06-10-216 LETTING ITEM 020

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	1
	ILLINOIS	CONTRACT NO. 61C79		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FOR INDEX OF SHEETS, SEE SHEET NO. 2

F.A.U. ROUTE 1339 (BIESTERFIELD ROAD) – MEACHAM ROAD TO ROHLWING ROAD
F.A.U. ROUTE 1331 (OAKTON STREET) – SHADYWOOD LANE TO HIGGINS ROAD
RESURFACING, TRAFFIC SIGNALS
SECTION 15–00062–00–RS
PROJECT M–4003(683)
VILLAGE OF ELK GROVE VILLAGE
COOK COUNTY
C–91–216–16

LOCATION OF SECTION INDICATED THUS: –

DESIGN SPEED

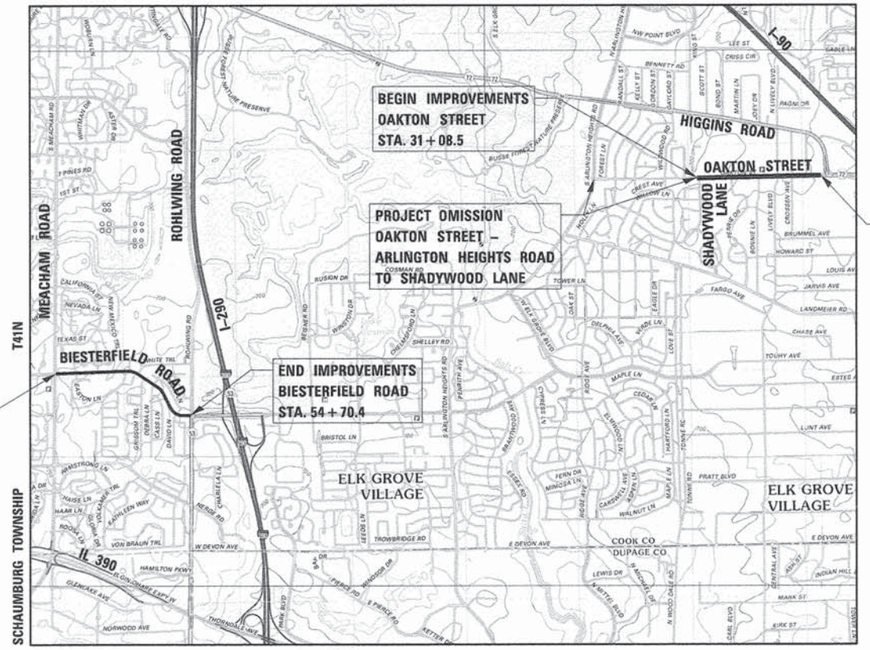
BIESTERFIELD ROAD – 30 MPH
OAKTON STREET – 40 MPH

POSTED SPEED

BIESTERFIELD ROAD – 25 MPH
OAKTON STREET – 35 MPH

FUNCTIONAL CLASSIFICATION

BIESTERFIELD ROAD – MINOR ARTERIAL (2015 ADT = 16,000)
OAKTON STREET – MINOR ARTERIAL (2015 ADT = 10,000)



LOCATION MAP
(NOT TO SCALE)

100' 200' 300' — 1" = 100'
10' 20' 30' — 1" = 10'
50' 100' — 1" = 50'
50' 100' — 1" = 40'
50' 100' — 1" = 30'
50' 100' — 1" = 20'

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

GROSS AND NET LENGTHS

BIESTERFIELD ROAD = 4399.6 FT. (0.833 MILES)
OAKTON STREET = 2951.2 FT. (0.569 MILES)
TOTAL = 7350.8 FT. (1.392 MILES)

REGISTERED P.E., STATE OF ILLINOIS
EXPIRES 11–30–2017
FOR DRAWINGS 1 TO 27 AND 42 TO 54

DAVID J. KREEGER
062-056261
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS

REGISTERED P.E., STATE OF ILLINOIS
EXPIRES 11–30–2017
FOR DRAWINGS 28 TO 41

JOSEPH J. EMRY
062-057496
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED March 2 20 16
DIRECTOR OF PUBLIC WORKS

PASSED MARCH 22, 2016 20
C. Christopher Holt
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW March 22 20 16
REGION ONE ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PLANS PREPARED BY:

CIVILTECH
450 E. Devon Ave, Suite 300 - Itasca, Illinois 60143
Tel: 630.773.3900 - Fax: 630.773.3975
www.civiltechinc.com

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E. (847) 705–4021 SCHAUMBURG, IL
CONSULTANT ENGINEER: DAVID J. KREEGER, P.E. CIVILTECH ENGINEERING, INC.

CONTRACT NO. 61C79

INDEX OF DRAWINGS

IDOT HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424011-02	CORNER PARALLEL CURB RAMPS FOR SIDEWALK
442201-03	CLASS C AND D PATCHES
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602301-04	INLET - TYPE A
602601-04	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-04	FRAMES AND LIDS TYPE 1
604051-04	FRAME AND GRATE TYPE 11
606001-06	CONCRETE CURB TYPE B & COMBINATION CONCRETE CURB & GUTTER
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM EDGE OF PAVEMENT
701301-04	LANE CLOSURE 2L, 2W SHORT TIME OPERATIONS
701427-04	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS <= 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701602-07	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-05	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
780001-05	TYPICAL PAVEMENT MARKINGS
878001-10	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

IDOT DISTRICT ONE STANDARDS

STANDARD NO.	DESCRIPTION
BD-07	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER
BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
TC-16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-26	DRIVEWAY ENTRANCE SIGN
TS-07	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ("STANDARD SPECIFICATIONS"), ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED APRIL 1, 2016; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" 2014, 7TH EDITION, THE DETAILS IN THE PLANS, AND THE SPECIAL PROVISIONS AND IDOT STANDARD DRAWINGS INCLUDED IN THE CONTRACT DOCUMENTS.
2. NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET AND APPROPRIATE PERMITS HAVE BEEN OBTAINED.
3. THE ENGINEER AND ALL UTILITY COMPANIES, SCHOOL DISTRICTS, AND LOCAL POLICE AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
5. WHEN REMOVING CURB AND GUTTER, PAVEMENT OR ANY OTHER STRUCTURE, THE CONTRACTOR SHALL TAKE PRECAUTIONS NECESSARY TO AVOID DAMAGE TO UNDERGROUND PUBLIC OR PRIVATE UTILITIES IN ACCORDANCE WITH ARTICLES 105.07, 107.20, AND 107.31. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL CONCRETE BREAKER BE ALLOWED.
6. THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE PROJECT LIMITS. ALL EXCESS OR WASTE MATERIAL SHALL BE EITHER HAULED AWAY FROM THE PROJECT SITE BY THE CONTRACTOR AND DEPOSITED AT LOCATIONS PROVIDED BY HIM, OR DISPOSED OF WITHIN THE RIGHT-OF-WAY IN A MANNER OTHER THAN BURNING, SUBJECT TO THE APPROVAL OF THE ENGINEER. NO EXTRA COMPENSATION WILL BE ALLOWED THE CONTRACTOR FOR ANY EXPENSE INCURRED BY COMPLYING WITH THE REQUIREMENTS OF THIS NOTE.

TREE REMOVAL, CLEARING AND HEDGE REMOVAL

1. THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF THE UTMOST IMPORTANCE TO THE VILLAGE. ALL TREE PROTECTION, TREE REMOVAL, TREE PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.
2. TEMPORARY FENCE SHALL BE ERECTED ALONG THE DRIP LINE OF EXISTING TREES TO REMAIN WHEN DIRECTED BY THE ENGINEER. AFTER TREES ARE SAFELY FENCED NOTHING IS TO BE STORED, DRIVEN, OR DISTURBED INSIDE THE FENCE. REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.

DRIVEWAY RECONSTRUCTION

1. ACCESS TO DRIVEWAY SHALL BE PROVIDED AT THE END OF EACH WORK DAY EXCEPT WHEN CURB AND GUTTER IS INSTALLED. FOR COMMERCIAL PROPERTIES, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. IF A COMMERICAL PROPERTY HAS ONLY ONE DRIVEWAY, THEN ALL WORK IN FRONT OF THE DRIVEWAY SHALL BE STAGED TO MAINTAIN ACCESS.
2. ALL WORK THAT IMPACTS THE DRIVEWAYS OR ACCESS TO THE FIRE STATION ON OAKTON STREET SHALL BE COORDINATED WITH THE ENGINEER. ACCESS TO ONE OF THE DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES.

EXCAVATION

1. EXCAVATION BENEATH PAVEMENT PATCHES, WHEN REQUIRED BY THE ENGINEER, SHALL BE PAID FOR AS "EARTH EXCAVATION". BACKFILL OF THIS AREA SHALL BE PAID FOR SEPARATELY AS "AGGREGATE BASE COURSE, TYPE B".

SIGNING

1. ALL EXISTING SIGNS AND POSTS REMOVED AND NOT REINSTALLED SHALL BE RETURNED TO THE JURISDICTION FROM WHICH IT WAS REMOVED.
2. EXISTING SIGNS SHOWN TO BE REPLACED SHALL NOT BE REMOVED UNTIL THE DAY THAT THE THE NEW SIGN IS INSTALLED.

LANDSCAPING

1. WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER.
2. THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR AT HIS EXPENSE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

PAVING, CURB & GUTTER AND SIDEWALK

1. THE PAVEMENT PATCHING AND CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATIONS BASED ON FIELD INVESTIGATIONS. THE ENGINEER SHALL MAKE THE FINAL DETERMINATION ON THE LOCATION OF PAVEMENT PATCHES AND CURB AND GUTTER REMOVAL AND REPLACEMENT IN THE FIELD.
2. THE CONTRACTOR SHALL SAW CUT PAVEMENT, CURB & GUTTER, DRIVEWAYS, AND SIDEWALK AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING MATERIAL TO BE REMOVED BY MEANS OF AN APPROVED CONCRETE SAW TO A DEPTH AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

   THE CONTRACTOR SHALL BE REQUIRED TO SAW VERTICAL CUTS SO AS TO FORM CLEAN VERTICAL JOINTS. SHOULD THE CONTRACTOR DEFACE ANY EDGE, A NEW SAWED JOINT SHALL BE PROVIDED AND ANY ADDITIONAL WORK, INCLUDING REMOVAL AND REPLACEMENT, SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.
3. HOT-MIX ASPHALT BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
4. HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL EARTH EXCAVATION, TOPSOIL PLACEMENT, AND HOT-MIX ASPHALT BINDER COURSE HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.
5. THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACE, BINDER, OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED.
6. THE MAXIMUM CROSS SLOPE AT ANY POINT IN THE TRAVERSABLE AREA OF THE SIDEWALK, INCLUDING THE AREAS THROUGH DRIVEWAYS, SHALL BE 2.00%. ALL AREAS OF NEW SIDEWALK THAT EXCEED THIS MAXIMUM SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.
7. ALL SAW CUTS REQUIRED FOR THE CONSTRUCTION OF CLASS D PATCHES WILL NOT BE PAID FOR SEPERATELY, BUT SHALL BE INCLUDED IN THE COST OF "CLASS D PATCHES" OF THE TYPE AND THICKNESS SPECIFIED.
8. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS, AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER, MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EXPOXY COATED, UNLESS NOTED ON THE PLAN.

FILE NAME = ...\3224_Notes_01.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
$MODEL_NAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
INDEX, GENERAL NOTES, AND STANDARDS

SHEET 1 OF 2 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	2
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

UTILITIES

1. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.

2. COORDINATION OF ANY UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.

3. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS, WATER, PETROLEUM, SEWER AND CABLE TELEVISION FACILITIES (48 HOURS NOTIFICATION IS REQUIRED).

4. THE COUNTY IS NOT PART OF JULIE FOR LOCATION OF TRAFFIC SIGNAL EQUIPMENT. THE CONTRACTOR SHALL CONTACT THE MECHANICAL, ELECTRICAL, ARCHITECTURAL, AND LANDSCAPING DIVISION AT 312-603-1730.

5. PRIOR TO STARTING ANY WORK ON OAKTON STREET, THE CONTRACTOR SHALL CONTACT WEST SHORE PIPELINE (BILL O'MALLEY, 847-439-0270, 847-878-3428 (CELL)).

6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE UTILTY OWNER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL NOTICY ALL UTILITY OWNERS OF HIS CONSTRUCTION SCHEDULE AND SHALL COORDINATE CONSTRUCTION OPERATIONS WITH THE UTILITY OWNERS SO THAT THE RELOCATION OF UTILITY LINES AND STRUCTURES MAY PROCEED IN AN ORDERLY MANEER. NOTIFICATION SHALL BE IN WRITING, WITH COPIES TRANSMITTED TO THE ENGINEER.

7. WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE STORM SEWERS AND DRAINAGE STRUCTURES ADJUSTED OR RECONSTRUCTED AS PART OF THIS PROJECT.

8. ANY EXISTING OR PROPOSED SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO COST.

8. THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR CONSTRUCTION STAGING NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT AND/OR FOR DELAYS CAUSED BY UTILITY RELOCATION OR ADJUSTMENT.

10. THE CONTRACTOR SHALL FURNISH ALL LABOR, EQUIPMENT AND MATERIAL NECESSARY FOR DEWATERING TRENCH EXCAVATIONS AS WELL AS SHORING TRENCH WALLS DURING STORM SEWER AND WATERMAIN OPERATIONS. THE COST TO COMPLY WITH THE ABOVE SHALL BE INCLUDED IN THE COST OF THE STORM SEWERS AND DRAINAGE STRUCTURES INSTALLED AS PART OF THIS PROJECT.

11. STRUCTURE ADJUSTMENTS AND RECONSTRUCTIONS HAVE BEEN SHOWN BASED ON FIELD INVESTIGATIONS. THE FINAL DETERMINATION FOR WHETHER THE WORK TO BE PERFORMED IS AN ADJUSTMENT OR RECONSTRUCTION WILL BE MADE BY THE ENGINEER IN THE FIELD.

12. THE MAXIMUM HEIGHT OF ADJUSTING RINGS ON UTILITY STRUCTURES SHALL BE 8". CONCRETE ADJUSTMENT RINGS LESS THAN 4 INCHES SHALL NOT BE ALLOWED. HIGH DENSITY POLYETHYLENE (HDPE) PLASTIC RINGS AND RING WEDGES SHALL BE USED FOR ALL ADJUSTMENTS LESS THAN 4" OR IN COMBINATION WITH 4 INCH MINIMUM CONCRETE ADJUSTMENT RINGS. BRICKS SHALL NOT BE ALLOWED.

STORM & SANITARY SEWER

1. UNLESS OTHERWISE NOTED ON THE PLANS, THE EXISTING DRAINAGE FACILITIES SHALL REMAIN IN USE DURING THE PERIOD OF CONSTRUCTION. LOCATIONS OF EXISTING DRAINAGE STRUCTURES AND SEWERS AS SHOWN ON THE PLANS ARE APPROXIMATE. PRIOR TO COMMENCING WORK THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL DETERMINE THE EXACT LOCATIONS OF EXISTING STRUCTURES WHICH ARE WITHIN THE PROPOSED CONSTRUCTION LIMITS.

   DURING CONSTRUCTION, IF THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWERS, UNDERDRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, HE SHALL SO INFORM THE ENGINEER, WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF MAINTAINED. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF THE NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE. SHOULD THE ENGINEER HAVE DIRECTED THE REPLACEMENT OF A FACILITY, THE NECESSARY WORK AND PAYMENT SHALL BE IN ACCORDANCE WITH SECTIONS 550 AND 601, AND ARTICLE 104.02 OF THE STANDARD SPECIFICATIONS.

2. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET. HE SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWER ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE STORM SEWERS AND DRAINAGE STRUCTURES INSTALLED AS PART OF THIS PROJECT.

EROSION CONTROL

1. ALL VEGETATIVE AND STRUCTURAL EROSION CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE "ILLINOIS PROCEDURES AND STANDARDS FOR URBAN SOIL EROSION AND SEDIMENTATION CONTROL" AND THE "STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL" OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

2. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.

3. THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RUNOFF-PRODUCING RAINFALL EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF SAID MEASURES SHALL BE MADE IMMEDIATELY.

4. ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT. MUD AND SEDIMENT DEPOSITS SHALL BE REMOVED FROM THE ROADWAY AT THE END OF EACH WORK DAY BY SHOVELING AND/OR SWEEPING.

5. ALL SLOPES SHALL BE COVERED WITH SOD AS GRADING AND PLACEMENT OF TOPSOIL HAS BEEN COMPLETED. THE LIMITS OF THE SOD SHALL BE THE LIMITS OF GRADING.

6. INLET FILTERS SHALL BE PLACED ON ALL CATCH BASINS, INLETS, AND MANHOLES WITH OPEN GRATES IN THE CURB AND GUTTER AND SHOULDERS.

7. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER.

9. THE SURFACE OF ALL STRIPPED AREAS SHALL BE PERMANENTLY OR TEMPORARILY PROTECTED FROM SOIL EROSION WITHIN 14 DAYS AFTER FINAL GRADE IS REACHED. STRIPPED AREAS THAT WILL REMAIN UNDISTURBED FOR MORE THAN 14 DAYS AFTER INITIAL DISTURBANCE SHALL BE PROTECTED FROM EROSION WITH THE USE OF TEMPORARY EROSION CONTROL SEEDING. TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES SHALL BE MAINTAINED CONTINUOUSLY UNTIL PERMANENT COVER IS ESTABLISHED.

MISCELLANEOUS

1. THE CONTRACTOR SHALL PAY SPECIAL ATTENTION TO ARTICLE 201.01(A) OF THE STANDARD SPECIFICATIONS. REMOVAL OF ALL OBSTRUCTIONS IN THE RIGHT-OF-WAY, THAT ARE NOT INCLUDED IN A SPECIFIC REMOVAL ITEM SHALL BE CONSIDERED CLEARING AND INCLUDED IN THE COST OF THE CONTRACT. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, FENCES, WALLS, FOUNDATIONS, BUILDINGS, WOODEN POWER POLES, WOODEN PLANTERS, GATES, AND ALL VEGETATION, TREES, SHRUBS, ETC. LESS THAN 6" IN DIAMETER.

2. THE CONTRACTOR SHALL NOT CROSS COMPLETED BINDER COURSE, OR EXISTING PAVEMENT NOT SCHEDULED TO BE REMOVED, WITH CONSTRUCTION EQUIPMENT WHICH MAY DAMAGE THE PAVEMENT.

3. THE CONTRACTOR SHALL CONTACT THE IDOT TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

CCDD REPORTS

1. CONTRACTORS THAT WANT TO VIEW THE FULL CCDD REPORT SHOULD CONTACT THE OWNER OF RECORD. TO MAKE ARRANGEMENTS FOR ACCESS TO THIS INFORMATION, PLEASE CONTACT:

   MR. BRIAN LOVERING
   CHIEF INFRASTRUCTURE ENGINEER
   VILLAGE OF ELK GROVE VILLAGE
   (847) 734-8044

STAKING

1. ALIGNMENT, TIES AND BENCHMARKS ARE NOT PROVIDED IN THE PLANS DUE TO THE SCOPE OF THE WORK SHOWN ON THE PLANS. EXISTING TOPOGRAPHY IS SHOWN BASED ON AERIAL IMAGERY.

2. AN EXISTING CENTERLINE HAS BEEN SHOWN FOR ALL ROADWAYS. IN GENERAL, THE CENTERLINE REPRESENTS THE CENTER OF ROADWAY. THE EXISTING CENTERLINE IS ONLY A BEST-FIT APPROXIMATION BASED ON AERIAL IMAGERY AND RECORD PLANS. ITS PURPOSE IS ONLY TO PROVIDE A GENERAL LENGTH OF ROADWAY IMPROVEMENTS.

3. ALL DIMENSIONS SHOWN ON THE PLANS ARE APPROXIMATE BASED ON FIELD INVESTIGATIONS. FINAL LENGTHS AND AREAS OF PROPOSED WORK WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

FILE NAME = ...\3234_Notes_02.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
INDEX, GENERAL NOTES, AND STANDARDS
SHEET 2 OF 2 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	3
		CONTRACT NO. 61C79		
ILLINOIS	FED. AID PROJECT M-4003(683)			

SPECIAL PROVISION	SPECIALTY ITEM	CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
						0005	0031	0005
						ROADWAY RESURFACING	NON PARTICIPATING	ROADWAY RESURFACING
						BIESTERFIELD ROAD		OAKTON STREET
X		20101000	TEMPORARY FENCE	FOOT	480	320		160
X	X	20101200	TREE ROOT PRUNING	EACH	12	8		4
X	X	20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	34	25		9
X	X	20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	10	7		3
		20200100	EARTH EXCAVATION	CU YD	649	328		321
		20800150	TRENCH BACKFILL	CU YD	48	48		
		21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1372	744		628
		25000400	NITROGEN FERTILIZER NUTRIENT	POUND	16	8		8
		25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	16	8		8
		25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	16	8		8
		25200100	SODDING	SQ YD	1313	685		628
		25200200	SUPPLEMENTAL WATERING	UNIT	19	10		9
		28000510	INLET FILTERS	EACH	145	88		57
		31101180	SUBBASE GRANULAR MATERIAL, TYPE B 2"	SQ YD	1201	648		553
		31101600	SUBBASE GRANULAR MATERIAL, TYPE B 8"	SQ YD	685	290		395
		35101500	AGGREGATE BASE COURSE, TYPE B	CU YD	559	292		267
		40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	440	400		40
		40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	22150	11845		10305
		40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	18	10		8
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	543	353		190
		40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	5428	2895		2533
		42001300	PROTECTIVE COAT	SQ YD	2511	1248		1263
		44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	48467	25848		22619
		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	603	225		378
		44000600	SIDEWALK REMOVAL	SQ FT	10977	6420		4557
		44201785	CLASS D PATCHES, TYPE I, 12 INCH	SQ YD	503	263		240
		44201789	CLASS D PATCHES, TYPE II, 12 INCH	SQ YD	1173	613		560
		44201794	CLASS D PATCHES, TYPE III, 12 INCH	SQ YD	1173	613		560

FILE NAME = ...\3824_Sumquant_01.dgn
$MODELNAME$
USER NAME = djk
PLOT SCALE = 22.2222 ' / in.
PLOT DATE = 3/12/2016

DESIGNED - JAT | REVISED -
DRAWN - JAT | REVISED -
CHECKED - DJK | REVISED -
DATE - 3/14/16 | REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
SUMMARY OF QUANTITIES

SCALE: | SHEET 1 OF 4 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3339/1331	15-00062-00-RS	COOK	54	4

CONTRACT NO. 61C79
ILLINOIS FED. AID PROJECT M-4003(683)

SPECIAL PROVISION	SPECIALTY ITEM	CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
						0005	0031	0005
						ROADWAY RESURFACING	NON PARTICIPATING	ROADWAY RESURFACING
						BIESTERFIELD ROAD		OAKTON STREET
		44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SQ YD	530	290		240
		44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	59000	28000		31000
		45100100	CRACK ROUTING (PAVEMENT)	FOOT	46000	24000		22000
		55100500	STORM SEWER REMOVAL 12"	FOOT	200	200		
		60108104	PIPE UNDERDRAINS, TYPE 1, 4"	FOOT	150	150		
		60201105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	4	4		
		60207905	CATCH BASINS, TYPE C, TYPE 11 FRAME AND GRATE	EACH	1	1		
		60236800	INLETS, TYPE A, TYPE 11 FRAME AND GRATE	EACH	2	2		
		60266600	VALVE BOXES TO BE ADJUSTED	EACH	10	7		3
		60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	9	3		6
		60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	4	2		2
		60404800	FRAMES AND GRATES, TYPE 11	EACH	4			4
		60500050	REMOVING CATCH BASINS	EACH	4	4		
		60500060	REMOVING INLETS	EACH	2	2		
		67100100	MOBILIZATION	LSUM	1	0.5		0.5
		70106800	CHANGEABLE MESSAGE SIGN	CAL MO	12	6		6
		70300100	SHORT TERM PAVEMENT MARKING	FOOT	14414	7426		6988
		70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	7407	3549		3858
	X	72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	27	26		1
	X	72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	4	4		
	X	72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	10	10		
	X	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	572	400		172
	X	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	10458	5604		4854
	X	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2878	1829		1049
	X	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	255	118		137
	X	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	363	125		238
	X	78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	50	50		
	X	78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	600	600		

FILE NAME = ...\3224_Sumquant_21.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BIESTERFIELD ROAD & OAKTON STREET RESURFACING SUMMARY OF QUANTITIES
		DRAWN - JAT	REVISED -		
	PLOT SCALE = 20.0000 '/ in.	CHECKED - DJK	REVISED -		
#MODELNAME#	PLOT DATE = 3/16/2016	DATE - 3/14/16	REVISED -		SCALE: SHEET 2 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	5
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

SPECIAL PROVISION	SPECIALTY ITEM	CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 0005 ROADWAY RESURFACING BIESTERFIELD ROAD	CONSTRUCTION CODE 0031 NON PARTICIPATING	CONSTRUCTION CODE 0005 ROADWAY RESURFACING OAKTON STREET
	X	78001150	PAINT PAVEMENT MARKING - LINE 12"	FOOT	74	74		
	X	78001180	PAINT PAVEMENT MARKING - LINE 24"	FOOT	73	73		
X	X	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	5			5
X	X	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	3	2		1
	X	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	39			39
X	X	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	21			21
X	X	87800100	CONCRETE FOUNDATION, TYPE A	FOOT	4			4
	X	87900200	DRILL EXISTING HANDHOLE	EACH	1			1
X	X	88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8			8
X	X	88600100	DETECTOR LOOP, TYPE I	FOOT	876	300		576
X	X	88600600	DETECTOR LOOP REPLACEMENT	FOOT	587	468		119
X	X	88800100	PEDESTRIAN PUSH-BUTTON	EACH	8			8
	X	89502200	MODIFY EXISTING CONTROLLER	EACH	1			1
X	X	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1			1
X		K1005481	SHREDDED BARK MULCH 3"	SQ YD	59	59		
X		Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	3362	1785		1577
X		Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	77	41		36
X		Z0017700	DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED	EACH	15	7		8
X		Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	403	89		314
X		Z0056604	STORM SEWER (WATER MAIN REQUIREMENTS) 8 INCH	FOOT	121	121		
X		Z0056608	STORM SEWER (WATER MAIN REQUIREMENTS) 12 INCH	FOOT	200	200		
X	X	X0327018	DECORATIVE SIGN POST	EACH	31		31	
X		X2130010	EXPLORATION TRENCH, SPECIAL	FOOT	200	100		100
X		X4230800	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH, SPECIAL	SQ YD	563	185		378
X		X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	6444	5581		863
X		X4240460	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH, SPECIAL	SQ FT	4312	360		3952
X		X4240800	DETECTABLE WARNINGS (SPECIAL)	SQ FT	702	382		320
X		X4400220	CURB REMOVAL AND REPLACEMENT	FOOT	52	52		

SPECIAL PROVISION	SPECIALTY ITEM	CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
						0005	0031	0005
						ROADWAY RESURFACING	NON PARTICIPATING	ROADWAY RESURFACING
						BIESTERFIELD ROAD		OAKTON STREET
X		X4405030	LONGITUDINAL PARTIAL DEPTH REMOVAL 3"	FOOT	2850	1850		1000
X		X4420900	LONGITUDINAL PARTIAL DEPTH PATCHING	TON	106	69		37
X		X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	15	9		6
X		X6040205	FRAMES AND LIDS, SPECIAL	EACH	2	1		1
X		X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	LSUM	1	0.5		0.5
X	X	X7200105	SIGN PANEL - TYPE 1 (SPECIAL)	SQ FT	223		223	
X	X	X8760055	PEDESTRIAN PUSH-BUTTON POST, TYPE A	EACH	1			1
X	X	XX008864	INSTALL SIGN	EACH	31		31	

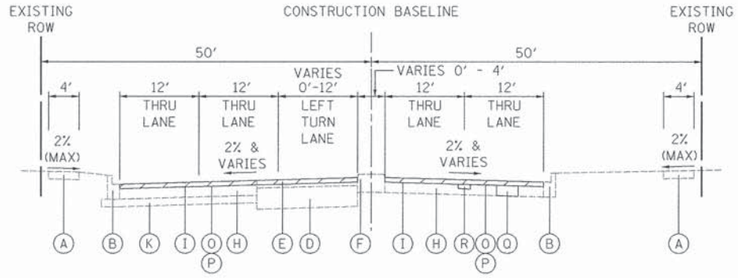
FILE NAME = ...\13224_Sumquant_01.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 22.2222 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/16/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
SUMMARY OF QUANTITIES

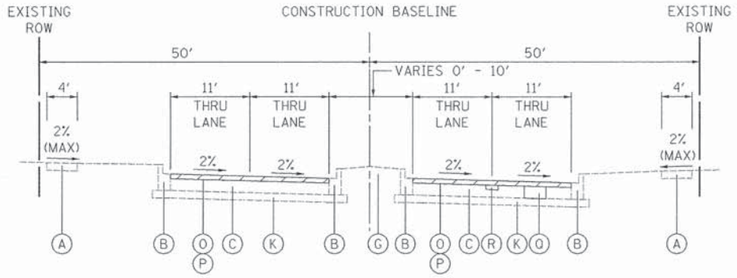
SCALE: | SHEET 4 OF 4 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	7
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			



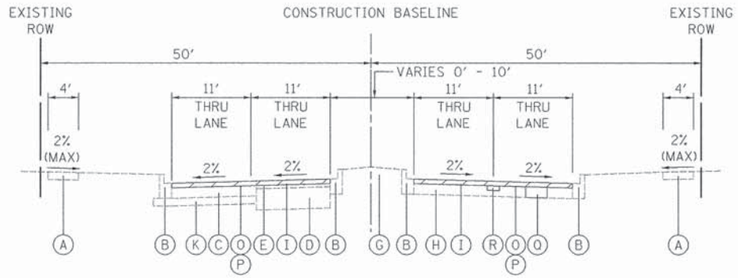
TYPICAL SECTION

BIESTERFIELD ROAD
STA. 10+70.8 TO STA. 14+14.7



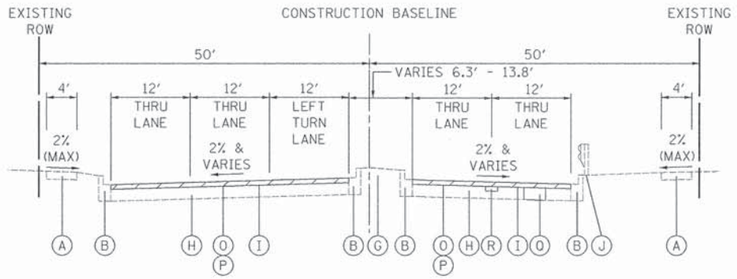
TYPICAL SECTION

BIESTERFIELD ROAD
STA. 41+87.0 TO STA. 44+32.0



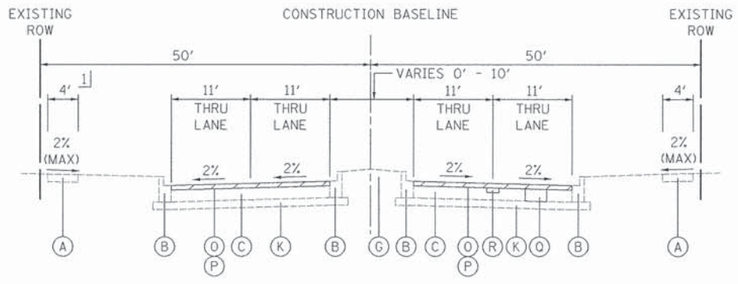
TYPICAL SECTION

BIESTERFIELD ROAD
STA. 14+14.7 TO STA. 22+00.0



TYPICAL SECTION

BIESTERFIELD ROAD
STA. 50+68.0 TO STA. 52+22.1



1 10' - STA. 22+00 TO STA. 34+00

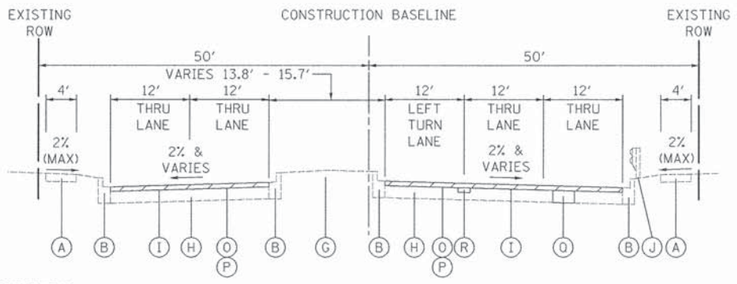
TYPICAL SECTION

BIESTERFIELD ROAD
STA. 22+00.0 TO STA. 41+87.0
STA. 44+32.0 TO STA. 50+68.0

LEGEND

- (A) EXISTING CONCRETE SIDEWALK
- (B) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (C) EXISTING HOT-MIX ASPHALT, 12" & VARIES
- (D) EXISTING AGGREGATE BASE COURSE, 14"
- (E) EXISTING HOT-MIX ASPHALT, 5¼"
- (F) EXISTING CORRUGATED MEDIAN
- (G) EXISTING LANDSCAPED MEDIAN
- (H) EXISTING HOT-MIX ASPHALT, 11"
- (I) EXISTING AREA REFLECTIVE CRACK CONTROL TREATMENT
- (J) EXISTING STEEL PLATE BEAM GUARDRAIL
- (K) EXISTING AGGREGATE BASE COURSE, VARIES 4"-6"
- (L) EXISTING HOT-MIX ASPHALT, 4"
- (M) EXISTING HOT-MIX ASPHALT, 9"
- (N) EXISTING PORTLAND CEMENT CONCRETE, 10"
- (O) PROPOSED STRIP REFLECTIVE CRACK CONTROL (LOCATION TO BE DETERMINED IN THE FIELD BY ENGINEER)
- (P) PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- (Q) PROPOSED CLASS D PATCH, 12" (LOCATION TO BE DETERMINED IN THE FIELD BY ENGINEER)
- (R) PROPOSED LONGITUDINAL PARTIAL DEPTH PATCH (3") (LOCATION TO BE DETERMINED IN THE FIELD BY ENGINEER)

