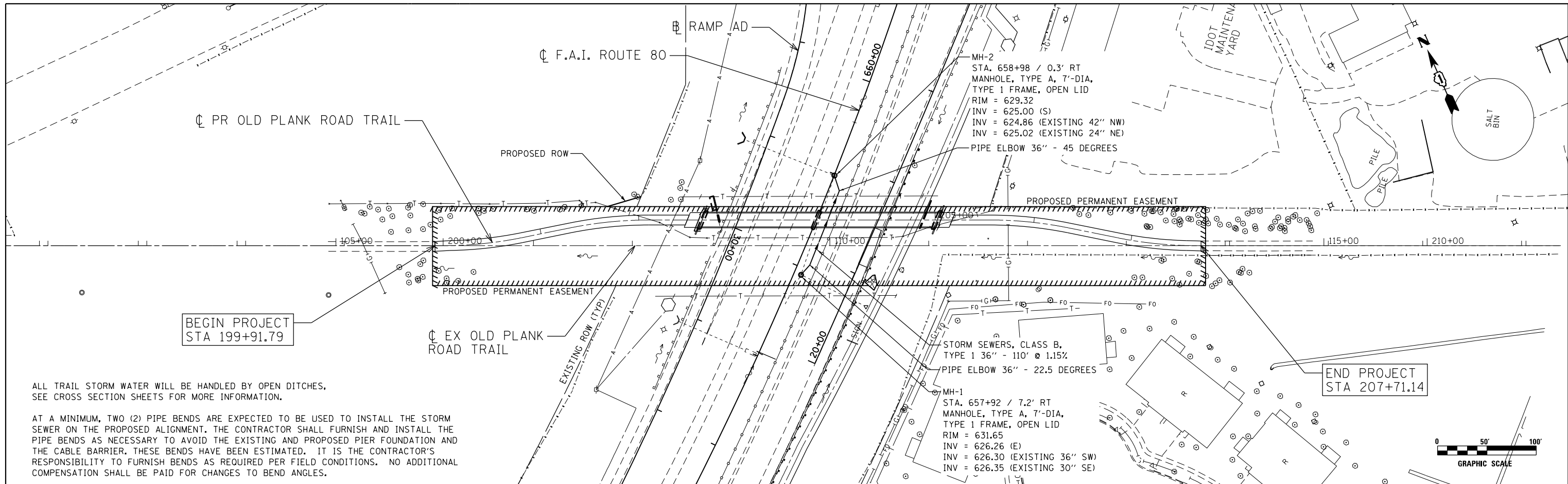
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80  
 EROSION AND SEDIMENT CONTROL PLAN**

SCALE: 1" = 40' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	20
CONTRACT NO. 60V71				
ILLINOIS FED. AID PROJECT				

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ALL TRAIL STORM WATER WILL BE HANDLED BY OPEN DITCHES, SEE CROSS SECTION SHEETS FOR MORE INFORMATION.

AT A MINIMUM, TWO (2) PIPE BENDS ARE EXPECTED TO BE USED TO INSTALL THE STORM SEWER ON THE PROPOSED ALIGNMENT. THE CONTRACTOR SHALL FURNISH AND INSTALL THE PIPE BENDS AS NECESSARY TO AVOID THE EXISTING AND PROPOSED PIER FOUNDATION AND THE CABLE BARRIER. THESE BENDS HAVE BEEN ESTIMATED. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FURNISH BENDS AS REQUIRED PER FIELD CONDITIONS. NO ADDITIONAL COMPENSATION SHALL BE PAID FOR CHANGES TO BEND ANGLES.



197+00	198+00	199+00	200+00	201+00	202+00	203+00	204+00	205+00	206+00	207+00	208+00	209+00	210+00																						
648.60	649.14	649.81	649.91	650.35	651.17	650.07	652.39	649.66	653.32	652.05	653.91	651.61	654.22	645.48	654.48	645.92	654.75	646.36	655.01	646.80	655.28	654.04	655.54	657.12	655.81	657.65	656.07	657.78	656.34	656.56	656.60	656.94	656.87	657.09	657.26

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 Email: cigrp@clorba.com

USER NAME = ahern	DESIGNED - DMM	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN - DMM	REVISED -
PLOT DATE = 3/16/2016	CHECKED - TW	REVISED -
	DATE - 8/14/2014	REVISED -

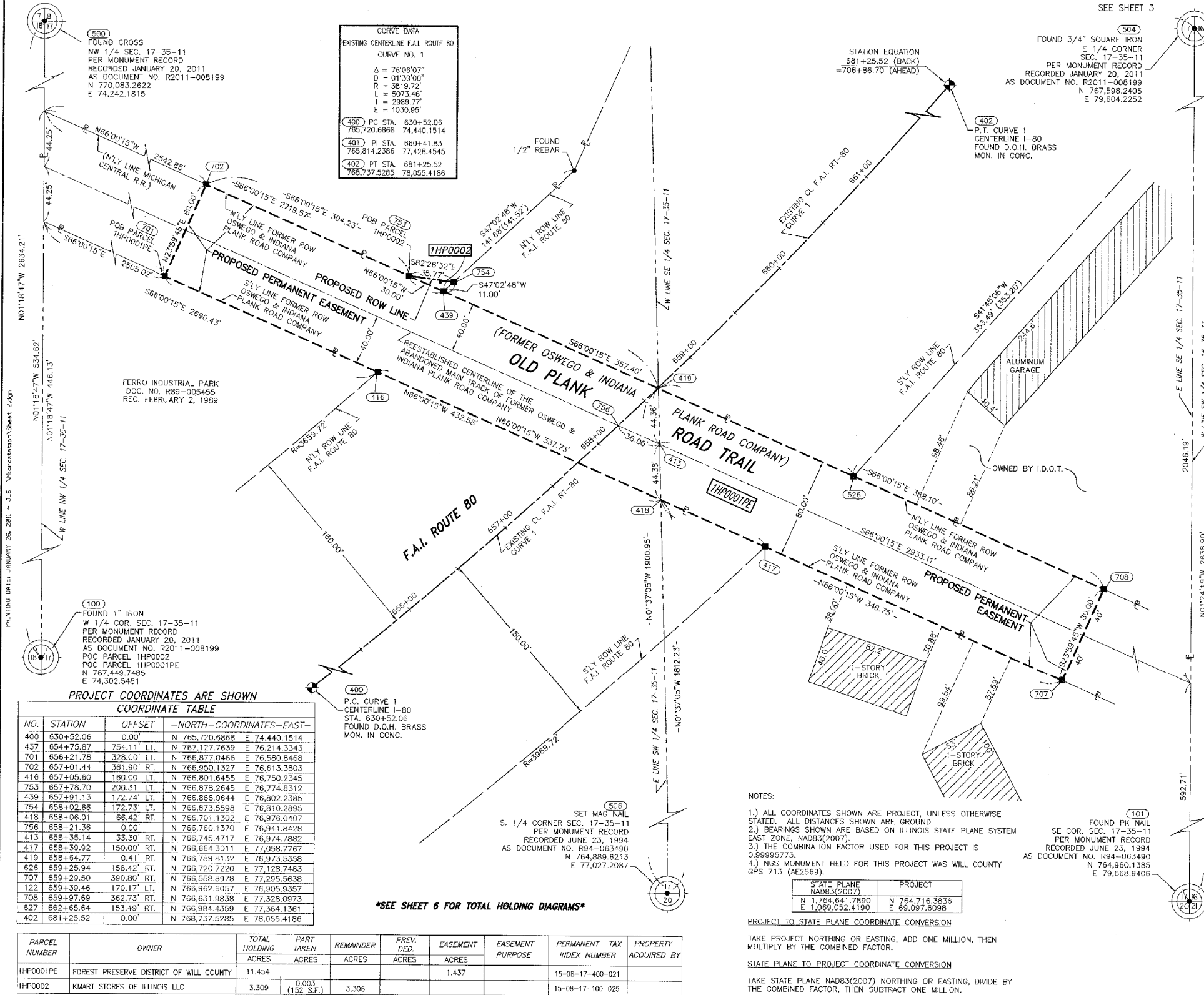
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80  
 PROPOSED DRAINAGE PLAN AND PROFILE**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	21
CONTRACT NO. 60V71				
ILLINOIS FED. AID PROJECT				

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PART OF THE S. 1/2 OF SECTION 17 IN T35N, R11E OF THE 3rd PM, NEW LENOX TOWNSHIP, WILL COUNTY, ILLINOIS.



**CURVE DATA**  
EXISTING CENTERLINE F.A.I. ROUTE 80  
CURVE NO. 1

Δ = 76°06'07"
D = 01°30'00"
R = 3819.72'
L = 5073.46'
T = 2989.77'
E = 1030.95'

400 PC STA. 630+52.06  
765,720.6868 74,440.1514  
401 PI STA. 660+41.83  
765,814.2386 77,428.4545  
402 PT STA. 681+25.52  
768,737.9285 78,055.4186

**LEGEND**

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APL
- APPARENT PROPERTY LINE
- CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- AC
- EXISTING ACCESS CONTROL LINE
- PROPOSED ACCESS CONTROL LINE
- MEASURED DIMENSION METRIC WITH IMPERIAL
- COMPUTED DIMENSION METRIC WITH IMPERIAL
- RECORD DATA
- EXISTING BUILDING

IRON PIPE OR ROD FOUND  
CUT CROSS FOUND OR SET  
T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION  
T2 SET 5/8 INCH REBAR FLUSH WITH GROUND TO BE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP AND BEARING SURVEYORS PROFESSIONAL NUMBER.  
T3 BY COLORED PLASTIC CAP AND BEARING SURVEYORS PROFESSIONAL NUMBER.  
BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION, BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.  
BT2 BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.  
BT3 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.  
M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.  
SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS PROFESSIONAL NUMBER.  
M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.  
BURIED 5/8 INCH REBAR 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS PROFESSIONAL NUMBER.  
PERMANENT SURVEY MARKER, IDCT STD. 2135 (TO BE SET BY OTHERS)  
RIGHT OF WAY STAKING PROPOSED TO BE SET.

PRINTING DATE: JANUARY 26, 2011 - 3:15 Microstation/Sheet 2.dgn

**PROJECT COORDINATES ARE SHOWN COORDINATE TABLE**

NO.	STATION	OFFSET	NORTH	EAST
400	630+52.06	0.00'	N 765,720.6868	E 74,440.1514
437	654+75.87	754.11' LT.	N 767,127.7639	E 76,214.3343
701	656+21.78	328.00' LT.	N 766,877.0466	E 76,580.8468
702	657+01.44	361.90' RT.	N 766,950.1327	E 76,613.3803
416	657+05.60	180.00' LT.	N 766,801.8455	E 76,750.2345
703	657+29.70	200.31' LT.	N 766,878.2645	E 76,774.8312
439	657+31.13	172.74' LT.	N 766,868.0844	E 76,802.2385
754	658+02.66	172.73' LT.	N 766,873.5598	E 76,810.2895
418	658+06.01	66.42' RT.	N 766,701.1302	E 76,976.0407
756	658+21.36	0.00'	N 766,760.1370	E 76,941.8428
413	658+35.14	33.30' RT.	N 766,745.4717	E 76,974.7882
417	658+39.92	150.00' RT.	N 766,864.3011	E 77,058.7767
419	658+64.77	0.41' RT.	N 766,789.8132	E 76,973.5358
626	659+25.94	158.42' RT.	N 766,720.7220	E 77,128.7483
707	659+29.50	390.80' RT.	N 766,558.8978	E 77,295.5638
122	659+39.46	170.17' LT.	N 766,962.6057	E 76,905.9357
708	659+97.69	362.73' RT.	N 766,631.9838	E 77,328.0973
627	662+65.64	153.49' RT.	N 766,984.4359	E 77,364.1361
402	681+25.52	0.00'	N 768,737.9285	E 78,055.4186

PARCEL NUMBER	OWNER	TOTAL HOLDING ACRES	PART TAKEN ACRES	REMAINDER ACRES	PREV. DED. ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT TAX INDEX NUMBER	PROPERTY ACQUIRED BY
IHP0001PE	FOREST PRESERVE DISTRICT OF WILL COUNTY	11.454				1.437		15-08-17-400-021	
IHP0002	KMART STORES OF ILLINOIS LLC	3.309	0.003 (152 SF.)	3.306				15-08-17-100-025	

F.A.I. ROUTE 80 (US 30 TO US 45), JOB. NO. R-91-012-10

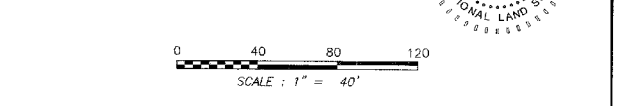
- STATE OF ILLINOIS )  
COUNTY OF WILL ) SS
- THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 17, TOWNSHIP 35 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.
- DATED AT JOLIET, ILLINOIS THIS 28<sup>TH</sup> DAY OF JANUARY, 2011 A.D.
- David A. Claassen*  
DAVID A. CLAASSEN VICE PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002962  
LICENSE EXPIRES NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

STATE OF ILLINOIS )  
COUNTY OF WILL ) SS

THIS IS TO CERTIFY THAT WE, CLAASSEN, WHITE & ASSOCIATES, P.C., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION NUMBER 184-004039, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 17, TOWNSHIP 35 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, WILL COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. THIS SURVEY CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

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**Claassen, White & Associates, P.C.**  
121 AIRPORT DRIVE, UNIT 1  
JOLIET, ILLINOIS 60431  
(815) 744-3720

SHEET 1 IS A COVER SHEET AND NOT RECORDED.

**PLAT OF HIGHWAYS  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.I. ROUTE 80**

SECTION US 30 TO US 45 WILL COUNTY  
PROJECT JOB NO. R-91-012-10  
STATION 630+50 TO STATION 681+50  
SCALE: 1" = 40' SHEET 2 OF 9

**BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196-1096**

**NOTES:**

- 1.) ALL COORDINATES SHOWN ARE PROJECT, UNLESS OTHERWISE STATED. ALL DISTANCES SHOWN ARE GROUND.
- 2.) BEARINGS SHOWN ARE BASED ON ILLINOIS STATE PLANE SYSTEM EAST ZONE, NAD83(2007).
- 3.) THE COMBINATION FACTOR USED FOR THIS PROJECT IS 0.99995773.
- 4.) NGS MONUMENT HELD FOR THIS PROJECT WAS WILL COUNTY GPS 713 (AE2569).

PROJECT TO STATE PLANE COORDINATE CONVERSION  
TAKE PROJECT NORTHING OR EASTING, ADD ONE MILLION, THEN MULTIPLY BY THE COMBINED FACTOR.

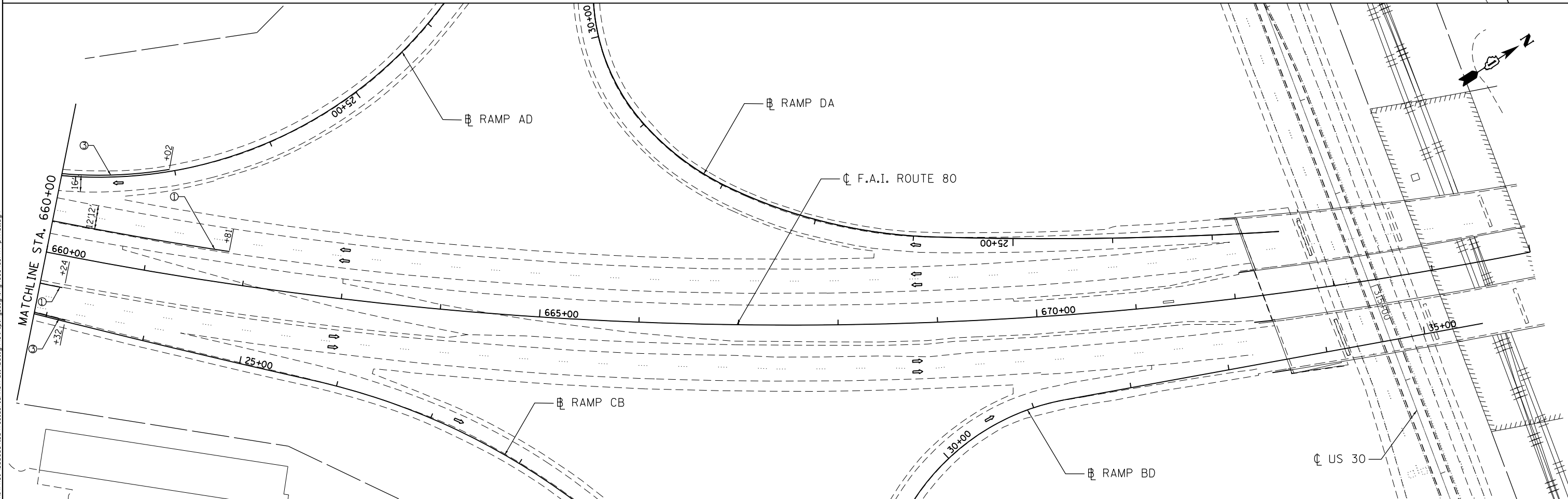
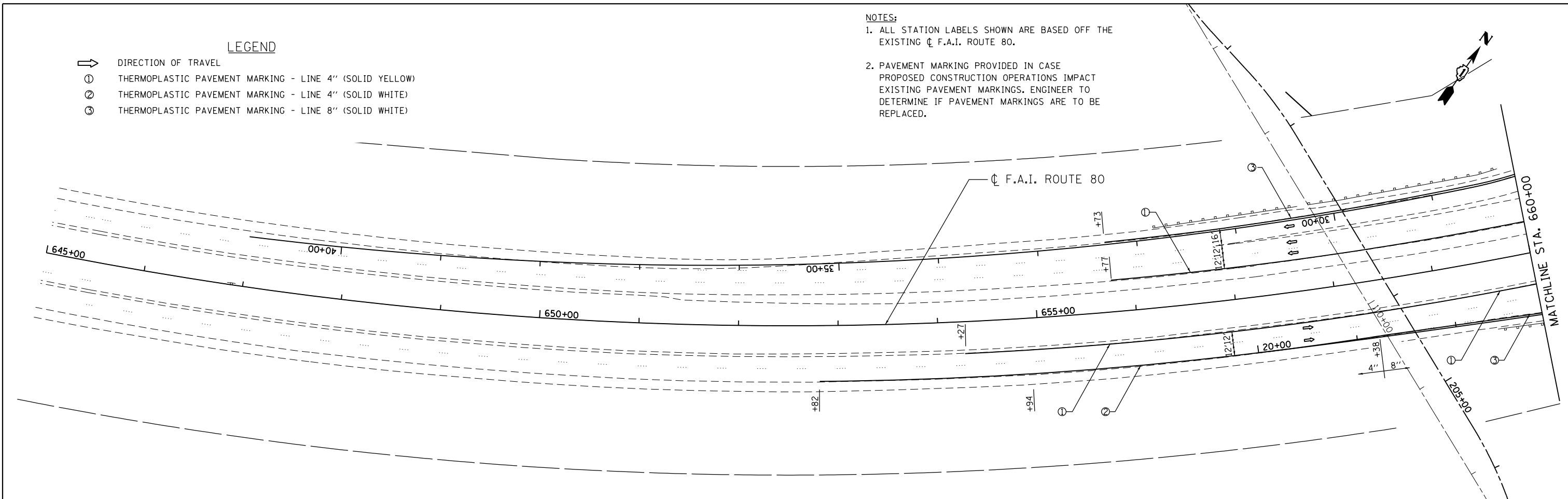
STATE PLANE TO PROJECT COORDINATE CONVERSION  
TAKE STATE PLANE NAD83(2007) NORTHING OR EASTING, DIVIDE BY THE COMBINED FACTOR, THEN SUBTRACT ONE MILLION.

REVISION DATE: REVISION: MADE BY:



- LEGEND**
- ➔ DIRECTION OF TRAVEL
  - ⊙ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID YELLOW)
  - ⊙ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID WHITE)
  - ⊙ THERMOPLASTIC PAVEMENT MARKING - LINE 8" (SOLID WHITE)

- NOTES:**
1. ALL STATION LABELS SHOWN ARE BASED OFF THE EXISTING C.F.A.I. ROUTE 80.
  2. PAVEMENT MARKING PROVIDED IN CASE PROPOSED CONSTRUCTION OPERATIONS IMPACT EXISTING PAVEMENT MARKINGS. ENGINEER TO DETERMINE IF PAVEMENT MARKINGS ARE TO BE REPLACED.



