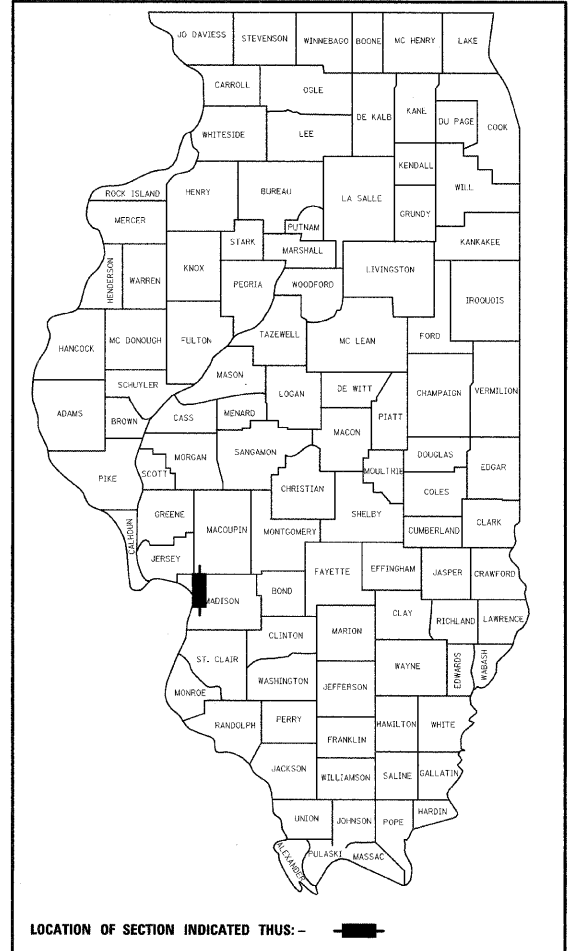


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	1

FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT

\* (662 - 1 + 6 = 667)

D-98-113-96



# STATE OF ILLINOIS

## DEPARTMENT OF TRANSPORTATION

### DIVISION OF HIGHWAYS

# PLANS FOR PROPOSED FEDERAL AID HIGHWAY

**FAP 310 (US 67)**  
**PROJECT NO. ACNHF-HPD-1570(001)**  
**SECTION 60-16**  
**C-98-121-01**  
**MADISON COUNTY**

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- 3 COMMITMENTS ~~DELETED~~
- 4 SCHEMATIC DIAGRAM
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- 214 PIPE UNDERDRAIN DETAILS
- 215 RAMP A AND RAMP B DETAILS
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- 217 RAMP B ENTRANCE TERMINAL
- 218-220 SHEAR LINE DETAILS
- 221-568 MAINLINE CROSS SECTIONS
- 569-662 SIDEROAD CROSS SECTIONS
- XXX ~~DEMOLITION PLANS~~  
(TO BE PROVIDED & INSERTED BY THE DISTRICT)

END SECTION 60-16  
STATION 225+00.00

STATION EQUATION  
STA. 47+03.34(RAMP B)BK=  
STA. 122+00.00(FAP 310)AH

STATION EQUATION  
STA. 47+89.46 (U.S. 67)BK=  
STA. 0+00.00 (RAMP B)AH

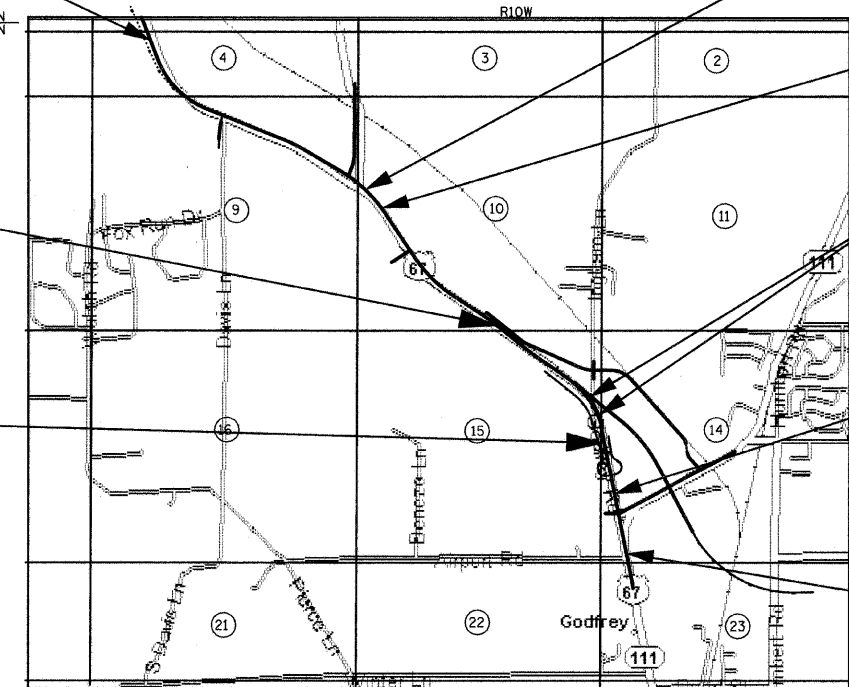
OMISSION No. 3  
SECTION 60-16-1B  
STATION 165+05.98 TO  
STATION 166+38.14

OMISSION No. 2  
SECTION 60-16B  
STATION 159+64.94 TO  
STATION 160+83.17

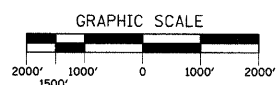
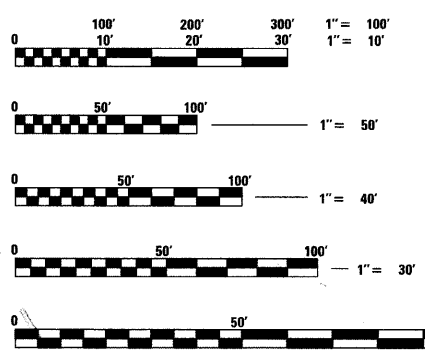
OMISSION No. 1  
SECTION 60-15HB-3  
STATION 15+33.00 TO  
STATION 21+64.00

STATION EQUATION  
STA. 177+10.39 (U.S. 67)BK=  
STA. 45+00.00 (U.S. 67)AH

BEGIN SECTION 60-16  
STATION 165+45.00



LOCATION MAP SCALE: 1" = 2000'  
GODFREY TOWNSHIP



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

**CONTRACT NO. 76311**

### DESIGN DESIGNATION

FAP 310 - 3350 (24) RURAL MAJOR, 7.11 (FD-20)

ADT = 15, 525 (2004)  
ADT = 17, 300 (2005)  
ADT = 21, 035 (2014)  
PU = 87%  
Su = 6%  
Mu = 7%

GROSS LENGTH SECTION 60-16 = 16,458.19 FEET = 3.117 MILES  
OMISSION No. 1 = 631.00 FEET = 0.120 MILES  
OMISSION No. 2 = 118.23 FEET = 0.022 MILES  
OMISSION No. 3 = 132.16 FEET = 0.025 MILES  
NET LENGTH SECTION 60-16 = 15,576.80 FEET = 2.950 MILES



*Mark A. Reitz*  
MARK A. REITZ P.E.#062-047531 DATE 9-7-10-09  
HURST-ROSCH ENGINEERS, INC.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED Dec 10 20 09  
My Crami  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 7 20 10  
Scott E. Stitt, P.E.  
acting ENGINEER OF DESIGN AND ENVIRONMENT

May 7 20 10  
Christine M. Reed  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

PROJECT ENGINEER: FOM-MANNING-1069-040-0485 PATTI LEBEAU 618 396-3179  
SQUAD LEADER: WENDA GOUTHERLAND (618) 346-0492 CHERYL KEPLAR 618 346-3186

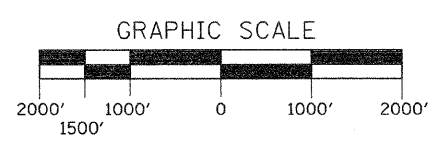


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	4
STA.		TO STA.		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				



END CONST  
SECTION 60-16  
STA 229+00.00

BEGIN CONST  
SECTION 60-16  
STA 165+45.00



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

## SCHEMATIC DIAGRAM

SCALE: NO SCALE  
DATE: 09-04-09

DRAWN BY: U.J.  
CHECKED BY: J.J.C.

ILLINOIS DEPARTMENT OF TRANSPORTATION			URBAN	CONSTRUCTION TYPE CODE						
SUMMARY OF QUANTITIES				TOTAL QUANTITIES	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 15% STATE 5% CITY		
CODE NO	ITEM	UNIT		I000-2A	I000-2A	I000-2A	Y031-F			
X0326854	PIPE UNDERDRAINS CROSS ROAD	FOOT	208			208				
63800024	GLARE SCREEN BLADES 24"	EACH	794	794						
X0326296	TEMPORARY STORM SEWER 24"	FOOT	288	288						
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	1726	1671	55					
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	2943	2698	149	96				
20100500	TREE REMOVAL, ACRES	ACRE	16.5	16.5						
20200100	EARTH EXCAVATION	CU YD	750411	725079	15529	9803				
20800150	TRENCH BACKFILL	CU YD	2864	1600	1096	168				
20900110	POROUS GRANULAR BACKFILL	CU YD	16			16				
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	560	560						
25000200	SEEDING, CLASS 2	ACRE	61.25	57.25	2.5	1.5				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	5512	5152	225	135				
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	5512	5152	225	135				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	5512	5152	225	135				
25100115	MULCH, METHOD 2	ACRE	32	29	2	1				
25100630	EROSION CONTROL BLANKET	SQ YD	142427	136501	3073	2853				
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	12250	11450	500	300				
28000305	TEMPORARY DITCH CHECKS	FOOT	8886	8604	184	93				
28000400	PERIMETER EROSION BARRIER	FOOT	15896	15896						
28000500	INLET AND PIPE PROTECTION	EACH	147	118	23	6				
28100107	STONE RIPRAP, CLASS A4	SQ YD	592	578		14				
28200200	FILTER FABRIC	SQ YD	592	578		14				
30200650	PROCESSING MODIFIED SOIL 12"	SQ YD	140076	140076						
30201500	LIME	TON	2550	2550						
30201800	SLAG-MODIFIED PORTLAND CEMENT	TON	149	149						
31100910	SUB-BASE GRANULAR MATERIAL, TYPE A 12"	SQ YD	45312	32393	8959	3960				
31101810	SUB-BASE GRANULAR MATERIAL, TYPE B 12"	SQ YD	10510	10510						
31101900	SUB-BASE GRANULAR MATERIAL, TYPE C	TON	20518	19117	216	1185				
35100100	AGGREGATE BASE COURSE, TYPE A	TON	6413	6354	59					
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	2746	2746						
40200100	AGGREGATE SURFACE COURSE, TYPE A	TON	2146	2146						
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	203.7	158.6	32.6	12.5				
40600300	AGGREGATE (PRIME COAT)	TON	131	91	29	11				
40600645	LEVELING BINDER (MACHINE METHOD), N90	TON	2643	11	2238	394				
40600895	CONSTRUCTING TEST STRIP	EACH	3	3						
40600990	TEMPORARY RAMP	SQ YD	36		36					
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	2963	2963						

PLOT DATE = #DATE#  
FILE NAME = #FILE#  
PLOT SCALE = #SCALE#  
REFERENCE = #REF#

Rev.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	6
STA.		TO STA.		
FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT		
CONTRACT NO. 76311				

ILLINOIS DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY	ROADWAY	ROADWAY	ROADWAY				
				80% FEDERAL 20% STATE	80% FEDERAL 20% STATE	80% FEDERAL 20% STATE	80% FEDERAL 15% STATE 5% CITY				
				I000-2A	I000-2A	I000-2A	Y031-F				
40603090	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	TON	7766	134	5422	2210					
40603315	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70	TON	575	575							
40603345	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	TON	1841	16	1402	423					
40701871	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9 1/2"	SQ YD	37738	37738							
40701966	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 14 1/4"	SQ YD	6560	6560							
40701996	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15 3/4"	SQ YD	80011	80011							
40800020	BITUMINOUS MATERIALS (PRIME COAT)	TON	13.7	13.4	0.3						
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	1276	1254	22						
42001165	BRIDGE APPROACH PAVEMENT	SQ YD	220	220							
42001300	PROTECTIVE COAT	SQ YD	8370	5820	1220	1330					
42001430	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	SQ YD	723	723							
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	2136	1910	226						
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	706	706							
44000100	PAVEMENT REMOVAL	SQ YD	50608	48747	1712	149					
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	3351		2052	1299					
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	4099	2667	1432						
44000400	GUTTER REMOVAL	FOOT	14667	12543	2124						
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1350			1350					
44003100	MEDIAN REMOVAL	SQ FT	3375			3375					
44201777	CLASS D PATCHES, TYPE II, 11 INCH	SQ YD	175	84	13	78					
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	8266		8266						
48101200	AGGREGATE SHOULDERS, TYPE B	TON	15978	15255	333	390					
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	47433	46343	664	426					
50100300	REMOVAL OF EXISTING STRUCTURES NO. 1	EACH	1		1						
54001003	BOX CULVERT END SECTION, CULVERT NO.3	EACH	2	2							
54001026	BOX CULVERT END SECTION, CULVERT NO.26	EACH	2	2							
54001034	BOX CULVERT END SECTION, CULVERT NO.34	EACH	2	2							
54010504	PRECAST CONCRETE BOX CULVERT 5' X 4'	FOOT	322	322							
54010603	PRECAST CONCRETE BOX CULVERT 6' X 3'	FOOT	42	42							
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	5	1	3	1					

PLOT DATE = #DATE#  
 FILE NAME = #FILE#  
 PLOT SCALE = #SCALE#  
 REFERENCE = #REF#

Rev.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	7
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 76311				

ILLINOIS DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 15% STATE 5% CITY			
				I000-2A	I000-2A	I000-2A	Y031-F			
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	20	20						
54213666	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 21"	EACH	1	1						
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	29	26	2	1				
54213675	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 30"	EACH	5	5						
54213681	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 36"	EACH	2	2						
54214509	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 24"	EACH	1	1						
54214521	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 36"	EACH	3	1		2				
54216195	REINFORCED CONCRETE PIPE TEE, 21" PIPE WITH 12" RISER	EACH	2		2					
54217680	REINFORCED CONCRETE PIPE TEE, 24" PIPE WITH 24" RISER	EACH	1	1						
5421D012	PIPE CULVERTS, CLASS D, TYPE 1 12" (TEMPORARY)	FOOT	452	452						
5421D015	PIPE CULVERTS, CLASS D, TYPE 1 15" (TEMPORARY)	FOOT	512	512						
5421D018	PIPE CULVERTS, CLASS D, TYPE 1 18" (TEMPORARY)	FOOT	38	38						
5421D024	PIPE CULVERTS, CLASS D, TYPE 1 24" (TEMPORARY)	FOOT	136	136						
5422D012	PIPE CULVERTS, CLASS D, TYPE 2 12" (TEMPORARY)	FOOT	42	42						
5422D024	PIPE CULVERTS, CLASS D, TYPE 2 24" (TEMPORARY)	FOOT	7	7						
54244405	FLUSH INLET BOX FOR MEDIAN, STANDARD 542546	EACH	12	12						
54244505	FLUSH INLET BOX FOR MEDIAN, STANDARD 542546, SPECIAL	EACH	6	6						
54244805	INLET BOX, STANDARD 542501	EACH	9	9						
542A0223	PIPE CULVERTS, CLASS A, TYPE 1 18"	FOOT	385	385						
542A0229	PIPE CULVERTS, CLASS A, TYPE 1 24"	FOOT	450	326	124					
542A0235	PIPE CULVERTS, CLASS A, TYPE 1 30"	FOOT	70	70						
542A0241	PIPE CULVERTS, CLASS A, TYPE 1 36"	FOOT	46	46						
542A1069	PIPE CULVERTS, CLASS A, TYPE 2 24"	FOOT	1210	1210						
542A5491	PIPE CULVERTS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 36"	FOOT	62	62						
542A8221	PIPE CULVERTS, CLASS A, TYPE 2 EQUIVALENT ROUND-SIZE 36"	FOOT	100			100				
542D0217	PIPE CULVERTS, CLASS D, TYPE 1 12"	FOOT	1406	1406						
542D0220	PIPE CULVERTS, CLASS D, TYPE 1 15"	FOOT	100	100						

PLOT DATE = 12/15/11  
 PLOT NAME = 12/15/11  
 PLOT SCALE = AS SHOWN  
 REFERENCE = #REF#

ILLINOIS DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 15% STATE 5% CITY			
			QUANTITIES	I000-2A	I000-2A	I000-2A	Y031-F			
542D0223	PIPE CULVERTS, CLASS D, TYPE 1 18"	FOOT	132	132						
542D0229	PIPE CULVERTS, CLASS D, TYPE 1 24"	FOOT	1022	1022						
542D0235	PIPE CULVERTS, CLASS D, TYPE 1 30"	FOOT	246	246						
542D0241	PIPE CULVERTS, CLASS D, TYPE 1 36"	FOOT	46	46						
542D1057	PIPE CULVERTS, CLASS D, TYPE 2 12"	FOOT	182	182						
542D1069	PIPE CULVERTS, CLASS D, TYPE 2 24"	FOOT	156	156						
550A4100	STORM SEWERS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 24"	FOOT	58	58						
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	1033	572	385	76				
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	408	155	253					
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	781	720	61					
550A0110	STORM SEWERS, CLASS A, TYPE 1 21"	FOOT	821	44	777					
550A0120	STORM SEWERS, CLASS A, TYPE 1 24"	FOOT	314	52	46	216				
550A0140	STORM SEWERS, CLASS A, TYPE 1 30"	FOOT	199	199						
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	68	68						
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	212	212						
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	492	492						
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	696	696						
550A0430	STORM SEWERS, CLASS A, TYPE 2 30"	FOOT	273	273						
60100060	CONCRETE HEADWALL FOR PIPE DRAINS	EACH	102	94	2	6				
60107600	PIPE UNDERDRAINS 4"	FOOT	46636	44308	1366	962				
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	1493	1433	30	30				
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1		1					
60219000	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	2		2					
60219100	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 9 FRAME AND GRATE	EACH	2	2						
60219500	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 15 FRAME AND LID	EACH	1	1						
60219510	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	1		1					
60221000	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1						
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	5	2	3					
60221200	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 3 FRAME AND GRATE	EACH	2		2					
60221700	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 8 GRATE	EACH	1		1					
60221800	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 9 FRAME AND GRATE	EACH	5	5						

ILLINOIS DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 15% STATE 5% CITY				
				I000-2A	I000-2A	I000-2A	Y031-F				
60222200	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 15 FRAME AND LID	EACH	2	2							
60222210	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	3		1	2					
60223100	MANHOLES, TYPE A, SPECIAL, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1							
60235700	INLETS, TYPE A, TYPE 3 FRAME AND GRATE	EACH	2	1		1					
60236600	INLETS, TYPE A, TYPE 9 FRAME AND GRATE	EACH	1	1							
60237000	INLETS, TYPE A, TYPE 15 FRAME AND LID	EACH	4	4							
60237420	INLETS, TYPE A, TYPE 20 FRAME AND GRATE	EACH	8	1	6	1					
60240220	INLETS, TYPE B, TYPE 3 FRAME AND GRATE	EACH	3	2		1					
60240303	INLETS, TYPE B, TYPE 9 FRAME AND GRATE	EACH	4	4							
60240320	INLETS, TYPE B, TYPE 15 FRAME AND LID	EACH	11	10	1						
60240324	INLETS, TYPE B, TYPE 20 FRAME AND GRATE	EACH	5		5						
60247180	DRAINAGE STRUCTURES, TYPE 3 WITH TWO TYPE 21 FRAMES AND GRATES	EACH	3	3							
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	29.4	25.3		4.1					
60602500	CONCRETE GUTTER, TYPE A	FOOT	1819	1819							
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	5178	154	3710	1314					
60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	FOOT	110.5		110.5						
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	19671	11295		8376					
60900315	TYPE D INLET BOX, STANDARD 609006	EACH	2	2							
* 63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	5012.5	5012.5							
* 63000130	STEEL PLATE BEAM GUARD RAIL, TYPE A (SPECIAL)	FOOT	18	18							
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	10	10							
* 63100070	TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	6	6							
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	14	14							
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	12	12							
63200310	GUARDRAIL REMOVAL	FOOT	1599	279	1320						
63700900	CONCRETE BARRIER BASE	FOOT	1056	1056							
64200105	SHOULDER RUMBLE STRIP	FOOT	47437	47437							
66500105	WOVEN WIRE FENCE, 4'	FOOT	31783	29023	2113	647					
66501400	WOVEN WIRE GATES, 4' X 12' DOUBLE	EACH	7	7							
66501500	WOVEN WIRE GATES, 4' X 14' DOUBLE	EACH	2	2							
66501600	WOVEN WIRE GATES, 4' X 16' DOUBLE	EACH	7	7							
66502000	WOVEN WIRE GATES, 4' X 24' DOUBLE	EACH	15	15							

\*Specialty Items

DATE = #DATE\*  
 PLOT DATE = #PLOT DATE\*  
 PLOT SCALE = #SCALE\*  
 REFERENCE = #REF\*



ILLINOIS DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY	ROADWAY	ROADWAY	ROADWAY				
				80% FEDERAL 20% STATE	80% FEDERAL 20% STATE	80% FEDERAL 20% STATE	80% FEDERAL 15% STATE 5% CITY				
				I000-2A	I000-2A	I000-2A	Y031-F				
66600105	FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS	EACH	179	166	7	6					
66700205	PERMANENT SURVEY MARKERS, TYPE I	EACH	55	55							
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	24	24							
67000600	ENGINEER'S FIELD LABORATORY	CAL MO	24	24							
67100100	MOBILIZATION	L SUM	1	1							
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1							
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1							
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1							
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1							
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	548	548							
70104490	TRAFFIC CONTROL AND PROTECTION (SPECIAL), LOCATION 1	EACH	6	6							
70104495	TRAFFIC CONTROL AND PROTECTION (SPECIAL), LOCATION 2	EACH	1	1							
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	4851	3637	1064	150					
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	307		219	88					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	76966	53758	17588	5620					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	126		86	40					
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	26359	18186	6170	2003					
70400100	TEMPORARY CONCRETE BARRIER	FOOT	2200	2200							
70500100	TEMPORARY STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	1475	1475							
70500615	TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 1	EACH	3	3							
* 72000100	SIGN PANEL - TYPE 1	SQ FT	1217	990	146	81					
* 72000200	SIGN PANEL - TYPE 2	SQ FT	363	235	72	18	38				
* 72000300	SIGN PANEL - TYPE 3	SQ FT	70	70							
* 73000100	WOOD SIGN SUPPORT	FOOT	3070	2574	295	201					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	898	584	222	92					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	105302	92376	10212	2714					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	2163	1638	75	450					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1975	745	755	475					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	271	145	84	42					

\* Specialty Items

PLOT DATE = #DATE#  
 PLOT SCALE = #SCALE#  
 REFERENCE = #REF#

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	8C
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 76311				

ILLINOIS DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 20% STATE	ROADWAY 80% FEDERAL 15% STATE 5% CITY			
				I000-2A	I000-2A	I000-2A	Y031-F			
78008200	POLYUREA PAVEMENT MARKING TYPE I - LETTERS AND SYMBOLS	SQ FT	16	16						
78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	2461	2461						
78008240	POLYUREA PAVEMENT MARKING TYPE I - LINE 8"	FOOT	168	168						
* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	18	18						
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	585	420	148	17				
78200300	PRISMATIC CURB REFLECTOR	EACH	17			17				
78200410	GUARDRAIL MARKERS, TYPE A	EACH	81	81						
78200520	BARRIER WALL MARKERS, TYPE B	EACH	31	31						
78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	12	12						
78300100	PAVEMENT MARKING REMOVAL	SQ FT	1590	405	958	227				
80500100	SERVICE INSTALLATION, TYPE A	EACH	1			1				
81012300	CONDUIT IN TRENCH, 1" DIA., PVC	FOOT	190			190				
81012500	CONDUIT IN TRENCH, 1 1/2" DIA., PVC	FOOT	680			680				
81012600	CONDUIT IN TRENCH, 2" DIA., PVC	FOOT	115			115				
81012700	CONDUIT IN TRENCH, 2 1/2" DIA., PVC	FOOT	145			145				
81012900	CONDUIT IN TRENCH, 3 1/2" DIA., PVC	FOOT	10			10				
81013000	CONDUIT IN TRENCH, 4" DIA., PVC	FOOT	20			20				
81018400	CONDUIT PUSHED, 1 1/2" DIA., GALVANIZED STEEL	FOOT	25			25				
81018600	CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT	170			170				
81018800	CONDUIT PUSHED, 3 1/2" DIA., GALVANIZED STEEL	FOOT	75			75				
81400700	HANDHOLE, PORTLAND CEMENT CONCRETE	EACH	8			8				
* 81400720	DOUBLE HANDHOLE, PORTLAND CEMENT CONCRETE	EACH	1			1				
81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	975			975				
81900205	TRENCH AND BACKFILL FOR ELECTRICAL WORK (SPECIAL)	FOOT	10			10				
85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1			1				
85706000	INTERSECTION MONITOR UNIT	EACH	1			1				
87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1745			1745				
87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	685			685				
87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	4180			4180				
87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	35			35				

\*Specialty Items

PLOT DATE = DATE  
 PLOT SCALE = SCALE  
 PLOT SCALE = SCALE  
 REFERENCE = REF

ILLINOIS DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY	ROADWAY	ROADWAY	ROADWAY				
				80% FEDERAL 20% STATE	80% FEDERAL 20% STATE	80% FEDERAL 20% STATE	80% FEDERAL 15% STATE 5% CITY				
				I000-2A	I000-2A	I000-2A	Y031-F				
87502680	TRAFFIC SIGNAL POST, ALUMINUM 14 FT.	EACH	3				3				
87700270	STEEL MAST ARM ASSEMBLY AND POLE, 46 FT.	EACH	1				1				
87702710	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 46 FT.	EACH	1				1				
87800100	CONCRETE FOUNDATION, TYPE A	FOOT	9				9				
87800200	CONCRETE FOUNDATION, TYPE D	FOOT	3				3				
87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	26				26				
88040070	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	3				3				
88040090	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	3				3				
88040160	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	3				3				
88040230	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2				2				
88200100	TRAFFIC SIGNAL BACKPLATE	EACH	6				6				
88500100	INDUCTIVE LOOP DETECTOR	EACH	16				16				
88600100	DETECTOR LOOP, TYPE I	FOOT	2138				2138				
X6063600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	FOOT	1046	1046							
X6064201	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06	FOOT	998	998							
X6370250	CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT	FOOT	1056	1056							
X7800200	PAINT PAVEMENT MARKING CURB	FOOT	2555	2555							
X6020182	DRAINAGE STRUCTURE SPECIAL	L SUM	1	1							
XX006648	TEMPORARY STORM SEWER 12"	FOOT	520	520							
XX006983	CLEARING	ACRE	7.5	6.8	0.5	0.2					
Z0022800	FENCE REMOVAL	FOOT	6651	6651							
Z0027800	GEOTECHNICAL FABRIC	SQ YD	24		24						
Z0030070	IMPACT ATTENUATORS (SEVERE USE, NARROW), TEST LEVEL 3	EACH	1				1				
Z0030260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	4				4				
① Z0076600	TRAINEES	HOUR	4,000	4,000							

① 4080

\*Specialty Items

Rev.