HOT-MIX ASPHALT SURFACE REMOVAL, 2"



TYPICAL SECTION

BIESTERFIELD ROAD
STA. 52+22.1 TO STA. 54+83.3

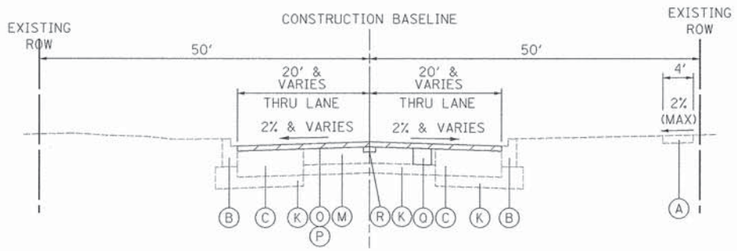
FILE NAME = ...\03-Typicals\3824_Typ_01.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DJK	REVISED -
©MODELNAME©	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
TYPICAL SECTIONS

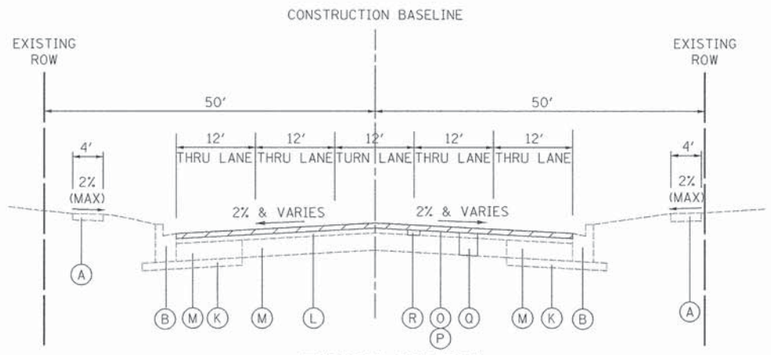
SCALE: N.T.S. | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	8
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			



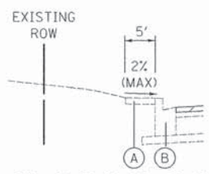
TYPICAL SECTION

OAKTON STREET
STA. 31+08.5 TO STA. 51+89.5

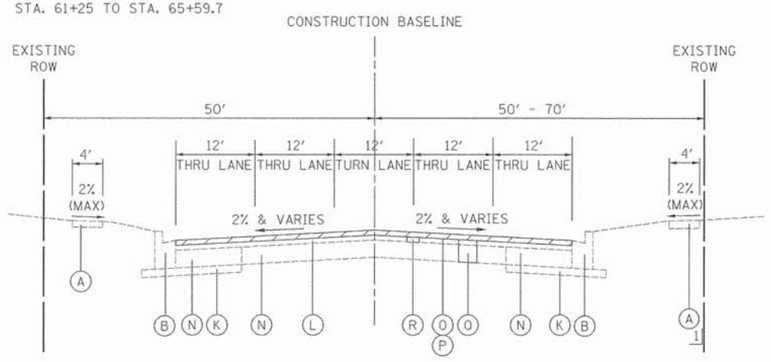


TYPICAL SECTION

OAKTON STREET
STATION 51+89.5 TO STATION 56+37.0



STA. 61+25 TO STA. 65+59.7



TYPICAL SECTION

1] STA. 56+37.0 TO STA. 59+07.4

OAKTON STREET
STATION 56+37.0 TO STATION 65+59.7

LEGEND

- (A) EXISTING CONCRETE SIDEWALK
- (B) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (C) EXISTING HOT-MIX ASPHALT, 12" & VARIES
- (D) EXISTING AGGREGATE BASE COURSE, 14"
- (E) EXISTING HOT-MIX ASPHALT, 5 1/4"
- (F) EXISTING CORRUGATED MEDIAN
- (G) EXISTING LANDSCAPED MEDIAN
- (H) EXISTING HOT-MIX ASPHALT, 11"
- (I) EXISTING AREA REFLECTIVE CRACK CONTROL TREATMENT
- (J) EXISTING STEEL PLATE BEAM GUARDRAIL
- (K) EXISTING AGGREGATE BASE COURSE, VARIES 4"-6"
- (L) EXISTING HOT-MIX ASPHALT, 4"
- (M) EXISTING HOT-MIX ASPHALT, 9"
- (N) EXISTING PORTLAND CEMENT CONCRETE, 10"
- (O) PROPOSED STRIP REFLECTIVE CRACK CONTROL (LOCATION TO BE DETERMINED IN THE FIELD BY ENGINEER)
- (P) PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2"
- (Q) PROPOSED CLASS D PATCH, 12" (LOCATION TO BE DETERMINED IN THE FIELD BY ENGINEER)
- (R) PROPOSED LONGITUDINAL PARTIAL DEPTH PATCH (3") (LOCATION TO BE DETERMINED IN THE FIELD BY ENGINEER)

HOT-MIX ASPHALT SURFACE REMOVAL, 2"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
PROPOSED RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm); 2"	4% @ 70 GYR.
LONGITUDINAL PARTIAL DEPTH PATCHING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm); 3"	4% @ 70 GYR.
CLASS D PATCH, 12"	
CLASS D PATCH (HMA BINDER IL-19MM); 12" (3 LIFTS)	4% @ 70 GYR.

NOTES:
1. THE UNIT WEIGHT TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LB/SY-IN.

2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

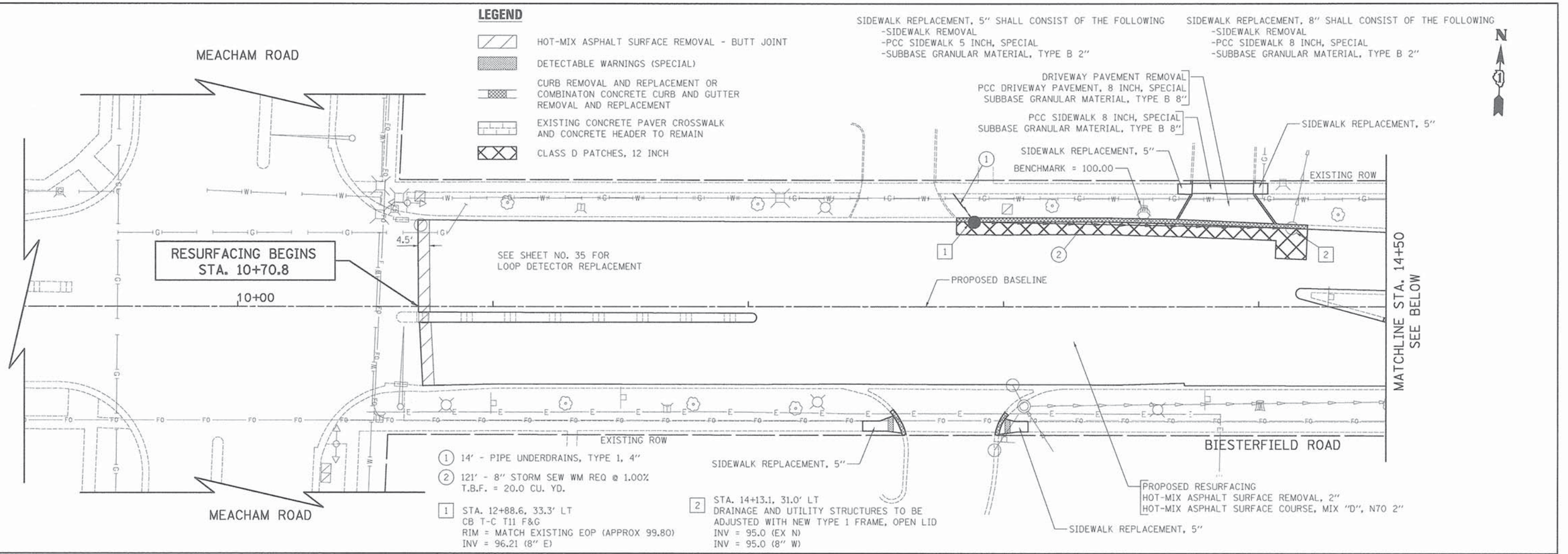
FILE NAME = ...\03-Typicals\3204_Typ_21.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
TYPICAL SECTIONS

SCALE: NTS | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	9
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			



FILE NAME = ...\3024_Plan_01.dgn	USER NAME = d.jk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/16/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
RESURFACING PLAN

SCALE: 1" = 20' | SHEET 1 OF 11 SHEETS | STA. 10+00 TO STA. 19+50

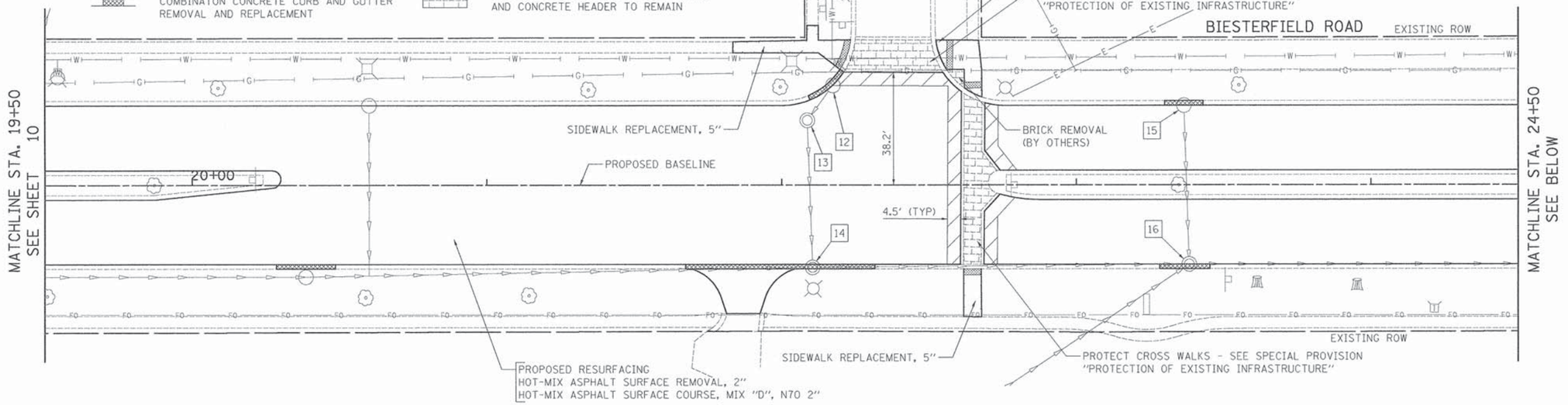
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	10
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- DETECTABLE WARNINGS (SPECIAL)
- CURB REMOVAL AND REPLACEMENT OR COMBINATON CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- CLASS D PATCHES, 12 INCH
- EXISTING CONCRETE PAVER CROSSWALK AND CONCRETE HEADER TO REMAIN

BENCHMARK (NW FLANGE BOLT) = 716.28

12 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
13 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
14 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
15 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
16 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED



(3) 8' - STORM SEWER REMOVAL, 12"
8' - 12" S.S., WM REQ @ 16%
TBF = 1.1 CU. YD.

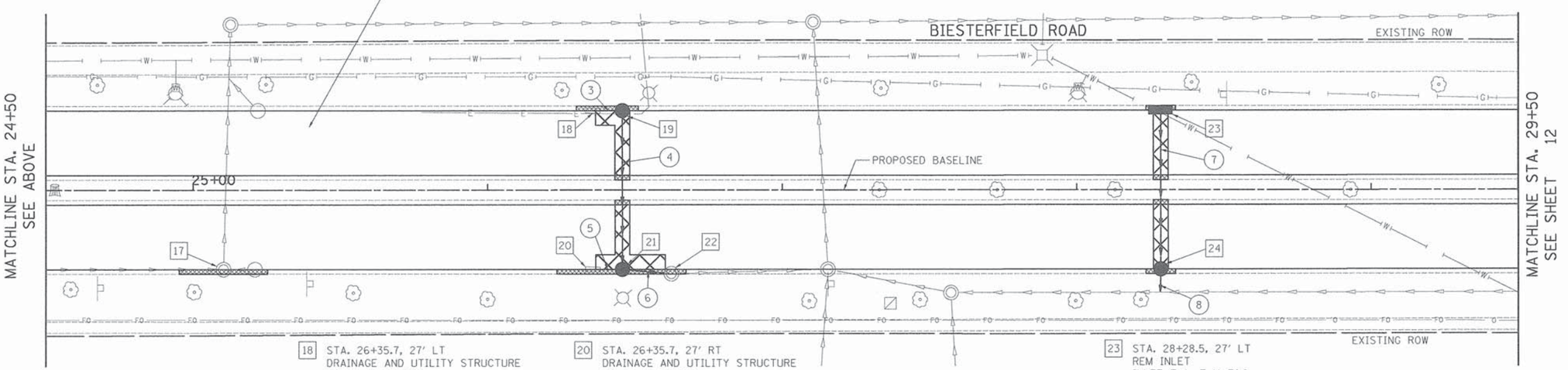
(4) 51' - STORM SEWER REMOVAL, 12"
51' - 12" S.S., WM REQ @ 0.50%
TBF = 6.8 CU. YD.

(5) 8' - STORM SEWER REMOVAL, 12"
8' - 12" S.S., WM REQ @ 17%
TBF = 1.1 CU. YD.

(6) 14' - STORM SEWER REMOVAL, 12"
14' - 12" S.S., WM REQ @ 0.50%
TBF = 2.2 CU. YD.

(7) 52' - STORM SEWER REMOVAL, 12"
52' - 12" S.S., WM REQ @ 0.50%
TBF = 7.4 CU. YD.

(8) 3' - STORM SEWER REMOVAL, 12"
3' - 12" S.S., WM REQ @ 0.50%
(CONNECT TO EXISTING STORM SEWER)
TBF = 0.6 CU. YD.



17 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

18 STA. 26+35.7, 27' LT
DRAINAGE AND UTILITY STRUCTURE TO BE ADJUSTED
INV = 710.33 (12" E) (EX)

19 STA. 26+45.7, 27' LT
REM CB
CB T-A, 4'-DIA., T-11 F&G
RIM = 711.98
INV = 709.06 (12" S&W)
INV = 709.88 (4" SE)

20 STA. 26+35.7, 27' RT
DRAINAGE AND UTILITY STRUCTURE TO BE ADJUSTED
INV = 710.36 (12" E) (EX)

21 STA. 26+45.7, 27' RT
REM CB
CB T-A, 4'-DIA., T-11 F&G
RIM = 711.97
INV = 708.80 (12" E&N)
INV = 708.97 (12" W)

22 STA. 26+62.3, 28.5' RT
DRAINAGE AND UTILITY STRUCTURE TO BE RECONSTRUCTED
INV = 708.73 (E) (EX)
INV = 708.98 (W) (EX)
INV = 708.73 (12" W) (PR)

23 STA. 28+28.5, 27' LT
REM INLET
INLET T-A, T-11 F&G
RIM = 713.72
INV = 710.72 (12" S)

24 STA. 28+28.5, 27' RT
REM CB
CB T-A, 4'-DIA., T-11 F&G
RIM = 713.73
INV = 710.47 (12" N&S)

0 20 40
SCALE IN FEET

FILE NAME = ...\3024_Plan_22.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
#MODELNAME#	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

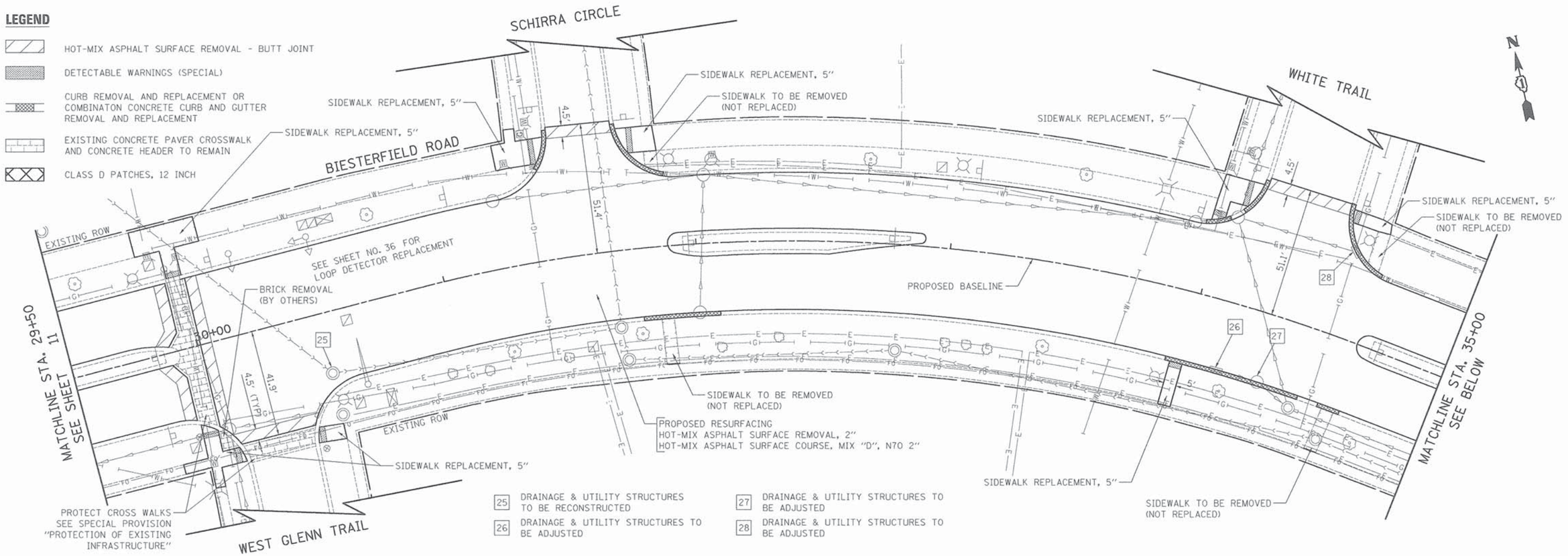
BIESTERFIELD ROAD & OAKTON STREET RESURFACING
RESURFACING PLAN

SCALE: 1" = 20' | SHEET 2 OF 11 SHEETS | STA. 19+50 TO STA. 29+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1338	15-00062-00-RS	COOK	54	11
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- DETECTABLE WARNINGS (SPECIAL)
- CURB REMOVAL AND REPLACEMENT OR COMBINATON CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- EXISTING CONCRETE PAVER CROSSWALK AND CONCRETE HEADER TO REMAIN
- CLASS D PATCHES, 12 INCH



25 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED

26 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

27 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

28 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

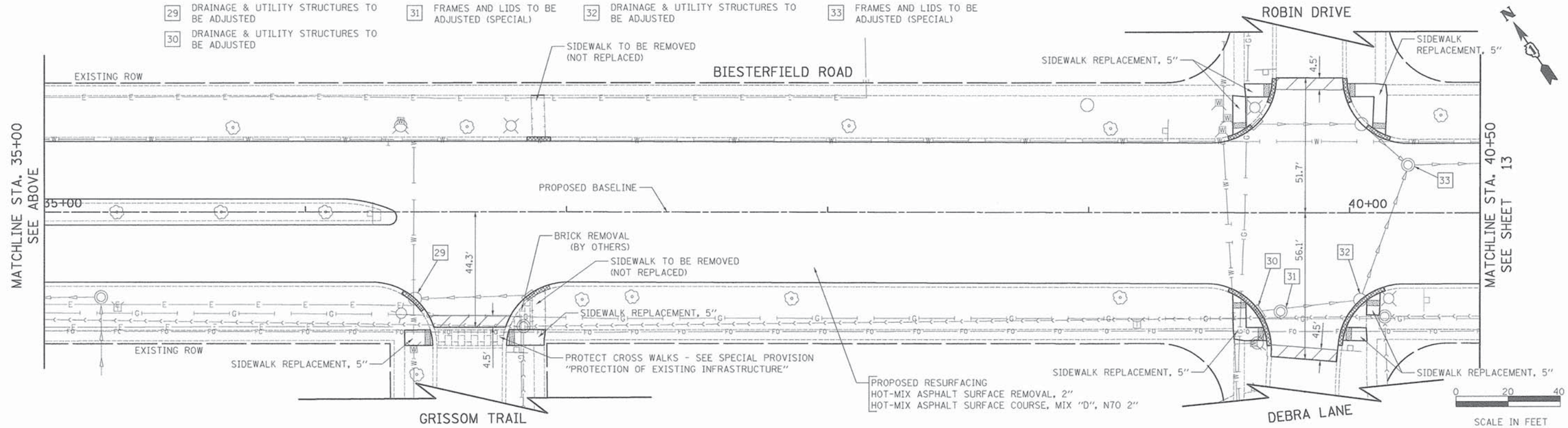
29 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

30 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

31 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

32 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

33 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)



FILE NAME = ...\3004_Plan_23.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
RESURFACING PLAN

SCALE: 1" = 20' | SHEET 3 OF 11 SHEETS | STA. 29+50 TO STA. 40+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1339	15-00062-00-RS	COOK	54	12
		CONTRACT NO. 61C79		
		ILLINOIS FED. AID PROJECT M-4003(683)		

LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- DETECTABLE WARNINGS (SPECIAL)
- CURB REMOVAL AND REPLACEMENT OR COMBINATON CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- EXISTING CONCRETE PAVER CROSSWALK AND CONCRETE HEADER TO REMAIN
- CLASS D PATCHES, 12 INCH

34 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

35 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

36 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

37 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

38 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

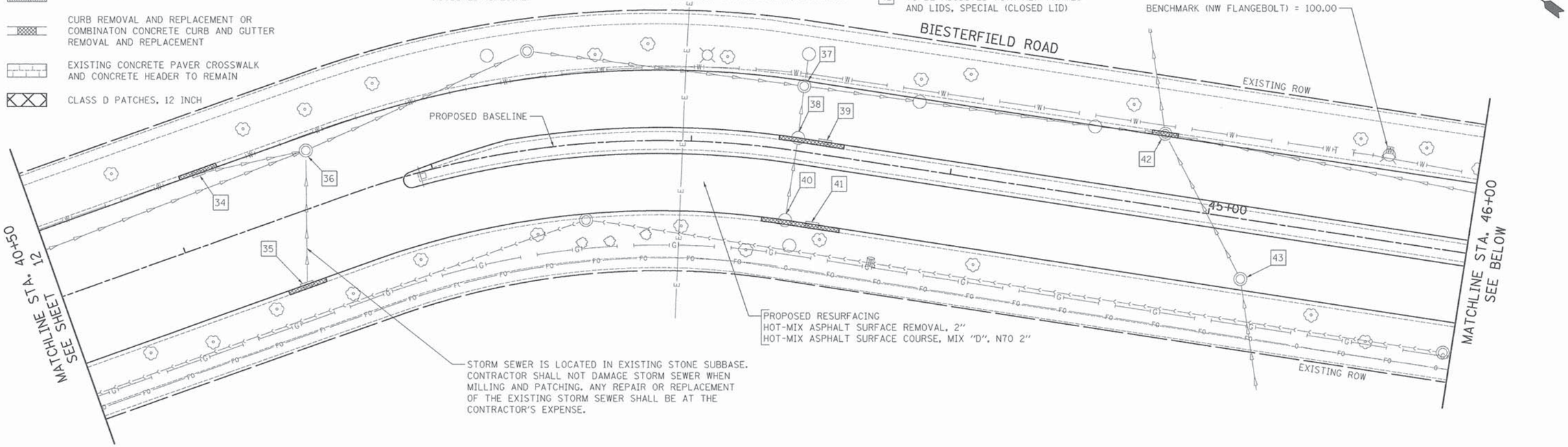
39 DRAINAGE STRUCTURE TO BE ADJUSTED, WITH NEW TYPE 1 FRAME, OPEN LID

40 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

41 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

42 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW FRAMES AND LIDS, SPECIAL (CLOSED LID)

43 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED



44 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED
INV = 94.66 (12" NW&SW) (EX)

45 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

46 STA. 46+82.0, 27' RT
REMOVE INLET
INLET T-A, T-11 F&G
RIM = 97.76
INV = 94.91 (12" SE)
INV = 96.06 (4" NE)

47 STA. 46+97.1, 27' RT
REMOVE CB
CB T-A, 4'-DIA., T-11 F&G
RIM = 97.84
INV = 94.84 (12" NE&NW)
INV = 95.29 (8" S)

48 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED

49 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

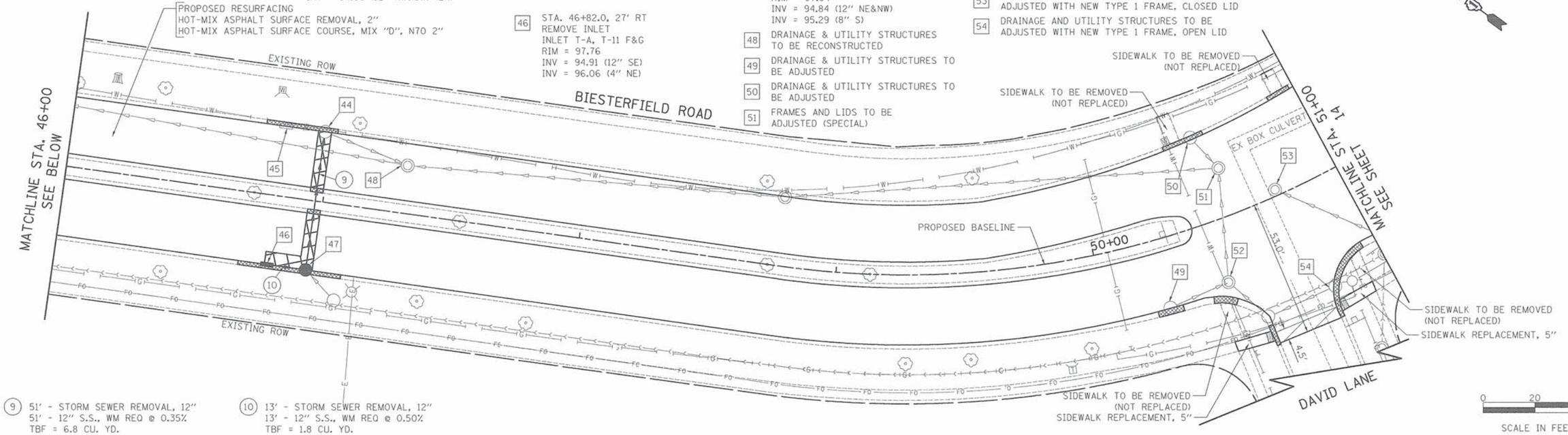
50 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

51 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

52 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

53 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID

54 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID



(9) 51' - STORM SEWER REMOVAL, 12"
51' - 12" S.S., WM REQ @ 0.35%
TBF = 6.8 CU. YD.

(10) 13' - STORM SEWER REMOVAL, 12"
13' - 12" S.S., WM REQ @ 0.50%
TBF = 1.8 CU. YD.

