

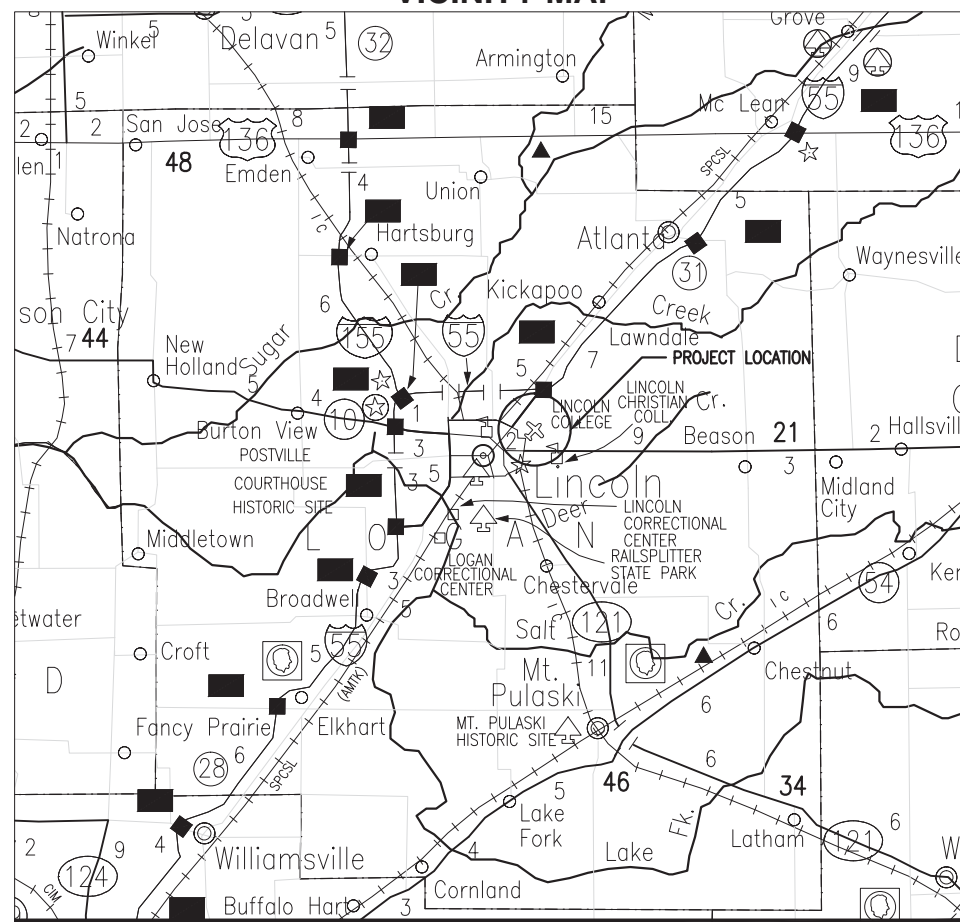
CONSTRUCTION PLANS

CRACK REPAIR & SEALING MAINTENANCE ON
RUNWAY 3-21

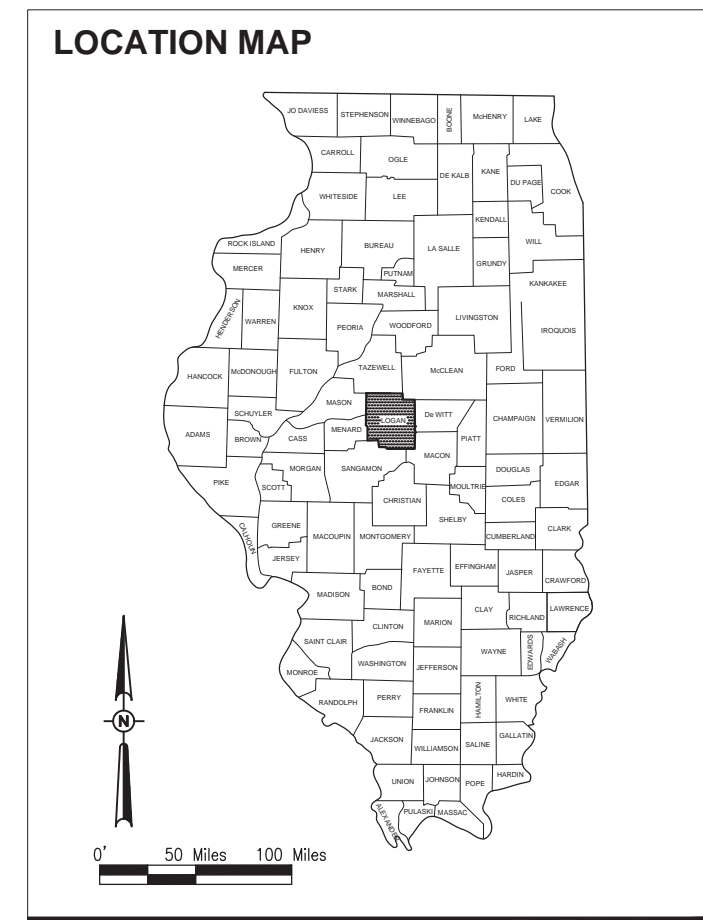
LOGAN COUNTY BOARD
LOGAN COUNTY AIRPORT (AAA)
LINCOLN, LOGAN COUNTY, ILLINOIS

SBG PROJECT NO.: 3-17-SBGP-111/120
IDA PROJECT NO.: AAA-4438

VICINITY MAP



LOCATION MAP



NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

No.	Issue/Description	Sheets Changed	Date	By



April 15, 2016

Robert A. Waller, P.E. Lic. Exp. 11/30/2017 Date
Civil Engineer



HANSON PROFESSIONAL SERVICES INC.
1525 South Sixth Street
Springfield, Illinois 62703-2886
Telephone: 217.788.2450
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Logan County Airport