**ENGINEERING CONSULTANT**  
**Clorba Group, Inc.**  
 CONSULTING ENGINEERS  
 8007 North Cumberland Avenue, Suite 402  
 Chicago, Illinois 60655  
 Tel. 773.775.4009 Fax 773.775.4014  
 Email: cigrp@clorba.com

USER NAME = ahern	DESIGNED - EPS	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN - JB	REVISED -
PLOT DATE = 3/16/2016	CHECKED - DJO	REVISED -
	DATE - 8/14/2014	REVISED -

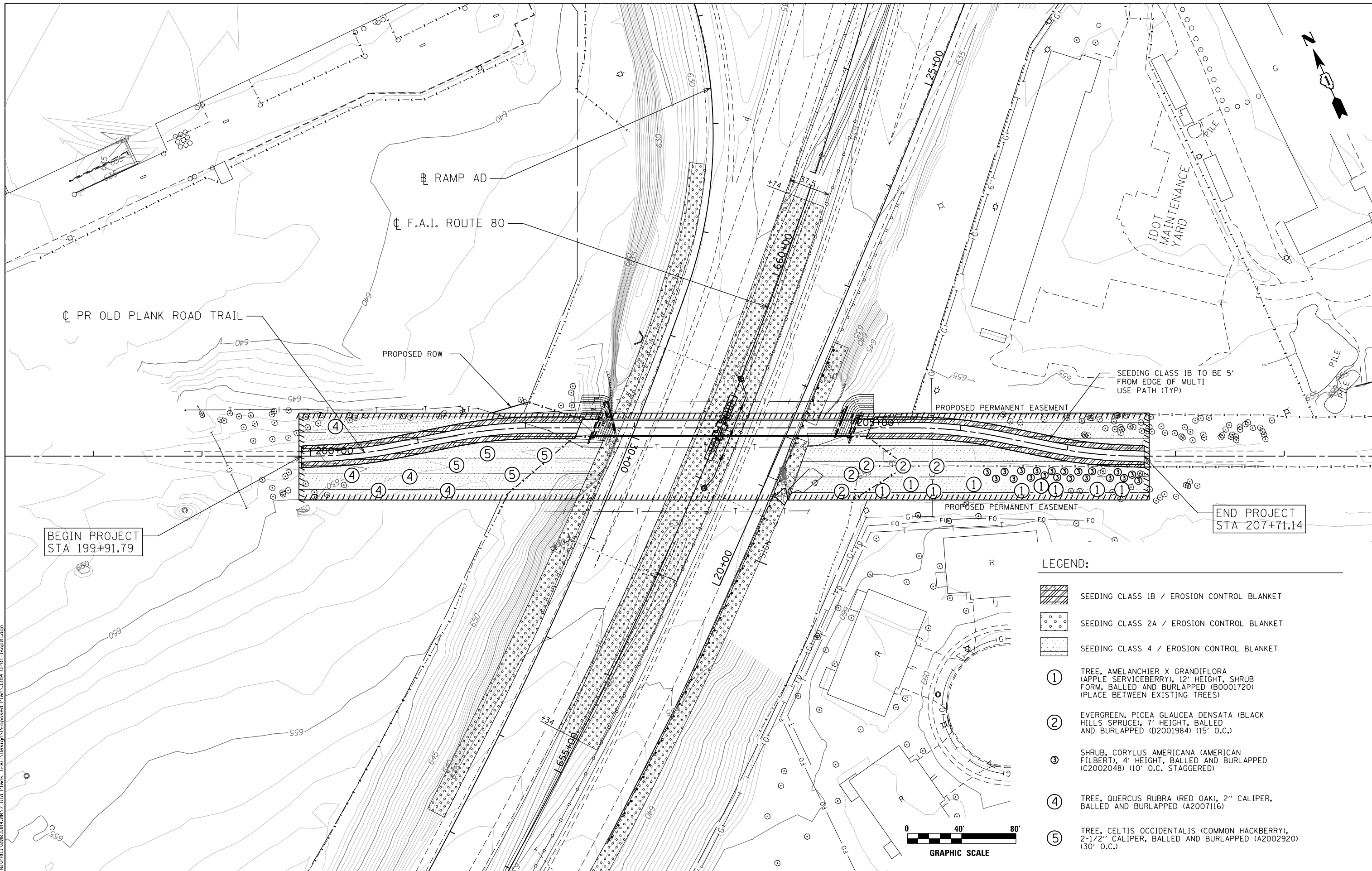
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80**  
**PAVEMENT MARKING PLAN**

SCALE: 1" = 50'    SHEET NO.    OF    SHEETS    STA.    TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	23
CONTRACT NO. 60V71				
ILLINOIS FED. AID PROJECT				

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- LEGEND:**
- SEEDING CLASS 1B / EROSION CONTROL BLANKET
  - SEEDING CLASS 2A / EROSION CONTROL BLANKET
  - SEEDING CLASS 4 / EROSION CONTROL BLANKET
  - ① TREE, AMELANCHIER X GRANDIFLORA (APPLE SERVICEBERRY), 12' HEIGHT, SHRUB FORM, BALLED AND BURLAPPED (B0001720) (PLACE BETWEEN EXISTING TREES)
  - ② EVERGREEN, PICEA GLAUCEA DENSATA (BLACK HILLS SPRUCE), 7' HEIGHT, BALLED AND BURLAPPED (D2001984) (15' O.C.)
  - ③ SHRUB, CORYLUS AMERICANA (AMERICAN FILBERT), 4' HEIGHT, BALLED AND BURLAPPED (C2002048) (10' O.C. STAGGERED)
  - ④ TREE, QUERCUS RUBRA (RED OAK), 2" CALIPER, BALLED AND BURLAPPED (A2007116)
  - ⑤ TREE, CELTIS OCCIDENTALIS (COMMON HACKBERRY), 2-1/2" CALIPER, BALLED AND BURLAPPED (A2002920) (30' O.C.)



**ENGINEERING CONSULTANT**  
**Clorba Group, Inc.**  
 CONSULTING ENGINEERS  
 8007 North Cumberland Avenue, Suite 402  
 Chicago, Illinois 60625  
 Tel. 773.775.4009 Fax 773.775.4014  
 Email: cigrp@clorba.com

USER NAME = ahern  
 PLOT SCALE = 80.0000' / 1" =  
 PLOT DATE = 3/16/2016

DESIGNED -  
 DRAWN -  
 CHECKED -  
 DATE - 8/14/2014

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80**  
**LANDSCAPING PLAN**  
 SCALE: 1" = 40' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	24
CONTRACT NO. 60V71				

ILLINOIS FED. AID PROJECT

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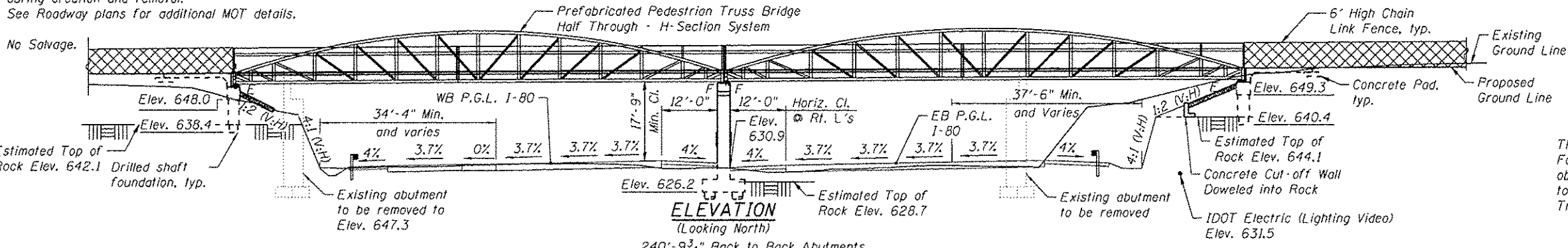


Benchmark: Brass Disk on northwest wingwall of existing bridge Sta. 658+03.23, 83.9' LT. 0/S. Elev. 650.75

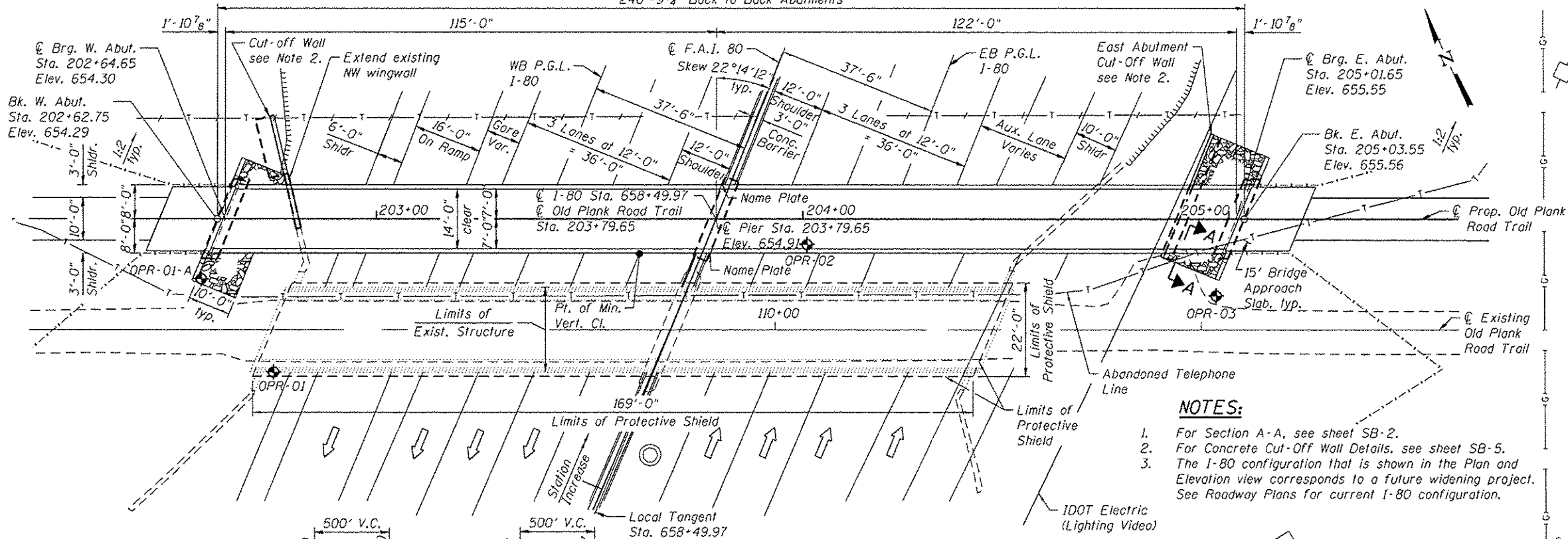
**Existing Structure:**  
 S.N. 099-0235 Built in 1964 under Section 99-4-1-SB as a Railroad Bridge. Superstructure consists of 2-span thru plate girder with span lengths of 94'-0" & 74'-6", 19'-0" center to center girders, 171'-6" face to face of backwalls. Substructure consists of reinforced concrete pier and abutments founded on spread footings on rock. Remove existing East Abut. & Pier to 2' below proposed grade and existing West Abut. to Elev. 647.3. Pedestrian bike traffic to be maintained on existing bridge during construction. Temporary lane closures will be utilized on I-80 during erection and removal. See Roadway plans for additional MOT details.

**SCOPE OF WORK**

1. Remove Existing Northeast wingwall to elevation 650.0 and Northwest wingwall to elevation 647.3 to erect proposed bridge.
2. Construct proposed substructure, concrete cut-off walls, extend NW wingwall, and erect proposed bridge.
3. Remove existing pier to 2' below proposed grade west abutment to elevation 647.3 to match existing bearing seat and entire east abutment.



The contractor will contact Karen Fonte of the Will County Forest Preserve District at 815-727-8700 in order to obtain a Special Use Permit minimally 48 hours prior to bridge placement or bridge removal of the Old Plank Trail bridge over I-80.



STATION 658+49.97  
 BUILT 2011 BY  
 STATE OF ILLINOIS  
 FOR THE FOREST PRESERVE  
 DISTRICT OF WILL COUNTY  
 F.A.I. RT. 80 SEC. 99-4-1-SB-B-R  
 LOADING H-10  
 STRUCTURE NO. 099-3404

**NAME PLATE**  
 See Std. 515001

**DESIGN STRESSES**  
**FIELD UNITS**

f<sub>c</sub> = 3,500 psi  
 f<sub>y</sub> = 60,000 psi (Reinforcement)  
 f<sub>y</sub> = 50,000 psi (M270 Grade 50W) - Truss

**SEISMIC DATA**

Seismic Performance Zone (SPZ) = 1  
 Design Spectral Acceleration at 1.0 sec. (S<sub>01</sub>) = .067g  
 Design Spectral Acceleration at 0.2 sec. (S<sub>05</sub>) = .124g  
 Soil Site Class = C

**DESIGN SPECIFICATIONS**

2009 AASHTO LRFD Guide Specifications for Design of Pedestrian Bridges

2010 AASHTO LRFD Bridge Design Specifications with 2010 Interims

**LOADING**

Uniform Live Loading = 90 psf  
 Vehicle Load = H-10 Load  
 (No allowance for future wearing surface)

**GENERAL PLAN AND ELEVATION**  
**OLD PLANK ROAD TRAIL OVER F.A.I. 80**

**SECTION 99-4-1-SB-B-R**

**WILL COUNTY**

**STATION 203+79.65**

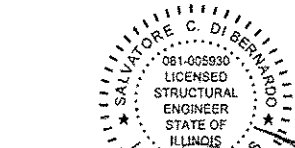
**STRUCTURE NO. 099-3404**

**F.A.I. - 80**  
**CURVE DATA**

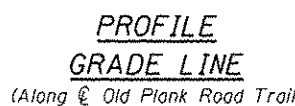
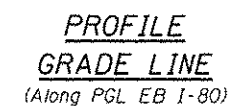
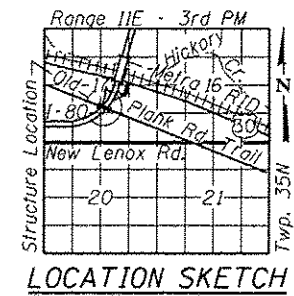
Δ = 76°06'07" (LT)  
 D = 1°30'00"  
 T = 2,989.62'  
 L = 5,073.21'  
 E = 1,030.89'  
 R = 3,819.53'  
 S.E. = 3.7%  
 P.C. = Sta. 630+52.05  
 P.T. = Sta. 681+25.27  
 P.I. = Sta. 660+41.68

**APPROVED**  
 For Structural Adequacy Only

*Salvatore C. DiBernardo*  
 Engineer of Bridges & Structures



DATE: 3/16/2016  
 SEAL EXPIRES: 11/30/2016



**LEGEND**

◆ Soil Boring

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

SHEET NO. SB-1 OF SB-8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	25

CONTRACT NO. 60V71  
 ILLINOIS FED. AID PROJECT

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 User: jgallagher  
 Plot Date: 3/16/2016  
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 Plot Date: 3/16/2016  
 User: jgallagher  
 Plot Date: 3/16/2016



USER NAME	DESIGNED	CHECKED	DRAWN	CHECKED	REVISED
ahorn	BWS	DL	RD	DL	-

**GENERAL NOTES:**

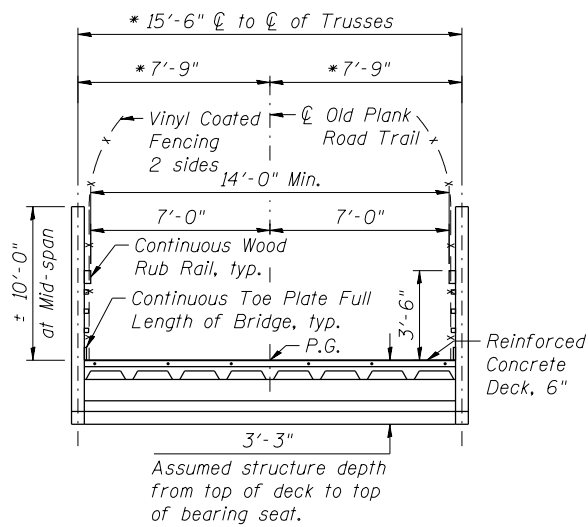
1. Reinforcement bars designated (E) shall be epoxy coated.
2. Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of 1/8" (0.01 ft.). Adjustment shall be made either by grinding the surface or by shimming the bearings.
3. The embankment configuration shown shall be the minimum that must be placed and compacted prior to construction of the abutments.
4. Concrete Sealer shall be applied to the designated areas:  
Abutments: inside face of backwall, top of bridge seat, and front face of abutment.  
Pier: top of bridge seat and the entire exposed surface of pier wall.
5. The Prefabricated Pedestrian Truss shall not be painted.

**INDEX OF SHEETS**

- SB-1 General Plan and Elevation
- SB-2 General Notes, Index of Sheets and Bill of Material
- SB-3 Approach Slab Details
- SB-4 Abutment Details I
- SB-5 Abutment Details II
- SB-6 Pier Details
- SB-7 Soil Boring Logs-1
- SB-8 Soil Boring Logs-2

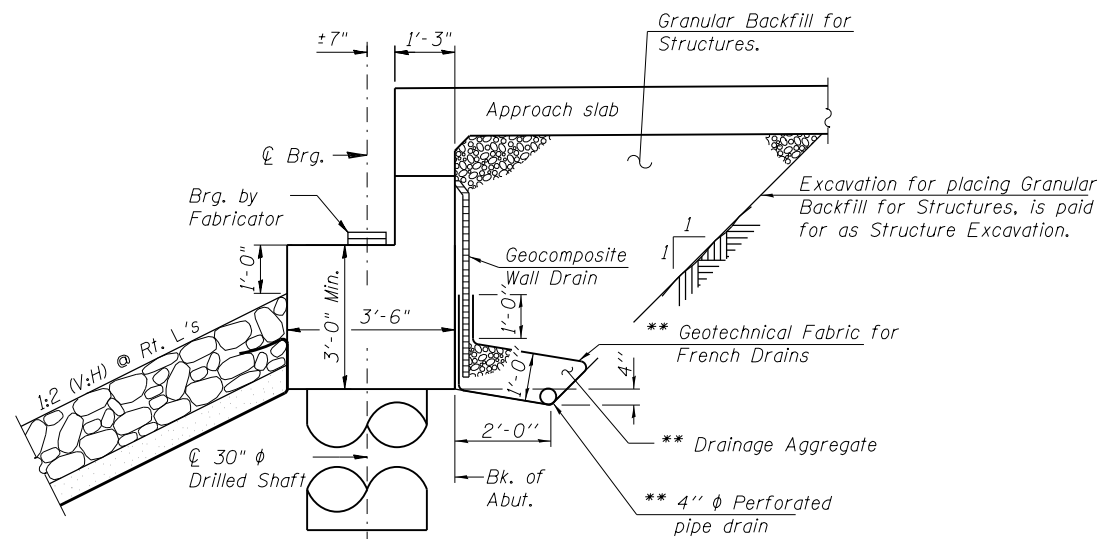
**TRUSS MANUFACTURER**

The substructure is designed per AASHTO LRFD and based on the assumed truss dead loads (including deck) shown below:  
 Total factored superstructure dead load at east and west abutment = 120,400 pounds.  
 Total factored superstructure dead load at center pier = 234,000 pounds.  
 Truss manufacturer shall camber the truss as necessary to provide allowance for dead load deflection.  
 Bridge bearing seat elevations are subject to revision based on the approved pedestrian truss superstructure shop drawings. Contractor shall verify all dimensions and elevations with final approved shop drawings.  
 Truss manufacturer shall provide the reinforced concrete deck design. Concrete deck to utilize stay-in-place galvanized forms. Reinforcement shall be epoxy coated. Contractor shall place the concrete deck after truss is set. Cost included with Pedestrian Truss Superstructure.



**CROSS SECTION THRU TRUSS**

\* Subject to refinement per Truss Manufacturer  
 Truss Supplier shall provide joint covers over all open joints between span units and at abutments. (typ. 3 locations)  
 Cost included with Pedestrian Truss Superstructure.

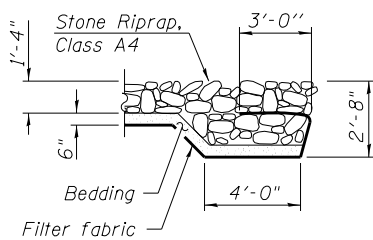


**SECTION THRU ABUTMENT**

(Dimensions @ right angles)

\*\* Included in the cost of Pipe Underdrains for Structures 4".

Note: All Drainage System components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).