FILE NAME = ...\3204_Plan_04.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
RESURFACING PLAN

SCALE: 1" = 20' | SHEET 4 OF 11 SHEETS | STA. 40+50 | TO STA. 51+00

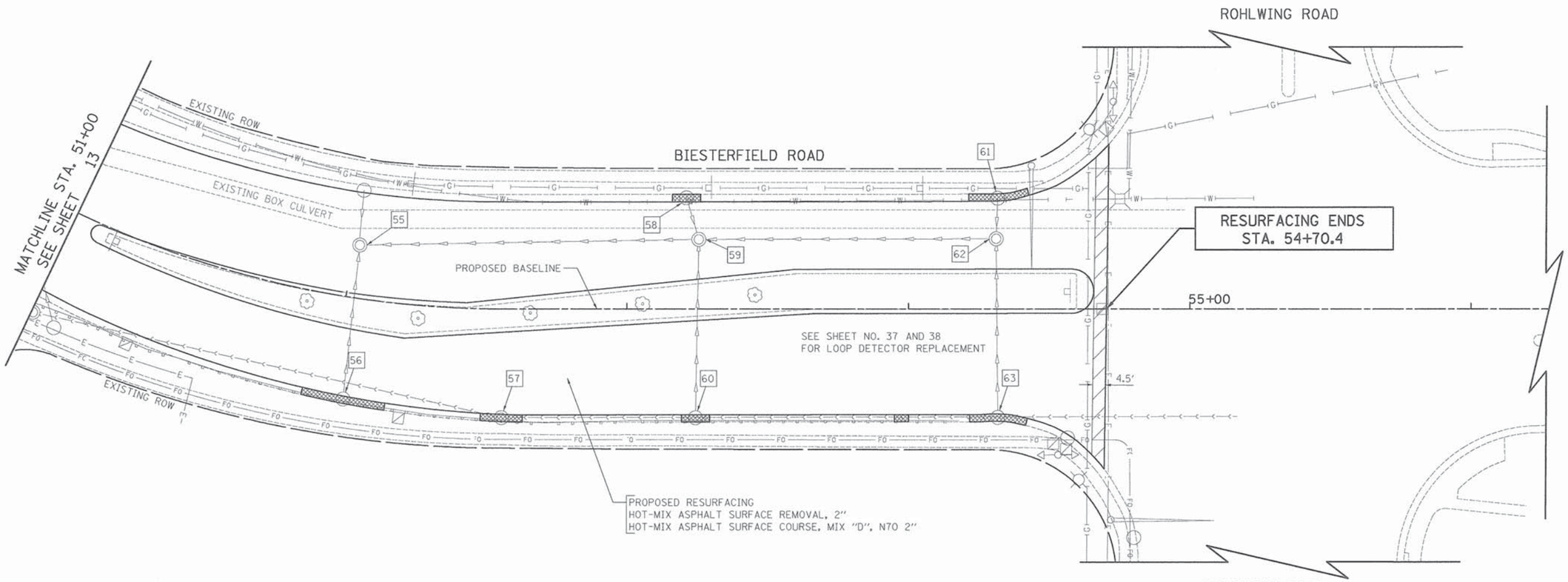
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	13
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- DETECTABLE WARNINGS (SPECIAL)
- CURB REMOVAL AND REPLACEMENT OR COMBINATON CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- EXISTING CONCRETE PAVER CROSSWALK AND CONCRETE HEADER TO REMAIN
- CLASS D PATCHES, 12 INCH

55 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED
56 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
57 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
58 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
59 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED
60 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
61 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
62 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID
63 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

N



0 20 40
SCALE IN FEET

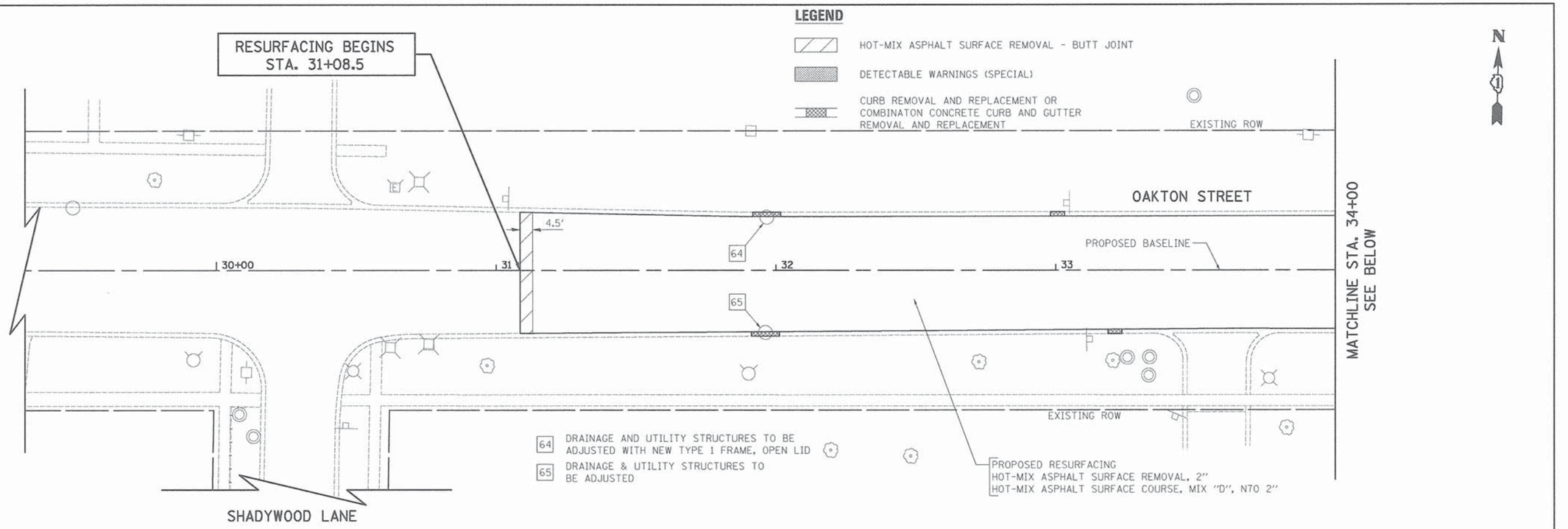
FILE NAME = ...\3824_Plan_25.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
⊕MODELNAME⊕	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
RESURFACING PLAN

SCALE: 1" = 20' | SHEET 5 OF 11 SHEETS | STA. 51+00 | TO STA. 55+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	14
		CONTRACT NO. 61C79		
		ILLINOIS	FED. AID PROJECT M-4003(683)	



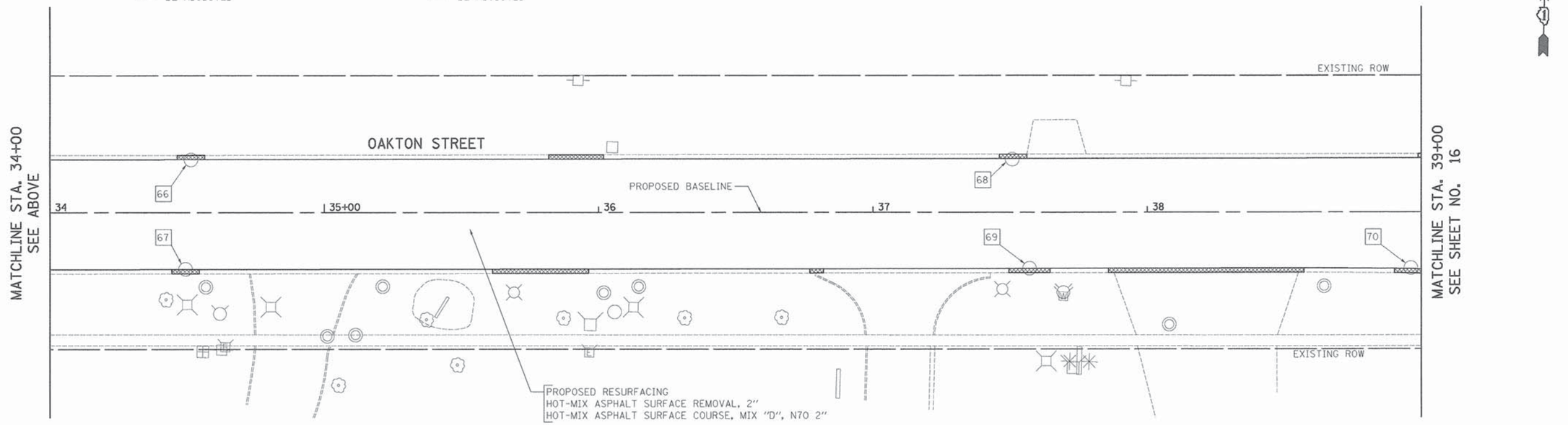
66 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID

67 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

68 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID

69 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

70 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED



FILE NAME = ...\3224_Plan_21.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BIESTERFIELD ROAD & OAKTON STREET RESURFACING RESURFACING PLAN
		DRAWN - JAT	REVISED -		
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -		SCALE: 1" = 20' SHEET 6 OF 11 SHEETS STA. 31+10 TO STA. 39+00
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -		

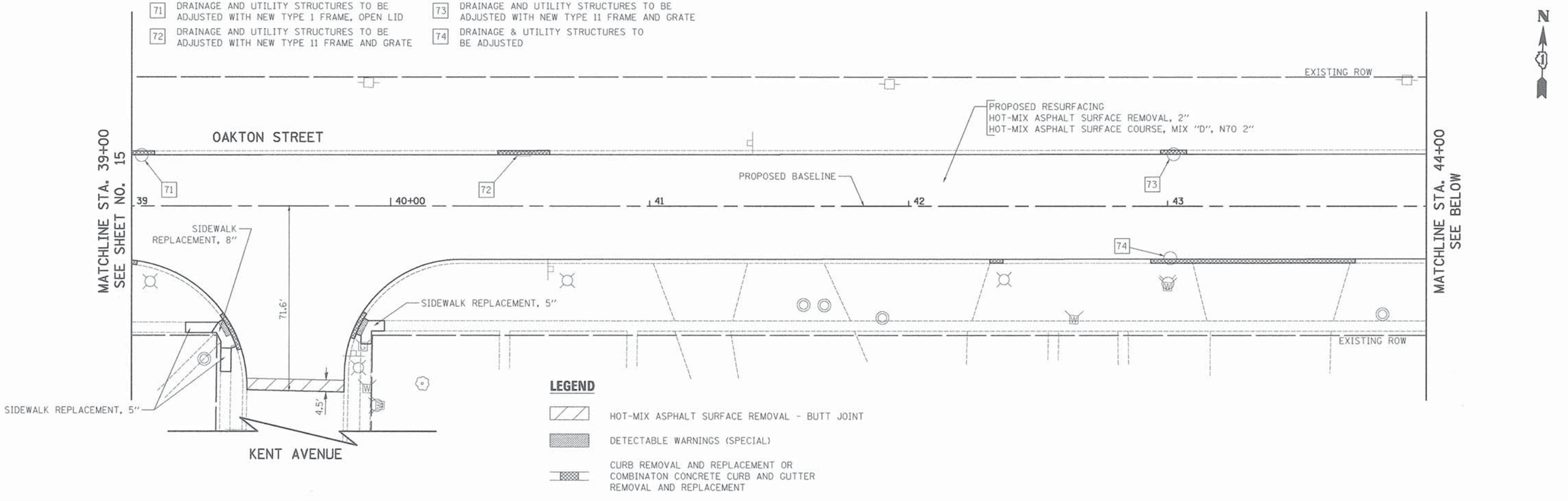
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	15
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

71 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID

72 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE

73 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE

74 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED



LEGEND

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

DETECTABLE WARNINGS (SPECIAL)

CURB REMOVAL AND REPLACEMENT OR COMBINATON CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

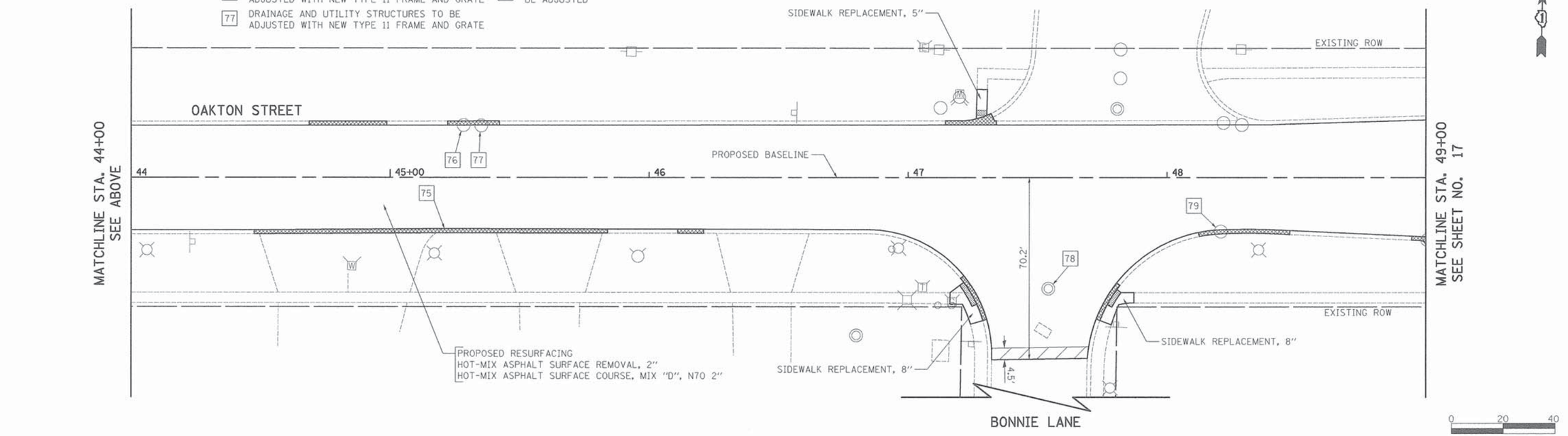
75 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

76 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE

77 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE

78 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

79 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED



FILE NAME = ...\3224_Plan_02.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
RESURFACING PLAN

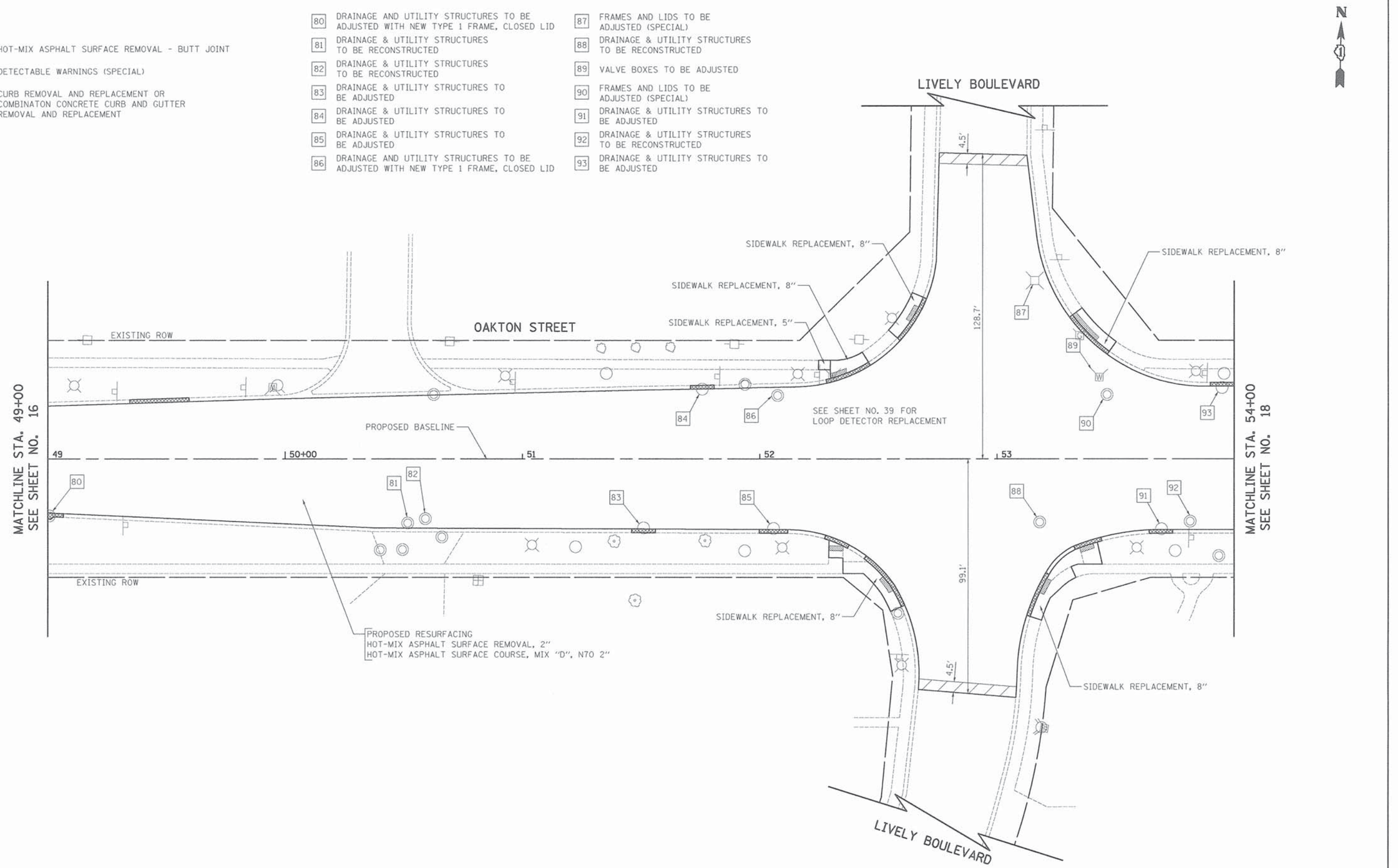
SCALE: 1" = 20' | SHEET 7 OF 11 SHEETS | STA. 39+00 TO STA. 49+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1338	15-00062-00-RS	COOK	54	16
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- DETECTABLE WARNINGS (SPECIAL)
- CURB REMOVAL AND REPLACEMENT OR COMBINATON CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

80 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID
81 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED
82 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED
83 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
84 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
85 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
86 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID
87 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
88 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED
89 VALVE BOXES TO BE ADJUSTED
90 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
91 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
92 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED
93 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED



FILE NAME = ...\3024_Plan_23.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
RESURFACING PLAN

SCALE: 1" = 20' | SHEET 8 OF 11 SHEETS | STA. 49+00 | TO STA. 54+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	17
		CONTRACT NO. 61C79		
		ILLINOIS FED. AID PROJECT M-4003(683)		

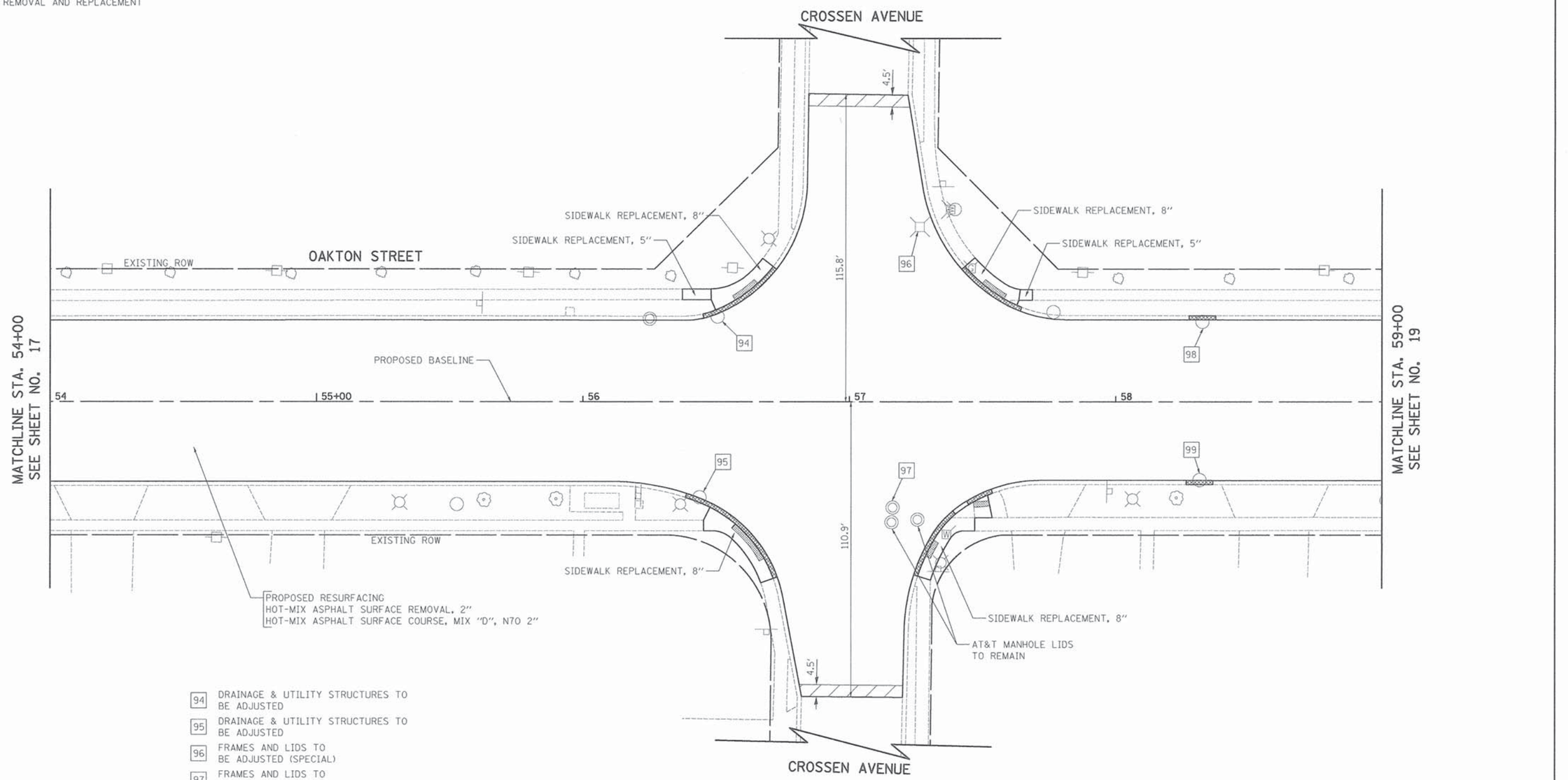
LEGEND

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

DETECTABLE WARNINGS (SPECIAL)

CURB REMOVAL AND REPLACEMENT OR
COMBINATON CONCRETE CURB AND GUTTER
REMOVAL AND REPLACEMENT

N



94 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

95 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

96 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

97 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

98 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

99 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

0 20 40

SCALE IN FEET

FILE NAME = ...\3284_Plan_04.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
RESURFACING PLAN

SCALE: 1" = 20' | SHEET 9 OF 11 SHEETS | STA. 54+00 | TO STA. 59+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1339	15-00062-00-RS	COOK	54	18

CONTRACT NO. 61C79

ILLINOIS | FED. AID PROJECT M-4003(683)

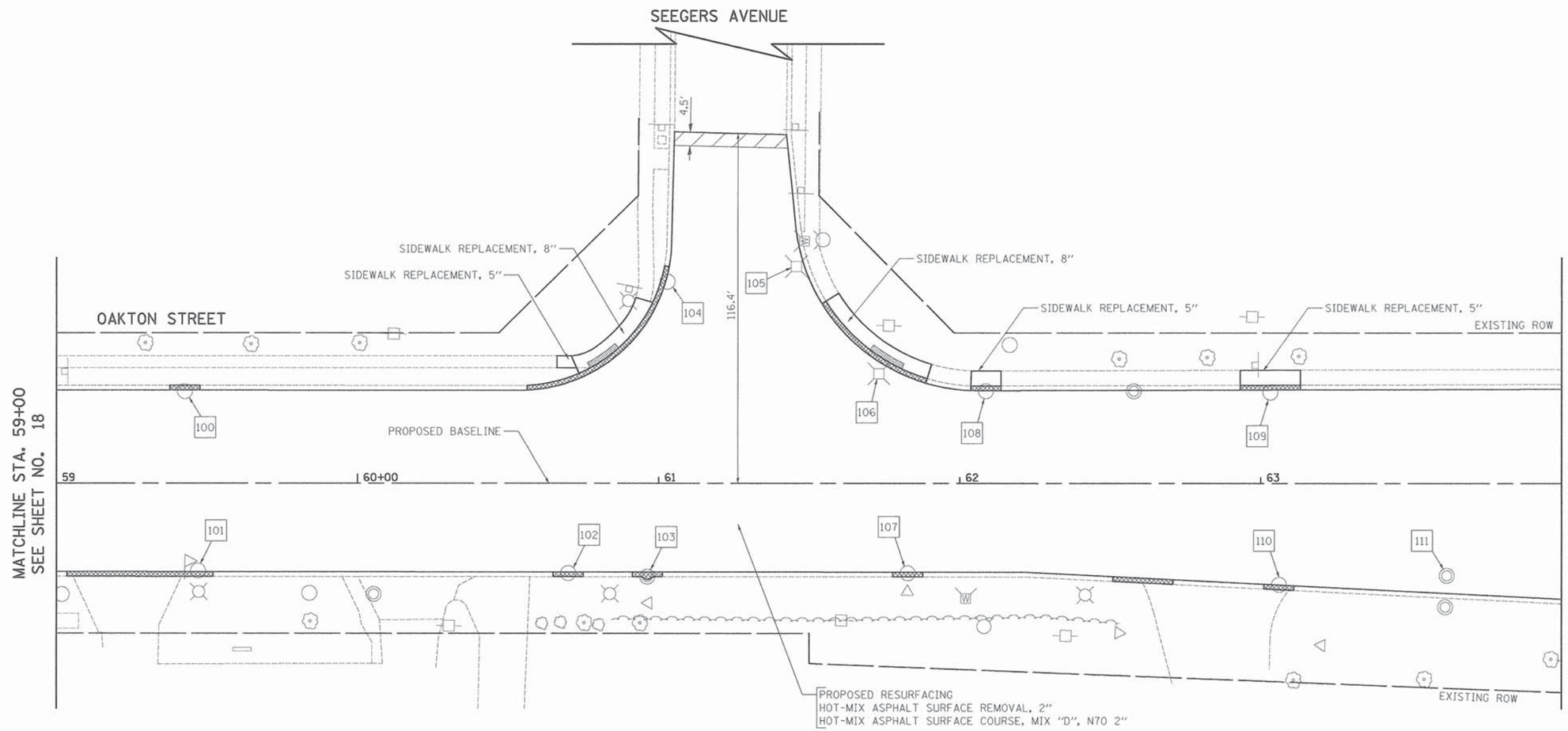
LEGEND

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

DETECTABLE WARNINGS (SPECIAL)

CURB REMOVAL AND REPLACEMENT OR COMBINATON CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

N



100 DRAINAGE AND UTILITY STRUCTURES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, OPEN LID

101 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

102 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

103 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

104 DRAINAGE AND UTILITY STRUCTURES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, OPEN LID

105 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED

106 DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED

107 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

108 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

109 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

110 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

111 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

0 20 40

SCALE IN FEET

FILE NAME = ...\3284_Plan_25.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

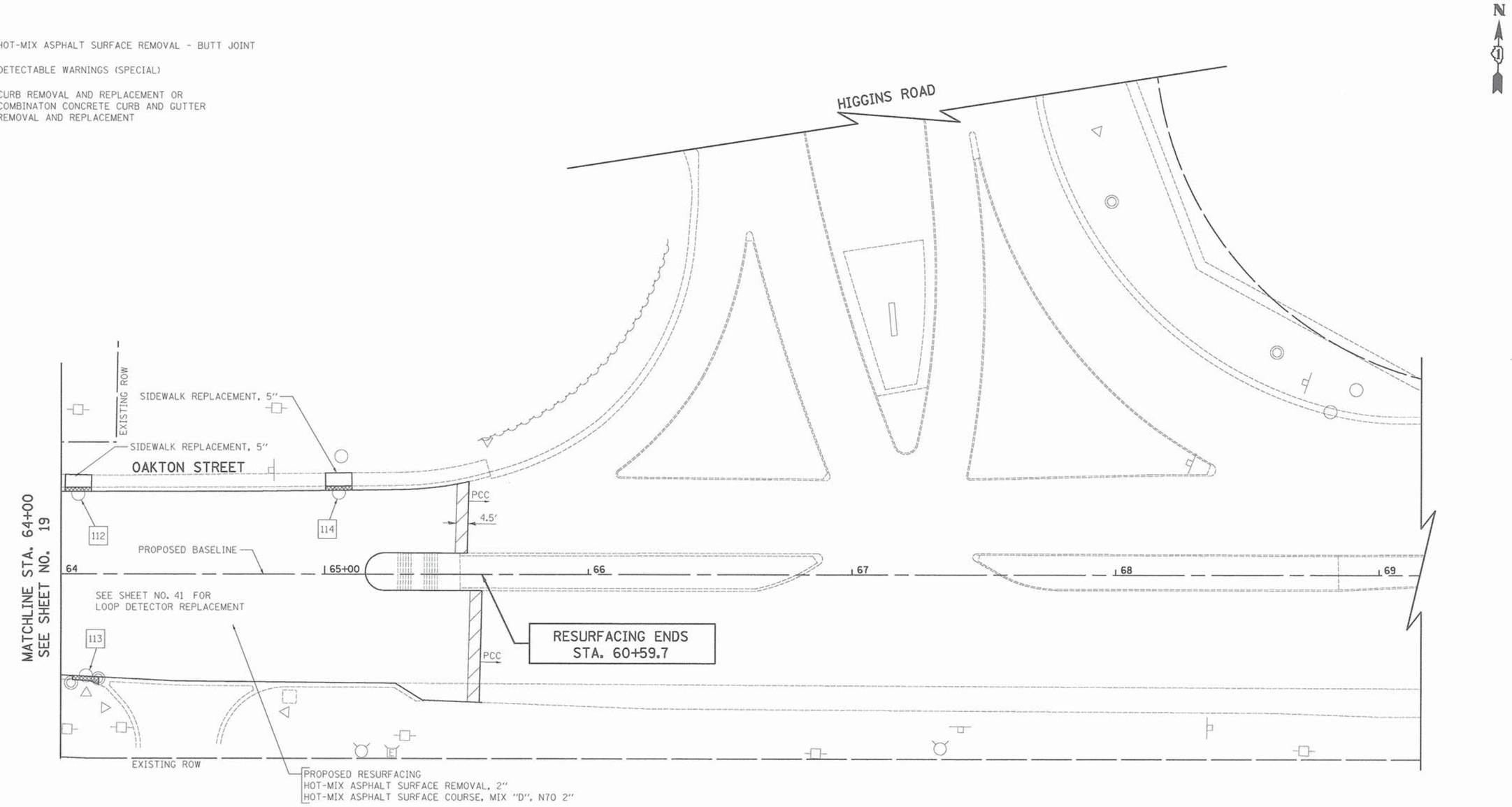
BIESTERFIELD ROAD & OAKTON STREET RESURFACING
RESURFACING PLAN

SCALE: 1" = 20' | SHEET 10 OF 11 SHEETS | STA. 59+00 | TO STA. 64+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	19
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- DETECTABLE WARNINGS (SPECIAL)
- CURB REMOVAL AND REPLACEMENT OR COMBINATON CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT



112 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

113 DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED WITH FRAMES AND LIDS, SPECIAL (OPEN LID)

114 DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED

0 20 40

SCALE IN FEET

FILE NAME = ...\3284_Plan_26.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
RESURFACING PLAN

SCALE: 1" = 20' | SHEET 11 OF 11 SHEETS | STA. 64+00 | TO STA. 65+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	20
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)-705-4470 A MINIMUM OF 72 HOURS IN ADVAMCE OF BEGINNING WORK.

2. TRAFFIC CONTROL DEPICTED IN THESE PLANS AND THE APPLICABLE IDOT DETAILS AND STANDARDS ARE THE MINIMUM REQUIREMENTS. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, DIVISION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL, UNLESS HEREIN REVISED.

3. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

4. ALL CONSTRUCTION WARNING SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS.

6. ALL SIGNS SHALL BE MOUNTED ON METAL POSTS, 7 FEET ABOVE THE EXISTING GROUND AND DRIVEN A MINIMUM OF 3 FEET INTO THE GROUND, UNLESS OTHERWISE NOTED. A J.U.L.I.E. LOCATE SHALL BE PERFORMED PRIOR TO THE INSTALLATION OF THE POSTS.

6. DRUMS WILL BE REQUIRED ADJACENT TO PAVEMENT EDGES WHERE WIDENING, CURB AND GUTTER OR OVERLAYING WORK IS BEING DONE, AS SPECIFIED IN SECTION 701 OF THE STANDARD SPECIFICATIONS, EXCEPT THAT THE BARRICADES SHALL BE DRUMS, NON-METALLIC WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS. SPACING SHALL BE AS SHOWN ON THE HIGHWAY STANDARDS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOPS OF THE BARRICADES ARE IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 701901.

7. DRUMS EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LAW ENFORCEMENT AGENCIES. BARRICADES SHALL BE PLACED AT 50' CENTERS ALONG TANGENTS, 20' CENTERS ALONG TAPERS, AND 10' CENTERS IN CURVES AND RADII.

8. DRUMS AND BARRICADES SHALL MEET THE REQUIREMENTS OF THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND THE STANDARD SPECIFICATIONS.

9. TYPE III BARRICADES ARE TO BE PLACED IN ACCORDANCE WITH STANDARD 701901 UNLESS AUTHORIZED BY THE ENGINEER TO USE AN ALTERNATE ARRANGEMENT.

10. THE CONTRACTOR SHALL INFORM THE ENGINEER OF ANY STAGE CHANGE AT LEAST TWO WEEKS IN ADVANCE OF THE CHANGE.

11. EXISTING TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE REMOVED OR RELOCATED BY THE CONTRACTOR AFTER THE TRAFFIC CONTROL REQUIREMENTS ARE MET OR AS AUTHORIZED BY THE ENGINEER; ANY SIGNS OR DEVICES LEFT IN PLACE ARE TO BE PROTECTED FROM DAMAGE AND MAINTAINED. ANY DAMAGE CAUSED BY THIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AND AT THE EXPENSE OF THE CONTRACTOR.

12. THE FIRST WARNING SIGNS IN EACH DIRECTION OF TRAVEL SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS DURING HOURS OF DARKNESS. FLAGS ARE OPTIONAL.

13. EXISTING TRAFFIC CONTROL DEVICES ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.

14. EXCEPT FOR APPROVED CLOSURES AS DEPICTED ON THE MAINTENANCE OF TRAFFIC PLANS, ALL ROADS SHALL BE KEPT OPEN TO TRAFFIC DURING THE ENTIRE CONSTRUCTION PERIOD. THE CONTRACTOR MAY CLOSE ONE LANE OF TRAFFIC (DUE TO CONSTRUCTION) ONLY BETWEEN THE HOURS OF 9:00 AM AND 7:00 PM FOR EASTBOUND BIESTERFIELD ROAD AND BETWEEN THE HOURS OF 7:00 AM AND 3:00 PM FOR WESTBOUND BIESTERFIELD ROAD.

15. W21-1 "WORKERS" SIGNS SHALL ONLY BE ERECTED WHEN WORKERS ARE PRESENT. SIGN MUST BE COVERED OR REMOVED WHEN NO WORKERS ARE PRESENT.

16. "FRESH OIL" SIGNS (W21-2-4848) WITH DATE SIGNS SHALL BE ERECTED 48 HOURS PRIOR TO PRIMING. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

17. FLASHING ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES, AND SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

18. THE COST OF SUPPLYING, ERECTING, AND MAINTAINING BARRICADES, DRUMS, WARNING LIGHTS, AND SIGNS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

SIDEWALK MAINTENANCE NOTE

1. THE SIDEWALK ON ONE SIDE OF THE STREET MUST REMAIN OPEN AND ACCESSIBLE AT ALL TIMES. CONSTRUCTION STAGING SHALL BE COORDINATED WITH THE ENGINEER AND CONTRACTOR TO ENSURE ONE SIDEWALK REMAINS OPEN. SIGNING DIRECTING PEDESTRIANS TO THE OPEN SIDEWALK SHALL BE IN ACCORDANCE WITH IDOT HIGHWAY STANDARD 701801-05. THE WORK REQUIRED TO COMPLY WITH THESE REQUIREMENTS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)."

CONSTRUCTION REQUIREMENTS

1. ALL WORK SHALL BE IN ACCORDANCE WITH IDOT'S SAFETY ENGINEERING POLICY MEMORANDUM, SAFETY 4-15, INCLUDING THE REQUIREMENT FOR USE OF TEMPORARY OR MILLED SLOPE EDGES (MIN OF 1:3). THIS MAY REQUIRE ADDITIONAL PASSES OF THE MILLING MACHINE OR THE USE OF A SECONDARY, SMALLER MILLING MACHINE TO CREATE THE REQUIRED EDGE. THE COST TO COMPLY WITH THESE REQUIREMENTS SHALL BE INCLUDED IN THE COST OF "HOT-MIX ASPHALT SURFACE REMOVAL" OF THE THICKNESS SPECIFIED.