LOGAN COUNTY BOARD
County Courthouse
Lincoln, Illinois 62656
Telephone: 217.732.6400

Gene Rohlf, Airport Committee Chairman Date 4/15/16



Offices Nationwide
www.hanson-inc.com

Hanson Professional Services Inc.
1525 S. 6th Street
Springfield, IL 62703
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Illinois Licensed
Professional Service Corporation
#184-001084

LOGAN COUNTY AIRPORT

1351 AIRPORT RD
LINCOLN, IL 62656

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR150520	MOBILIZATION	L.S.	1	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	L.F.	45,300	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	220	
AR800473	LIGHTNING POTHOLE REPAIR	L.S.	1	

INDEX TO SHEETS

SHEET NUMBER	SHEET TITLE
1	COVER SHEET
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	PROPOSED SAFETY PLAN
4	PROPOSED SAFETY PLAN NOTES
5	PROPOSED CONSTRUCTION PLANS STA. 0+00 TO 21+00
6	PROPOSED CONSTRUCTION PLANS STA. 21+00 TO 42+25

**CRACK REPAIR &
SEALING
MAINTENANCE ON
RUNWAY 3-21**

**SBG PROJECT NO.:
3-17-SBGP-111/120
IDA No.: AAA-4438**

Contract No. LO030

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 15, 2016

PROJECT NO: 14A0045D

CAD FILE: G-002-FLP-2.DWG

DESIGN BY: JRH 12/11/2014

DRAWN BY: JRH 12/11/2014

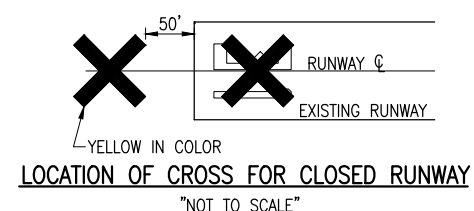
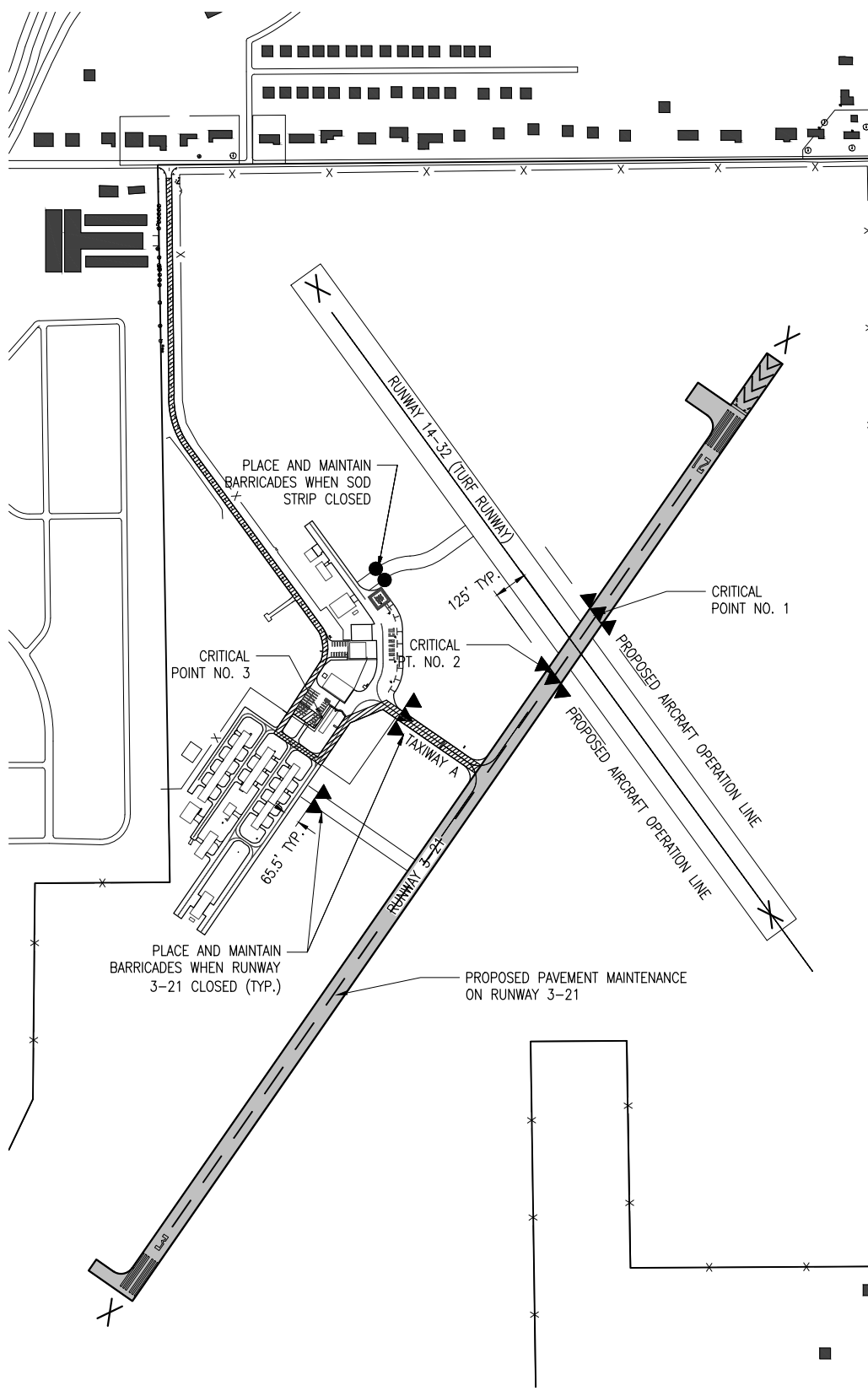
REVIEWED BY: RAW 3/12/2016

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SHEET TITLE

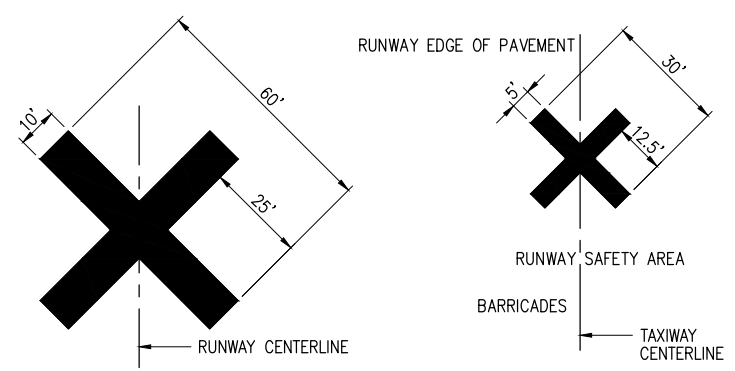
**SUMMARY OF
QUANTITIES AND
INDEX TO SHEETS**

CRITICAL POINT DATA					
POINT NO.	DESCRIPTION	LATITUDE	LONGITUDE	GROUND ELEV. (MSL)	HEIGHT (AGL)
1	CONST. EQUIP.	40° 09' 36.58"	89° 20' 01.12"	594.8'	25'
2	CONST. EQUIP.	40° 09' 34.53"	89° 20' 03.01"	594.1'	25'
3	CONST. EQUIP.	40° 09' 33.19"	89° 20' 14.19"	594.4'	25'



NOTE:

COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED EITHER OVER THE NUMERALS OR OFF THE RUNWAY END AS SHOWN ABOVE AND SECURED IN A MANNER APPROVED BY THE MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



1. TEMPORARY "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE "AVIATION YELLOW"
2. TEMPORARY "CLOSED RUNWAY" MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, DOUBLE-LAYERED SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.
3. TEMPORARY "CLOSED RUNWAY" MARKINGS SHALL BE PLACED OVER THE RUNWAY DESIGNATION NUMBERS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE TEMPORARY "CLOSED RUNWAY" MARKINGS FOR THE SOD STRIP WILL BE PLACED ON THE SOD STRIP AT THE RUNWAY END INDICATORS.
4. TEMPORARY "CLOSED TAXIWAY" MARKINGS SHALL BE CONSTRUCTED USING TEMPORARY PAINT SUCH AS SEYMOUR TEMPORARY MARKER, PART NO. 20-636, OR AN EQUIVALENT PAINT EASILY REMOVED WITH WATER WITHOUT DEFACING THE PAVEMENT.
5. TEMPORARY "CLOSED TAXIWAY" MARKINGS SHALL BE PLACED WHEN THE RUNWAY IS OPEN AND THE TAXIWAY IS CLOSED MORE THAN 72 HOURS. THE "CLOSED TAXIWAY" MARKINGS SHALL BE PLACED AS SHOWN ON THIS SHEET. TEMPORARY TAXIWAY CLOSURE MARKING IS NOT ANTICIPATED AS PART OF THIS PROJECT
6. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY OR TAXIWAY IS CLOSED AND REMOVED WHEN THE RUNWAY OR TAXIWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL NOT BE A PAY ITEM AND SHALL BE INCIDENTAL TO OTHER CONTRACT BID ITEMS.

TEMPORARY CLOSURE CROSS DETAIL
NOT TO SCALE

LEGEND

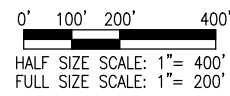
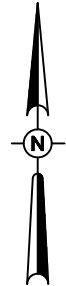
- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA
- PROPOSED BENCHMARK
- PROPOSED BARRICADES FOR RWY 3-21 CLOSURE
- PROPOSED BARRICADES FOR SODSTRIP 14-32 CLOSURE

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

J.U.L.I.E. INFORMATION

COUNTY.....LOGAN
CITY.....LINCOLN
TOWNSHIP.....EAST LINCOLN
SECTION NO.....29
ADDRESS.....LOGAN COUNTY AIRPORT
RR#4, AIRPORT ROAD
LINCOLN, ILLINOIS 62656



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CRACK REPAIR & SEALING MAINTENANCE ON RUNWAY 3-21

SBG PROJECT NO.:
3-17-SBGP-111/120
IDA No.: AAA-4438

Contract No. LO030

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 15, 2016
PROJECT NO: 14A0045D
CAD FILE: G-004-SFY.DWG
DESIGN BY: JRH 12/11/2014
DRAWN BY: JRH 12/11/2014
REVIEWED BY: RAW 3/12/2016
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SHEET TITLE

PROPOSED SAFETY PLAN

PROPOSED SAFETY PLAN

- ALL CONSTRUCTION/OPERATIONS ARE TO BE PERFORMED IN ACCORDANCE WITH THE LATEST EDITION OF FAA ADVISORY CIRCULAR 150/5370-2F, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" AND AC 150/5300-13A "AIRPORT DESIGN".
- ALL CONSTRUCTION EQUIPMENT ON THE AIRPORT SHALL BE MARKED, LIGHTED AND/OR FLAGGED IN ACCORDANCE WITH AC 150/5210-5 AND 70/7460-1.
- GENERAL – THE LOGAN COUNTY AIRPORT IS COMPRISED OF ONE PAVED RUNWAY AND A SOD STRIP RUNWAY. THE PROPOSED CONSTRUCTION WILL NECESSITATE CLOSING THE PAVED RUNWAY 3-21 FOR THE ENTIRE PROJECT EXCEPT WEEKENDS. THE CONTRACTOR WILL CLOSE RUNWAY 3-21 AT THE START OF CONSTRUCTION WEEK AND RE-OPEN THE RUNWAY UPON COMPLETION OF THE PROJECT OR ON FRIDAY EVENING. THE SOD STRIP WILL BE CLOSED WHENEVER THE CONTRACTOR IS WORKING WITHIN 125' OF THE SOD STRIP RUNWAY CENTERLINE. WHEN CLOSING THE PAVED RUNWAY, THE CONTRACTOR WILL BE RESPONSIBLE TO ENSURE THAT THE NAVAIDS AND RUNWAY LIGHTS ARE TURNED OFF PRIOR TO WORKING ON THE RUNWAY. UPON COMPLETION OF WORK, IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT THE NAVAIDS AND RUNWAY LIGHTS ARE TURNED BACK ON. PRIOR TO OPENING THE RUNWAY, THE CONTRACTOR WILL SMOOTH GRADE ALL DISTURBED AREAS WITHIN THE RUNWAY SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER, INSURE ALL LIGHTING CIRCUITS ARE WORKING AND RE-OPEN THE RUNWAY. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR WILL BE REQUIRED TO SUBMIT TO THE RESIDENT ENGINEER/RESIDENT TECHNICIAN, FOR APPROVAL BY THE AIRPORT MANAGEMENT, A SCHEDULE OF CONSTRUCTION ACTIVITIES, TO INCLUDE A SCHEDULE OF RUNWAY AND TAXIWAY CLOSURES PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR IS REQUIRED TO ALLOW SUFFICIENT TIME PRIOR TO THE ENDING OF THE CLOSURE TIME FOR CURING OF PLACED MATERIALS AND CLEAN UP OF THE WORK AREA.
- THE CONTRACTOR IS REQUIRED TO NOTIFY THE AIRPORT MANAGER AND THE RESIDENT ENGINEER/RESIDENT TECHNICIAN, A MINIMUM OF 7 DAYS PRIOR TO THE START OF CONSTRUCTION. THIS WILL ALLOW THE AIRPORT MANAGER SUFFICIENT TIME TO ISSUE ALL NECESSARY NOTAMS. THE CONTRACTOR IS REQUIRED TO PROVIDE A MINIMUM OF 24 HOUR NOTICE TO THE AIRPORT MANAGER PRIOR TO CLOSING A RUNWAY.
- THE CONTRACTOR IS REQUIRED TO IMPLEMENT A PRACTICAL AND EFFECTIVE STAGING PLAN THAT WILL MINIMIZE DISRUPTION TO NORMAL AIRPORT ACTIVITY WHILE NOT COMPROMISING SAFETY OF PERSONNEL OR THE QUALITY OF THE PROJECT.
- THE AIRPORT MANAGEMENT, OR DESIGNATED REPRESENTATIVE, WILL ISSUE ALL NOTICES TO AIRMAN (NOTAM) RELATED TO OPENING AND CLOSING PAVEMENTS THROUGHOUT THE PROJECT.

AIRCRAFT OPERATION LINE

- THE AIRCRAFT OPERATION LINE PARALLELS THE RUNWAY AT A DISTANCE OF 125' FROM THE SODSTRIP CENTERLINE. THE CONTRACTOR WILL LOCATE THIS LINE AT THE START OF CONSTRUCTION AND WILL PLACE A FRANGIBLE MARKER EVERY 150' ALONG IT FOR 500' EITHER SIDE OF THE PAVED RUNWAY 3-21 CENTERLINE. THIS LINE WILL BE THE LIMITS THAT ALL CONTRACTOR PERSONNEL MAY VENTURE WHEN THE SODSTRIP IS NOT CLOSED. THE CONTRACTOR WILL MAINTAIN THIS LINE FOR SODSTRIP RUNWAY AND REMOVE THE MARKERS AT THE CONCLUSION OF THE PROJECT.

EROSION CONTROL

- THIS PROJECT WILL NOT DISTURB MORE THAN 1 ACRE OF LAND, THEREFORE A STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IS NOT REQUIRED.

HEIGHT OF CONSTRUCTION EQUIPMENT

- THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A DUMP TRUCK.

HAUL ROUTE AND EQUIPMENT PARKING/MATERIAL STORAGE AREA

- THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTES AND EQUIPMENT PARKING/MATERIAL STORAGE AREAS AS SHOWN ON THIS SHEET. THE PROPOSED PARKING/MATERIAL STORAGE AREA WILL BE APPROXIMATELY 50' X 100'. THE EQUIPMENT PARKING/MATERIAL STORAGE AREA SHALL BE LOCATED IN THE PARKING LOT WEST OF THE MUSEUM. NO EQUIPMENT WILL BE ALLOWED TO PARK OVERNIGHT ON THE RUNWAYS. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND STAGING AREAS THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL RESTORE THE HAUL ROUTES AND STAGING AREAS TO ITS PRE-CONSTRUCTION STATE. RESTORATION OF THE HAUL ROUTES AND STAGING AREAS WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CONTRACTOR RESPONSIBILITIES

- THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.
- THE CONTRACTOR IS REQUIRED TO LIMIT THE USE OF CONSTRUCTION EQUIPMENT ON THE EXISTING PAVEMENTS. ONLY EQUIPMENT NEEDED TO COMPLETE THE SPECIFIC WORK ON THE EXISTING PAVEMENT WILL BE PERMITTED. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE TO EXISTING PAVEMENTS CAUSED BY HIS PERSONNEL OR EQUIPMENT.
- NO OPEN HOLES OR TRENCHES WILL BE ALLOWED WITHIN 200' OF ACTIVE RUNWAY 3-21 OR 125' OF ACTIVE RUNWAY 14-32, WITHIN 65.5' OF AN ACTIVE TAXIWAY OR 57.5' OF AN ACTIVE TAXILANE, NOR WILL EITHER HOLES OR OPEN TRENCHES BE ALLOWED TO REMAIN EXPOSED OR OPEN OVERNIGHT.
- WHEN A CONTRACTOR IS WORKING WITHIN 65.5' OF AN ACTIVE TAXIWAY OR 57.5' OF AN ACTIVE TAXILANE, THE CONTRACTOR WILL BE REQUIRED TO CLOSE THAT TAXIWAY OR TAXILANE.
- IDENTIFICATION – WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.
- RADIO CONTROL – THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (122.80 MHz.) WITH THE AIRPORT UNICOM WHENEVER HIS PERSONNEL ARE ON THE AIRPORT PROPERTY. THIS WILL ALLOW THE CONTRACTOR TO RESPOND TO AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

AIRPORT SECURITY NOTE

- AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL INCORPORATE A LOCK INTO THE GATE ACCESSING THE T-HANGAR AREA. THIS WILL BE THE ONLY ACCESS TO AND FROM THE SITE AND WILL BE CLOSED AND LOCKED AT THE END OF EACH CONSTRUCTION DAY.

SCOPE OF WORK

- THIS PROJECT CONSISTS OF BITUMINOUS PAVEMENT REPAIRS AND CRACK CLEANING & SEALING OF THE EXISTING BITUMINOUS RUNWAY 3-21.

CERTIFIED PAYROLLS

- THE RESIDENT ENGINEER CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

MATERIAL CERTIFICATION

- MATERIAL TO BE INCORPORATED INTO THE PROJECT CANNOT BE USED WITHOUT PRIOR APPROVAL. ALL MATERIAL TO BE USED IN THE PROJECT MUST BE SUBMITTED TO THE RESIDENT ENGINEER FOR APPROVAL. USE OF MATERIAL WITHOUT PRIOR APPROVAL AND ULTIMATELY DETERMINED TO BE UNACCEPTABLE BY THE ILLINOIS DIVISION OF AERONAUTICS ARE SUBJECT TO REMOVAL AND/OR NON-PAYMENT.

TEMPORARY RUNWAY CLOSURE NOTE

- FAA CRITERIA REQUIRES RUNWAY 3-21 BE CLOSED IF CONSTRUCTION ACTIVITIES OR PERSONNEL ARE WITHIN 200 FT. OF RUNWAY 3-21 CENTERLINE AND THE SODSTRIP RUNWAY 14-32 BE CLOSED IF CONSTRUCTION ACTIVITIES OR PERSONNEL ARE WITHIN 125 FT. OF RUNWAY 14-32 CENTERLINE.
- COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. ALL RUNWAY CLOSURES WILL BE IN ACCORDANCE WITH ALL FAA ADVISORY CIRCULARS THAT RELATE TO RUNWAY CLOSURES DURING CONSTRUCTION OPERATIONS. THE RUNWAY CLOSURE PROCEDURES SHALL BE REVIEWED BY THE AIRPORT MANAGER AND COORDINATED WITH THE RESIDENT ENGINEER/RESIDENT TECHNICIAN. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL, AS APPROVED BY THE RESIDENT ENGINEER/RESIDENT TECHNICIAN AND REVIEWED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED OVER THE NUMERALS OR OFF EACH END OF THE RUNWAY AND SECURED IN A MANNER APPROVED BY THE AIRPORT MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

3. RUNWAY CLOSURE PROCEDURES:

- CONTACT THE AIRPORT MANAGEMENT OR ASSIGNED REPRESENTATIVE.
- ISSUANCE OF NOTAM BY THE AIRPORT MANAGEMENT OR ASSIGNED REPRESENTATIVE.
- RUNWAY LIGHTS AND NAVAIDS ARE TURNED OFF.
- PLACEMENT OF CROSSES (SEE DETAIL PREVIOUS SHEET).
- PLACEMENT OF LIGHTED BARRICADES AND/OR CONES.

ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS WITHIN 200 FT. OF RUNWAY 3-21 CENTERLINE OR RUNWAY 14-32 CENTERLINE BEGIN.

4. RUNWAY RE-OPENING PROCEDURES:

- REMOVE CROSSES.
- REMOVE LIGHTED BARRICADES AND CONES.
- TURN ON THE RUNWAY LIGHTS AND NAVAIDS AND VERIFY IN WORKING ORDER.
- NOTIFY THE AIRPORT MANAGEMENT OR REPRESENTATIVE TO CANCEL THE NOTAM.
- CANCELLATION OF THE NOTAM.

A CLOSED RUNWAY WILL NOT BE RE-OPENED UNTIL ALL EQUIPMENT AND WORK ARE FURTHER THAN 200 FT. FROM THE RUNWAY 3-21 CENTERLINE OR RUNWAY 14-32 CENTERLINE.

BARRICADES AND TRAFFIC CONES

- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS SHOWN ON THE SAFETY PLAN AND AS DIRECTED BY THE AIRPORT MANAGER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED INCIDENTAL TO THE PROJECT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CRACK REPAIR & SEALING MAINTENANCE ON RUNWAY 3-21

SBG PROJECT NO.: 3-17-SBGP-111/120
IDA No.: AAA-4438

Contract No. LO030

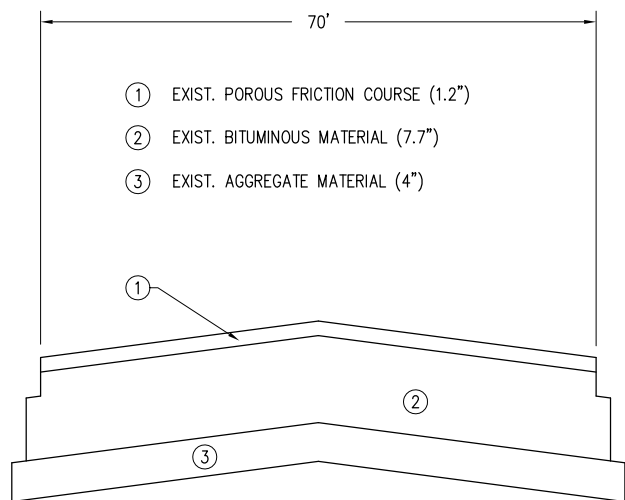
NO.	DATE	DESCRIPTION		
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ISSUE: APRIL 15, 2016
PROJECT NO: 14A0045D
CAD FILE: G-004-SFY.DWG
DESIGN BY: JRH 12/11/2014
DRAWN BY: KDM 2/12/2016
REVIEWED BY: RAW 3/12/2016
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SHEET TITLE

PROPOSED SAFETY PLAN NOTES

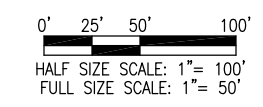
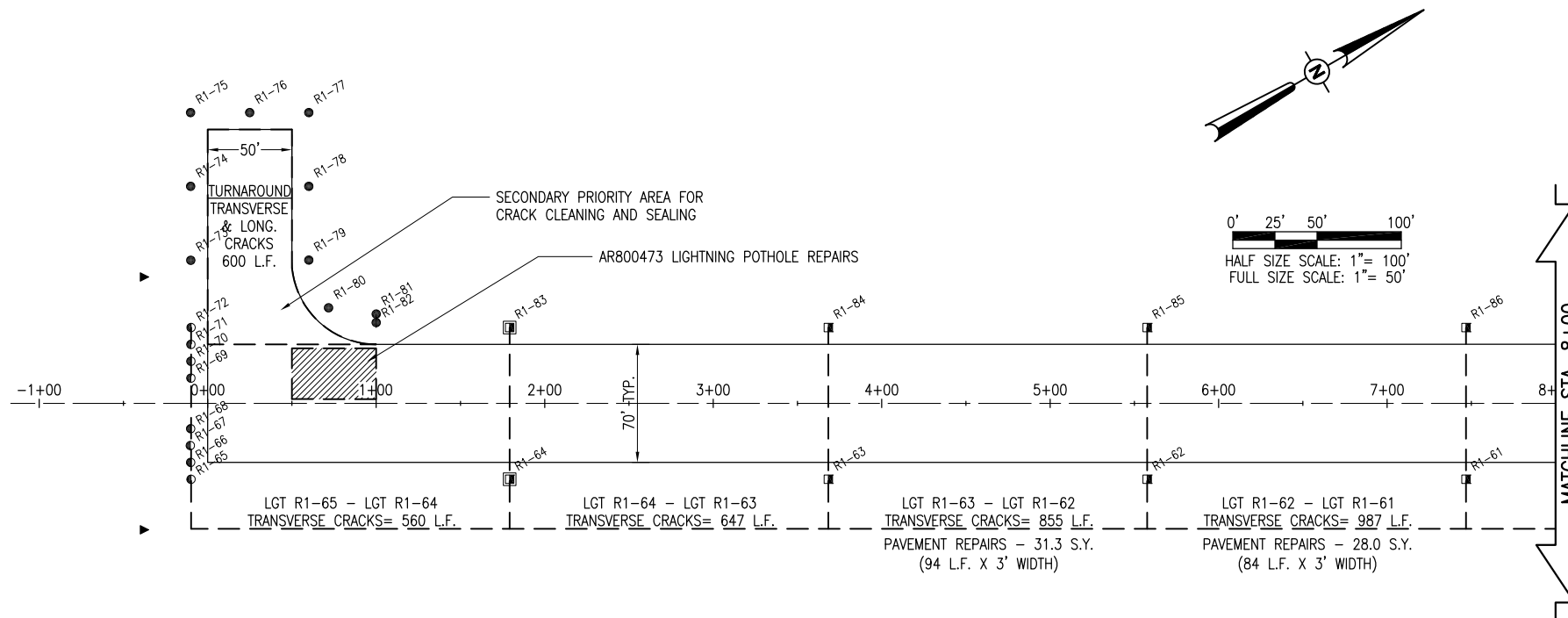
CLEAN & SEAL BITUMINOUS CRACKS

1. ALL CRACKS DESIGNATED BY THE RESIDENT ENGINEER/RESIDENT TECHNICIAN FOR REPAIR WILL BE COMPLETED AS STATED IN THE SPECIAL PROVISIONS.
2. THE EXACT AMOUNT OF CRACKS TO BE CLEANED AND SEALED WILL BE THE NUMBER OF LINEAR FEET OF CRACKS MARKED BY THE RESIDENT ENGINEER/RESIDENT TECHNICIAN AT THE TIME OF CONSTRUCTION.
3. THE PAVEMENT CONDITIONS AND CRACKS WERE IDENTIFIED AND LOCATED AS SHOWN DURING A SURVEY IN DECEMBER OF 2014.
4. THE BITUMINOUS CRACK CLEANING AND SEALING QUANTITY HAS BEEN INCREASED BY APPROXIMATELY THREE (3%) PERCENT TO ACCOUNT FOR ANY CRACKS THAT WILL DEVELOP BETWEEN THE FIELD SURVEY AND THE TIME OF CONSTRUCTION.
5. THE PROPOSED BITUMINOUS CRACK CLEANING AND SEALING WILL BE PAID FOR UNDER ITEM:
AR201661 - CLEAN & SEAL BITUMINOUS CRACKS - PER L.F.



EXISTING TYPICAL SECTION RUNWAY 3-21

(SECTION A-A)
(NOT TO SCALE)



NOTE TO CONTRACTOR

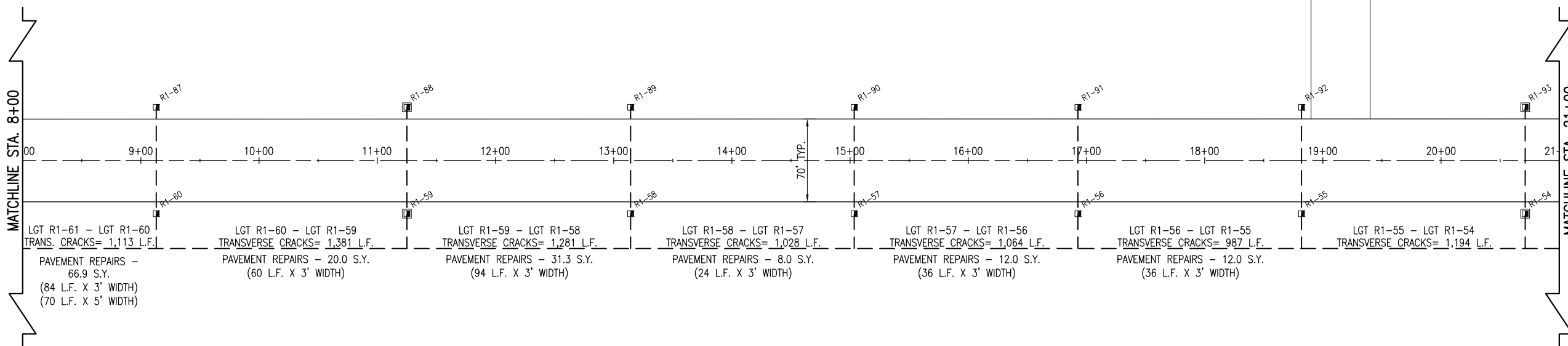
THE CRACKS SHOWN TO BE CLEANED AND SEALED ON THIS SHEET AND THE FOLLOWING SHEET WERE DETERMINED DURING A FIELD SURVEY CONDUCTED IN DECEMBER OF 2014. THE QUANTITY OF CRACKS TO BE CLEANED AND SEALED WILL BE THE QUANTITY MARKED AND ACCEPTED IN THE FIELD BY THE RESIDENT ENGINEER/RESIDENT TECHNICIAN. ANY QUANTITY BEYOND THE PLAN QUANTITY WILL HAVE TO BE APPROVED BY THE DIVISION OF AERONAUTICS AND/OR THE AIRPORT.

MANY OF THE PAVEMENT REPAIRS ARE PREVIOUS PATCHES THAT HAVE REFLECTED THROUGH TO THE SURFACE. TO ACCOUNT FOR ADDITIONAL REPAIRS THAT MAY BE NECESSARY, THE QUANTITY OF REMOVE & REPLACE PAVEMENT WAS INCREASED BY 5 PERCENT.

LIGHTNING POTHOLE REPAIR

THE EXISTING PAVEMENT WAS POSSIBLY STRUCK BY LIGHTNING CAUSING APPROXIMATELY 12 POTHOLES IN THE PAVEMENT VARYING IN SIZE FROM 1" TO 6" IN DIAMETER TO 1" TO 2" IN DEPTH. THE RESIDENT ENGINEER WILL MARK THE POTHOLES TO BE ADDRESSED. ONCE MARKED, THE CONTRACTOR WILL HEAT THE EXISTING PAVEMENT THOROUGHLY USING A "WEED-BURNING TORCH" OR SIMILAR METHOD TO MAKE THE EXISTING BITUMINOUS PAVEMENT WORKABLE. THE CONTRACTOR WILL INSTALL AND WORK BITUMINOUS SURFACE COURSE INTO THE POTHOLES TO PROVIDE A SMOOTH, DURABLE REPAIR. THE CONTRACTOR WILL ROLL EACH POTHOLE WITH A SMOOTH DRUM ROLLER TO PROVIDE COMPACTION TO THE SATISFACTION OF THE RESIDENT ENGINEER/RESIDENT TECHNICIAN.

ALL OF THE LIGHTNING POTHOLES WILL BE PAID TOGETHER UNDER ITEM: AR800473 - "LIGHTNING POTHOLE REPAIR" - PER LUMP SUM.



APR 18, 2016 5:16 PM HARR01115 I:\14\JOBS\14A0045\CAD\AIRPORTSHEET\2ND BID\C-121-CON-2

CRACK REPAIR & SEALING MAINTENANCE ON RUNWAY 3-21

SBG PROJECT NO.: 3-17-SBGP-111/120
IDA No.: AAA-4438

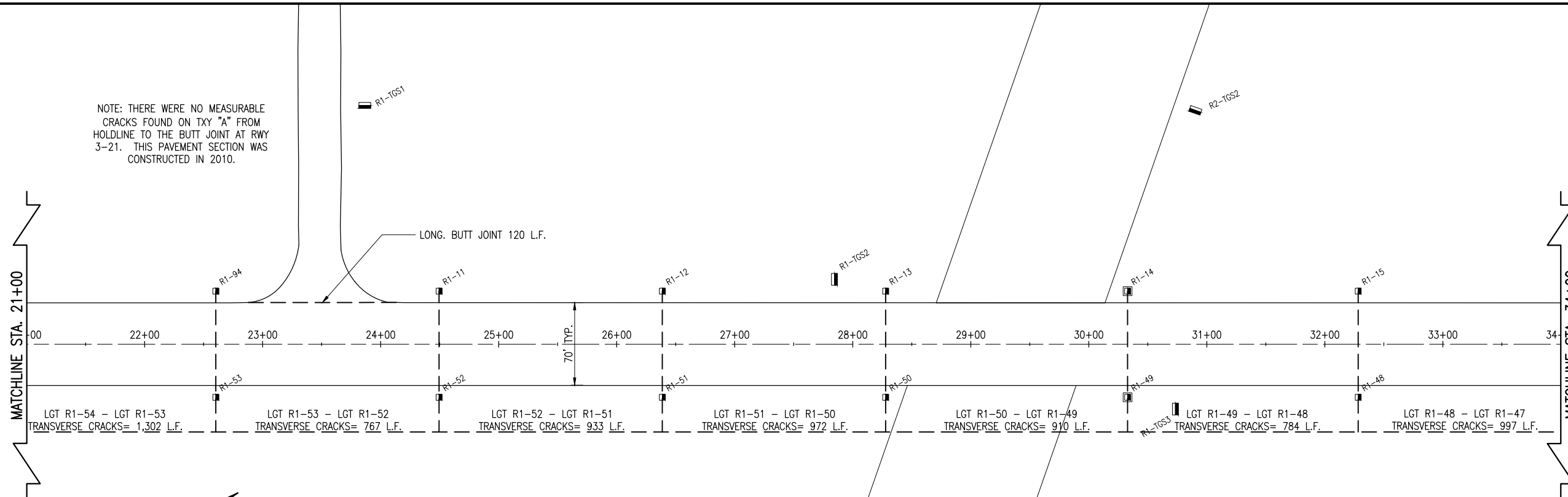
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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

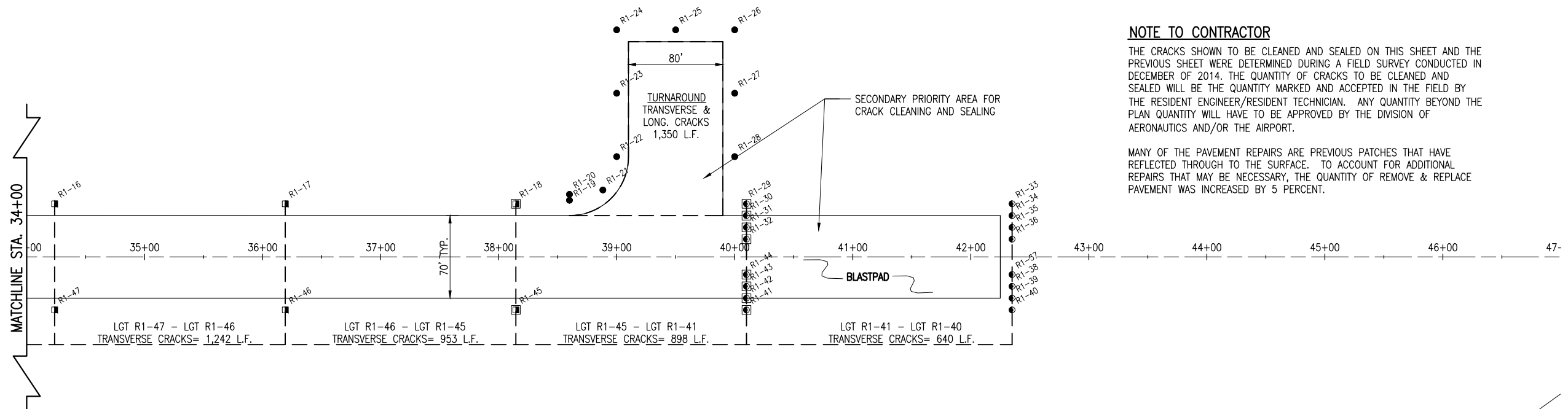
ISSUE: APRIL 15, 2016
PROJECT NO: 14A0045D
CAD FILE: C-121-CON-2.DWG
DESIGN BY: JRH 12/11/2014
DRAWN BY: JRH 12/11/2014
REVIEWED BY: RAW 3/12/2016
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SHEET TITLE

PROPOSED CONSTRUCTION PLANS STA. 0+00 TO 21+00

NOTE: THERE WERE NO MEASURABLE CRACKS FOUND ON TXY "A" FROM HOLDLINE TO THE BUTT JOINT AT RWY 3-21. THIS PAVEMENT SECTION WAS CONSTRUCTED IN 2010.



0' 25' 50' 100'
HALF SIZE SCALE: 1" = 100'
FULL SIZE SCALE: 1" = 50'



NOTE TO CONTRACTOR

THE CRACKS SHOWN TO BE CLEANED AND SEALED ON THIS SHEET AND THE PREVIOUS SHEET WERE DETERMINED DURING A FIELD SURVEY CONDUCTED IN DECEMBER OF 2014. THE QUANTITY OF CRACKS TO BE CLEANED AND SEALED WILL BE THE QUANTITY MARKED AND ACCEPTED IN THE FIELD BY THE RESIDENT ENGINEER/RESIDENT TECHNICIAN. ANY QUANTITY BEYOND THE PLAN QUANTITY WILL HAVE TO BE APPROVED BY THE DIVISION OF AERONAUTICS AND/OR THE AIRPORT.

MANY OF THE PAVEMENT REPAIRS ARE PREVIOUS PATCHES THAT HAVE REFLECTED THROUGH TO THE SURFACE. TO ACCOUNT FOR ADDITIONAL REPAIRS THAT MAY BE NECESSARY, THE QUANTITY OF REMOVE & REPLACE PAVEMENT WAS INCREASED BY 5 PERCENT.

CRACK REPAIR & SEALING MAINTENANCE ON RUNWAY 3-21

SBG PROJECT NO.:
3-17-SBGP-111/120
IDA No.: AAA-4438

Contract No. LO030

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PROPOSED CONSTRUCTION PLANS STA. 21+00 TO 42+25