**SECTION A-A**

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
Stone Riprap, Class A4	Sq Yd		65	65
Filter Fabric	Sq Yd		65	65
Removal Of Existing Structures	Each		1	1
Concrete Removal	Cu Yd		26.1	26.1
Protective Shield	Sq Yd	414		414
Structure Excavation	Cu Yd		82	82
Rock Excavation For Structures	Cu Yd		20	20
Concrete Structures	Cu Yd		112.4	112.4
Concrete Superstructure (Approach Slab)	Cu Yd	11.1		11.1
Protective Coat	Sq Yd	50		50
Reinforcement Bars	Pound		890	890
Reinforcement Bars, Epoxy Coated	Pound	3210	16600	19810
Name Plates	Each		2	2
Drilled Shaft In Soil	Cu Yd		4.1	4.1
Drilled Shaft In Rock	Cu Yd		4.8	4.8
Concrete Sealer	Sq Ft		1181	1181
Geocomposite Wall Drain	Sq Yd		19	19
Pedestrian Truss Superstructure	Sq Ft	3372		3372
Granular Backfill For Structures	Cu Yd		30	30
Pipe Underdrains For Structures 4"	Foot		83	83

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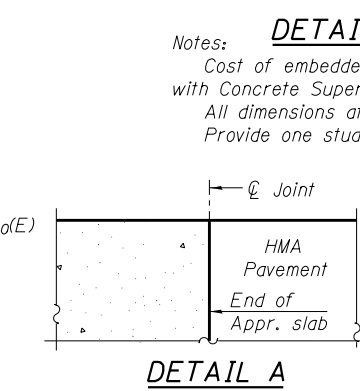
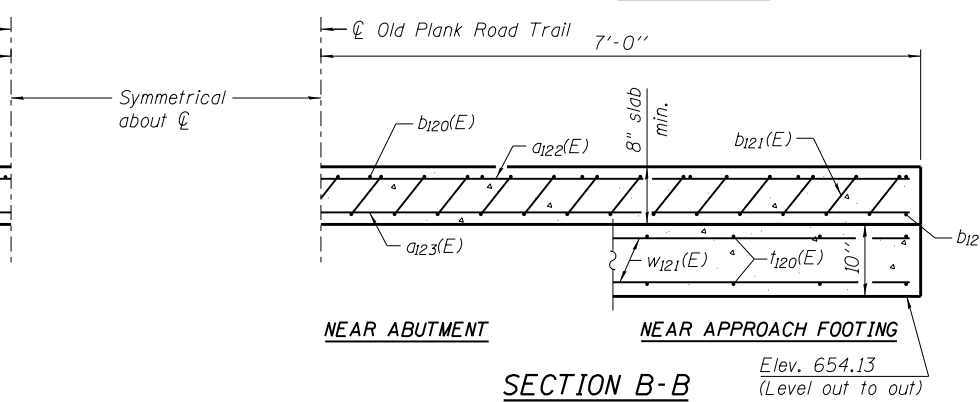
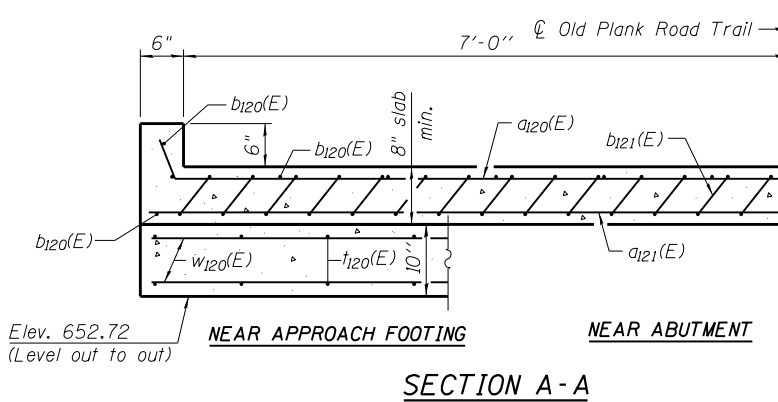
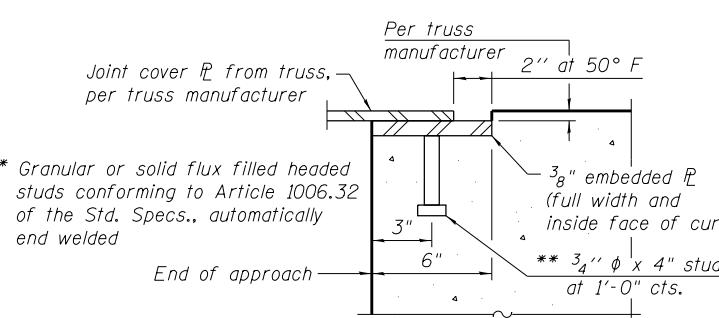
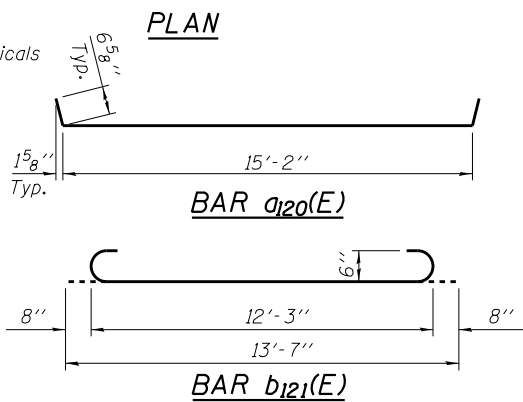
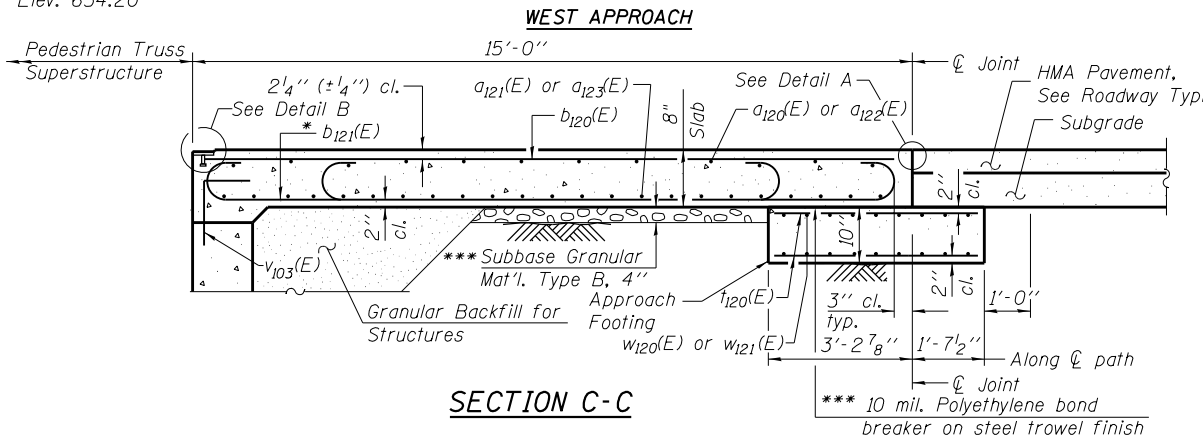
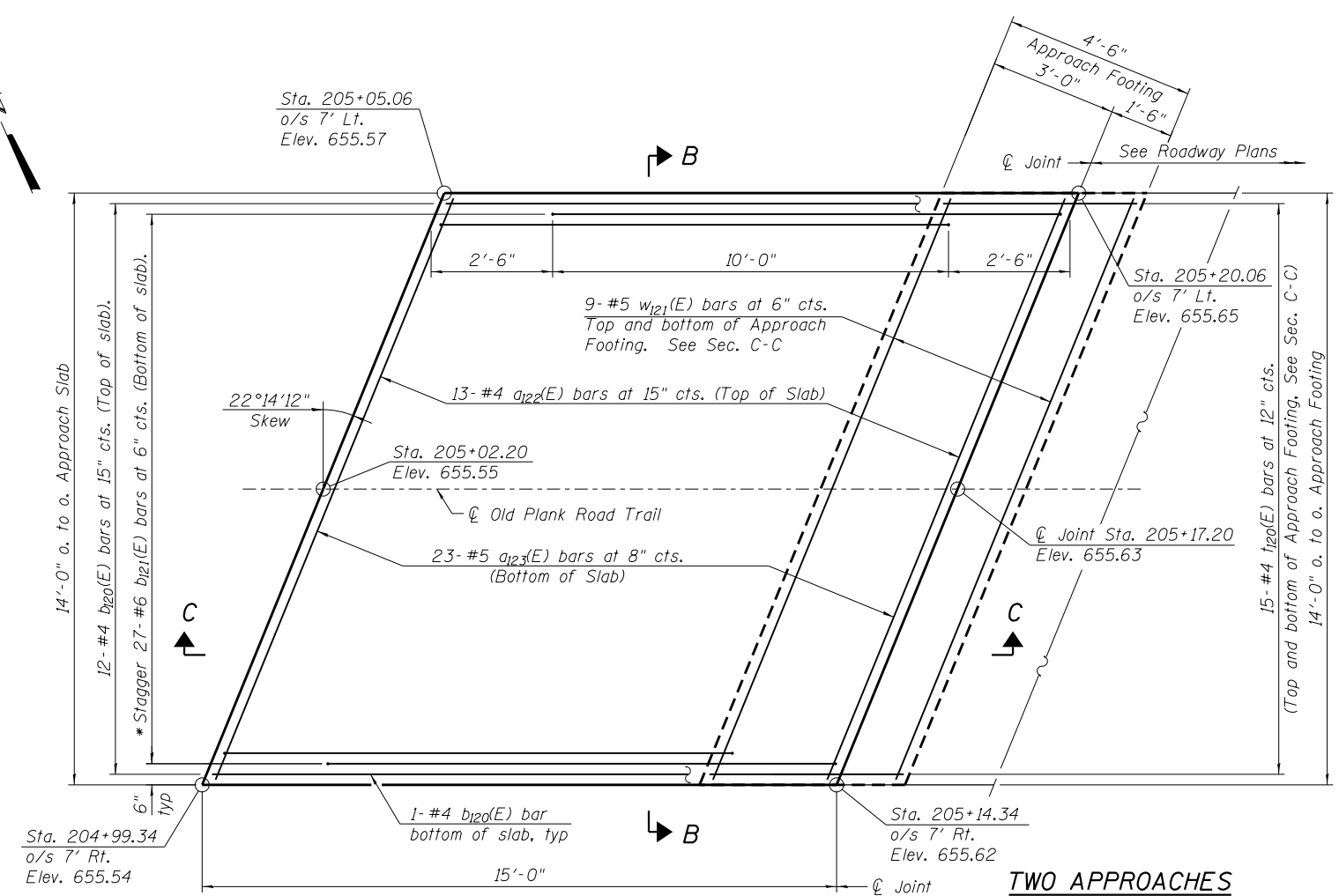
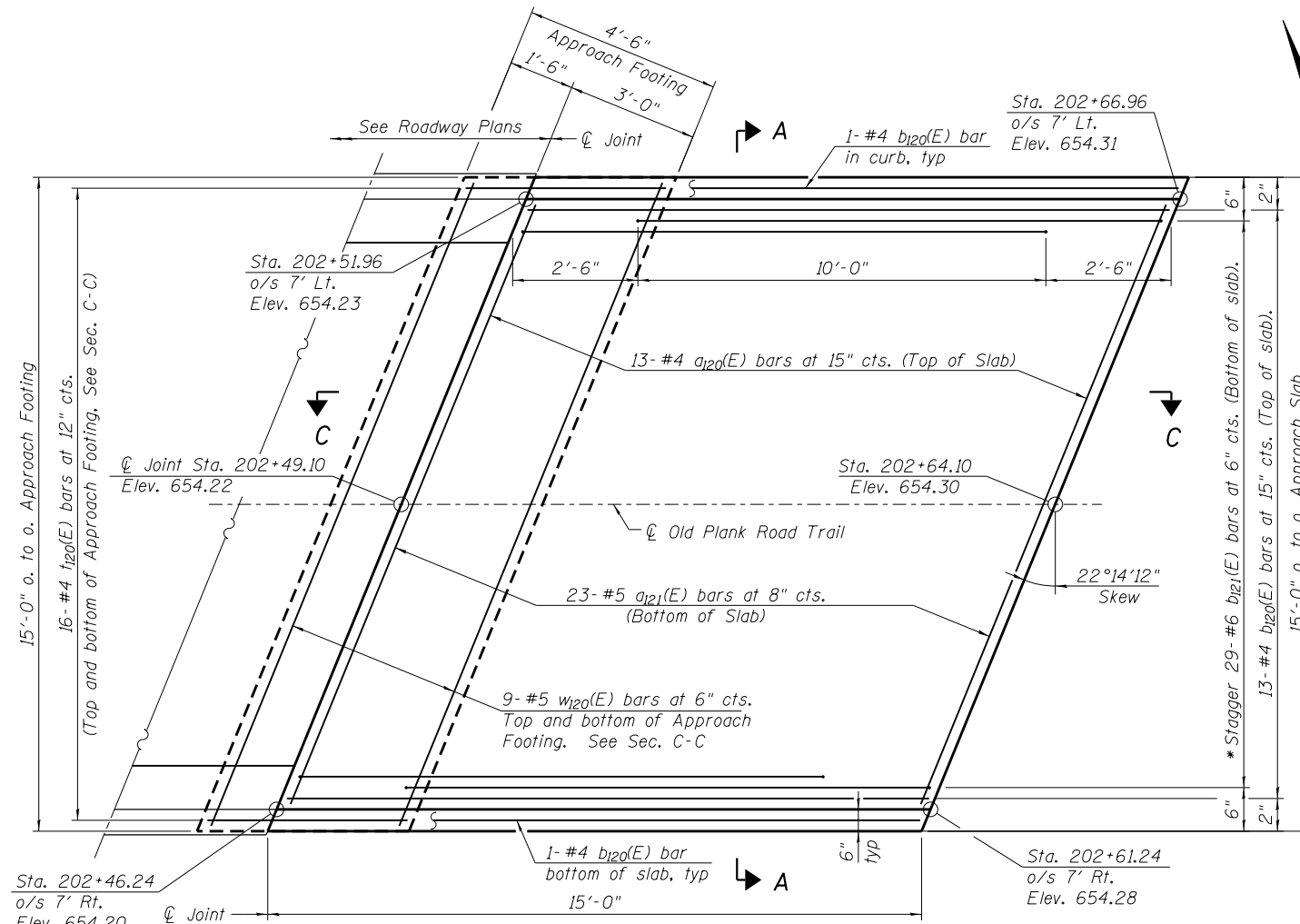
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PLOT DATE = 3/16/2016	CHECKED - DL	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, INDEX OF SHEETS AND BILL OF MATERIAL  
 STRUCTURE NO. 099-3404**

SHEET NO. SB-2 OF SB-8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	26
CONTRACT NO. 60V71			ILLINOIS FED. AID PROJECT	



**TWO APPROACHES  
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a120(E)	13	# 4	16'-3"	
a121(E)	23	# 5	15'-10"	
a122(E)	13	# 4	14'-9"	
a123(E)	23	# 5	14'-9"	
b120(E)	31	# 4	14'-7"	
b121(E)	56	# 6	13'-7"	
t120(E)	62	# 4	4'-6"	
w120(E)	18	# 5	15'-10"	
w121(E)	18	# 5	14'-9"	
Protective Coat		Sq. Yd.	50	
Concrete Superstructure (Approach Slab)		Cu. Yd.	11.1	
Concrete Structures		Cu. Yd.	4.4	
Reinforcement Bars, Epoxy Coated		Pound	3,210	

**Notes:**  
 Cost of embedded plates and studs included with Concrete Superstructure (Approach Slab).  
 All dimensions at right angles.  
 Provide one stud in each curb.

\* Tilt b121(E) bars to maintain clearance.  
 \*\*\* Cost included with Concrete Superstructure (Approach Slab).

**Notes:**  
 Concrete for approach slab and curb shall be paid for as Concrete Superstructure (Approach Slab). Approach footing concrete shall be paid for as Concrete Structures. Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated. For v103(E) bar details, see sheet SB-4. The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf. Cost of excavation for approach footing included with Concrete Structures. For Granular Backfill for Structures and drainage treatment details, see sheet SB-4.

N:\PROJECTS\0003384\007\_01d\_Planck\_Traffic\Design\Structural\CAD\3384-3\_Approach\_Slab\_Details.sxdn



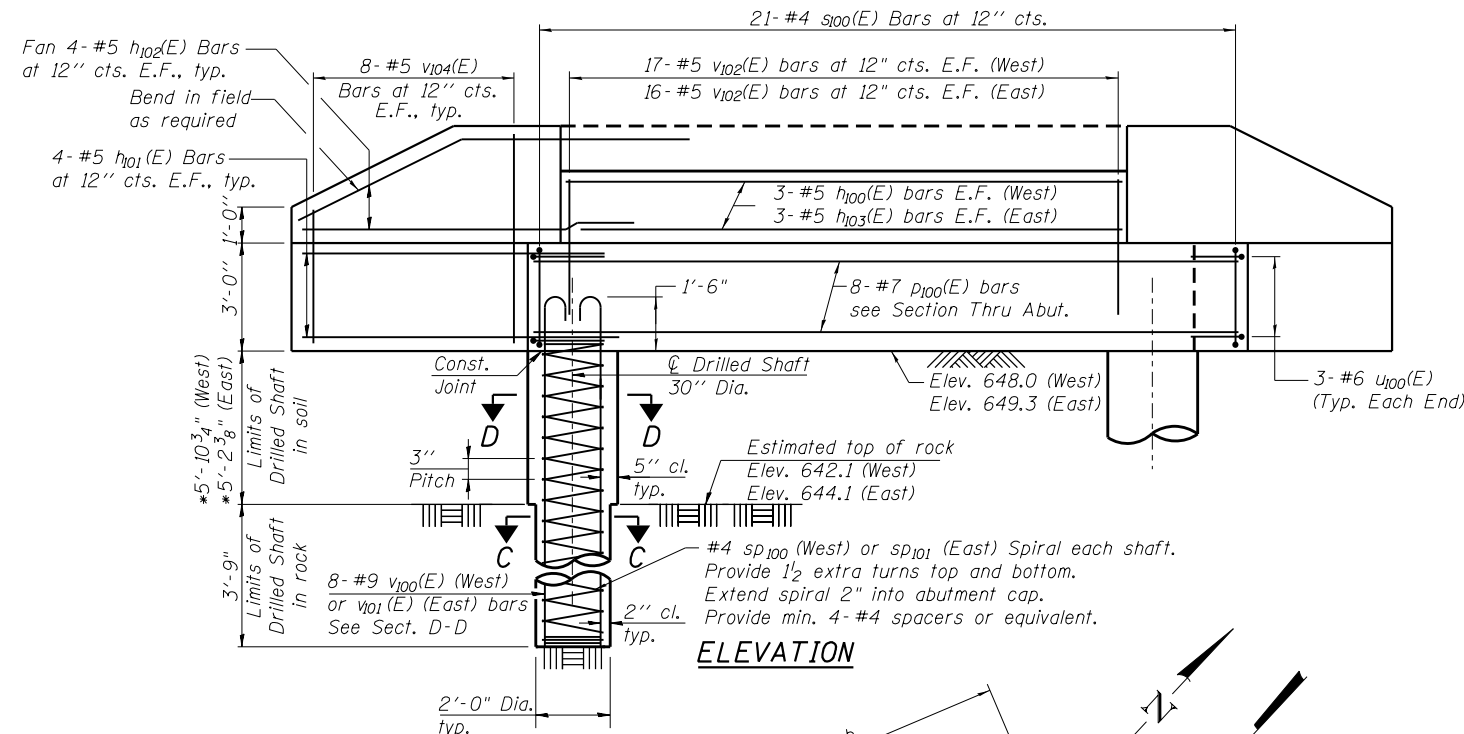
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

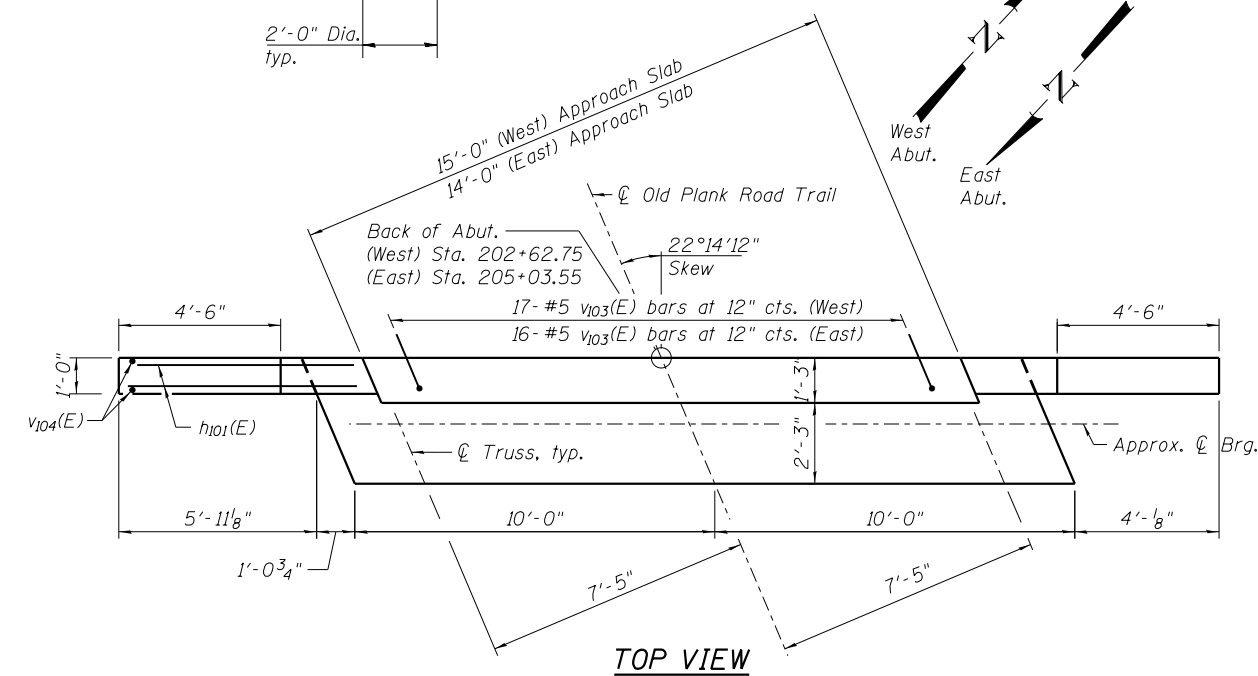
**APPROACH SLAB DETAILS  
STRUCTURE NO. 099-3404**

SHEET NO. SB-3 OF SB-8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	27
CONTRACT NO. 60V71			ILLINOIS FED. AID PROJECT	