2. "UNEVEN LANE" SIGNS (W8-1-4848) SHALL BE PLACED AT THE INTERVALS REQUIRED BY THE ENGINEER WHEN TRAFFIC IS ADJACENT TO THE MILLED SURFACE. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)".

COOK COUNTY NOTES – APPLY TO WORK WITHIN MEACHAM ROAD R.O.W.

1. DURING CONSTRUCTION ALL TRAFFIC SHALL REMAIN OPEN AT ALL TIMES AT THE INTERSECTION OF MEACHAM ROAD AND BIESTERFIELD ROAD BY FOLLOWING APPLICABLE IDOT TRAFFIC CONTROL STANDARDS.

2. ANY SHORT TERM ACTIVITY THAT REQUIRES ENCROACHMENT TO THE LANE OPEN FOR TRAFFIC SHALL BE RESTRICTED TO WITHIN THE HOURS OF 9:00 AM TO 3:00 PM BY FOLLOWING IDOT TRAFFIC CONTROL STANDARDS.

3. THE REINSTALLATION OF PERMANENT PAVEMENT MARKING ALONG EAST APPROACH OF BIESTERFEILD ROAD SHALL BE THE SAME AS EXISTING WITHOUT ANY CHANGE.

CONSTRUCTION SEQUENCE

THIS CONSTRUCTION SEQUENCE WAS DEVELOPED TO MINIMIZE IMPACTS TO PROPERTY OWNERS AND TO PROVIDE AN ADEQUATE METHOD OF INSPECTING THE CONDITION OF THE PAVEMENT BASE AND CURB AND GUTTER. THIS CONSTRUCTION SEQUENCE SHALL BE FOLLOWED UNLESS AN ALTERNATE SEQUENCE IS APPROVED BY THE ENGINEER.

1. SET UP APPLICABLE TRAFFIC CONTROL MEASURES USING IDOT HIGHWAY STANDARDS AND DISTRICT ONE DETAILS PROVIDED IN THE PLANS. DAILY LANE CLOSURES SHALL BE USED FOR ALL WORK DEPICTED IN THESE PLANS. PERMANENT LANE CLOSURES SHALL NOT BE ALLOWED UNLESS OTHERWISE APPROVED BY THE ENGINEER.

2. SET UP EROSION AND SEDIMENT CONTROL MEASURES / TREE PRUNING.

3. CONSTRUCT STORM SEWER STRUCTURES AND LATERALS CROSSING BIESTERFIELD ROAD.

4. REMOVE AND REPLACE CURB AND GUTTER AS DETERMINED BY THE ENGINEER.

5. INSTALL SIDEWALK AND DETECTABLE WARNINGS.

6. LANDSCAPE RESTORATION.

7. REMOVE HOT-MIX ASPHALT PAVEMENT SURFACE.

8. THE ENGINEER SHALL INSPECT THE CONDITION OF THE PAVEMENT AND MARK THE AREAS REQUIRING PAVEMENT PATCHING. UNDER NO CONDITION SHALL THE CONTRACTOR PROCEED WITH THIS WORK WITHOUT PRIOR CONSENT FROM THE ENGINEER.

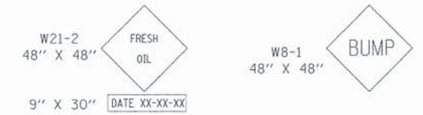
9. INSTALL HMA SURFACE.

10. INSTALL PERMANENT PAVEMENT MARKINGS.

11. REMOVE EROSION CONTROL AND TRAFFIC CONTROL.

12. REMOVE EXISTING SIGNING AND INSTALL NEW SIGNING USING DAILY LANE CLOSURES IN ACCORDANCE WITH THE APPLICABLE IDOT HIGHWAY STANDARDS.

CONSTRUCTION SIGNS



THESE SIGNS SHALL BE PLACED AS DIRECTED BY THE ENGINEER. THE COST SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".

FILE NAME = ...\28-MOT\3204_MOT_Notes.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
MAINTENANCE OF TRAFFIC – GENERAL NOTES
SHEET 1 OF 3 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1338	15-00062-00-RS	COOK	54	21
		CONTRACT NO. 61C79		
	ILLINOIS	FED. AID PROJECT M-4003(683)		

NOTES:

1) THE WORK SHOWN ON THIS SHEET SHALL BE PERFORMED BETWEEN THE HOURS OF 9:00 AM AND 2:00 PM ON WEEKDAYS UNLESS OTHERWISE APPROVED BY THE ENGINEER. TRAFFIC SHALL BE RETURNED TO ITS ORIGINAL CONFIGURATION BY 3:00 PM EACH DAY. THE STORM SEWER TRENCHES SHALL BE RESTORED WITH "AGGREGATE FOR TEMPORARY ACCESS" PRIOR TO RE-OPENING THE LANES. THE PERMANENT HMA PATCH SHALL BE CONSTRUCTED WITHIN 48 HOURS OF THE COMPLETION OF THE STORM SEWER INSTALLATION. A "BUMP" SIGN (W8-1-48) SHALL BE INSTALLED IN ADVANCE OF EACH TRENCH UNTIL THE PATCH IS CONSTRUCTED.

2) ALL "ROAD CONSTRUCTION AHEAD" AND "WORKER AHEAD" SIGNS INSTALLED AT THE BEGINNING OF THE PROJECT SHALL REMAIN THOUGH NOT SHOWN ON THIS PLAN.

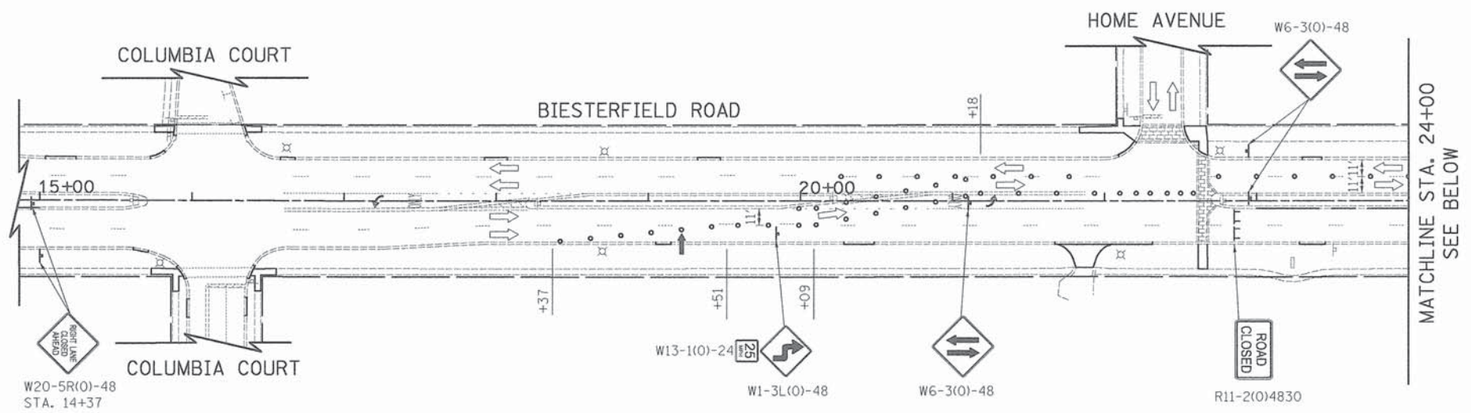
3) THE TRAFFIC SIGNAL AT WEST GLENN TRAIL SHALL BE PLACED IN FLASHING RED MODE WHILE THE LANE ASSIGNMENTS SHOWN ON THIS SHEET ARE IN EFFECT. THE SIGNAL SHALL BE RETURNED TO ITS NORMAL OPERATION WHEN TRAFFIC IS IN ITS NORMAL CONFIGURATION. THIS WORK SHALL BE INCLUDED IN THE COST OF "MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION".

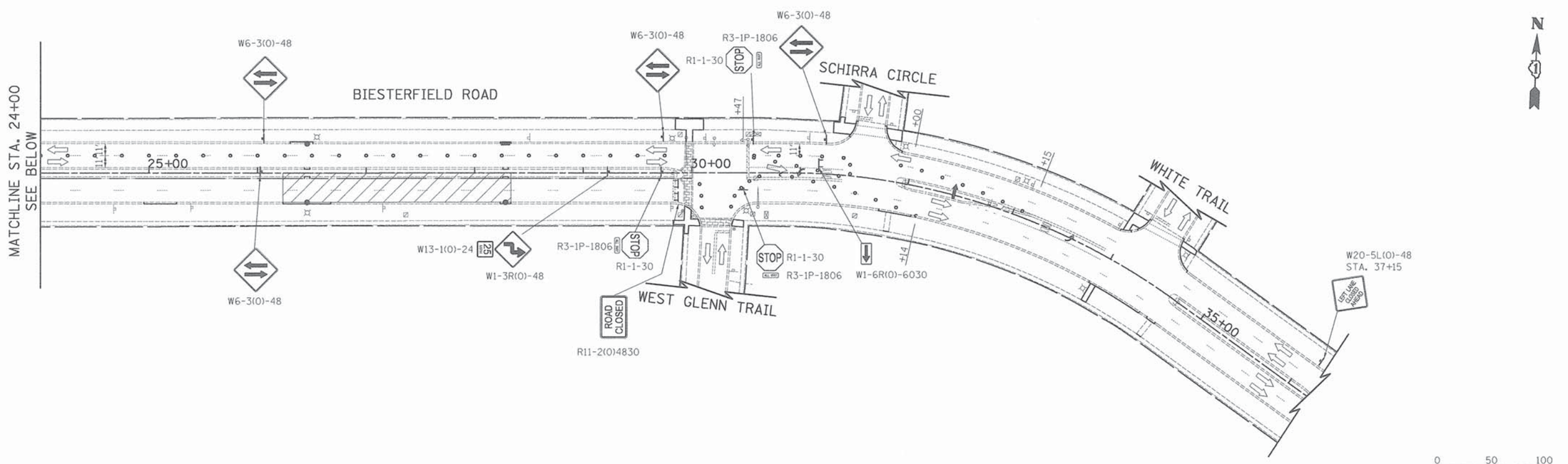
4) FLAGGERS SHALL BE PRESENT AT ALL ENTRANCES AND EXITS TO THE WORK ZONE.

5) ALL SIGNS SHOWN ON THIS SHEET SHALL BE MOUNTED ON TEMPORARY POSTS.

LEGEND

- CONES OR DRUMS (AT 25' C-C (TYP), 10' C-C IN CURVES, RADII, AND INTERSECTIONS AND 20' C-C IN TAPERS)
- DIRECTION OF TRAVEL
- TEMPORARY TRAFFIC SIGN
- TYPE III BARRICADE WITH TWO FLASHING LIGHTS
- FLASHING ARROW BOARD
- WORK ZONE





FILE NAME = ...\08-MOT\3004_MOT_WEST_01.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
MAINTENANCE OF TRAFFIC – STAGE 1 LATERAL CONSTRUCTION

SCALE: 1" = 50' | SHEET 2 OF 3 SHEETS | STA. 10+00 TO STA. 36+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	22
		CONTRACT NO. 61C79		
		ILLINOIS	FED. AID PROJECT M-4003(683)	

NOTES:

1) THE WORK SHOWN ON THIS SHEET SHALL BE PERFORMED BETWEEN THE HOURS OF 9:00 AM AND 2:00 PM ON WEEKDAYS UNLESS OTHERWISE APPROVED BY THE ENGINEER. TRAFFIC SHALL BE RETURNED TO ITS ORIGINAL CONFIGURATION BY 3:00 PM EACH DAY. THE STORM SEWER TRENCHES SHALL BE RESTORED WITH "AGGREGATE FOR TEMPORARY ACCESS" PRIOR TO RE-OPENING THE LANES. THE PERMANENT HMA PATCH SHALL BE CONSTRUCTED WITHIN 48 HOURS OF THE COMPLETION OF THE STORM SEWER INSTALLATION. A "BUMP" SIGN (W8-1-48) SHALL BE INSTALLED IN ADVANCE OF EACH TRENCH UNTIL THE PATCH IS CONSTRUCTED.

2) ALL "ROAD CONSTRUCTION AHEAD" AND "WORKER AHEAD" SIGNS INSTALLED AT THE BEGINNING OF THE PROJECT SHALL REMAIN THOUGH NOT SHOWN ON THIS PLAN.

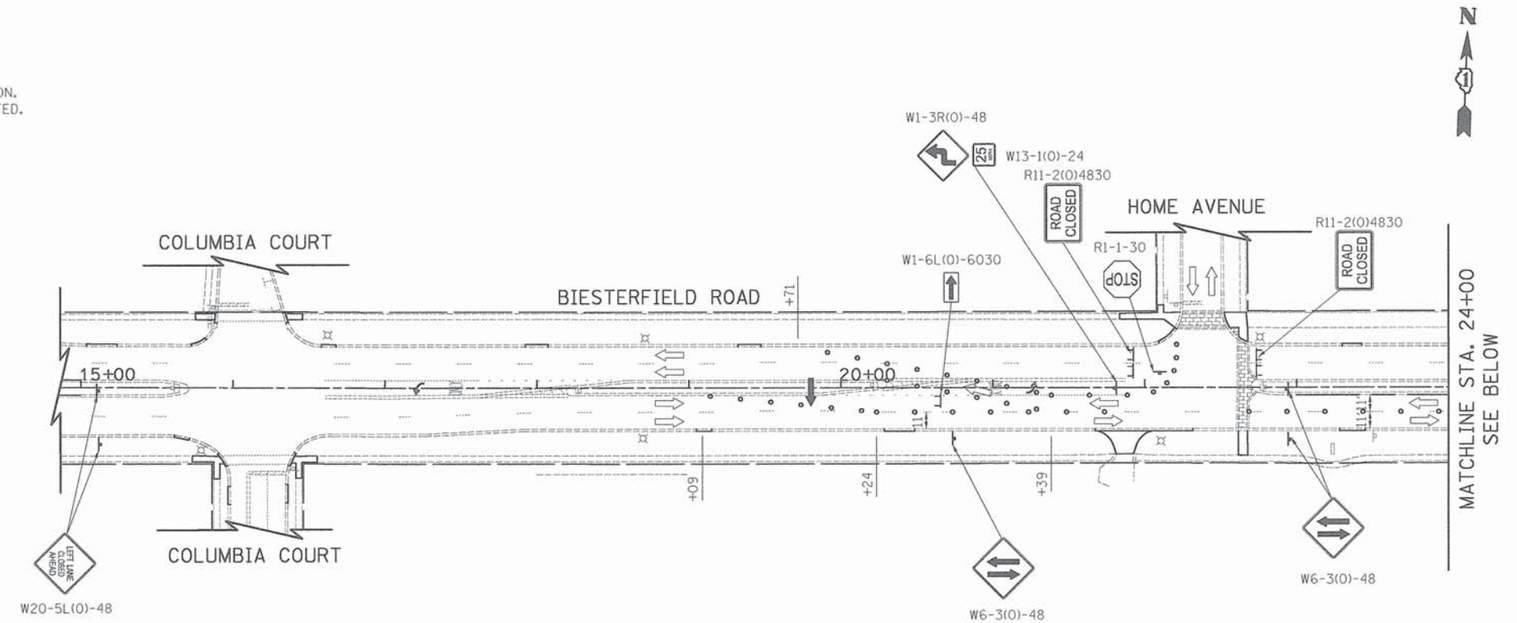
3) THE TRAFFIC SIGNAL AT WEST GLENN TRAIL SHALL BE PLACED IN FLASHING RED MODE WHILE THE LANE ASSIGNMENTS SHOWN ON THIS SHEET ARE IN EFFECT. THE SIGNAL SHALL BE RETURNED TO ITS NORMAL OPERATION WHEN TRAFFIC IS IN ITS NORMAL CONFIGURATION. THIS WORK SHALL BE INCLUDED IN THE COST OF "MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION".

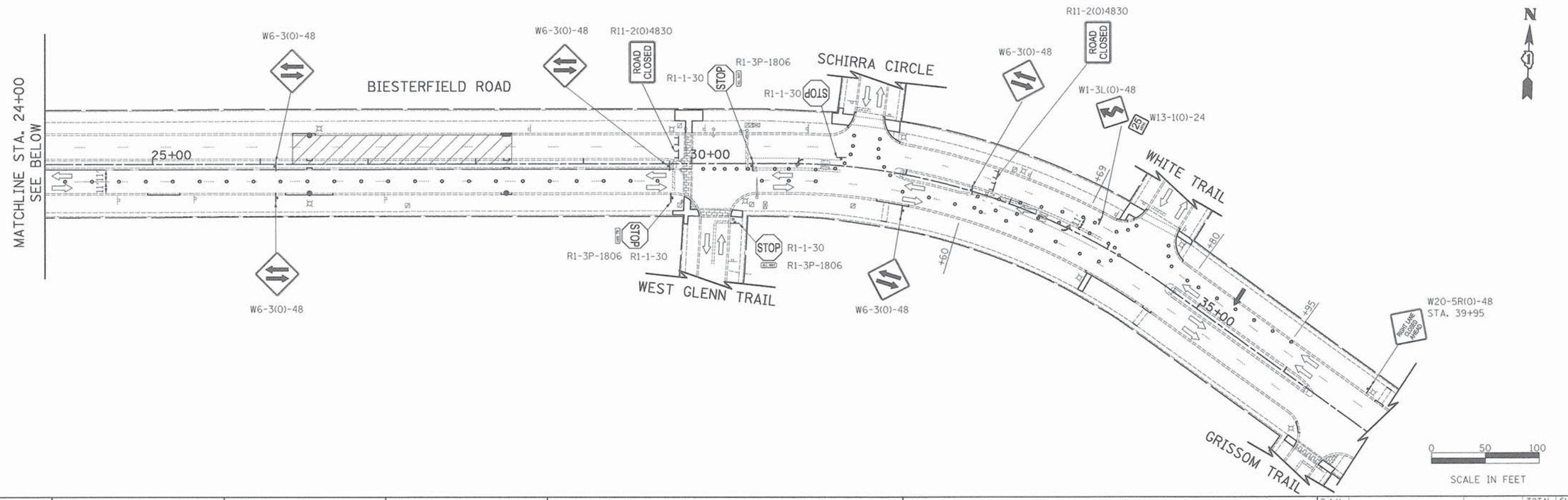
4) FLAGGERS SHALL BE PRESENT AT ALL ENTRANCES AND EXITS TO THE WORK ZONE.

5) ALL SIGNS SHOWN ON THIS SHEET SHALL BE MOUNTED ON TEMPORARY POSTS.

LEGEND

- CONES OR DRUMS (AT 25' C-C (TYP), 10' C-C IN CURVES, RADII, AND INTERSECTIONS AND 20' C-C IN TAPERS)
- DIRECTION OF TRAVEL
- TEMPORARY TRAFFIC SIGN
- TYPE III BARRICADE WITH TWO FLASHING LIGHTS
- FLASHING ARROW BOARD
- WORK ZONE





FILE NAME = ...\28-MOT\3024_MOT_WEST_02.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
MAINTENANCE OF TRAFFIC – STAGE 2 LATERAL CONSTRUCTION

SCALE: 1" = 50' | SHEET 3 OF 3 SHEETS | STA. 10+00 TO STA. 36+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3339/1331	15-00062-00-RS	COOK	54	23
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

LEGEND

REMOVE	REMOVE SIGN PANEL AND POSTS
REM & NEW	REMOVE SIGN PANEL AND POSTS REPLACE WITH NEW SIGN AND POST
REMAIN	SIGN PANEL AND POST TO REMAIN

NOTES:

1) ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.

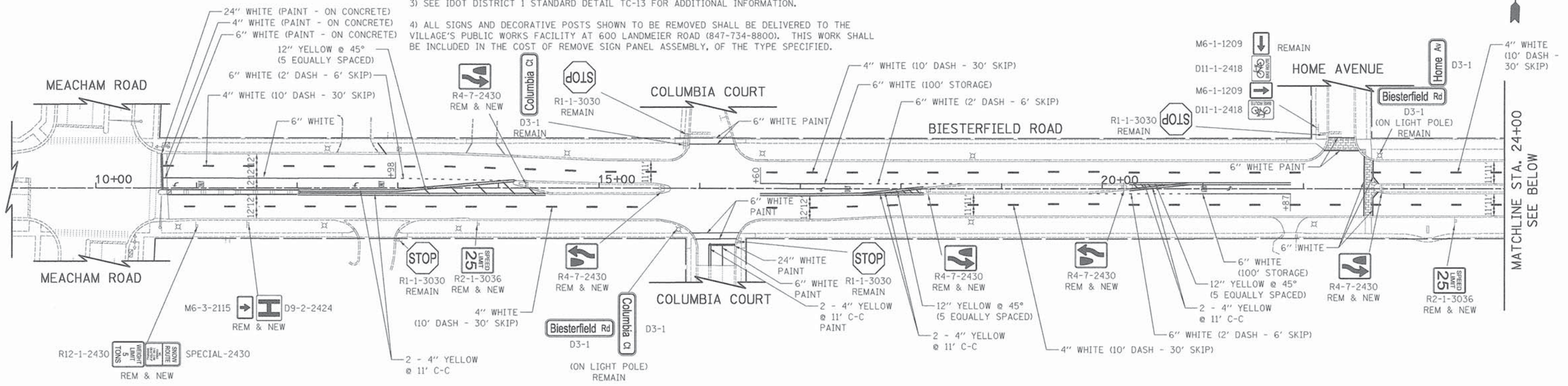
2) DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.

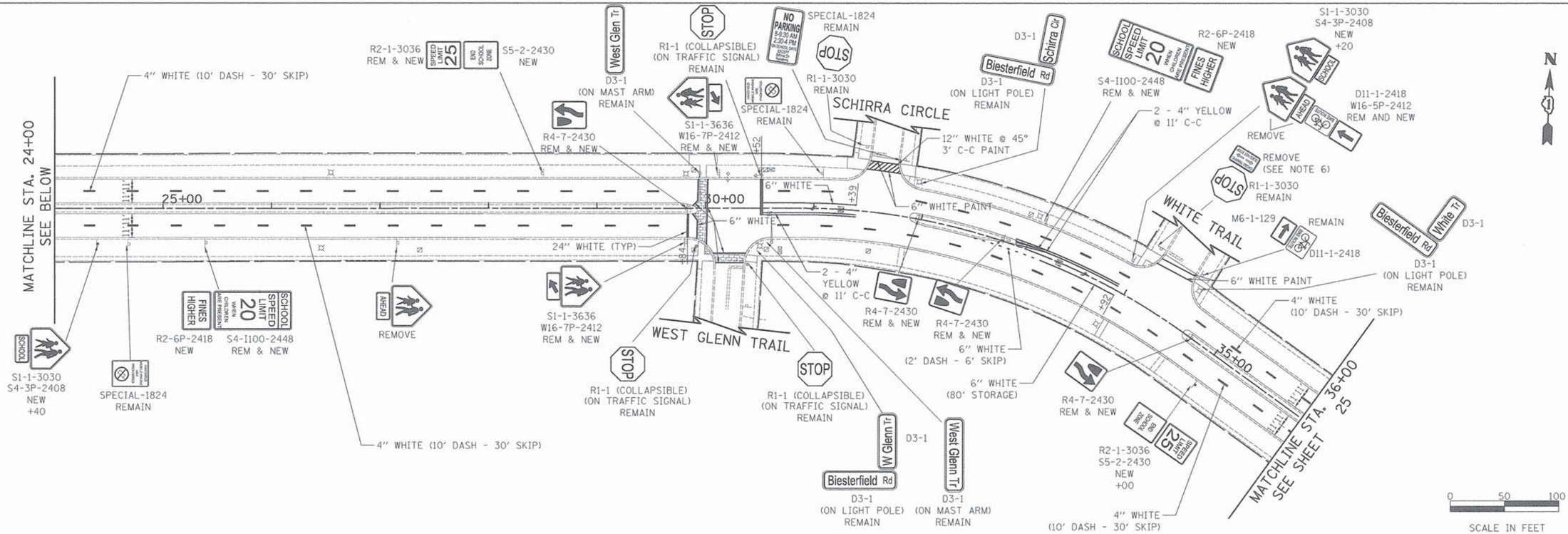
3) SEE IDOT DISTRICT 1 STANDARD DETAIL TC-13 FOR ADDITIONAL INFORMATION.

4) ALL SIGNS AND DECORATIVE POSTS SHOWN TO BE REMOVED SHALL BE DELIVERED TO THE VILLAGE'S PUBLIC WORKS FACILITY AT 600 LANDMEIER ROAD (847-734-8800). THIS WORK SHALL BE INCLUDED IN THE COST OF REMOVE SIGN PANEL ASSEMBLY, OF THE TYPE SPECIFIED.

5) ANY SIGNS OR SUPPORTS SHOWN TO REMAIN THAT ARE DAMAGED BY THE CONTRACTOR SHALL BE REPLACED BY THE CONTRACTOR AT NO EXPENSE TO THE CONTRACT. THIS SHALL INCLUDE REPLACING THE SIGN OR SUPPORT WITH THE SAME DECORATIVE MATERIALS AS CURRENTLY EXISTS.

6) REMOVE SIGN PANEL, FRAME, AND MOUNTING HARDWARE. THE POST SHALL REMAIN. THIS WORK SHALL BE PAID FOR AS "REMOVE SIGN PANEL - TYPE 1."





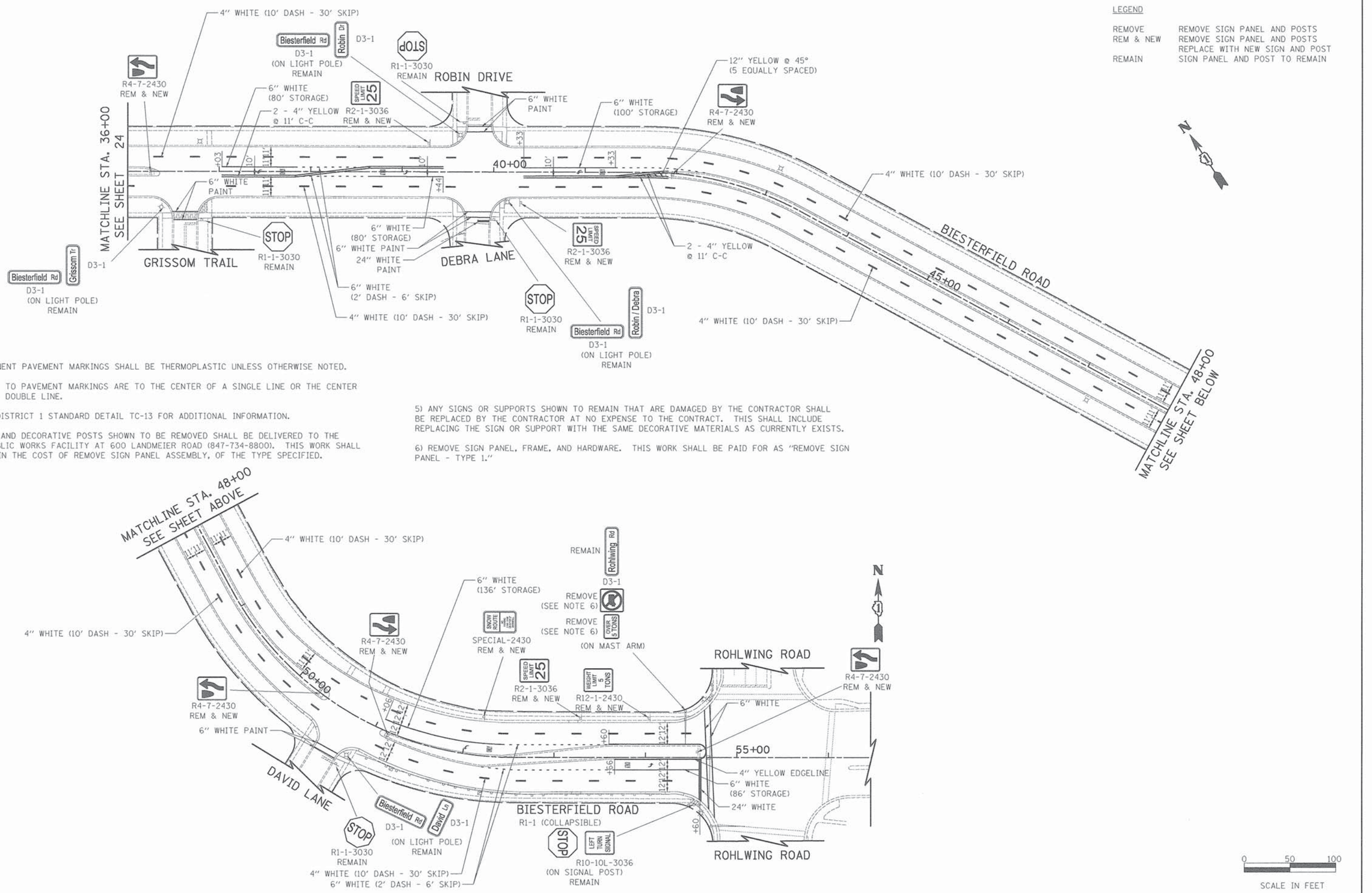
FILE NAME = ...\3824_PMK_01.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/16/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
SIGNING AND STRIPING PLAN

SCALE: 1" = 50' | SHEET 1 OF 4 SHEETS | STA. 10+00 TO STA. 36+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	24
		CONTRACT NO. 61C79		
		ILLINOIS FED. AID PROJECT M-4003(683)		



NOTES:

1) ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.

2) DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.

3) SEE IDOT DISTRICT 1 STANDARD DETAIL TC-13 FOR ADDITIONAL INFORMATION.

4) ALL SIGNS AND DECORATIVE POSTS SHOWN TO BE REMOVED SHALL BE DELIVERED TO THE VILLAGE'S PUBLIC WORKS FACILITY AT 600 LANDMEIER ROAD (847-734-8800). THIS WORK SHALL BE INCLUDED IN THE COST OF REMOVE SIGN PANEL ASSEMBLY, OF THE TYPE SPECIFIED.

5) ANY SIGNS OR SUPPORTS SHOWN TO REMAIN THAT ARE DAMAGED BY THE CONTRACTOR SHALL BE REPLACED BY THE CONTRACTOR AT NO EXPENSE TO THE CONTRACT. THIS SHALL INCLUDE REPLACING THE SIGN OR SUPPORT WITH THE SAME DECORATIVE MATERIALS AS CURRENTLY EXISTS.

6) REMOVE SIGN PANEL, FRAME, AND HARDWARE. THIS WORK SHALL BE PAID FOR AS "REMOVE SIGN PANEL - TYPE 1."

FILE NAME = ...\3284_PMK_02.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/16/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
SIGNING AND STRIPING PLAN

SCALE: 1" = 50' | SHEET 2 OF 4 SHEETS | STA. 36+00 TO STA. 55+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	25
		CONTRACT NO. 61C79		
	ILLINOIS	FED. AID PROJECT M-4003(683)		

LEGEND

REMOVE	REMOVE SIGN PANEL AND POSTS
REM & NEW	REMOVE SIGN PANEL AND POSTS REPLACE WITH NEW SIGN AND POST
REMAIN	SIGN PANEL AND POST TO REMAIN

NOTES:

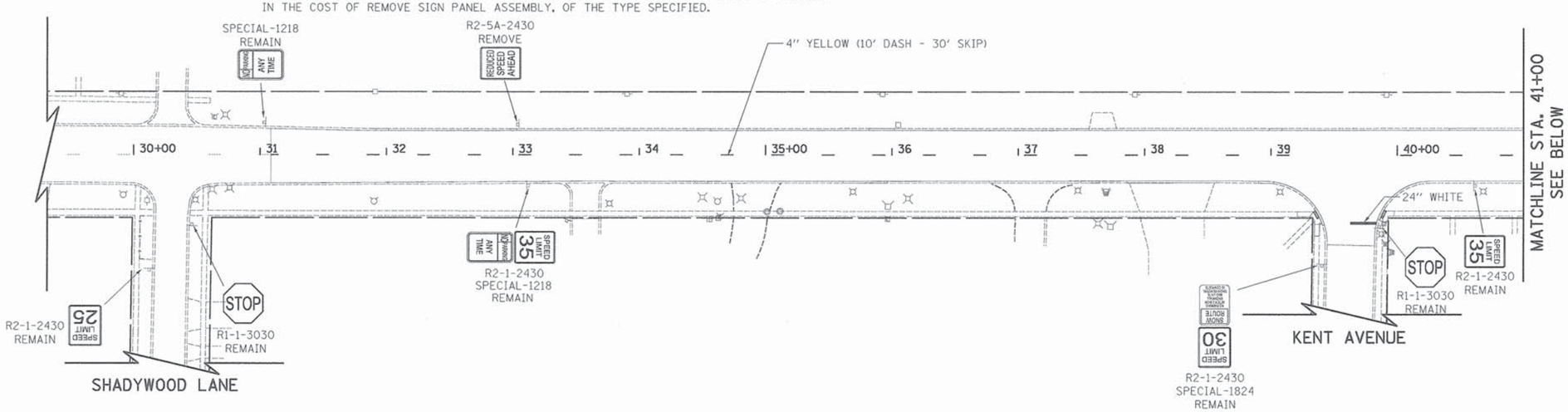
1) ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.