PLAT DATE = #DATE  
FILE NAME = #FILE#  
PLAT SCALE = #SCALE#  
REFERENCE = #REF#

MIXTURE REQUIREMENTS

The following mixture requirements are applicable for this project.

Location US 67/ IL111 (Godfrey Road), IL 111/ US 67 (Godfrey Road)

20 YR. ESAL'S: 4.14

MIXTURE USE	SURFACE	BINDER	SHOULDER	PATCHING	TEMP RAMP
AC/PG	PG 64-22	PG 64-22	PG 64-22	PG 64-22	PG 64-22
RAP%(MAX)	10%	10%	30%	10%	10%
DESIGN AIR Voids	4% @ Ndes=90	4% @ Ndes=90	2% @ Ndes=30	4% @ Ndes=90	4% @ Ndes=90
MIX COMPOSITION (GRADIATION MIXTURE)		IL 19.0		IL 19.0	
FRICITION AGG	MIXTURE D	MIXTURE B	BAM	MIXTURE B	MIXTURE C

Location FAP 310 AND RAMPS A & B

20 YR. ESAL'S: 4.14

MIXTURE USE	POLYMERIZED SURFACE * FULL DEPTH	BINDER * FULL DEPTH	SHOULDERS	TOP LIFT SHOULDER
AC/PG	SBS PG 76-22	PG 64-22	PG 64-22	PG 64-22
RAP%(MAX)	10%	10%	30%	30%
DESIGN AIR Voids	4.0% @ Ndes=90	4.0% @ Ndes=90	2.0% @ Ndes=30	1.5% @ Ndes=30
MIX COMPOSITION (GRADIATION MIXTURE)		IL 19.0		
FRICITION AGG	MIXTURE D	MIXTURE B	BAM	BAM

\* SEE TYPICAL SECTION FOR FULL-DEPTH DETAILS

Location ACCESS ROADS 1, 2, & 3

20 YR. ESAL'S: 0.01

MIXTURE USE	SURFACE	BINDER	SHOULDER	INCIDENTAL
AC/PG	PG 64-22	PG 64-22	PG 64-22	PG 64-22
RAP%(MAX)	10%	15%	30%	10%
DESIGN AIR Voids	4% @ Ndes=70	4% @ Ndes=70	2% @ Ndes=30	4% @ Ndes=70
MIX COMPOSITION (GRADIATION MIXTURE)		IL 19.0		
FRICITION AGG	MIXTURE C	MIXTURE B	BAM	MIXTURE C

Location LAGEMANN LANE, BUETTEL ROAD, DAVIS LANE AND BENJAMIN DRIVE

20 YR. ESAL'S: 0.01

MIXTURE USE	SURFACE	BINDER
AC/PG	PG 64-22	PG 64-22
RAP%(MAX)	10%	15%
DESIGN AIR Voids	4% @ Ndes=70	4% @ Ndes=70
MIX COMPOSITION (GRADIATION MIXTURE)		IL 19.0
FRICITION AGG	MIXTURE C	MIXTURE B

ENTRANCES

20 YR. ESAL'S:

MIXTURE USE	INCIDENTAL
AC/PG	PG 64-22
RAP%(MAX)	10%
DESIGN AIR Voids	4% @ Ndes=70
MIX COMPOSITION (GRADIATION MIXTURE)	
FRICITION AGG	MIXTURE C

REVISIONS	
NAME	DATE

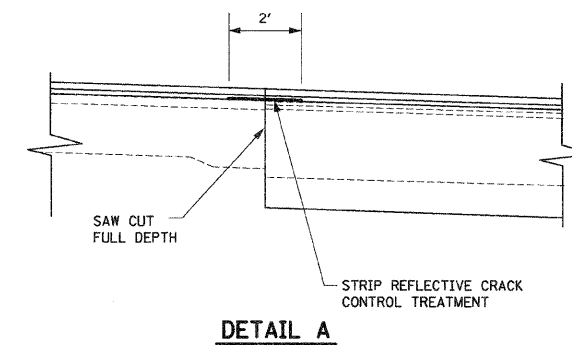
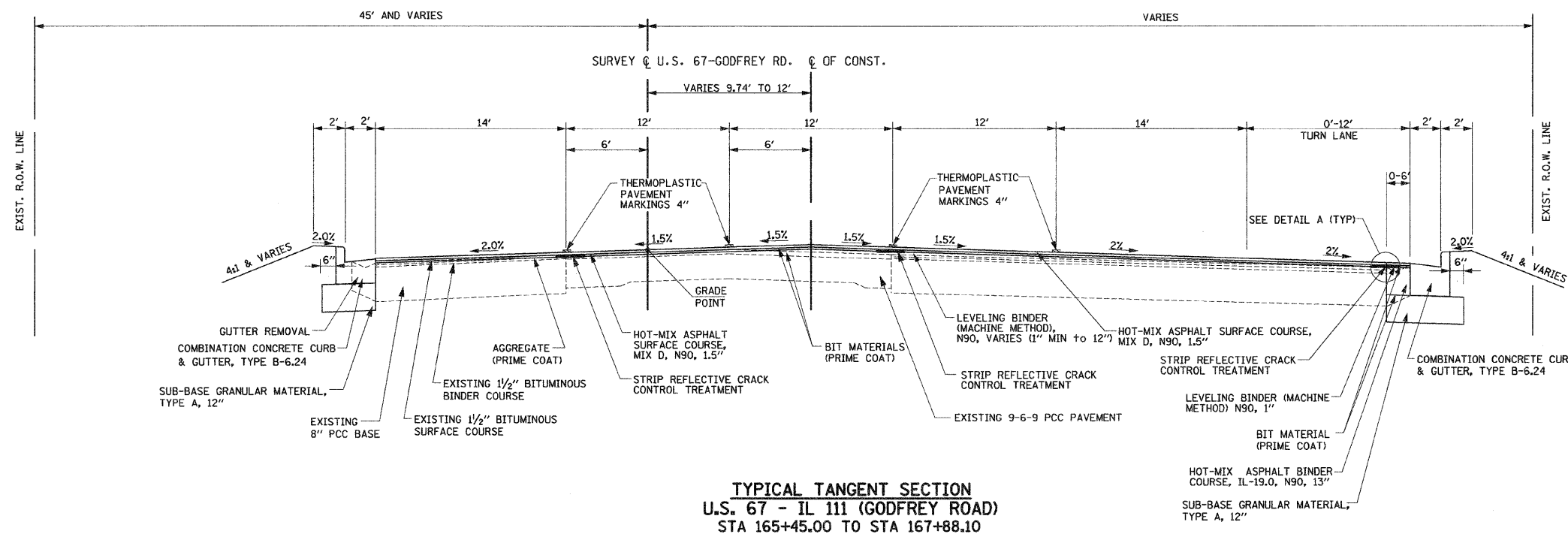
ILLINOIS DEPARTMENT OF TRANSPORTATION

# MIXTURE REQUIREMENTS

SCALE: NO SCALE  
DATE: 09-04-09

DRAWN BY: U.J.J.  
CHECKED BY: J.J.C.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	10
STA.		TO STA.		
FED. ROAD DIST. NO. 8		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 76311				



\*\* PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 FROM STA 167+40.00 TO 167+88.10

STRUCTURAL DESIGN DATA  
U.S. 67 - IL 111 (GODFREY ROAD)

STRUCTURAL DESIGN TRAFFIC:	YEAR	2014
PV=	24521	SU= 626 MU= 939
ROAD/STREET CLASSIFICATION:	CLASS	I

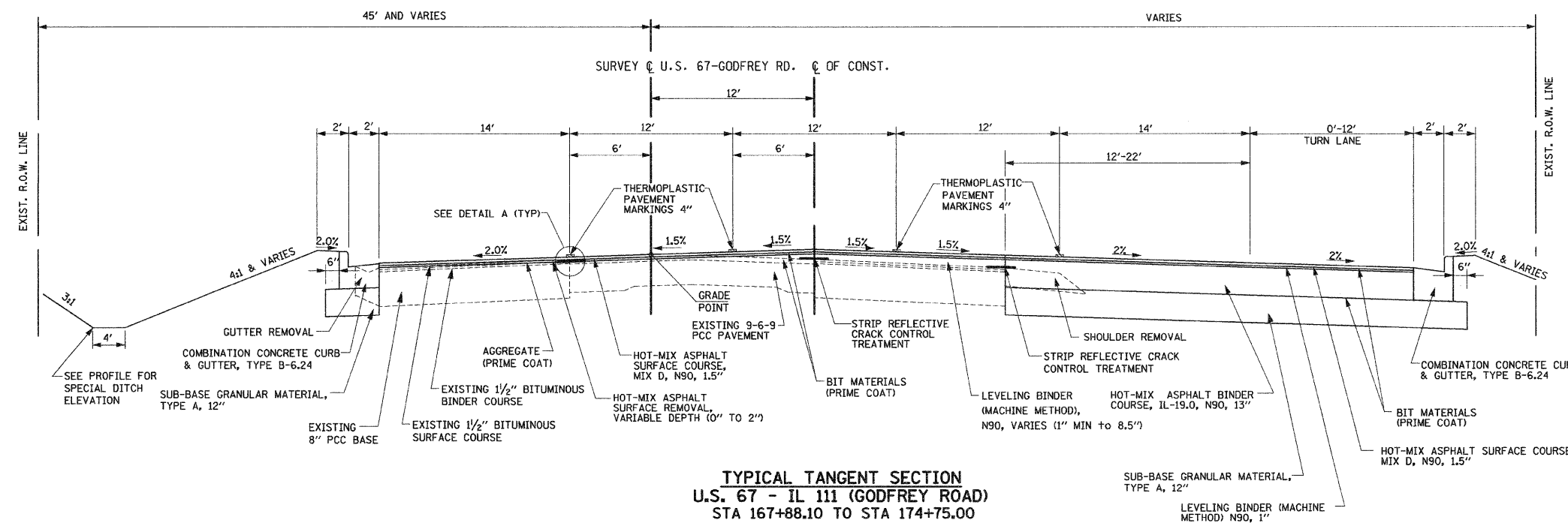
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P=	32	S=	45	M=	45
----	----	----	----	----	----

TRAFFIC FACTOR: ACTUAL TF= 4.85  
MINIMUM TF= 4.27 AC TYPE= AC-20

SUBGRADE SUPPORT RATING:  
SSR= POOR (STA 165+45 TO 167+71.74 )  
SSR= POOR (STA 167+71.74 TO 172+06.10 )

USE: HOT-MIX ASPHALT SURFACE COURSE, 1.5"  
LEVELING BINDER, 1"  
HOT MIX ASPHALT BINDER COURSE, 13"



REVISIONS	
NAME	DATE

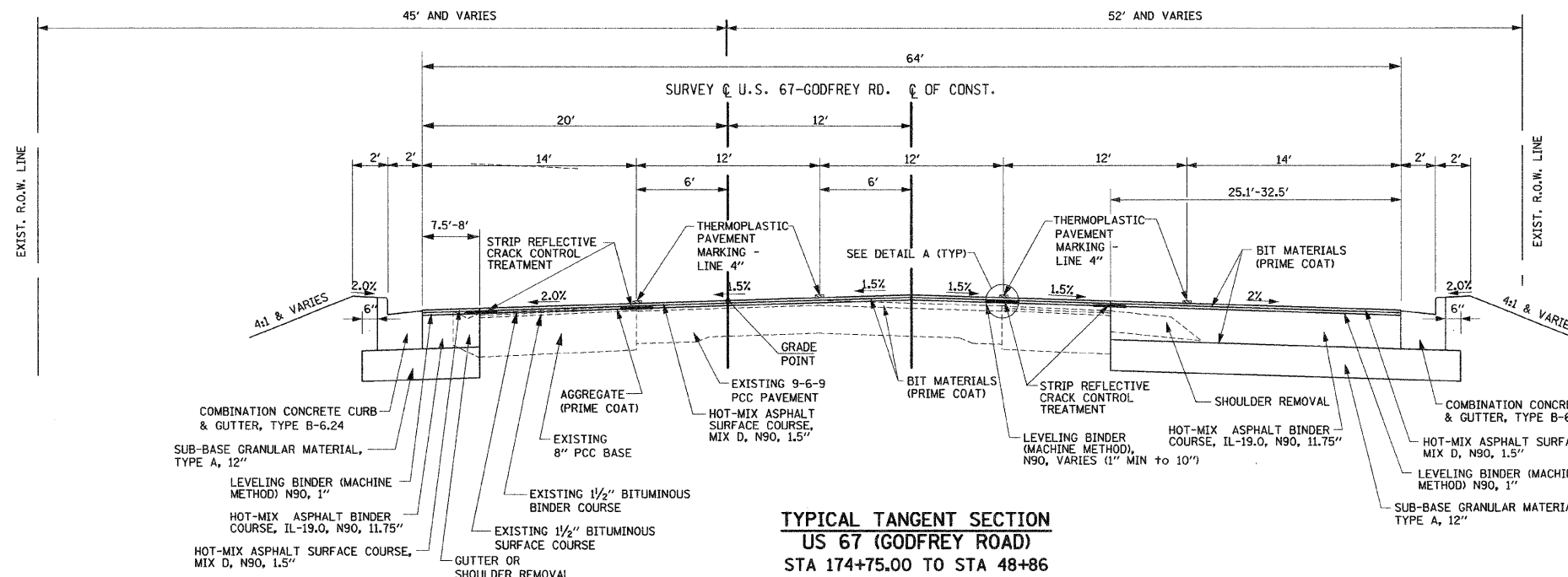
ILLINOIS DEPARTMENT OF TRANSPORTATION

# TYPICAL SECTIONS

SCALE: NO SCALE  
DATE: 09-04-09

DRAWN BY: U.J.  
CHECKED BY: J.J.C.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	11
STA.		TO STA.		
FED. ROAD DIST. NO. 8		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 76311				



STRUCTURAL DESIGN DATA  
US 67 (GODFREY ROAD)

STRUCTURAL DESIGN TRAFFIC: YEAR 2014  
 PV= 13111 SU= 906 MU= 1088

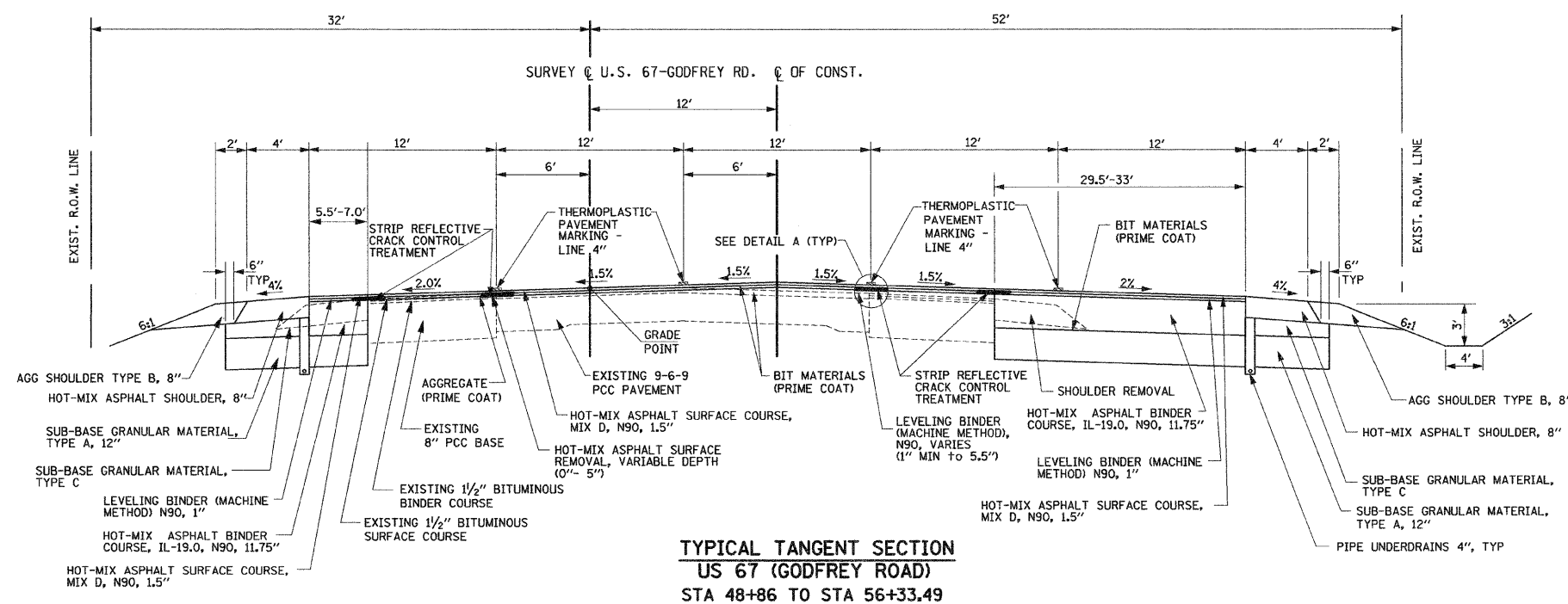
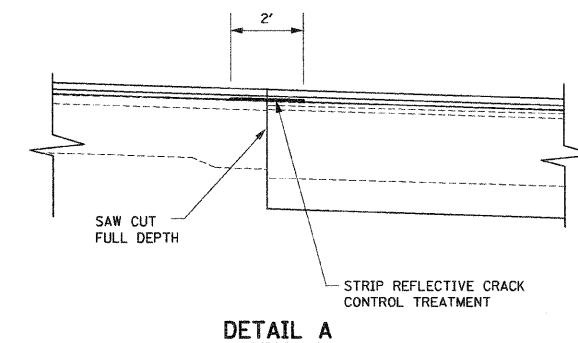
ROAD/STREET CLASSIFICATION: CLASS I

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:  
 P= 32 S= 45 M= 45

TRAFFIC FACTOR: ACTUAL TF= 5.82  
 MINIMUM TF= 4.27 AC TYPE= AC-20

SUBGRADE SUPPORT RATING:  
 SSR= POOR (STA 172+50 TO 48+86 )  
 SSR= POOR (STA 48+86 TO 58+77.67 )

USE: HOT-MIX ASPHALT SURFACE COURSE, 1.5"  
 LEVELING BINDER, 1"  
 HOT MIX ASPHALT BINDER COURSE, 11.75"



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

# TYPICAL SECTIONS

SCALE: NO SCALE  
 DATE: 09-04-09

DRAWN BY: U.J.  
 CHECKED BY: J.J.C.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	12
STA.		TO STA.		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				

**STRUCTURAL DESIGN DATA**  
RAMP A

STRUCTURAL DESIGN TRAFFIC: YEAR 2014  
PV= 6556 SU= 453 MU= 544

ROAD/STREET CLASSIFICATION: CLASS I

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:  
P= 100% S= 100% M= 100%

TRAFFIC FACTOR: ACTUAL TF= 6.47  
MINIMUM TF= 4.74 AC TYPE= 20

SUBGRADE SUPPORT RATING:  
SSR= POOR (STA 2+97.08 TO 19+84.11)

USE: HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 14.25"  
LIME MODIFIED SOIL, 12"

**STRUCTURAL DESIGN DATA**  
RAMP B

STRUCTURAL DESIGN TRAFFIC: YEAR 2014  
PV= 6556 SU= 453 MU= 544

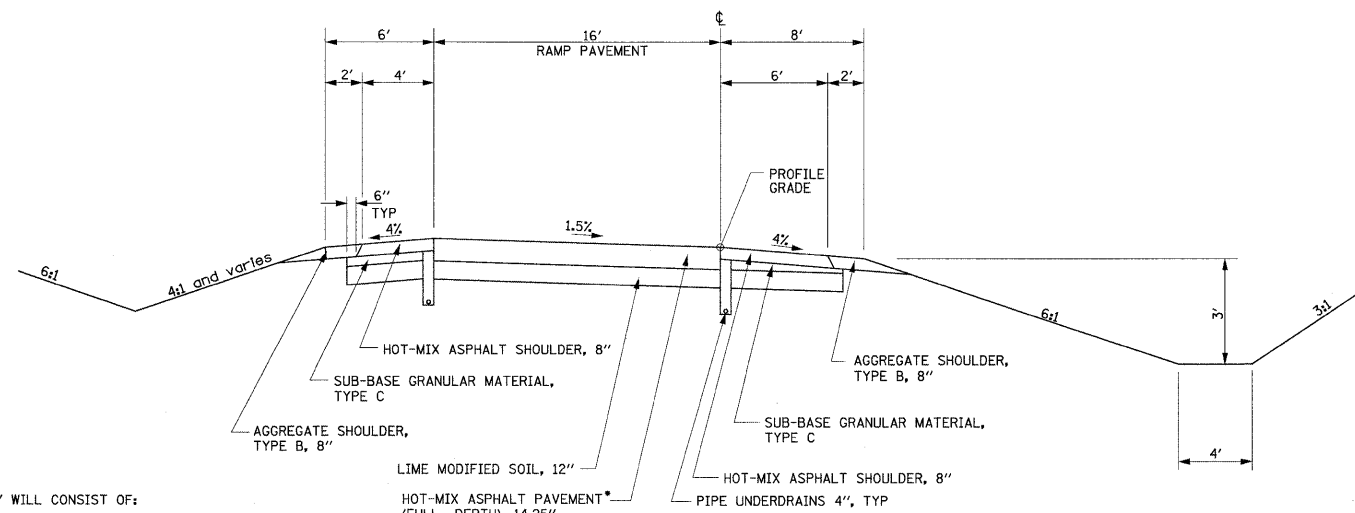
ROAD/STREET CLASSIFICATION: CLASS I

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:  
P= 100% S= 100% M= 100%

TRAFFIC FACTOR: ACTUAL TF= 6.47  
MINIMUM TF= 4.74 AC TYPE= 20

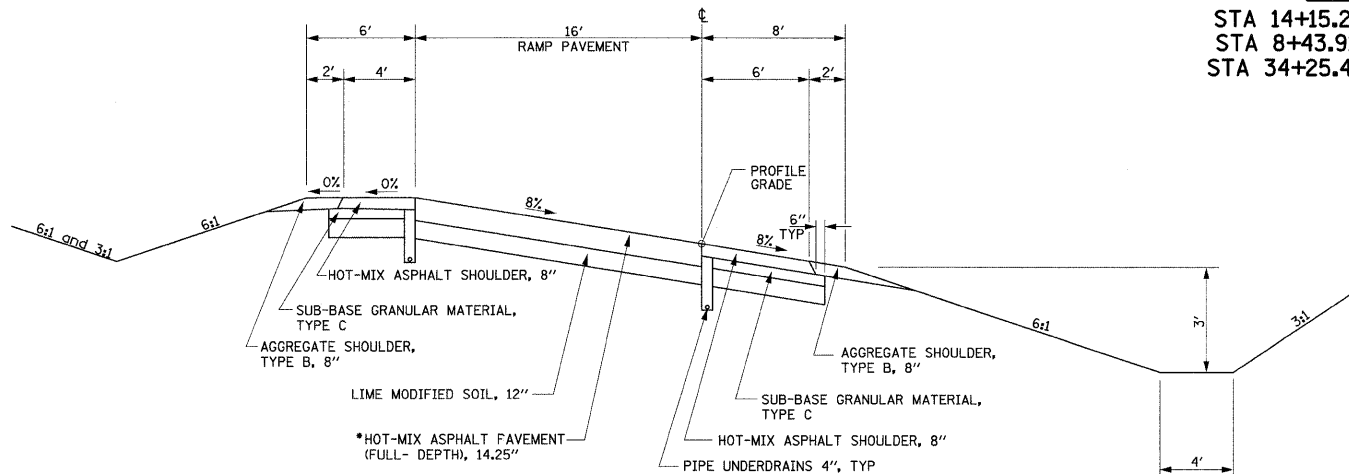
SUBGRADE SUPPORT RATING:  
SSR= POOR (STA 8+43.91 TO 38+54.57)

USE: HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 14.25"  
PROCESSING LIME MODIFIED SOIL 12"



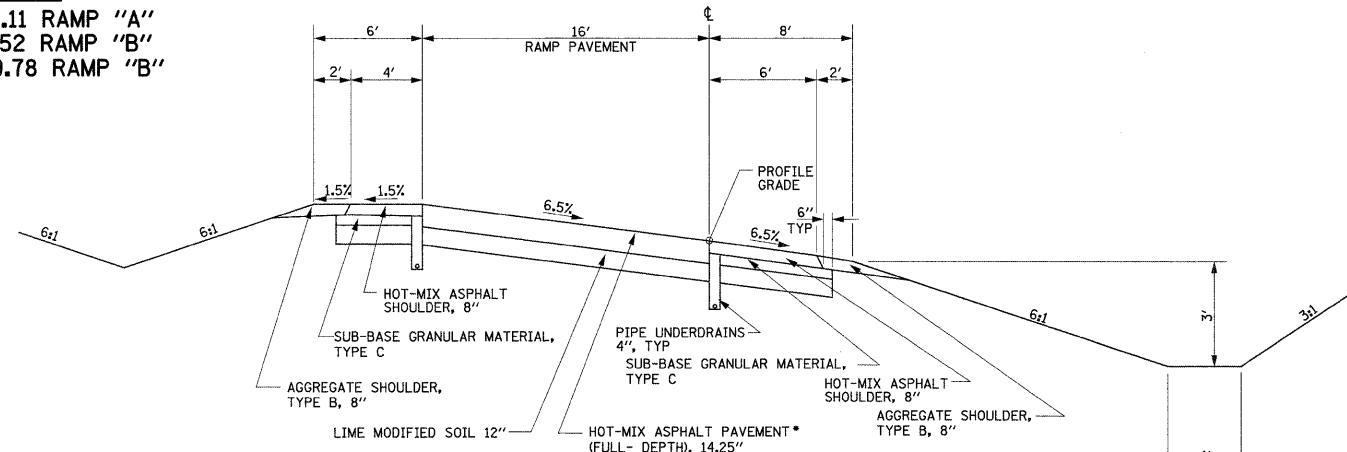
\* HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH) 14.25" WILL CONSIST OF:  
HOT-MIX ASPHALT POLYMER SURFACE COURSE, MIX D, N90, 2"  
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 12.25"

**TYPICAL RAMP SECTION**  
STA 14+15.20 TO STA 19+84.11 RAMP "A"  
STA 8+43.91 TO STA 12+89.52 RAMP "B"  
STA 34+25.49 TO STA 38+59.78 RAMP "B"

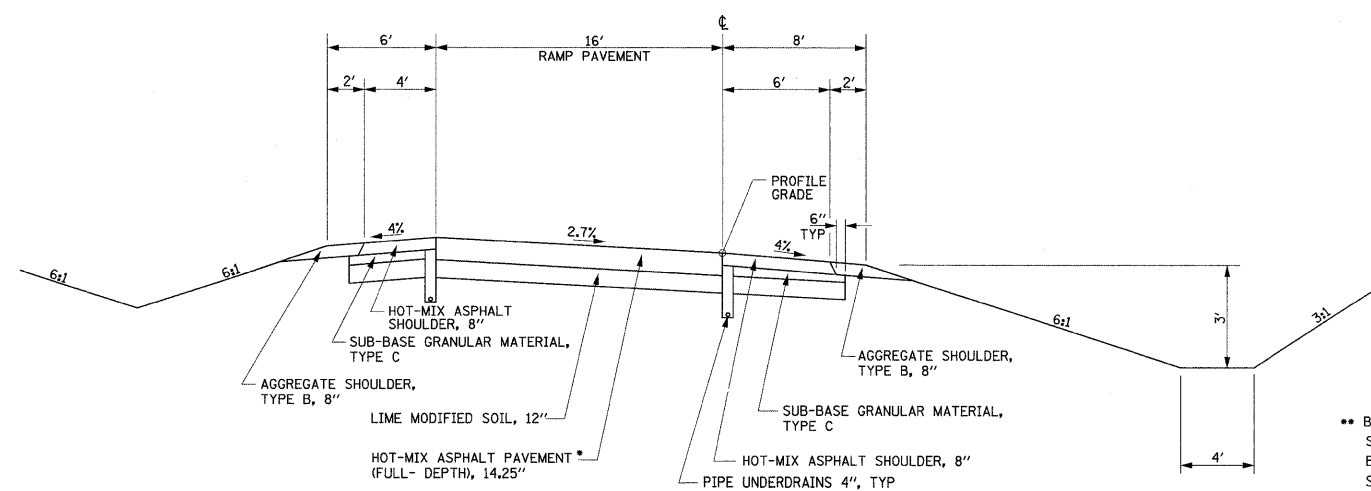


**EXIT RAMP "A"**  
STA 8+08.70 TO STA 14+15.20

\* HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH) 14.25" WILL CONSIST OF:  
HOT-MIX ASPHALT POLYMER SURFACE COURSE, MIX D, N90, 2"  
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 12.25"



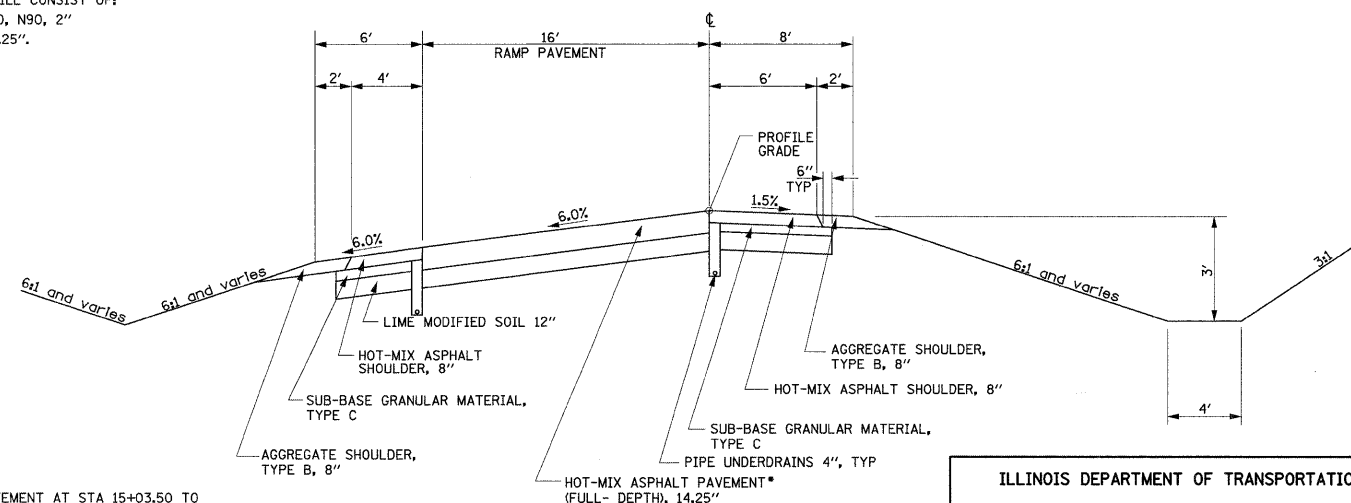
**ENTRANCE RAMP "B"**  
STA 31+08.68 TO STA 34+25.49



**TYPICAL RAMP SECTION**  
STA 2+97.08 TO STA 8+08.70 RAMP "A"

\*\* BRIDGE APPROACH PAVEMENT AT STA 15+33.50 TO STA 15+33.50 AND STA 21+63.45 TO STA 21+93.45  
BRIDGE APPROACH PAVEMENT CONNECTOR AT STA 14+03.50 TO STA 15+03.50 AND STA 21+93.45 TO STA 22+93.45.

\* HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH) 14.25" WILL CONSIST OF:  
HOT-MIX ASPHALT POLYMER SURFACE COURSE, MIX D, N90, 2"  
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 12.25"



**ENTRANCE RAMP "B"**  
STA 12+89.52 TO STA 15+33.50  
STA 21+63.40 TO STA 31+08.68

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

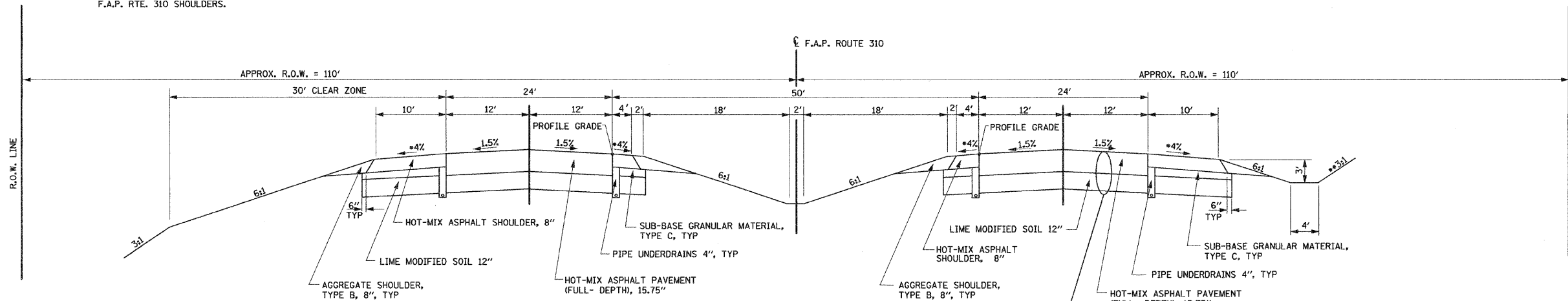
# TYPICAL SECTIONS

SCALE: NO SCALE  
DATE: 09-04-09

DRAWN BY: U.J.  
CHECKED BY: J.J.C.

REVISED 01-12-2010 BLA

NOTE:  
RUMBLE STRIPS SHALL BE PROVIDED ON ALL MAINLINE  
F.A.P. RTE. 310 SHOULDERS.



- END PAVEMENT CONSTRUCTION SOUTHBOUND LANES LT STA 228+25.  
END PAVEMENT CONSTRUCTION NORTHBOUND LANES RT STA 225+00.
- FOR CUTS LESS THAN 10' USE 3:1 BACKSLOPE.  
FOR CUTS 10' TO 15' USE 2:1 BACKSLOPE.  
FOR CUTS 15' TO 30' USE 2:1 BACKSLOPE WITH  
A 13' BENCH AT A 13:1 SLOPE LOCATED 1/2 OF  
THE DEPTH OF CUT.  
FOR CUTS GREATER THAN 30' USE 2:1 BACKSLOPE  
WITH A 13' BENCH LOCATED 1/3 AND 2/3 OF THE  
DEPTH OF CUT.
- SOUTHBOUND LANES BEGIN CONSTRUCTION AT STA 91+50.  
NORTHBOUND LANES BEGIN CONSTRUCTION AT STA 92+50.
- SEE SHEET 19 FOR ADDITIONAL INFORMATION ON TURN LANE SECTIONS.

SEE SHEET 17 FOR  
TURN LANE SECTIONS

**TYPICAL SECTION  
F.A.P. ROUTE 310**

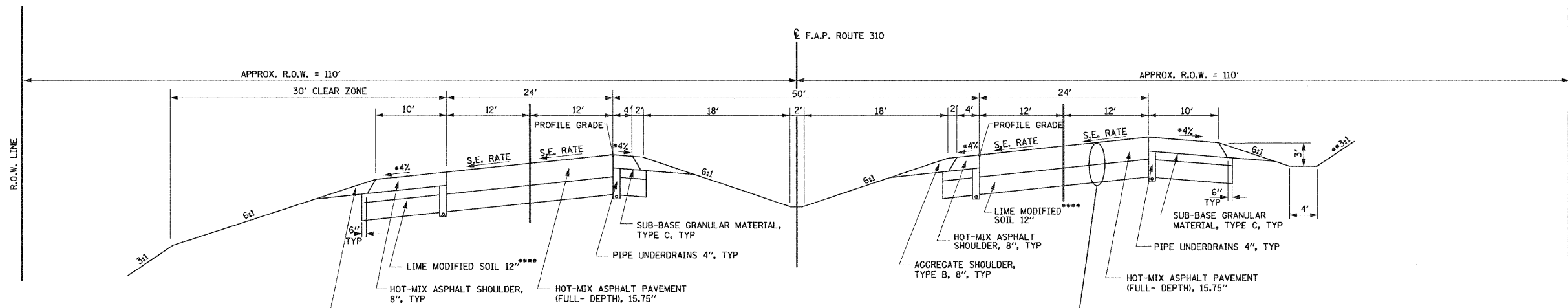
- STA 91+50.00\*\* TO STA 100+28.54
- STA 105+37.40 TO STA 107+41.67
- STA 115+73.63 TO STA 121+55.12
- STA 127+70.65 TO STA 133+14.00
- STA 146+44.95 TO STA 158+11.69\*\*\*\*
- STA 172+63.86 TO STA 182+76.78
- STA 187+68.97 TO STA 206+69.03\*\*\*\*
- STA 223+09.32 TO STA 228+25.00\*

SURFACE	2"
BINDER	13 3/4"
LIME MODIFIED SOIL	12"

STRUCTURAL DESIGN DATA  
F.A.P. ROUTE 310

STRUCTURAL DESIGN TRAFFIC:	YEAR	2014
PV= 15073	SU= 1040	MU= 1213
ROAD/STREET CLASSIFICATION:	CLASS	I
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P= 32	S= 45	M= 45
TRAFFIC FACTOR:	ACTUAL TF=	6.52
MINIMUM TF= 7.11	AC TYPE=	20
SUBGRADE SUPPORT RATING:		
SSR= POOR	(STA 87+00.91 TO 228+25.00 )	

USE: HOT-MIX ASPHALT PAVEMENT,  
(FULL- DEPTH), 15.75"  
LIME MODIFIED SOIL, 12"



- FOR SHOULDER SLOPES SEE STANDARD 482001  
WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS  
BETWEEN 0% AND 4%, THE SHOULDER SHALL BE SLOPED AT 4%.  
WHEN THE SUPERELEVATION RATE OF THE PAVEMENT  
EXCEEDS 4%, THE SHOULDER SHALL BE SLOPED SO THAT THE  
ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDER  
WILL NOT BE GREATER THAN 8%.
- FOR CUTS LESS THAN 10' USE 3:1 BACKSLOPE.  
FOR CUTS 10' TO 15' USE 2:1 BACKSLOPE.  
FOR CUTS 15' TO 30' USE 2:1 BACKSLOPE WITH  
A 13' BENCH AT A 13:1 SLOPE LOCATED 1/2 OF  
THE DEPTH OF CUT.  
FOR CUTS GREATER THAN 30' USE 2:1 BACKSLOPE  
WITH A 13' BENCH LOCATED 1/3 AND 2/3 OF THE  
DEPTH OF CUT.
- SEE SHEET 19 FOR ADDITIONAL INFORMATION ON TURN LANE SECTIONS.
- SLAG-MODIFIED PORTLAND CEMENT TO BE USED IN LIEU OF LIME  
FOR STA 214+00 TO STA 222+00 (COST TO BE INCLUDED IN PROCESSING  
MODIFIED SOIL).

SEE SHEET 17 FOR  
TURN LANE SECTIONS

**SUPERELEVATED TYPICAL SECTION  
F.A.P. ROUTE 310**

- STA 100+28.54 TO STA 105+37.40
- STA 107+41.67 TO STA 115+73.63
- STA 121+55.12 TO STA 127+70.65
- STA 133+14.00 TO STA 146+44.95 \*\*\*
- STA 158+11.69 TO STA 160+06.06 LT
- STA 160+29.93 TO STA 165+43.79 LT \*\*\*
- STA 166+61.56 TO STA 172+63.86 LT \*\*\*
- STA 158+11.69 TO STA 159+60.00 RT
- STA 160+86.04 TO STA 165+08.75 RT \*\*\*
- STA 166+40.84 TO STA 172+63.86 RT \*\*\*
- STA 182+76.78 TO STA 187+68.97
- STA 206+69.03 TO STA 223+09.32 \*\*\*

SURFACE	2"
BINDER	13 3/4"
LIME MODIFIED SOIL	12"

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

# TYPICAL SECTIONS

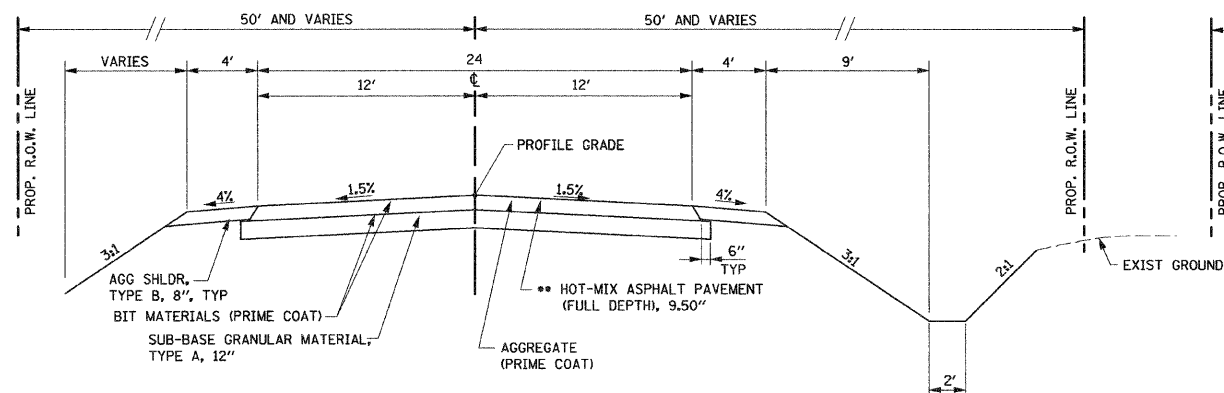
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DATE: 09-04-09

DRAWN BY: U.J.  
CHECKED BY: J.J.C.





F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	15
STA.		TO STA.		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				

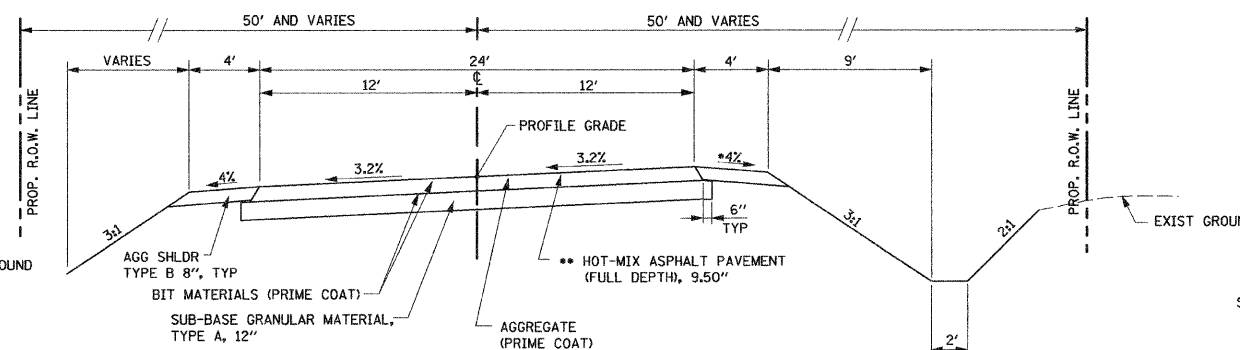


**TANGENT TYPICAL SECTION  
ACCESS ROAD 1**

STA 10+87.05 TO STA 13+47.34  
 STA 14+98.43 TO STA 17+13.63  
 STA 18+64.72 TO STA 36+19.46  
 STA 41+70.13 TO STA 45+41.13  
 STA 48+81.17 TO STA 60+45.36  
 STA 61+54.39 TO STA 64+77.12\*  
 STA 65+38.96 TO STA 73+22.00

TURNAROUND PAVEMENT BEYOND ACCESS ROAD 1 AS SHOWN ON SHEET 78 TO BE HMA (FULL DEPTH) 9.5" & SUB-BASE GRANULAR MATERIAL, TYPE A 12"

\* SEE CONCRETE BARRIER DETAIL FOR STA 63+57 TO STA 73+66 LT

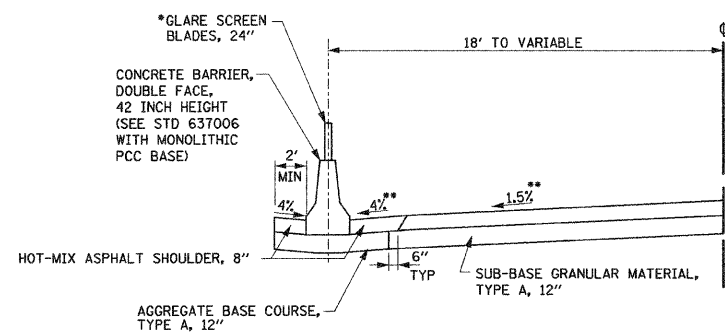


**SUPERELEVATED TYPICAL SECTION  
ACCESS ROAD 1**

STA 13+47.34 TO STA 14+98.43, 2%  
 STA 17+13.63 TO STA 18+64.72, 8%  
 STA 36+19.46 TO STA 41+70.13, 5.7%  
 STA 45+41.13 TO STA 48+81.17, 5%  
 STA 60+45.36 TO STA 61+54.39, 5.7%  
 STA 64+77.12 TO STA 65+38.96, 4.65%

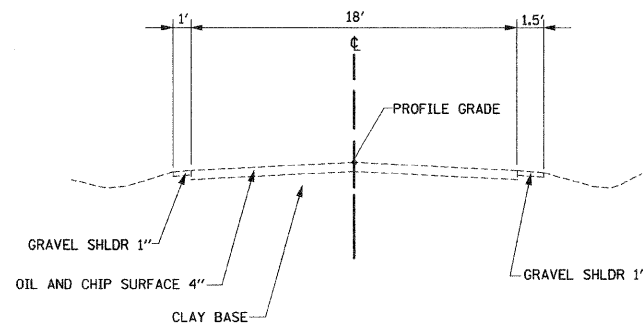
STRUCTURAL DESIGN DATA  
ACCESS ROAD 1, & INGHAM LANE

STRUCTURAL DESIGN TRAFFIC:	YEAR	2014
PV= 343	SU= 35	MU= 12
ROAD/STREET CLASSIFICATION:	CLASS	IV
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P= 50	S= 50	M= 50
TRAFFIC FACTOR:	ACTUAL TF=	0.0132
MINIMUM TF= 0.5	AC TYPE=	20
SUBGRADE SUPPORT RATING:		
SSR= POOR	(STA 10+00.00 TO 73+34.17)	

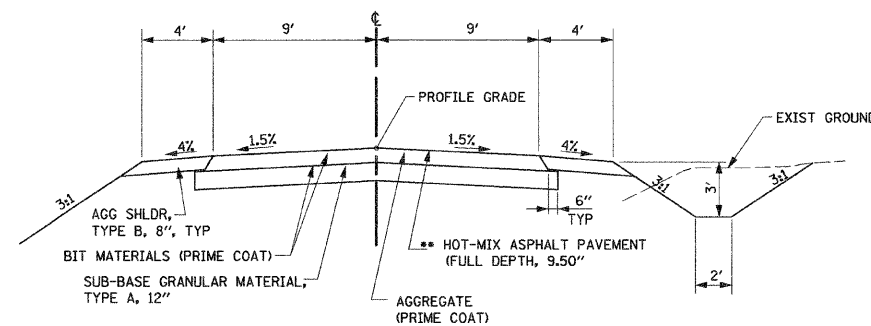


**TYPICAL SECTION WITH CONC BARRIER  
ACCESS ROAD 1**  
STA 63+10.28 TO STA 73+66.00

\* GLARE SCREEN BLADE INSTALLATION ANGLE IS 70°. SPACING IS EVERY 1.33' BASED ON A CUTOFF ANGLE OF 20° (SEE STD 638001)  
 \*\* SEE FAP 310 CROSS SECTIONS FOR VARIED SLOPES



**EXISTING TYPICAL SECTION  
INGHAM LANE**

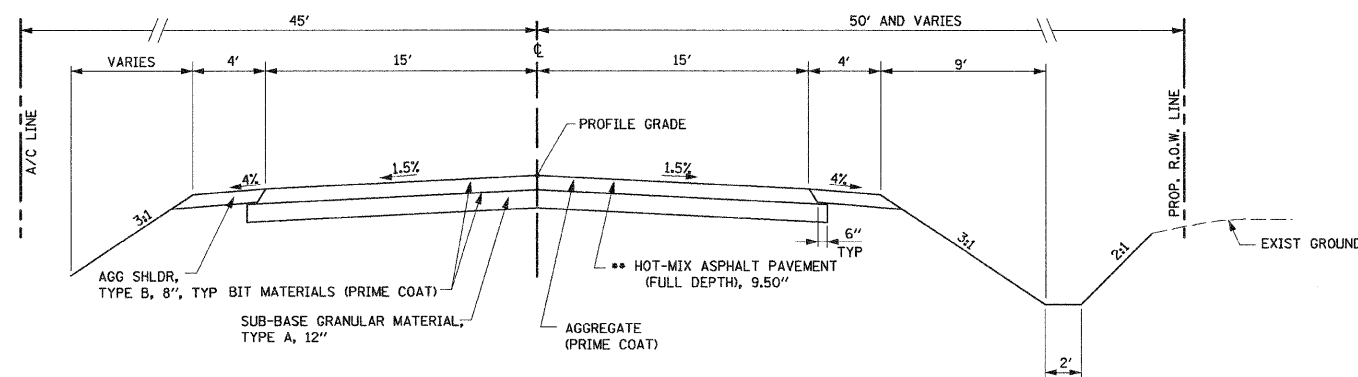


**TYPICAL SECTION  
INGHAM LANE**

STA 8+10.00 TO STA 9+88.00  
 STA 10+12.00 TO STA 11+50.00

STRUCTURAL DESIGN DATA  
ACCESS ROAD 3

STRUCTURAL DESIGN TRAFFIC:	YEAR	2014
PV= 107	SU= 11	MU= 4
ROAD/STREET CLASSIFICATION:	CLASS	IV
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P= 50	S= 50	M= 50
TRAFFIC FACTOR:	ACTUAL TF=	0.0041
MINIMUM TF= 0.7	AC TYPE=	20
SUBGRADE SUPPORT RATING:		
SSR= POOR	(STA 10+42.00 TO 21+48.06)	

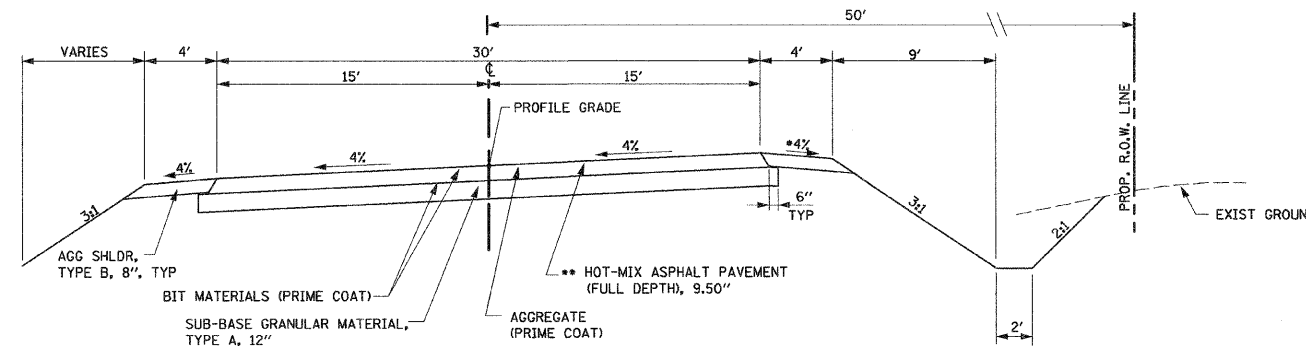


**TANGENT TYPICAL SECTION  
ACCESS ROAD 3**

STA 10+43.00 TO STA 11+14.22  
 STA 15+13.89 TO STA 15+91.15  
 STA 17+38.23 TO STA 21+35.06

TURNAROUND PAVEMENT BEYOND ACCESS ROAD 3 AS SHOWN ON SHEET 82 (BETHANY LANE) TO BE HMA (FULL DEPTH) 9.5" & SUB-BASE GRANULAR MATERIAL, TYPE A 12"

\*\* HOT-MIX ASPHALT PAVEMENT (FULL DEPTH) 9.50" WILL CONSIST OF:  
 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N70, 2" AND  
 HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70, 7.50"



**SUPERELEVATED TYPICAL SECTION  
ACCESS ROAD 3**

STA 11+14.22 TO STA 15+13.89  
 STA 15+91.15 TO STA 17+38.23

\* WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4%, THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDER WILL NOT BE GREATER THAN 8%.

REVISIONS	
NAME	DATE

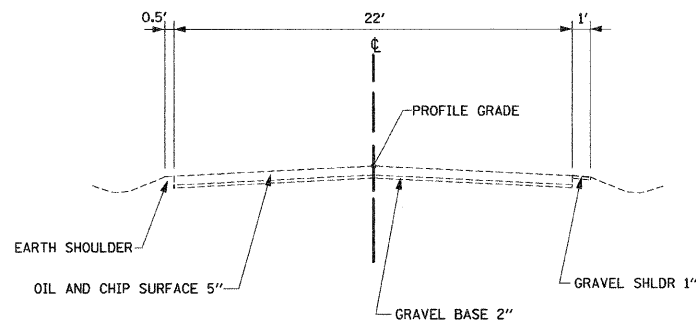
ILLINOIS DEPARTMENT OF TRANSPORTATION

**TYPICAL SECTIONS**

SCALE: NO SCALE  
 DATE: 09-04-09  
 DRAWN BY: U.J.  
 CHECKED BY: J.J.C.

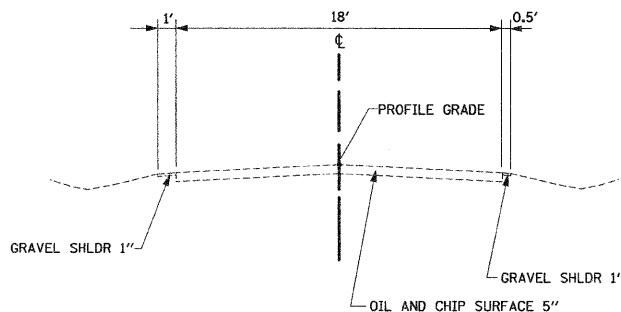
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	16

STA. TO STA.  
 FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT  
 CONTRACT NO. 76311



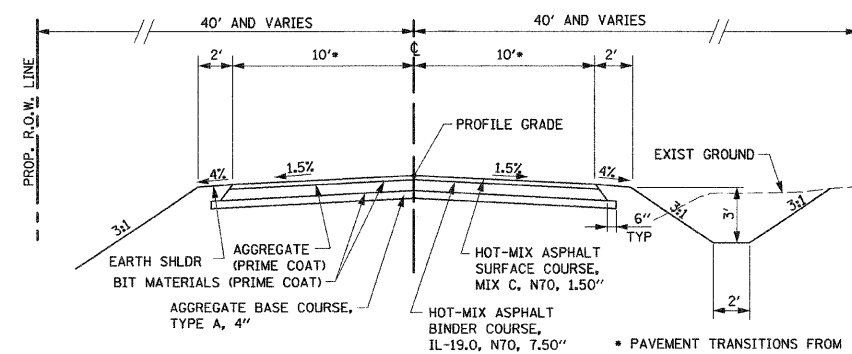
**EXISTING TYPICAL SECTION**

**LAGEMANN ROAD  
(NORTH-SOUTH)**



**EXISTING TYPICAL SECTION**

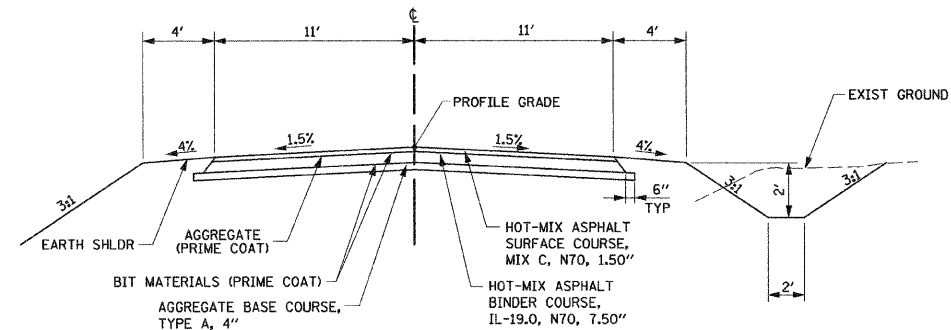
**DAVIS LANE  
(NORTH-SOUTH)**



**TYPICAL AT GRADE CROSSROAD**

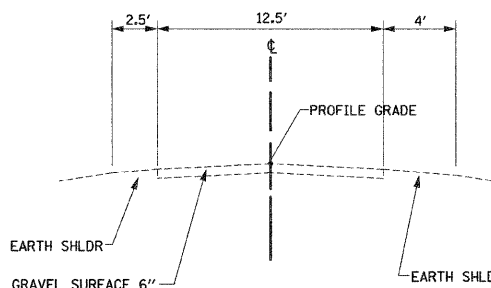
**DAVIS LANE (TR 16)  
STA 2+75.00 TO STA 4+31.60  
STA 8+21.51 TO STA 9+50.24**

**BEUTTEL ROAD  
STA 5+20.00 TO STA 9+51.00**



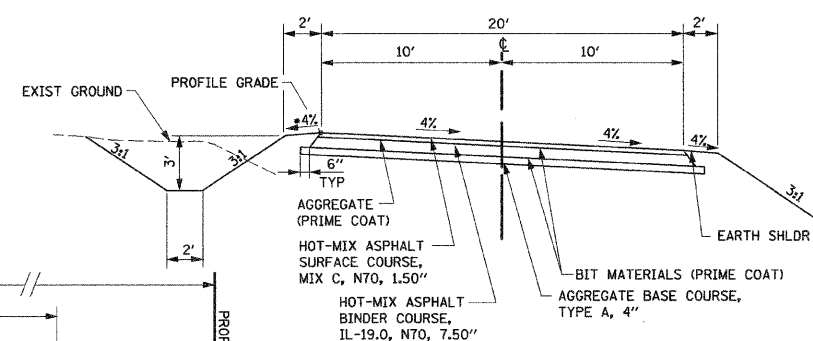
**TANGENT TYPICAL SECTION**

**LAGEMANN ROAD (CH 36)  
STA 10+50.69 TO STA 14+32.48  
STA 18+06.84 TO STA 20+16.31  
STA 23+19.66 TO STA 24+00.00**



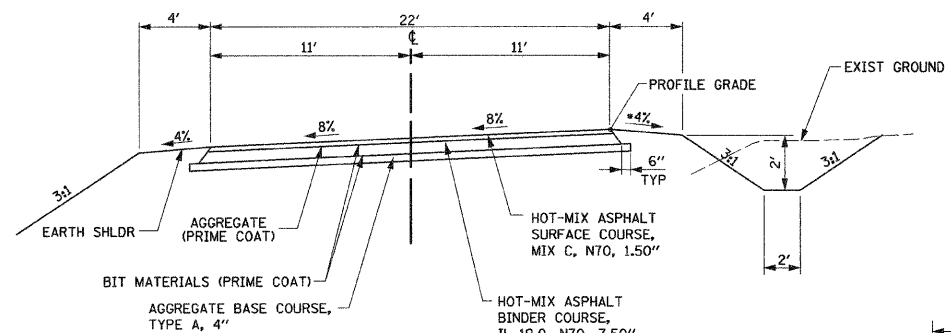
**EXISTING TYPICAL SECTION**

**BEUTTEL ROAD**



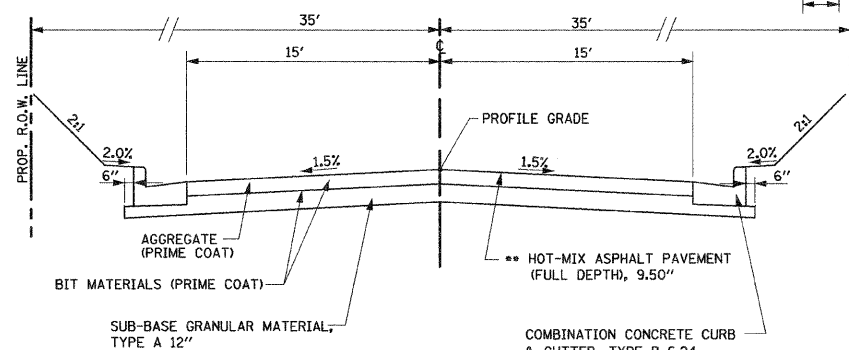
**SUPERELEVATED TYPICAL SECTION**

**DAVIS LANE (TR 16)  
STA 4+31.60 TO STA 8+21.57**



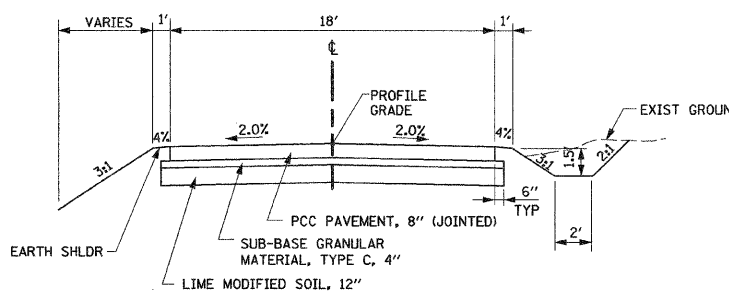
**SUPERELEVATED TYPICAL SECTION**

**LAGEMANN ROAD (CH36)  
STA 14+32.48 TO STA 18+06.84  
STA 20+16.31 TO STA 23+19.66**



**TYPICAL SECTION**

**BENJAMIN DRIVE  
STA 10+24.00 TO STA 11+50.00**



**TYPICAL SERVICE DRIVE**

**LT STA 19+61.50 (LAGEMANN RD),  
STA 0+11 TO 3+53**

**STRUCTURAL DESIGN DATA**  
LAGEMANN LANE (CH 36)

STRUCTURAL DESIGN TRAFFIC:	YEAR	2014
PV= 600	SU= 61	MU= 20
ROAD/STREET CLASSIFICATION:	CLASS	IV
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P= 50	S= 50	M= 50
TRAFFIC FACTOR:	ACTUAL TF=	0.0231
MINIMUM TF=	N/A	AC TYPE= 20
SUBGRADE SUPPORT RATING:		
SSR= POOR	(STA 10+36.29 TO 20+90.00)	

MODIFIED AASHTO  
 USE: HOT-MIX ASPHALT SURFACE COURSE,  
 MIX C, N70, 1.50"  
 HOT-MIX ASPHALT BINDER COURSE,  
 IL-19.0, N70, 7.50"  
 AGGREGATE BASE COURSE,  
 TYPE A, 4"

**STRUCTURAL DESIGN DATA**  
BEUTTEL ROAD

STRUCTURAL DESIGN TRAFFIC:	YEAR	2014
PV= 176	SU= 18	MU= 6
ROAD/STREET CLASSIFICATION:	CLASS	IV
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P= 50	S= 50	M= 50
TRAFFIC FACTOR:	ACTUAL TF=	0.0068
MINIMUM TF=	N/A	AC TYPE= 20
SUBGRADE SUPPORT RATING:		
SSR= POOR	(STA 6+50.00 TO 9+63.00)	

MODIFIED AASHTO  
 USE: HOT-MIX ASPHALT SURFACE COURSE,  
 MIX C, N70, 1.50"  
 HOT-MIX ASPHALT BINDER COURSE,  
 IL-19.0, N70, 7.50"  
 AGGREGATE BASE COURSE,  
 TYPE A, 4"

**STRUCTURAL DESIGN DATA**  
DAVIS LANE (TR 16)

STRUCTURAL DESIGN TRAFFIC:	YEAR	2014
PV= 226	SU= 23	MU= 8
ROAD/STREET CLASSIFICATION:	CLASS	IV
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P= 50	S= 50	M= 50
TRAFFIC FACTOR:	ACTUAL TF=	0.0087
MINIMUM TF=	N/A	AC TYPE= 20
SUBGRADE SUPPORT RATING:		
SSR= POOR	(STA 3+63.86 TO 9+62.43)	

MODIFIED AASHTO  
 USE: HOT-MIX ASPHALT SURFACE COURSE,  
 MIX C, N70, 1.50"  
 HOT-MIX ASPHALT BINDER COURSE,  
 IL-19.0, N70, 7.50"  
 AGGREGATE BASE COURSE,  
 TYPE A, 4"

ILLINOIS DEPARTMENT OF TRANSPORTATION

**TYPICAL SECTIONS**

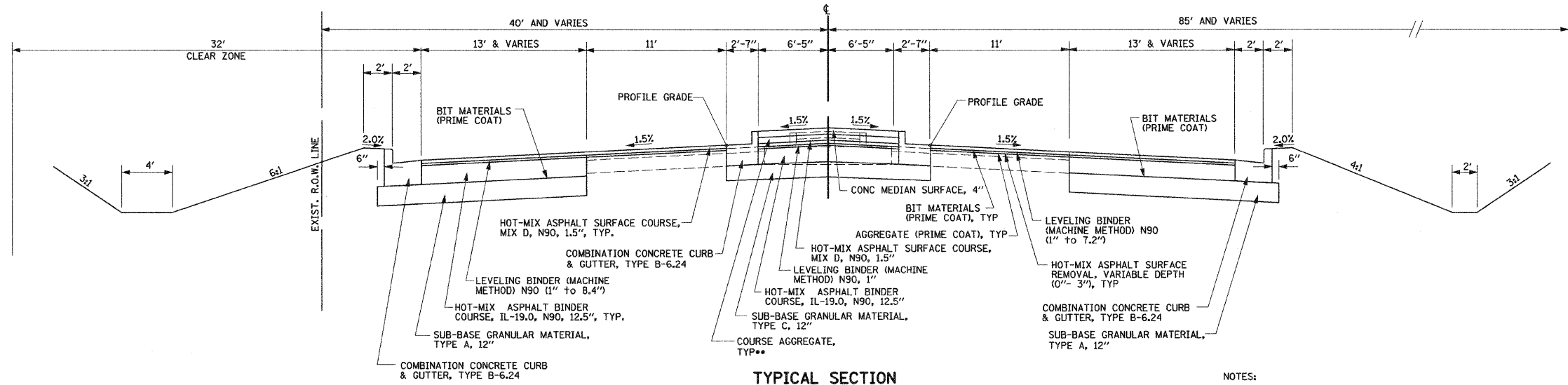
REVISIONS	
NAME	DATE

SCALE: NO SCALE  
 DATE: 09-04-09  
 DRAWN BY: U.J.  
 CHECKED BY: J.J.C.

• WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4%, THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDER WILL NOT BE GREATER THAN 8%.

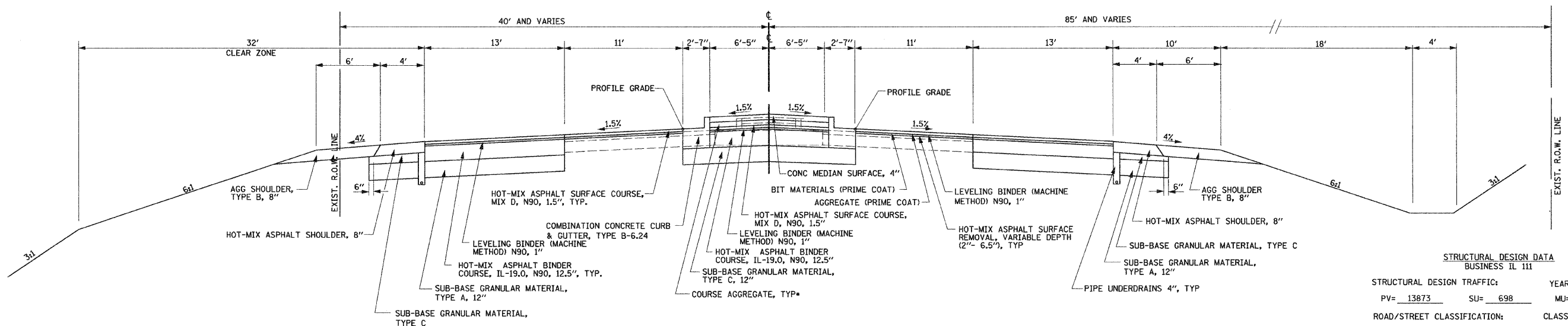
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	17
STA.		TO STA.		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				



**TYPICAL SECTION**  
**IL 111**  
**STA 1172+56.10 TO STA 1174+86.83\***

NOTES:  
 SEE SCHEDULE FOR LOCATION OF CROSS ROAD 4" PIPE UNDERDRAINS  
 \* STA 1174+30.84 TO STA 1174+86.83 RT BEGIN HOT-MIX ASPHALT SHOULDER  
 \*\* COST INCLUDED IN CONCRETE MEDIAN SURFACE, 4"

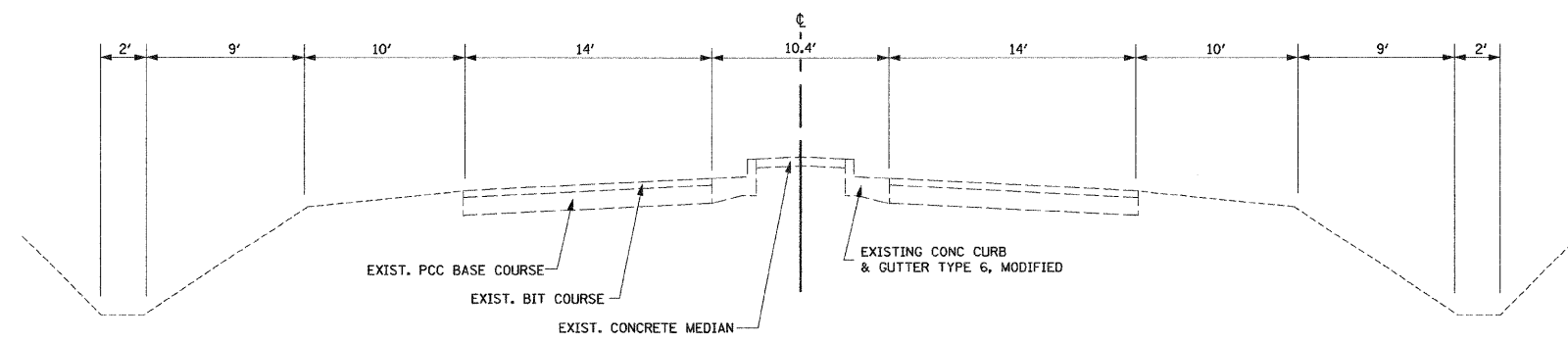


**TYPICAL SECTION**  
**IL 111**  
**STA 1174+86.83 TO STA 1179+40.00**

NOTES:  
 SEE SCHEDULE FOR LOCATION OF CROSS ROAD 4" PIPE UNDERDRAINS  
 \* COST INCLUDED IN CONCRETE MEDIAN SURFACE, 4"

**STRUCTURAL DESIGN DATA**  
 BUSINESS IL 111

STRUCTURAL DESIGN TRAFFIC:	YEAR	2014
PV= 13873	SU= 698	MU= 930
ROAD/STREET CLASSIFICATION:	CLASS	I
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P= 32	S= 45	M= 45
TRAFFIC FACTOR:	ACTUAL TF=	4.88
MINIMUM TF= 4.27	AC TYPE=	20
SUBGRADE SUPPORT RATING:		
SSR= POOR	(STA 172+50.10 TO 180+00)	
USE: HOT-MIX ASPHALT PAVEMENT, (FULL DEPTH) 15" (SUB-BASE GRANULAR MATERIAL, TYPE A, 12")		



**EXISTING TYPICAL SECTION**  
**IL 111**  
**STA 1172+56.10 TO STA 1180+00.00**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

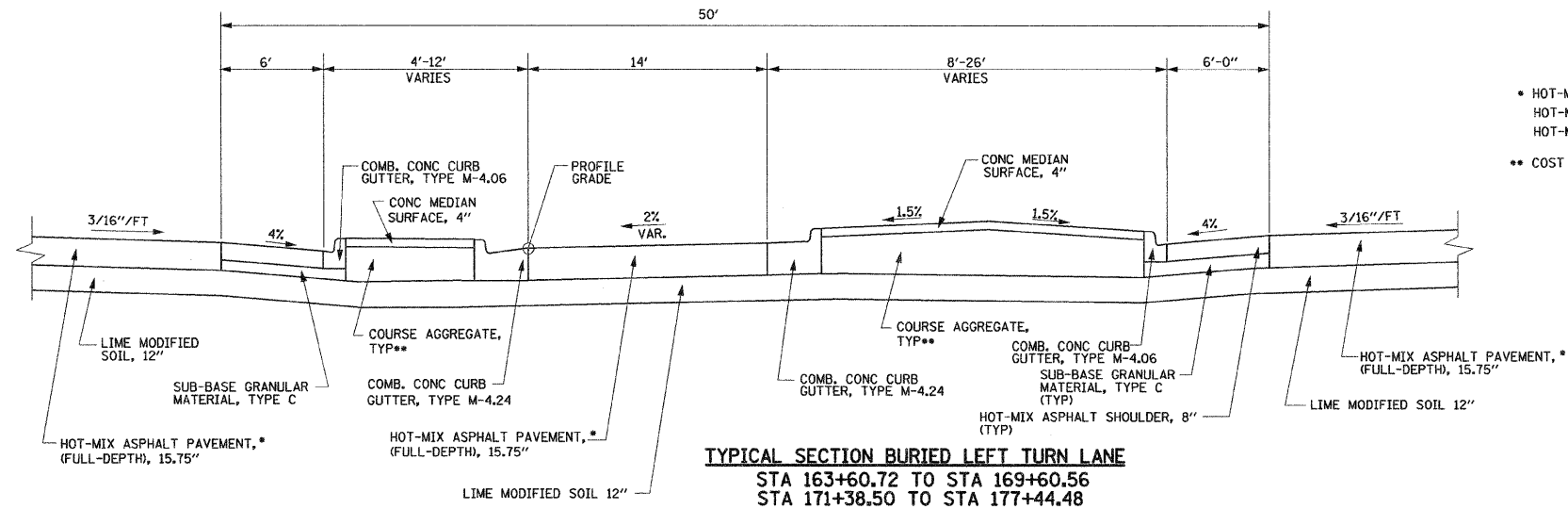
# TYPICAL SECTIONS

SCALE: NO SCALE  
 DATE: 09-04-09

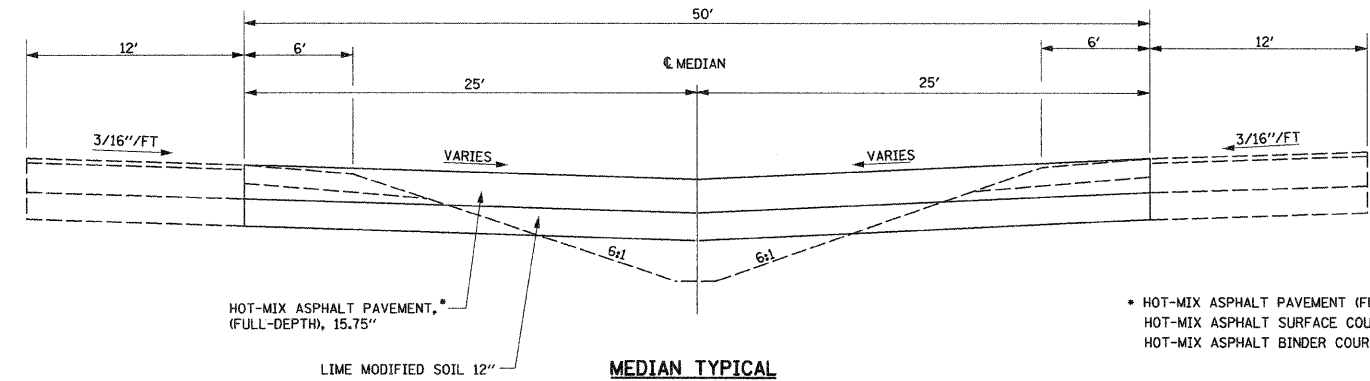
DRAWN BY: U.J.  
 CHECKED BY: J.J.C.



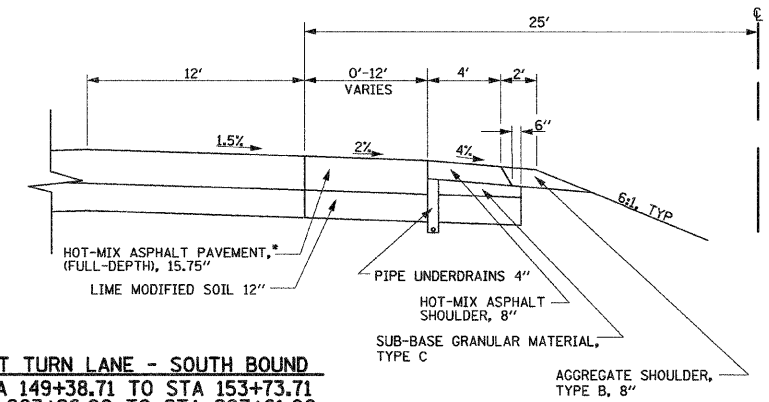
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	19
STA.		TO STA.		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				



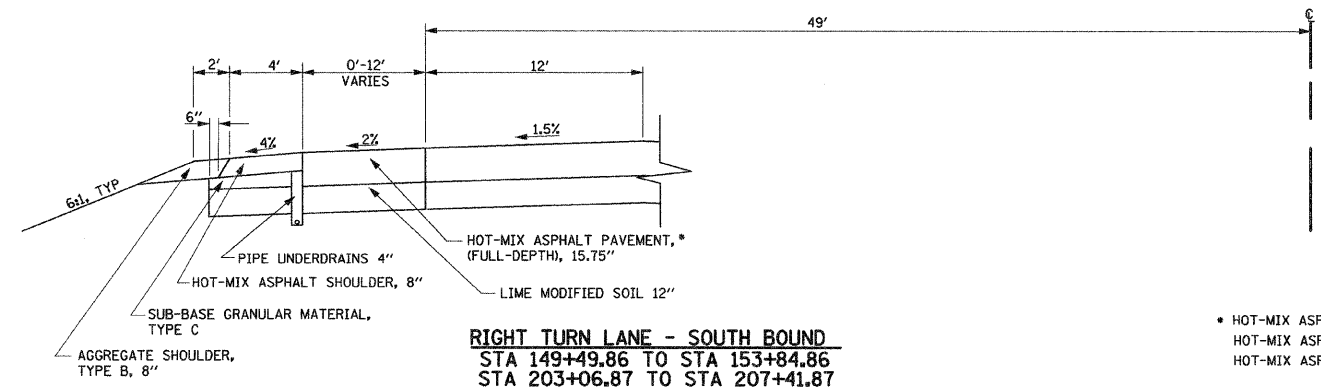
- HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH) 15.75" WILL CONSIST OF:  
 HOT-MIX ASPHALT SURFACE COURSE, MIX D, N90, 2"  
 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 13.75".
- COST INCLUDED IN CONCRETE MEDIAN SURFACE, 4"



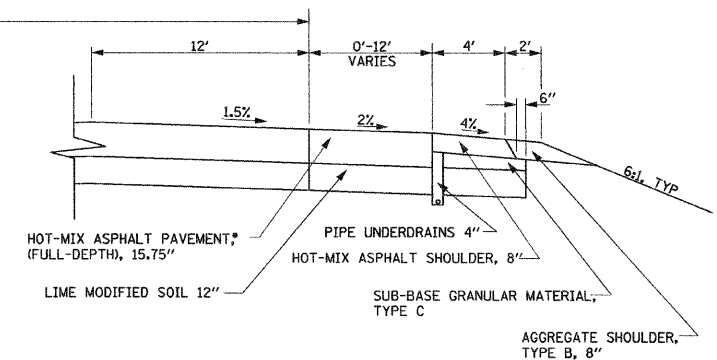
- HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH) 15.75" WILL CONSIST OF:  
 HOT-MIX ASPHALT SURFACE COURSE, MIX D, N90, 2"  
 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 13.75".



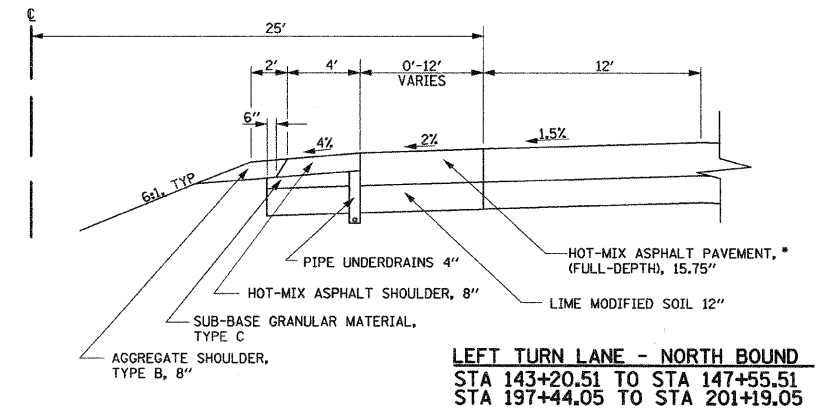
**LEFT TURN LANE - SOUTH BOUND**  
 STA 149+38.71 TO STA 153+73.71  
 STA 203+26.00 TO STA 207+61.00



- HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH) 15.75" WILL CONSIST OF:  
 HOT-MIX ASPHALT SURFACE COURSE, MIX D, N90, 2"  
 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 13.75".



**RIGHT TURN LANE - NORTH BOUND**  
 STA 165+67.73 TO STA 169+71.34



- HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH) 15.75" WILL CONSIST OF:  
 HOT-MIX ASPHALT SURFACE COURSE, MIX D, N90, 2"  
 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 13.75".

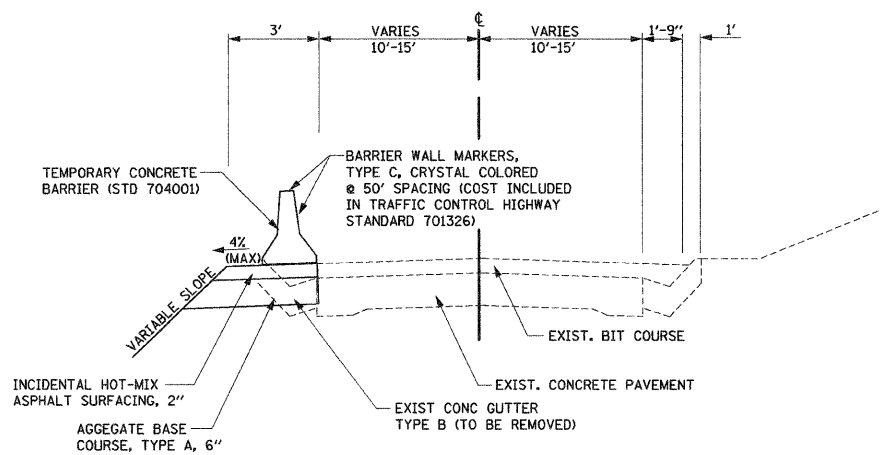
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

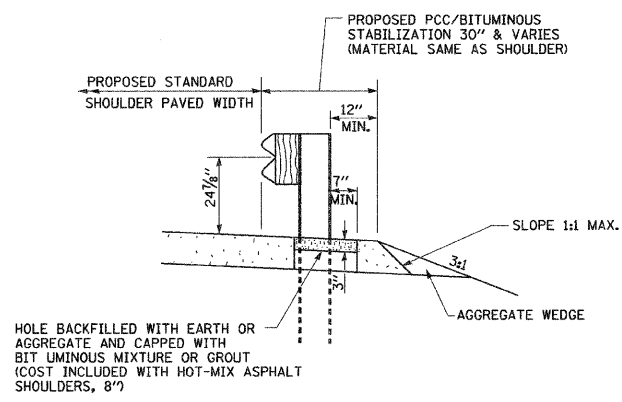
# TYPICAL SECTIONS

SCALE: NO SCALE  
 DATE: 09-04-09

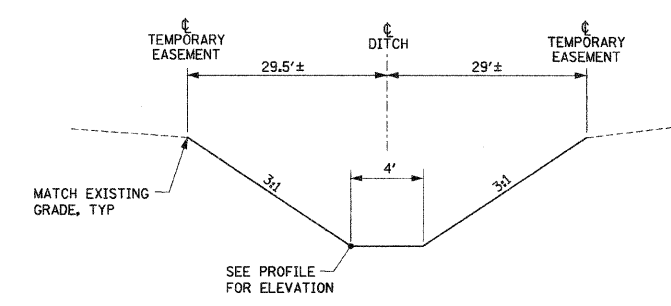
DRAWN BY: U.J.  
 CHECKED BY: J.J.C.



**FOUNDATION PAD FOR TEMP CONC BARRIER**  
 FAP 310 (US 67)  
 STA 173+00 TO STA 180+00  
 STA 206+00 TO STA 221+00

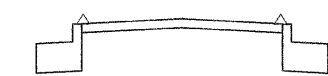


**SHOULDER WITH GUARDRAIL DETAIL**

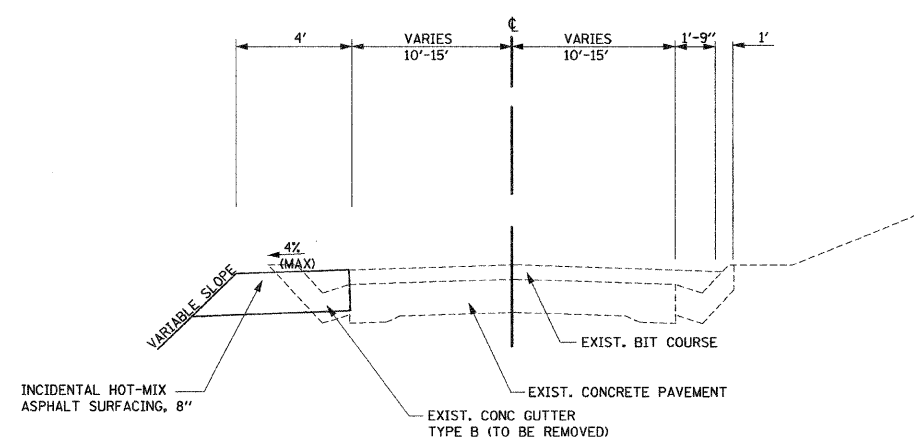


**SPECIAL DITCH NO. 1**  
 STA 10+65.00 TO 14+25.00

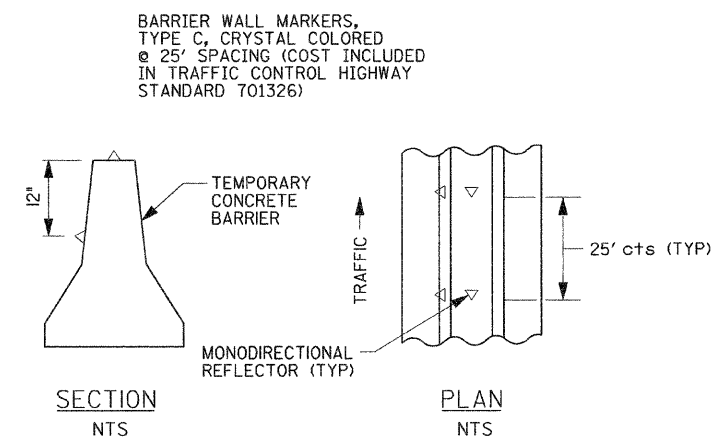
PRISMATIC CURB REFLECTOR, TYPE B,  
 AMBER COLORED @ 80' SPACING



**SECTION**  
 NTS  
**PRISMATIC CURB REFLECTORS DETAIL**



**FOUNDATION PAD FOR ATTENUATOR TEST LEVEL 3**  
 FAP 310 (US 67)  
 STA 172+75 TO STA 173+00  
 STA 180+00 TO STA 180+25  
 STA 205+75 TO STA 206+00  
 STA 221+00 TO STA 221+25



**BARRIER WALL REFLECTORS DETAIL**  
 FOR TEMPORARY CONCRETE BARRIER

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

# DETAILS

SCALE: NO SCALE  
 DATE: 09-04-09

DRAWN BY: U.J.  
 CHECKED BY: J.J.C.

THERMOPLASTIC PAVEMENT MARKING SCHEDULE										RAISED REFLECTIVE PAVEMENT MARKERS			PRISMATIC CURB REFLECTOR
DESIGNATION	BEGIN STATION	END STATION	4" YELLOW	4" WHITE	8" WHITE	12" WHITE	12" YELLOW	24" WHITE	LETTERS & SYMBOLS*	ONE WAY CRYSTAL	ONE WAY AMBER	TWO WAY AMBER	ONE WAY AMBER
			(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(SQ FT)	(EACH)	(EACH)	(EACH)	(EACH)
US 67 (Godfrey Road)	165+45.00	168+35.00	968	255			35		31.2	8	8	6	
US 67 (Godfrey Road)	168+35.00	171+00.00	544	638	46	22			62.4	19	2	6	
US 67 (Godfrey Road)	171+00.00	177+00.00	1068	675	29		8	84	62.4	22	6	7	
US 67 (Godfrey Road)	177+00.00	57+00.00	3812	2252			690		65.9	24	32	8	
			6392	3820	75	22	733	84	222	73	48	27	0
IL 111	1172+06.00	1174+00.00	240	114	360	475		42	76.3	3			3
IL 111	1174+00.00	1179+40.00	1080	1280	90				15.6	14			14
			1320	1394	450	475	0	42	92	17	0	0	17
Ramp A	00+00.00	19+17.61	1262	2020	720				6.9	40			
Ramp B	09+11.00	35+00.00	1964	1911					6.9				
Ramp B	35+00.00	47+03.00		1747	40				1.7				
FAP Route 310	91+50.00	110+00.00	3600	3850					19.1	45			
FAP Route 310	110+00.00	132+00.00	4400	4665		210			24.2	55			
FAP Route 310	132+00.00	142+00.00	2000	2500					13.9	25			
FAP Route 310	142+00.00	152+50.00	1736	3193				18	156.0	44			
FAP Route 310	152+50.00	163+50.00	1970	2531					13.9	28			
FAP Route 310	163+50.00	174+50.00	1957	2630	648	254	203		131.7	54			
FAP Route 310	174+50.00	198+00.00	4405	5950	230	30			43.3	67			
FAP Route 310	198+00.00	209+00.00	1829	3370					147.3	46			
FAP Route 310	209+00.00	225+00.00	1690	2774				20	19.1	16			
Benjamin Road	10+00.00	11+50.00	30										24
Access Road 1	10+00.00	73+22.00	1565	12,342		48							12
Access Road 2	10+00.00	41+08.00	770	6,140									22
Access Road 3	10+00.00	21+33.00	270	2,245									21
Beuttel Road	05+20.00	10+00.00											12
Lagemann Road	10+00.00	24+00.00	2560	2,500									16
Davis Lane	02+75.00	10+00.00											
			32008	60368	1638	542	203	145	584	420	0	0	0
TOTAL			39720	65582	2163	1039	936	271	898	510	48	27	17

\* UNDERDRAIN TRIANGLES WILL BE PAID FOR AS THERMOPLASTIC LETTERS & SYMBOLS (1 TRIANGLE EQUALS 1.73 SQ FT)

SHORT TERM PAVEMENT MARKING						
DESIGNATION	LOCATION		1st APPLICATION		2nd APPLICATION	
	BEGIN STA	END STA	4" YELLOW (LIN FT)	4" WHITE (LIN FT)	4" YELLOW (LIN FT)	4" WHITE (LIN FT)
US 67 (Godfrey Road)	165+45.00	56+44.00	610	454		
IL 111	1172+00	1179+40	150		1268	
SIDE ROADS AND ACCESS ROADS			1268			
FAP Route 310 SB	104+77	136+00		312		
FAP Route 310 NB	139+58	179+00		394		
FAP Route 310 SB	181+50	221+04		395		
TOTAL			2028	1555	1268	0

WORK ZONE PAVEMENT MARKING REMOVAL					
DESIGNATION	LOCATION		LENGTH (FT)	SIZE (IN)	AREA (SQ FT)
	BEGIN STA	END STA			
US 67 (Godfrey Road)	165+45.00	57+00.00	17588	4	5863
US 67 (Godfrey Road)	168+23.00	57+00.00	86	12	86
US 67 (Godfrey Road)	165+45.00	168+23.00	31.2		32
US 67 (Godfrey Road)	168+23.00	171+00.00	62.4		63
US 67 (Godfrey Road)	171+00.00	177+00.00	62.4		63
US 67 (Godfrey Road)	177+00.00	57+00.00	62.4		63
IL 111	1172+86	1179+40	5620	4	1874
IL 111	1171+50	1172+00	40	12	40
IL 111	1172+06	1174+00	72.8		73
IL 111	1174+00	1180+00	15.6		16
US 67 (Godfrey Road) - FAP 310	57+00	103+95	1965	4	655
CROSS OVER DETAIL NO. 1	136+00	142+50	3000	4	1000
CROSS OVER DETAIL NO. 1	0+00	7+11	2352	4	784
FAP Route 310 SB	111+00	136+00	5630	4	1877
CROSS OVER DETAIL NO. 2	0+00	13+06	5624	4	1875
CROSS OVER DETAIL NO. 2	0+00	5+78	2712	4	904
FAP Route 310 NB	144+25	179+00	8740	4	2914
CROSS OVER DETAIL NO. 3	0+00	11+85	5140	4	1714
CROSS OVER DETAIL NO. 4	0+00	8+04	3616	4	1206
FAP Route 310 SB	185+86	221+00	7908	4	2636
CROSS OVER DETAIL NO. 4 (ST 2)	217+50	227+50	4000	4	1334
CROSS OVER DETAIL NO. 4 (ST 3)	217+40	229+00	3860	4	1287
TOTAL					26359

POLYUREA PAVEMENT MARKING SCHEDULE								
STRUCTURE NO.	DESIGNATION	BEGIN STATION	END STATION	4" YELLOW	4" WHITE	8" WHITE	12" WHITE	LETTERS & SYMBOLS
				(LIN FT)	(LIN FT)	(LIN FT)	(LIN FT)	(SQ FT)
060-0332	RAMP B	15+06.00	21+91.00	688	685			
060-0250	FAP 310	159+65.00	160+83.00	118	148			
060-0328	FAP 310	160+11.00	161+23.00	112	142			
060-0251	FAP 310	165+06.00	166+38.00	132	176	168	18	16
060-0329	FAP 310	165+45.00	166+60.00	115	145			
TOTAL				1165	1296	168	18	16

POLYUREA PAVEMENT MARKING IS TO BE USED IN PLACE OF THERMOPLASTIC PAVEMENT MARKINGS ON ALL PCC SURFACES

PAVEMENT MARKING REMOVAL			
DESIGNATION	Length (ft)	Size (in)	SQ FT
US 67 (Godfrey Road)	165+45 to 56+44	2875	4
IL 111	1172+60 to 1179+40	680	4
Crossover Det. 1	103+95 to 106+45	123	4
Connector Det. 1	139+00 to 142+18	369	4
Connector Det. 2	181+30 to 184+76	413	4
Crossover Det. 4	226+35 to 229+00	311	4
Total			1590

PAINT PAVEMENT MARKING CURB	
LOCATION	FOOT
Intersection of US 67 (Godfrey Road) & IL 111	
Island (US 67 to IL 111)	300.0
Median Nose on IL 111	50.0
Island (IL 111 to US 67)	125.0
Intersection of FAP 310 & Lagemann Road	
STA. 167+00	525.0
Burled Left Turn Lane to FE	505.0
Burled Left Turn Lane to Lagemann Road	420.0
STA. 174+50	630.0
Total	2555

TEMPORARY PAVEMENT MARKING						
DESIGNATION	LOCATION		4" YELLOW (LIN FT)	4" WHITE (LIN FT)	24" WHITE (LIN FT)	LTRS & SYM (SQ FT)
	BEGIN STA	END STA				
US 67 (Godfrey Road)	165+45.00	57+00.00	10740	6848	86	218.4
U.S. 67 (Godfrey Road) - FAP 310	57+00.00	103+95	1965			
IL 111	1171+50	1179+40	3000	2620	40	88.4
CONNECTOR DETAIL NO. 1	136+00	142+50	1700	1300		
CROSS OVER DETAIL NO. 1	0+00	7+11	1641	711		
FAP Route 310 SB	111+00	136+00	630	5000		
CROSS OVER DETAIL NO. 2	0+00	13+06	3012	2612		
CONNECTOR DETAIL NO. 2	0+00	5+78	1556	1156		
FAP Route 310 NB	144+25	179+00	870	7870		
CROSS OVER DETAIL NO. 3	0+00	11+85	2770	2370		
CROSS OVER DETAIL NO. 4	0+00	8+04	2008	1608		
FAP Route 310 SB	185+86	217+50	791	6328		
CROSS OVER DETAIL NO. 4 (ST 2)	217+50	227+50	2000	2000		
CROSS OVER DETAIL NO. 4 (ST 3)	217+40	229+00	2310	1550		
TOTAL			34993	41973	126	306.8

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

# SCHEDULES OF QUANTITIES

SCALE: NO SCALE  
DATE: 09-04-09

DRAWN BY: C.J.C.  
CHECKED BY: J.J.C.



TREE REMOVAL (6 TO 15 UNITS DIAMETER)				
US 67 (Godfrey Road)		Offset	size(units)	
171+45.10	Rt	70.87	12	
171+69.20	Rt	92.01	12	
173+30.94	Rt	66.9	10	
47+03.11	Rt	44.12	15	
58+08.89	Lt	46.4	6	
		Subtotal	55	
FAP Route 310				
98+70.19	Rt	209.11	11	
98+77.56	Rt	217.69	11	
101+01.32	Rt	151.96	12	
101+06.52	Rt	157.65	12	
101+11.42	Rt	162.72	12	
101+16.56	Rt	171.52	12	
101+22.70	Rt	179.8	12	
101+25.43	Rt	165.9	12	
101+28.84	Rt	188.92	12	
101+36.42	Rt	183.52	12	
101+99.28	Rt	188.23	12	
102+41.46	Rt	152.75	10	
102+49.04	Rt	138.86	12	
102+58.43	Rt	158.99	6	
102+61.35	Rt	119.6	12	
102+70.81	Rt	158.32	11	
102+73.66	Rt	139.53	11	
102+74.68	Rt	92.99	9	
102+95.44	Rt	219.23	10	
103+27.51	Rt	164.3	13	
104+03.75	Rt	134.46	12	
105+18.54	Rt	179.45	15	
105+70.06	Rt	98.81	12	
105+93.15	Rt	132.61	15	
107+03.92	Lt	2.25	15	
107+15.30	Lt	25.73	9	
107+36.56	Lt	23.91	11	
107+46.77	Lt	4.88	13	
107+76.97	Lt	5.72	13	
108+44.89	Lt	38.81	15	
108+45.37	Lt	22.32	10	
108+65.07	Lt	184.62	12	
110+11.66	Lt	43.67	12	
110+19.21	Lt	6.87	12	
110+22.59	Lt	52.15	12	
110+26.73	Lt	34	12	
110+41.46	Lt	55.92	12	
110+41.73	Lt	42.75	12	
111+97.30	Lt	39.98	8	
113+08.71	Lt	14.28	15	
113+84.06	Lt	31.91	12	
113+84.81	Lt	71.64	12	
114+34.55	Lt	36	12	
114+36.56	Lt	57.18	12	
114+40.30	Lt	45.98	12	
114+43.69	Lt	83.19	15	
121+08.52	Lt	80.75	12	
122+01.86	Lt	20.75	15	
148+09.88	Lt	138.49	8	
148+12.17	Lt	162.75	8	
148+14.80	Lt	284.94	15	
148+17.23	Lt	137.51	8	
148+19.94	Lt	162.47	8	
150+69.11	Rt	25.78	8	
150+70.31	Rt	9.85	10	
150+70.54	Lt	9.85	14	
150+83.76	Rt	109.26	12	
151+16.29	Rt	125.48	12	
151+16.64	Rt	19.65	12	
151+29.00	Rt	125	12	
151+34.95	Rt	20.71	12	
151+53.73	Rt	23.26	12	
151+61.92	Rt	95.55	12	
151+63.79	Rt	54.07	12	
151+71.61	Rt	24.67	12	

TREE REMOVAL (6 TO 15 UNITS DIAMETER)				
FAP Route 310		Offset	size(units)	
169+54.05	Rt	70.58	6	
169+73.69	Rt	67.2	12	
169+75.56	Rt	101.25	13	
176+43.00	Rt	34.8	12	
177+83.24	Rt	12.65	8	
177+94.65	Rt	32.96	12	
178+61.45	Rt	14.85	12	
178+70.98	Rt	67.02	12	
179+26.36	Rt	61.04	12	
179+66.52	Rt	39.42	6	
181+39.40	Rt	50.33	6	
185+49.83	Rt	82.33	12	
185+81.60	Rt	87.15	13	
198+37.26	Lt	89.14	6	
198+61.09	Lt	10.31	10	
198+75.38	Lt	90	14	
198+88.05	Lt	48.86	14	
199+02.17	Lt	12.8	10	
199+30.57	Lt	53.33	8	
199+42.40	Lt	14.7	10	
199+73.46	Lt	55.68	14	
199+79.70	Lt	16.41	12	
199+90.96	Lt	92.47	14	
200+04.82	Lt	55.84	6	
201+14.21	Lt	7.91	10	
201+26.51	C	0	8	
201+31.42	Rt	74.39	6	
201+44.26	Rt	74.91	6	
201+46.80	Lt	8.36	11	
201+55.23	Rt	74.81	6	
201+67.81	Rt	74.87	6	
201+69.95	Lt	101.16	12	
201+90.49	Lt	149.58	12	
202+31.76	Lt	114	14	
202+32.74	Lt	119.95	14	
202+34.69	Lt	90.96	8	
202+44.97	Lt	82.05	12	
202+46.87	Lt	62.29	7	
202+57.46	Rt	66	12	
202+67.69	Rt	66.5	12	
202+77.51	Rt	67.01	12	
202+88.47	Rt	66.5	12	
203+01.79	Rt	66.06	12	
203+44.48	Rt	68.03	13	
203+50.39	Rt	72.66	9	
207+77.81	Rt	101.43	12	
208+03.90	Rt	99.67	12	
208+29.02	Rt	110.67	12	
208+64.80	Rt	105.84	12	
208+81.04	Rt	118.18	10	
209+87.48	Rt	117.46	15	
212+99.95	Lt	52.34	15	
		Subtotal	1316	
Access Road 1				
45+67.25	Lt	0.31	6	
49+76.05	Rt	17.52	12	
50+59.82	Lt	10.81	11	
50+63.64	Lt	0.2	10	
50+73.47	Rt	24.31	11	
50+76.98	Rt	33.67	8	
51+00.56	Lt	17.11	10	
52+74.61	Rt	27.48	12	
54+60.63	Lt	24.6	12	
54+75.20	Lt	14.2	12	
55+82.25	Lt	21.6	12	
55+84.81	Lt	12.37	12	
55+90.62	C	0	12	
55+93.66	Rt	9.9	12	
55+98.79	Rt	17.98	12	
56+02.38	Rt	27.16	12	
60+79.90	Rt	159.39	12	
60+90.91	Rt	169.06	8	
61+07.22	Rt	192.99	12	
		Subtotal	208	

TREE REMOVAL (6 TO 15 UNITS DIAMETER)				
Lagemann Road		Offset	size(units)	
10+96.67	Rt	78.54	12	
11+14.55	Rt	78.25	12	
11+19.85	Lt	3.43	14	
11+22.38	Rt	76.61	12	
11+28.35	Rt	72.32	12	
11+48.26	Lt	23.49	14	
12+21.11	Rt	40.76	12	
12+46.84	Rt	34.19	14	
12+57.39	Rt	29.87	14	
14+67.81	Lt	68.67	8	
15+29.14	Lt	88.3	12	
22+94.53	Lt	27.98	11	
		Subtotal	147	
		Total	1726	

TREE REMOVAL (OVER 15 UNITS DIAMETER)				
US 67 (Godfrey Road)		Offset	size(units)	
171+61.63	Rt	116.62	18	
171+83.80	Rt	128.12	18	
46+50.89	Rt	56.59	24	
46+77.38	Rt	49.48	18	
47+09.05	Rt	59.93	18	
47+28.42	Rt	55.48	18	
47+34.75	Rt	55.62	18	
47+83.39	Rt	58.51	17	
		Subtotal	149	
IL III				
1174+48.00	Rt	85.35	16	
1174+20.75	Rt	70.64	16	
1174+84.16	Lt	85.79	17	
1178+33.08	Lt	91.63	30	
1178+93.16	Lt	68.52	17	
		Subtotal	96	
FAP Route 310				
98+07.51	Rt	122.39	24	
98+52.13	Rt	218.9	28	
101+65.82	Rt	222.5	18	
101+73.48	Rt	198.74	18	
102+00.67	Rt	83.11	20	
102+16.90	Rt	167.36	18	
103+07.48	Rt	131.75	40	
103+34.09	Rt	161.59	84	
103+83.76	Rt	94.4	42	
104+51.45	Rt	197.65	18	
105+34.97	Rt	109.94	24	
105+82.08	Rt	182.48	29	
105+93.78	Rt	200.1	36	
106+15.50	Rt	164.22	16	
106+42.09	Rt	123.5	20	
107+25.28	Lt	2.27	18	
107+61.12	Lt	22.95	24	
107+94.17	Lt	21.69	24	
108+25.95	Lt	21.84	24	
108+99.56	Lt	53.21	26	
109+21.50	Lt	5.4	18	
109+22.76	Lt	54.89	24	
109+65.53	Lt	6.43	18	
112+44.06	Lt	34.64	18	
113+09.55	Lt	43.39	24	
114+29.46	Lt	102.62	25	
114+76.48	Lt	14.28	24	
121+30.30	Lt	37.63	24	
122+26.88	Lt	65.04	19	
122+33.41	Lt	11.82	30	
122+71.57	Lt	32.75	51	
123+07.38	Rt	8.75	18	
125+44.55	Rt	76.55	24	
125+81.94	Rt	75.3	24	
126+96.20	Rt	78.3	16	
127+37.72	Rt	78.04	20	
148+14.03	Lt	203.61	16	
148+13.68	Lt	237.37	21	
150+75.78	Rt	91.43	38	
151+02.34	Lt	16.53	24	
151+06.78	Rt	98.97	20	
151+14.14	Rt	50.46	24	
151+36.35	Lt	4.79	30	
151+64.71	Lt	8.55	30	
151+93.64	Lt	156.05	24	
169+37.61	Rt	16.31	24	
176+43.42	Rt	62.07	24	
176+44.24	Rt	74.69	24	
176+90.52	Rt	89.64	24	
176+91.45	Rt	50.39	16	
177+08.49	Rt	16.73	42	
177+51.34	Rt	9.07	27	
177+52.84	Rt	87.21	24	
177+79.16	Rt	53.99	30	
178+38.83	Rt	10.76	24	
178+73.85	Rt	16.73	16	
178+83.12	Rt	40.84	24	
179+25.83	Rt	84.3	24	
179+27.65	Rt	40.62	16	
185+08.08	Rt	83.12	18	
185+37.07	Rt	81.75	22	

TREE REMOVAL (OVER 15 UNITS DIAMETER)				
FAP Route 310		Offset	size(units)	
200+17.71	Lt	17.79	16	
202+17.14	Lt	97.8	32	
205+69.56	Rt	108.5	21	
206+97.73	Rt	169.78	30	
207+05.02	Rt	170.49	30	
209+06.03	Rt	119.11	30	
209+83.18	Rt	102.45	31	
210+79.04	Lt	15.41	24	
210+88.18	Lt	17.07	32	
211+61.52	Lt	32.03	24	
212+16.93	Lt	40.3	24	
212+46.58	Lt	229.09	18	
212+52.42	Lt	70.85	24	
212+54.26	Lt	46.77	16	
212+60.36	Lt	246.7	18	
212+61.64	Lt	103.48	24	
212+69.79	Lt	136.4	24	
212+76.84	Lt	171.61	38	
212+82.47	Lt	203.7	40	

PERIMETER EROSION BARRIER					
FAP Route 310	LT/RT	Stage	FOOT		
95+80.00	to	96+80.00	Rt	3	100
117+50.00	to	119+00.00	Lt	1	150
143+00.00	to	145+00.00	Rt	1	200
143+00.00	to	145+50.00	Rt	1	250
143+00.00	to	145+50.00	Rt	1	250
147+75.00	to	148+20.00	Lt	3	45
148+75.00	to	160+15.00	Lt	3	1140
150+00.00	to	154+50.00	Rt	1	450
155+00.00	to	157+00.00	Rt	1	200
160+59.00	to	164+95.00	Rt	1	436
163+00.00	to	166+10.00	Lt	3	310
163+25.00	to	164+95.00	Rt	1	170
163+25.00	to	165+12.00	Rt	1	187
165+82.00	to	166+24.00	Rt	1	42
165+82.00	to	166+24.00	Rt	1	42
165+97.00	to	166+38.00	Rt	1	41
171+00.00	to	175+50.00	Rt	1	450
185+00.00	to	187+66.00	Rt	3	266
185+75.00	to	187+25.00	Rt	3	150
185+75.00	to	187+25.00	Rt	3	150
187+66.00	to	188+50.00	Rt	3	110
188+50.00	to	189+13.00	Rt	3	63
189+31.00	to	191+66.00	Rt	3	235
190+64.00	to	197+50.00	Lt	1	686
191+82.00	to	193+70.00	Rt	3	188
192+32.00	to	195+75.00	Lt	1	343
192+32.00	to	195+75.00	Lt	1	343
193+85.00	to	197+70.00	Rt	3	385
197+94.00	to	199+25.00	Rt	3	131
199+41.00	to	200+73.00	Rt	3	132
200+87.00	to	201+50.00	Rt	3	63
202+27.00	to	204+75.00	Lt	1	248
202+58.00	to	204+75.00	Lt	1	217
203+00.00	to	205+00.00	Lt	1	200
207+25.00	to	215+75.00	Rt	3	850
211+85.00	to	218+25.00	Lt	1	640
213+25.00	to	214+75.00	Rt	3	150
225+50.00	to	227+75.00	Lt	1	225
228+00.00	to	228+75.00	Rt	3	75
228+25.00	to	228+75.00	Rt	3	50
228+00.00	to	229+00.00	Rt	3	100
<b>Ramp A</b>					
2+00.00	to	8+75.00	Rt	1	675
8+25.00	to	9+60.00	Rt	1	135
12+85.00	to	17+15.00	Rt	1	430
<b>Ramp B</b>					
10+00.00	to	15+00.00	Rt	2	500
10+50.00	to	15+00.00	Lt	2	450
13+00.00	to	14+50.00	Rt	2	150
13+00.00	to	15+00.00	Lt	2	200
22+00.00	to	25+00.00	Rt	2	300
22+00.00	to	24+00.00	Lt	2	200
22+25.00	to	22+75.00	Lt	2	50
28+50.00	to	31+00.00	Rt	2	250
30+00.00	to	32+00.00	Rt	2	200
<b>Access Road 1</b>					
26+50.00	to	28+63.00	Lt	1	213
44+50.00	to	45+21.00	Rt	1	71
44+50.00	to	45+23.00	Lt	1	73
49+65.00	to	53+00.00	Rt	1	335
<b>Access Road 2</b>					
31+00.00	to	32+50.00	Rt	1	150
31+20.00	to	33+00.00	Lt	1	180
33+00.00	to	35+00.00	Lt	1	200
35+00.00	to	37+50.00	Lt	1	250
<b>Davis Lane</b>					
2+75.00	to	3+50.00	Rt	1	75
5+98.00	to	8+39.00	Lt	1	241
6+16.00	to	6+70.00	Rt	1	54
7+83.00	to	8+29.00	Rt	1	51
<b>TOTAL</b>					
					15896

EROSION CONTROL BLANKET					
FAP Route 310 - Stage 1				Area (sq)	
97+50.00	to	155+50.00	Rt/Lt	18227.7	
166+50.00	to	190+50.00	Rt/Lt	7624.3	
197+50.00	to	228+25.00	Rt/Lt	12042.0	
				Subtotal	37894.0
<b>FAP Route 310 - Stage 2</b>					
102+50.00	to	114+50.00	Rt/Lt	1286.4	
128+00.00	to	185+00.00	Rt/Lt	4218.1	
222+50.00	to	228+50.00	Rt/Lt	218.9	
				Subtotal	5723.4
<b>FAP Route 310 - Stage 3</b>					
91+50.00	to	102+50.00	Rt/Lt	1868.0	
136+00.00	to	225+00.00	Rt/Lt	23897.6	
				Subtotal	25765.6
<b>US 67</b>					
166+50.00	to	57+50.00	Rt/Lt	3073.3	
<b>IL III - Stage 1</b>					
11+73.00	to	1179+40.00	Rt	1217.7	
<b>IL III - Stage 2</b>					
11+73.00	to	1179+40.00	Lt	1635.1	
<b>Benjamin Road</b>					
10+50.00	to	11+50.00	Rt/Lt	210.0	
<b>Beuttel Road</b>					
5+20.00	to	9+00.00	Rt/Lt	1954.2	
<b>Ingham Road</b>					
8+05.00	to	11+50.00	Rt/Lt	1002.0	
<b>Lagemann Road</b>					
10+50.69	to	24+00.00	Rt/Lt	9642.0	
<b>Davis Lane</b>					
2+75.25	to	9+00.00	Rt/Lt	2603.1	
<b>Ramp A</b>					
7+50.04	to	18+50.00	Rt/Lt	3163.8	
<b>Ramp B - Stage 2</b>					
10+00.00	to	33+00.00	Rt/Lt	11789.0	
<b>Ramp B - Stage 3</b>					
10+00.00	to	15+50.00	Rt/Lt	2663.9	
				Subtotal	14452.9
<b>Access Road 1</b>					
11+00.00	to	61+50.00	Rt/Lt	14869.3	
<b>Access Road 2</b>					
11+00.00	to	41+00.00	Rt/Lt	16817.3	
<b>Access Road 3</b>					
11+00.00	to	17+00.00	Rt/Lt	2403.1	
				Total	139354

INLET AND PIPE PROTECTION					
US 67 (Godfrey Road)	Stage	Offset	Each		
167+75	LT	1	27.88	1	
167+75	LT	1	20.00	1	
168+00	RT	1	53.94	1	
169+15	LT	1	20.00	1	
169+25	RT	1	56.00	1	
170+00	LT	1	28.13	1	
170+00	LT	1	20.00	1	
170+60	RT	1	58.90	1	
170+75	LT	1	20.80	1	
171+33	RT	1	44.00	1	
171+86	LT	1	35.40	1	
171+93	LT	1	42.00	1	
172+48	LT	1	38.20	1	
173+50	RT	1	50.00	1	
174+00	LT	1	34.29	1	
174+00	LT	1	20.00	1	
174+00	RT	1	44.00	1	
174+00	RT	1	51.89	1	
176+00	RT	1	55.11	1	
176+00	LT	1	44.00	1	
176+00	RT	1	20.00	1	
177+10	RT	1	56.50	1	
48+42	RT	1	67.80	1	
				SubTotal	23
<b>IL III</b>					
1172+92	LT	2	38.00	1	
1173+20	RT	1	26.00	1	
1173+40	LT	1	44.80	1	
1173+93	RT	1	41.90	1	
1174+05	RT	1	39.70	1	
1174+74	LT	1	52.38	1	
				SubTotal	6
<b>Access Road 1</b>					
10+17	RT	1	31.82	1	
13+42	RT	1	25.53	1	
15+25	LT	1	25.88	1	
17+83	LT	1	24.22	1	
18+60	LT	1	26.96	1	
28+74	LT	1	26.00	1	
42+00	RT	1	22.64	1	
44+50	LT	1	24.57	1	
46+00	LT	1	23.92	1	
51+50	RT	1	33.57	1	
59+22	RT	1	25.87	1	
59+98	LT	1	24.90	1	
66+47	RT	1	20.88	1	
72+04	RT	1	25.83	1	
73+12	RT	1	21.14	1	
<b>Access Road 2</b>					
11+12	LT	1	36.50	1	
13+30	RT	1	28.32	1	
13+65	LT	1	34.79	1	
18+44	LT	1	40.20	1	
21+66	LT	1	33.35	1	
23+75	RT	1	19.43	1	
23+82	LT	1	29.84	1	
24+72	LT	1	29.51	1	
32+50	RT	1	34.59	1	
38+73	LT	1	21.83	1	
40+50	LT	1	21.91	1	
<b>Access Road 3</b>					
12+00	LT	1	33.56	1	
13+24	RT	1	31.35	1	
14+25	RT	1	31.45	1	
17+22	RT	1	28.00	1	
18+67	RT	1	28.54	1	
20+24	RT	1	28.42	1	
21+00	RT	1	31.07	1	
<b>Benjamin Drive</b>					
10+84	LT	1	15.00	1	
10+84	RT	1	15.00	1	
10+94	LT	1	15.00	1	
10+94	RT	1	15.00	1	
<b>Ingham Lane</b>					
10+97	LT	1	20.85	1	
<b>Beuttel Road</b>					
06+94	RT	3	21.93	1	
06+96	LT	3	20.12	1	
09+00	RT	3	24.14	1	
				SubTotal	118
				Total	147

INLET AND PIPE PROTECTION				
Lagemann Road				
10+98	LT	1	37.34	1
16+65	LT/RT	1	24.86	2
19+20	LT	1	19.95	1
20+89	LT	1	25.06	1
22+37	LT	1	25.16	1
23+74	LT	1	21.16	1
<b>Lagemann Road - Private Drive 19+61</b>				
00+25	RT	1	15.92	1
01+07	RT	1	14.21	1
03+05	RT	1	13.99	1
<b>Davis Lane</b>				
07+36	RT	1	37.16	1
<b>FAP Route 310</b>				
94+50	LT	3	82.22	1
95+84	RT	3	87.01	1
96+70	MED	3	0.00	1
100+50	LT	1	53.25	1
102+00	MED	1	0.00	1
105+34	RT	2	85.04	1
109+50	LT	1	60.90	1
110+00	LT	1	60.77	1
111+26	LT	1	60.68	1
113+00	LT	1	4.45	1
114+27	LT	1	93.52	1
117+50	LT	1	5.07	1
121+25	LT	1	77.71	1
125+61	LT	1	77.94	1
127+00	LT	1	5.34	1
128+33	RT	2	76.81	1
130+54	LT	1	77.62	1
135+53	LT	1	4.81	1
136+53	RT	2	77.43	1
142+68	LT	2	77.51	1
143+00	RT	1	4.96	1
143+78	LT	2	78.22	1
147+50	LT	1	6.67	1
148+13	RT	1	90.12	1
163+50	LT	1	3.86	1
167+25	LT	1	19.00	1
167+50	LT	1	19.00	1
167+85	RT	1	19.00	1
168+10	LT	1	19.00	1
168+35	LT	1	19.00	1
168+35	RT	1	19.00	1
170+72	LT	3	91.36	1
171+00	LT	1	19.00	1
171+25	LT	1	19.00	1
171+75	RT	1	19.00	1
60+50	RT	1	2.00	1
171+97	RT	1	19.00	1
60+69	RT	1	2.00	1
172+25	RT	1	19.00	1
61+20	RT	1	2.00	1
61+50	LT	1	2.00	1
175+00	RT	3	5.08	1
177+50	LT	3	5.10	1
179+68	LT	3	79.10	1
184+42	RT	3	81.15	1
187+00	MED	1	0.00	1
187+00	LT	1	78.45	1
190+00	MED	1	0.00	1
190+65	RT	3	60.20	1
192+00	RT	3	60.32	1
193+00	MED	1</		

TEMPORARY DITCH CHECKS				
FAP Route	310	Stage	offset	FOOT
90+65.10	CL	1	0.0	27
90+80.00	RT	1	89.2	23
93+75.00	RT	3	82.2	23
94+40.00	RT	3	89.2	17
95+04.80	RT	3	95.7	17
95+69.30	RT	3	104.4	17
95+90.10	RT	3	103.9	17
96+55.10	RT	3	96.7	23
97+20.10	RT	3	87.2	19
97+84.40	RT	3	83.3	19
101+87.90	RT	3	79.7	23
102+11.00	RT	3	79.7	23
104+84.90	CL	1	0.0	27
105+14.70	RT	3	83.7	23
105+52.50	RT	3	85.7	23
107+04.40	CL	1	0.0	27
109+15.00	CL	1	0.0	27
112+69.00	LT	1	92.9	23
112+89.80	CL	1	0.0	27
113+09.60	CL	1	0.0	27
117+39.40	CL	1	0.0	27
117+39.80	LT	1	97.0	23
117+59.60	CL	1	0.0	27
117+59.80	LT	1	95.2	23
122+00.00	CL	1	0.0	27
123+30.00	CL	1	0.0	27
123+30.00	LT	1	80.0	23
124+41.00	LT	1	80.0	23
124+52.00	CL	1	0.0	27
125+53.00	LT	1	78.0	23
125+70.00	CL	1	0.0	27
126+25.00	RT	2	108.0	23
126+40.00	LT	1	79.0	23
126+92.00	CL	1	0.0	27
128+00.00	LT	1	79.0	23
128+10.00	RT	2	80.0	23
128+20.00	CL	1	0.0	27
129+30.00	RT	2	76.4	23
129+40.00	CL	1	0.0	27
130+50.00	LT	1	77.5	23
130+50.00	RT	2	79.0	23
130+60.00	CL	1	0.0	27
131+30.00	LT	1	78.0	23
131+70.00	RT	2	78.0	23
131+80.00	CL	1	0.0	27
132+50.20	LT	1	78.0	23
132+90.20	RT	2	77.5	23
133+00.00	CL	1	0.0	27
133+69.00	LT	1	77.4	23
134+20.00	CL	1	0.0	27
134+71.80	RT	2	77.3	23
134+89.90	LT	1	77.9	23
135+31.10	RT	2	77.1	23
135+40.10	CL	1	0.0	27
136+48.00	RT	2	78.7	23
136+94.20	CL	3	0.0	27
137+62.10	RT	2	77.3	23
138+08.70	CL	3	0.0	27
138+68.20	LT	3	77.2	23
138+81.70	RT	2	78.0	23
139+30.70	CL	3	0.0	27
140+00.60	RT	1	76.2	23
140+39.40	LT	3	77.7	23
140+56.40	CL	3	0.0	27
141+20.50	RT	1	76.8	23
141+59.00	LT	3	77.7	23
141+72.30	CL	3	0.0	27
142+41.10	RT	1	77.5	23
142+55.40	LT	3	77.4	23
142+92.10	CL	3	0.0	27
143+73.20	LT	3	77.9	23
144+35.40	CL	1	0.0	27
145+24.90	LT	3	77.9	23
145+28.10	CL	1	0.0	27
146+00.80	LT	3	77.8	23

TEMPORARY DITCH CHECKS				
FAP Route	310	Stage	offset	FOOT
146+09.80	CL	1	0.0	27
146+20.70	RT	1	78.2	21
146+74.30	LT	3	78.1	21
146+80.20	CL	1	0.0	27
146+91.00	RT	1	78.3	21
147+40.10	CL	1	0.0	27
147+40.20	LT	3	96.6	21
148+00.50	LT	3	100.8	21
148+85.90	RT	1	81.6	21
149+34.90	RT	1	78.0	21
149+60.70	CL	1	0.0	27
149+74.90	LT	3	85.4	23
150+14.70	CL	1	0.0	27
150+24.60	LT	3	84.9	23
150+44.70	RT	1	78.0	21
150+70.20	CL	1	0.0	27
150+75.40	LT	3	85.4	23
150+99.70	RT	1	78.0	21
151+25.10	CL	1	0.0	27
151+25.20	LT	3	84.9	23
151+54.90	RT	1	78.0	21
151+75.50	LT	3	84.4	23
151+80.10	CL	1	0.0	27
152+10.20	RT	1	78.0	21
152+25.70	LT	3	79.6	23
152+35.10	CL	1	0.0	27
152+65.50	RT	1	78.0	21
152+75.50	LT	3	79.6	23
152+94.10	CL	1	0.0	27
153+20.10	RT	1	78.6	21
153+25.20	LT	3	78.6	23
153+45.30	CL	1	0.0	27
153+75.30	RT	1	78.6	21
153+75.50	LT	3	78.6	21
154+00.60	CL	1	0.0	27
154+26.30	LT	3	78.6	21
154+30.50	RT	1	77.9	23
154+54.70	CL	1	0.0	27
154+75.00	LT	3	78.6	21
154+80.90	RT	1	78.5	23
155+09.30	CL	1	0.0	27
155+24.80	LT	3	78.6	21
155+40.90	RT	1	77.9	23
155+63.80	CL	1	0.0	27
155+75.00	LT	3	78.1	21
156+19.90	CL	1	0.0	27
156+25.30	LT	3	78.6	21
156+75.10	LT	3	78.1	21
156+76.40	CL	1	0.0	27
157+29.70	LT	3	77.8	21
157+30.10	CL	1	0.0	27
157+84.70	LT	3	78.1	21
157+85.20	CL	1	0.0	27
158+44.80	CL	1	0.0	27
158+50.60	LT	3	77.6	21
159+10.70	CL	1	0.0	27
159+21.00	LT	3	77.9	21
159+95.40	CL	1	0.0	27
159+98.00	LT	3	82.1	21
162+41.30	LT	3	84.3	23
163+39.70	LT	3	88.3	23
163+39.70	CL	1	3.9	27
163+41.10	LT	3	88.3	23
163+41.10	CL	1	3.9	27
164+52.20	LT	3	79.0	23
166+21.30	LT	3	94.3	23
166+58.30	RT	1	98.0	23
167+73.60	LT	3	100.8	23
169+20.60	LT	3	99.8	23
170+97.10	LT	3	91.2	23
171+78.90	LT	3	92.0	23
172+20.20	RT	1	78.8	21
172+72.50	LT	3	93.2	23
173+30.00	RT	1	78.2	21
173+47.50	RT	3	7.4	21

TEMPORARY DITCH CHECKS				
FAP Route	310	Stage	offset	FOOT
173+60.10	LT	3	93.1	23
174+40.20	RT	3	78.0	21
174+50.40	LT	3	93.0	23
174+58.80	RT	3	5.0	21
175+12.60	RT	3	4.7	21
175+50.20	RT	1	77.8	21
175+60.10	CL	3	0.0	27
175+60.20	LT	3	80.0	23
176+60.00	RT	1	78.2	21
177+62.20	CL	3	0.0	27
177+80.30	RT	3	78.6	21
178+10.80	LT	3	87.1	23
178+94.10	CL	3	0.0	27
179+69.70	LT	3	77.6	23
180+85.60	CL	3	0.0	27
181+89.90	RT	3	82.4	23
185+38.90	RT	3	81.9	17
186+22.00	LT	1	79.7	23
186+87.90	CL	1	0.0	27
186+89.60	LT	1	84.6	23
187+09.90	LT	1	85.5	23
187+50.30	RT	3	78.6	23
189+91.20	CL	1	0.0	27
190+13.70	CL	1	0.0	27
190+80.70	LT	1	93.8	23
192+90.50	CL	1	0.0	27
193+10.90	CL	1	0.0	27
195+00.20	CL	1	0.0	27
196+54.10	LT	1	3.1	23
197+10.10	LT	1	78.2	23
197+88.10	CL	1	4.0	23
198+40.60	LT	1	78.2	23
199+06.20	LT	1	3.4	23
199+71.00	LT	1	77.6	23
200+40.40	LT	1	3.8	23
200+99.60	LT	1	78.2	23
203+40.10	RT	3	78.1	23
203+66.20	RT	3	9.3	23
204+70.00	RT	3	78.3	23
204+85.00	LT	1	84.6	23
204+94.80	RT	3	8.1	23
205+50.60	LT	1	78.3	23
206+35.20	LT	1	80.1	23
206+51.00	RT	3	8.0	23
211+86.70	LT	1	79.9	21
212+71.90	CL	3	0.0	27
212+88.70	RT	3	77.8	21
212+92.00	LT	1	80.0	21
214+08.80	CL	3	0.0	27
214+21.00	RT	3	78.1	21
214+24.20	LT	1	79.9	21
215+15.20	CL	3	0.0	27
215+26.00	RT	3	77.9	21
215+28.90	LT	1	80.6	21
216+03.60	CL	3	0.0	27
216+15.50	RT	3	77.8	21
216+19.40	LT	1	80.2	21
216+84.30	CL	3	0.0	27
216+98.90	RT	3	77.9	21
217+00.00	LT	1	77.9	21
217+54.00	CL	3	0.0	27
217+67.50	LT	1	78.0	21
217+67.70	RT	3	77.9	21
218+19.70	CL	3	0.0	27
218+31.70	RT	3	77.3	23
218+33.50	LT	1	78.4	21
218+80.40	CL	3	0.0	27
218+91.90	LT	1	77.6	23
218+93.70	RT	3	77.5	23
219+39.60	CL	3	0.0	27
219+49.10	LT	1	77.5	23
220+00.20	CL	3	0.0	27
220+04.80	LT	1	78.1	23
220+41.80	RT	3	82.0	23
220+55.00	CL	1	0.0	27

TEMPORARY DITCH CHECKS				
FAP Route	310	Stage	offset	FOOT
220+57.80	LT	1	78.0	23
220+98.70	RT	3	78.1	23
221+07.70	CL	1	0.0	27
221+14.40	LT	1	78.2	23
221+34.80	RT	3	79.1	23
221+65.30	CL	1	0.0	27
221+67.60	LT	1	78.3	23
222+15.80	RT	3	78.3	23
222+19.90	CL	1	0.0	27
222+24.30	LT	1	78.6	23
222+72.60	RT	3	77.9	23
222+75.10	CL	1	0.0	27
222+76.80	LT	1	78.5	23
223+25.50	RT	3	77.8	23
223+29.60	CL	1	0.0	27
223+41.00	LT	1	78.5	23
223+80.60	RT	3	77.8	23
223+85.00	CL	1	0.0	27
224+02.40	LT	1	80.1	23
224+35.20	RT	3	77.8	23
224+40.20	RT	1	11.1	23
224+73.60	LT	1	85.7	23
224+89.90	RT	3	77.8	23
225+80.20	LT	1	77.6	21
226+35.30	LT	1	6.9	21
226+40.10</				

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	25
STA.		TO STA.		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				

FURNISHING AND ERECTING RIGHT-OF-WAY				
US 67 (Godfrey Road)		Offset (ft)	EACH	
48+00.00	Lt	40	1	
48+00.00	Rt	53	1	
57+71.86	Rt	123	1	
59+39.35	Rt	144.9	1	
171+68.80	Lt	45	1	
172+70.00	Lt	50	1	
173+77.25	Rt	58	1	
		SubTotal	7	
<b>IL III</b>				
1173+77.31	Lt	95	1	
1178+41.54	Lt	95	1	
1179+89.32	Lt	124	1	
1188+75.75	Lt	95	1	
1193+55.59	Lt	75	1	
1195+51.89	Lt	75	1	
		SubTotal	6	
<b>Access Road 3</b>				
11+14.22	Rt	50	1	
15+13.89	Rt	50	1	
15+91.15	Rt	50	1	
17+38.23	Rt	50	1	
21+05.03	Rt	50	1	
<b>Ramp C</b>				
10+41.83	Rt	115.52	1	
10+51.67	Rt	75	1	
12+50.14	Rt	75	1	
14+66.68	Rt	75	1	
17+15.75	Rt	75	1	
<b>Benjamin Road</b>				
10+60.00	Lt	35	1	
10+80.00	Rt	35	1	
11+65.00	Lt	25.12	1	
11+65.00	Lt	35	1	
11+65.00	Rt	24.88	1	
11+65.00	Rt	35	1	
<b>FAP Route 310</b>				
78+82.47	Rt	268.06	1	
77+03.43	Rt	181.5	1	
79+03.10	Lt	180.73	1	
79+67.37	Rt	154.33	1	
79+92.44	Rt	152.92	1	
80+08.92	Lt	170.03	1	
82+71.74	Lt	155.84	1	
83+07.31	Lt	198.94	1	
83+47.20	Lt	151.72	1	
85+44.02	Lt	140.98	1	
87+02.24	Rt	136.87	1	
91+53.28	Rt	127.84	1	
93+80.62	Rt	265.3	1	
106+88.60	Lt	120.03	1	
107+41.67	Lt	80	1	
111+00.00	Lt	80	1	
112+00.00	Lt	120	1	
115+73.63	Lt	120	1	
120+20.70	Lt	155.32	1	
120+48.50	Lt	120	1	
121+18.92	Lt	232.64	1	
122+08.51	Lt	120	1	
125+80.00	Rt	100	1	
125+80.00	Rt	150	1	
127+70.65	Rt	100	1	
127+70.66	Lt	120	1	
131+00.00	Rt	100	1	
133+14.00	Lt	120	1	
133+14.00	Rt	120	1	
141+27.05	Rt	120	1	
141+29.04	Lt	120	1	
141+38.25	Lt	105	1	
141+68.66	Rt	157.14	1	
141+81.82	Lt	105	1	
152+60.94	Lt	180	1	
158+11.69	Rt	190	1	
158+11.70	Lt	180	1	
160+46.84	Rt	190	1	
160+83.66	Rt	140	1	
161+58.05	Lt	180	1	
161+68.17	Lt	150	1	
163+79.84	Rt	140	1	
163+79.85	Lt	150	1	
169+55.27	Rt	140	1	
172+63.86	Lt	150	1	
172+30.00	Rt	150	1	
173+58.41	Rt	150	1	
175+00.00	Lt	150	1	
180+00.00	Lt	115	1	

FURNISHING AND ERECTING RIGHT-OF-WAY				
FAP Route 310				
182+76.78	Lt	115	1	
183+82.41	Rt	259.75	1	
183+90.84	Rt	135	1	
184+73.14	Rt	291.01	1	
187+68.97	Lt	115	1	
187+68.97	Rt	135	1	
188+50.00	Rt	67	1	
198+63.23	Lt	192.81	1	
198+63.25	Lt	115	1	
203+06.36	Lt	145	1	
203+20.59	Rt	87	1	
204+50.46	Rt	448.77	1	
209+86.30	Rt	190	1	
210+84.23	Lt	190	1	
214+37.11	Lt	190	1	
219+00.00	Lt	140	1	
223+09.32	Lt	140	1	
223+09.32	Rt	190	1	
224+36.97	Rt	190	1	
224+61.71	Rt	120	1	
227+00.00	Rt	120	1	
229+00.00	Rt	170	1	
232+20.02	Rt	170	1	
233+55.74	Lt	140	1	
<b>Access Road 2</b>				
11+03.22	Lt	60	1	
15+02.89	Lt	60	1	
15+77.99	Lt	60	1	
17+34.60	Lt	60	1	
21+83.44	Lt	60	1	
21+85.11	Lt	65.36	1	
21+97.58	Lt	60	1	
21+98.00	Lt	61.31	1	
25+99.68	Lt	60	1	
32+20.51	Lt	60	1	
33+00.00	Lt	60	1	
35+00.00	Lt	60	1	
41+09.06	Lt	45	1	
<b>Beuttel Road</b>				
5+00.00	Lt	40	1	
5+00.00	Rt	40	1	
6+90.00	Lt	40	1	
6+90.00	Lt	222.01	1	
6+95.00	Rt	40	1	
7+35.00	Lt	214.27	1	
7+50.00	Lt	40	1	
8+48.31	Lt	81.08	1	
8+63.81	Rt	72.91	1	
<b>Davis Lane</b>				
2+61.81	Lt	60.95	1	
2+61.96	Lt	20.95	1	
2+62.15	Rt	26.55	1	
2+62.20	Rt	41.55	1	
5+79.29	Rt	35.67	1	
7+96.42	Rt	41.09	1	
<b>Access Road 1</b>				
11+10.20	Lt	50	1	
13+47.34	Lt	50	1	
14+26.11	Rt	50	1	
14+98.43	Lt	50	1	
14+98.43	Rt	50	1	
17+13.63	Rt	50	1	
17+13.63	Lt	50	1	
17+69.78	Rt	50	1	
17+75.35	Rt	68.8	1	
18+64.72	Lt	50	1	
36+19.46	Lt	50	1	
37+72.22	Rt	69.45	1	
37+76.48	Rt	50	1	
41+70.13	Rt	50	1	
41+70.13	Lt	50	1	
44+64.82	Lt	50	1	
44+65.02	Rt	50	1	
45+70.61	Rt	90	1	
45+92.00	Rt	69.46	1	
47+59.00	Rt	43.33	1	
48+81.17	Rt	40	1	
49+84.25	Rt	40	1	
50+37.12	Rt	160.96	1	
52+75.11	Rt	40	1	
52+82.70	Rt	58.14	1	
60+99.88	Rt	37.88	1	
65+08.04	Rt	39.32	1	
67+72.48	Rt	40	1	
67+72.48	Rt	45	1	

FURNISHING AND ERECTING RIGHT-OF-WAY				
Ingham Road				
7+72.20	Rt	35.96	1	
9+10.32	Rt	34.35	1	
10+90.31	Rt	32.27	1	
11+60.00	Rt	31.46	1	
11+60.00	Lt	30	1	
<b>Lagemann Road</b>				
12+03.48	Rt	90	1	
12+45.00	Lt	90	1	
14+55.00	Rt	90	1	
15+67.00	Lt	72.75	1	
17+50.00	Rt	50	1	
19+46.47	Lt	56.01	1	
19+87.71	Lt	353.16	1	
20+05.90	Lt	47.76	1	
20+25.78	Lt	45	1	
22+10.87	Rt	37.36	1	
23+19.66	Lt	45	1	
24+25.00	Lt	25	1	
24+25.00	Lt	45	1	
24+25.00	Rt	25	1	
24+25.00	Rt	40	1	
		SubTotal	166	
		Total	179	

MULCH, METHOD 2				
FAP Route 310			ACRE	
<b>Stage 1</b>				
97+50	to	228+25	12.05	
<b>Stage 2</b>				
102+50	to	228+50	2.27	
<b>Stage 3</b>				
91+50	to	225+00	8.92	
<b>RAMP A</b>				
07+50	to	18+50	1.24	
<b>RAMP B</b>				
<b>Stage 2</b>				
10+00	to	33+00	2.31	
<b>Stage 3</b>				
10+00	to	15+50	0.26	
<b>IL III</b>				
<b>Stage 1</b>				
1172+50	to	1179+40	0.52	
<b>Stage 2</b>				
1172+50	to	1179+40	0.48	
<b>US 67</b>				
<b>Stage 1</b>				
165+50	to	57+00	1.91	
<b>Stage 2</b>				
54+50	to	57+50	0.04	
<b>Stage 3</b>				
56+50	to	57+50	0.05	
<b>Access Road 1</b>				
10+87	to	61+50	0.35	
<b>Access Road 2</b>				
10+18	to	41+00	0.21	
<b>Access Road 3</b>				
10+43	to	17+00	0.02	
<b>Benjamin Road</b>				
10+50	to	11+50	0.01	
<b>Beuttel Road</b>				
05+20	to	08+50	0.07	
<b>Davis Lane</b>				
02+75	to	09+00	0.20	
<b>Ingham Road</b>				
07+40	to	11+50	0.04	
<b>Lagemann Road</b>				
10+51	to	24+00	0.76	
<b>Special Ditch</b>				
10+65	to	14+30	0.34	
		Total	32.0	

REMOVAL OF EXISTING STRUCTURES NO. 1			
US 67 (Godfrey Road)			EACH
57+50.00			1
		Total	1

STONE RIPRAP SCHEDULE				
LOCATION	CLASS A 4	FILTER FABRIC		
		SQ YD	SQ YD	SQ YD
FAP 310				
147+50.00	98	Rt	295	295
187+10.00	112	Rt	95	95
<b>Access Road 1</b>				
60+25.00	25	Rt	100	100
<b>Access Road 2</b>				
23+72.82	18	Lt	16	16
<b>Lagemann Service Drive</b>				
01+07.00	12	Rt	36	36
03+09.00	12	Rt	36	36
<b>IL III</b>				
1174+60.00	46	Lt	14	14
TOTALS			592	592

GEOTECHNICAL FABRIC FOR GROUND STABILIZATION							
Station - Ramp B	Side	Dist	Width	Area	Quantity	Total	Total
		ft	ft	sq ft	1ft Vertical	sq ft	sq yd
22+35	Rt		9		3		
22+50	Rt	15	9	135	3	405	45
23+00	Rt	50	9	450	11	3150	350
23+15	Rt	15	9	135	11	1485	165
					Total	5040	560

SEEDING, CLASS 2				
FAP Route 310			ACRE	
<b>Stage 1</b>				
97+50	to	228+25	19.88	
<b>Stage 2</b>				
105+50	to	228+50	3.45	
<b>Stage 3</b>				
91+50	to	225+00	14.25	
<b>RAMP A</b>				
08+00	to	18+50	1.89	
<b>RAMP B</b>				
<b>Stage 2</b>				
10+00	to	33+00	4.75	
<b>Stage 3</b>				
10+00	to	15+50	0.81	
<b>IL III</b>				
<b>Stage 1</b>				
1172+50	to	1179+40	0.77	
<b>Stage 2</b>				
1172+50	to	1179+40	0.73	
<b>US 67</b>				
<b>Stage 1</b>				
165+50	to	57+00	2.41	
<b>Stage 2</b>				
54+50	to	57+50	0.04	
<b>Stage 3</b>				
56+50	to	57+50	0.05	
<b>Access Road 1</b>				
10+87	to	61+50	3.42	
<b>Access Road 2</b>				
10+18	to	41+00	3.	

SIGNING AND WOOD SIGN SUPPORTS					
STATION	SIGN	TYPE 1 SQ FT	TYPE 2 SQ FT	TYPE 3 SQ FT	WOOD SIGN SUPPORT FOOT
<i>US 67 (Godfrey Road)</i>					
164+00 RT	W3-3-36	9.00			16
165+45 LT	D2-1-6018	7.50			25
165+50 RT	M1-4-2424	4.00			15
	M6-3-2115	2.19			
	M1-1100-3024	5.00			
	M5-1R-2115	2.19			
167+35 LT	R2-1-2430	5.00			14
168+35 RT	R3-1100R-2424	4.00			14
168+75 RT	D1-2-8430		17.50		28
169+25 LT	M3-3-2412 (2)	4.00			15
	M1-1100-3024	5.00			
	M1-4-2424	4.00			
170+25 RT	M1-1100-3024	5.00			15
	M6-1-2115	2.19			
171+50 RT	M1-4-2424	4.00			15
	M6-3-2115	2.19			
Traffic Signals	R10-12-2430 (4)	20.00			On Mast Arms
	Street Name (3)		37.50		On Mast Arms
173+00 LT	M1-4-2424	2.00			15
	M6-3-2115	2.19			
	M1-1100-3024	5.00			
	M6-6L-2115	2.19			
174+00 RT	M3-1-2412	2.00			15
	M1-4-2424	4.00			
175+00 LT	D1-2-8430		17.50		28
176+00 RT	R2-1-2430	5.00			14
177+00 LT	M2-1-3015	3.13			17
	M1-1100-3636	9.00			
	M6-6L-3015	3.13			
46+70 RT	W4-2-36	9.00			16
47+00 LT	W3-3-36	9.00			16
55+00 LT	R2-1-2430	5.00			17
SUBTOTAL		145.9	72.5	0.0	295.0
<i>IL III</i>					
1172+85 LT	M1-4-2424	4.00			15
	M6-4-2115	2.19			
	M1-1100-3024	5.00			
	M6-1-2115	2.19			
1173+17 RT	W4-3R-36	9.00			16
1173+40 RT	W4-3R-36	9.00			16
1175+00 LT	D1-2-8430		17.50		35
1176+00 LT	M3-1-2412	2.00			17
	M1-1100-3024	5.00			
1177+00 LT	M1-1100-3024	5.00			18
	M5-1L-2115	2.19			
1178+00 RT	R2-1-2430	5.00			16
1178+50 LT	W3-3-36	9.00			17
1180+00 LT	M2-1-3015	3.13			19
	M1-4-3636	9.00			
1180+00 RT	D2-1-7218	9.00			32
SUBTOTAL		80.7	17.5	0.0	201.0
<i>Ramp A</i>					
7+00 LT	E5-1-3636	9.00			16
15+00 RT	M3-3-2412	2.00			17
	M1-4-2424	4.00			
<i>FAP Route 310</i>					
94+00 LT	M3-3-3015	3.13	11.25		18
	M1-1100-4536				
104+75 LT	M3-3-3015	3.13			20
	M1-4-3636	9.00			
	M6-2-3018	3.75			
104+75 LT	M3-3-3015	3.13			21
	M1-1100-4536		11.25		
	M6-3-3018	3.75			
105+00 RT	W4-1-48		16.00		18
111+00 LT	M3-3-3015 (2)	6.26			41
	M1-1100-4536		11.25		
	M6-3-3018	3.75			
	M5-2(R)-3018	3.75			
	M1-4-3636	9.00			
117+00 LT	E-SPECIAL-12084			70.00	43
119+00 LT	R6-2-1824	3.00			16
121+50 LT	R6-2-1824	3.00			16
123+00 LT	M2-1-3015	3.13			18
	M1-1100-4536		11.25		
125+90 LT	R6-2-1824	3.00			16
126+00 RT	M3-1-3015	3.13			18
	M1-4-3636	9.00			
128+50 RT	R6-2-1824	3.00			16
130+80 LT	R6-2-1824	3.00			16
131+00 RT	R2-1-3648		12.00		21
	R2-2-3636	9.00			

SIGNING AND WOOD SIGN SUPPORTS					
STATION	SIGN	TYPE 1 SQ FT	TYPE 2 SQ FT	TYPE 3 SQ FT	WOOD SIGN SUPPORT FOOT
<i>FAP Route 310</i>					
136+00 RT	D2-2-8430		17.50		35
136+80	R6-2-1824	3.00			16
138+00 RT	W2-2L-48 (2)	32.00			40
	W17-1101-2415 (2)	5.00			
141+00 LT	R2-1-3648		12.00		21
	R2-2-3636	9.00			
143+00 LT	R6-2-1824	3.00			16
143+00 RT	D1-1-8430		10.50		32
144+20 LT	R6-2-1824	3.00			16
145+45 RT	R3-1100L-2424	4.00			16
146+00 LT	M3-3-3015	3.13			18
	M1-4-3636	9.00			
147+10 RT	R5-1A-3624 (2)	12.00			28
147+60 RT	R5-1-3030 (2)	12.50			28
147+90 LT/RT	R6-1R-3612 (2)	6.00			28
149+15 RT	R6-1R-3612	3.00			14
149+50 LT	R5-1-3030 (2)	12.50			28
150+00 LT	R5-1A-3624 (2)	12.00			28
151+00 RT	M3-1-3015	3.13			18
	M1-4-3636	9.00			
151+44 LT	R3-1100L-2424	4.00			16
151+55 LT	R3-1100R-2424	4.00			16
154+00 LT	D1-1-8418		10.50		32
155+00 RT	R2-1-3648		12.00		21
	R2-2-3636	9.00			
158+00 RT	W2-2R-36 (2)	18.00			40
	W17-1101-2415 (2)	6.25			
159+00 LT	W2-2R-36 (2)	18.00			40
	W17-1101-2415 (2)	6.25			
163+00 LT	R2-1-3648		12.00		21
	R2-2-3636	9.00			
163+00 RT	D1-1-8418		10.50		32
164+00 RT	R3-1100L-2424	4.00			16
166+78 RT	R3-1100R-2424	4.00			16
167+85 RT	R4-7-2430	5.00			16
168+00 LT	M3-3-3015	3.13			18
	M1-4-3636	9.00			
169+50 LT	R4-7-2430	5.00			16
	R5-1A-3624 (2)	12.00			28
169+80 RT	R5-1-3030	6.25			14
170+00 RT	R5-1-3030	6.25			14
	R6-1R-3612	3.00			14
171+00 LT	R6-1R-3612	3.00			14
	R5-1-3030 (2)	12.50			28
171+50 LT	R5-1A-3624 (2)	12.00			28
171+60 RT	R4-7-2430	5.00			16
172+80 LT	R4-7-2430	5.00			16
173+00 RT	M3-1-3015	3.13			18
	M1-4-3636	9.00			
175+65 LT	R3-1100L-2424	4.00			16
177+50 LT	D1-1-8418		10.50		32
178+00 RT	R2-1-3648		12.00		21
	R2-2-3636	9.00			
179+35 LT	R6-2-1824	3.00			16
182+50 LT	W2-2L-48 (2)	32.00			40
	W17-1101-3015 (2)	6.25			
184+50 RT	R6-2-1824	3.00			16
189+25 RT	R6-2-1824	3.00			16
191+75 RT	R6-2-1824	3.00			16
192+25 RT	W2-2L-48 (2)	32.00			40
	W17-1101-2415 (2)	5.00			
193+75 RT	R6-2-1824	3.00			16
195+00 LT	R2-1-3648		12.00		21
	R2-2-3636	9.00			
197+00 RT	D1-1-7218	9.00			32
199+09 RT	R3-1100L-2424	4.00			16
200+00 LT	M3-3-3015	3.13			18
	M1-4-3636	9.00			
202+89 RT	R6-1R-3612	3.00			14
	R5-1-3030	6.25			14
	R5-1-3030	6.25			14
202+95 RT	R5-1A-3624 (2)	12.00			28
203+40 LT	M3-3-3015	3.13			18
205+00 RT	M1-4-3636	9.00			
	R3-1100R-2424	4.00			16
205+16 LT	R3-1100L-2424	4.00			16
207+00 RT	R6-2-1824	3.00			16
208+00 LT	D-1-7218	9.00			32
212+25 LT	R6-2-1824	3.00			16
213+00 LT	W2-2R-48 (2)	32.00			40
	W17-1101-2415 (2)	5.00			
217+00 LT	R2-1-3648		12.00		21

SIGNING AND WOOD SIGN SUPPORTS					
STATION	SIGN	TYPE 1 SQ FT	TYPE 2 SQ FT	TYPE 3 SQ FT	WOOD SIGN SUPPORT FOOT
<i>FAP Route 310</i>					
220+50 RT	R5-1A-3624 (2)	12.00			28
221+00 LT	M3-3-3015	3.13			18
	M1-4-3636	9.00			
221+28 RT	R6-1R-3612	3.00			16
222+05 RT	R5-1-3030 (2)	12.50			28
	R1-1-30	6.25			16
	R1-1100-3018	3.75			
<i>Beuttel Road</i>					
6+80 RT	W3-1A-30	6.25			16
8+00 LT	R2-1-2430	5.00			16
9+23 RT	R1-1-30	6.25			18
	R1-1100-3018	3.75			
<i>Lagemann Road</i>					
11+22 LT	R1-1-30	6.25			18
	R1-1100-3018	3.75			
13+00 RT	R2-1-2430	5.00			16
14+20 LT	W3-1A-30	6.25			16
17+50 LT	W1-2R-30	6.25			16
<i>Davis Lane</i>					
5+00 RT	W3-1A-30	6.25			16
8+00 LT	R2-1-2430	5.00			16
9+04 RT	R1-1-30	6.25			18
	R1-1100-3018	3.75			
<i>Access Road 1</i>					
10+62 LT	R1-1-30	6.25			18
	R1-1100-3018	3.75			
11+50 RT	W1-3R-30	6.25			16
14+50 LT	W3-1A-30	6.25			16
23+00 LT	W1-3R-30	6.25			16
33+00 RT	W1-2L-30	6.25			16
39+00 RT	W3-1A-30	6.25			16
44+98 RT	R1-1-30	6.25			14
	R1-4-2109	1.31			
45+79 LT	R1-1-30	6.25			14
	R1-4-2109	1.31			
46+30 RT	W14-1-30	6.25			16
51+00 LT					

GUARDRAIL & BARRIER WALL MARKER SCHEDULE								
ROUTE	FAP 310		FAP 310		FAP 310		FAP 310	
LOCATION	NB RT SHOULDER		NB MD SHOULDER		SB RT SHOULDER		SB MD SHOULDER	
SPACING*	150'		150'		150'		150'	
NUMBER	STA	TYPE	STA	TYPE	STA	TYPE	STA	TYPE
1	157+00.00	GRS	156+25.00	GRA	169+20.00	GRS	167+90.00	GRA
2	157+80.00	GRS	157+05.00	GRA	168+40.00	GRS	167+10.00	GRA
3	158+60.00	GRS	157+85.00	GRA	167+60.00	GRS	166+30.00	GRA
4	159+40.00	GRS	158+65.00	GRA	166+80.00	GRS	165+50.00	GRA
5	160+20.00	BWS	159+45.00	GRA	166+00.00	BWS	164+70.00	GRA
6	161+70.00	GRS	160+95.00	GRA	164+50.00	GRS	163+20.00	GRA
7	163+20.00	GRS	162+45.00	GRA	163+00.00	GRS	161+70.00	GRA
8	164+70.00	GRS	163+95.00	GRA	161+50.00	GRS	160+20.00	GRA
9	166+20.00	GRS	165+45.00	GRA	160+00.00	GRS		
10	167+70.00	GRS	166+95.00	GRA	92+25.00	GRS		
11					91+45.00	GRS		
TOTAL GR	9	S	10	A	10	S	8	A
TOTAL BW	1	S	0		1	S	0	

GUARDRAIL & BARRIER WALL MARKER SCHEDULE								
ROUTE	FAP 310		FAP 310		Ramp B		Ramp B	
LOCATION	NB MD SHOULDER		SB MD SHOULDER		RT SHOULDER		MD SHOULDER	
SPACING*	125'		125'		125'		125'	
NUMBER	STA	TYPE	STA	TYPE	STA	TYPE	STA	TYPE
1	89+70.00	GRA	97+00.00	GRA	11+50.00	GRS	11+00.00	GRA
2	90+50.00	GRA	96+20.00	GRA	12+30.00	GRS	11+80.00	GRA
3	91+30.00	GRA	95+40.00	GRA	13+10.00	GRS	12+60.00	GRA
4	92+10.00	GRA	94+60.00	GRA	13+90.00	GRS	13+40.00	GRA
5	92+90.00	GRA	93+80.00	GRA	14+70.00	GRS	14+20.00	GRA
6					15+95.00	BWS	15+45.00	BWA
7					17+20.00	BWS	16+70.00	BWA
8					18+45.00	BWS	17+95.00	BWA
9					19+70.00	BWS	19+20.00	BWA
10					20+95.00	BWS	20+45.00	BWA
11					22+20.00	GRS	21+70.00	BWA
12							22+95.00	GRA
TOTAL GR	5	A	5	A	6	S	6	A
TOTAL BW	0		0		5	S	6	A