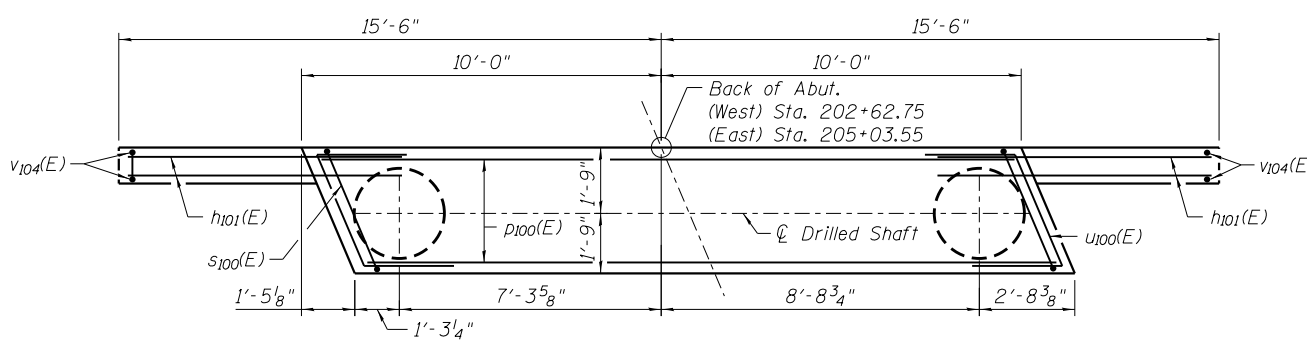


**ELEVATION**



**TOP VIEW**

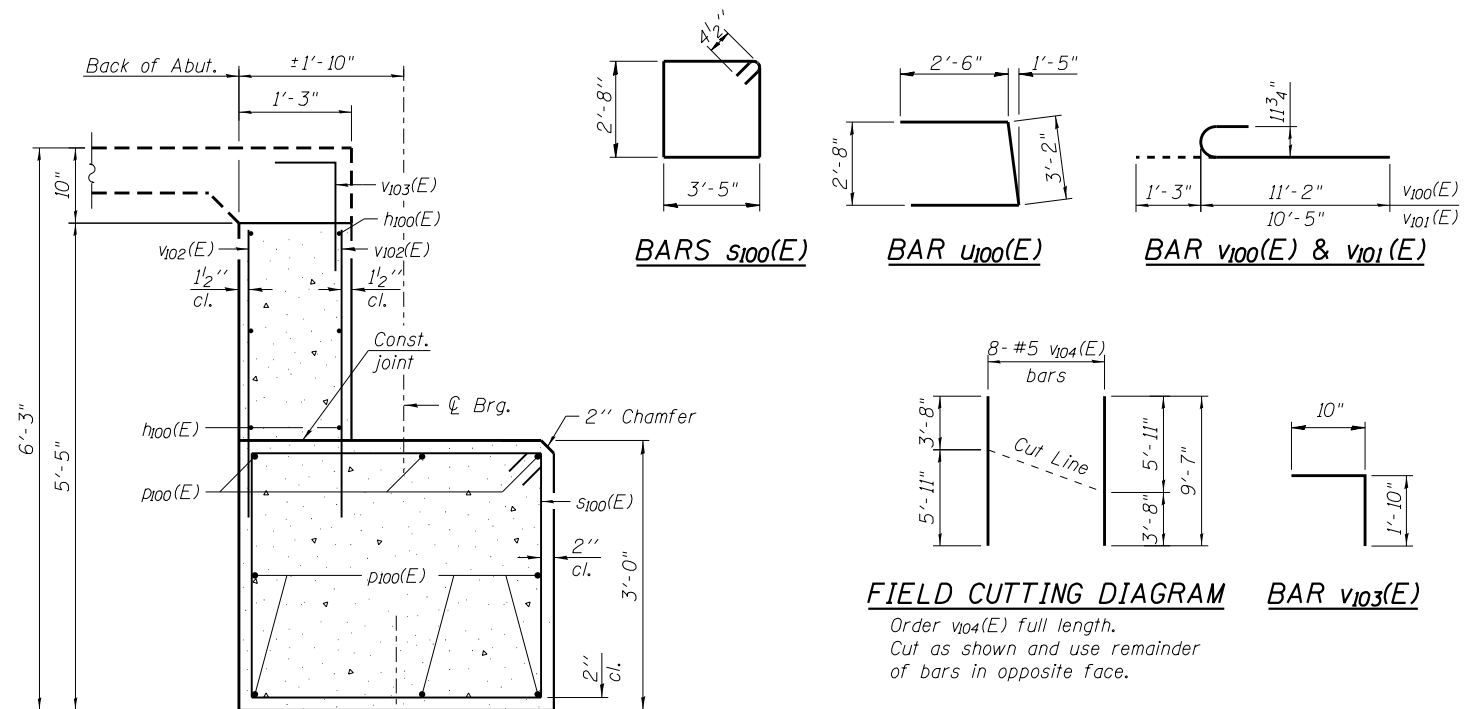
(West Abutment shown, East Abutment similar opposite hand)



**PLAN-PILE CAP**

(West Abutment shown, East Abutment similar opposite hand)

\* The quantities and reinforcement detailing are based on the top of shaft and the estimated top of rock elevations shown and may change based on the actual top of rock encountered at each shaft and the final top of shaft elevations.



**SECTION THRU ABUTMENT**

(Horiz. dim. at Rt. L's)

**FIELD CUTTING DIAGRAM BAR v103(E)**

Order v104(E) full length. Cut as shown and use remainder of bars in opposite face.

**BILL OF MATERIAL (2 ABUTMENTS)**

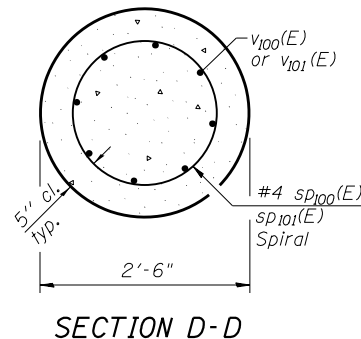
Bar	No.	Size	Length	Shape
h100(E)	6	# 5	15'-11"	—
h101(E)	32	# 5	8'-0"	—
h102(E)	32	# 5	10'-0"	—
h103(E)	6	# 5	14'-11"	—
p100(E)	16	# 7	19'-8"	—
s100(E)	42	# 4	12'-11"	□
** sD100	2	# 4	9'-8"	⋈
** sD101	2	# 4	9'-2"	⋈
u100(E)	12	# 6	8'-2"	—
v100(E)	16	# 9	12'-5"	⌋
v101(E)	16	# 9	11'-8"	⌋
v102(E)	66	# 5	3'-4"	—
v103(E)	33	# 5	2'-8"	⌋
v104(E)	32	# 5	9'-7"	—
Concrete Structures		Cu. Yd.	22.9	
Reinforcement Bars, Epoxy Coated		Pound	3,900	
Reinforcement Bars		Pound	570	
Drilled Shaft in Soil		Cu. Yd.	4.1	
Drilled Shaft in Rock		Cu. Yd.	1.8	
Concrete Sealer		Sq. Ft.	228	
Structure Excavation		Cu. Yd.	41	
Pipe Underdrains for Structures 4"		Foot	83	
Geocomposite Wall Drain		Sq. Yd.	19	
Granular Backfill for Structures		Cu. Yd.	30	

\*\* Length is Height of spiral.

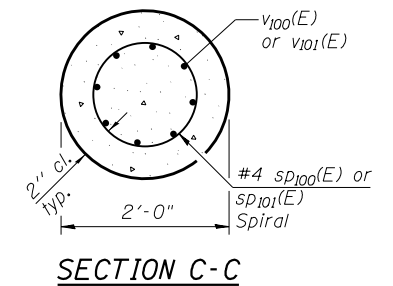
**BEARING SEAT ELEVATIONS**

E. Abut.	652.3
W. Abut.	651.0

Seat elevations may change based on truss manufacturer's design drawings. Contractor shall verify elevations and dimensions prior to ordering materials.



**SECTION D-D**



**SECTION C-C**

**NOTES:**

- For approach slab details see sheet SB-3.
- Anchor Bolt design and locations are per Truss Manufacturer. The Contractor shall place top cap reinforcement to miss anchor bolt locations. Cost of Anchor Bolts included in cost of Pedestrian Truss Superstructure.
- E.F. denotes each face.
- Concrete Sealer shall be applied to the backwall, bearing seat and the exposed portion of the pile cap.

N:\PROJECTS\0003384\007\_01d\_Planck\_Truss\Design\Structural\CAD\3384\_4\_Abutment\_Details.dgn

**Clorba Group, Inc.**  
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 Suite 402, Chicago, Illinois 60656  
 Tel: 773.724.0016  
 Fax: 773.775.4014  
 Email: clorba@clorba.com

USER NAME = aharn	DESIGNED - BWS	REVISED -
PLOT SCALE = 5.333333' / 1"	CHECKED - AMK	REVISED -
PLOT DATE = 3/16/2016	DRAWN - RD	REVISED -
	CHECKED - AMK	REVISED -

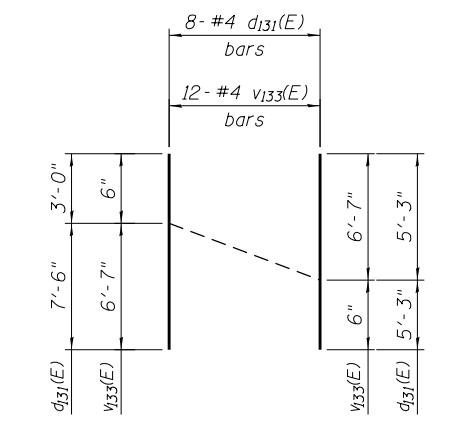
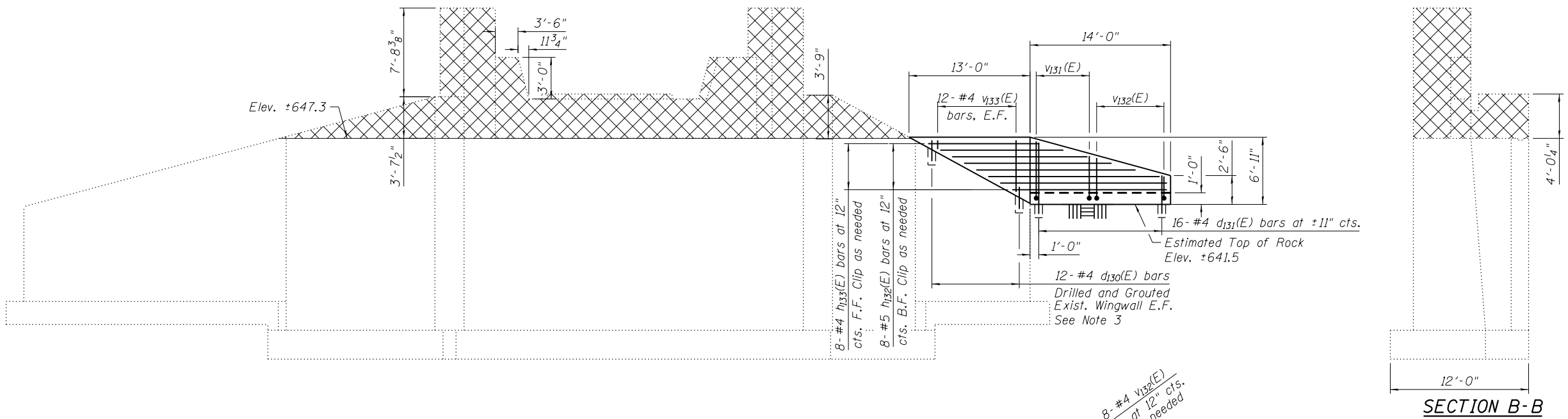
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ABUTMENT DETAILS I  
 STRUCTURE NO. 099-3404**

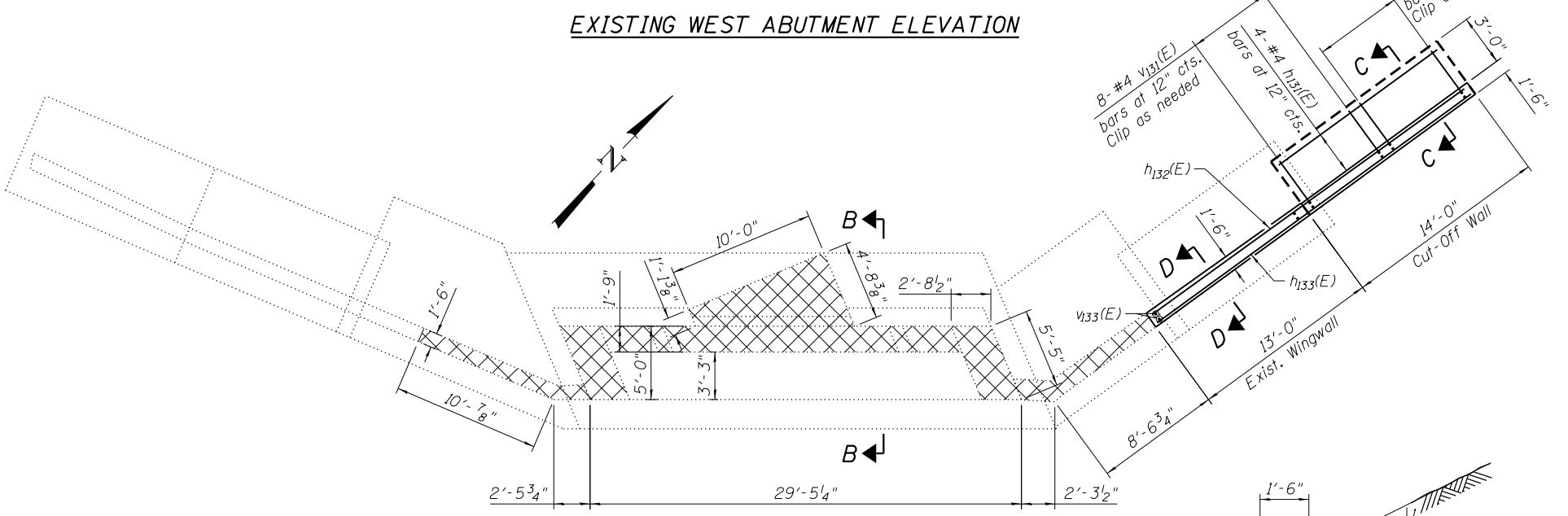
SHEET NO. SB-4 OF SB-8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	28
CONTRACT NO. 60V71				
ILLINOIS FED. AID PROJECT				

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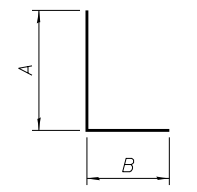


**CUTTING DIAGRAM**  
Order bars full length  
Cut as shown and use  
remainder of bars in  
opposite face



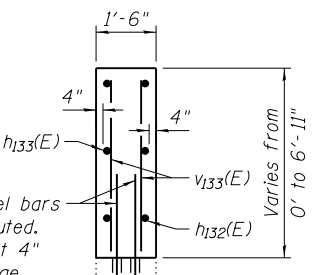
**NOTES:**

1. The cost of the removal of the East Abutment and Pier is included with the cost of Removal of Existing Structures No. 3.
2. E.F. denotes Each Face.  
B.F. denotes Back Face.
3. Drill and grout  $d_{130}(E)$  and  $d_{131}(E)$  bars 9" min. in accordance with Article 584 of the Standard Specifications. Cost included with Reinforcement Bars, Epoxy Coated.
4. Wingwall Extension to be completed before construction of West Abutment. Concrete Removal to be completed after existing superstructure is removed.
5. Cut-Off Wall Footing to be formed on sound rock.
6. The maximum applied service bearing pressure= $Q_{max}$   
East Abutment Cut-off Wall: 4.3 ksf  
West Abutment Cut-off Wall: 5.4 ksf



Bar	A	B
$v_{130}(E)$	2'-8"	1'-10"
$v_{131}(E)$	4'-8"	3'-9"
$v_{132}(E)$	3'-8"	3'-0"

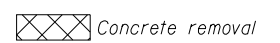
**$v_{130}(E)$  THRU  
 $v_{132}(E)$  BARS**



#4  $d_{130}(E)$  dowel bars  
Drilled and Grouted.  
Must be at least 4"  
clear of the edge  
of existing concrete.  
See Note 3.

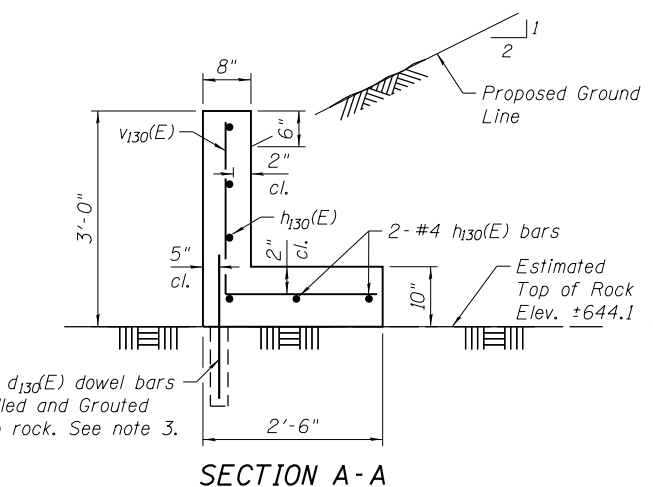
**SECTION D-D**

**LEGEND:**

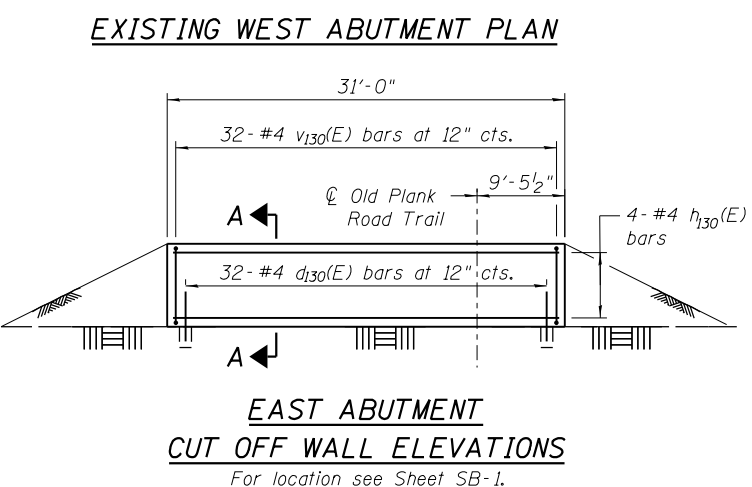


**BILL OF MATERIAL**

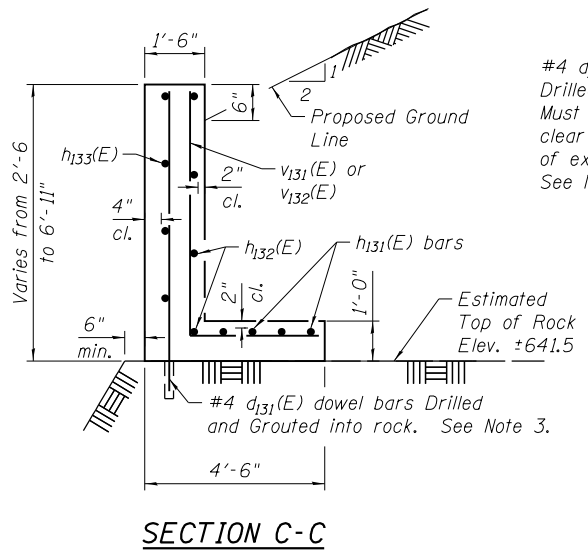
Bar	No.	Size	Length	Shape
$d_{130}(E)$	56	# 4	2'-3"	—
$d_{131}(E)$	8	# 4	10'-6"	—
$h_{130}(E)$	6	# 4	30'-8"	—
$h_{131}(E)$	4	# 4	13'-7"	—
$h_{132}(E)$	8	# 5	14'-4"	—
$h_{133}(E)$	8	# 4	14'-4"	—
$v_{130}(E)$	32	# 4	4'-6"	L
$v_{131}(E)$	8	# 4	8'-5"	L
$v_{132}(E)$	8	# 4	6'-8"	L
$v_{133}(E)$	12	# 4	7'-1"	—
Concrete Structures			Cu. Yd.	11.8
Concrete Removal			Cu. Yd.	26.1
Reinforcement Bars, Epoxy Coated			Pound	730



**SECTION A-A**



**EAST ABUTMENT  
CUT OFF WALL ELEVATIONS**  
For location see Sheet SB-1.



**SECTION C-C**



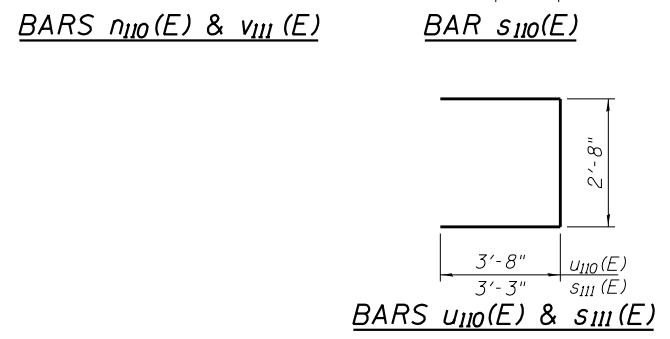
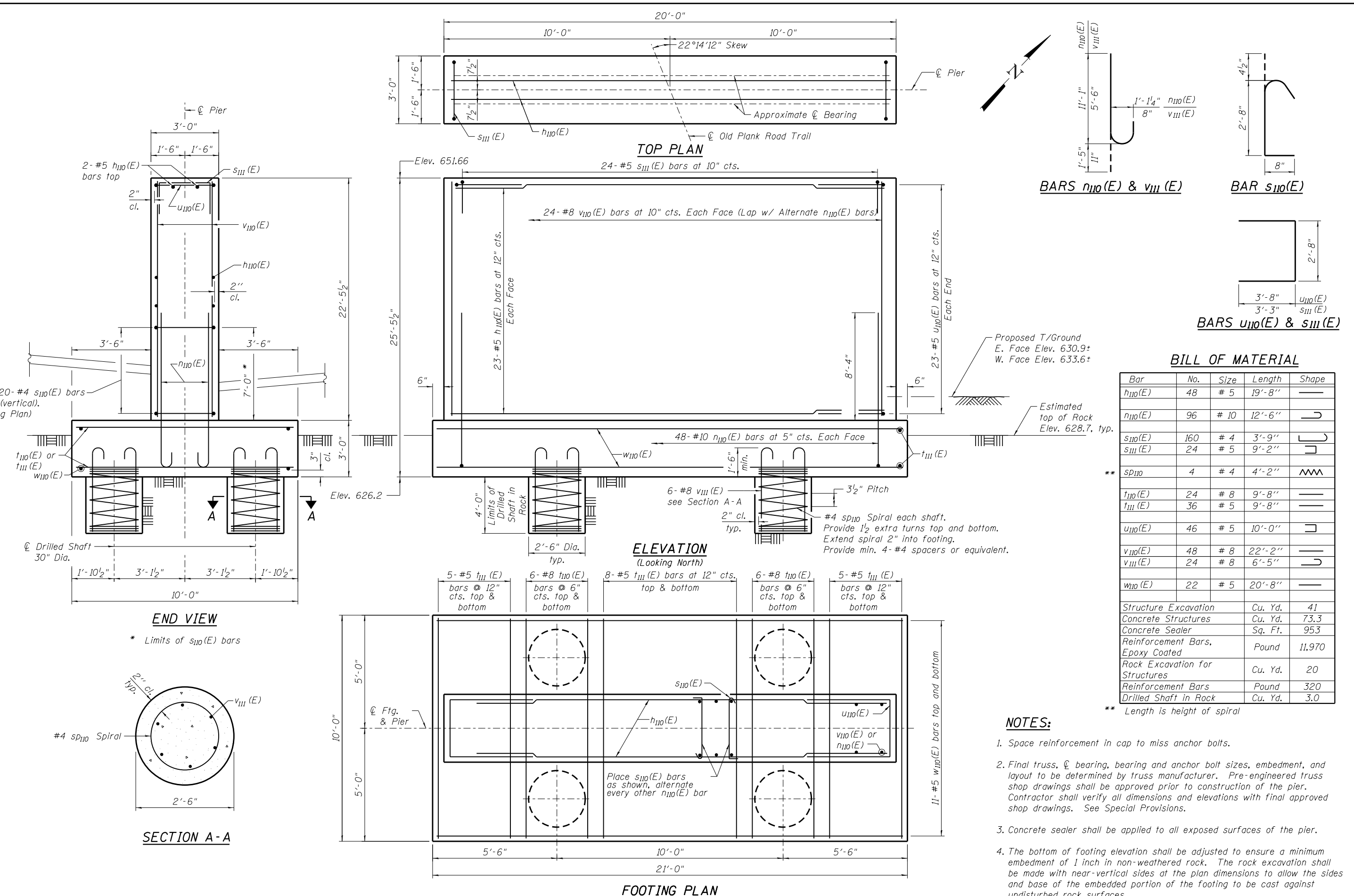
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ABUTMENT DETAILS II  
STRUCTURE NO. 099-3404**  
SHEET NO. SB-5 OF SB-8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	29
CONTRACT NO. 60V71				
ILLINOIS FED. AID PROJECT				