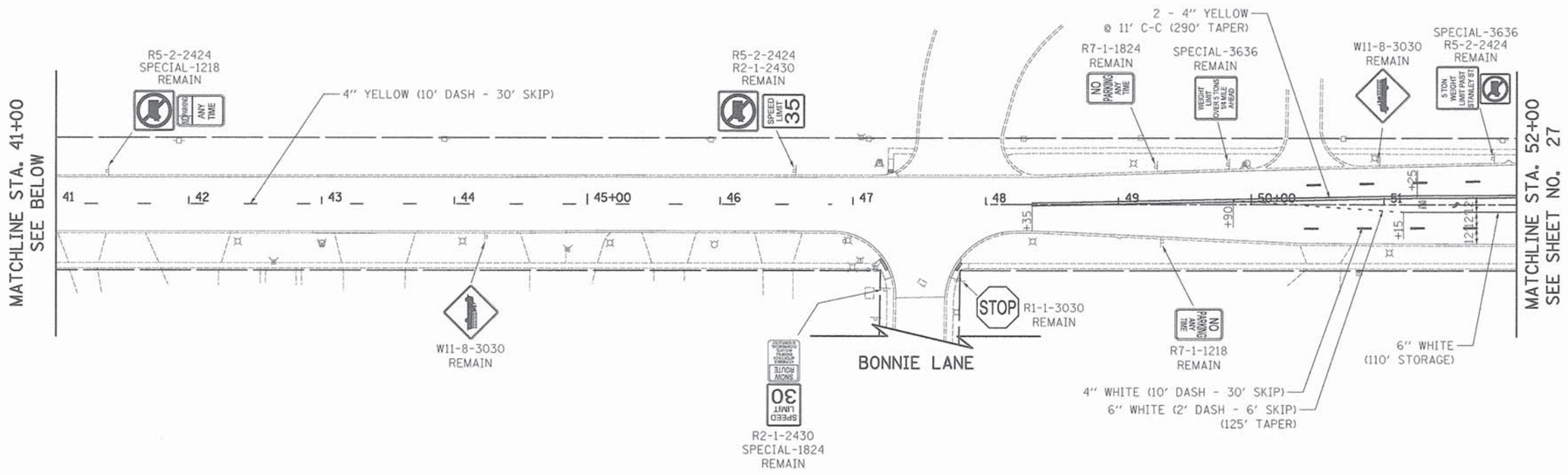
2) DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.

3) SEE IDOT STANDARD DETAIL TC-13 FOR ADDITIONAL INFORMATION.

4) ALL SIGNS AND DECORATIVE POSTS SHOWN TO BE REMOVED SHALL BE DELIVERED TO THE VILLAGE'S PUBLIC WORKS FACILITY AT 600 LANDMEIER ROAD. THIS WORK SHALL BE INCLUDED IN THE COST OF REMOVE SIGN PANEL ASSEMBLY, OF THE TYPE SPECIFIED.

5) ANY SIGNS OR SUPPORTS SHOWN TO REMAIN THAT ARE DAMAGED BY THE CONTRACTOR SHALL BE REPLACED BY THE CONTRACTOR AT NO EXPENSE TO THE CONTRACT. THIS SHALL INCLUDE REPLACING THE SIGN OR SUPPORT WITH THE SAME DECORATIVE MATERIALS AS CURRENTLY EXISTS.





N

0 50 100

SCALE IN FEET

FILE NAME = ...\3024_PMK_01.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DJK	REVISED -
©MODELNAME©	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

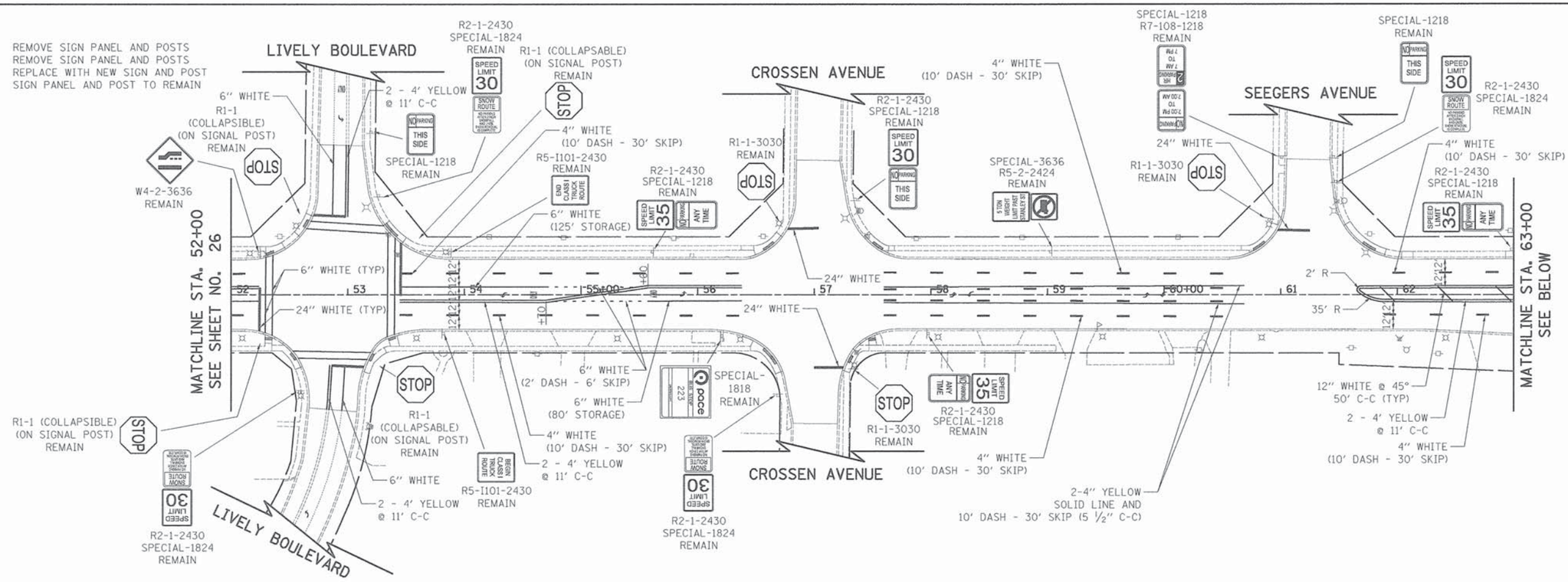
BIESTERFIELD ROAD & OAKTON STREET RESURFACING
SIGNING AND STRIPING PLAN

SCALE: 1" = 50' | SHEET 3 OF 4 SHEETS | STA. 30+00 TO STA. 52+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8339/1331	15-00062-00-RS	COOK	54	26
		CONTRACT NO. 61C79		
	ILLINOIS	FED. AID PROJECT M-4003(683)		

LEGEND

REMOVE	REMOVE SIGN PANEL AND POSTS
REM & NEW	REMOVE SIGN PANEL AND POSTS REPLACE WITH NEW SIGN AND POST
REMAIN	SIGN PANEL AND POST TO REMAIN



NOTES:

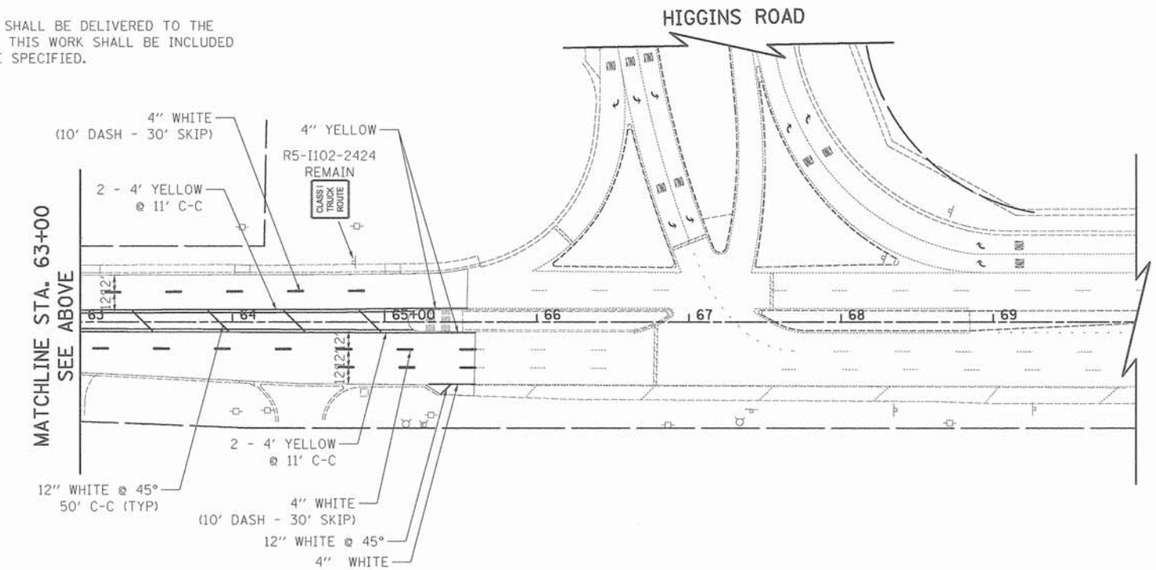
1) ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.

2) DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.

3) SEE IDOT STANDARD DETAIL TC-13 FOR ADDITIONAL INFORMATION.

4) ALL SIGNS AND DECORATIVE POSTS SHOWN TO BE REMOVED SHALL BE DELIVERED TO THE VILLAGE'S PUBLIC WORKS FACILITY AT 600 LANDMEIER ROAD. THIS WORK SHALL BE INCLUDED IN THE COST OF REMOVE SIGN PANEL ASSEMBLY, OF THE TYPE SPECIFIED.

5) ANY SIGNS OR SUPPORTS SHOWN TO REMAIN THAT ARE DAMAGED BY THE CONTRACTOR SHALL BE REPLACED BY THE CONTRACTOR AT NO EXPENSE TO THE CONTRACT. THIS SHALL INCLUDE REPLACING THE SIGN OR SUPPORT WITH THE SAME DECORATIVE MATERIALS AS CURRENTLY EXISTS.



SCALE IN FEET

FILE NAME = ...\3204_PMK_22.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BIESTERFIELD ROAD & OAKTON STREET RESURFACING SIGNING AND STRIPING PLAN
		DRAWN - JAT	REVISED -		
	PLOT SCALE = 50.2222 ' / in.	CHECKED - DJK	REVISED -		
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -		SCALE: 1" = 50' SHEET 4 OF 4 SHEETS STA. 52+00 TO STA. 69+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
(3339/1333)	15-00062-00-RS	COOK	54	27
		CONTRACT NO. 61C79		
	ILLINOIS	FED. AID PROJECT M-4003(683)		

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET	R		
RAILROAD CONTROL CABINET			
COMMUNICATIONS CABINET	R CC	ECC	CC
MASTER CONTROLLER		EMC	MC
MASTER MASTER CONTROLLER		EMMC	MMC
UNINTERRUPTABLE POWER SUPPLY	R UPS	EUPS	UPS
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT	R	P	P
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT	R T	P T	P T
STEEL MAST ARM ASSEMBLY AND POLE	R		
ALUMINUM MAST ARM ASSEMBLY AND POLE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA	R PTZ	PTZ	PTZ
SIGNAL POST	R		
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM	R		
GUY WIRE	R		
SIGNAL HEAD	R		
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)			2
SIGNAL HEAD WITH BACKPLATE	R		
SIGNAL HEAD OPTICALLY PROGRAMMED	R "P"	"P"	"P"
FLASHER INSTALLATION (S DENOTES SOLAR POWER)	R "F"	"F"	"F"
PEDESTRIAN SIGNAL HEAD	R		
PEDESTRIAN PUSHBUTTON DETECTOR	R		
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR	R APS	APS	APS
ILLUMINATED SIGN "NO LEFT TURN"	R		
ILLUMINATED SIGN "NO RIGHT TURN"	R		
DETECTOR LOOP, TYPE I			
PREFORMED DETECTOR LOOP		P	P
MICROWAVE VEHICLE SENSOR	R M	M	M
VIDEO DETECTION CAMERA	R V	V	V
VIDEO DETECTION ZONE			
PAN, TILT, ZOOM CAMERA	R PTZ	PTZ	PTZ
WIRELESS DETECTOR SENSOR	R W	W	W
WIRELESS ACCESS POINT	R		

ITEM	REMOVAL	EXISTING	PROPOSED
EMERGENCY VEHICLE LIGHT DETECTOR	R		
CONFIRMATION BEACON	R		
HANDHOLE	R		
HEAVY DUTY HANDHOLE	R H	H	H
DOUBLE HANDHOLE	R		
JUNCTION BOX	R		
UNDERGROUND CONDUIT, GALVANIZED STEEL (UC)			
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE	R		
COMMON TRENCH			CT
COILABLE NONMETALLIC CONDUIT (EMPTY)			CNC
SYSTEM ITEM		S	S
INTERSECTION ITEM		I	IP
REMOVE ITEM	R		
RELOCATE ITEM	RL		
ABANDON ITEM	A		
12" (300mm) TRAFFIC SIGNAL SECTION		R	R
12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE		R Y G	
SIGNAL FACE		R Y G ←Y ←G	R Y G ←Y ←G
SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD "RB" INDICATES REFLECTIVE BACKPLATE		R Y G ←Y ←G "P"	R Y G ←Y ←G "P"
12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL		DW W	
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED			
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID			
PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER		C D	C D
RADIO INTERCONNECT	R		
RADIO REPEATER	R ERR	ERR	RR
DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED		5	5
GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)		1	1

ITEM	REMOVAL	EXISTING	PROPOSED
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE		1	1
COAXIAL CABLE		C	C
VENDOR CABLE FOR CAMERA		V	V
COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED		6	6
FIBER OPTIC CABLE NO. 62.5/125, MM12F		12F	
FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F		24F	24F
FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F		36F	36F
GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE		C	C
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED	RCF		
STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED	RMF		
SIGNAL POST AND FOUNDATION TO BE REMOVED	RPF		
INTERSECTION & SAMPLING (SYSTEM) DETECTOR		IS	IS
SAMPLING (SYSTEM) DETECTOR		S	S
QUEUE DETECTOR		Q	Q
PREFORMED QUEUE DETECTOR		PQ	PQ
PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		PIS	PIS
PREFORMED SAMPLING (SYSTEM) DETECTOR		PS	PS

RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET	R	R
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

FILE NAME = c:\pw_work\pw=idot\footemj\d0120315\ts85.dgn	USER NAME = footemj	DESIGNED - DAG/BCK	REVISED - DAG 1-1-14
	PLOT SCALE = 50.0000 ' / in.	DRAWN - BCK	REVISED -
	PLOT DATE = 1/13/2014	CHECKED - DAD	REVISED -
		DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 1 OF 7 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	28
	TS-05	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-4003(683)	

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

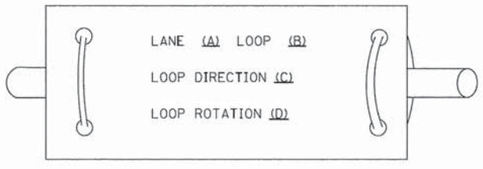
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

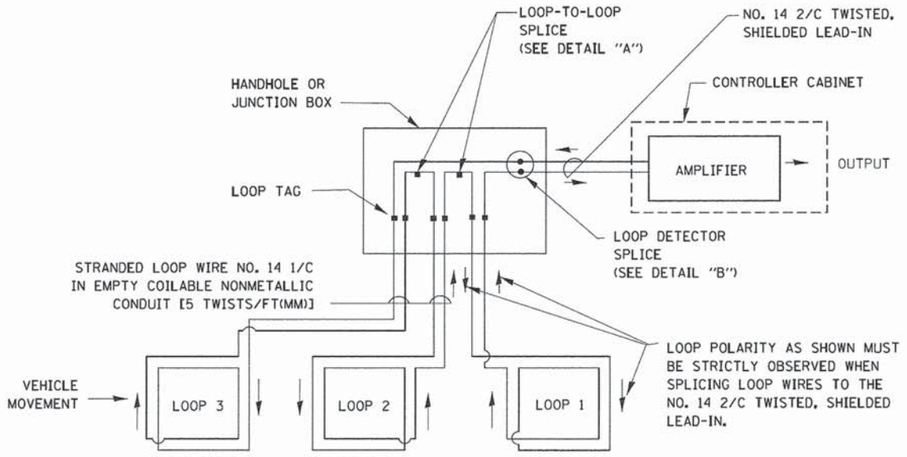


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

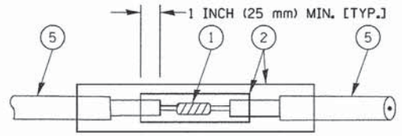
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

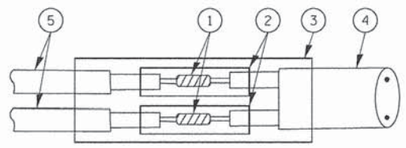


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

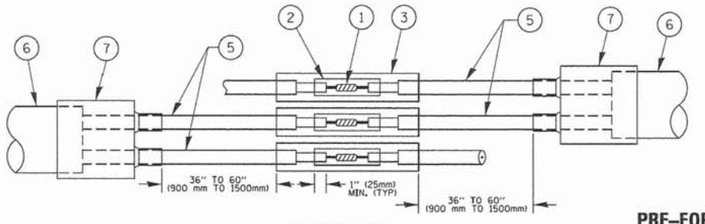


DETAIL "A"
LOOP-TO-LOOP SPLICE



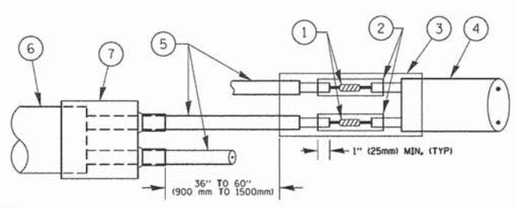
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

(6) PRE-FORMED LOOP

(7) XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME = c:\pw_work\pwidot\footemj\d0188315\ts05.dgn	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

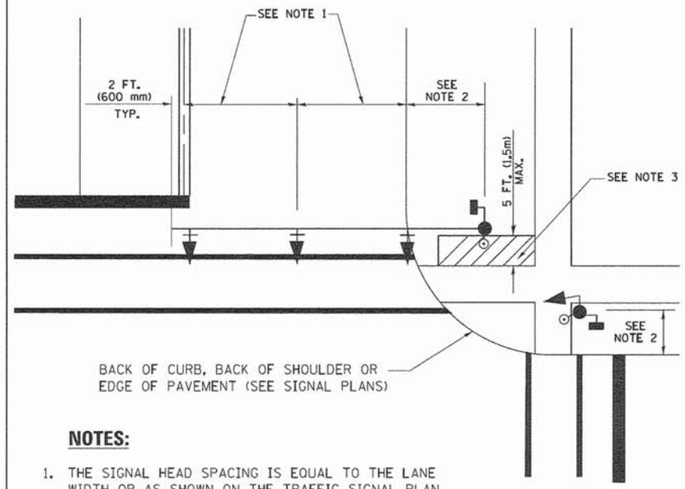
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 2 OF 7 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	29
	TS-05	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

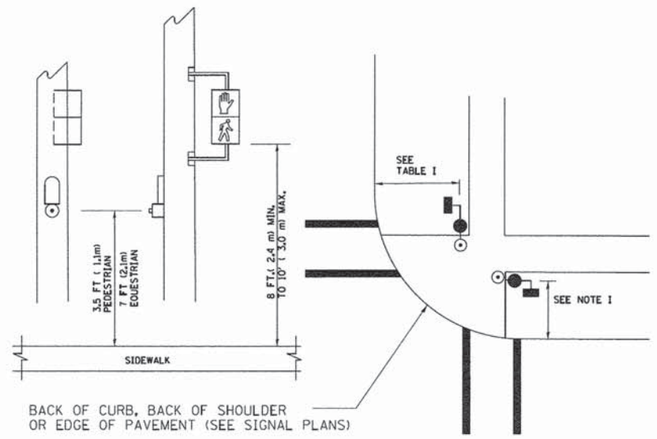
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

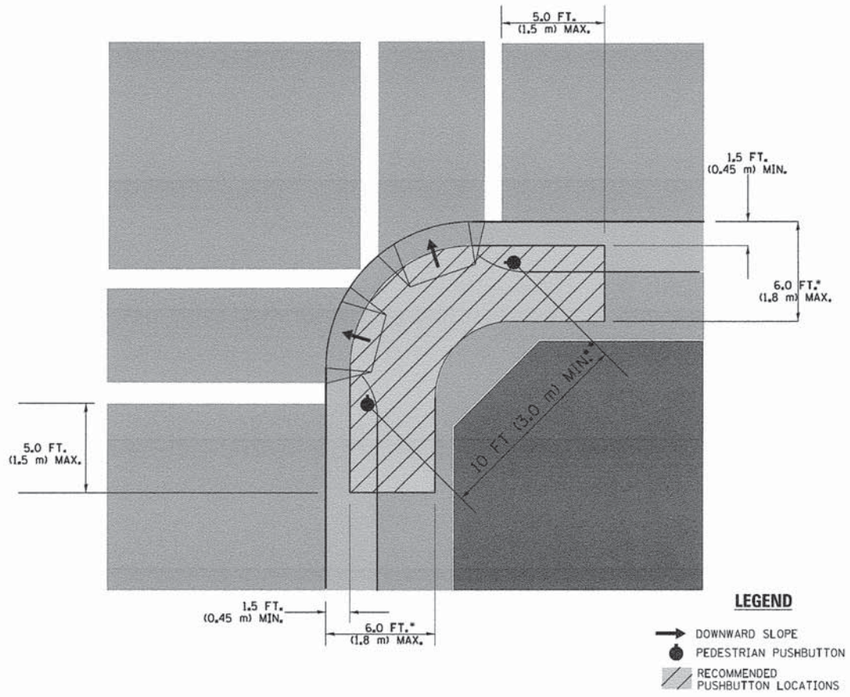
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



* WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

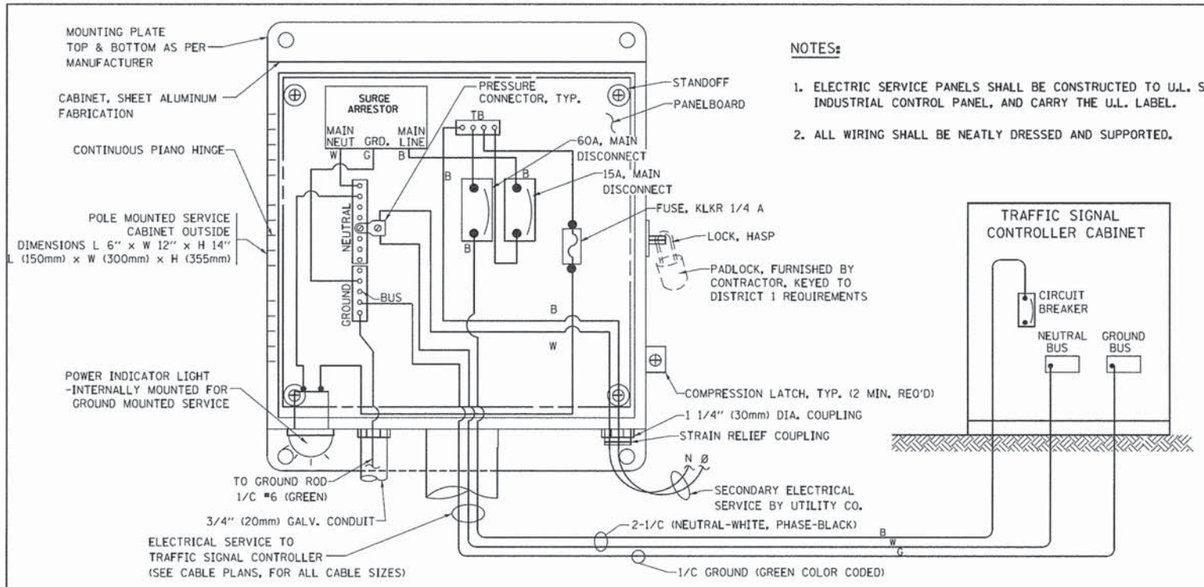
FILE NAME = c:\pw_work\pwidot\footemj\d0108315\ts05.dgn	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 3 OF 7 SHEETS | STA. | TO STA.

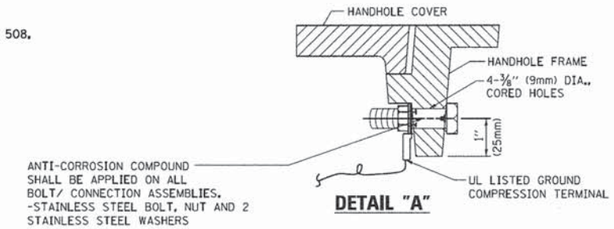
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	30
	TS-05	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-4003(683)	



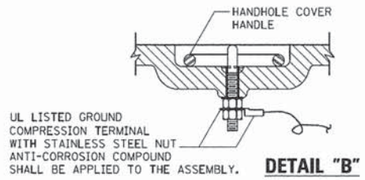
ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.



DETAIL "A"

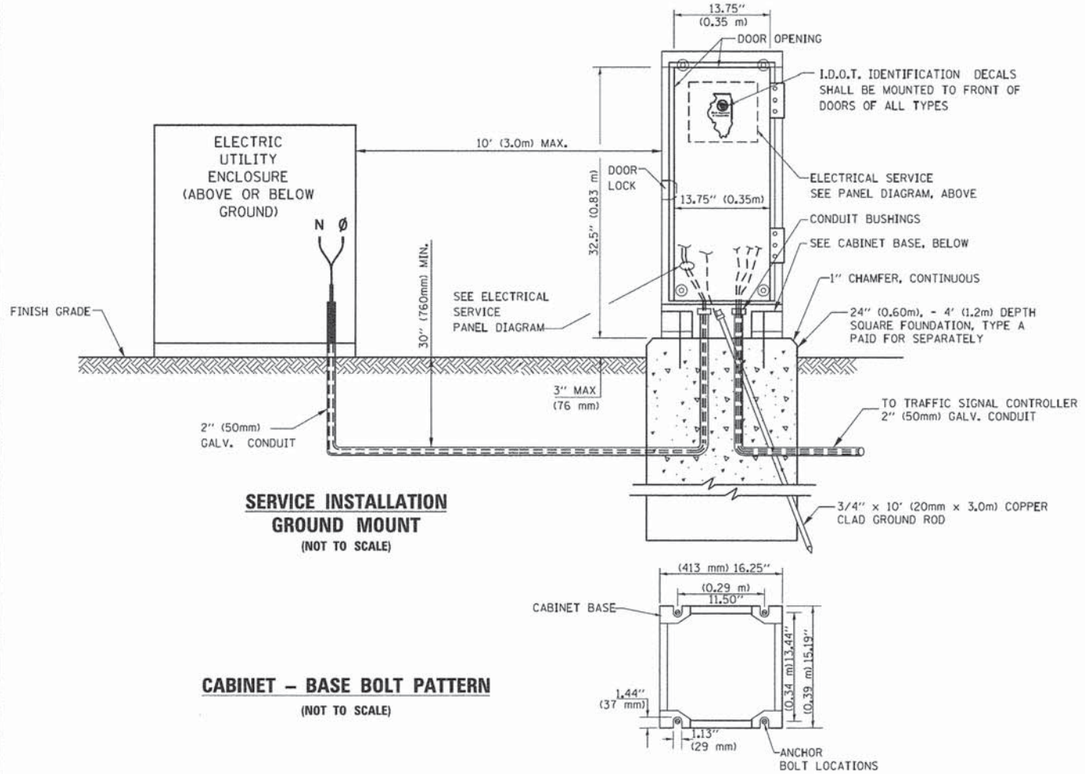


DETAIL "B"

NOTES: GROUNDING SYSTEM

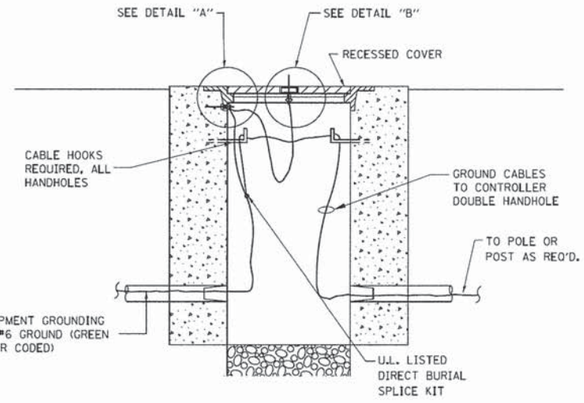
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)

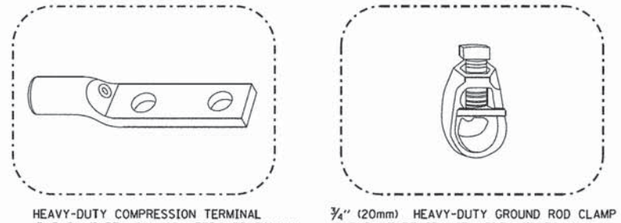


SERVICE INSTALLATION GROUND MOUNT
(NOT TO SCALE)

CABINET – BASE BOLT PATTERN
(NOT TO SCALE)



HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)

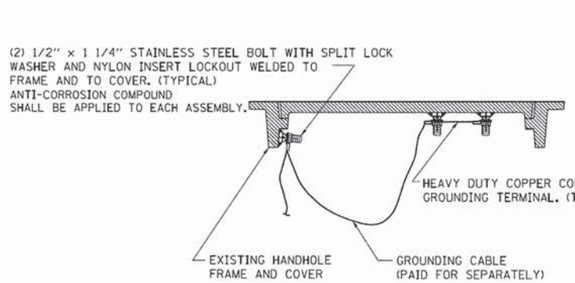


HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)

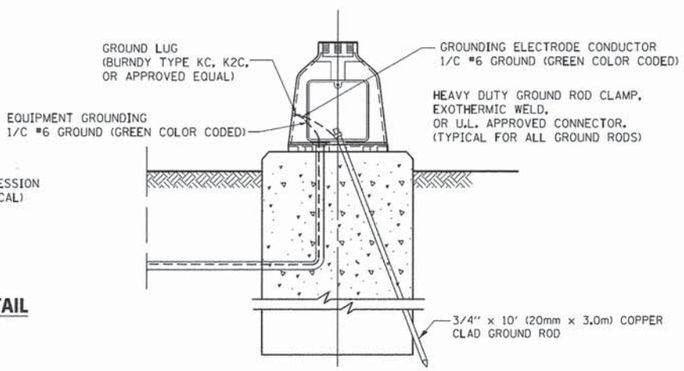
3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
  6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
  13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
  5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)



MAST ARM POLE / POST–GROUNDING DETAIL
(NOT TO SCALE)

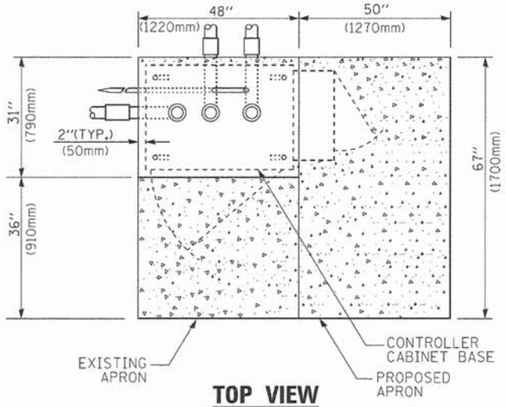
FILE NAME = c:\pw_work\pwidot\footemj\d0188315\ts05.dgn	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

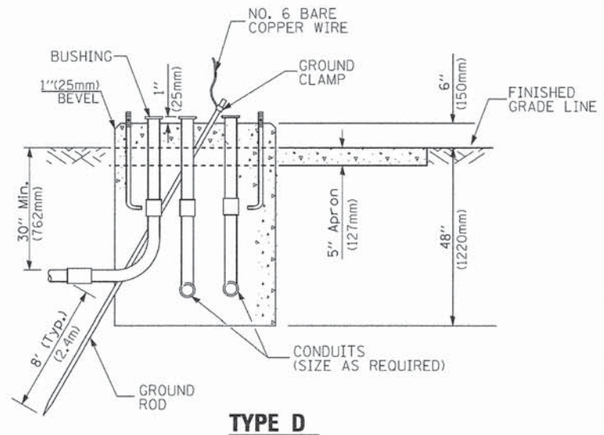
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 4 OF 7 SHEETS | STA. | TO STA.

