

|                     |                |                    |              |           |
|---------------------|----------------|--------------------|--------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY             | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT              | 76           | 1         |
| FED. ROAD DIST. NO. | ILLINOIS       | CONTRACT NO. 91516 |              |           |

INDEX OF SHEETS 06-10-2016 LETTING ITEM 161

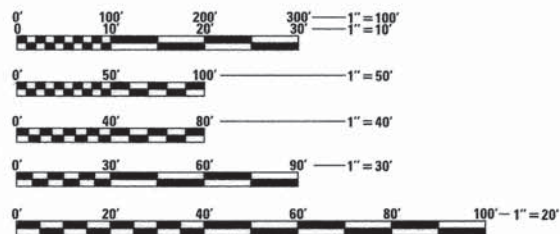
**SHEET NO. DESCRIPTION**

- 1. COVER SHEET
- 2. LOCATION MAP
- 3. GENERAL NOTES AND HIGHWAY STANDARDS
- 4.-9. SUMMARY OF QUANTITIES
- 10.-26. TYPICAL SECTIONS
- 27. ALIGNMENT AND CONTROL
- 28.-36. SCHEDULES OF QUANTITIES
- 37. REMOVAL PLAN PIATT STREET
- 38. REMOVAL PLAN WASHINGTON STREET
- 39. REMOVAL PLAN INDEPENDENCE STREET
- 40.-41. REMOVAL PLAN MARION STREET
- 42.-44. REMOVAL PLAN ALLERTON ROAD
- 45. PROPOSED PLAN PIATT STREET
- 46. PROPOSED PLAN WASHINGTON STREET
- 47. PROPOSED PLAN INDEPENDENCE STREET
- 48.-49. PROPOSED PLAN MARION STREET
- 50.-52. PROPOSED PLAN ALLERTON ROAD
- 53. INTERSECTION DETAILS PIATT STREET
- 54. INTERSECTION DETAILS WASHINGTON STREET
- 55.-57. INTERSECTION DETAILS INDEPENDENCE STREET
- 58.-59. INTERSECTION DETAILS MARION STREET
- 60. PAVEMENT MARKING INDEPENDENCE STREET
- 61.-62. PAVEMENT MARKING MARION STREET
- 63.-65. PAVEMENT MARKING ALLERTON ROAD
- 66.-67. INCIDENTAL HMA SURFACING DETAILS
- 68. SIDEWALK CURB RAMP DETAILS
- 69. PCC DRIVEWAY PAVEMENT DETAILS
- 70. MISCELLANEOUS DETAILS
- 71.-76. DISTRICT 5 DETAILS

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
**PROPOSED**  
**HIGHWAY PLANS**  
**CITY OF MONTICELLO**  
**VARIOUS ROUTES**  
**SECTION 14-00023-00-RS**  
**PROJECT M-5090(003)**  
**RESURFACING (LAFO)**  
**PIATT COUNTY**  
**JOB NO. C-95-320-14**  
**FUNDING: FEDERAL STU AND MFT**



| ROUTE                              | FUNCTIONAL CLASSIFICATION | CURRENT ADT |
|------------------------------------|---------------------------|-------------|
| ALLERTON ROAD (FAU 7540 / FAS 541) | MAJOR COLLECTOR           | 800         |
| MARION STREET (FAU 7538)           | MAJOR COLLECTOR           | 2,750       |
| INDEPENDENCE STREET                | LOCAL STREET              | 750         |
| PIATT STREET (FAU 7544)            | MAJOR COLLECTOR           | 1,450       |
| WASHINGTON STREET (FAU 7536)       | MAJOR COLLECTOR           | 1,850       |



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

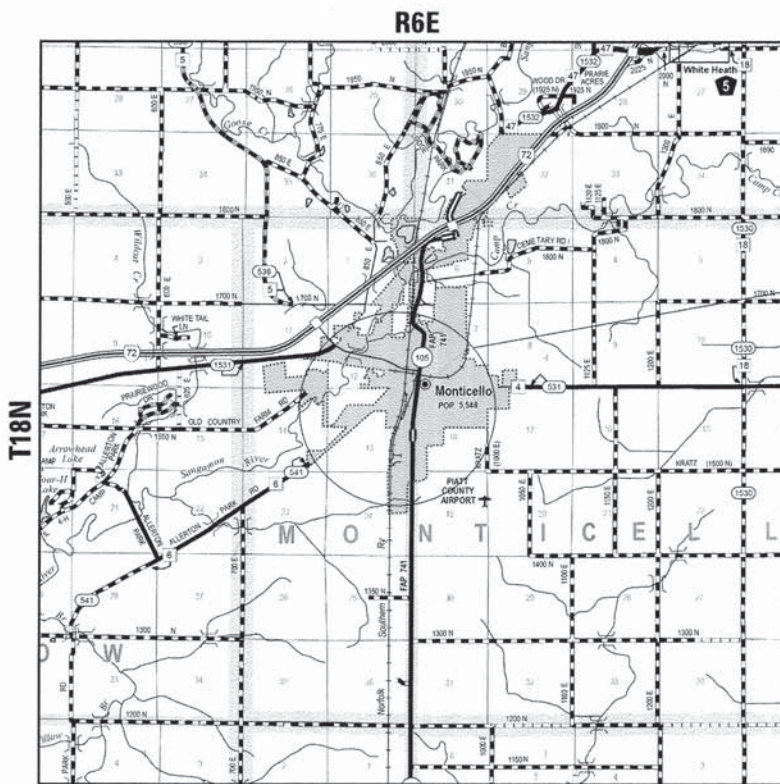
J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

CITY OF MONTICELLO  
 (217) 762-2583

CONTRACT NO.: 91516



*James M. Sullivan* DATE: 3/2/2016  
 ILLINOIS PROFESSIONAL LICENSE NO. 44479  
 (EXPIRATION DATE: 11-30-17)



**LOCATION MAP**  
 NOT TO SCALE

N  
  
 PROJECT LOCATION  
 SEE SHEET NO. 2 FOR  
 DETAILED LOCATION MAP

APPROVED:

Mayor, City of Monticello

3/3/16  
 Date

PASSED MARCH 8 2016

DISTRICT FIVE ENGINEER OF  
 LOCAL ROADS & STREETS

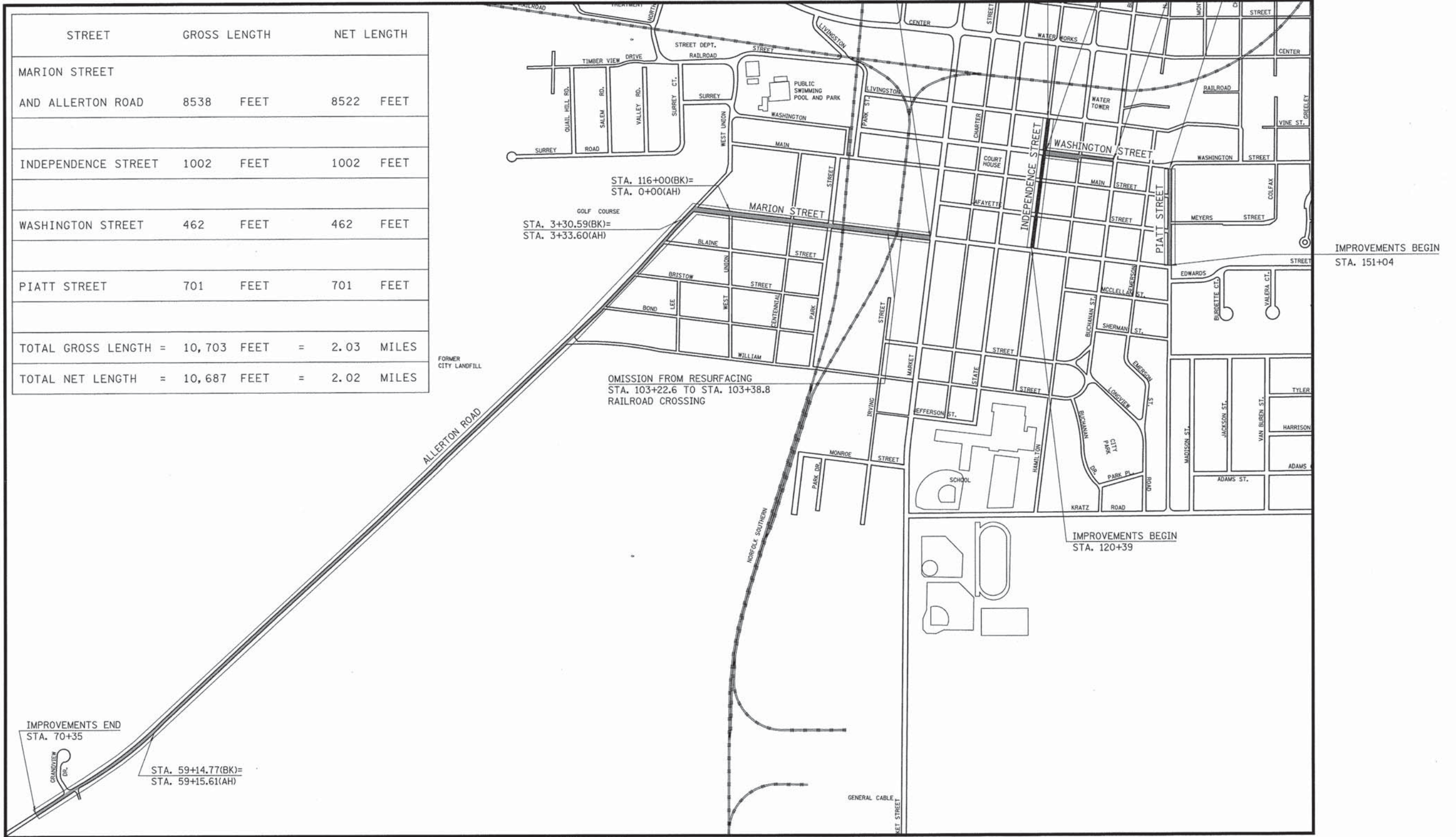
Releasing For Bid Based on Limited Review March 8 2016

REGION THREE ENGINEER  
 STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**PRINTED BY THE AUTHORITY  
 OF THE STATE OF ILLINOIS**



| STREET                             | GROSS LENGTH | NET LENGTH   |
|------------------------------------|--------------|--------------|
| MARION STREET<br>AND ALLERTON ROAD | 8538 FEET    | 8522 FEET    |
| INDEPENDENCE STREET                | 1002 FEET    | 1002 FEET    |
| WASHINGTON STREET                  | 462 FEET     | 462 FEET     |
| PIATT STREET                       | 701 FEET     | 701 FEET     |
| TOTAL GROSS LENGTH =               | 10,703 FEET  | = 2.03 MILES |
| TOTAL NET LENGTH =                 | 10,687 FEET  | = 2.02 MILES |



PRINT DRIVER = L:\PLOT\081616\081616.dwg  
 PLOT DATE = 3/2/2016 10:39:59 AM  
 PLOT SCALE = 8.8833 / 1 in.  
 FILE NAME = C:\PLOT\081616\081616.dwg



|                                  |                |           |
|----------------------------------|----------------|-----------|
| USER NAME = skm                  | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63          | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 8.8833 / 1 in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/2/2016 10:39:59 AM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**LOCATION MAP**

SCALE 1" = 400' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                       | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 2         |
| CONTRACT NO. 91516                              |                |        |              |           |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        |              |           |



**GENERAL NOTES**

- WHEREVER IN THE PLANS AND SPECIFICATIONS THE TERM "STANDARD SPECIFICATION" IS USED IT SHALL BE UNDERSTOOD TO MEAN THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" AS PREPARED BY THE DEPARTMENT OF TRANSPORTATION OF THE STATE OF ILLINOIS AND ADOPTED ON APRIL 1, 2016.
- ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS SHALL BE INTERPRETED TO BE THE LATEST STANDARDS OF THE DEPARTMENT OF TRANSPORTATION WITH THE REVISION NUMBER AS SHOWN ON SHEET NO. 3 OF THE PLANS.
- WHERE SECTION OR SUBSECTION MONUMENTS, BENCHMARKS, OR IRON PIPE MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL MONUMENTS UNTIL AN ILLINOIS REGISTERED LAND SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN ILLINOIS REGISTERED LAND SURVEYOR REESTABLISH ANY MONUMENTS DESTROYED BY HIS OR HER OPERATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING, STORING AND RESETTING ALL SIGNS AFFECTED BY THE CONSTRUCTION. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS AND THE COST TO PERFORM THE WORK WILL BE CONSIDERED INCLUDED IN THE UNIT BID PRICE OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS. THE JULIE NUMBER IS 1-800-892-0123. A MINIMUM OF 48 HOURS ADVANCE NOTICE IS REQUIRED.
- ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH STATE AND LOCAL REGULATIONS REGARDING AIR, WATER AND NOISE POLLUTION.
- ALL SALVAGEABLE FRAMES AND GRATES NOT INCORPORATED IN THE WORK SHALL REMAIN THE PROPERTY OF THE CITY OF MONTICELLO.
- NO SIDEWALK REMOVALS WILL BE PERMITTED UNTIL AN APPROVED ROOT-SAW DEVICE HAS BEEN USED TO CUT TREE ROOTS ON BOTH SIDES OF THE SIDEWALK TO BE REMOVED IN AREAS WHERE TREE ROOTS MAY BE PRESENT. SUCH DEVICE SHALL BE EQUIVALENT TO A VERMEER MODEL V-1550, DISC TRENCHER / ROOT SAW, CAPABLE OF CUTTING PARALLEL TO THE SIDEWALK AT A DISTANCE OF 6 INCHES AND TO A DEPTH OF AT LEAST 12 INCHES. THIS WORK WILL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL GUARD PLASTIC (FRESH) CONCRETE UNTIL IT SETS AND HARDENS SUFFICIENTLY TO PREVENT PEOPLE FROM WRITING, WALKING, RIDING BICYCLES OR OTHERWISE MARKING OR DEFACING THE CONCRETE IN A PERMANENT FASHION. A PERMANENT FASHION IS CONSIDERED A DEPRESSION IN THE FRESH CONCRETE SURFACE. CONCRETE SO MARKED SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR. THE EXTENT OF THE AREA TO BE REMOVED WILL BE AT THE SOLE DISCRETION OF THE ENGINEER, BUT IN NO CASE SHALL BE LESS THAN ONE FULL PANEL OF SIDEWALK OR PAVEMENT. THE COST OF PROTECTING THE CONCRETE AND THE COST OF REPLACING DEFACED CONCRETE WILL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- LOOSE, DISTURBED SOIL SHALL BE REMOVED FROM EXCAVATED AREAS PRIOR TO COMPACTION OF THE SUBGRADE. EXCAVATED OR DISTURBED SOIL SHALL NOT BE USED AS A BASE FOR PROPOSED PORTLAND CEMENT CONCRETE CONSTRUCTION, HMA CONSTRUCTION OR AGGREGATE BASE / SUBBASE CONSTRUCTION.
- EXISTING CONCRETE CURBS THAT HAVE BEEN POURED MONOLITHIC WITH THE EXISTING SIDEWALKS SHALL BE REMOVED AND THE WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR SIDEWALK REMOVAL. THE TOP SURFACE AREA OF THE CURB WILL BE MEASURED AND INCLUDED FOR PAYMENT.
- EXISTING CONCRETE CURBS THAT HAVE BEEN POURED MONOLITHIC WITH THE EXISTING PCC DRIVEWAYS SHALL BE REMOVED AND THE WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR DRIVEWAY PAVEMENT REMOVAL. THE TOP SURFACE AREA OF THE CURB WILL BE MEASURED AND INCLUDED FOR PAYMENT.
- BUTT JOINT LOCATIONS ASSOCIATED WITH THE HMA AND PCC SURFACE REMOVAL ARE SHOWN ON THE PLANS. A VERTICAL FACE TO THE SPECIFIED DEPTH OF SURFACE REMOVAL SHALL BE PROVIDED AT THE BUTT JOINTS.
- ENGLISH UNITS OF MEASUREMENT SHALL GOVERN OVER AND SUPERSEDE ANY METRIC UNITS SHOWN IN THIS CONTRACT. WHERE INCLUDED, METRIC UNITS ARE FOR INFORMATION ONLY.
- ALL ELEVATIONS SHOWN IN THE PLANS ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988. (NAVD 88)
- THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE LOCAL RAILROAD CONTACT IS:

MS. CARRIE REYNOLDS  
ASSISTANT DIVISION OFFICE MANAGER  
ILLINOIS DIVISION  
NORFOLK SOUTHERN RAILWAY COMPANY  
1735 EAST CONDUIT STREET  
DECATUR, IL. 62521  
(217) 425-2063

SPECIAL ATTENTION IS CALLED TO ARTICLE 107.12 REGARDING RAILROAD FLAGGERS.  
THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE RAILROAD FLAGGER CONTACT IS:

MS. CARRIE REYNOLDS  
ASSISTANT DIVISION OFFICE MANAGER  
ILLINOIS DIVISION  
NORFOLK SOUTHERN RAILWAY COMPANY  
1735 EAST CONDUIT STREET  
DECATUR, IL. 62521  
(217) 425-2063

- GRADING SHALL BE DONE BY HAND AROUND LIGHT POLES, UTILITY POLES, SIGN POSTS, SHRUBS, TREES OR OTHER NATURAL OR MAN-MADE OBJECTS WHERE SHALLOW FILLS OR CUTS ARE ADJACENT TO THE ITEMS. IT IS THE INTENT THAT THE LIMITS OF CONSTRUCTION BE SUCH AS TO PRESERVE IN THE ORIGINAL STATE AS MUCH AREA OF TEMPORARY EASEMENTS AS POSSIBLE. THE DECISION AS TO ITEMS TO REMAIN IN PLACE SHALL BE AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE QUANTITIES INCLUDED IN THE PLANS FOR HOT-MIX ASPHALT RESURFACING ARE INTENDED TO GIVE THE COVERAGE SHOWN ON THE TYPICAL CROSS SECTIONS. IT IS NOT INTENDED TO INCREASE THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE IN ORDER TO USE ALL OF THE QUANTITIES INCLUDED IN THE CONTRACT.
- MIXTURE REQUIREMENTS

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

| MIXTURE USE(S):            | SURFACE COURSE & INCIDENTAL | F.G. LEVELING BINDER | CLASS D PATCHING |
|----------------------------|-----------------------------|----------------------|------------------|
| AC/PG:                     | PG 64-22                    | PG 64-22             | PG 64-22         |
| DESIGN AIR VOIDS:          | 4% @ NDES = 50              | 4% @ NDES = 50       | 4% @ NDES = 50   |
| MIXTURE COMPOSITION:       | IL 9.5                      | IL 9.5 FG            | IL 19            |
| FRICTION AGGREGATE:        | MIX C                       | MIX C                | N.A.             |
| MIXTURE WEIGHT             | 112                         | 112                  | 112              |
| QUALITY MANAGEMENT PROGRAM | QC/QA                       | QC/QA                | QC/QA            |
| SUBLOT SIZE                | N.A.                        | N.A.                 | N.A.             |

- THE EXISTING TIE BARS BETWEEN THE EXISTING PAVEMENT AND EXISTING MEDIANS, GUTTERS AND/OR COMBINATION CURB AND GUTTERS THAT ARE FOUND SUITABLE FOR REUSE SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY EXISTING TIE BARS THAT ARE FOUND UNSUITABLE TO BE INCORPORATED INTO THE PROPOSED CONSTRUCTION DUE TO EXCESSIVE RUSTING OR DISTRESS SHALL BE REMOVED FLUSH WITH THE FACE OF THE EXISTING CONCRETE AND DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE VARIOUS REMOVAL PAY ITEMS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE PATCHING SCHEDULES INCLUDED IN THE PLANS REPRESENT THE BEST INFORMATION AVAILABLE AT THE TIME OF COMPLETION OF THE PLANS FOR LETTING. VARIATIONS IN LOCATION AND SIZES OF FULL-DEPTH PATCHES MAY OCCUR.
- SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE PAVEMENT AFTER ANY OF THE FOLLOWING: COLD MILLING AND/OR PLACING BITUMINOUS MATERIALS (TACK COAT), LEVELING BINDER (MACHINE METHOD), BINDER AND SURFACE COURSES. SHORT TERM PAVEMENT MARKING PLACED ON THE SURFACE, SHALL COINCIDE WITH THE FINAL PAVEMENT STRIPING. SHORT TERM PAVEMENT MARKING PLACED PRIOR TO THE SURFACE SHALL COINCIDE WITH THE EXISTING PAVEMENT MARKINGS. USE 4 FEET PER 40 FEET (OR 10% PER STATION).
- COARSE AGGREGATE GRADATION CA-10 MAY BE USED WHENEVER COARSE AGGREGATE CA-6 IS SPECIFIED IN THE STANDARD SPECIFICATIONS.
- TEMPORARY RAMPS SHALL BE CONSTRUCTED AT ALL BUTT JOINT LOCATIONS ACCORDING TO ARTICLE 406.08 OF THE STANDARD SPECIFICATIONS AND PAID FOR ACCORDING TO ARTICLE 406.14.
- TEMPORARY RAMPS SHALL BE CONSTRUCTED TO MAINTAIN ACCESS TO ENTRANCES AND FOR THE ADA RAMPS. COLD-MILL HMA TAILINGS SUITABLE TO THE ENGINEER AND COLD MIX WILL BE ACCEPTABLE AT THESE LOCATIONS TO PROVIDE A MAXIMUM LIP HEIGHT OF 1/2". THE TEMPORARY RAMPS SHALL BE REMOVED JUST PRIOR TO PLACING THE PROPOSED HMA LEVEL BINDER AND SURFACE. THIS WORK WILL NOT BE MEASURED AND PAID FOR SEPARATELY AND SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

**ILLINOIS HIGHWAY STANDARD DRAWINGS**

| STANDARD NO.  | DESCRIPTION  |
|---|--|
| 000001-06   | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS  |
| 001001-02<br>001006   | AREAS OF REINFORCEMENT BARS<br>DECIMAL OF AN INCH AND OF A FOOT  |
| 280001-07   | TEMPORARY EROSION CONTROL SYSTEMS  |
| 420001-08   | PAVEMENT JOINTS  |
| 424001-08<br>424021-03<br>442201-03   | PERPENDICULAR CURB RAMPS FOR SIDEWALKS<br>DEPRESSED CORNER FOR SIDEWALKS<br>CLASS C AND D PATCHES  |
| 602301-04<br>604001-04<br>604056-04<br>604086-03<br>606001-06   | INLET - TYPE A<br>FRAME AND LIDS TYPE 1<br>FRAME AND LIDS TYPE 11V<br>FRAME AND LIDS TYPE 23<br>CONCRETE CURB TYPE B AND<br>COMBINATION CONCRETE CURB AND GUTTER   |
| 701006-05<br>701201-04<br>701301-04<br>701306-03<br>701311-03<br>701501-06<br>701801-06<br>701901-05<br>720001-01<br>720006-04<br>780001-05 | OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE<br>LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS > 45 MPH<br>LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS<br>LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45 MPH<br>LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY<br>URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED<br>SIDEWALK, CORNER OR CROSSWALK CLOSURE<br>TRAFFIC CONTROL DEVICES<br>SIGN PANEL MOUNTING DETAILS<br>SIGN PANEL ERECTION DETAILS<br>TYPICAL PAVEMENT MARKINGS |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES & HIGHWAY STANDARDS

| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| VAR   | 14-00023-00-RS | PIATT  | 76           | 3         |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |

SCALE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

ESCA CONSULTANTS, INC. 1001 W. MONROE ST. SUITE 200 DECATUR, IL 62521  
 PHONE: (217) 425-2063 FAX: (217) 425-2064  
 WWW.ESCA-CONSULTANTS.COM



|                                  |                |           |
|----------------------------------|----------------|-----------|
| USER NAME = skm                  | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63          | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8833' / in.       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 12:59:19 PM | DATE - 02/2016 | REVISED - |



| SUMMARY OF QUANTITIES |   | CONSTRUCTION CODE |                   | 0005   | 0005  | 0005  | 0005  | 0005  |
|-----------------------|---|-------------------|-------------------|--|---|---|---|---|
|                       |   | FUNDING           |                   | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>PIATT<br>STREET | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>WASHINGTON<br>STREET | 100% LOCAL<br>ROADWAY<br>INDEPENDENCE<br>STREET | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>MARION<br>STREET | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>ALLERTON<br>ROAD |
| CODE<br>NO.           | ITEM  | UNIT              | TOTAL<br>QUANTITY |  |   |   |   |   |
| 20800150              | TRENCH BACKFILL                                       | CU YD             | 3                 |  | 2   | 1   |   |   |
| 25000110              | SEEDING, CLASS 1A                                     | ACRE              | 0.25              | 0.03   | 0.03  | 0.15  | 0.04  |   |
| 25000400              | NITROGEN FERTILIZER NUTRIENT                          | POUND             | 23                | 3  | 3   | 14  | 3   |   |
| 25000500              | PHOSPHORUS FERTILIZER NUTRIENT                        | POUND             | 23                | 3  | 3   | 14  | 3   |   |
| 25000600              | POTASSIUM FERTILIZER NUTRIENT                         | POUND             | 23                | 3  | 3   | 14  | 3   |   |
| 25100125              | MULCH, METHOD 3                                       | ACRE              | 0.25              | 0.03   | 0.03  | 0.15  | 0.04  |   |
| 35101800              | AGGREGATE BASE COURSE, TYPE B 6"                      | SQ YD             | 113               |  |   |   | 26  | 87  |
| 40600290              | BITUMINOUS MATERIALS (TACK COAT)                      | POUND             | 20433             | 1283   | 1016  | 2702  | 4494  | 10938   |
| 40600627              | LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N50       | TON               | 1969              | 160  | 60  | 212   | 188   | 1349  |
| 40600982              | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT          | SQ YD             | 74                |  |   |   |   | 74  |
| 40600985              | PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT | SQ YD             | 155               |  |   | 17  | 18  | 120   |
| 40600990              | TEMPORARY RAMP  | SQ YD             | 467               | 100  | 60  | 172   | 122   | 13  |
| 40603310              | HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50          | TON               | 2633              | 160  | 130   | 354   | 627   | 1362  |
| 40800029              | BITUMINOUS MATERIALS (TACK COAT)                      | POUND             | 611               |  | 16  | 40  | 69  | 486   |

⊗ SPECIALTY ITEM

PRINTED BY: JAMES W. BROWN, JR.  
 DATE: 3/1/2016 1:01:08 PM  
 FILE: 14-00023-00-RS.dwg



|                                   |                |           |
|-----------------------------------|----------------|-----------|
| USER NAME = skm                   | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63           | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0833' / 1" = 1/12" | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:01:08 PM   | DATE - 2/16    | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE SHEET NO. 1 OF 6 SHEETS STA. TO STA.

|                     |                |                           |                 |              |
|---------------------|----------------|---------------------------|-----------------|--------------|
| F.A.P.<br>RTE.      | SECTION        | COUNTY                    | TOTAL<br>SHEETS | SHEET<br>NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76              | 4            |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |                 |              |
|                     |                | CONTRACT NO. 91516        |                 |              |



| SUMMARY OF QUANTITIES |  | CONSTRUCTION CODE |                   | 0005   | 0005  | 0005  | 0005  | 0005  |
|-----------------------|--|-------------------|-------------------|--|---|---|---|---|
|                       |  | FUNDING           |                   | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>PIATT<br>STREET | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>WASHINGTON<br>STREET | 100% LOCAL<br>ROADWAY<br>INDEPENDENCE<br>STREET | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>MARION<br>STREET | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>ALLERTON<br>ROAD |
| CODE<br>NO.           | ITEM   | UNIT              | TOTAL<br>QUANTITY |  |   |   |   |   |
| 40800050              | INCIDENTAL HOT-MIX ASPHALT SURFACING               | TON               | 157               |  | 3   | 5   | 8   | 141   |
| 42300200              | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH | SQ YD             | 70                |  | 57  | 13  |   |   |
| 42300400              | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH | SQ YD             | 47                |  |   |   | 47  |   |
| 42400300              | PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH           | SQ FT             | 6526              | 385  | 1201  | 3776  | 1164  |   |
| 42400800              | DETECTABLE WARNINGS                                | SQ FT             | 537               | 40   | 102   | 296   | 99  |   |
| 44000100              | PAVEMENT REMOVAL                                   | SQ YD             | 23                |  | 9   |   | 14  |   |
| 44000155              | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"            | SQ YD             | 1525              |  |   |   | 605   | 920   |
| 44000156              | HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"            | SQ YD             | 5893              |  |   |   | 5893  |   |
| 44000157              | HOT-MIX ASPHALT SURFACE REMOVAL, 2"                | SQ YD             | 1407              |  | 1407  |   |   |   |
| 44000160              | HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"            | SQ YD             | 1900              | 1900   |   |   |   |   |
| 44000200              | DRIVEWAY PAVEMENT REMOVAL                          | SQ YD             | 107               |  | 56  |   | 51  |   |
| 44000500              | COMBINATION CURB AND GUTTER REMOVAL                | FOOT              | 1628              | 94   | 364   | 808   | 362   |   |
| 44000600              | SIDEWALK REMOVAL                                   | SQ FT             | 6027              | 359  | 1170  | 3539  | 959   |   |
| 44201325              | CLASS C PATCHES, TYPE I, 8 INCH                    | SQ YD             | 5                 |  | 3   | 2   |   |   |

⊗ SPECIALTY ITEM

PRINT: 3/1/2016 1:01:23 PM  
 USER: skm  
 PROJECT: 14-00023-00-RS  
 PLOT: 14-00023-00-RS.dwg



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719463         | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.8833' / 1" =     | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:01:23 PM | DATE - 2/16    | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE SHEET NO. 2 OF 6 SHEETS STA. TO STA.

|   |                |        |                    |              |
|---|----------------|--------|--------------------|--------------|
| F.A.P.<br>RTE.                                  | SECTION        | COUNTY | TOTAL<br>SHEETS    | SHEET<br>NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76                 | 5            |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        | CONTRACT NO. 91516 |              |



| SUMMARY OF QUANTITIES |   | CONSTRUCTION CODE |                | 0005                     | 0005                         | 0005                           | 0005                     | 0005                     |
|-----------------------|---|-------------------|----------------|--------------------------|------------------------------|--------------------------------|--------------------------|--------------------------|
|                       |   | FUNDING           |                | 80% FEDERAL<br>20% LOCAL | 80% FEDERAL<br>20% LOCAL     | 100% LOCAL                     | 80% FEDERAL<br>20% LOCAL | 80% FEDERAL<br>20% LOCAL |
| CODE NO.              | ITEM  | UNIT              | TOTAL QUANTITY | ROADWAY<br>PIATT STREET  | ROADWAY<br>WASHINGTON STREET | ROADWAY<br>INDEPENDENCE STREET | ROADWAY<br>MARION STREET | ROADWAY<br>ALLERTON ROAD |
| 44201741              | CLASS D PATCHES, TYPE II, 8 INCH                          | SQ YD             | 39             | 15                       | 10                           | 14                             |                          |                          |
| 44201747              | CLASS D PATCHES, TYPE IV, 8 INCH                          | SQ YD             | 32             | 32                       |                              |                                |                          |                          |
| 44201777              | CLASS D PATCHES, TYPE II, 11 INCH                         | SQ YD             | 12             |                          |                              |                                |                          | 12                       |
| 44201781              | CLASS D PATCHES, TYPE III, 11 INCH                        | SQ YD             | 23             |                          |                              |                                |                          | 23                       |
| 44201783              | CLASS D PATCHES, TYPE IV, 11 INCH                         | SQ YD             | 55             |                          |                              |                                | 55                       |                          |
| 48101200              | AGGREGATE SHOULDERS, TYPE B                               | TON               | 386            |                          |                              |                                |                          | 386                      |
| 54248510              | CONCRETE COLLAR   | CU YD             | 0.3            |                          | 0.3                          |                                |                          |                          |
| 550B0040              | STORM SEWERS, CLASS B, TYPE 1 10"                         | FOOT              | 15             |                          | 10                           | 5                              |                          |                          |
| 550B0050              | STORM SEWERS, CLASS B, TYPE 1 12"                         | FOOT              | 2              |                          | 2                            |                                |                          |                          |
| 60100905              | PIPE DRAINS 4"  | FOOT              | 2              |                          | 2                            |                                |                          |                          |
| 60236825              | INLETS, TYPE A, TYPE 11V FRAME AND GRATE                  | EACH              | 3              |                          | 2                            | 1                              |                          |                          |
| 60255500              | MANHOLES TO BE ADJUSTED                                   | EACH              | 2              |                          |                              |                                | 1                        | 1                        |
| 60255800              | MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID | EACH              | 3              |                          |                              |                                |                          | 3                        |
| 60260100              | INLETS TO BE ADJUSTED                                     | EACH              | 1              |                          |                              |                                | 1                        |                          |

⊕ SPECIALTY ITEM

PRINT CENTER = ESCA CONSULTANTS INC  
 MODEL NAME = R10100  
 SHEET NO. = 14-0023-00-RS



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 71963          | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0833' / in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:02:16 PM | DATE - 2/16    | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE SHEET NO. 3 OF 6 SHEETS STA. TO STA.

|                     |               |                           |              |           |
|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P.<br>RTE.      | SECTION       | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-0023-00-RS | PIATT                     | 76           | 6         |
| FED. ROAD DIST. NO. |               | ILLINOIS FED. AID PROJECT |              |           |
|                     |               | CONTRACT NO. 91516        |              |           |



| SUMMARY OF QUANTITIES |  | CONSTRUCTION CODE |                   | 0005   | 0005  | 0005  | 0005  | 0005  |
|-----------------------|--|-------------------|-------------------|--|---|---|---|---|
|                       |  | FUNDING           |                   | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>PIATT<br>STREET | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>WASHINGTON<br>STREET | 100% LOCAL<br>ROADWAY<br>INDEPENDENCE<br>STREET | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>MARION<br>STREET | 80% FEDERAL<br>20% LOCAL<br>ROADWAY<br>ALLERTON<br>ROAD |
| CODE<br>NO.           | ITEM   | UNIT              | TOTAL<br>QUANTITY |  |   |   |   |   |
| 60260300              | INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID                          | EACH              | 1                 |  | 1   |   |   |   |
| 60261320              | INLETS TO BE ADJUSTED WITH NEW TYPE 11V FRAME AND GRATE                        | EACH              | 12                |  | 1   | 6   | 5   |   |
| 60261530              | INLETS TO BE ADJUSTED WITH NEW TYPE 23 FRAME AND GRATE                         | EACH              | 7                 |  |   | 4   | 3   |   |
| 60266600              | VALVE BOXES TO BE ADJUSTED   | EACH              | 1                 |  |   | 1   |   |   |
| 60500060              | REMOVING INLETS  | EACH              | 1                 |  | 1   |   |   |   |
| 60600095              | CLASS SI CONCRETE (OUTLET)   | CU YD             | 2.2               |  |   |   | 2.2   |   |
| 60603900              | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (ABUTTING EXISTING PAVEMENT) | FOOT              | 998               |  | 106   | 624   | 268   |   |
| 60604500              | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (ABUTTING EXISTING PAVEMENT) | FOOT              | 675               | 94   | 269   | 184   | 128   |   |
| 67100100              | MOBILIZATION   | LSUM              | 1                 | 0.1  | 0.1   | 0.1   | 0.1   | 0.6   |
| 70100450              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201                                | LSUM              | 1                 |  |   |   |   | 1   |
| 70100460              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306                                | LSUM              | 1                 |  |   |   |   | 1   |
| 70102620              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701501                                | LSUM              | 1                 | 0.25   | 0.25  | 0.25  | 0.25  |   |
| 70102640              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801                                | LSUM              | 1                 | 0.25   | 0.25  | 0.25  | 0.25  |   |
| 70300100              | SHORT TERM PAVEMENT MARKING  | FOOT              | 2196              |  | 120   | 132   | 204   | 1740  |

⊗ SPECIALTY ITEM

PRINT: 03/16/2016 10:41:41 AM  
 USER: eskm  
 MODEL: 14-00023-00-RS  
 PLOT: 14-00023-00-RS.dwg  
 PLOT DATE: 3/1/2016 10:41:41 PM



|                                  |                |           |
|----------------------------------|----------------|-----------|
| USER NAME = eskm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63          | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0033' / 1"        | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 10:41:41 PM | DATE - 2/16    | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: SHEET NO. 4 OF 6 SHEETS STA. TO STA.

|   |                |        |                    |           |
|---|----------------|--------|--------------------|-----------|
| F.A.P. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76                 | 7         |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        | CONTRACT NO. 91516 |           |







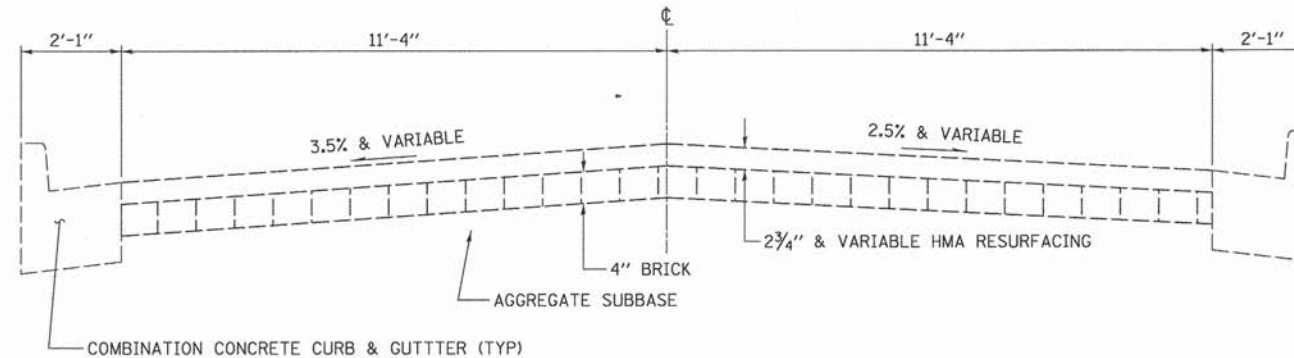




**EXISTING TYPICAL CROSS SECTION ①**

**PIATT STREET**

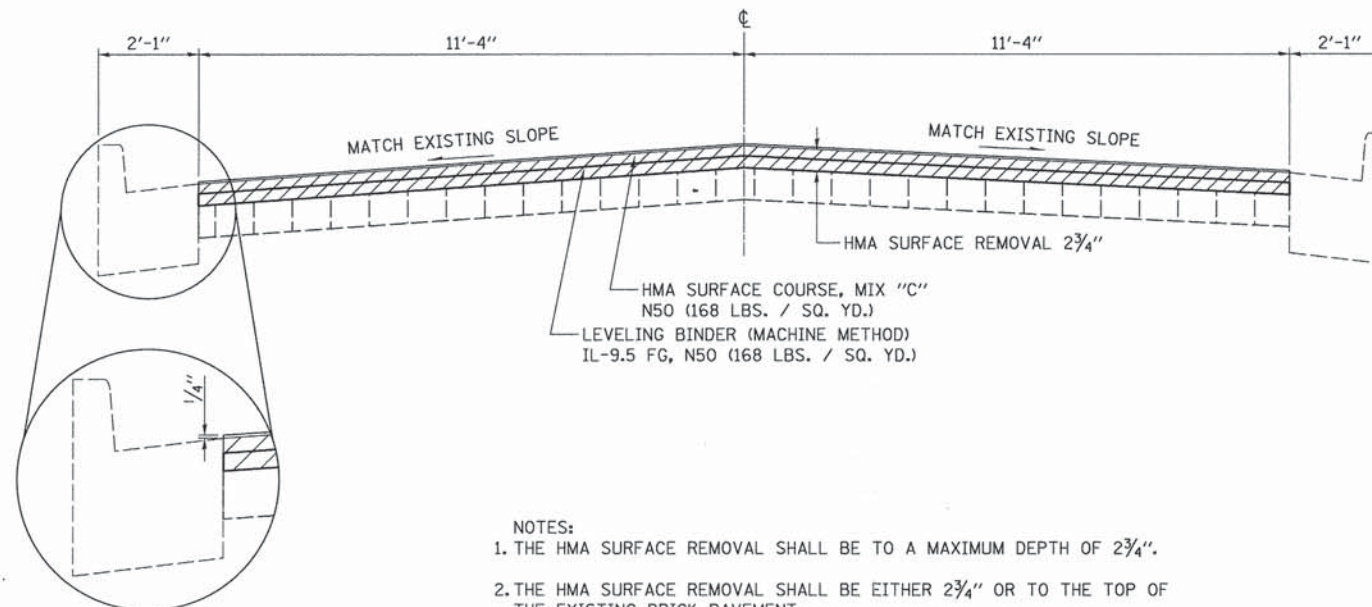
STATION 151+04 TO STATION 158+05



**PROPOSED TYPICAL CROSS SECTION ①**

**PIATT STREET**

STATION 151+04 TO STATION 158+05



HMA RESURFACING DETAIL (TYP)  
SEE STANDARD 606001

- NOTES:
1. THE HMA SURFACE REMOVAL SHALL BE TO A MAXIMUM DEPTH OF 2 3/4".
  2. THE HMA SURFACE REMOVAL SHALL BE EITHER 2 3/4" OR TO THE TOP OF THE EXISTING BRICK PAVEMENT.

PLOT DATE = 3/1/2016 1:22:19 PM  
 USER NAME = skm  
 ESCA PROJECT NO. 719.61  
 PLOT SCALE = 0.8933' / in.  
 DESIGNED - JMS  
 DRAWN - JLF  
 CHECKED - JMS  
 DATE - 01/2016  
 REVISED -  
 REVISED -  
 REVISED -  
 REVISED -



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8933' / in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:22:19 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 1 OF 17 SHEETS STA. TO STA.

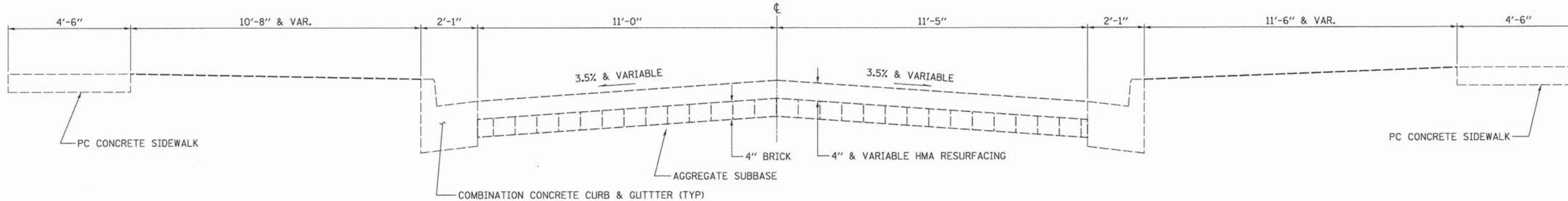
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|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 10        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |



**EXISTING TYPICAL CROSS SECTION ②**

**WASHINGTON STREET**

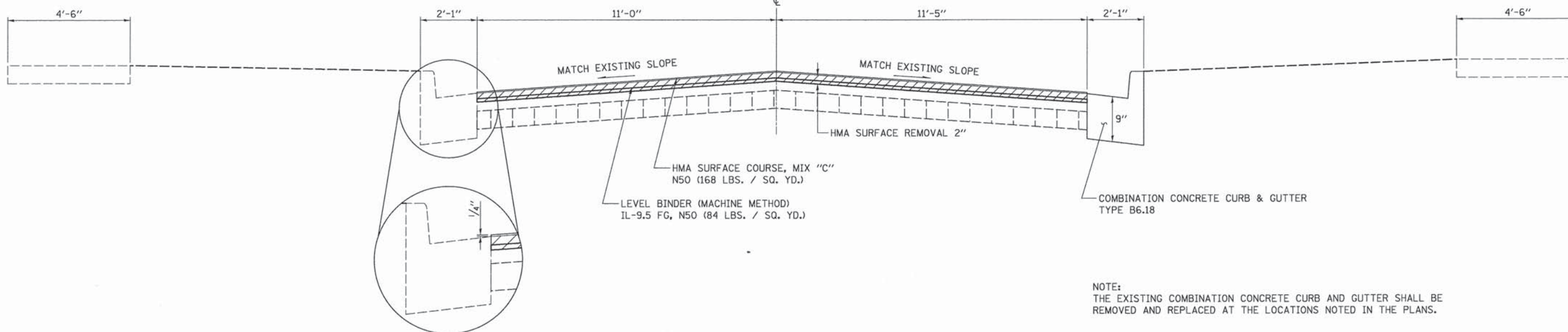
STATION TO STATION  
 294+90 296+00  
 297+00 299+52



**PROPOSED TYPICAL CROSS SECTION ②**

**WASHINGTON STREET**

STATION TO STATION  
 294+90 296+00  
 297+00 299+52



HMA RESURFACING DETAIL (TYP)  
 SEE STANDARD 606001

NOTE:  
 THE EXISTING COMBINATION CONCRETE CURB AND GUTTER SHALL BE REMOVED AND REPLACED AT THE LOCATIONS NOTED IN THE PLANS.

PLOT DATE = 3/1/2016 1:22:27 PM  
 USER NAME = skm  
 ESCA PROJECT NO. 719.61  
 PLOT SCALE = 0.8833' / in.  
 DESIGNED - JMS  
 DRAWN - JLF  
 CHECKED - JMS  
 DATE - 01/2016  
 REVISED -  
 REVISED -  
 REVISED -  
 REVISED -



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8833' / in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:22:27 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 2 OF 17 SHEETS STA. TO STA.

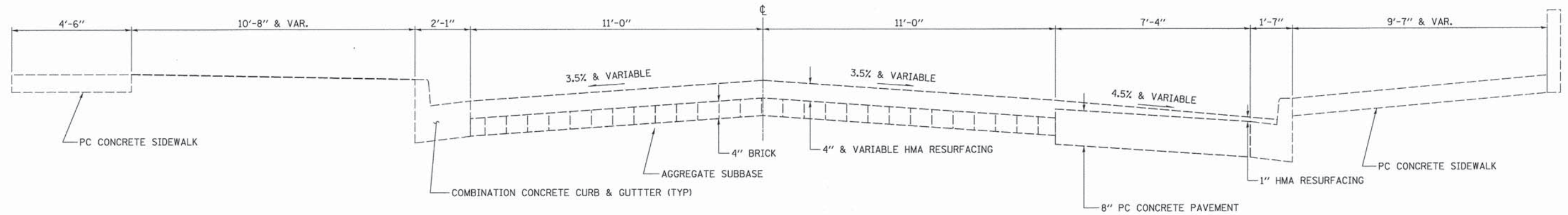
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|---------------------|---------------------------|--------|--------------|-----------|
| F.A. RTE.           | SECTION                   | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS            | PIATT  | 76           | 11        |
| CONTRACT NO. 91516  |                           |        |              |           |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT |        |              |           |



**EXISTING TYPICAL CROSS SECTION ③**

**WASHINGTON STREET**

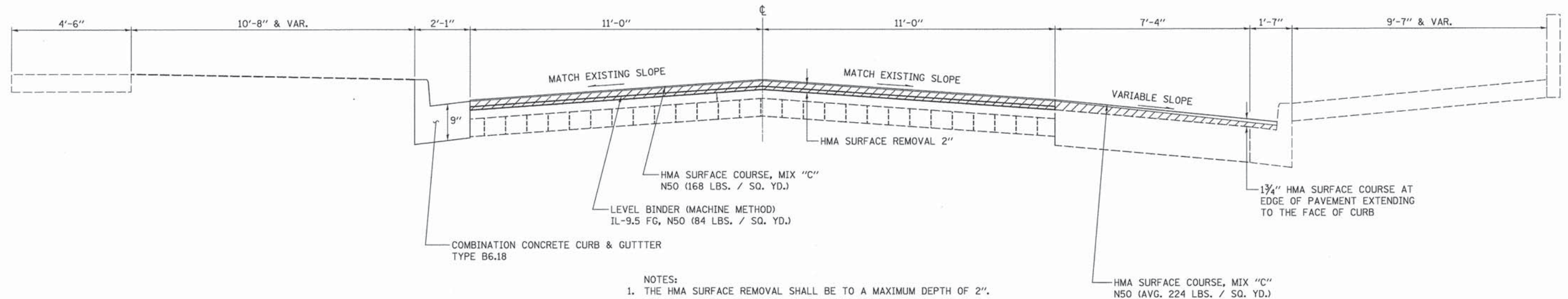
STATION TO STATION  
296+00 297+00



**PROPOSED TYPICAL CROSS SECTION ③**

**WASHINGTON STREET**

STATION TO STATION  
296+00 297+00



- NOTES:
1. THE HMA SURFACE REMOVAL SHALL BE TO A MAXIMUM DEPTH OF 2".
  2. THE HMA SURFACE REMOVAL SHALL BE EITHER 2" OR TO THE TOP OF THE EXISTING PC CONCRETE PAVEMENT.
  3. THE EXISTING HMA RESURFACING ON THE COMBINATION CONCRETE CURB AND GUTTER FLAG SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COLD MILLING (SPECIAL).

PRINT DRIVER - J:\S\14-00023-00-RS\14-00023-00-RS.dwg  
 PLOT SCALE = 0.8933' / 1"



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8933' / 1"       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:22:35 PM | DATE - 01/2016 | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|                  |                                       |
|------------------|---------------------------------------|
| TYPICAL SECTIONS |                                       |
| SCALE            | SHEET NO. 3 OF 17 SHEETS STA. TO STA. |

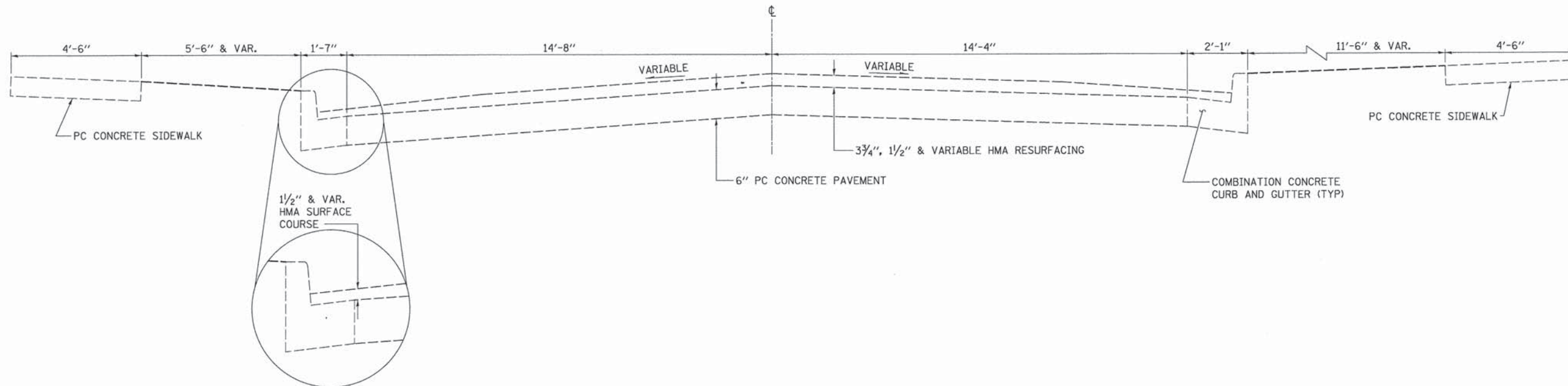
|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 12        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |



**EXISTING TYPICAL CROSS SECTION ④**

**INDEPENDENCE STREET**

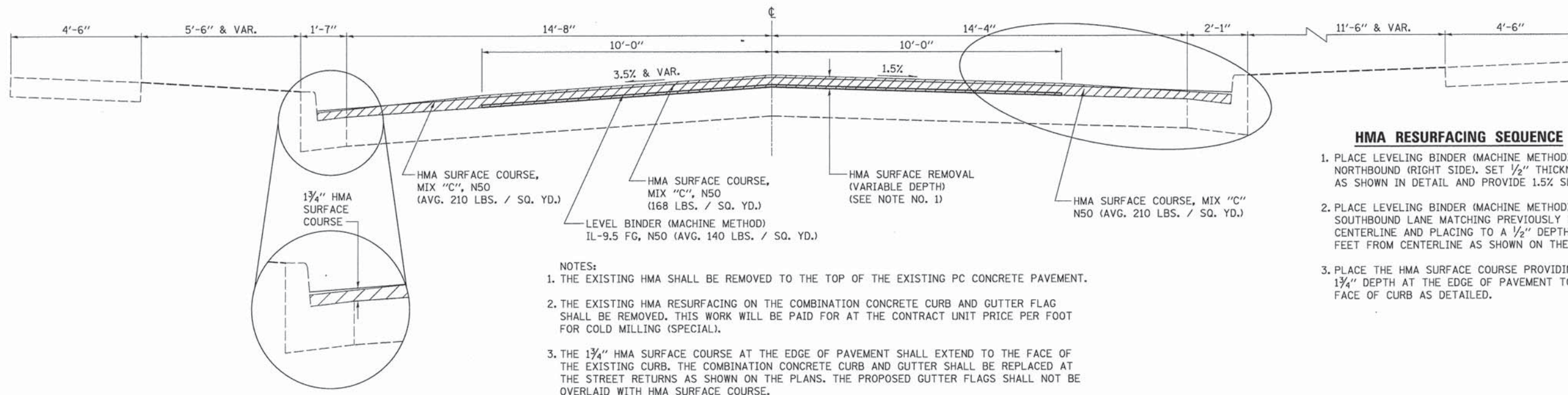
STATION 120+39 TO STATION 123+00



**PROPOSED TYPICAL CROSS SECTION ④**

**INDEPENDENCE STREET**

STATION 120+39 TO STATION 123+00



HMA RESURFACING DETAIL (TYP)  
SEE NOTES 2 AND 3

**HMA RESURFACING SEQUENCE**

1. PLACE LEVELING BINDER (MACHINE METHOD) ON NORTHBOUND (RIGHT SIDE). SET 1/2" THICKNESS AS SHOWN IN DETAIL AND PROVIDE 1.5% SLOPE.
2. PLACE LEVELING BINDER (MACHINE METHOD) ON SOUTHBOUND LANE MATCHING PREVIOUSLY LAID CENTERLINE AND PLACING TO A 1/2" DEPTH 10 FEET FROM CENTERLINE AS SHOWN ON THE DETAIL.
3. PLACE THE HMA SURFACE COURSE PROVIDING 1 3/4" DEPTH AT THE EDGE OF PAVEMENT TO THE FACE OF CURB AS DETAILED.

- NOTES:
1. THE EXISTING HMA SHALL BE REMOVED TO THE TOP OF THE EXISTING PC CONCRETE PAVEMENT.
  2. THE EXISTING HMA RESURFACING ON THE COMBINATION CONCRETE CURB AND GUTTER FLAG SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COLD MILLING (SPECIAL).
  3. THE 1 3/4" HMA SURFACE COURSE AT THE EDGE OF PAVEMENT SHALL EXTEND TO THE FACE OF THE EXISTING CURB. THE COMBINATION CONCRETE CURB AND GUTTER SHALL BE REPLACED AT THE STREET RETURNS AS SHOWN ON THE PLANS. THE PROPOSED GUTTER FLAGS SHALL NOT BE OVERLAID WITH HMA SURFACE COURSE.

PRINT DRIVER = I:\D\14023\14023.dwg  
 PLOT DATE = 3/1/2016 1:35:49 PM  
 PLOT SCALE = 0.8833 / in.  
 USER NAME = skm  
 ESCA PROJECT NO. 719.63  
 CHECKED = JMS  
 DATE = 01/2016



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8833 / in.       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:35:49 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 4 OF 17 SHEETS STA. TO STA.

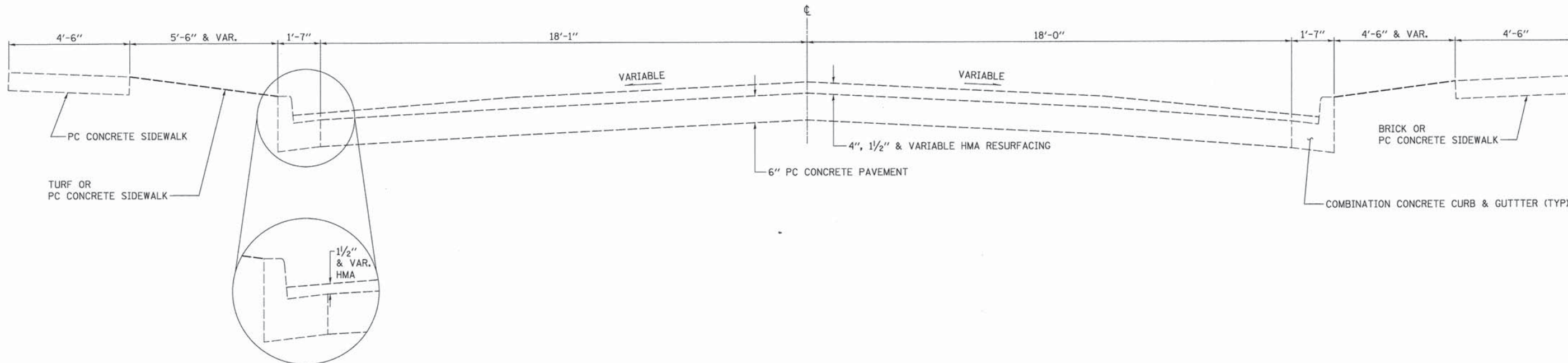
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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION       | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-0023-00-RS | PIATT                     | 76           | 13        |
| CONTRACT NO. 91516  |               |                           |              |           |
| FED. ROAD DIST. NO. |               | ILLINOIS FED. AID PROJECT |              |           |



**EXISTING TYPICAL CROSS SECTION ⑤**

**INDEPENDENCE STREET**

STATION TO STATION  
123+00 TO 125+26  
127+87 TO 130+41



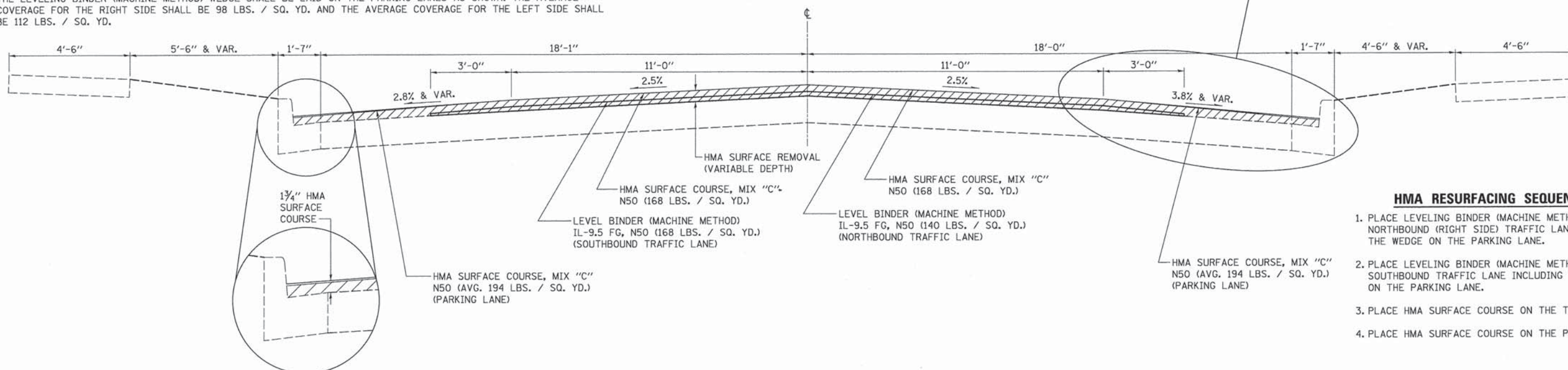
**NOTES:**

1. THE EXISTING HMA SHALL BE REMOVED TO THE TOP OF THE EXISTING PC CONCRETE PAVEMENT.
2. THE EXISTING HMA RESURFACING ON THE COMBINATION CONCRETE CURB AND GUTTER FLAG SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COLD MILLING (SPECIAL).
3. THE 1 3/4" HMA SURFACE COURSE AT THE EDGE OF PAVEMENT SHALL EXTEND TO THE FACE OF THE EXISTING CURB. THE COMBINATION CONCRETE CURB AND GUTTER SHALL BE REPLACED AT THE STREET RETURNS AS SHOWN ON THE PLANS. THE PROPOSED GUTTER FLAGS SHALL NOT BE OVERLAID WITH HMA SURFACE COURSE.
4. THE LBS. PER SQ. YD. SHOWN FOR THE LEVEL BINDER (MACHINE METHOD) WILL PROVIDE A MINIMUM THICKNESS OF 1" AND PROVIDE ADDITIONAL TONNAGE TO MAINTAIN THE 2.5% SLOPE ON THE TRAFFIC LANES.
5. THE LEVELING BINDER (MACHINE METHOD) WEDGE SHALL BE LAID ON THE PARKING LANES AS SHOWN. THE AVERAGE COVERAGE FOR THE RIGHT SIDE SHALL BE 98 LBS. / SQ. YD. AND THE AVERAGE COVERAGE FOR THE LEFT SIDE SHALL BE 112 LBS. / SQ. YD.