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**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h110(E)	48	# 5	19'-8"	—
n110(E)	96	# 10	12'-6"	U
s110(E)	160	# 4	3'-9"	U
s111(E)	24	# 5	9'-2"	U
sp110	4	# 4	4'-2"	W
t110(E)	24	# 8	9'-8"	—
t111(E)	36	# 5	9'-8"	—
u110(E)	46	# 5	10'-0"	U
v110(E)	48	# 8	22'-2"	—
v111(E)	24	# 8	6'-5"	U
w110(E)	22	# 5	20'-8"	—
Structure Excavation		Cu. Yd.	41	
Concrete Structures		Cu. Yd.	73.3	
Concrete Sealer		Sq. Ft.	953	
Reinforcement Bars, Epoxy Coated		Pound	11,970	
Rock Excavation for Structures		Cu. Yd.	20	
Reinforcement Bars		Pound	320	
Drilled Shaft in Rock		Cu. Yd.	3.0	

\*\* Length is height of spiral

**NOTES:**

- Space reinforcement in cap to miss anchor bolts.
- Final truss, bearing, bearing and anchor bolt sizes, embedment, and layout to be determined by truss manufacturer. Pre-engineered truss shop drawings shall be approved prior to construction of the pier. Contractor shall verify all dimensions and elevations with final approved shop drawings. See Special Provisions.
- Concrete sealer shall be applied to all exposed surfaces of the pier.
- The bottom of footing elevation shall be adjusted to ensure a minimum embedment of 1 inch in non-weathered rock. The rock excavation shall be made with near-vertical sides at the plan dimensions to allow the sides and base of the embedded portion of the footing to be cast against undisturbed rock surfaces.



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### BORING LOG OPR-01

Page 1 of 1

WEI Job No.: 775-13-01

Client: **Ciorba Group, Inc.**  
Project: **FAI-80 from US 30 to US 45**  
Location: **Will County**

Datum: NGVD  
Elevation: 651.93 ft  
North: 1766722.76 ft  
East: 1076794.56 ft  
Station: 202+76  
Offset: 36' RT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type recovery	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type recovery	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
651.3	8-inch thick, brown SILTY CLAY --TOPSOIL--							631.4	Loose, brown and gray GRAVELLY SANDY LOAM --FILL--	9		8 2 7	NP	16	
	Very loose to medium dense, brown, fine to coarse SAND --FILL--	1	X	1	3 3 3	NP	6								
		2	X	2	3 2 2	NP	5	628.9	Very dense, gray DOLOSTONE fragments --WEATHERED BEDROCK--	10	X	50/2	NP		
		3	X	3	3 2 1	NP	7	626.9	Good to excellent quality, buff to gray, slightly weathered DOLOSTONE	11			NP		
		4	X	4	3 3 3	NP	6		Run #1, 25' to 30' Recovery = 100%, RQD = 77%						
		5	X	5	3 1 2	NP	6		Run #2, 30' to 35' Recovery = 100%, RQD = 100%						
		6	X	6	4 4 4	NP	8								
		7	X	7	6 7 8	NP	6								
		8	X	8	8 5 3	NP	6								
		20						616.9	Boring terminated at 35.00 ft	35					

#### GENERAL NOTES

Begin Drilling **05-02-2011** Complete Drilling **05-02-2011**  
Drilling Contractor **WTS** Drill Rig **B-57 TMR**  
Driller **K&R** Logger **B. Wilson** Checked by **CLM**  
Drilling Method **3.25 IDA HSA; Boring backfilled upon completion**

#### WATER LEVEL DATA

While Drilling  **DRY**  
At Completion of Drilling  **DRY**  
Time After Drilling **NA**  
Depth to Water  **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.



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### BORING LOG OPR-01-A1

Page 1 of 1

WEI Job No.: 775-13-01

Client: **Ciorba Group, Inc.**  
Project: **FAI-80 from US 30 to US 45**  
Location: **Will County**

Datum: NGVD  
Elevation: 652.05 ft  
North: 1766738.92 ft  
East: 1076792.19 ft  
Station: 202+59  
Offset: 14' RT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type recovery	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type recovery	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
	--BLIND DRILL to 10 feet bgs--							642.1	Moderately strong to strong, very poor quality, buff, moderately weathered, vuggy porosity DOLOSTONE Run #1, 10' to 15' Recovery = 100%, RQD = 9% Q <sub>u</sub> = 7,680 psi	10		1		NP	
		15							Run #2, 15' to 20' Recovery = 100%, RQD = 8% Q <sub>u</sub> = 6,680 psi			2		NP	
		20						632.1	Boring terminated at 20.00 ft	20					

#### GENERAL NOTES

Begin Drilling **05-03-2011** Complete Drilling **05-03-2011**  
Drilling Contractor **WTS** Drill Rig **B-57 TMR**  
Driller **K&R** Logger **B. Wilson** Checked by **CLM**  
Drilling Method **3.25 IDA HSA; Boring backfilled upon completion**

#### WATER LEVEL DATA

While Drilling  **DRY**  
At Completion of Drilling  **N/A**  
Time After Drilling **NA**  
Depth to Water  **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

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WANGENG 7751301.GPJ; WANGENG.GDT 5/27/11

WANGENG 7751301.GPJ; WANGENG.GDT 5/27/11



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CHECKED - DL  
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PLOT DATE = 3/16/2016

REVISOR -  
REVISION -  
REVISOR -  
REVISION -  
REVISOR -  
REVISION -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS 1  
STRUCTURE NO. 099-3404

SHEET NO. SB-7 OF SB-8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	31
CONTRACT NO. 60V71			ILLINOIS FED. AID PROJECT	



### BORING LOG OPR-02

Page 1 of 1

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WEI Job No.: 775-13-01  
 Client: **Ciorba Group, Inc.**  
 Project: **FAI-80 from US 30 to US 45**  
 Location: **Will County**

Datum: NGVD  
 Elevation: 634.67 ft  
 North: 1766709.89 ft  
 East: 1076937.61 ft  
 Station: 204-01  
 Offset: 6' RT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)
633.7	12-inch thick, brown CLAY LOAM --TOPSOIL--												
	Very stiff, brown CLAY LOAM, little gravel --FILL--	4	1	4	3.50	16							
631.7	Very loose, brown, medium SAND --WET--	5	2	3	NP	17							
629.7	Good quality, buff to gray, slightly weathered DOLOSTONE Run #1, 6' to 16' Recovery = 100%, RQD = 76%												
618.7	Boring terminated at 16.00 ft												

#### GENERAL NOTES

Begin Drilling **05-03-2011** Complete Drilling **05-03-2011**  
 Drilling Contractor **WTS** Drill Rig **B-57 TMR**  
 Driller **K&R** Logger **B. Wilson** Checked by **CLM**  
 Drilling Method **3.25 IDA HSA; Boring backfilled upon completion**

#### WATER LEVEL DATA

While Drilling  **3.00 ft**  
 At Completion of Drilling  **N/A**  
 Time After Drilling **NA**  
 Depth to Water  **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.



### BORING LOG OPR-03

Page 1 of 1

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 Fax: 630 953-9938

WEI Job No.: 775-13-01  
 Client: **Ciorba Group, Inc.**  
 Project: **FAI-80 from US 30 to US 45**  
 Location: **Will County**

Datum: NGVD  
 Elevation: 654.11 ft  
 North: 1766643.38 ft  
 East: 1077014.93 ft  
 Station: 204-97  
 Offset: 18' RT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)
653.4	8-inch thick, black SILTY CLAY --TOPSOIL--												
	12-inch thick, brown gravelly LOAM --FILL--	3	1	3	3.50	20							
	Stiff to very stiff, brown SILTY CLAY, trace gravel	4		4									
		5	2	6	2.21	19							
		7		7									
	Fair quality, gray, slightly weathered DOLOSTONE Run #3, 23' to 33' Recovery = 100%, RQD = 60%												
	--6-inch void at 28 feet--												
646.1	Soft, brown SILTY CLAY with DOLOSTONE fragments --WEATHERED BEDROCK--	10	3	3	0.98	17							
		19	4	3	0.25	19							
644.1	Very poor quality, buff to gray, slightly weathered, vuggy porosity DOLOSTONE Run #1, 10' to 16' Recovery = 100%, RQD = 12%												
		30											
	--6-inch void at 30 feet--												
621.1	Boring terminated at 33.00 ft												
	Very poor quality, gray, moderately weathered DOLOSTONE Run #2, 16' to 23' Recovery = 100%, RQD = 5%												
		35											
		40											

#### GENERAL NOTES

Begin Drilling **05-06-2011** Complete Drilling **05-06-2011**  
 Drilling Contractor **WTS** Drill Rig **B-57 TMR**  
 Driller **K&R** Logger **B. Wilson** Checked by **CLM**  
 Drilling Method **3.25 IDA HSA; Boring backfilled upon completion**

#### WATER LEVEL DATA

While Drilling  **DRY**  
 At Completion of Drilling  **N/A**  
 Time After Drilling **NA**  
 Depth to Water  **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

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WANGENG 7751301.GPJ WANGENG.GDT 5/27/11



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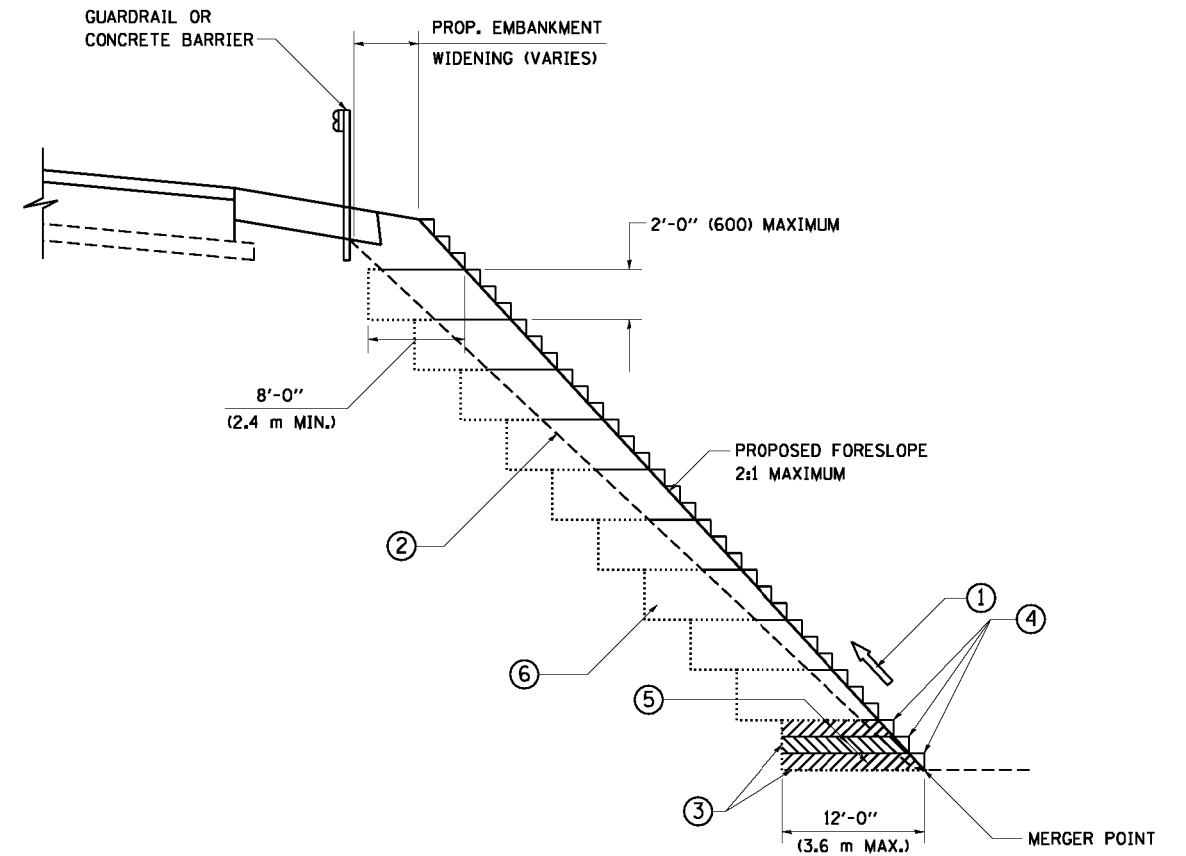
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS 2  
 STRUCTURE NO. 099-3404

SHEET NO. SB-8 OF SB-8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	32
CONTRACT NO. 60V71				
ILLINOIS FED. AID PROJECT				





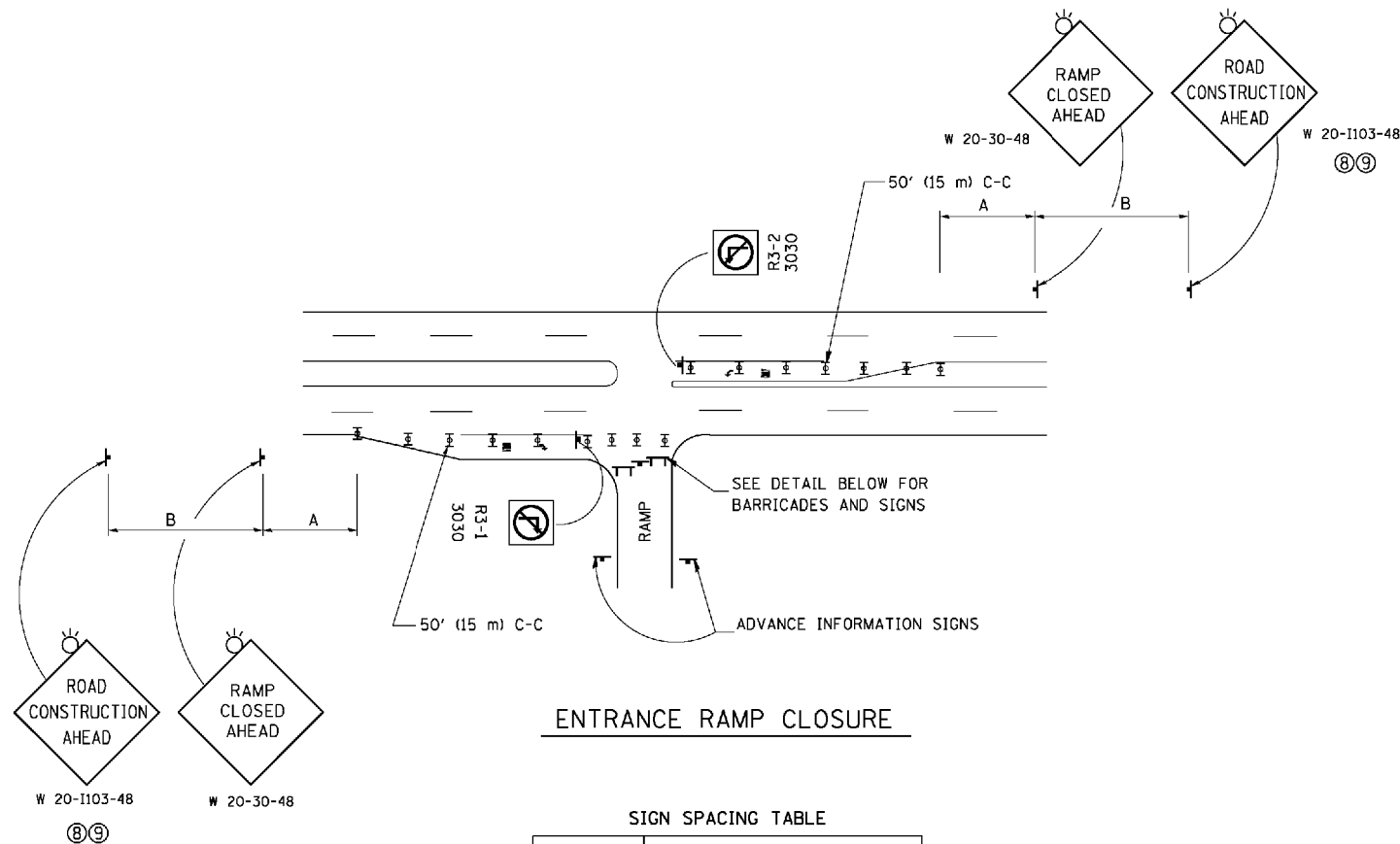
**TYPICAL BENCHING DETAIL  
FOR EMBANKMENT**

**NOTES:**

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd51.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BENCHING DETAIL FOR EMBANKMENT WIDENING</b>		FALL RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLDT DATE = 1/4/2008	CHECKED - S.E.B.	DATE - 06-16-04	REVISED -					<b>BD-51</b>		<b>CONTRACT NO. 60V71</b>	
							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

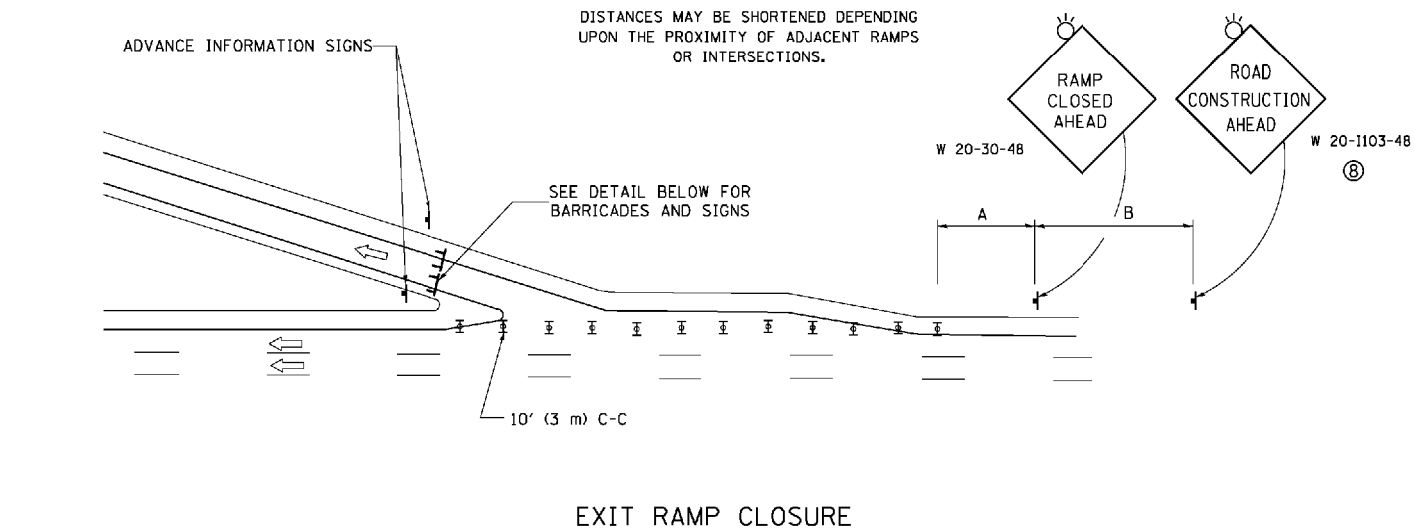


**ENTRANCE RAMP CLOSURE**

**SIGN SPACING TABLE**

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

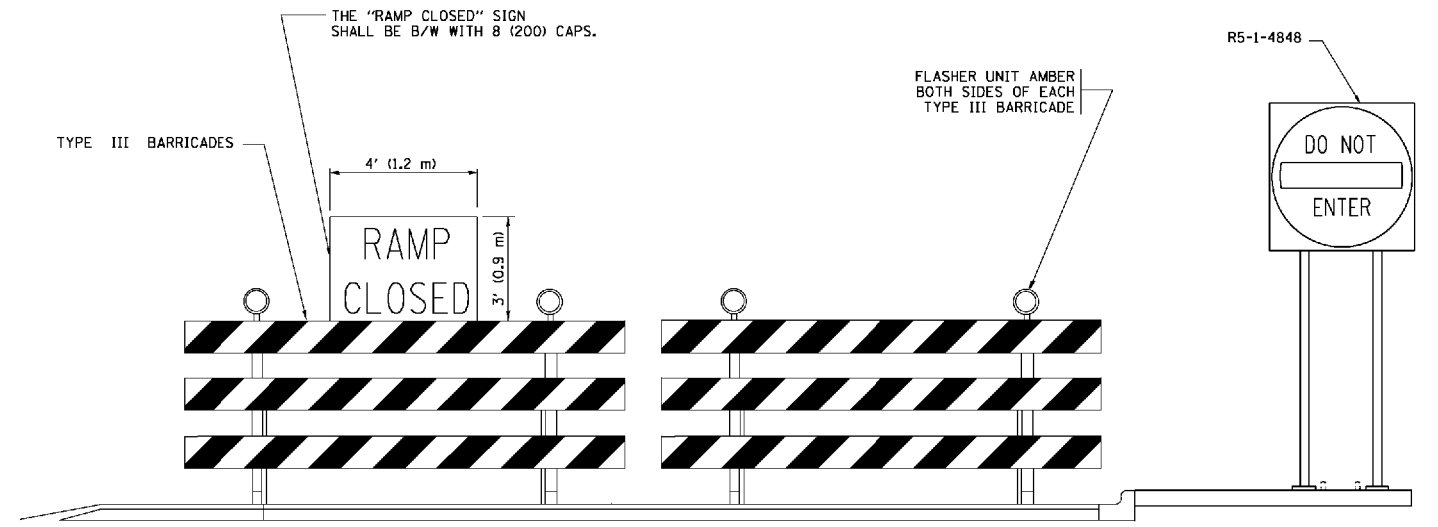
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