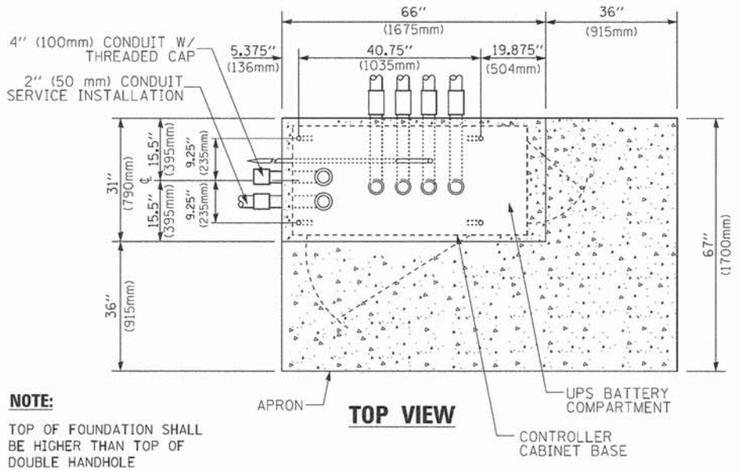
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	31
	TS-05	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		



TOP VIEW



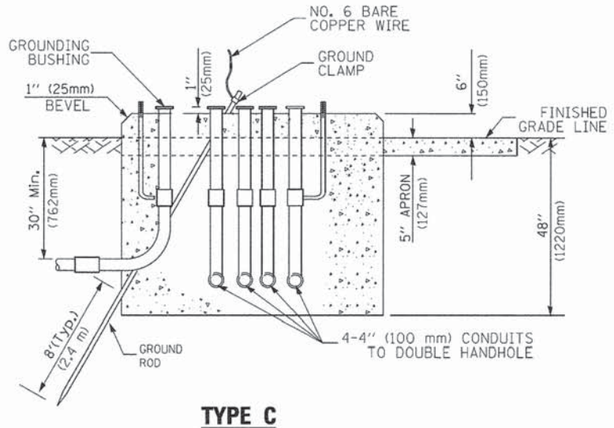
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



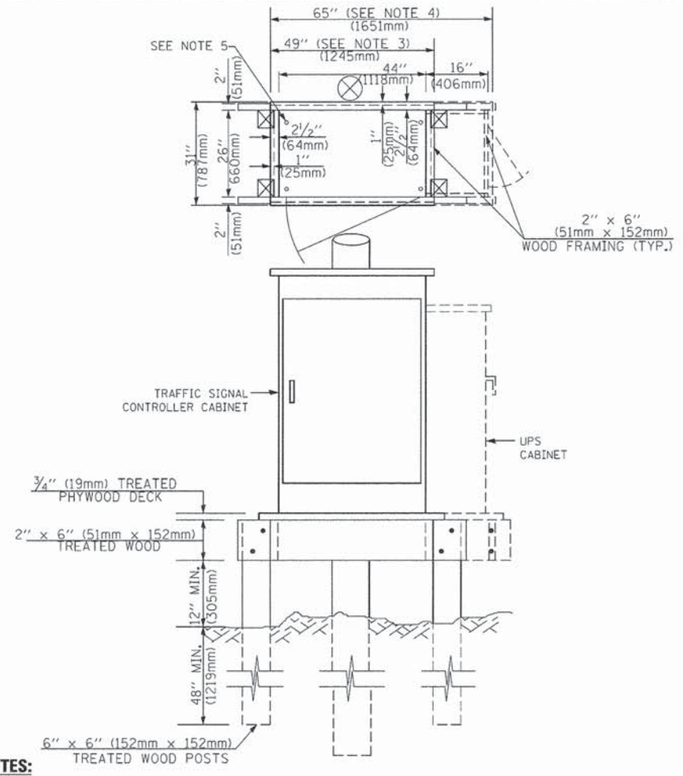
NOTE:

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE

TOP VIEW



TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

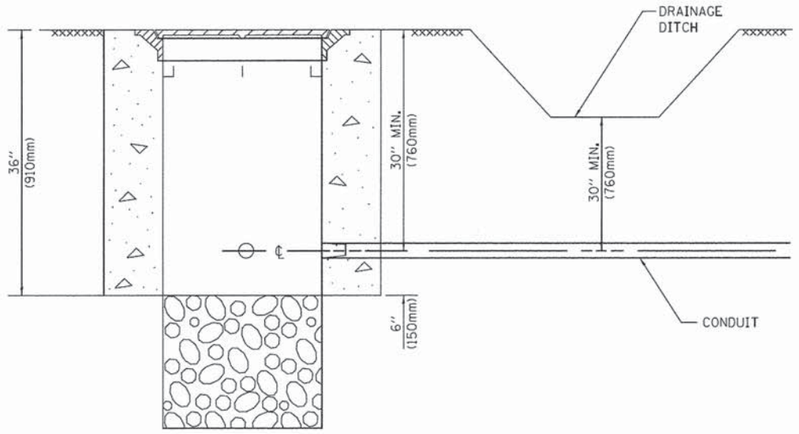
FILE NAME = c:\pw_work\pwidot\footemj\d0183151\ts05.dgn	USER NAME = footemj	DESIGNED - DAG	REVISED - DAG 1-1-14
		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 5 OF 7 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	32
	TS-05	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		

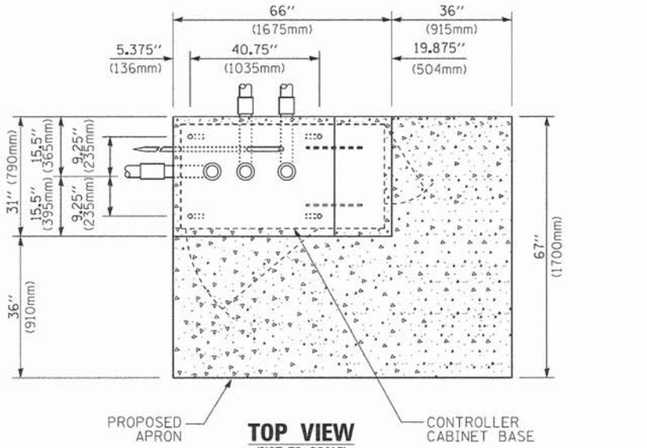


NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

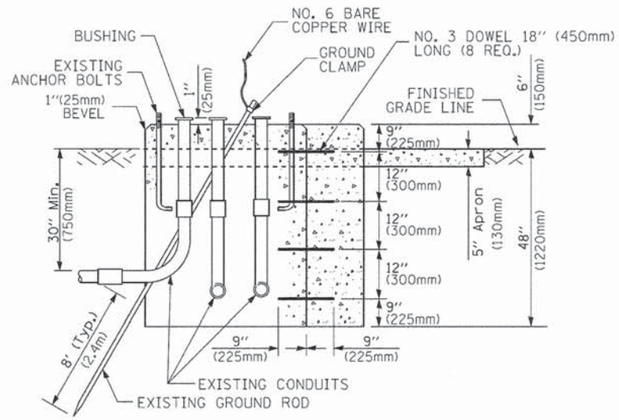
HANDHOLE WITH MINIMUM CONDUIT DEPTH

(NOT TO SCALE)



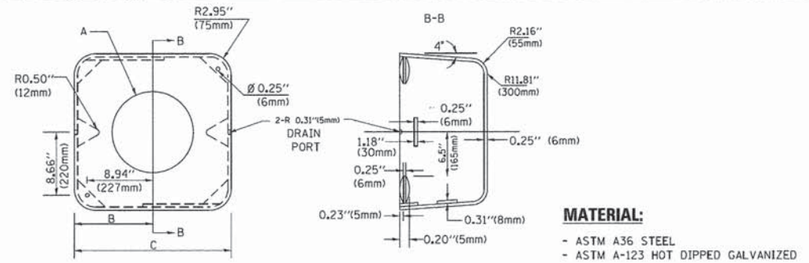
TOP VIEW

(NOT TO SCALE)



MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)



MATERIAL:

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

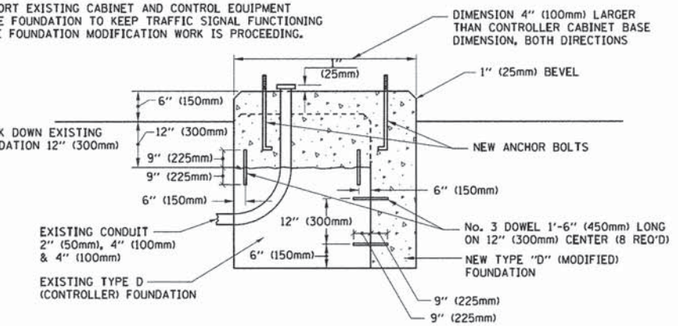
SHROUD

NOTES:

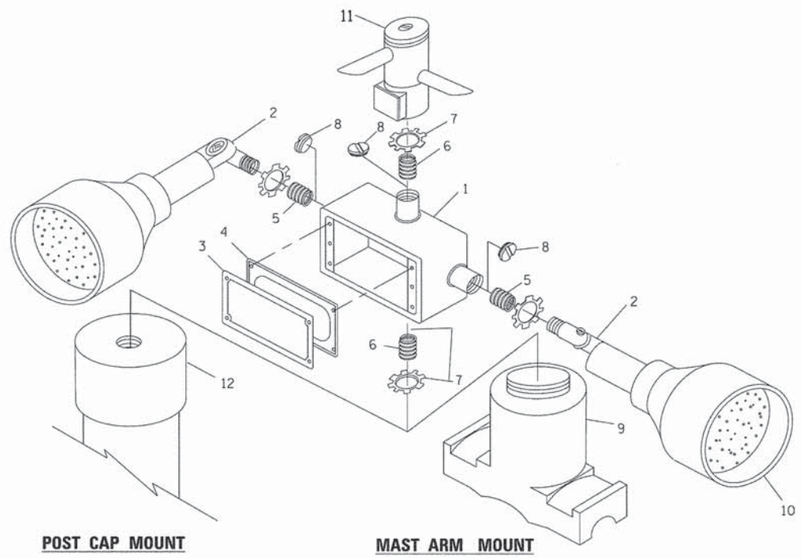
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



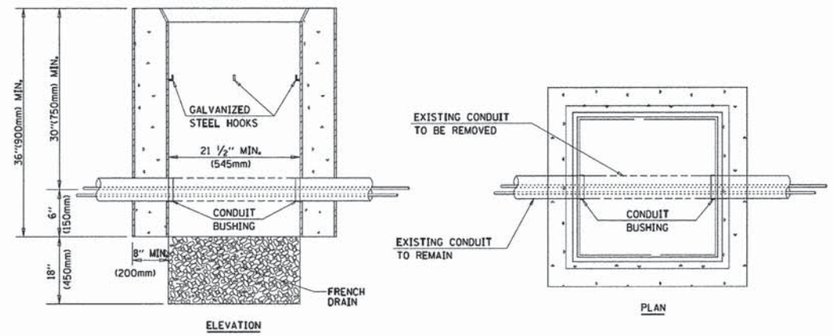
POST CAP MOUNT **MAST ARM MOUNT**

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾"(19 mm) CLOSE NIPPLE
7	¾"(19 mm) LOCKNUT
8	¾"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

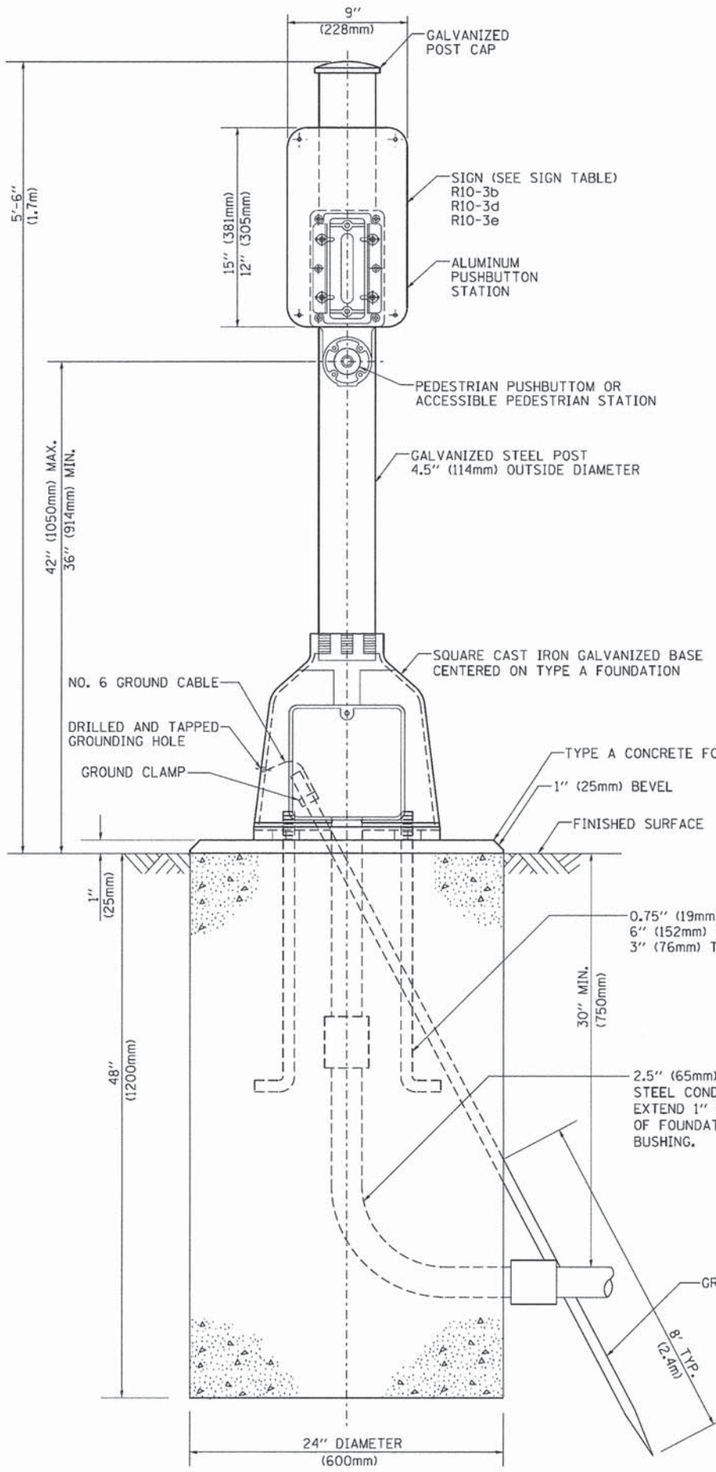
FILE NAME = c:\pw_work\pwidot\footemj\d0108315\ts05.dgn	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

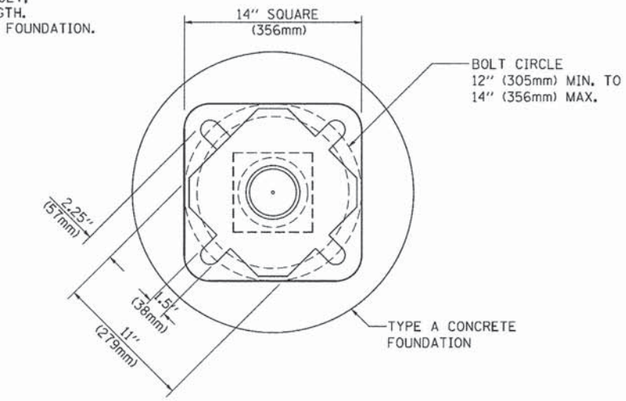
SCALE: NONE | SHEET NO. 6 OF 7 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	33
	TS-05	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		



SIGN TABLE

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



BOLT PATTERN

PEDESTRIAN PUSH BUTTON POST, TYPE A

FILE NAME = c:\pw_work\pwidot\footemj\d0108315\ts05.dgn	USER NAME = footemj	DESIGNED - DAG	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - GND	REVISED -					1339/1331	15-00062-00-RS	COOK	54	34
	PLOT SCALE = 50.0000 ' / in.	CHECKED - DAD	REVISED -						TS-05		CONTRACT NO. 61C79	
	PLOT DATE = 1/13/2014	DATE - 10/1/2012	REVISED -		SCALE: NONE	SHEET NO. 7 OF 7 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		

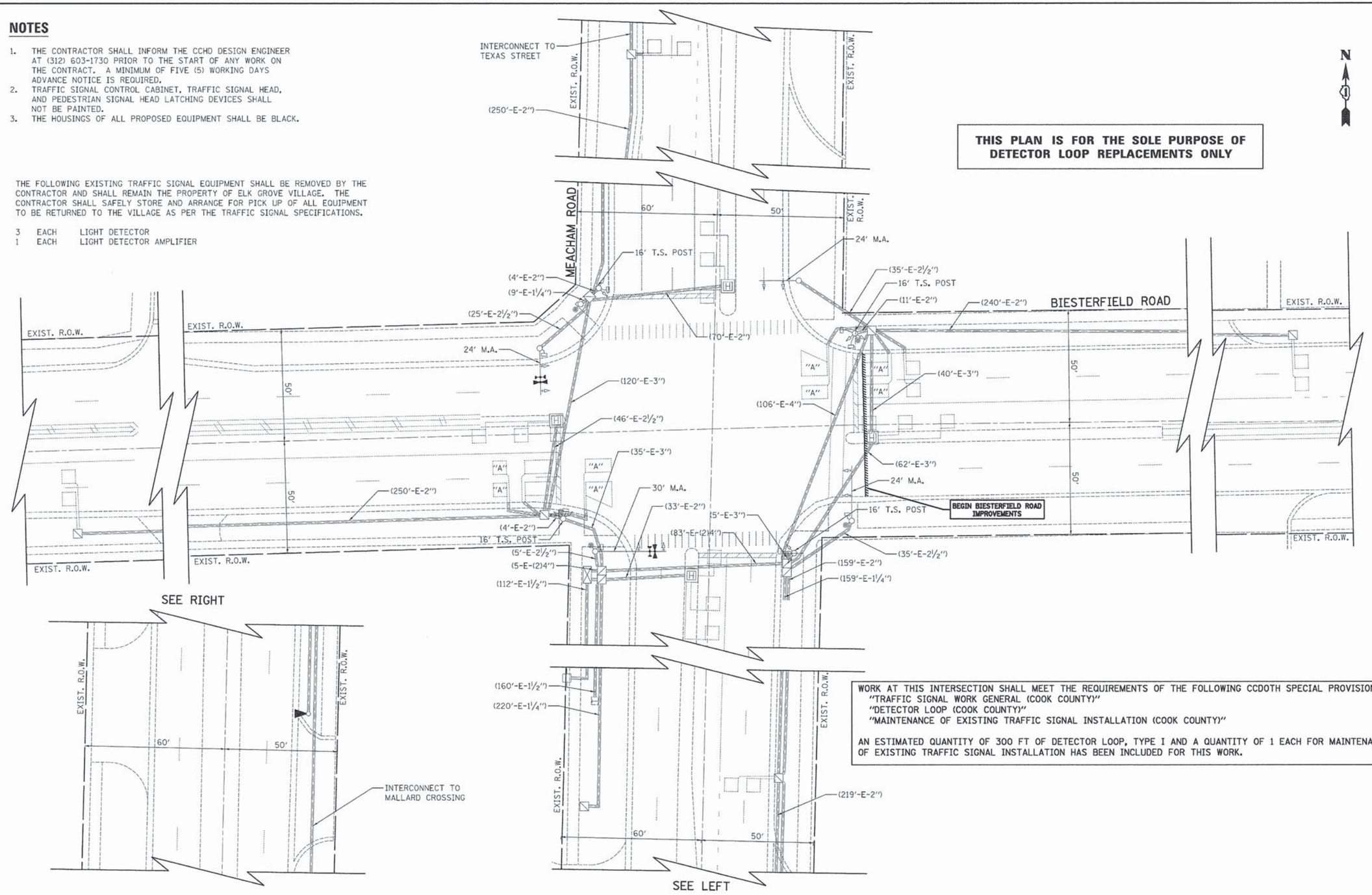
NOTES

1. THE CONTRACTOR SHALL INFORM THE CCHD DESIGN ENGINEER AT (312) 603-1730 PRIOR TO THE START OF ANY WORK ON THE CONTRACT. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCE NOTICE IS REQUIRED.
2. TRAFFIC SIGNAL CONTROL CABINET, TRAFFIC SIGNAL HEAD, AND PEDESTRIAN SIGNAL HEAD LATCHING DEVICES SHALL NOT BE PAINTED.
3. THE HOUSINGS OF ALL PROPOSED EQUIPMENT SHALL BE BLACK.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF ELK GROVE VILLAGE. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE VILLAGE AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

3 EACH LIGHT DETECTOR
1 EACH LIGHT DETECTOR AMPLIFIER

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY



WORK AT THIS INTERSECTION SHALL MEET THE REQUIREMENTS OF THE FOLLOWING CCDOTH SPECIAL PROVISIONS:
"TRAFFIC SIGNAL WORK GENERAL (COOK COUNTY)"
"DETECTOR LOOP (COOK COUNTY)"
"MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION (COOK COUNTY)"

AN ESTIMATED QUANTITY OF 300 FT OF DETECTOR LOOP, TYPE I AND A QUANTITY OF 1 EACH FOR MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION HAS BEEN INCLUDED FOR THIS WORK.

FILE NAME = ...\TS-11A_Meacham_Biest_Intersection.dgn	USER NAME = djk	DESIGNED - BRD	REVISED -
		DRAWN - MFB	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - JJE	REVISED -
	PLOT DATE = 1/22/2014	DATE - 01/22/2014	REVISED -

VILLAGE OF ELK GROVE VILLAGE

TRAFFIC SIGNAL MODIFICATION PLAN
BIESTERFIELD ROAD AT MEACHAM ROAD
1" = 20' | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		COOK	37	21
		CONTRACT NO.		
		ILLINOIS FED. AID PROJECT		

FILE NAME = ...\3024_Signal_RecordPlans.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 1.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
DETECTOR LOOP REPLACEMENT PLAN – BIESTERFIELD ROAD AND MEACHAM ROAD
SCALE: NTS | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	35
		CONTRACT NO. 61C79		
		ILLINOIS FED. AID PROJECT M-4003(683)		

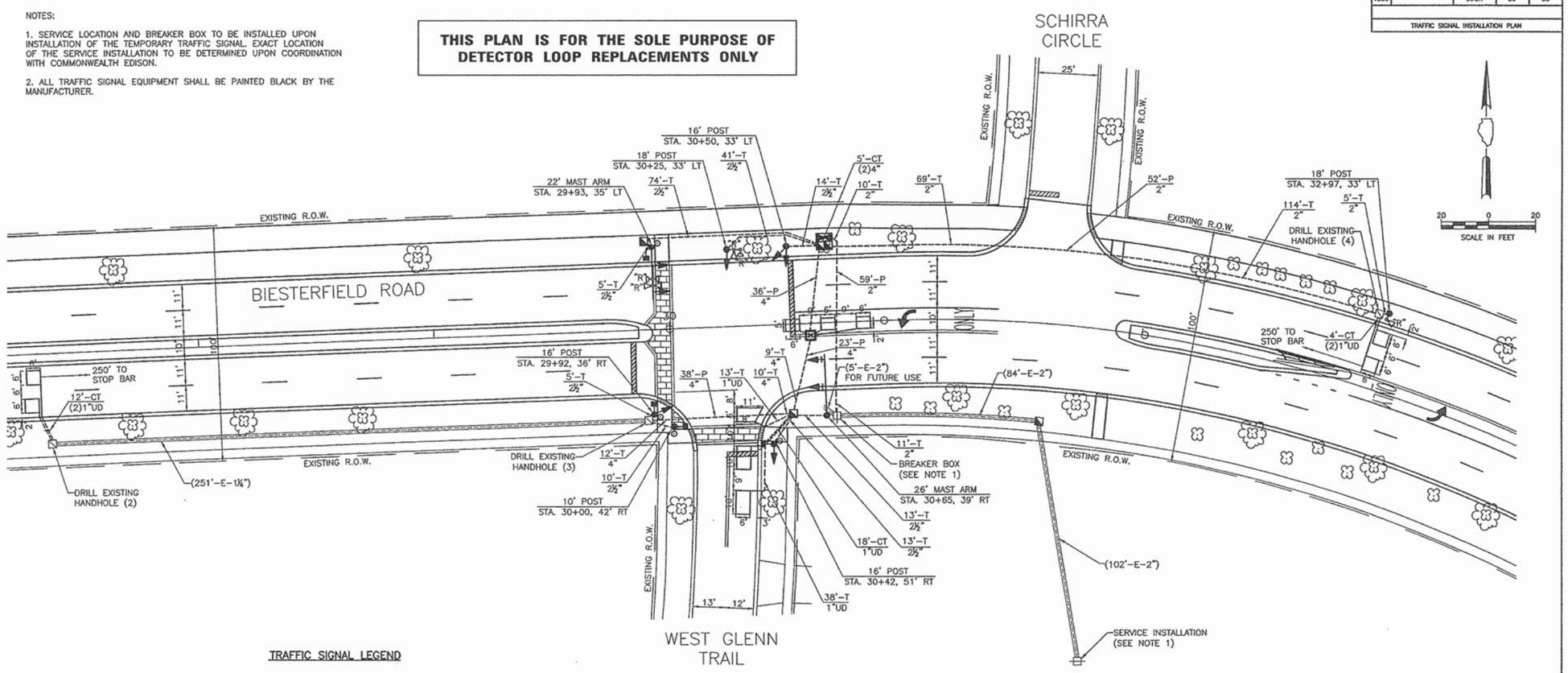
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339		COOK	59	39
		TRAFFIC SIGNAL INSTALLATION PLAN		

NOTES:

1. SERVICE LOCATION AND BREAKER BOX TO BE INSTALLED UPON INSTALLATION OF THE TEMPORARY TRAFFIC SIGNAL. EXACT LOCATION OF THE SERVICE INSTALLATION TO BE DETERMINED UPON COORDINATION WITH COMMONWEALTH EDISON.

2. ALL TRAFFIC SIGNAL EQUIPMENT SHALL BE PAINTED BLACK BY THE MANUFACTURER.

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY



TRAFFIC SIGNAL LEGEND

	EXISTING	PROPOSED		EXISTING	PROPOSED
CONTROLLER	⊠	⊠	DETECTOR LOOP	⊏⊐	□
SERVICE INSTALLATION	⊡	⊡	CAST IRON JUNCTION BOX	⊕ "E"	⊕
SIGNAL HEAD	▷	→	EMERGENCY VEHICLE SYSTEM DETECTOR	◁	◀
SIGNAL HEAD WITH BACKPLATE	▷	→	CONFIRMATION BEACON	◁	◀
SIGNAL HEAD, PEDESTRIAN	▯	▮	SIGNAL HEAD OPTICALLY PROGRAMMED	▷ "P"	→ "P"
SIGNAL POST	○	●	CONDUIT SPLICE		
MAST ARM ASSEMBLY AND POLE, STEEL			WOOD POLE	⊗ "E"	⊗
MAST ARM ASSEMBLY AND POLE, ALUMINUM			RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	"E"	
COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE			VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
COMMON TRENCH		CT	RAILROAD CONTROLLER	"E"	
UNIT DUCT		UD	TELEPHONE CONNECTION	T	T
HANDHOLE			ILLUMINATED SIGN, "NO LEFT TURN"	"E"	
HEAVY DUTY HANDHOLE	H	H	ILLUMINATED SIGN, "NO RIGHT TURN"	"E"	
DOUBLE HANDHOLE			RELOCATED SIGNAL EQUIPMENT		"R"
G.S. CONDUIT IN TRENCH OR PUSHED					
PEDESTRIAN PUSHBUTTON DETECTOR	●	⊙			

WORK AT THIS INTERSECTION SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

AN ESTIMATED QUANTITY OF 264 FT OF DETECTOR LOOP REPLACEMENT HAS BEEN INCLUDED FOR THIS WORK.

*LOOP DETECTORS TO BE 6'-12" FROM HEADER BAND OF BRICK CROSSWALK. EXACT LOCATION OF LOOP DETECTORS TO BE DETERMINED IN THE FIELD.

REVISIONS	
NAME	DATE

VILLAGE OF ELK GROVE VILLAGE

TRAFFIC SIGNAL INSTALLATION PLAN

BIESTERFIELD ROAD & WEST GLENN TRAIL

SCALE: 1"=20'
DATE: 02/16/04

DRAWN BY: BRD
DESIGNED BY: BRD
CHECKED BY: KMM

FILE NAME = ...\3004_Signal_RecordPlans.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 1.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

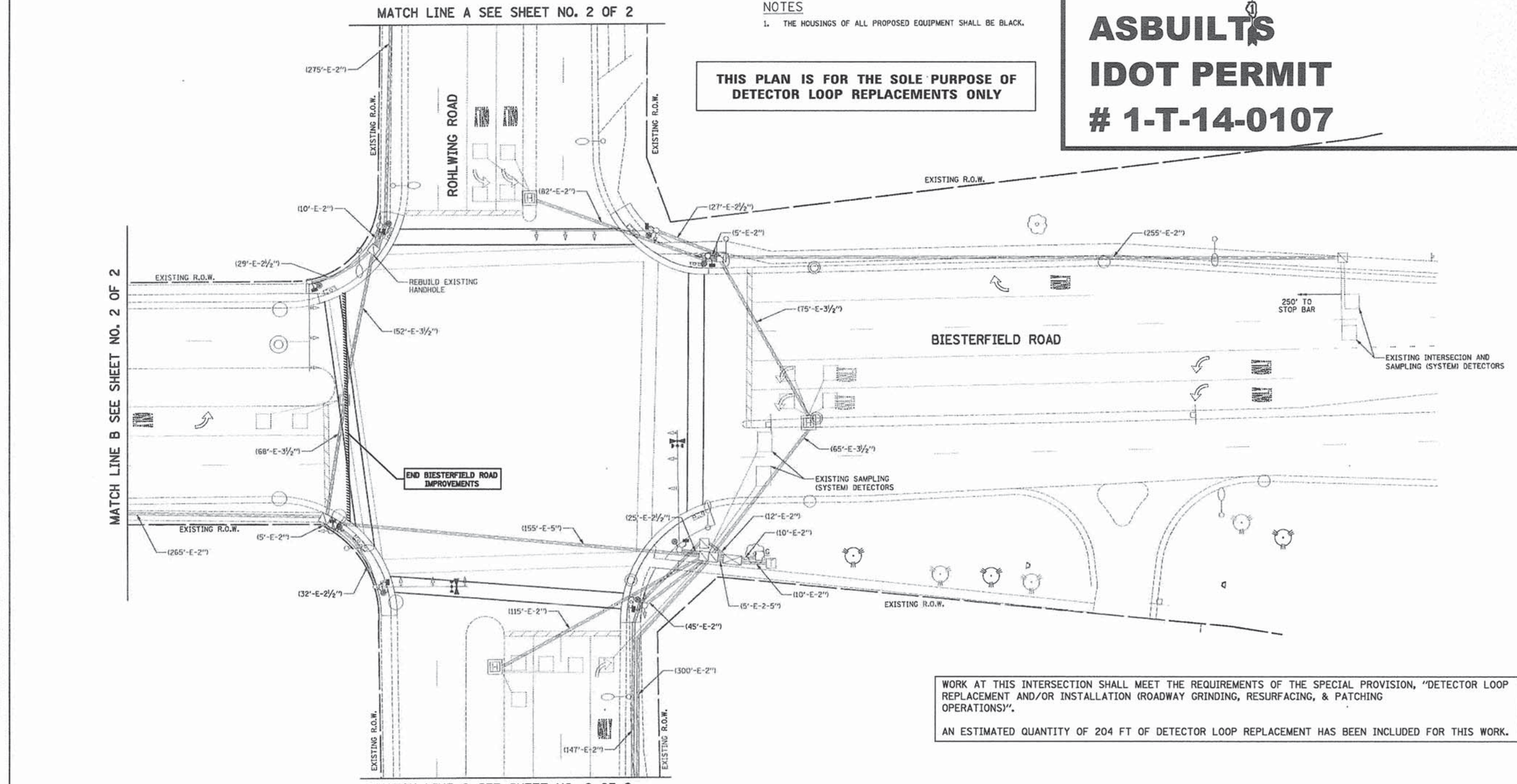
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
DETECTOR LOOP REPLACEMENT PLAN – BIESTERFIELD RD AND WEST GLENN TR

SCALE: NTS | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	36
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

THORNE ELECTRIC INC.
JOB # 14-1109
ASBUILTS
IDOT PERMIT
 # 1-T-14-0107



NOTES

1. THE HOUSINGS OF ALL PROPOSED EQUIPMENT SHALL BE BLACK.

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

WORK AT THIS INTERSECTION SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

AN ESTIMATED QUANTITY OF 204 FT OF DETECTOR LOOP REPLACEMENT HAS BEEN INCLUDED FOR THIS WORK.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF ELK GROVE VILLAGE. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE VILLAGE AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

3 EACH LIGHT DETECTOR
1 EACH LIGHT DETECTOR AMPLIFIER

0 20 40
SCALE IN FEET

PLAN — PLOTTED, PLOTTED AUTOMATIC DECODE, GRADES CHECKED, CADD FILE NAME, NOTE BOOK, NO.