**PROPOSED TYPICAL CROSS SECTION ⑤**

**INDEPENDENCE STREET**

STATION TO STATION  
123+00 TO 125+26  
127+87 TO 130+41



HMA RESURFACING DETAIL (TYP)  
SEE NOTES 2 AND 3

**HMA RESURFACING SEQUENCE**

1. PLACE LEVELING BINDER (MACHINE METHOD) ON NORTHBOUND (RIGHT SIDE) TRAFFIC LANE INCLUDING THE WEDGE ON THE PARKING LANE.
2. PLACE LEVELING BINDER (MACHINE METHOD) ON THE SOUTHBOUND TRAFFIC LANE INCLUDING THE WEDGE ON THE PARKING LANE.
3. PLACE HMA SURFACE COURSE ON THE TRAFFIC LANES.
4. PLACE HMA SURFACE COURSE ON THE PARKING LANES.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

|                     |                           |        |              |           |
|---------------------|---------------------------|--------|--------------|-----------|
| F.A. RTE.           | SECTION                   | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS            | PIATT  | 76           | 14        |
| CONTRACT NO. 91516  |                           |        |              |           |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT |        |              |           |

|                             |                |           |
|-----------------------------|----------------|-----------|
| USER NAME = skm             | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63     | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.0933' / 1" = | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016        | DATE - 01/2016 | REVISED - |

SCALE SHEET NO. 5 OF 17 SHEETS STA. TO STA.

PRINT DATE = 3/1/2016 1:22:52 PM  
DRAWN BY = JLF  
CHECKED BY = JMS  
DATE = 01/2016

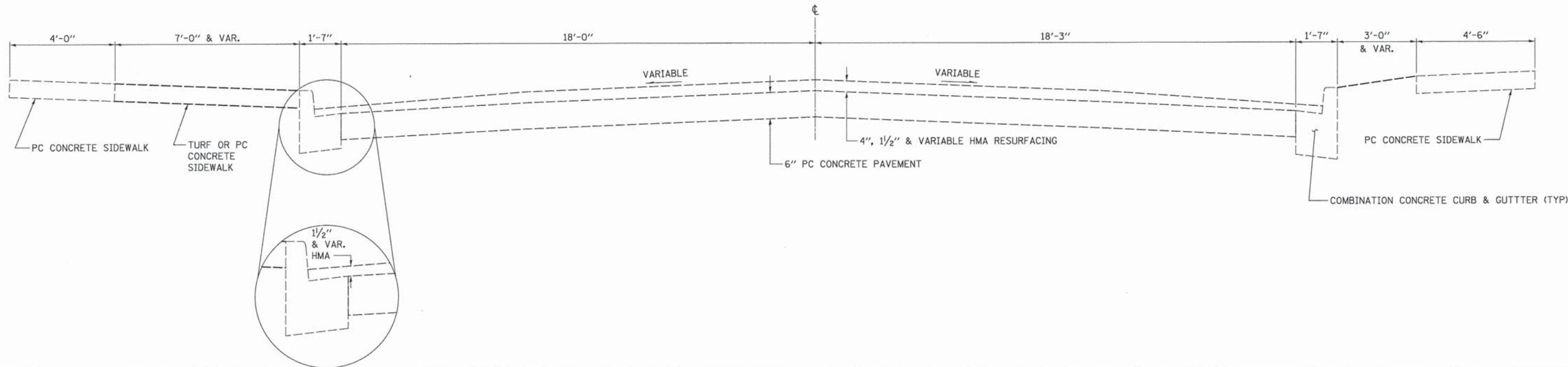




**EXISTING TYPICAL CROSS SECTION ⑥**

**INDEPENDENCE STREET**

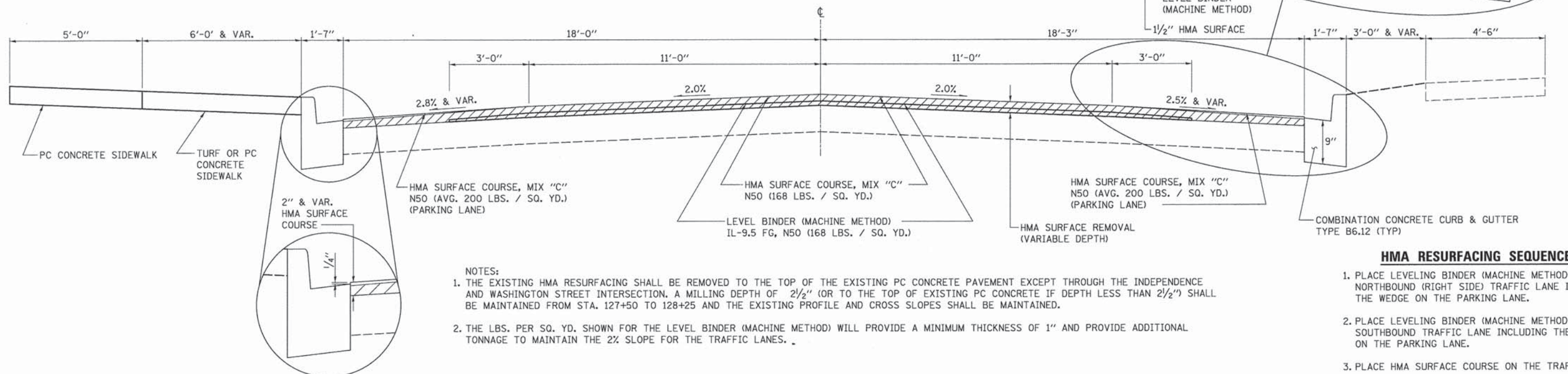
STATION 126+04 TO STATION 127+87



**PROPOSED TYPICAL CROSS SECTION ⑥**

**INDEPENDENCE STREET**

STATION 126+04 TO STATION 127+87



- NOTES:**
1. THE EXISTING HMA RESURFACING SHALL BE REMOVED TO THE TOP OF THE EXISTING PC CONCRETE PAVEMENT EXCEPT THROUGH THE INDEPENDENCE AND WASHINGTON STREET INTERSECTION. A MILLING DEPTH OF 2 1/2" (OR TO THE TOP OF EXISTING PC CONCRETE IF DEPTH LESS THAN 2 1/2") SHALL BE MAINTAINED FROM STA. 127+50 TO 128+25 AND THE EXISTING PROFILE AND CROSS SLOPES SHALL BE MAINTAINED.
  2. THE LBS. PER SQ. YD. SHOWN FOR THE LEVEL BINDER (MACHINE METHOD) WILL PROVIDE A MINIMUM THICKNESS OF 1" AND PROVIDE ADDITIONAL TONNAGE TO MAINTAIN THE 2% SLOPE FOR THE TRAFFIC LANES.

- HMA RESURFACING SEQUENCE**
1. PLACE LEVELING BINDER (MACHINE METHOD) ON NORTHBOUND (RIGHT SIDE) TRAFFIC LANE INCLUDING THE WEDGE ON THE PARKING LANE.
  2. PLACE LEVELING BINDER (MACHINE METHOD) ON THE SOUTHBOUND TRAFFIC LANE INCLUDING THE WEDGE ON THE PARKING LANE.
  3. PLACE HMA SURFACE COURSE ON THE TRAFFIC LANES.
  4. PLACE HMA SURFACE COURSE ON THE PARKING LANES.

HMA RESURFACING DETAIL (TYP)  
SEE STANDARD 606001

PRINT DRIVER = J:\01-ESCA\1416\1416.dwg  
 PLOT DATE = 3/1/2016 1:23:00 PM  
 PLOT SCALE = 0.0833' / 1"



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.0833' / 1"       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:23:00 PM | DATE - 01/2013 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 6 OF 17 SHEETS STA. TO STA.

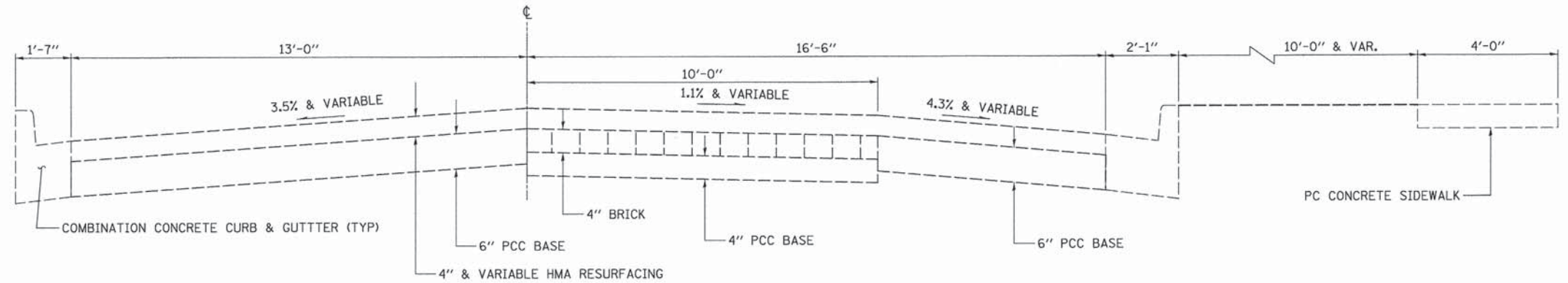
|                    |                |        |                           |           |
|--------------------|----------------|--------|---------------------------|-----------|
| F.A. RTE.          | SECTION        | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR                | 14-00023-00-RS | PIATT  | 76                        | 15        |
| CONTRACT NO. 91516 |                |        | ILLINOIS FED. AID PROJECT |           |



**EXISTING TYPICAL CROSS SECTION ⑦**

**MARION STREET**

STATION 100+93 TO STATION 103+22

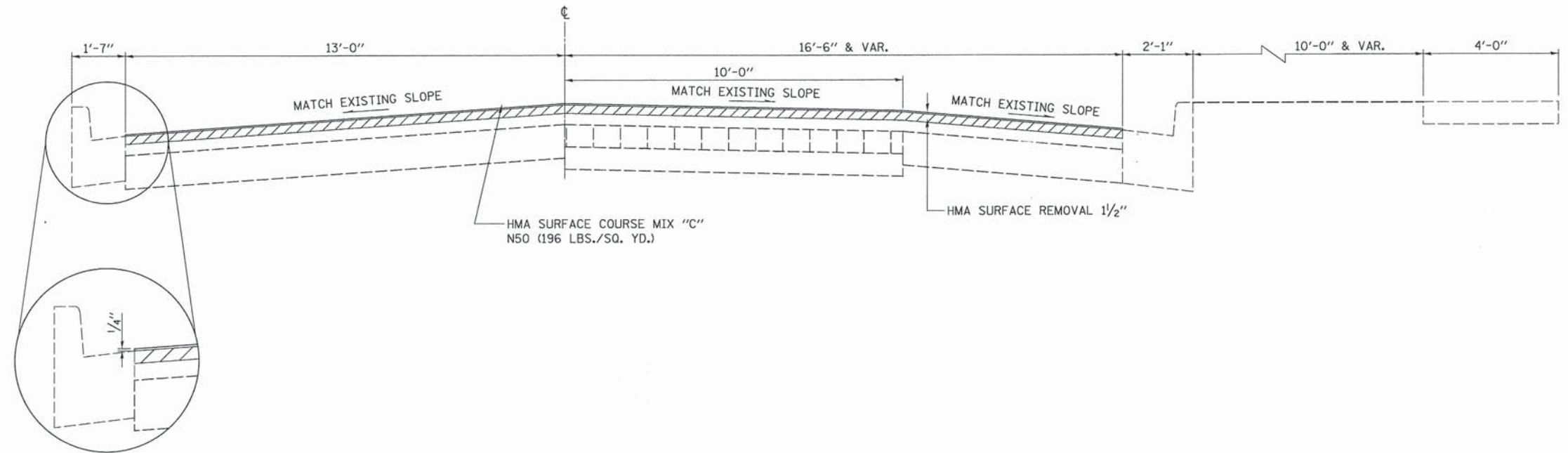


NOTE: THE EXISTING PAVEMENT STRUCTURE IS PC CONCRETE PAVEMENT, 9" AT SOME LOCATIONS. REFER TO THE PLAN SHEETS FOR DETAILS.

**PROPOSED TYPICAL CROSS SECTION ⑦**

**MABION STREET**

STATION 100+93 TO STATION 103+22.6



HMA RESURFACING DETAIL (TYP)  
SEE STANDARD 606001

PLOT FILENAME = I:\14-00023-00-RS\14-00023-00-RS-TYPICAL SECTIONS.dwg  
 PLOT LABEL = 14-00023-00-RS-TYPICAL SECTIONS.dwg  
 FILE NAME = 14-00023-00-RS-TYPICAL SECTIONS.dwg



|                                   |                |           |
|-----------------------------------|----------------|-----------|
| USER NAME = skm                   | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61           | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.0033' / 1" = 1/300 | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:23:08 PM   | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 7 OF 17 SHEETS STA. TO STA.

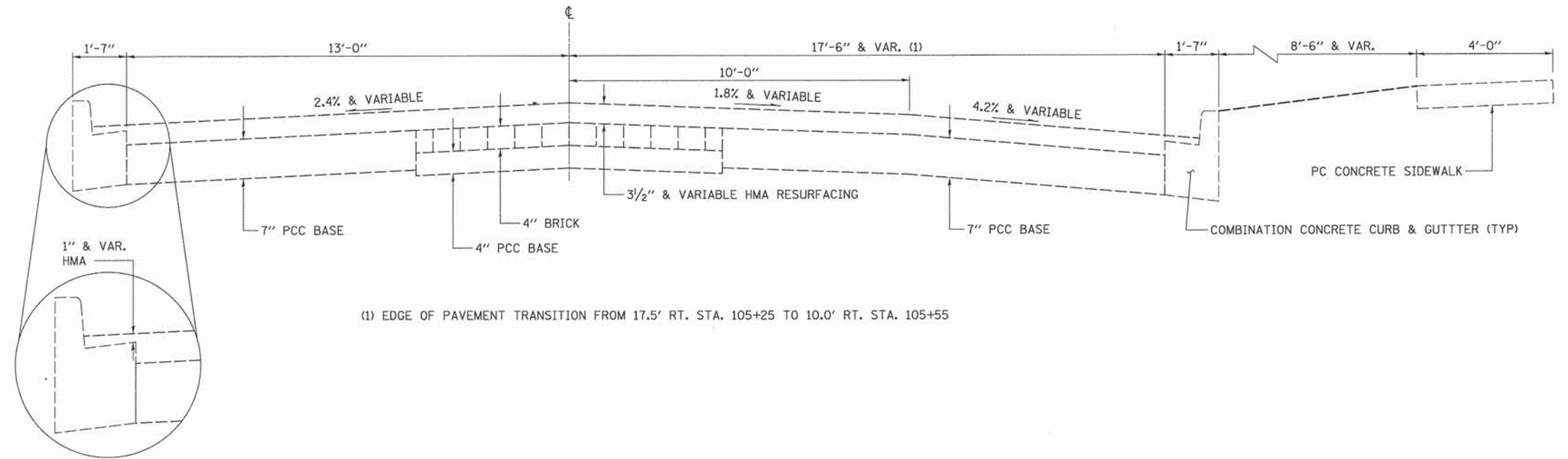
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| VAR   | 14-00023-00-RS | PIATT  | 76           | 16        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |



**EXISTING TYPICAL CROSS SECTION ⑧**

**MARION STREET**

STATION TO STATION  
103+38.8 105+55

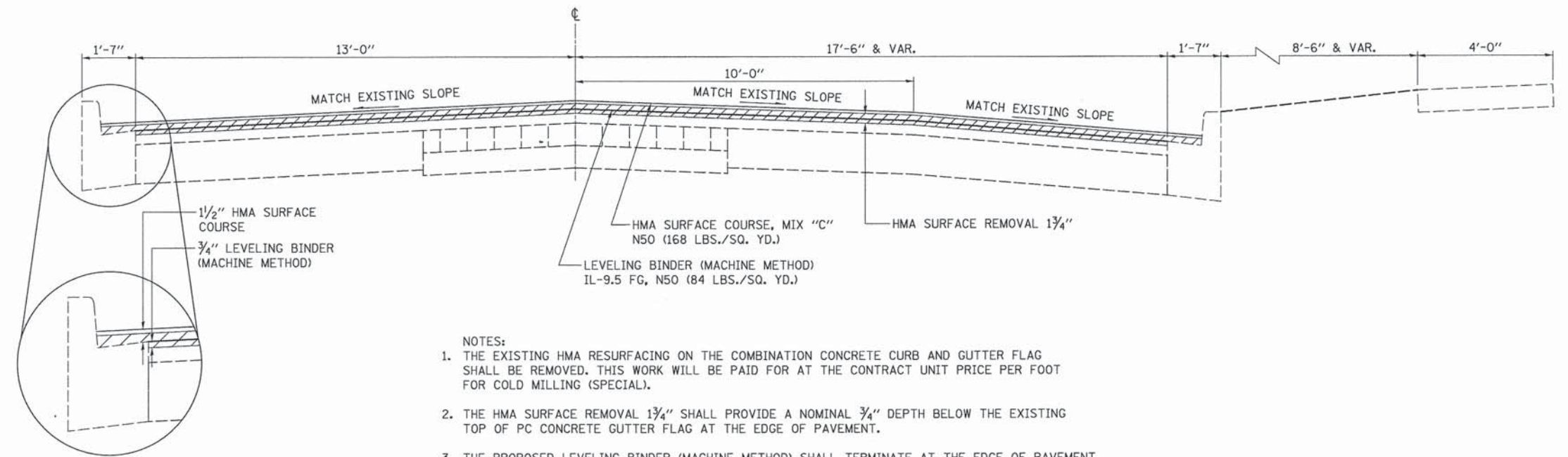


(1) EDGE OF PAVEMENT TRANSITION FROM 17.5' RT. STA. 105+25 TO 10.0' RT. STA. 105+55

**PROPOSED TYPICAL CROSS SECTION ⑧**

**MARION STREET**

STATION TO STATION  
103+38.8 105+55



- NOTES:
1. THE EXISTING HMA RESURFACING ON THE COMBINATION CONCRETE CURB AND GUTTER FLAG SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COLD MILLING (SPECIAL).
  2. THE HMA SURFACE REMOVAL 1 3/4" SHALL PROVIDE A NOMINAL 3/4" DEPTH BELOW THE EXISTING TOP OF PC CONCRETE GUTTER FLAG AT THE EDGE OF PAVEMENT.
  3. THE PROPOSED LEVELING BINDER (MACHINE METHOD) SHALL TERMINATE AT THE EDGE OF PAVEMENT AND THE TOP SHALL MATCH THE EXISTING TOP OF PC CONCRETE GUTTER. THE 1 1/2" HMA SURFACE COURSE AT THE EDGE OF PAVEMENT SHALL EXTEND TO THE FACE OF THE EXISTING CURB.

HMA RESURFACING DETAIL (TYP)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

|                     |                |                           |              |           |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76           | 17        |
| CONTRACT NO. 91516  |                |                           |              |           |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |              |           |

SCALE SHEET NO. 8 OF 17 SHEETS STA. TO STA.



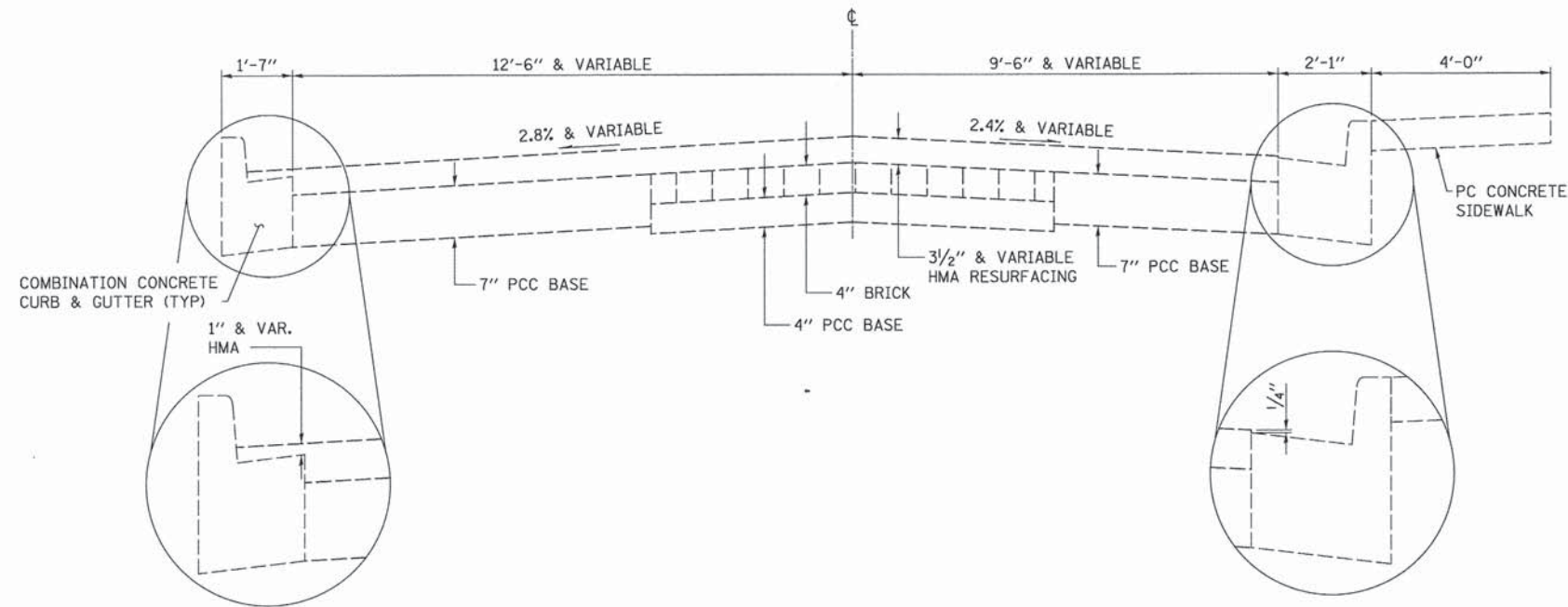
|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8833' / in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:23:16 PM | DATE - 01/2016 | REVISED - |



**EXISTING TYPICAL CROSS SECTION ⑨**

**MARION STREET**

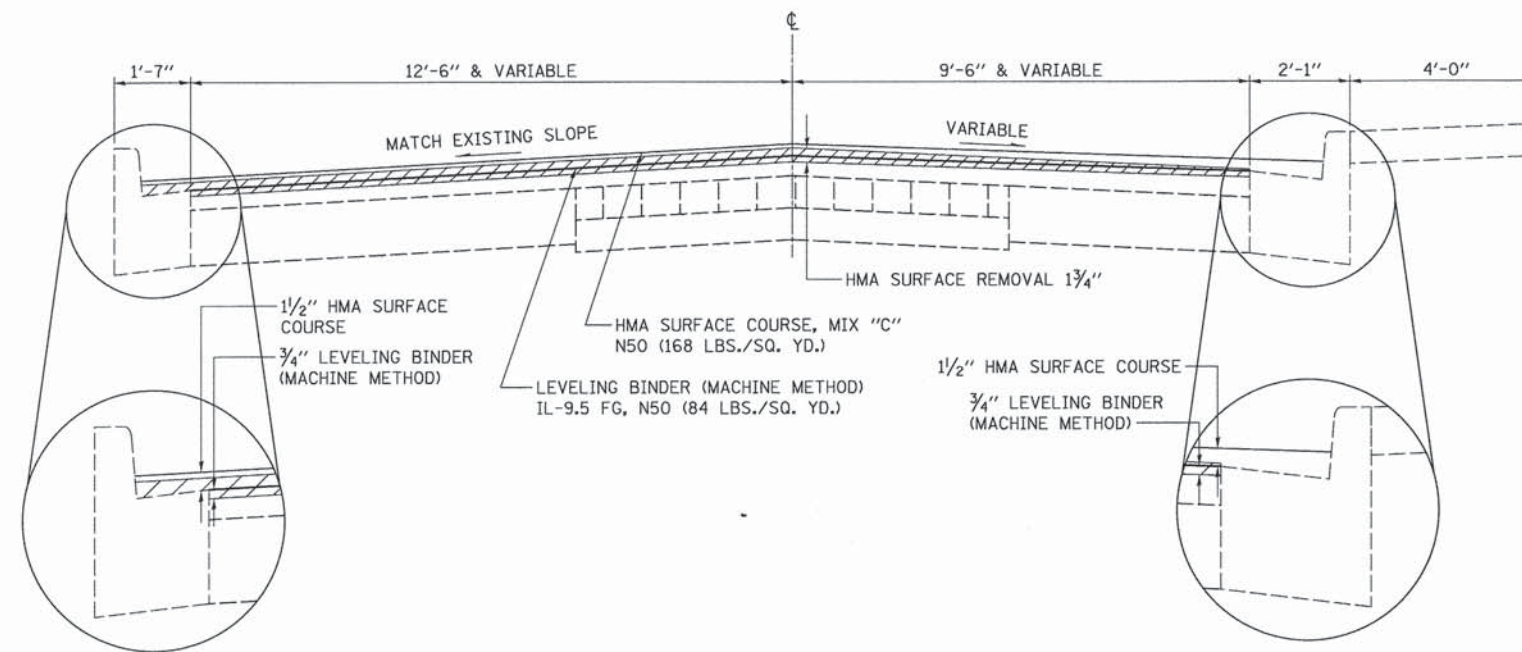
STATION TO STATION  
105+55 TO 107+12  
107+81 TO 108+64



**PROPOSED TYPICAL CROSS SECTION ⑨**

**MARION STREET**

STATION TO STATION  
105+55 TO 107+12  
107+81 TO 108+64



**NOTES:**

1. THE EXISTING HMA RESURFACING ON THE COMBINATION CONCRETE CURB AND GUTTER FLAG SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COLD MILLING (SPECIAL).
2. THE HMA SURFACE REMOVAL 1 3/4" SHALL PROVIDE A NOMINAL 3/4" DEPTH BELOW THE EXISTING TOP OF PC CONCRETE GUTTER FLAG AT THE EDGE OF PAVEMENT.
3. THE PROPOSED LEVELING BINDER (MACHINE METHOD) SHALL TERMINATE AT THE EDGE OF PAVEMENT AND THE TOP SHALL MATCH THE EXISTING TOP OF PC CONCRETE GUTTER. THE 1/2" HMA SURFACE COURSE AT THE EDGE OF PAVEMENT SHALL EXTEND TO THE FACE OF THE EXISTING CURB.
4. THE HMA SURFACE REMOVAL WILL VARY FROM 1 3/4" AT CENTERLINE TO 3/4" DEPTH AT THE EXISTING EDGE OF PAVEMENT FROM RT. STA. 105+55 TO RT. STA. 106+86. THE MILLING DEPTH AT THE EDGE OF PAVEMENT WILL BE 3/4" BELOW THE EXPOSED CONCRETE GUTTER FLAG.
5. TRANSITION THE LEVELING BINDER (MACHINE METHOD) FROM 3/4" AT STA. 106+97 TO 0" AT STA. 107+12 AND FROM 0" AT STA. 107+81 TO 3/4" AT STA. 107+96.

HMA RESURFACING DETAIL FOR PREVIOUSLY RESURFACED GUTTER FLAGS  
SEE NOTE NOS. 1, 2, & 3

HMA RESURFACING DETAIL FOR GUTTER FLAGS NOT PREVIOUSLY RESURFACED.  
SEE NOTE NOS. 3 & 4

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.0033' / 1"       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:23:26 PM | DATE - 01/2016 | REVISED - |

SCALE SHEET NO. 9 OF 17 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 18        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |

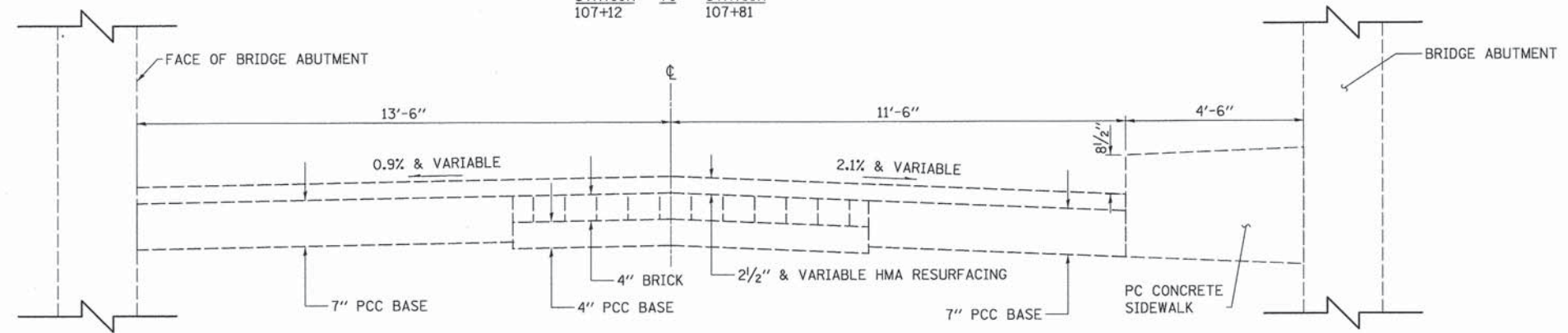




**EXISTING TYPICAL CROSS SECTION ⑩**

**MARION STREET**

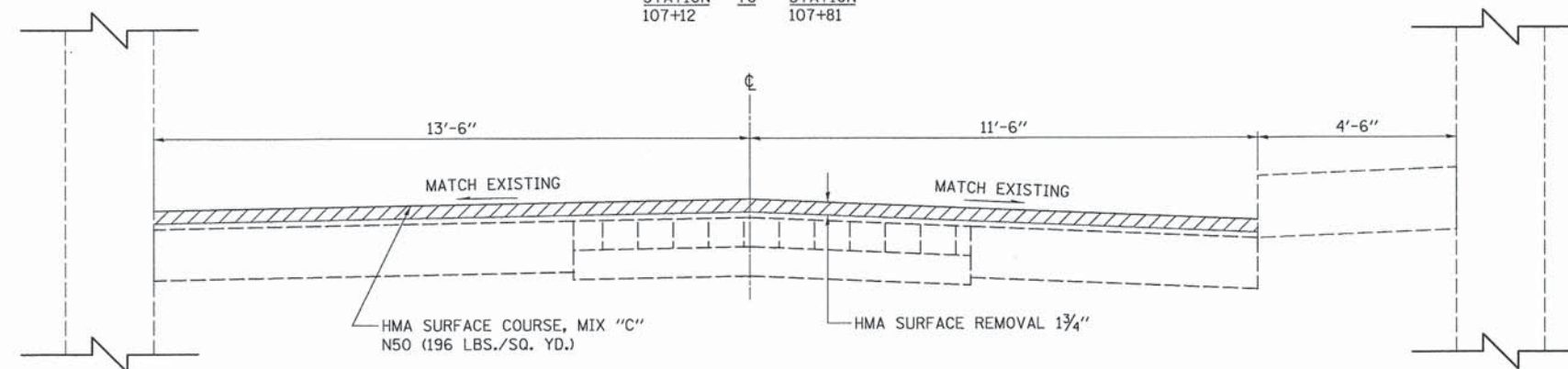
STATION 107+12 TO STATION 107+81



**PROPOSED TYPICAL CROSS SECTION ⑩**

**MARION STREET**

STATION 107+12 TO STATION 107+81



NOTE: THE EXISTING VERTICAL CLEARANCE SHALL BE MAINTAINED BETWEEN THE TOP OF PAVEMENT AND THE BOTTOM OF THE NORFOLK AND SOUTHERN BRIDGE SUPERSTRUCTURE.

PRINT DATE: 3/1/2016 1:23:34 PM  
 PLOT SCALE: 0.0833' / 1"



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.0833' / 1"       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:23:34 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 10 OF 17 SHEETS STA. TO STA.

|   |                |        |                    |           |
|---|----------------|--------|--------------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76                 | 19        |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        | CONTRACT NO. 91516 |           |

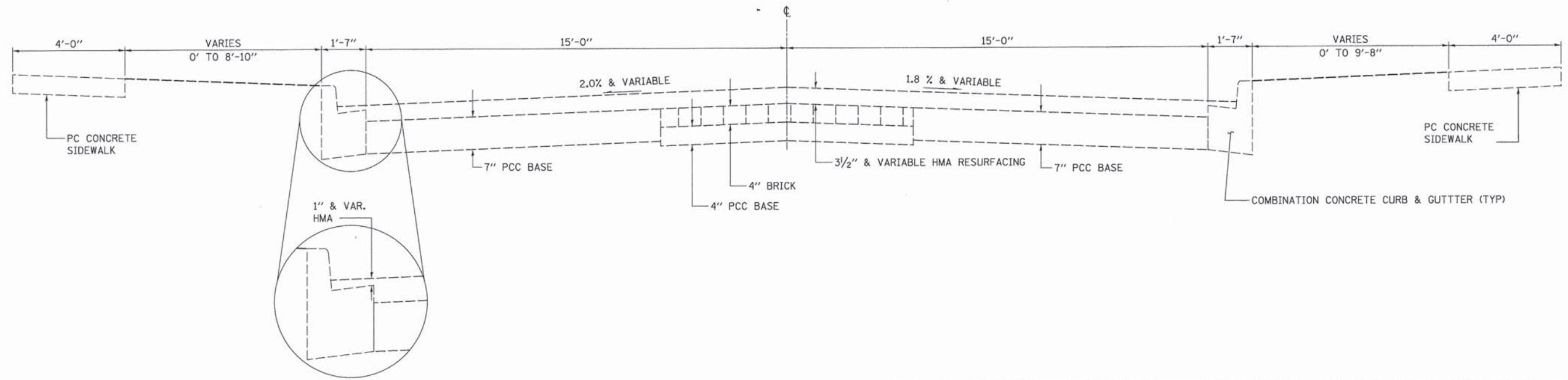


**EXISTING TYPICAL CROSS SECTION ⑪**

**MARION STREET**

STATION TO STATION  
108+64 TO 116+00  
0+00 TO 2+07

STA. EQUATION 116+00(BK) = 0+00(AH)

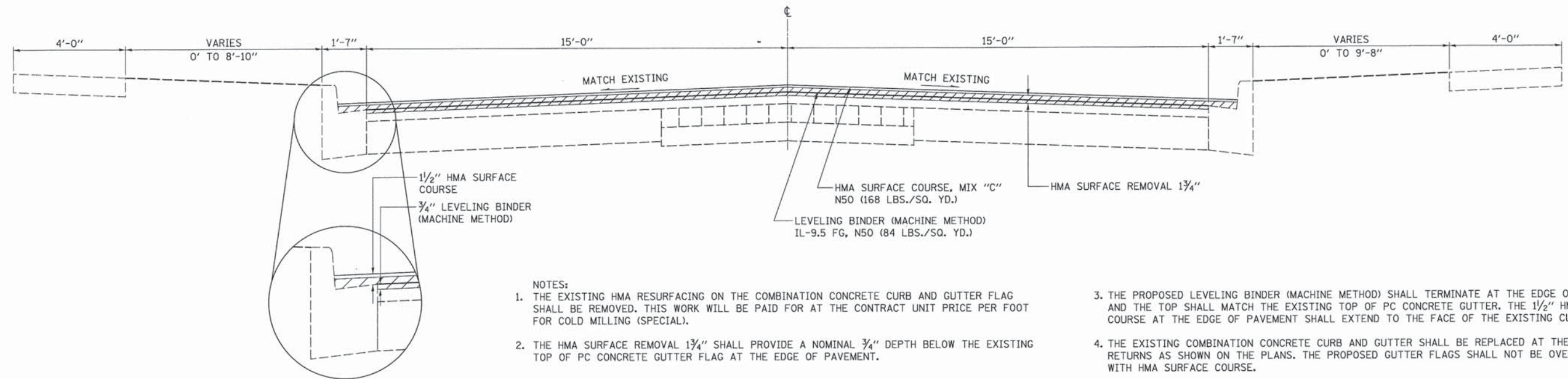


**PROPOSED TYPICAL CROSS SECTION ⑪**

**MARION STREET**

STATION TO STATION  
108+64 TO 116+00  
0+00 TO 2+07

STA. EQUATION 116+00(BK) = 0+00(AH)



- NOTES:
1. THE EXISTING HMA RESURFACING ON THE COMBINATION CONCRETE CURB AND GUTTER FLAG SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COLD MILLING (SPECIAL).
  2. THE HMA SURFACE REMOVAL 1 3/4" SHALL PROVIDE A NOMINAL 3/4" DEPTH BELOW THE EXISTING TOP OF PC CONCRETE GUTTER FLAG AT THE EDGE OF PAVEMENT.
  3. THE PROPOSED LEVELING BINDER (MACHINE METHOD) SHALL TERMINATE AT THE EDGE OF PAVEMENT AND THE TOP SHALL MATCH THE EXISTING TOP OF PC CONCRETE GUTTER. THE 1 1/2" HMA SURFACE COURSE AT THE EDGE OF PAVEMENT SHALL EXTEND TO THE FACE OF THE EXISTING CURB.
  4. THE EXISTING COMBINATION CONCRETE CURB AND GUTTER SHALL BE REPLACED AT THE STREET RETURNS AS SHOWN ON THE PLANS. THE PROPOSED GUTTER FLAGS SHALL NOT BE OVERLAID WITH HMA SURFACE COURSE.

HMA RESURFACING DETAIL (TYP)

PRINT DRIVER = I:\D.E.B. & J.L.F. 3/11/2016  
 PLOT DATE = 3/11/2016 1:23:46 PM  
 PLOT SCALE = 0.0833" / 1"



|                                  |                |           |
|----------------------------------|----------------|-----------|
| USER NAME = skm                  | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61          | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.0833" / 1"        | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/11/2016 1:23:46 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 11 OF 17 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 20        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |

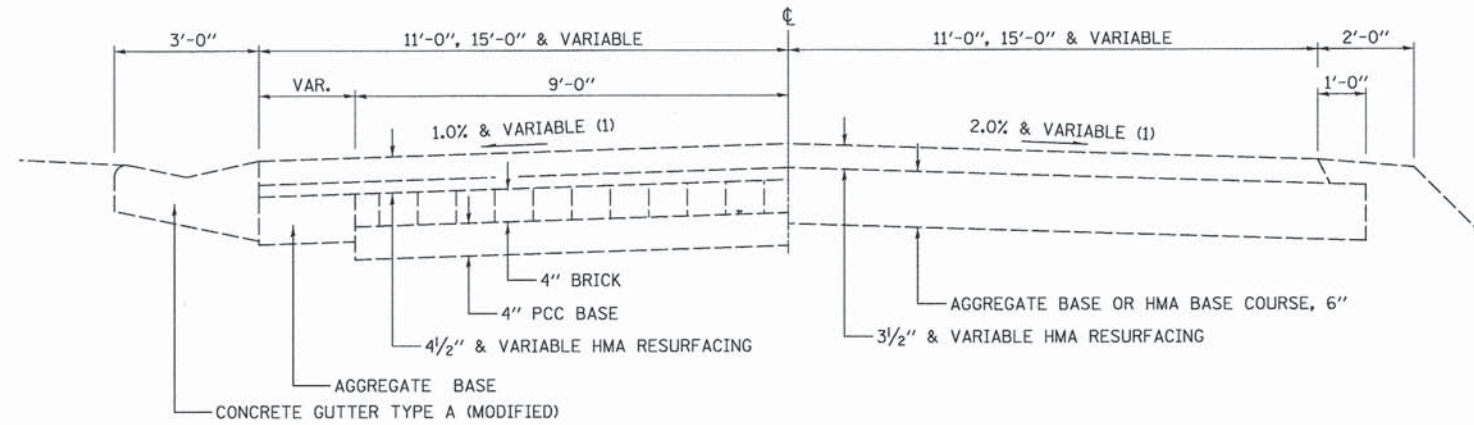


**EXISTING TYPICAL CROSS SECTION ⑫**

**ALLERTON ROAD**

STATION TO STATION  
2+07 4+05

STA. EQUATION 3+30.59(BK) = 3+33.60(AH)



(1) SUPERELEVATION TRANSITION FROM STA. 3+81.81 TO STA. 4+81.81. REFER TO TYPICAL SECTION ⑬

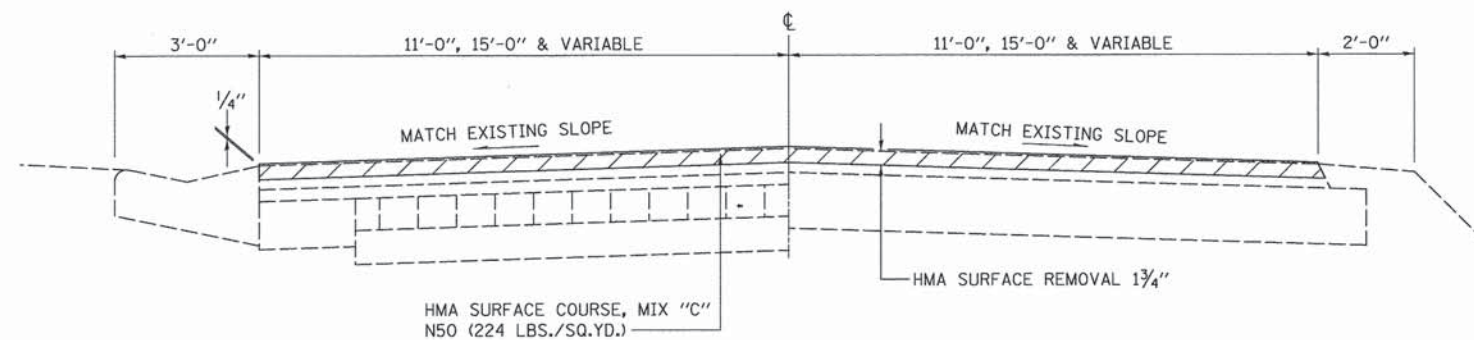
NOTE: COMBINATION CONCRETE CURB AND GUTTER TYPE B 6.12  
RT. STA. 2+07 TO 2+30.

**PROPOSED TYPICAL CROSS SECTION ⑫**

**ALLERTON ROAD**

STATION TO STATION  
2+07 4+05

STA. EQUATION 3+30.59(BK) = 3+33.60(AH)



NOTE: TRANSITION HMA SURFACE REMOVAL  
FROM 1 3/4" AT STA. 3+70 TO 0" AT STA. 4+05.

PLOT DATE = 3/1/2016 1:23:56 PM  
 USER NAME = gkm  
 PROJECT NO. = 719.61  
 PLOT SCALE = 0.0833' / in.  
 ESCA CONSULTANTS, INC.  
 1501 E. Lake Street  
 Chicago, IL 60607  
 TEL: 312.977.1981 FAX: 312.977.1982



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = gkm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0833' / in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:23:56 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 12 OF 17 SHEETS STA. TO STA.

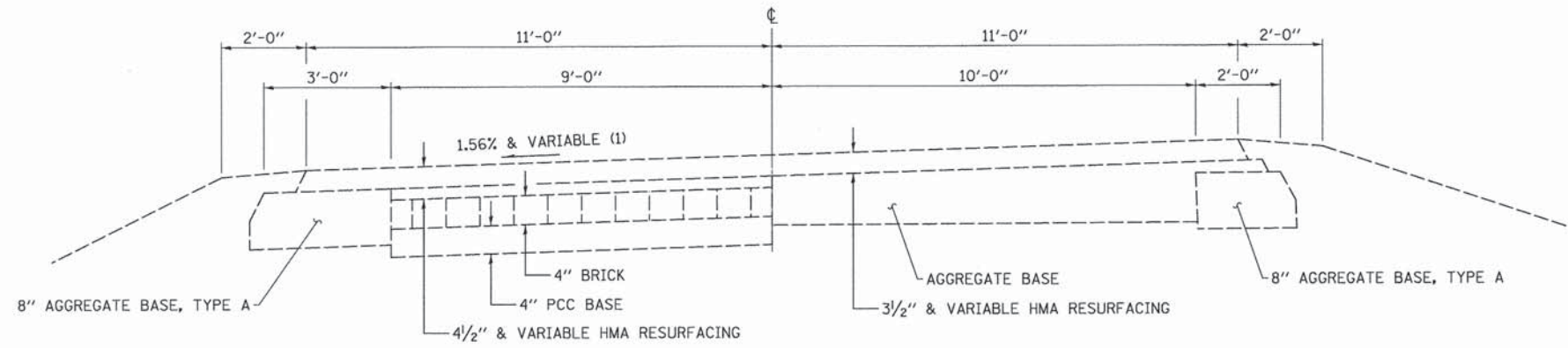
|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 21        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |



**EXISTING TYPICAL CROSS SECTION ⑬**

**ALLERTON ROAD**

STATION TO STATION  
4+05 TO 6+53.50

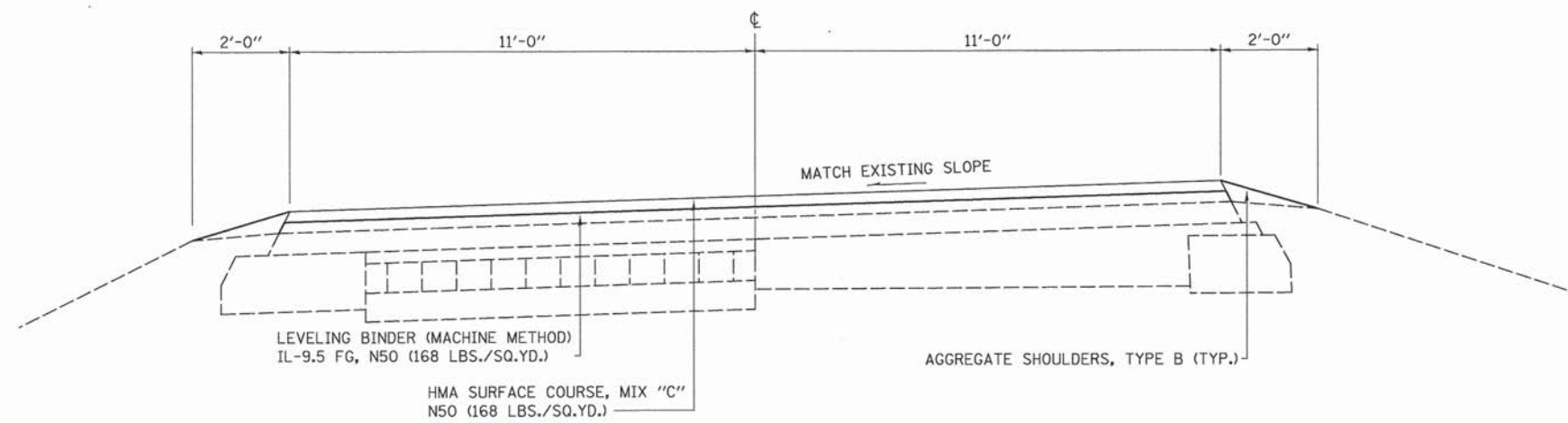


(1) SUPERELEVATION TRANSITION FROM STA. 5+53.50 TO STA. 6+53.50

**PROPOSED TYPICAL CROSS SECTION ⑬**

**ALLERTON ROAD**

STATION TO STATION  
4+05 TO 6+53.50



NOTE: TRANSITION THE LEVELING BINDER (MACHINE METHOD) FROM 0" AT STA. 4+05 TO 1 1/2" THICKNESS AT STA. 4+35

PLOT DATE = 3/1/2016 1:24:06 PM  
 USER NAME = skm  
 ESCA PROJECT NO. 719.61  
 PLOT SCALE = 0.0033' / in.  
 FILE NAME = C:\2016\719.61\14-00023-00-RS.dwg



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0033' / in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:24:06 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 13 OF 17 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                       | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 22        |
| CONTRACT NO. 91516                              |                |        |              |           |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        |              |           |

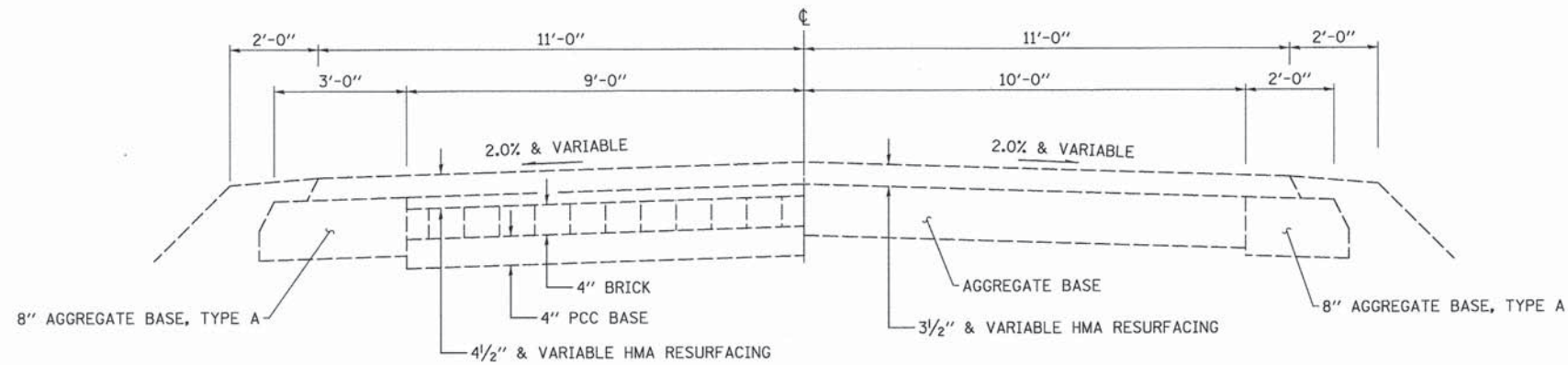


**EXISTING TYPICAL CROSS SECTION ⑭**

**ALLERTON ROAD**

| STATION  | TO | STATION  |
|----------|----|----------|
| 6+53.50  |    | 15+00    |
| 18+16    |    | 36+60    |
| 41+30    |    | 61+52.30 |
| 65+51.79 |    | 70+35    |

STA. EQUATION 59+14.77(BK) = 59+15.61(AH)

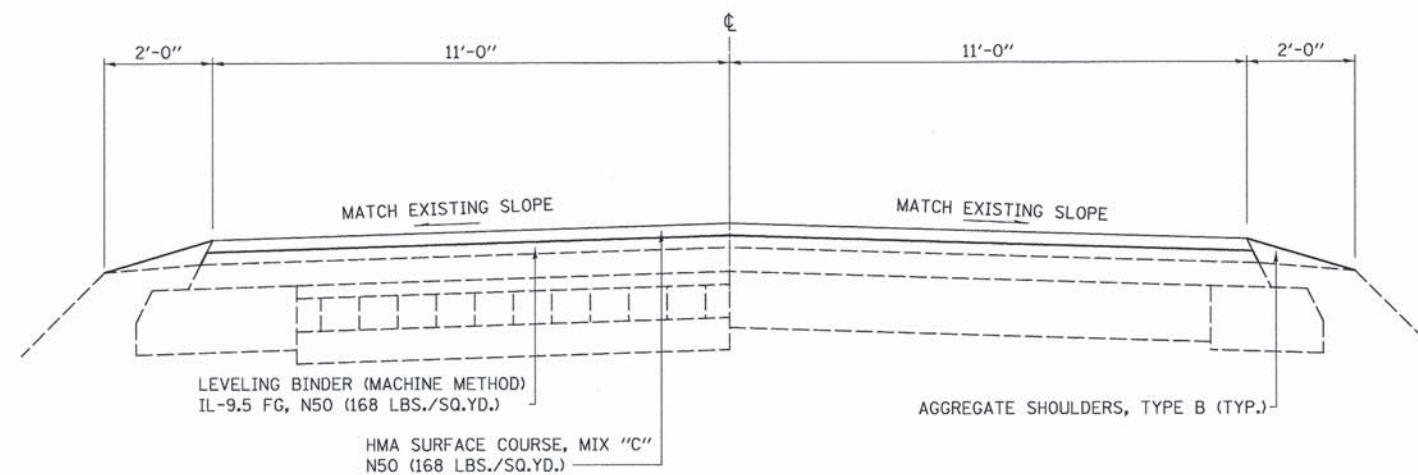


**PROPOSED TYPICAL CROSS SECTION ⑭**

**ALLERTON ROAD**

| STATION  | TO | STATION  |
|----------|----|----------|
| 6+53.50  |    | 15+00    |
| 18+16    |    | 36+60    |
| 41+30    |    | 61+52.30 |
| 65+51.79 |    | 70+35    |

STA. EQUATION 59+14.77(BK) = 59+15.61(AH)



NOTE: TRANSITION THE LEVELING BINDER (MACHINE METHOD) FROM 1/2" AT STA. 69+75 TO 0" THICKNESS AT STA. 70+05

PRINTED AT: ESCA CONSULTANTS, INC. 1001 W. WASHINGTON ST. DEPT. 1000 CHICAGO, IL 60607-1000  
 FILE NAME: C:\2016\14-00023-RS-14-11-16.dwg  
 PLOT DATE: 3/1/2016 1:24:13 PM



|                                   |                |           |
|-----------------------------------|----------------|-----------|
| USER NAME = skm                   | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61           | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0033' / 1" = 1/300 | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:24:13 PM   | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 14 OF 17 SHEETS STA. TO STA.

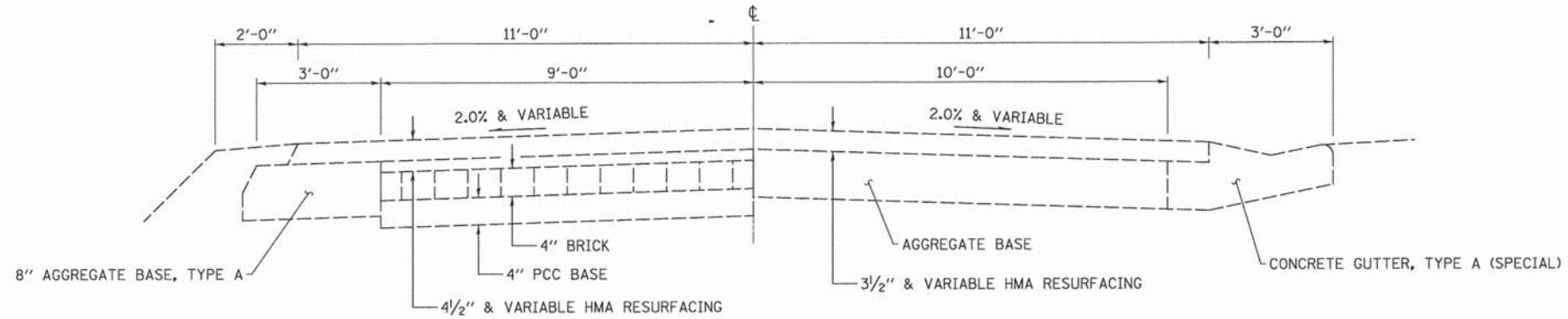
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| VAR   | 14-00023-00-RS | PIATT  | 76           | 23        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |



**EXISTING TYPICAL CROSS SECTION 15**

**ALLERTON ROAD**

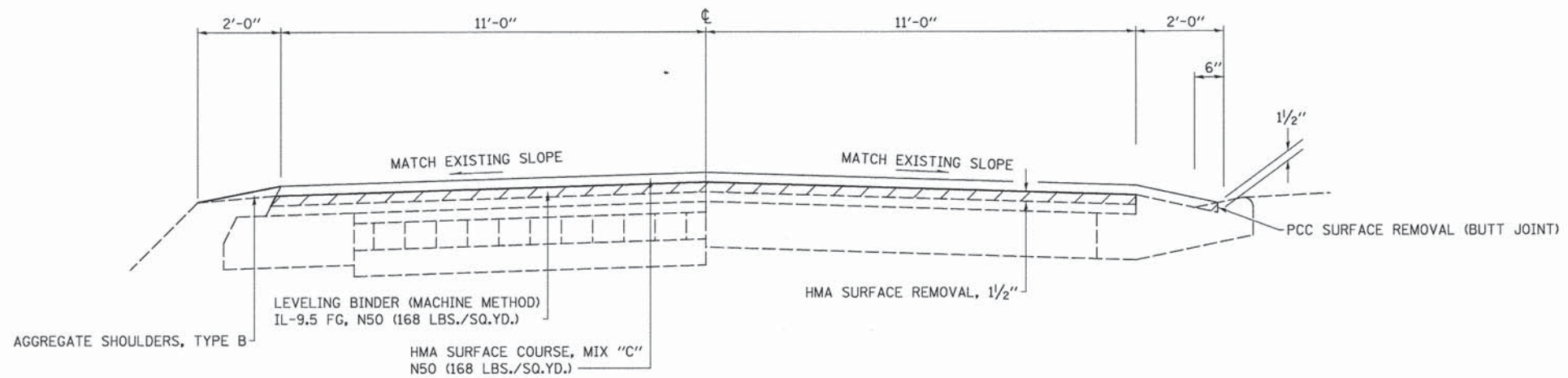
STATION TO STATION  
15+00 18+16



**PROPOSED TYPICAL CROSS SECTION 15**

**ALLERTON ROAD**

STATION TO STATION  
15+00 18+16



NOTE: HMA SURFACE REMOVAL TRANSITIONS:  
FROM 0" AT STA. 14+70 TO 1 1/2" THICKNESS AT STA. 15+00  
FROM 1 1/2" AT STA. 18+16 TO 0" THICKNESS AT STA. 18+46

P:\14\1400\1400-00023-RS\1400-00023-RS.dwg  
 USER: skm  
 PLOT DATE: 3/1/2016 1:24:23 PM



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0833' / 1in.     | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:24:23 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

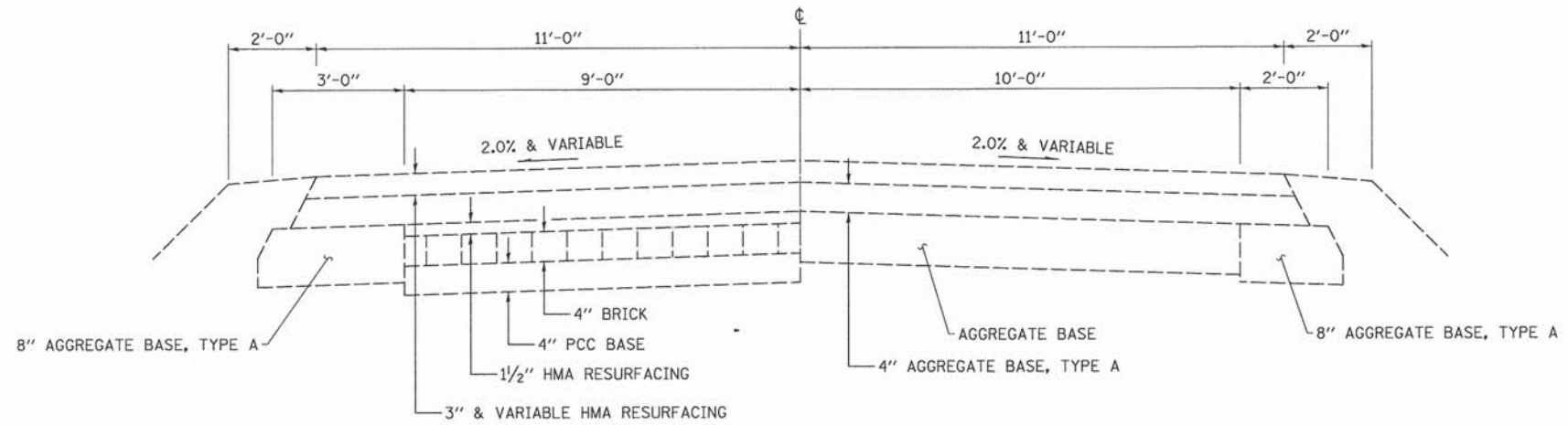
SCALE SHEET NO. 15 OF 17 SHEETS STA. TO STA.

|                     |                |        |                           |           |
|---------------------|----------------|--------|---------------------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT  | 76                        | 24        |
| FED. ROAD DIST. NO. |                |        | ILLINOIS FED. AID PROJECT |           |
| CONTRACT NO. 91516  |                |        |                           |           |

**EXISTING TYPICAL CROSS SECTION ⑩**

**ALLERTON ROAD**

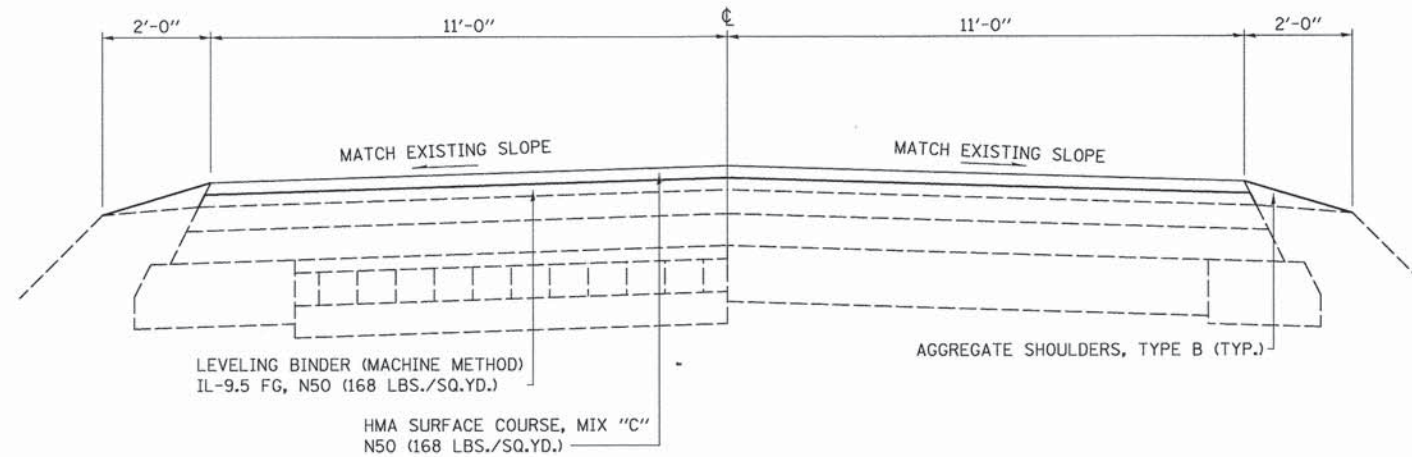
STATION TO STATION  
36+60 41+30



**PROPOSED TYPICAL CROSS SECTION ⑩**

**ALLERTON ROAD**

STATION TO STATION  
36+60 41+30



PRINT DRIVER = L:\00-ESCA\14-15\14-00023-RS\14-00023-RS-16.dwg  
 PLOT DATE = 3/1/2016 12:41:38 PM  
 PLOT SCALE = 0.0833' / 1"



|                                  |                |           |
|----------------------------------|----------------|-----------|
| USER NAME = skm                  | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61          | DRAWN - SKM    | REVISED - |
| PLDT SCALE = 0.0833' / 1"        | CHECKED - JMS  | REVISED - |
| PLDT DATE = 3/1/2016 12:41:38 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 16 OF 17 SHEETS STA. TO STA.