**EXIT RAMP CLOSURE**

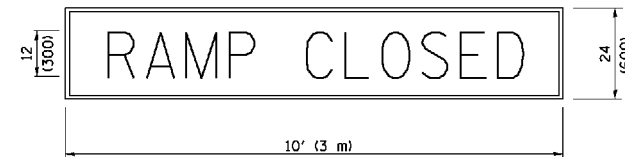
**SYMBOLS**

- ⊥ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ⊔ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



**DETAIL FOR REQUIRED BARRICADES & SIGNS**

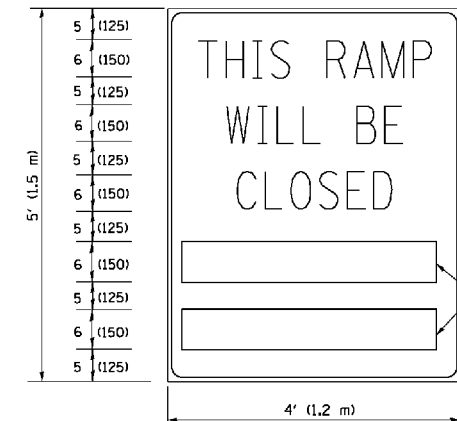
**RAMP CLOSURE ADVANCE WARNING SIGN**



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY  
E MOD FONT  
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

**RAMP CLOSURE ADVANCE INFORMATION SIGN**



BLACK LEGEND ON WHITE BACKGROUND  
1/2 (12) BORDER  
E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

**GENERAL NOTES:**

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = faotemj	DESIGNED - DWS	REVISED - JAF 02-06
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		CHECKED -	REVISED - SPB 12-09
		DATE - 02-83	REVISED - MD 06-13

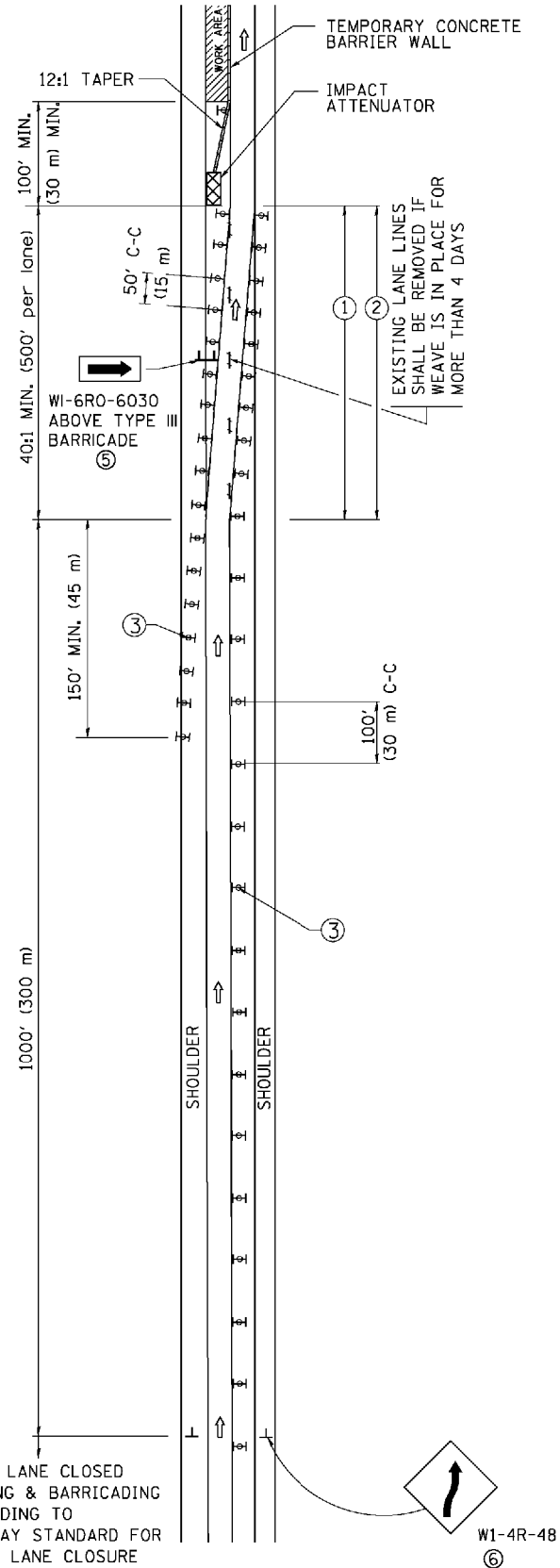
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP  
CLOSURE DETAILS**

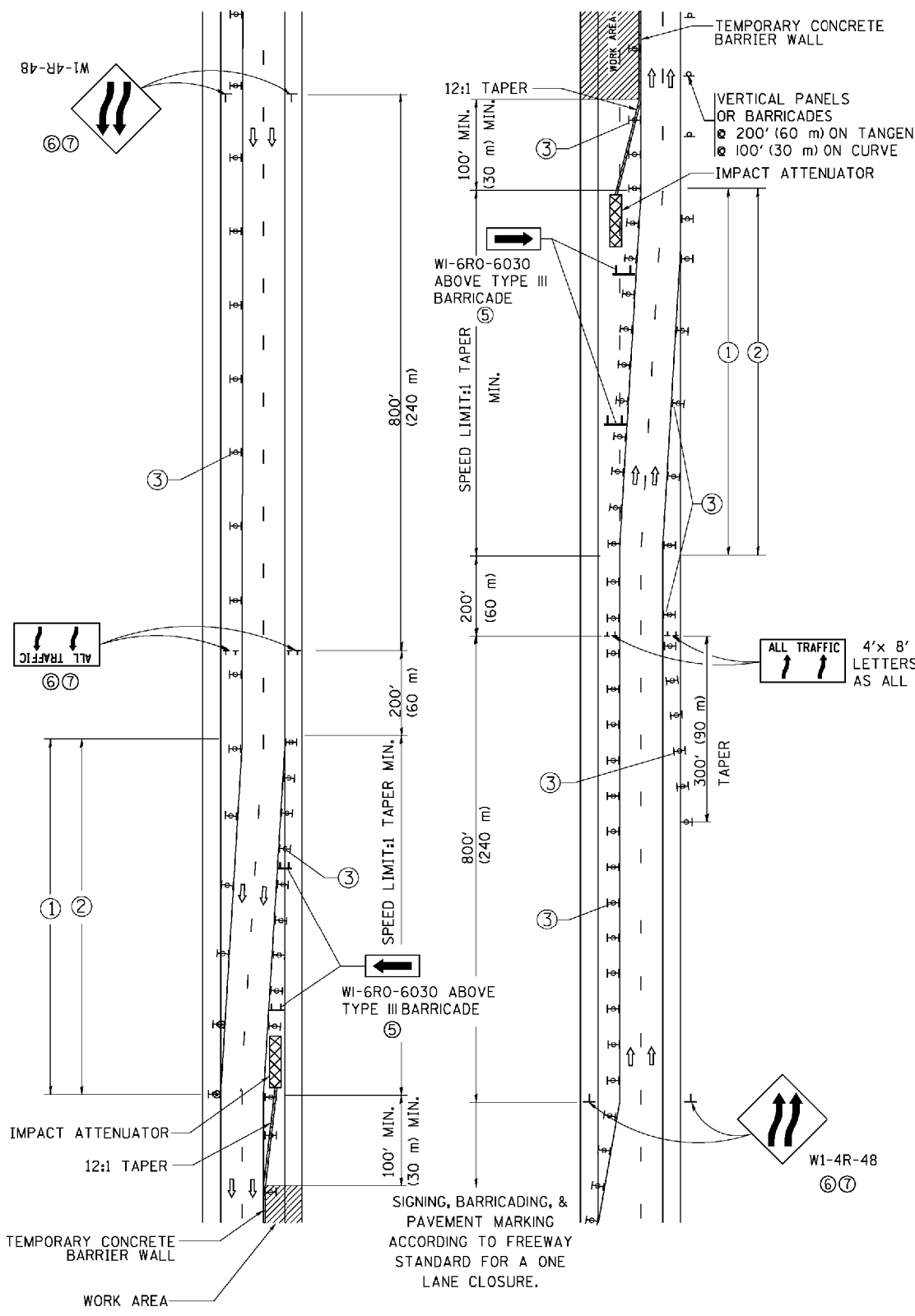
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F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	34
TC-08			CONTRACT NO. 60V71	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

# SINGLE LANE WEAVE



# MULTI-LANE WEAVE



### GENERAL NOTES

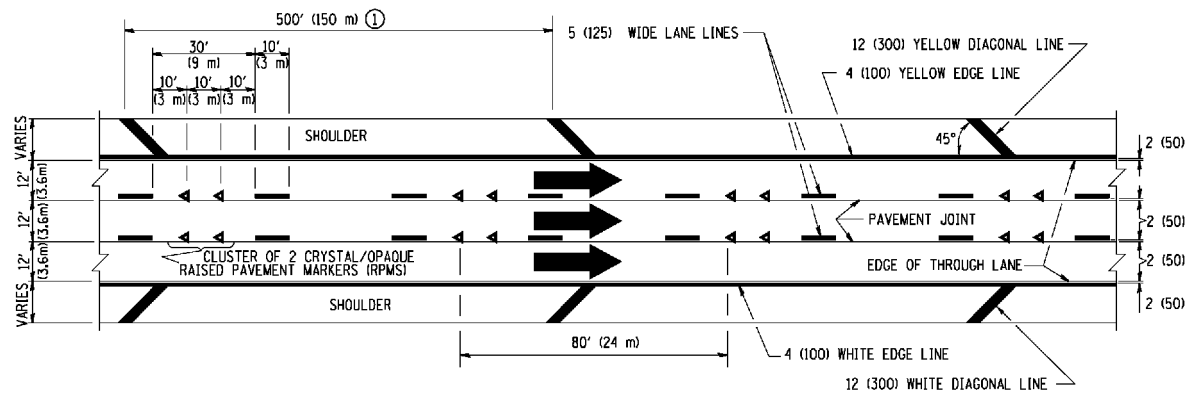
- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

### SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

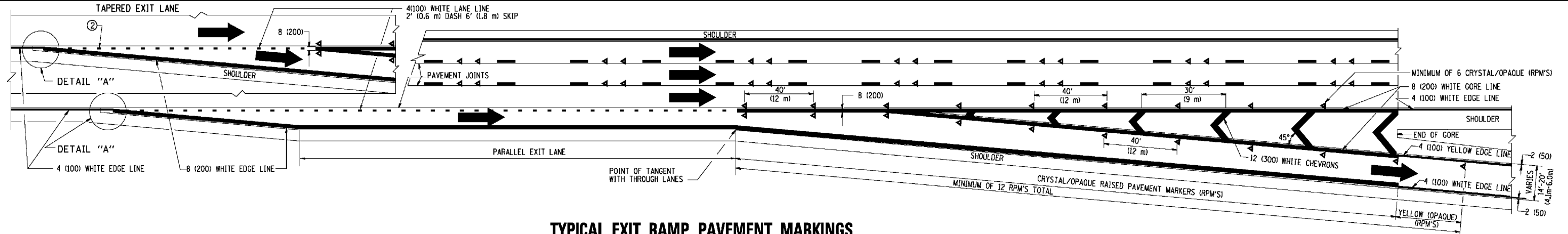
FILE NAME =	USER NAME = faotemj	DESIGNED - DWS	REVISED - JAF 02-06	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE &amp; MULTI-LANE WEAVE</b>	F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 7/1/2013	DATE - 02-87	REVISED - MD 06-13	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



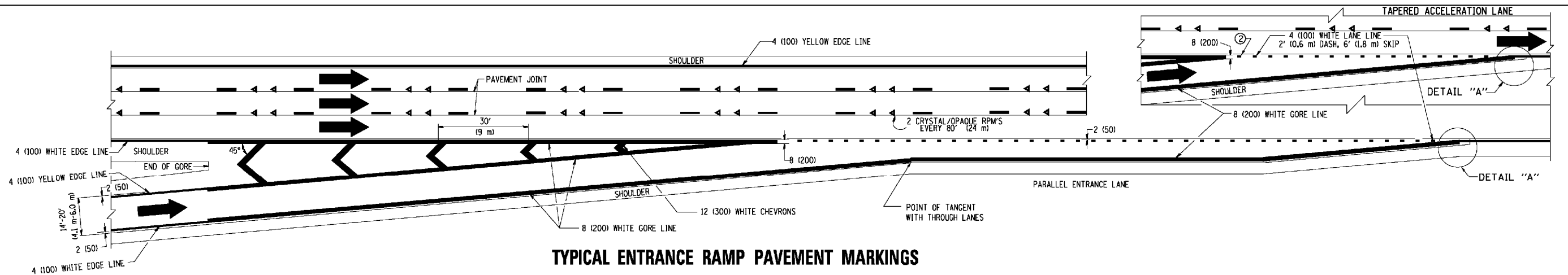
**TYPICAL EDGE LINES & LANE LINES**

**PAVEMENT MARKING MATERIALS**

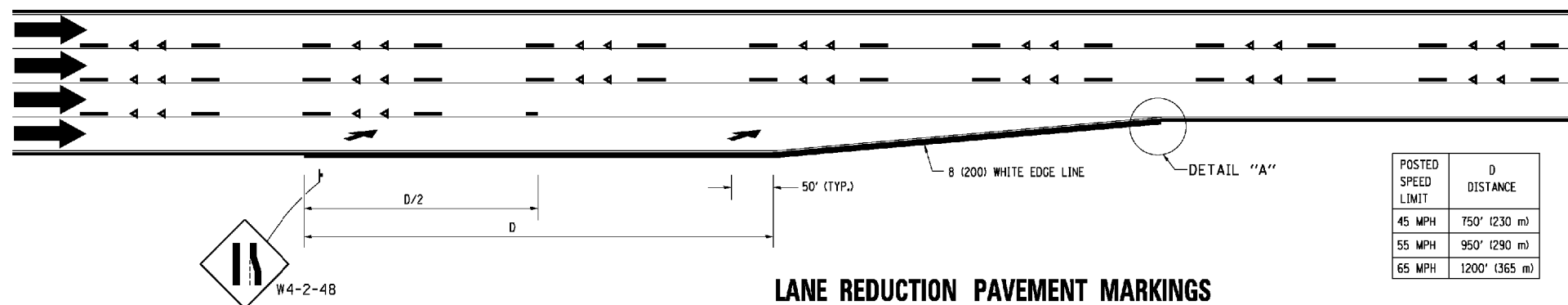
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE; INLAID OR GROOVED IN SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENT PROJECTS.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC PROJECTS.



**TYPICAL EXIT RAMP PAVEMENT MARKINGS**

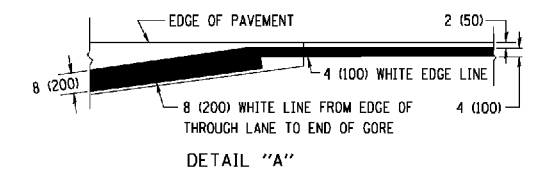


**TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS**



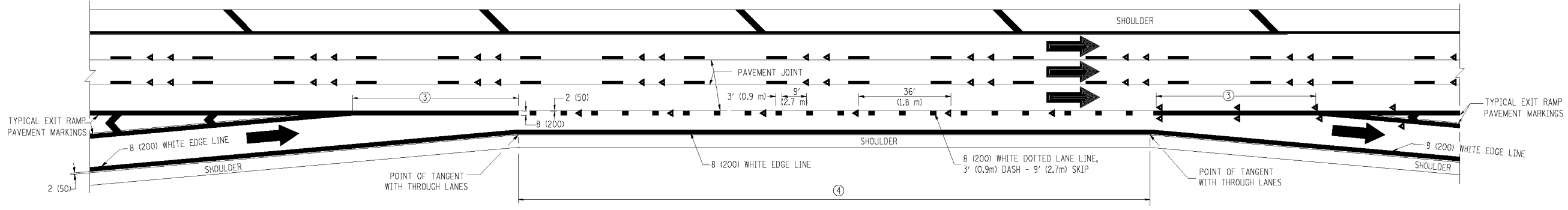
**LANE REDUCTION PAVEMENT MARKINGS**

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)

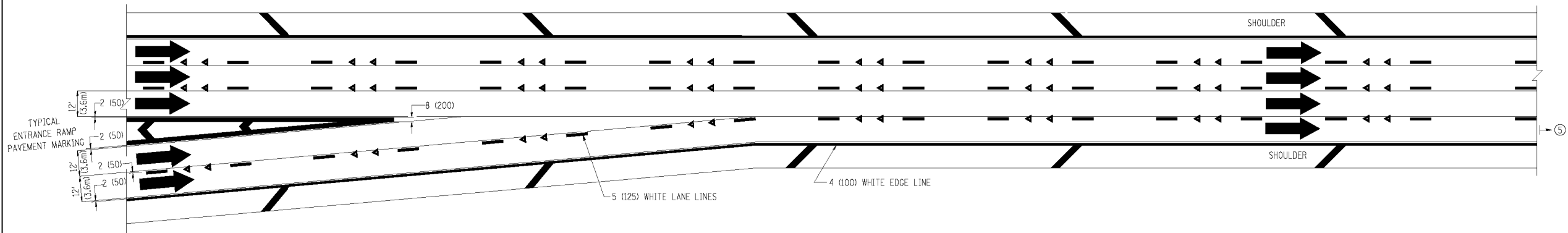


**NOTES:**

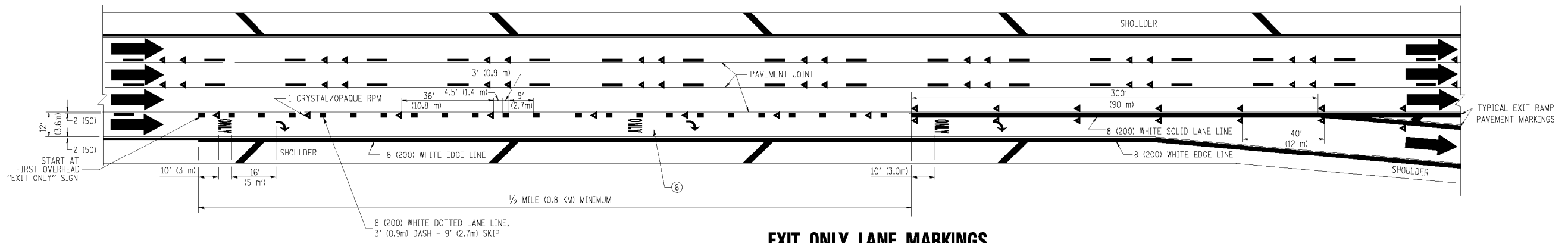
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.



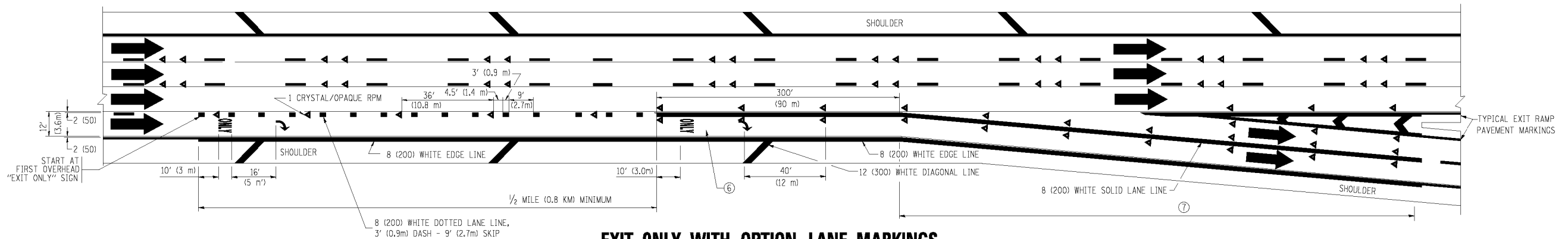
**AUXILIARY LANE MARKINGS**



**TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS**



**EXIT ONLY LANE MARKINGS**



**EXIT ONLY WITH OPTION LANE MARKINGS**

- NOTES
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
  - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
  - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
  - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
  - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED CORE.