PROFILE — PLOTTED, PLOTTED AUTOMATIC DECODE, GRADES CHECKED, STRUCTURE NUMBERS, NOTE BOOK, NO.

FILE NAME = ...\Rohlwing Intersection.dgn

CIVILTECH	USER NAME = d.jk	DESIGNED - MFB	REVISED -
		DRAWN - MFB	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED - JJE	REVISED -
	PLOT DATE = 1/3/2014	DATE - 01/06/2014	REVISED -

VILLAGE OF ELK GROVE VILLAGE

BIESTERFIELD ROAD SIGNAL AND LIGHTING PROJECT
TRAFFIC SIGNAL MODIFICATION PLAN
SCALE: 1" = 20' | SHEET NO. 1 OF 2 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		COOK	37	17
		CONTRACT NO.		
	ILLINOIS	FED. AID PROJECT		

FILE NAME = ...\3004_Signal_RecordPlans.dgn
#MODELNAME#

USER NAME = d.jk	DESIGNED - JAT	REVISED -
	DRAWN - JAT	REVISED -
PLOT SCALE = 1.0000 ' / in.	CHECKED - DJK	REVISED -
PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
DETECTOR LOOP REPLACEMENT PLAN – BIESTERFIELD ROAD AND ROHLWING ROAD
SCALE: NTS | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3339/1339	15-00062-00-RS	COOK	54	37
		CONTRACT NO. 61C79		
	ILLINOIS	FED. AID PROJECT M-4003(683)		

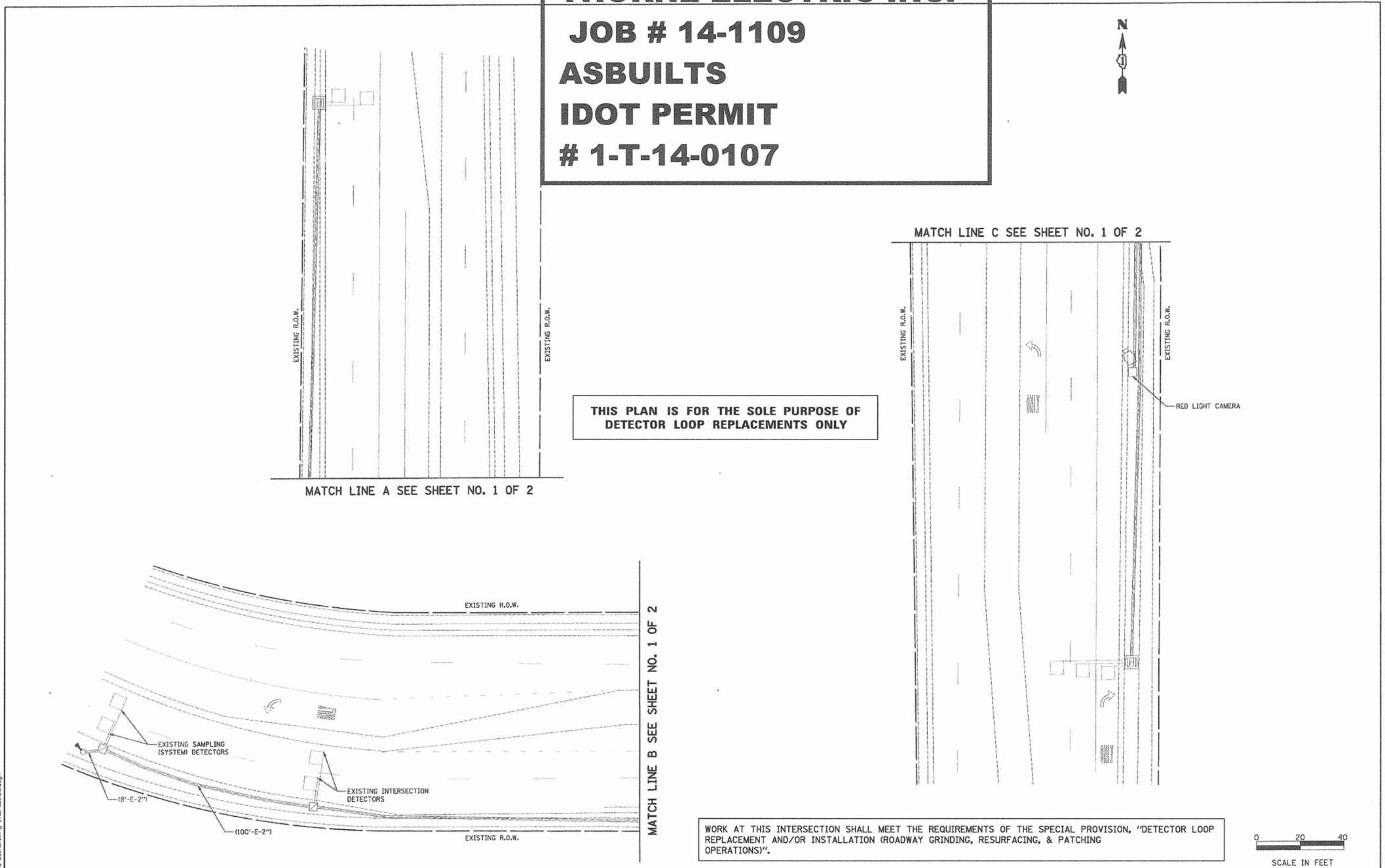
THORNE ELECTRIC INC.

JOB # 14-1109

ASBUILTS

IDOT PERMIT

 # 1-T-14-0107



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

WORK AT THIS INTERSECTION SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

FILE NAME = ...\TS-08_Rohlwing Intersection.dgn	USER NAME = djk	DESIGNED - MFB	REVISED -
CIVILTECH		DRAWN - MFB	REVISED -
	PLOT SCALE = 20.0000 '/ in.	CHECKED - JJE	REVISED -
	PLOT DATE = 1/3/2014	DATE - 01/06/2014	REVISED -

VILLAGE OF ELK GROVE VILLAGE

BIESTERFIELD ROAD SIGNAL AND LIGHTING PROJECT
TRAFFIC SIGNAL MODIFICATION PLAN

SCALE: 1" = 20' | SHEET NO. 2 OF 2 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		COOK	37	18
		CONTRACT NO.		
ILLINOIS FED. AID PROJECT				

FILE NAME = ...\3004_Signal_RecordPlans.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 1.0000 '/ in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
DETECTOR LOOP REPLACEMENT PLAN – BIESTERFIELD ROAD AND ROHLWING ROAD

SCALE: NTS | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	38
		CONTRACT NO. 61C79		
ILLINOIS FED. AID PROJECT M-4003(683)				

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

6	EACH	PEDESTRIAN SIGNAL HEAD, 1-FACE
1	EACH	PEDESTRIAN SIGNAL HEAD, 2-FACE
7	EACH	PEDESTRIAN PUSH-BUTTON

TRAFFIC SIGNAL NOTES

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

FILE NAME = ...\28.3324_Proposed Signal plan.dgn	USER NAME = djk	DESIGNED - LEP	REVISED -
		DRAWN - LEP	REVISED -
$MODELNAME$	PLOT SCALE = 20.2222 ' / in.	CHECKED - BRD	REVISED -
	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

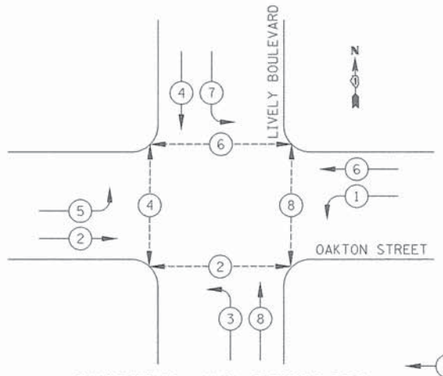
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
OAKTON STREET AND LIVELY BOULEVARD PROPOSED SIGNAL PLAN

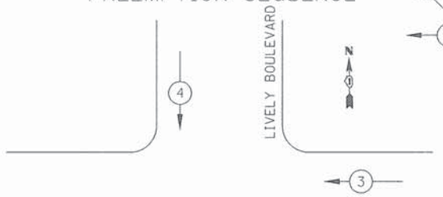
SCALE: | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1333	15-00062-00-RS	COOK	54	39
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

PROPOSED CONTROLLER SEQUENCE

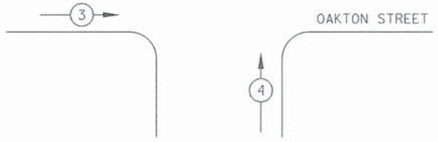


EXISTING AND PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE

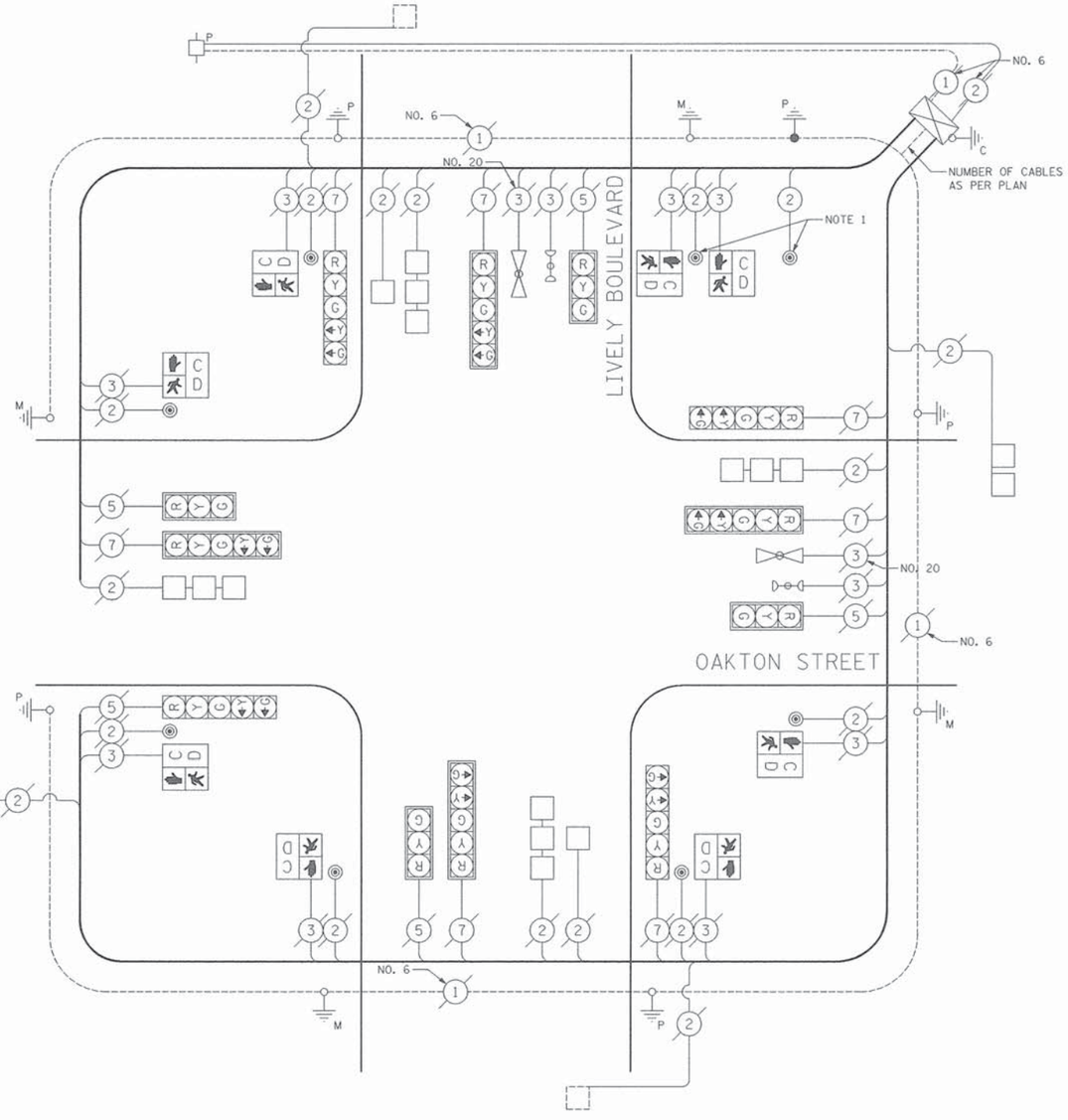


LEGEND

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OL — OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	←	↑↓



CABLE PLAN

NOT TO SCALE

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	% OPERATION	
SIGNAL (RED)	12		17	0.50	102
(YELLOW)	12		25	0.25	75
(GREEN)	12		15	0.25	45
ARROW	16		12	0.10	19
PED. SIGNAL	8		25	1.00	200
CONTROLLER	1		100	1.00	100
FLASHER				0.50	
				TOTAL =	541

ENERGY COSTS TO: VILLAGE OF ELK GROVE VILLAGE
901 WELLINGTON AVENUE
ELK GROVE VILLAGE, IL 60007

ENERGY SUPPLY: CONTACT: COM ED
PHONE: (866) 639-3532
COMPANY: COM ED

SCHEDULE OF QUANTITIES

PAY ITEM	UNIT	QNTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	5
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	39
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	21
CONCRETE FOUNDATION, TYPE A	FOOT	4
DRILL EXISTING HANDHOLE	EACH	1
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8
DETECTOR LOOP, TYPE I	FOOT	576
PEDESTRIAN PUSH-BUTTON	EACH	8
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN PUSH-BUTTON POST, TYPE A	EACH	1

NOTES

1. THE EXISTING PUSH-BUTTON ON THE NORTHEAST CORNER PLACES A CALL TO PHASES 6 AND 8. THE TWO PROPOSED PUSH-BUTTONS ON THE NORTHEAST CORNER SHALL CALL PHASES 6 AND 8 SEPARATELY. THIS SHALL BE PAID FOR AS "MODIFY EXISTING CONTROLLER."

FILE NAME = ...\29_3024_Proposed Cable plan.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 3.0488 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
OAKTON STREET AND LIVELY BOULEVARD PROPOSED SIGNAL PLAN

SCALE: NTS | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1333	15-00062-00-RS	COOK	54	40
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

341	31TS-3	COOK	
STA.		TO STA.	
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	

D-91-168-94

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

N

LOCATED IN TAPER

3' 12' 6' 6'

6'

10'-CT
(2) 1" UD

HIGGINS (ILL 72)

250' (TYP.)

(250'-E-1 1/4")

TYPE 4 CABINET W/NEW CONTROLLER & EQUIPMENT.
REMOVE EXISTING TYPE 3 CABINET & REBUILD
EXISTING TYPE D FOUNDATION

(205'-E-24)

(40'-E-2")

(10'-E-1 1/4")

(21'-E-2 1/2")

24" WHITE (TYP.)

(20'-E-2")

(2X25'-E-1")

(45'-E-3")

(99'-E-2")

(4'-E-2")

ABANDON

(62'-E-2 1/2")

(25'-E-1")

(18'-E-2")

(50'-E-2")

(86'-E-2 1/2")

(118'-E-2")

(20'-E-1")

24" WHITE (TYP.)

REPLACE EXISTING SIGNAL HEADS, 1-FACE 3-SECTION, BRACKET MOUNTED (12")

(40'-E-2")

(8'-E-2")

INTERCONNECT TO BUSSE RD. & OAKTON ST.
PROPOSED 2" GS-CONDUIT

85'-P
2"

OAKTON

STREET

6'
6'
12'
3'

5'-CT
(3) 1" UD

230'-T
2"

END OAKTON STREET IMPROVEMENTS

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
CONTROLLER		
SERVICE INSTALLATION		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD, PEDESTRIAN		
SIGNAL POST		
MAST ARM ASSEMBLY AND POLE, STEEL		
MAST ARM ASSEMBLY AND POLE, ALUMINUM		
COMMON TRENCH	CT	
UNIT DUCT	UD	
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
G.S. CONDUIT IN TRENCH OR PUSHED		
PEDESTRIAN PUSHBUTTON DETECTOR		
DETECTOR LOOP		
CAST IRON JUNCTION BOX		
EMERGENCY VEHICLE SYSTEM DETECTOR		
SIGNAL HEAD OPTICALLY PROGRAMMED		
CONDUIT SPLICE		
WOOD POLE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RAILROAD CONTROL CABINET		

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, & PATCHING OPERATIONS)".

AN ESTIMATED QUANTITY OF 119 FT OF DETECTOR LOOP REPLACEMENT HAS BEEN INCLUDED FOR THIS WORK.

REVISIONS	
NAME	DATE
TPC	6-25-96

ILLINOIS DEPARTMENT OF TRANSPORTATION

ILL 72 (HIGGINS) AT ILL 72 (OAKTON) (WEST)

TRAFFIC SIGNAL PLAN

SCALE: VERT. 1" = 20'
HORIZ.
DATE 1-17-95

DRAWN BY PF
DESIGNED BY TP
CHECKED BY DA

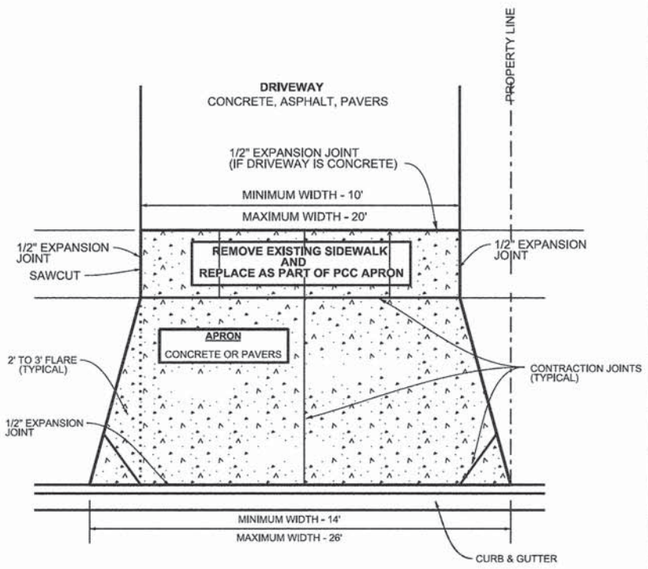
FILE NAME = ...\3024_Signal_RecordPlans.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 1.2000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
DETECTOR LOOP REPLACEMENT PLAN – OAKTON STREET AND HIGGINS ROAD

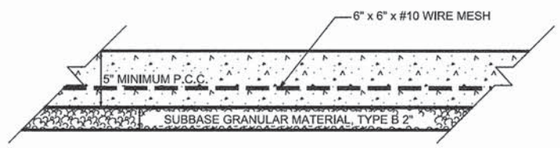
SCALE: NTS | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
6339/1339	15-00062-00-RS	COOK	54	41
		CONTRACT NO. 61C79		
	ILLINOIS FED. AID PROJECT M-4003(683)			

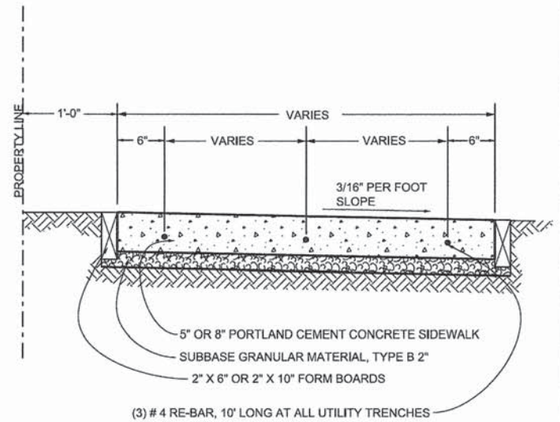


NOTE:

1. ADDITIONAL CONTRACTION JOINTS MAY BE NEEDED. THE PORTION OF THE APRON AT THE SIDEWALK SHALL HAVE CONTRACTION JOINTS ON 4-FOOT CENTERS.
2. A QUALIFIED CONCRETE FINISHER MUST BE ON-SITE WHEN POURING CONCRETE.
3. "CALIFORNIA CORNERS" OR STAMPED CONCRETE MAY BE USED ON THE PRIVATE CONCRETE DRIVEWAY AND APRON (HOLD HARMLESS AGREEMENT REQUIRED). THE PUBLIC SIDEWALK PORTION OF THE APRON MUST HAVE A BROOM FINSH.



RESIDENTIAL DRIVE APRON DETAIL



NOTE:

1. A QUALIFIED CONCRETE FINISHER MUST BE ON-SITE WHEN POURING CONCRETE.
2. "CALIFORNIA CORNERS" OR STAMPED CONCRETE MAY BE USED ON THE PRIVATE CONCRETE SIDEWALK. THE PUBLIC SIDEWALK MUST HAVE A BROOM FINSH.
3. PROVIDE 1/2" EXPANSION JOINTS AT 50' INTERVALS.
4. DETECTABLE WARNINGS PANEL SHALL BE ONE OF THE PRODUCTS SPECIFIED IN THE SPECIAL PROVISIONS.

PCC SIDEWALK

VILLAGE OF ELK GROVE VILLAGE

NO.	REVISIONS	BY	DATE
1	REVISED		4/10/14

PCC DETAIL SHEET 01

DESIGNED BY: D.C.B.
DRAWN BY: D.C.B.
NTS

PCC - 01
SHEET NO.

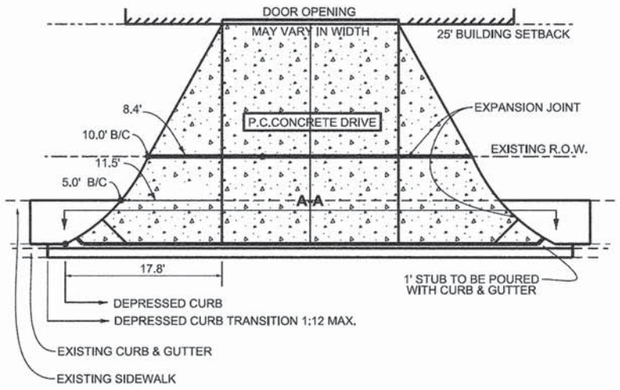
FILE NAME = ...\17-Details\3024_Details.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 1.2000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
CONSTRUCTION DETAILS
SCALE: NTS | SHEET 1 OF 2 SHEETS | STA. | TO STA.

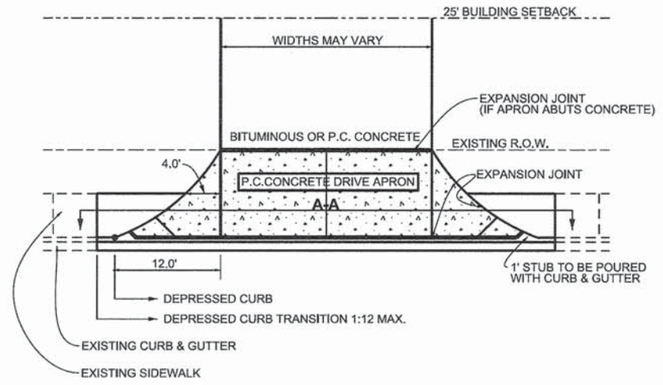
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	42

CONTRACT NO. 61C79
ILLINOIS | FED. AID PROJECT M-4003(683)

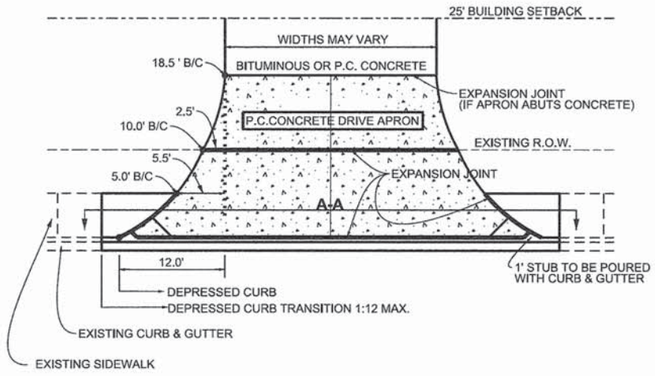


NOTE: 1. CONTRACTION JOINTS SHALL NOT BE SPACED MORE THAN 15' APART.

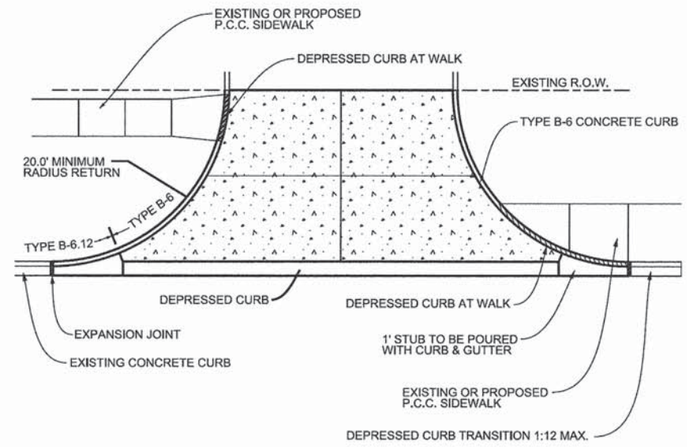
COMMERCIAL / INDUSTRIAL DRIVE APRON ON-STREET LOADING DOCK



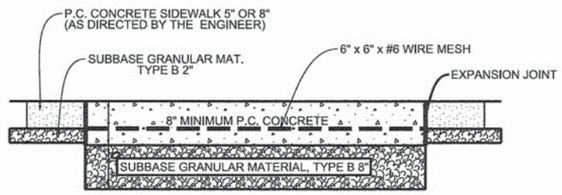
COMMERCIAL / INDUSTRIAL PARKING LOT DRIVE APRON WITHOUT CURB



COMMERCIAL / INDUSTRIAL PARKING LOT DRIVE APRON WITH AN OFF-STREET LOADING DOCK



COMMERCIAL / INDUSTRIAL PARKING LOT DRIVE APRON WITH CURB



SECTION A-A

VILLAGE OF ELK GROVE VILLAGE

NO.	REVISIONS	BY	DATE

NO.	REVISIONS	BY	DATE
1	REVISED		4/10/14

PCC DETAIL SHEET 02

DESIGNED BY: D.C.B.

DRAWN BY: D.C.B.

NTS

PCC - 02

SHEET NO.

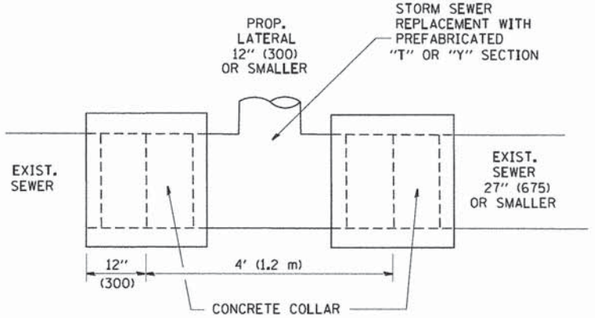
FILE NAME = ...\17-Details\3024_Details.dgn	USER NAME = djk	DESIGNED - JAT	REVISED -
		DRAWN - JAT	REVISED -
	PLOT SCALE = 1.0000 ' / in.	CHECKED - DJK	REVISED -
$MODELNAME$	PLOT DATE = 3/9/2016	DATE - 3/14/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BIESTERFIELD ROAD & OAKTON STREET RESURFACING
CONSTRUCTION DETAILS

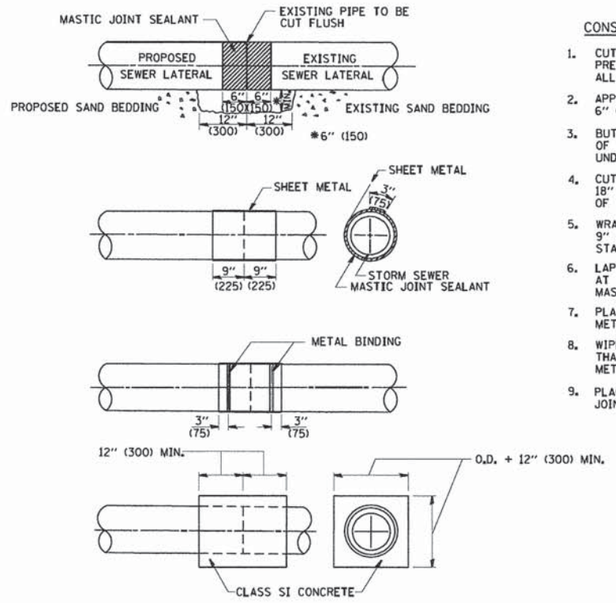
SCALE: NTS | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	43
		CONTRACT NO. 61C79		
	ILLINOIS	FED. AID PROJECT M-4003(683)		



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER
OF 27" (675) OR SMALLER

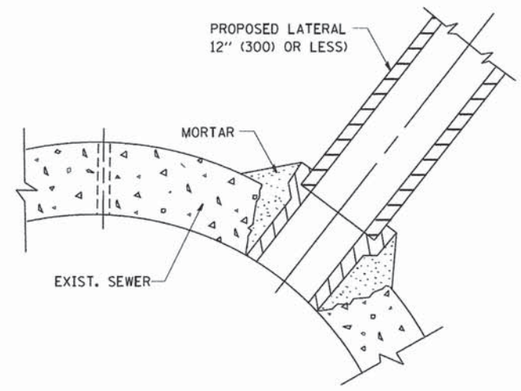


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 I.I (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERANCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL
CONNECTION TO EXISTING SEWER
OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.

II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:

A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".

B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

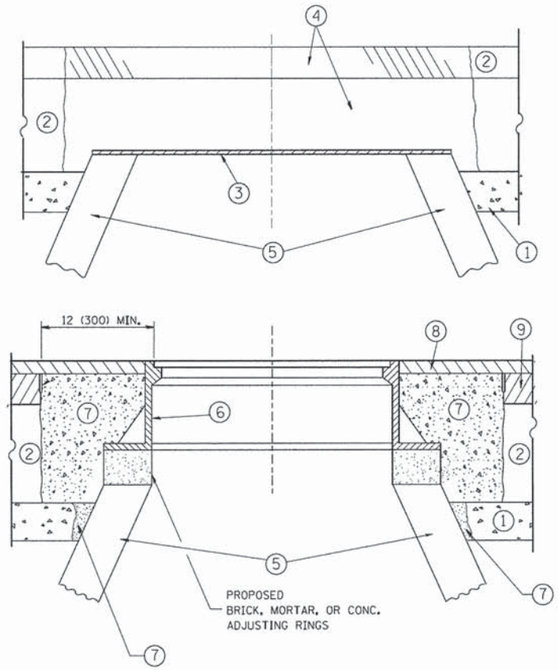
REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd07.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. SHAH 09-09-94					1339/1331	15-00062-00-RS	COOK	54	44
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - R. SHAH 10-25-94						BD500-01 (BD-7)		CONTRACT NO. 61C79	
	PLOT DATE = 1/4/2008	DATE - 07-25-90	REVISED - R. SHAH 06-12-96		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.		FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-1* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

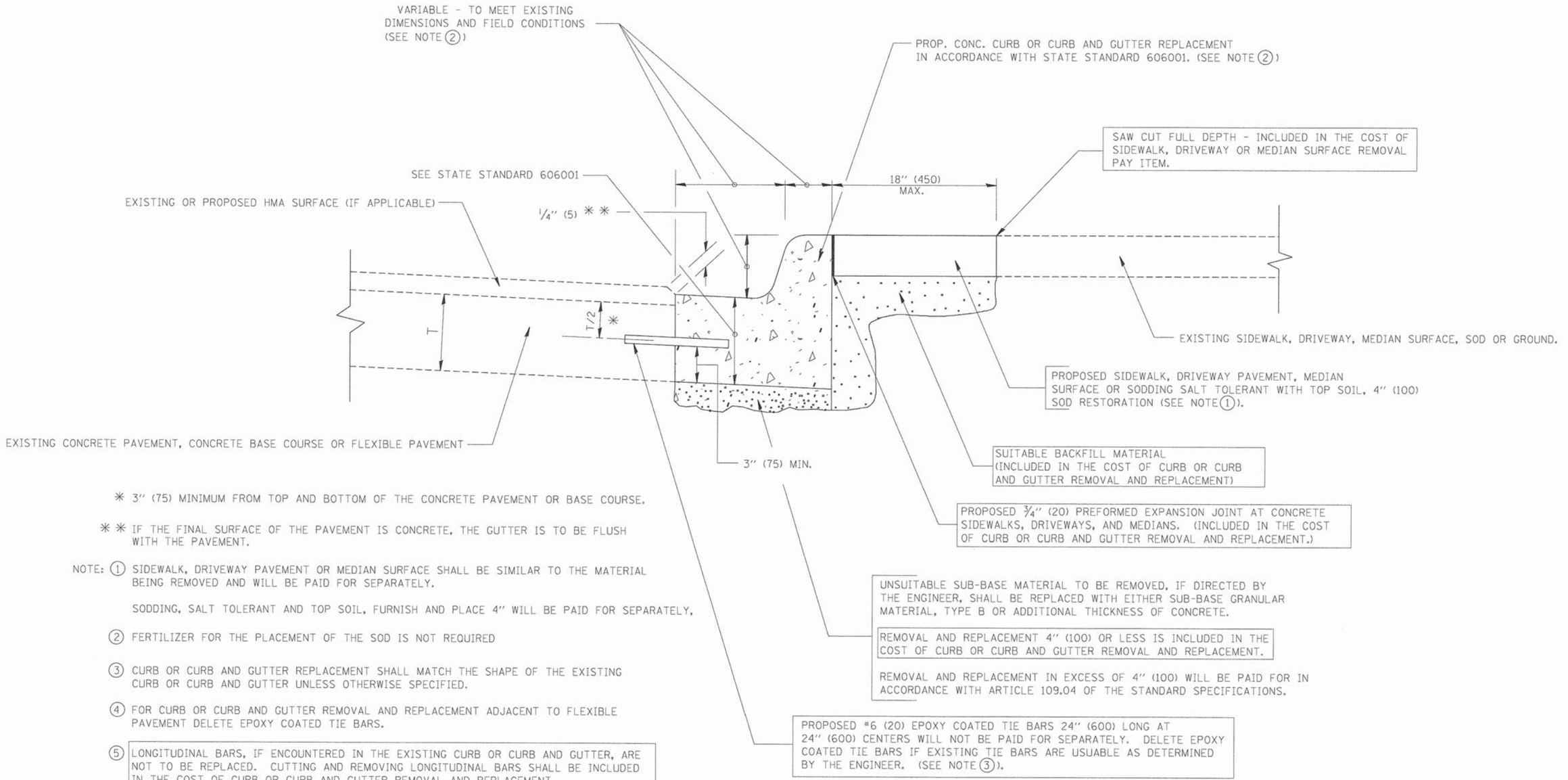
FILE NAME = c:\pw_work\pwidot\bauerdl\d0128315\bd08.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1968.5000 ' / m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	45
	BD600-03 (BD-8)	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

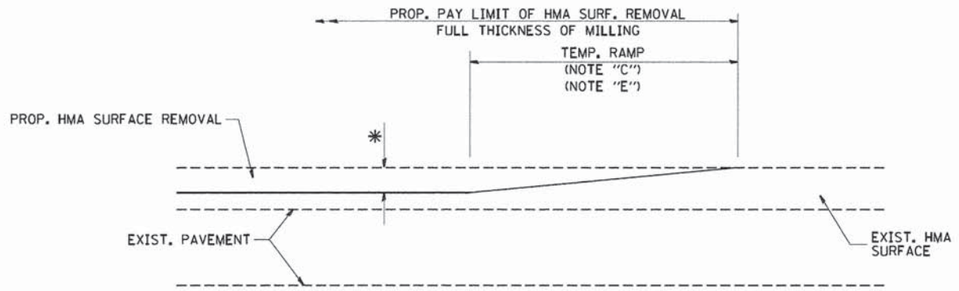
FILE NAME = c:\pw_work\pwidot\drivakosgn\d0188315\bd24.dgn	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

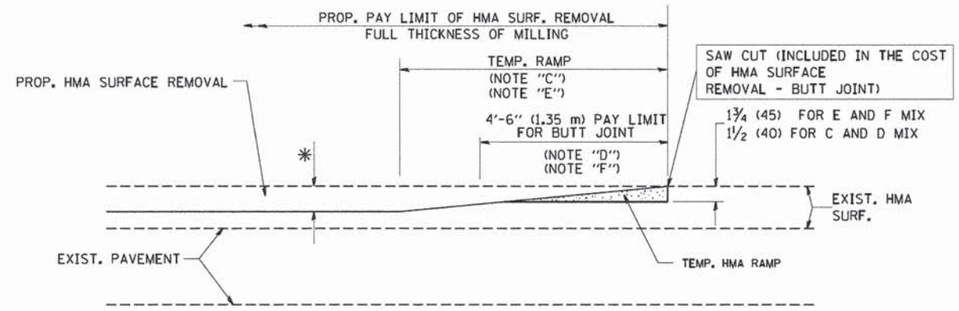
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	46
	BD600-06 (BD-24)	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

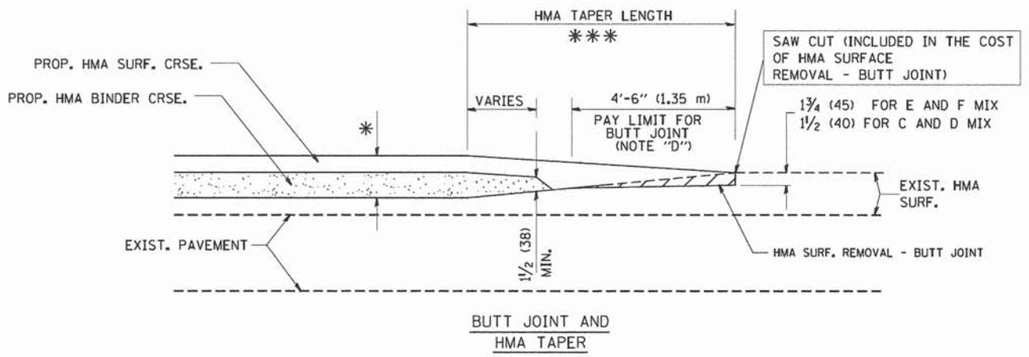


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

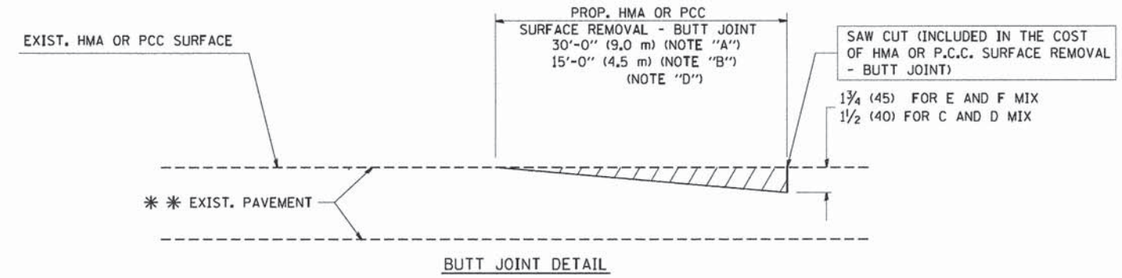
OPTION 2

TYPICAL TEMPORARY RAMP

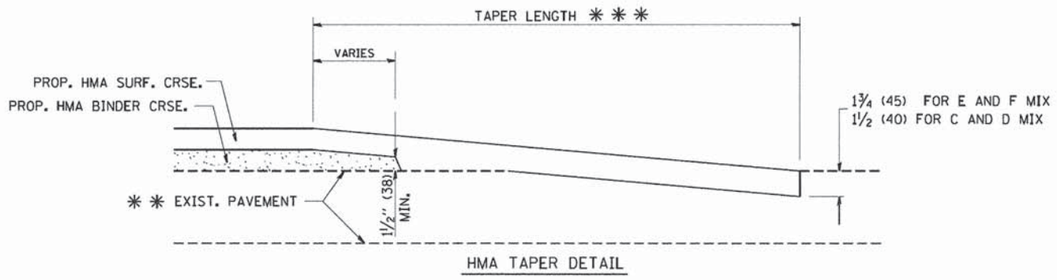


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

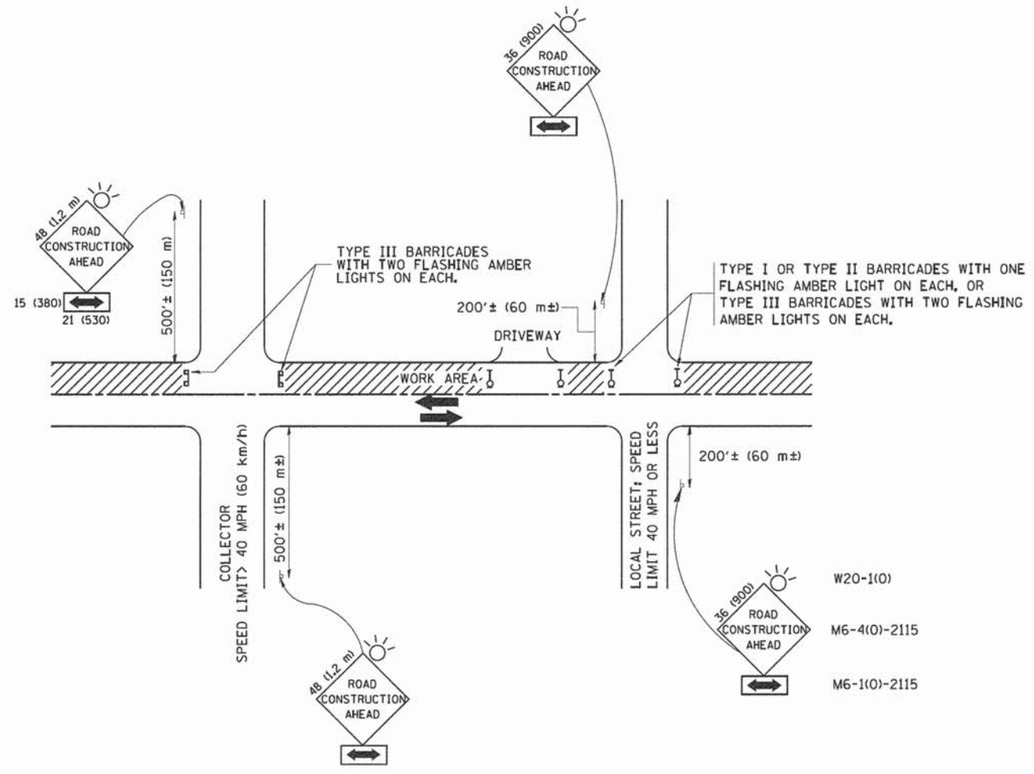
FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
6339/1331	15-00062-00-RS	COOK	54	47
	BD400-05 BD32	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

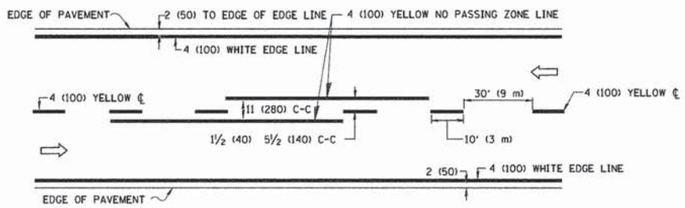
FILE NAME = W:\diststa\22x34\tc10.dgn	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 '/ IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

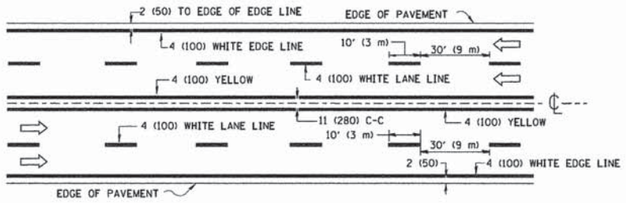
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

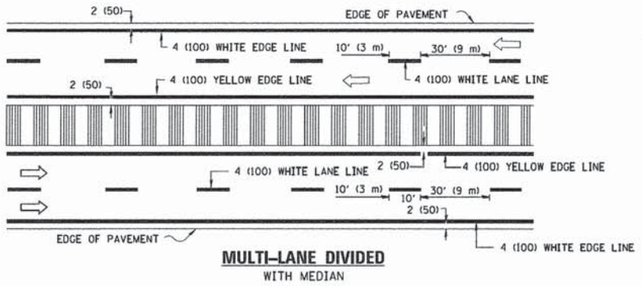
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	48
	TC-10	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		



2–LANE ROADWAY

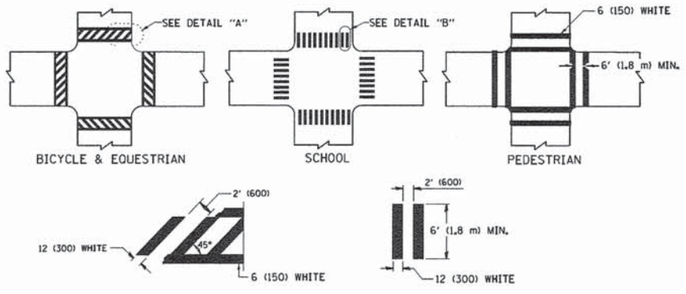


MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

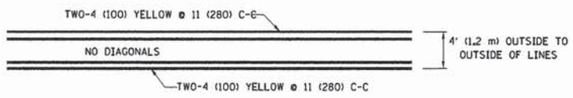
TYPICAL LANE AND EDGE LINE MARKING



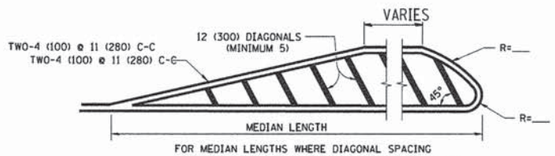
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



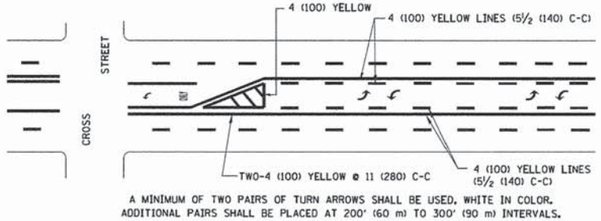
4' (1.2 m) WIDE MEDIANS ONLY



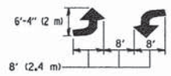
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

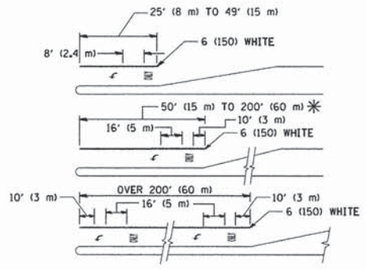


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

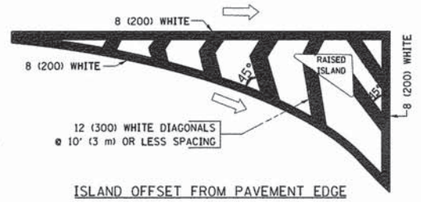


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m²) ↑ ONLY AREA = 20.8 SQ. FT. (1.9 m²)

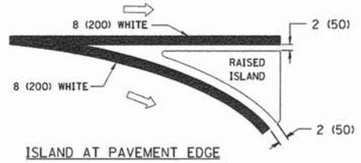
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

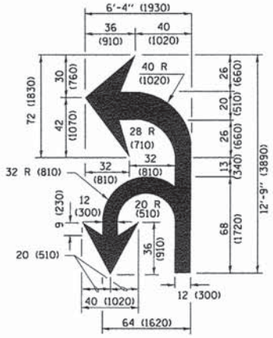


ISLAND OFFSET FROM PAVEMENT EDGE

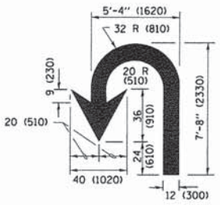


ISLAND AT PAVEMENT EDGE

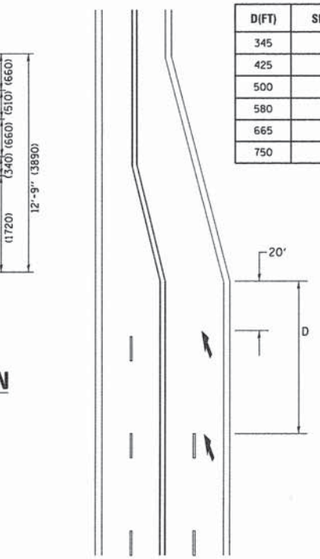
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

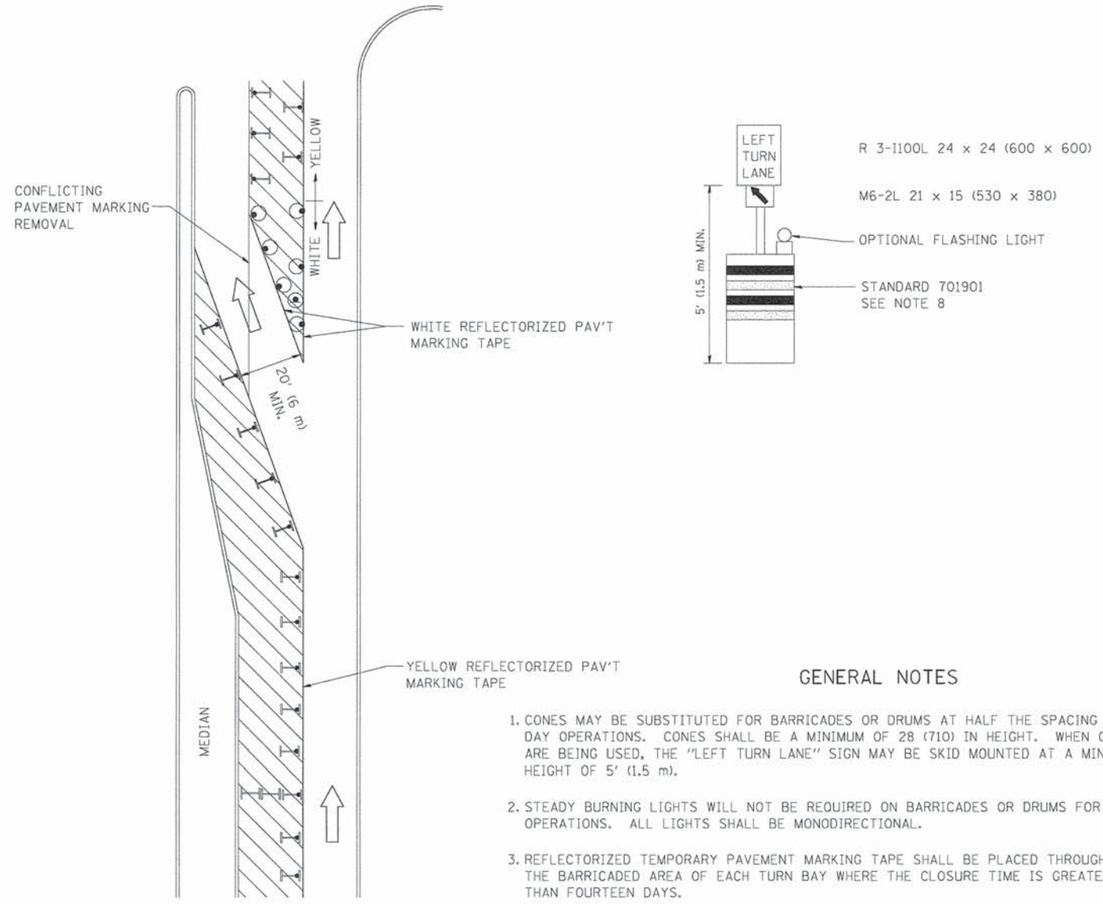
FILE NAME = pw:\\IL084EBIDINTEG.illinois.govPWIDOT\Documents\IDOT Offices\District 1\Projects\District 1 CADData\CADsheets\tc13.dgn	USER NAME = lszekrf	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
Default	PLOT SCALE = 50.000 ' / in.	DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT DATE = 12/21/2015	CHECKED -	REVISED - C. JUCIUS 07-01-13
		DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5339/1331	15-00062-00-RS	COOK	54	49
	TC-13	CONTRACT NO. 61C79		
		ILLINOIS FED. AID PROJECT M-4003(683)		



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

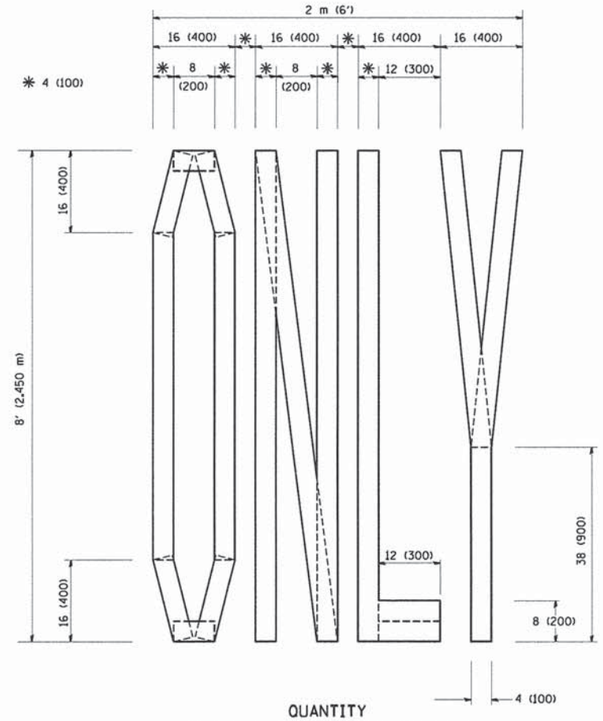
FILE NAME = c:\pw_work\PWIDOT\DRIVAKOSGN\d0180315\tc14.dgn	USER NAME = drivakosgn	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	PLOT SCALE = 49.9999 ' / IN.	REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT DATE = 9/14/2009	REVISED - A. HOUSEH 10-12-96	REVISED -
		REVISED -T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

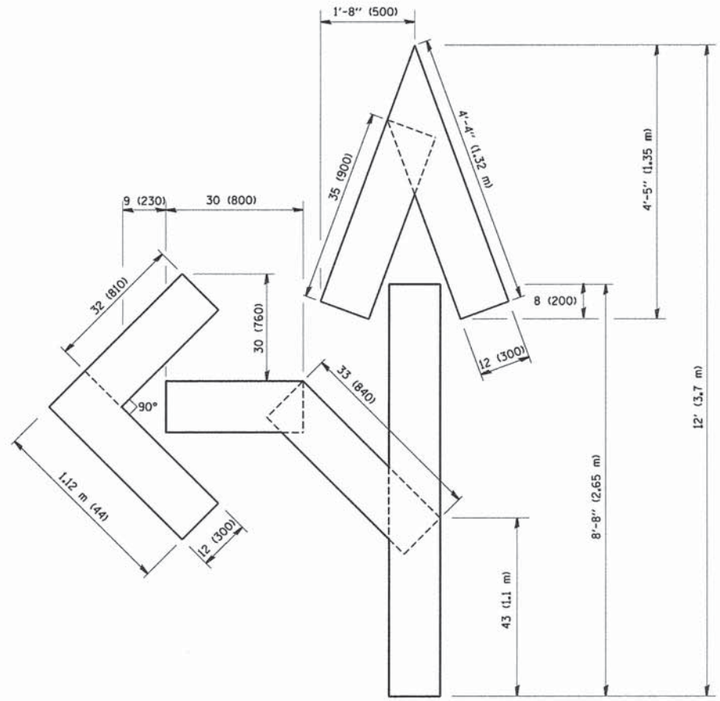
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

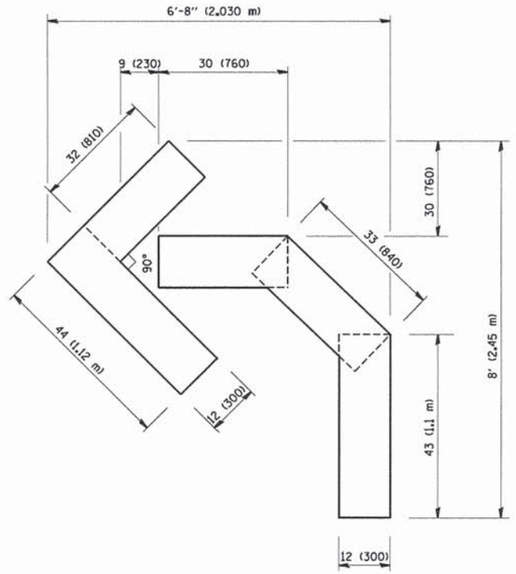
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	50
	TC-14	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



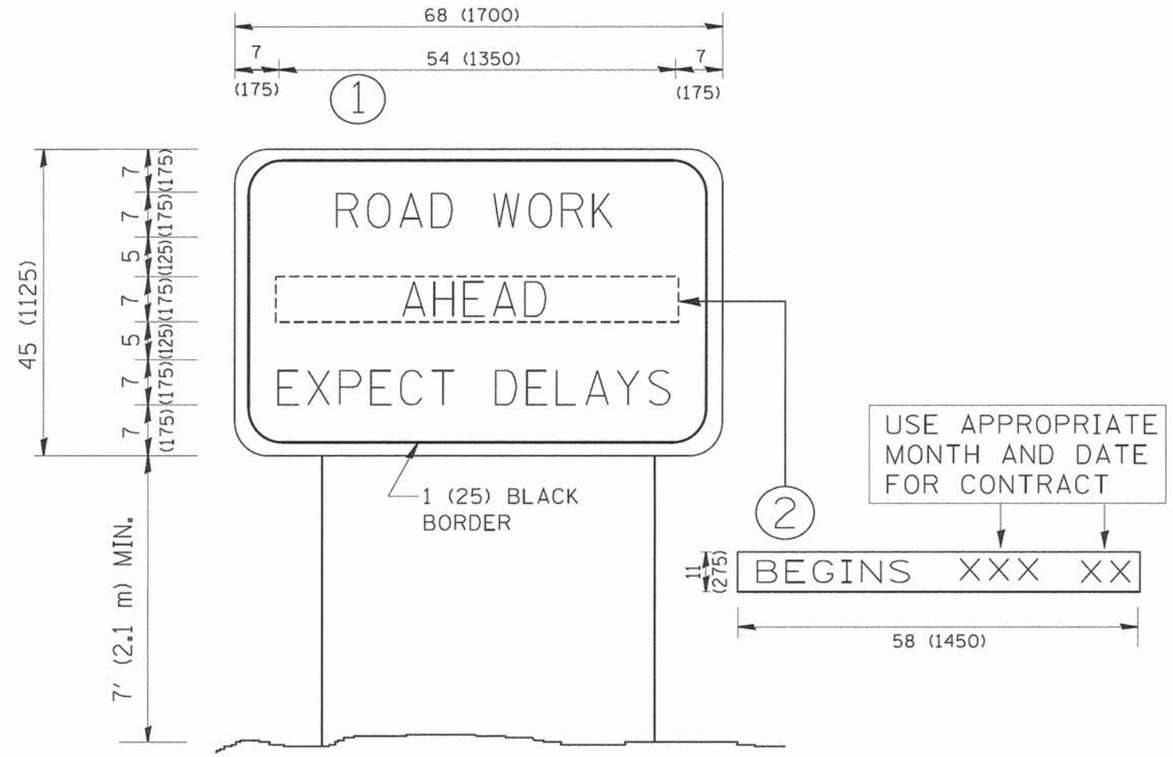
QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststa\22x34\tc16.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -T. RAMMACHER 11-04-97						1339/1333	15-00062-00-RS	COOK	54	51
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98							TC-16		CONTRACT NO. 61C79	
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststa\22x34\tc22.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1333	15-00062-00-RS	COOK	54	52
	TC-22	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		

3.0'' RADIUS, 0.5'' BORDER, WHITE ON GREEN; REFLECTORIZED
''DRIVEWAY'' D; ''ENTRANCE'' D; STANDARD ARROW CUSTOM 12.0'' x 5.0''

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM ''TEMPORARY INFORMATION SIGNING''.

FILE NAME = c:\pw_work\pwidot\gaglianobt\d0108315\tc26.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
		DRAWN -	REVISED -
	PLOT SCALE = 50.000 '/ in.	CHECKED -	REVISED -
	PLOT DATE = 12/13/2012	DATE -	REVISED -

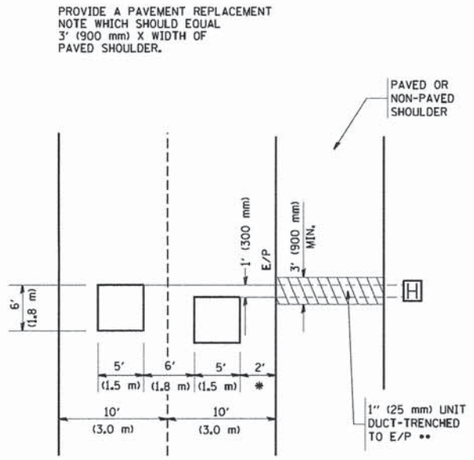
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
(339/333)	15-00062-00-RS	COOK	54	53
	TC-26	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		

LOOPS NEXT TO SHOULDERS

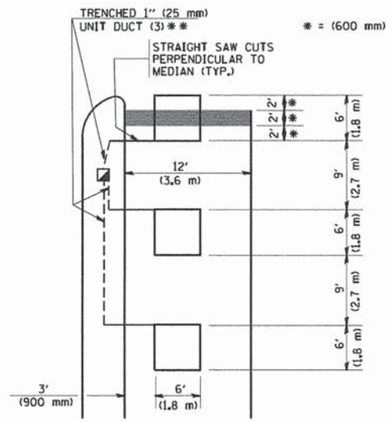


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

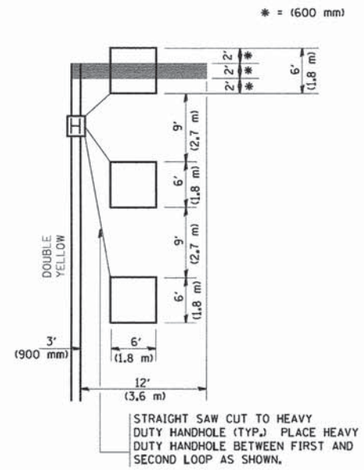


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

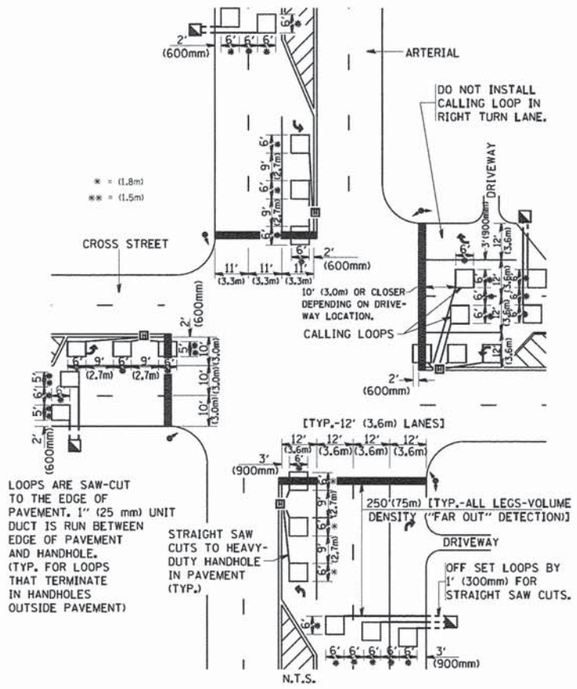
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



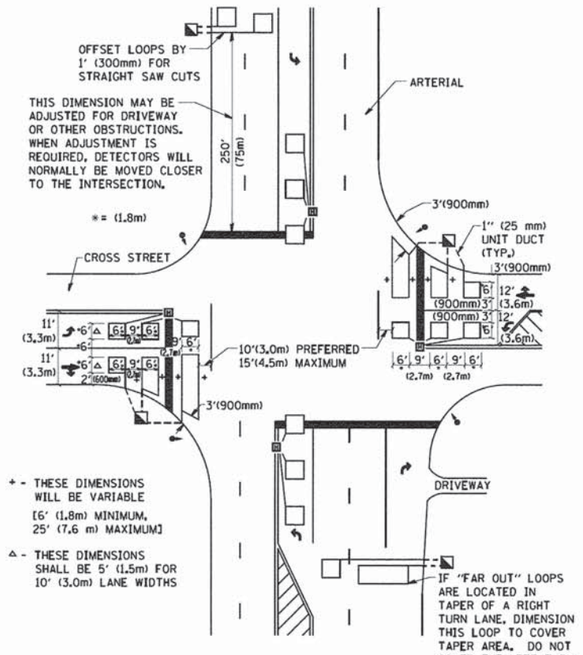
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn	USER NAME = geglienobt	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - R.K.F.	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1339/1331	15-00062-00-RS	COOK	54	54
	TS-07	CONTRACT NO. 61C79		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(683)		