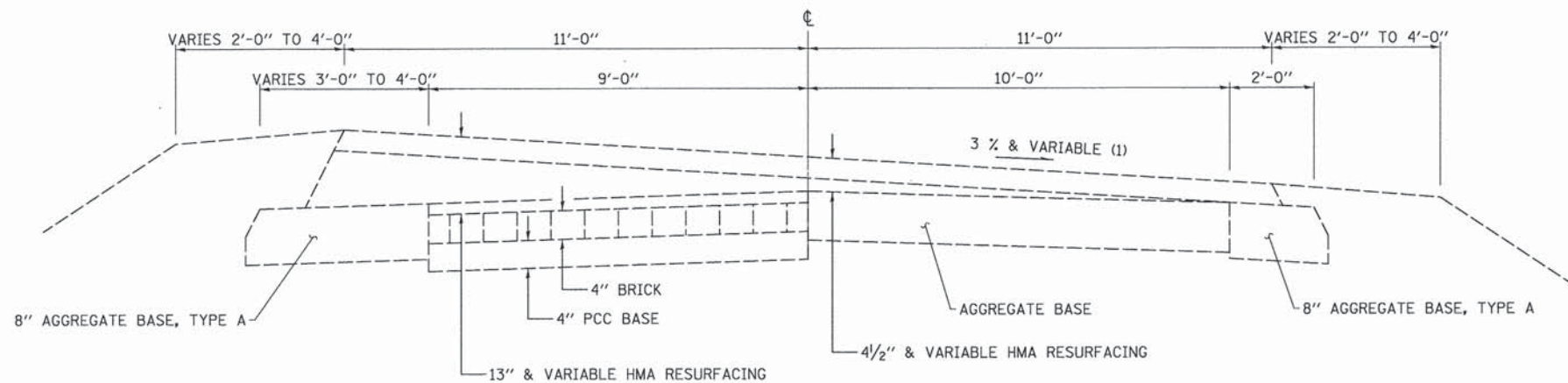
|                     |                |                           |              |           |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76           | 25        |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |              |           |
| CONTRACT NO. 91516  |                |                           |              |           |



**EXISTING TYPICAL CROSS SECTION ⑰**

**ALLERTON ROAD**

STATION TO STATION  
61+52.30 TO 65+51.79



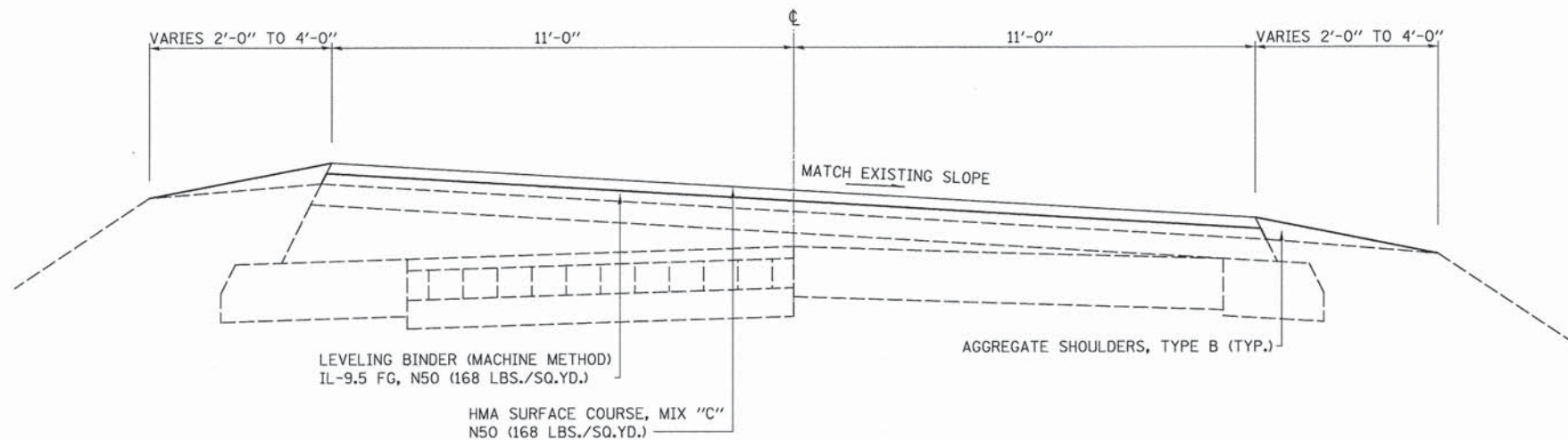
(1) SUPERELEVATION TRANSITIONS:  
STA. 61+52.30 TO STA. 63+02.30  
STA. 64+01.79 TO STA. 65+51.79

NOTE: THE EXISTING PAVEMENT STRUCTURE  
FROM STA. 63+23 TO STA. 63+73 IS PCC PAVEMENT, 8".

**PROPOSED TYPICAL CROSS SECTION ⑰**

**ALLERTON ROAD**

STATION TO STATION  
61+52.30 TO 65+51.79



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PLOT NAME = 719.61.dwg  
PLOT DATE = 3/1/2016 1:24:48 PM



|                                   |                |           |
|-----------------------------------|----------------|-----------|
| USER NAME = skm                   | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61           | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0033' / 1" = 1/300 | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:24:48 PM   | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE SHEET NO. 17 OF 17 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 26        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |

| CENTERLINE CONTROL COORDINATE TABLE |  |              |             |
|-------------------------------------|--|--------------|-------------|
| ALIGNMENT                           | DESCRIPTION  | NORTHING     | EASTING     |
| ☉ INDEPENDENCE ST.                  | P.O.T. STA. 120+00.00                                  | 1222433.5797 | 917603.3169 |
|                                     | P.O.T. STA. 123+00.00/<br>LAFAYETTE ST. STA. 135+36.50 | 1222730.8281 | 917643.9901 |
|                                     | P.O.T. STA. 123+00.00/<br>LAFAYETTE ST. STA. 135+39.80 | 1222731.2408 | 917640.7042 |
|                                     | P.O.T. STA. 125+67.15                                  | 1222995.9000 | 917677.2509 |
|                                     | P.O.T. STA. 130+64.37                                  | 1223489.2530 | 917739.1560 |
| ☉ WASHINGTON ST.                    | P.O.T. STA. 294+00.00                                  | 1223222.7063 | 917653.4704 |
|                                     | P.I. STA. 294+51.87                                    | 1223214.4091 | 917704.6690 |
|                                     | P.O.T. STA. 297+28.88                                  | 1223180.5853 | 917979.6079 |
|                                     | P.I. STA. 299+00.00                                    | 1223159.6910 | 918149.4493 |
|                                     | P.O.T. STA. 299+55.76                                  | 1223152.8020 | 918204.7850 |
| ☉ PIATT ST.                         | ☉ STA. 150+00.00                                       | 1222263.4982 | 918658.8629 |
|                                     | P.O.T. STA. 158+50.00                                  | 1223113.4705 | 918665.3958 |
| ☉ MARION ST.                        | P.O.T. STA. 100+00.00                                  | 1222546.3769 | 916899.0149 |
|                                     | P.I. STA. 115+34.60                                    | 1222737.6677 | 915376.3868 |
|                                     | P.O.T. STA. 116+00.00                                  | 1222745.3334 | 915311.4348 |
|                                     | P.C. STA. 2+32.66                                      | 1222776.1610 | 915080.8259 |

| CONTROL POINT COORDINATE TABLE |                           |                             |              |             |              |
|--------------------------------|---------------------------|-----------------------------|--------------|-------------|--------------|
| LOCATION                       | STATION AND OFFSET        | DESCRIPTION                 | NORTHING     | EASTING     | BM ELEVATION |
| INDEPENDENCE ST.               | OFF CHAIN                 | PK NAIL IN SIDEWALK         | 1222498.9870 | 917589.5406 | 661.91       |
|                                | STA. 123+46.23 31.28' RT. | PK NAIL IN ASPHALT          | 1222772.8002 | 917680.7777 | 663.80       |
|                                | STA. 125+18.15 24.41' RT. | PK NAIL IN SIDEWALK         | 1222944.2401 | 917695.3680 | 667.73       |
| WASHINGTON ST.                 | STA. 294+66.46 33.02' LT. | PK NAIL IN PAVEMENT         | 1223245.3975 | 917723.1882 | 667.75       |
|                                | STA. 295+97.75 27.36' RT. | PK NAIL IN SIDEWALK         | 1223169.4380 | 917846.1154 | 668.10       |
|                                | STA. 297+65.58 20.44' LT. | 5/8" REBAR WITH CAP IN TURF | 1223196.3866 | 918018.5328 | 667.47       |
|                                | OFF CHAIN                 | PK NAIL IN SIDEWALK         | 1223187.8250 | 918232.7000 | 668.75       |
| PIATT ST.                      | STA. 151+55.09 15.45' LT. | 5/8" REBAR IN TURF          | 1222418.7030 | 918644.6090 | 674.21       |
|                                | STA. 155+74.19 16.21' RT. | 5/8" REBAR IN TURF          | 1222837.5420 | 918679.4840 | 673.09       |
| MARION ST.                     | STA. 108+02.23 17.69' LT. | 5/8" REBAR IN TURF          | 1222628.8240 | 916100.8410 | 662.73       |
|                                | STA. 111+74.11 26.88' RT. | PK NAIL IN SIDEWALK         | 1222719.4022 | 915737.4123 | 673.88       |
|                                | STA. 115+47.33 24.69' LT. | PK NAIL IN SIDEWALK         | 1222714.6369 | 915360.8431 | 676.81       |
|                                | STA. 1+91.31 18.69' LT.   | 5/8" REBAR WITH CAP IN TURF | 1222752.1534 | 915119.3332 | 675.33       |
|                                | STA. 6+54.90 12.44' RT.   | 5/8" REBAR IN TURF          | 1222516.0301 | 914765.6122 | 678.47       |
|                                | STA. 14+79.88 12.52' LT.  | PK NAIL IN E.O.P            | 1221899.4224 | 914218.0771 | 674.31       |
|                                | STA. 22+36.78 13.97' RT.  | PK NAIL IN PAVEMENT         | N/A          | N/A         | 675.24       |
|                                | STA. 31+00.36 13.59' RT.  | 5/8" REBAR WITH CAP IN TURF | N/A          | N/A         | 682.06       |
|                                | STA. 47+93.30 14.07' LT.  | 5/8" REBAR IN TURF          | N/A          | N/A         | 669.94       |
|                                | STA. 60+46.34 14.75' LT.  | 5/8" REBAR WITH CAP IN TURF | N/A          | N/A         | 660.97       |

EXISTING ☉  
ALLERTON ROAD  
CURVE DATA  
Δ = 53°19'30"  
D = 54°27'9.4"  
R = 105.22'  
T = 52.83'  
L = 97.93'  
E = 12.52  
P.C. STA. = 2+32.66  
P.I. STA. = 2+85.49  
P.T. STA. = 3+03.79

EXISTING ☉  
ALLERTON ROAD  
CURVE DATA  
Δ = 4°30'10"  
D = 3°15'16.33"  
R = 1760.49'  
T = 69.21'  
L = 138.35'  
E = 1.36  
P.C. STA. = 4+48.48  
P.I. STA. = 5+17.69  
P.T. STA. = 5+86.83  
S.E. 0.0156 FT PER FT  
S.E. ATTAINED:  
STA. 3+81.81 TO STA. 4+81.81  
STA. 5+53.50 TO STA. 6+53.50

EXISTING ☉  
ALLERTON ROAD  
CURVE DATA  
Δ = 3°16'40"  
D = 1°03'22.28"  
R = 5424.77'  
T = 155.21'  
L = 310.34'  
E = 2.22  
P.C. STA. = 7+15.53  
P.I. STA. = 8+80.74  
P.T. STA. = 10+25.87

EXISTING ☉  
ALLERTON ROAD  
CURVE DATA  
Δ = 3°21'20"  
D = 0°57'10.3"  
R = 6013.09'  
T = 176.13'  
L = 352.16'  
E = 2.59  
P.C. STA. = 10+25.87  
P.I. STA. = 12+02.00  
P.T. STA. = 13+78.03

EXISTING ☉  
ALLERTON ROAD  
CURVE DATA  
Δ = 0°34'40"  
D = 0°13'14.52"  
R = 25961.03'  
T = 130.90'  
L = 261.79'  
E = 0.33  
P.C. STA. = 27+74.20  
P.I. STA. = 30+35.99  
P.T. STA. = 29+05.10

EXISTING ☉  
ALLERTON ROAD  
CURVE DATA  
Δ = 0°49'30"  
D = 1°03'30.39"  
R = 5401.89'  
T = 38.89'  
L = 77.78'  
E = 0.14  
P.C. STA. = BK 59+14.77  
AH 59+15.61  
P.I. STA. = 59+54.50  
P.T. STA. = 59+93.39

EXISTING ☉  
ALLERTON ROAD  
CURVE DATA  
Δ = 10°04'00"  
D = 5°02'46.76"  
R = 1135.00'  
T = 100'  
L = 199.49'  
E = 4.40  
P.C. STA. = 62+52.30  
P.I. STA. = 63+52.30  
P.T. STA. = 64+51.79  
S.E. = 0.03 FT. PER FT.  
S.E. ATTAINED:  
STA. 61+52.30 TO STA. 63+02.30  
STA. 64+01.79 TO STA. 65+51.79

EXISTING ☉  
ALLERTON ROAD  
CURVE DATA  
Δ = 1°11'40"  
D = 0°46'41.44"  
R = 7362.81'  
T = 76.75'  
L = 153.49'  
E = 0.40  
P.C. STA. = 67+11.29  
P.I. STA. = 67+88.04  
P.T. STA. = 68+64.78

### STATION EQUATIONS

MARION STREET STA. 116+00(BK) = STA. 0+00(AH)  
ALLERTON ROAD STA. 3+30.59(BK) = STA. 3+33.60(AH)  
ALLERTON ROAD STA. 59+14.77(BK) = STA. 59+15.61(AH)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ALIGNMENT AND CONTROL

|                     |                |                           |              |           |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76           | 27        |
| CONTRACT NO. 91516  |                |                           |              |           |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |              |           |

SCALE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.8933' / in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:08:00 PM | DATE - 02/2016 | REVISED - |













| INDEPENDENCE STREET<br>PCC REMOVAL AND REPLACEMENT |                     |                               |                                   |                       |                        |                         |                                    |                                    |
|--|---------------------|-------------------------------|-----------------------------------|-----------------------|------------------------|-------------------------|------------------------------------|------------------------------------|
| LOCATION   | 44000600            | 44000500                      | 42300200                          | 42400300              | 42400800               | Z0042300                | 60603900                           | 60604500                           |
|  | SIDEWALK<br>REMOVAL | COMBINATION<br>C&G<br>REMOVAL | PCC<br>DRIVEWAY<br>PAVEMENT<br>6" | PCC<br>SIDEWALK<br>6" | DETECTABLE<br>WARNINGS | PCC<br>SIDEWALK<br>CURB | COMB. CC&G<br>TYPE B-6.12<br>(AEP) | COMB. CC&G<br>TYPE B-6.18<br>(AEP) |
|  | SQ FT               | FOOT                          | SQ YD                             | SQ FT                 | SQ FT                  | FOOT                    | FOOT                               | FOOT                               |
| NW QUADRANT INDEPENDENCE ST. / MARION ST.          | 141.7               |                               |                                   | 150.1                 | 20.0                   |                         |                                    |                                    |
| NE QUADRANT INDEPENDENCE ST. / MARION ST.          | 153.6               |                               |                                   | 183.9                 | 20.0                   |                         |                                    |                                    |
| RT. STA. 120+39.3 TO RT. STA. 120+67.6             |                     | 39.8                          |                                   |                       |                        |                         |                                    |                                    |
| RT. STA. 120+39.3 TO RT. STA. 120+43.1             |                     |                               |                                   |                       |                        | 10.7                    |                                    |                                    |
| RT. STA. 120+43.1 TO RT. STA. 120+67.6             |                     |                               |                                   |                       |                        |                         | 29.1                               |                                    |
| LT. STA. 120+41.4 TO LT. STA. 120+70.2             |                     | 39.6                          |                                   |                       |                        | 39.6                    |                                    |                                    |
| LT. STA. 121+61.6 TO LT. STA. 121+67.6             |                     | 6.0                           |                                   |                       |                        | 6.0                     |                                    |                                    |
| SW QUADRANT INDEPENDENCE ST. / LAFAYETTE ST.       | 152.4               |                               |                                   | 174.1                 | 24.5                   |                         |                                    |                                    |
| SE QUADRANT INDEPENDENCE ST. / LAFAYETTE ST.       | 211.7               |                               |                                   | 248.2                 | 20.0                   |                         |                                    |                                    |
| LT. STA. 122+55.1 TO RT. STA. 135+28.9             |                     | 31.2                          |                                   |                       |                        | 31.2                    |                                    |                                    |
| RT. STA. 122+60.0 TO RT. STA. 135+98.6             |                     | 41.0                          |                                   |                       |                        |                         |                                    |                                    |
| RT. STA. 122+60.0 TO RT. STA. 135+87.9             |                     |                               |                                   |                       |                        |                         | 30.3                               |                                    |
| RT. STA. 135+87.9 TO RT. STA. 135+98.6             |                     |                               |                                   |                       |                        | 10.7                    |                                    |                                    |
| NW QUADRANT INDEPENDENCE ST. / LAFAYETTE ST.       | 130.6               |                               |                                   | 164.0                 | 21.0                   |                         |                                    |                                    |
| NE QUADRANT INDEPENDENCE ST. / LAFAYETTE ST.       | 209.5               |                               |                                   | 244.1                 | 20.0                   |                         |                                    |                                    |
| LT. STA. 135+28.7 TO LT. STA. 123+41.2             |                     | 33.0                          |                                   |                       |                        | 33.0                    |                                    |                                    |
| LT. STA. 135+96.2 TO RT. STA. 123+29.3             |                     | 21.0                          |                                   |                       |                        | 21.0                    |                                    |                                    |
| LT. STA. 124+26.3 TO LT. STA. 124+76.3             |                     | 50.0                          |                                   |                       |                        | 50.0                    |                                    |                                    |
| SW QUADRANT INDEPENDENCE ST. / MAIN ST.            | 266.9               |                               |                                   | 268.1                 | 15.9                   |                         |                                    |                                    |
| SE QUADRANT INDEPENDENCE ST. / MAIN ST.            | 182.4               |                               |                                   | 194.0                 | 20.0                   |                         |                                    |                                    |
| RT. STA. 125+30.2 TO RT. STA. 141+07.0             |                     | 47.9                          |                                   |                       |                        |                         |                                    |                                    |
| RT. STA. 125+30.2 TO RT. STA. 125+42.6             |                     |                               |                                   |                       |                        | 12.4                    |                                    |                                    |
| RT. STA. 125+42.6 TO RT. STA. 141+07.0             |                     |                               |                                   |                       |                        |                         | 35.5                               |                                    |
| LT. STA. 125+30.5 TO RT. STA. 140+25.7             |                     | 25.6                          |                                   |                       |                        |                         |                                    |                                    |
| LT. STA. 125+30.5 TO LT. STA. 125+37.8             |                     |                               |                                   |                       |                        | 7.3                     |                                    |                                    |
| LT. STA. 125+37.8 TO RT. STA. 140+25.7             |                     |                               |                                   |                       |                        |                         | 18.3                               |                                    |
| NW QUADRANT INDEPENDENCE ST. / MAIN ST.            | 153.0               |                               |                                   | 175.5                 | 16.5                   |                         |                                    |                                    |
| NE QUADRANT INDEPENDENCE ST. / MAIN ST.            | 229.9               |                               |                                   | 273.0                 | 20.0                   |                         |                                    |                                    |
| LT. STA. 140+32.4 TO RT. STA. 294+15.5             |                     | 194.3                         |                                   |                       |                        |                         |                                    |                                    |
| LT. STA. 140+32.4 TO LT. STA. 126+09.6             |                     |                               |                                   |                       |                        |                         | 25.6                               |                                    |
| LT. STA. 126+09.6 TO RT. STA. 294+26.5             |                     |                               |                                   |                       |                        | 157.4                   |                                    |                                    |
| RT. STA. 294+26.5 TO RT. STA. 294+15.5             |                     |                               |                                   |                       |                        |                         | 11.3                               |                                    |
| LT. STA. 141+00.3 TO RT. STA. 294+90.9             |                     | 217.6                         |                                   |                       |                        |                         |                                    |                                    |
| LT. STA. 141+00.3 TO LT. STA. 140+90.4             |                     |                               |                                   |                       |                        |                         | 10.2                               |                                    |
| LT. STA. 140+90.4 TO RT. STA. 294+79.0             |                     |                               |                                   |                       |                        | 195.1                   |                                    |                                    |
| RT. STA. 294+79.0 TO RT. STA. 294+90.9             |                     |                               |                                   |                       |                        |                         | 12.3                               |                                    |
| LT. STA. 126+82.8 TO LT. STA. 127+03.2             |                     |                               | 12.9                              |                       |                        |                         |                                    |                                    |
| SW QUADRANT INDEPENDENCE ST. / WASHINGTON ST.      | 1035.5              |                               |                                   | 851.2                 | 20.0                   |                         |                                    |                                    |
| SE QUADRANT INDEPENDENCE ST. / WASHINGTON ST.      | 210.3               |                               |                                   | 243.9                 | 29.2                   |                         |                                    |                                    |
| NE QUADRANT INDEPENDENCE ST. / WASHINGTON ST.      | 155.0               |                               |                                   | 179.8                 | 28.6                   |                         |                                    |                                    |
| LT. STA. 294+89.4 TO RT. STA. 128+21.8             |                     | 32.7                          |                                   |                       |                        |                         |                                    |                                    |
| LT. STA. 294+89.4 TO LT. STA. 294+79.0             |                     |                               |                                   |                       |                        |                         | 10.9                               |                                    |
| LT. STA. 294+79.0 TO RT. STA. 128+21.8             |                     |                               |                                   |                       |                        | 21.8                    |                                    |                                    |
| SE QUADRANT INDEPENDENCE ST. / LIVINGSTON ST.      | 305.7               |                               |                                   | 425.8                 | 20.0                   | 22.8                    |                                    |                                    |
| RT. STA. 130+22.7 TO RT. STA. 130+39.3             |                     | 27.5                          |                                   |                       |                        | 27.5                    |                                    |                                    |
| TOTALS   | 3539                | 808                           | 13                                | 3776                  | 296                    | 23                      | 624                                | 184                                |

| INDEPENDENCE STREET<br>ENTRANCE RESURFACING |   |  |   |                                |
|---|---|--|---|--------------------------------|
| LOCATION                                    | 40600985                                | X4400196                               | 40800029  | 40800050                       |
|   | PCC<br>SURFACE<br>REMOVAL<br>BUTT JOINT | HMA<br>SURFACE<br>REMOVAL<br>(SPECIAL) | BITUMINOUS<br>MATERIALS<br>(TACK COAT)<br>0.05 LB/SQ. FT. | INCIDENTAL<br>HMA<br>SURFACING |
|   | SQ YD                                   | SQ YD                                  | POUND   | TON                            |
| LT. STA. 121+34.5 TO LT. STA. 121+92.4      |   |  | 10.1  | 1.26                           |
| RT. STA. 121+51.5 TO RT. STA. 121+72.2      |   | 6.3                                    | 2.8   | 0.35                           |
| LT. STA. 122+79.7 TO LT. STA. 123+15.5      | 16.7                                    |  |   |                                |
| RT. STA. 124+22.3 TO RT. STA. 124+51.4      |   |  | 4.3   | 0.54                           |
| LT. STA. 125+25.9 TO RT. STA. 140+25.6      |   |  | 3.4   | 0.42                           |
| RT. STA. 125+25.9 TO RT. STA. 141+07.0      |   |  | 5.2   | 0.65                           |
| LT. STA. 140+28.7 TO LT. STA. 126+03.9      |   |  | 2.3   | 0.29                           |
| LT. STA. 141+00.3 TO RT. STA. 126+03.9      |   |  | 3.9   | 0.49                           |
| RT. STA. 128+91.5 TO RT. STA. 129+34.8      |   |  | 7.2   | 0.90                           |
| TOTALS                                      | 17                                      | 7                                      | 40  | 5                              |

PRINT PATH: I:\02-ESCA\1416\1416.dwg  
 USER: JLM  
 PLOT DATE: 3/1/2016 1:25:16 PM



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8833' / in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:25:16 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

| SCALE       |             | SCHEDULE OF QUANTITIES |         |
|-------------|-------------|------------------------|---------|
| SHEET NO. 3 | OF 9 SHEETS | STA.                   | TO STA. |

|   |                |        |                    |           |
|---|----------------|--------|--------------------|-----------|
| F.A. RTE.                                       | SECTION        | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76                 | 30        |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        | CONTRACT NO. 91516 |           |







**ALLERTON ROAD RESURFACING**

| STATION       | TO | STATION       | AVERAGE RESURFACING ROADWAY WIDTH | LENGTH | AREA    | 40600982                         | 44000155                  | 40600290                             | 40600627  | 40600627   | 40603310                                      |
|---------------|----|---------------|-----------------------------------|--------|---------|----------------------------------|---------------------------|--------------------------------------|---|--|---|
|               |    |               |                                   |        |         | HMA SURFACE REMOVAL - BUTT JOINT | HMA SURFACE REMOVAL, 1/2" | BITUMINOUS MATERIALS (TACK COAT) (2) | LEVELING BINDER (MM), IL-9.5FG, N50 84 LB/SQ. YD. | LEVELING BINDER (MM), IL-9.5FG, N50 168 LB/SQ. YD. | HMA SURFACE COURSE MIX "C" N50 168 LB/SQ. YD. |
|               |    |               | FOOT                              | FOOT   | SQ YD   | SQ YD                            | SQ YD                     | POUND                                | TON   | TON  | TON   |
| 4+05          |    | 4+35          | 22.0                              | 30.0   | 73.3    |                                  |                           |                                      | 3.1   |  |   |
| 4+05          |    | 59+14.77 (BK) | 22.0                              | 5509.8 | 13468.3 |                                  |                           | 9091.1                               |   |  | 1131.3  |
| 59+15.61 (AH) |    | 70+35         | 22.0                              | 1119.4 | 2736.3  |                                  |                           | 1847.0                               |   |  | 229.8   |
| 4+35          |    | 59+14.77 (BK) | 22.0                              | 5479.8 | 13395.0 |                                  |                           |                                      |   | 1125.2   |   |
| 59+15.61 (AH) |    | 69+75         | 22.0                              | 1059.4 | 2589.6  |                                  |                           |                                      |   | 217.5  |   |
| 14+70         |    | 18+46         | 22.0                              | 376.0  | 919.1   |                                  | 919.1                     |                                      |   |  |   |
| 69+75         |    | 70+05         | 22.0                              | 30.0   | 73.3    |                                  |                           |                                      | 3.1   |  |   |
| 70+05         |    | 70+35         | 22.0                              | 30.0   | 73.3    | 73.3                             |                           |                                      |   |  |   |
| TOTALS        |    |               |                                   |        |         | 74                               | 920                       | 10938                                | 6.2   | 1342.7   | 1362  |
|               |    |               |                                   |        |         |                                  |                           |                                      | 1349  |  |   |

(2) THE QUANTITY FOR TACK COAT INCLUDES AN APPLICATION RATE OF 0.05 LBS/SQ. FT. ON THE MILLED SURFACES AND ON THE EXISTING NON-MILLED SURFACES. IN ADDITION, AN APPLICATION RATE OF 0.025 LBS/SQ. FT. HAS BEEN INCLUDED FOR TACK COAT BETWEEN HMA LIFTS. THE TACK COAT BETWEEN HMA LIFTS SHALL BE APPLIED AS DIRECTED BY THE ENGINEER.

**ALLERTON ROAD SIDEROAD AND ENTRANCE RESURFACING**

| STATION         | TYPE          | DISTANCE FROM E.O.P. TO BACK OF RESURFACING | AVERAGE RESURFACING THICKNESS | AREA TO BE RESURFACED | X4400196                      | 40600985                         | 35101800                         | 40800029  | 40800050               |
|-----------------|---------------|---|-------------------------------|-----------------------|-------------------------------|----------------------------------|----------------------------------|---|------------------------|
|                 |               |   |                               |                       | HMA SURFACE REMOVAL (SPECIAL) | PCC SURFACE REMOVAL - BUTT JOINT | AGGREGATE BASE COURSE. TYPE B 6" | BITUMINOUS MATERIALS (TACK COAT) 0.05 LB/SQ.FT. | INCIDENTAL HMA SURFACE |
|                 |               |   | FOOT                          | INCH                  | SQ YD                         | SQ YD                            | SQ YD                            | POUND   | TON                    |
| GOLF CLUB ENTR. | C.E.          | 10.0  | 2.0                           | 38.9                  | 38.9                          |                                  |                                  | 17.5  | 4.4                    |
| RT. 5+08        | C.E.          | 10.0  | 2.25                          | 20.2                  |                               |                                  |                                  | 9.1   | 2.5                    |
| LT. 6+22        | BLANE ST.     | 15.0  | 2.25                          | 68.3                  | 68.3                          |                                  |                                  | 30.8  | 8.6                    |
| LT. 7+28        | P.E.          | 19.5  | 2.25                          | 78.2                  |                               |                                  |                                  | 35.2  | 9.9                    |
| LT. 9+60        | BRISTOW ST.   | 15.0  | 2.25                          | 79.2                  | 79.2                          |                                  |                                  | 35.6  | 10.0                   |
| LT. 10+70       | P.E.          | 10.0  | 2.25                          | 16.2                  |                               | 16.2                             |                                  | 7.3   | 2.0                    |
| LT. 12+98       | BOND ST.      | 15.0  | 2.25                          | 79.7                  | 79.7                          |                                  |                                  | 35.9  | 10.0                   |
| LT. 14+38       | P.E.          | 8.0   | 2.25                          | 99.1                  | 99.1                          |                                  |                                  | 44.6  | 12.5                   |
| RT. 15+00 (1)   | GUTTER        | 2.0   | 1.5                           | 70.2                  |                               | 17.6                             |                                  | 31.6  | 5.9                    |
| LT. 16+80       | WILLIAMS ST.  | 15.0  | 1.5                           | 106.8                 | 106.8                         |                                  |                                  | 48.1  | 9.0                    |
| RT. 20+32       | C.E.          | 10.0  | 2.5                           | 46.5                  |                               |                                  | 53.3                             |   | 6.5                    |
| LT. 22+15       | F.E.          | 3.0   | 2.75                          | 12.5                  |                               |                                  |                                  | 5.6   | 1.9                    |
| RT. 22+40       | C.E.          | 10.0  | 2.25                          | 40.3                  | 40.3                          |                                  |                                  | 18.2  | 5.1                    |
| RT. 26+05       | C.E.          | 10.0  | 2.25                          | 25.3                  | 25.3                          |                                  |                                  | 11.4  | 3.2                    |
| RT. 35+75       | P.E.          | 10.0  | 2.25                          | 23.2                  |                               |                                  |                                  | 10.4  | 2.9                    |
| RT. 36+85       | P.E.          | 10.0  | 2.25                          | 20.5                  |                               |                                  |                                  | 9.2   | 2.6                    |
| LT. 36+10       | F.E.          | 3.0   | 2.75                          | 5.4                   |                               |                                  |                                  | 2.4   | 0.8                    |
| LT. 36+64       | M.B. TURNOUT  | 6.0   | 2.5                           | 27.7                  |                               |                                  |                                  | 12.5  | 3.9                    |
| RT. 48+68       | P.E.          | 10.0  | 2.25                          | 22.3                  |                               |                                  |                                  | 10.0  | 2.8                    |
| LT. 48+75       | M.B. TURNOUT  | 6.0   | 2.5                           | 33.0                  |                               |                                  |                                  | 14.9  | 4.6                    |
| RT. 55+88       | F.E.          | 3.0   | 2.75                          | 11.6                  |                               |                                  |                                  | 5.2   | 1.8                    |
| RT. 57+37       | F.E.          | 3.0   | 2.75                          | 11.6                  |                               |                                  |                                  | 5.2   | 1.8                    |
| RT. 57+90       | P.E.          | 10.0  | 2.25                          | 25.3                  |                               | 25.3                             |                                  | 11.4  | 3.2                    |
| LT. 57+90       | M.B. TURNOUT  | 6.0   | 2.5                           | 26.3                  |                               |                                  | 33.1                             |   | 3.7                    |
| RT. 59+94       | F.E.          | 3.0   | 2.75                          | 8.9                   |                               |                                  |                                  | 4.0   | 1.4                    |
| RT. 61+80       | P.E.          | 10.0  | 2.25                          | 22.7                  | 22.7                          |                                  |                                  | 10.2  | 2.9                    |
| LT. 67+30       | M.B. TURNOUT  | 6.0   | 2.5                           | 30.4                  |                               |                                  |                                  | 13.7  | 4.3                    |
| LT. 67+50       | P.E.          | 10.0  | 2.25                          | 40.0                  | 40.0                          |                                  |                                  | 18.0  | 5.0                    |
| RT. 68+28       | GRANDVIEW DR. | 10.0  | 2.25                          | 60.8                  |                               | 60.8                             |                                  | 27.4  | 7.7                    |
| TOTALS          |               |   |                               |                       | 601                           | 120                              | 87                               | 486   | 141                    |

(1) THE INCIDENTAL HMA SURFACING IS FROM RT. STA. 15+00 TO RT. STA. 18+16 AND REQUIRES A 6" WIDE PCC SURFACE REMOVAL - BUTT JOINT

**ALLERTON ROAD AGGREGATE SHOULDERS**

| SHOULDER LOCATION | STATION       | TO | STATION       | AVERAGE SHOULDER WIDTH | LENGTH | CROSS SECTION AREA | 48101200                                   |
|-------------------|---------------|----|---------------|------------------------|--------|--------------------|--|
|                   |               |    |               |                        |        |                    | AGGREGATE SHOULDERS, TYPE B 1.9 TON/CU.YD. |
|                   |               |    |               | FOOT                   | FOOT   | SQ FT              | TON  |
| LT.               | 4+05          |    | 6+00          | 2.0                    | 195.0  | 0.42               | 5.8  |
| LT.               | 6+41          |    | 9+45          | 2.0                    | 304.0  | 0.42               | 9.0  |
| LT.               | 9+75          |    | 12+83         | 2.0                    | 308.0  | 0.42               | 9.1  |
| LT.               | 13+43         |    | 15+00         | 2.0                    | 157.0  | 0.42               | 4.6  |
| LT.               | 15+00         |    | 16+62         | 2.0                    | 162.0  | 0.29               | 3.3  |
| LT.               | 17+00         |    | 18+16         | 2.0                    | 116.0  | 0.29               | 2.4  |
| LT.               | 18+16         |    | 59+14.77 (BK) | 2.0                    | 4098.8 | 0.42               | 121.1                                      |
| LT.               | 59+15.61 (AH) |    | 61+52.3       | 2.0                    | 236.7  | 0.42               | 7.0  |
| LT.               | 61+52.3       |    | 65+51.79      | 3.0                    | 399.5  | 0.63               | 17.7                                       |
| LT.               | 65+51.79      |    | 70+35         | 2.0                    | 483.2  | 0.42               | 14.3                                       |
| RT.               | 4+05          |    | 15+00         | 2.0                    | 1095.0 | 0.42               | 32.4                                       |
| RT.               | 18+16         |    | 59+14.77 (BK) | 2.0                    | 4098.8 | 0.42               | 121.1                                      |
| RT.               | 59+15.61 (AH) |    | 61+52.3       | 2.0                    | 236.7  | 0.42               | 7.0  |
| RT.               | 61+52.3       |    | 65+51.79      | 3.0                    | 399.5  | 0.63               | 17.7                                       |
| RT.               | 65+51.79      |    | 68+10         | 2.0                    | 258.2  | 0.42               | 7.6  |
| RT.               | 68+45         |    | 70+35         | 2.0                    | 190.0  | 0.42               | 5.6  |
| TOTAL             |               |    |               |                        |        |                    | 386  |

NOTE: THE AGGREGATE SHOULDERS, TYPE B QUANTITY PROVIDES MATERIAL TO CONSTRUCT THE WEDGE SHOWN ON THE TYPICAL CROSS SECTIONS PLUS 1" ADDITIONAL DEPTH FOR LOW SHOULDERS.

PRINTED AT: 10/11/16 10:08 AM  
 FILE NAME: C:\Users\jms\Documents\14-00023-00-RS\14-00023-00-RS.dwg  
 PLOT DATE: 10/11/16 10:08 AM



USER NAME = jms  
 ESCA PROJECT NO. 719.61  
 PLOT SCALE = 0.8033' / 1"

DESIGNED - JMS  
 DRAWN - SKM  
 CHECKED - JMS  
 DATE - 01/2016

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES**

SCALE SHEET NO. 5 OF 9 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                       | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 32        |
| CONTRACT NO. 91516                              |                |        |              |           |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        |              |           |



**PAVEMENT MARKING SCHEDULE**

| LOCATION                           | DESCRIPTION            | 78000100   | 78000200   | 78000200  | 78000600  | 78000600   | 78000650  | 78001110                                 | 78001110                                  | 78300100                 | 70300100                    | 70300150                            |
|------------------------------------|------------------------|--|--|---|---|--|---|--|---|--------------------------|-----------------------------|-------------------------------------|
|                                    |                        | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE) | THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW) | THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE) | THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW) | THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE) | PAINT PAVEMENT MARKING - LINE 4" (WHITE) | PAINT PAVEMENT MARKING - LINE 4" (YELLOW) | PAVEMENT MARKING REMOVAL | SHORT TERM PAVEMENT MARKING | SHORT TERM PAVEMENT MARKING REMOVAL |
|                                    |                        | SQ FT  | FOOT   | FOOT  | FOOT  | FOOT   | FOOT  | FOOT                                     | FOOT                                      | SQ FT                    | FOOT                        | SQ FT                               |
| WASHINGTON STREET                  |                        |  |  |   |   |  |   |  |   |                          |                             |                                     |
| STA. 295+18.9 TO STA. 299+28.9     | CENTERLINE SKIP        |  |  | 110.0   |   |  |   |  |   |                          | 120.0                       | 13.3                                |
| SUBTOTALS                          |                        |  |  | 110.0   |   |  |   |  |   |                          | 120.0                       | 13.3                                |
| MARION STREET                      |                        |  |  |   |   |  |   |  |   |                          |                             |                                     |
| STA. 100+81.5 TO STA. 100+93.2     | PAINTED MEDIAN         |  |  |   |   |  |   |  |   | 21.6                     |                             |                                     |
| STA. 100+82.3 TO STA. 101+42.2     | PAINTED MEDIAN         |  |  | 245.2   |   |  |   |  |   |                          | 24.0                        | 2.7                                 |
| STA. 100+86.4 TO STA. 101+33.0     | PAINTED MEDIAN         |  |  |   |   | 19.6   |   |  |   |                          |                             |                                     |
| STA. 101+39.8 TO STA. 103+20.7     | DOUBLE CENTERLINE      |  |  | 361.8   |   |  |   |  |   |                          | 60.0                        | 6.7                                 |
| RT. STA. 101+60.7                  | RR STOP BAR            |  |  |   |   |  |   | 16.0                                     |   |                          |                             |                                     |
| RT. STA. 101+85.6                  | RR CROSSING SYMBOL     | 61.2   |  |   |   |  |   |  |   |                          |                             |                                     |
| RT. STA. 102+10.7                  | RR STOP BAR            |  |  |   |   |  |   | 16.0                                     |   |                          |                             |                                     |
| RT. STA. 103+09.7                  | RR STOP BAR            |  |  |   |   |  |   | 16.5                                     |   |                          |                             |                                     |
| STA. 103+40.4 TO STA. 105+11.4     | DOUBLE CENTERLINE      |  |  | 342.0   |   |  |   |  |   |                          | 48.0                        | 5.3                                 |
| LT. STA. 103+51                    | RR STOP BAR            |  |  |   |   |  |   | 11.7                                     |   |                          |                             |                                     |
| RT. STA. 104+45 TO RT. STA. 105+42 | PAINTED TRANSITION     |  | 120.3  |   |   |  |   |  |   |                          |                             |                                     |
| RT. STA. 104+35 TO RT. STA. 105+56 | PAINTED TRANSITION     |  |  |   | 39.5  |  |   |  |   |                          |                             |                                     |
| LT. STA. 104+50.4                  | RR STOP BAR            |  |  |   |   |  |   | 11.7                                     |   |                          |                             |                                     |
| LT. STA. 104+79.0                  | RR CROSSING SYMBOL     | 61.2   |  |   |   |  |   |  |   |                          |                             |                                     |
| RT. STA. 105+00.4                  | STOP BAR               |  |  |   |   |  |   | 11.7                                     |   |                          |                             |                                     |
| STA. 105+41.4 TO STA. 107+91.4     | CENTERLINE SKIP        |  |  | 70.0  |   |  |   |  |   |                          | 72.0                        | 8.0                                 |
| RT. STA. 2+08.1                    | STOP BAR               |  |  |   |   |  |   | 15.2                                     |   |                          |                             |                                     |
| SUBTOTALS                          |                        | 122.4  | 120.3  | 1019.0  | 39.5  | 19.6   | 98.8  |  |   | 21.6                     | 204.0                       | 22.7                                |
| ALLERTON ROAD                      |                        |  |  |   |   |  |   |  |   |                          |                             |                                     |
| RT. ALLERTON RD TO RT. STA. 18+20  | EDGE LINE AND STOP BAR |  | 1580.6   |   |   |  |   | 11.0                                     |   |                          |                             |                                     |
| LT. STA. 3+19.7                    | STOP BAR               |  |  |   |   |  |   | 12.2                                     |   |                          |                             |                                     |
| STA. 3+46 TO STA. 18+20            | CENTERLINE SKIP        |  |  | 370.0   |   |  |   |  |   |                          | 408.0                       | 45.3                                |
| LT. STA. 3+19 TO LT. STA. 5+70     | EDGE LINE              |  | 251.2  |   |   |  |   |  |   |                          |                             |                                     |
| LT. STA. 6+41 TO LT. STA. 9+08     | EDGE LINE              |  | 267.4  |   |   |  |   |  |   |                          |                             |                                     |
| LT. STA. 9+76 TO LT. STA. 12+44    | EDGE LINE              |  | 267.6  |   |   |  |   |  |   |                          |                             |                                     |
| LT. STA. 13+14 TO LT. STA. 16+05   | EDGE LINE              |  | 291.8  |   |   |  |   |  |   |                          |                             |                                     |
| LT. STA. 17+04 TO LT. STA. 18+20   | EDGE LINE              |  | 116.0  |   |   |  |   |  |   |                          |                             |                                     |
| RT. STA. 18+20 TO RT. STA. 70+35   | EDGE LINE              |  |  |   |   |  |   | 5215.0                                   |   |                          |                             |                                     |
| STA. 18+20 TO STA. 63+00           | CENTERLINE SKIP        |  |  |   |   |  |   |  | 1120.0                                    |                          | 1224.0                      | 136.0                               |
| LT. STA. 18+20 TO LT. STA. 70+35   | EDGE LINE              |  |  |   |   |  |   | 5215.0                                   |   |                          |                             |                                     |
| RT. STA. 22+60 TO RT. STA. 27+59   | NO PASSING LINE        |  |  |   |   |  |   |  | 499.0                                     |                          |                             |                                     |
| LT. STA. 30+61 TO LT. STA. 32+89   | NO PASSING LINE        |  |  |   |   |  |   |  | 228.0                                     |                          |                             |                                     |
| LT. STA. 37+30 TO LT. STA. 43+91   | NO PASSING LINE        |  |  |   |   |  |   |  | 661.0                                     |                          |                             |                                     |
| RT. STA. 51+62 TO RT. STA. 58+63   | NO PASSING LINE        |  |  |   |   |  |   |  | 701.0                                     |                          |                             |                                     |
| LT. STA. 59+50 TO LT. STA. 66+60   | NO PASSING LINE        |  |  |   |   |  |   |  | 710.0                                     |                          |                             |                                     |
| RT. STA. 63+00 TO RT. STA. 66+60   | NO PASSING LINE        |  |  |   |   |  |   |  | 360.0                                     |                          |                             |                                     |
| STA. 66+60 TO STA. 70+35           | CENTERLINE SKIP        |  |  |   |   |  |   |  | 100.0                                     |                          | 108.0                       | 12.0                                |
| RT. STA. 69+33 TO RT. STA. 70+35   | NO PASSING LINE        |  |  |   |   |  |   |  | 102.0                                     |                          |                             |                                     |
| SUBTOTALS                          |                        |  | 2774.6   | 370.0   |   |  | 23.2  | 10430.0                                  | 4481.0                                    |                          | 1740.0                      | 193.3                               |
| TOTALS                             |                        | 123  | 4394   |   | 60  |  | 122   | 14911                                    |   | 22                       | 2064                        | 230                                 |

NOTE: SHORT TERM PAVEMENT MARKING QUANTITIES REFLECT THREE APPLICATIONS AND ONE REMOVAL.

PLOT DATE = 3/1/2016 1:25:28 PM  
 USER NAME = skm  
 PROJECT NAME = ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PLOT SCALE = 0.8033' / in.



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.8033' / in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:25:28 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES**

SCALE SHEET NO. 6 OF 9 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 33        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |







**DRAINAGE STRUCTURE AND WATER VALVE BOX ADJUSTMENTS**

| LOCATION            | STRUCTURE NO. | STATION<br>± STRUCTURE | OFFSET<br>± STRUCTURE | 60255500                | X6025600                          | 60255800  | X6025800   | X6025602  | 60260100              | 60260300  | 60261320  | 60261530   | 60236825                               | 60266600                   | X6026624                             |
|---------------------|---------------|------------------------|-----------------------|-------------------------|-----------------------------------|---|--|---|-----------------------|---|---|--|--|----------------------------|--------------------------------------|
|                     |               |                        |                       | MANHOLES TO BE ADJUSTED | MANHOLES TO BE ADJUSTED (SPECIAL) | MANHOLES TO BE ADJUSTED W/ NEW TYPE 1 FRAME, CLOSED LID | MANHOLES TO BE ADJ. W/ NEW TYPE 1 FRAME, CLOSED LID, SPECIAL | MANHOLES TO BE ADJUSTED W/ FRAME & GRATE, (SPECIAL) | INLETS TO BE ADJUSTED | INLETS TO BE ADJUSTED W/ NEW TYPE 1 FRAME, OPEN LID | INLETS TO BE ADJUSTED W/ NEW TYPE 11V FRAME & GRATE | INLETS TO BE ADJUSTED W/ NEW TYPE 23 FRAME & GRATE | INLETS, TYPE A, TYPE 11V FRAME & GRATE | VALVE BOXES TO BE ADJUSTED | VALVE BOXES TO BE ADJUSTED (SPECIAL) |
|                     |               |                        |                       | EACH                    | EACH                              | EACH  | EACH   | EACH  | EACH                  | EACH  | EACH  | EACH   | EACH                                   | EACH                       | EACH                                 |
| WASHINGTON STREET   | ①             | 296+92.28              | 19.13' RT.            |                         |                                   |   |  |   |                       |   |   |  | 1                                      |                            |                                      |
|                     | ②             | 296+94.80              | 16.80' RT.            |                         |                                   |   |  |   |                       | 1   |   |  |  |                            |                                      |
|                     | ③             | 297+65.88              | 13.00' RT.            |                         |                                   |   |  |   |                       |   |   |  | 1                                      |                            |                                      |
|                     | ④             | 297+11.65              | 15.78' LT.            |                         |                                   |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ⑤             | 146+02.20              | 15.90' LT.            |                         | 1                                 |   |  |   |                       |   |   | 1  |  |                            |                                      |
| SUBTOTALS           |               |                        |                       |                         | 1                                 |   |  |   |                       | 1   | 1   |  | 2                                      |                            |                                      |
| INDEPENDENCE STREET | ⑥             | 120+45.14              | 23.19' RT.            |                         |                                   |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ⑦             | 120+48.06              | 19.24' LT.            |                         |                                   |   |  |   |                       |   |   |  |  | 1                          |                                      |
|                     | ⑧             | 120+49.57              | 18.10' LT.            |                         |                                   |   |  |   |                       |   |   |  | 1                                      |                            |                                      |
|                     | ⑨             | 121+64.63              | 15.33' LT.            |                         |                                   |   |  |   |                       |   |   |  | 1                                      |                            |                                      |
|                     | ⑩             | 123+14.52              | 17.76' LT.            |                         | 1                                 |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ⑪             | 123+15.09              | 18.06' RT.            |                         | 1                                 |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ⑫             | 123+19.00              | 22.19' RT.            |                         |                                   |   |  |   |                       |   |   | 1  |  |                            |                                      |
|                     | ⑬             | 123+20.96              | 23.47' LT.            |                         |                                   |   |  |   |                       |   |   |  | 1                                      |                            |                                      |
|                     | ⑭             | 125+45.83              | 19.60' RT.            |                         |                                   |   |  |   |                       |   |   | 1  |  |                            |                                      |
|                     | ⑮             | 125+88.38              | 21.24' RT.            |                         |                                   |   |  |   |                       |   |   | 1  |  |                            |                                      |
|                     | ⑯             | 125+90.38              | 26.91' LT.            |                         |                                   |   |  |   |                       |   |   |  | 1                                      |                            |                                      |
|                     | ⑰             | 127+56.27              | 19.83' LT.            |                         |                                   |   |  |   |                       |   |   |  | 1                                      |                            |                                      |
|                     | ⑱             | 127+95.88              | 12.64' LT.            |                         |                                   |   |  |   |                       |   |   |  |  |                            | 1                                    |
|                     | ⑲             | 128+04.01              | 16.80' LT.            |                         |                                   |   |  |   |                       |   |   |  |  |                            | 1                                    |
|                     | ⑳             | 128+04.78              | 18.77' LT.            |                         |                                   |   | 1  |   |                       |   |   |  |  |                            |                                      |
|                     | ㉑             | 130+34.56              | 24.76' RT.            |                         |                                   |   |  | 1   |                       |   |   | 1  |  |                            |                                      |
|                     | ㉒             | 130+35.75              | 20.74' RT.            |                         |                                   |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ㉓             | 130+36.10              | 26.87' LT.            |                         |                                   |   |  |   |                       |   |   | 1  |  |                            |                                      |
| SUBTOTALS           |               |                        |                       |                         | 2                                 |   | 2  |   |                       |   | 6   | 4  | 1                                      | 1                          | 2                                    |
| MARION STREET       | ㉔             | 102+71.24              | 17.85' RT.            |                         |                                   |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ㉕             | 103+04.21              | 10.16' LT.            | 1                       |                                   |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ㉖             | 103+42.15              | 2.63' LT.             |                         | 1                                 |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ㉗             | 103+49.03              | 18.73' RT.            |                         |                                   |   |  |   |                       |   |   |  | 1                                      |                            |                                      |
|                     | ㉘             | 103+54.80              | 13.83' LT.            |                         |                                   |   |  |   |                       |   |   |  | 1                                      |                            |                                      |
|                     | ㉙             | 104+34.55              | 9.06' LT.             |                         | 1                                 |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ㉚             | 104+59.44              | 18.16' RT.            |                         |                                   |   |  |   |                       | 1   |   |  |  |                            |                                      |
|                     | ㉛             | 106+78.84              | 3.05' LT.             |                         |                                   |   |  |   | 1                     |   |   |  |  |                            |                                      |
|                     | ㉜             | 106+78.84              | 11.18' RT.            |                         |                                   |   |  |   |                       |   |   | 1  |  |                            |                                      |
|                     | ㉝             | 107+80.38              | 5.16' LT.             |                         |                                   |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ㉞             | 108+42.90              | 12.58' LT.            |                         |                                   |   | 1 (2)  |   |                       |   |   |  |  |                            |                                      |
|                     | ㉟             | 108+56.89              | 16.45' RT.            |                         |                                   |   |  |   |                       |   |   |  | 1                                      |                            |                                      |
|                     | ㊱             | 108+59.17              | 16.18' LT.            |                         |                                   |   |  |   |                       |   |   |  | 1                                      |                            |                                      |
|                     | ㊲             | 111+22.37              | 21.35' RT.            |                         |                                   |   |  |   |                       |   |   |  |  |                            | 1                                    |
|                     | ㊳             | 111+32.00              | 0.51' LT.             |                         |                                   |   | 1 (2)  |   |                       |   |   |  |  |                            |                                      |
| ㊴                   | 111+60.51     | 16.97' LT.             |                       |                         |                                   |   |  |   |                       |   | 1   |  |  |                            |                                      |
| ㊵                   | 111+60.73     | 16.30' RT.             |                       |                         |                                   |   |  |   |                       |   | 1   |  |  |                            |                                      |
| ㊶                   | 113+42.82     | 11.59' RT.             |                       |                         |                                   |   |  | 1   |                       |   |   |  |  |                            |                                      |
| ㊷                   | 114+56.52     | 0.17' RT.              |                       |                         |                                   | 1 (2)   |  |   |                       |   |   |  |  |                            |                                      |
| ㊸                   | 115+99.57     | 1.94' RT.              |                       |                         |                                   | 1 (2)   |  |   |                       |   |   |  |  |                            |                                      |
| SUBTOTALS           |               |                        |                       | 1                       | 2                                 |   | 4  | 3   | 1                     |   | 5   | 3  |  |                            | 1                                    |
| ALLERTON ROAD       | ㊹             | 6+15.76                | 13.57' LT.            |                         |                                   |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ㊺             | 9+40.31                | 23.04' LT.            |                         |                                   |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ㊻             | 12+82.08               | 18.50' LT.            |                         |                                   |   |  |   |                       |   |   |  |  |                            |                                      |
|                     | ㊼             | 16+56.48               | 17.19' LT.            | 1                       |                                   |   |  |   |                       |   |   |  |  |                            |                                      |
| SUBTOTALS           |               |                        |                       | 1                       |                                   | 3   |  |   |                       |   |   |  |  |                            |                                      |
| TOTALS              |               |                        |                       | 2                       | 5                                 | 3   | 6  | 3   | 1                     | 1   | 12  | 7  | 3                                      | 1                          | 3                                    |

(1) THE NEW FRAME AND GRATE SHALL BE A TYPE 1 FRAME, OPEN LID.  
 (2) THE EXISTING STRUCTURE IS A SANITARY MANHOLE. THE WORD SANITARY SHALL BE CAST INTO THE PROPOSED LID.