FILE NAME =	USER NAME = l1e9aa	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96
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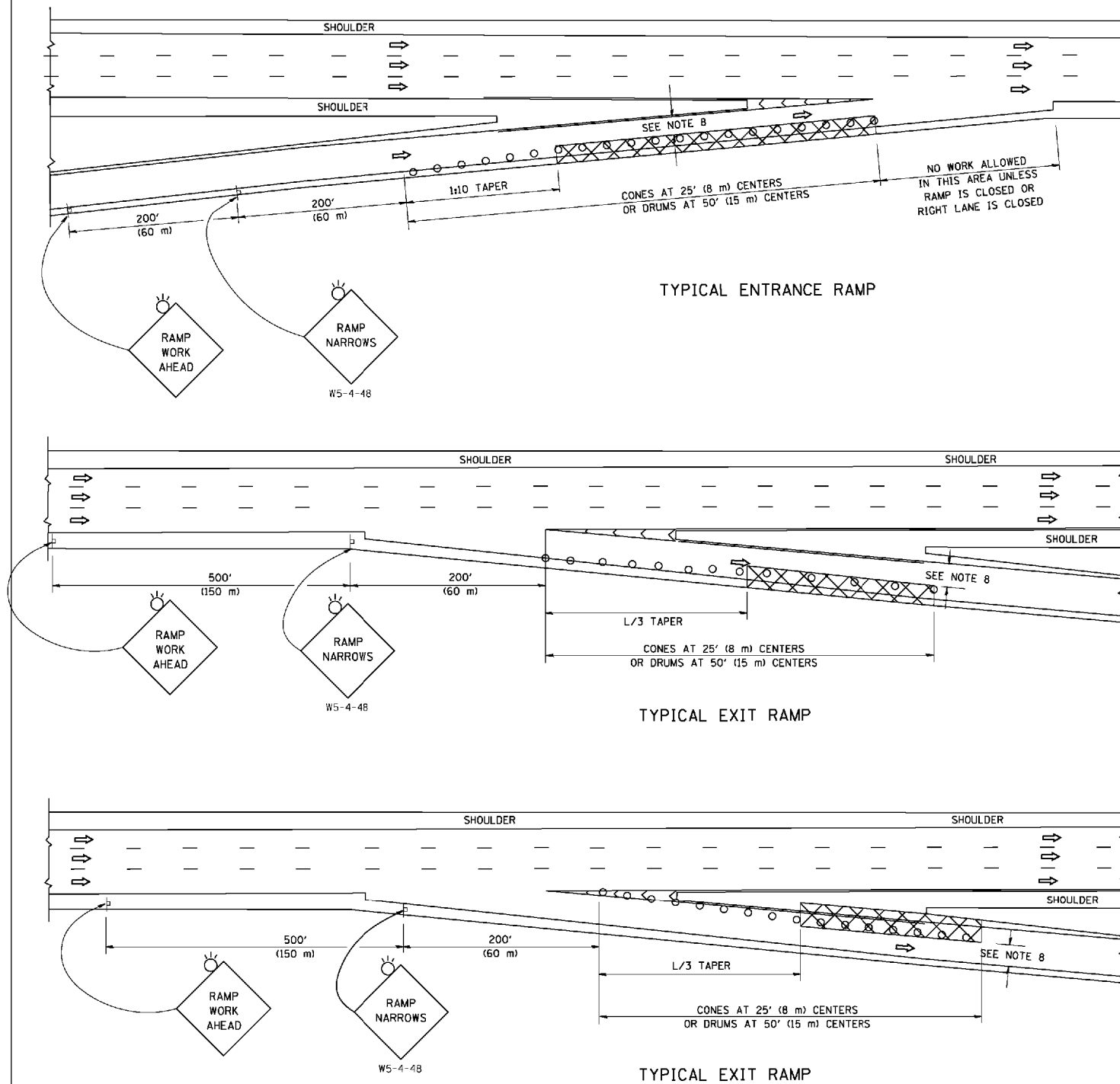
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MULTI-LANE FREEWAY  
PAVEMENT MARKING DETAILS**

SCALE: NONE SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	37
TC-12		CONTRACT NO. 60V71		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP

TYPICAL EXIT RAMP

TYPICAL EXIT RAMP

SYMBOLS

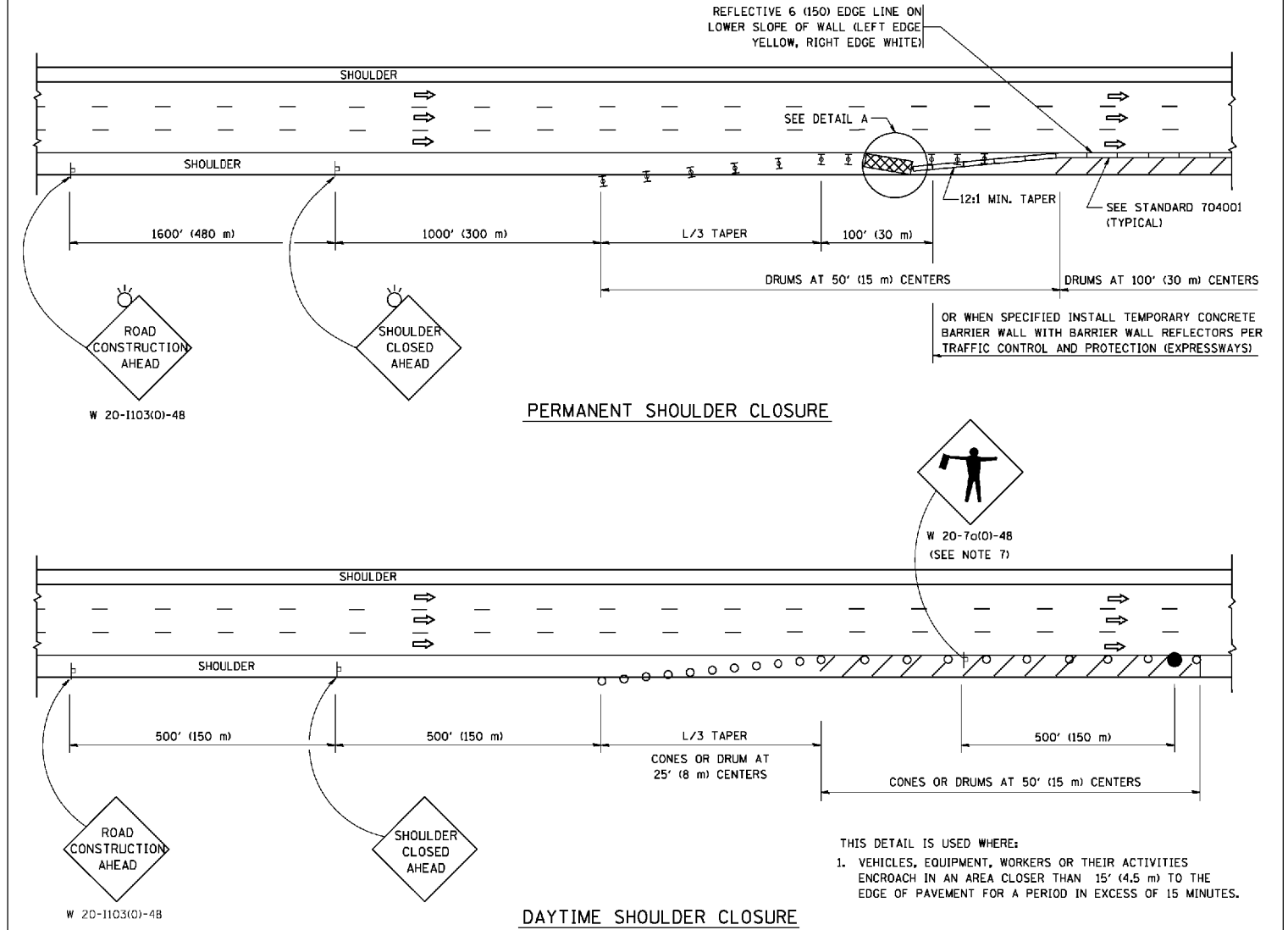
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

1. THE "L" DISTANCE EQUALS:
 

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC    ENGLISH L=0.65(W/S)    L=(W/S)
W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)	
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

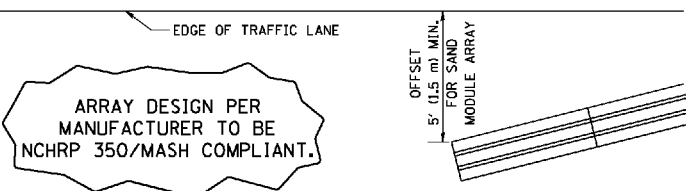
SHOULDER CLOSURE DETAILS



PERMANENT SHOULDER CLOSURE

DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:  
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"  
IMPACT ATTENUATOR, TEMPORARY  
(SEE NOTE 5)

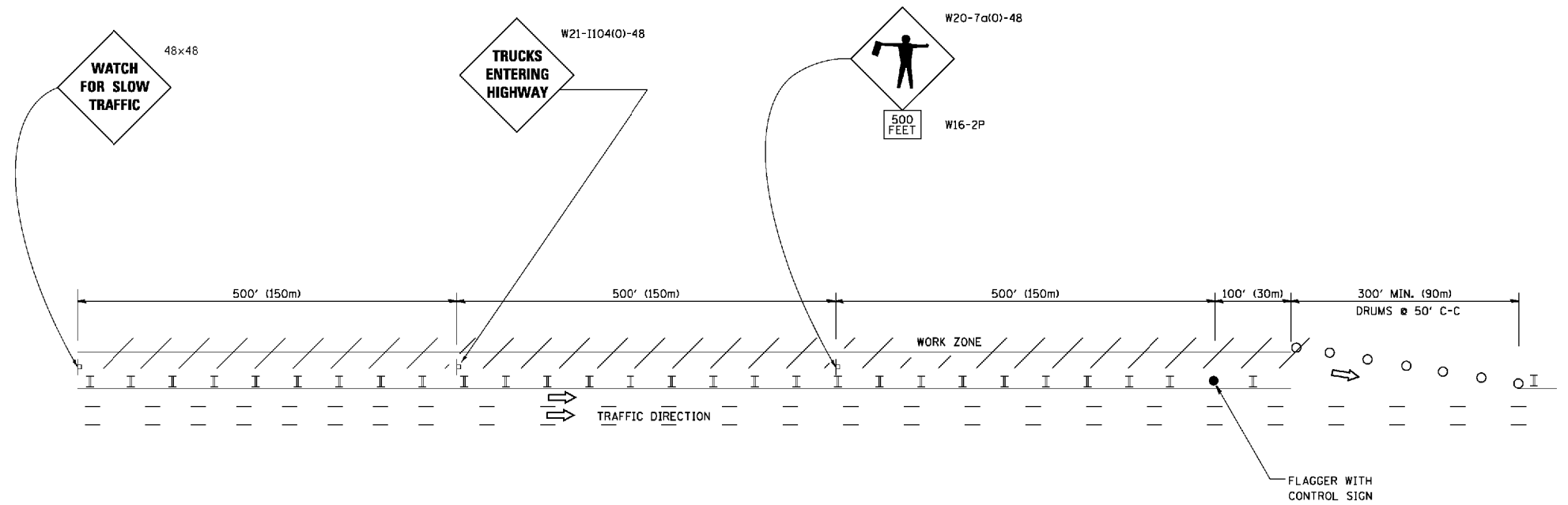
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12" MIN. WIDTH TANGENT SECTION  
16" MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

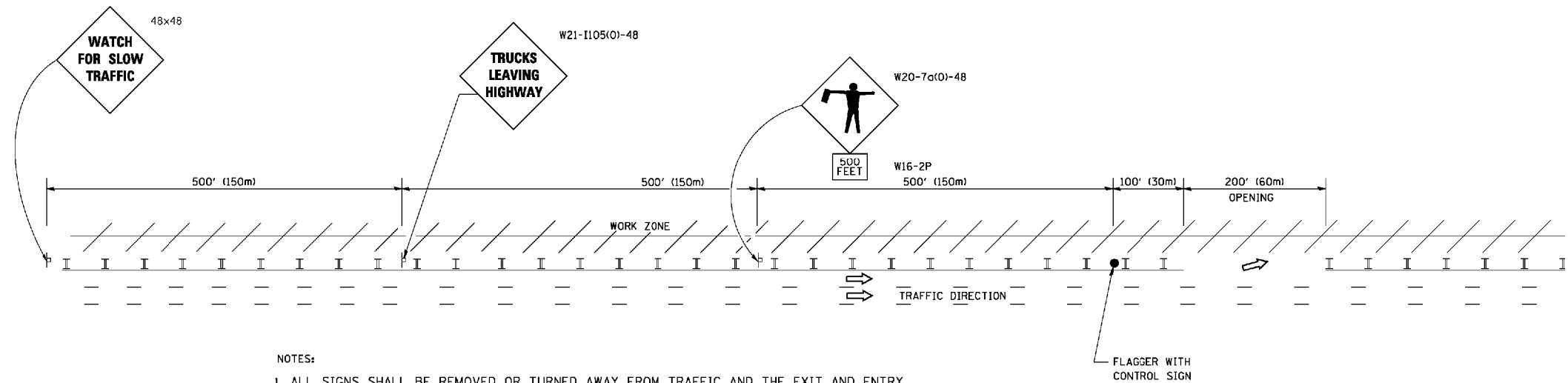
FILE NAME =	USER NAME = faatemj	DESIGNED -	REVISED - J.A.F. 12-06	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES</b>	F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\pwwork\faatemj\00188315\sc17.dgn		DRAWN - D.W.S.	REVISED - S.P.B. 01-07			80	99-4-1-SB-B-R	WILL	46	38
PLOT SCALE = 5/8"=1'-0"		CHECKED -	REVISED - S.P.B. 12-09			<b>TC-17</b>		CONTRACT NO. 60V71		
PLOT DATE = 7/1/2013		DATE - 11-96	REVISED - M.D. 06-13			SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

# SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

## WORK ZONE EXIT OPENING



## WORK ZONE ENTRY OPENING

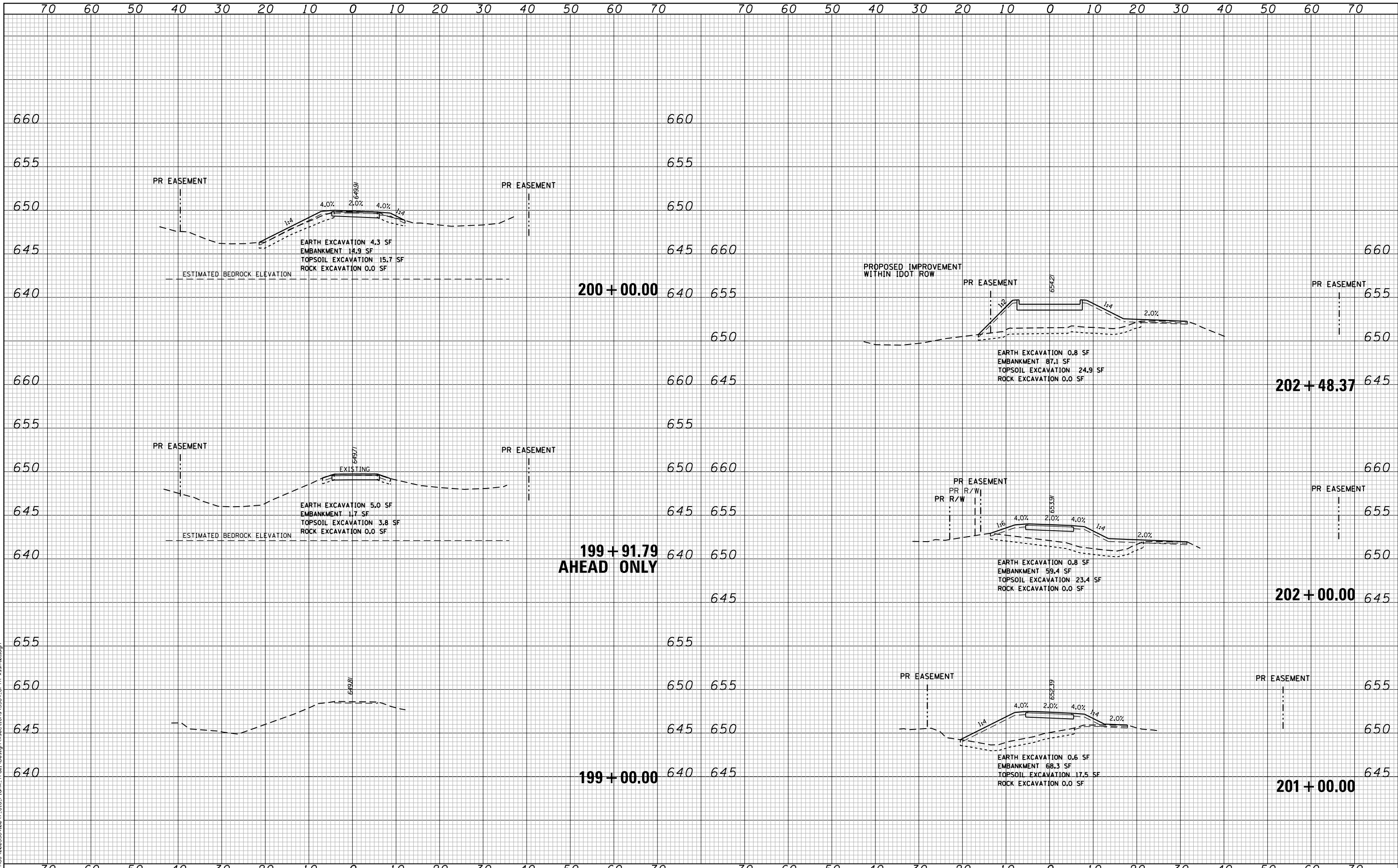


### NOTES:

1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMP.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = faatemj	DESIGNED -	REVISED - J.A.F. 02-06	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>FREEWAY/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS</b>	F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\faatemj\00188315\sc18.dgn		DRAWN -	REVISED - S.P.B. 01-07			80	99-4-1-SB-B-R	WILL	46	39
		CHECKED -	REVISED - S.P.B. 12-09			<b>TC-18</b>		CONTRACT NO. 60V71		
		DATE -	REVISED - M.D. 06-13			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			



FILE NAME = N:\PROJ\002334-00\7\_01\1\_Planck\_Trail\Design\Xsect\cross\3384\_01R-rash.dwg

**CG** Ciorba Group, Inc.  
 CONSULTING ENGINEERS  
 5507 North Cumberland Avenue, Suite 402  
 Chicago, Illinois 60656  
 Tel. 773.775.4009 Fax 773.775.4014

USER NAME = ahern	DESIGNED - NT	REVISED -
	DRAWN - NT	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - DJD	REVISED -
PLOT DATE = 3/16/2016	DATE - 8/14/2014	REVISED -

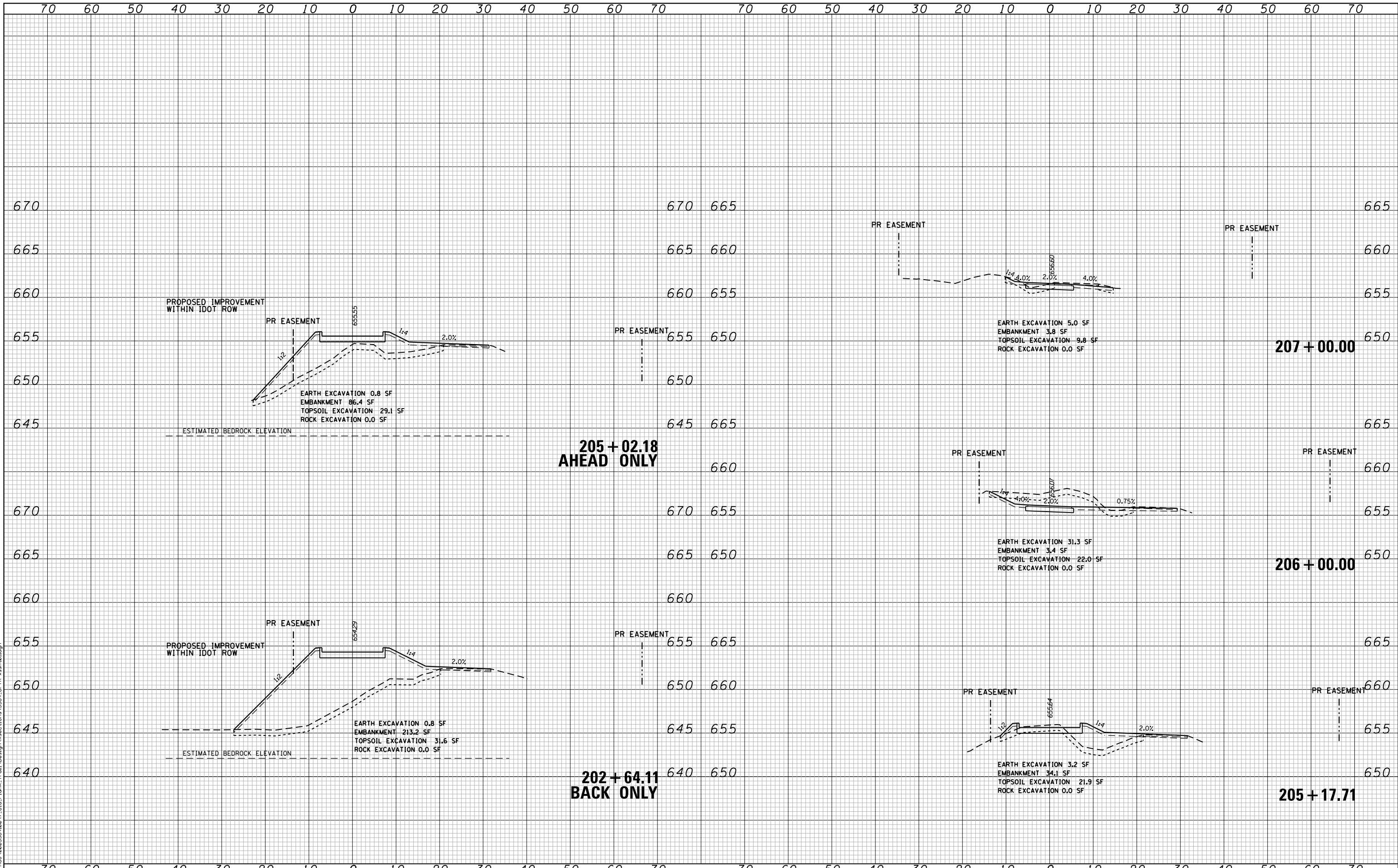
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80  
 CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 199+00.00 TO STA. 202+48.37

