

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	(L,K)RS-2	EFFINGHAM	58	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO.	74317	

*58+1=59

D-97-033-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAS ROUTE 2796 (US 40)
SECTION (L,K)RS-2

MILLING AND RESURFACING
EFFINGHAM COUNTY

C-97-078-08

FOR INDEX OF SHEETS, SEE SHEET NO. 2

STATION EQUATIONS:

- STA. 1897 + 10 (BK) = STA. 1202 + 24 (AH)
- STA. 1219 + 01.8 (BK) = STA. 1219 + 02 (AH)
- STA. 1366 + 28 (BK) = STA. 1366 + 00 (AH)

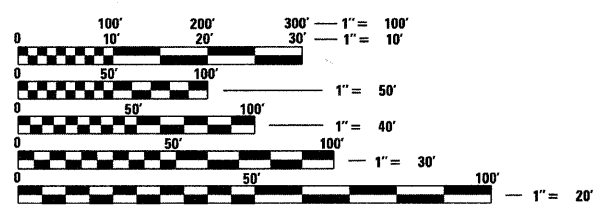
PAVING OMISSIONS

- STA. 1320 + 41.00 TO STA. 1322 + 26.00
- STA. 1333 + 85.00 TO STA. 1337 + 15.00
- STA. 1342 + 35.00 TO STA. 1345 + 65.00
- STA. 1386 + 50.00 TO STA. 1399 + 75.00

2009 ADT = 4,900

FAS RTE. 2796 (US RTE 40)
SECTION (L,K)RS-2
EFFINGHAM COUNTY
BEGINS STA. 1889 + 18

FAS RTE. 2796 (US RTE 40)
SECTION (L,K)RS-2
EFFINGHAM COUNTY
ENDS STA. 1450 + 65

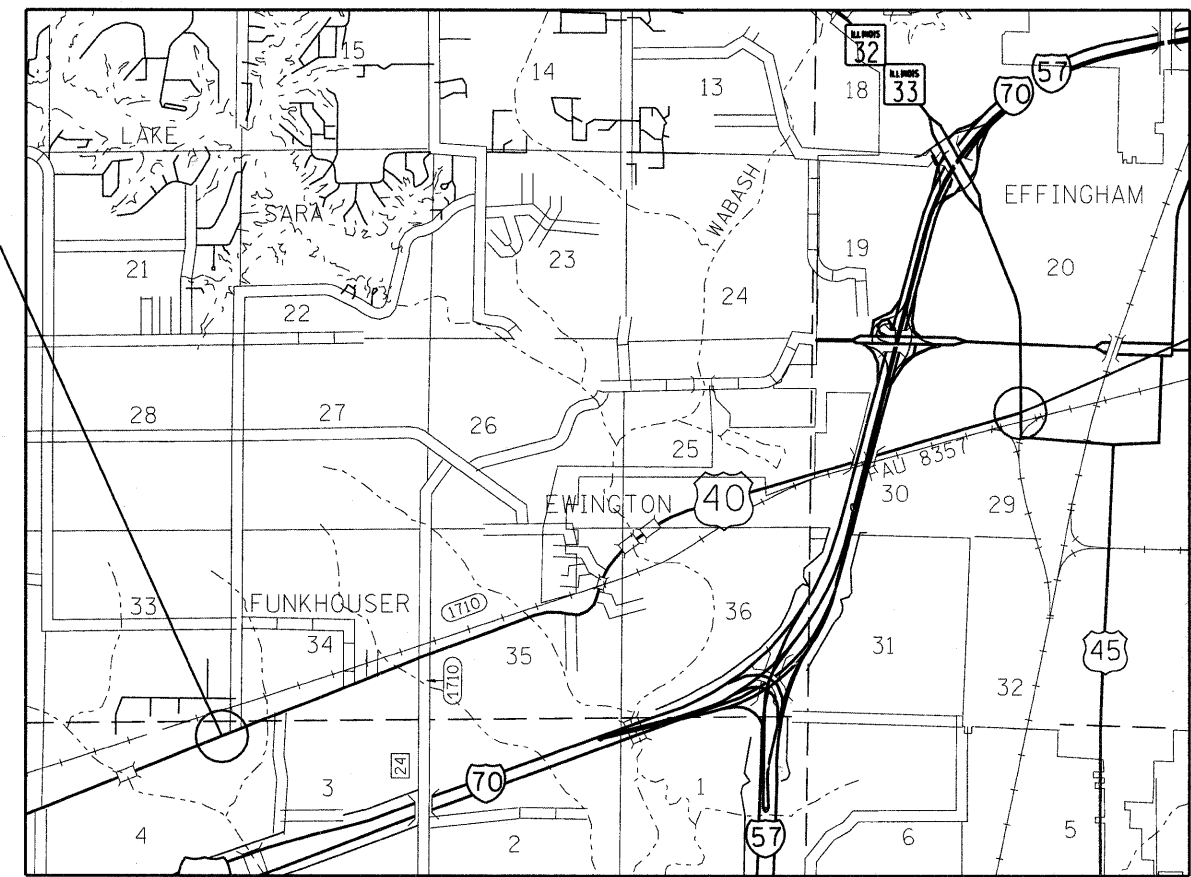


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

TOWNSHIP: SUMMIT

PROJECT ENGINEER: TOM RONAN
PROJECT MANAGER: JENNIFER WENTHE
PHONE NO.: (217) 342-8361
CONTRACT NO. 74317



GROSS LENGTH = 25,661 FT. = 4.86 MILE
NET LENGTH = 23,491 FT. = 4.45 MILE



LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED 3-18 20 10
By: *Boon J. Onakell*
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 7 20 10
By: *Scott E. Stitt, P.E.*
Acting ENGINEER OF DESIGN AND ENVIRONMENT

May 7 20 10
By: *Christina M. Roeder*
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

GENERAL NOTES

THIS SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PLANS; THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JANUARY 1, 2007; AND THE SPECIAL PROVISIONS INCLUDED IN THE PROPOSAL.

THIS PROJECT IS LOCATED ON FAS ROUTE 2796 (US 40) IN EFFINGHAM COUNTY, FROM LAKE SARA ROAD TO HENRIETTA STREET IN EFFINGHAM. THE WORK INCLUDED IN SECTION (L,K)RS-2 CONSISTS OF PAVEMENT PATCHING, MILLING, HOT-MIX ASPHALT RESURFACING, HMA AND AGGREGATE SHOULDERS, PAVEMENT MARKING, MINOR BRIDGE REPAIRS, AND ANY OTHER WORK NECESSARY TO COMPLETE THE SECTION.

PAVEMENT MARKING SHALL BE APPLIED IN ACCORDANCE WITH SECTION 780 OF THE STANDARD SPECIFICATIONS. SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE MILLED SURFACE, BITUMINOUS MATERIALS (PRIME COAT), LEVELING BINDER, AND HOT-MIX ASPHALT SURFACE COURSE AS SPECIFIED IN SECTION 703 OF THE STANDARD SPECIFICATIONS. TEMPORARY TAPE SHALL BE USED ON THE SURFACE COURSE AND PAINT SHALL BE USED ON MILLED SURFACES.

THE TOTAL QUANTITY OF PAINT PAVEMENT MARKING-LINE 4" CONSISTS OF 37,748 FEET OF WHITE AND 27,126 FEET OF YELLOW.

THE TOTAL QUANTITY OF THERMOPLASTIC PAVEMENT MARKING-LINE 4" CONSISTS OF 10,180 FEET OF WHITE AND 7,506 FEET OF YELLOW.

THE TOTAL QUANTITY OF THERMOPLASTIC PAVEMENT MARKING-LINE 6" CONSISTS OF 125 FEET OF WHITE.

THE TOTAL QUANTITY OF THERMOPLASTIC PAVEMENT MARKING-LINE 12" CONSISTS OF 594 FEET OF YELLOW.

THE TOTAL QUANTITY OF THERMOPLASTIC PAVEMENT MARKING-LETTERS AND SYMBOLS CONSISTS OF 31 SQ FEET OF WHITE.

THE TOTAL QUANTITY OF RAISED REFLECTIVE PAVEMENT MARKERS IS 36 ONE-WAY CRYSTAL, 156 ONE-WAY AMBER, AND 311 TWO-WAY AMBER.

THE MATERIAL USED FOR AGGREGATE SHOULDERS, TYPE B SHALL BE CRUSHED STONE, CRUSHED CONCRETE, OR RAP. THE MATERIAL USED FOR AGGREGATE SURFACE COURSE, TYPE B SHALL BE CRUSHED STONE OR CRUSHED CONCRETE.

THE EXISTING PAVEMENT SHALL BE PATCHED IN ACCORDANCE WITH SECTION 442 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS. THE QUANTITY OF PATCHING SHOWN ON THE PLANS IS AN ESTIMATE ONLY AND THE FINAL LOCATIONS AND QUANTITY SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

THE PAY ITEM TEMPORARY RAMP HAS BEEN INCLUDED FOR THE CONSTRUCTION OF TEMPORARY RAMPS IN ACCORDANCE WITH ARTICLE 406.08 OF THE STANDARD SPECIFICATIONS. THE COST SHALL INCLUDE BOTH THE INSTALLATION AND THE REMOVAL OF THE TEMPORARY RAMPS.

TEMPORARY TAPERS WILL BE REQUIRED AFTER THE HMA SURFACE REMOVAL, 2 1/4" OPERATION HAS BEEN PERFORMED AND ANY SUBSEQUENT RESURFACING OPERATION NEEDING TEMPORARY TAPERS. THE CONTRACTOR SHALL CONSTRUCT TEMPORARY TAPERS (MAX. 2 FEET IN LENGTH) AT ENTRANCES IN AREAS THAT HAVE BEEN MILLED 2 1/4". THIS WORK SHALL BE INCLUDED IN THE COST OF HMA SURFACE REMOVAL.

THE RESIDENT ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE CURING TIME FOR THE VARIOUS HOT-MIX ASPHALT LIFTS.

THE CONTRACTOR SHALL PROVIDE INTERNET ACCESSIBILITY TO THE HOT-MIX ASPHALT PLANT QUALITY CONTROL LAB SO THAT HOT-MIX ASPHALT PLANT REPORTS CAN BE E-MAILED TO THE DISTRICT HEADQUARTERS. THIS WORK SHALL BE INCLUDED IN THE COST OF ALL HOT-MIX ASPHALT ITEMS.

THE CONTRACTOR SHALL USE EITHER RC-70, SS1H, OR SS-1HP, APPLIED AT THE RATE DIRECTED BY THE ENGINEER, FOR THE PAY ITEM BITUMINOUS MATERIALS (PRIME COAT).

A UNIFORMLY STRAIGHT SAW CUT SHALL BE MADE AT LOCATIONS WHERE PROPOSED NEW CONSTRUCTION WILL ABUT EXISTING HOT-MIX ASPHALT SURFACES. THE SAW CUT SHALL BE MADE FULL DEPTH THROUGH THE EXISTING SURFACE. THIS WORK WILL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT ITEMS INVOLVED AND NO EXTRA COMPENSATION WILL BE ALLOWED.

BASE COURSE WIDENING SHALL INCLUDE ALL EXCAVATION FOR CONSTRUCTION OF THE WIDENING. THE WIDENING SHALL, AT THE CONTRACTOR'S OPTION BE CONSTRUCTED OF EITHER PORTLAND CEMENT CONCRETE 8" THICK, OR HOT-MIX ASPHALT, 10" THICK. THE WIDENING SHALL REMAIN IN PLACE AT THE CONCLUSION OF THE JOB.

EXCAVATION REQUIRED FOR THE CONSTRUCTION OF BITUMINOUS AND AGGREGATE SHOULDERS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER UNIT FOR EXCAVATING AND GRADING EXISTING SHOULDERS, WHICH PRICE SHALL INCLUDE DISPOSAL OF EXCESS MATERIAL OFF RIGHT OF WAY.

THE PAY ITEM HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4" ASSUMES USING TWO PASSES TO ALLEVIATE THE 2" LANE DIFFERENTIAL.

GENERAL NOTES (Cont'd)

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

MIXTURE USE:	SURFACE COURSE	LEVEL BINDER
PG GRADE:	PG 64-22	PG 64-22
DESIGN AIR VOIDS:	4.0% @ NDESIGN = 70	4.0% @ NDESIGN = 70
MIXTURE COMPOSITION:	IL-9.5	IL-9.5
FRICTION AGGREGATE:	MIXTURE D	N/A

MIXTURE USE:	INCIDENTAL	CLASS D PATCHING
PG GRADE:	PG 64-22	PG 64-22
DESIGN AIR VOIDS:	4.0% @ NDESIGN = 70	4.0% @ NDESIGN = 70
MIXTURE COMPOSITION:	IL-9.5	IL-19.0
FRICTION AGGREGATE:	MIXTURE C	N/A

MIXTURE USE:	HMA SHOULDERS
PG GRADE:	PG 58-22
DESIGN AIR VOIDS:	4.0% @ NDESIGN = 30
MIXTURE COMPOSITION:	IL-19.0L
FRICTION AGGREGATE:	N/A

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN THE CALCULATING PLAN QUANTITIES:

AGGREGATE SURFACE COURSE	2.05 TONS/CU YD
AGGREGATE SHOULDERS	2.05 TONS/CU YD
BITUMINOUS MATERIALS (PRIME COAT)	0.10 GAL/SQ YD
AGGREGATE (PRIM. COAT)	4 LBS/SQ YD
HOT-MIX ASPHALT	112 LBS/SQ YD/INCH

INDEX OF SHEETS

SHEET NO.	ITEM
1	COVER SHEET
2	INDEX OF SHEETS AND GENERAL NOTES
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12	CONSTRUCTION DETAILS
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17-19	TIE POINTS
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29-36	STAGE CONSTRUCTION - SN 051-0016 & 051-0017
37-40	PAVEMENT MARKING DETAIL
41-44	ILLUMINATED DELINEATOR SYSTEM DETAILS
45-58	BRIDGE REPAIR PLANS

THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED AFTER SHEET NO. 58:

STD. NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
667101-01	PERMANENT SURVEY MARKERS
701001-02	OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' AWAY
701006-03	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011-02	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
701311-03	LANE CLOSURE 2L, 2W, MOVING OPERATIONS - DAY ONLY
701316-04	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR, FOR SPEEDS > 45 MPH
701321-10	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER
701326-03	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS >= 45 MPH
701336-05	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS >= 45 MPH
701901-01	TRAFFIC CONTROL DEVICES
704001-06	TEMPORARY CONCRETE BARRIER
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS

SUMMARY OF QUANTITIES			100% STATE	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	RURAL 1000	URBAN 1000	025-0087 SFTY-2A	025-0079 SFTY-2A	025-0086 SFTY-2A
X0320913	ILLUMINATED DELINEATOR SYSTEM	L SUM	1	1				
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	390	297	93			
35650700	BASE COURSE WIDENING	SQ YD	288			288		
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	120	100	20			
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	7887	5614	2273			
40600300	AGGREGATE (PRIME COAT)	TON	157	112	45			
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	2908	2046	862			
40600895	CONSTRUCTING TEST STRIP	EACH	1	1				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	600	513	87			
40600990	TEMPORARY RAMP	SQ YD	301	121	180			
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	6626	4716	1910			
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	228	108	120			
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	20252	5933	14319			
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	48716	42550	6166			
44000400	GUTTER REMOVAL	FOOT	301			301		
44004250	PAVED SHOULDER REMOVAL	SQ YD	656	569	87			
44201827	CLASS D PATCHES, TYPE II, 15 INCH	SQ YD	408	344	64			
44201831	CLASS D PATCHES, TYPE III, 15 INCH	SQ YD	440	251	189			
44201833	CLASS D PATCHES, TYPE IV, 15 INCH	SQ YD	113	26	87			
48101200	AGGREGATE SHOULDERS, TYPE B	TON	1241	942	299			
48203023	HOT-MIX ASPHALT SHOULDERS, 6 1/2"	SQ YD	9284	7122	2162			
50102400	CONCRETE REMOVAL	CU YD	13.2			10.7	2.5	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	14.7			12.2	2.5	
50300260	BRIDGE DECK GROOVING	SQ YD	733			733		
50300300	PROTECTIVE COAT	SQ YD	785			785		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2230			1900	330	
50800515	BAR SPLICERS	EACH	32			28	4	
51500200	RELOCATING NAME PLATES	EACH	1			1		
52000110	PREFORMED JOINT STRIP SEAL	FOOT	136			94	42	
67000500	ENGINEER'S FIELD OFFICE, TYPE B	CAL MO	5	1	1	1	1	1
67100100	MOBILIZATION	L SUM	1	0.2	0.2	0.2	0.2	0.2
70100100	TRAFFIC CONTROL AND PROTECTION, STANDARD 701316	EACH	1				1	
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1			1		
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	0.4	0.4			0.2
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1			1		
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	0.7	0.3			
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	3	1	1	1		
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	2			1	1	
70106700	TEMPORARY RUMBLE STRIP	EACH	12			6	6	

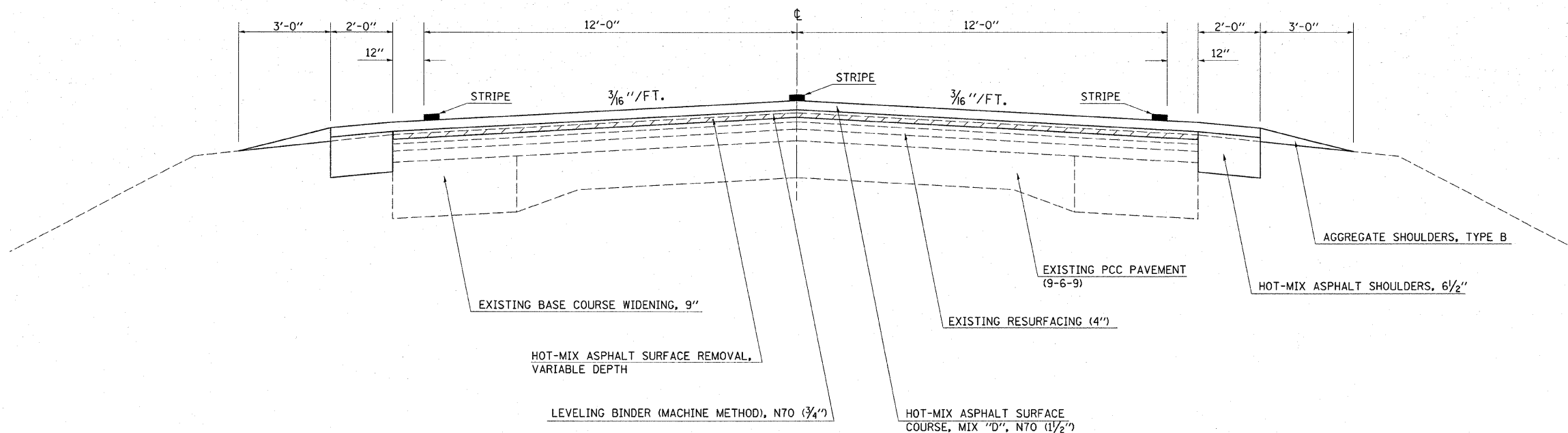
SUMMARY OF QUANTITIES (Cont'd)			100% STATE	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	RURAL 1000	URBAN 1000	025-0087 SFTY-2A	025-0079 SFTY-2A	025-0086 SFTY-2A
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	10288	7168	3120			
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	31		31			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	82559	59560	22999			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	125		125			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	594		594			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	857	597	260			
70400100	TEMPORARY CONCRETE BARRIER	FOOT	425			425		
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	337			337		
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	31		31			
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	17686		17686			
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	125		125			
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	594		594			
78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	64873	59561	5312			
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	503	255	248			
80500100	SERVICE INSTALLATION, TYPE A	EACH	1	1				
81012600	CONDUIT IN TRENCH, 2" DIA., PVC	FOOT	2963	2963				
81013000	CONDUIT IN TRENCH, 4" DIA., PVC	FOOT	52	52				
81021550	CONDUIT, AUGERED 2" DIA., PVC	FOOT	186	186				
81021590	CONDUIT, AUGERED 4" DIA., PVC	FOOT	84	84				
81100400	CONDUIT ATTACHED TO STRUCTURE, 1 1/4" DIA., GALVANIZED STEEL	FOOT	88	88				
81100600	CONDUIT ATTACHED TO STRUCTURE, 2" DIA., GALVANIZED STEEL	FOOT	195	195				
81500100	GULFBOX JUNCTION	EACH	5	5				
81702120	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 8	FOOT	9964	9964				
X0323076	SILICONE JOINT SEALER, 1 3/4"	FOOT	42				42	
X0323077	SILICONE JOINT SEALER, 2 3/4"	FOOT	84					84
X0325303	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SO FT	25			25		
XZ193500	BRIDGE DECK MICROSILICA CONCRETE OVERLAY 2 1/4"	SO YD	753			753		
Z0006204	BRIDGE DECK HYDRO-SCARIFICATION 1/2"	SO YD	753			753		
Z0025500	FURNISHING AND INSTALLING PROPERTY MARKERS	EACH	21	9	12			
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2			2		
Z0030260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	1			1		
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2			2		

*

* SPECIALTY ITEMS

TYPICAL CROSS SECTION

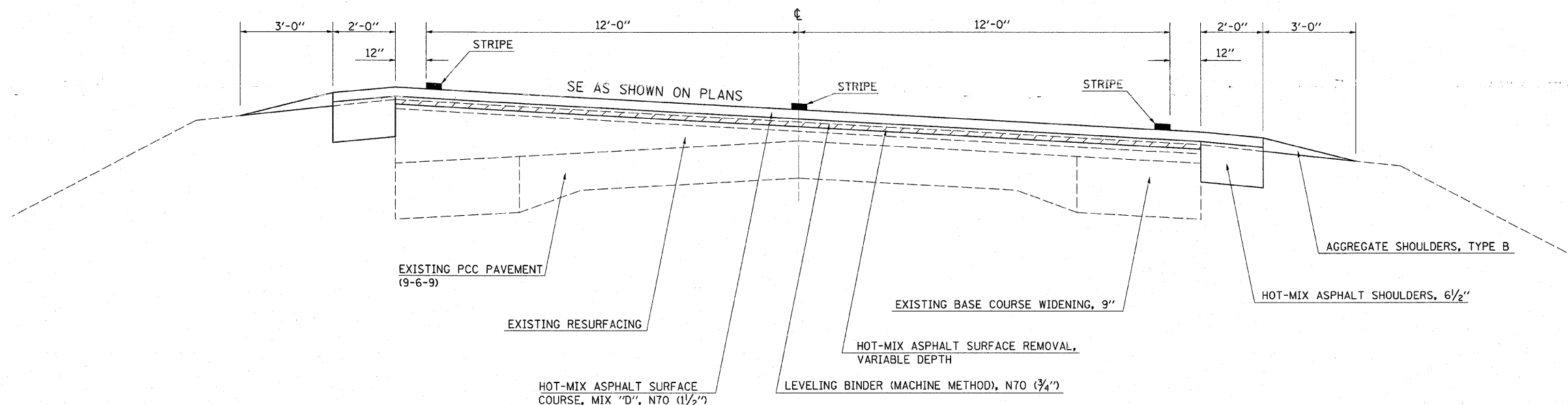
STATION 1889+18.00 TO STATION 1300+97.10
 STATION 1305+55.02 TO STATION 1308+07.34
 STATION 1365+90.00 TO STATION 1377+38.00
 STATION 1399+75.00 TO STATION 1414+75.00



NOTE: NOT DRAWN TO SCALE

TYPICAL CROSS SECTION

STATION 1300+97.10 TO STATION 1305+55.02 (SE LT HIGH)
 STATION 1308+07.34 TO STATION 1310+85.00 (SE RT HIGH)

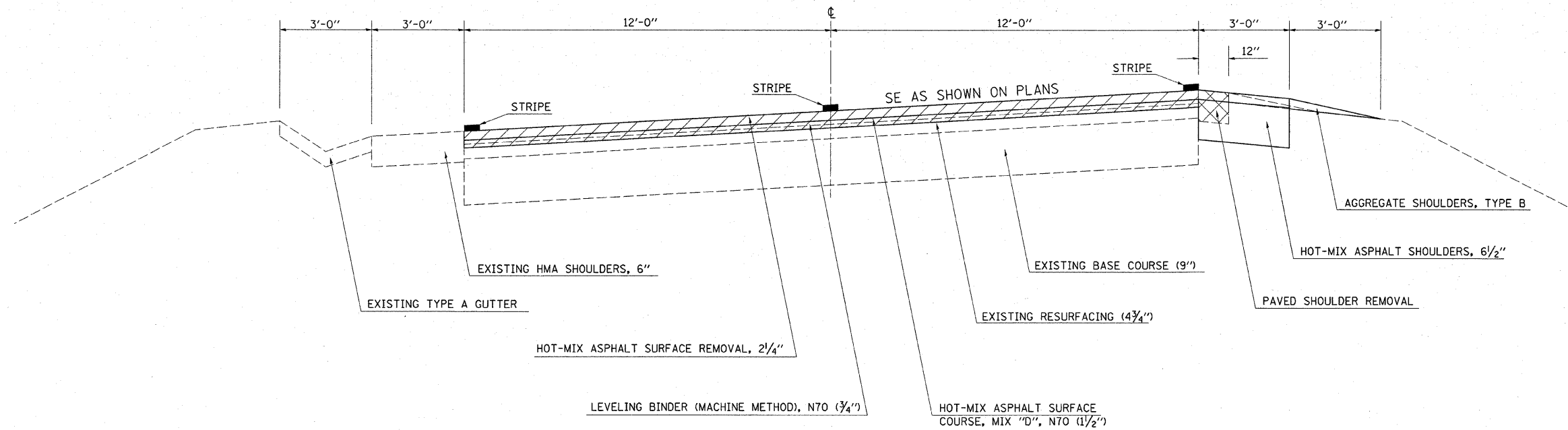


NOTE: NOT DRAWN TO SCALE

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	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -					CONTRACT NO. 74317					
	PLOT DATE = 3/17/2018	DATE -	REVISED -					ILLINOIS FED. AID PROJECT					

TYPICAL CROSS SECTION

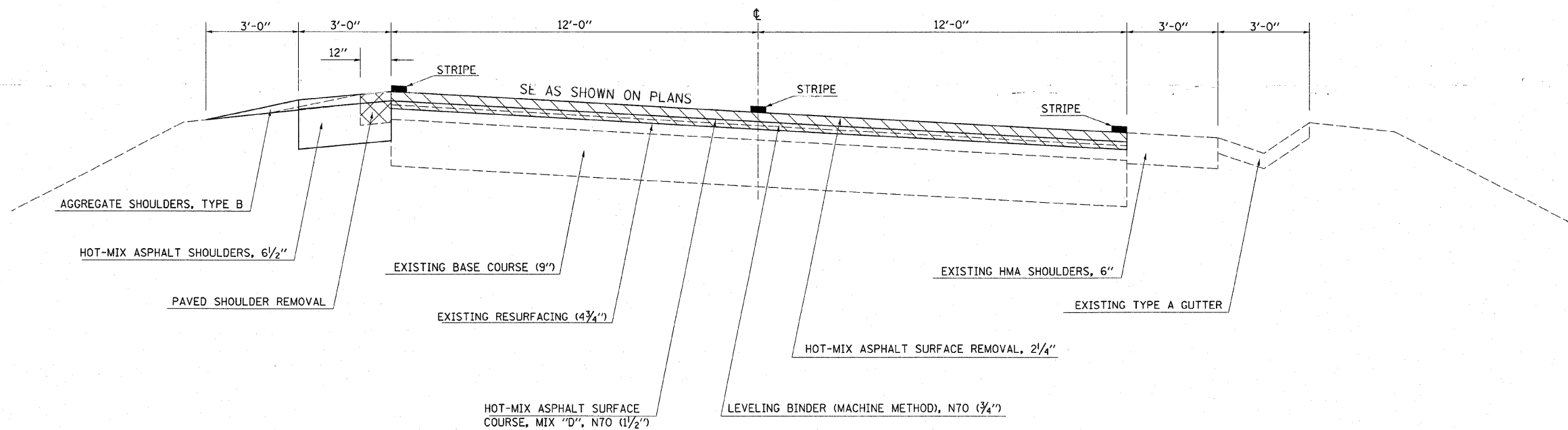
STATION 1310+85 TO STATION 1320+41



NOTE: NOT DRAWN TO SCALE

TYPICAL CROSS SECTION

STATION 1322+26 TO STATION 1330+05

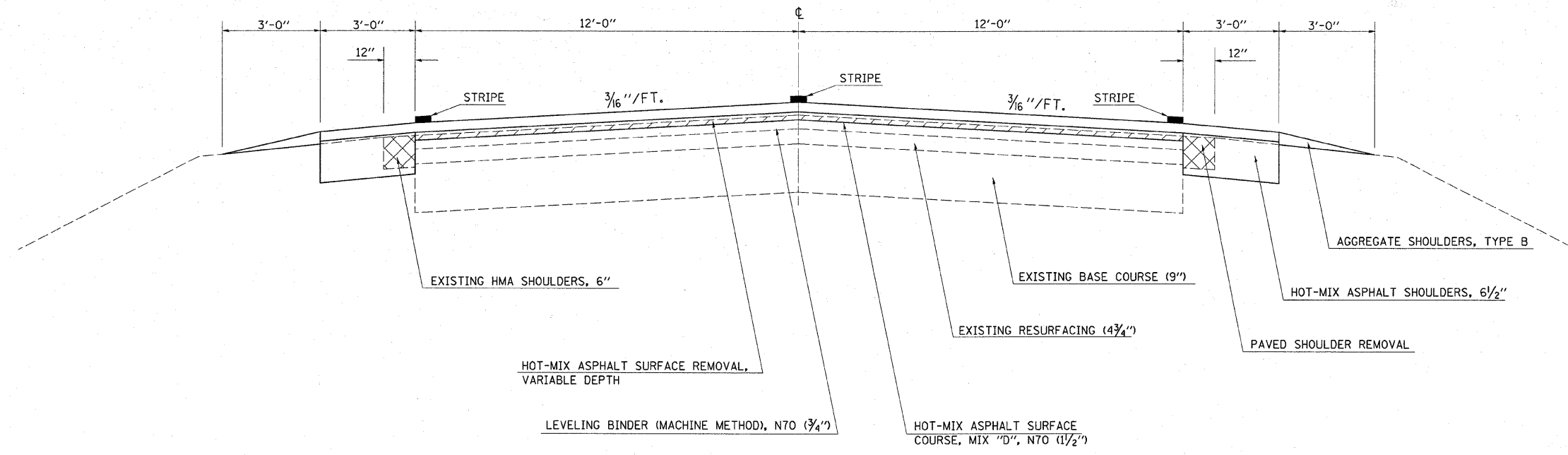


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	PLOT DATE = 3/17/2010	DATE -	REVISED -							ILLINOIS FED. AID PROJECT			

TYPICAL CROSS SECTION

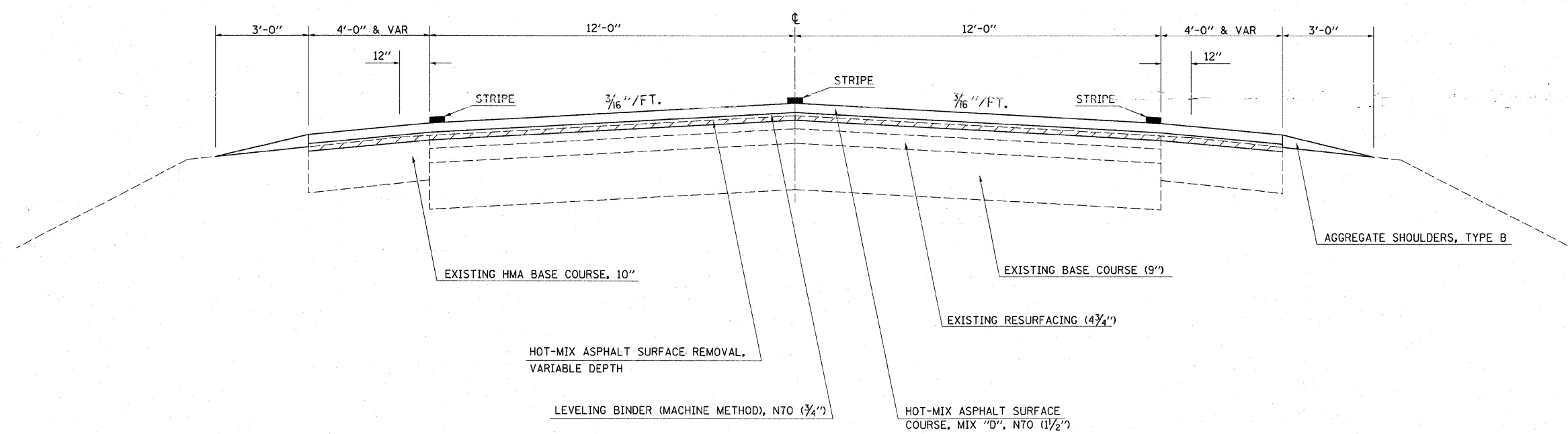
STATION 1330+05 TO STATION 1333+50
STATION 1346+00 TO STATION 1359+50



NOTE: NOT DRAWN TO SCALE

TYPICAL CROSS SECTION

STATION 1337+50 TO STATION 1342+00

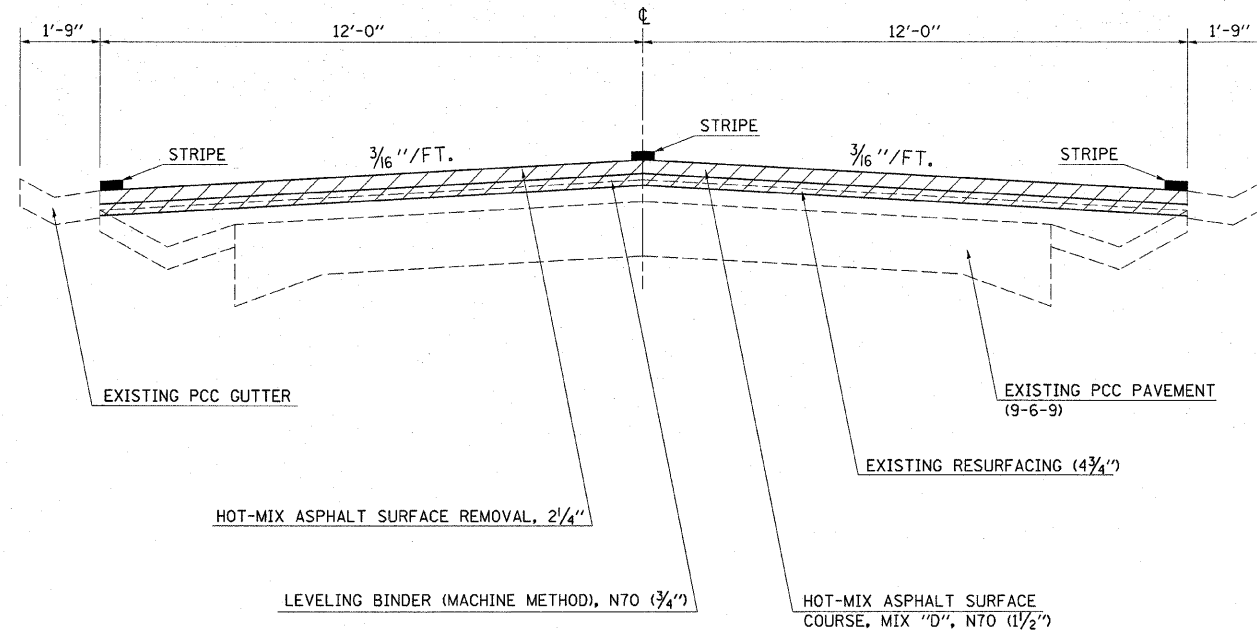


NOTE: NOT DRAWN TO SCALE

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PLOT SCALE = 20.0000' / IN.		CHECKED -	REVISED -		SCALE: NA	SHEET NO. 3 OF 7 SHEETS	STA.	TO STA.	CONTRACT NO. 74317			
PLOT DATE = 3/17/2010		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

TYPICAL CROSS SECTION

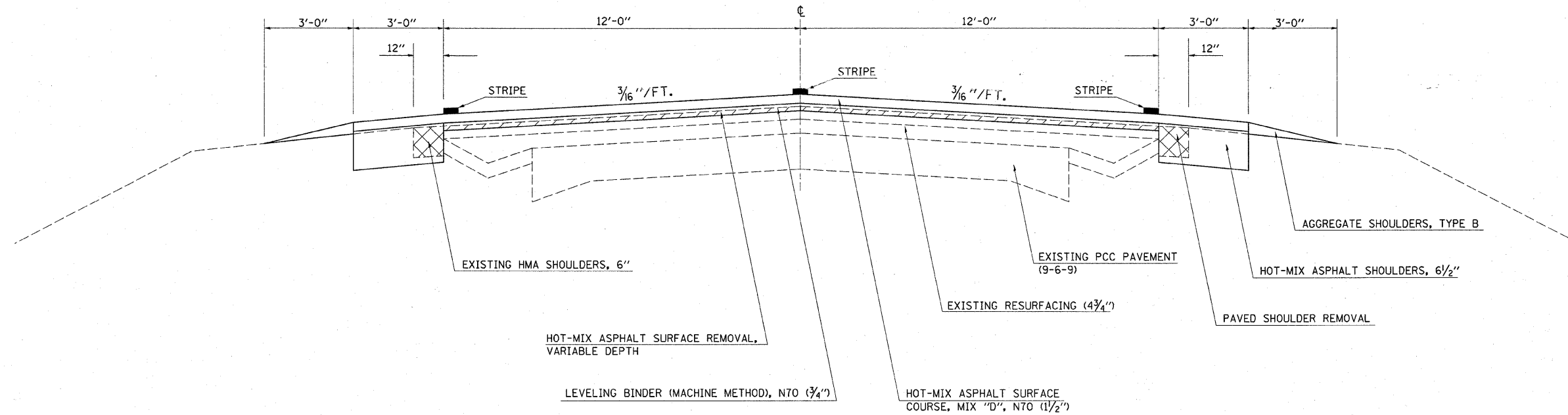
STATION 1359+50 TO STATION 1365+90
 STATION 1377+38 TO STATION 1381+30
 STATION 1385+21 TO STATION 1386+50