P:\1400\14000000\14000000.dwg  
 ESCA PROJECT NO. 719.61  
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USER NAME = skm  
 ESCA PROJECT NO. 719.61  
 PLOT SCALE = 0.8033' / in.  
 PLOT DATE = 3/1/2016 1:29:38 PM

DESIGNED - JMS  
 DRAWN - JLF  
 CHECKED - JMS  
 DATE - 01/2016

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES**  
 SCALE SHEET NO. 8 OF 9 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 35        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |



| TEMPORARY RAMP      |          |    |          |        |          |                |
|---------------------|----------|----|----------|--------|----------|----------------|
| LOCATION            | STATION  | TO | STATION  | LENGTH | WIDTH    | 40600990       |
|                     |          |    |          |        |          | TEMPORARY RAMP |
|                     |          |    |          | FOOT   | FOOT     | SQ YD          |
| PIATT STREET        | 151+04   |    | 151+13.2 | 9.2    | 22.0     | 22.5           |
| LAFAYETTE STREET    | SIDEROAD |    | LT.      | 9.2    | 31.0     | 31.7           |
| MAIN STREET         | SIDEROAD |    | LT.      | 9.2    | 23.0     | 23.5           |
| PIATT STREET        | 157+95.8 |    | 158+05   | 9.2    | 22.0     | 22.5           |
|                     |          |    |          |        | SUBTOTAL | 100.2          |
| HAMILTON STREET     | 145+29   |    | 145+35.7 | 6.7    | 28.9     | 21.5           |
| HAMILTON STREET     | 146+07.3 |    | 146+14   | 6.7    | 28.9     | 21.5           |
| WASHINGTON STREET   | 299+45.3 |    | 299+52   | 6.7    | 23.0     | 17.1           |
|                     |          |    |          |        | SUBTOTAL | 60.1           |
| INDEPENDENCE STREET | 120+39   |    | 120+45.7 | 6.7    | 28.9     | 21.5           |
| LAFAYETTE STREET    | 135+33.0 |    | 135+39.7 | 6.7    | 35.7     | 26.6           |
| LAFAYETTE STREET    | 135+91.9 |    | 135+98.6 | 6.7    | 29.2     | 21.7           |
| INDEPENDENCE STREET | 125+19.3 |    | 125+26   | 6.7    | 31.3     | 23.3           |
| INDEPENDENCE STREET | 126+04   |    | 126+10.7 | 6.7    | 31.9     | 23.7           |
| WASHINGTON STREET   | 294+15.5 |    | 294+22.2 | 6.7    | 39.0     | 29.0           |
| INDEPENDENCE STREET | 130+34.3 |    | 130+41   | 6.7    | 34.9     | 26.0           |
|                     |          |    |          |        | SUBTOTAL | 171.8          |
| MARION STREET       | 100+93.2 |    | 100+98.2 | 5.0    | 30.0     | 16.7           |
| MARION STREET       | 103+17.6 |    | 103+22.6 | 5.0    | 31.6     | 17.6           |
| MARION STREET       | 103+38.8 |    | 103+44.6 | 5.8    | 30.0     | 19.3           |
| PARK STREET         | SIDEROAD |    | LT.      | 5.8    | 23.8     | 15.3           |
| PARK STREET         | SIDEROAD |    | RT.      | 5.8    | 22.3     | 14.4           |
| CENTENNIAL STREET   | SIDEROAD |    | LT.      | 5.8    | 20.9     | 13.5           |
| CENTENNIAL STREET   | SIDEROAD |    | RT.      | 5.8    | 21.4     | 13.8           |
| UNION STREET        | SIDEROAD |    | LT.      | 5.8    | 18.2     | 11.7           |
|                     |          |    |          |        | SUBTOTAL | 122.3          |
| ALLERTON ROAD       | 70+30    |    | 70+35    | 5.0    | 23.0     | 12.8           |
|                     |          |    |          |        | SUBTOTAL | 12.8           |
|                     |          |    |          |        | TOTAL    | 467            |

| PAVEMENT PATCHES    |              |    |              |                             |                              |                              |                               |                                |                               |    |
|---------------------|--------------|----|--------------|-----------------------------|------------------------------|------------------------------|-------------------------------|--------------------------------|-------------------------------|----|
| LOCATION            | STATION      | TO | STATION      | 44201325                    | 44201741                     | 44201747                     | 44201777                      | 44201781                       | 44201783                      |    |
|                     |              |    |              | CLASS C PATCHES, TYPE I, 8" | CLASS D PATCHES, TYPE II, 8" | CLASS D PATCHES, TYPE IV, 8" | CLASS D PATCHES, TYPE II, 11" | CLASS D PATCHES, TYPE III, 11" | CLASS D PATCHES, TYPE IV, 11" |    |
|                     |              |    |              | SQ YD                       | SQ YD                        | SQ YD                        | SQ YD                         | SQ YD                          | SQ YD                         |    |
| PIATT STREET        | RT. 151+47.7 |    | RT. 151+64.6 |                             | 14.6                         |                              |                               |                                |                               |    |
|                     | RT. 153+00   |    | RT. 153+35   |                             |                              | 31.1                         |                               |                                |                               |    |
|                     |              |    |              | SUBTOTALS                   | 14.6                         | 31.1                         |                               |                                |                               |    |
| WASHINGTON STREET   | RT. 296+91.9 |    | RT. 296+97.9 | 2.7                         |                              |                              |                               |                                |                               |    |
|                     | LT. 298+42.3 |    | LT. 298+57.3 |                             | 10.0                         |                              |                               |                                |                               |    |
|                     |              |    |              | SUBTOTALS                   | 2.7                          | 10.0                         |                               |                                |                               |    |
| INDEPENDENCE STREET | LT. 120+44.1 |    | LT. 120+50.4 | 1.6                         |                              |                              |                               |                                |                               |    |
|                     | RT. 121+92.5 |    | RT. 122+07.5 |                             | 13.3                         |                              |                               |                                |                               |    |
|                     |              |    |              | SUBTOTALS                   | 1.6                          | 13.3                         |                               |                                |                               |    |
| MARION STREET       | RT. 102+99.4 |    | RT. 103+14.4 |                             |                              |                              |                               |                                | 28.9                          |    |
|                     | LT. 2+14.9   |    | LT. 2+29.9   |                             |                              |                              |                               |                                | 26.5                          |    |
|                     |              |    |              | SUBTOTAL                    |                              |                              |                               |                                | 55.4                          |    |
| ALLERTON ROAD       | LT.3+70.0    |    | LT. 4+05.0   |                             |                              | 11.7                         |                               |                                |                               |    |
|                     | RT. 15+41.6  |    | RT. 15+61.6  |                             |                              |                              | 23.1                          |                                |                               |    |
|                     |              |    |              | SUBTOTALS                   |                              | 11.7                         | 23.1                          |                                |                               |    |
|                     |              |    |              | TOTALS                      | 5                            | 39                           | 32                            | 12                             | 23                            | 55 |

| STORM SEWER                            |                        |                                    |                                    |                |                 |                 |                 |
|--|------------------------|------------------------------------|------------------------------------|----------------|-----------------|-----------------|-----------------|
| LOCATION                               | STRUCTURE TO STRUCTURE | 550B0040                           | 550B0050                           | 60100905       | 54248510        | 20800150        | 60500060        |
|  |                        | STORM SEWERS, CLASS B, TYPE 1, 10" | STORM SEWERS, CLASS B, TYPE 1, 12" | PIPE DRAINS 4" | CONCRETE COLLAR | TRENCH BACKFILL | REMOVING INLETS |
|  |                        | FOOT                               | FOOT                               | FOOT           | CU YD           | CU YD           | EACH            |
| SW QUAD. WASHINGTON ST. & HAMILTON ST. | EX. - 1                |                                    |                                    | 2              |                 |                 |                 |
| SW QUAD. WASHINGTON ST. & HAMILTON ST. | 1 - 2                  |                                    | 2                                  |                |                 | 0.2             |                 |
| SE QUAD. WASHINGTON ST. & HAMILTON ST. | 3 - CONC. COLLAR       | 10                                 |                                    |                | 0.3             | 1.7             |                 |
| SE QUAD. WASHINGTON ST. & HAMILTON ST. | RT. STA. 297+62.03     |                                    |                                    |                |                 |                 | 1               |
| NW QUAD. INDEPENDENCE ST. & MARION ST. | EX. - 8                | 5                                  |                                    |                |                 | 0.6             |                 |
| TOTALS                                 |                        | 15                                 | 2                                  | 2              | 0.3             | 3               | 1               |

| SEEDING                                   |                             |                  |                |                              |                                |                               |
|---|-----------------------------|------------------|----------------|------------------------------|--------------------------------|-------------------------------|
| LOCATION                                  | DESCRIPTION                 | 25000110         | 25100125       | 25000400                     | 25000500                       | 25000600                      |
|   |                             | SEEDING CLASS 1A | MULCH METHOD 3 | NITROGEN FERTILIZER NUTRIENT | PHOSPHORUS FERTILIZER NUTRIENT | POTASSIUM FERTILIZER NUTRIENT |
|   |                             | ACRE             | ACRE           | POUND                        | POUND                          | POUND                         |
| PIATT STREET, WASHINGTON ST. & MARION ST. | PLAN SHEET NOS. 45,46,48-49 | 0.10             | 0.10           | 9                            | 9                              | 9                             |
| INDEPENDENCE STREET                       | PLAN SHEET NO. 47           | 0.15             | 0.15           | 14                           | 14                             | 14                            |
| TOTALS                                    |                             | 0.25             | 0.25           | 23                           | 23                             | 23                            |

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 PLOT: 14-00023-00-RS  
 SCALE: 1"=40'



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 ESCA PROJECT NO. 719.63  
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DESIGNED - JMS  
 DRAWN - JLF  
 CHECKED - JMS  
 DATE - 02/2016

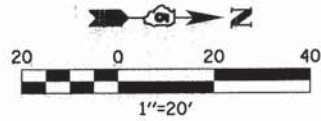
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES  
 SCALE SHEET NO. 9 OF 9 SHEETS STA. TO STA.

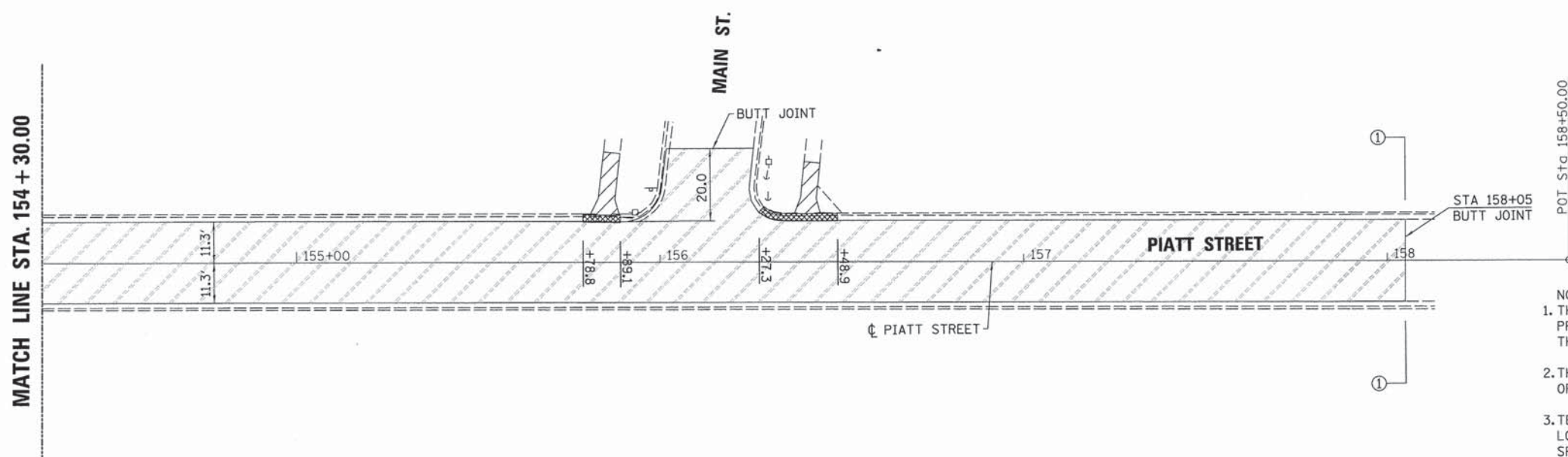
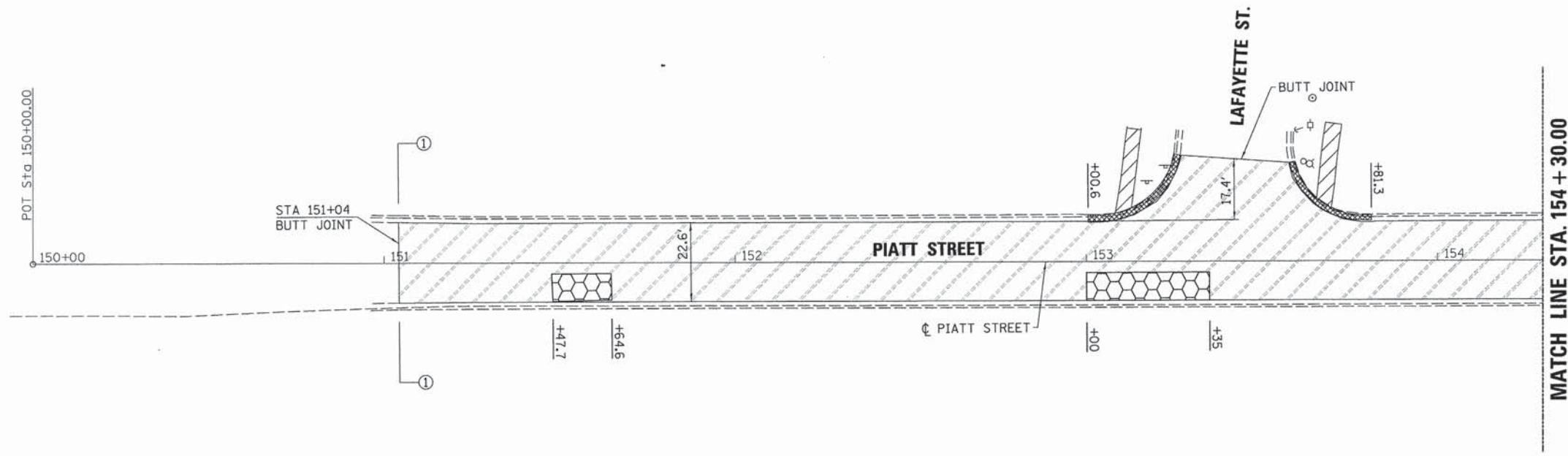
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| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 36        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |





**LEGEND**

- SIDEWALK REMOVAL
- CCC&G REMOVAL
- HMA SURFACE REMOVAL 2 3/4"
- CLASS D PATCHES
- DRAINAGE STRUCTURE TO BE ADJUSTED
- WATER VALVE BOX TO BE ADJUSTED



- NOTES:
1. THE LOCATIONS OF THE CLASS D PATCHES ARE APPROXIMATE. PROPOSED PAVEMENT WILL BE MARKED FOR REMOVAL AFTER THE COMPLETION OF THE HMA SURFACE REMOVAL.
  2. THE HMA SURFACE REMOVAL SHALL BE TO A MAXIMUM DEPTH OF 2 3/4" OR TO THE TOP OF THE EXISTING BRICK PAVEMENT.
  3. TEMPORARY RAMPS SHALL BE CONSTRUCTED AT ALL BUTT JOINT LOCATIONS ACCORDING TO ARTICLE 406.08 OF THE STANDARD SPECIFICATIONS AND PAID FOR ACCORDING TO ARTICLE 406.14.
  4. TEMPORARY RAMPS SHALL BE CONSTRUCTED TO MAINTAIN ACCESS TO ENTRANCES AND FOR THE ADA RAMPS. COLD-MILL HMA TAILINGS SUITABLE TO THE ENGINEER AND COLD MIX WILL BE ACCEPTABLE AT THESE LOCATIONS TO PROVIDE A MAXIMUM LIP HEIGHT OF 1 1/2". THE TEMPORARY RAMPS SHALL BE REMOVED JUST PRIOR TO PLACING THE PROPOSED HMA LEVEL BINDER AND SURFACE. THIS WORK WILL NOT BE MEASURED AND PAID FOR SEPARATELY AND SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

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 PLOT SIZE: 11x17  
 PLOT ORIENT: Landscape  
 PLOT RANGE: All in sheet



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 28,0000 ' / 1"     | CHECKED - JMS  | REVISED - |
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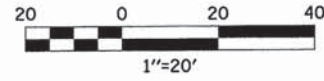
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN  
PIATT STREET**

SCALE: 1"=20'    SHEET NO. 1 OF 1 SHEETS    STA.    TO STA.

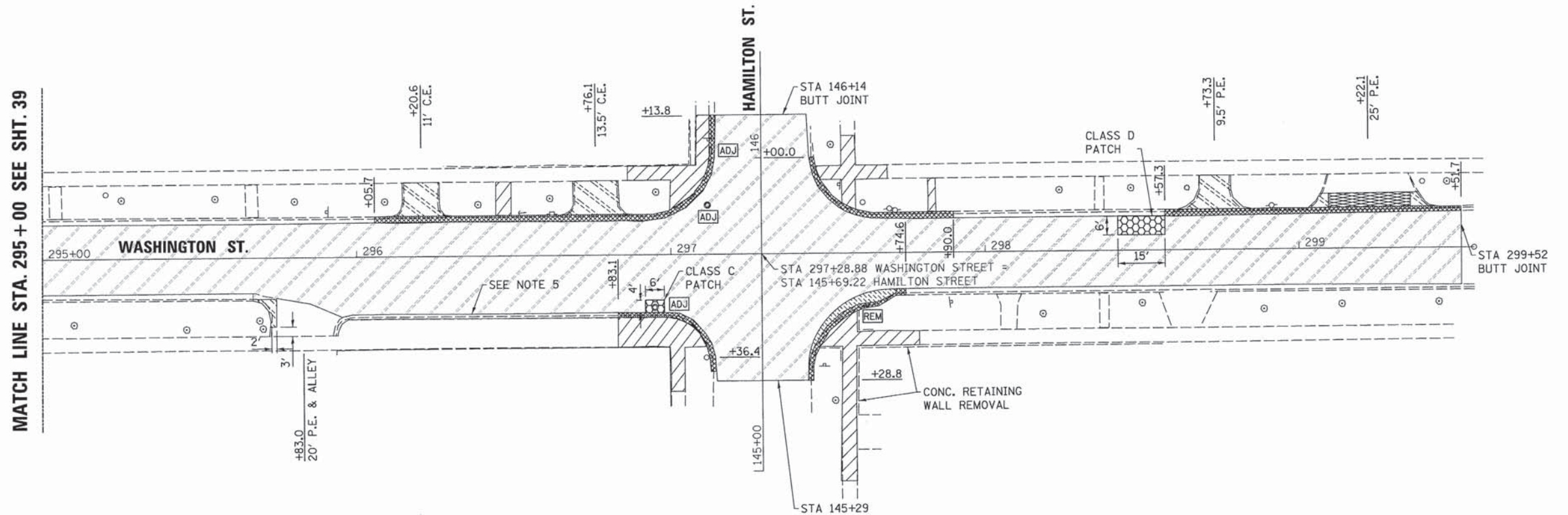
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| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 37        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |





**LEGEND**

- SIDEWALK REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- PAVEMENT REMOVAL
- CCC&G REMOVAL
- HMA SURFACE REMOVAL 2"
- HMA SURFACE REMOVAL (SPECIAL)
- CLASS C & D PATCHES
- DRAINAGE STRUCTURE TO BE ADJUSTED
- DRAINAGE STRUCTURE TO BE REMOVED
- WATER VALVE BOX TO BE ADJUSTED



**NOTES:**

1. THE LOCATIONS OF THE CLASS D PATCHES ARE APPROXIMATE. PAVEMENT PATCHES WILL BE MARKED FOR REMOVAL AND PATCHING SHALL BE PERFORMED AFTER THE COMPLETION OF THE HMA SURFACE REMOVAL.
2. THE HMA SURFACE REMOVAL SHALL BE TO A MAXIMUM DEPTH OF 2". THE EXISTING HMA THICKNESS ON THE PARKING LANE RT. STA. 295+94 TO RT. STA. 296+83 DECREASES TO 1" AND HMA SURFACE REMOVAL SHALL STOP AT THE TOP OF CONCRETE.
3. TEMPORARY RAMPS SHALL BE CONSTRUCTED AT ALL BUTT JOINT LOCATIONS ACCORDING TO ARTICLE 406.08 OF THE STANDARD SPECIFICATIONS AND PAID FOR ACCORDING TO ARTICLE 406.14.
4. TEMPORARY RAMPS SHALL BE CONSTRUCTED TO MAINTAIN ACCESS TO ENTRANCES AND FOR THE ADA RAMPS. COLD-MILL HMA TAILINGS SUITABLE TO THE ENGINEER AND COLD MIX WILL BE ACCEPTABLE AT THESE LOCATIONS TO PROVIDE A MAXIMUM LIP HEIGHT OF 1 1/2". THE TEMPORARY RAMPS SHALL BE REMOVED JUST PRIOR TO PLACING THE PROPOSED HMA LEVEL BINDER AND SURFACE. THIS WORK WILL NOT BE MEASURED AND PAID FOR SEPARATELY AND SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
5. THE EXISTING HMA RESURFACING PLACED ON THE COMBINATION CURB AND GUTTER FLAG FROM RT. STA. 295+94 TO RT. STA. 296+83 SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COLD MILLING (SPECIAL).

ESCA CONSULTANTS, INC.  
 CONSULTANTS, INC.  
 1100 N. WASHINGTON ST., SUITE 200  
 CHICAGO, IL 60610  
 TEL: (773) 399-1000  
 FAX: (773) 399-1001  
 WWW.ESCA-CONSULTANTS.COM



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 20.0000' / in.     | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:30:51 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN  
WASHINGTON STREET**

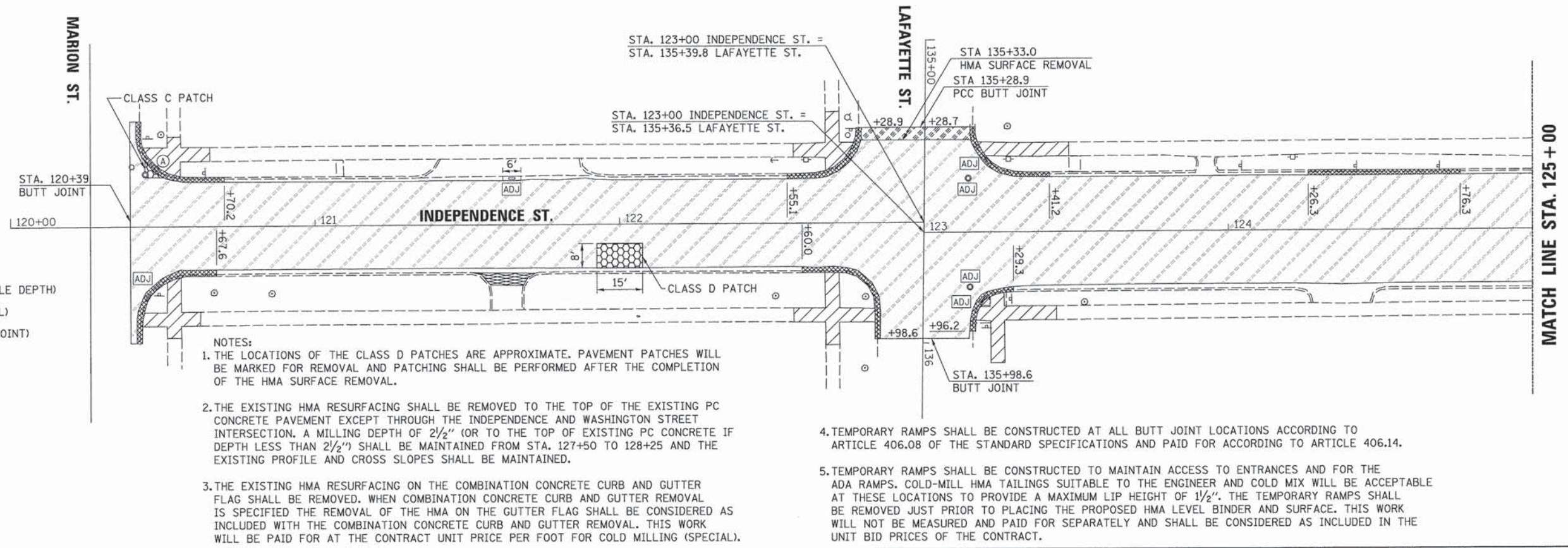
SCALE: 1"=20' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|                     |                           |        |              |           |
|---------------------|---------------------------|--------|--------------|-----------|
| F.A. RTE.           | SECTION                   | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS            | PIATT  | 76           | 38        |
| CONTRACT NO. 91516  |                           |        |              |           |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT |        |              |           |

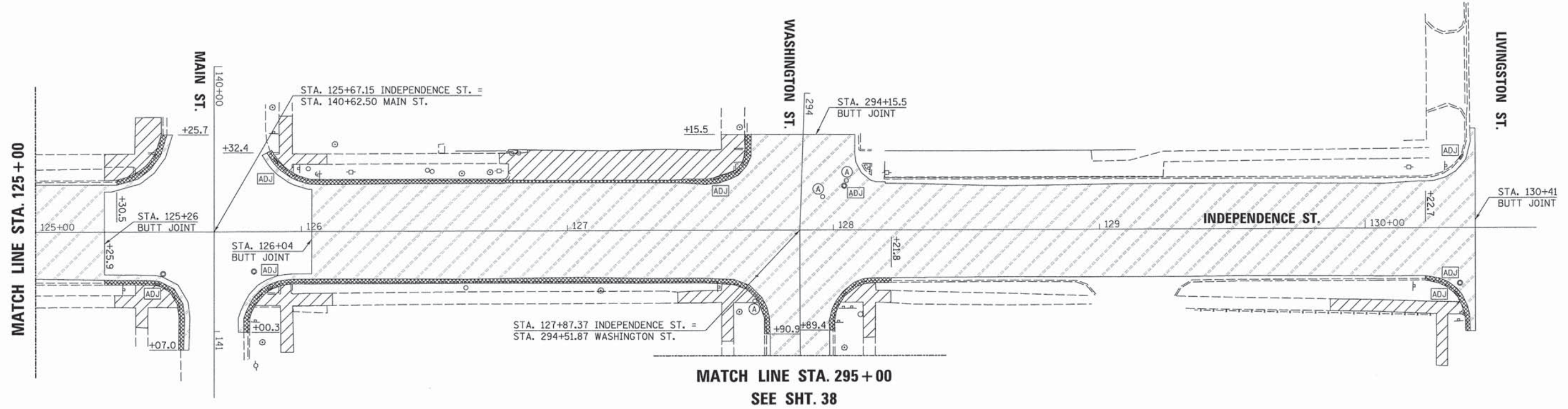




- LEGEND**
- SIDEWALK REMOVAL
  - DRIVEWAY PAVEMENT REMOVAL
  - PAVEMENT REMOVAL
  - CCC&G REMOVAL
  - HMA SURFACE REMOVAL (VARIABLE DEPTH)
  - HMA SURFACE REMOVAL (SPECIAL)
  - PCC SURFACE REMOVAL (BUTT JOINT)
  - CLASS C & D PATCHES
  - DRAINAGE STRUCTURE TO BE ADJUSTED
  - WATER VALVE BOX TO BE ADJUSTED



- NOTES:**
1. THE LOCATIONS OF THE CLASS D PATCHES ARE APPROXIMATE. PAVEMENT PATCHES WILL BE MARKED FOR REMOVAL AND PATCHING SHALL BE PERFORMED AFTER THE COMPLETION OF THE HMA SURFACE REMOVAL.
  2. THE EXISTING HMA RESURFACING SHALL BE REMOVED TO THE TOP OF THE EXISTING PC CONCRETE PAVEMENT EXCEPT THROUGH THE INDEPENDENCE AND WASHINGTON STREET INTERSECTION. A MILLING DEPTH OF 2 1/2" (OR TO THE TOP OF EXISTING PC CONCRETE IF DEPTH LESS THAN 2 1/2") SHALL BE MAINTAINED FROM STA. 127+50 TO 128+25 AND THE EXISTING PROFILE AND CROSS SLOPES SHALL BE MAINTAINED.
  3. THE EXISTING HMA RESURFACING ON THE COMBINATION CONCRETE CURB AND GUTTER FLAG SHALL BE REMOVED. WHEN COMBINATION CONCRETE CURB AND GUTTER REMOVAL IS SPECIFIED THE REMOVAL OF THE HMA ON THE GUTTER FLAG SHALL BE CONSIDERED AS INCLUDED WITH THE COMBINATION CONCRETE CURB AND GUTTER REMOVAL. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COLD MILLING (SPECIAL).
  4. TEMPORARY RAMPS SHALL BE CONSTRUCTED AT ALL BUTT JOINT LOCATIONS ACCORDING TO ARTICLE 406.08 OF THE STANDARD SPECIFICATIONS AND PAID FOR ACCORDING TO ARTICLE 406.14.
  5. TEMPORARY RAMPS SHALL BE CONSTRUCTED TO MAINTAIN ACCESS TO ENTRANCES AND FOR THE ADA RAMPS. COLD-MILL HMA TAILINGS SUITABLE TO THE ENGINEER AND COLD MIX WILL BE ACCEPTABLE AT THESE LOCATIONS TO PROVIDE A MAXIMUM LIP HEIGHT OF 1/2". THE TEMPORARY RAMPS SHALL BE REMOVED JUST PRIOR TO PLACING THE PROPOSED HMA LEVEL BINDER AND SURFACE. THIS WORK WILL NOT BE MEASURED AND PAID FOR SEPARATELY AND SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.



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|                         |                |           |
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| ESCA PROJECT NO. 719.61 | DRAWN - JLF    | REVISED - |
| PLOT DATE = 3/1/2016    | CHECKED - JMS  | REVISED - |
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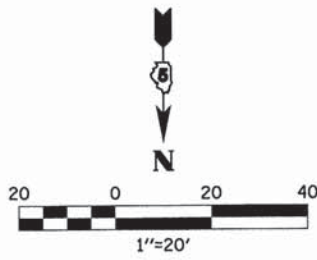
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN  
INDEPENDENCE STREET**

SCALE: 1"=20'    SHEET NO. 1 OF 1 SHEETS    STA.    TO STA.

|                     |                |                           |              |           |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76           | 39        |
| CONTRACT NO. 91516  |                |                           |              |           |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |              |           |





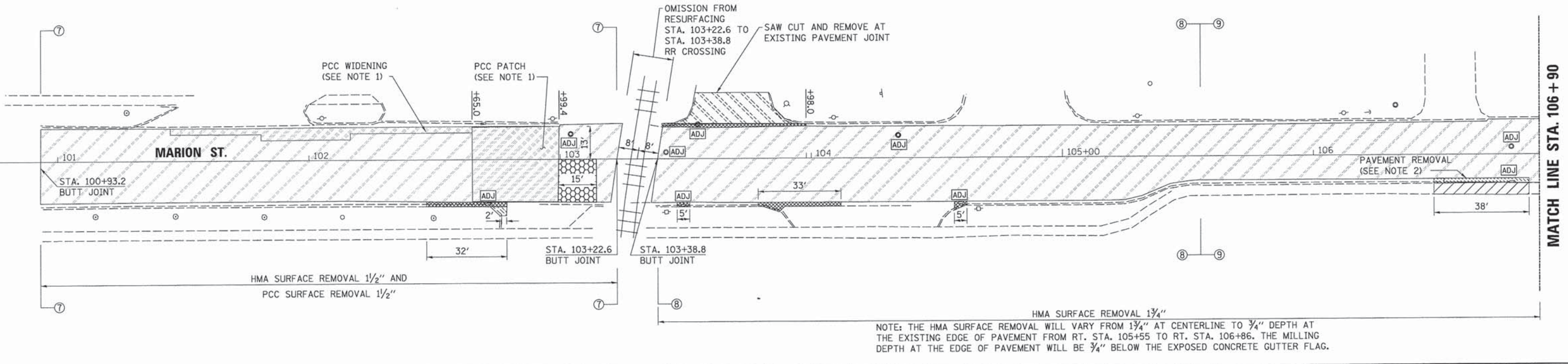
**LEGEND**

|  |                           |  |                                   |
|--|---------------------------|--|-----------------------------------|
|  | SIDEWALK REMOVAL          |  | CLASS D PATCHING                  |
|  | DRIVEWAY PAVEMENT REMOVAL |  | HMA SURFACE REMOVAL 1/2" & 1 3/4" |
|  | PAVEMENT REMOVAL          |  | HMA SURFACE REMOVAL (SPECIAL)     |
|  | CCC&G REMOVAL             |  | DRAINAGE STRUCTURE TO BE ADJUSTED |
|  | PCC SURFACE REMOVAL 1/2"  |  | WATER VALVE BOX TO BE ADJUSTED    |

**NOTES:**

1. COLD MILL THE EXISTING PCC SURFACE. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR PCC SURFACE REMOVAL 1/2".
2. SAW CUT AND REMOVE THE EXISTING PAVEMENT TO MATCH THE WIDTH OF THE ADJACENT COMBINATION CONCRETE CURB AND GUTTER. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR PAVEMENT REMOVAL.

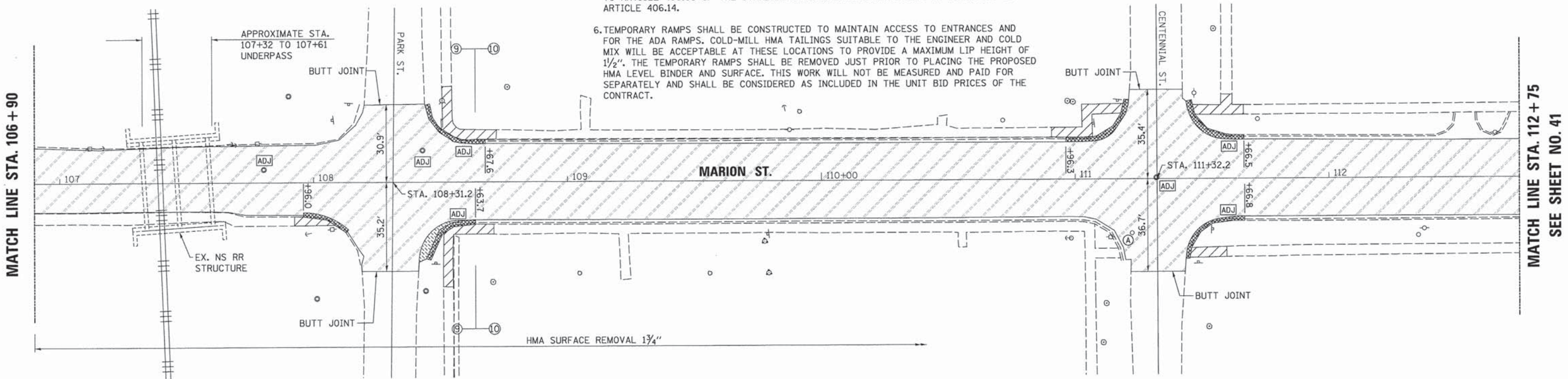
3. THE EXISTING HMA RESURFACING PLACED ON THE COMBINATION CONCRETE CURB AND GUTTER FLAG SHALL BE REMOVED AS SHOWN ON THE SCHEDULE (PLAN SHEET NO. 31) AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COLD MILLING (SPECIAL).
4. THE LOCATIONS OF THE CLASS D PATCHES ARE APPROXIMATE. PAVEMENT PATCHES WILL BE MARKED FOR REMOVAL AND PATCHING PERFORMED AFTER THE COMPLETION OF THE HMA SURFACE REMOVAL.



NOTE: THE HMA SURFACE REMOVAL WILL VARY FROM 1 3/4" AT CENTERLINE TO 3/4" DEPTH AT THE EXISTING EDGE OF PAVEMENT FROM RT. STA. 105+55 TO RT. STA. 106+86. THE MILLING DEPTH AT THE EDGE OF PAVEMENT WILL BE 3/4" BELOW THE EXPOSED CONCRETE GUTTER FLAG.

**NOTES (CONT.):**

5. TEMPORARY RAMPS SHALL BE CONSTRUCTED AT ALL BUTT JOINT LOCATIONS ACCORDING TO ARTICLE 406.08 OF THE STANDARD SPECIFICATIONS AND PAID FOR ACCORDING TO ARTICLE 406.14.
6. TEMPORARY RAMPS SHALL BE CONSTRUCTED TO MAINTAIN ACCESS TO ENTRANCES AND FOR THE ADA RAMPS. COLD-MILL HMA TAILINGS SUITABLE TO THE ENGINEER AND COLD MIX WILL BE ACCEPTABLE AT THESE LOCATIONS TO PROVIDE A MAXIMUM LIP HEIGHT OF 1/2". THE TEMPORARY RAMPS SHALL BE REMOVED JUST PRIOR TO PLACING THE PROPOSED HMA LEVEL BINDER AND SURFACE. THIS WORK WILL NOT BE MEASURED AND PAID FOR SEPARATELY AND SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.



PRINTED BY: ESCA CONSULTANTS, INC. 10/10/16 10:00 AM  
 PLOT SCALE: 1/4" = 10'  
 PLOT DATE: 3/1/2016 1:09:51 PM



|                                 |                |           |
|---------------------------------|----------------|-----------|
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| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
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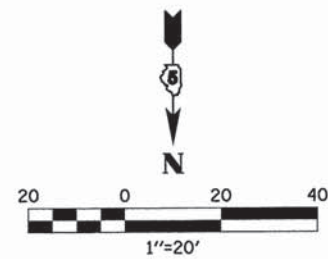
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN  
MARION STREET**

SCALE 1"=20' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

|                     |                |                           |              |           |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76           | 40        |
| CONTRACT NO. 91516  |                |                           |              |           |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |              |           |

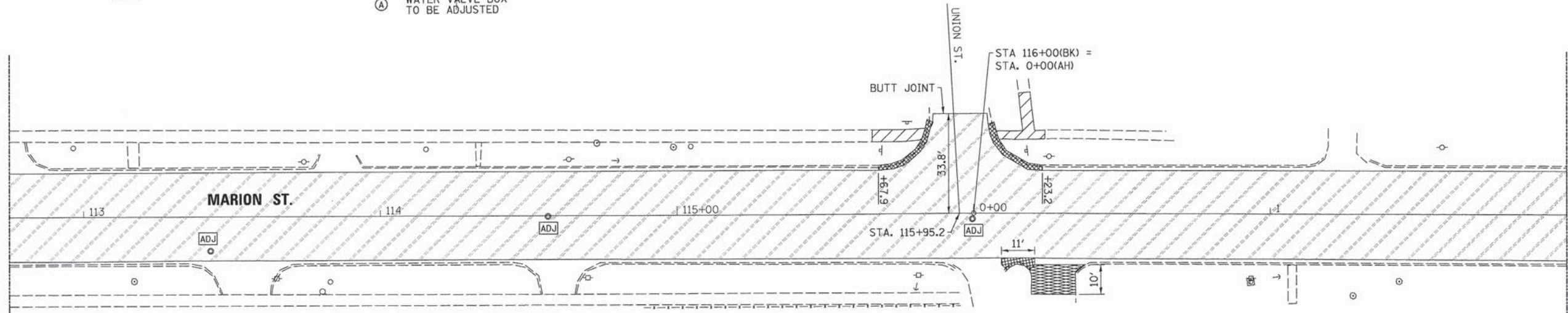




**LEGEND**

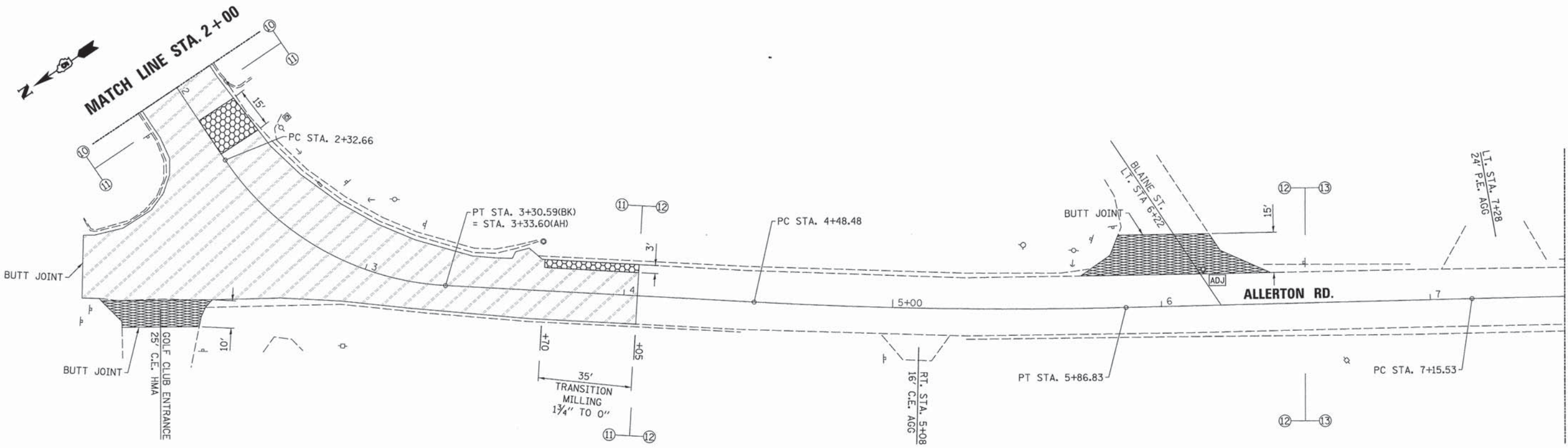
|  |                                  |  |                                   |
|--|----------------------------------|--|-----------------------------------|
|  | SIDEWALK REMOVAL                 |  | CLASS D PATCHING                  |
|  | DRIVEWAY PAVEMENT REMOVAL        |  | HMA SURFACE REMOVAL 1 3/4"        |
|  | PAVEMENT REMOVAL                 |  | HMA SURFACE REMOVAL (SPECIAL)     |
|  | CCC&G REMOVAL                    |  | DRAINAGE STRUCTURE TO BE ADJUSTED |
|  | PCC SURFACE REMOVAL (BUTT JOINT) |  | WATER VALVE BOX TO BE ADJUSTED    |

MATCH LINE STA. 112+75  
SEE SHEET NO. 40



MATCH LINE STA. 2+00

- NOTES:
1. THE EXISTING HMA RESURFACING PLACED ON THE COMBINATION CONCRETE CURB AND GUTTER FLAG SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COLD MILLING (SPECIAL). REFER TO THE SCHEDULE ON SHEET NO. 31.
  2. TRANSITION THE COLD MILLING DEPTH FROM 1 3/4" TO 0" STA. 3+70 TO STA. 4+05.
  3. THE LOCATIONS OF THE CLASS D PATCHES ARE APPROXIMATE. PAVEMENT PATCHES WILL BE MARKED FOR REMOVAL AND PATCHING PERFORMED AFTER THE COMPLETION OF THE HMA SURFACE REMOVAL.
  4. TEMPORARY RAMPS SHALL BE CONSTRUCTED AT ALL BUTT JOINT LOCATIONS ACCORDING TO ARTICLE 406.08 OF THE STANDARD SPECIFICATIONS AND PAID FOR ACCORDING TO ARTICLE 406.14.
  5. TEMPORARY RAMPS SHALL BE CONSTRUCTED TO MAINTAIN ACCESS TO ENTRANCES AND FOR THE ADA RAMPS. COLD-MILL HMA TAILINGS SUITABLE TO THE ENGINEER AND COLD MIX WILL BE ACCEPTABLE AT THESE LOCATIONS TO PROVIDE A MAXIMUM LIP HEIGHT OF 1/2". THE TEMPORARY RAMPS SHALL BE REMOVED JUST PRIOR TO PLACING THE PROPOSED HMA LEVEL BINDER AND SURFACE. THIS WORK WILL NOT BE MEASURED AND PAID FOR SEPARATELY AND SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.



MATCH LINE STA. 7+50  
SEE SHEET NO. 42

PRINT NUMBER = 1405271833-akt-rwd2.dgn  
 PLOT DATE = 3/1/2016 1:10:51 PM



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = akm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
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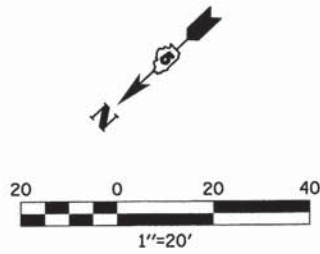
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN  
MARION STREET**

SCALE 1"=20' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

|                     |                |                           |                    |           |
|---------------------|----------------|---------------------------|--------------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS       | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76                 | 41        |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT | CONTRACT NO. 91516 |           |

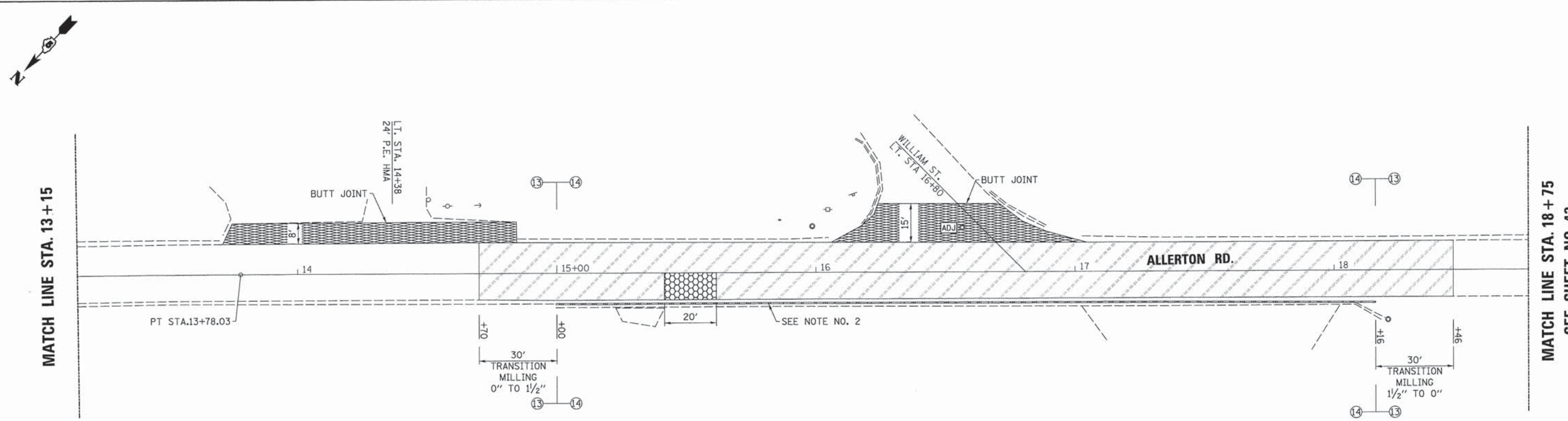
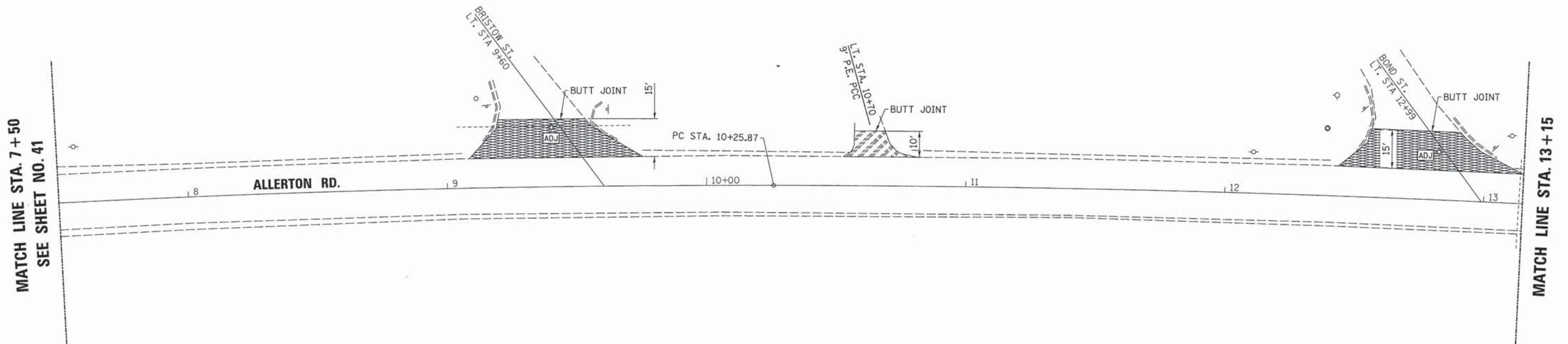




**LEGEND**

|  |                                  |  |                                   |
|--|----------------------------------|--|-----------------------------------|
|  | SIDEWALK REMOVAL                 |  | CLASS D PATCHING                  |
|  | DRIVEWAY PAVEMENT REMOVAL        |  | HMA SURFACE REMOVAL 1 1/2"        |
|  | PAVEMENT REMOVAL                 |  | HMA SURFACE REMOVAL (SPECIAL)     |
|  | CCC&G REMOVAL                    |  | DRAINAGE STRUCTURE TO BE ADJUSTED |
|  | PCC SURFACE REMOVAL (BUTT JOINT) |  | WATER VALVE BOX TO BE ADJUSTED    |

- NOTES:
1. TRANSITION COLD MILLING DEPTH FROM 0" AT STA. 14+70 TO 1 1/2" AT STA. 15+00. TRANSITION COLD MILLING DEPTH FROM 1 1/2" AT STA. 18+16 TO 0" AT STA 18+46.
  2. PROVIDE A COLD MILLING BUTT JOINT ON THE EXISTING CONCRETE GUTTER RT. STA. 15+00 TO RT. STA. 18+36. SEE DETAIL ON SHEET NO. 67 OF THE PLANS. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR PCC SURFACE REMOVAL (BUTT JOINT).
  3. THE LOCATIONS OF THE CLASS D PATCHES ARE APPROXIMATE. PAVEMENT PATCHES WILL BE MARKED FOR REMOVAL AND PATCHING PERFORMED AFTER THE COMPLETION OF THE HMA SURFACE REMOVAL.



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| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
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| PLOT DATE = 3/1/2016 1:53:13 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN  
ALLERTON ROAD**

SCALE 1"=20'    SHEET NO. 1 OF 3 SHEETS    STA.    TO STA.

|                     |                |                           |              |           |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76           | 42        |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |              |           |
| CONTRACT NO. 91516  |                |                           |              |           |

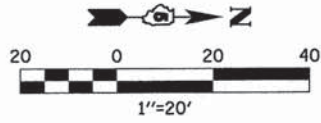






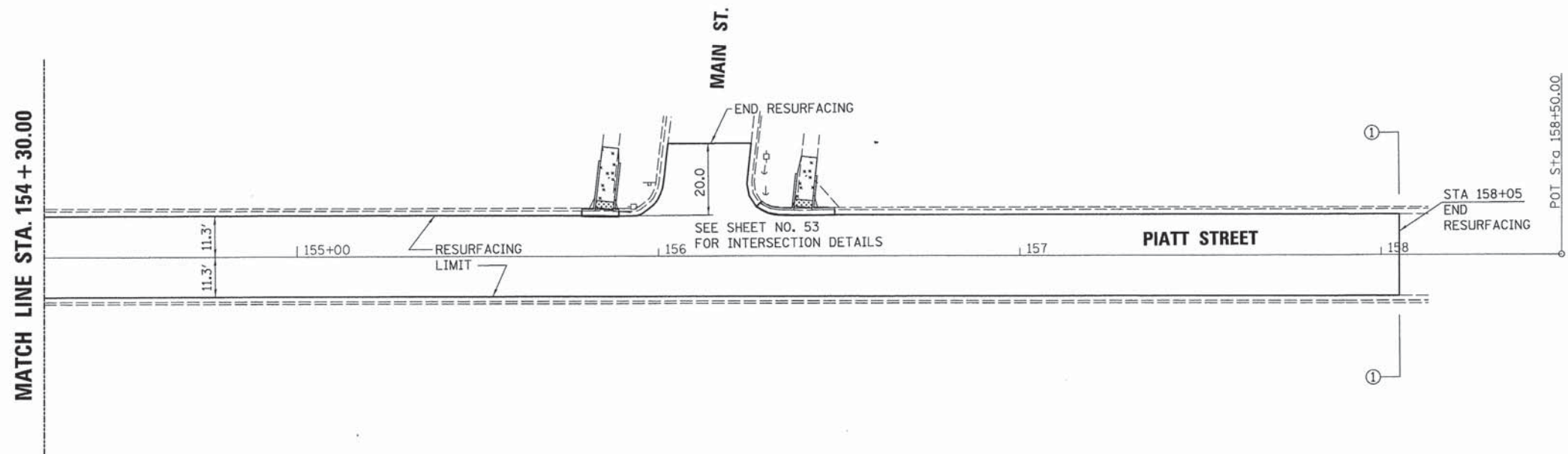
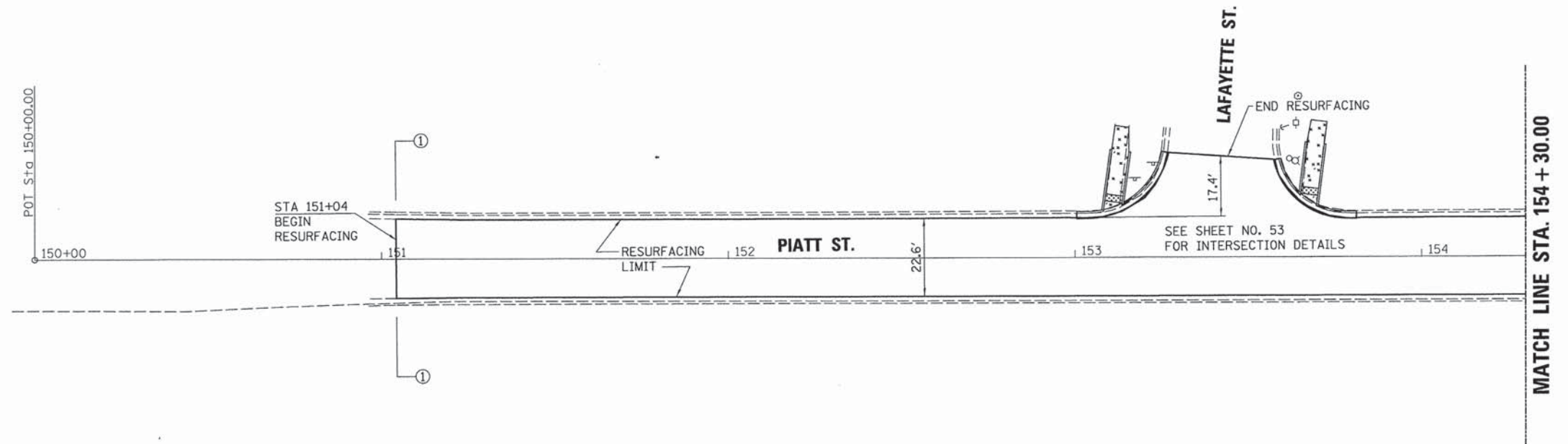






**LEGEND**

- PCC SIDEWALK 6"
- DETECTABLE WARNINGS



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| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
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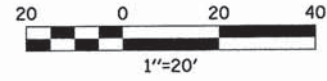
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN  
PIATT STREET**










SCALE: 1"=20'      SHEET NO. 1 OF 1 SHEETS      STA.      TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 45        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |





**LEGEND**

-  PCC SIDEWALK, 6"
-  PCC DRIVEWAY PAVEMENT, 6"
-  DETECTABLE WARNINGS
-  INCIDENTAL HMA RESURFACING
-  ADJ DRAINAGE STRUCTURE TO BE ADJUSTED
-  REM DRAINAGE STRUCTURE TO BE REMOVED
-  (A) WATER VALVE BOX TO BE ADJUSTED
-  PROPOSED INLET
-  PROPOSED STORM SEWER

**PAVEMENT MARKING LEGEND**

- ① 4" WHITE
- ② 4" YELLOW
- ③ 6" WHITE
- ④ 12" YELLOW
- ⑤ 24" WHITE
- ⑥ 4" DOUBLE YELLOW

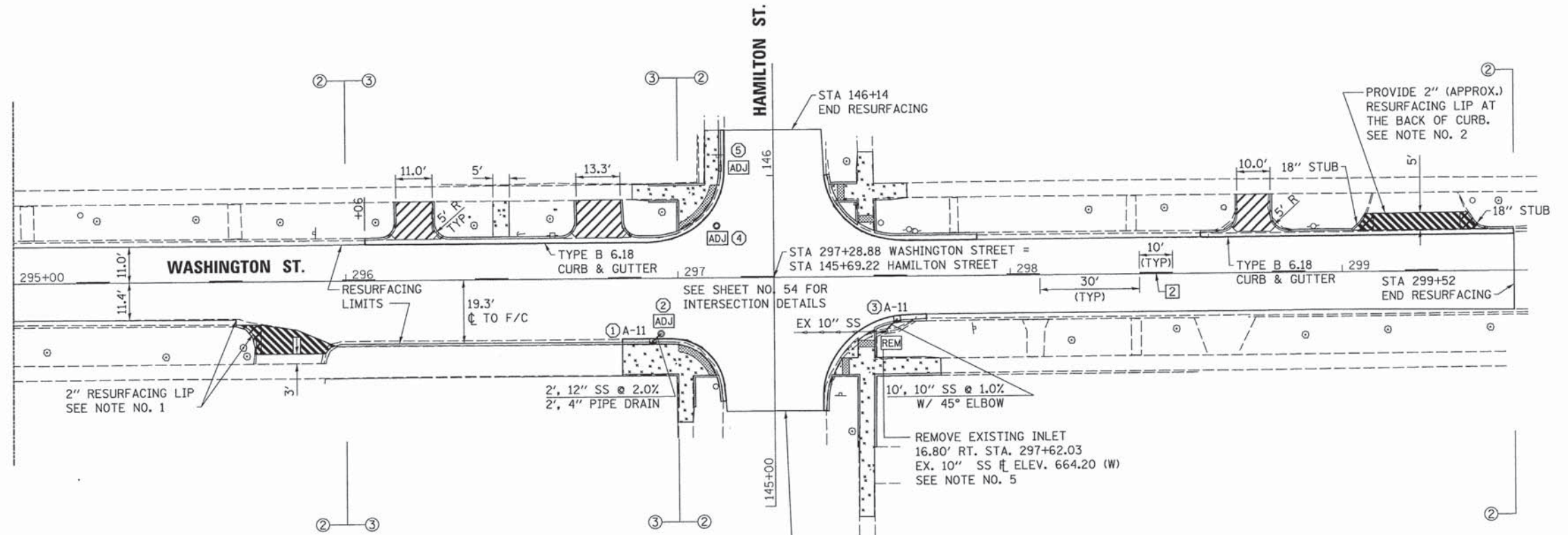
**PAVEMENT MARKING NOTES:**

1. THE PROPOSED 10 FOOT LONG SKIP-DASH YELLOW LINE SHALL BE POSITIONED TO PROVIDE A 30 FOOT SKIP FROM THE EXISTING MARKINGS EAST OF THE PROJECT LIMITS.
2. SEE SHEET NO. 33 FOR PAVEMENT MARKING QUANTITIES.

**NOTES:**

1. POUR PCC DRIVEWAY PAVEMENT 6" AND TYPE B6.18 CURB AND GUTTER MONOLITHIC. PROVIDE A 2" DEPTH RESURFACING LIP AS SHOWN. PLACE THE HMA RESURFACING ON THE PROPOSED PCC DRIVEWAY PAVEMENT 6" AND THE EXISTING DRIVEWAY PAVEMENT TO THE LIMITS SHOWN.
2. THE DEPRESSED TYPE B6.18 CURB AND GUTTER SHALL BE CONSTRUCTED ACCORDING TO STANDARD 606001 AND THE PCC DRIVEWAY PAVEMENT DETAIL ON SHEET 69. A MINIMUM 1/2" HMA RESURFACING LIP SHALL BE PROVIDED AT THE BACK OF THE DEPRESSED CURB AND GUTTER AND ALONG THE 18" STUBS FOR THE DRIVEWAY. PLACE INCIDENTAL HMA RESURFACING ON THE PROPOSED PCC DRIVEWAY PAVEMENT AND THE EXISTING DRIVEWAY PAVEMENT TO THE LIMITS SHOWN.
3. REMOVE THE EXISTING WALL BY EITHER CORE DRILLING OR SAW CUTTING WITH HAND REMOVAL TO ALLOW PLACEMENT OF THE PROPOSED 12" STORM SEWER. PLACEMENT OF THE PROPOSED STORM SEWER INTO EXISTING DRAINAGE STRUCTURES WILL NOT BE PAID SEPARATELY AND SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
4. STORM SEWER TRENCHES SHALL BE BACKFILLED WITH TRENCH BACKFILL MATERIAL IN ACCORDANCE WITH SECTION 208 OF THE STANDARD SPECIFICATIONS.
5. CONNECT THE PROPOSED 10" STORM SEWER, CLASS B, TYPE 1 TO THE EXISTING 10" CONCRETE STORM SEWER AND PROVIDE A CONCRETE COLLAR. THE PROPOSED 10" STORM SEWER, CLASS B, TYPE 1 WILL INCLUDE A 45° ELBOW TO ALLOW CONNECTION TO THE PROPOSED A-11 INLET AT 13.0' RT. STA. 297+65.88

MATCH LINE STA. 295 + 00 SEE SHT. 47



- ① A-11 19.13 RT. STA. 296+92.28  
TYPE A INLET W/ TYPE 11V F&G  
E.O.P. 667.00  
PROP. 12" SS  $\bar{H}$  ELEV. = 664.13 (NE)  
PROP. 4" PIPE DRAIN  $\bar{H}$  ELEV. = 665.65± (S)  
NOTE: CONNECT PROPOSED 4" PIPE DRAIN TO THE EXISTING 4" PVC AND OUTLET INTO THE INLET.
- ② EX. 2' DIA. INLET TO BE ADJUSTED  
W/ TYPE 1 FRAME & OPEN LID  
16.8' RT. STA. 296+94.80  
TOP OF LID ELEV. 667.04  
EX. 12" SS  $\bar{H}$  ELEV. 663.33 (NE)  
EX. 12" SS  $\bar{H}$  ELEV. 663.28 (SE)  
EX. 4" PVC  $\bar{H}$  665.6 (SW) (PLUG/REMOVE)  
PROP. 12" SS  $\bar{H}$  ELEV. 664.09 (SW)  
SEE NOTE NO. 3
- ③ A-11 13.00 RT. STA. 297+65.88  
TYPE A INLET W/ TYPE 11V F&G  
E.O.P. ELEV. 667.10  
PROP. 10" SS  $\bar{H}$  ELEV. = 664.30 (SW)

PRINTED UNDER THE SUPERVISION OF THE ENGINEER IN CHARGE OF THE PROJECT  
PROJECT NO. 14-00023-00-RS  
DATE: 01/11/2016 1:32:03 PM



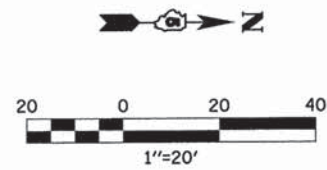
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| PLOT SCALE = 28.0000' / in.     | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:32:03 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|  |                         |
|--|-------------------------|
| <b>PROPOSED PLAN<br/>WASHINGTON STREET</b> |                         |
| SCALE: 1"=20'                              | SHEET NO. 1 OF 1 SHEETS |
| STA. TO STA.                               |                         |

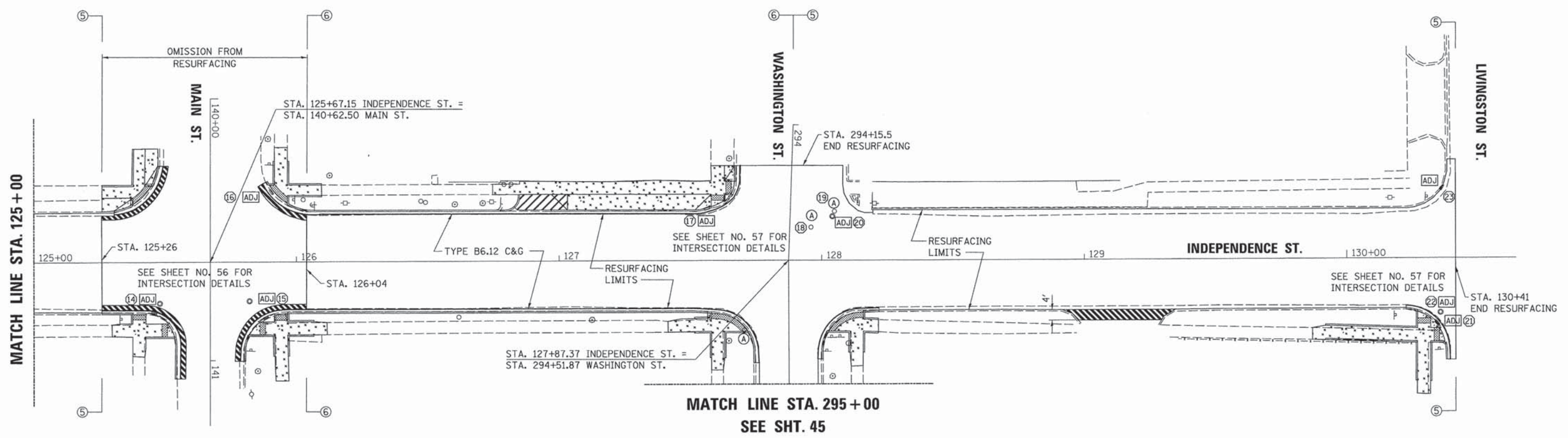
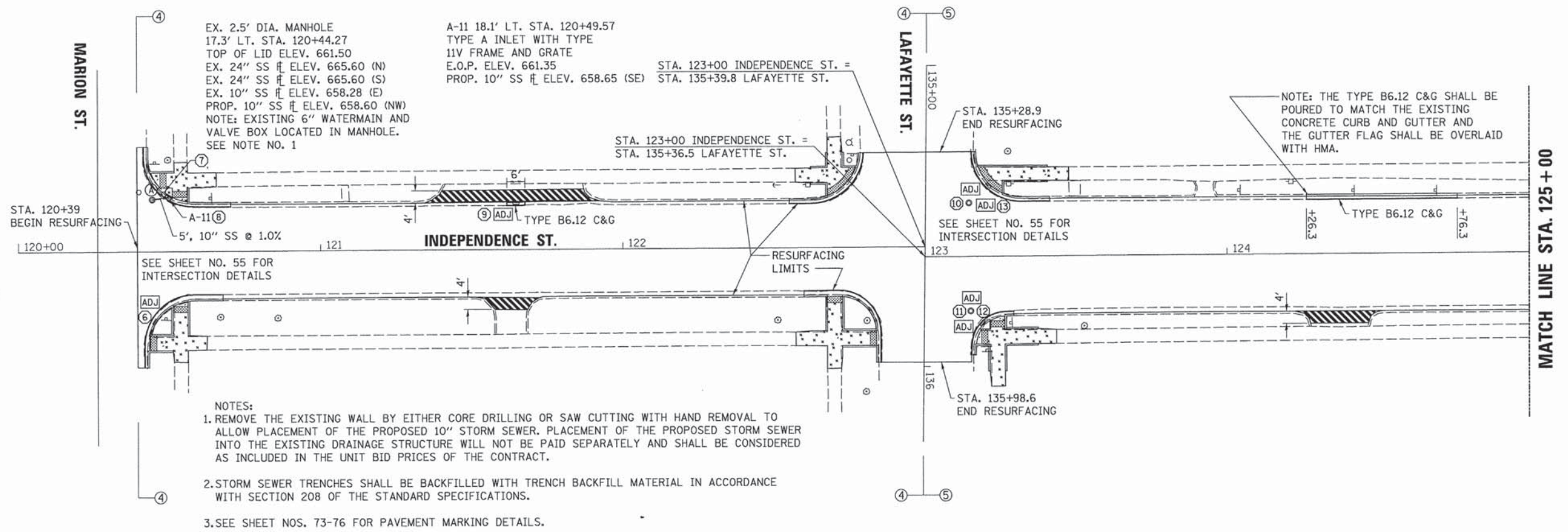
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| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|   | 14-00023-00-RS | PIATT  | 76           | 46        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |





**LEGEND**

- PCC SIDEWALK, 6"
- PCC DRIVEWAY PAVEMENT, 6"
- DETECTABLE WARNINGS
- INCIDENTAL HMA RESURFACING
- DRAINAGE STRUCTURE TO BE ADJUSTED
- WATER VALVE BOX TO BE ADJUSTED



PRINT DRIVER = JLD...  
 PLOT DATE = 3/1/2016 1:32:44 PM



|                                  |                |           |
|----------------------------------|----------------|-----------|
| USER NAME = skm                  | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61          | DRAWN - JLF    | REVISED - |
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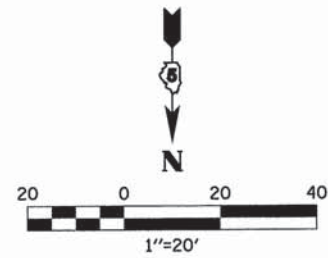
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN  
INDEPENDENCE STREET**

SCALE: 1"=20' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 47        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |





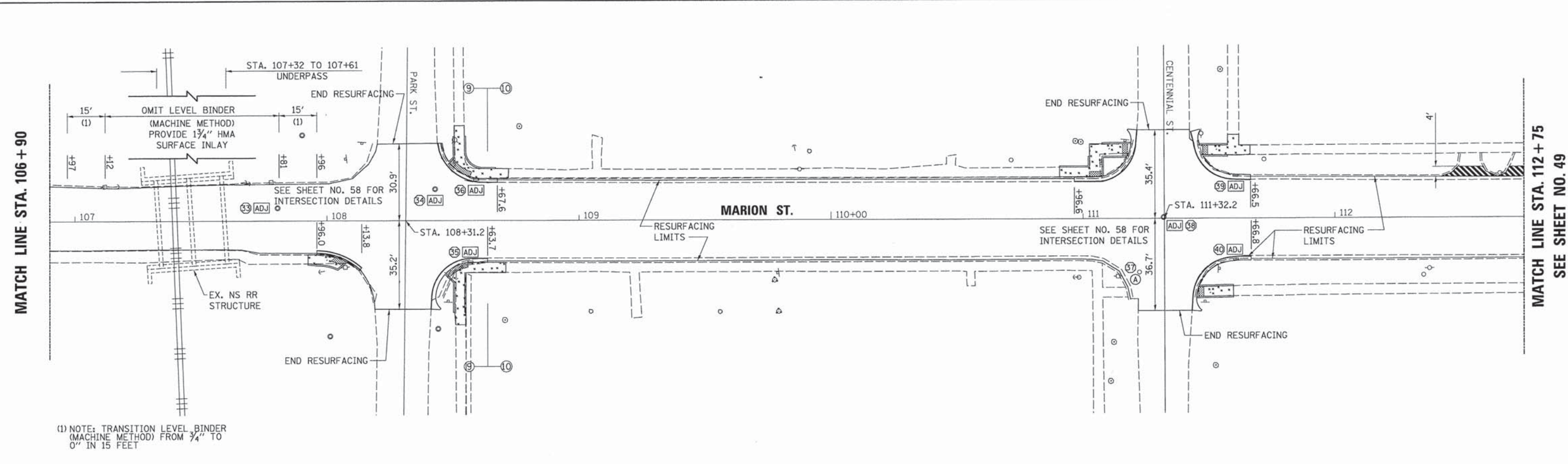
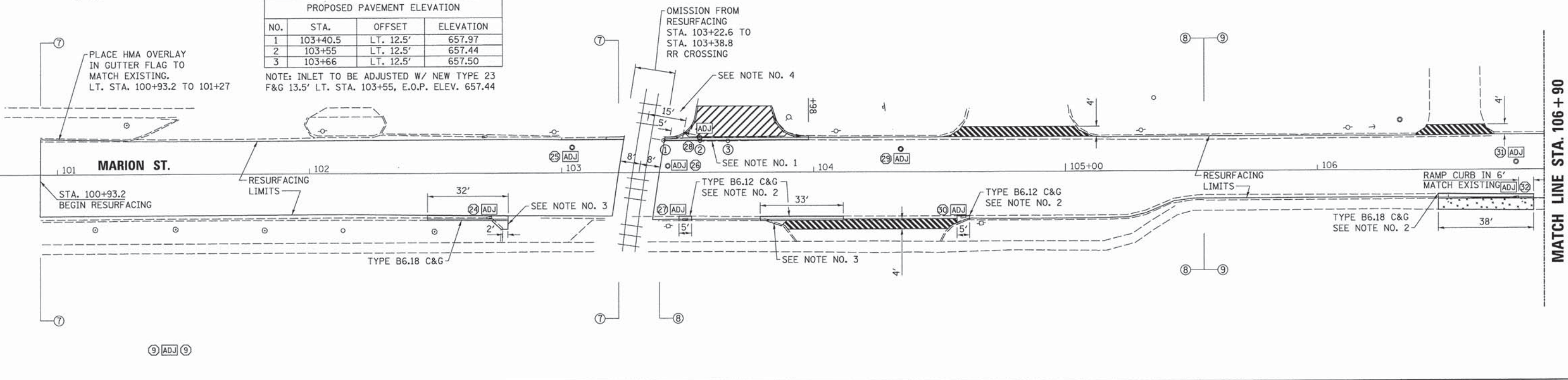
**LEGEND**

|  |                            |  |                                       |
|--|----------------------------|--|---------------------------------------|
|  | PCC SIDEWALK, 6"           |  | ADJ DRAINAGE STRUCTURE TO BE ADJUSTED |
|  | PCC DRIVEWAY PAVEMENT, 8"  |  | (A) WATER VALVE BOX TO BE ADJUSTED    |
|  | DETECTABLE WARNINGS        |  |                                       |
|  | INCIDENTAL HMA RESURFACING |  |                                       |

| PROPOSED PAVEMENT ELEVATION |          |           |           |
|-----------------------------|----------|-----------|-----------|
| NO.                         | STA.     | OFFSET    | ELEVATION |
| 1                           | 103+40.5 | LT. 12.5' | 657.97    |
| 2                           | 103+55   | LT. 12.5' | 657.44    |
| 3                           | 103+66   | LT. 12.5' | 657.50    |

NOTE: INLET TO BE ADJUSTED W/ NEW TYPE 23 F&G 13.5' LT. STA. 103+55, E.O.P. ELEV. 657.44

- NOTES:
1. THE PROPOSED COMBINATION CONCRETE CURB AND GUTTER TYPE B6.18 SHALL BE CONSTRUCTED TO THE ELEVATIONS SHOWN AND THE GUTTER FLAG SHALL NOT BE OVERLAID. THE RESURFACING LIP SHALL VARY TO  $\pm 6"$  TO PROVIDE THE PROPOSED ELEVATION AT THE INLET.
  2. THE PROPOSED COMBINATION CONCRETE CURB AND GUTTER TYPE SHALL BE POURED TO MATCH THE EXISTING COMBINATION CONCRETE CURB AND GUTTER. THE PROPOSED GUTTER FLAG SHALL RECEIVE A  $1\frac{1}{2}"$  HMA OVERLAY AND THE FRAME AND GRATES SHALL BE ADJUSTED TO PROVIDE A  $1\frac{1}{4}"$  LIP FOR THE PROPOSED HMA RESURFACING.
  3. THE DRIVEWAY PAVEMENT AND STUB SHALL BE POURED MONOLITHIC WITH THE PROPOSED COMBINATION CONCRETE CURB AND GUTTER AND THE COST TO PERFORM THE WORK WILL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR THE COMBINATION CONCRETE CURB AND GUTTER SPECIFIED.
  4. THE FULL HEIGHT CURB SHALL END 15' FROM THE CENTERLINE OF TRACK. THE TRANSITION FROM FULL HEIGHT CURB TO DEPRESSED CURB SHALL BE MADE IN 5 FEET.



(1) NOTE: TRANSITION LEVEL BINDER (MACHINE METHOD) FROM  $\frac{3}{4}"$  TO 0" IN 15 FEET

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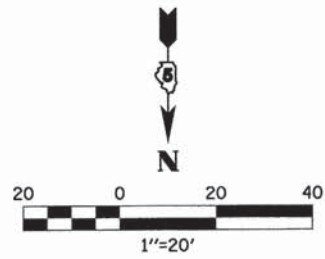
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| PLOT DATE = 3/1/2016 1:12:38 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|  |                         |
|--|-------------------------|
| <b>PROPOSED PLAN<br/>MARION STREET</b> |                         |
| SCALE 1"=20'                           | SHEET NO. 1 OF 2 SHEETS |
| STA.                                   | TO STA.                 |

|                     |                |                           |                    |           |
|---------------------|----------------|---------------------------|--------------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS       | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76                 | 48        |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT | CONTRACT NO. 91516 |           |

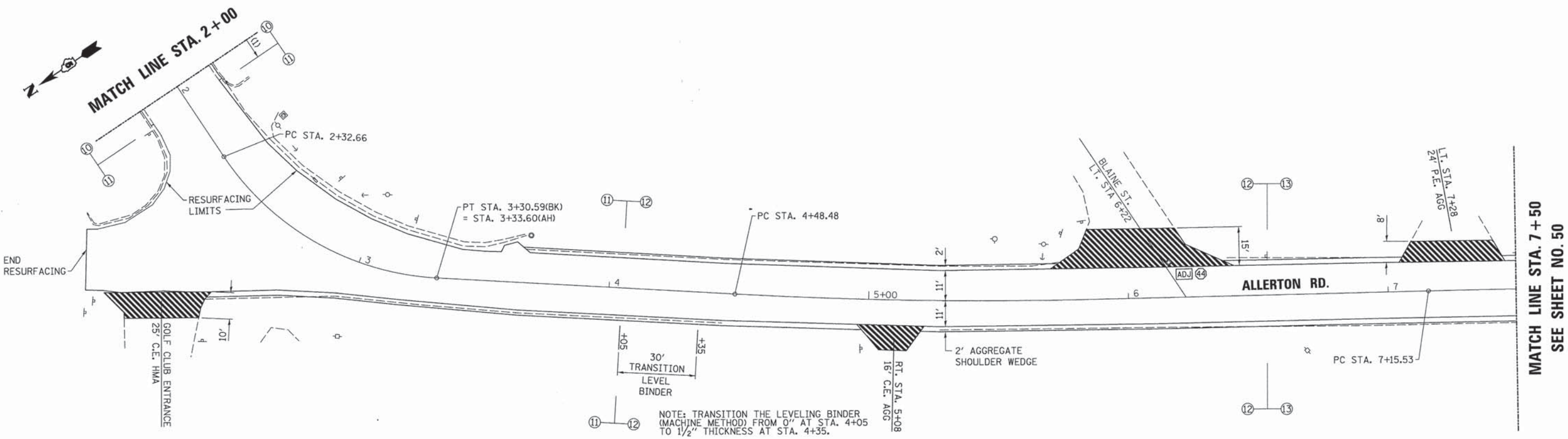
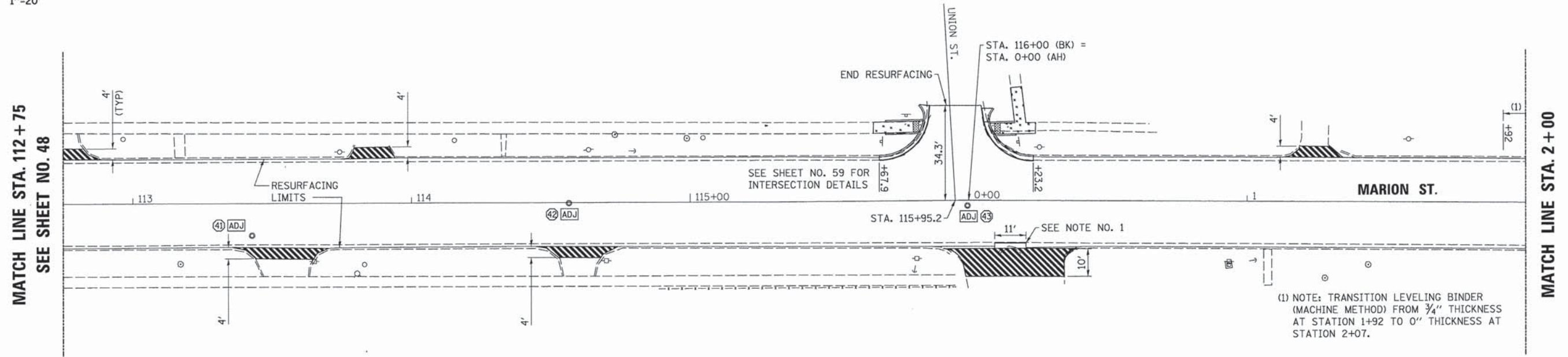




**LEGEND**

- PCC SIDEWALK, 6"
- PCC DRIVEWAY PAVEMENT, 8"
- DETECTABLE WARNINGS
- INCIDENTAL HMA RESURFACING
- DRAINAGE STRUCTURE TO BE ADJUSTED
- WATER VALVE BOX TO BE ADJUSTED

NOTES:  
 1. THE PROPOSED COMBINATION CONCRETE CURB AND GUTTER TYPE B6.12 (DEPRESSED) SHALL BE POURED TO MATCH THE EXISTING COMBINATION CONCRETE CURB AND GUTTER. THE PROPOSED CONCRETE CURB AND GUTTER SHALL RECEIVE A 1/2" HMA SURFACE OVERLAY.



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 PEN TABLE = D:\PEN\PRN200  
 FILE NAME = I:\PLOT\B\1414\9



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 ESCA PROJECT NO. 719.63  
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DESIGNED - JMS  
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 CHECKED - JMS  
 DATE - 01/2016

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

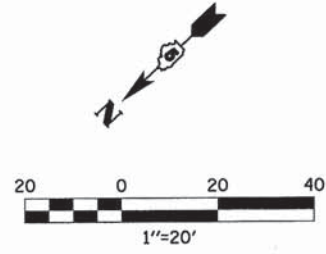
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN  
 MARION STREET**

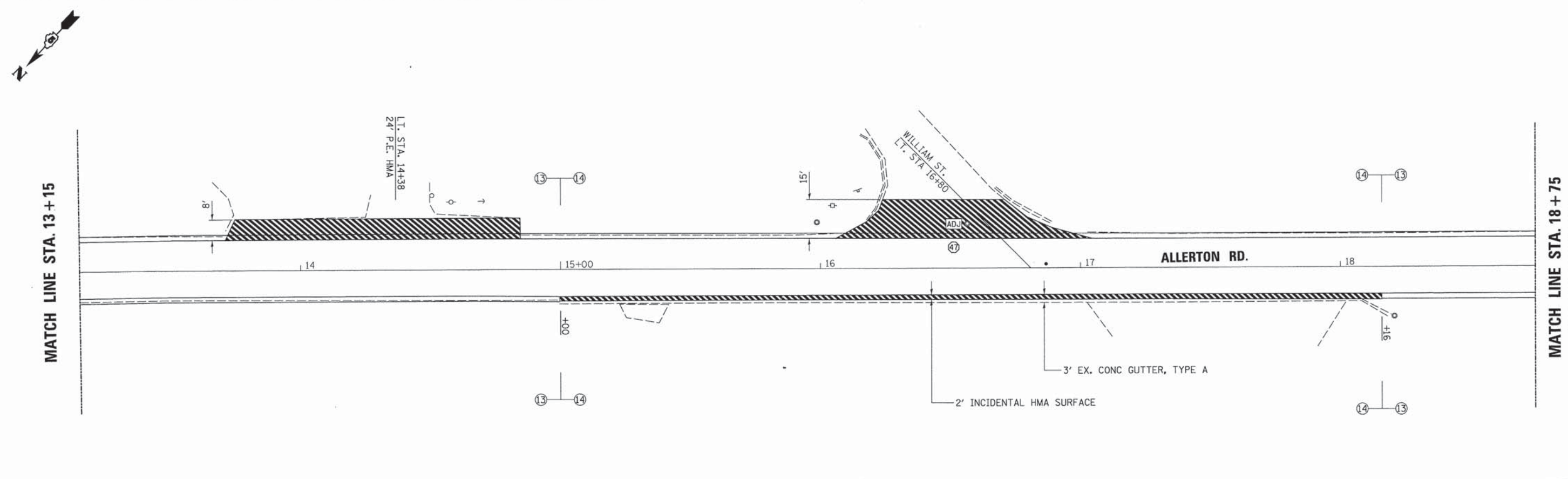
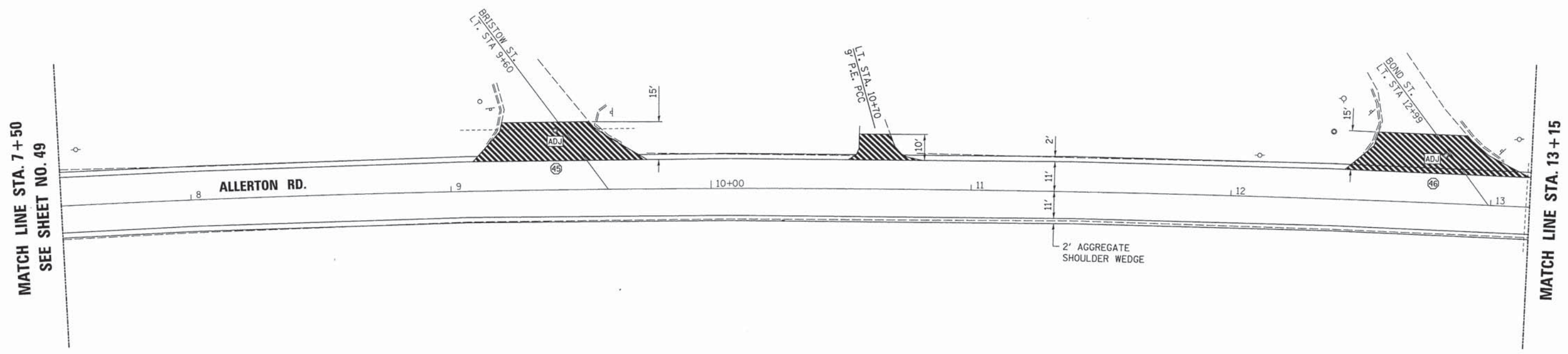
SCALE 1"=20' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 49        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |





- LEGEND**
- INCIDENTAL HMA RESURFACING
  - DRAINAGE STRUCTURE TO BE ADJUSTED
  - WATER VALVE BOX TO BE ADJUSTED



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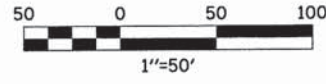
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN  
ALLERTON ROAD**

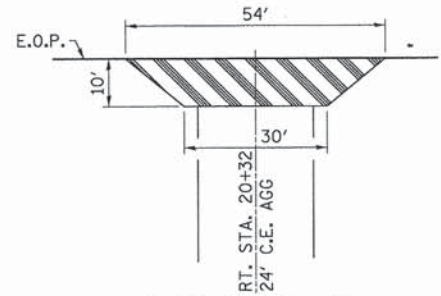
SCALE 1"=20'    SHEET NO. 1 OF 3 SHEETS    STA.    TO STA.

|                     |                |                           |              |           |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76           | 50        |
| CONTRACT NO. 91516  |                |                           |              |           |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |              |           |



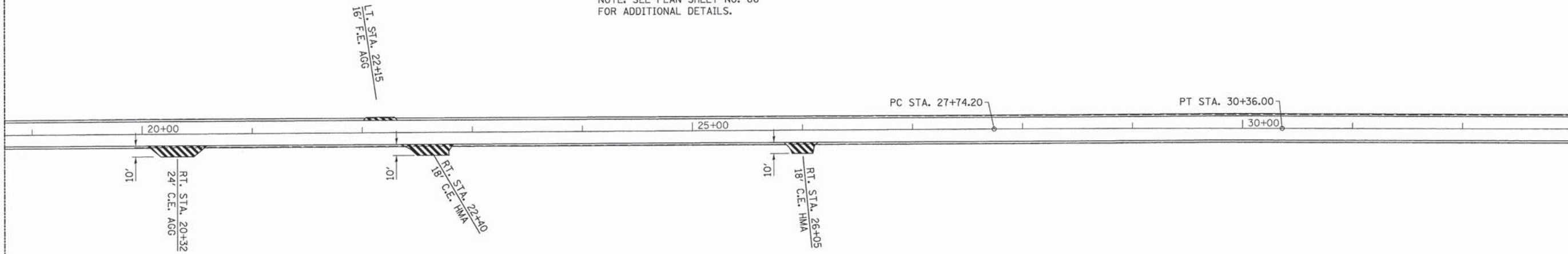


**LEGEND**  
 INCIDENTAL HMA RESURFACING



**C.E. RT. STA. 20+32**  
**INCIDENTAL HMA SURFACING DETAIL**  
 NOTE: SEE PLAN SHEET NO. 66 FOR ADDITIONAL DETAILS.

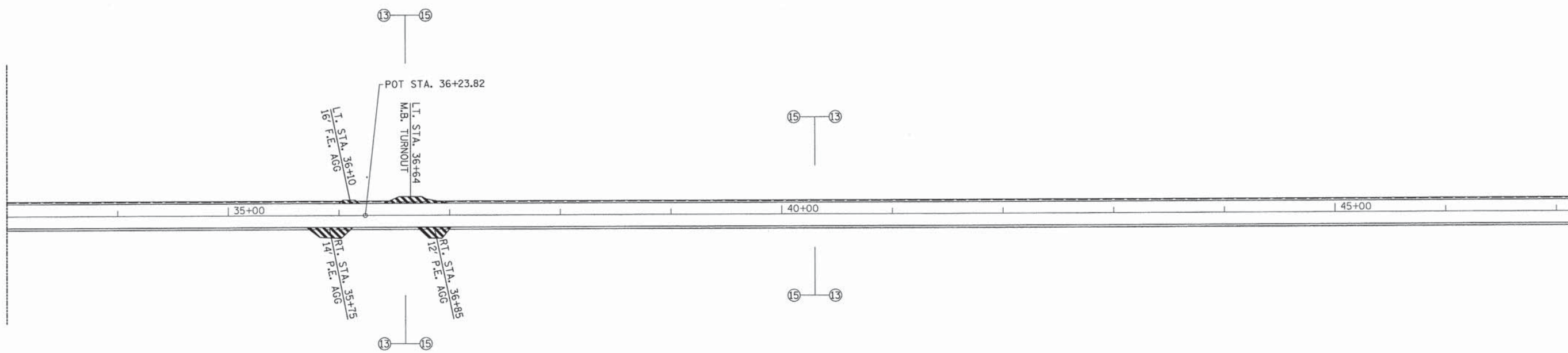
MATCH LINE STA. 18+75  
 SEE SHEET NO. 50



MATCH LINE STA. 33+00



MATCH LINE STA. 33+00



MATCH LINE STA. 47+20  
 SEE SHEET NO. 52

PRINT NUMBER: 140566\_06\_06\_16.dwg  
 PLOT DATE: 3/1/2016 1:13:09 PM  
 PLOT SCALE: 0.0833' / 1"



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.0833' / 1"       | CHECKED - JMS  | REVISED - |
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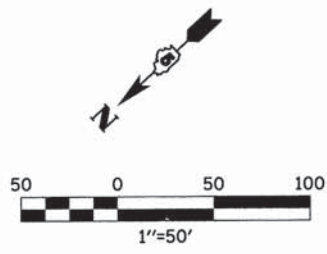
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN**  
**ALLERTON ROAD**

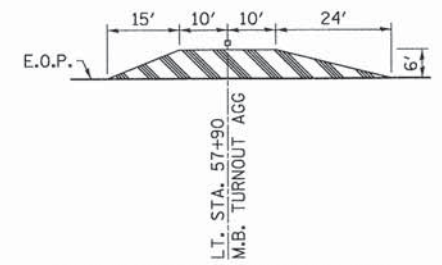
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|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 51        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |





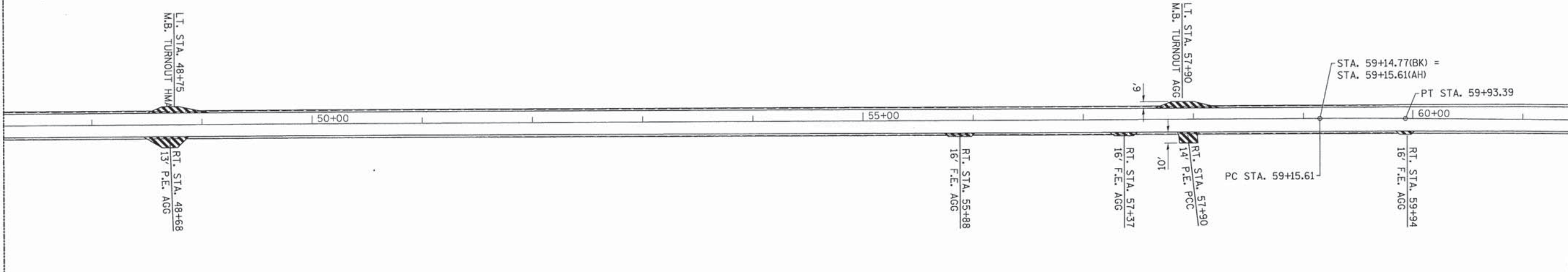
**LEGEND**  
 INCIDENTAL HMA RESURFACING



**M.B. TURNOUT LT. STA. 57 + 90  
 INCIDENTAL HMA SURFACING DETAIL**

NOTE: SEE PLAN SHEET NO. 66  
 FOR ADDITIONAL DETAILS.

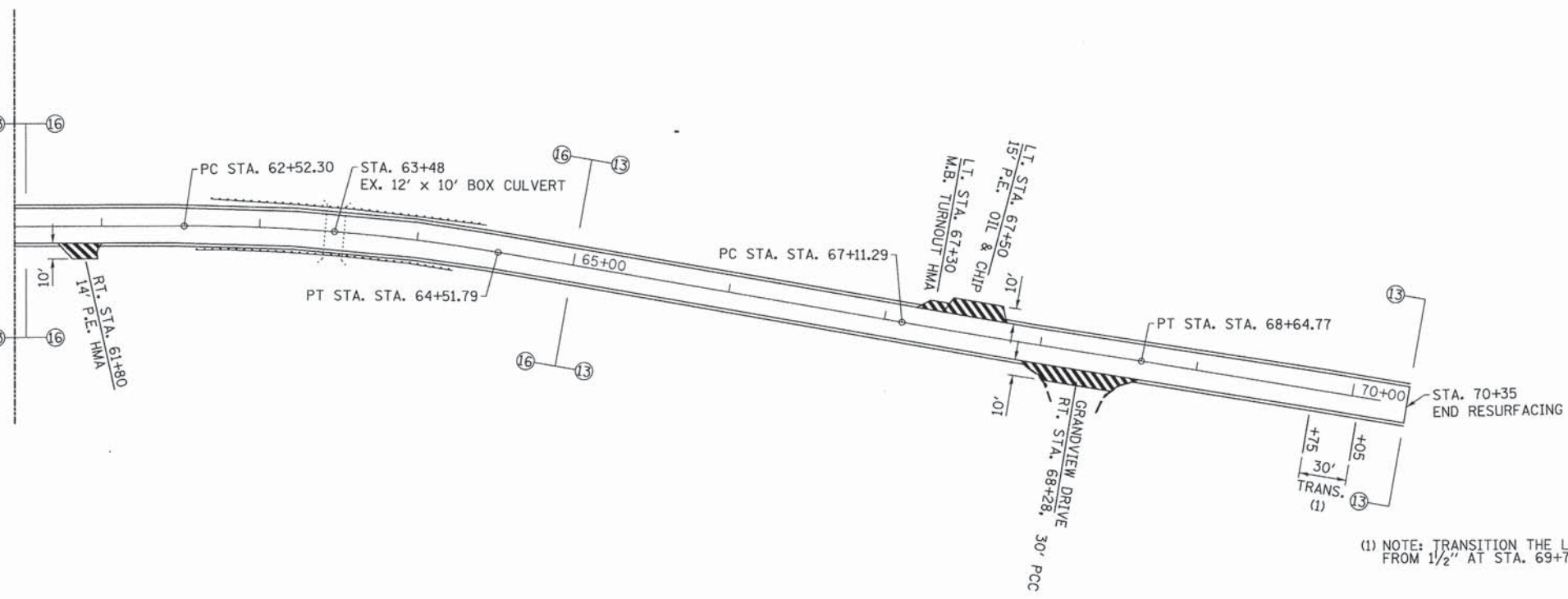
MATCH LINE STA. 47 + 20  
 SEE SHEET NO. 51



MATCH LINE STA. 61 + 45



MATCH LINE STA. 61 + 45



(1) NOTE: TRANSITION THE LEVELING BINDER (MACHINE METHOD)  
 FROM 1/2" AT STA. 69+75 TO 0" AT STA. 70+05

PRINT: 01/19/2016 1:13:19 PM  
 PLOT: 01/19/2016 1:13:19 PM  
 FILE: 14-00023-00-RS-01.dwg



|                                 |                |           |
|---------------------------------|----------------|-----------|
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| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
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| PLOT DATE = 3/1/2016 1:13:19 PM | DATE - 01/2016 | REVISED - |

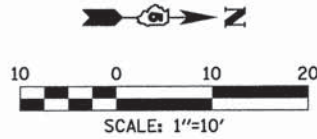
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN  
 ALLERTON ROAD**

SCALE 1"=50' SHEET NO. 3 OF 3 SHEETS STA. TO STA.

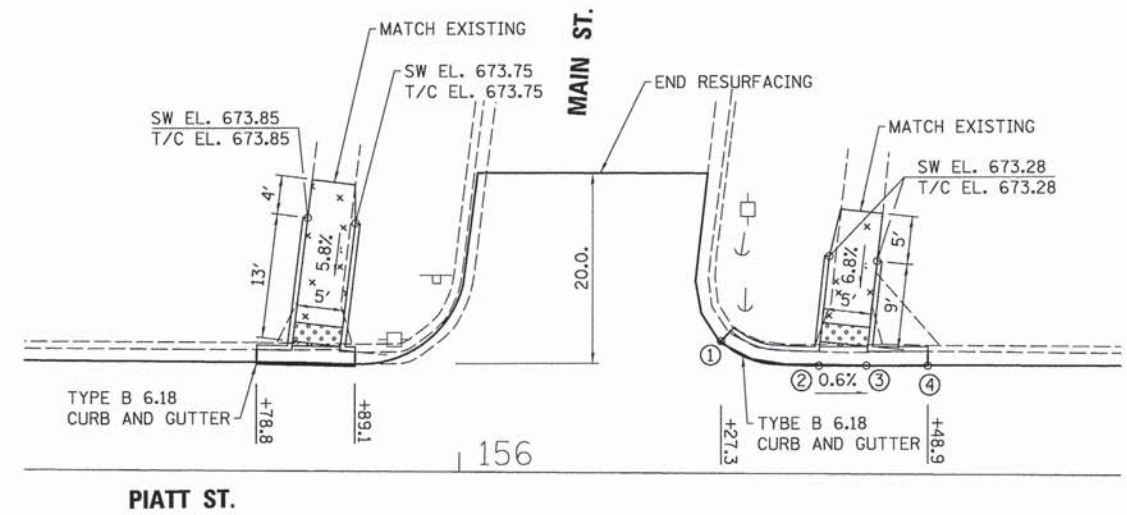
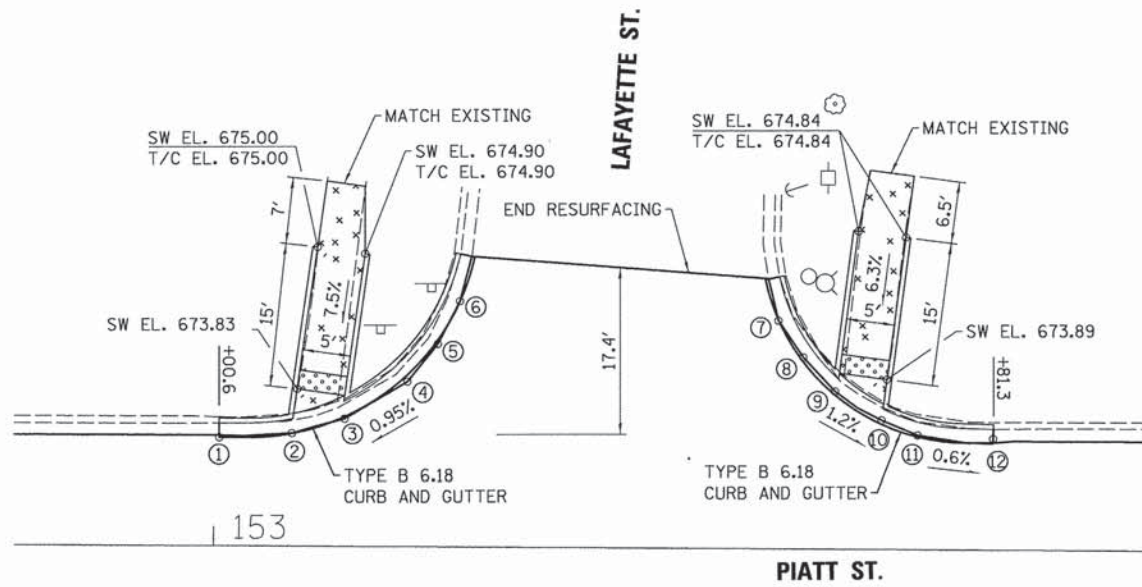
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|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                       | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 52        |
| CONTRACT NO. 91516                              |                |        |              |           |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        |              |           |





**LEGEND**

- PCC SIDEWALK, 6"
- DETECTABLE WARNINGS



| PROPOSED PAVEMENT ELEVATION<br>PIATT ST. AND LAFAYETTE ST. |           |            |           |
|--|-----------|------------|-----------|
| NO.  | STA.      | OFFSET     | ELEVATION |
| 1  | 153+00.59 | LT. 11.27' | 673.74    |
| 2  | 153+08.17 | LT. 11.75' | 673.82    |
| 3  | 153+13.66 | LT. 13.29' | 673.87    |
| 4  | 153+20.21 | LT. 17.18' | 673.94    |
| 5  | 153+23.39 | LT. 21.16' | 673.98    |
| 6  | 153+25.70 | LT. 25.65' | 674.03    |

| PROPOSED PAVEMENT ELEVATION<br>PIATT ST. AND LAFAYETTE ST. |           |            |           |
|--|-----------|------------|-----------|
| NO.  | STA.      | OFFSET     | ELEVATION |
| 7  | 153+58.89 | LT. 23.80' | 674.06    |
| 8  | 153+61.47 | LT. 19.99' | 674.00    |
| 9  | 153+64.79 | LT. 16.43' | 673.94    |
| 10   | 153+69.63 | LT. 13.45' | 673.88    |
| 11   | 153+73.38 | LT. 11.91' | 673.86    |
| 12   | 153+81.26 | LT. 11.57' | 673.81    |

| PROPOSED PAVEMENT ELEVATION<br>PIATT ST. AND MAIN ST. |           |            |           |
|---|-----------|------------|-----------|
| NO.   | STA.      | OFFSET     | ELEVATION |
| 1   | 156+27.31 | LT. 13.81' | 672.78    |
| 2   | 156+37.53 | LT. 11.28' | 672.72    |
| 3   | 156+42.48 | LT. 11.31' | 672.69    |
| 4   | 156+48.87 | LT. 11.34' | 672.57    |

- NOTES:
- THE SIDEWALK CURB RAMPS SHALL CONFORM TO THE SIDEWALK CURB RAMP DETAILS ON SHEET NO. 68 AND ALSO CONFORM TO THE DETAILS SHOWN ON STANDARD 424001.
  - THE ELEVATIONS FOR THE SIDEWALK AND THE % SLOPES ARE FOR GENERAL LAYOUT AND INFORMATION ONLY. THE MAXIMUM HANDICAP RAMP SLOPES AND SIDEWALK CROSS SLOPES SHALL BE ACCORDING TO STANDARD 424001. FIELD ADJUSTMENTS SHALL MADE AS NECESSARY FOR THE SIDEWALK AND RAMPS TO CONFORM TO THE STANDARD.
  - THE PROPOSED CURB SHALL BE CONSTRUCTED ACCORDING TO THE DETAILS SHOWN ON SHEET NO. 70. THE CURB THAT IS EQUAL TO OR LESS THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE SIDEWALK MONOLITHIC CURB DETAIL AND THE CURB HEIGHT THAT IS GREATER THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE PORTLAND CEMENT CONCRETE SIDEWALK CURB DETAIL.

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|                                 |                |           |
|---------------------------------|----------------|-----------|
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| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
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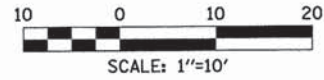
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSECTION DETAILS  
PIATT STREET**

SCALE 1"=10' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|                     |                |        |                           |           |
|---------------------|----------------|--------|---------------------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT  | 76                        | 53        |
| FED. ROAD DIST. NO. |                |        | ILLINOIS FED. AID PROJECT |           |
|                     |                |        | CONTRACT NO. 91516        |           |



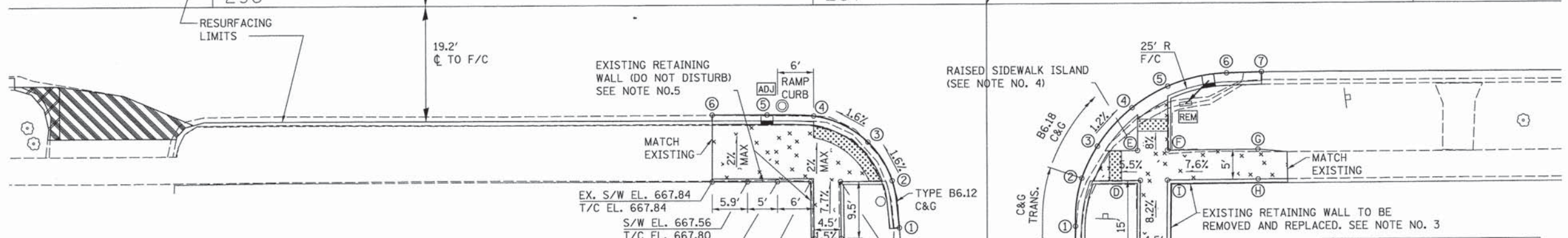
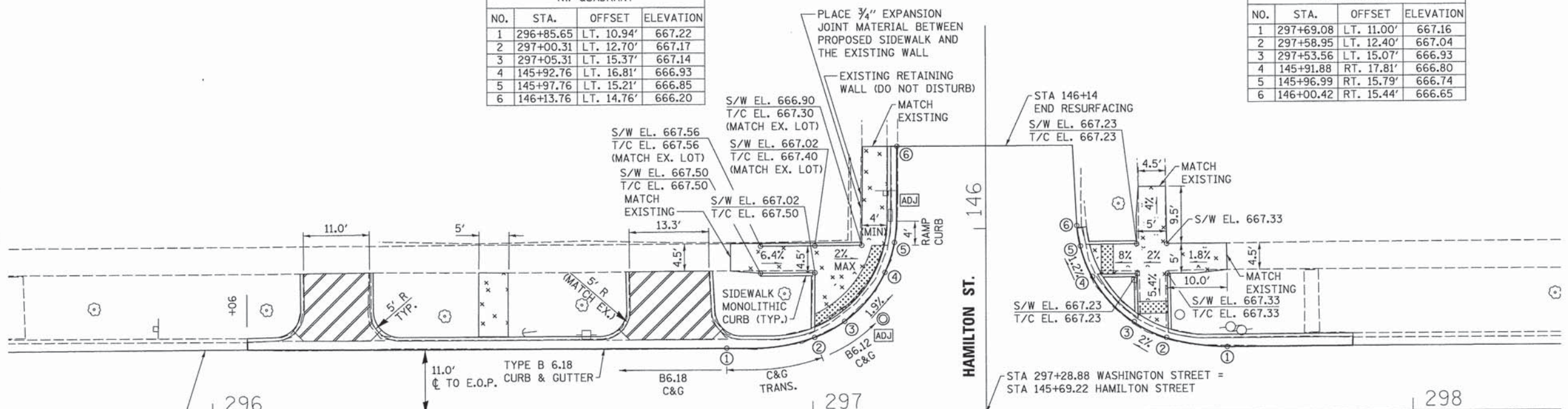


**LEGEND**

- PCC SIDEWALK, 6"
- PCC DRIVEWAY PAVEMENT, 6"
- DETECTABLE WARNINGS
- INCIDENTAL HMA RESURFACING
- DRAINAGE STRUCTURE TO BE ADJUSTED
- DRAINAGE STRUCTURE TO BE REMOVED
- WATER VALVE BOX TO BE ADJUSTED
- PROPOSED INLET
- PROPOSED STORM SEWER

| PROPOSED PAVEMENT ELEVATION NW QUADRANT |           |            |           |
|---|-----------|------------|-----------|
| NO.                                     | STA.      | OFFSET     | ELEVATION |
| 1                                       | 296+85.65 | LT. 10.94' | 667.22    |
| 2                                       | 297+00.31 | LT. 12.70' | 667.17    |
| 3                                       | 297+05.31 | LT. 15.37' | 667.14    |
| 4                                       | 145+92.76 | LT. 16.81' | 666.93    |
| 5                                       | 145+97.76 | LT. 15.21' | 666.85    |
| 6                                       | 146+13.76 | LT. 14.76' | 666.20    |

| PROPOSED PAVEMENT ELEVATION NE QUADRANT |           |            |           |
|---|-----------|------------|-----------|
| NO.                                     | STA.      | OFFSET     | ELEVATION |
| 1                                       | 297+69.08 | LT. 11.00' | 667.16    |
| 2                                       | 297+58.95 | LT. 12.40' | 667.04    |
| 3                                       | 297+53.56 | LT. 15.07' | 666.93    |
| 4                                       | 145+91.88 | RT. 17.81' | 666.80    |
| 5                                       | 145+96.99 | RT. 15.79' | 666.74    |
| 6                                       | 146+00.42 | RT. 15.44' | 666.65    |



| PROPOSED PAVEMENT ELEVATION SW QUADRANT |           |            |           |
|---|-----------|------------|-----------|
| NO.                                     | STA.      | OFFSET     | ELEVATION |
| 1                                       | 145+31.92 | LT. 14.63' | 667.50    |
| 2                                       | 145+39.80 | LT. 15.81' | 667.37    |
| 3                                       | 145+46.27 | LT. 19.83' | 667.24    |
| 4                                       | 297+00.05 | RT. 18.54' | 667.10    |
| 5                                       | 296+92.28 | RT. 18.33' | 667.00    |
| 6                                       | 296+83.15 | RT. 18.32' | 667.03    |

| PROPOSED PAVEMENT ELEVATION SE QUADRANT |           |            |           |
|---|-----------|------------|-----------|
| NO.                                     | STA.      | OFFSET     | ELEVATION |
| 1                                       | 145+32.03 | RT. 14.69' | 667.58    |
| 2                                       | 145+40.04 | RT. 15.80' | 667.48    |
| 3                                       | 145+45.40 | RT. 18.43' | 667.41    |
| 4                                       | 297+53.11 | RT. 17.43' | 667.28    |
| 5                                       | 297+59.07 | RT. 13.85' | 667.20    |
| 6                                       | 297+68.87 | RT. 11.52' | 667.13    |
| 7                                       | 297+74.68 | RT. 11.52' | 667.17    |

| PROPOSED SIDEWALK ELEVATION SE QUADRANT |           |            |               |               |
|---|-----------|------------|---------------|---------------|
| NO.                                     | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
| A                                       | 145+06.65 | RT. 25.29' | 669.61        | -             |
| B                                       | 145+17.65 | RT. 25.38' | 669.45        | 669.45        |
| C                                       | 145+24.61 | RT. 25.44' | 668.90        | 669.40        |
| D                                       | 145+39.62 | RT. 25.56' | 667.73        | 668.23        |
| E                                       | 297+53.99 | RT. 24.60' | 667.63        | 667.90        |
| F                                       | 297+58.99 | RT. 24.57' | 667.63        | 667.63        |
| G                                       | 297+74.05 | RT. 24.47' | 668.64        | -             |
| H                                       | 297+74.08 | RT. 29.47' | 668.71        | 669.35        |
| I                                       | 145+39.65 | RT. 30.06' | 667.73        | 669.23        |
| J                                       | 145+24.61 | RT. 29.94' | 668.96        | 669.86        |
| K                                       | 145+17.65 | RT. 29.88' | 669.52        | 670.15        |
| L                                       | 145+06.65 | RT. 29.79' | 669.68        | 670.31        |

- NOTES:**
- THE SIDEWALK CURB RAMPS SHALL CONFORM TO THE SIDEWALK CURB RAMP DETAILS ON SHEET NO. 68 AND ALSO CONFORM TO THE DETAILS SHOWN ON STANDARD 424001 AND STANDARD 424021.
  - THE ELEVATIONS FOR THE SIDEWALK AND THE % SLOPES ARE FOR GENERAL LAYOUT AND INFORMATION ONLY. THE MAXIMUM HANDICAP RAMP SLOPES AND SIDEWALK CROSS SLOPES SHALL BE ACCORDING TO STANDARD 424001 AND STANDARD 424021. FIELD ADJUSTMENTS SHALL MADE AS NECESSARY FOR THE SIDEWALK AND RAMPS TO CONFORM TO THE STANDARD.
  - THE PROPOSED CURB SHALL BE CONSTRUCTED ACCORDING TO THE DETAILS SHOWN ON SHEET NO. 70. THE CURB THAT IS EQUAL TO OR LESS THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE SIDEWALK MONOLITHIC CURB DETAIL AND THE CURB HEIGHT THAT IS GREATER THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE PORTLAND CEMENT CONCRETE SIDEWALK CURB DETAIL.
  - THE RAISED SIDEWALK ISLAND SHALL BE POURED MONOLITHIC WITH THE PC CONCRETE SIDEWALK 6". THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PORTLAND CEMENT CONCRETE SIDEWALK 6".
  - THE EXISTING RETAINING WALL SHALL NOT BE DISTURBED IN THE SOUTHWEST QUADRANT. A PROPOSED MONOLITHIC CURB SHALL BE CONSTRUCTED ADJACENT TO THE EXISTING RETAINING WALL TO THE ELEVATIONS SHOWN ON THE PLANS.
  - PROPOSED INLETS AND STORM SEWER SHOWN IN THE SE AND SW QUADRANTS ARE DETAILED ON SHEET NO. 46.
  - SAW THE EXISTING SIDEWALK ALONG THE PROPOSED BACK OF CURB AND REMOVE. PROVIDE A 7/8" RISER FROM THE PROPOSED SIDEWALK TO THE TOP OF THE EXISTING SIDEWALK STEP.

ESCA CONSULTANTS, INC.  
 1001 W. 14TH ST., SUITE 100  
 CHICAGO, IL 60604  
 TEL: 312.666.8800  
 FAX: 312.666.8801  
 WWW.ESCA-CONSULTANTS.COM



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.0833' / 1"       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:33:16 PM | DATE - 01/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSECTION DETAILS  
WASHINGTON STREET**

SCALE 1"=10' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                       | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 54        |
| CONTRACT NO. 91516                              |                |        |              |           |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        |              |           |





**LEGEND**

|  |                            |  |                                   |
|--|----------------------------|--|-----------------------------------|
|  | PCC SIDEWALK, 6"           |  | PROPOSED INLET                    |
|  | PCC DRIVEWAY PAVEMENT, 6"  |  | DRAINAGE STRUCTURE TO BE ADJUSTED |
|  | DETECTABLE WARNINGS        |  | WATER VALVE BOX TO BE ADJUSTED    |
|  | INCIDENTAL HMA RESURFACING |  |                                   |

**PROPOSED PAVEMENT ELEVATION  
NW QUAD. INDEPENDENCE ST. & MARION ST.**

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 120+41.34 | LT. 34.89' | 661.37    |
| 2   | 120+42.18 | LT. 26.27' | 661.45    |
| 3   | 120+44.89 | LT. 21.26' | 661.40    |
| 4   | 120+47.80 | LT. 18.38' | 661.35    |
| 5   | 120+50.98 | LT. 16.46' | 661.40    |
| 6   | 120+55.98 | LT. 15.06' | 661.45    |

**PROPOSED SIDEWALK ELEVATION  
NW QUAD. INDEPENDENCE ST. & MARION ST.**

| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 120+50.98 | LT. 21.26' | 661.62        | 661.87        |
| B   | 120+51.51 | LT. 26.24' | 661.72        | 661.72        |
| C   | 120+55.98 | LT. 26.24' | 661.72        | -             |
| D   | 120+55.98 | LT. 21.24' | 661.62        | 661.62        |

**PROPOSED PAVEMENT ELEVATION  
SW QUAD. INDEPENDENCE ST. & LAFAYETTE ST.**

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 135+28.85 | RT. 20.70' | 662.16    |
| 2   | 135+33.77 | RT. 21.56' | 662.18    |
| 3   | 122+75.04 | LT. 20.32' | 662.28    |
| 4   | 122+67.59 | LT. 15.54' | 662.37    |

**PROPOSED SIDEWALK ELEVATION  
SW QUAD. INDEPENDENCE ST. & LAFAYETTE ST.**

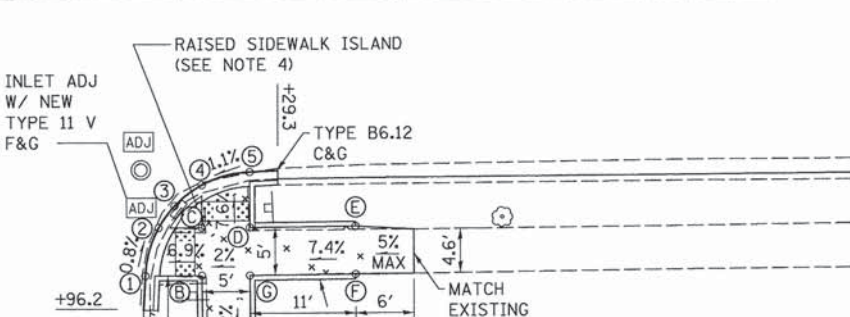
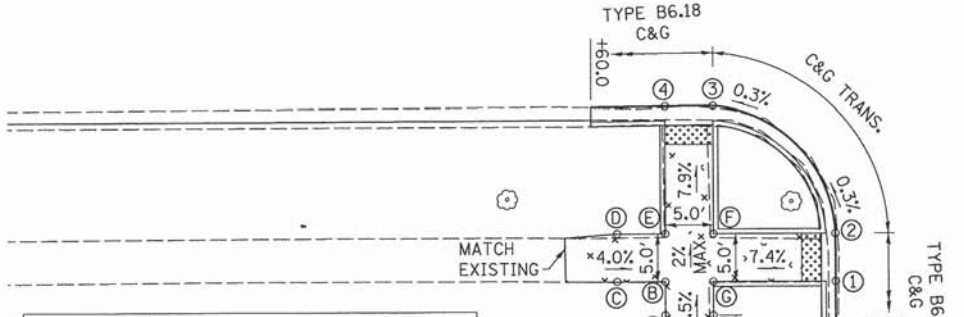
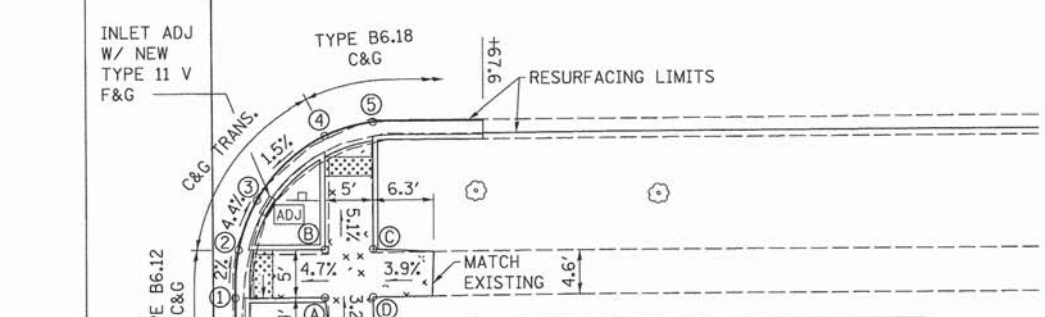
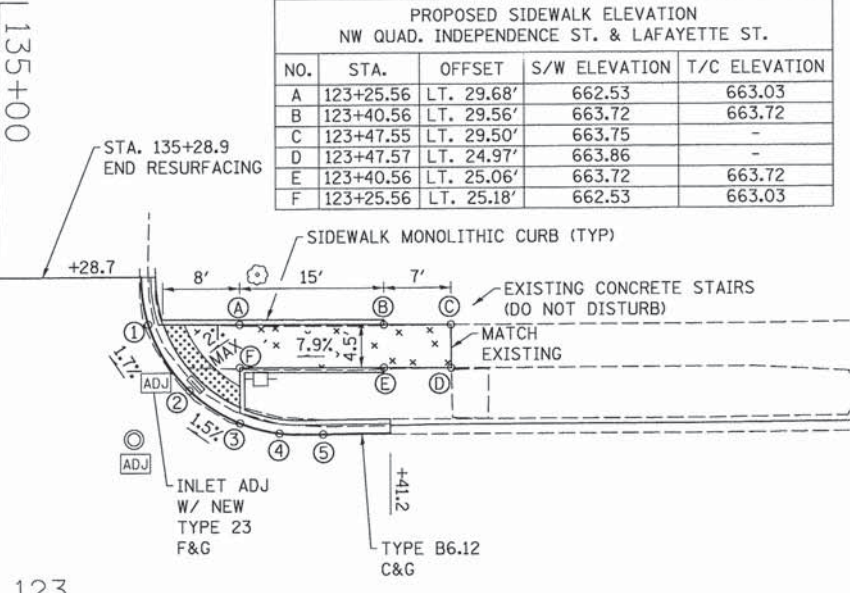
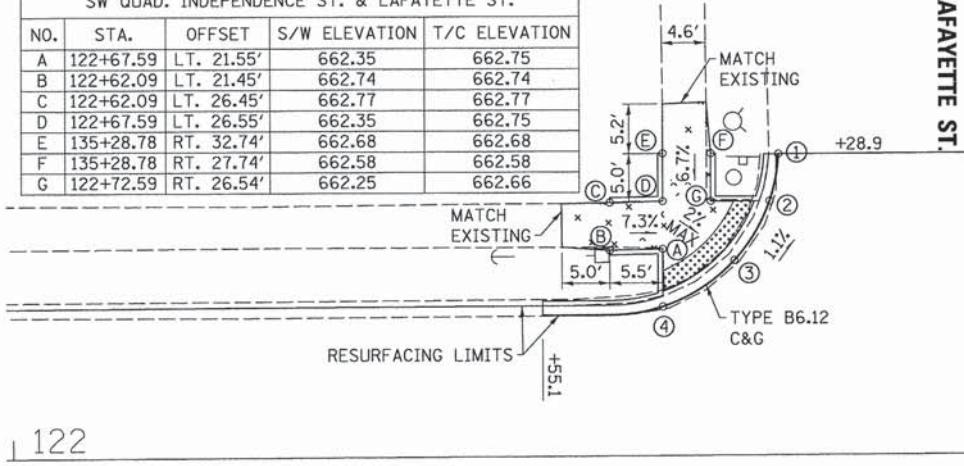
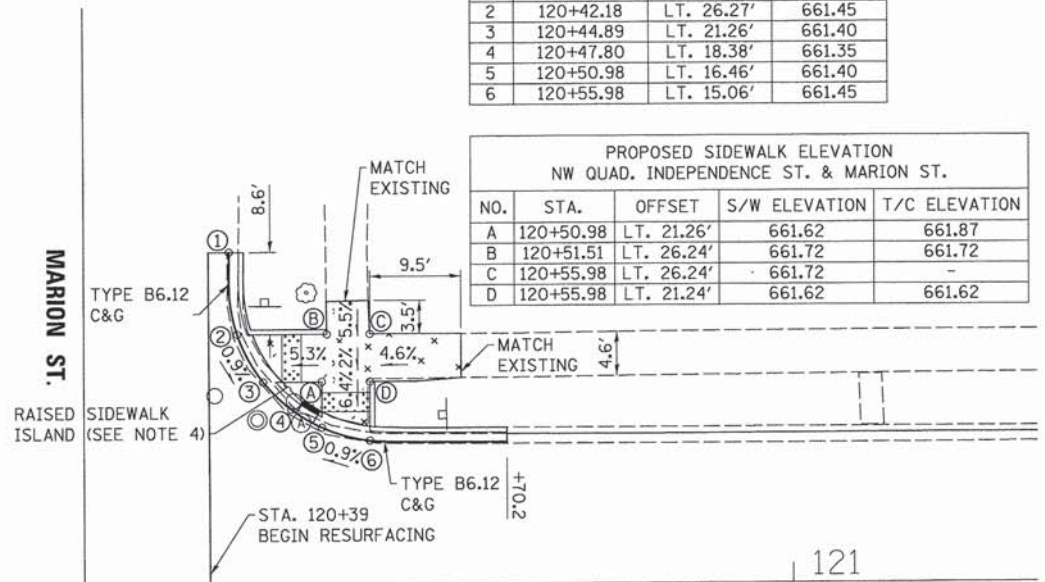
| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 122+67.59 | LT. 21.55' | 662.35        | 662.75        |
| B   | 122+62.09 | LT. 21.45' | 662.74        | 662.74        |
| C   | 122+62.09 | LT. 26.45' | 662.77        | 662.77        |
| D   | 122+67.59 | LT. 26.55' | 662.35        | 662.75        |
| E   | 135+28.78 | RT. 32.74' | 662.68        | 662.68        |
| F   | 135+28.78 | RT. 27.74' | 662.58        | 662.58        |
| G   | 122+72.59 | RT. 26.54' | 662.25        | 662.66        |

**PROPOSED PAVEMENT ELEVATION  
NW QUAD. INDEPENDENCE ST. & LAFAYETTE ST.**

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 135+33.75 | LT. 15.79' | 662.38    |
| 2   | 135+40.68 | LT. 20.12' | 662.50    |
| 3   | 123+25.56 | LT. 19.36' | 662.60    |
| 4   | 123+29.67 | LT. 18.25' | 662.72    |
| 5   | 123+34.22 | LT. 18.13' | 662.83    |

**PROPOSED SIDEWALK ELEVATION  
NW QUAD. INDEPENDENCE ST. & LAFAYETTE ST.**

| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 123+25.56 | LT. 29.68' | 662.53        | 663.03        |
| B   | 123+40.56 | LT. 29.56' | 663.72        | 663.72        |
| C   | 123+47.55 | LT. 29.50' | 663.75        | -             |
| D   | 123+47.57 | LT. 24.97' | 663.86        | -             |
| E   | 123+40.56 | LT. 25.06' | 663.72        | 663.72        |
| F   | 123+25.56 | LT. 25.18' | 662.53        | 663.03        |



**PROPOSED PAVEMENT ELEVATION  
NE QUAD. INDEPENDENCE ST. & MARION ST.**

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 120+41.78 | RT. 32.78' | 661.74    |
| 2   | 120+42.14 | RT. 27.78' | 661.64    |
| 3   | 120+44.06 | RT. 22.59' | 661.40    |
| 4   | 120+51.13 | RT. 15.87' | 661.54    |
| 5   | 120+56.13 | RT. 14.44' | 661.61    |

**PROPOSED PAVEMENT ELEVATION  
SE QUAD. INDEPENDENCE ST. & LAFAYETTE ST.**

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 135+92.91 | RT. 14.27' | 662.76    |
| 2   | 135+87.91 | RT. 14.35' | 662.74    |
| 3   | 122+67.66 | RT. 14.26' | 662.68    |
| 4   | 122+72.66 | RT. 14.28' | 662.66    |

**PROPOSED PAVEMENT ELEVATION  
NE QUAD. INDEPENDENCE ST. & LAFAYETTE ST.**

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 135+92.60 | LT. 15.87' | 662.66    |
| 2   | 135+87.60 | LT. 17.21' | 662.62    |
| 3   | 135+85.32 | LT. 18.85' | 662.60    |
| 4   | 123+21.42 | RT. 19.67' | 662.64    |
| 5   | 123+26.42 | RT. 18.35' | 662.70    |

**PROPOSED SIDEWALK ELEVATION  
NE QUAD. INDEPENDENCE ST. & MARION ST.**

| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 120+51.12 | RT. 32.81' | 662.02        | 662.02        |
| B   | 120+51.13 | RT. 27.81' | 662.02        | 662.02        |
| C   | 120+56.13 | RT. 27.79' | 662.02        | 662.02        |
| D   | 120+56.12 | RT. 32.79' | 662.10        | -             |

**PROPOSED SIDEWALK ELEVATION  
SE QUAD. INDEPENDENCE ST. & LAFAYETTE ST.**

| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 135+96.41 | RT. 31.97' | 663.69        | -             |
| B   | 122+67.66 | RT. 32.58' | 663.60        | -             |
| C   | 122+62.66 | RT. 32.56' | 663.80        | -             |
| D   | 122+62.66 | RT. 27.56' | 663.70        | -             |
| E   | 122+67.66 | RT. 27.58' | 663.50        | 663.50        |
| F   | 122+72.66 | RT. 27.59' | 663.50        | 663.50        |
| G   | 122+72.66 | RT. 32.59' | 663.60        | 663.60        |
| H   | 135+96.41 | RT. 26.97' | 663.69        | -             |

**PROPOSED SIDEWALK ELEVATION  
NE QUAD. INDEPENDENCE ST. & LAFAYETTE ST.**

| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 136+01.60 | LT. 21.95' | 663.59        | 663.59        |
| B   | 123+21.42 | RT. 29.12' | 662.85        | 663.20        |
| C   | 123+21.42 | RT. 24.12' | 662.85        | 663.20        |
| D   | 123+26.42 | RT. 24.13' | 662.95        | 663.20        |
| E   | 123+37.42 | RT. 24.08' | 663.76        | 663.76        |
| F   | 123+37.42 | RT. 29.08' | 663.76        | 663.76        |
| G   | 123+26.42 | RT. 29.13' | 662.95        | 663.50        |
| H   | 136+01.60 | LT. 26.95' | 663.69        | 663.69        |

- NOTES:**
- THE SIDEWALK CURB RAMPS SHALL CONFORM TO THE SIDEWALK CURB RAMP DETAILS ON SHEET NO. 68 AND ALSO CONFORM TO THE DETAILS SHOWN ON STANDARD 424001 AND STANDARD 424021.
  - THE ELEVATIONS FOR THE SIDEWALK AND THE % SLOPES ARE FOR GENERAL LAYOUT AND INFORMATION ONLY. THE MAXIMUM HANDICAP RAMP SLOPES AND SIDEWALK CROSS SLOPES SHALL BE ACCORDING TO STANDARD 424001 AND STANDARD 424021. FIELD ADJUSTMENTS SHALL MADE AS NECESSARY FOR THE SIDEWALK AND RAMPS TO CONFORM TO THE STANDARD.

- THE PROPOSED CURB SHALL BE CONSTRUCTED ACCORDING TO THE DETAILS SHOWN ON SHEET NO. 70. THE CURB THAT IS EQUAL TO OR LESS THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE SIDEWALK MONOLITHIC CURB DETAIL AND THE CURB HEIGHT THAT IS GREATER THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE PORTLAND CEMENT CONCRETE SIDEWALK CURB DETAIL.
- THE RAISED SIDEWALK ISLAND SHALL BE POURED MONOLITHIC WITH THE PC CONCRETE SIDEWALK 6", THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PORTLAND CEMENT CONCRETE SIDEWALK 6".

5. PROPOSED INLET AND STORM SEWER IN THE NW QUADRANT OF MARION STREET AND INDEPENDENCE STREET ARE DETAILED ON SHEET NO. 47.

DRAWN BY: JLF  
 CHECKED BY: JMS  
 DATE: 01/2016  
 PLOT DATE: 3/1/2016 1:33:27 PM



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8833' / in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:33:27 PM | DATE - 01/2016 | REVISED - |

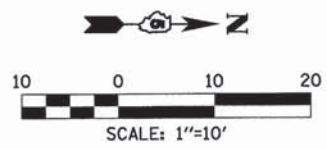
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSECTION DETAILS  
INDEPENDENCE STREET**

SCALE 1"=10' SHEET NO. 1 OF 3 SHEETS STA. TO STA.

|   |                        |              |                 |              |
|---|------------------------|--------------|-----------------|--------------|
| F.A. RTE. VAR                                   | SECTION 14-00023-00-RS | COUNTY PIATT | TOTAL SHEETS 76 | SHEET NO. 55 |
| CONTRACT NO. 91516                              |                        |              |                 |              |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                        |              |                 |              |





**LEGEND**

|  |                            |  |                                       |
|--|----------------------------|--|---------------------------------------|
|  | PCC SIDEWALK, 6"           |  | ADJ DRAINAGE STRUCTURE TO BE ADJUSTED |
|  | PCC DRIVEWAY PAVEMENT, 6"  |  | (A) WATER VALVE BOX TO BE ADJUSTED    |
|  | DETECTABLE WARNINGS        |  |                                       |
|  | INCIDENTAL HMA RESURFACING |  |                                       |

PROPOSED PAVEMENT ELEVATION  
SW QUAD. INDEPENDENCE ST. & MAIN ST.

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 140+25.66 | RT. 17.98' | 668.43    |
| 2   | 140+33.38 | RT. 20.31' | 668.35    |
| 3   | 125+37.74 | LT. 20.15' | 668.16    |
| 4   | 125+30.52 | LT. 18.40' | 668.08    |

PROPOSED SIDEWALK ELEVATION  
SW QUAD. INDEPENDENCE ST. & MAIN ST.

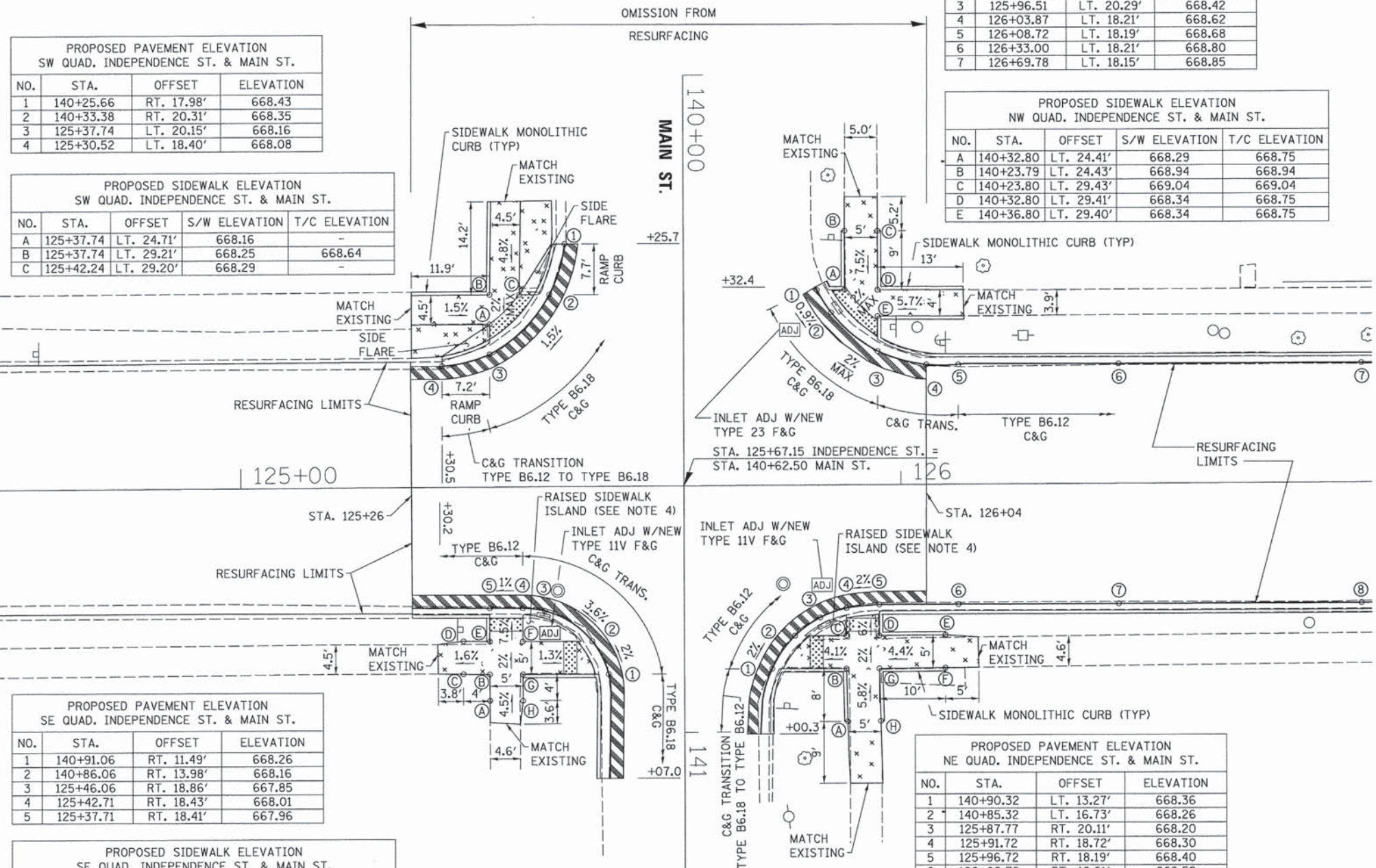
| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 125+37.74 | LT. 24.71' | 668.16        | -             |
| B   | 125+37.74 | LT. 29.21' | 668.25        | 668.64        |
| C   | 125+42.24 | LT. 29.20' | 668.29        | -             |

PROPOSED PAVEMENT ELEVATION  
NW QUAD. INDEPENDENCE ST. & MAIN ST.

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 140+32.80 | LT. 20.21' | 668.31    |
| 2   | 140+36.23 | LT. 22.38' | 668.26    |
| 3   | 125+96.51 | LT. 20.29' | 668.42    |
| 4   | 126+03.87 | LT. 18.21' | 668.62    |
| 5   | 126+08.72 | LT. 18.19' | 668.68    |
| 6   | 126+33.00 | LT. 18.21' | 668.80    |
| 7   | 126+69.78 | LT. 18.15' | 668.85    |

PROPOSED SIDEWALK ELEVATION  
NW QUAD. INDEPENDENCE ST. & MAIN ST.

| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 140+32.80 | LT. 24.41' | 668.29        | 668.75        |
| B   | 140+23.79 | LT. 24.43' | 668.94        | 668.94        |
| C   | 140+23.80 | LT. 29.43' | 669.04        | 669.04        |
| D   | 140+32.80 | LT. 29.41' | 668.34        | 668.75        |
| E   | 140+36.80 | LT. 29.40' | 668.34        | 668.75        |



PROPOSED PAVEMENT ELEVATION  
SE QUAD. INDEPENDENCE ST. & MAIN ST.

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 140+91.06 | RT. 11.49' | 668.26    |
| 2   | 140+86.06 | RT. 13.98' | 668.16    |
| 3   | 125+46.06 | RT. 18.86' | 667.85    |
| 4   | 125+42.71 | RT. 18.43' | 668.01    |
| 5   | 125+37.71 | RT. 18.41' | 667.96    |

PROPOSED SIDEWALK ELEVATION  
SE QUAD. INDEPENDENCE ST. & MAIN ST.

| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 140+95.06 | RT. 29.48' | 668.52        | -             |
| B   | 125+37.71 | RT. 28.46' | 668.34        | -             |
| C   | 125+33.71 | RT. 28.39' | 668.26        | -             |
| D   | 125+33.71 | RT. 23.39' | 668.20        | -             |
| E   | 125+37.71 | RT. 23.46' | 668.24        | 668.24        |
| F   | 125+42.71 | RT. 23.48' | 668.24        | 668.50        |
| G   | 125+42.71 | RT. 28.48' | 668.34        | 668.34        |
| H   | 140+95.06 | RT. 24.48' | 668.52        | -             |

PROPOSED PAVEMENT ELEVATION  
NE QUAD. INDEPENDENCE ST. & MAIN ST.

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 140+90.32 | LT. 13.27' | 668.36    |
| 2   | 140+85.32 | LT. 16.73' | 668.26    |
| 3   | 125+87.77 | RT. 20.11' | 668.20    |
| 4   | 125+91.72 | RT. 18.72' | 668.30    |
| 5   | 125+96.72 | RT. 18.19' | 668.40    |
| 6   | 126+08.72 | RT. 18.21' | 668.52    |
| 7   | 126+33.00 | RT. 18.24' | 668.73    |
| 8   | 126+69.78 | RT. 18.28' | 668.79    |

PROPOSED SIDEWALK ELEVATION  
NE QUAD. INDEPENDENCE ST. & MAIN ST.

| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 140+98.37 | LT. 24.72' | 669.05        | 669.05        |
| B   | 125+91.72 | RT. 27.90' | 668.59        | 668.93        |
| C   | 125+91.72 | RT. 22.90' | 668.49        | 668.75        |
| D   | 125+96.72 | RT. 22.91' | 668.49        | 668.85        |
| E   | 126+06.72 | RT. 22.86' | 668.98        | 668.98        |
| F   | 126+06.72 | RT. 27.86  | 668.98        | 668.98        |
| G   | 125+96.72 | RT. 27.91' | 668.59        | 668.93        |
| H   | 140+98.32 | LT. 29.73  | 669.05        | 669.05        |

- NOTES:
- THE SIDEWALK CURB RAMP SHALL CONFORM TO THE SIDEWALK CURB RAMP DETAILS ON SHEET NO. 68 AND ALSO CONFORM TO THE DETAILS SHOWN ON STANDARD 424001 AND STANDARD 424021.
  - THE ELEVATIONS FOR THE SIDEWALK AND THE % SLOPES ARE FOR GENERAL LAYOUT AND INFORMATION ONLY. THE MAXIMUM HANDICAP RAMP SLOPES AND SIDEWALK CROSS SLOPES SHALL BE ACCORDING TO STANDARD 424001 AND STANDARD 424021. FIELD ADJUSTMENTS SHALL MADE AS NECESSARY FOR THE SIDEWALK AND RAMP TO CONFORM TO THE STANDARD.
  - THE PROPOSED CURB SHALL BE CONSTRUCTED ACCORDING TO THE DETAILS SHOWN ON SHEET NO. 70. THE CURB THAT IS EQUAL TO OR LESS THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE SIDEWALK MONOLITHIC CURB DETAIL AND THE CURB HEIGHT THAT IS GREATER THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE PORTLAND CEMENT CONCRETE SIDEWALK CURB DETAIL.
  - THE RAISED SIDEWALK ISLAND SHALL BE POURED MONOLITHIC WITH THE PC CONCRETE SIDEWALK 6". THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PORTLAND CEMENT CONCRETE SIDEWALK 6".

PRINTED BY: J. D. B. & S. L. INC.  
 FILE NAME: 14-00023-00-RS-INT-01.dwg  
 DATE: 01/27/2016 10:52:27 AM



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8933' / 1"       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:33:40 PM | DATE - 01/2016 | REVISED - |

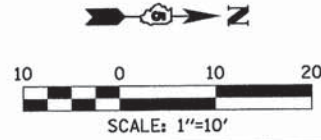
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

INTERSECTION DETAILS  
INDEPENDENCE STREET

SCALE 1"=10' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

|                     |                |                           |              |           |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76           | 56        |
| CONTRACT NO. 91516  |                |                           |              |           |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |              |           |





**LEGEND**

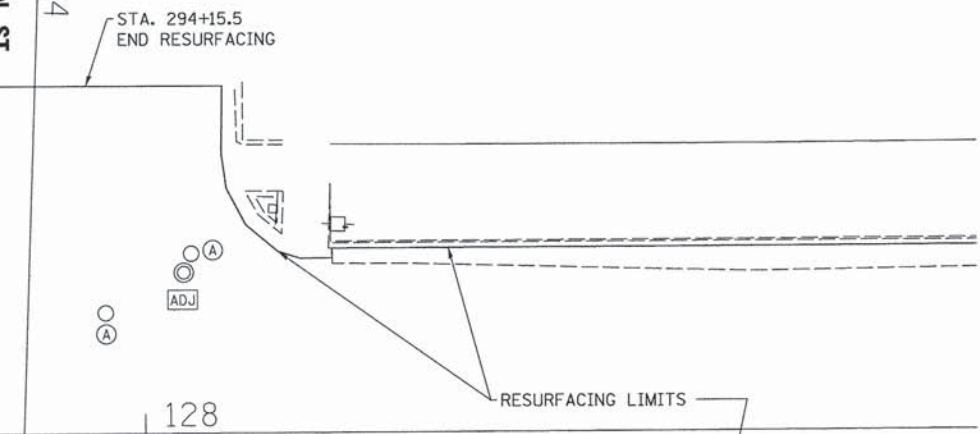
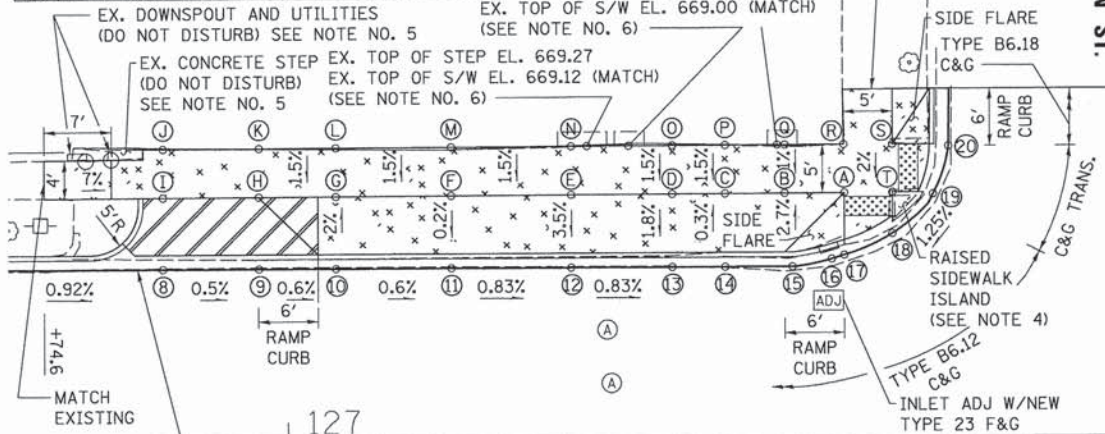
|  |                            |  |                                       |
|--|----------------------------|--|---------------------------------------|
|  | PCC SIDEWALK, 6"           |  | ADJ DRAINAGE STRUCTURE TO BE ADJUSTED |
|  | PCC DRIVEWAY PAVEMENT, 6"  |  | (A) WATER VALVE BOX TO BE ADJUSTED    |
|  | DETECTABLE WARNINGS        |  |                                       |
|  | INCIDENTAL HMA RESURFACING |  |                                       |

**PROPOSED PAVEMENT ELEVATION  
SW QUAD. INDEPENDENCE ST. & WASHINGTON ST.**

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 8   | 126+87.00 | LT. 18.11' | 668.69    |
| 9   | 126+97.00 | LT. 18.08' | 668.64    |
| 10  | 127+05.00 | LT. 18.05' | 668.59    |
| 11  | 127+17.00 | LT. 18.02' | 668.52    |
| 12  | 127+29.48 | LT. 17.98' | 668.42    |
| 13  | 127+40.00 | LT. 17.95' | 668.33    |
| 14  | 127+45.50 | LT. 17.93' | 668.28    |
| 15  | 127+52.50 | LT. 17.91' | 668.23    |
| 16  | 127+56.58 | LT. 18.68' | 668.14    |
| 17  | 127+57.90 | LT. 19.08' | 668.16    |
| 18  | 127+62.90 | LT. 21.28' | 668.22    |
| 19  | 294+21.54 | RT. 21.07' | 668.32    |
| 20  | 294+26.54 | RT. 19.63' | 668.38    |

**PROPOSED SIDEWALK ELEVATION  
SW QUAD. INDEPENDENCE ST. & WASHINGTON ST.**

| NO. | STA.      | OFFSET     | S/W ELEVATION | NO. | STA.      | OFFSET     | S/W ELEVATION |
|-----|-----------|------------|---------------|-----|-----------|------------|---------------|
| A   | 127+57.90 | LT. 25.54' | 668.45        | K   | 126+97.00 | LT. 30.63' | 668.92        |
| B   | 127+51.70 | LT. 25.54' | 668.49        | L   | 127+05.00 | LT. 30.62' | 668.96        |
| C   | 127+45.50 | LT. 25.54' | 668.68        | M   | 127+17.00 | LT. 30.62' | 669.02        |
| D   | 127+40.00 | LT. 25.54' | 668.86        | N   | 127+29.48 | LT. 30.61' | 669.12        |
| E   | 127+29.48 | LT. 25.54' | 669.05        | O   | 127+40.00 | LT. 30.60' | 668.93        |
| F   | 127+17.00 | LT. 25.54' | 668.95        | P   | 127+45.50 | LT. 30.60' | 668.75        |
| G   | 127+05.00 | LT. 25.54' | 668.89        | Q   | 127+51.70 | LT. 30.60' | 668.54        |
| H   | 126+97.00 | LT. 25.54' | 668.85        | R   | 127+57.90 | LT. 30.59' | 668.55        |
| I   | 126+87.00 | LT. 25.54' | 668.84        | S   | 127+62.90 | LT. 30.58' | 668.55        |
| J   | 126+87.00 | LT. 30.63' | 668.91        | T   | 127+62.90 | LT. 25.58' | 668.45        |



**PROPOSED SIDEWALK ELEVATION  
SE QUAD. INDEPENDENCE ST. & WASHINGTON ST.**

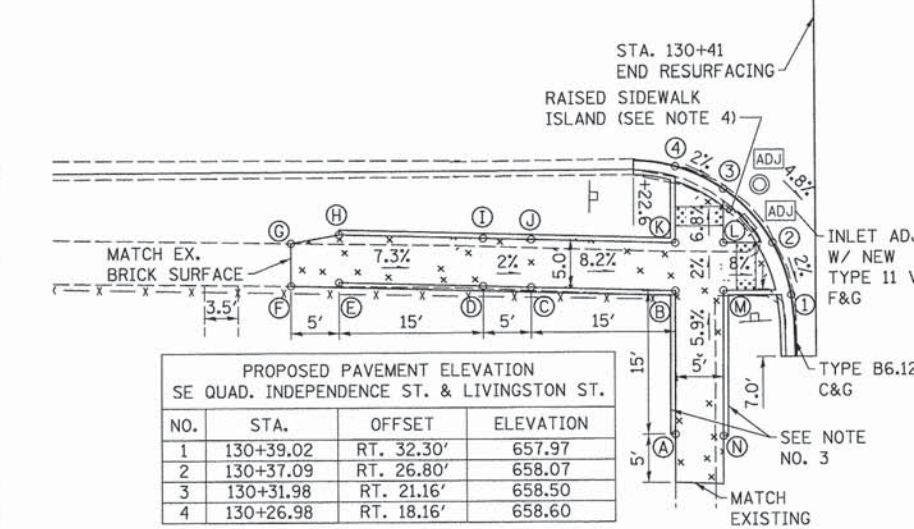
| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 294+89.00 | RT. 29.46' | 668.65        | 668.65        |
| B   | 127+58.09 | RT. 27.06' | 668.38        | 668.74        |
| C   | 127+51.59 | RT. 27.19' | 668.74        | 668.74        |
| D   | 127+51.59 | RT. 22.19' | 668.66        | 668.66        |
| E   | 127+58.09 | RT. 22.06' | 668.26        | 668.66        |
| F   | 127+63.09 | RT. 27.07' | 668.28        | 668.74        |
| G   | 294+89.00 | RT. 24.46' | 668.60        | 668.60        |

**PROPOSED PAVEMENT ELEVATION  
NE QUAD. INDEPENDENCE ST. & WASHINGTON ST.**

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 294+89.47 | LT. 11.00' | 668.07    |
| 2   | 294+78.90 | LT. 13.28' | 668.02    |
| 3   | 128+15.89 | RT. 18.43' | 667.82    |
| 4   | 128+21.81 | RT. 18.22' | 667.56    |

**PROPOSED SIDEWALK ELEVATION  
NE QUAD. INDEPENDENCE ST. & WASHINGTON ST.**

| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 294+86.90 | LT. 23.28' | 668.34        | 668.34        |
| B   | 294+78.90 | LT. 23.37' | 667.92        | 668.30        |
| C   | 294+74.29 | LT. 28.39' | 667.84        | 668.26        |
| D   | 294+78.90 | LT. 28.37' | 667.92        | 668.30        |
| E   | 294+86.90 | LT. 28.28' | 668.24        | 668.24        |



**PROPOSED PAVEMENT ELEVATION  
SE QUAD. INDEPENDENCE ST. & LIVINGSTON ST.**

| NO. | STA.      | OFFSET     | ELEVATION |
|-----|-----------|------------|-----------|
| 1   | 130+39.02 | RT. 32.30' | 657.97    |
| 2   | 130+37.09 | RT. 26.80' | 658.07    |
| 3   | 130+31.98 | RT. 21.16' | 658.50    |
| 4   | 130+26.98 | RT. 18.16' | 658.60    |

**PROPOSED SIDEWALK ELEVATION  
SE QUAD. INDEPENDENCE ST. & LIVINGSTON ST.**

| NO. | STA.      | OFFSET     | S/W ELEVATION | T/C ELEVATION |
|-----|-----------|------------|---------------|---------------|
| A   | 130+26.94 | RT. 46.77' | 659.23        | 659.23        |
| B   | 130+26.98 | RT. 31.76' | 658.38        | 659.25        |
| C   | 130+11.98 | RT. 31.28' | 659.61        | 659.92        |
| D   | 130+06.98 | RT. 31.11' | 659.71        | 660.14        |
| E   | 129+91.98 | RT. 30.62' | 660.80        | 660.80        |
| F   | 129+86.98 | RT. 30.96' | 660.94        | -             |
| G   | 129+86.98 | RT. 26.49' | 660.86        | -             |
| H   | 129+91.98 | RT. 25.62' | 660.80        | 660.80        |
| I   | 130+06.98 | RT. 26.11' | 659.71        | 659.98        |
| J   | 130+11.98 | RT. 26.27' | 659.61        | 659.71        |
| K   | 130+26.98 | RT. 26.76' | 658.38        | 658.90        |
| L   | 130+31.98 | RT. 26.78' | 658.27        | 658.90        |
| M   | 130+31.98 | RT. 31.78' | 658.27        | 658.90        |
| N   | 130+31.94 | RT. 46.99' | 659.18        | 659.18        |

- NOTES:**
- THE SIDEWALK CURB RAMPS SHALL CONFORM TO THE SIDEWALK CURB RAMP DETAILS ON SHEET NO. 68 AND ALSO CONFORM TO THE DETAILS SHOWN ON STANDARD 424001 AND STANDARD 424021.
  - THE ELEVATIONS FOR THE SIDEWALK AND THE % SLOPES ARE FOR GENERAL LAYOUT AND INFORMATION ONLY. THE MAXIMUM HANDICAP RAMP SLOPES AND SIDEWALK CROSS SLOPES SHALL BE ACCORDING TO STANDARD 424001 AND STANDARD 424021. FIELD ADJUSTMENTS SHALL MADE AS NECESSARY FOR THE SIDEWALK AND RAMPS TO CONFORM TO THE STANDARD.
  - THE PROPOSED CURB SHALL BE CONSTRUCTED ACCORDING TO THE DETAILS SHOWN ON SHEET NO. 70. THE CURB THAT IS EQUAL TO OR LESS THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE SIDEWALK MONOLITHIC CURB DETAIL AND THE CURB HEIGHT THAT IS GREATER THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE PORTLAND CEMENT CONCRETE SIDEWALK CURB DETAIL.
  - THE RAISED SIDEWALK ISLAND SHALL BE POURED MONOLITHIC WITH THE PC CONCRETE SIDEWALK 6". THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PORTLAND CEMENT CONCRETE SIDEWALK 6".

- THE PROPOSED SIDEWALK SHALL BE POURED TO PROVIDE A NOMINAL 11" RISER FOR THE EXISTING STEP AND PROVIDE AN ADA COMPLIANT RAMP SLOPE AS SHOWN. A 6" HIGH CURB SHALL BE POURED MONOLITHIC WITH THE SIDEWALK TO PROTECT THE EXISTING UTILITIES AND DOWNPOUT ADJACENT TO THE STEP AS DIRECTED BY THE ENGINEER.
- THE EXISTING CONCRETE STEPS AT THE DOORWAYS SHALL NOT BE DISTURBED. THE PROPOSED SIDEWALK ELEVATION SHALL MATCH THE EXISTING SIDEWALK ELEVATION OR AS DIRECTED BY THE ENGINEER.
- THE EXISTING TOURIST INFORMATION SIGN SHALL NOT BE DISTURBED. THE EXISTING LANDSCAPING THAT CONFLICTS WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED BY THE CITY.



|                           |                |           |
|---------------------------|----------------|-----------|
| USER NAME = skm           | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61   | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8833' / 1" | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016      | DATE - 01/2016 | REVISED - |

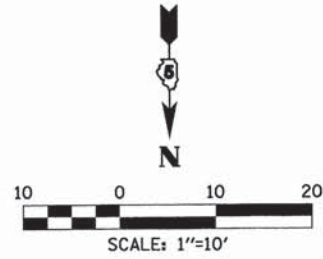
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSECTION DETAILS  
INDEPENDENCE STREET**

SCALE 1"=10' SHEET NO. 3 OF 3 SHEETS STA. TO STA.

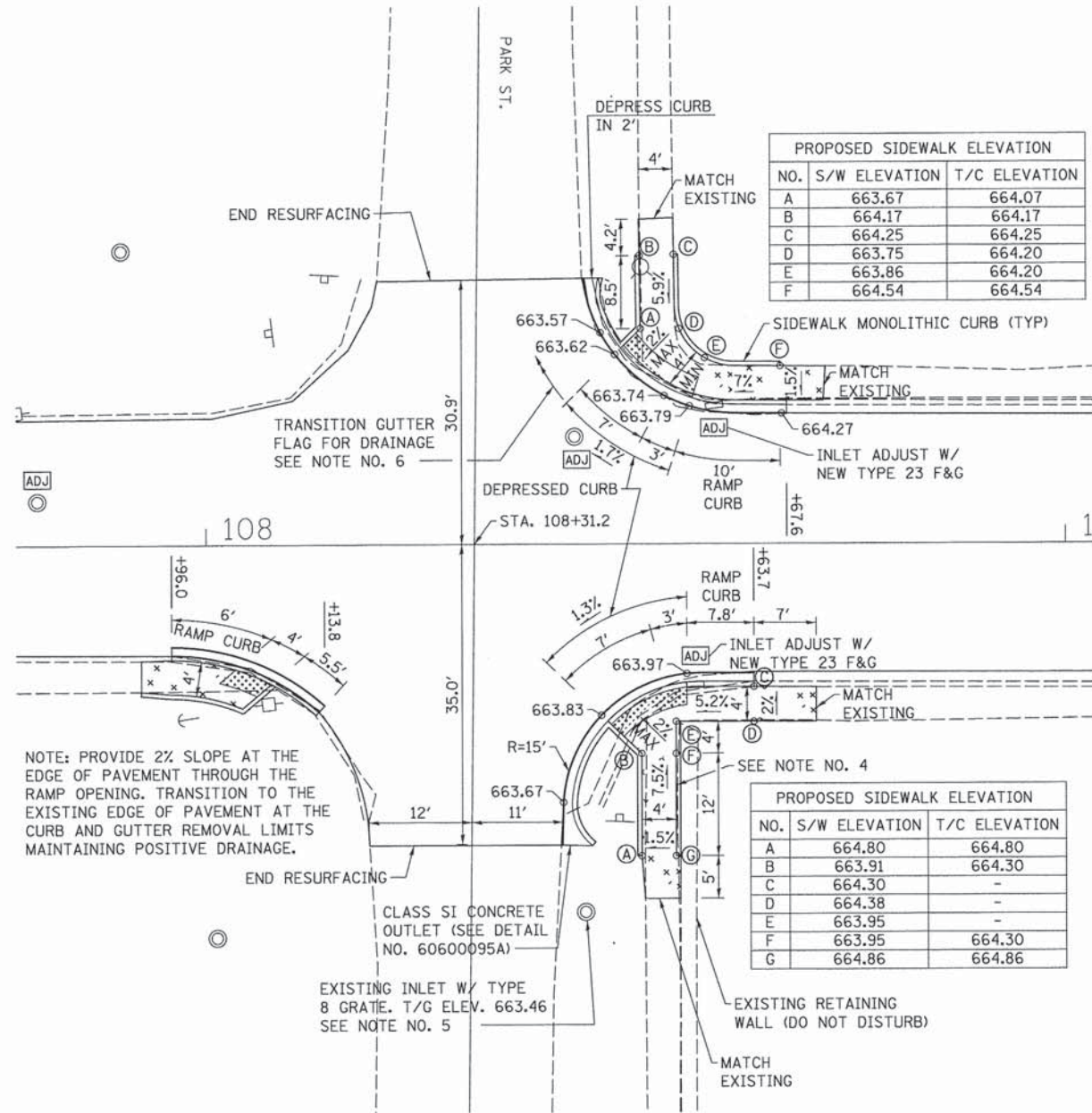
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|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 57        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |





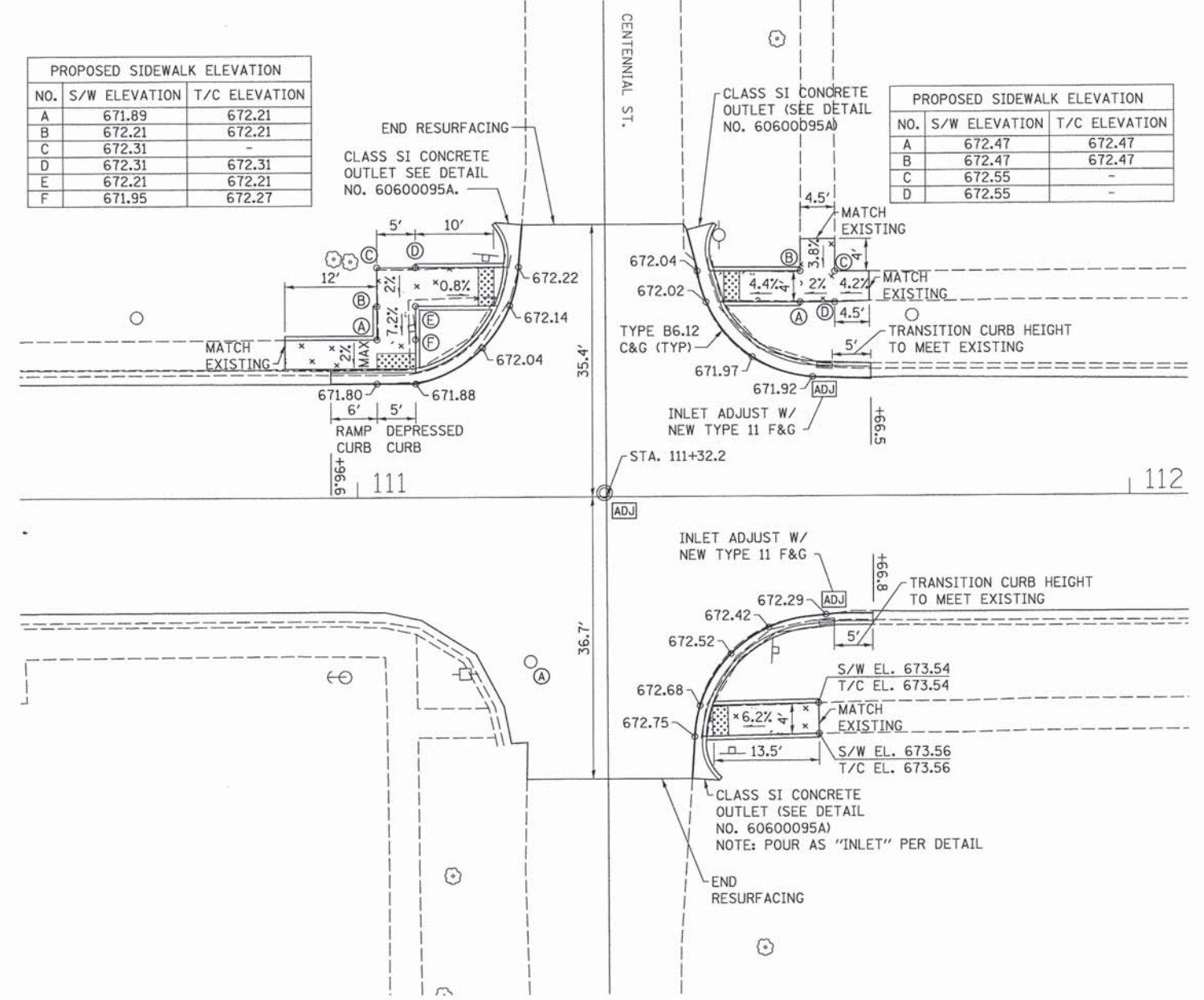
**LEGEND**

|  |                            |  |                                       |
|--|----------------------------|--|---------------------------------------|
|  | PCC SIDEWALK, 6"           |  | ADJ DRAINAGE STRUCTURE TO BE ADJUSTED |
|  | PCC DRIVEWAY PAVEMENT, 6"  |  | (A) WATER VALVE BOX TO BE ADJUSTED    |
|  | DETECTABLE WARNINGS        |  |                                       |
|  | INCIDENTAL HMA RESURFACING |  |                                       |



PROPOSED SIDEWALK ELEVATION

| NO. | S/W ELEVATION | T/C ELEVATION |
|-----|---------------|---------------|
| A   | 671.89        | 672.21        |
| B   | 672.21        | 672.21        |
| C   | 672.31        | -             |
| D   | 672.31        | 672.31        |
| E   | 672.21        | 672.21        |
| F   | 671.95        | 672.27        |



- NOTES:
1. THE SIDEWALK CURB RAMPS SHALL CONFORM TO THE SIDEWALK CURB RAMP DETAILS ON SHEET NO. 68 AND ALSO CONFORM TO THE DETAILS SHOWN ON STANDARD 424001 AND STANDARD 424021.
  2. THE ELEVATIONS FOR THE SIDEWALK AND THE % SLOPES ARE FOR GENERAL LAYOUT AND INFORMATION ONLY. THE MAXIMUM HANDICAP RAMP SLOPES AND SIDEWALK CROSS SLOPES SHALL BE ACCORDING TO STANDARD 424001 AND STANDARD 424021. FIELD ADJUSTMENTS SHALL BE MADE AS NECESSARY FOR THE SIDEWALK AND RAMPS TO CONFORM TO THE STANDARD.

3. THE PROPOSED CURB SHALL BE CONSTRUCTED ACCORDING TO THE DETAILS SHOWN ON SHEET NO. 70. THE CURB THAT IS EQUAL TO OR LESS THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE SIDEWALK MONOLITHIC CURB DETAIL AND THE CURB HEIGHT THAT IS GREATER THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE PORTLAND CEMENT CONCRETE SIDEWALK CURB DETAIL.
4. THE PROPOSED SIDEWALK MONOLITHIC CURB ADJACENT TO THE EXISTING RETAINING WALL MAY BE ELIMINATED AS DIRECTED BY THE ENGINEER DEPENDENT ON THE CONDITION OF THE WALL AFTER EXCAVATION.

5. THE EXISTING DITCH SHALL BE REGRADED AS NECESSARY TO PROVIDE POSITIVE DRAINAGE FROM THE CLASS SI CONCRETE OUTLET TO THE EXISTING INLET.
6. THE PROPOSED GUTTER FLAG SHALL BE POURED TO DRAIN AWAY FROM THE CURB AT A MINIMUM SLOPE OF 1%. THE WATER SHALL DRAIN TO THE NORTHEAST ACROSS THE STREET RETURN.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

INTERSECTION DETAILS  
MARION STREET

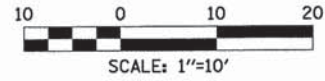
|                     |                           |        |              |           |
|---------------------|---------------------------|--------|--------------|-----------|
| F.A. RTE.           | SECTION                   | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS            | PIATT  | 76           | 58        |
| CONTRACT NO. 91516  |                           |        |              |           |
| FED. ROAD DIST. NO. | ILLINOIS/FED. AID PROJECT |        |              |           |

|                            |                |           |
|----------------------------|----------------|-----------|
| USER NAME = skm            | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63    | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8833' / in. | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016       | DATE - 01/2016 | REVISED - |

SCALE 1"=10' SHEET NO. 1 OF 2 SHEETS STA. TO STA.







**LEGEND**

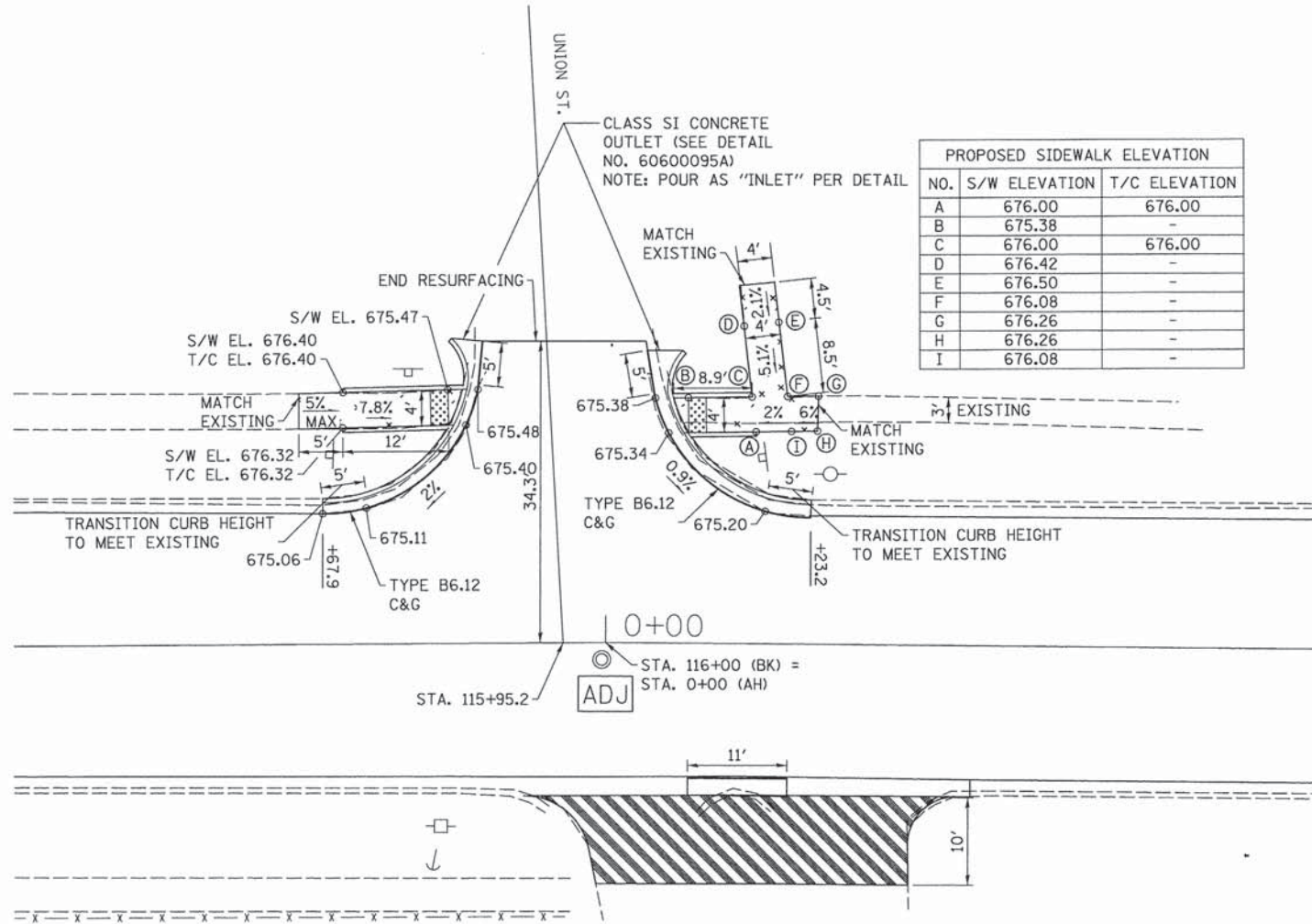
- PCC SIDEWALK, 6"
- PCC DRIVEWAY PAVEMENT, 6"
- DETECTABLE WARNINGS
- INCIDENTAL HMA RESURFACING

- DRAINAGE STRUCTURE TO BE ADJUSTED
- WATER VALVE BOX TO BE ADJUSTED

**NOTES:**

1. THE SIDEWALK CURB RAMPS SHALL CONFORM TO THE SIDEWALK CURB RAMP DETAILS ON SHEET NO. 68 AND ALSO CONFORM TO THE DETAILS SHOWN ON STANDARD 424001.
2. THE ELEVATIONS FOR THE SIDEWALK AND THE % SLOPES ARE FOR GENERAL LAYOUT AND INFORMATION ONLY. THE MAXIMUM HANDICAP RAMP SLOPES AND SIDEWALK CROSS SLOPES SHALL BE ACCORDING TO STANDARD 424001 AND STANDARD 424021. FIELD ADJUSTMENTS SHALL BE MADE AS NECESSARY FOR THE SIDEWALK AND RAMPS TO CONFORM TO THE STANDARD.

3. THE PROPOSED CURB SHALL BE CONSTRUCTED ACCORDING TO THE DETAILS SHOWN ON SHEET NO. 70. THE CURB THAT IS EQUAL TO OR LESS THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE SIDEWALK MONOLITHIC CURB DETAIL AND THE CURB HEIGHT THAT IS GREATER THAN 7" IN HEIGHT SHALL BE CONSTRUCTED ACCORDING TO THE PORTLAND CEMENT CONCRETE SIDEWALK CURB DETAIL.



PLOT DATE = 3/1/2016 11:34:40 PM  
 USER NAME = skm  
 ESCA PROJECT NO. 719.63  
 PLOT SCALE = 0.0033' / in.  
 DATE = 01/2016  
 DESIGNED - JMS  
 DRAWN - JLF  
 CHECKED - JMS  
 REVISED -  
 REVISED -  
 REVISED -  
 REVISED -



|                                  |                |           |
|----------------------------------|----------------|-----------|
| USER NAME = skm                  | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63          | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.0033' / in.       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 11:34:40 PM | DATE = 01/2016 | REVISED - |

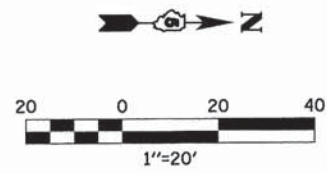
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSECTION DETAILS  
MARION STREET**

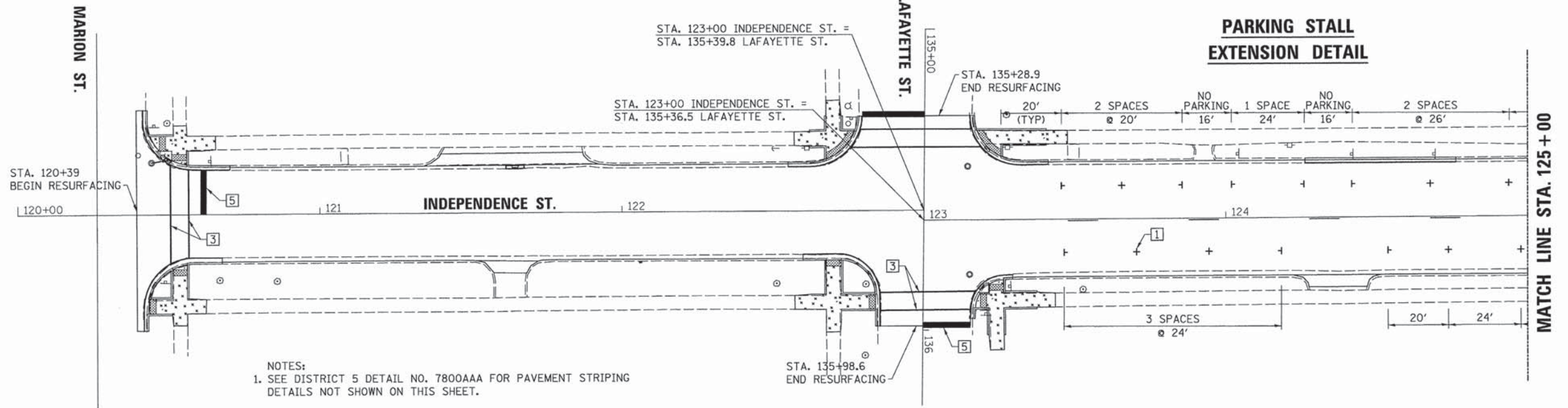
SCALE 1"=10' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

|                     |                |        |                           |           |
|---------------------|----------------|--------|---------------------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT  | 76                        | 59        |
| FED. ROAD DIST. NO. |                |        | ILLINOIS FED. AID PROJECT |           |
| CONTRACT NO. 91516  |                |        |                           |           |

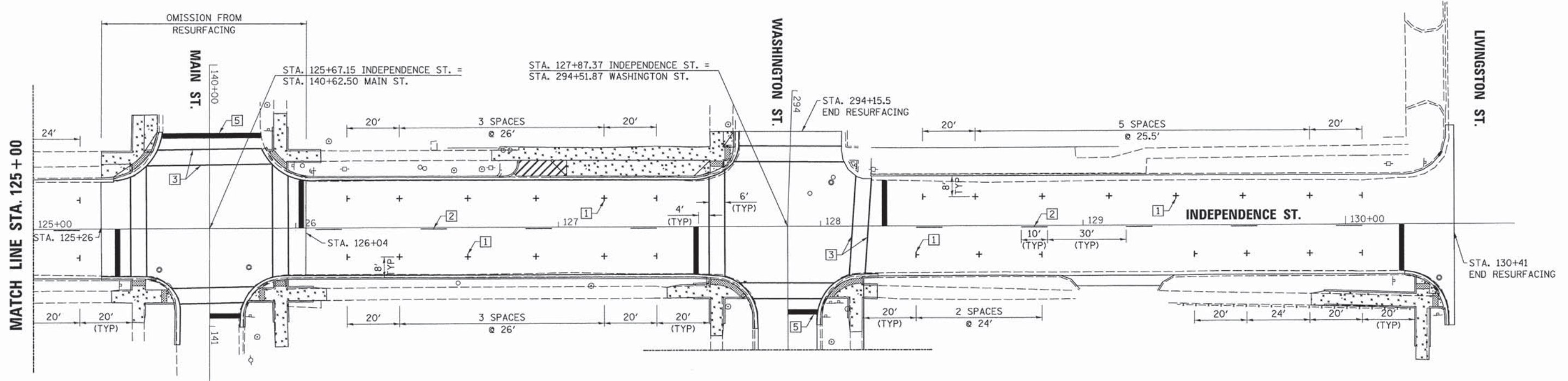




| PAVEMENT MARKING LEGEND |                  |
|-------------------------|------------------|
| 1                       | 4" WHITE         |
| 2                       | 4" YELLOW        |
| 3                       | 6" WHITE         |
| 4                       | 12" YELLOW       |
| 5                       | 24" WHITE        |
| 6                       | 4" DOUBLE YELLOW |



- NOTES:
- SEE DISTRICT 5 DETAIL NO. 7800AAA FOR PAVEMENT STRIPING DETAILS NOT SHOWN ON THIS SHEET.
  - SEE SHEET NO. 34 FOR PAVEMENT MARKING QUANTITIES.
  - THE PARKING STALLS SHALL BE 8 FEET WIDE FROM THE FACE OF CURB.



PRINT DRIVER = JLD-LS-08/16/16  
 MODEL NAME = 000001  
 FILE NAME = 000001.dwg



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 28.0000' / 1"      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:34:08 PM | DATE - 01/2016 | REVISED - |

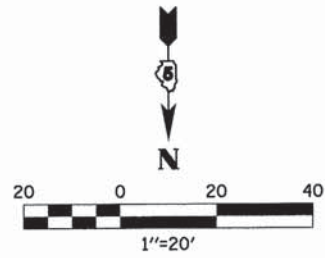
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING  
INDEPENDENCE STREET**

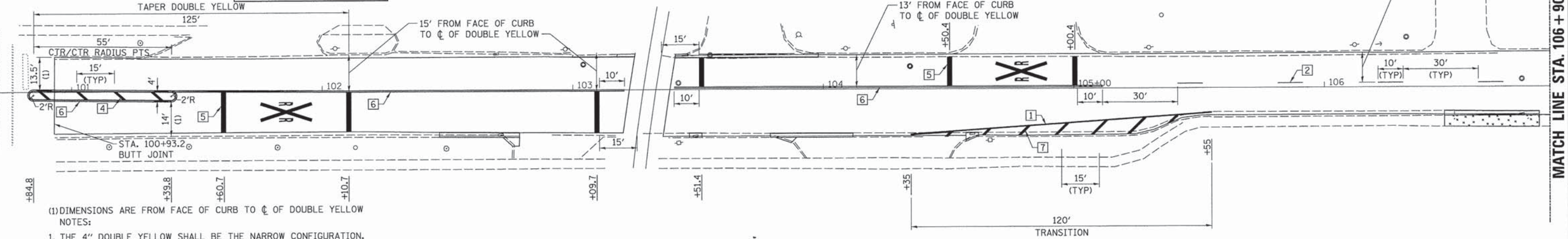
SCALE: 1"=20' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 60        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |

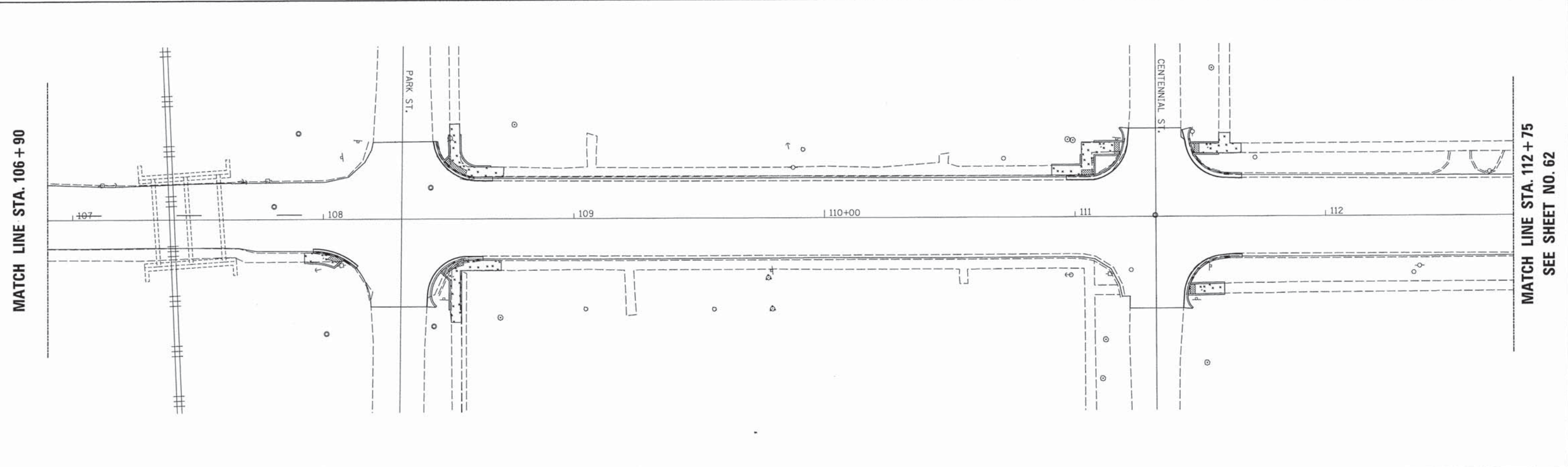




| PAVEMENT MARKING LEGEND |                  |
|-------------------------|------------------|
| 1                       | 4" WHITE         |
| 2                       | 4" YELLOW        |
| 3                       | 6" WHITE         |
| 4                       | 12" YELLOW       |
| 5                       | 24" WHITE        |
| 6                       | 4" DOUBLE YELLOW |
| 7                       | 12" WHITE        |



- (1) DIMENSIONS ARE FROM FACE OF CURB TO  $\phi$  OF DOUBLE YELLOW
- NOTES:
1. THE 4" DOUBLE YELLOW SHALL BE THE NARROW CONFIGURATION.
  2. SEE SHEET NO. 33 FOR PAVEMENT MARKING QUANTITIES.
  3. THE EXISTING PAINTED ISLAND SHALL BE REMOVED FROM STA. 100+81.5 TO STA. 100+93.2.
  4. SEE DISTRICT 5 DETAIL NO. 7800AAA FOR PAVEMENT STRIPING DETAILS NOT SHOWN ON THIS SHEET.



PRINT DRIVER = J.P. E.D. 01/21/16  
 PEN NAME = J.P. E.D. 01/21/16  
 FILE NAME = 1400023-00-RS-PIATT-STRIPING.dwg



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8833' / 1" =     | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:13:52 PM | DATE - 01/2016 | REVISED - |

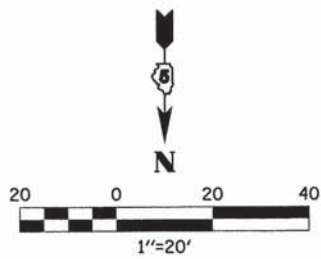
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING**  
**MARION STREET**

SCALE 1"=20' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

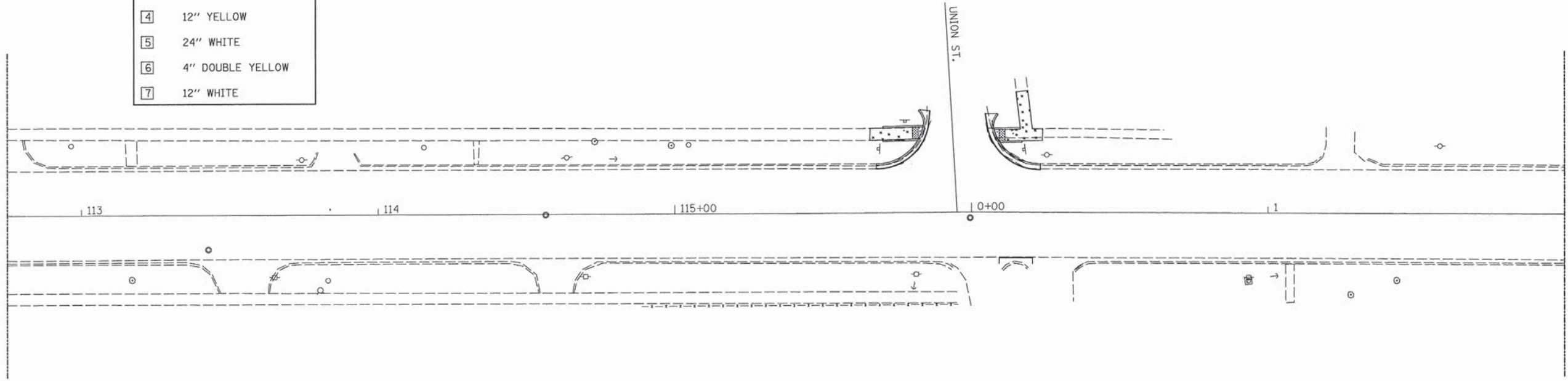
|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                       | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 61        |
| CONTRACT NO. 91516                              |                |        |              |           |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        |              |           |





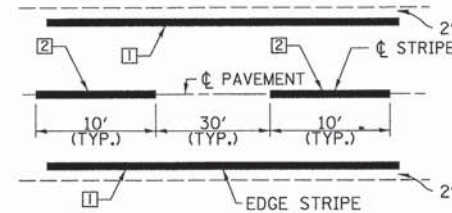
| PAVEMENT MARKING LEGEND |                  |
|-------------------------|------------------|
| 1                       | 4" WHITE         |
| 2                       | 4" YELLOW        |
| 3                       | 6" WHITE         |
| 4                       | 12" YELLOW       |
| 5                       | 24" WHITE        |
| 6                       | 4" DOUBLE YELLOW |
| 7                       | 12" WHITE        |

MATCH LINE STA. 112 + 75  
SEE SHEET NO. 61



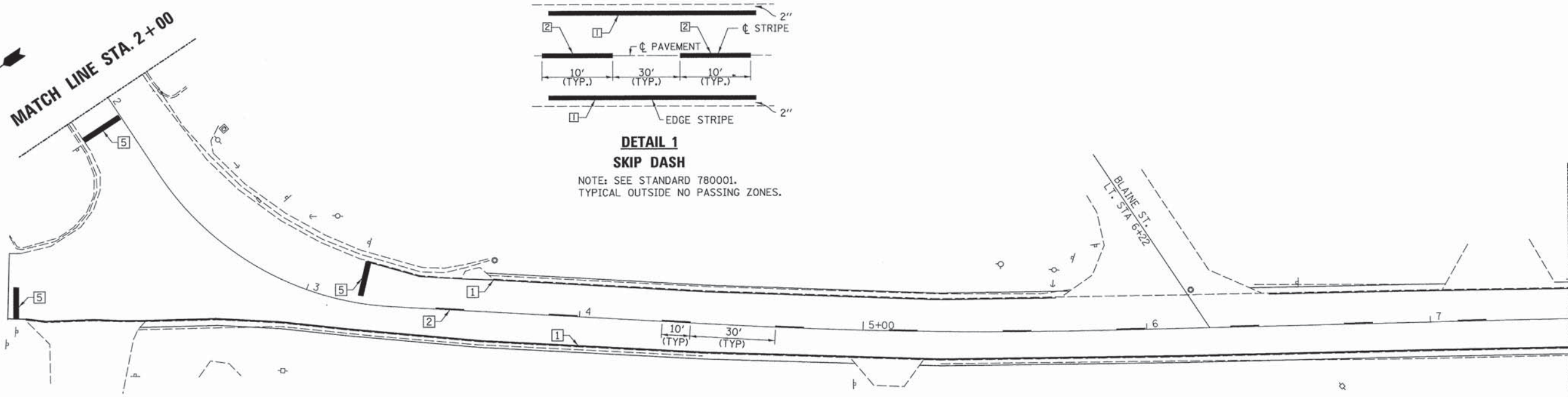
MATCH LINE STA. 2 + 00

MATCH LINE STA. 2 + 00



**DETAIL 1**  
**SKIP DASH**

NOTE: SEE STANDARD 780001.  
TYPICAL OUTSIDE NO PASSING ZONES.



MATCH LINE STA. 7 + 50  
SEE SHEET NO. 63

**NOTES:**

1. SEE SHEET NO. 33 FOR PAVEMENT MARKING QUANTITIES.
2. SEE DISTRICT 5 DETAIL NO. 7800AAA FOR PAVEMENT STRIPING DETAILS NOT SHOWN ON THIS SHEET.

PRINT DESIGNER = L.D.S. E. 04/16/16  
 PLOT DATE = 3/1/2016 1:14:10 PM  
 FILE NAME = 625071783-011.psd



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0033' / 1"       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:14:10 PM | DATE - 01/2016 | REVISED - |

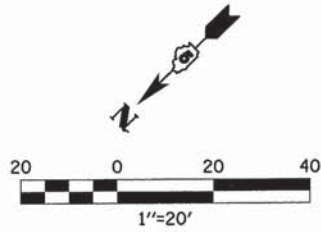
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING**  
**MARION STREET AND ALLERTON ROAD**

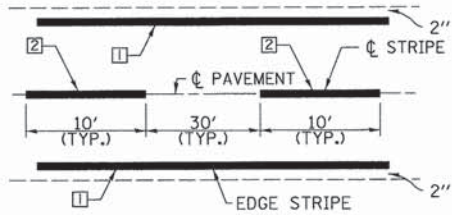
SCALE 1"=20' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

| F.A. RTE.                                       | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| VAR   | 14-00023-00-RS | PIATT  | 76           | 62        |
| CONTRACT NO. 91516                              |                |        |              |           |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        |              |           |





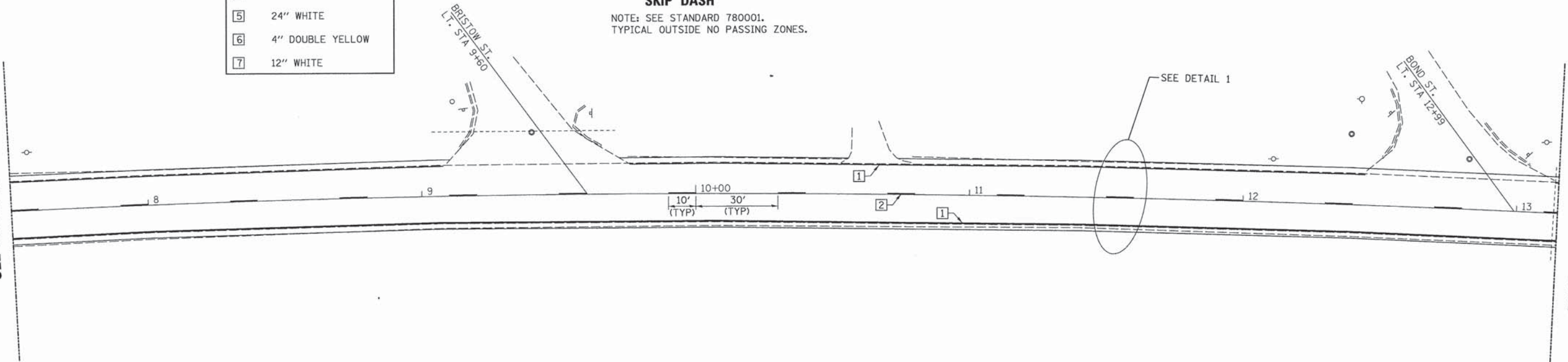
| PAVEMENT MARKING LEGEND |                  |
|-------------------------|------------------|
| 1                       | 4" WHITE         |
| 2                       | 4" YELLOW        |
| 3                       | 6" WHITE         |
| 4                       | 12" YELLOW       |
| 5                       | 24" WHITE        |
| 6                       | 4" DOUBLE YELLOW |
| 7                       | 12" WHITE        |



**DETAIL 1  
SKIP DASH**

NOTE: SEE STANDARD 780001.  
TYPICAL OUTSIDE NO PASSING ZONES.

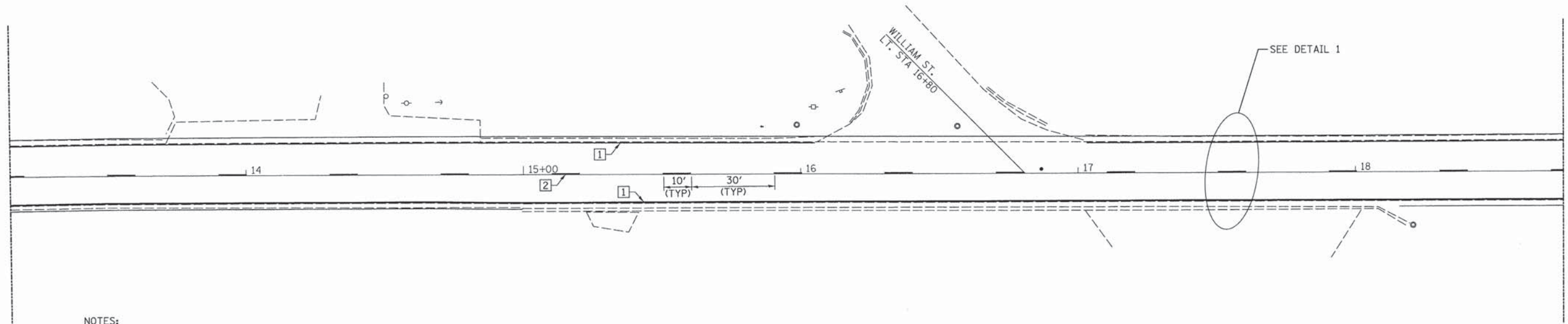
MATCH LINE STA. 7+50  
SEE SHEET NO. 62



MATCH LINE STA. 13+15



MATCH LINE STA. 13+15



MATCH LINE STA. 18+75  
SEE SHEET NO. 64

**NOTES:**

- SEE SHEET NO. 33 FOR PAVEMENT MARKING QUANTITIES.
- SEE DISTRICT 5 DETAIL NO. 7800AAA FOR PAVEMENT STRIPING DETAILS NOT SHOWN ON THIS SHEET.

PRINT DRIVER: J:\05-00\0501\0501.dwg  
 PLOT DATE: 3/11/2016 1:14:19 PM  
 PLOT SCALE: 0.0033 1/ in.  
 USER: skm



|                                  |                |           |
|----------------------------------|----------------|-----------|
| USER NAME = skm                  | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63          | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0033 1/ in.       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/11/2016 1:14:19 PM | DATE - 01/2016 | REVISED - |

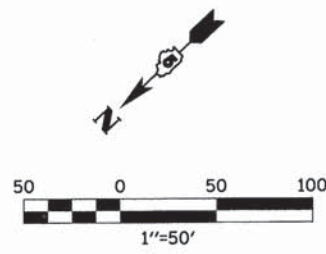
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING  
ALLERTON ROAD**

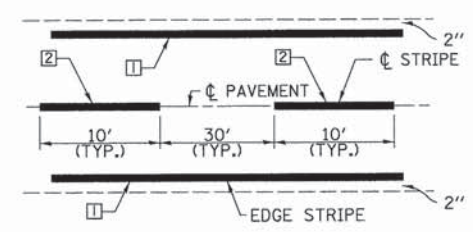
SCALE 1"=20' SHEET NO. 1 OF 3 SHEETS STA. TO STA.

|                     |                           |        |              |           |
|---------------------|---------------------------|--------|--------------|-----------|
| F.A. RTE.           | SECTION                   | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS            | PIATT  | 76           | 63        |
| CONTRACT NO. 91516  |                           |        |              |           |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT |        |              |           |

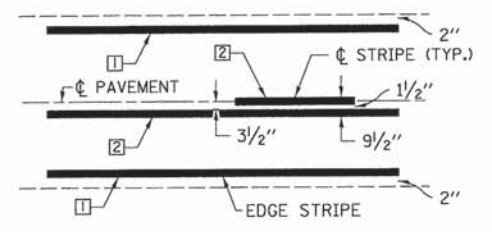




| PAVEMENT MARKING LEGEND |                  |
|-------------------------|------------------|
| ①                       | 4" WHITE         |
| ②                       | 4" YELLOW        |
| ③                       | 6" WHITE         |
| ④                       | 12" YELLOW       |
| ⑤                       | 24" WHITE        |
| ⑥                       | 4" DOUBLE YELLOW |

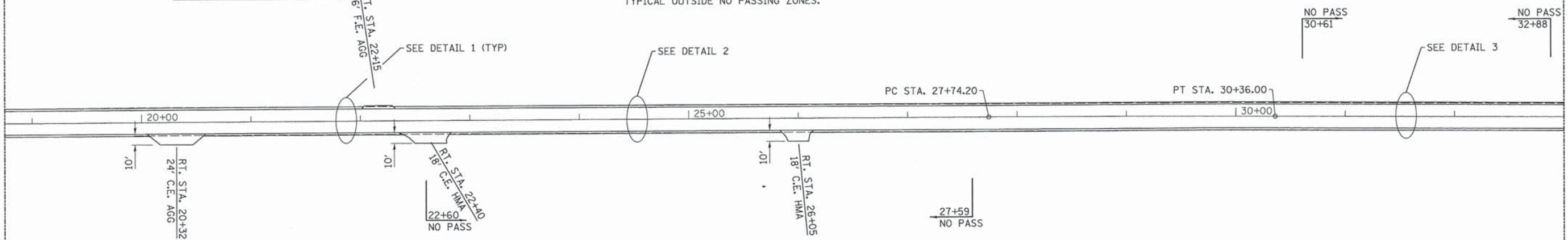


**DETAIL 1  
SKIP DASH**  
NOTE: SEE STANDARD 780001.  
TYPICAL OUTSIDE NO PASSING ZONES.

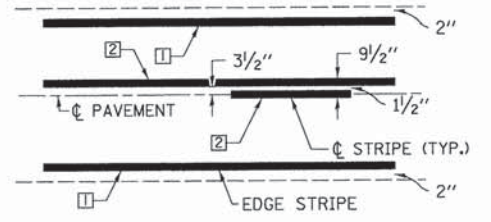


**DETAIL 2  
SINGLE NO PASSING**  
NOTE: SEE STANDARD 780001.

MATCH LINE STA. 18 + 75  
SEE SHEET NO. 63

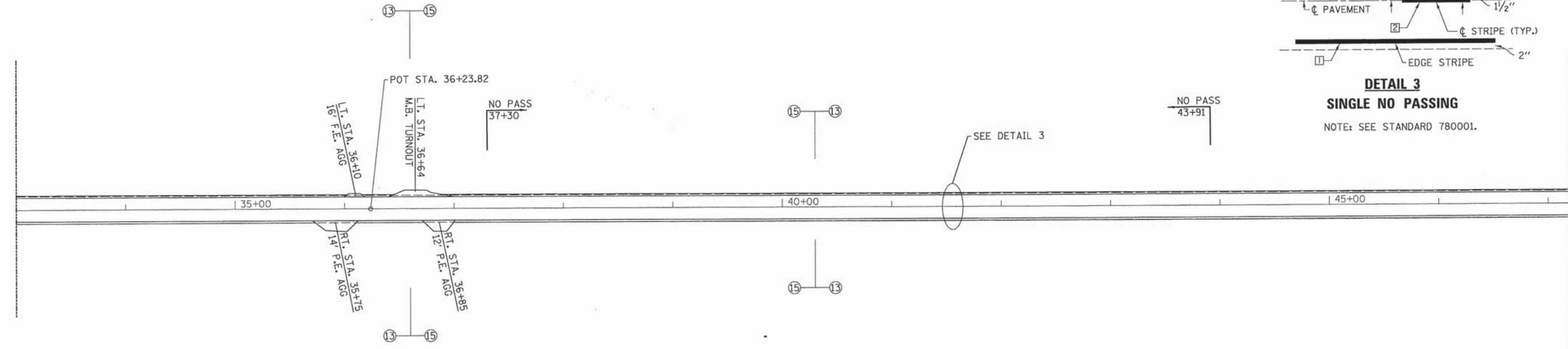


MATCH LINE STA. 33 + 00



**DETAIL 3  
SINGLE NO PASSING**  
NOTE: SEE STANDARD 780001.

MATCH LINE STA. 33 + 00



MATCH LINE STA. 47 + 20  
SEE SHEET NO. 65

ESCA CONSULTANTS, INC.  
 1400 N. WILSON AVENUE  
 SUITE 200  
 DEERFIELD, ILLINOIS 60015  
 TEL: 847.440.0000  
 FAX: 847.440.0001  
 WWW.ESCA-CONSULTANTS.COM



|                                   |                |           |
|-----------------------------------|----------------|-----------|
| USER NAME = skm                   | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63           | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0033' / 1" = 1/300 | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:15:01 PM   | DATE - 01/2016 | REVISED - |

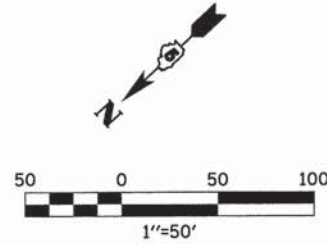
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING  
ALLERTON ROAD

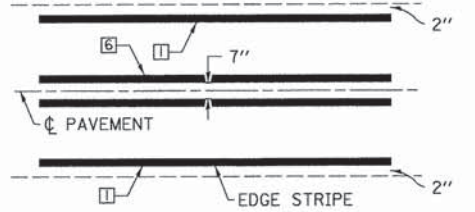
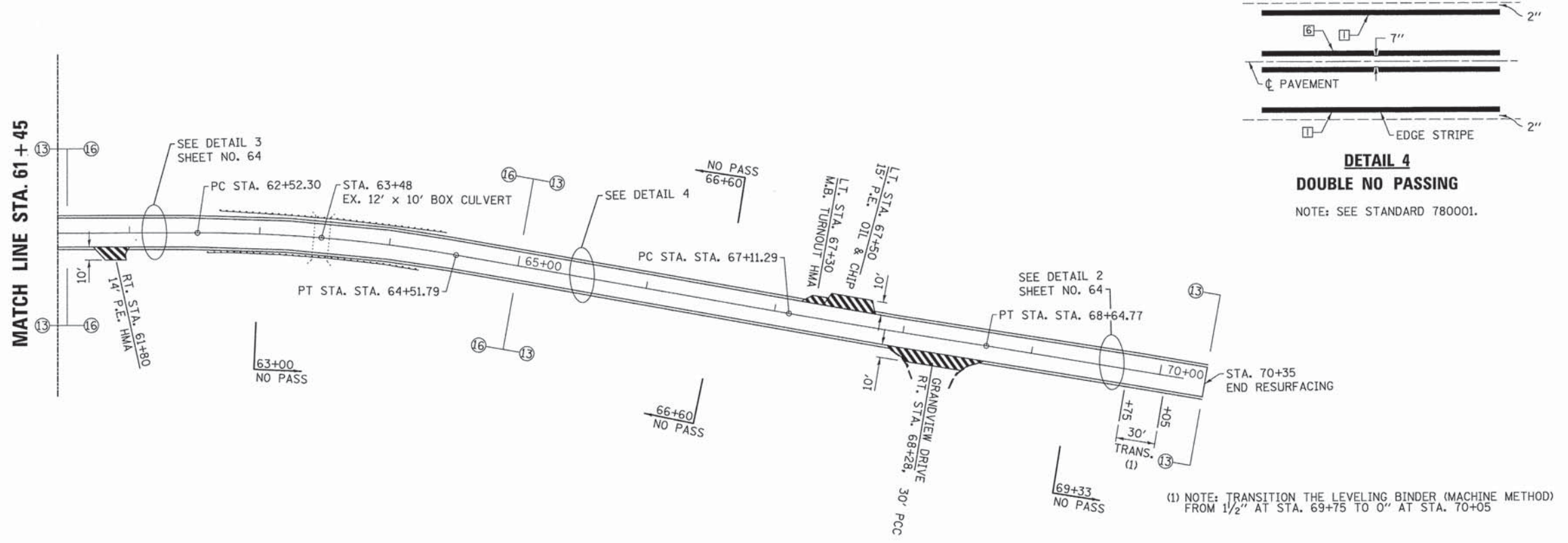
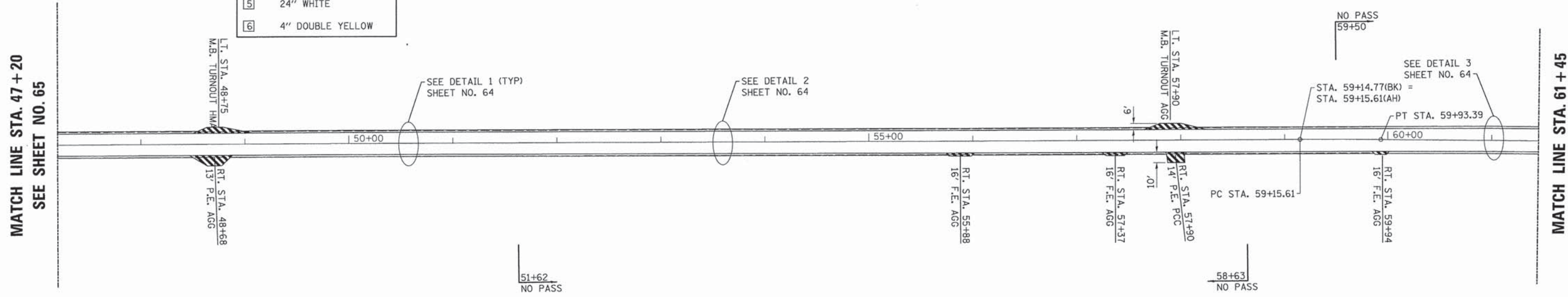
SCALE 1"=50' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

|                     |                |        |                           |           |
|---------------------|----------------|--------|---------------------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT  | 76                        | 64        |
| FED. ROAD DIST. NO. |                |        | ILLINOIS FED. AID PROJECT |           |
| CONTRACT NO. 91516  |                |        |                           |           |





| PAVEMENT MARKING LEGEND |                  |
|-------------------------|------------------|
| 1                       | 4" WHITE         |
| 2                       | 4" YELLOW        |
| 3                       | 6" WHITE         |
| 4                       | 12" YELLOW       |
| 5                       | 24" WHITE        |
| 6                       | 4" DOUBLE YELLOW |



**DETAIL 4**  
**DOUBLE NO PASSING**  
 NOTE: SEE STANDARD 780001.

PLOT DATE: 3/1/2016 11:58:09 PM  
 USER: skm  
 PROJECT: 719.63  
 SCALE: 1/4" = 100'  
 DATE: 01/2016  
 DESIGNED: JMS  
 DRAWN: SKM  
 CHECKED: JMS  
 REVISIONS:  
 1. 01/2016  
 2. 01/2016  
 3. 01/2016  
 4. 01/2016



|                                  |                |           |
|----------------------------------|----------------|-----------|
| USER NAME = skm                  | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63          | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 1/4" = 100'         | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 11:58:09 PM | DATE - 01/2016 | REVISED - |

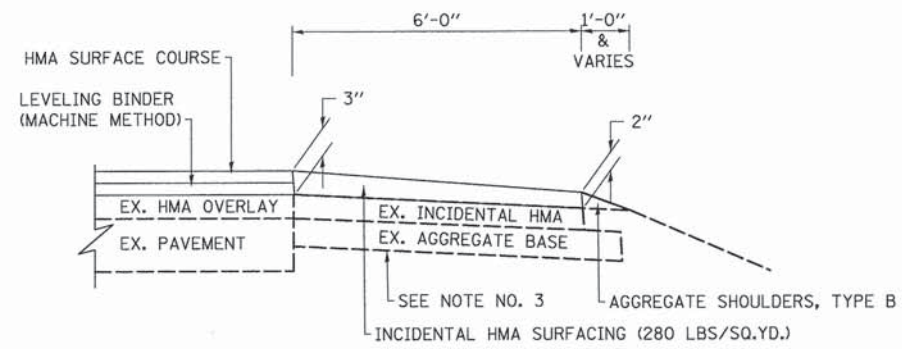
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING**  
**ALLERTON ROAD**

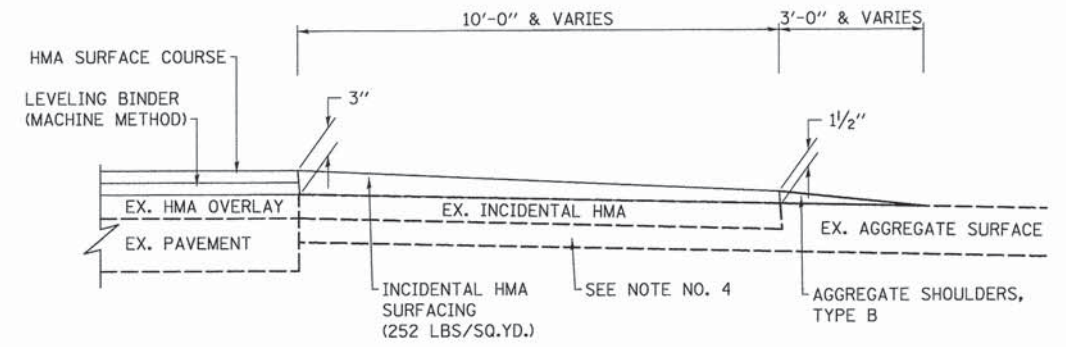
SCALE 1"=50' SHEET NO. 3 OF 3 SHEETS STA. TO STA.

|                     |                |                           |              |           |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76           | 65        |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |              |           |
| CONTRACT NO. 91516  |                |                           |              |           |

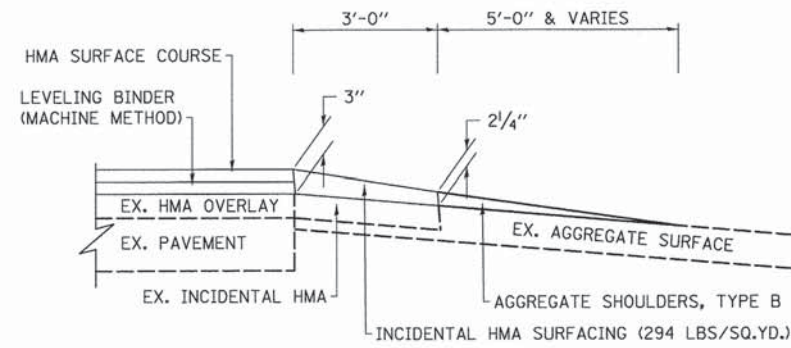




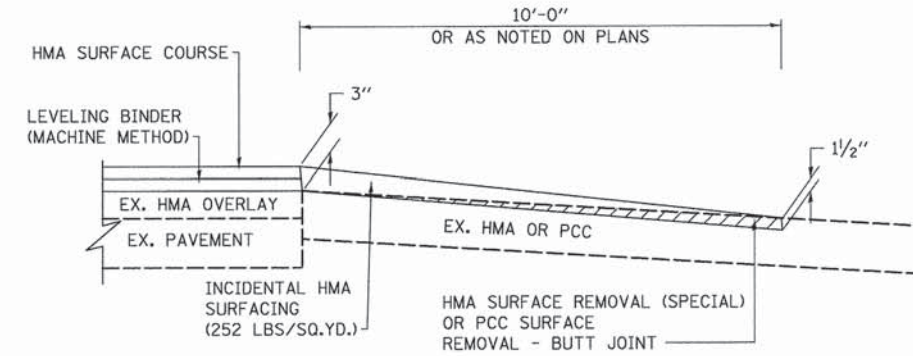
**HMA RESURFACING FOR MAILBOX TURNOUTS (RURAL)**



**HMA RESURFACING FOR PRIVATE AND COMMERCIAL ENTRANCES (RURAL)  
AGGREGATE ENTRANCE WITH EXISTING HMA APRON**



**HMA RESURFACING FOR FIELD ENTRANCES (RURAL)  
AGGREGATE ENTRANCE WITH EXISTING HMA APRON**



**HMA RESURFACING FOR PRIVATE AND COMMERCIAL ENTRANCES (RURAL)  
EXISTING HMA OR PCC ENTRANCE**

**NOTES:**

1. THE PROPOSED INCIDENTAL HMA SURFACING SHALL BE PLACED TO THE SAME DIMENSIONS AS THE EXISTING HMA APRON.
2. AGGREGATE SHOULDERS, TYPE B SHALL BE USED ALONG THE EDGES OF THE PROPOSED INCIDENTAL HMA SURFACING TO ELIMINATE THE DIFFERENTIAL BETWEEN THE PROPOSED HMA AND THE EXISTING SHOULDER.
3. THE MAILBOX TURNOUT AT LT. STA. 57+90 DOES NOT HAVE AN EXISTING INCIDENTAL HMA SURFACE. AGGREGATE BASE COURSE, TYPE B, 6" THICKNESS SHALL BE CONSTRUCTED 12" WIDER THAN THE SURFACE DIMENSIONS SHOWN ON THE PLANS. THE INCIDENTAL HMA SURFACING THICKNESS SHALL BE 2" MINIMUM.
4. THE EXISTING MAILBOX FOR PROPOSED MAILBOX TURNOUT AT LT. STA. 57+90 SHALL BE RELOCATED AS NEEDED. THE MAILBOX SHALL BE MOUNTED SUCH THAT THE FACE OF THE MAILBOX IS 6" TO 12" AND THE POST A MINIMUM OF 24" FROM THE EDGE OF THE TURNOUT.
5. THE AGGREGATE COMMERCIAL ENTRANCE AT RT. STA. 20+32 DOES NOT HAVE AN EXISTING INCIDENTAL HMA APRON. AGGREGATE BASE COURSE, TYPE B, 6" THICKNESS SHALL BE CONSTRUCTED 12" WIDER THAN THE SURFACE DIMENSIONS SHOWN ON THE PLANS. THE INCIDENTAL HMA SURFACING THICKNESS SHALL BE 2" MINIMUM.

PENNY ANDERSON - 3101 W. KENTON AVENUE  
 PLOTTING - 11/15/2016 10:54 AM  
 FILE NAME - 140023-00-RS-01.DWG



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.0033' / 1in.     | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:34:21 PM | DATE - 01/2016 | REVISED - |

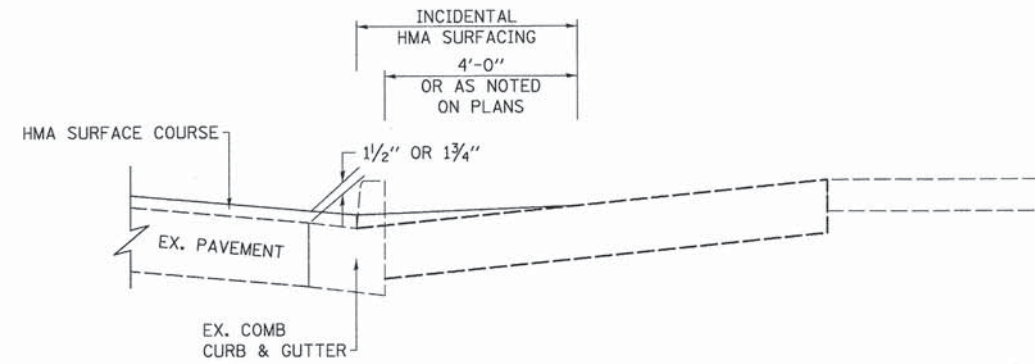
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INCIDENTAL HMA SURFACING DETAILS**

SCALE SHEET NO. 1 OF 2 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 66        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |

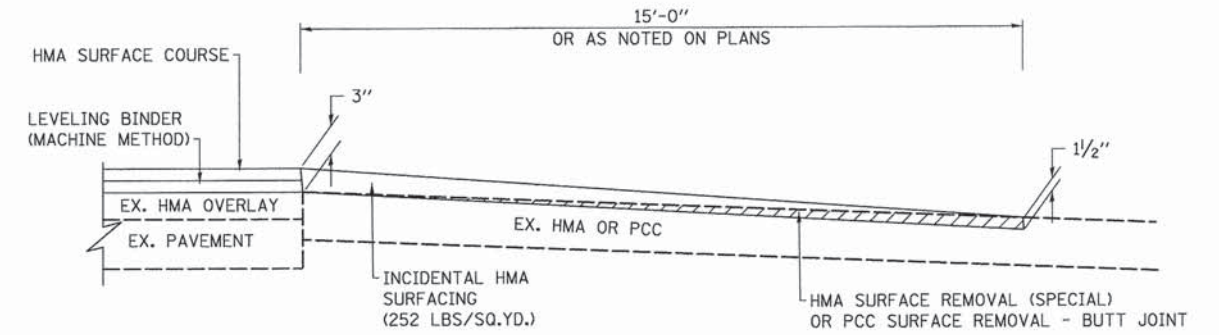




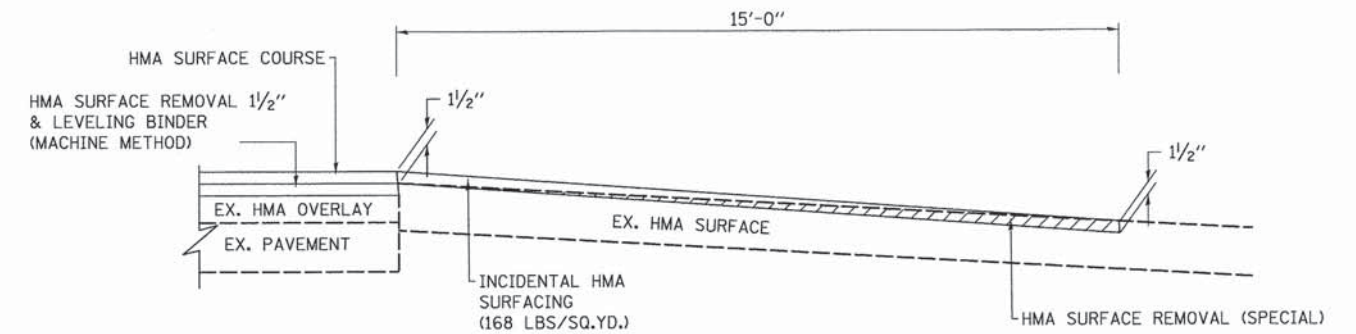
**HMA RESURFACING FOR ENTRANCES (URBAN)**  
**ENTRANCES ADJACENT TO EXISTING GUTTER FLAGS**  
**TO BE OVERLAID WITH HMA SURFACE COURSE**

**NOTES:**

1. THE PROPOSED HMA SURFACE COURSE SHALL BE PLACED THROUGH THE ENTRANCE TO THE FACE OF CURB AND COMPACTED TO MAINTAIN THE FLOWLINE THROUGH THE ENTRANCE. THE INCIDENTAL HMA SURFACING SHALL BE LAID AS A SEPARATE OPERATION ON THE EXISTING ENTRANCE AFTER THE HMA SURFACE COURSE HAS BEEN PLACED AND COMPACTED.
2. THE ENGINEER WILL CHECK THE ENTRANCES AFTER PLACEMENT OF THE HMA SURFACE COURSE TO DETERMINE THE FINAL WIDTH OF PLACEMENT NEEDED FOR THE INCIDENTAL HMA SURFACING TO PROVIDE POSITIVE DRAINAGE.



**HMA RESURFACING FOR SIDEROADS (RURAL)**  
**EXISTING HMA OR PCC SIDEROAD**



**HMA RESURFACING FOR SIDEROAD LT. STA. 16+80 (WILLIAM STREET)**

**NOTES:**

1. AGGREGATE SHOULDERS, TYPE B SHALL BE USED ALONG THE EDGES OF THE PROPOSED INCIDENTAL HMA SURFACING TO ELIMINATE THE DIFFERENTIAL BETWEEN THE PROPOSED HMA AND THE EXISTING SHOULDER.

P:\14-00023-00-RS\14-00023-00-RS-111-111.dwg  
 PLOT DATE = 3/1/2016 1:34:34 PM



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - SKM    | REVISED - |
| PLOT SCALE = 0.8033' / 1in.     | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:34:34 PM | DATE - 01/2016 | REVISED - |

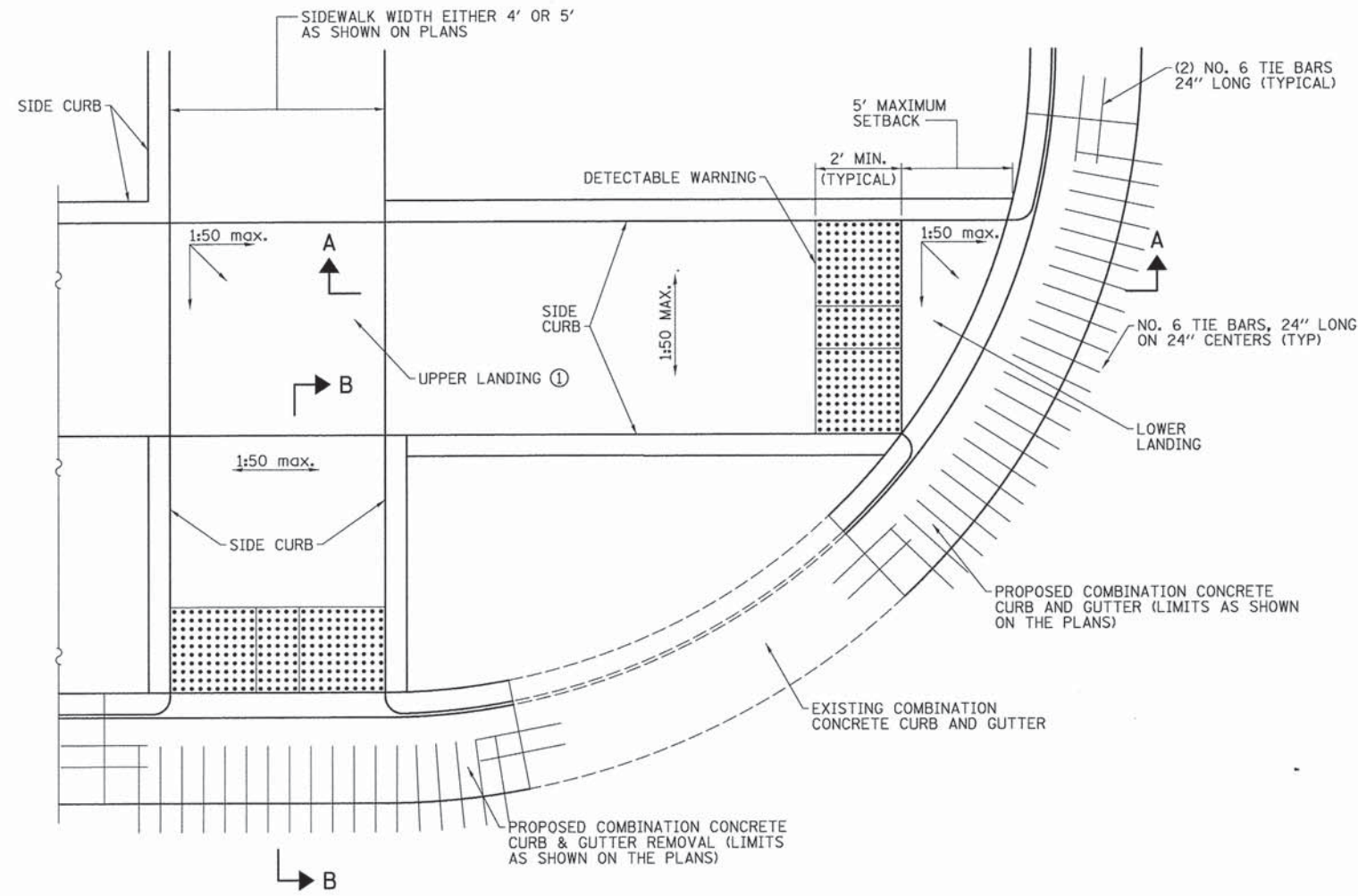
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

INCIDENTAL HMA SURFACING DETAILS

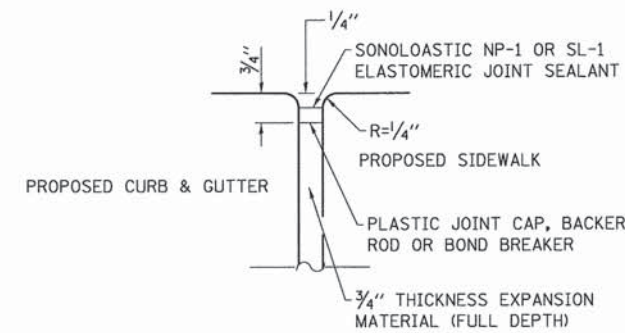
SCALE SHEET NO. 2 OF 2 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 67        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |

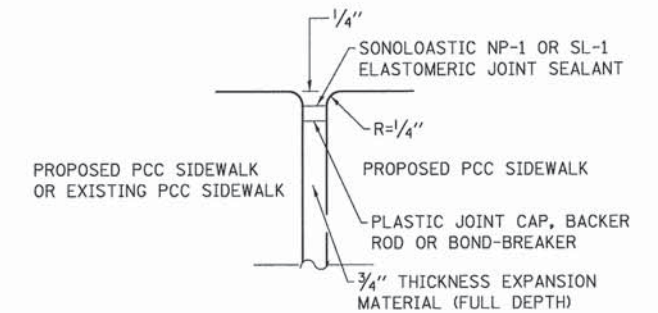




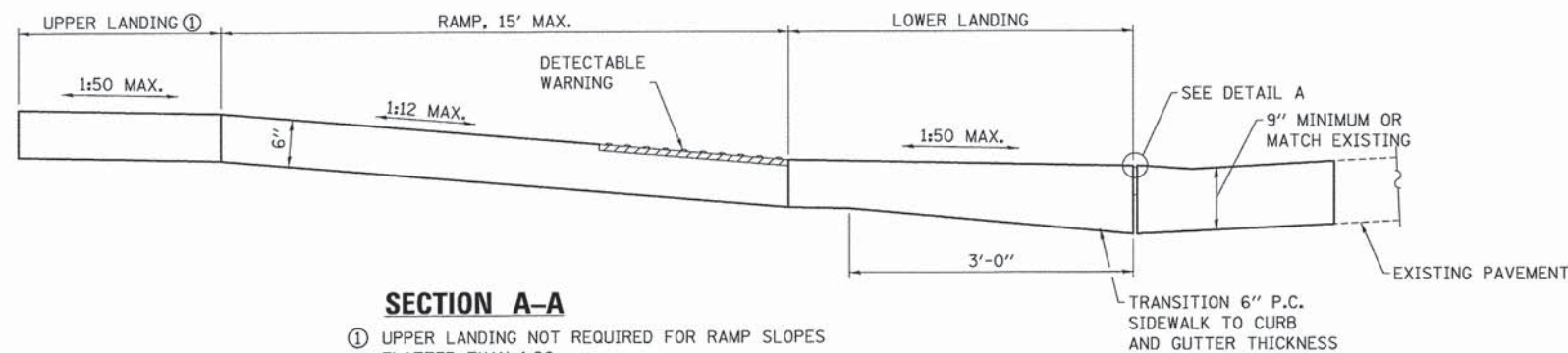
**SIDEWALK CURB RAMP DETAIL**



**DETAIL A**

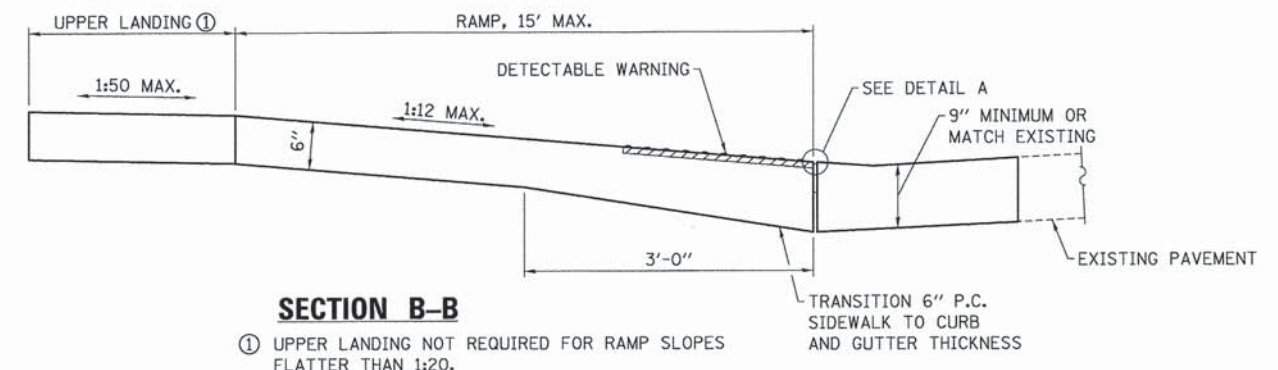


**TRANSVERSE EXPANSION JOINT DETAIL**



**SECTION A-A**

① UPPER LANDING NOT REQUIRED FOR RAMP SLOPES FLATTER THAN 1:20.



**SECTION B-B**

① UPPER LANDING NOT REQUIRED FOR RAMP SLOPES FLATTER THAN 1:20.

**NOTES:**

1. THE SIDEWALK CURB RAMP SHALL CONFORM TO THESE DETAILS AND ALSO CONFORM TO THE DETAILS SHOWN ON STANDARD 424001.
2. SPOT ELEVATIONS AND % SLOPES MAY BE SHOWN ON THE PLAN DETAILS FOR INDIVIDUAL RAMP LOCATIONS. THE INFORMATION IS SHOWN FOR GENERAL LAYOUT ONLY. THE MAXIMUM HANDICAP RAMP SLOPES AND SIDEWALK CROSS SLOPES SHALL BE ACCORDING TO STANDARD 424001.
3. EXPANSION JOINTS SHALL BE PLACED AS SHOWN ON DETAIL A. ADDITIONAL EXPANSION JOINTS PER THE DETAIL FOR TRANSVERSE EXPANSION JOINT DETAIL SHALL BE PLACED ON TWO CORNERS OF THE UPPER LANDING AS DIRECTED BY THE ENGINEER.
4. THE NO. 6 TIE BARS SHALL BE INSTALLED IN ACCORDANCE WITH THE LONGITUDINAL CONSTRUCTION JOINT (TIE BAR GROUTED IN PLACE) DETAIL SHOWN ON STANDARD 420001, PAVEMENT JOINTS.
5. SEE STANDARD 606001 FOR DETAILS OF DEPRESSED CURB ADJACENT TO CURB RAMP ACCESSIBLE TO THE DISABLED.
6. SIDE CURBS FOR RAMP SHALL BE CONSTRUCTED AS SHOWN ON THE PLAN DETAILS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PORTLAND CEMENT CONCRETE SIDEWALK, 6". THE AREA MEASURED WILL BE THE TOP SURFACE AREA OF THE CURBS.

DRAWN BY: JLF  
 CHECKED BY: JMS  
 DATE: 02/2016  
 PLOT DATE: 3/1/2016 1:34:48 PM



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = eskm                | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8833' / 11"      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:34:48 PM | DATE - 02/2016 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**



**SIDEWALK CURB RAMP DETAILS**

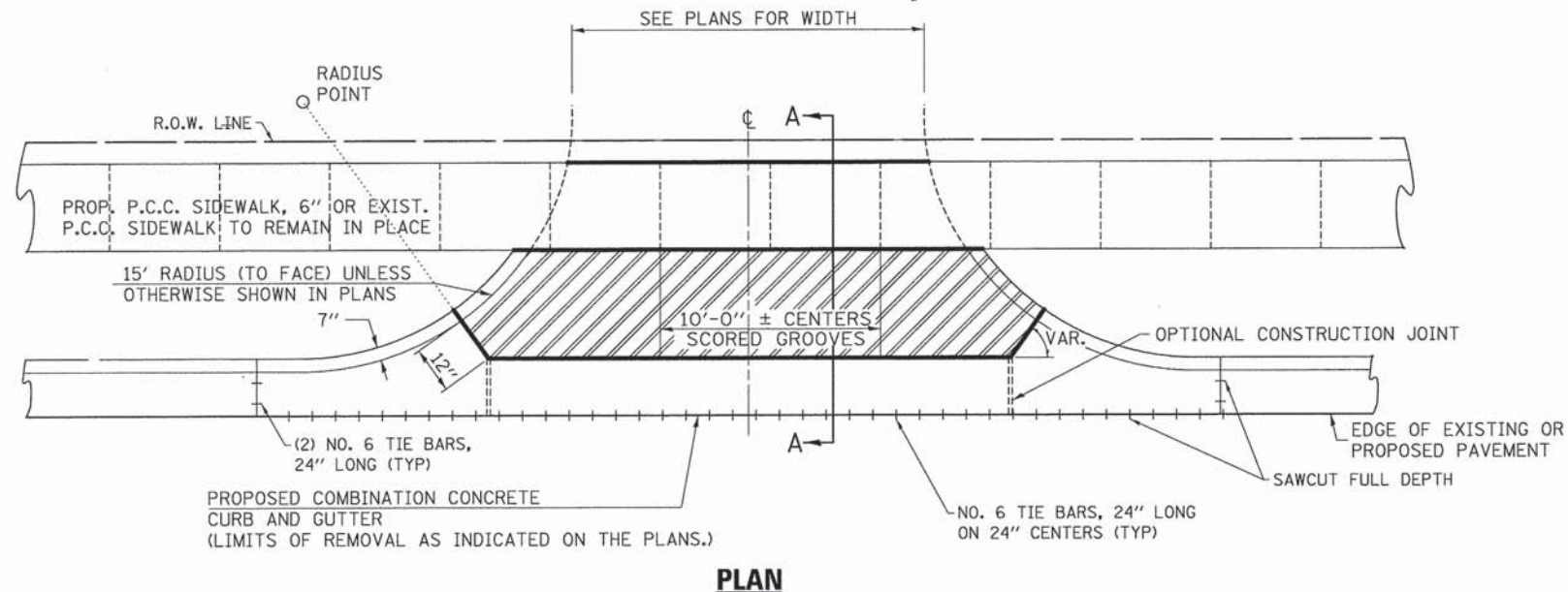
SCALE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 68        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |



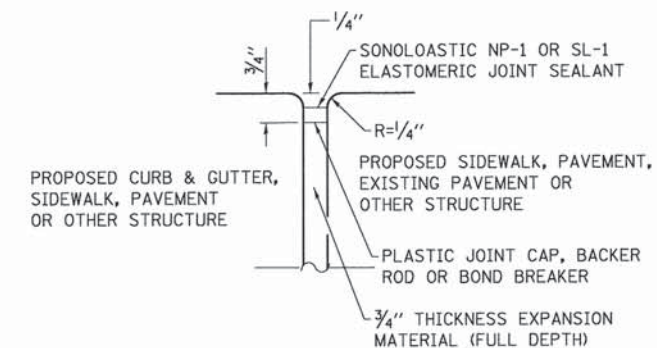
**LEGEND**

-  PROP. P.C. CONCRETE DRIVEWAY PAVEMENT, 8"
-  3/4" LONGITUDINAL EXPANSION JOINTS

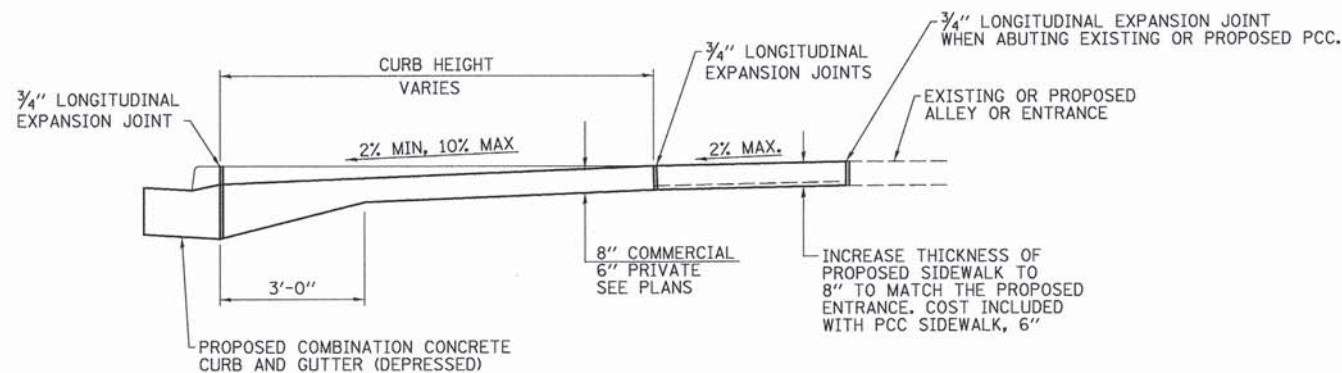


**NOTES:**

1. THE LONGITUDINAL EXPANSION JOINTS SHALL CONFORM TO SECTION 1051 OF THE STANDARD SPECIFICATIONS AND THE TECHNICAL SPECIFICATIONS. REFER TO THE LONGITUDINAL EXPANSION/ISOLATION JOINT DETAIL.
2. THE ENTRANCE GRADES WILL MEET THE MAXIMUM AND MINIMUM SLOPES AS DETAILED. THE ACTUAL GRADES WILL BE AS SHOWN ON THE PLAN DETAILS AND AS DIRECTED BY THE ENGINEER
3. THE ENTRANCE WIDTHS SHOWN ON THE PLANS SHALL BE INTERPRETED TO BE THE WIDTHS AT THE COMPLETED RADIUS WHICH MAY BE LOCATED BEHIND THE R.O.W. LINE.
4. THE P.C. CONCRETE DRIVEWAY PAVEMENT SHALL BE CONSTRUCTED WITH SCORED GROOVES AS SPECIFIED IN ARTICLE 424.06 OF THE STANDARD SPECIFICATIONS. THE SCORED GROOVES WILL BE PLACED AT APPROXIMATELY EVERY OTHER JOINT TO MATCH THE GROOVES EITHER IN THE EXISTING SIDEWALK OR THE PROPOSED SIDEWALK.
5. THE PROPOSED P.C. CONCRETE SIDEWALK SHALL BE CONSTRUCTED PRIOR TO CONSTRUCTING THE P.C. CONCRETE DRIVEWAY PAVEMENT.
6. THE COMBINATION CONCRETE CURB AND GUTTER SHALL BE CONSTRUCTED AS SHOWN AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LIN. FT. FOR COMBINATION CONCRETE CURB AND GUTTER, OF THE TYPE SPECIFIED.
7. THE VARIABLE HEIGHT CURB ADJACENT TO THE P.C. CONCRETE DRIVEWAY PAVEMENT SHALL BE CONSTRUCTED MONOLITHIC WITH THE P.C. CONCRETE DRIVEWAY. THE MONOLITHIC CURBS, THE SCORED GROOVES, THE LONGITUDINAL EXPANSION JOINTS, AND THE ADDITIONAL THICKNESS REQUIRED ADJACENT TO THE DEPRESSED COMBINATION CONCRETE CURB AND GUTTER WILL BE INCLUDED WITH AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT OF THE THICKNESS SPECIFIED.
8. THE NO. 6 TIE BARS SHALL BE INSTALLED IN ACCORDANCE WITH THE LONGITUDINAL CONSTRUCTION JOINT (TIE BAR GROUTED IN PLACE) DETAIL SHOWN ON STANDARD 420001, PAVEMENT JOINTS.



**LONGITUDINAL EXPANSION / ISOLATION JOINT DETAIL**



**SECTION A-A**

**PCC DRIVEWAY PAVEMENT DETAILS**  
NOT TO SCALE

PLOT DATE = 3/1/2016 1:35:00 PM  
 FILE NAME = 020507180-141-10001.dwg  
 PEN COLOR = 100  
 PEN WIDTH = 0.002  
 PLOT SCALE = 0.8933' / 1"



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.61         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 0.8933' / 1"       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:35:00 PM | DATE - 02/2016 | REVISED - |

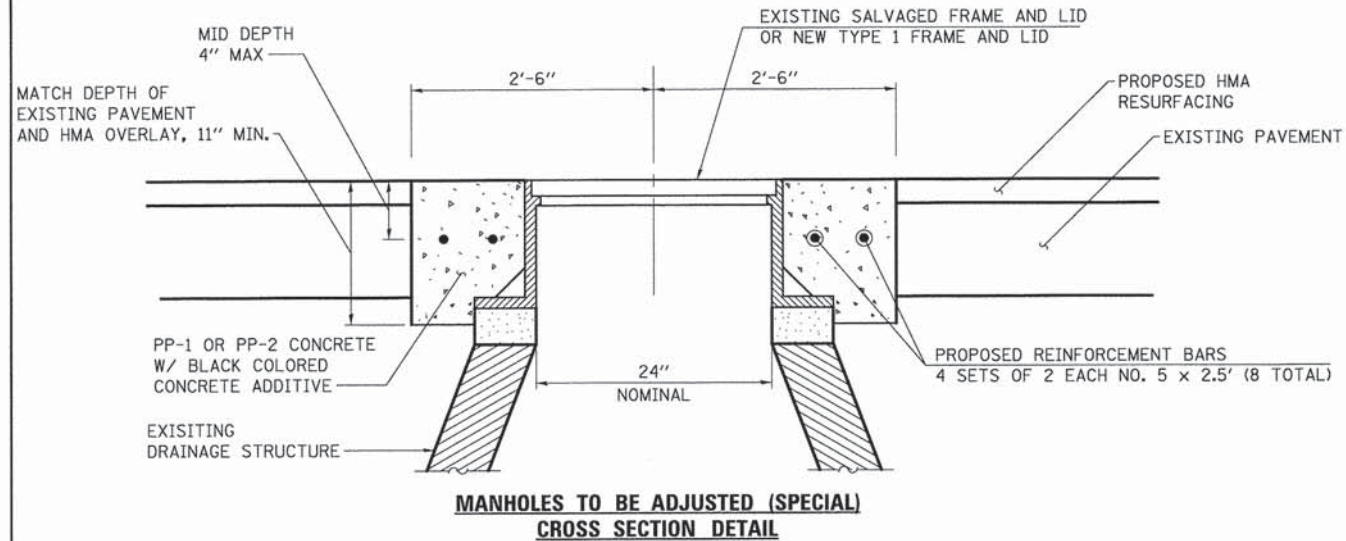
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**PCC DRIVEWAY PAVEMENT DETAILS**

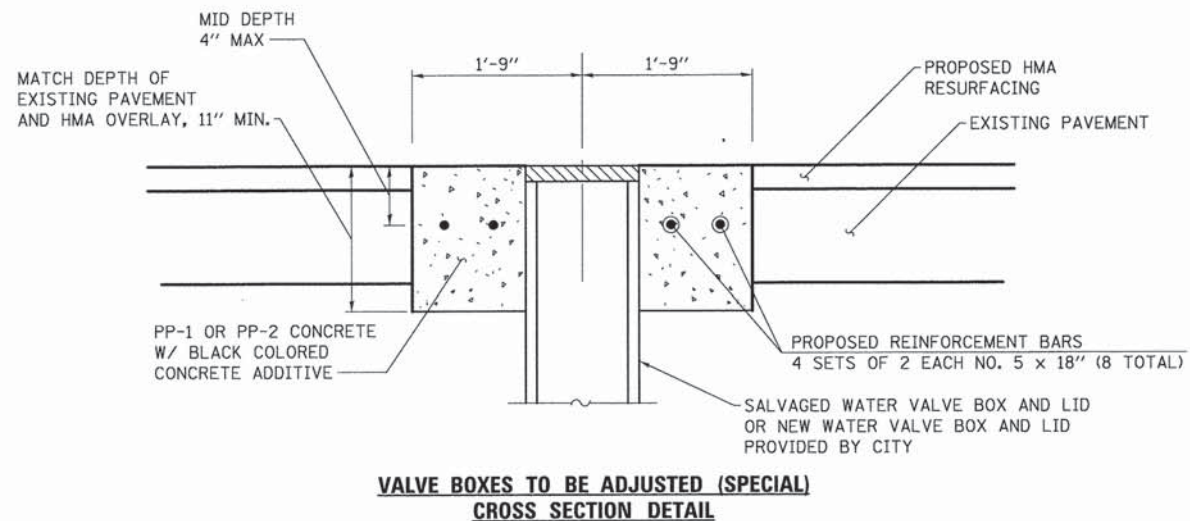
SCALE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                       | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 69        |
| CONTRACT NO. 91516                              |                |        |              |           |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |                |        |              |           |

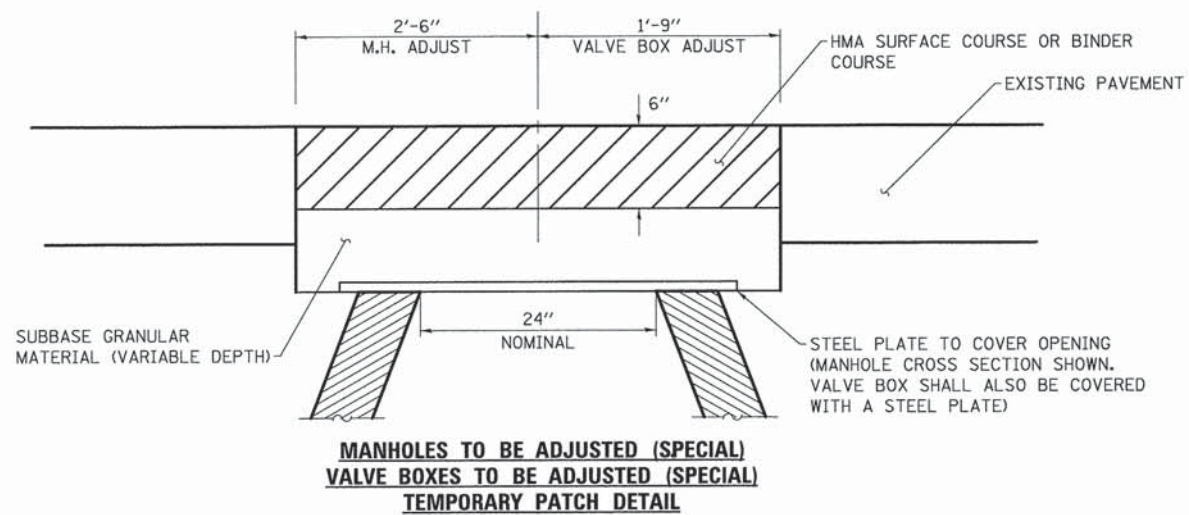




**MANHOLES TO BE ADJUSTED (SPECIAL)  
CROSS SECTION DETAIL**



**VALVE BOXES TO BE ADJUSTED (SPECIAL)  
CROSS SECTION DETAIL**

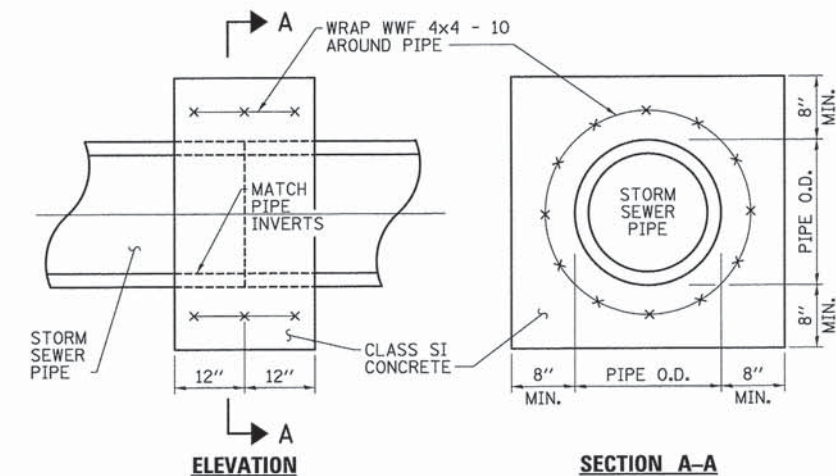


**MANHOLES TO BE ADJUSTED (SPECIAL)  
VALVE BOXES TO BE ADJUSTED (SPECIAL)  
TEMPORARY PATCH DETAIL**

**NOTES:**

1. PRIOR TO HMA SURFACE REMOVAL THE PAVEMENT SHALL BE SAWED FULL DEPTH AND THE FRAME AND LID (OR VALVE BOX) AND SURROUNDING PAVEMENT SHALL BE REMOVED. THE FRAME AND LID (OR VALVE BOX) SHALL BE SALVAGED OR REPLACED AS INDICATED ON THE PLANS.
2. A STEEL PLATE SHALL BE PLACED AND A TEMPORARY PATCH CONSTRUCTED AS DETAILED.
3. AFTER THE HMA SURFACE REMOVAL AND THE PROPOSED HMA RESURFACING ARE COMPLETE THE TEMPORARY PATCHES SHALL BE REMOVED AND THE MANHOLES AND VALVE BOXES SHALL BE ADJUSTED TO GRADE.
4. THE REINFORCEMENT BARS SHALL BE SPACED UNIFORMLY AROUND THE FRAME AND LID OR VALVE BOX. SEE STANDARD 420101-05 FOR TYPICAL PLACEMENT OF REINFORCEMENT IN PAVEMENT BLOCK-OUTS.
5. REFER TO THE SPECIAL PROVISIONS FOR ADDITIONAL REQUIREMENTS AND THE BASIS OF PAYMENT.

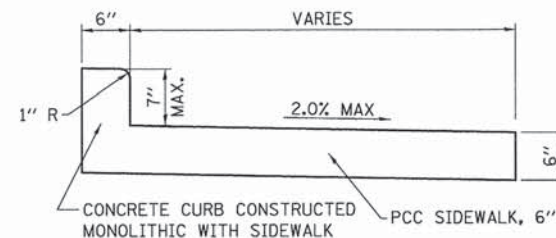
**MANHOLES TO BE ADJUSTED (SPECIAL)  
VALVE BOXES TO BE ADJUSTED (SPECIAL)  
DETAILS**



**NOTE:**

THE CONCRETE COLLARS SHALL BE UTILIZED WHERE CONNECTING STORM SEWERS OF DIFFERENT TYPES OR AS DIRECTED BY THE ENGINEER. THE COST OF CONSTRUCTING THE CONCRETE COLLARS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR CONCRETE COLLAR.

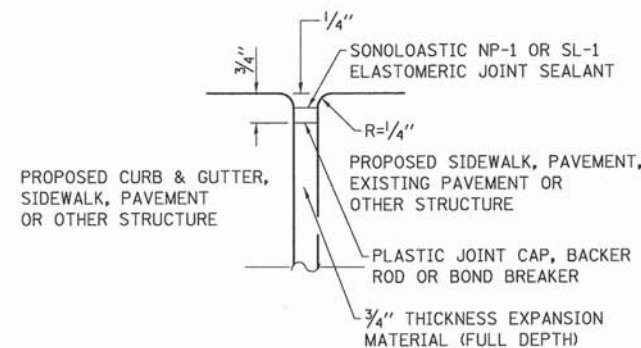
**CONCRETE COLLAR DETAIL**



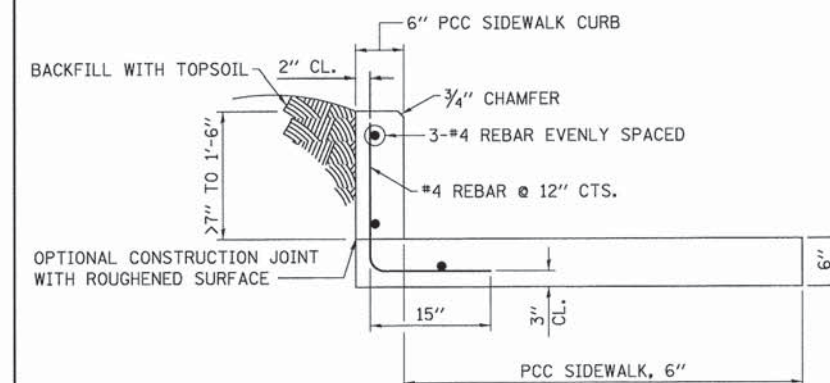
**NOTE:**

THIS WORK WILL BE PAID FOR AS PORTLAND CEMENT CONCRETE SIDEWALK 6" AT THE CONTRACT UNIT PRICE PER SQUARE FOOT OF TOP SURFACE AREA INCLUDING THE TOP SURFACE AREA OF THE CURB. THE COST OF CONSTRUCTING THE CURB TO THE HEIGHT SHOWN SHALL BE INCLUDED IN THE COST OF THE SIDEWALK.

**SIDEWALK MONOLITHIC CURB DETAIL**



**LONGITUDINAL EXPANSION / ISOLATION  
JOINT DETAIL**



**NOTES:**

1. SIDEWALK CURB MAY BE EITHER POURED MONOLITHIC WITH THE SIDEWALK OR A CONSTRUCTION JOINT CAN BE MADE AS NOTED.
2. THE COST OF CONSTRUCTING THE ADDITIONAL SIDEWALK WIDTH BELOW THE SIDEWALK CURB AND FURNISHING AND INSTALLING #4 REBARS SHALL BE INCLUDED IN THE COST OF THE PORTLAND CEMENT CONCRETE SIDEWALK CURB. THE REINFORCED CONCRETE SIDEWALK CURB WILL BE MEASURED FOR PAYMENT IN FEET ALONG THE FACE OF THE CURB. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR PORTLAND CEMENT CONCRETE SIDEWALK CURB. THE PCC SIDEWALK 6" ADJACENT TO THE SIDEWALK CURB WILL BE PAID FOR SEPARATELY.

**PORTLAND CEMENT CONCRETE  
SIDEWALK CURB DETAIL**

ESCA CONSULTANTS, INC.  
100 N. W. 10th St., Suite 100  
Fort Lauderdale, FL 33304  
Tel: (954) 574-1100  
Fax: (954) 574-1101  
www.escaconsultants.com



USER NAME = eskm  
ESCA PROJECT NO. 719.61  
PLOT SCALE = 0.8833' / 1" = 11"  
PLOT DATE = 3/1/2016 1:35:12 PM

DESIGNED - JMS  
DRAWN - JLF  
CHECKED - JMS  
DATE - 02/2016

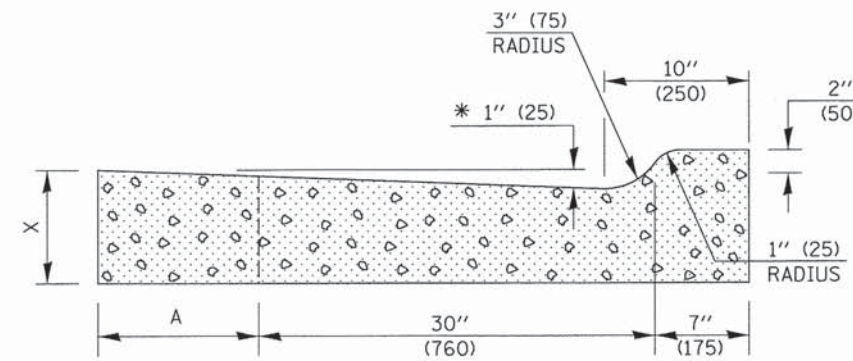
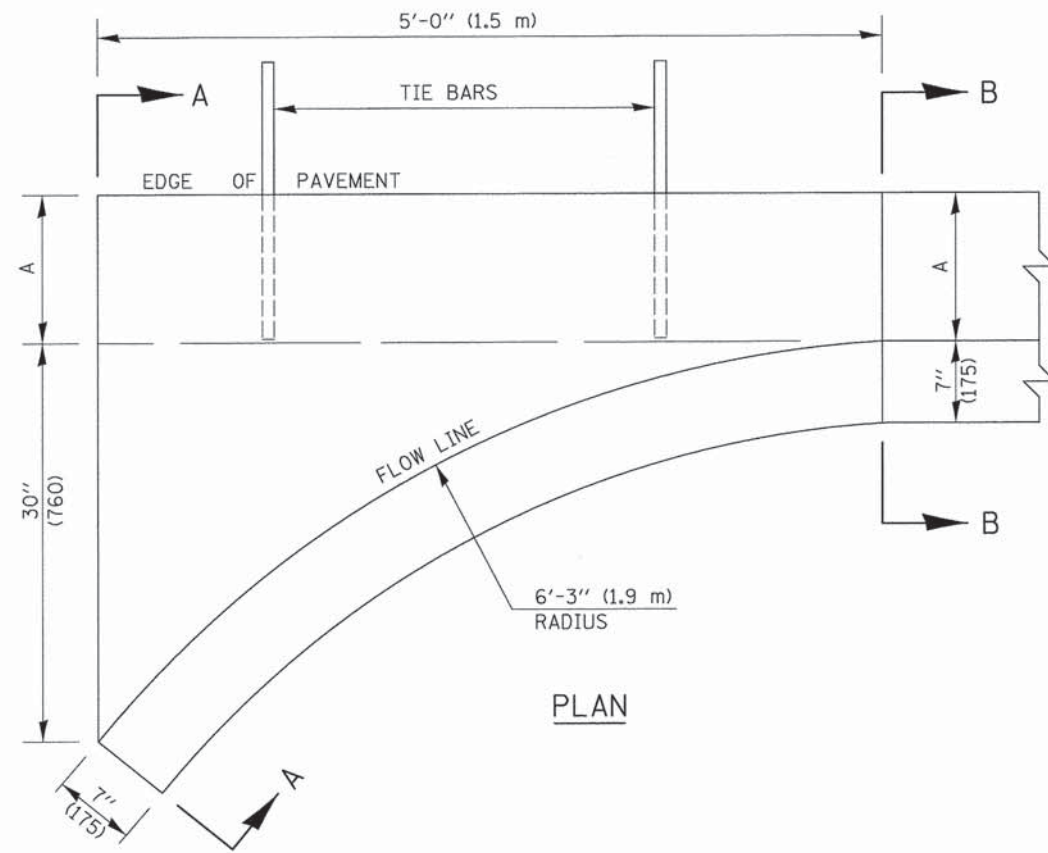
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS DETAILS

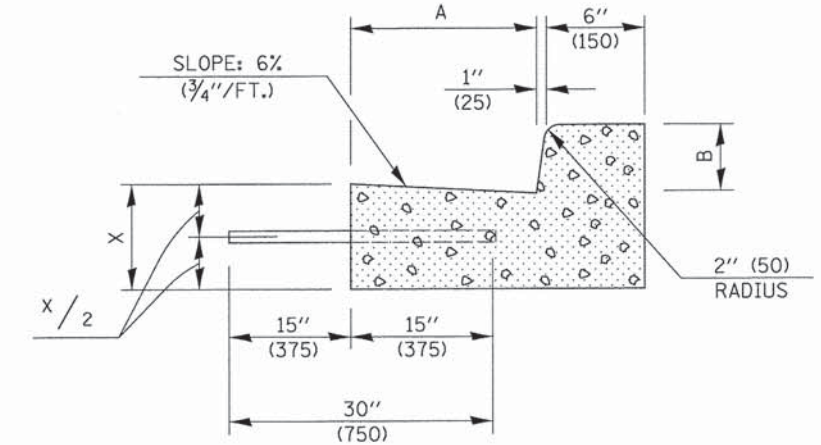
SCALE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR.  | 14-00023-00-RS | PIATT  | 76           | 70        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |





SECTION A-A



SECTION B-B

\* INCREASE TO 2" (50 mm) WHERE IN THE PLANS IT IS SPECIFIED THAT THESE SPECIAL INLETS ARE TO BE CONSTRUCTED AS OUTLETS. ALL OUTLET LOCATIONS WILL BE CONFIRMED BY THE ENGINEER.

GENERAL NOTES

1. CLASS SI CONCRETE SHALL BE USED THROUGHOUT.
2. TIE BARS SHALL BE NO. 6 (NO. 20) AT 24" (600 mm) CENTERS UNLESS OTHERWISE SHOWN. SPECIAL INLETS AND OUTLETS SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.
3. TIE BARS SHOWN ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL TO CLASS SI CONCRETE (OUTLET).
4. WHEN SPECIAL INLET IS CONSTRUCTED ADJACENT TO FLEXIBLE PAVEMENT, THE TIE BARS SHALL BE OMITTED AND ALL CONSTRUCTION JOINTS SHALL BE PROVIDED WITH A DOWEL BAR CONFORMING TO ARTICLE 1006.11(b).
5. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CU. YD. (m<sup>3</sup>) FOR CLASS SI CONCRETE (OUTLET) WHICH PRICE SHALL INCLUDE ALL LABOR AND MATERIAL AS SPECIFIED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

|                            | B-6.12<br>(B-15.30) | B-9.12<br>(B-22.30) | B-6.18<br>(B-15.45) | B-9.18<br>(B-22.45) | B-6.24<br>(B-15.60) | B-9.24<br>(B-22.60) |
|----------------------------|---------------------|---------------------|---------------------|---------------------|---------------------|---------------------|
| A                          | 12" (300)           | 12" (300)           | 18" (450)           | 18" (450)           | 24" (600)           | 24" (600)           |
| B                          | 6" (150)            | 9" (225)            | 6" (150)            | 9" (225)            | 6" (150)            | 9" (225)            |
| X                          | 9" (225)            | 10" (250)           | 9" (225)            | 10" (250)           | 9" (225)            | 10" (250)           |
| CU. YD. (m <sup>3</sup> )  | 0.37 (0.28)         | 0.42 (0.32)         | 0.38 (0.29)         | 0.42 (0.32)         | 0.44 (0.34)         | 0.49 (0.37)         |
| CLASS SI CONCRETE (OUTLET) |                     |                     |                     |                     |                     |                     |

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 5 DETAIL NO. 6060095A



|                                  |                |           |
|----------------------------------|----------------|-----------|
| USER NAME = skm                  | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63          | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 28.0000" / 1m       | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 11:52:25 PM | DATE - 02/2016 | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

INLET - COMBINATION CONCRETE CURB & GUTTER (BARRIER CURB)

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|                    |                |        |                           |           |
|--------------------|----------------|--------|---------------------------|-----------|
| F.A. RTE.          | SECTION        | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR                | 14-00023-00-RS | PIATT  | 76                        | 71        |
| CONTRACT NO. 91516 |                |        | ILLINOIS FED. AID PROJECT |           |

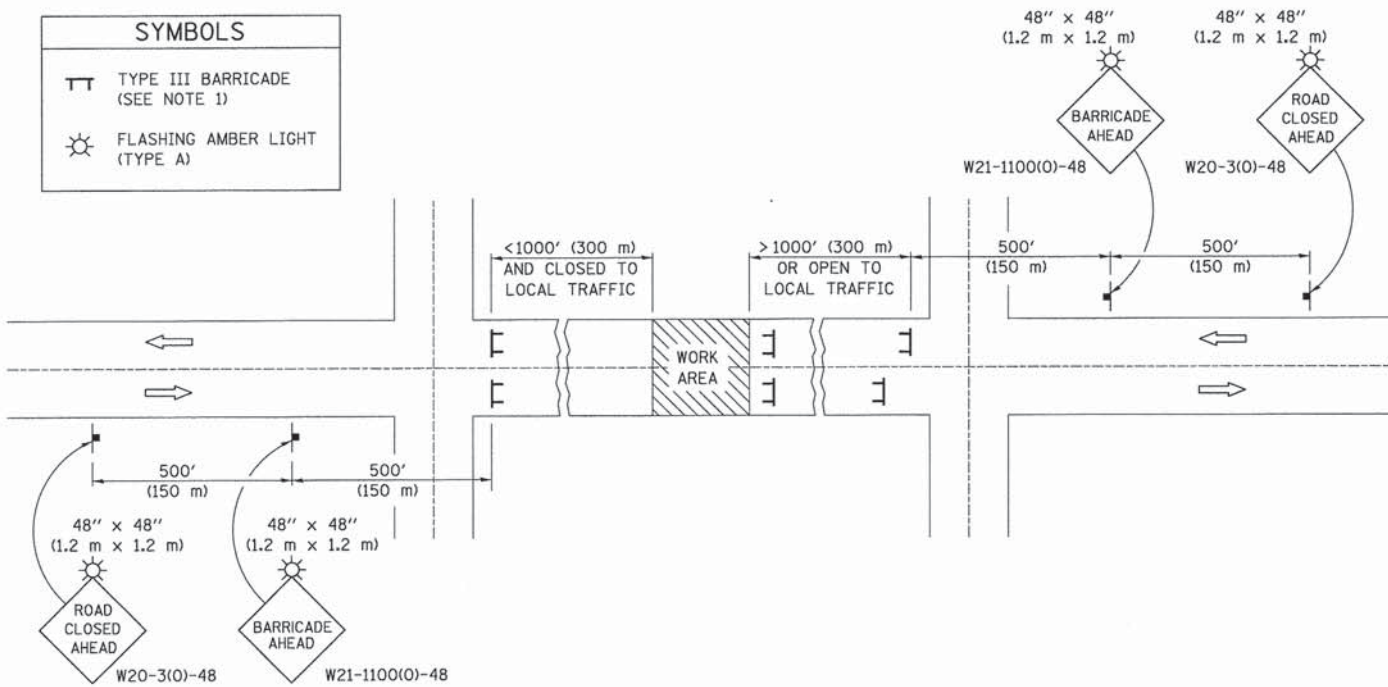
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# ROAD CLOSURE

# SIDEROAD / STREET CLOSURE

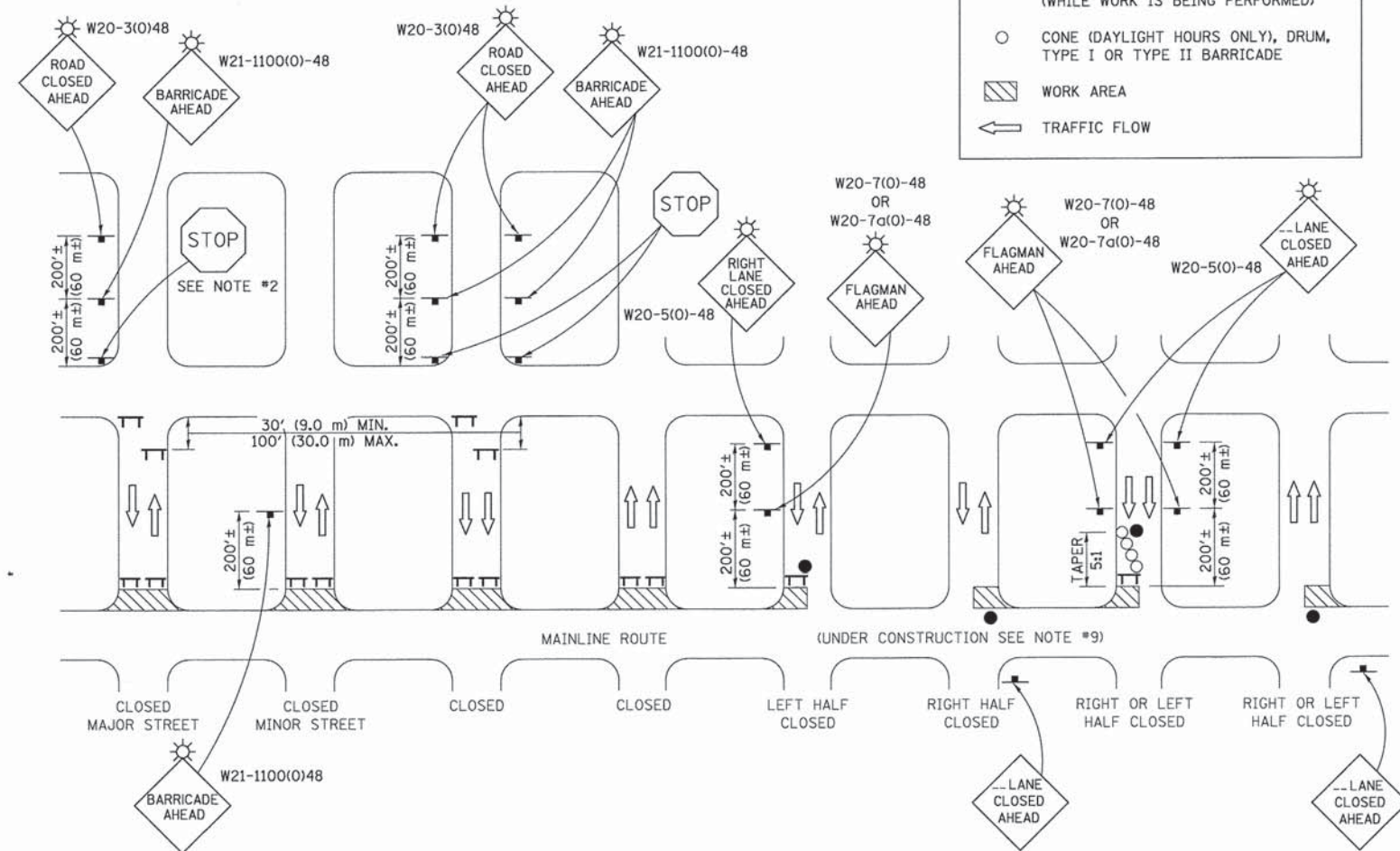
| SYMBOLS |                                 |
|---------|---------------------------------|
|         | TYPE III BARRICADE (SEE NOTE 1) |
|         | FLASHING AMBER LIGHT (TYPE A)   |



## GENERAL NOTES

- TYPE III BARRICADES SHALL BE AS SHOWN ON STANDARD 701901 "TYPICAL APPLICATIONS OF TYPE III BARRICADES CLOSING A ROAD". EACH TYPE III BARRICADE SHALL HAVE TWO FLASHING AMBER LIGHTS MOUNTED ABOVE IT.
- IF THE ROAD IS OPEN TO LOCAL TRAFFIC OR EXCEEDS 1000' (300 m), ANOTHER SET OF TYPE III BARRICADES, EQUIPPED AS IN NOTE 1 ABOVE, SHALL BE PLACED AT EACH END OF THE WORK AREA.
- WHEN A STOP CONDITION EXISTS, NO SIGNS ARE REQUIRED IN ADVANCE OF THE "STOP" SIGN WHEN THE ROAD IS CLOSED WITHIN 100' (30 m) OF THE INTERSECTION.
- STANDARD 701901 SHALL APPLY FOR THE PLACEMENT & DESIGN OF TYPE III BARRICADES.
- IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON AN NCHRP 350 TEMPORARY SIGN SUPPORT DIRECTLY IN FRONT OF THE BARRICADE.
- REFLECTORIZED STRIPING SHALL APPEAR ON BOTH SIDES OF THE TYPE III BARRICADES IF ROAD IS OPEN TO LOCAL TRAFFIC.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- A MINIMUM OF TWO FLASHING LIGHTS SHALL BE USED AT NIGHT ON EACH APPROACH IN ADVANCE OF THE WORK AREA. FLASHING LIGHTS SHALL BE INSTALLED ABOVE THE FIRST TWO SIGNS IN THE SERIES.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
- FORMS BT. 725 AND BT. 726 ARE REQUIRED.
- WHEN A SIDEROAD INTERSECTS THE HIGHWAY ON WHICH WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC DEVICES SHALL BE ERECTED AND PROVIDED AS DIRECTED BY THE ENGINEER.
- AN ADDITIONAL SIGN MAY BE REQUIRED AT A MAJOR INTERSECTING ROAD IN ADVANCE OF THE CLOSURE. THE ADDITIONAL SIGN SHALL GIVE THE DISTANCE TO THE BARRICADE IN MILES OR FRACTIONS OF A MILE.

| SYMBOLS |   |
|---------|---|
|         | TYPE III BARRICADE (SEE NOTE)                                     |
|         | FLASHING LIGHT  |
|         | FLAGGER WITH TRAFFIC CONTROL SIGN (WHILE WORK IS BEING PERFORMED) |
|         | CONE (DAYLIGHT HOURS ONLY), DRUM, TYPE I OR TYPE II BARRICADE     |
|         | WORK AREA   |
|         | TRAFFIC FLOW  |



## GENERAL NOTES

- TYPE III BARRICADES SHALL BE AS SHOWN ON "TYPICAL APPLICATIONS OF TYPE III BARRICADES CLOSING A ROAD". EACH TYPE III BARRICADE SHALL HAVE TWO FLASHING AMBER LIGHTS MOUNTED ABOVE IT.
- WHERE A STOP CONDITION EXISTS, AS SHOWN ABOVE, WARNING SIGNS MAY BE OMITTED IN ADVANCE OF THE "STOP" SIGN.
- STANDARD 701901 SHALL APPLY FOR THE PLACEMENT & MANUFACTURE OF TYPE III BARRICADES.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ONE FLASHING LIGHT IS REQUIRED ABOVE EACH ADVANCE WARNING SIGN DURING HOURS OF DARKNESS.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
- FORMS BT 725 AND BT 726 ARE REQUIRED.
- THE MAINLINE ROUTE TEMPORARY TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE PLANS, SPECIAL PROVISIONS AND STANDARD SPECIFICATIONS.
- ALL FLAGGERS REQUIRED AT SIDE ROADS AND ENTRANCES REMAINING OPEN TO TRAFFIC AND/OR ADDITIONAL BARRICADES REQUIRED BY THE ENGINEER TO CLOSE SIDE ROADS AND ENTRANCES WILL BE PAID FOR ACCORDING TO ARTICLE 109.04.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

## DISTRICT 5 DETAIL NO. 70200000

DRAWN BY: JLF  
 CHECKED BY: JMS  
 DATE: 02/2016  
 PLOT DATE: 3/1/2016 11:19:07 PM



|                                  |                |           |
|----------------------------------|----------------|-----------|
| USER NAME = skm                  | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63          | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 28.0000' / in.      | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 11:19:07 PM | DATE - 02/2016 | REVISED - |

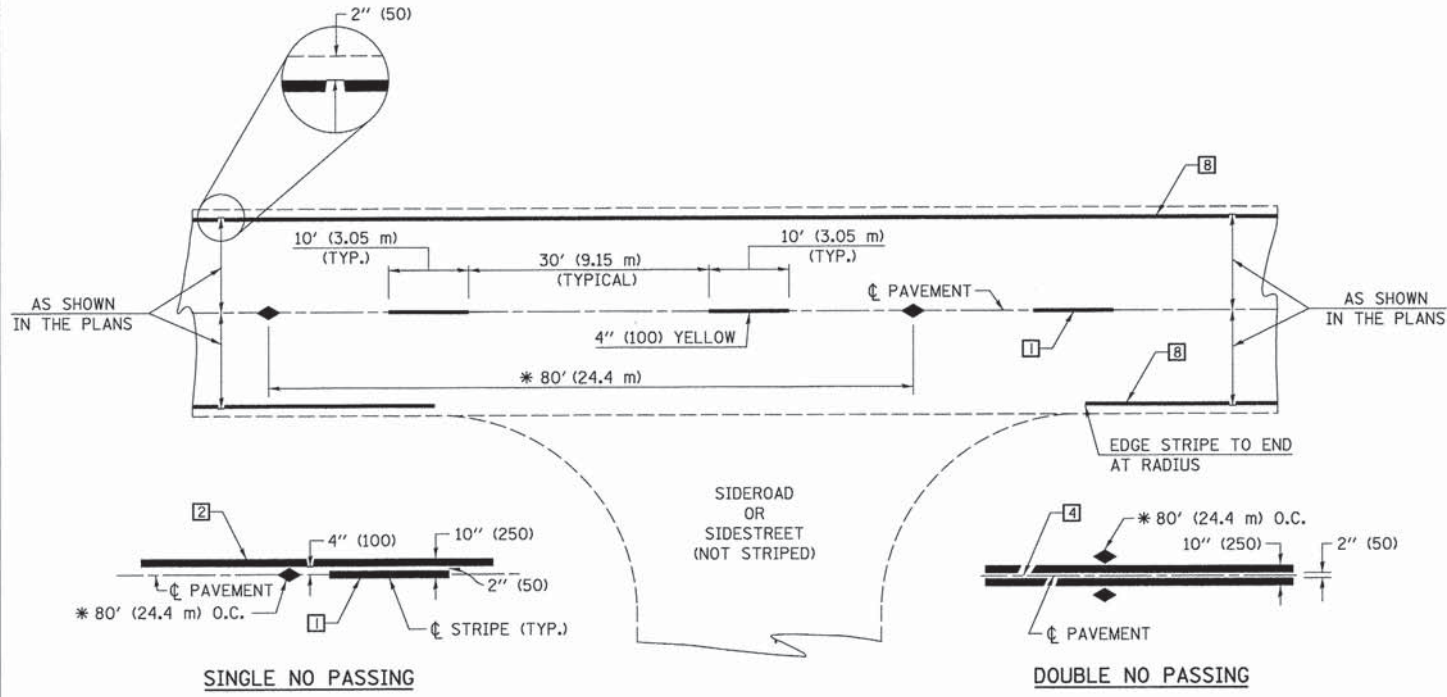
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

## TRAFFIC CONTROL & PROTECTION DEVICES (ROAD & SIDEROAD/STREET CLOSURES)

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR.  | 14-00023-00-RS | PIATT  | 76           | 72        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |





\* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

**TWO LANE/TWO WAY**

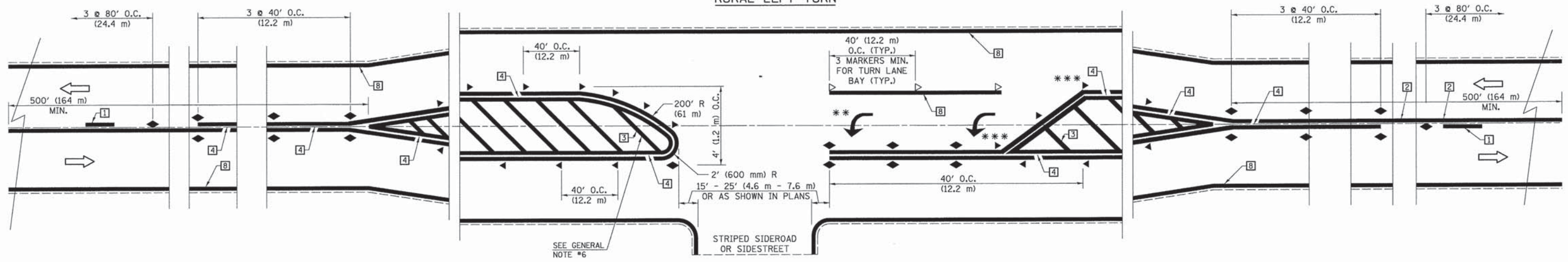
**TYPICAL PAVEMENT MARKING LEGEND**

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 4" (100) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE

**TYPICAL PAVEMENT MARKERS LEGEND**

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

**RURAL LEFT TURN**



\*\*\* REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.  
 \*\* TURN ARROWS SHALL BE PLACED AS SHOWN ON SHEET #2.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

PRINT NUMBER: 14-00023-00-RS-76-73  
 FILE NAME: 14-00023-00-RS-76-73.dwg



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = gkm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 20,0000 1/1 in.    | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:20:02 PM | DATE - 02/2016 | REVISED - |

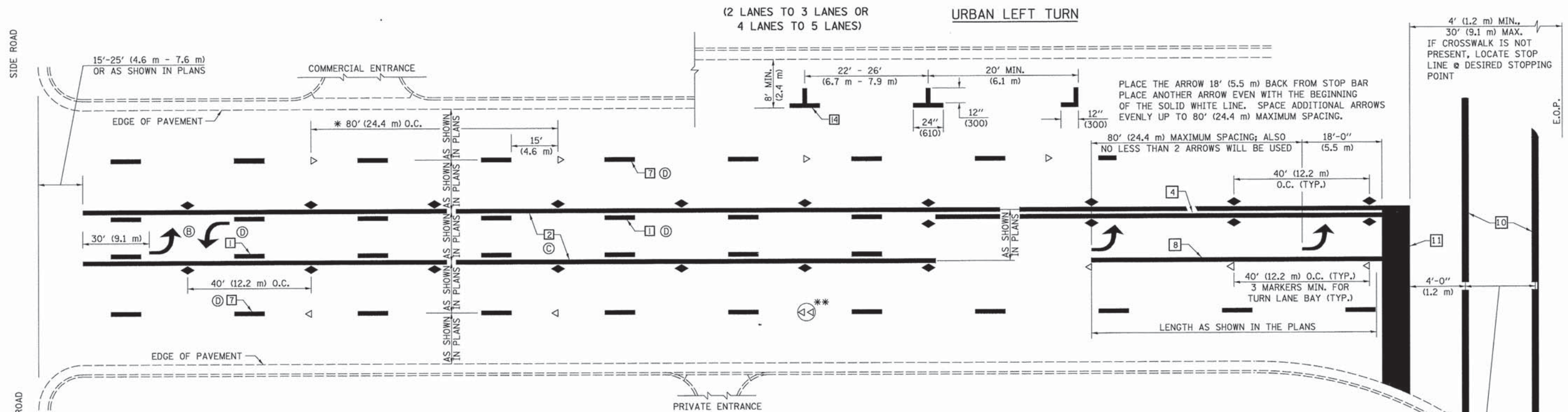
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND MARKERS  
(RURAL & URBAN APPLICATIONS)**

SCALE: SHEET NO. 1 OF 4 SHEETS STA. TO STA.

|   |                |        |              |           |
|---|----------------|--------|--------------|-----------|
| <b>DISTRICT 5 DETAIL NO. 7800AAAA</b>         |                |        |              |           |
| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR   | 14-00023-00-RS | PIATT  | 76           | 73        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |

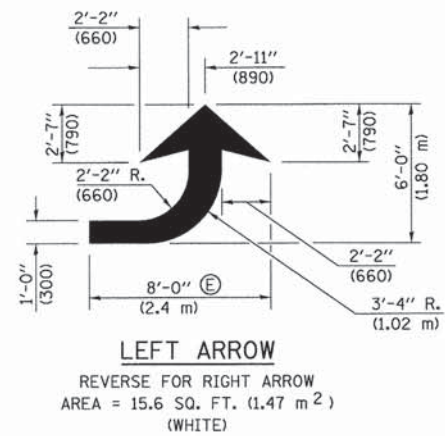




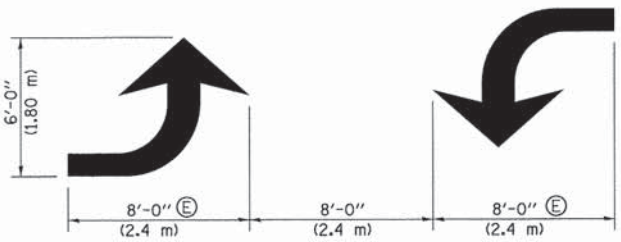
- \* REDUCE TO 40 FEET (12.2 METERS) ON CENTER ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.
- \*\* DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

**GENERAL NOTES:**

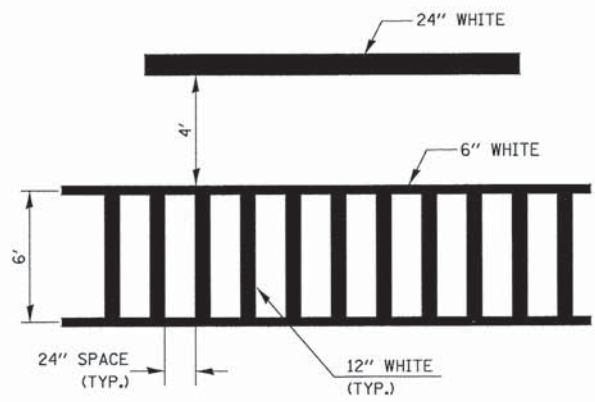
- ⓑ TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE.
- ⓒ THE SOLID YELLOW PAVEMENT MARKINGS ② SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
- ⓓ THE SKIP-DASH PAVEMENT MARKINGS ① OR ⑦ SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER. SEE EXAMPLE ON SHEET 2 OF 3.
- ⓔ USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)



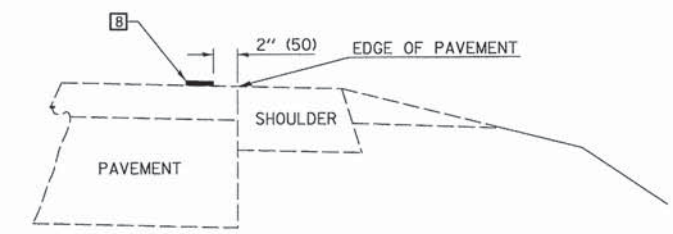
**TYPICAL DOUBLE TURN ARROWS (WHITE)**



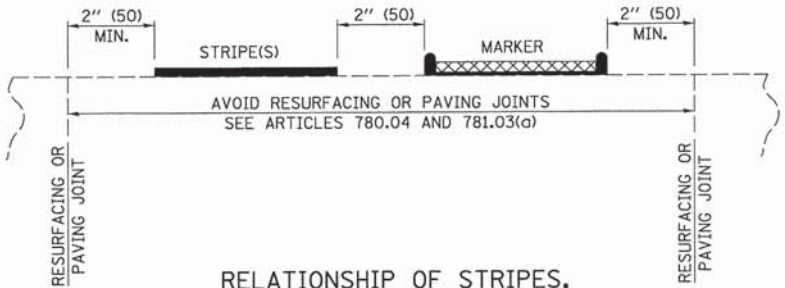
**BLOOMINGTON-NORMAL CITY LIMITS ONLY**



**TYPICAL SPACING FOR CROSSWALKS & STOP BARS**



**RELATIONSHIP OF EDGE LINE TO EDGE OF PAVEMENT (SAFETY SHOULDER OR PAVED SURFACE) SEE ARTICLE 780.04**



**RELATIONSHIP OF STRIPES, MARKERS AND JOINTS**

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 5 DETAIL NO. 7800AAAA**



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 28.0000' / 1" =    | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:20:25 PM | DATE - 02/2016 | REVISED - |

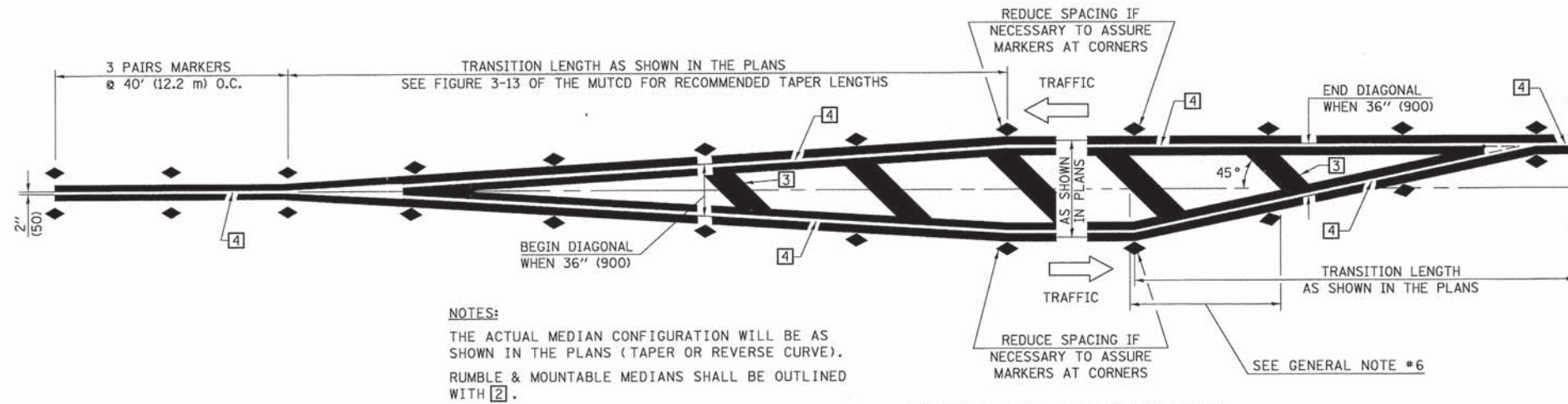
**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND MARKERS (RURAL & URBAN APPLICATIONS)**

SCALE: SHEET NO. 2 OF 4 SHEETS STA. TO STA.

| F.A. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| VAR   | 14-00023-00-RS | PIATT  | 76           | 74        |
| CONTRACT NO. 91516                            |                |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                |        |              |           |



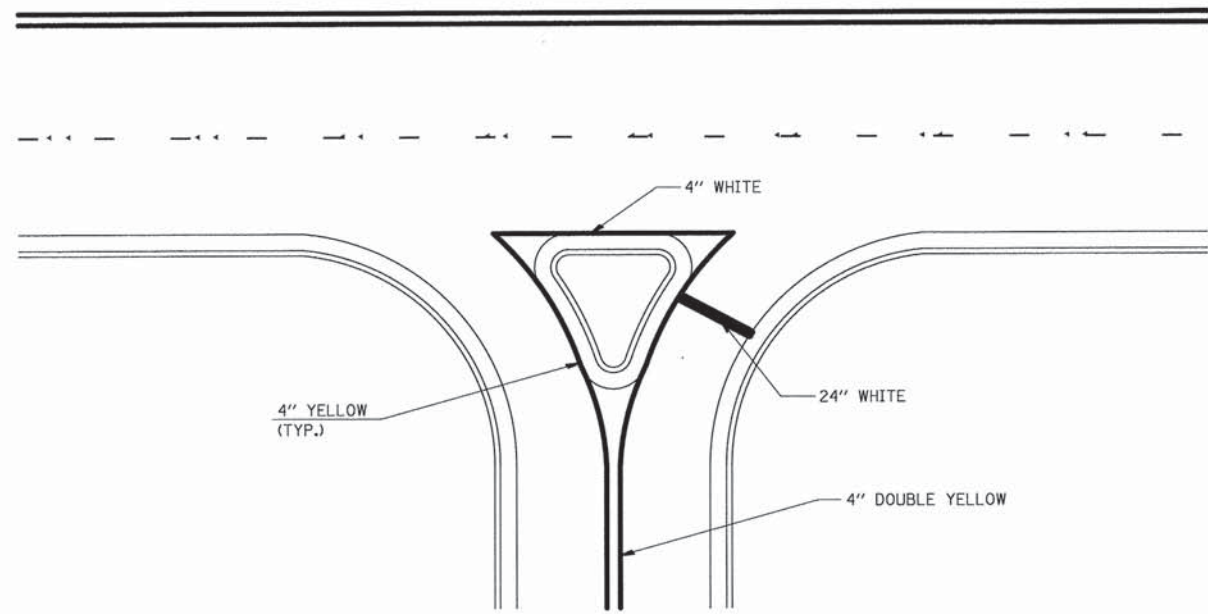


**NOTES:**  
 THE ACTUAL MEDIAN CONFIGURATION WILL BE AS SHOWN IN THE PLANS (TAPER OR REVERSE CURVE).  
 RUMBLE & MOUNTABLE MEDIANS SHALL BE OUTLINED WITH [2].

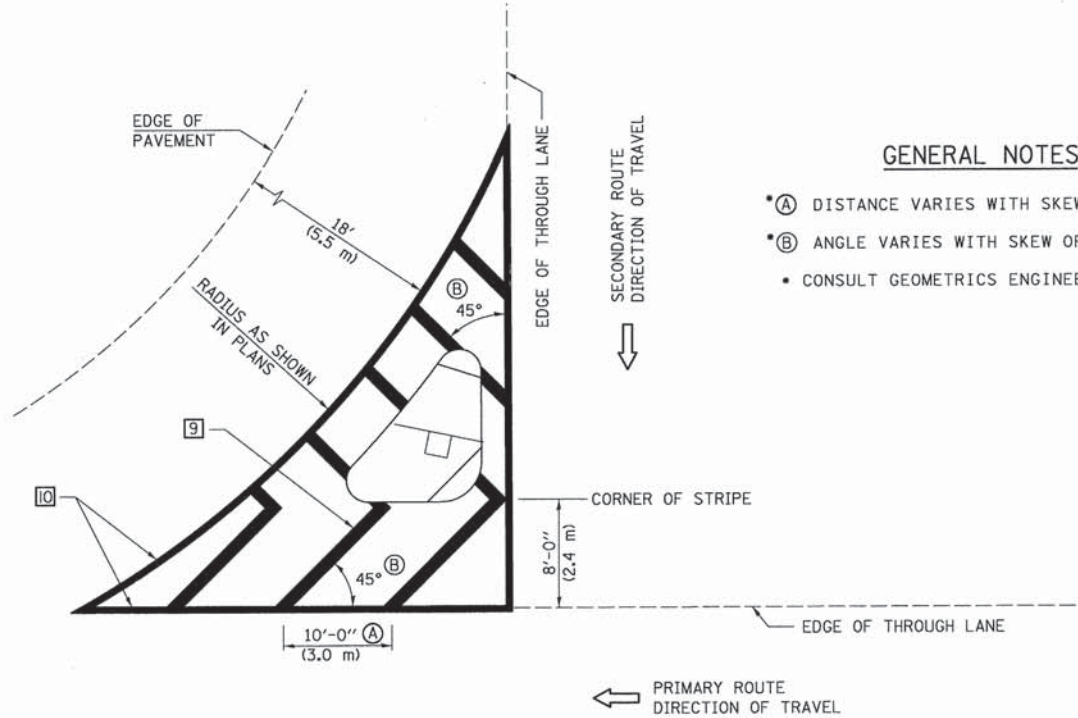
TYPICAL MEDIAN TRANSITIONS

**GENERAL NOTES**

1. WHEN MEDIANS ARE PRESENT, PAVEMENT MARKINGS ARE TO BE PLACED ADJACENT TO MEDIANS.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. A STRIPING KEY IS AVAILABLE ELSEWHERE AND SHALL BE SHOWN WHERE THE QUANTITIES ARE LISTED.
5. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
6. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING,  
 < 30 MPH USE 15' (< 50 km/h USE 4.5 m)  
 30-45 MPH USE 20' (50-75 km/h USE 6.0 m)  
 > 45 MPH USE 30' (> 75 km/h USE 9.0 m)



RIGHT IN - RIGHT OUT ACCESS



**GENERAL NOTES**

- (A) DISTANCE VARIES WITH SKEW OF INTERSECTION.
- (B) ANGLE VARIES WITH SKEW OF INTERSECTION.
- CONSULT GEOMETRICS ENGINEER

ISLAND

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 5 DETAIL NO. 7800AAAA**

|                     |                |                           |              |           |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| VAR                 | 14-00023-00-RS | PIATT                     | 76           | 75        |
| CONTRACT NO. 91516  |                |                           |              |           |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |              |           |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND MARKERS  
 (RURAL & URBAN APPLICATIONS)

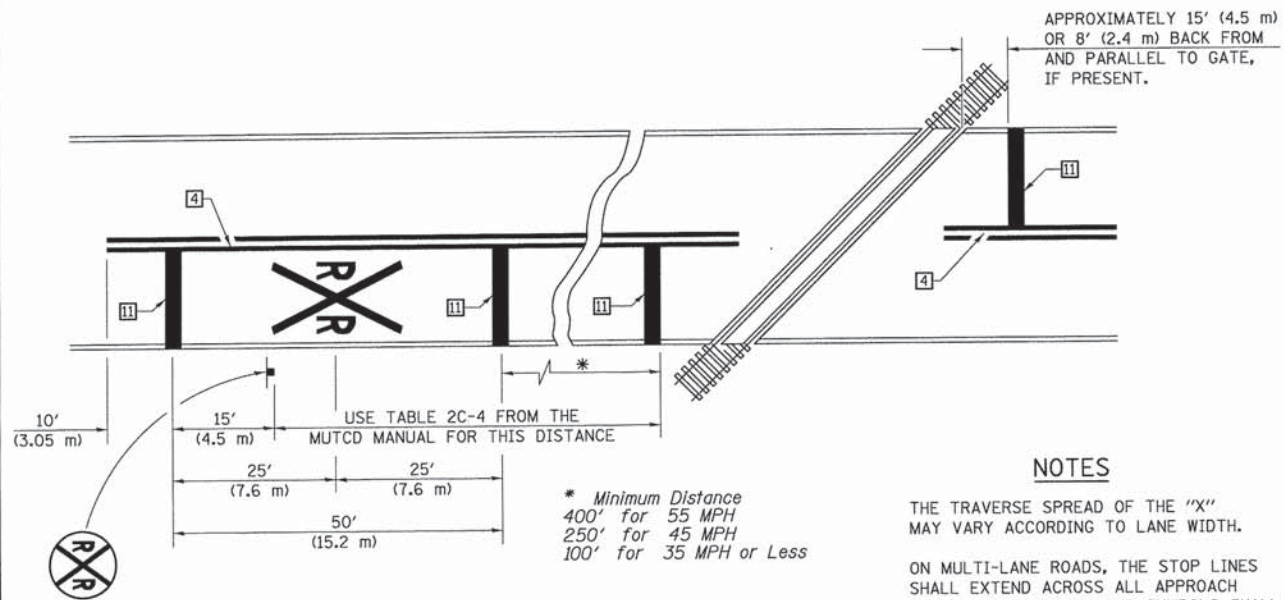
SCALE: SHEET NO. 3 OF 4 SHEETS STA. TO STA.

|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 20,0000' / 1" =    | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:21:03 PM | DATE - 02/2016 | REVISED - |



PRINT CONTROL = 1401\_CADDAPP6  
 MODEL NAME = 172\_PAVEMENTS.DWG  
 PLOT DATE = 3/1/2016 1:21:03 PM





PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING

NOTES

APPROXIMATELY 15' (4.5 m) OR 8' (2.4 m) BACK FROM AND PARALLEL TO GATE, IF PRESENT.

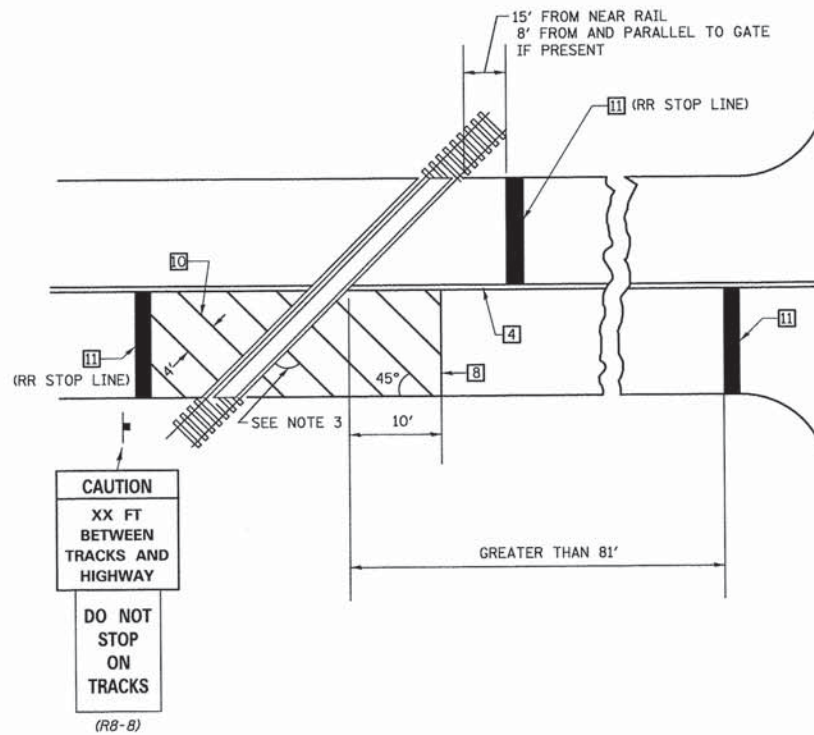
THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

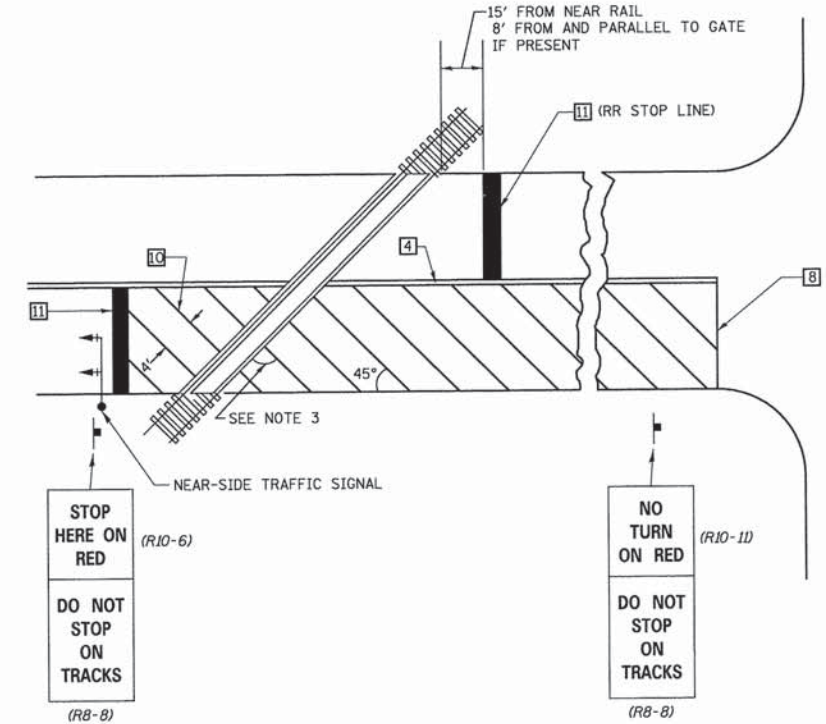
WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.

\* Minimum Distance  
 400' for 55 MPH  
 250' for 45 MPH  
 100' for 35 MPH or Less

RAILROAD CROSSING WITH INTERCONNECT ONLY



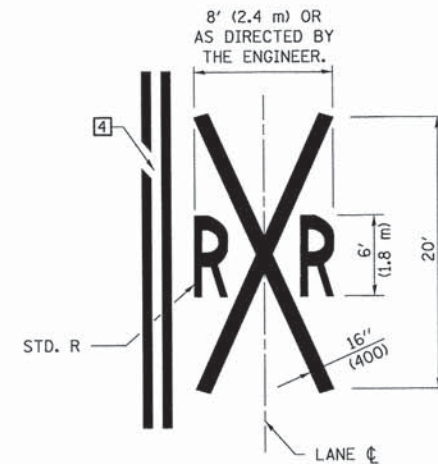
RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

GENERAL NOTES

- SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.
- WHERE THE ANGLE BETWEEN THE DIAGONAL PAVEMENT MARKINGS AND THE TRACK WOULD BE LESS THAN 20°, THE PAVEMENT MARKINGS SHOULD BE PLACED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.



Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 5 DETAIL NO. 7800AAAA



|                                 |                |           |
|---------------------------------|----------------|-----------|
| USER NAME = skm                 | DESIGNED - JMS | REVISED - |
| ESCA PROJECT NO. 719.63         | DRAWN - JLF    | REVISED - |
| PLOT SCALE = 20,0000' / 1" =    | CHECKED - JMS  | REVISED - |
| PLOT DATE = 3/1/2016 1:21:36 PM | DATE - 02/2016 | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND MARKERS  
(RURAL & URBAN APPLICATIONS)

SCALE: SHEET NO. 4 OF 4 SHEETS STA. TO STA.

| F.A. RTE.           | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|---------------------------|--------------|-----------|
| VAR                 | 14-00023-00-RS | PIATT                     | 76           | 76        |
| FED. ROAD DIST. NO. |                | ILLINOIS FED. AID PROJECT |              |           |
|                     |                | CONTRACT NO. 91516        |              |           |

PLOT DATE = 3/1/2016 1:21:36 PM  
 PLOT SCALE = 20,0000' / 1" =  
 ESCA PROJECT NO. 719.63  
 USER NAME = skm