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	40
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 60V71	





**205 + 02.18  
AHEAD ONLY**

**202 + 64.11  
BACK ONLY**

**207 + 00.00**

**206 + 00.00**

**205 + 17.71**

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**CG** Ciorba Group, Inc.  
CONSULTING ENGINEERS  
5507 North Cumberland Avenue, Suite 402  
Chicago, Illinois 60656  
Tel. 773.775.4009 Fax 773.775.4014

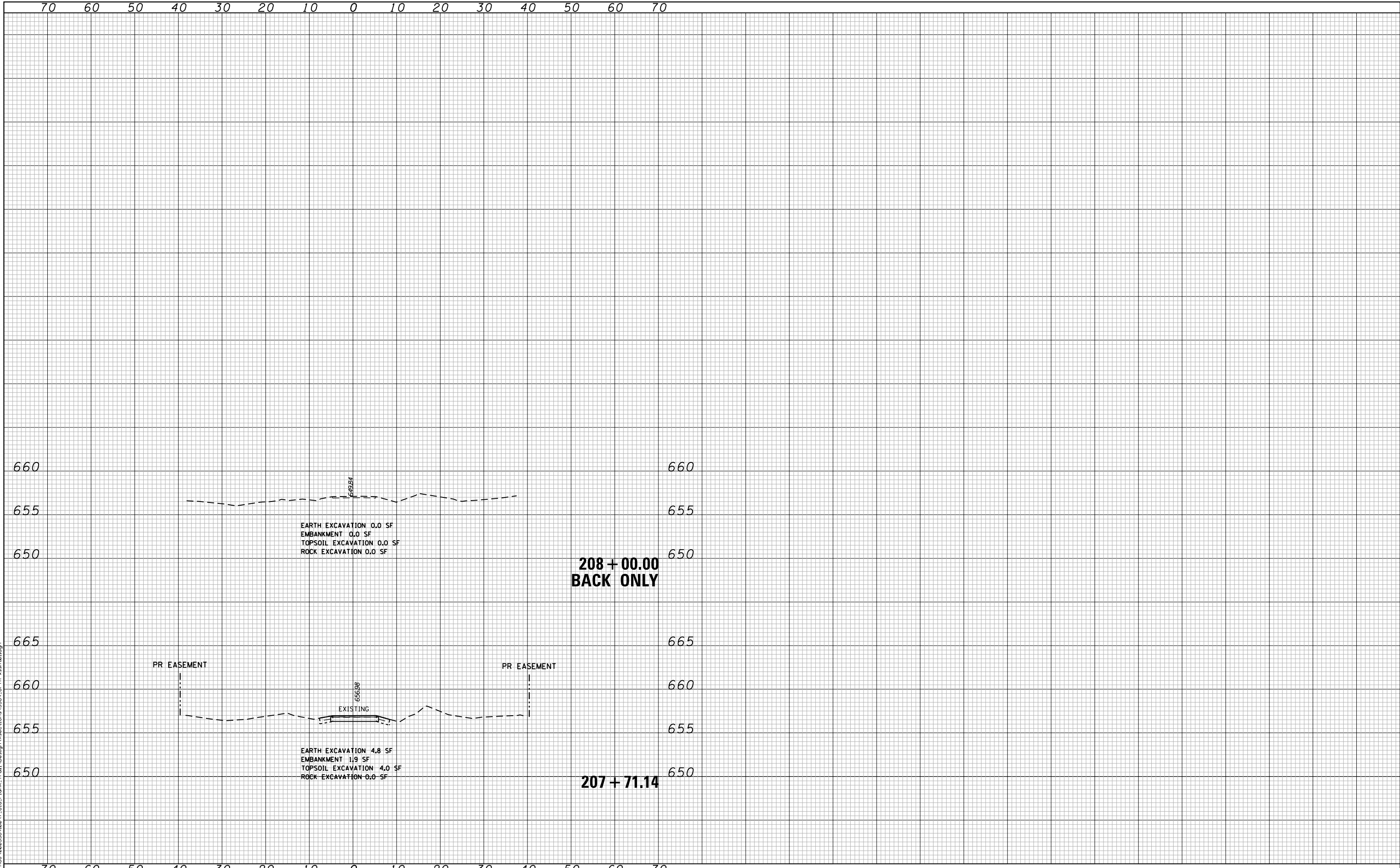
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80  
CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 202+64.11 TO STA. 207+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	41
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 60V71	



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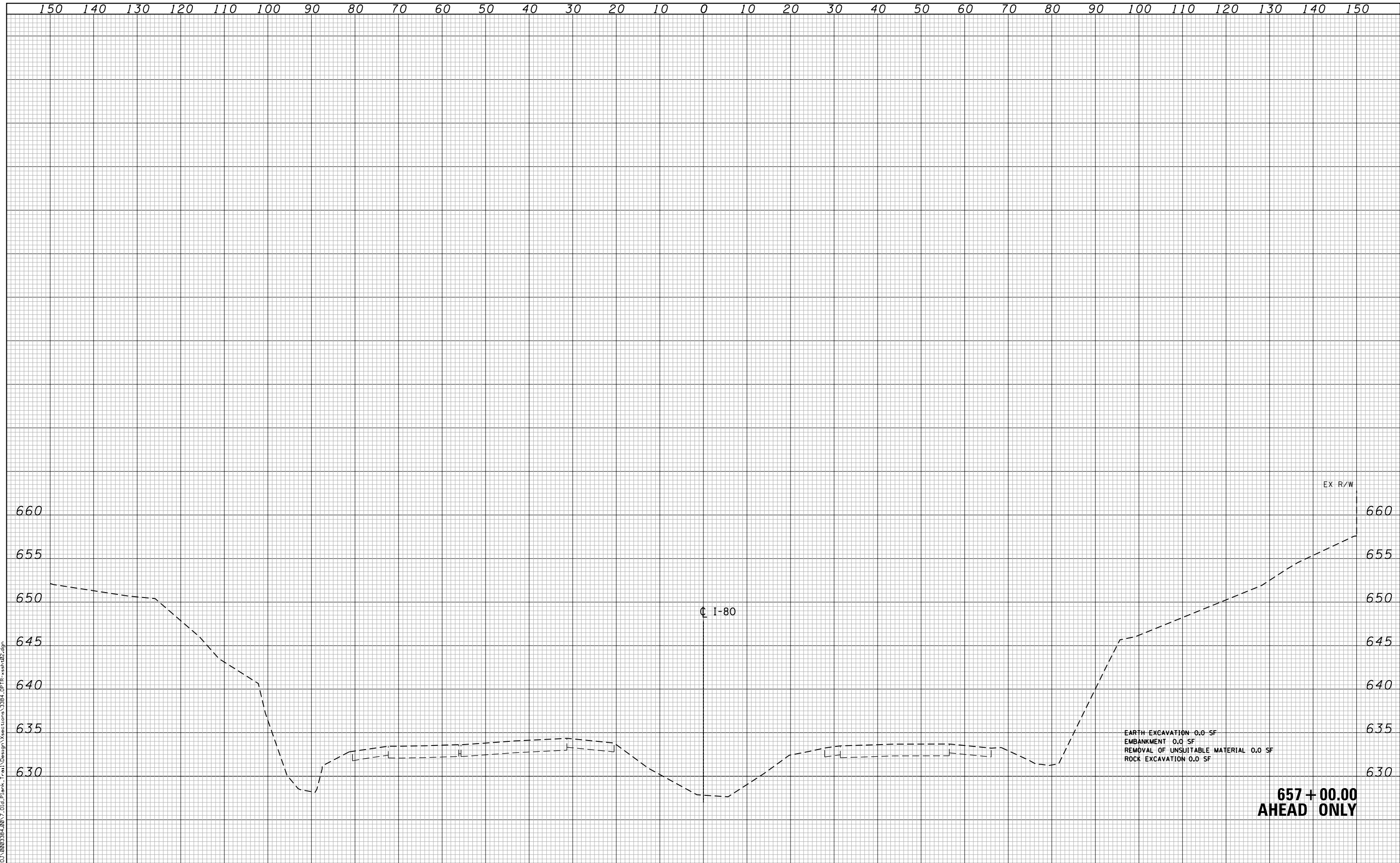
<p><b>Ciorba Group, Inc.</b> CONSULTING ENGINEERS 5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656 Tel. 773.775.4009 Fax 773.775.4014</p>	USER NAME = ahern	DESIGNED - NT	REVISED -
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	PLOT DATE = 3/16/2016	CHECKED - DJO	REVISED -
		DATE - 8/14/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80  
CROSS SECTIONS**

SCALE:      SHEET NO.    OF    SHEETS    STA. 207+71.14    TO STA. 208+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	42
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 60V71	



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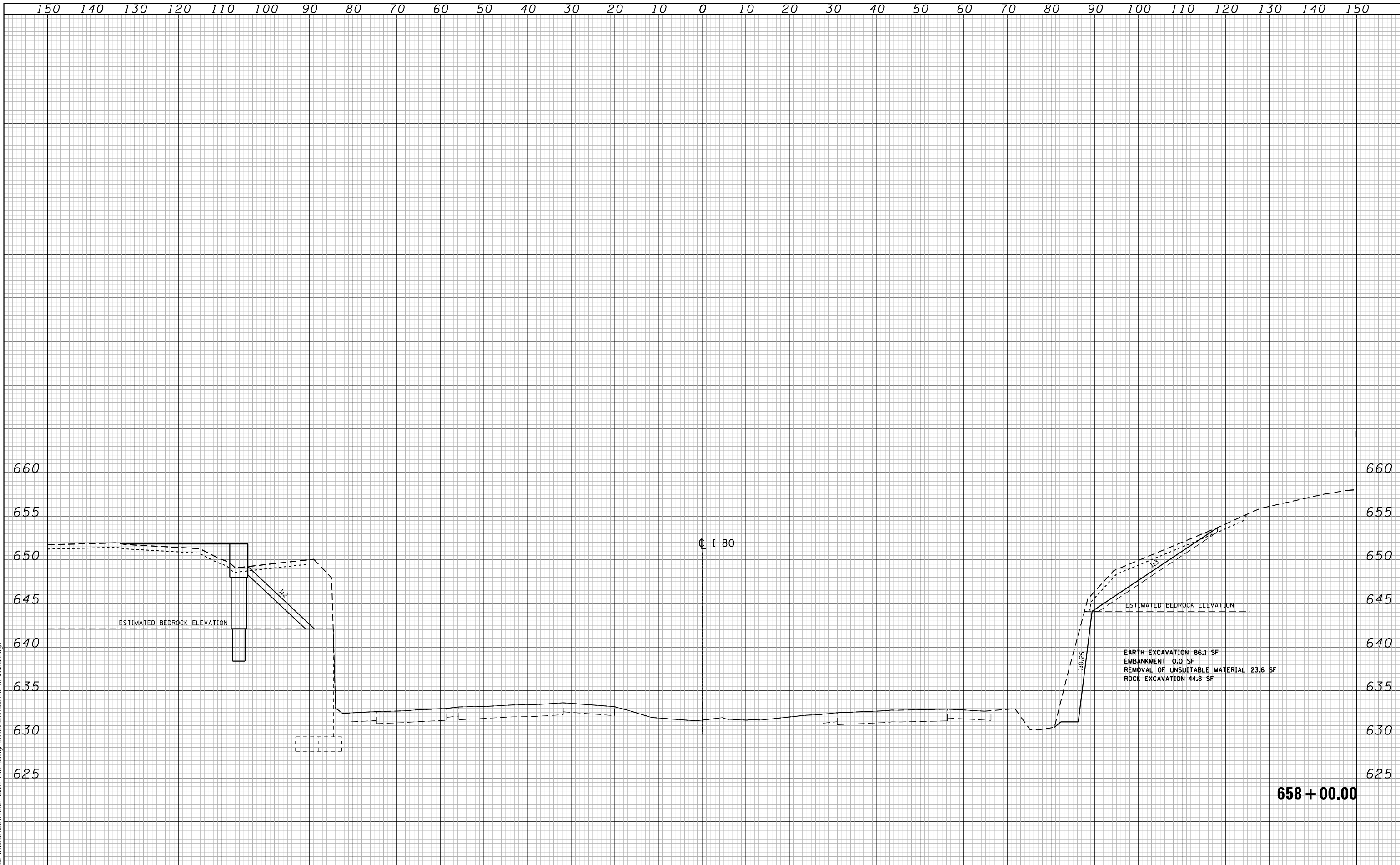
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PLOT DATE = 3/16/2016	DATE - 8/14/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80  
 CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 657+00.00 TO STA. 657+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	43
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 60V71	



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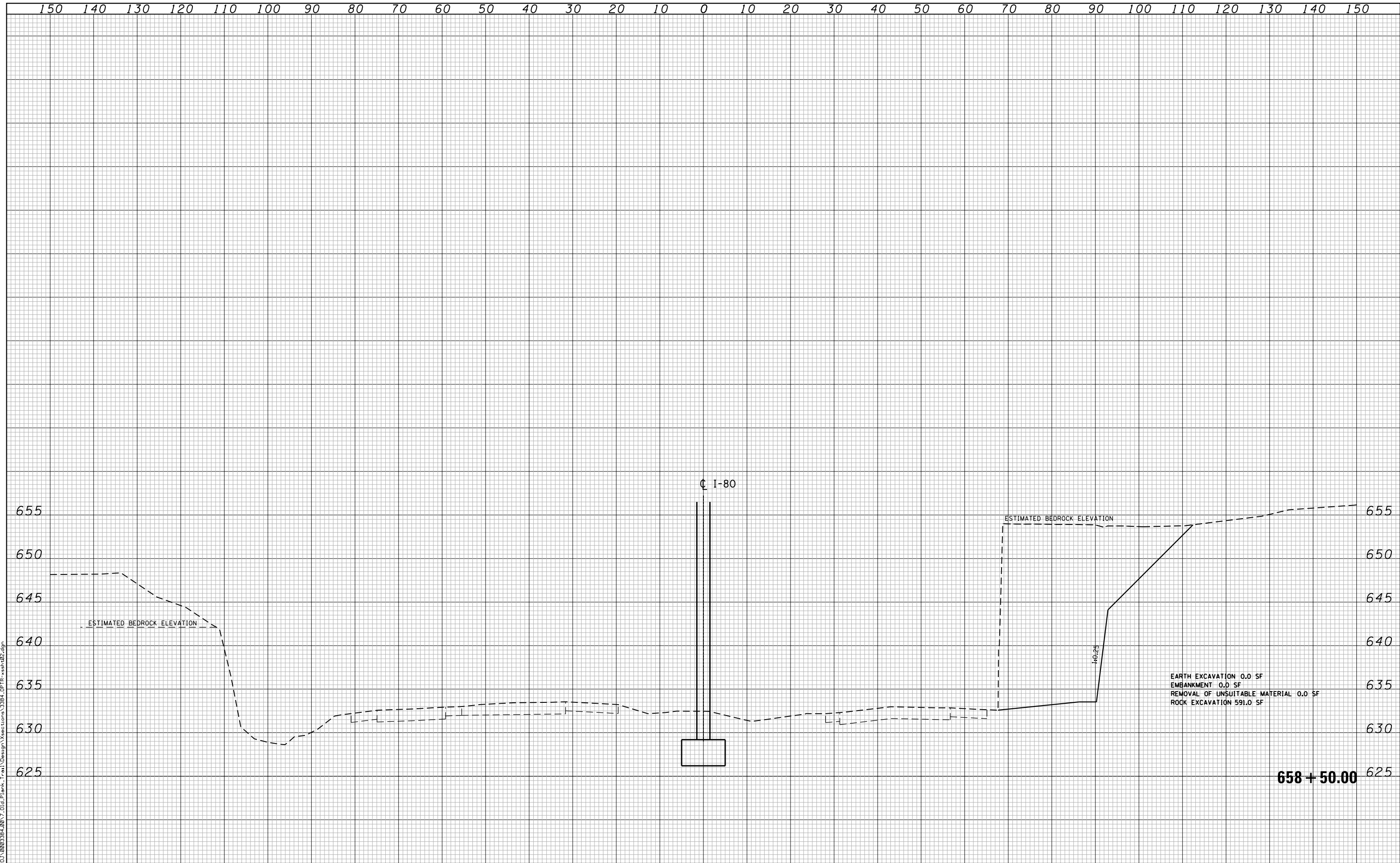
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PLOT DATE = 3/16/2016	DATE - 8/14/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80  
 CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 658+00.00 TO STA. 658+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	44
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 60V71	



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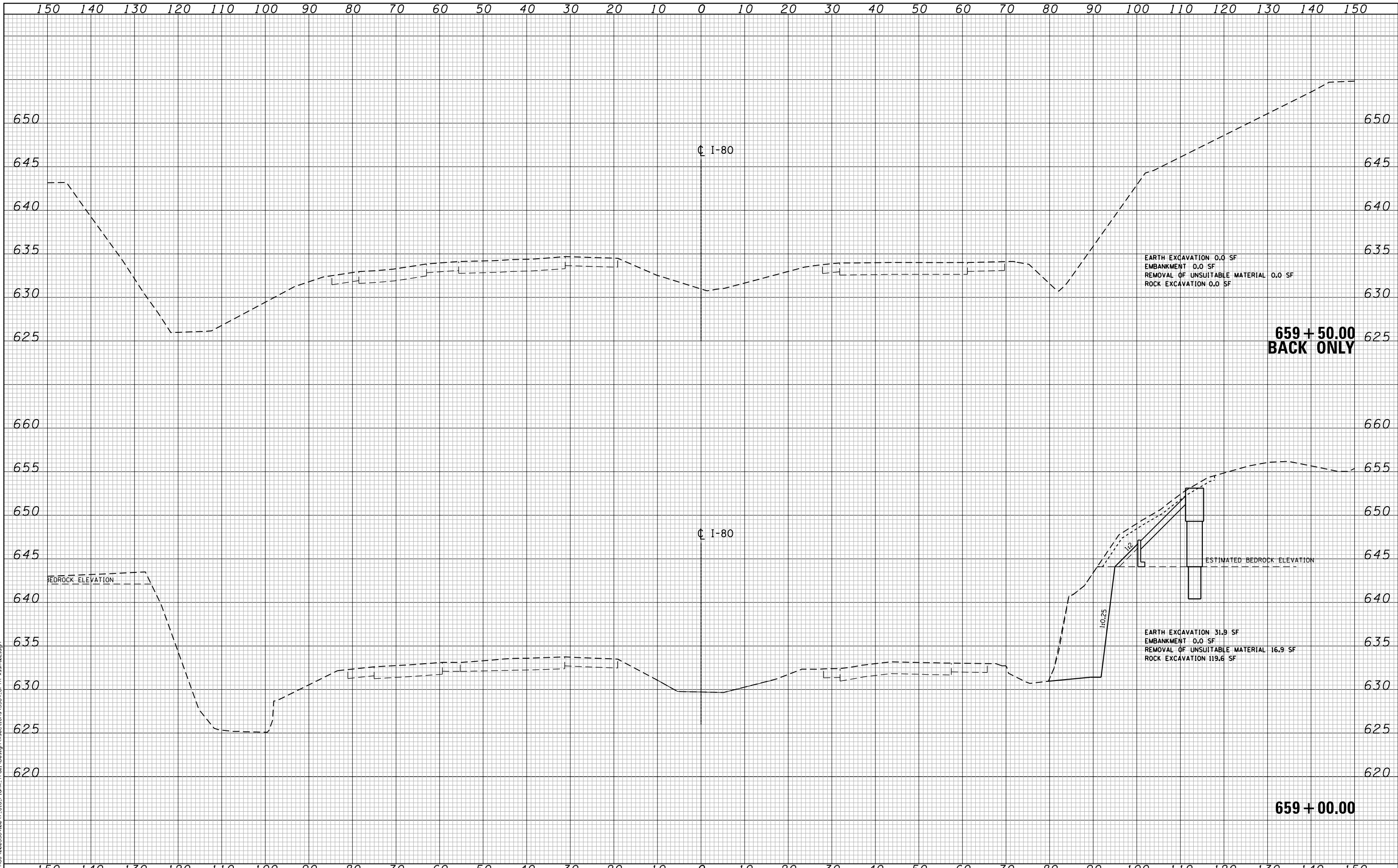
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PLOT DATE = 3/16/2016	DATE - 8/14/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80  
 CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 658+50.00 TO STA. 658+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	45
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 60V71	



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PLOT DATE = 3/16/2016	DATE - 8/14/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**OLD PLANK ROAD TRAIL OVER F.A.I. ROUTE 80  
 CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 659+00.00 TO STA. 660+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	99-4-1-SB-B-R	WILL	46	46
CONTRACT NO. 60V71			FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT	