

| | | | | |
|-----------------------|-----------|----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 762 | (1,2)RS-3 | Moultrie | 21 | 1 |
| ILLINOIS CONTRACT NO. | | | 74413 | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

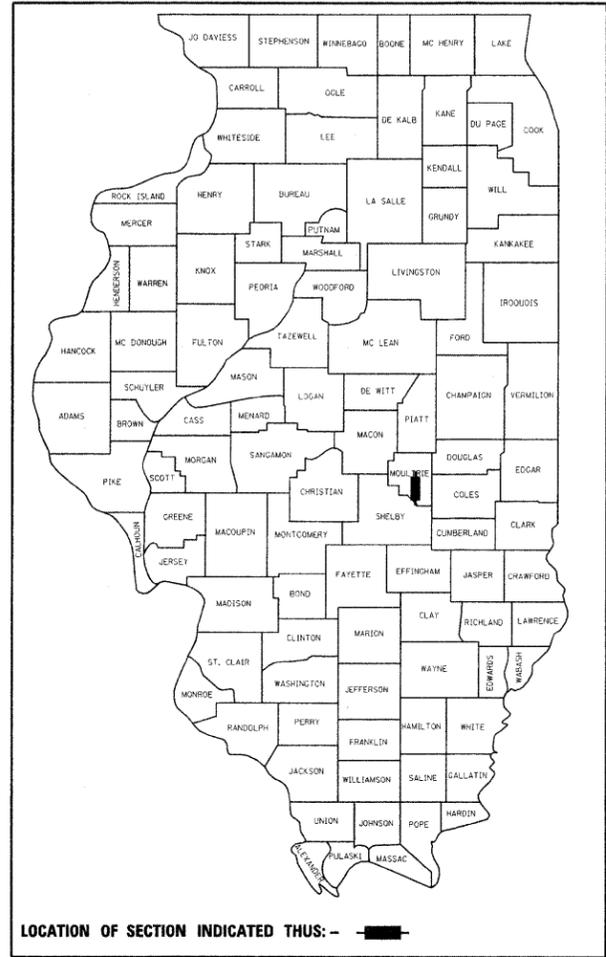
**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 762 (ILL 32)
SECTION (1,2)RS-3
PROJECT — —
HMA RESURFACING/BOX CULVERT REPLACEMENT
MOULTRIE COUNTY

C-97-127-09

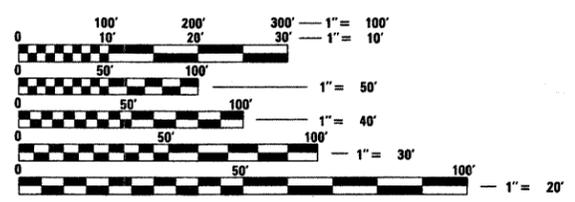
FOR INDEX OF SHEETS, SEE SHEET NO. 2

D97-071-09



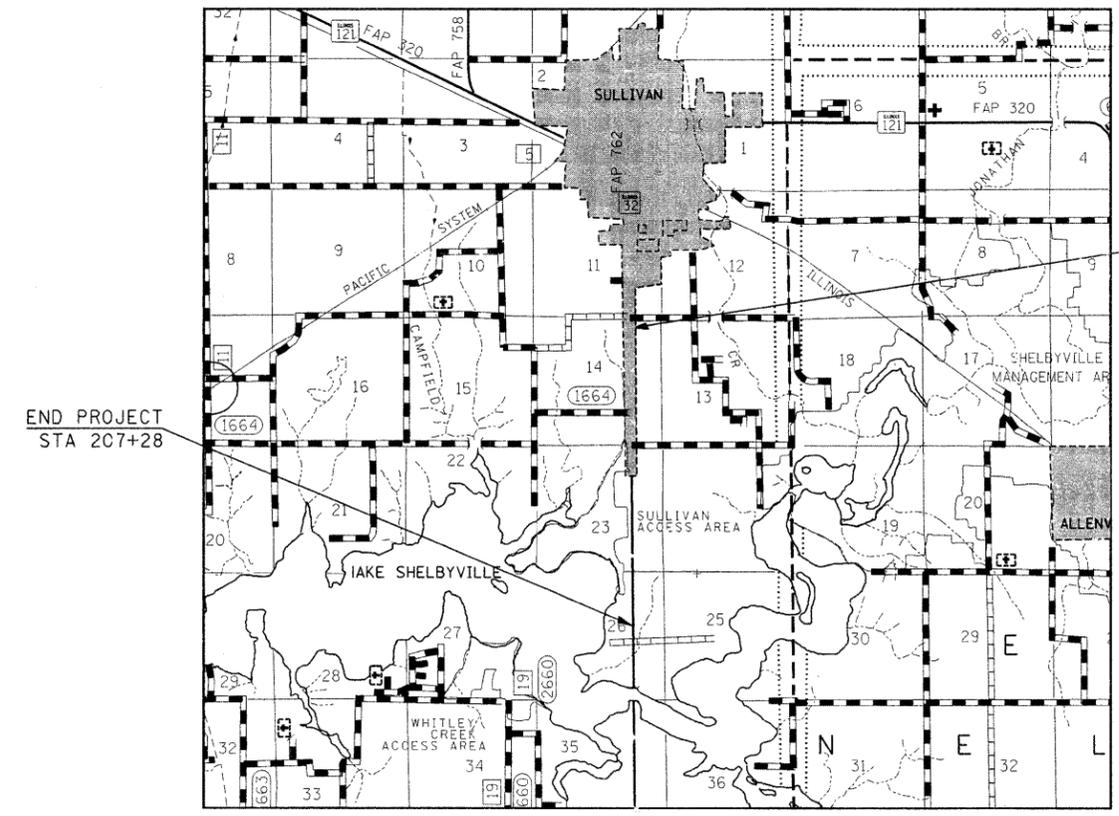
LOCATION OF SECTION INDICATED THIS: - [black rectangle]

ADT = 4300(2007)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



BEGIN PROJECT
STA 80+05

END PROJECT
STA 207+28

TOTAL LENGTH OF PROJECT = 12,723 FT 2.41 MILES

PROJECT ENGINEER: MARK DAUGHERTY
PROJECT MANAGER: KALEB HIRTZEL

CONTRACT NO. 74413

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 22 2010
Roger Z. Dinkel
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 7 2010
Scott E. Stett, P.E./bc
ENGINEER OF DESIGN AND ENVIRONMENT

May 7 2010
Christine M. Reed/bc
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

GENERAL NOTES

THE PROPOSED PROJECT IS LOCATED ON ILL 32 IN MOULTRIE COUNTY. THE PROJECT BEGINS AT COUNTY ROAD 1200 N AND ENDS AT COUNTY ROAD 950 N. THE WORK INCLUDED IN SECTION (1,2)RS-3 CONSISTS OF PAVEMENT PATCHING, MILLING, LEVELING BINDER, HOT-MIX ASPHALT SURFACE COURSE, AGGREGATE SHOULDERS, BOX CULVERT REMOVAL/REPLACEMENT AND ANY OTHER WORK NECESSARY TO COMPLETE THE SECTION.

THE CONTRACTOR SHALL PROVIDE INTERNET ACCESS TO THE HOT-MIX ASPHALT PLANT QUALITY CONTROL LAB SO THAT HOT-MIX ASPHALT PLANT REPORTS CAN BE E-MAILED TO THE DISTRICT HEADQUARTERS. THIS WORK SHALL BE INCLUDED IN THE COST OF HOT-MIX ASPHALT ITEMS.

For the pay item Bituminous Materials (Prime Coat), the Contractor shall use either RC-70, SS-1H, or SS-1HP applied at the rate directed by the engineer.

AGGREGATE SHOULDERS, TYPE B SHALL BE CRUSHED STONE, CRUSHED CONCRETE, OR RAP.

AGGREGATE SURFACE COURSE, TYPE B SHALL BE CRUSHED STONE OR CRUSHED CONCRETE.

PAVEMENT MARKING SHALL BE APPLIED IN ACCORDANCE WITH SECTION 780 OF THE STANDARD SPECIFICATIONS. SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE MILLED SURFACE, PRIMED SURFACE, LEVELING BINDER SURFACE AND HOT-MIX ASPHALT SURFACE COURSE AS SPECIFIED IN SECTION 703 OF THE STANDARD SPECIFICATIONS. TEMPORARY TAPE SHALL BE USED ON THE SURFACE COURSE AND PAINT SHALL BE USED ON THE MILLED, PRIMED, AND LEVELING BINDER SURFACES.

THE TOTAL QUANTITY OF PAINT PAVEMENT MARKING - LINE 4 INCH CONSISTS OF 5,052 FEET OF YELLOW AND 25,126 FEET OF WHITE.

THE EXISTING PAVEMENT SHALL BE PATCHED IN ACCORDANCE WITH SECTION 442 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS. THE QUANTITY OF PATCHING SHOWN ON THE PLANS IS AN ESTIMATE ONLY AND THE FINAL LOCATIONS AND QUANTITY SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

AGGREGATE (PRIME COAT) 4.0 LB/SQ YD
 BIT MATLS (PRIME COAT) 0.1 GAL/SQ YD
 BITUMINOUS CONCRETE 112 LBS/SQ YD/INCH

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

SURFACE

MIXTURE USE: SURFACE COURSE - 1 1/2"
 APPLICATION: HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70
 PG GRADE: PG 64-22
 DESIGN AIR VOIDS: 4.0% @ NDESIGN = 70
 MIXTURE COMPOSITION: IL-9.5
 FRICTION AGGREGATE: MIXTURE C

LEVELING BINDER

MIXTURE USE: LEVELING BINDER - 3/4"
 APPLICATION: LEVELING BINDER (MACHINE METHOD) N70
 PG GRADE: PG 64-22
 DESIGN AIR VOIDS: 4.0% @ NDESIGN = 70
 MIXTURE COMPOSITION: IL-9.5
 FRICTION AGGREGATE: N/A

INCIDENTAL

MIXTURE USE: INCIDENTAL BITUMINOUS SURFACE (DRIVEWAYS)
 APPLICATION: HOT-MIX ASPHALT SURFACE COURSE, MIX "C" N70
 PG GRADE: PG 64-22
 DESIGN AIR VOIDS: 4.0% @ NDESIGN = 70
 MIXTURE COMPOSITION: IL-9.5
 FRICTION AGGREGATE: MIXTURE C

PAVEMENT PATCHING

MIXTURE USE: PAVEMENT PATCHING
 APPLICATION: HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70
 PG GRADE: PG 64-22
 DESIGN AIR VOIDS: 4.0% @ NDESIGN = 70
 MIXTURE COMPOSITION: IL - 19.0
 FRICTION AGGREGATE: N/A

INDEX OF SHEETS

| SHEET NO. | ITEM |
|-----------|---|
| 1 | COVER SHEET |
| 2 | INDEX OF SHEETS, GENERAL NOTES AND LIST OF APPLICABLE HIGHWAY STANDARDS |
| 3 | SUMMARY OF QUANTITIES |
| 4 | TYPICAL SECTIONS |
| 5 | ALIGNMENT, TIES AND BENCHMARKS |
| 6-10 | STREET LOCATION MAP |
| 11 | MISCELLANEOUS DETAIL |
| 12 | SCHEDULES |
| 13 | ENTRANCE SCHEDULE AND MAILBOX TURNOUT DETAIL |
| 14-18 | BOX CULVERT REMOVAL AND REPLACEMENT DETAILS |
| 19 | STONE RIPRAP DETAIL |
| 20-21 | TYPICAL APPLICATIONS OF PAVEMENT MARKINGS |

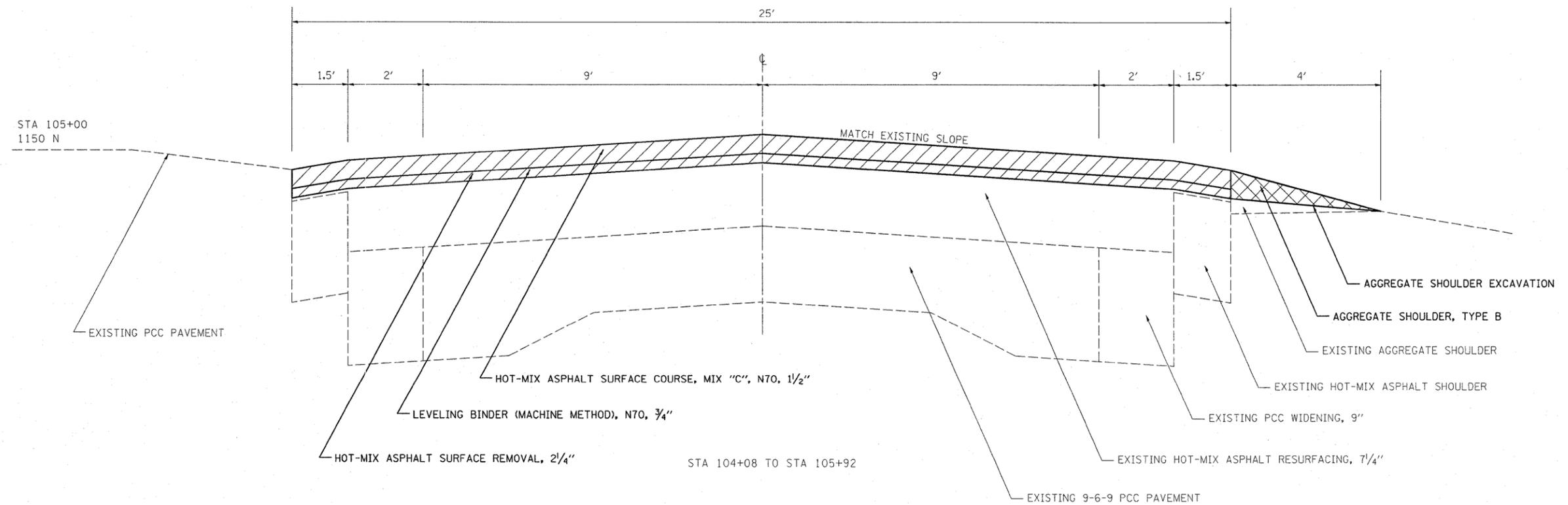
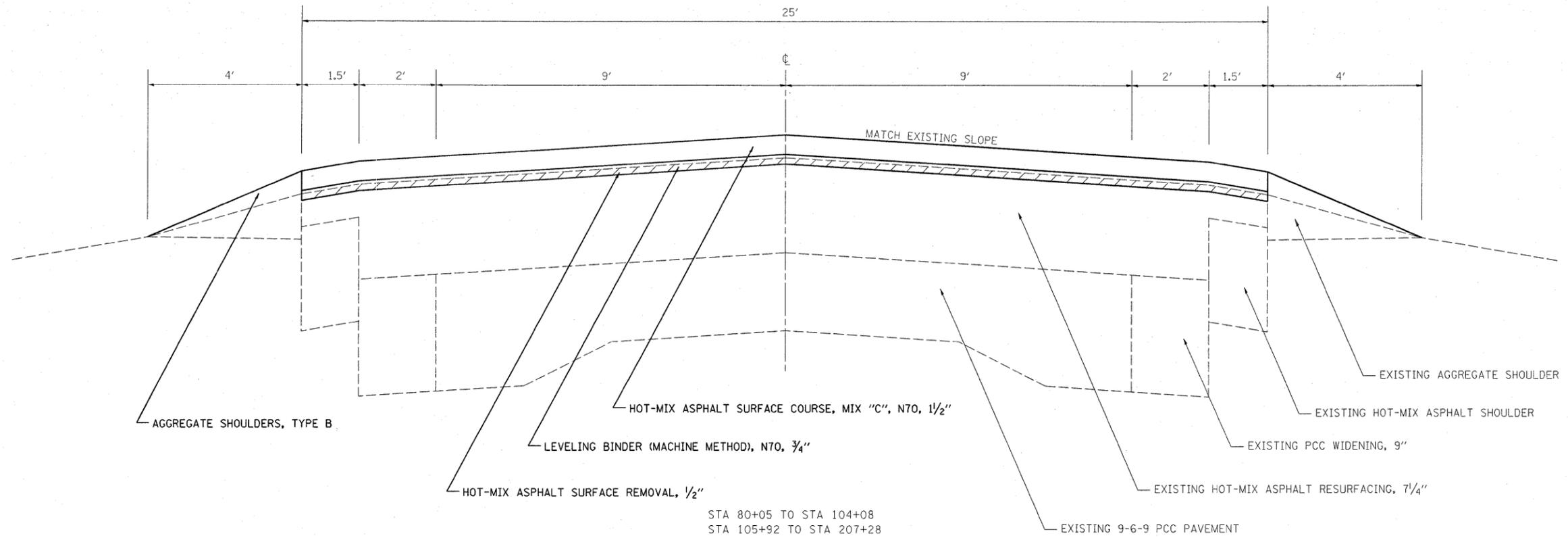
THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED AFTER SHEET NO. 21

| STD. NO. | DESCRIPTION |
|-----------|--|
| 000001-05 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 001006 | DECIMAL OF AN INCH AND OF A FOOT |
| 406201-01 | MAILBOX TURNOUT |
| 442201-03 | CLASS C AND D PATCHES |
| 630001-08 | STEEL PLATE BEAM GUARDRAIL |
| 701201-03 | LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS > 45 MPH |
| 701301-03 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701306-02 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45 MPH |
| 701311-03 | LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY |
| 701336-05 | LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS > 45 MPH |
| 701901-01 | TRAFFIC CONTROL DEVICES |
| 780001-02 | TYPICAL PAVEMENT MARKINGS |
| 781001-03 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS |

| SUMMARY OF QUANTITIES | | | TOTAL QUANTITIES | CONSTRUCTION TYPE CODE | | |
|-----------------------|--|------------------|------------------|------------------------|-----------------|-------|
| CODE NO | ITEM | UNIT | | 1000 100% STATE | Y007 100% STATE | ----- |
| 28100109 | STONE RIPRAP, CLASS A5 | SQ YD | 172 | | 172 | |
| 28200200 | FILTER FABRIC | SQ YD | 172 | | 172 | |
| 35600716 | HOT-MIX ASPHALT BASE COURSE WIDENING, 10" | SQ YD | 150 | 150 | | |
| 40200800 | AGGREGATE SURFACE COURSE, TYPE B | TON | 16 | 16 | | |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 3534 | 3534 | | |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 71 | 71 | | |
| 40600635 | LEVELING BINDER (MACHINE METHOD), N70 | TON | 1484 | 1484 | | |
| 40600895 | CONSTRUCTING TEST STRIP | EACH | 1 | 1 | | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 194 | 194 | | |
| 40600990 | TEMPORARY RAMP | SQ YD | 70 | 70 | | |
| 40600995 | TEMPORARY RAMP, SPECIAL | SQ YD | 33 | 33 | | |
| 40603315 | HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70 | TON | 2969 | 2969 | | |
| 40800050 | INCIDENTAL HOT-MIX ASPHALT SURFACING | TON | 202 | 202 | | |
| 44000151 | HOT-MIX ASPHALT SURFACE REMOVAL, 1/2" | SQ YD | 34622 | 34622 | | |
| 44000158 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4" | SQ YD | 511 | 511 | | |
| 44201823 | CLASS D PATCHES, TYPE I, 15 INCH | SQ YD | 50 | 50 | | |
| 44201827 | CLASS D PATCHES, TYPE II, 15 INCH | SQ YD | 330 | 330 | | |
| 44201831 | CLASS D PATCHES, TYPE III, 15 INCH | SQ YD | 75 | 75 | | |
| 44201833 | CLASS D PATCHES, TYPE IV, 15 INCH | SQ YD | 89 | 89 | | |
| 48101200 | AGGREGATE SHOULDERS, TYPE B | TON | 602 | 602 | | |
| 50100100 | REMOVAL OF EXISTING STRUCTURES | EACH | 1 | | 1 | |
| 54001001 | BOX CULVERT END SECTION, CULVERT NO. 1 | EACH | 2 | | 2 | |
| 54010804 | PRECAST CONCRETE BOX CULVERT 8' X 4' | FOOT | 49 | | 49 | |
| 67000500 | ENGINEER'S FIELD OFFICE, TYPE B | CAL MO | 2 | 2 | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | |
| 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 | L SUM | 1 | 1 | | |
| 70100460 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | L SUM | 1 | 1 | | |
| 70100600 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701336 | L SUM | 1 | 1 | | |
| 70300100 | SHORT-TERM PAVEMENT MARKING | FOOT | 5089 | 5089 | | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 30178 | 30178 | | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 424 | 424 | | |
| * 78001110 | PAINT PAVEMENT MARKING - LINE 4" | FOOT | 30178 | 30178 | | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 159 | 159 | | |
| X0324118 | GRANULAR CULVERT BACKFILL | CU YD | 192 | | 192 | |

* Specialty Items

| | | | | | | | | | | | | | | |
|--|-----------------------------|------------|------------|---|------------------------------|-----------|-----------|------|-------------|---------|-----------|--------------|-----------|---|
| FILE NAME = | USER NAME = teasleyck | DESIGNED - | REVISIED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
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| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISIED - | | CONTRACT NO. 74413 | | | | | | | | | |
| | PLOT DATE = 3/22/2010 | DATE | REVISIED - | | ILLINOIS FED. AID PROJECT | | | | | | | | | |



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DESIGNED -
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 DATE -

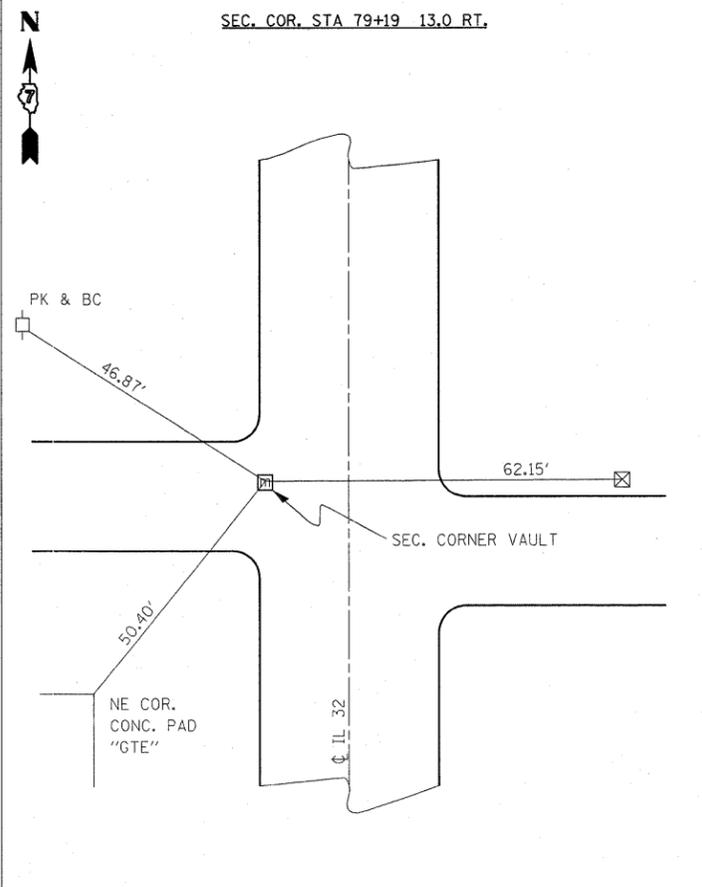
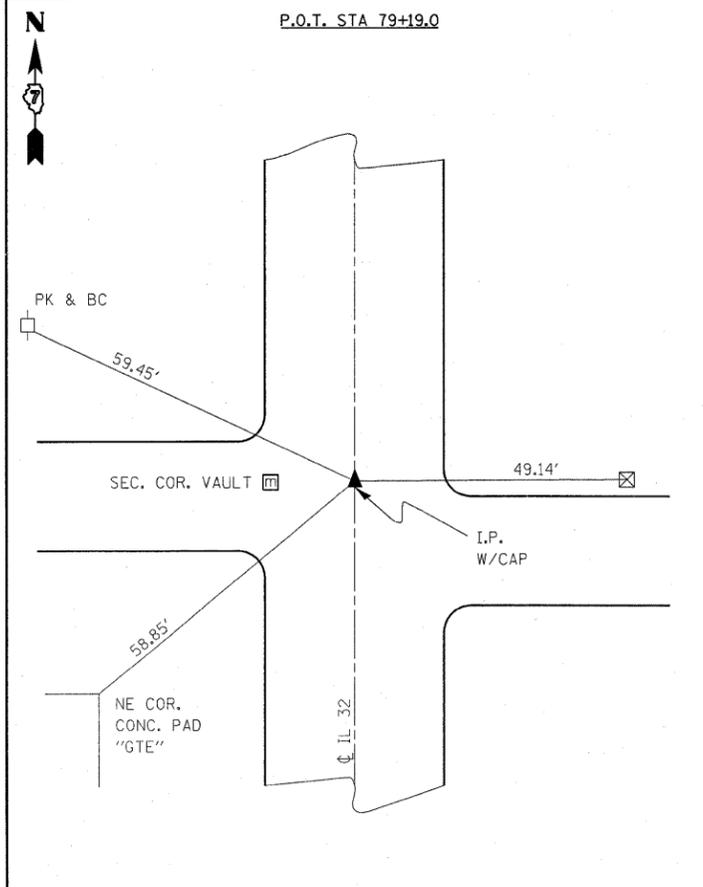
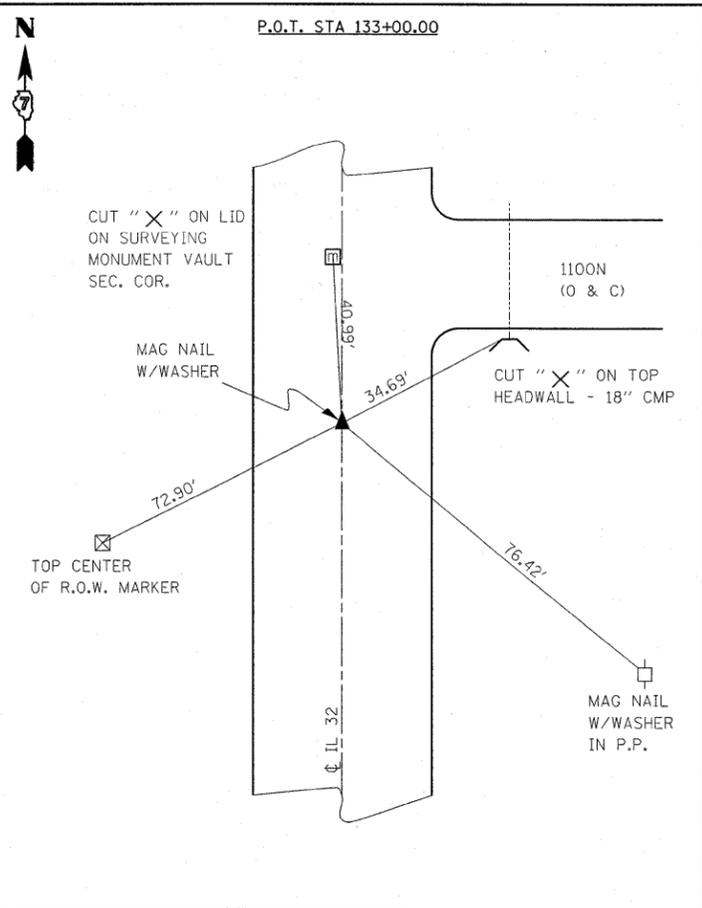
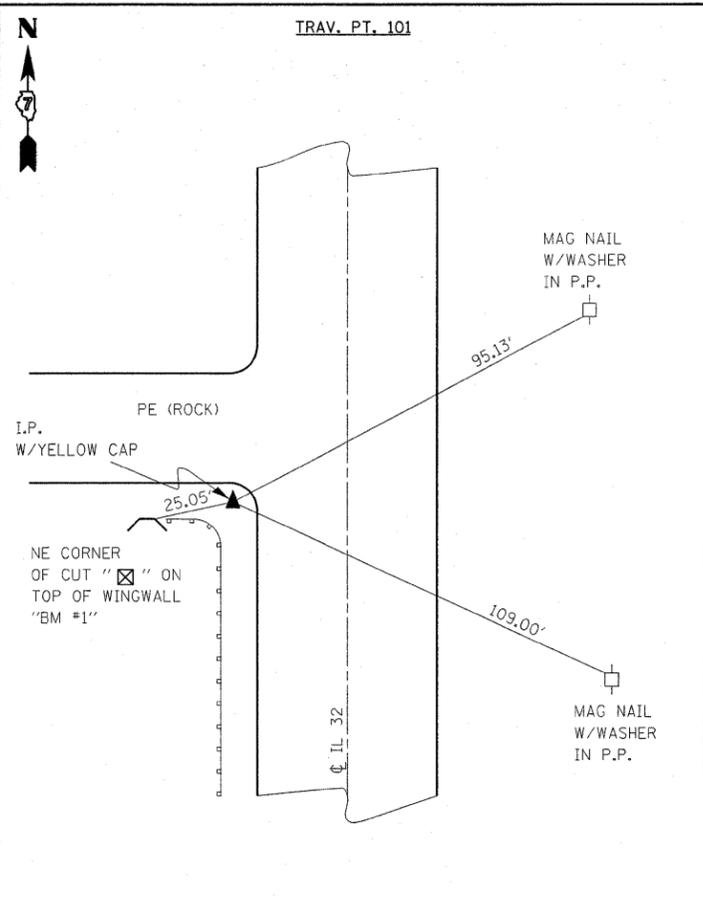
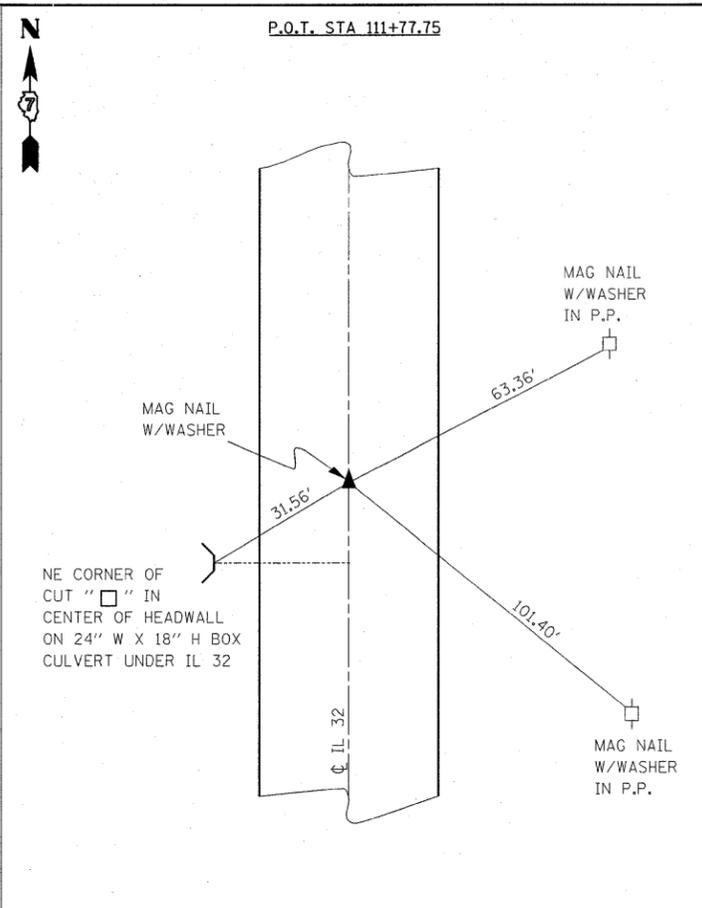
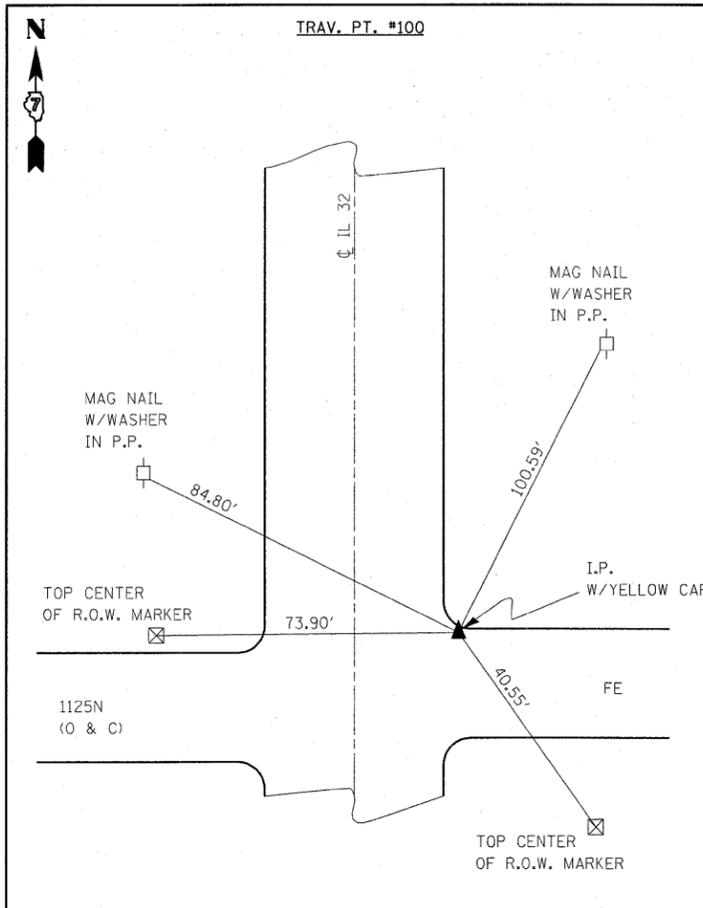
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|---------------------|--------------------|--------------------|----------------|
| F.A.P. RTE. 762 | SECTION 1,2,RS-3 | COUNTY Moultrie | TOTAL SHEETS 21 | SHEET NO. 4 |
| CONTRACT NO. 74413 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



| BENCHMARK INFORMATION | | | | |
|-----------------------|-----------|---------|----------|---|
| BENCHMARK | ELEVATION | STATION | OFFSET | DESCRIPTION |
| #1 | 630.92 | 128+98 | 49.5' RT | CUT "X" ON SE WINGWALL OF BOX CULVERT 5' H X 11' W UNDER P.E. |

| | | | |
|---|-----------------------|------------|-----------|
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| PLOT DATE = 3/23/2010 | | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| SURVEY TIES | | | | |
|-------------|-----------|-----------|------|---------|
| SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. |
| | | | | |

| | | | | |
|---------------------------|-----------|----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 762 | (1,2)RS-3 | Moultrie | 21 | 5 |
| CONTRACT NO. 74413 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



BEGIN PROJECT
80+05

85+00

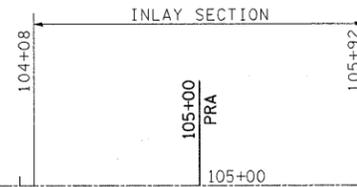
90+00

95+00



95+00

100+00



110+00

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REVISED -
REVISED -

PLOT SCALE = 50.0000' / IN.
PLOT DATE = 3/23/2010

CHECKED -
DATE -

REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STREET LOCATION MAP

SCALE: SHEET NO. OF SHEETS STA. 80+05 TO STA. 110+00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|----------|--------------------|-----------|
| 762 | (1,2)RS-3 | Moultrie | 21 | 6 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 74413 | |



BOX CULVERT REMOVAL/REPLACEMENT
STA 121+61

110+00 115+00 120+00 125+00

118+79
PRA



132+13
PRA

125+00 130+00 135+00 140+00

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DESIGNED -
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REVISED -
REVISED -

PLOT SCALE = 50.0000' / IN.

CHECKED -
DATE -

REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STREET LOCATION MAP

SCALE: SHEET NO. OF SHEETS STA. 110+00 TO STA. 140+00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|----------|--------------------|-----------|
| 762 | (1,2)RS-3 | Moultrie | 21 | 7 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 74413 | |



140+00

145+00

150+00

155+00



155+00

160+00

165+00

170+00

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74413-ehs-plan.dgn

DESIGNED -
DRAWN -

REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STREET LOCATION MAP

F.A.P. SECTION COUNTY TOTAL SHEET SHEET
RTE. 762 (1,2)RS-3 Moultrie 21 8

PLOT SCALE = 50.0000' / IN.
PLOT DATE = 3/23/2018

CHECKED -
DATE -

REVISED -
REVISED -

SCALE: SHEET NO. OF SHEETS STA. 140+00 TO STA. 170+00

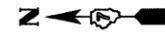
CONTRACT NO. 74413

ILLINOIS FED. AID PROJECT



170+00 | 175+00 | 180+00 | 185+00

172+27
PRA



185+00 | 190+00 | 195+00 | 200+00

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| | PLOT SCALE = 50,0000' / IN. PLOT DATE = 3/23/2010 | CHECKED - | REVISED - | | | | | ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 74413 | |
| | DATE - | REVISED - | | SCALE: | SHEET NO. OF SHEETS | STA. 170+00 TO STA. 200+00 | | | | | | |



200+00

205+00

END PROJECT
207+28

POT Sta 208+00.00

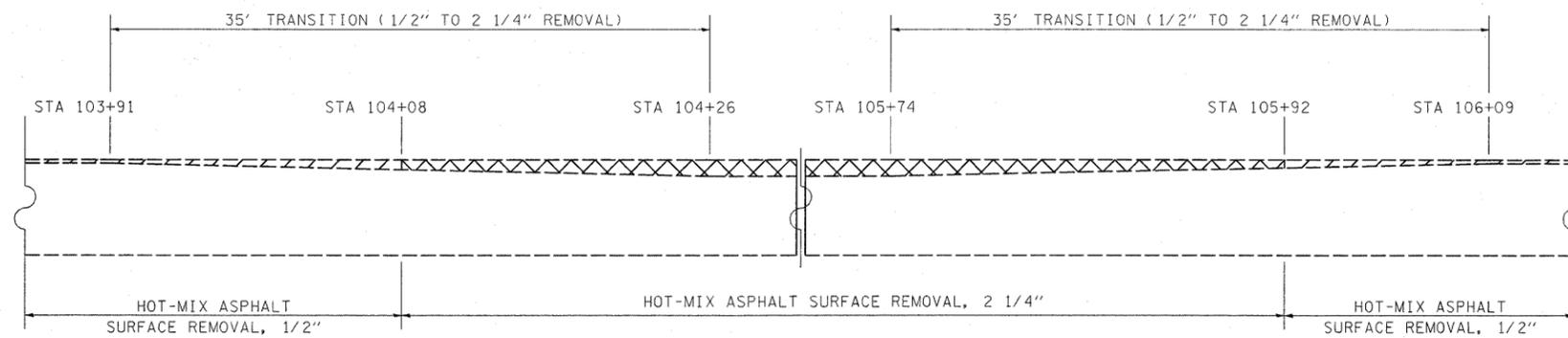
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| | PLOT DATE = 3/23/2010 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

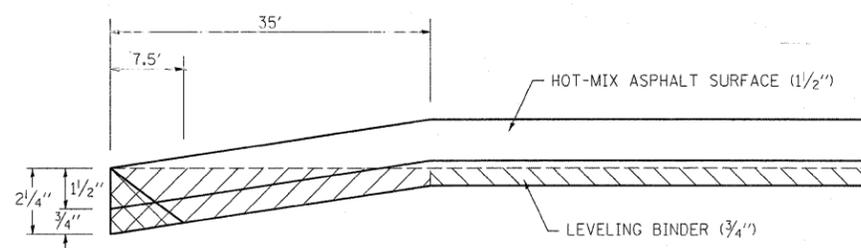
STREET LOCATION MAP

| | | | | |
|--------|-----------|-----------|-------------|----------------|
| SCALE: | SHEET NO. | OF SHEETS | STA. 200+00 | TO STA. 208+00 |
|--------|-----------|-----------|-------------|----------------|

| | | | | |
|---------------------------|-----------|----------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 762 | (1,2)RS-3 | Moultrie | 21 | 10 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 74413 | |



HMA SURFACE REMOVAL TRANSITION DETAIL



- HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT
- HOT-MIX ASPHALT SURFACE REMOVAL (1/2")
- TEMPORARY RAMP

BUTT JOINT DETAIL

STA 80+05 TO STA 80+40
STA 206+93 TO STA 207+28

| | | | |
|--|-----------------------------|------------|-----------|
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| | PLOT DATE = 3/23/2010 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS DETAIL

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|----------|--------------|-----------|
| 762 | (1,2)RS-3 | Moultrie | 21 | 11 |
| CONTRACT NO. 74413 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

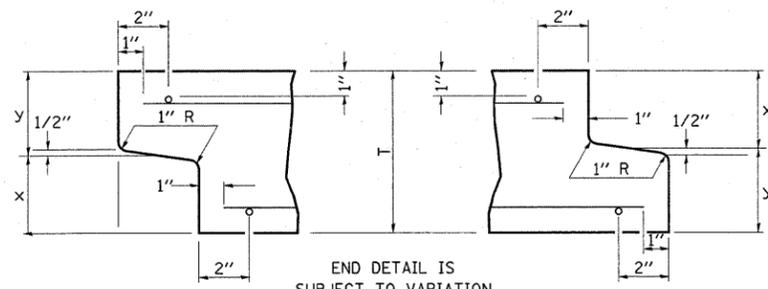
PAVEMENT MARKING SCHEDULE

| STATION TO STATION | LENGTH | SHORT TERM PAVEMENT MARKING | WORK ZONE PAVEMENT MARKING REMOVAL | PAINT PAVEMENT MARKING - LINE 4" | TEMPORARY PAVEMENT MARKING - LINE 4" | RAISED REFLECTIVE PAVEMENT MARKER |
|--------------------|--------|-----------------------------|------------------------------------|----------------------------------|--------------------------------------|-----------------------------------|
| | FEET | FT | SQ FT | FT | FT | EACH |
| 80+05 TO 137+62 | 5757.0 | 2302.8 | 191.9 | 12713.3 | 12713.3 | 72 |
| 137+62 TO 146+33 | 871.0 | 348.4 | 29.0 | 2830.8 | 2830.8 | 11 |
| 146+33 TO 150+64 | 431.0 | 172.4 | 14.4 | 969.8 | 969.8 | 5 |
| 150+64 TO 153+61 | 297.0 | 118.8 | 9.9 | 965.3 | 965.3 | 4 |
| 153+61 TO 155+86 | 225.0 | 90.0 | 7.5 | 900.0 | 900.0 | 3 |
| 155+86 TO 158+95 | 309.0 | 123.6 | 10.3 | 1004.3 | 1004.3 | 4 |
| 158+95 TO 207+28 | 4833.0 | 1933.2 | 161.1 | 10794.3 | 10794.3 | 60 |
| TOTALS | 12723 | 5089 | 424 | 30178 | 30178 | 159 |

PAVING SCHEDULE

| STATION TO STATION | LENGTH | PAVEMENT WIDTH | AREA | BITUMINOUS MATERIALS (PRIME COAT) | AGGREGATE (PRIME COAT) | HMA SURFACE COURSE, MIX "C", NTO | LEVELING BINDER (MACHINE METHOD), NTO | CLASS D PATCHES, TYPE IV, 15 INCH | HMA SURFACE REMOVAL - BUTT JOINT | HMA SURFACE REMOVAL, 1/2" | HOT-MIX ASPHALT REMOVAL, 2 1/4" | AGGREGATE SHOULDERS, TYPE B | TEMPORARY RAMP (SPECIAL) | TEMPORARY RAMP |
|--------------------|--------|----------------|---------|-----------------------------------|------------------------|----------------------------------|---------------------------------------|-----------------------------------|----------------------------------|---------------------------|---------------------------------|-----------------------------|--------------------------|----------------|
| | FEET | FEET | SQ YD | GALLON | TON | TON | TON | SQ YD | SQ YD | SQ YD | SQ YD | TON | SQ YD | SQ YD |
| 80+05 TO 104+08 | 2403.0 | 25.0 | 6675.0 | 667.5 | 13.4 | 560.7 | 280.4 | 0.0 | 97.2 | 6550.0 | 0.0 | 111.7 | 0.0 | 34.7 |
| 104+08 TO 105+92 | 184.0 | 25.0 | 511.1 | 51.1 | 1.0 | 42.9 | 21.5 | 0.0 | 0.0 | 0.0 | 511.0 | 6.3 | 32.9 | 0.0 |
| 105+92 TO 121+45 | 1553.0 | 25.0 | 4313.9 | 431.4 | 8.6 | 362.4 | 181.2 | 0.0 | 0.0 | 4313.9 | 0.0 | 79.3 | 0.0 | 0.0 |
| 121+45 TO 121+77 | 32.0 | 25.0 | 88.9 | 8.9 | 0.2 | 7.5 | 3.7 | 88.9 | 0.0 | 88.9 | 0.0 | 1.8 | 0.0 | 0.0 |
| 121+77 TO 207+28 | 8551.0 | 25.0 | 23752.8 | 2375.3 | 47.5 | 1995.2 | 997.6 | 0.0 | 97.2 | 23669.4 | 0.0 | 403.0 | 0.0 | 34.7 |
| TOTALS | 12723 | - | 35342 | 3534 | 71 | 2969 | 1484 | 89 | 194 | 34622 | 511 | 602 | 33 | 70 |

DETAIL OF PRECAST CONCRETE BOX CULVERT M259



END DETAIL IS SUBJECT TO VARIATION BY FABRICATOR
DETAIL A (TYP. INLET END)
DETAIL B (TYP. OUTLET END)
 NOTE: INLET AND OUTLET ENDS SHALL BE COMPATIBLE.

AASHTO DESIGNATION M259 (WITH 2' OR MORE COVER)

DESIGN LOADING:
 TABLE 1 = HS20
 TABLE 2 = INTERSTATE LOADING

| CULVERT SIZE | DIMENSIONS ** | | |
|--------------|---------------|------------|------------|
| | T (INCHES) | A FT-IN | B FT-IN |
| * 2x2 | 4 | 2-8 | 2-8 |
| 3x2 | 4 | 3-8 | 2-8 |
| 3x3 | 4 | 3-8 | 3-8 |
| 4x2 | 5 | 4-10 | 2-10 |
| 4x3 | 5 | 4-10 | 3-10 |
| 4x4 | 5 | 4-10 | 4-10 |
| * 5x2 | 6 | 6-0 | 3-0 |
| 5x3 | 6 | 6-0 | 4-0 |
| 5x4 | 6 | 6-0 | 5-0 |
| 5x5 | 6 | 6-0 | 6-0 |
| * 6x2 | 7 | 7-2 | 3-2 |
| 6x3 | 7 | 7-2 | 4-2 |
| 6x4 | 7 | 7-2 | 5-2 |
| 6x5 | 7 | 7-2 | 6-2 |
| 6x6 | 7 | 7-2 | 7-2 |
| 7x4 | 8 | 8-4 | 5-4 |
| 7x5 | 8 | 8-4 | 6-4 |
| 7x6 | 8 | 8-4 | 7-4 |
| 7x7 | 8 | 8-4 | 8-4 |
| 8x4 | 8 | 9-4 | 5-4 |
| 8x5 | 8 | 9-4 | 6-4 |
| 8x6 | 8 | 9-4 | 7-4 |
| 8x7 | 8 | 9-4 | 8-4 |
| 8x8 | 8 | 9-4 | 9-4 |
| 9x5 | 9 | 10-6 | 6-6 |
| 9x6 | 9 | 10-6 | 7-6 |
| 9x7 | 9 | 10-6 | 8-6 |
| 9x8 | 9 | 10-6 | 9-6 |
| 9x9 | 9 | 10-6 | 10-6 |
| 10x5 | 10 | 11-8 | 6-8 |
| 10x6 | 10 | 11-8 | 7-8 |
| 10x7 | 10 | 11-8 | 8-8 |
| 10x8 | 10 | 11-8 | 9-8 |
| 10x9 | 10 | 11-8 | 10-8 |
| 10x10 | 10 | 11-8 | 11-8 |
| 11x4 | 11 | 12-10 | 5-10 |
| 11x6 | 11 | 12-10 | 7-10 |
| 11x8 | 11 | 12-10 | 9-10 |
| 11x10 | 11 | 12-10 | 11-10 |
| 11x11 | 11 | 12-10 | 12-10 |
| 12x4 | 12 | 14-0 | 6-0 |
| 12x6 | 12 | 14-0 | 8-0 |
| 12x8 | 12 | 14-0 | 10-0 |
| 12x10 | 12 | 14-0 | 12-0 |
| 12x12 | 12 | 14-0 | 14-0 |

GENERAL NOTES

SHOP PLANS FOR THE REINFORCEMENT SHALL BE SUBMITTED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 540.06 OF THE STANDARD SPECIFICATIONS.

MINIMUM CONCRETE STRENGTH SHALL BE 5000 PSI AFTER 28 DAYS.

THE JOINTS OF THE PRECAST BOX SECTIONS SHALL BE SEALED IN ACCORDANCE WITH ARTICLE 540.06 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

THE TERMS A_{s1} , A_{s2} , A_{s3} , AND A_{s4} DENOTE THE REQUIRED STEEL AREAS FOR REINFORCEMENT AS SPECIFIED IN AASHTO M259. REINFORCEMENT SHALL BE WELDED WIRE FABRIC CONFORMING TO AASHTO SPECIFICATIONS M55 OR M221.

LIFTING HOLES SHALL BE FILLED IN ACCORDANCE WITH 540.06 OR PROJECT SPECIAL PROVISIONS.

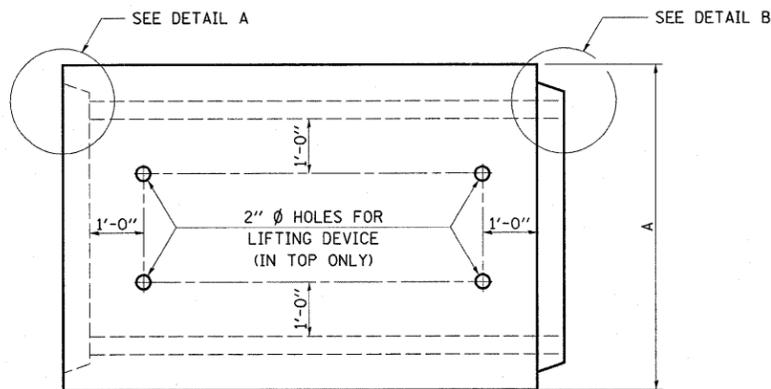
DRAINAGE OPENINGS SHALL BE PROVIDED IN ACCORDANCE WITH ARTICLE 503.11 OF THE STANDARD SPECIFICATIONS. LOCATION AND SPACING OF THE OPENINGS SHALL BE SHOWN ON THE SHOP DRAWINGS.

DESIGN FILL HEIGHT (F):

MAXIMUM FILL HEIGHT (f_{max}) AND MINIMUM FILL HEIGHT (f_{min}) ARE MEASURED BETWEEN THE EXTREME EDGES OF SHOULDER.

| f_{max}/f_{min} (FT) | F (FT) | AASHTO |
|------------------------|-----------|--------|
| $f_{min} < 2'$ | $< 2'$ | M273 |
| $2' \leq f_{min} < 3'$ | 2' | M259 |
| $3' \leq f_{min} < 4'$ | 3' | M259 |
| $f_{min} \geq 4'$ | f_{max} | M259 |

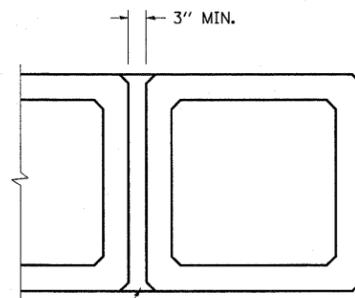
DESIGN FILL HEIGHTS APPLY TO ENTIRE CULVERT, INCLUDING EXTENSIONS AND END SECTIONS.



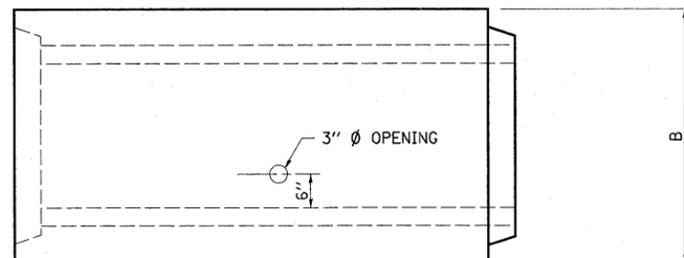
PLAN
 LOCATION OF LIFTING HOLES MAY BE VARIED AS NEEDED TO CLEAR REINFORCEMENT.

SECTION LENGTH

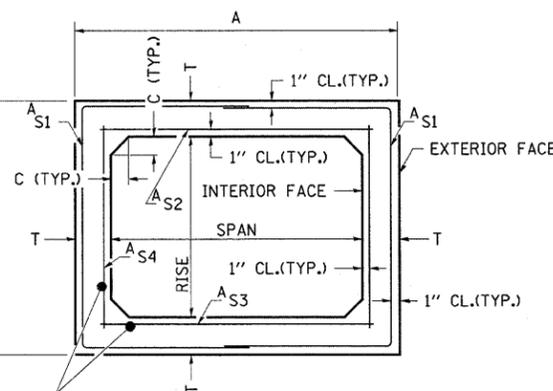
WHEN EXTENDING AN EXISTING BOX, PLACE THIS END AGAINST THE EXISTING HEADWALL.



CLASS SI CONCRETE
MULTIPLE UNIT PLACEMENT



ELEVATION



LONGITUDINAL REINFORCEMENT
CROSS SECTION

FILE NAME =
 ca:\pwwork\pwwid\TEASLEYCK\0148533\07

USER NAME = teasleyck
 74413-shr-deta1.dgn

DESIGNED -
 DRAWN -

REVISED - JEH 06-01
 REVISED - RGW 09-01

REVISOR -
 CHECKED -
 DATE -

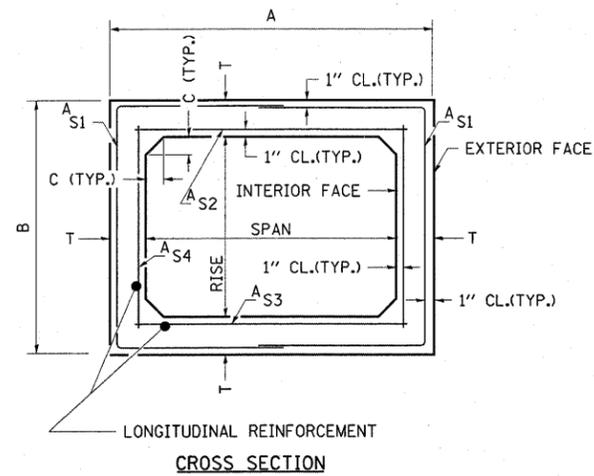
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DETAIL OF PRECAST CONCRETE BOX CULVERT SECTION

F.A.P. RTE. 762 SECTION (1,2)RS-3 COUNTY Moultrie TOTAL SHEETS 21 SHEET NO. 14 CONTRACT NO. 74413

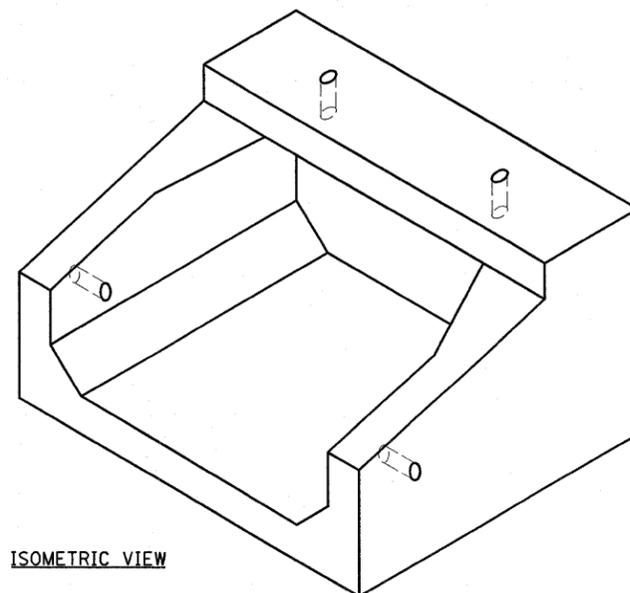
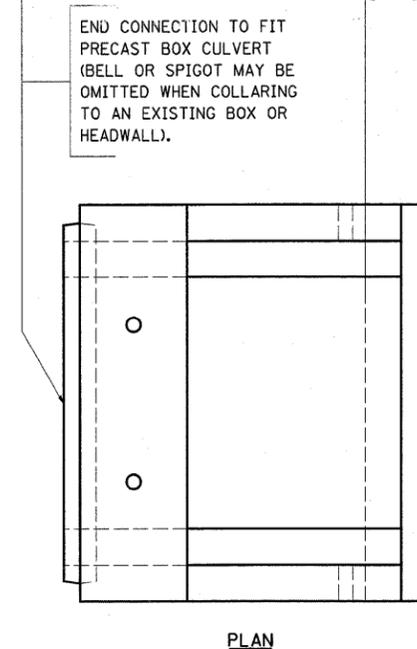
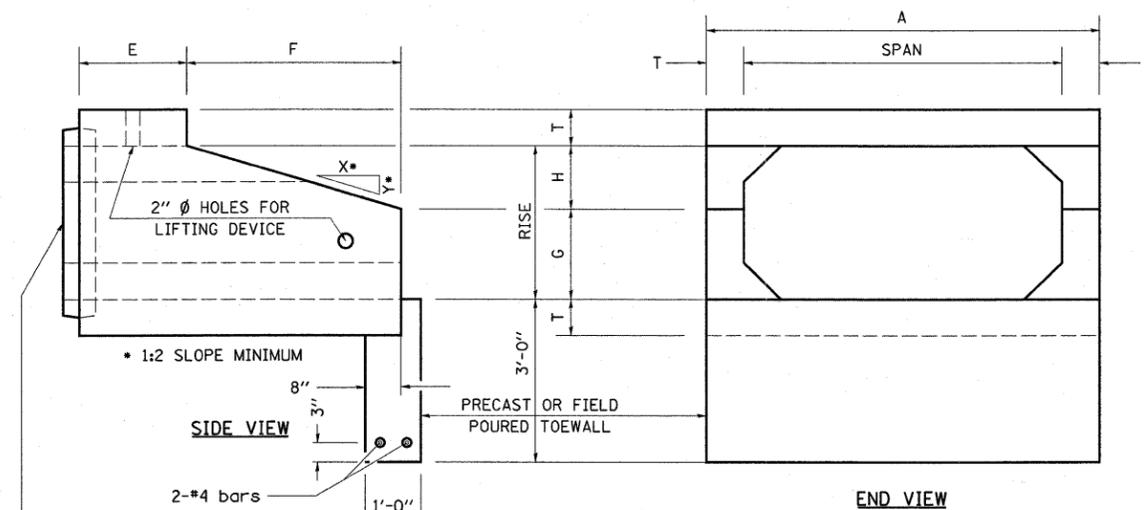
SCALE: SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

DETAIL OF PRECAST CONCRETE BOX CULVERT END SECTION



AASHTO DESIGNATION M259 (2' OR MORE COVER)

DESIGN LOADING:
TABLE 1 = HS20
TABLE 2 = INTERSTATE LOADING



GENERAL NOTES

SHOP PLANS FOR THE REINFORCEMENT SHALL BE SUBMITTED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 540.06 OF THE STANDARD SPECIFICATIONS.

MINIMUM CONCRETE STRENGTH SHALL BE 5000 PSI AFTER 28 DAYS.

THE JOINTS OF THE PRECAST BOX SECTIONS SHALL BE SEALED IN ACCORDANCE WITH ARTICLE 504.06 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

THE TERMS A_{s1} , A_{s2} , A_{s3} , AND A_{s4} DENOTE THE REQUIRED STEEL AREAS FOR REINFORCEMENT AS SPECIFIED IN AASHTO M273.

REINFORCEMENT SHALL BE WELDED WIRE FABRIC CONFORMING TO AASHTO SPECIFICATIONS M55 OR M221.

LIFTING HOLES SHALL BE FILLED IN ACCORDANCE WITH 540.02 OR PROJECT SPECIAL PROVISIONS.

DESIGN FILL HEIGHT (F):

MAXIMUM FILL HEIGHT (f_{max}) AND MINIMUM FILL HEIGHT (f_{min}) ARE MEASURED BETWEEN THE EXTREME EDGES OF SHOULDER.

| f_{max}/f_{min} (FT) | F (FT) | AASHTO |
|------------------------|-----------|--------|
| $f_{min} < 2'$ | $< 2'$ | M273 |
| $2' \leq f_{min} < 3'$ | 2' | M259 |
| $3' \leq f_{min} < 4'$ | 3' | M259 |
| $f_{min} \geq 4'$ | f_{max} | M259 |

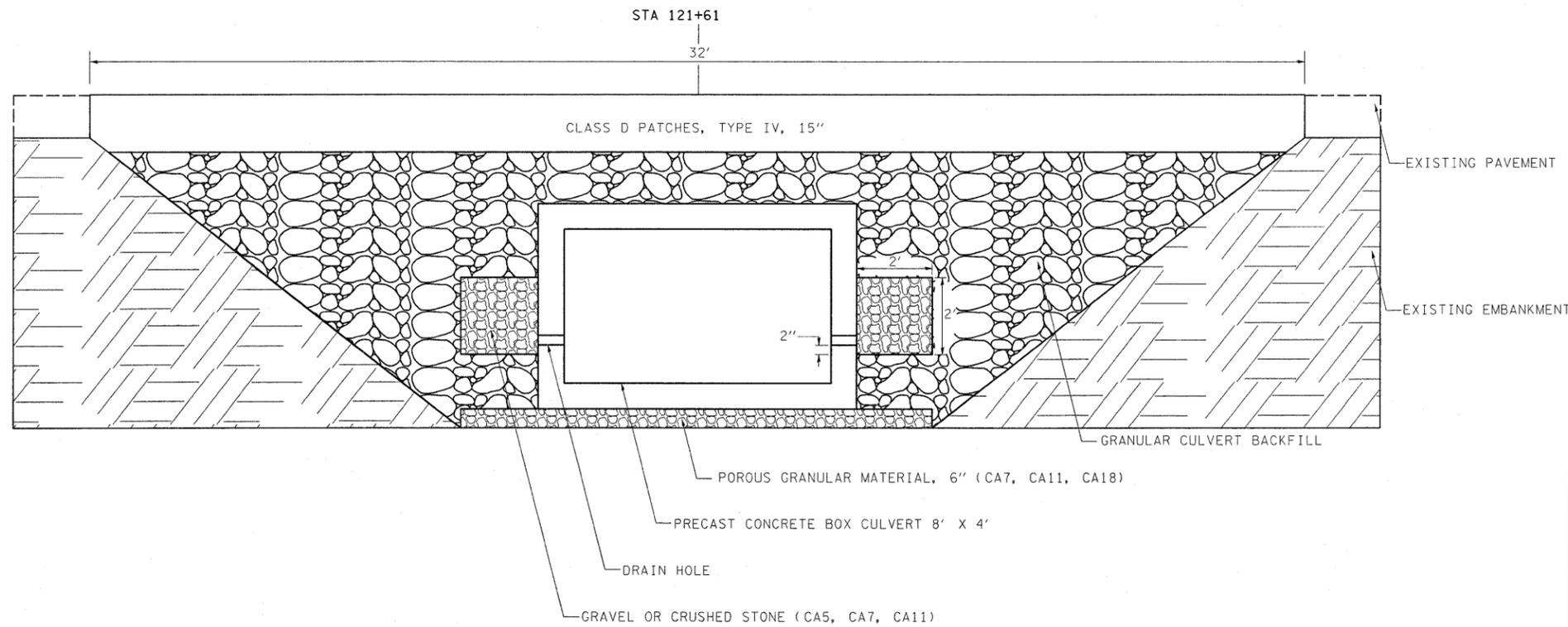
DESIGN FILL HEIGHTS APPLY TO ENTIRE CULVERT, INCLUDING EXTENSIONS AND END SECTIONS.

| CULVERT SIZE | DIMENSIONS ** | | | | | | | | | |
|--------------|------------------|-------|-------|-------|------|-------|-------|-------|-------|-------|
| | SPANxRISE (FOOT) | T | A | B | C | E | F | G | H | SLOPE |
| | | INCH | FT-IN | FT-IN | INCH | FT-IN | FT-IN | FT-IN | FT-IN | Y:X |
| * 2x2 | 4 | 2-8 | 2-8 | 4 | 3-0 | 3-0 | 1-0 | 1-0 | 1-0 | 1:3 |
| 3x2 | 4 | 3-8 | 2-8 | 4 | 3-0 | 3-0 | 1-0 | 1-0 | 1-0 | 1:3 |
| 3x3 | 4 | 3-8 | 3-8 | 4 | 2-0 | 4-0 | 1-8 | 1-4 | 1-4 | 1:3 |
| 4x2 | 5 | 4-10 | 2-10 | 5 | 3-0 | 3-0 | 1-0 | 1-0 | 1-0 | 1:3 |
| 4x3 | 5 | 4-10 | 3-10 | 5 | 2-0 | 4-0 | 1-8 | 1-4 | 1-4 | 1:3 |
| 4x4 | 5 | 4-10 | 4-10 | 5 | 2-0 | 4-0 | 2-0 | 2-0 | 2-0 | 1:2 |
| * 4x6 | 7 | 5-2 | 7-2 | 7 | 2-0 | 6-0 | 3-0 | 3-0 | 3-0 | 1:2 |
| * 5x2 | 5 | 5-10 | 2-10 | 6 | 3-0 | 3-0 | 1-0 | 1-0 | 1-0 | 1:3 |
| 5x3 | 6 | 6-0 | 4-0 | 6 | 2-0 | 4-0 | 1-8 | 1-4 | 1-4 | 1:3 |
| 5x4 | 6 | 6-0 | 5-0 | 6 | 2-0 | 4-0 | 2-0 | 2-0 | 2-0 | 1:2 |
| 5x5 | 6 | 6-0 | 6-0 | 6 | 2-0 | 4-0 | 3-0 | 2-0 | 2-0 | 1:2 |
| * 6x2 | 7 | 7-2 | 3-2 | 7 | 3-0 | 3-0 | 1-0 | 1-0 | 1-0 | 1:3 |
| 6x3 | 7 | 7-2 | 4-2 | 7 | 2-0 | 4-0 | 1-8 | 1-4 | 1-4 | 1:3 |
| 6x4 | 7 | 7-2 | 5-2 | 7 | 2-0 | 4-0 | 2-0 | 2-0 | 2-0 | 1:2 |
| 6x5 | 7 | 7-2 | 6-2 | 7 | | | | | | |
| 6x6 | 7 | 7-2 | 7-2 | 7 | 2-0 | 6-0 | 3-0 | 3-0 | 3-0 | 1:2 |
| 7x4 | 8 | 8-4 | 5-4 | 8 | 2-0 | 4-0 | 2-0 | 2-0 | 2-0 | 1:2 |
| 7x5 | 8 | 8-4 | 6-4 | 8 | | | | | | |
| 7x6 | 8 | 8-4 | 7-4 | 8 | | | | | | |
| 7x7 | 8 | 8-4 | 8-4 | 8 | | | | | | |
| 8x4 | 8 | 9-4 | 5-4 | 8 | 2-0 | 4-0 | 2-0 | 2-0 | 2-0 | 1:2 |
| 8x5 | 8 | 9-4 | 6-4 | 8 | | | | | | |
| 8x6 | 8 | 9-4 | 7-4 | 8 | | | | | | |
| 8x7 | 8 | 9-4 | 8-4 | 8 | | | | | | |
| 8x8 | 8 | 9-4 | 9-4 | 8 | | | | | | |
| 9x5 | 9 | 10-6 | 6-6 | 9 | | | | | | |
| 9x6 | 9 | 10-6 | 7-6 | 9 | | | | | | |
| 9x7 | 9 | 10-6 | 8-6 | 9 | | | | | | |
| 9x8 | 9 | 10-6 | 9-6 | 9 | | | | | | |
| 9x9 | 9 | 10-6 | 10-6 | 9 | | | | | | |
| 10x5 | 10 | 11-8 | 6-8 | 10 | | | | | | |
| 10x6 | 10 | 11-8 | 7-8 | 10 | | | | | | |
| 10x7 | 10 | 11-8 | 8-8 | 10 | | | | | | |
| 10x8 | 10 | 11-8 | 9-8 | 10 | | | | | | |
| 10x9 | 10 | 11-8 | 10-8 | 10 | | | | | | |
| 10x10 | 10 | 11-8 | 11-8 | 10 | | | | | | |
| 11x4 | 11 | 12-10 | 5-10 | 11 | | | | | | |
| 11x6 | 11 | 12-10 | 7-10 | 11 | | | | | | |
| 11x8 | 11 | 12-10 | 9-10 | 11 | | | | | | |
| 11x10 | 11 | 12-10 | 11-10 | 11 | | | | | | |
| 11x11 | 11 | 12-10 | 12-10 | 11 | | | | | | |
| 12x4 | 12 | 14-0 | 6-0 | 12 | | | | | | |
| 12x6 | 12 | 14-0 | 8-0 | 12 | | | | | | |
| 12x8 | 12 | 14-0 | 10-0 | 12 | | | | | | |
| 12x10 | 12 | 14-0 | 12-0 | 12 | | | | | | |
| 12x12 | 12 | 14-0 | 14-0 | 12 | | | | | | |

CULVERTS WITH RISE > 6' REQUIRE CAST-IN-PLACE END SECTIONS

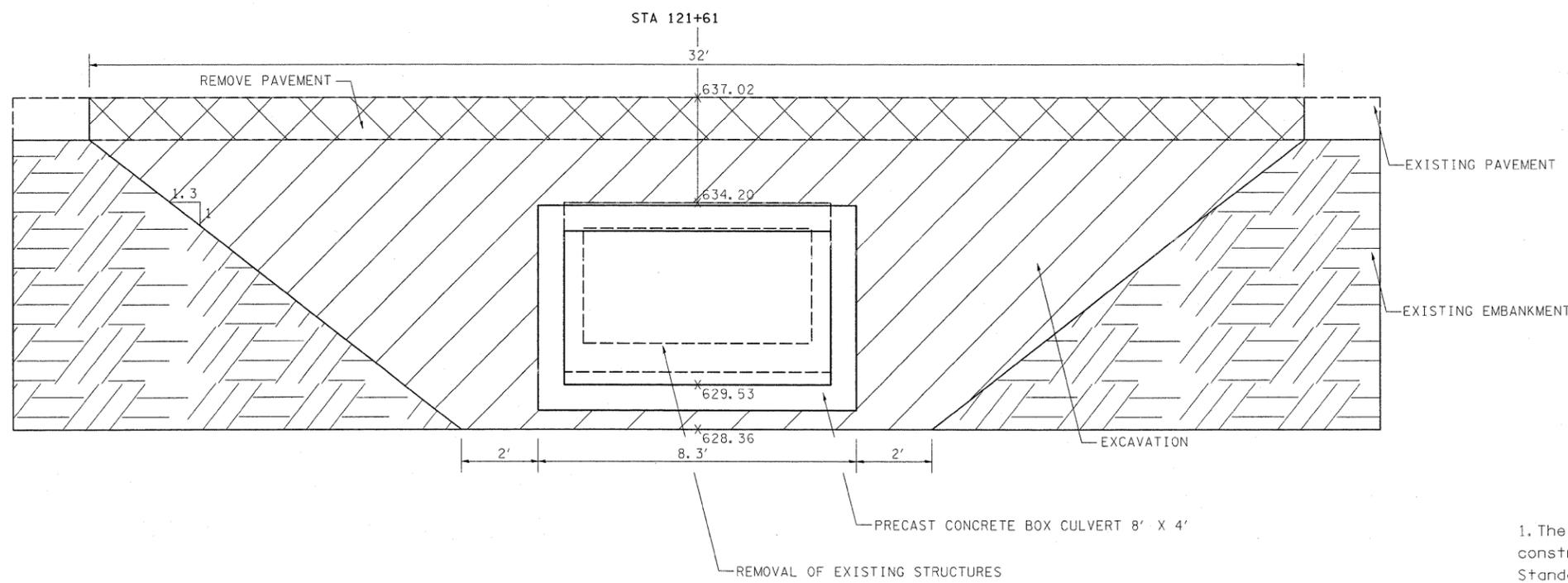
* NON-STANDARD SIZES PER AASHTO.

** "T" MAY BE LARGER THAN SHOWN ON TABLE, PER AASHTO SPECIFICATIONS.



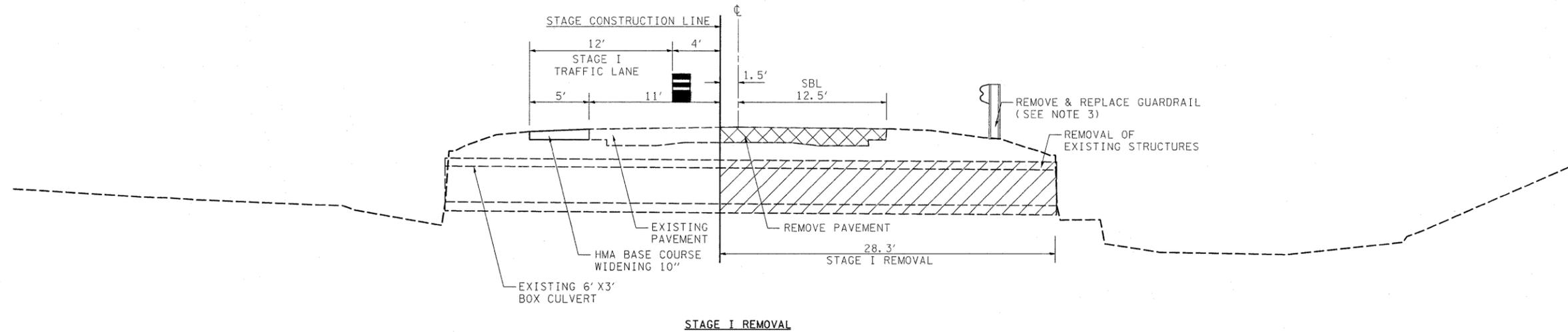
TOTAL BILL OF MATERIAL

| ITEM | UNIT | QUANTITY |
|--------------------------------------|-------|----------|
| REMOVAL OF EXISTING STRUCTURES | EACH | 1 |
| PRECAST CONCRETE BOX CULVERT 8' X 4' | FOOT | 49 |
| BOX CULVERT END SECTIONS | EACH | 2 |
| GRANULAR CULVERT BACKFILL | CU YD | 192 |
| STONE RIPRAP, CLASS A5 | SQ YD | 172 |
| FILTER FABRIC | SQ YD | 172 |

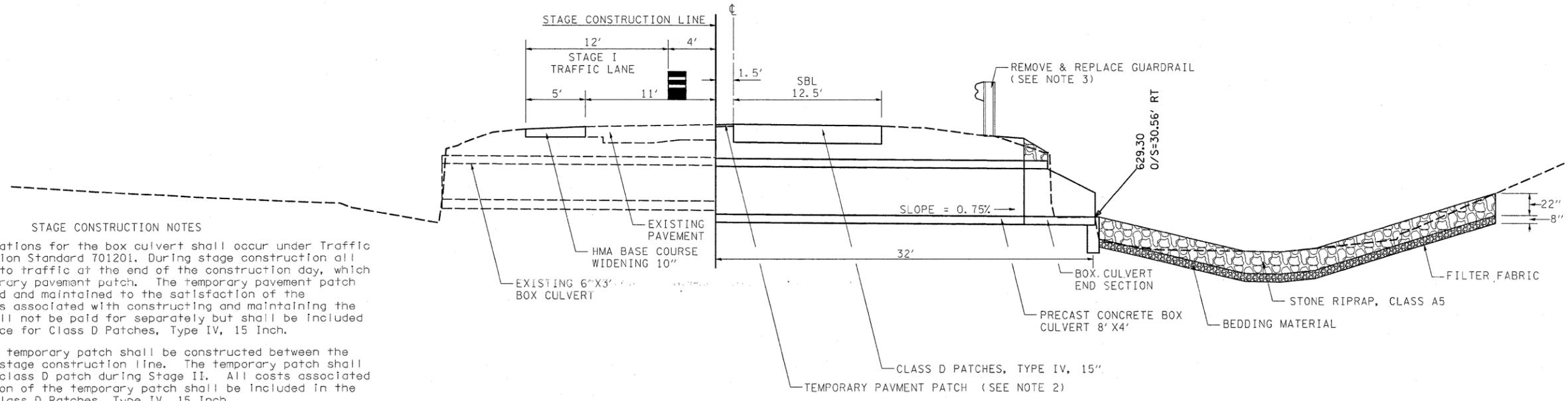


GENERAL NOTES

1. The Excavation required for the proposed box culvert shall be constructed according to the applicable sections of Article 502 of the Standard Specifications.
2. All costs associated with the construction of the Excavation shall not be paid for separately but shall be included in the per unit price of the Precast Concrete Box Culvert 8' x 4'.
3. All elevations shown are at the centerline of IL 32.



STAGE I REMOVAL



STAGE I CONSTRUCTION

STAGE CONSTRUCTION NOTES

1. Construction operations for the box culvert shall occur under Traffic Control and Protection Standard 701201. During stage construction all lanes must be open to traffic at the end of the construction day, which may require a temporary pavement patch. The temporary pavement patch shall be constructed and maintained to the satisfaction of the Engineer. All costs associated with constructing and maintaining the temporary patch shall not be paid for separately but shall be included in the per unit price for Class D Patches, Type IV, 15 Inch.

2. During Stage I, a temporary patch shall be constructed between the centerline and the stage construction line. The temporary patch shall be replaced with a class D patch during Stage II. All costs associated with the construction of the temporary patch shall be included in the per unit price of Class D Patches, Type IV, 15 Inch.

3. During Stage I, a segment of the existing guardrail shall be removed as needed to provide access for equipment required to construct the proposed box culvert. Prior to opening the roadway to traffic, drums or cones with bidirectional flashing lights shall be placed in the area of the guardrail removal to the satisfaction of the Engineer. The removed segment of guardrail shall be replaced with new steel plate beam guardrail, type A prior to any work on Stage II. All costs associated with the removal and replacement of the guardrail shall be included in the per unit cost of the Precast Concrete Box Culvert 8' x 4'.

4. Seeding, Class 2A shall be applied to all areas outside of the proposed aggregate shoulders that are disturbed during construction of the box culvert. The seeding shall be applied according to the applicable sections of Article 250 of the standard specifications. Seeding, Class 2A shall not be paid for separately, but shall be included in the per unit price of Precast Concrete Box Culvert 8' x 4'.

5. During lane closures the aggregate shoulders in the construction zone shall be maintained to support traffic per the satisfaction of the Engineer. The material used on the shoulders shall conform to Article 481.02 of the Standard Specifications. All costs associated with maintaining the shoulders shall not be paid for separately but shall be included in the per unit cost of the Precast Concrete Box Culvert 8' x 4'.

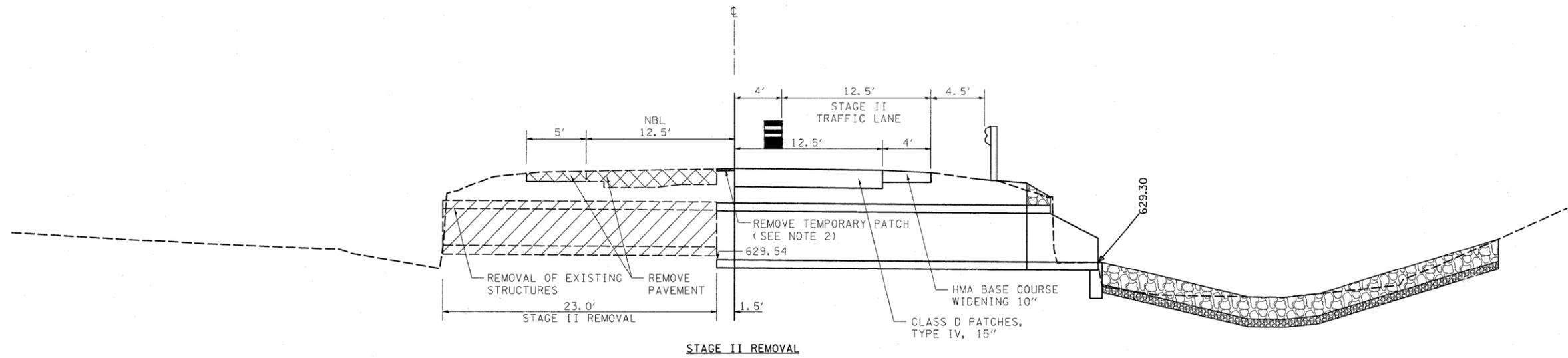
| | | | |
|---|-----------------------|------------|-----------|
| FILE NAME = | USER NAME = teasleyck | DESIGNED - | REVISED - |
| ca:\p\work\PM\DOT\TEASLEYCK\d0148533\074413-sht-details.dgn | 74413-sht-details.dgn | DRAWN - | REVISED - |
| PLOT SCALE = 50.0000' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 4/30/2010 | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

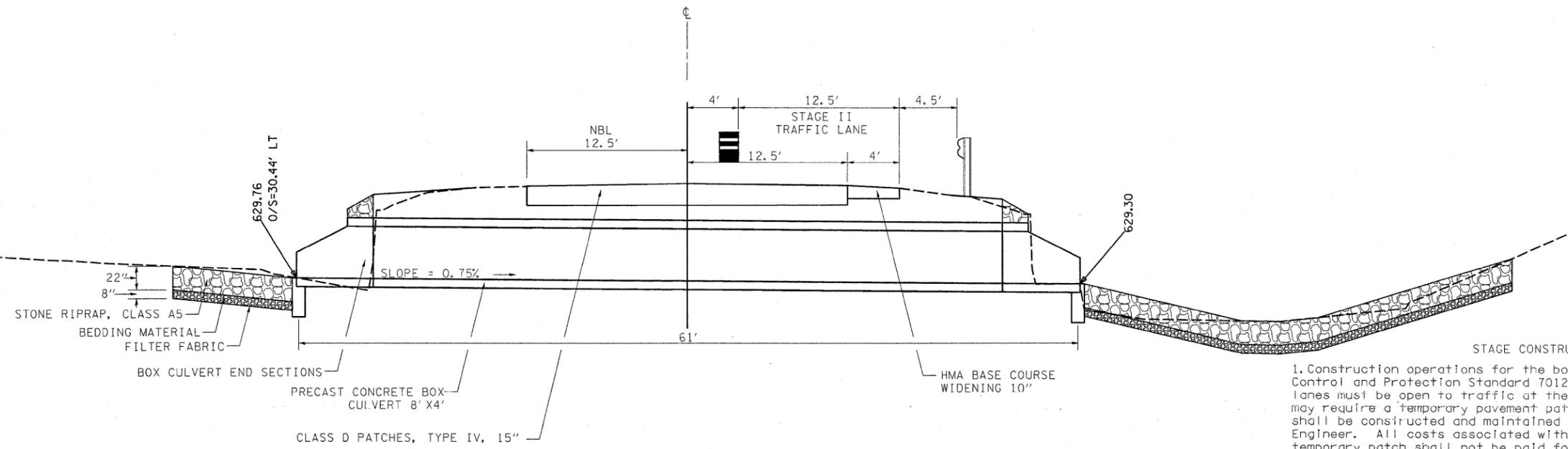
STAGE I CONSTRUCTION DETAILS
STA 121 + 61

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---------------------------|-----------|----------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 762 | (1,2)RS-3 | Moultrie | 21 | 17 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 74413 | |



STAGE II REMOVAL



STAGE II CONSTRUCTION

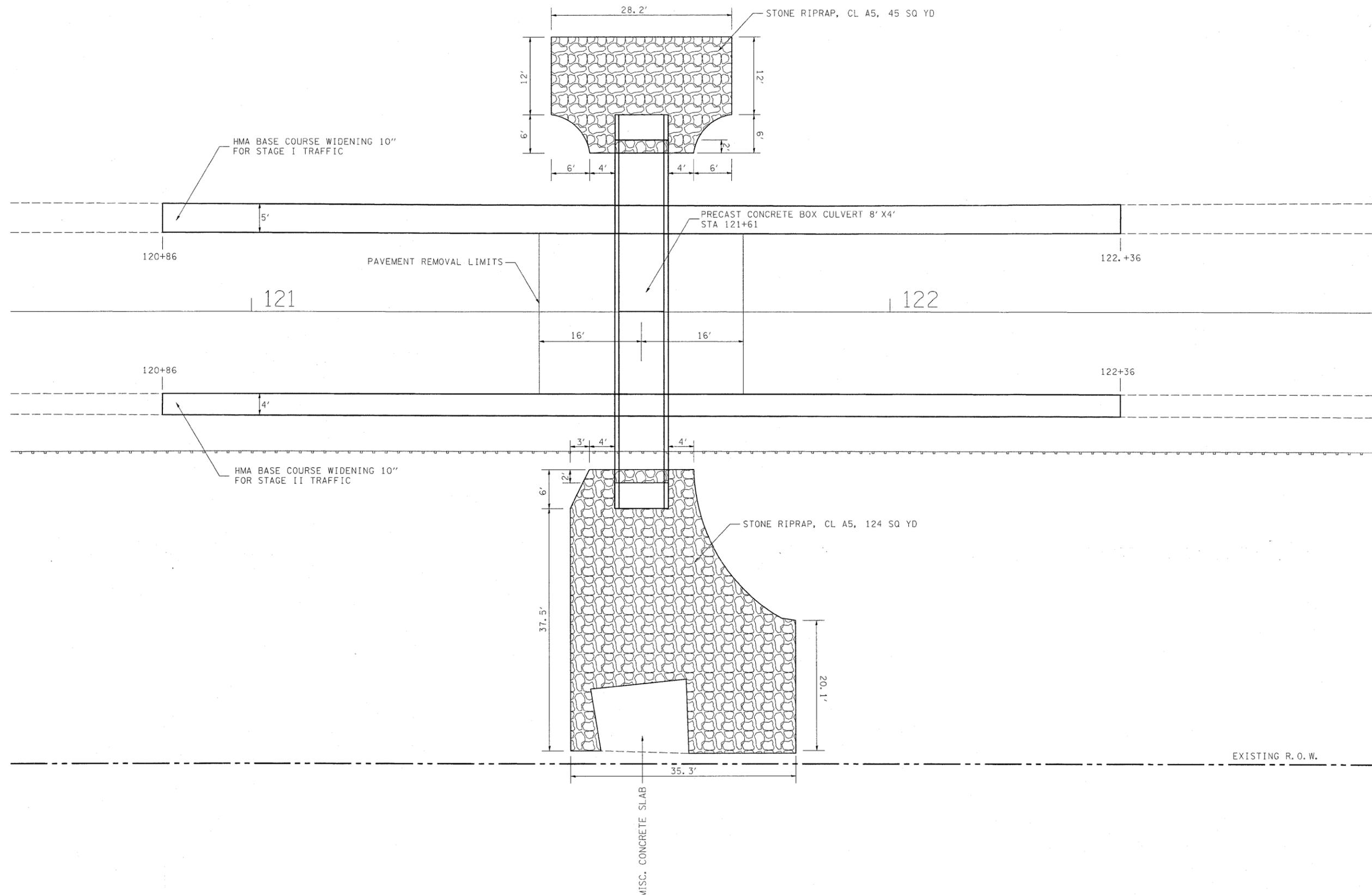
STAGE CONSTRUCTION NOTES

1. Construction operations for the box culvert shall occur under Traffic Control and Protection Standard 701201. During stage construction all lanes must be open to traffic at the end of the construction day, which may require a temporary pavement patch. The temporary pavement patch shall be constructed and maintained to the satisfaction of the Engineer. All costs associated with constructing and maintaining the temporary patch shall not be paid for separately but shall be included in the per unit price for Class D Patches, Type IV, 15 Inch.
2. During Stage I, a temporary patch shall be constructed between the centerline and the stage construction line. The temporary patch shall be replaced with a class D patch during Stage II. All costs associated with the construction of the temporary patch shall be included in the per unit price of Class D Patches, Type IV, 15 Inch.
3. During Stage I, a segment of the existing guardrail shall be removed as needed to provide access for equipment required to construct the proposed box culvert. Prior to opening the roadway to traffic, drums or cones with bidirectional flashing lights shall be placed in the area of the guardrail removal to the satisfaction of the Engineer. The removed segment of guardrail shall be replaced with new steel plate beam guardrail, type A prior to any work on Stage II. All costs associated with the removal and replacement of the guardrail shall be included in the per unit cost of the Precast Concrete Box Culvert 8' x 4'.
4. Seeding, Class 2A shall be applied to all areas outside of the proposed aggregate shoulders that are disturbed during construction of the box culvert. The seeding shall be applied according to the applicable sections of Article 250 of the standard specifications. Seeding, Class 2A shall not be paid for separately, but shall be included in the per unit price of Precast Concrete Box Culvert 8' x 4'.
5. During lane closures the aggregate shoulders in the construction zone shall be maintained to support traffic per the satisfaction of the Engineer. The material used on the shoulders shall conform to Article 481.02 of the Standard Specifications. All costs associated with maintaining the shoulders shall not be paid for separately but shall be included in the per unit cost of the Precast Concrete Box Culvert 8' x 4'.

| | | | | | | | | | | | | |
|--|--|------------|-----------|---|---|-----------|----------|---------------------------|---------|--------|-----------------|--------------|
| FILE NAME = | USER NAME = teasleyck | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STAGE II CONSTRUCTION DETAILS STA 121 + 61 | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ca:\pwwork\k\PIWIDOT\TEASLEYCK\00148633\07 | 74413-shit-details.dgn | DRAWN - | REVISED - | | 762 | (1,2)RS-3 | Moultrie | 21 | 18 | | | |
| | PLOT SCALE = 50.0000 "/td> <td>CHECKED -</td> <td>REVISED -</td> <td colspan="3"></td> <td colspan="2" style="text-align: center;">CONTRACT NO. 74413</td> | CHECKED - | REVISED - | | | | | CONTRACT NO. 74413 | | | | |
| | PLOT DATE = 4/30/2010 | DATE - | REVISED - | | SCALE: SHEET NO. OF SHEETS STA. TO STA. | | | ILLINOIS FED. AID PROJECT | | | | |

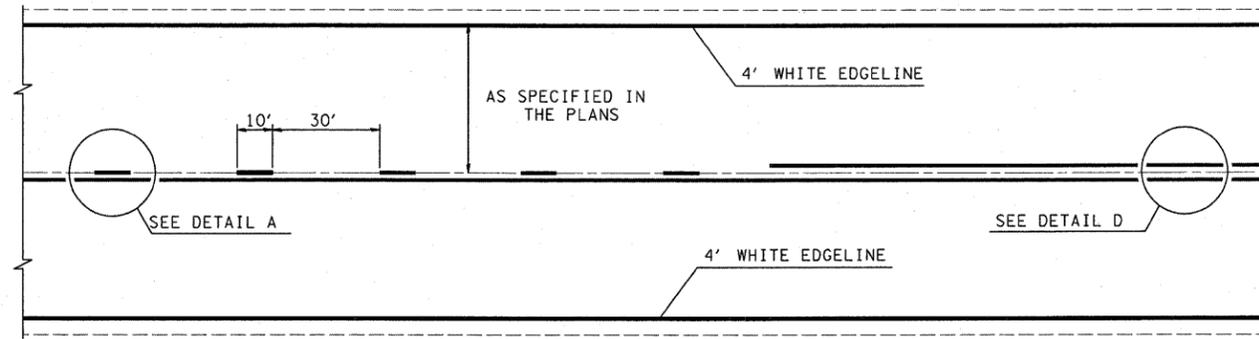


EXISTING R. O. W.

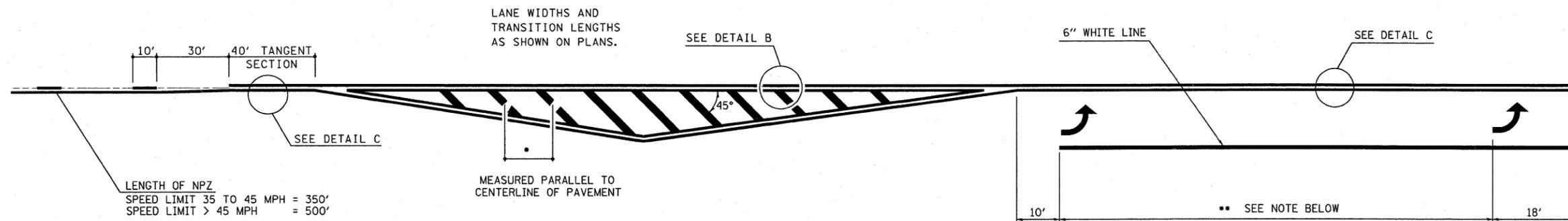


EXISTING R. O. W.

| | | | | | | | | | | | | |
|---|-----------------------------|------------|-----------|---|----------------------------|-----------|-----------|----------------|---------|--------------------|-----------------|--------------|
| FILE NAME = | USER NAME = teasleyck | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STONE RIPRAP DETAIL | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| cr\p\work\PIWIDOT\TEASLEYCK\08148533\07 | 74413-sht-detail.dgn | DRAWN - | REVISED - | | 762 | (1,2)RS-3 | Moultrie | 21 | 19 | | | |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. 74413 | | |
| | PLOT DATE = 4/30/2010 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |



TYPICAL CENTERLINE, NO PASSING ZONE LINE (NPZ) & EDGELINE MARKINGS
2-LANE PAVEMENT



LENGTH OF NPZ
SPEED LIMIT 35 TO 45 MPH = 350'
SPEED LIMIT > 45 MPH = 500'

LANE WIDTHS AND
TRANSITION LENGTHS
AS SHOWN ON PLANS.

MEASURED PARALLEL TO
CENTERLINE OF PAVEMENT

DIAGONAL SPACING

| | | |
|-------------|--------------|-------|
| SPEED LIMIT | < 30 MPH | = 15' |
| SPEED LIMIT | 30 TO 45 MPH | = 20' |
| SPEED LIMIT | > 45 MPH | = 30' |

** PLACE THE FIRST ARROW EVEN WITH THE BEGINNING OF THE TURN BAY.
PLACE ANOTHER ARROW 18' BACK FROM THE STOP BAR. IF REQUIRED,
ADD ADDITIONAL ARROWS TO MAINTAIN A MAXIMUM OF 80' SPACING.

NOTES:

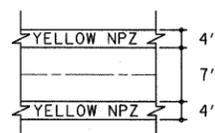
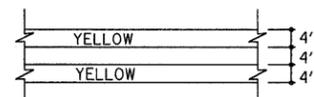
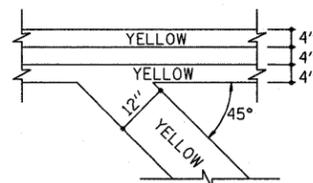
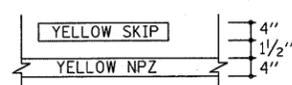
1. OMIT EDGELINE THRU SIDE ROADS.
2. TAPER RATES, LANE WIDTHS, AND STORAGE LENGTHS AS SHOWN ON THE PLANS.
3. WHEN MEDIANS ARE PRESENT, PLACE A SINGLE YELLOW STRIPE AROUND ITS PERIMETER.

DETAIL A

DETAIL B

DETAIL C

DETAIL D



NOT TO SCALE

| | | | |
|--|-----------------------------|------------|---------------------|
| FILE NAME = | USER NAME = teasleyok | DESIGNED - | REVISED - MMO 12-99 |
| ca:\pw_work\pvidot\teasleyok\d8148533\07 | 4413-sht-details.dgn | DRAWN - | REVISED - DRM 08-04 |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - MKS 04-08 |
| | PLOT DATE = 3/23/2010 | DATE - | REVISED - |

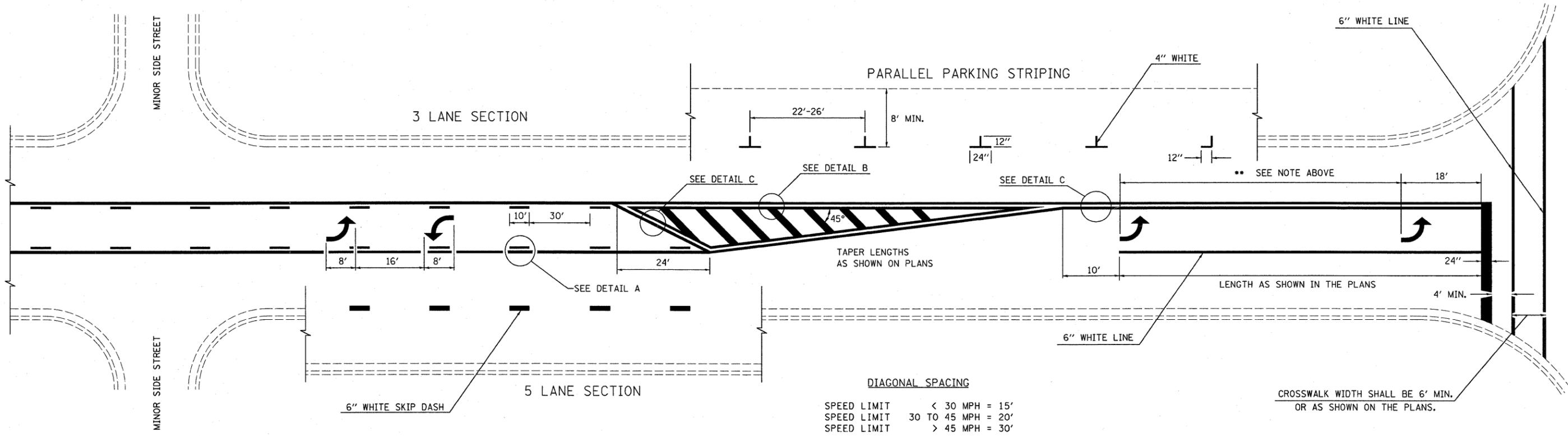
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS OF PAVEMENT MARKINGS
SHEET 1 OF 2

| | | | | |
|--------------------|-----------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 762 | (1,2)RS-3 | Moultrie | 21 | 20 |
| CONTRACT NO. 74413 | | | | |
| SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. |
| | | | | |

ILLINOIS FED. AID PROJECT

•• PLACE THE FIRST ARROW EVEN WITH THE BEGINNING OF THE TURN BAY.
 PLACE ANOTHER ARROW 18' BACK FROM THE STOP BAR. IF REQUIRED,
 ADD ADDITIONAL ARROWS TO MAINTAIN A MAXIMUM OF 80' SPACING.



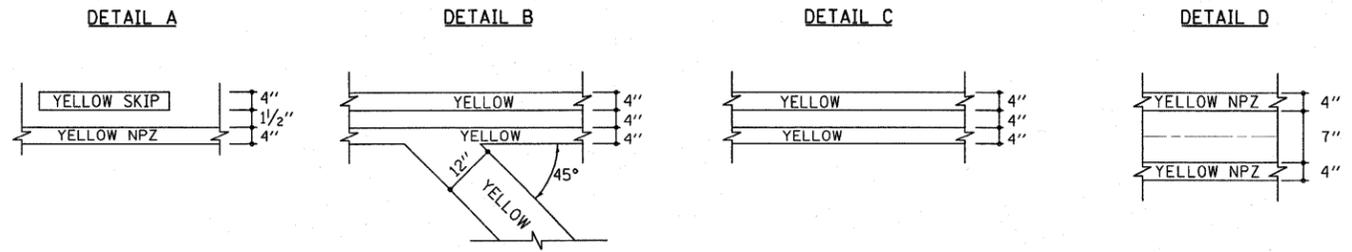
DIAGONAL SPACING

| | | |
|-------------|--------------|-------|
| SPEED LIMIT | < 30 MPH | = 15' |
| SPEED LIMIT | 30 TO 45 MPH | = 20' |
| SPEED LIMIT | > 45 MPH | = 30' |

CROSSWALK WIDTH SHALL BE 6' MIN.
 OR AS SHOWN ON THE PLANS.

TYPICAL BI-DIRECTIONAL TURN LANE MARKINGS
 (SHOWING TRANSITION TO EXCLUSIVE LEFT TURN LANE)

- NOTE:**
1. NO LESS THAN 2 ARROWS SHALL BE USED.
 2. TAPER RATES, LANE WIDTHS, AND STORAGE LENGTHS AS SHOWN ON THE PLANS.
 3. AVOID PLACING BI-DIRECTIONAL ARROWS IN INTERSECTIONS.
 4. BI-DIRECTIONAL ARROWS SHALL BE SPACED A MAXIMUM OF 250'.



NOT TO SCALE