NOTE: NOT DRAWN TO SCALE

TYPICAL CROSS SECTION

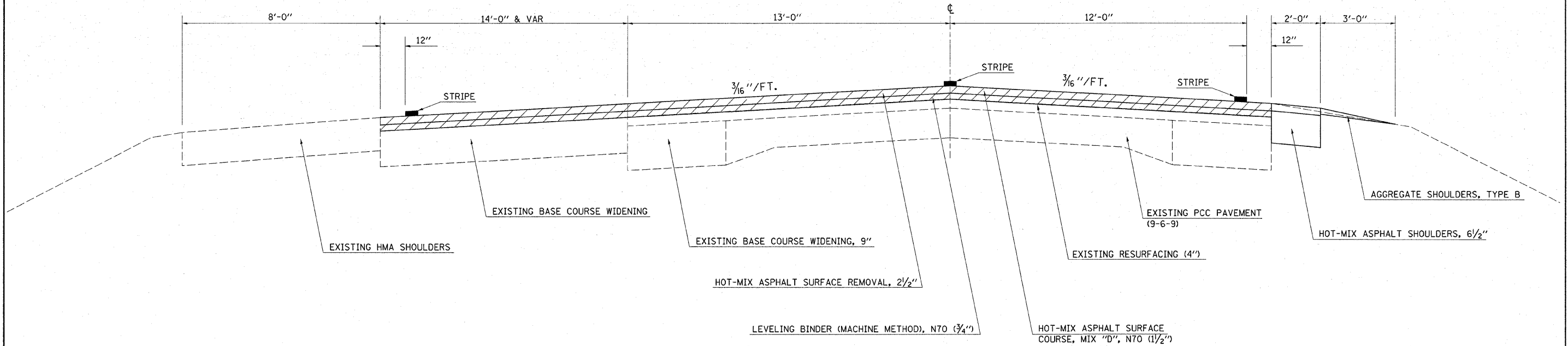
STATION 1381+30 TO STATION 1385+21



NOTE: NOT DRAWN TO SCALE

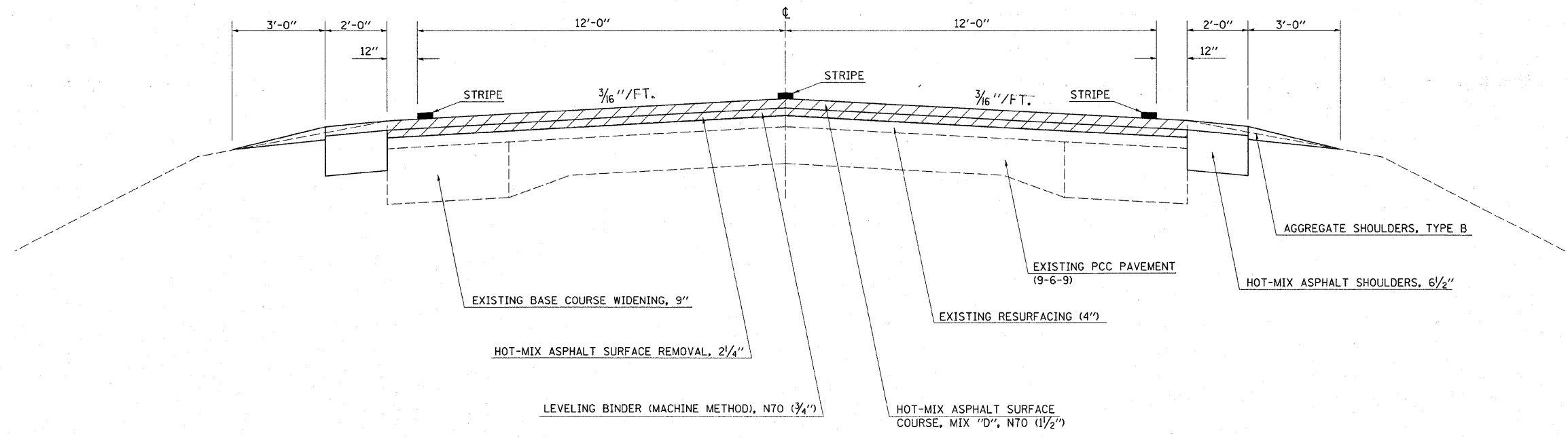
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	PLOT SCALE = 20,0000' / IN.	CHECKED -	REVISED -										
	PLOT DATE = 3/17/2010	DATE -	REVISED -										
											CONTRACT NO. 74317		
											ILLINOIS FED. AID PROJECT		

TYPICAL CROSS SECTION
STATION 1414+75 TO STATION 1431+70



NOTE: NOT DRAWN TO SCALE

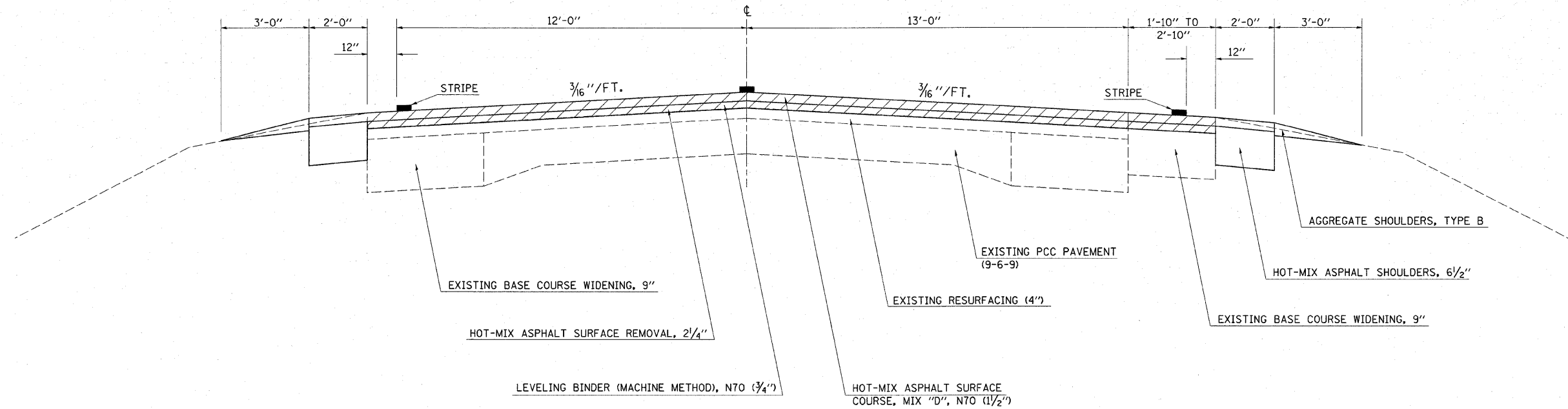
TYPICAL CROSS SECTION
STATION 1431+70 TO STATION 1443+50



NOTE: NOT DRAWN TO SCALE

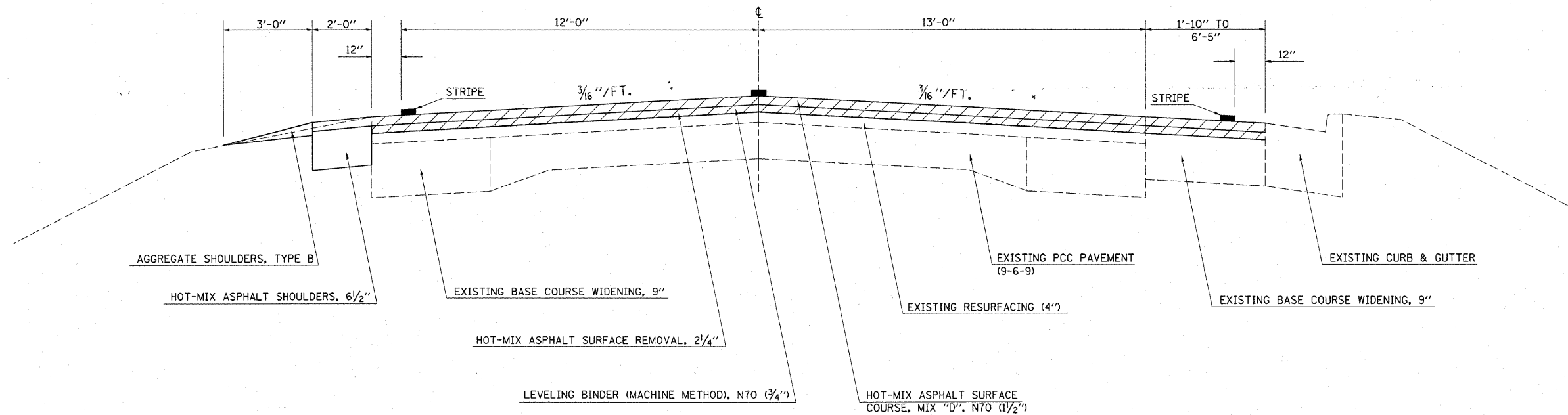
FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\PWIDOT\SWARTZRW\dms54414\sh	tpicoals_74317.dgn	DRAWN -	REVISED -					2796	(L,K)RS-2	EFFINGHAM	58	9
PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -	REVISED -		SCALE: NA			CONTRACT NO. 74317				
PLOT DATE = 3/17/2018	DATE -	REVISED -	REVISED -		SHEET NO. 5 OF 7 SHEETS			ILLINOIS FED. AID PROJECT				
					STA. TO STA.							

TYPICAL CROSS SECTION
STATION 1443+50 TO STATION 1445+00



NOTE: NOT DRAWN TO SCALE

TYPICAL CROSS SECTION
STATION 1445+00 TO STATION 1448+25

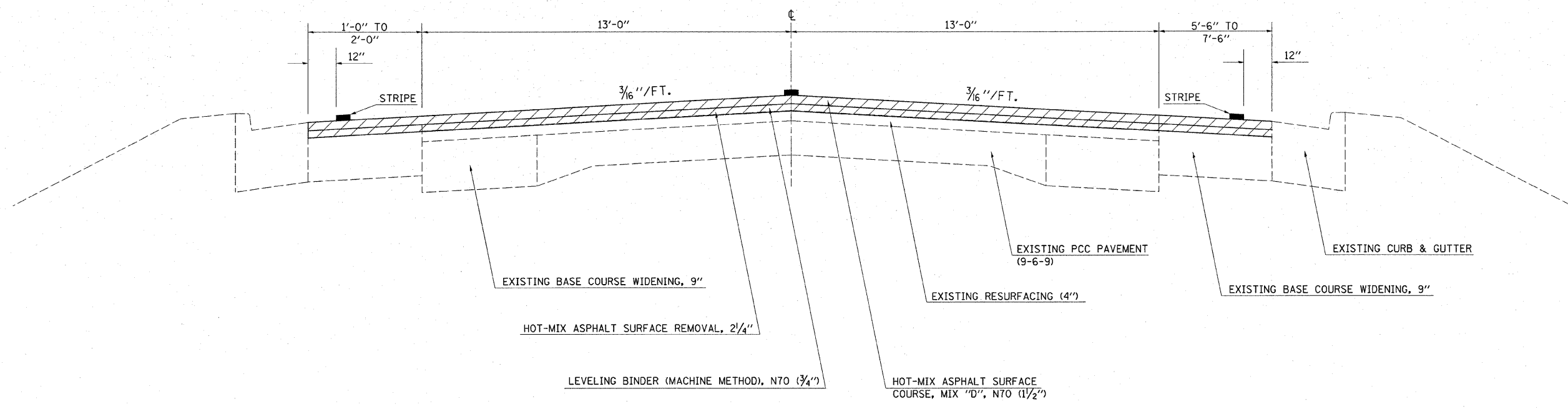


NOTE: NOT DRAWN TO SCALE

FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\PW100T\SWARTZRW\dms54414\sh	typicals_74317.dgn	DRAWN -	REVISED -					2796	(L,K)RS-2	EFFINGHAM	58	10
PLOT SCALE = 20,0000 ' / IN.	CHECKED -	REVISED -	REVISED -		SCALE: NA SHEET NO. 6 OF 7 SHEETS STA. TO STA.			CONTRACT NO. 74317				
PLOT DATE = 3/17/2010	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT							

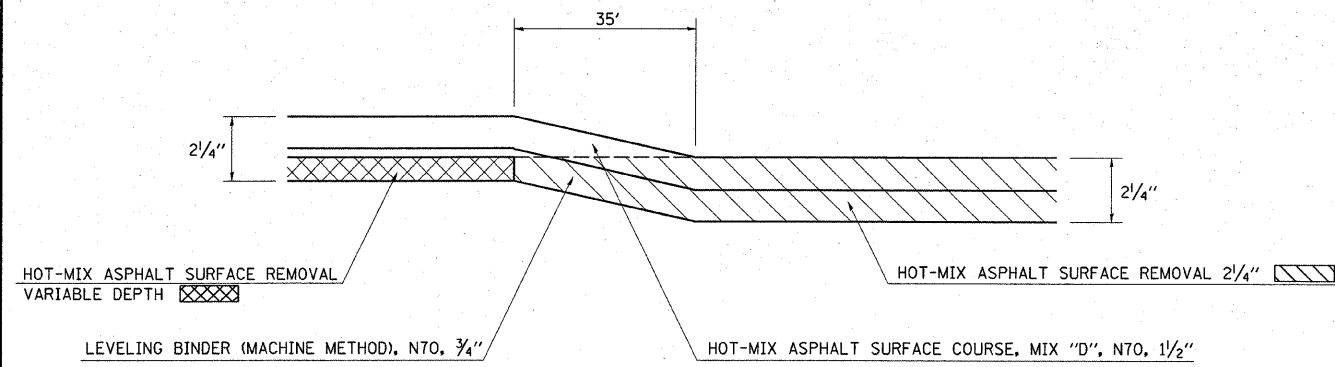
TYPICAL CROSS SECTION

STATION 1448+25 TO STATION 1450+65



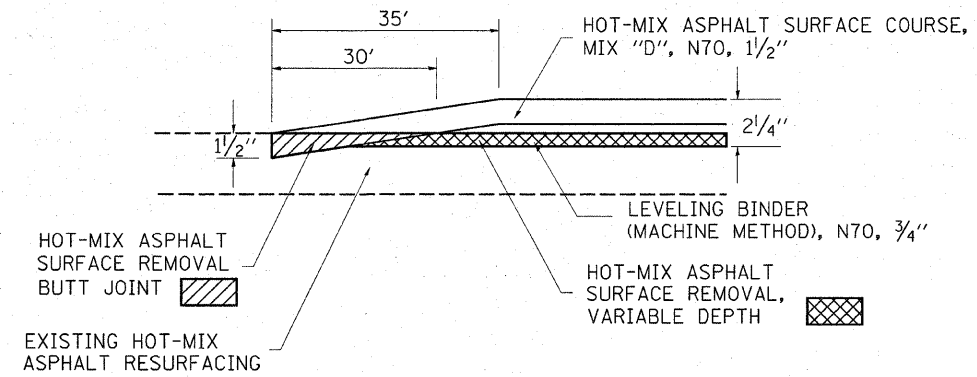
NOTE: NOT DRAWN TO SCALE

FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\PWIDOT\SWARTZRW\dms54414\sh	typicals_74317.dgn	DRAWN -	REVISED -		SCALE: NA	SHEET NO. 7 OF 7 SHEETS	STA.	2796	(L,K)RS-2	EFFINGHAM	58	11
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -				TO STA.					
	PLOT DATE = 3/17/2018	DATE -	REVISED -									
											CONTRACT NO. 74317	
											ILLINOIS FED. AID PROJECT	



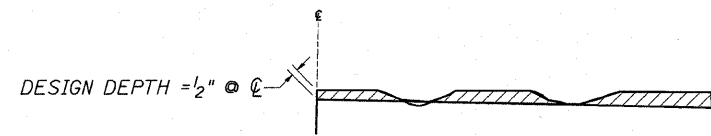
MILLING TRANSITION DETAIL

- STA. 1310+50.00 TO STA. 1310+85.00
- STA. 1318+75.00 TO STA. 1319+10.00
- STA. 1323+91.00 TO STA. 1324+26.00
- STA. 1330+05.00 TO STA. 1330+40.00
- STA. 1359+15.00 TO STA. 1359+50.00
- STA. 1365+90.00 TO STA. 1366+25.00
- STA. 1377+03.00 TO STA. 1377+38.00
- STA. 1381+30.00 TO STA. 1381+65.00
- STA. 1384+86.00 TO STA. 1385+21.00
- STA. 1414+40.00 TO STA. 1414+75.00



BUTT JOINT DETAIL

- STA. 1889+18.00 TO STA. 1889+53.00
- STA. 1333+50.00 TO STA. 1333+85.00
- STA. 1337+15.00 TO STA. 1337+50.00
- STA. 1342+00.00 TO STA. 1342+35.00
- STA. 1345+65.00 TO STA. 1346+00.00
- STA. 1399+75.00 TO STA. 1400+10.00



NOTES:

1. MILLING SHALL BE DONE TO ATTAIN A $\frac{3}{16}$ "/FT. SLOPE IN CROWN SECTIONS
2. EXISTING S.E. AND S.E. TRANSITIONS SHALL BE MAINTAINED UNLESS OTHERWISE SHOWN ON THE PLANS.
3. MILLING TO THE BOTTOM OF WHEEL RUTS SHALL NOT BE NECESSARY UNLESS REQUIRED TO OBTAIN SLOPE OR THE DESIGN DEPTH AT CENTERLINE.
4. THE AVERAGE DEPTH OF MILLING IS ESTIMATED TO BE $\frac{1}{2}''$ BUT MAY VARY IN ISOLATED LOCATIONS.

HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH) DETAIL

FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CONSTRUCTION DETAILS			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd:\pw\work\PWIDOT\SWARTZRW\dms54414\sh	details_74317.dgn	DRAWN -	REVISED -					2796	(L,K)RS-2	EFFINGHAM	58	12
	PLOT SCALE = 20,0000 ' / IN.	CHECKED -	REVISED -		SCALE: NA SHEET NO. 1 OF 1 SHEETS STA. TO STA.			CONTRACT NO. 74317				
	PLOT DATE = 3/17/2018	DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

RESURFACING SCHEDULE

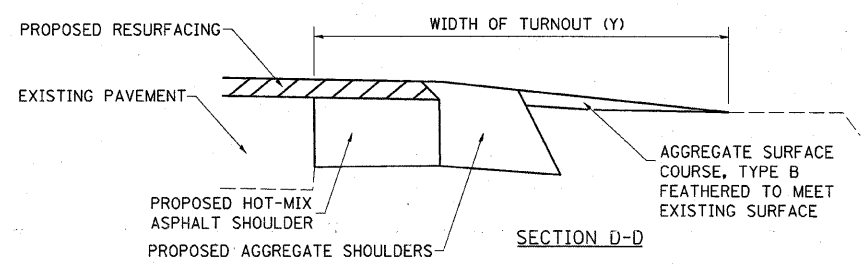
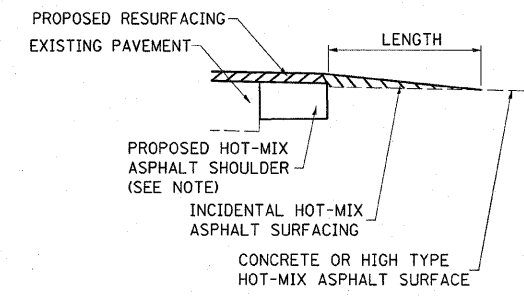
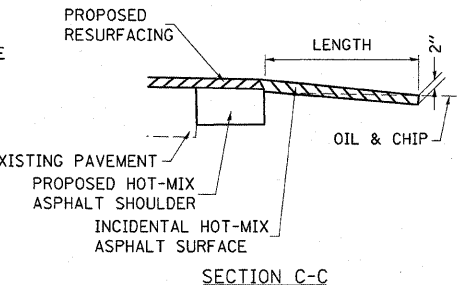
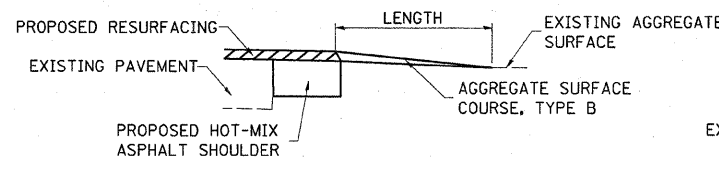
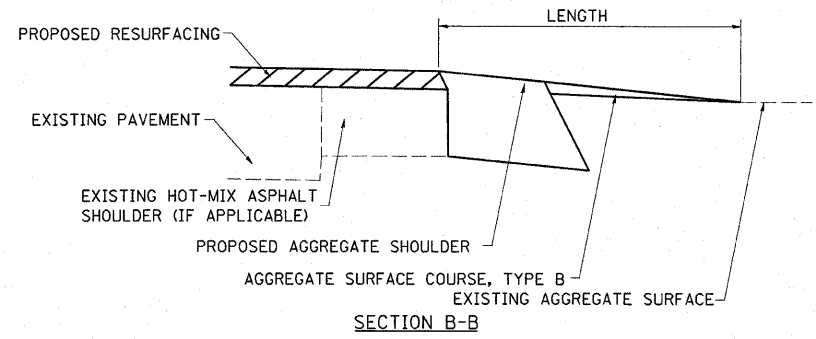
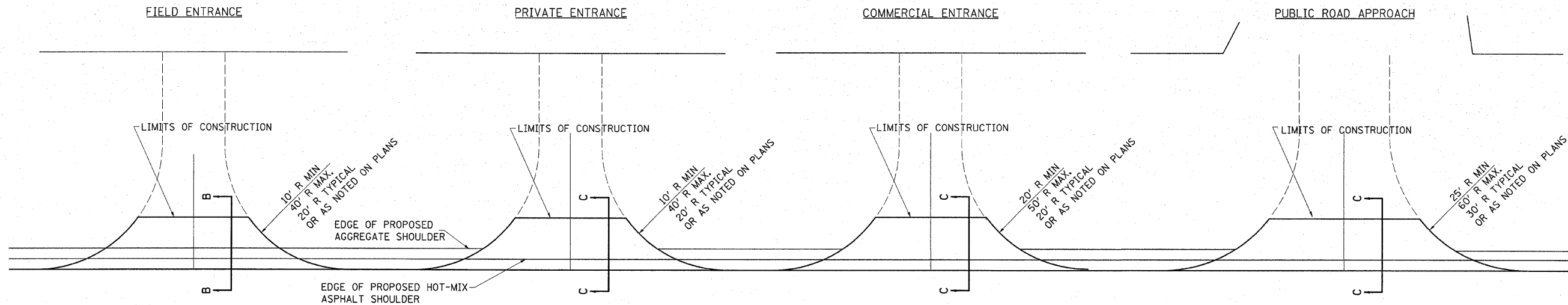
STATION TO STATION	LENGTH	AVERAGE PAVEMENT WIDTH	AREA	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	BITUMINOUS MATERIALS (PRIME COAT)	AGGREGATE (PRIME COAT)	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	LEVELING BINDER (MACHINE METHOD), N70	HOT-MIX ASPHALT SHOULDERS, 6 1/2"	PAVED SHOULDER REMOVAL	EXCAVATING AND GRADING EXISTING SHOULDER	AGGREGATE SHOULDERS, TYPE B	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	TEMPORARY RAMP
	FOOT	FOOT	SQ FT	SQ YD	SQ YD	GALLON	TON	TON	TON	SQ YD	SQ YD	UNIT	TON	SQ YD	SQ YD
1889+18.00 TO 1889+53.00	35.0	30.0	1050.0	14.4	0.0	11.7	0.2	9.8	2.1	15.6	0.0	0.7	2.3	86.7	14.4
1889+53.00 TO 1897+10.00	757.0	30.0	22710.0	2186.9	0.0	252.3	5.0	212.0	91.8	336.4	0.0	15.1	43.3	0.0	0.0
1897+10.00 BK= 1202+24.00 A	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
1202+24.00 TO 1219+01.80	1677.8	30.0	50334.0	4847.0	0.0	559.3	11.2	469.8	203.6	745.7	0.0	33.6	95.3	0.0	0.0
1219+01.80 BK= 1219+02.00 A	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
1219+02.00 TO 1310+50.00	9148.0	30.0	274440.0	26427.6	0.0	3049.3	61.0	2561.4	1110.0	4065.8	0.0	183.0	547.9	0.0	0.0
1310+50.00 TO 1310+85.00	35.0	30.0	1050.0	0.0	101.1	11.7	0.2	9.8	4.2	15.6	0.0	0.7	2.3	0.0	0.0
1310+85.00 TO 1318+75.00	790.0	27.0	21330.0	0.0	2106.7	237.0	4.7	199.1	88.5	263.3	87.8	7.9	22.1	0.0	0.0
1318+75.00 TO 1318+95.00	20.0	31.0	620.0	0.0	53.3	6.9	0.1	5.8	2.6	6.7	2.2	0.2	0.7	0.0	0.0
1318+95.00 TO 1319+10.00	15.0	32.0	480.0	0.0	40.0	5.3	0.1	4.5	2.2	0.0	1.7	0.2	1.0	0.0	0.0
1319+10.00 TO 1320+41.00	131.0	32.0	4192.0	465.8	0.0	46.6	0.9	39.1	19.6	0.0	14.6	0.0	6.7	0.0	17.8
1320+41.00 TO 1322+26.00	185.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
1322+26.00 TO 1323+61.00	135.0	32.0	4320.0	480.0	0.0	48.0	1.0	40.3	20.2	0.0	15.0	0.0	9.0	0.0	17.8
1323+61.00 TO 1323+91.00	30.0	31.0	930.0	93.3	0.0	10.3	0.2	8.7	3.9	10.0	3.3	0.3	2.0	0.0	0.0
1323+91.00 TO 1324+26.00	35.0	27.0	945.0	0.0	93.3	10.5	0.2	8.8	3.9	11.7	3.9	0.4	2.3	0.0	0.0
1324+26.00 TO 1330+05.00	579.0	27.0	15633.0	0.0	1544.0	173.7	3.5	145.9	64.8	193.0	64.3	5.8	15.5	0.0	0.0
1330+05.00 TO 1330+40.00	35.0	30.0	1050.0	0.0	93.3	11.7	0.2	9.8	3.9	23.3	7.8	0.7	2.3	0.0	0.0
1330+40.00 TO 1333+50.00	310.0	30.0	9300.0	826.7	0.0	103.3	2.1	86.8	34.7	206.7	68.9	6.2	17.0	0.0	0.0
1333+50.00 TO 1333+85.00	35.0	32.0	1120.0	17.8	0.0	12.4	0.2	10.5	2.6	0.0	0.0	0.0	2.3	106.7	17.8
1333+85.00 TO 1337+15.00	330.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
1337+15.00 TO 1337+50.00	35.0	32.0	1120.0	17.8	0.0	12.4	0.2	10.5	2.6	0.0	0.0	0.0	2.3	106.7	17.8
1337+50.00 TO 1342+00.00	450.0	32.0	14400.0	1600.0	0.0	160.0	3.2	134.4	67.2	0.0	0.0	0.0	29.9	8.0	0.0
1342+00.00 TO 1342+35.00	35.0	32.0	1120.0	17.8	0.0	12.4	0.2	10.5	2.6	0.0	0.0	0.0	2.3	106.7	17.8
1342+35.00 TO 1345+65.00	330.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
1345+65.00 TO 1346+00.00	35.0	32.0	1120.0	17.8	0.0	12.4	0.2	10.5	2.6	0.0	0.0	0.0	2.3	106.7	17.8
1346+00.00 TO 1359+15.00	1315.0	30.0	39450.0	3506.7	0.0	438.3	8.8	368.2	147.3	876.7	292.2	26.3	87.4	0.0	0.0
1359+15.00 TO 1359+50.00	35.0	30.0	1050.0	0.0	93.3	11.7	0.2	9.8	3.9	23.3	7.8	0.7	2.3	0.0	0.0
1359+50.00 TO 1365+90.00	640.0	24.0	15360.0	0.0	1706.7	170.7	3.4	143.4	71.7	0.0	0.0	0.0	0.0	0.0	0.0
1365+90.00 TO 1366+25.00	35.0	30.0	1050.0	0.0	101.1	11.7	0.2	9.8	4.2	15.6	0.0	0.7	2.3	0.0	0.0
1366+25.00 TO 1366+28.00	3.0	30.0	90.0	8.7	0.0	1.0	0.0	0.8	0.4	1.3	0.0	0.1	0.2	0.0	0.0
1366+28.00 BK= 1366+00.00 A	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
1366+00.00 TO 1373+00.00	700.0	30.0	21000.0	2022.2	0.0	233.3	4.7	196.0	84.9	311.1	0.0	14.0	40.8	0.0	0.0
1373+00.00 TO 1377+03.00	403.0	30.0	12090.0	1164.2	0.0	134.3	2.7	112.8	48.9	179.1	0.0	8.1	18.6	0.0	0.0
1377+03.00 TO 1377+38.00	35.0	30.0	1050.0	0.0	101.1	11.7	0.2	9.8	4.2	15.6	0.0	0.7	2.3	0.0	0.0
1377+38.00 TO 1381+30.00	392.0	24.0	9408.0	0.0	1045.3	104.5	2.1	87.8	43.9	0.0	0.0	0.0	0.0	0.0	0.0

FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pv_work\PW\DOT\SWARTZRW\dms54414\sh	sch_74317.dgn	DRAWN -	REVISED -		SCALE: NA	SHEET NO. 1 OF 3 SHEETS	STA.	TO STA.	2796	(L,K)RS-2	EFFINGHAM	58	13
	PLOT SCALE = 28.0000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 74317								
	PLOT DATE = 3/17/2018	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

RESURFACING SCHEDULE (Cont'd)			LENGTH	AVERAGE PAVEMENT WIDTH	AREA	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	BITUMINOUS MATERIALS (PRIME COAT)	AGGREGATE (PRIME COAT)	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	LEVELING BINDER (MACHINE METHOD), N70	HOT-MIX ASPHALT SHOULDERS, 6 1/2"	PAVED SHOULDER REMOVAL	EXCAVATING AND GRADING EXISTING SHOULDER	AGGREGATE SHOULDERS, TYPE B	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	TEMPORARY RAMP
STATION	TO	STATION															
			FOOT	FOOT	SQ FT	SQ YD	SQ YD	GALLON	TON	TON	TON	TON	SQ YD	UNIT	TON	SQ YD	SQ YD
1381+30.00	TO	1381+65.00	35.0	30.0	1050.0	0.0	93.3	11.7	0.2	9.8	3.9	23.3	7.8	0.7	2.3	0.0	0.0
1381+65.00	TO	1384+86.00	321.0	30.0	9630.0	856.0	0.0	107.0	2.1	89.9	36.0	214.0	71.3	6.4	21.3	0.0	0.0
1384+86.00	TO	1385+21.00	35.0	30.0	1050.0	0.0	93.3	11.7	0.2	9.8	3.9	23.3	7.8	0.7	2.3	0.0	0.0
1385+21.00	TO	1386+50.00	129.0	24.0	3096.0	0.0	344.0	34.4	0.7	28.9	14.4	0.0	0.0	0.0	0.0	0.0	20.0
1386+50.00	TO	1399+75.00	1325.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
1399+75.00	TO	1400+10.00	35.0	30.0	1050.0	14.4	0.0	11.7	0.2	9.8	2.1	15.6	0.0	0.7	2.3	86.7	14.4
1400+10.00	TO	1414+40.00	1430.0	30.0	42900.0	4131.1	0.0	476.7	9.5	400.4	173.5	635.6	0.0	28.6	95.0	0.0	0.0
1414+40.00	TO	1414+75.00	35.0	30.0	1050.0	0.0	101.1	11.7	0.2	9.8	4.2	15.6	0.0	0.7	2.3	0.0	0.0
1414+75.00	TO	1415+57.00	82.0	28.0	2296.0	0.0	236.9	25.5	0.5	21.4	9.9	18.2	0.0	0.8	0.4	0.0	0.0
1415+57.00	TO	1422+73.00	716.0	VAR	25060.0	0.0	2625.3	278.4	5.6	233.9	110.3	159.1	0.0	7.2	23.8	0.0	0.0
1422+73.00	TO	1425+58.00	285.0	42.0	11970.0	0.0	1266.7	133.0	2.7	111.7	53.2	63.3	0.0	2.9	9.5	0.0	0.0
1425+58.00	TO	1431+70.00	612.0	VAR	21420.0	0.0	2244.0	238.0	4.8	199.9	94.2	136.0	0.0	6.1	20.3	0.0	0.0
1431+70.00	TO	1443+50.00	1180.0	30.0	35400.0	0.0	3408.9	393.3	7.9	330.4	143.2	524.4	0.0	23.6	78.4	0.0	0.0
1443+50.00	TO	1445+00.00	150.0	32.0	4800.0	0.0	466.7	53.3	1.1	44.8	19.6	66.7	0.0	3.0	10.0	0.0	0.0
1445+00.00	TO	1448+25.00	325.0	35.0	11375.0	0.0	1191.7	126.4	2.5	106.2	50.1	72.2	0.0	3.3	10.8	0.0	0.0
1448+25.00	TO	1450+65.00	240.0	VAR	9907.0	0.0	1100.8	110.1	2.2	92.5	46.2	0.0	0.0	0.0	0.0	0.0	145.8
TOTALS			25661.0	1238.0	709866.0	48716.0	20252.0	7887.0	157.0	6626.0	2908.0	9284.0	656.0	390.0	1241.0	600.0	301.0

FILE NAME =	USER NAME = swartzr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw_work\PWIDOT\SWARTZR\dms54414\sh	sch_74317.dgn	DRAWN -	REVISED -		SCALE: NA	SHEET NO. 2 OF 3 SHEETS	STA.	TO STA.	2796	ILKRS-2	EFFINGHAM	58	14
	PLOT SCALE = 20,0000' / IN.	CHECKED -	REVISED -								CONTRACT NO. 74317		
	PLOT DATE = 3/17/2010	DATE -	REVISED -								ILLINOIS FED. AID PROJECT		

PAVEMENT MARKING SCHEDULE	LENGTH	PAINT PAVEMENT MARKING - LINE 4" WHITE	PAINT PAVEMENT MARKING - LINE 4" YELLOW	THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE	THERMOPLASTIC PAVEMENT MARKING - LINE 4" YELLOW	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS	TEMPORARY PAVEMENT MARKING - LINE 4"	TEMPORARY PAVEMENT MARKING - LINE 6"	TEMPORARY PAVEMENT MARKING - LINE 12"	TEMPORARY PAVEMENT MARKING - LETTERS & SYMBOLS	SHORT-TERM PAVEMENT MARKING	WORK ZONE PAVEMENT MARKING REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER
		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	FOOT	FOOT	FOOT	SQ FT	FOOT	SQ FT	EACH
1889+18.00 TO 1897+10.00	792.0	1584.0	1584.0	0.0	0.0	0.0	0.0	0.0	0.0	3168.0	0.0	0.0	0.0	320.0	26.7	10.0
1897+10.00 BK= 1202+24.00 AH	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
1202+24.00 TO 1219+01.80	1677.8	3255.6	3355.6	0.0	0.0	0.0	0.0	0.0	0.0	6611.2	0.0	0.0	0.0	672.0	56.0	21.0
1219+01.80 BK= 1219+02.00 AH	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
1219+02.00 TO 1229+95.00	1093.0	2134.0	2186.0	0.0	0.0	0.0	0.0	0.0	0.0	4320.0	0.0	0.0	0.0	432.0	36.0	14.0
1229+95.00 TO 1241+00.00	1105.0	2210.0	1381.3	0.0	0.0	0.0	0.0	0.0	0.0	3591.3	0.0	0.0	0.0	448.0	37.3	14.0
1241+00.00 TO 1264+10.00	2310.0	4503.0	577.5	0.0	0.0	0.0	0.0	0.0	0.0	5080.5	0.0	0.0	0.0	928.0	77.3	29.0
1264+10.00 TO 1276+16.00	1206.0	2236.0	1507.5	0.0	0.0	0.0	0.0	0.0	0.0	3743.5	0.0	0.0	0.0	480.0	40.0	15.0
1276+16.00 TO 1323+31.00	4715.0	9214.0	9430.0	0.0	0.0	0.0	0.0	0.0	0.0	18644.0	0.0	0.0	0.0	1888.0	157.3	78.0
1323+31.00 TO 1332+07.00	876.0	1669.0	1095.0	0.0	0.0	0.0	0.0	0.0	0.0	2764.0	0.0	0.0	0.0	352.0	29.3	22.0
1332+07.00 TO 1355+23.00	2316.0	4632.0	579.0	0.0	0.0	0.0	0.0	0.0	0.0	5211.0	0.0	0.0	0.0	928.0	77.3	29.0
1355+23.00 TO 1365+80.00	1057.0	2114.0	1321.3	0.0	0.0	0.0	0.0	0.0	0.0	3435.3	0.0	0.0	0.0	416.0	34.7	13.0
1365+80.00 TO 1366+28.00	48.0	96.0	96.0	0.0	0.0	0.0	0.0	0.0	0.0	192.0	0.0	0.0	0.0	16.0	1.3	1.0
1366+28.00 BK= 1366+00.00 AH	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
1366+00.00 TO 1373+00.00	700.0	1400.0	1400.0	0.0	0.0	0.0	0.0	0.0	0.0	2800.0	0.0	0.0	0.0	288.0	24.0	9.0
1373+00.00 TO 1386+00.00	1300.0	2600.0	2600.0	0.0	0.0	0.0	0.0	0.0	0.0	5200.0	0.0	0.0	0.0	528.0	44.0	16.0
1386+00.00 TO 1386+50.00	50.0	100.0	12.5	0.0	0.0	0.0	0.0	0.0	0.0	112.5	0.0	0.0	0.0	16.0	1.3	1.0
1386+50.00 TO 1399+75.00	1325.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	69.0
1399+75.00 TO 1408+40.00	865.0	0.0	0.0	1730.0	1081.3	0.0	0.0	0.0	0.0	2811.3	0.0	0.0	0.0	352.0	29.3	11.0
1408+40.00 TO 1410+15.00	175.0	0.0	0.0	350.0	43.8	0.0	0.0	0.0	0.0	393.8	0.0	0.0	0.0	64.0	5.3	2.0
1410+15.00 TO 1414+77.00	462.0	0.0	0.0	924.0	577.5	0.0	0.0	0.0	0.0	1501.5	0.0	0.0	0.0	192.0	16.0	6.0
1414+77.00 TO 1415+57.00	80.0	0.0	0.0	160.0	160.0	0.0	0.0	0.0	0.0	320.0	0.0	0.0	0.0	32.0	2.7	6.0
1415+57.00 TO 1422+73.00	716.0	0.0	0.0	1432.0	1432.0	0.0	181.5	0.0	0.0	2864.0	0.0	181.5	0.0	576.0	48.0	40.0
1422+73.00 TO 1423+05.00	32.0	0.0	0.0	64.0	64.0	0.0	0.0	0.0	0.0	128.0	0.0	0.0	0.0	32.0	2.7	0.0
1423+05.00 TO 1424+30.00	125.0	0.0	0.0	250.0	250.0	125.0	0.0	0.0	0.0	500.0	125.0	0.0	0.0	96.0	8.0	9.0
1424+30.00 TO 1425+70.00	140.0	0.0	0.0	280.0	0.0	0.0	0.0	31.2	0.0	280.0	0.0	0.0	31.2	0.0	0.0	2.0
1425+70.00 TO 1431+60.00	590.0	0.0	0.0	1180.0	1180.0	0.0	155.1	0.0	0.0	2360.0	0.0	155.1	0.0	480.0	40.0	30.0
1431+60.00 TO 1432+40.00	80.0	0.0	0.0	160.0	160.0	0.0	0.0	0.0	0.0	320.0	0.0	0.0	0.0	32.0	2.7	6.0
1432+40.00 TO 1436+36.00	396.0	0.0	0.0	792.0	495.0	0.0	0.0	0.0	0.0	1287.0	0.0	0.0	0.0	160.0	13.3	5.0
1436+36.00 TO 1438+95.00	259.0	0.0	0.0	518.0	64.8	0.0	0.0	0.0	0.0	582.8	0.0	0.0	0.0	96.0	8.0	3.0
1438+95.00 TO 1443+51.00	456.0	0.0	0.0	912.0	570.0	0.0	0.0	0.0	0.0	1482.0	0.0	0.0	0.0	176.0	14.7	6.0
1443+51.00 TO 1450+65.00	714.0	0.0	0.0	1428.0	1428.0	0.0	257.2	0.0	0.0	2856.0	0.0	257.2	0.0	288.0	24.0	36.0
TOTALS		37748.0	27126.0	10180.0	7506.0	125.0	594.0	31.0	82559.0	125.0	594.0	31.0	10288.0	857.0	503.0	



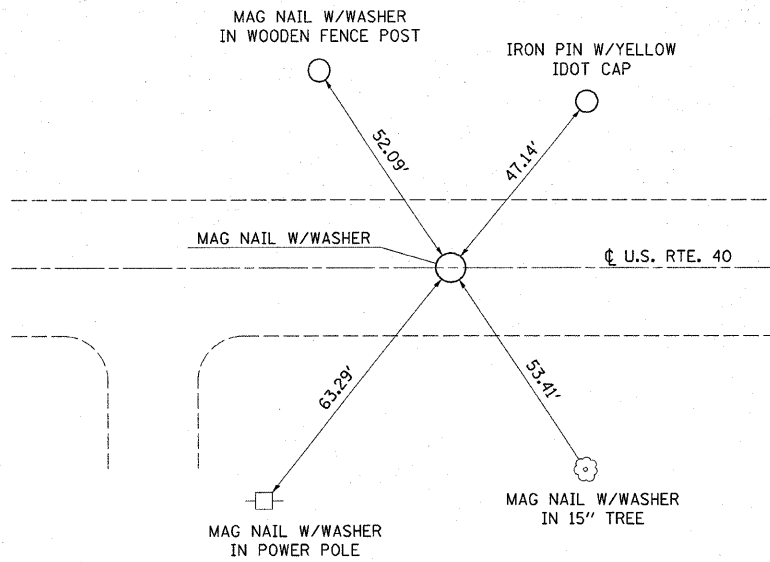
TYPICAL SECTION AT MAILBOX TURNOUT
 NOTE: SEE STANDARD 406201 FOR MAILBOX TURNOUT DETAILS

NOTES
 LENGTH = 10' UNLESS OTHERWISE NOTED ON PLANS
 THE THICKNESS OF THE HOT-MIX ASPHALT SHOULDERS THROUGH COMMERCIAL ENTRANCES (HOT-MIX ASPHALT) AND PUBLIC ROADS SHALL BE 10". THE COST OF THE EXTRA THICKNESS SHALL BE INCLUDED WITH THE HOT-MIX ASPHALT SHOULDERS PAY ITEM.
 HOT-MIX ASPHALT SHOULDERS SHALL NOT BE CONSTRUCTED THROUGH A PCC ENTRANCE OR PCC PUBLIC ROAD APPROACH.
 THE COST OF EXCAVATION IS INCLUDED IN THE PAY ITEM INCIDENTAL HOT-MIX ASPHALT SURFACING.
 THE COST OF THE BITUMINOUS MATERIALS AND AGGREGATE (PRIME COAT) FOR ENTRANCES AND PUBLIC ROAD APPROACHES SHALL BE INCLUDED IN THE PAY ITEM INCIDENTAL HOT-MIX ASPHALT SURFACING.

SIDE	STATION	TYPE	INCIDENTAL HOT-MIX ASPHALT SURFACING		SIDE	STATION	TYPE	INCIDENTAL HOT-MIX ASPHALT SURFACING		SIDE	STATION	TYPE	INCIDENTAL HOT-MIX ASPHALT SURFACING		SIDE	STATION	TYPE	INCIDENTAL HOT-MIX ASPHALT SURFACING	
			TON	TON				TON	TON				TON	TON				TON	TON
LT	1889+20	MBT1	0.7		LT	1227+00	MBT2		1.4	LT	1260+56	CE	8.3		RT	1317+62	PE	4.2	
LT	1890+96	MBT1	0.7		RT	1227+24	PE		2.7	LT	1261+50	MBT2		1.4	LT	1317+85	PRA		2.0
LT	1891+15	PE	1.7		LT	1231+26	MBT1		0.7	RT	1261+52	CE	3.7		RT	1319+25	PRA		5.8
LT	1892+11	MBT1	0.7		LT	1231+51	PE		2.3	LT	1262+88	MBT2		1.4	RT	1324+45	FE		1.7
LT	1892+59	PE	1.8		RT	1232+20	CE		4.3	RT	1262+88	PE		1.9	LT	1324+45	PRA		7.6
RT	1892+88	PE	5.3		LT	1232+52	PE		2.1	RT	1266+34	PRA	11.6		RT	1330+85	PE		10.5
RT	1894+33	PE	2.2		LT	1232+78	MBT1		0.7	LT	1266+57	PRA	5.2		LT	1330+90	MBT2		1.4
LT	1896+84	MBT1	0.7		LT	1233+05	PE		2.5	RT	1273+92	FE		1.4	RT	1341+07	FE		9.0
LT	1896+88	PE	2.9		LT	1233+60	FE		2.4	LT	1280+37	PE		2.0	LT	1341+57	FE		6.1
LT	1203+00	MBT2	1.4		RT	1243+73	PE		1.9	LT	1280+60	MBT1		0.7	LT	1353+66	FE		4.1
RT	1203+02	PE	7.3		LT	1244+49	PE		2.0	LT	1281+67	MBT2		1.4	RT	1366+00	PE		2.6
LT	1207+83	PE	2.6		RT	1244+88	PE		2.5	RT	1281+93	PE		2.8	LT	1366+40	MBT2		1.4
LT	1208+90	PRA	9.9		LT	1245+50	PE		2.5	LT	1286+81	FE		1.6	RT	1371+43	CE		8.4
LT	1211+07	CE	6.0		LT	1245+94	PRA	6.1		LT	1288+68	MBT1		0.7	LT	1372+12	MBT1		0.7
LT	1213+85	PE	1.4		LT	1246+59	CE	7.2		LT	1289+11	PRA		3.8	LT	1372+40	PE		2.0
RT	1213+85	PE	2.5		RT	1245+95	PE	6.1		LT	1291+25	MBT1		0.7	RT	1374+23	CE		6.0
LT	1214+03	MBT1	0.7		RT	1246+87	CE	13.7		LT	1291+55	PE		2.5	LT	1374+29	CE		8.5
LT	1214+44	MBT2	1.4		LT	1249+40	PRA	5.2		RT	1293+65	PE		8.2	LT	1375+31	MBT2		1.4
LT	1215+03	PE	3.2		RT	1249+59	PE	1.7		LT	1293+85	MBT2		1.4	RT	1375+88	CE		8.5
RT	1215+43	PE	2.3		LT	1249+60	MBT2	1.4		RT	1308+80	PE	5.0		LT	1377+32	CE		14.5
LT	1215+71	MBT2	1.4		LT	1251+03	PE	2.3		RT	1310+20	PRA	5.8		RT	1381+77	FE		7.4
LT	1217+38	PE	1.9		LT	1251+21	MBT1	0.7		LT	1310+20	PE	4.8		LT	1401+64	FE		2.1
RT	1222+58	PRA	5.0		LT	1252+13	PE	7.4		LT	1310+32	MBT1		0.7	LT	1408+47	FE		3.3
LT	1226+11	FE	1.8		RT	1252+30	PE	2.2		RT	1315+57	PE	3.7		TOTALS			228.0	120.0

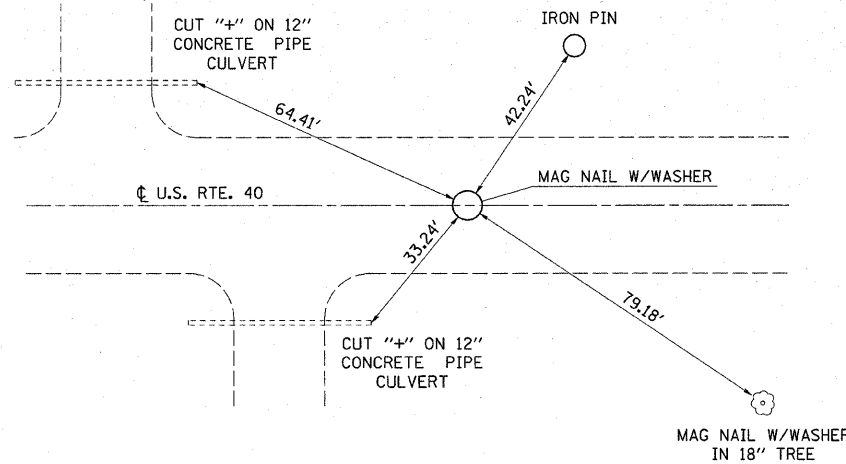
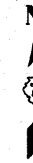
FE=FIELD ENTRANCE PRA - PUBLIC ROAD APPROACH
 PE=PRIVATE ENTRANCE MBT - MAILBOX TURNOUT
 CE=COMMERCIAL ENTRANCE

POT STA. 1203+90.68



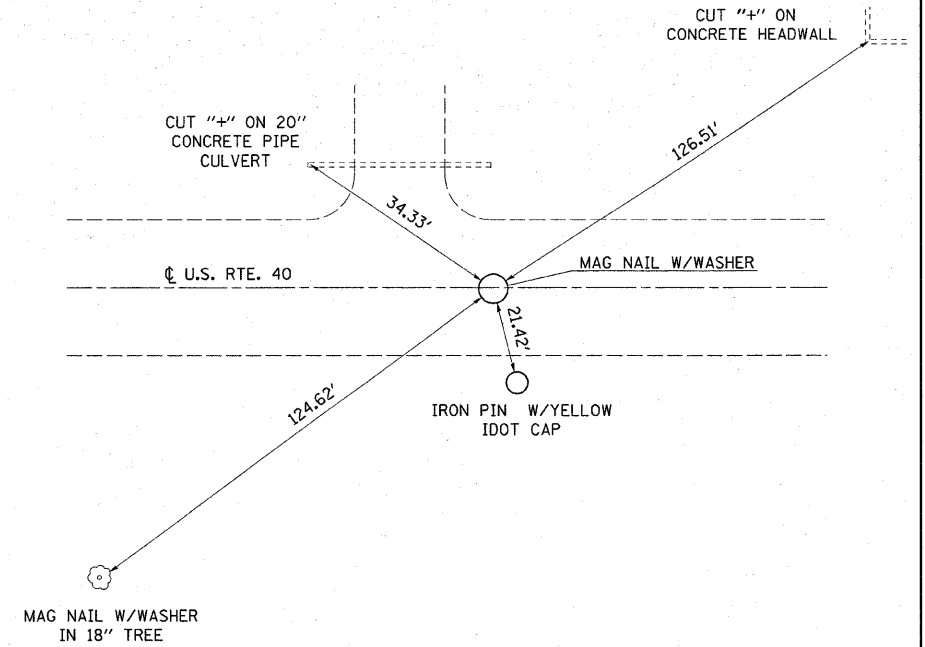
NOTE: NOT DRAWN TO SCALE

PC STA. 1216+08.98



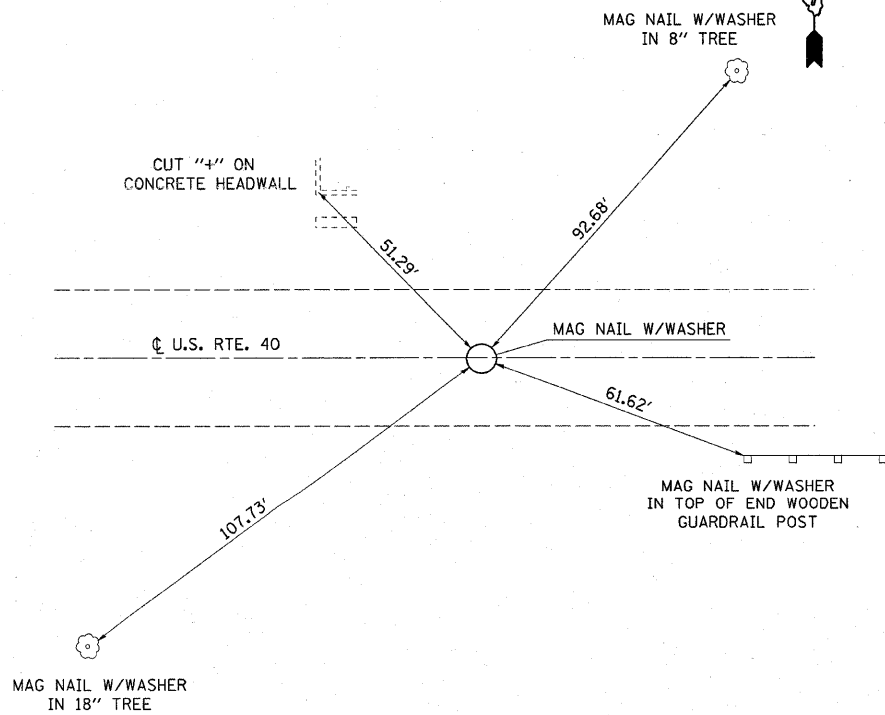
NOTE: NOT DRAWN TO SCALE

PI STA. 1217+78.52



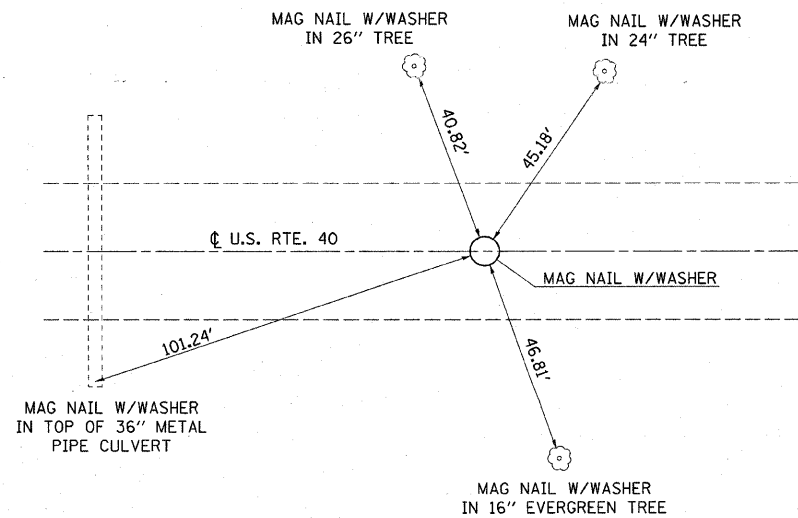
NOTE: NOT DRAWN TO SCALE

PT STA. 1219+48.05



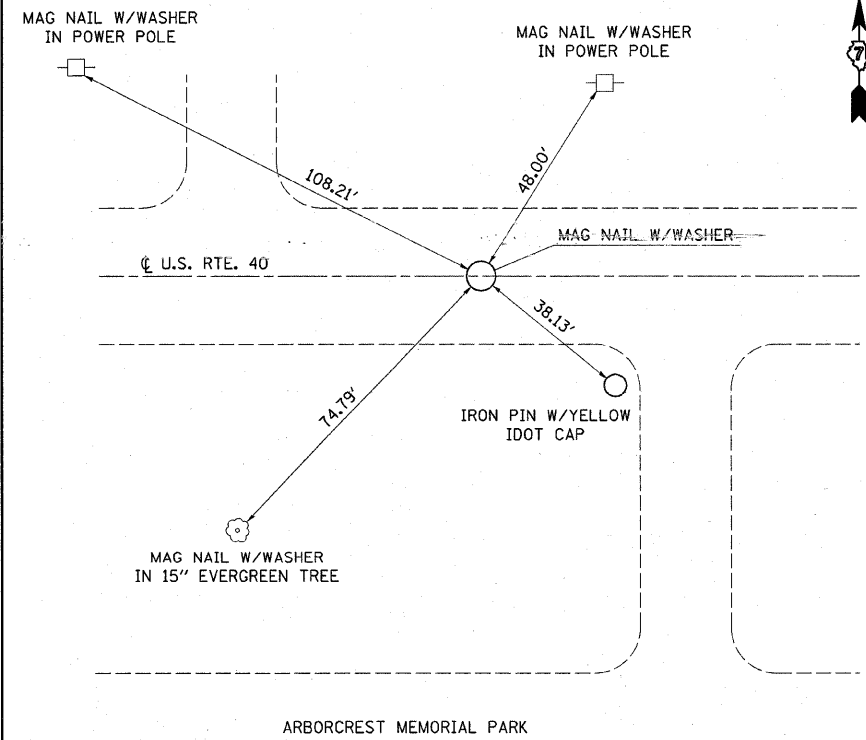
NOTE: NOT DRAWN TO SCALE

PC STA. 1230+36.79



NOTE: NOT DRAWN TO SCALE

PI STA. 1232+32.00



NOTE: NOT DRAWN TO SCALE

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -
c:\pwork\p\dot\swartzw\dms54414\shd\tails_74317.dgn		DRAWN -	REVISED -
	PLOT SCALE = 20,0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 3/17/2010	DATE -	REVISED -

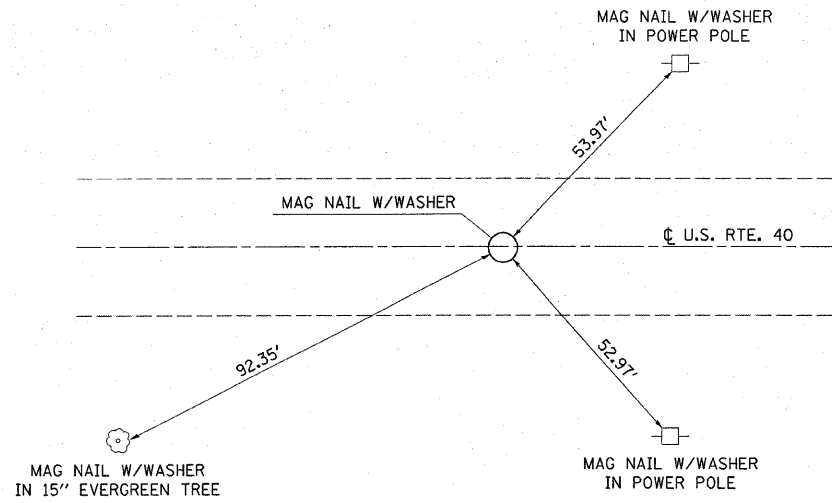
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TIE POINTS

SCALE: NA SHEET NO. 1 OF 3 SHEETS STA. TO STA.

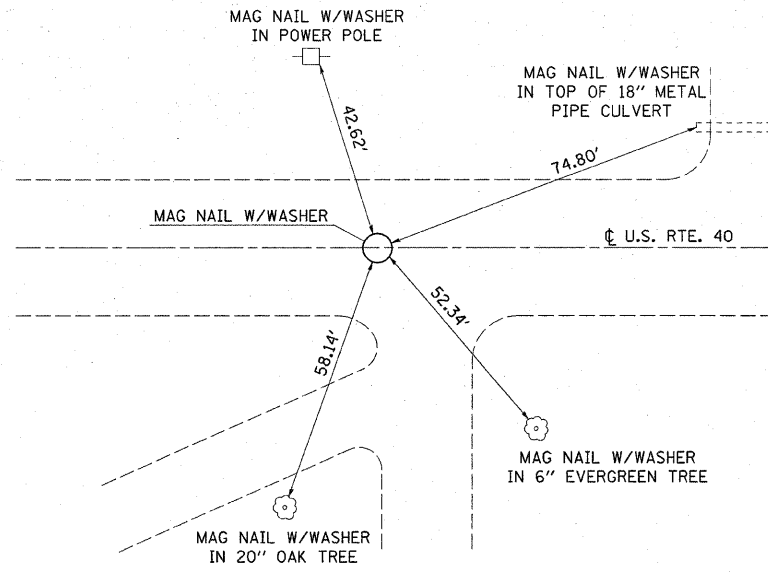
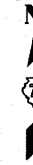
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	(L,K)RS-2	EFFINGHAM	58	17
CONTRACT NO. 74317			ILLINOIS FED. AID PROJECT	

PT STA. 1234+27.22



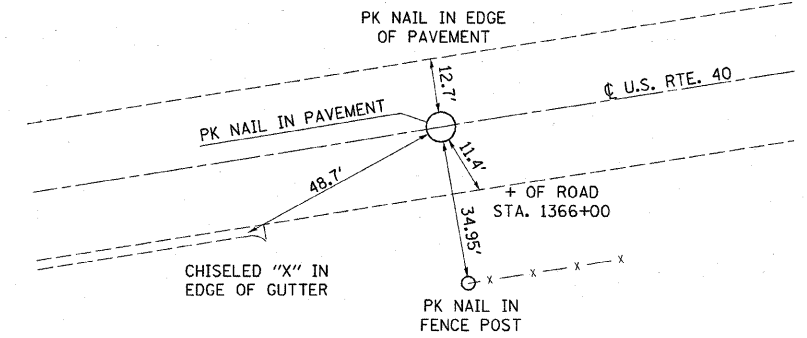
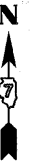
NOTE: NOT DRAWN TO SCALE

POT STA. 1243+97.09



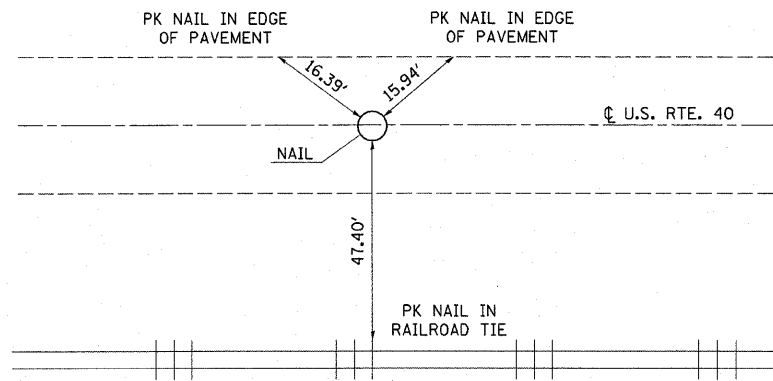
NOTE: NOT DRAWN TO SCALE

POT #355 STA. 1366+23.337



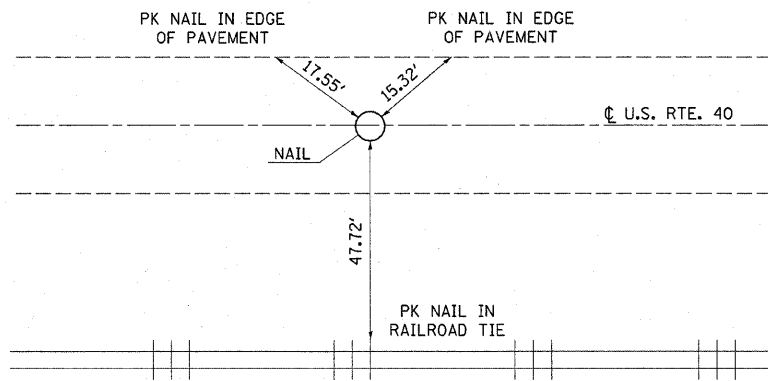
NOTE: NOT DRAWN TO SCALE

PT STA. 1403+12.011



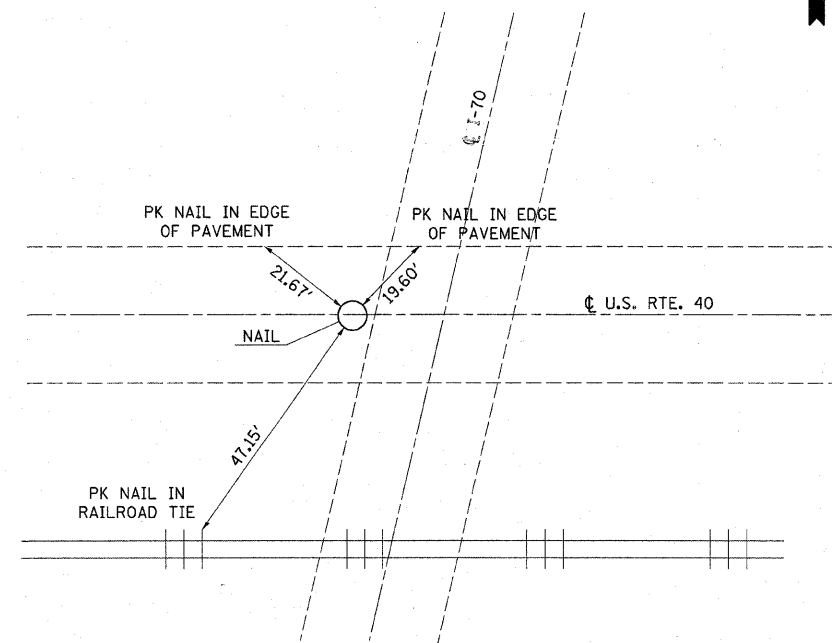
NOTE: NOT DRAWN TO SCALE

PC STA. 1403+34.41



NOTE: NOT DRAWN TO SCALE

PI STA. 1405+34.41



NOTE: NOT DRAWN TO SCALE

FILE NAME =
c:\pwork\pwork\swartzw\dms54414\shd

USER NAME = swartzw
date: 11-7-01
PLOT SCALE = 20.0000' / IN.
PLOT DATE = 3/17/2018

DESIGNED -
DRAWN -
CHECKED -
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

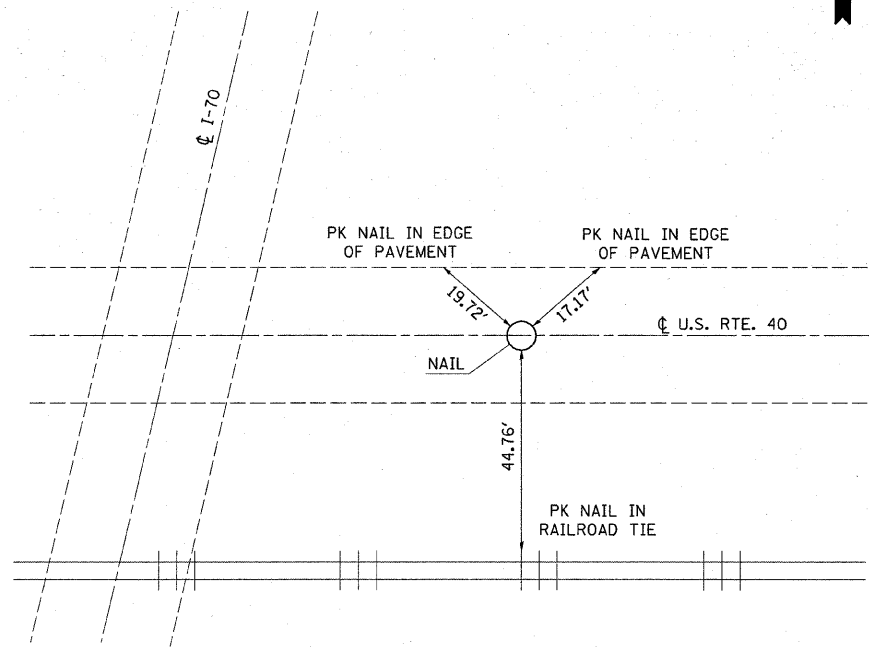
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TIE POINTS

SCALE: NA SHEET NO. 2 OF 3 SHEETS STA. TO STA.

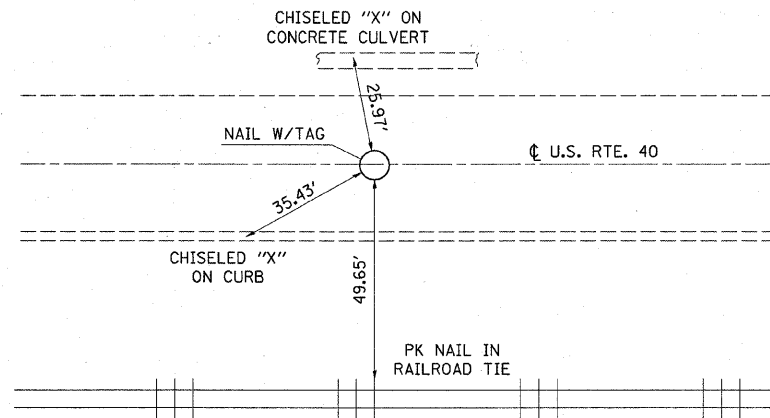
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	(L,K)RS-2	EFFINGHAM	58	18
CONTRACT NO. 74317				
ILLINOIS FED. AID PROJECT				

PT STA. 1407+34.41



NOTE: NOT DRAWN TO SCALE

PC STA. 1448+39.63

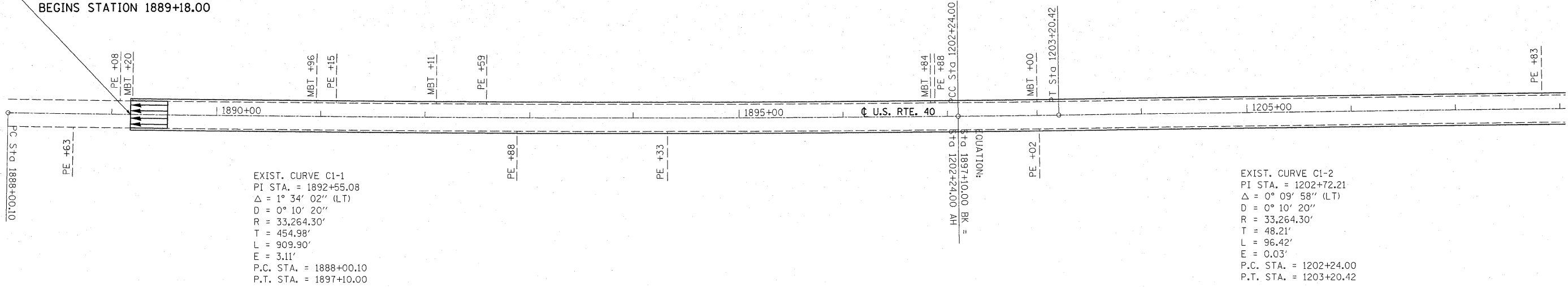
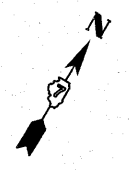


NOTE: NOT DRAWN TO SCALE

LOCATION	FURNISHING AND INSTALLING PROPERTY MARKERS
	EACH
1203+90.68	1.0
1216+08.98	1.0
1217+78.52	1.0
1219+48.05	1.0
1230+36.79	1.0
1232+32.00	1.0
1234+27.22	1.0
1243+97.09	1.0
1366+23.34	1.0
1375+21.65	1.0
1376+60.67	1.0
1378+60.47	1.0
1380+58.67	1.0
1380+71.39	1.0
1403+12.01	1.0
1403+34.41	1.0
1405+34.41	1.0
1407+34.41	1.0
1448+39.63	1.0
1448+92.50	1.0
1449+44.98	1.0
TOTAL	21.0

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TIE POINTS		F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\swartzw\dms54414\shd\details_74317.dgn		DRAWN -	REVISED -		2796	(L,K)RS-2	EFFINGHAM	58	19		
PLOT SCALE = 20.0000 "/ IN.		CHECKED -	REVISED -		CONTRACT NO. 74317			ILLINOIS FED. AID PROJECT			
PLOT DATE = 3/17/2018		DATE -	REVISED -		SCALE: NA	SHEET NO. 3 OF 3 SHEETS	STA.	TO STA.			

FAS 2796 (U.S. RTE. 40)
SECTION (L,K)RS-2
EFFINGHAM COUNTY
BEGINS STATION 1889+18.00



EXIST. CURVE C1-1
PI STA. = 1892+55.08
 $\Delta = 1^\circ 34' 02''$ (LT)
D = 0° 10' 20"
R = 33,264.30'
T = 454.98'
L = 909.90'
E = 3.11'
P.C. STA. = 1888+00.10
P.T. STA. = 1897+10.00

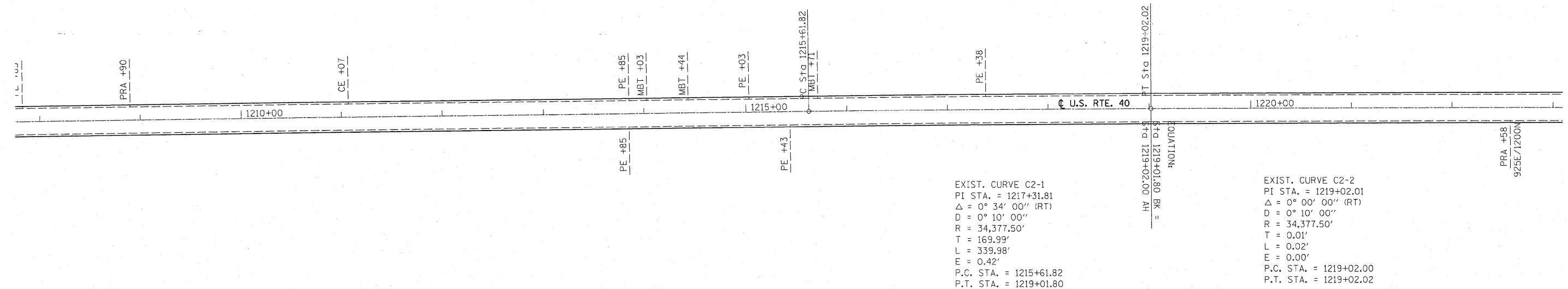
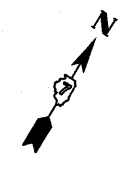
EXIST. CURVE C1-2
PI STA. = 1202+72.21
 $\Delta = 0^\circ 09' 58''$ (LT)
D = 0° 10' 20"
R = 33,264.30'
T = 48.21'
L = 96.42'
E = 0.03'
P.C. STA. = 1202+24.00
P.T. STA. = 1203+20.42

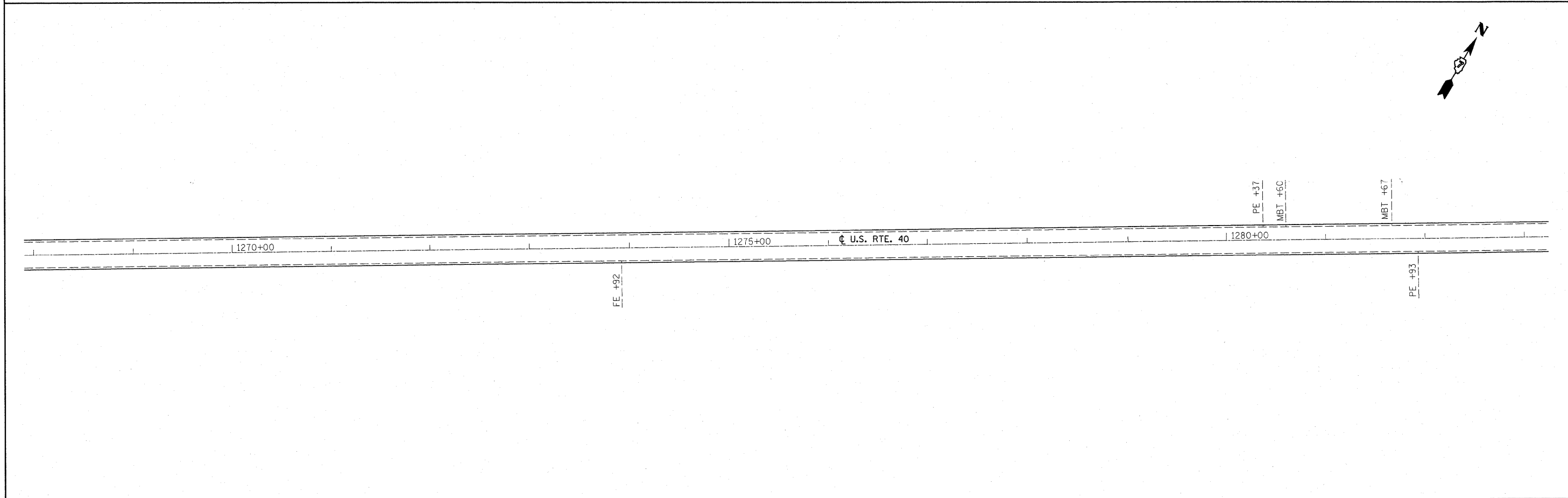
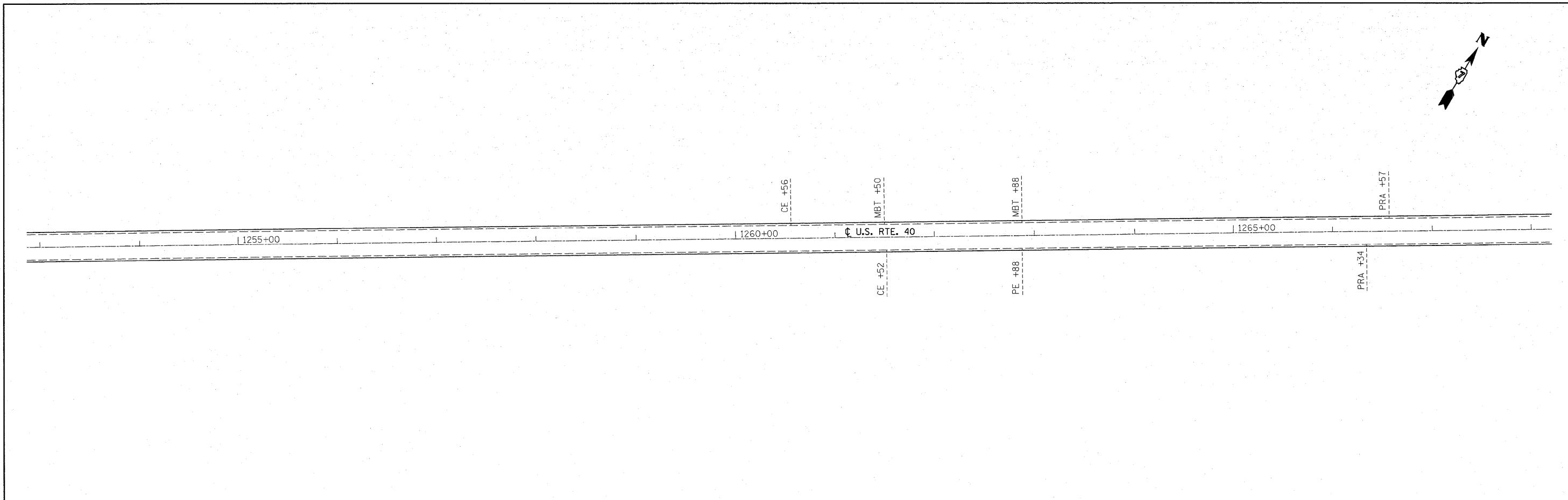
EQUATION:
STA 1897+10.00 BK =
HV 0°12'2+202+24.00

EQUATION:
STA 1219+02.00 BK =
HV 0°20'6+121+02.00

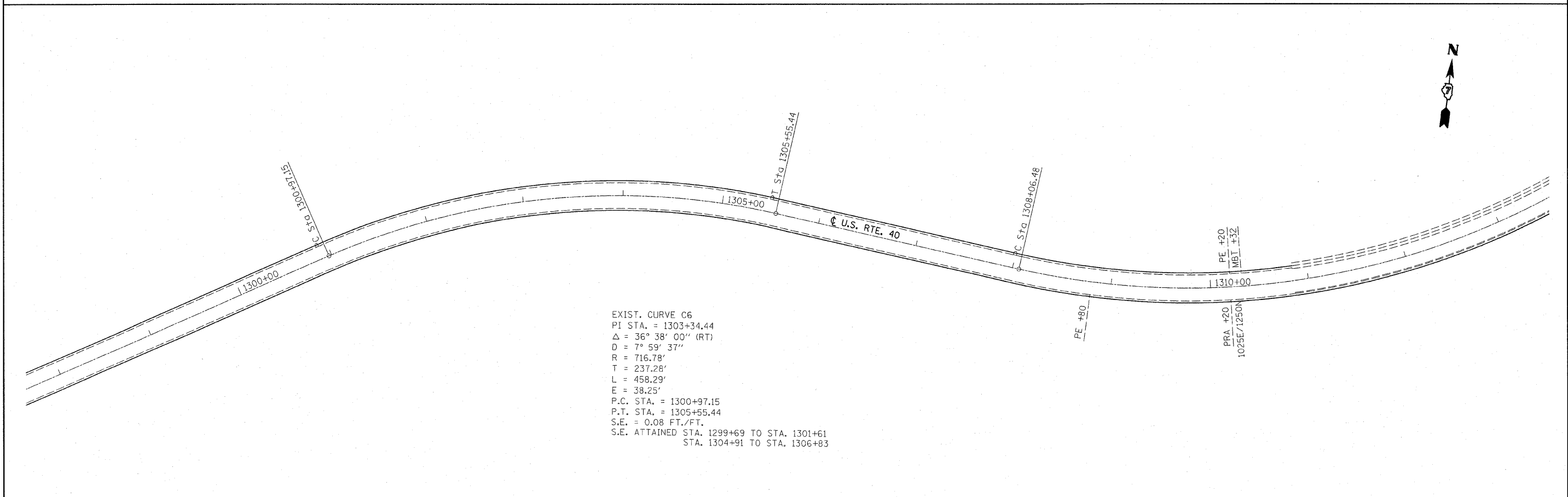
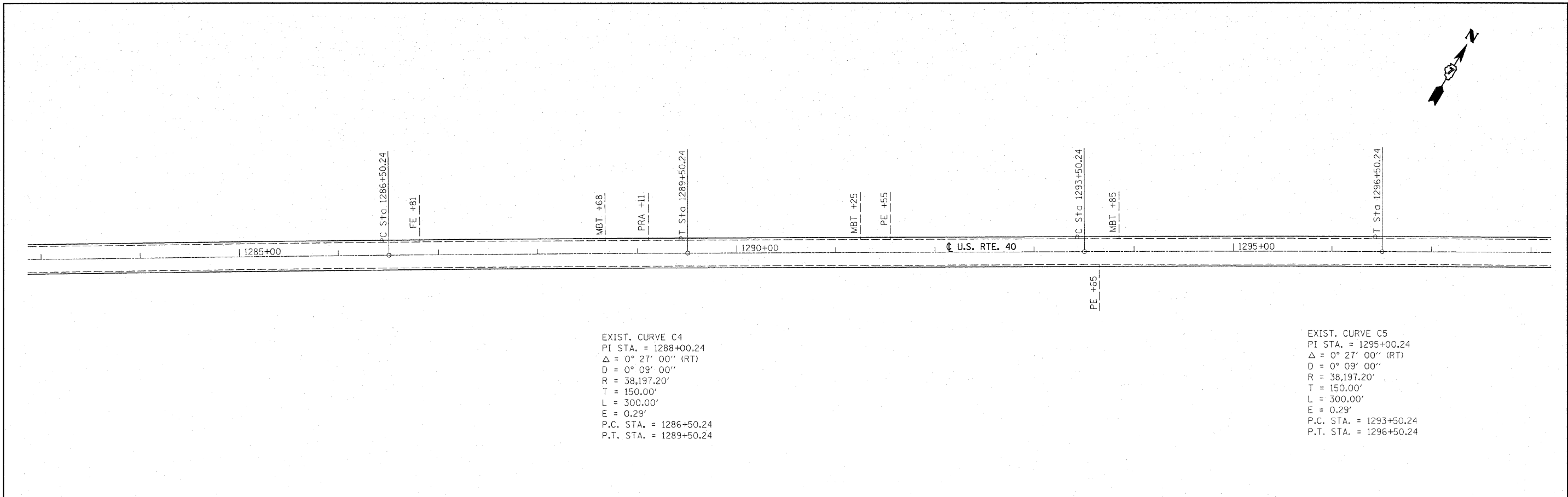
EXIST. CURVE C2-1
PI STA. = 1217+31.81
 $\Delta = 0^\circ 34' 00''$ (RT)
D = 0° 10' 00"
R = 34,377.50'
T = 169.99'
L = 339.98'
E = 0.42'
P.C. STA. = 1215+61.82
P.T. STA. = 1219+01.80

EXIST. CURVE C2-2
PI STA. = 1219+02.01
 $\Delta = 0^\circ 00' 00''$ (RT)
D = 0° 10' 00"
R = 34,377.50'
T = 0.01'
L = 0.02'
E = 0.00'
P.C. STA. = 1219+02.00
P.T. STA. = 1219+02.02



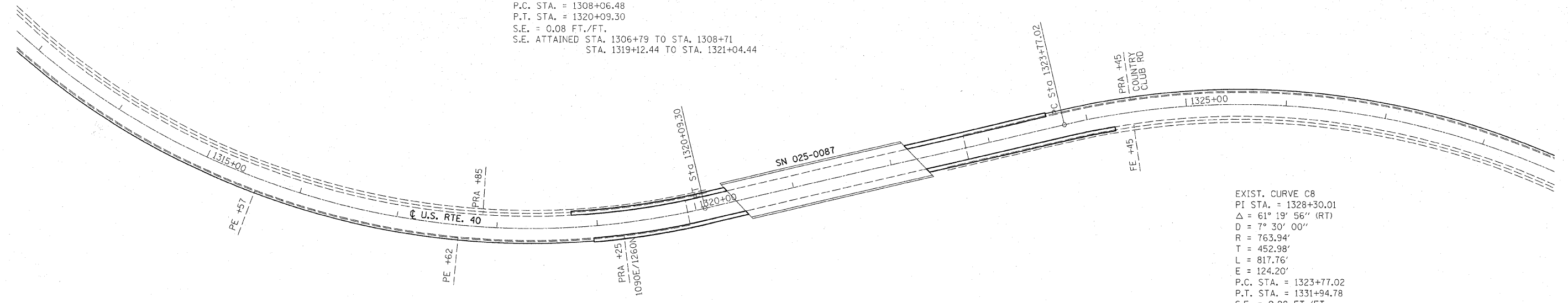


FILE NAME = c:\pwwork\pwwidot\SWARTZR\lms54414\sh... plan_74317.dgn	USER NAME = swartzr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEETS			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 51.1628 ' / IN.	CHECKED -	REVISED -		SCALE: 50	SHEET NO. 3 OF 9 SHEETS	STA. 1253+00 TO STA. 1283+00	2796	(L,K)RS-2	EFFINGHAM	58	22
PLOT DATE = 3/17/2010	DATE -	REVISED -			ILLINOIS FED. AID PROJECT							
CONTRACT NO. 74317												

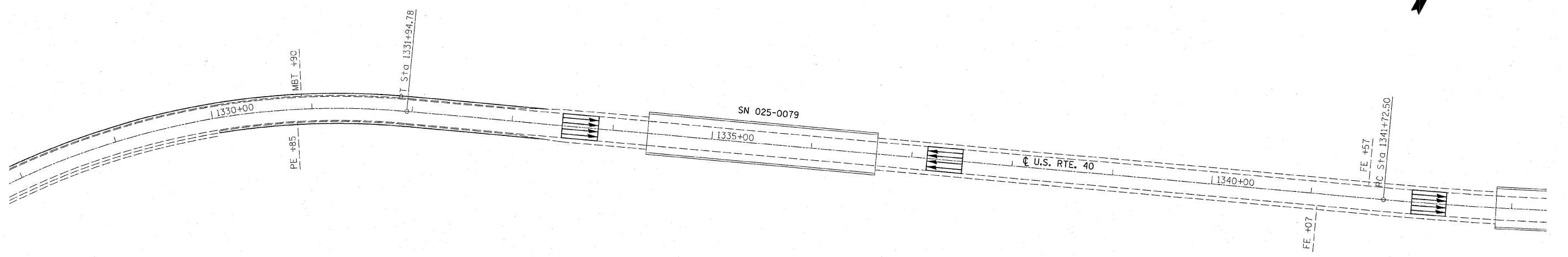
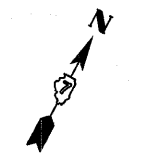


FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEETS		F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwwork\pwwork\SWARTZRW\dms84414\shp\plan_74317.dgn		DRAWN -	REVISED -		SCALE: 50	SHEET NO. 4 OF 9 SHEETS	STA. 1283+00 TO STA. 1313+00	2796	(L,K)RS-2	EFFINGHAM	58	23
PLOT SCALE = 58.0000 "/> <td></td> <td>CHECKED -</td> <td>REVISED -</td> <td colspan="2" style="text-align: center;">CONTRACT NO. 74317</td> <td colspan="6" style="text-align: center;">ILLINOIS FED. AID PROJECT</td>		CHECKED -	REVISED -		CONTRACT NO. 74317		ILLINOIS FED. AID PROJECT					
PLOT DATE = 3/17/2010		DATE -	REVISED -									

EXIST. CURVE C7
 PI STA. = 1315+72.62
 $\Delta = 90^\circ 04' 57''$ (LT)
 $D = 7^\circ 29' 21''$
 $R = 765.04'$
 $T = 766.14'$
 $L = 1,202.82'$
 $E = 317.67'$
 P.C. STA. = 1308+06.48
 P.T. STA. = 1320+09.30
 S.E. = 0.08 FT./FT.
 S.E. ATTAINED STA. 1306+79 TO STA. 1308+71
 STA. 1319+12.44 TO STA. 1321+04.44

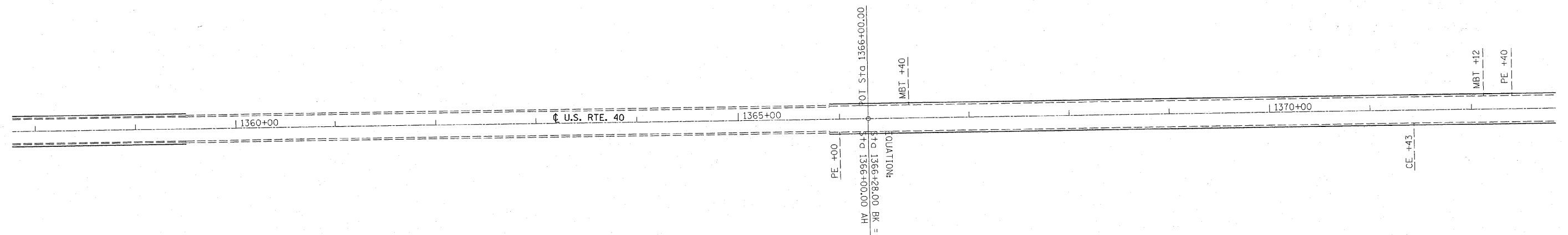
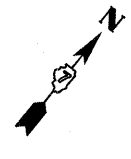
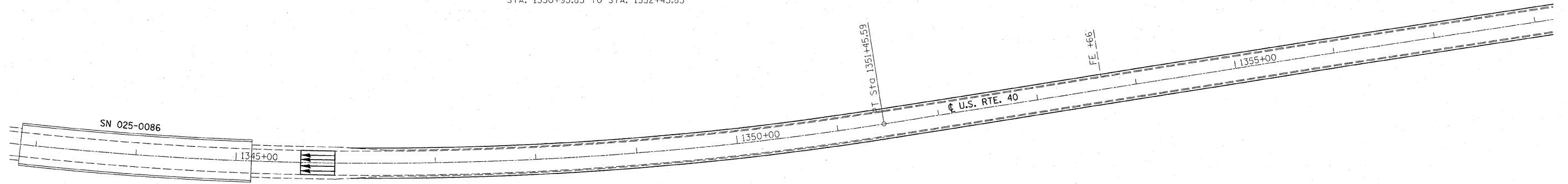
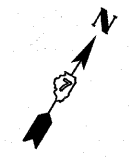


EXIST. CURVE C8
 PI STA. = 1328+30.01
 $\Delta = 61^\circ 19' 56''$ (RT)
 $D = 7^\circ 30' 00''$
 $R = 763.94'$
 $T = 452.98'$
 $L = 817.76'$
 $E = 124.20'$
 P.C. STA. = 1323+77.02
 P.T. STA. = 1331+94.78
 S.E. = 0.08 FT./FT.
 S.E. ATTAINED STA. 1322+49.26 TO STA. 1324+41.26
 STA. 1331+31.02 TO STA. 1333+23.02

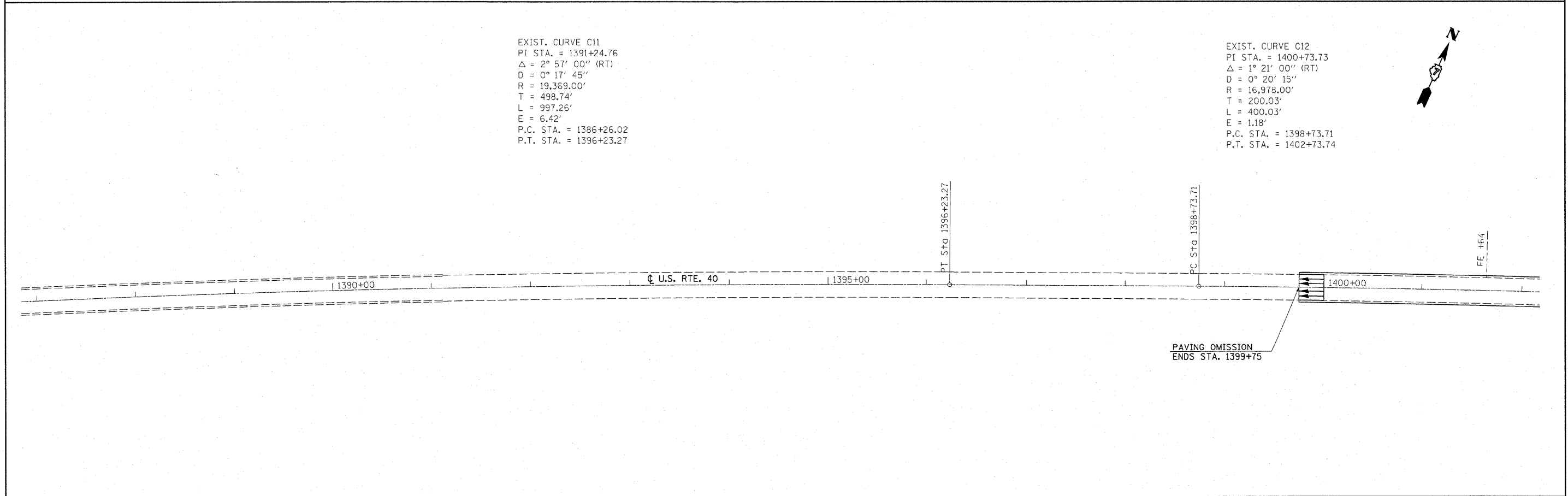
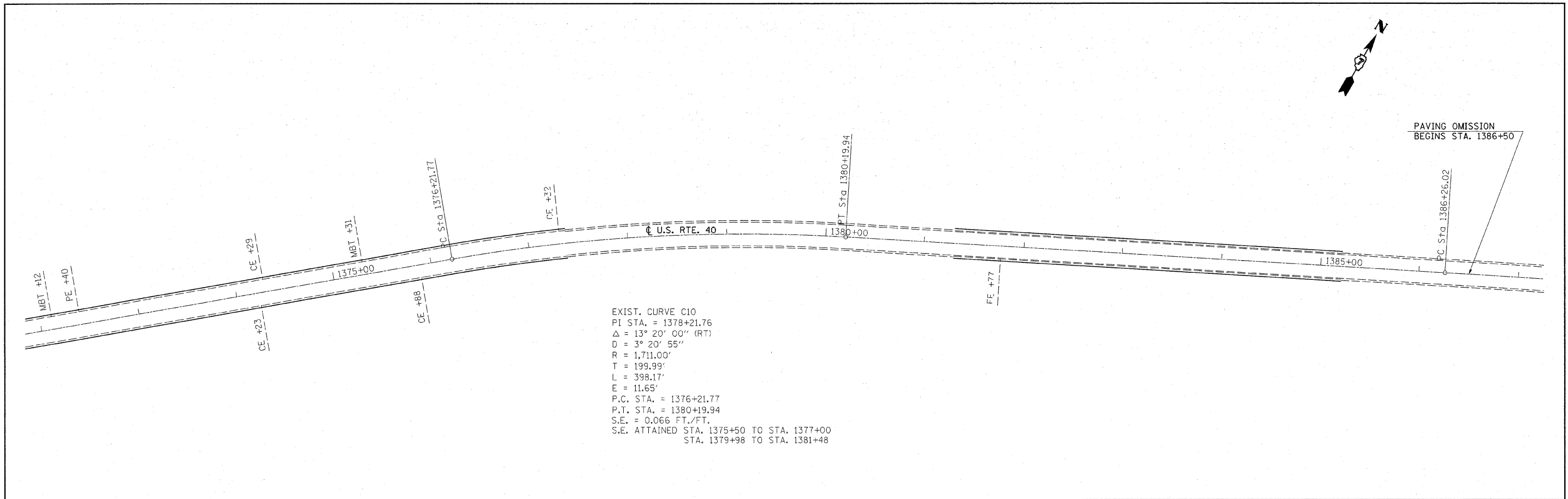


FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEETS			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\SWARTZRW\dms54414\shp\len.74317.dgn		DRAWN -	REVISED -		SCALE: 50	SHEET NO. 5 OF 9 SHEETS	STA. 1313+00 TO STA. 1343+00	2796	(L,K)RS-2	EFFINGHAM	58	24
		CHECKED -	REVISED -					CONTRACT NO. 74317				
		DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

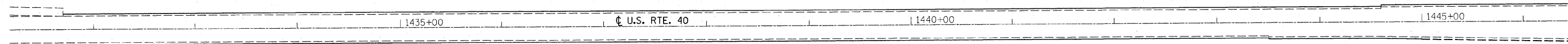
EXIST. CURVE C9
 P.I. STA. = 1346+62.66
 $\Delta = 17^\circ 01' 45''$ (LT)
 $D = 1^\circ 45' 00''$
 $R = 3,274.05'$
 $T = 490.16'$
 $L = 973.10'$
 $E = 36.49'$
 P.C. STA. = 1341+72.50
 P.T. STA. = 1351+45.59
 $S.E. = 0.035$ FT./FT.
 S.E. ATTAINED STA. 1340+72.73 TO STA. 1342+22.73
 STA. 1350+95.83 TO STA. 1352+45.83



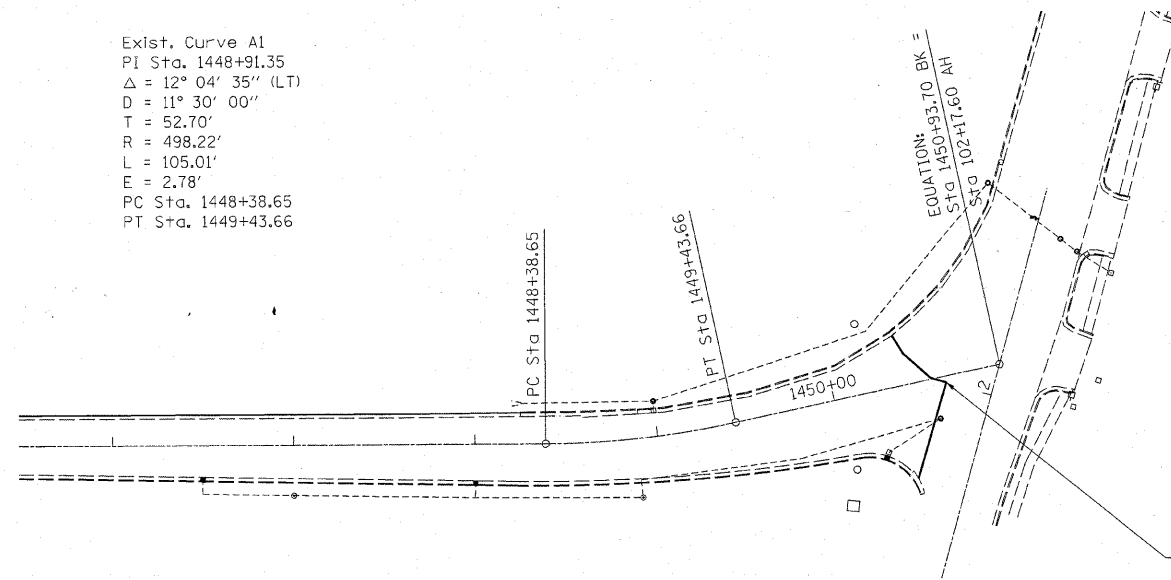
FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEETS			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cc:\pwork\PWIDOT\SWARTZRW\dms54414\ah\plan_74317.dgn		DRAWN -	REVISED -		SCALE: 50	SHEET NO. 6 OF 9 SHEETS	STA. 1343+00 TO STA. 1372+00	2796	(L,KIRS-2)	EFFINGHAM	58	25
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -					CONTRACT NO. 74317				
PLOT DATE = 3/17/2010		DATE -	REVISED -					ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEETS			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\PWIDOT\SWARTZRW\dms54414\shplan.74317.dgn		DRAWN -	REVISED -		SCALE: 50	SHEET NO. 7 OF 9 SHEETS	STA. 1372+00 TO STA. 1402+00	2796	(L,K)RS-2	EFFINGHAM	58	26
		CHECKED -	REVISED -					CONTRACT NO. 74317				
		DATE -	REVISED -					ILLINOIS FED. AID PROJECT				



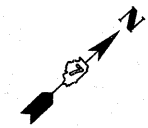
Exist. Curve A1
 PI Sta. 1448+91.35
 $\Delta = 12^\circ 04' 35''$ (LT)
 $D = 11^\circ 30' 00''$
 $T = 52.70'$
 $R = 498.22'$
 $L = 105.01'$
 $E = 2.78'$
 PC Sta. 1448+38.65
 PT Sta. 1449+43.66



FAS 2796 (U.S. RTE. 40)
 SECTION (L,K)RS-2
 EFFINGHAM COUNTY
 ENDS STATION 1450+65



FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEETS			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw_work\PWIDOT\SWARTZRW\dms54414\sh	plan_74317.dgn	DRAWN -	REVISED -		SCALE: 50	SHEET NO. 9 OF 9 SHEETS	STA. 1432+00 TO STA. 1450+65	2796	(L,K)RS-2	EFFINGHAM	58	28	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -										
	PLOT DATE = 3/17/2010	DATE -	REVISED -										
ILLINOIS FED. AID PROJECT													



BASE COURSE WIDENING

1318+75 TO 1320+33	70.2 SQ YD
1318+95 TO 1320+50	68.9 SQ YD
1322+18 TO 1323+61	63.6 SQ YD
1322+34 TO 1324+26	85.3 SQ YD
TOTAL =	288.0 SQ YD

IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3

1319+21	1.0 EACH
1323+46	1.0 EACH
TOTAL =	2.0 EACH

GUTTER REMOVAL

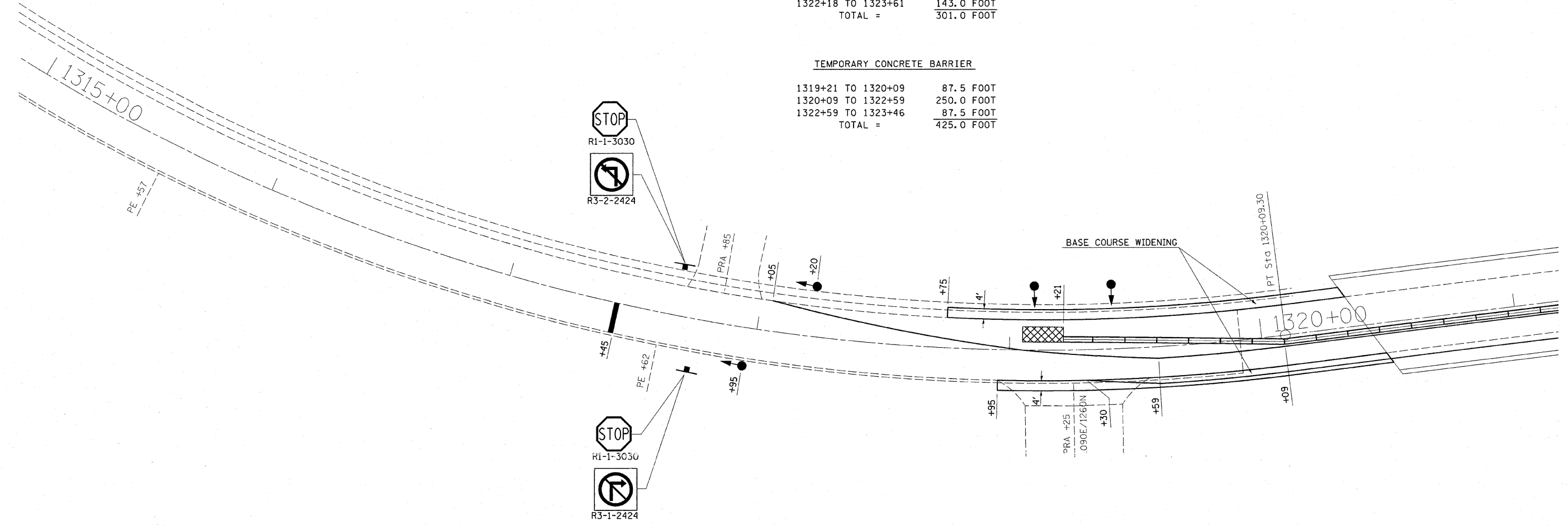
1318+75 TO 1320+33	158.0 FOOT
1322+18 TO 1323+61	143.0 FOOT
TOTAL =	301.0 FOOT

TEMPORARY BRIDGE TRAFFIC SIGNALS

S. N. 025-0087	1.0 EACH
TOTAL =	1.0 EACH

TEMPORARY CONCRETE BARRIER

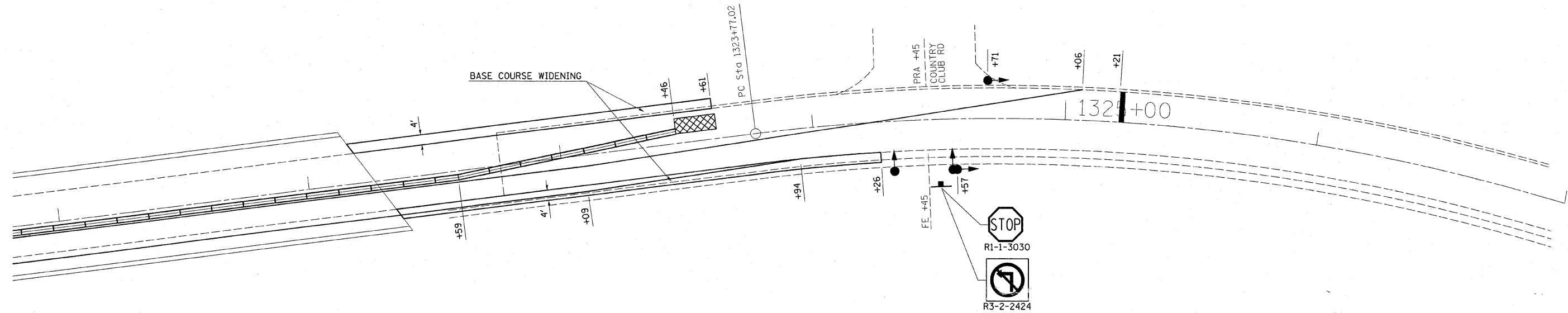
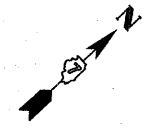
1319+21 TO 1320+09	87.5 FOOT
1320+09 TO 1322+59	250.0 FOOT
1322+59 TO 1323+46	87.5 FOOT
TOTAL =	425.0 FOOT







LEGEND

- TEMPORARY BRIDGE TRAFFIC SIGNALS
- IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE) TEST LEVEL 3
- TEMPORARY CONCRETE BARRIER
- STOP BAR

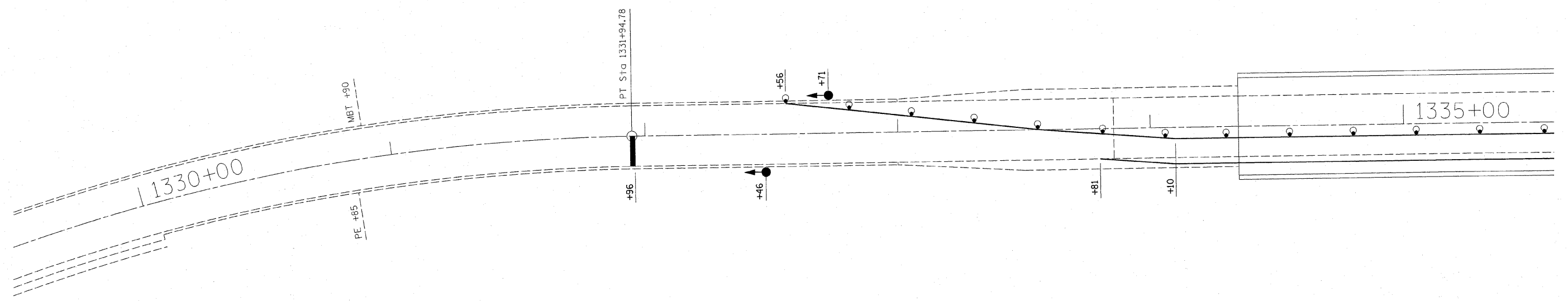
FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 1 TRAFFIC CONTROL SN 025-0087	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\PWIDOT\SWARTZRW\dms54414\d7	4317-sht-stagel.dgn	DRAWN -	REVISED -			2796	(L,K)RS-2	EFFINGHAM	58	29	
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 74317					
	PLOT DATE = 3/17/2010	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



LEGEND

-  TEMPORARY BRIDGE TRAFFIC SIGNALS
-  IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE) TEST LEVEL 3
-  TEMPORARY CONCRETE BARRIER
-  STOP BAR

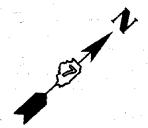
FILE NAME = c:\pwork\pwwdot\SWARTZRW\dms54414\d7	USER NAME = swartzrw 4317-aht-stage1.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 1 TRAFFIC CONTROL SN 025-0087			F.A.S. RTE. 2796	SECTION (L,KIRS-2)	COUNTY EFFINGHAM	TOTAL SHEETS 58	SHEET NO. 30
	PLOT SCALE = 20.0000' / INL	CHECKED -	REVISED -		SCALE: 20	SHEET NO. 2 OF 8 SHEETS	STA.	TO STA.	CONTRACT NO. 74317			
	PLOT DATE = 3/17/2018	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



LEGEND

- ◀● TEMPORARY BRIDGE TRAFFIC SIGNALS
- ▬ STOP BAR
- DRUM WITH STEADY BURNING LIGHT

FILE NAME = c:\pwork\pwidot\SWARTZRW\dms54414\d7	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 1 TRAFFIC CONTROL SN 025-0079			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	4317-ah1-stage1.dgn	DRAWN -	REVISED -		2796	(L,K)RS-2	EFFINGHAM	58	31			
PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -	SCALE: 20 SHEET NO. 3 OF 8 SHEETS STA. TO STA.			CONTRACT NO. 74317						
PLOT DATE = 3/17/2018	DATE -	REVISED -	ILLINOIS FED. AID PROJECT									



RELOCATE TEMPORARY CONCRETE BARRIER

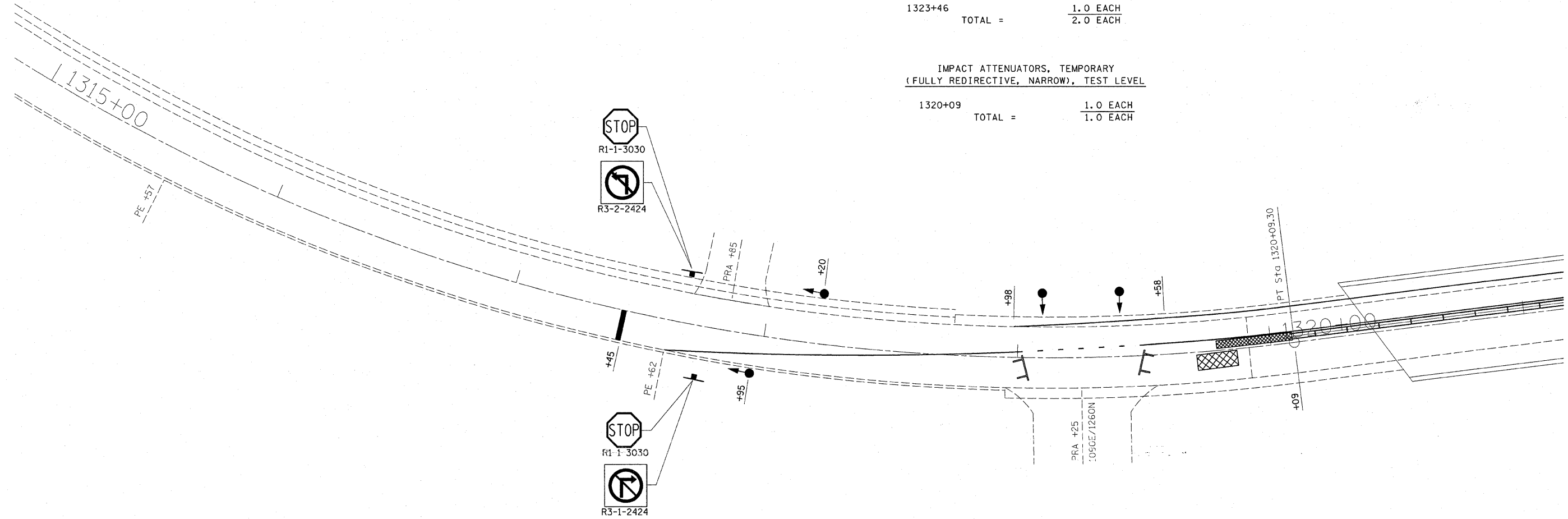
1320+09 TO 1322+59	250.0 FOOT
1322+59 TO 1323+46	87.5 FOOT
TOTAL =	337.5 FOOT

IMPACT ATTENUATORS, RELOCATE
(NON-REDIRECTIVE), TEST LEVEL 3

1319+75	1.0 EACH
1323+46	1.0 EACH
TOTAL =	2.0 EACH

IMPACT ATTENUATORS, TEMPORARY
(FULLY REDIRECTIVE, NARROW), TEST LEVEL

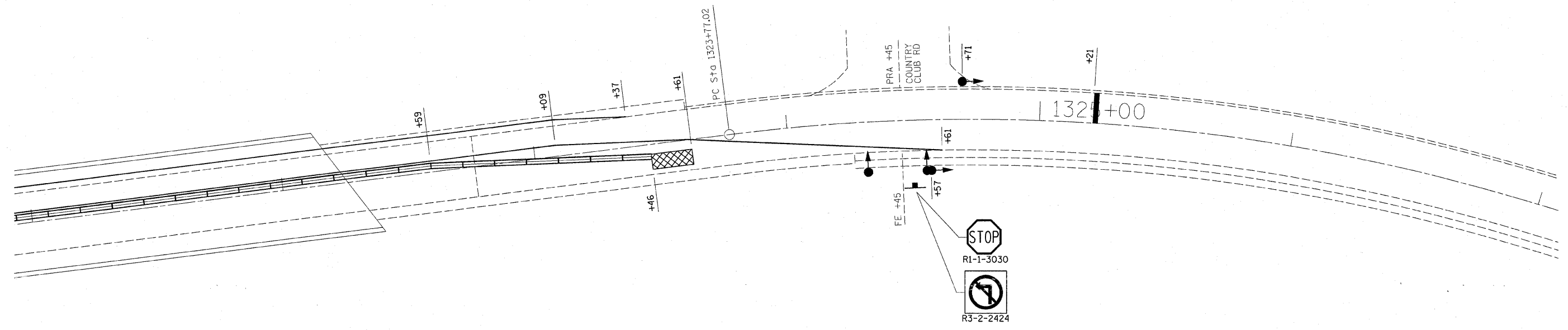
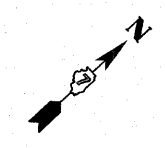
1320+09	1.0 EACH
TOTAL =	1.0 EACH









LEGEND

- TEMPORARY BRIDGE TRAFFIC SIGNALS
- IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE) TEST LEVEL 3
- TEMPORARY CONCRETE BARRIER
- STOP BAR
- IMPACT ATTENUATORS, TEMPORARY (FULLY-REDIRECTIVE, NARROW) TEST LEVEL 3
- TYPE III BARRICADE

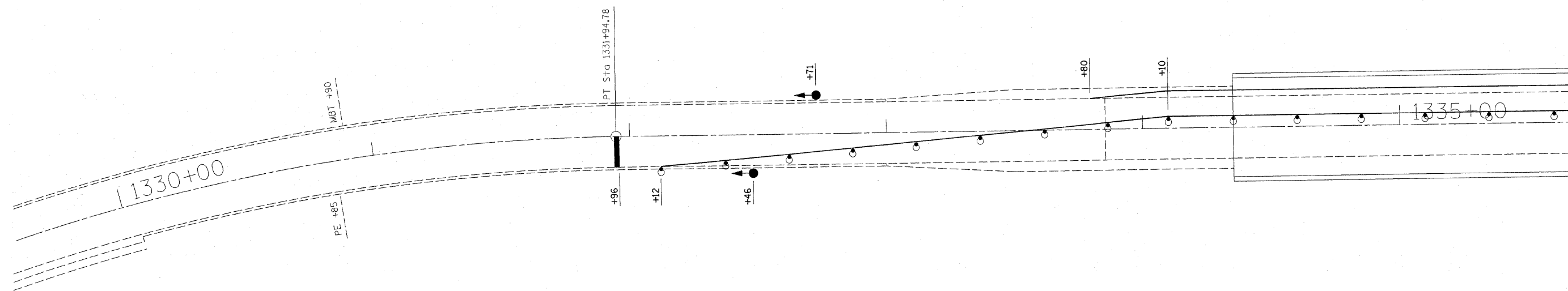
FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 2 TRAFFIC CONTROL SN 025-0087			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw_work\PWIDOT\SWARTZRW\dms54414\d7	4317-ahh-stage2.dgn	DRAWN -	REVISED -		2796	(L,K)RS-2	EFFINGHAM	58	33			
	PLOT SCALE = 28.0000' / IN.	CHECKED -	REVISED -		SCALE: 20			SHEET NO. 5 OF 8 SHEETS STA. TO STA.			CONTRACT NO. 74317	
	PLOT DATE = 3/17/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



LEGEND

-  TEMPORARY BRIDGE TRAFFIC SIGNALS
-  IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE) TEST LEVEL 3
-  TEMPORARY CONCRETE BARRIER
-  STOP BAR
-  IMPACT ATTENUATORS, TEMPORARY (FULLY-REDIRECTIVE, NARROW) TEST LEVEL 3
-  TYPE III BARRICADE

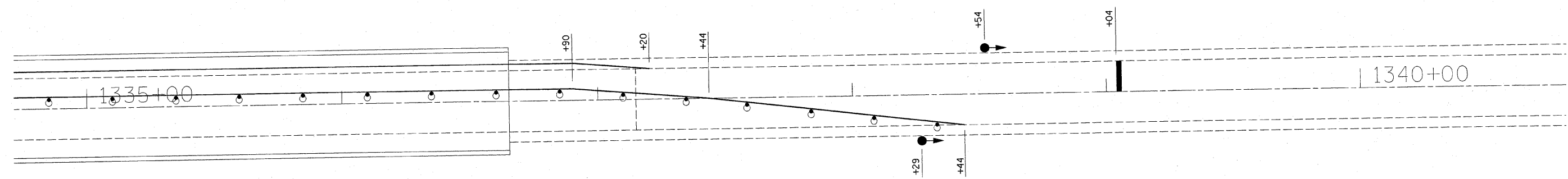
FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 2 TRAFFIC CONTROL SN 025-0087			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
o:\pwork\pwork\SWARTZRW\dms54414\d7	4317-ah1-stage2.dgn	DRAWN -	REVISED -		SCALE: 20	SHEET NO. 6	OF 8	SHEETS	STA.	TO STA.	2796	(L)KRS-2	EFFINGHAM	58	34
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 74317										
	PLOT DATE = 3/17/2018	DATE -	REVISED -		ILLINOIS FED. AID PROJECT										






LEGEND

- TEMPORARY BRIDGE TRAFFIC SIGNALS
- STOP BAR
- DRUM WITH STEADY BURNING LIGHT

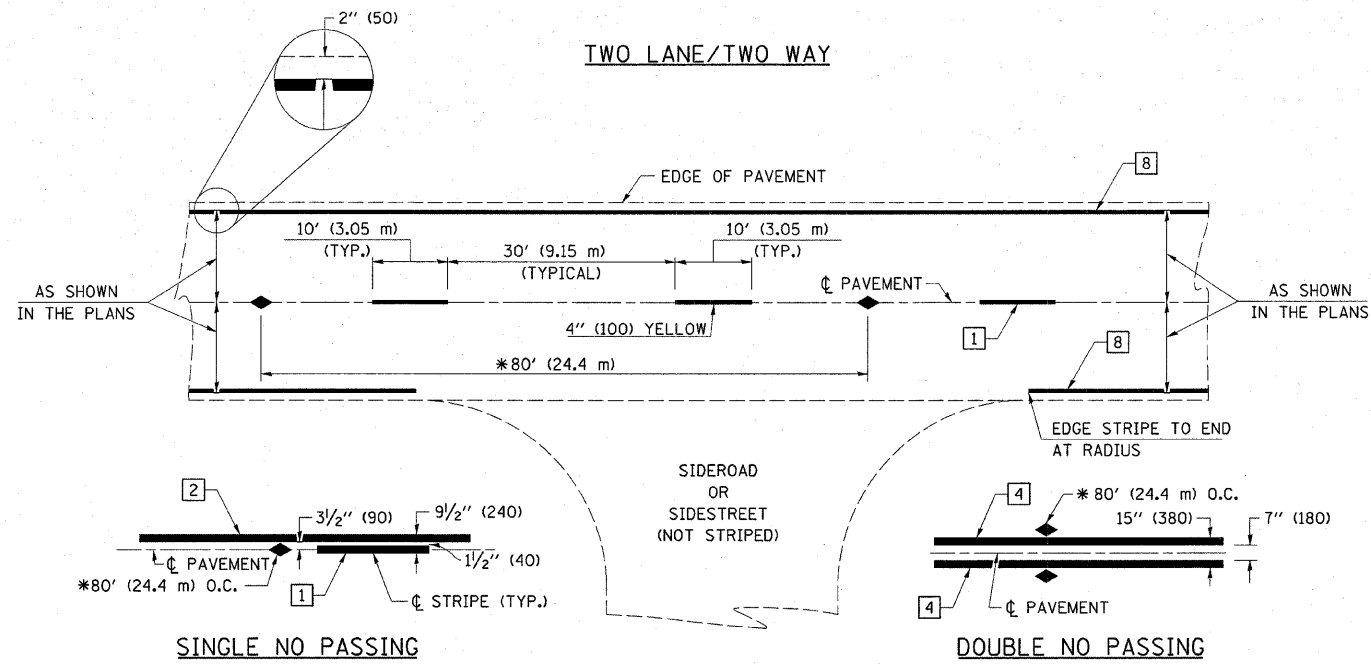
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ca:\pw_work\PW100T\SWARTZRW\dms84414\d7	4317-sh2-stage2.dgn	DRAWN -	REVISED -		SCALE: 20	SHEET NO. 7	OF 8	SHEETS	STA.	TO STA.	2796	58	35
	PLT SCALE = 20.0000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 74317								
	PLT DATE = 3/17/2018	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



LEGEND

-  TEMPORARY BRIDGE TRAFFIC SIGNALS
-  STOP BAR
-  DRUM WITH STEADY BURNING LIGHT

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 2 TRAFFIC CONTROL SN 025-0079			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
c:\pwork\PW1001\SWARTZRW\dms54414\d7	4317-shr-stage2.dgn	DRAWN -	REVISED -		SCALE: 20	SHEET NO. 8	OF 8	SHEETS	STA.	TO STA.	2796	(L,K)RS-2	EFFINGHAM	58	36
	PLOT SCALE = 20.0000 ' / IN.	CHECKED -	REVISED -		CONTRACT NO. 74317										
	PLOT DATE = 3/17/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT										



* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

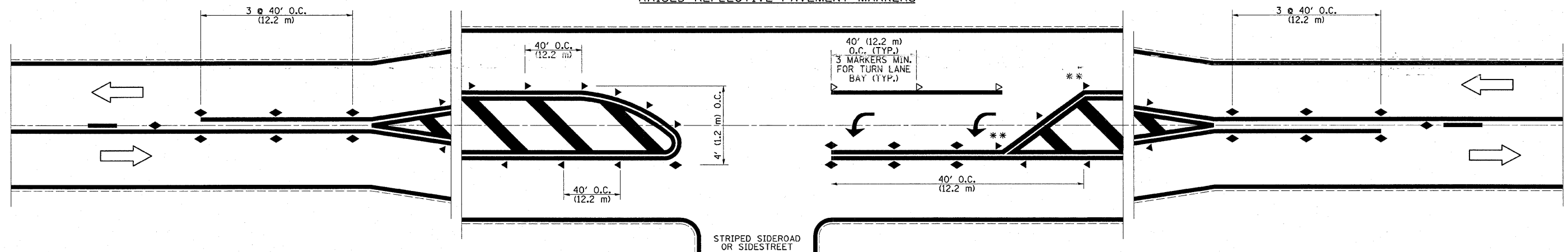
PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 6" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE

TYPICAL PAVEMENT MARKERS LEGEND

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

RAISED REFLECTIVE PAVEMENT MARKERS

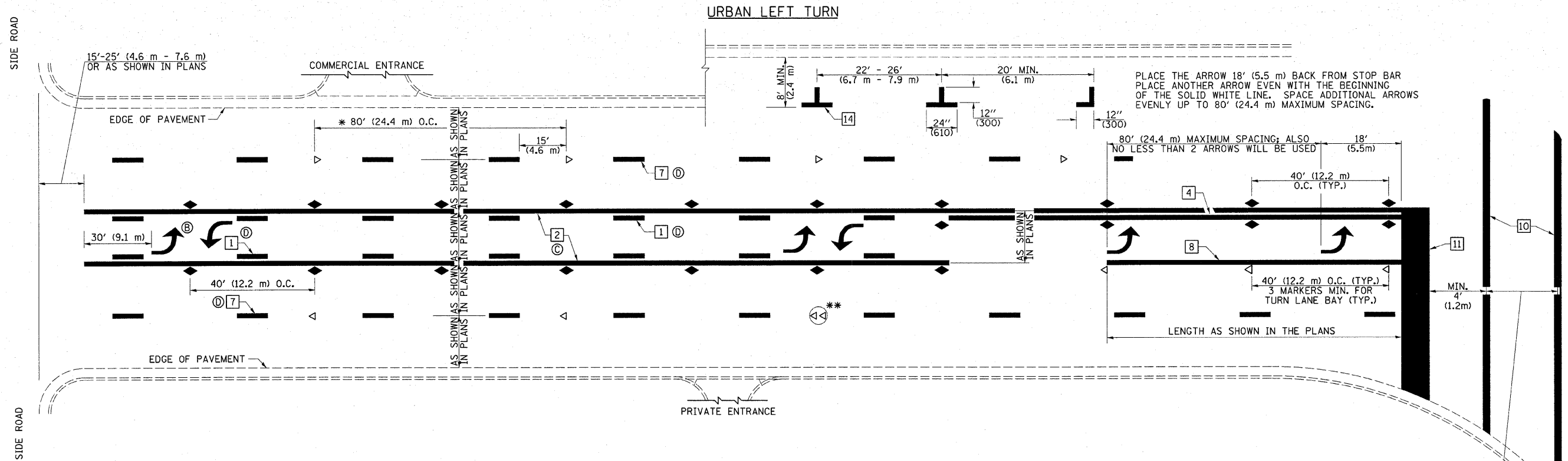


** REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.

NOT TO SCALE
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 78000001

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)		F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd:\pwork\pwork\swartzw\dms54414\shd	tails.74317.dgn	DRAWN -	REVISED -				2796	(L,K)RS-2	EFFINGHAM	58	37
	PLOT SCALE = 20,000' / IN.	CHECKED -	REVISED -				CONTRACT NO. 74317				
	PLOT DATE = 3/17/2018	DATE -	REVISED -				ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

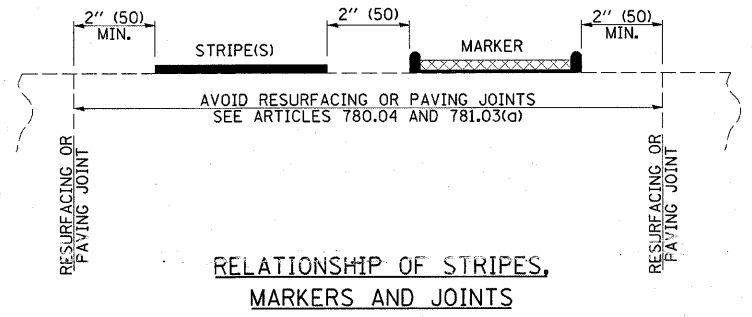
- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 6" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE

* REDUCE TO 40 FEET (12.2 METERS) SPACING ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

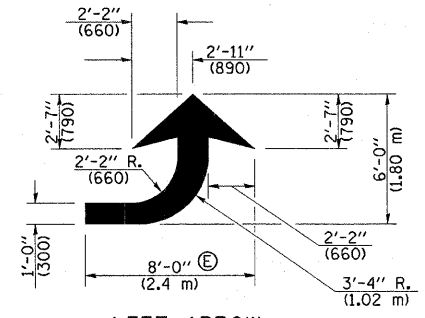
** DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

GENERAL NOTES

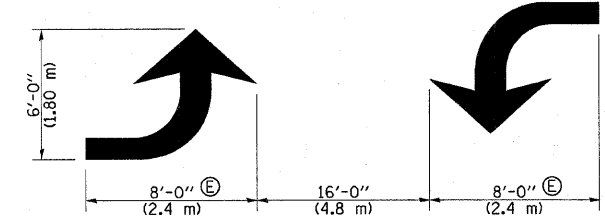
- (B) TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE. USE A MINIMUM OF TWO PAIRS PER BLOCK.
- (C) THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
- (D) THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER.
- (E) USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)



RELATIONSHIP OF STRIPES, MARKERS AND JOINTS



LEFT ARROW
REVERSE FOR RIGHT ARROW
AREA = 15.6 SQ. FT. (1.47 m²) (WHITE)

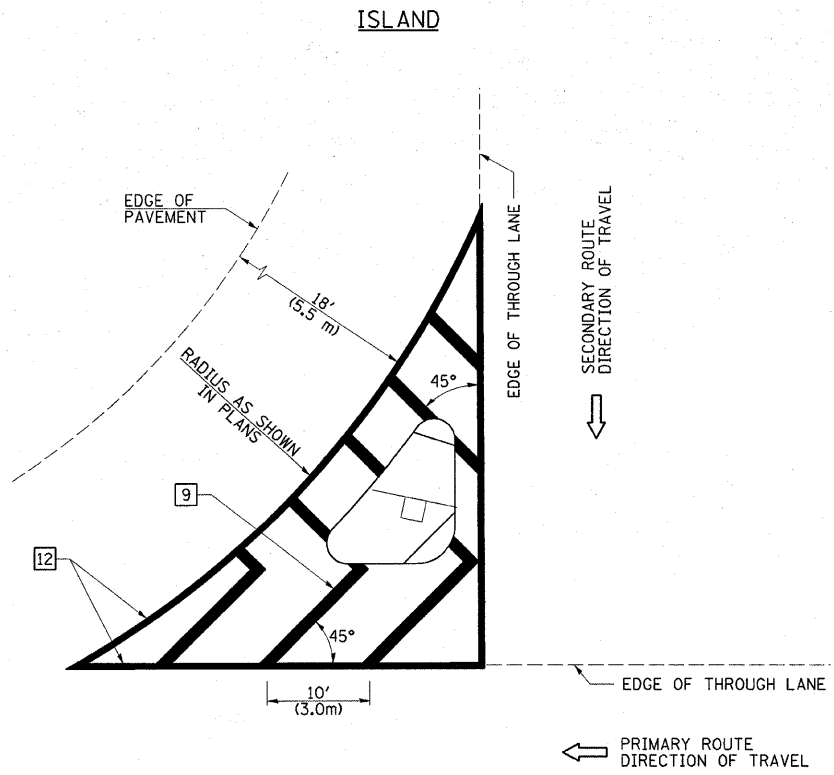


TYPICAL DOUBLE TURN ARROWS (WHITE)

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw_work\pwwdot\swartzw\dms54414\shd\details_74317.dgn		DRAWN -	REVISED -			2796	ILKRS-2	EFFINGHAM	58	38	
		CHECKED -	REVISED -			CONTRACT NO. 74317					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

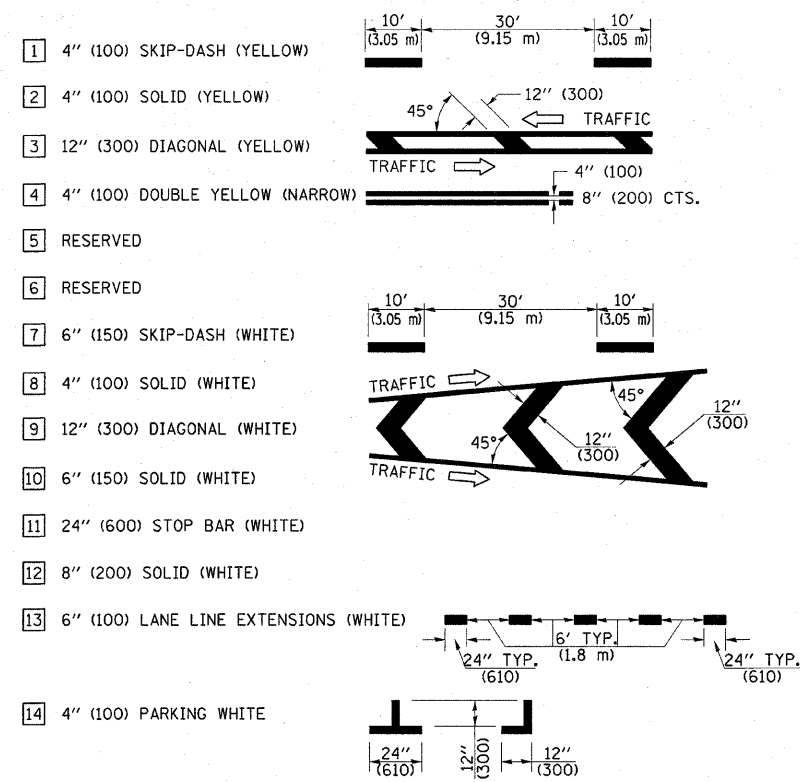


GENERAL NOTES

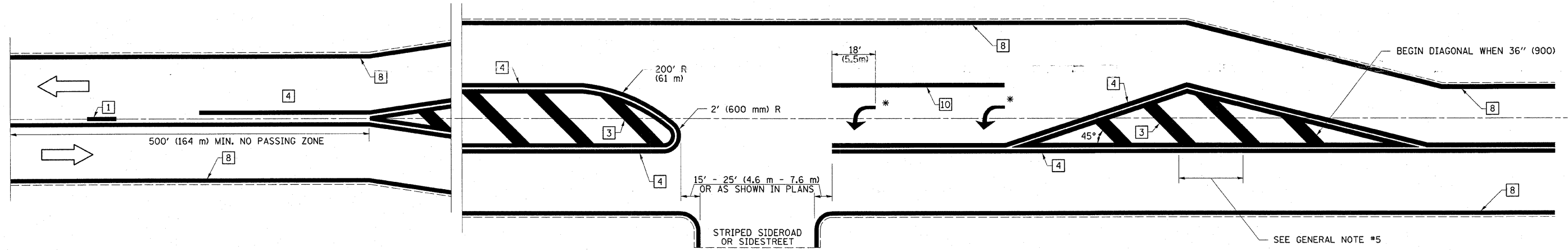
1. RAISED AND CORRUGATED MEDIANS SHALL BE OUTLINED WITH [2] IF PRESENT.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
5. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING:

< 30 MPH (< 50 km/h)	15' (4.5 m)
30-45 MPH (50-75 km/h)	20' (6.0 m)
> 45 MPH (> 75 km/h)	30' (9.0 m)

PAVEMENT MARKING LEGEND



RURAL LEFT TURN STRIPING



* PLACE AN ARROW 18' (5.5 m) BACK FROM STOP BAR. PLACE ANOTHER ARROW EVEN WITH THE BEGINNING OF THE SOLID WHITE LINE. SPACE ADDITIONAL ARROWS EVENLY UP TO 80' (24.4 m) MAXIMUM SPACING. USE MINIMUM OF 2 ARROWS.

SEE GENERAL NOTE #5

NOT TO SCALE

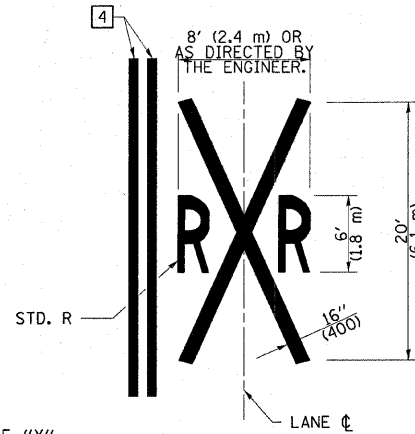
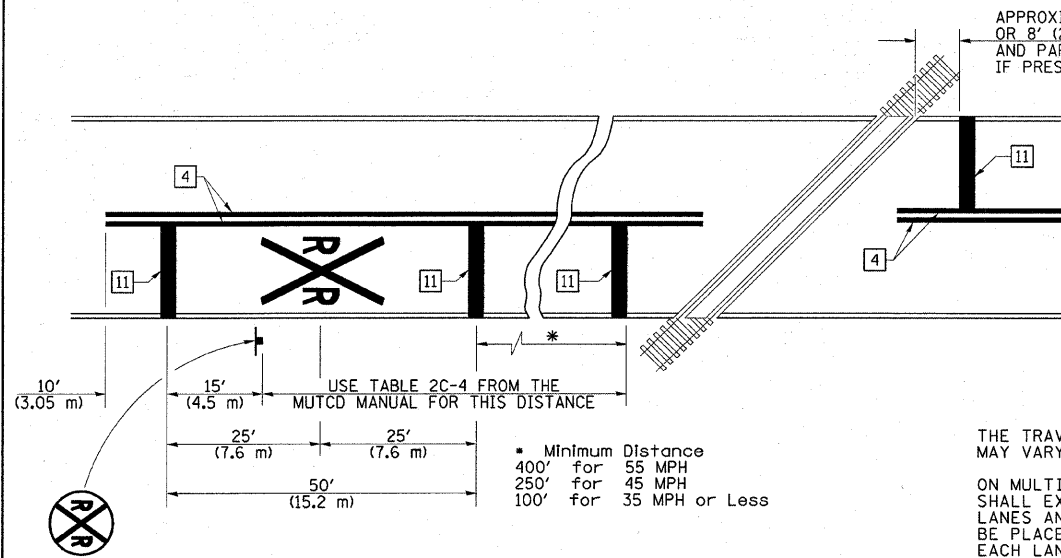
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 7800001

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw_work\pwwork\swartzw\dms54414\shd\detail-74317.dgn	2011-74317.dgn	DRAWN -	REVISED -			2796	(L,K)RS-2	EFFINGHAM	58	39	
PLOT SCALE = 28,0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 74317					
PLOT DATE = 3/17/2018		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING

PAVEMENT MARKING LEGEND



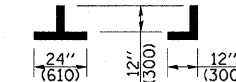
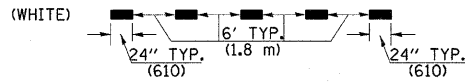
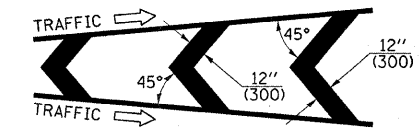
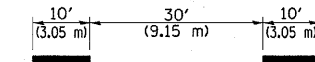
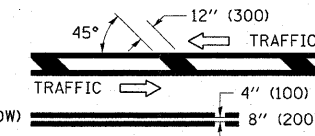
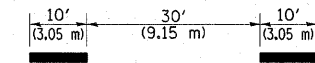
NOTES

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

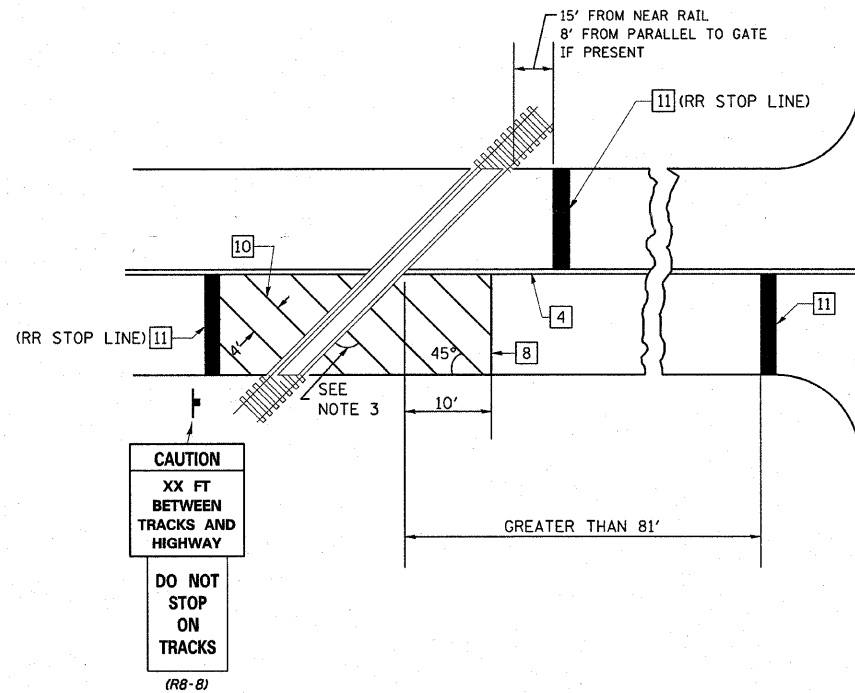
ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.

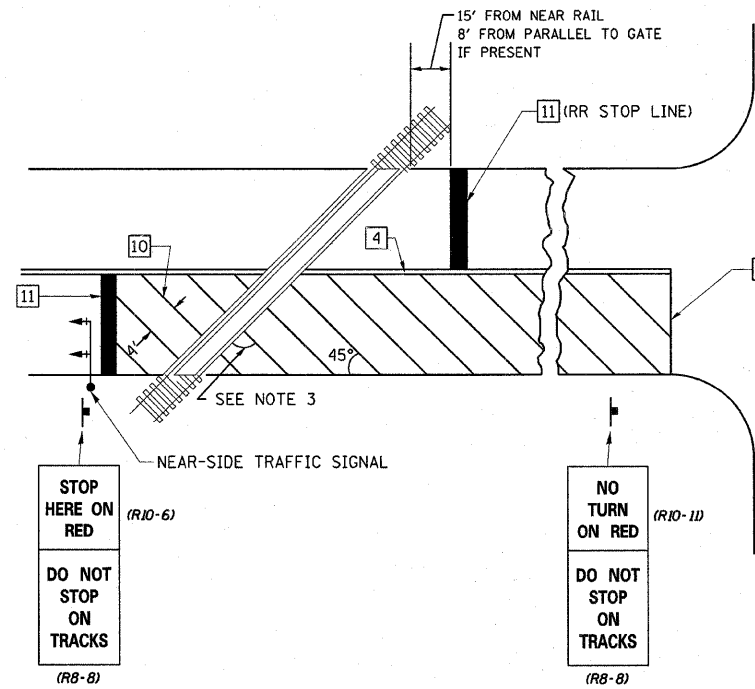
- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 6" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE



RAILROAD CROSSING WITH INTERCONNECT ONLY



RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



GENERAL NOTES

1. SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
2. EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.
3. WHERE THE ANGLE BETWEEN THE DIAGONAL PAVEMENT MARKINGS AND THE TRACK WOULD BE LESS THAN 20°, THE PAVEMENT MARKINGS SHOULD BE PLACED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

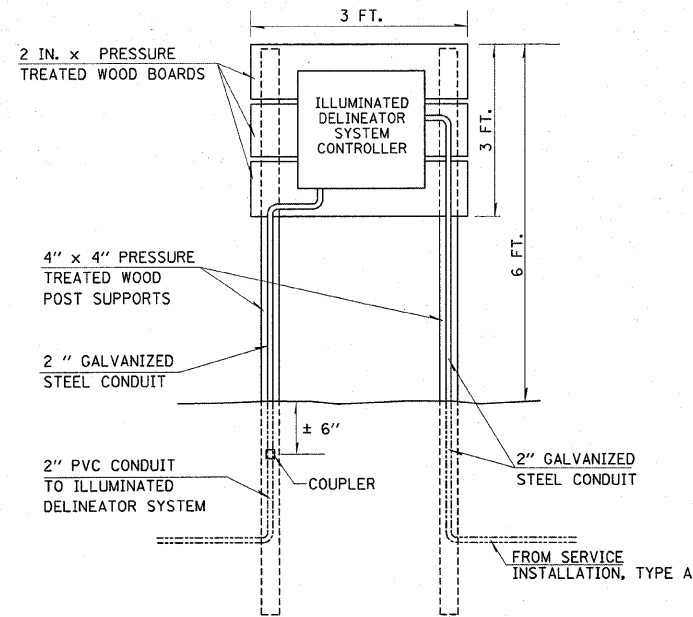
SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

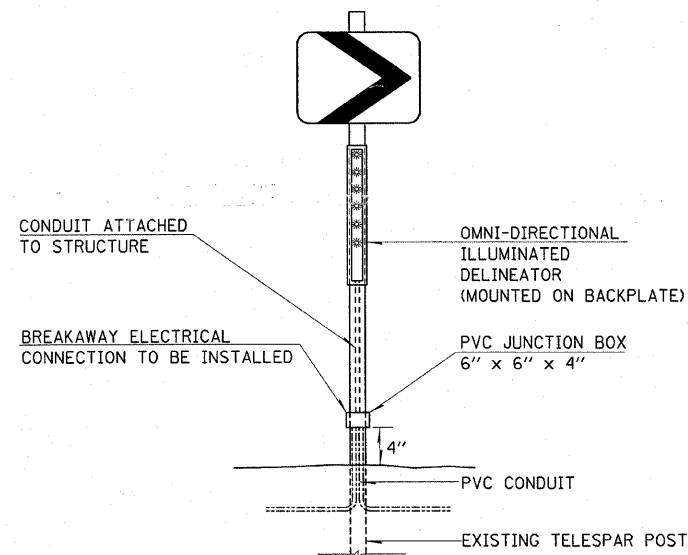
DISTRICT 7 DETAIL NO. 7800001

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwork\swartzw\dms54414\shoto\teils_74317.dgn		DRAWN -	REVISED -			2796	(L,K)RS-2	EFFINGHAM	58	40	
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 74317					
	PLOT DATE = 3/17/2010	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



THIS DETAIL SHALL BE USED IN CONJUNCTION WITH STANDARD 805001

DETAIL OF ILLUMINATED DELINEATOR SYSTEM CONTROLLER INSTALLATION



DETAIL OF ILLUMINATED DELINEATOR

GENERAL NOTES FOR ILLUMINATED DELINEATOR SYSTEM

THIS PROJECT HAS THREE (3) SEPARATE CIRCUIT LOCATIONS. LOCATION 1 IS CIRCUIT "A" AND IS LOCATED ALONG US 40 ON THE NORTH SIDE OF THE HIGHWAY FROM +/-STATION 1301+00 TO +/-STATION 1307+50, LOCATION 2 IS CIRCUIT "B" AND IS LOCATED ALONG US 40 ON THE SOUTH SIDE OF THE HIGHWAY FROM +/-STATION 1307+50 TO +/- STATION 1319+00 AND LOCATION 3 IS CIRCUIT "C" AND IS LOCATION ALONG US 40 ON THE NORTH SIDE OF TH EHIGHWAY FROM +/-STATION 1323+00 TO +/-STATION 1332+00, ALL IN THE COUNTY OF EFFINGHAM. THIS PROJECT CONSISTS OF THE INSTALLATION OF A NEW ILLUMINATED DELINEATOR SYSTEM AT ALL THREE LOCATIONS AND ALL OTHER WORK NECESSARY TO COMPLETE THE SYSTEM.

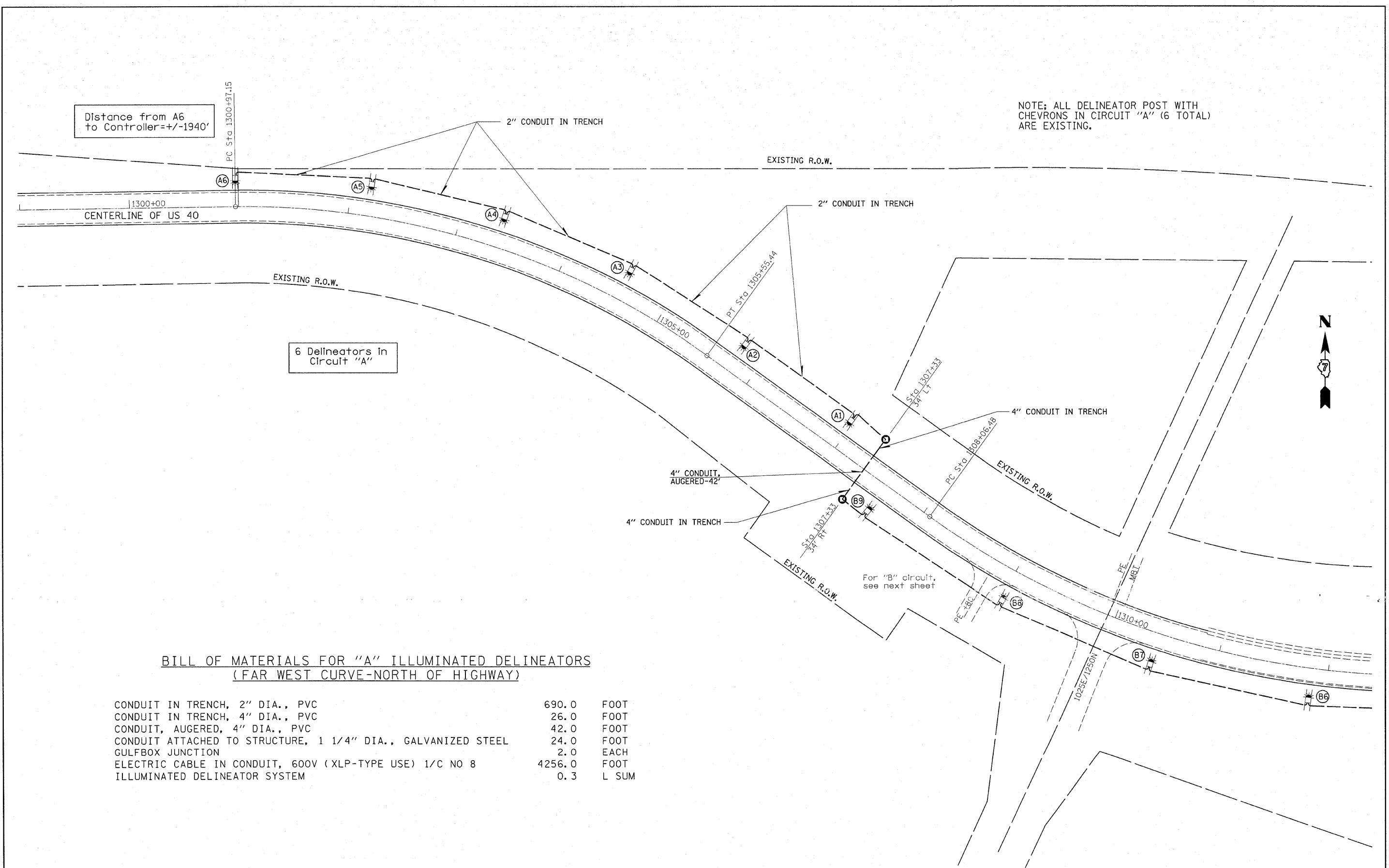
THE LOCATIONS OF ALL ILLUMINATED DELINEATOR SYSTEM COMPONENTS AS SHOWN ON THE PLANS ARE APPROXIMATE ONLY. THE FINAL LOCATION OF ALL COMPONENTS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

THE EXACT LOCATIONS OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE SYSTEM.

THE INSTALLATION OF THE ILLUMINATED DELINEATORS ARE TO BE ON EXISTING SIGN SUPPORTS THAT HAVE CHEVRONS INSTALLED.

THE TRAFFIC OPERATIONS TECHNICIAN (PHIL BOYER) AT THE ILLINOIS DEPARTMENT OF TRANSPORTATION SHALL BE NOTIFIED AT 217-342-8291 PRIOR TO CONSTRUCTION AND AT LEAST 72 HOURS PRIOR TO TURNING ON THE CONTROLLER UNIT.

FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILLUMINATED DELINEATOR SYSTEM GENERAL NOTES & DETAILS		F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca\pw_work\PWIDOT\SWARTZRW\dms54414\D7	4317-sht-Geometr.ccs.dgn	DRAWN -	REVISED -				2796	(L,K)RS-2	EFFINGHAM	58	41	
PLOT SCALE = 48.0000' / IN.		CHECKED -	REVISED -		SCALE: NA		SHEET NO. 1 OF 4 SHEETS		STA. TO STA.		CONTRACT NO. 74317	
PLOT DATE = 3/17/2010		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



NOTE: ALL DELINEATOR POST WITH CHEVRONS IN CIRCUIT "A" (6 TOTAL) ARE EXISTING.

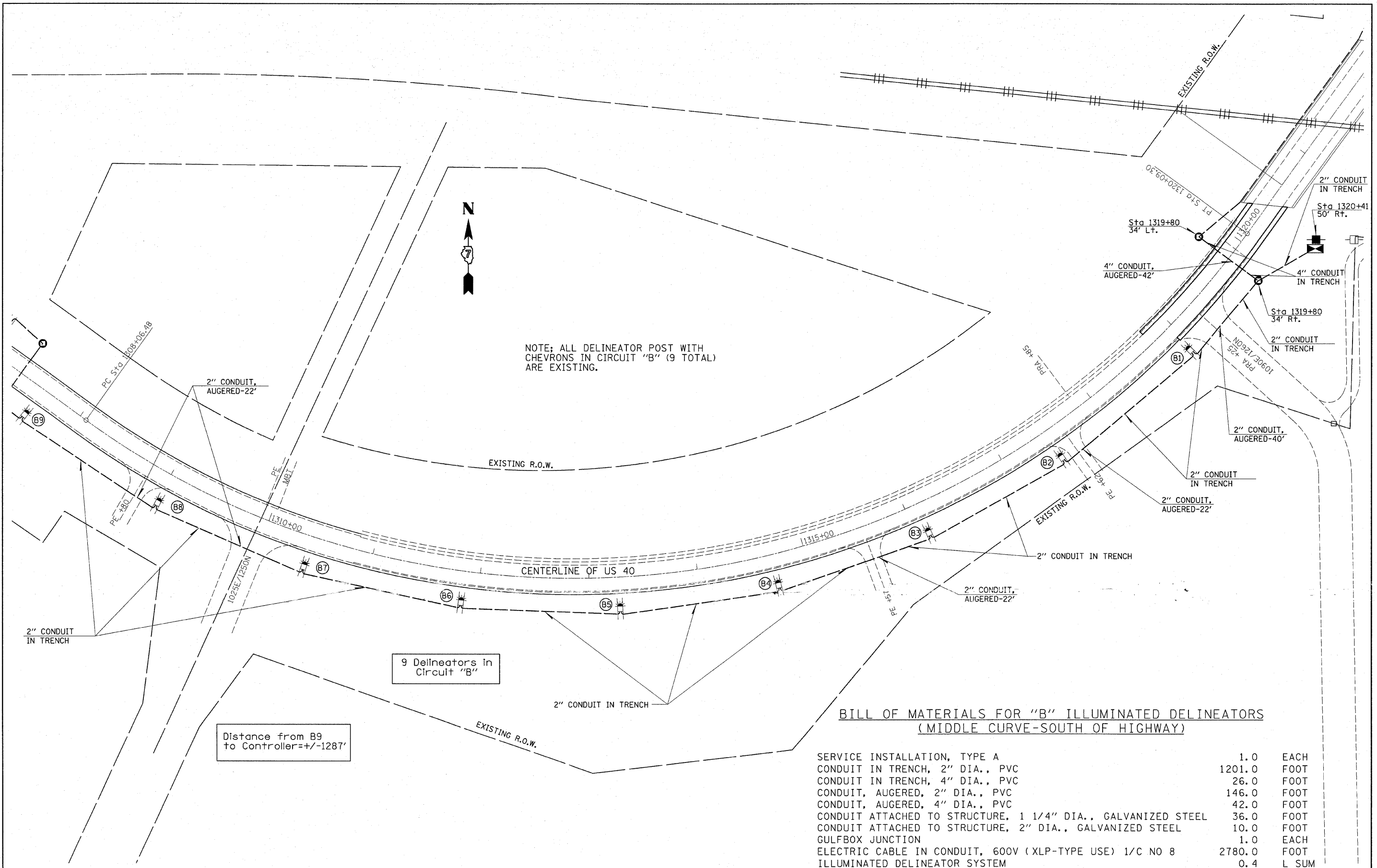
Distance from A6 to Controller = +/-1940'

6 Delineators in Circuit "A"

**BILL OF MATERIALS FOR "A" ILLUMINATED DELINEATORS
(FAR WEST CURVE-NORTH OF HIGHWAY)**

CONDUIT IN TRENCH, 2" DIA., PVC	690.0	FOOT
CONDUIT IN TRENCH, 4" DIA., PVC	26.0	FOOT
CONDUIT, AUGERED, 4" DIA., PVC	42.0	FOOT
CONDUIT ATTACHED TO STRUCTURE, 1 1/4" DIA., GALVANIZED STEEL	24.0	FOOT
GULFBOX JUNCTION	2.0	EACH
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO 8	4256.0	FOOT
ILLUMINATED DELINEATOR SYSTEM	0.3	L SUM

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILLUMINATED DELINEATOR SYSTEM CIRCUIT "A" PLAN LAYOUT			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\FW100T\SWARTZRW\dms54414\07	4317-shr-Geometrios.dgn	DRAWN -	REVISED -		SCALE: 1"=40'	SHEET NO. 2 OF 4 SHEETS	STA. 1301+00 TO STA. 1307+50	2796	(L,K)RS-2	EFFINGHAM	58	42
	PLOT SCALE = 48,0000 "/ IN.	CHECKED -	REVISED -		CONTRACT NO. 74317							
	PLOT DATE = 3/17/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



NOTE: ALL DELINEATOR POST WITH CHEVRONS IN CIRCUIT "B" (9 TOTAL) ARE EXISTING.

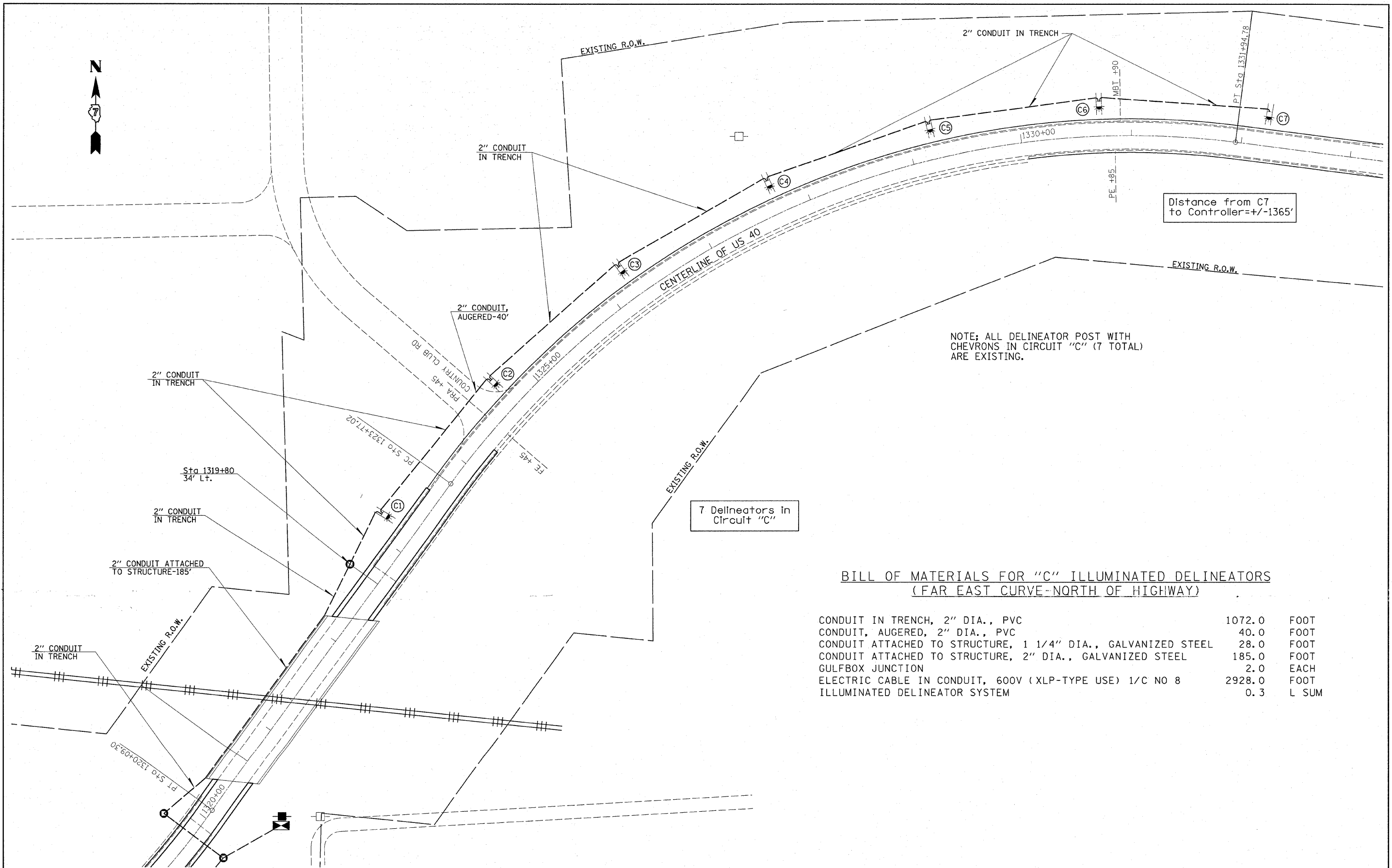
9 Delineators in Circuit "B"

Distance from B9 to Controller = +/-1287'

BILL OF MATERIALS FOR "B" ILLUMINATED DELINEATORS (MIDDLE CURVE-SOUTH OF HIGHWAY)

SERVICE INSTALLATION, TYPE A	1.0	EACH
CONDUIT IN TRENCH, 2" DIA., PVC	1201.0	FOOT
CONDUIT IN TRENCH, 4" DIA., PVC	26.0	FOOT
CONDUIT, AUGERED, 2" DIA., PVC	146.0	FOOT
CONDUIT, AUGERED, 4" DIA., PVC	42.0	FOOT
CONDUIT ATTACHED TO STRUCTURE, 1 1/4" DIA., GALVANIZED STEEL	36.0	FOOT
CONDUIT ATTACHED TO STRUCTURE, 2" DIA., GALVANIZED STEEL	10.0	FOOT
GULFBOX JUNCTION	1.0	EACH
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO 8	2780.0	FOOT
ILLUMINATED DELINEATOR SYSTEM	0.4	L SUM

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILLUMINATED DELINEATOR SYSTEM CIRCUIT "B" PLAN LAYOUT	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\PW\IDOT\SWARTZRW\dms54414\07	4317-shr-Geometrics.dgn	DRAWN -	REVISED -			2796	(L,K)RS-2	EFFINGHAM	58	43	
	PLOT SCALE = 40,0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 74317					
	PLOT DATE = 3/17/2010	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: 1"=40'	SHEET NO. 3 OF 4 SHEETS	STA. 1307+50 TO STA. 1320+00				

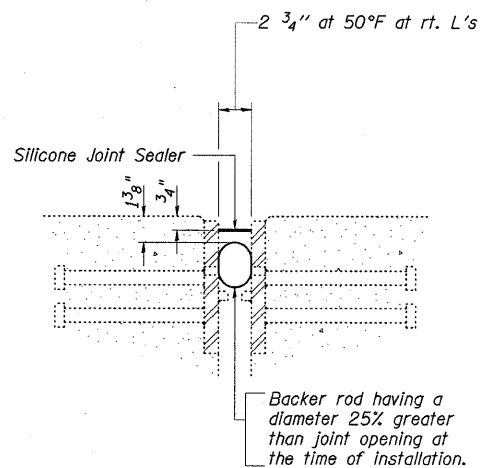
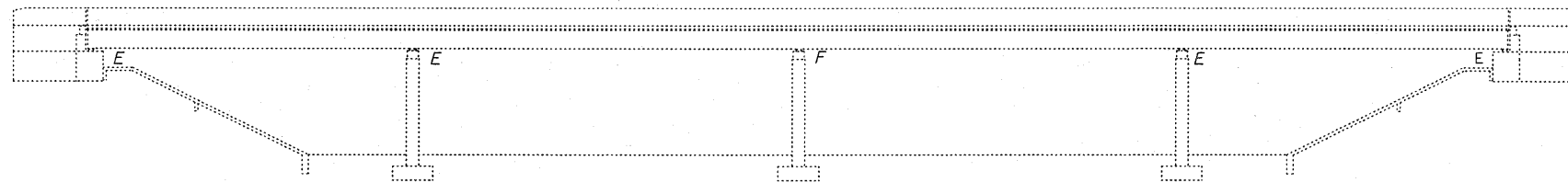


**BILL OF MATERIALS FOR "C" ILLUMINATED DELINEATORS
(FAR EAST CURVE-NORTH OF HIGHWAY)**

CONDUIT IN TRENCH, 2" DIA., PVC	1072.0	FOOT
CONDUIT, AUGERED, 2" DIA., PVC	40.0	FOOT
CONDUIT ATTACHED TO STRUCTURE, 1 1/4" DIA., GALVANIZED STEEL	28.0	FOOT
CONDUIT ATTACHED TO STRUCTURE, 2" DIA., GALVANIZED STEEL	185.0	FOOT
GULFBOX JUNCTION	2.0	EACH
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO 8	2928.0	FOOT
ILLUMINATED DELINEATOR SYSTEM	0.3	L SUM

FILE NAME = c:\pw\work\PWIDOT\SWARTZRW\dms54414\07	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILLUMINATED DELINEATOR SYSTEM CIRCUIT "C" PLAN LAYOUT	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	4317-shr-Geometrics.dgn	DRAWN -	REVISED -			2796	IL,KRS-2	EFFINGHAM	58	44	
	PLOT SCALE = 40,0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 74317					
	PLOT DATE = 3/17/2010	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: 1"=40'	SHEET NO. 4 OF 4 SHEETS		STA. 1320+00 TO STA. 1332+50			

The existing four span steel continuous multi-beam structure was constructed in 1978 as section L-3B-1 at station 1344+00. SN 025-0086 carries US-40 over the Little Wabash River Overflow. Structure repairs were completed in 1999 consisting of new preformed joint seal expansion joints and a new microsilica wearing surface. The proposed project consists of replacing the existing preformed joint seal with a silicone joint sealer.



SILICONE JOINT SEALER DETAIL

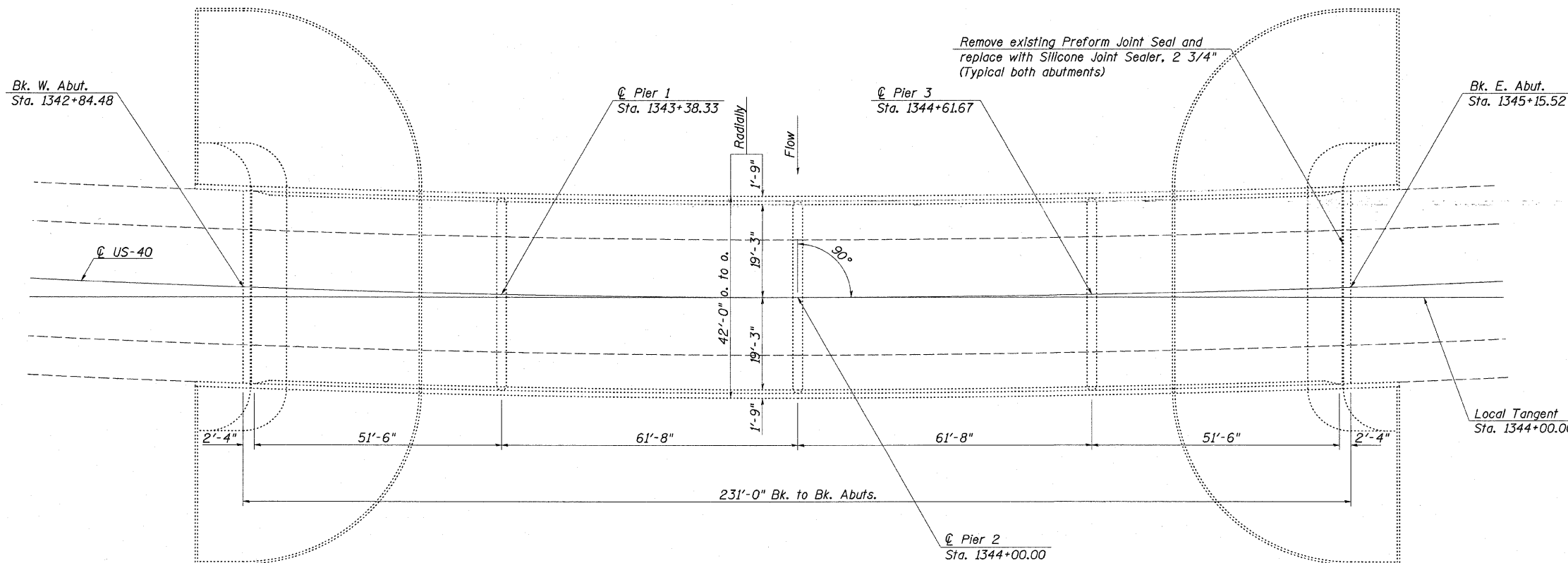
GENERAL NOTES

Plan dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to the construction or ordering of material. Such variations shall not be cause for additional compensation or a change in the scope of the work. The Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Price per lineal foot for Silicone Joint Sealer, 2 3/4" shall include the removal of the existing joint seal.

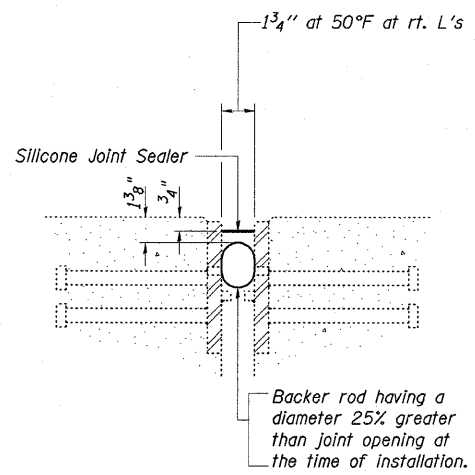
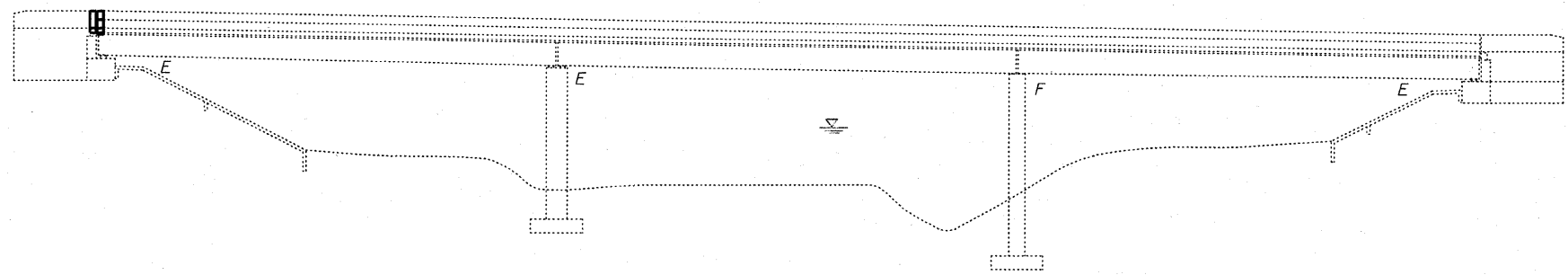
TOTAL BILL OF MATERIALS

ITEM DESCRIPTION	UNIT	QUANTITY
Silicone Joint Sealer, 2 3/4"	Foot	84

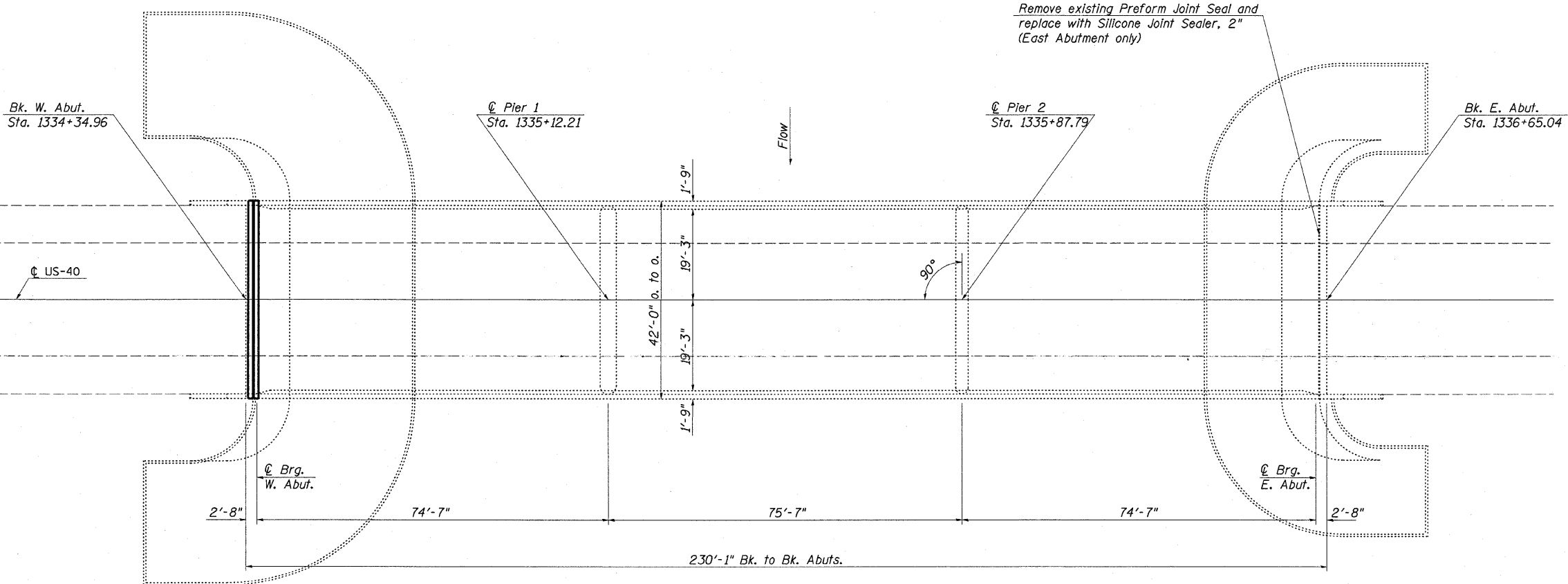


FILE NAME =	USER NAME = swartzw	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES, BILL OF MATERIALS GENERAL PLAN & ELEVATION S.N. 025-0086	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwork\swartzw\dms54418\d77	317-brp.lnprf-0250086.dgn	DRAWN - MEA	REVISED -			2796	(L,KIRS-2	EFFINGHAM	58	45	
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 74317					
	PLOT DATE = 3/18/2010	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

The existing three span prestressed concrete continuous multi-beam structure was constructed in 1977 as section L-3B at station 1344+50. SN 025-0079 carries US-40 over the Little Wabash River. Structure repairs were completed in 1999 consisting a new preformed joint seal expansion joint at the east abutment, new neoprene expansion joint at the west abutment and a new microsilica wearing surface. The proposed project consists of replacing the east abutment existing preformed joint seal with a silicone joint and replacement of the west abutment neoprene joint with a low profile preformed joint strip seal.



SILICONE JOINT SEALER DETAIL
(East Abutment Only)
Quantity = 42.0'



Expires 11/30/2010

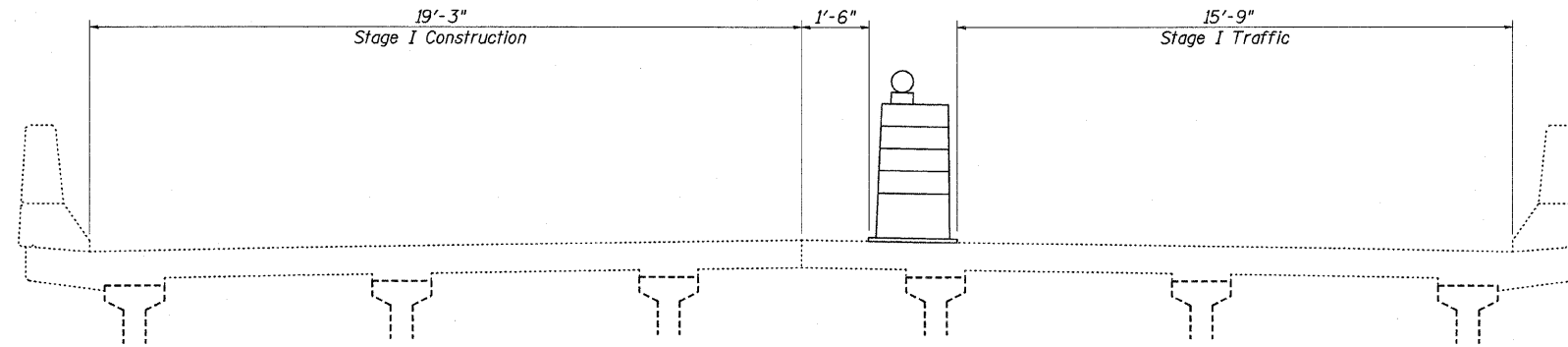
FILE NAME =	USER NAME = swartzrw	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN & ELEVATION S.N. 025-0079	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
01\pwwork\psidot\swartzrw\dms84418\d77-317-brp\Inpr\F-025-0079.dgn		DRAWN - MEA	REVISED -			2796	(L,K)RS-2	EFFINGHAM	58	46	
		CHECKED -	REVISED -			CONTRACT NO. 74317					
		DATE - 3/18/2010	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: NA	SHEET NO. 2 OF 15 SHEETS		STA.	TO STA.		

GENERAL NOTES

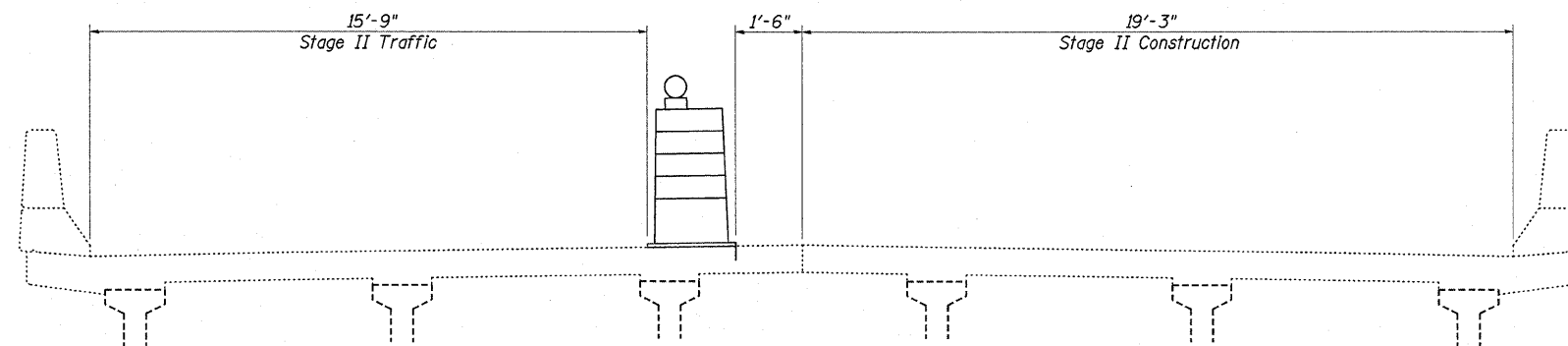
All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
 Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.
 Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
 Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
 Reinforcement bars designated (E) shall be epoxy coated.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work; however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 All structural steel shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type I. Cost included with Preformed Joint Strip Seal.

TOTAL BILL OF MATERIALS

ITEM DESCRIPTION	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	2.5
Concrete Superstructure	Cu. Yd.	2.5
Reinforcement Bars, Epoxy Coated	Pound	330
Bar Splicers	Each	4
Preformed Joint Strip Seal	Foot	42
Silicone Joint Sealer, 1 3/4"	Foot	42



STAGE I LOOKING EAST

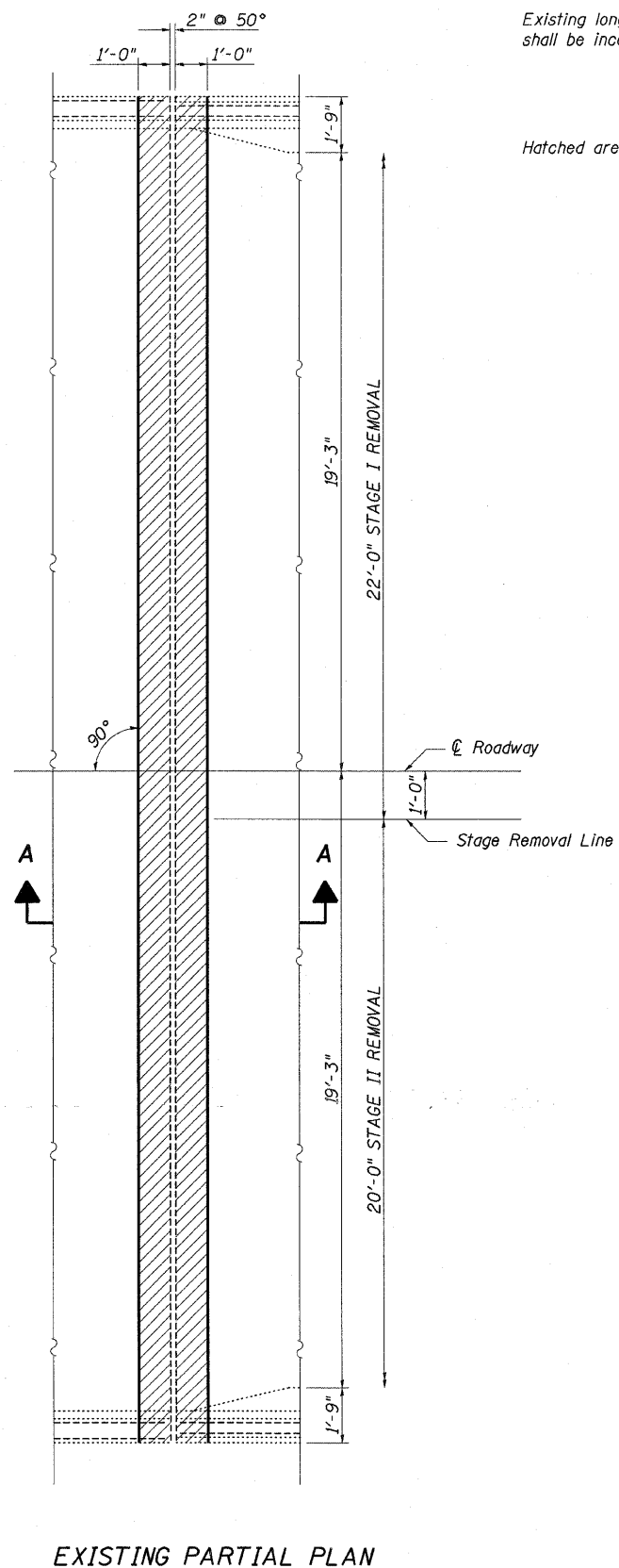
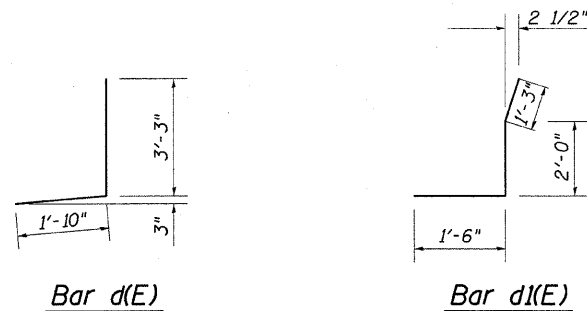


STAGE II LOOKING EAST

FILE NAME =	USER NAME = swartzw	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES, BILL OF MATERIALS, STAGING PLAN S.N. 025-0079	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwwork\pwwork\swartzw\dms54418\077	317-brgennotes-025-0079.dgn	DRAWN - MEA	REVISED -			2796	(L,K)RS-2	EFFINGHAM	58	47	
	PLOT SCALE = 20,0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 74317					
	PLOT DATE = 3/18/2010	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: NA	SHEET NO. 3 OF 15 SHEETS		STA.	TO STA.		

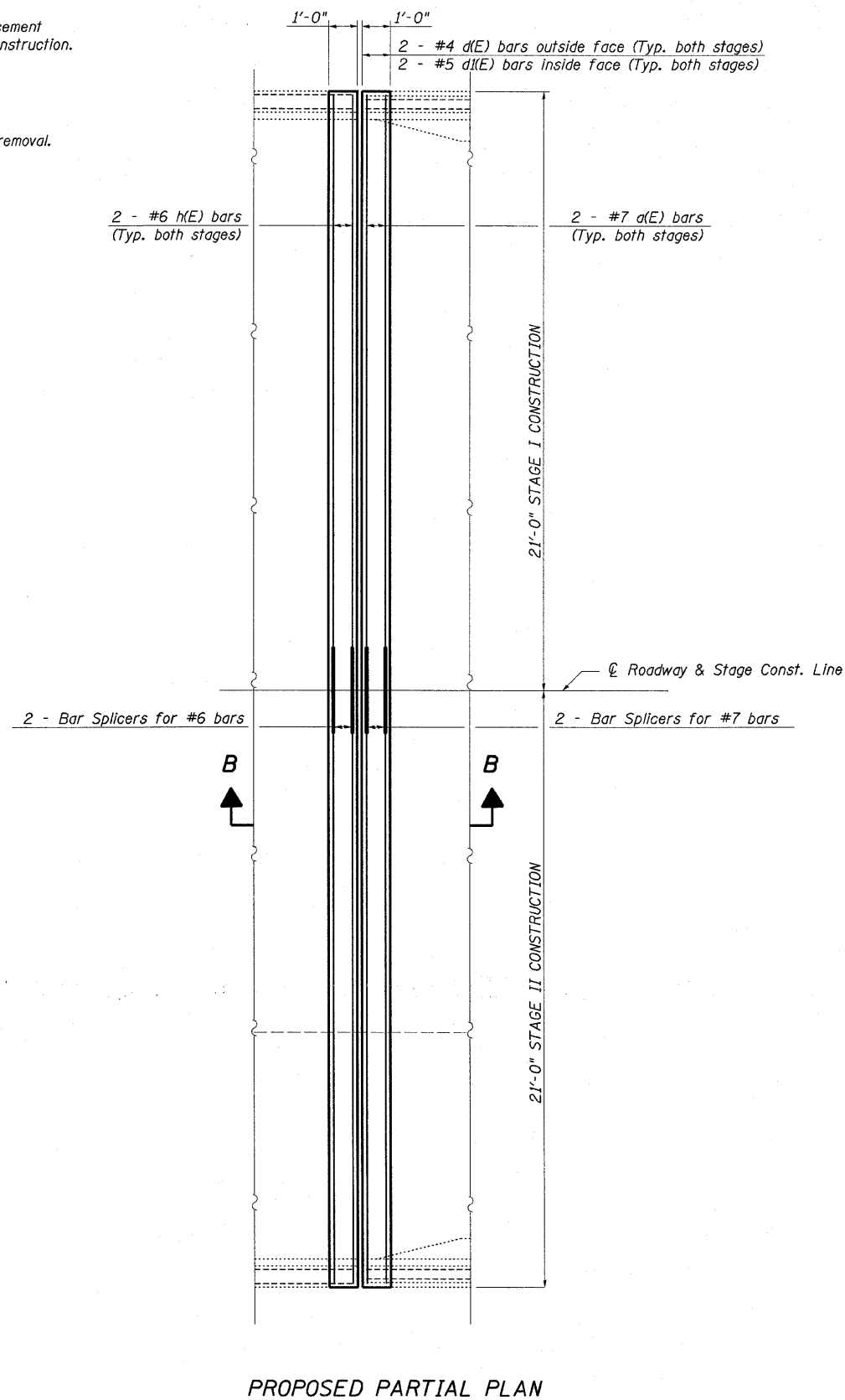
BILL OF MATERIAL

Bar	Number of Bars		Total	Size	Length	Shape
	Stage I	Stage II				
d(E)	2	2	4	#7	20'-8"	—
d(E)	2	2	4	#4	5'-1"	J
d(E)	2	2	4	#5	4'-9"	J
n(E)	2	2	4	#6	20'-8"	—
Concrete Removal			Cu. Yd.	2.5		
Reinforcement Bars (Epoxy Coated)			Lbs.	330		
Concrete Superstructure			Cu. Yd.	2.5		



Existing longitudinal deck reinforcement shall be incorporated into new construction.

Hatched area indicates concrete removal.



FILE NAME = c:\pwork\pwork\swartzw\dms54418\d774317-brgdetails-0250079.dgn

USER NAME = swartzw
 DESIGNED - MEA
 DRAWN - MEA
 CHECKED -
 DATE -
 PLOT SCALE = 20.0000 1 / IN.
 PLOT DATE = 3/18/2010

DESIGNED - MEA
 DRAWN - MEA
 CHECKED -
 DATE -
 REVISED -
 REVISED -
 REVISED -
 REVISED -

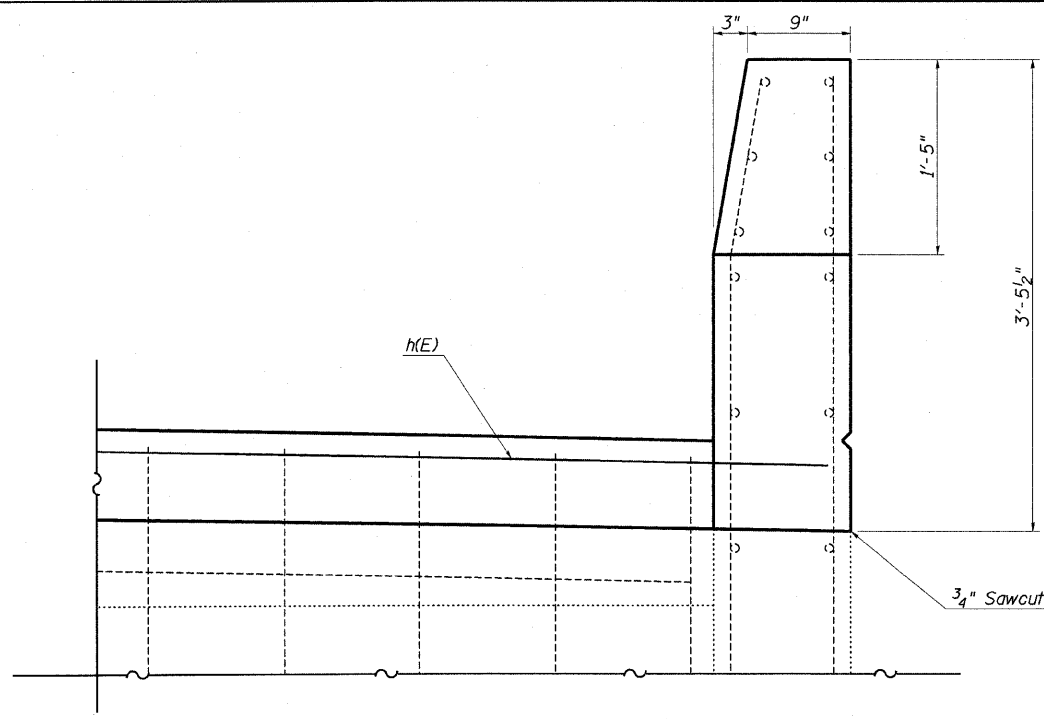
REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

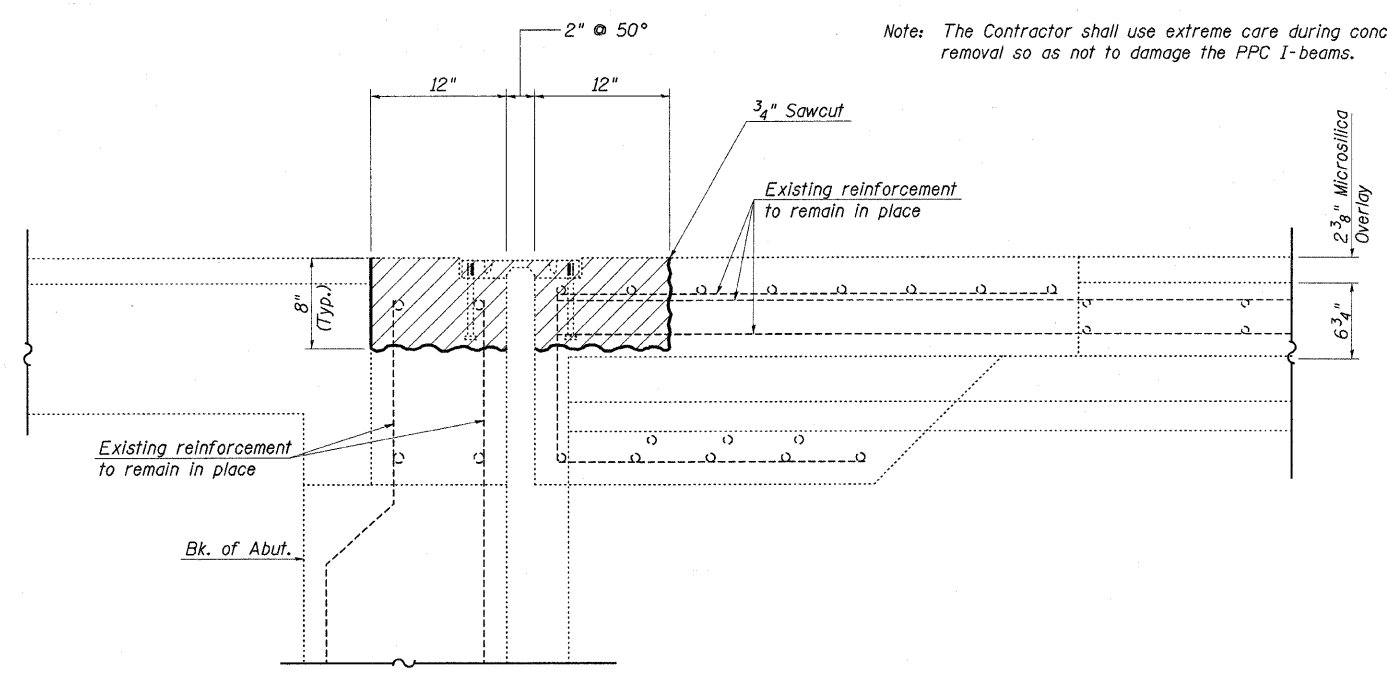
**W. ABUTMENT EXPANSION JOINT REPLACEMENT DETAILS
 S.N. 025-0079**

SCALE: NA SHEET NO. 4 OF 15 SHEETS STA. TO STA.

F.A.S. RTE. 2796	SECTION (L,K)RS-2	COUNTY EFFINGHAM	TOTAL SHEETS 58	SHEET NO. 48
CONTRACT NO. 74317			ILLINOIS FED. AID PROJECT	



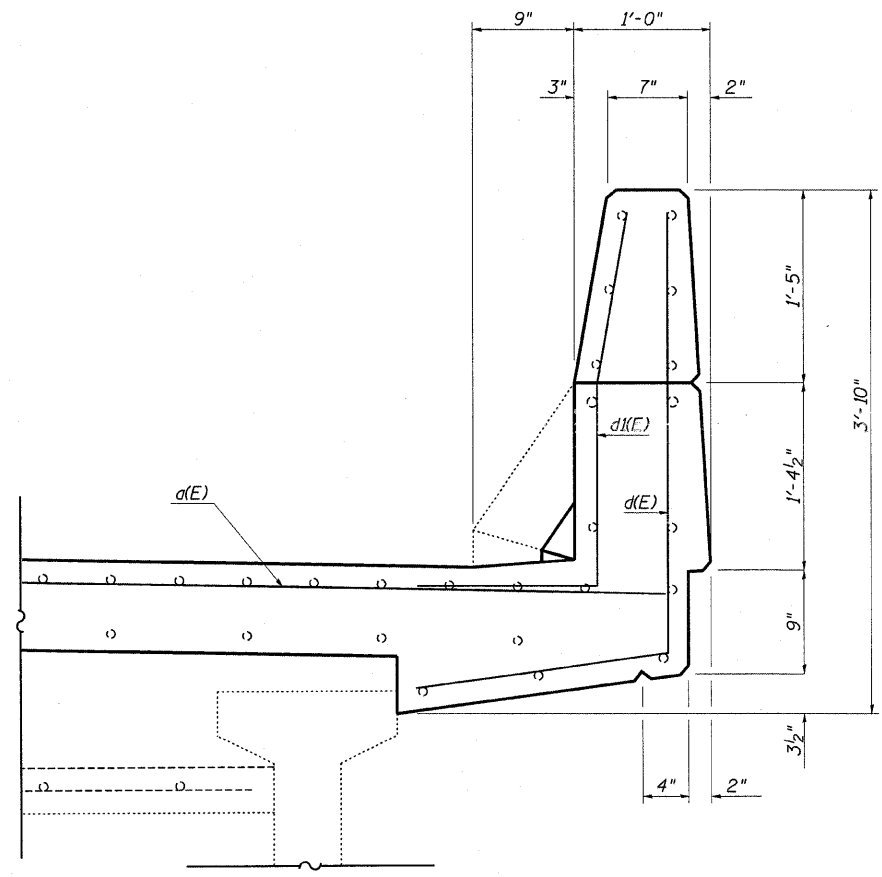
SECTION THRU WINGWALL



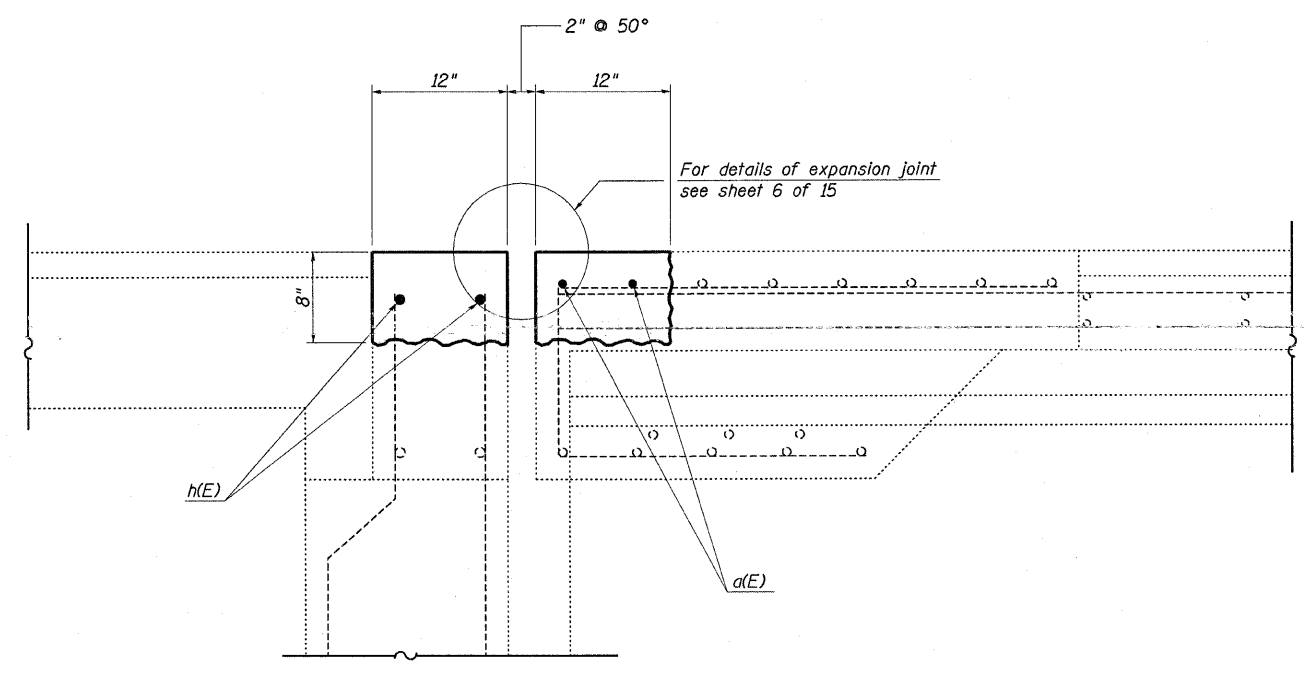
SECTION A-A

(Near \bar{C} of Roadway)

- Existing Reinforcement
- Proposed Reinforcement



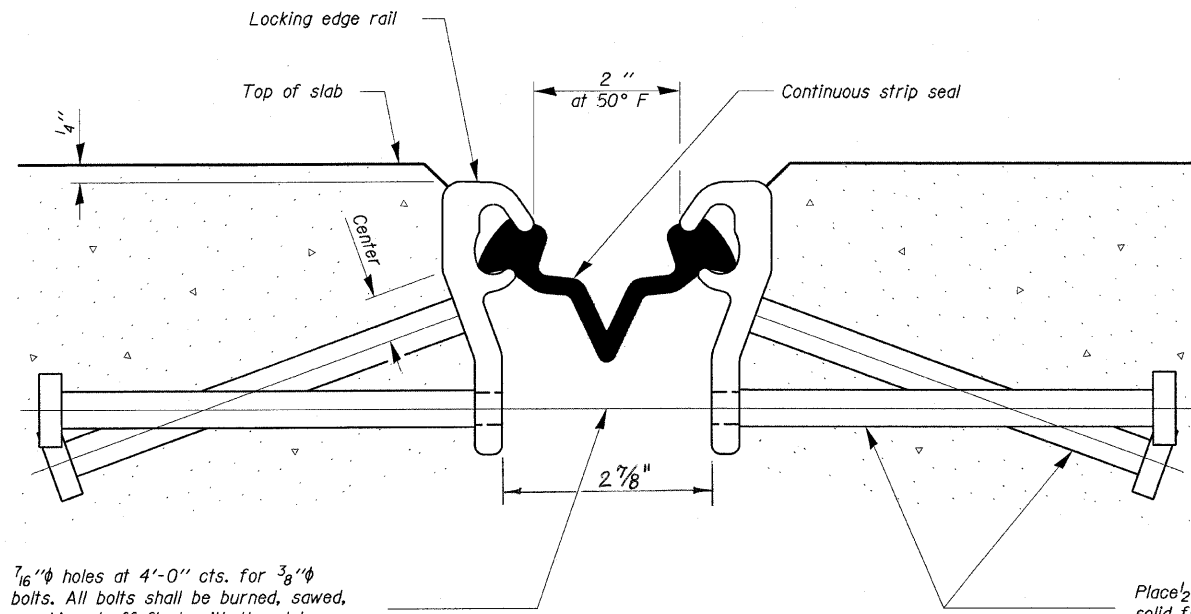
SECTION THRU PARAPET



SECTION B-B

(Near \bar{C} of Roadway)

FILE NAME =	USER NAME = swartzw	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	W. ABUTMENT EXPANSION JOINT REPLACEMENT DETAILS S.N. 025-0079		F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0:\pw_work\pw\dot\swartzw\dms54418\d77	317-brgdetail1-0250079.dgn	DRAWN - MEA	REVISED -				2796	(L,K)RS-2	EFFINGHAM	58	49
	PLOT SCALE = 20,0000 ' / IN.	CHECKED -	REVISED -				CONTRACT NO. 74317				
	PLOT DATE = 3/18/2010	DATE -	REVISED -				ILLINOIS FED. AID PROJECT				
					SCALE: NA	SHEET NO. 5 OF 15 SHEETS	STA.	TO STA.			

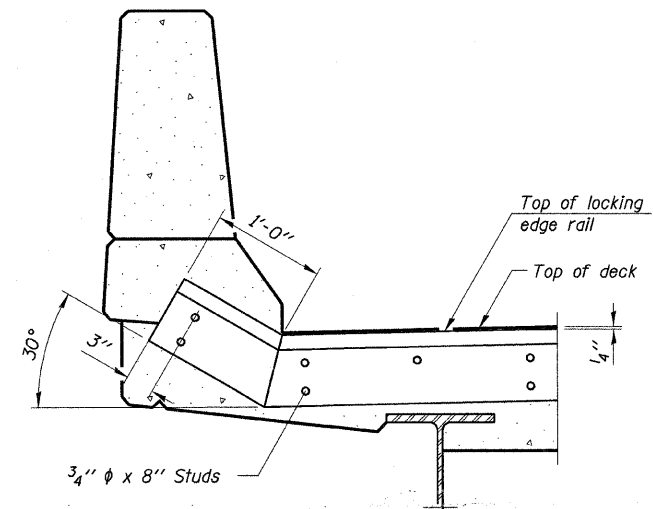


7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

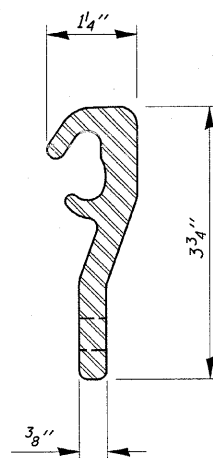
Place 1/2" \pm x 6" granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0" alt. cts.

SECTION THRU STRIP SEAL JOINT

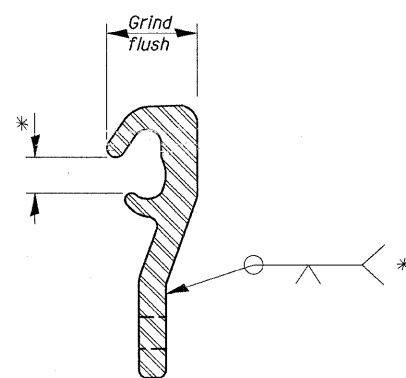
Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.
 The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.
 The inside of the Locking Edge Rail groove shall be free of weld residue.
 Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.
 The manufacturer's recommended installation methods shall be followed.
 Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.



AT PARAPET



LOCKING EDGE RAIL



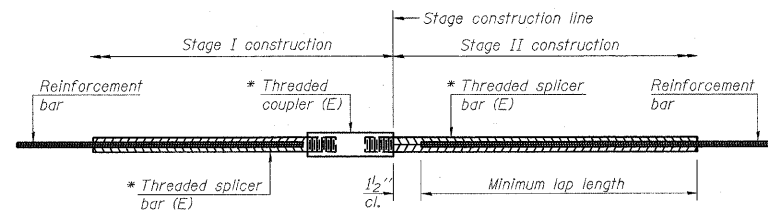
LOCKING EDGE RAIL SPLICE

* Omit weld at seal opening.

BILL OF MATERIAL

SN	Item	Unit	Total
025-0079	Preformed Joint Strip Seal	Foot	42

FILE NAME =	USER NAME = swartzn	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHALLOW PREFORMED JOINT STRIP SEAL S.N. 025-0079		F.A.S. RTE. 2796	SECTION (L,K)RS-2	COUNTY EFFINGHAM	TOTAL SHEETS 58	SHEET NO. 50	
es:\pw\work\pwwdot\swartzn\dms54418\d774317-brgdetails-0250079.dgn	PLOT SCALE = 20.0000 / IN.	DRAWN - MEA	REVISED -		SCALE: NA	SHEET NO. 6 OF 15 SHEETS	STA. TO STA.	CONTRACT NO. 74317				
	PLOT DATE = 3/18/2018	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



STANDARD BAR SPLICER ASSEMBLY

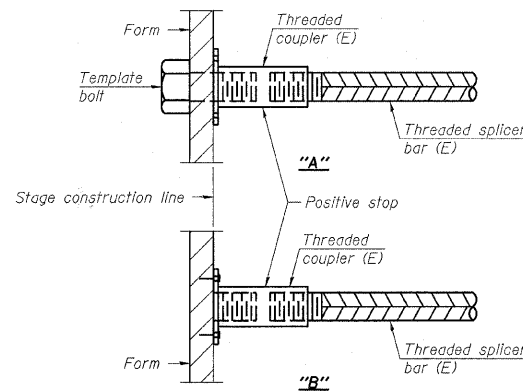
Bar size to be spliced	Minimum Lap Lengths			
	Table 1	Table 2	Table 3	Table 4
3, 4	1'-5"	1'-11"	2'-1"	2'-4"
5	1'-9"	2'-5"	2'-7"	2'-11"
6	2'-1"	2'-11"	3'-1"	3'-6"
7	2'-9"	3'-10"	4'-2"	4'-8"
8	3'-8"	5'-1"	5'-5"	6'-2"
9	4'-7"	6'-5"	6'-10"	7'-9"

Table 1: Black bar, 0.8 Class C
 Table 2: Black bar, Top bar lap, 0.8 Class C
 Table 3: Epoxy bar, 0.8 Class C
 Table 4: Epoxy bar, Top bar lap, 0.8 Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

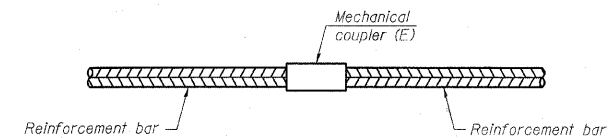
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
025-0079	#7	2	3
025-0079	#6	2	3



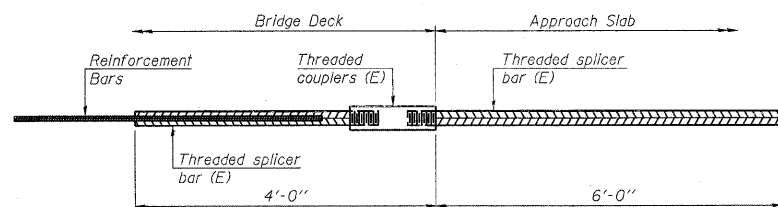
INSTALLATION AND SETTING METHODS

"A": Set bar splicer assembly by means of a template bolt.
 "B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



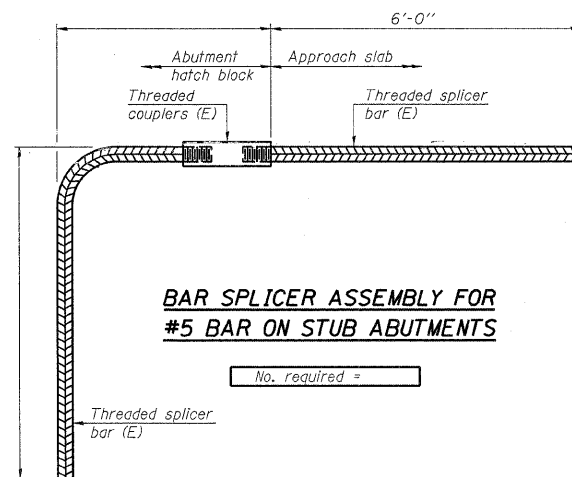
STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

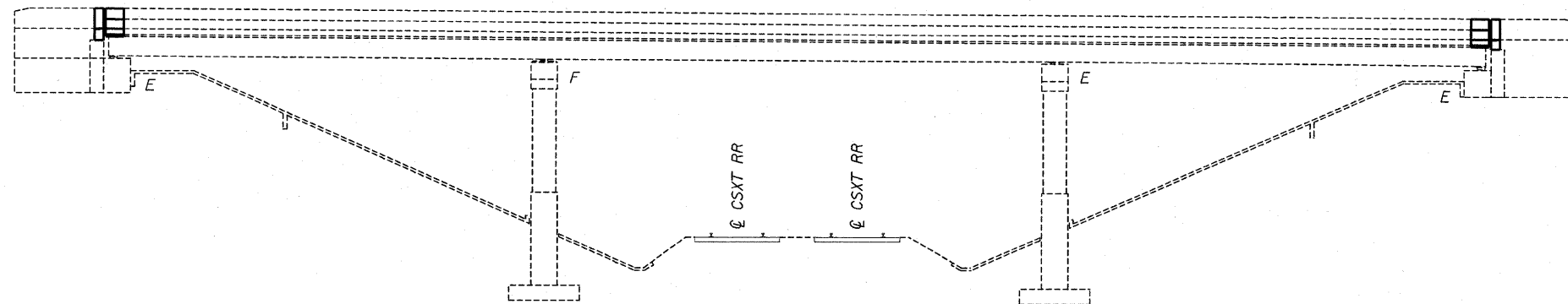
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 509 of the Standard Specifications.
 See special provision for Mechanical Splicers.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

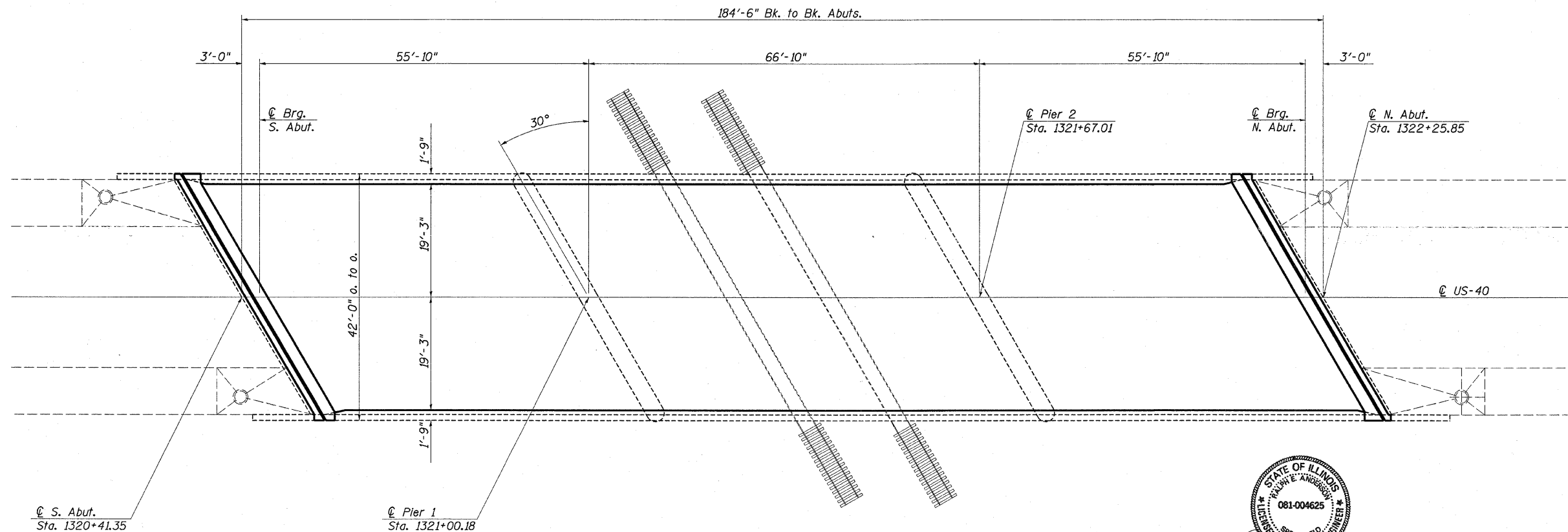
11-1-09

FILE NAME =	USER NAME = swartzw	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BAR SPLICER ASSEMBLY DETAILS S.N. 025-0079			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pw\work\pw\dot\swartzw\dms54418\d77	317-brgdetails-0250079.dgn	DRAWN - MEA	REVISED -					2796	(L,K)RS-2	EFFINGHAM	58	51
	PLOT SCALE = 28.0000' / 1" IN.	CHECKED -	REVISED -					CONTRACT NO. 74317				
	PLOT DATE = 3/18/2010	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				
								SCALE: NA	SHEET NO. 7 OF 15 SHEETS	STA.	TO STA.	

The existing three span steel continuous multi-beam structure was constructed in 1979 as section L-3VB at station 1321+33.29 carrying US-40 over the CSXT RR. The proposed project consists of replacing the existing expansion joints, new wearing surface and formed concrete repair.



ELEVATION



PLAN



Expires 11/30/2010

FILE NAME =	USER NAME = swartzw	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN & ELEVATION S.N. 025-0087		F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw_work\pwidot\swartzw\dms54418\d77	317-bridgeswork.dgn	DRAWN - MEA	REVISED -		SCALE: NA	SHEET NO. 8 OF 15 SHEETS	STA.	2796	(L,K)RS-2	EFFINGHAM	58	52
	PLOT SCALE = 28.0000' / IN.	CHECKED -	REVISED -				TO STA.					
	PLOT DATE = 3/19/2010	DATE -	REVISED -									
CONTRACT NO. 74317											ILLINOIS FED. AID PROJECT	

GENERAL NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the lead on this project.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions. Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work; however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

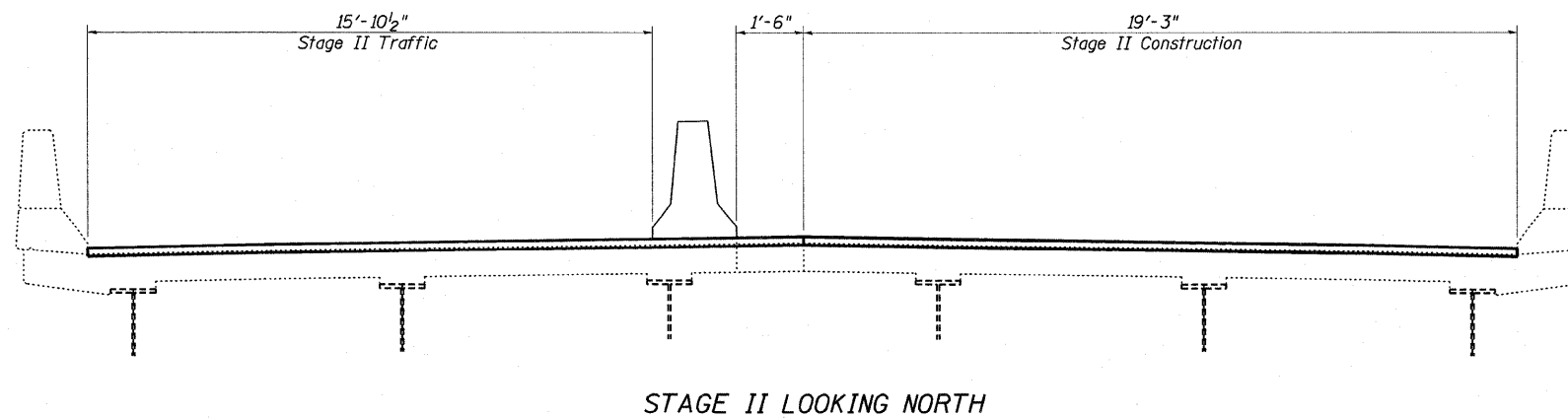
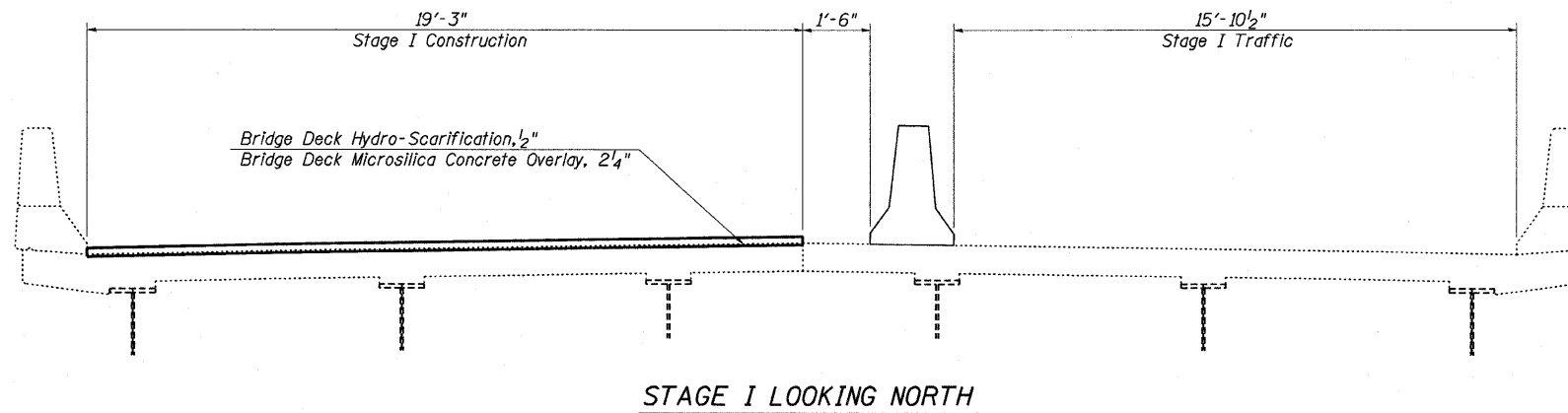
Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".

All structural steel shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Preformed Joint Strip Seal.

Removal and reinstallation of the existing name plate on the structure will be necessary for construction of the expansion joint. This work and all materials shall be included in the contract unit price for RELOCATING NAME PLATE.

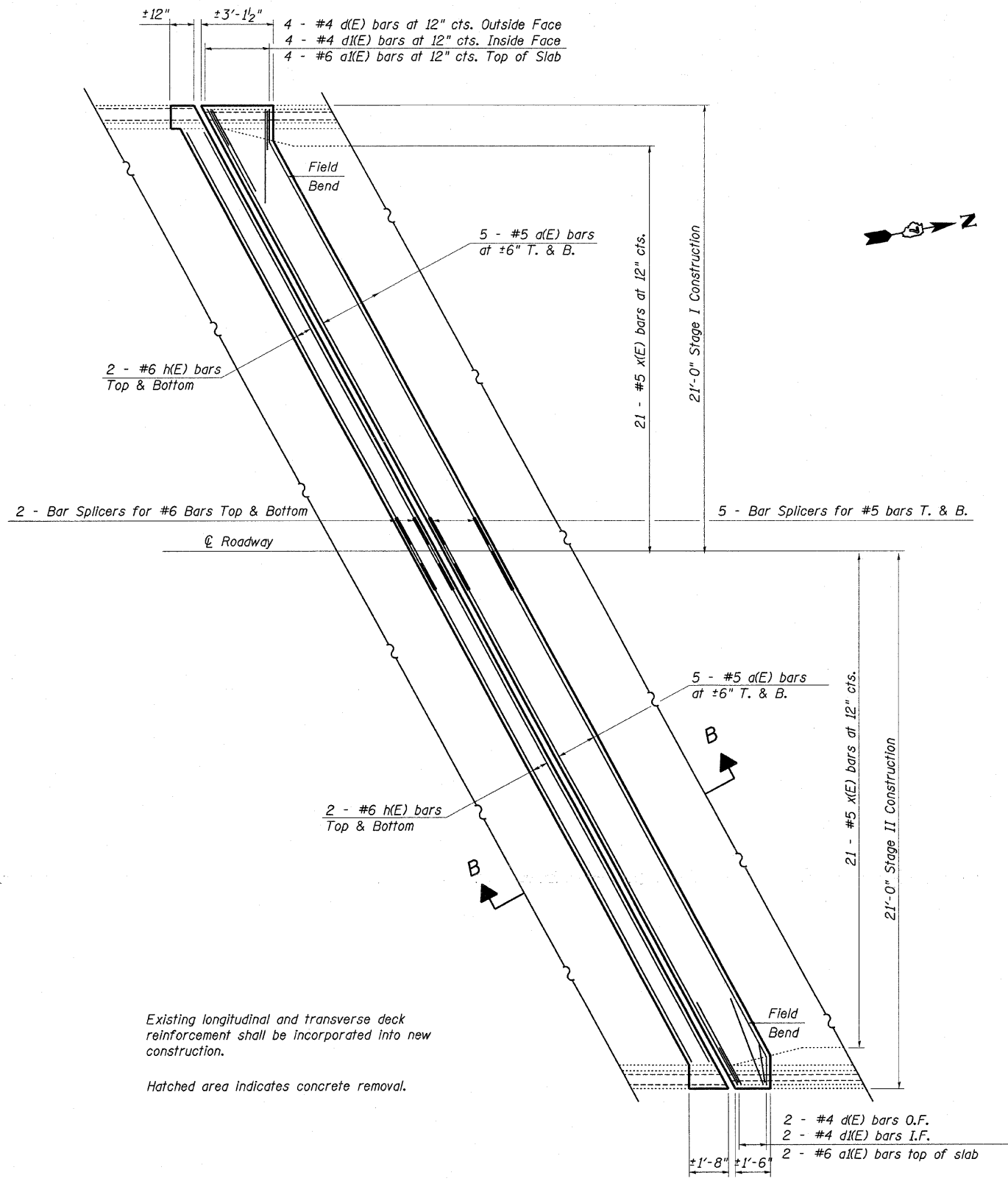
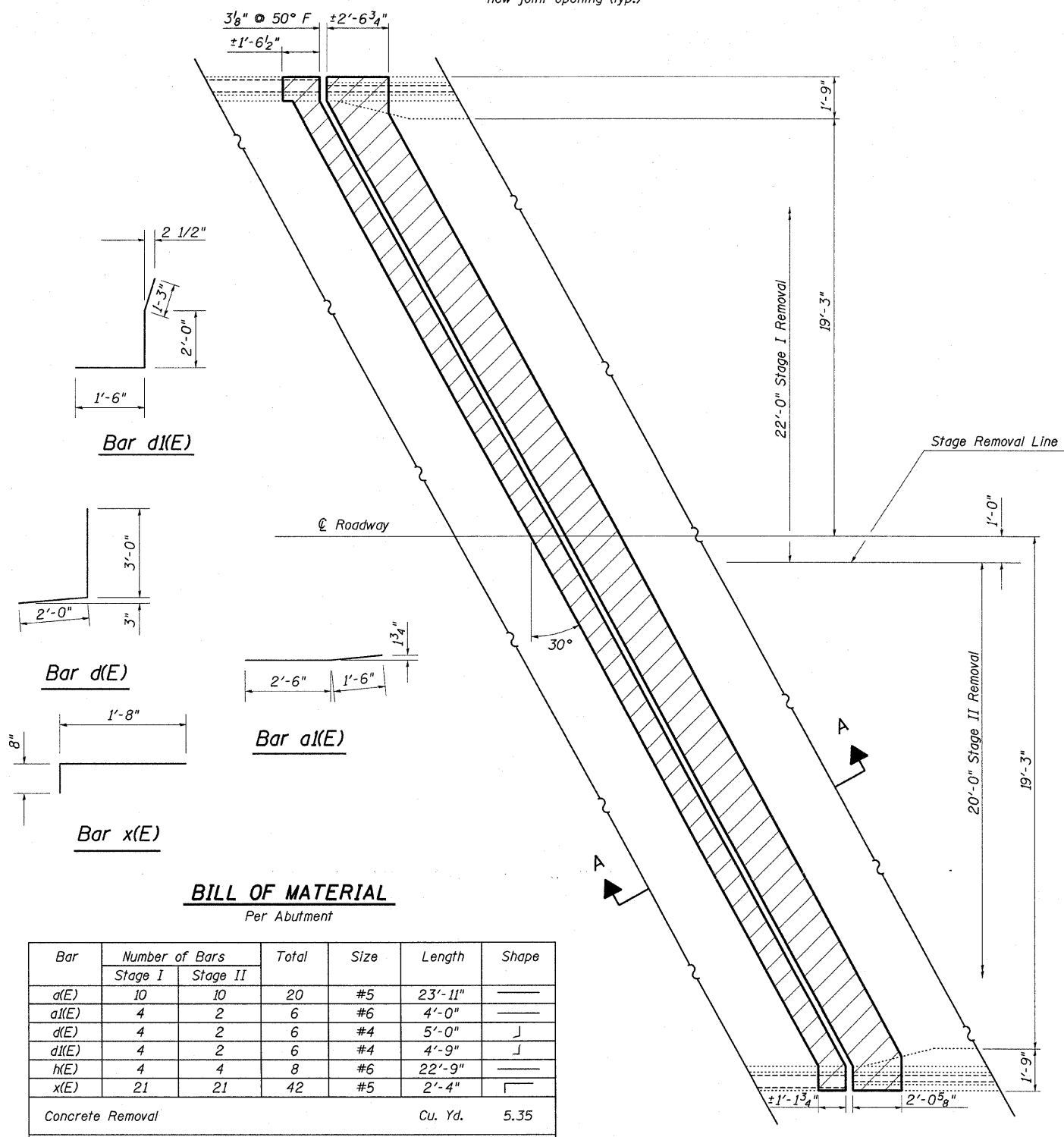
TOTAL BILL OF MATERIALS

ITEM DESCRIPTION	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	10.7
Concrete Superstructure	Cu. Yd.	12.2
Reinforcement Bars, Epoxy Coated	Pound	1900
Bar Splicers	Each	28
Bridge Deck Hydro-Scarification, 1/2 "	Sq. Yd.	753
Bridge Deck Microsilica Concrete Overlay, 2 1/4 "	Sq. Yd.	753
Bridge Deck Grooving	Sq. Yd.	733
Protective Coat	Sq. Yd.	785
Preformed Joint Strip Seal	Foot	94
Structural Repair of Concrete <5"	Sq. Ft.	25
Relocating Name Plates	Each	1



FILE NAME =	USER NAME = swartzw	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES, BILL OF MATERIALS, STAGING PLAN S.N. 025-0087	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwork\swartzw\dms54418\077-	317-brgenotes-0250087.dgn	DRAWN - MEA	REVISED -			2796	(L,K)RS-2	EFFINGHAM	58	53	
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 74317					
	PLOT DATE = 3/18/2010	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
				SCALE: NA	SHEET NO. 9 OF 15 SHEETS	STA.	TO STA.				

Note: Trim existing long. reinf. to accommodate new joint opening (typ.)

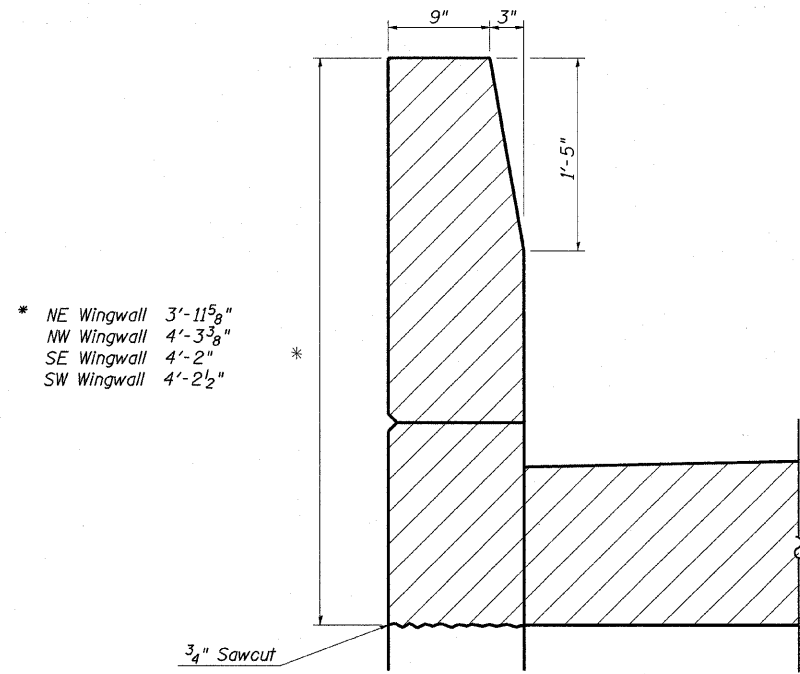


BILL OF MATERIAL
Per Abutment

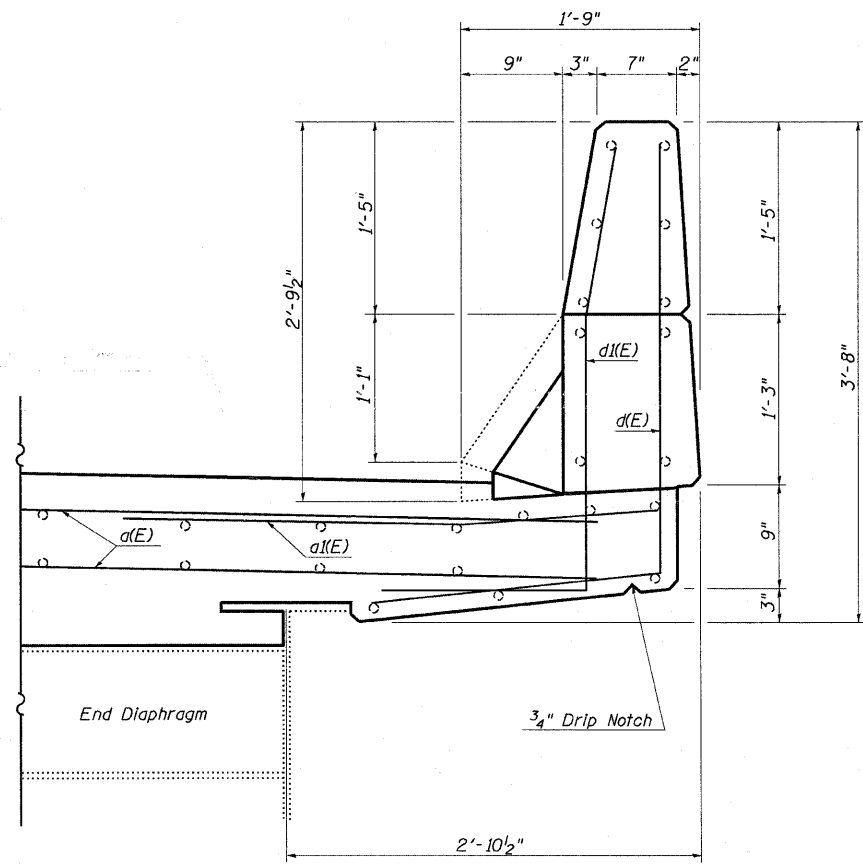
Bar	Number of Bars		Total	Size	Length	Shape
	Stage I	Stage II				
a(E)	10	10	20	#5	23'-11"	—
a1(E)	4	2	6	#6	4'-0"	—
d(E)	4	2	6	#4	5'-0"	J
d1(E)	4	2	6	#4	4'-9"	J
h(E)	4	4	8	#6	22'-9"	—
x(E)	21	21	42	#5	2'-4"	—
Concrete Removal				Cu. Yd.	5.35	
Reinforcement Bars (Epoxy Coated)				Lbs.	950	
Concrete Superstructure				Cu. Yd.	6.1	

EXISTING PARTIAL PLAN
(South Abutment shown; North Abutment similar by rotation)

PROPOSED PARTIAL PLAN
(South Abutment shown; North Abutment similar by rotation)

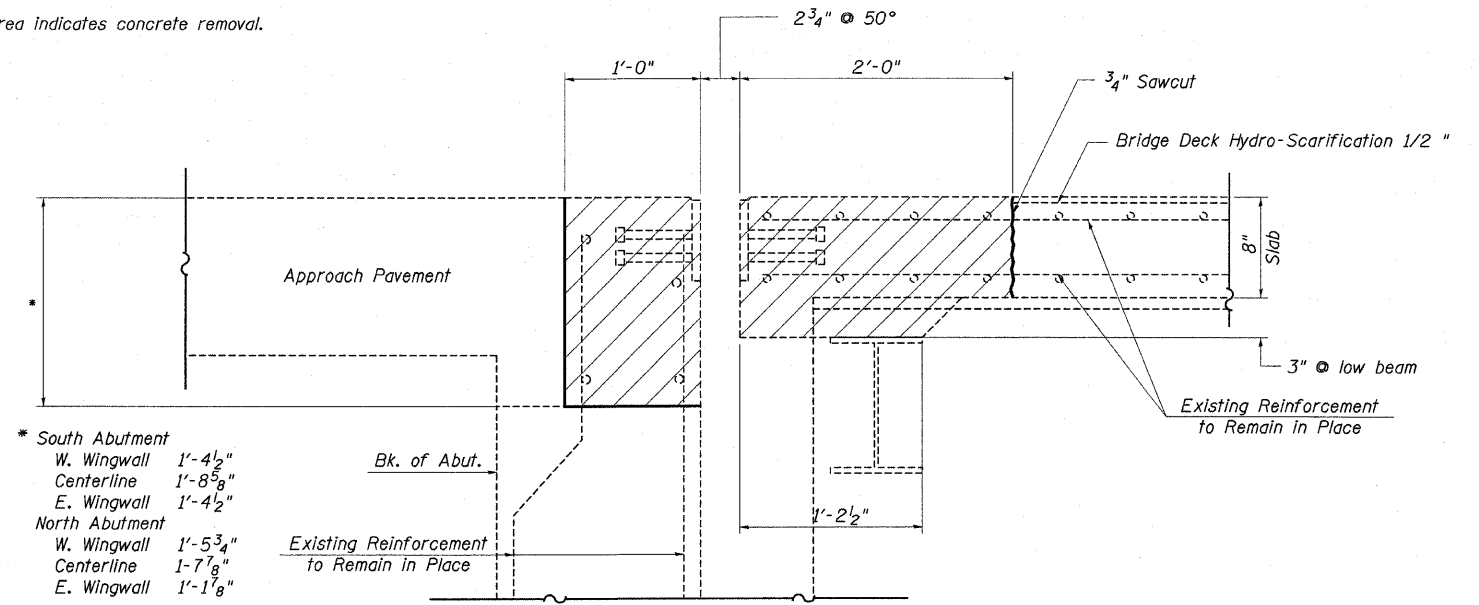


SECTION THRU WINGWALL AT ABUTMENT



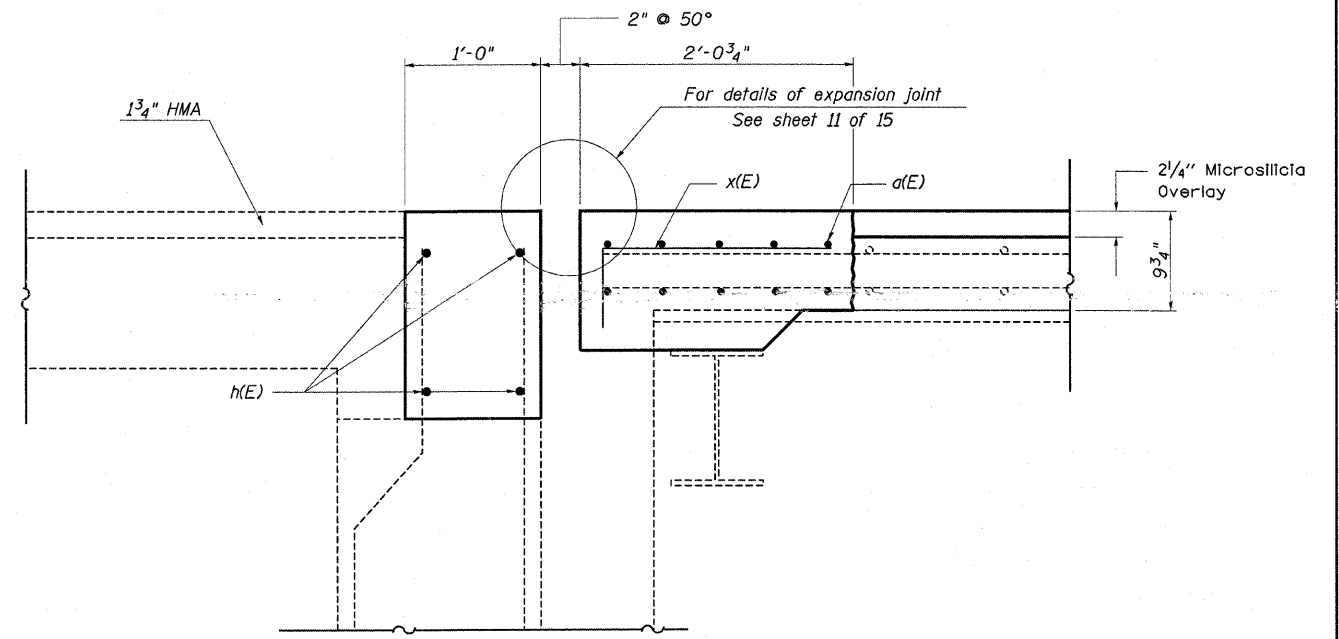
SECTION THRU PARPAET AT ABUTMENT

Hatched area indicates concrete removal.



SECTION A-A
 (Dimensions at RT L's to end of deck)

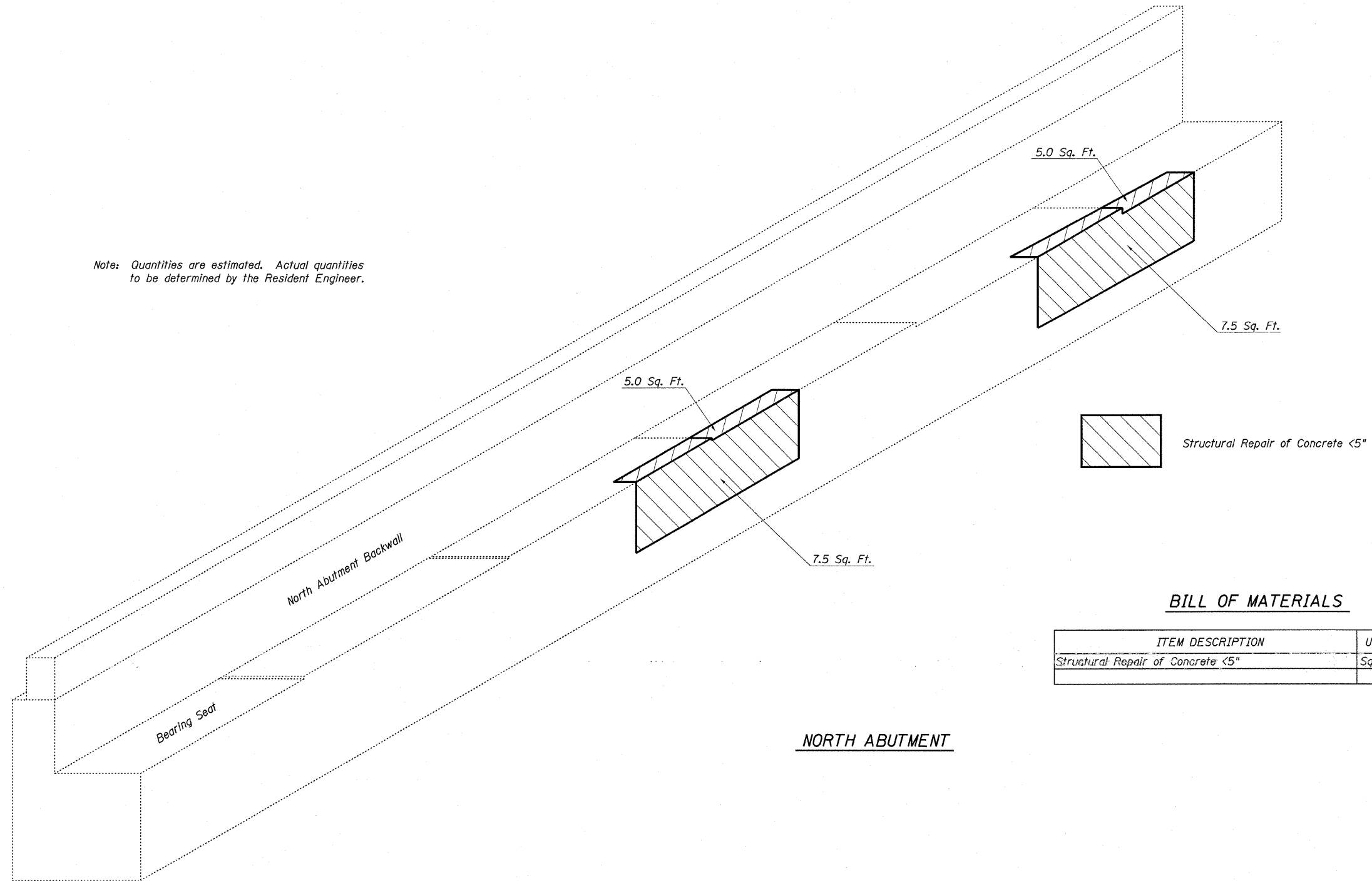
○ Existing Reinforcement
 ● Proposed Reinforcement



SECTION B-B
 (Dimensions at RT L's to end of deck)

FILE NAME =	USER NAME = swartzrw	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXPANSION JOINT REPLACEMENT DETAILS S.N. 025-0087		F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\swartzrw\dms54418\077317-brgdetails-025-0087.dgn	PLOT SCALE = 20,0000 / IN.	DRAWN - MEA	REVISED -				2796	(L,K)RS-2	EFFINGHAM	58	55
	PLOT DATE = 3/18/2010	CHECKED -	REVISED -				CONTRACT NO. 74317				
		DATE -	REVISED -				ILLINOIS FED. AID PROJECT				
				SCALE: NA	SHEET NO. 11 OF 15 SHEETS	STA.	TO STA.				

Note: Quantities are estimated. Actual quantities to be determined by the Resident Engineer.



BILL OF MATERIALS

ITEM DESCRIPTION	UNIT	QUANTITY
Structural Repair of Concrete <5"	Sq. Ft.	25

FILE NAME =
c:\pwork\pwork\swartzw\dms54418\077

USER NAME = swartzw
317-brgdetails-025-0087.dgn
PLOT SCALE = 28.0000' / IN.
PLOT DATE = 3/19/2010

DESIGNED - MEA
DRAWN - MEA
CHECKED -
DATE -

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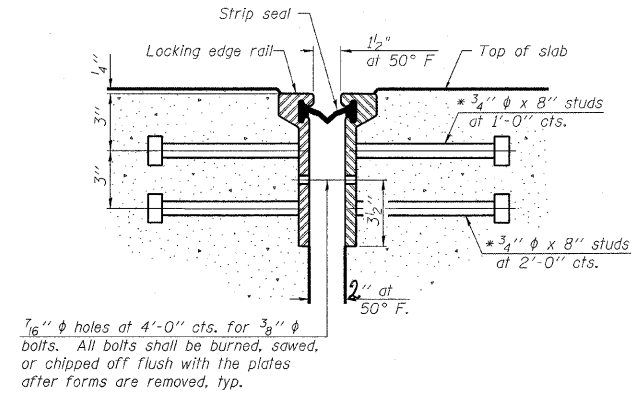
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STRUCTURAL REPAIR OF CONCRETE < 5"
S.N. 025-0087**

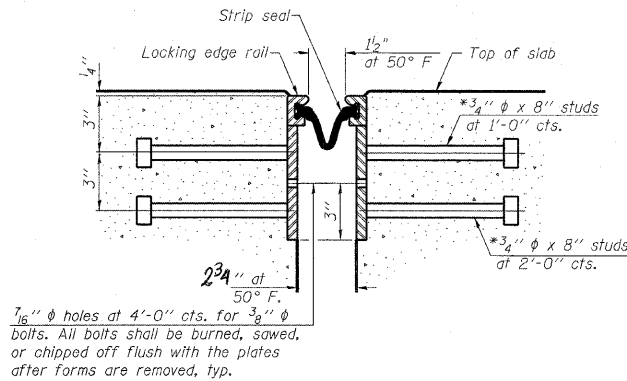
SCALE: NA SHEET NO. 12 OF 15 SHEETS STA. TO STA.

F.A.S. RTE. 2796	SECTION (L,K)RS-2	COUNTY EFFINGHAM	TOTAL SHEETS 58	SHEET NO. 56
ILLINOIS FED. AID PROJECT				CONTRACT NO. 74317

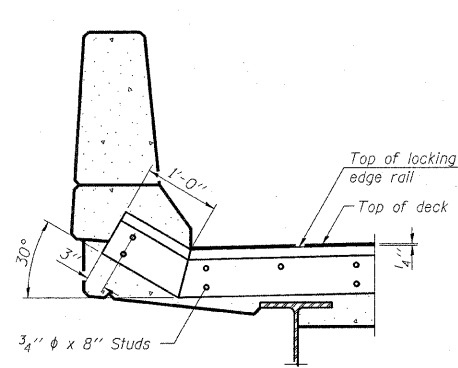
* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



SECTION THRU ROLLED RAIL JOINT

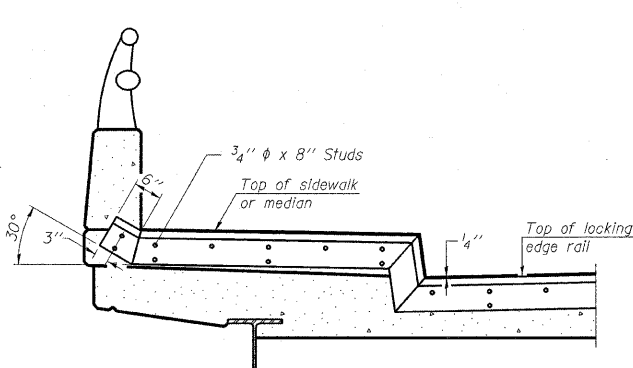


SECTION THRU WELDED RAIL JOINT



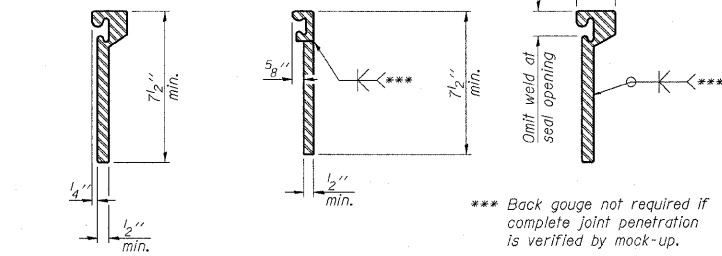
AT PARAPET

See Section A-A for end treatment of skews > 30°.



AT SIDEWALK OR MEDIAN

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



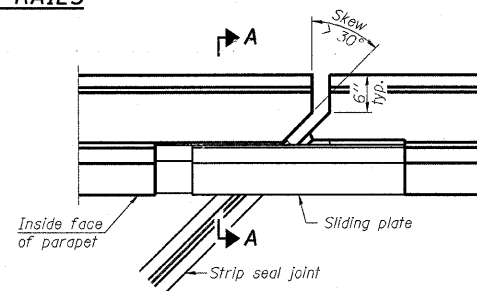
ROLLED EXTRUDED RAIL

WELDED RAIL

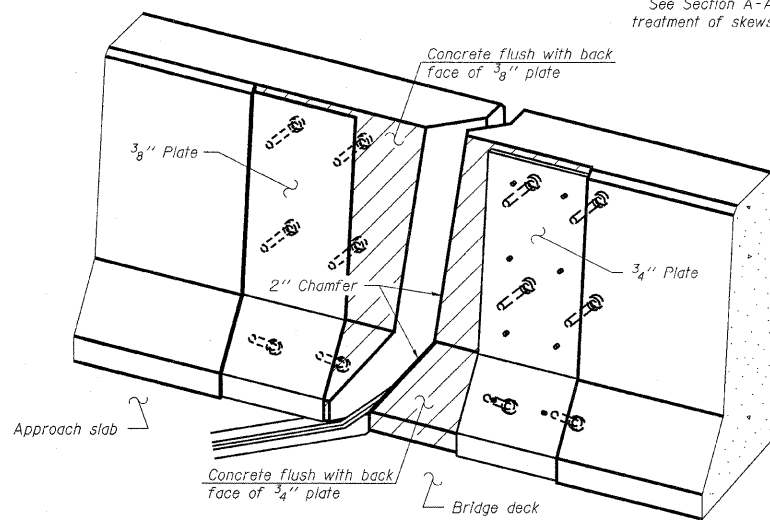
LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

LOCKING EDGE RAILS

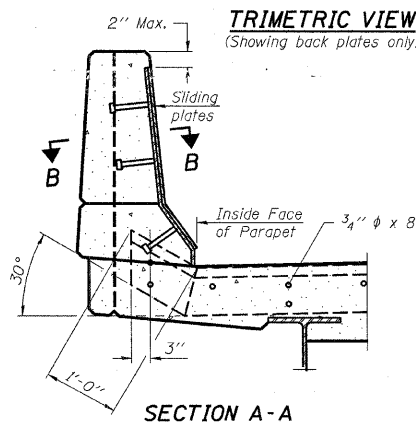


PLAN

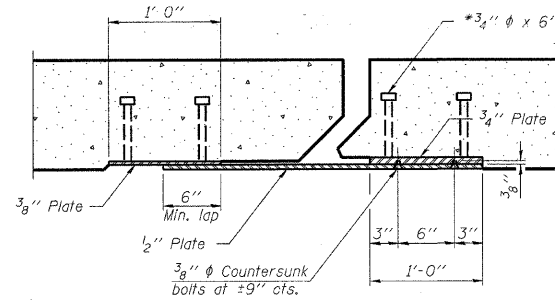


TYPICAL END TREATMENTS

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches. The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities. The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.



SECTION A-A



SECTION B-B

POINT BLOCK DETAILS (for skews > 30°)

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	94.0

EJ-SSJ

11-1-09

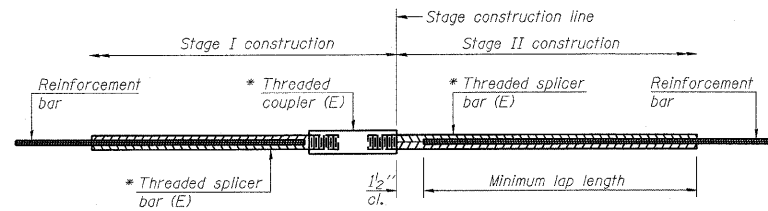
FILE NAME =	USER NAME = swartzw	DESIGNED - MEA	REVISED -
c:\pwork\pwork\swartzw\dms54418\077317-bridgedetails-025-0087.dgn		DRAWN - MEA	REVISED -
PLOT SCALE = 28.0000 "/ IN.		CHECKED -	REVISED -
PLOT DATE = 3/18/2010		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL
S.N. 025-0087

SCALE: NA SHEET NO. 13 OF 15 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	(L,K)RS-2	EFFINGHAM	58	57
CONTRACT NO. 74317			ILLINOIS FED. AID PROJECT	



STANDARD BAR SPLICER ASSEMBLY

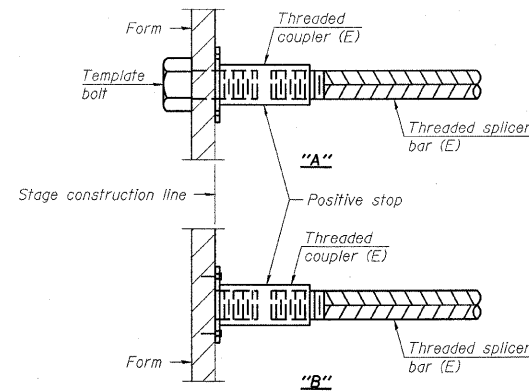
Bar size to be spliced	Minimum Lap Lengths			
	Table 1	Table 2	Table 3	Table 4
3, 4	1'-5"	1'-11"	2'-1"	2'-4"
5	1'-9"	2'-5"	2'-7"	2'-11"
6	2'-1"	2'-11"	3'-1"	3'-6"
7	2'-9"	3'-10"	4'-2"	4'-8"
8	3'-8"	5'-1"	5'-5"	6'-2"
9	4'-7"	6'-5"	6'-10"	7'-9"

Table 1: Black bar, 0.8 Class C
 Table 2: Black bar, Top bar lap, 0.8 Class C
 Table 3: Epoxy bar, 0.8 Class C
 Table 4: Epoxy bar, Top bar lap, 0.8 Class C

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

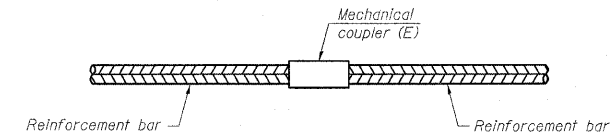
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
025-0087	#5	20	3
025-0087	#6	8	3



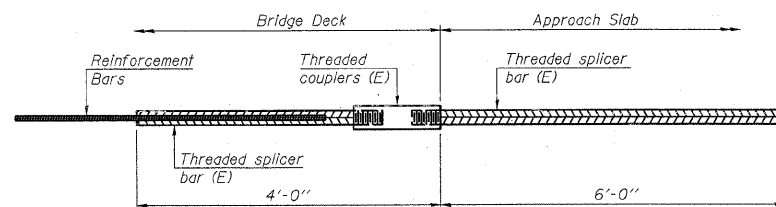
INSTALLATION AND SETTING METHODS

"A": Set bar splicer assembly by means of a template bolt.
 "B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E): Indicates epoxy coating.



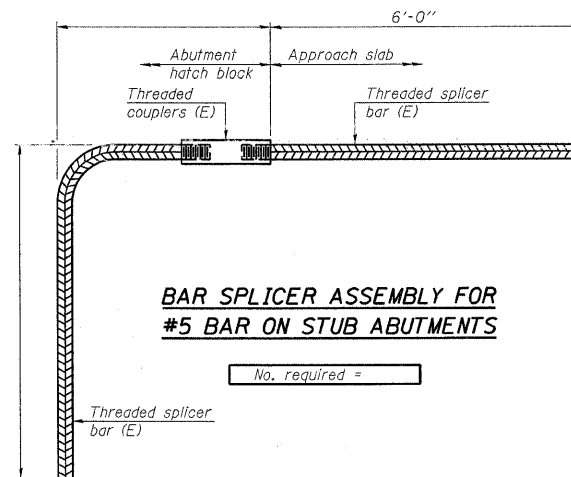
STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

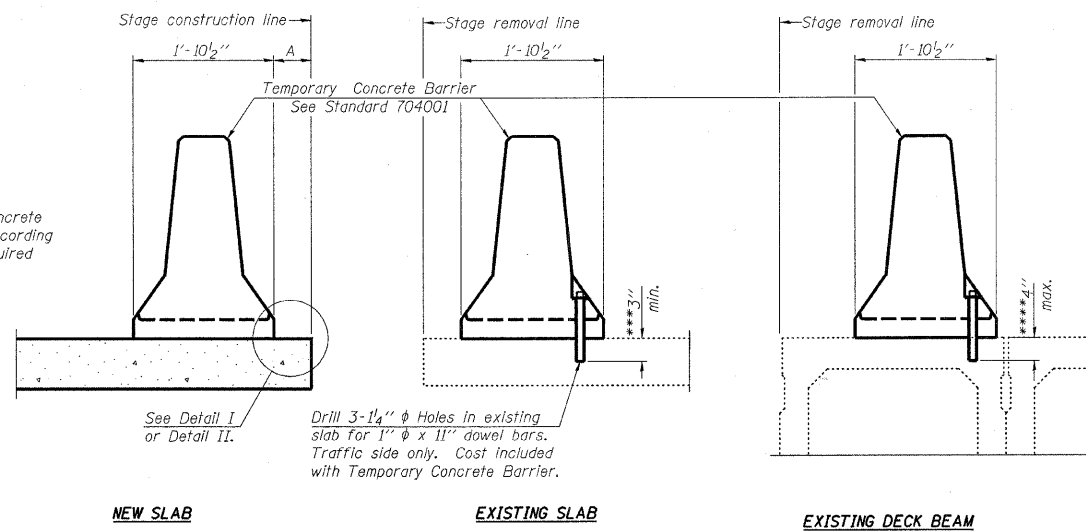
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 503 of the Standard Specifications.
 See special provision for Mechanical Splicers.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

11-1-09

FILE NAME =	USER NAME = swartzw	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BAR SPLICER ASSEMBLY DETAILS S.N. 025-0087	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pw_work\pwwork\swartzw\dms54418\d77	317-bridgdata1a-0250079.dgn	DRAWN - MEA	REVISED -			2796	(L,K)RS-2	EFFINGHAM	58	57A	
	PLOT SCALE = 20,0000 ' / IN.	CHECKED -	REVISED -			CONTRACT NO. 74317					
	PLOT DATE = 3/18/2010	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: NA	SHEET NO. 14 OF 15 SHEETS	STA.	TO STA.			

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

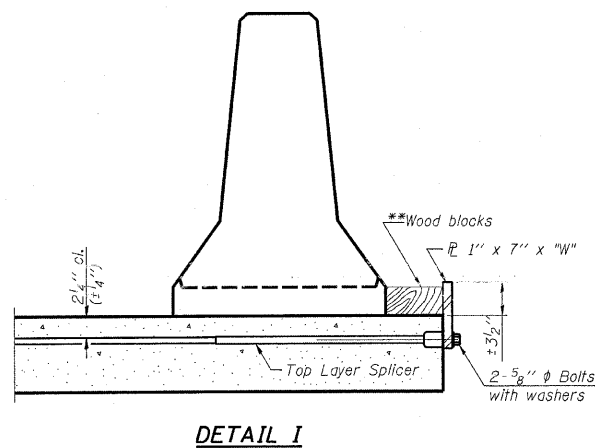
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel \bar{L} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel \bar{L} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

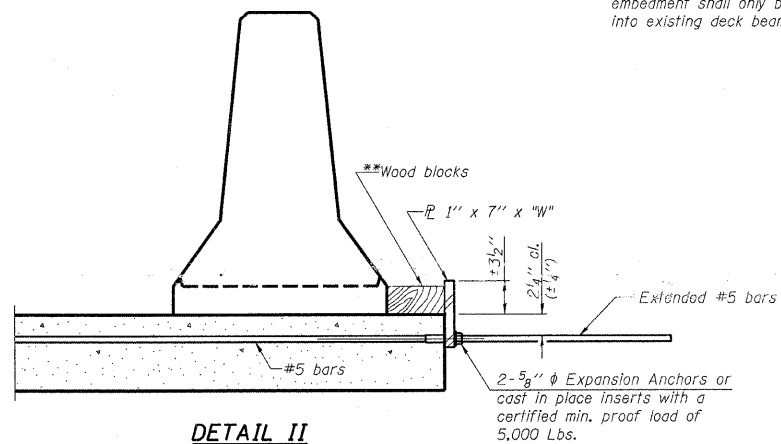
Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



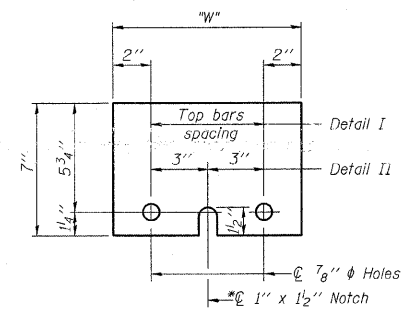
DETAIL I



DETAIL II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"



STEEL RETAINER \bar{L} 1" x 7" x 10"

* Required only with Detail II

R-27

11-1-09

FILE NAME =	USER NAME = skortzrw	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER S.N. 025-0087			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317-brgdetail1s-0250079.dgn	DRAWN - MEA	REVISED -	2796					IL,KIRS-2	EFFINGHAM	58	58	
PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 74317									
PLOT DATE = 3/18/2010	DATE -	REVISED -	SCALE: NA		SHEET NO. 15 OF 15 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				