• AFTER INTIAL 320' FROM STD 635001 & STD 635006

LEGEND  
 GR GUARDRAIL MARKER  
 BW BARRIER WALL MARKER  
 S SILVER OR CHRYSTAL COLOR  
 A AMBER COLOR

GUARDRAIL & BARRIER WALL MARKER SCHEDULE						
ROUTE	FAP 310		Access Road No. 1		FAP 310	
LOCATION	NB RT SHOULDER		SB RT SHOULDER		NB RT SHOULDER	
SPACING*	160'		160'		80'	
NUMBER	STA	TYPE	STA	TYPE	STA	TYPE
1	116+00.00	BWS	73+20.00	BWS	182+50.00	GRS
2	116+80.00	BWS	72+40.00	BWS	183+30.00	GRS
3	117+60.00	BWS	71+60.00	BWS	184+10.00	GRS
4	118+40.00	BWS	70+80.00	BWS	184+90.00	GRS
5	119+20.00	BWS	70+00.00	BWS	185+70.00	GRS
6	120+80.00	BWS	68+40.00	BWS	186+50.00	GRS
7	122+40.00	BWS	66+80.00	BWS	187+30.00	GRS
8	124+00.00	BWS	65+20.00	BWS		
9	125+60.00	BWS	63+60.00	BWS		
TOTAL GR	0		0		7	S
TOTAL BW	9	S	9	S	0	

GUARDRAIL & BARRIER WALL MARKER SCHEDULE				
ROUTE	FAP Route 310		FAP Route 310	
LOCATION	NB MD SHOULDER		SB MD SHOULDER	
SPACING*	150'		150'	
NUMBER	STA	TYPE	STA	TYPE
1	151+00.00	GRS	170+50.00	GRS
2	151+80.00	GRS	171+30.00	GRS
3	152+60.00	GRS	172+10.00	GRS
4	153+40.00	GRS	172+90.00	GRS
5	154+20.00	GRS	173+70.00	GRS
6	155+95.00	GRS	175+20.00	GRS
7	157+70.00	GRS	176+70.00	GRS
8			178+20.00	GRS
9				
10				
11				
12				
TOTAL GR	7	S	8	S
TOTAL BW	0		0	

PAVEMENT REMOVAL			
US 67 (Godfrey Road)		CAD sq(fft)	SQ YD
168+00.00	Rt turn lane	15404	1711.6
		SubTotal	1712
US 67 (Godfrey Rd)		F.A.P. Route 310	
56+33.49	to	160+40.51	168583 18731.4
FAP Route 310			
160+94.29	to	165+74.25	11500 1102.4
166+30.53	to	225+00.00	133140 14713.9
210+02.35	to	219+51.80	16633 1848.1
		Existing Lagemann Road	33202 3689.1
Connector Detail No. 1		width (fft)	
136+00.00	to	140+08.00	24 1220.2
Connector Detail No. 2			
00+00.00	to	3+32.00	24 957.7
Cross Over Detail No. 1			
01+12.00	to	5+90.00	12 454.1
Cross Over Detail No. 2			
02+59.00	to	11+59.00	24 2396.0
Cross Over Detail No. 3			
03+50.00	to	9+08.00	24 1573.0
Cross Over Detail No. 4			
00+00.00	to	11+56.14	24 1870.0
Ramp A		Temporary Pavement	
			1717.09 190.8
Business US 67 (IL 111)			
			1342.1 149.1
			SubTotal 48896
			Total 50608

CONCRETE MEDIAN SURFACE, 4 INCH			
FAP Route 310			SQ FT
167+00.00	to	169+60.51	2279.3
167+74.89	to	170+13.54	2919.34
170+95.62	to	172+88.05	2282.56
171+38.50	to	174+50.00	3813.26
IL 111			
1172+84.72	to	1173+23.04	355.91
1173+23.04	to	1179+40.00	8020.48
		Total	19671

MEDIAN REMOVAL			
IL 111		width(fft)	SQ FT
1172+65.00	to	1179+40.00	5 3375.0
		Total	3375

GUTTER REMOVAL				
US 67 (Godfrey Road)				FOOT
165+45.00	to	175+77.00	Lt	1032.0
165+45.00	to	167+71.74	Rt	226.7
166+40.00		Rt turn lane		865.0
		SubTotal		2124
F.A.P. Route 310				
167+00.00	to	228+38.69	Lt	6138.7
167+00.00	to	228+13.00	Rt	6113.0
Ramp B				
13+60.31	to	16+51.51	Rt	291.2
		SubTotal		12543
		Total		14667

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24				
F.A.P. Route 310				FOOT
167+00.00	to	169+60.51	Rt/Lt	260.5
167+77.35	to	169+99.54	Rt/Lt	222.0
169+99.54	to	170+13.54	Lt/Rt	34.4
170+95.62	to	171+06.42	Lt/Rt	32.6
171+06.42	to	172+88.05	Rt/Lt	183.7
171+38.50	to	174+50.00	Rt/Lt	312.9
		Total		1046.0

CONCRETE GUTTER, TYPE A			
FAP Route 310			FOOT
107+00.00	to	111+70.00	Lt 470.0
187+98.00	to	189+08.64	Rt 110.6
189+34.25	to	191+61.08	Rt 226.8
191+86.69	to	193+65.15	Rt 178.5
193+90.75	to	197+64.90	Rt 374.2
197+98.52	to	199+19.90	Rt 121.4
199+45.52	to	200+68.14	Rt 122.6
200+91.75	to	202+21.16	Rt 129.4
202+46.77	to	203+32.00	Rt 85.2
		Total	1819.0

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24				
US 67 (Godfrey Road)				FOOT
167+11.43	to	170+26.17	Rt	314.7
165+45.00	to	171+54.43	Lt	609.4
172+06.10		SE Median	Rt	165.7
172+06.10		SE Quad	Rt	344.4
172+06.10		SW Quad	Lt	68.8
172+06.10		NW Quad	Lt	88.3
172+06.10		NE Median	Rt	76.1
172+06.10		NE Quad	Rt	210.4
172+86.13	to	177+10.39	Lt	424.3
173+98.71	to	177+10.39	Rt	311.7
45+00.00	to	48+21.00	Rt	321.0
45+00.00	to	48+21.00	Lt	321.0
48+86.00		SE Quad	Rt	78.5
48+86.00		SW Quad	Lt	78.5
		Subtotal		3412.83
Benjamin Drive				
10+62.63	to	11+50.00	Lt	87.4
10+84.15	to	11+50.00	Rt	65.9
		Subtotal		153.30

CLASS SI CONCRETE (OUTLET)				
IL 111	transition	tal(fft)	(0.069cy/fft)	CU YD
1174+25.00	Lt	2.41	25	4.14
Access Road 2				(0.079cy/fft)
10+85.00	Lt	2.41	10	3.10
Access Road 3				(0.079cy/fft)
11+15.00	Rt	2.41	10	3.10
Entrances		depth (ft)	area (ft^2)	
189+08.64	to	189+34.25	0.5	141.76
191+61.08	to	191+86.69	0.5	141.75
193+65.15	to	193+90.75	0.5	141.75
197+64.90	to	197+98.52	0.5	193.78
199+19.90	to	199+45.52	0.5	141.75
200+68.14	to	200+91.75	0.5	128.75
202+21.16	to	202+46.77	0.5	141.75
		Total		29.4

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06				
F.A.P. Route 310				FOOT
167+00.00	to	169+60.51	Rt/Lt	258.8
167+77.35	to	170+13.54	Rt/Lt	235.5
170+95.62	to	172+88.05	Rt/Lt	191.3
171+38.50	to	174+50.00	Rt/Lt	312.6
		Total		998.0

TEMPORARY RAMP					
US 67 (Godfrey Road)		Width (ft)	length(fft)	SQ YD	
175+00.00		64	5	35.6	
				SubTotal	36
				Total	36

Based on HMA Surface Course rate of 1000 tons a day

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24				
US 67 (Godfrey Road)				FOOT
167+11.43	to	170+26.17	(median Lt)	118.2
165+45.00	to	171+54.43	(median Rt)	116.0
172+06.10	to	173+98.71	(median Lt)	540.0
172+06.10	to	173+98.71	(median Rt)	540.0
		Subtotal		1314.20
Driveways				
169+69.00	Lt	US 67 (Godfrey Road)		79.4
175+00.00	Rt	US 67 (Godfrey Road)		67.5
175+41.00	Lt	US 67 (Godfrey Road)		58.3
46+08.00	Rt	IL 267 (Godfrey Road)		47.4
11+15.00	Rt	Benjamin Drive		44.3
		Subtotal		296.90
		Total		5177.0

COMBINATION CURB AND GUTTER REMOVAL			
IL 111			FOOT
1172+65.00	to	1179+40.00	Lt 675.0
1172+65.00	to	1179+40.00	Rt 675.0
		Total	1350

REVISIONS	
NAME	DATE

**DRAINAGE STRUCTURES SCHEDULE**

STRUCTURE NUMBER / STATION	STAGE CONSTRUCTION	INLETS								MANHOLES										OTHER				
		INLET TY A TY 3 F&G	INLET TY A TY 9 F&G	INLET TY A TY 15 F&L	INLET TY A TY 20 F&G	INLET TY B TY 3 F&G	INLET TY B TY 9 F&G	INLET TY B TY 15 F&L	INLET TY B TY 20 F&G	MH TY A 4-DIA TY 1 FRAME CL	MH TY A 4-DIA TY 8 GRATE	MH TY A 4-DIA TY 9 F&G	MH TY A 4-DIA TY 15 F&L	MH TY A 4-DIA TY 20 F&G	MH TY A 5-DIA TY 1 FRAME CL	MH TY A 5-DIA TY 1 FRAME OL	MH TY A 5-DIA TY 3 F&G	MH TY A 5-DIA TY 8 GRATE	MH TY A 5-DIA TY 9 F&G	MH TY A 5-DIA TY 15 F&L	MH TY A 5-DIA TY 20 F&G	REINF CONC PIPE TEE 21" PIPE W/ 12" RISER	REINF CONC PIPE TEE 24" PIPE W/ 24" RISER	DRAINAGE STRUCT TY 3 W/ TWO TY 21 F&G
		EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH
<i>U.S. 67</i>																								
1-1	STAGE 1													1										
1-2	STAGE 1				1																			
1-3	STAGE 1																				1			
1-4	STAGE 1								1															
1-5	STAGE 1													1										
1-6	STAGE 1				1																			
1-7	STAGE 1																				1			
1-8	STAGE 1								1															
1-11	STAGE 1																							
1-12	STAGE 1																				1			
1-15	STAGE 1													1										
1-16	STAGE 1													1										
1-17	STAGE 1				1																			
1-18	STAGE 1													1										
1-19	STAGE 1				1																			
1-20	STAGE 1													1										
1-21	STAGE 1																							
1-22	STAGE 1													1										
1-23	STAGE 1																							
1-24	STAGE 1													1										
1-25	STAGE 1																							
1-26	STAGE 1																							
1-34	STAGE 1																							
1-35	STAGE 1																							
1-36	STAGE 1				1																			
1-37	STAGE 1				1																			
1-38	STAGE 1																							
1-39	STAGE 1																							
SUBTOTALS		0	0	0	6	0	0	1	5	1	2	0	0	1	3	0	2	1	0	0	1	2	0	0
<i>IL 111</i>																								
1-27	STAGE 1																							
1-28	STAGE 1																							
1-29	STAGE 1								1															
1-30	STAGE 1																							
1-31	STAGE 1	1																						
1-32	STAGE 1				1																			
1-33	STAGE 1																							
SUBTOTALS		1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0
<i>BENJAMIN DRIVE</i>																								
1-9	STAGE 1	1																						
1-10	STAGE 1								1															
1-13	STAGE 1				1																			
1-14	STAGE 1								1															
SUBTOTALS		1	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<i>ACCESS ROAD 3</i>																								
2-1	STAGE 1																							
2-2	STAGE 1																							
2-3	STAGE 1																							
2-4	STAGE 1																							
SUBTOTALS		0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0

TYPE D INLET BOX, STANDARD 609006		
FAP 310		
	Side	EACH
15+33.00	LT	1
21+64.00	LT	1
	Total	2

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

## SCHEDULES OF QUANTITIES

SCALE: NO SCALE  
DATE: 02-06-09

DRAWN BY: C.J.C.  
CHECKED BY: J.J.C.

### DRAINAGE STRUCTURES SCHEDULE

STRUCTURE NUMBER / STATION	STAGE CONSTRUCTION	INLETS								MANHOLES										OTHER					
		INLET TY A TY 3 F&G	INLET TY A TY 9 F&G	INLET TY A TY 15 F&L	INLET TY A TY 20 F&G	INLET TY B TY 3 F&G	INLET TY B TY 9 F&G	INLET TY B TY 15 F&L	INLET TY B TY 20 F&G	MH TY A 4-DIA TY 1 FRAME CL	MH TY A 4-DIA TY 8 GRATE	MH TY A 4-DIA TY 9 F&G	MH TY A 4-DIA TY 15 F&L	MH TY A 4-DIA TY 20 F&G	MH TY A 5-DIA TY 1 FRAME CL	MH TY A 5-DIA TY 1 FRAME OL	MH TY A 5-DIA TY 3 F&G	MH TY A 5-DIA TY 8 GRATE	MH TY A 5-DIA TY 9 F&G	MH TY A 5-DIA TY 15 F&L	MH TY A 5-DIA TY 20 F&G	REINF CONC PIPE TEE 21" PIPE W/ 12" RISER	REINF CONC PIPE TEE 24" PIPE W/ 24" RISER	DRAINAGE STRUCT TY 3 W/ TWO TY 21 F&G	
		EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH
<i>FAP 310</i>																									
LT 94+51	STAGE 1																								
3-1	STAGE 1		1																						
3-2	STAGE 1								1																
3-3	STAGE 1								1																
3-4	STAGE 1																								
4-1	STAGE 1																								
4-2	STAGE 1																								1
4-3	STAGE 1																								1
4-4	STAGE 1																								1
4-5	STAGE 1																								
4-6	STAGE 1																								
5-1	STAGE 1			1																					
5-2	STAGE 1																								
5-3	STAGE 1																				1				
5-7	STAGE 1																								
5-8	STAGE 1																								
5-9	STAGE 1																								
5-12	STAGE 1																								
5-13	STAGE 1																								
5-14	STAGE 1			1																					
5-15	STAGE 1																								
5-17	STAGE 1																								
5-18	STAGE 1																								
SUBTOTALS		0	1	2	0	0	2	6	0	0	0	0	0	0	0	1	0	0	0	2	0	0	0	3	
<i>FAP 310 (LEFT TURN LANE)</i>																									
5-4	STAGE 1																								
5-5	STAGE 1																								
5-6	STAGE 1			1																					
5-10	STAGE 1																								
5-11	STAGE 1																								
5-16	STAGE 1																								
5-19	STAGE 1			1																					
SUBTOTALS		0	0	2	0	0	0	4	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	
<i>FAP 310</i>																									
222+00	STAGE 2																								
222+50	STAGE 2																								1
SUBTOTALS		0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	0	
<i>FAP 310</i>																									
6-1	STAGE 3																								
6-2	STAGE 3																								
6-3	STAGE 3																								
6-4	STAGE 3																								
6-5	STAGE 3																								
6-6	STAGE 3																								
6-7	STAGE 3																								
6-8	STAGE 3																								
6-9	STAGE 3																								
6-10	STAGE 3																								
6-11	STAGE 3																								
SUBTOTALS		0	0	0	0	0	2	0	0	0	0	2	0	0	0	0	0	0	5	0	0	0	0	0	
TOTALS		2	1	4	8	3	4	11	5	1	2	2	1	1	5	1	2	1	5	2	3	2	1	3	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

## SCHEDULES OF QUANTITIES

SCALE: NO SCALE  
DATE: 02-06-09

DRAWN BY: C.J.C.  
CHECKED BY: J.J.C.



ENTRANCE PIPE CULVERT SCHEDULE

STATION	STAGE CONSTRUCTION	PIPE CULVERTS							
		CLASS D							
		TYPE 1				TYPE 2			
		12"	15"	18"	24"	30"	36"	12"	24"
FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT		
<i>FAP 310</i>									
114+50.00	STAGE 1				54				
121+50.00	STAGE 1	48							
125+86.00	STAGE 1		48						
130+80.00	STAGE 1			48					
148+46.82	STAGE 1					46			
212+26.00	STAGE 1	36							
227+89.00	STAGE 1				54				
<i>Access Rd 1</i>									
12+62.00	STAGE 1	60							
15+00.00	STAGE 1	60							
17+51.00	STAGE 1	60							
18+27.00	STAGE 1	60							
29+00.00	STAGE 1	54							
59+50.00	STAGE 1	48							
72+28.00	STAGE 1	42							
<i>Access Rd 2</i>									
11+50.00	STAGE 1			66					
14+00.00	STAGE 1			60					
14+00.00	STAGE 1			60					
19+00.00	STAGE 1							78	
19+00.00	STAGE 1							78	
21+92.00	STAGE 1			102					
21+92.00	STAGE 1			102					
24+00.00	STAGE 1			48					
24+00.00	STAGE 1			48					
25+02.50	STAGE 1			48					
25+02.50	STAGE 1			48					
39+00.00	STAGE 1		52						
<i>Access Rd 3</i>									
13+00.00	STAGE 1	54							
14+00.00	STAGE 1	54							
17+50.00	STAGE 1	54							
19+00.00	STAGE 1	60							
20+50.00	STAGE 1	54							
<i>Davis Lane</i>									
07+00.00	STAGE 1						72		
08+50.00	STAGE 1						50		
<i>Ingham Rd</i>									
08+37.00	STAGE 1	34							
11+16.00	STAGE 1	42							
<i>Lagemann Rd Service Drive</i>									
00+25.00	STAGE 1	42							
01+07.00	STAGE 1	40							
03+09.00	STAGE 1	40							
<i>Lagemann Rd</i>									
16+50.00	STAGE 1	36							
16+50.00	STAGE 1	36							
21+36.77	STAGE 1	78							
22+50.00	STAGE 1	36							
23+90.76	STAGE 1	42							
SUBTOTALS	STAGE 1	1170	100	0	684	54	46	122	156
<i>FAP 310</i>									
128+50.00	STAGE 2			48					
136+80.00	STAGE 2			57					
136+80.00	STAGE 2			57					
SUBTOTALS	STAGE 2	0	0	0	162	0	0	0	0

ENTRANCE PIPE CULVERT SCHEDULE

STATION	STAGE CONSTRUCTION	PIPE CULVERTS							
		CLASS D							
		TYPE 1				TYPE 2			
		12"	15"	18"	24"	30"	36"	12"	24"
FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT		
<i>FAP 310</i>									
142+00.00	STAGE 3				40				
142+00.00	STAGE 3				40				
143+08.00	STAGE 3					48			
143+08.00	STAGE 3					48			
144+28.00	STAGE 3					48			
144+28.00	STAGE 3					48			
170+40.00	STAGE 3				48				
170+40.00	STAGE 3				48				
179+50.00	STAGE 3	50							
184+50.00	STAGE 3	42							
207+00.00	STAGE 3							60	
221+80.00	STAGE 3			66					
221+80.00	STAGE 3			66					
<i>Beuttel Rd</i>									
06+35.00	STAGE 3	44							
07+15.00	STAGE 3	48							
07+15.00	STAGE 3	52							
SUBTOTALS	STAGE 3	236	0	132	176	192	0	60	0
TOTALS		1406	100	132	1022	246	46	182	156

TEMPORARY STRUCTURE

LOCATION	STAGE	STORM SEWER		FLUSH INLET BOX FOR MEDIAN, STANDARD 542546, SPECIAL EACH	MH TY A, SPL 5'-DIA TY 1 F CL EACH
		12"	24"		
		FOOT	FOOT		
<i>Fap 310</i>					
166+00.00 to 167+80.00	STAGE 1	178			
SUBTOTALS		178	0	0	0
<i>Fap 310</i>					
107+50.00	STAGE 2				
135+50.00	STAGE 2			1	
136+50.00	STAGE 2			1	
136+50.00 to 138+00.00	STAGE 2	146			
138+00.00	STAGE 2			1	
138+00.00 to 140+00.00	STAGE 2	196			
140+00.00	STAGE 2			1	
140+00.00 to 141+75.00	STAGE 2		172		
141+75.00	STAGE 2			1	
141+75.00 to 142+50.00	STAGE 2		70		
142+50.00	STAGE 2			1	
142+50.00 to 143+00.00	STAGE 2		46		
143+00.00	STAGE 2				1
SUBTOTALS		342	288	6	1
TOTALS		520	288	6	1

TEMPORARY PIPE CULVERTS

LOCATION	STAGE	PIPE CULVERT CLASS D, TYPE 1				PIPE CULVERT CLASS D, TYPE 2	
		12"	15"	18"	24"	12"	24"
		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT
<i>FAP 310</i>							
111+94.00	STAGE 1		40				
119+00.00	STAGE 1	46					
121+50.00	STAGE 1	46					
125+86.00	STAGE 1		42				
130+80.00	STAGE 1	44					
140+00.00	STAGE 1				74		
171+50.00	STAGE 1		70				
221+97.50 to 222+50.00	STAGE 1				48		
227+89.00	STAGE 1	32					
SUBTOTALS		168	152	0	122	0	0
<i>FAP 310</i>							
107+50.00	STAGE 2		200				
128+50.00	STAGE 2	44					
135+50.00	STAGE 2				14		
143+00.00	STAGE 2						7
144+28.00	STAGE 2	50					
SUBTOTALS		94	200	0	14	0	7
<i>FAP 310</i>							
142+00.00	STAGE 3		34				
143+08.00	STAGE 3		50				
179+50.00	STAGE 3	46					
184+50.00	STAGE 3	38					
189+21.50	STAGE 3					42	
191+75.00	STAGE 3	38					
193+79.00	STAGE 3			38			
197+82.00	STAGE 3		44				
199+33.00	STAGE 3		32				
200+80.00	STAGE 3	30					
207+00.00	STAGE 3	38					
SUBTOTALS		190	160	38	0	42	0
TOTALS		452	512	38	136	42	7

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

## SCHEDULES OF QUANTITIES

SCALE: NO SCALE  
DATE: 02-06-09

DRAWN BY: C.J.C.  
CHECKED BY: J.J.C.



STORM SEWER SCHEDULE															
LOCATION	STAGE CONST	TRENCH BACKFILL	CLASS A, TYPE 1					CLASS A, TYPE 2					STORM SEWERS, TY. 1 RC ELLIPTICAL PIPE, SPAN 30, RISE 19		
			12"	15"	18"	21"	24"	30"	12"	15"	18"	24"		30"	
			CU YD	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT		FOOT	FOOT
<i>U.S. 67 - IL 111 (Godfrey Road)</i>															
EXISTING BOX	to	STRUCTURE (1-1)	L+	STAGE 1	265.0				192						
STRUCTURE (1-1)	to	STRUCTURE (1-3)	L+	STAGE 1	107.3			134							
STRUCTURE (1-1)	to	STRUCTURE (1-2)	L+	STAGE 1	1.5	4									
STRUCTURE (1-20)	to	STRUCTURE (1-21)	R+	STAGE 1	7.2	24									
STRUCTURE (1-3)	to	STRUCTURE (1-4)	L+	STAGE 1	3.3	4									
STRUCTURE (1-3)	to	STRUCTURE (1-5)	L+	STAGE 1	95.0			80							
STRUCTURE (1-22)	to	STRUCTURE (1-23)	R+	STAGE 1	8.2	24									
STRUCTURE (1-5)	to	STRUCTURE (1-7)	L+	STAGE 1	55.3			69							
STRUCTURE (1-5)	to	STRUCTURE (1-6)	L+	STAGE 1	1.5	4									
STRUCTURE (1-24)	to	STRUCTURE (1-25)	R+	STAGE 1	10.1	28									
STRUCTURE (1-24)	to	STRUCTURE (1-26)	R+	STAGE 1	27.6	72									
STRUCTURE (1-7)	to	STRUCTURE (1-12)	L+	STAGE 1	110.5			107							
STRUCTURE (1-7)	to	STRUCTURE (1-8)	L+	STAGE 1	4.6	6									
STRUCTURE (1-12)	to	STRUCTURE (1-11)	L+	STAGE 1	4.5		7								
STRUCTURE (1-11)	to	STRUCTURE (1-15)	L+	STAGE 1	17.9		54								
STRUCTURE (1-15)	to	STRUCTURE (1-16)	L+	STAGE 1	58.9	148									
STRUCTURE (1-34)	to	STRUCTURE (1-35)	R+	STAGE 1	35.1			46							
STRUCTURE (1-16)	to	STRUCTURE (1-17)	L+	STAGE 1	2.6	10									
STRUCTURE (1-35)	to	STRUCTURE (1-37)	R+	STAGE 1	141.2			195							
STRUCTURE (1-19)	to	STRUCTURE (1-18)	L+	STAGE 1	1.4	4									
STRUCTURE (1-16)	to	STRUCTURE (1-18)	L+	STAGE 1	66.5	195									
STRUCTURE (1-36)	to	STRUCTURE (1-35)	R+	STAGE 1	1.4	4									
STRUCTURE (1-37)	to	STRUCTURE (1-38)	R+	STAGE 1	1.7	6									
STRUCTURE (1-38)	to	STRUCTURE (1-39)	R+	STAGE 1	67.4	105									
STRUCTURE (1-20)			R+	STAGE 1											
STRUCTURE (1-22)			R+	STAGE 1											
STRUCTURE (1-25)			R+	STAGE 1											
SUBTOTALS					1095.5	385	253	61	777	46	0	0	0	0	0
<i>Business U.S. 67 (IL 111)</i>															
STRUCTURE (1-32)	to	STRUCTURE (1-33)	L+	STAGE 1	10.9	46									
STRUCTURE (1-33)	to	STRUCTURE (1-28)	L/R	STAGE 1	19.5			74							
STRUCTURE (1-28)	to	STRUCTURE (1-27)	R+	STAGE 1	38.3			56							
STRUCTURE (1-34)	to	STRUCTURE (1-33)	L+	STAGE 1	90.2			86							
STRUCTURE (1-30)	to	STRUCTURE (1-29)	R+	STAGE 1	5.6	20									
STRUCTURE (1-31)	to	STRUCTURE (1-29)	R+	STAGE 1	3.4	10									
STRUCTURE (1-30)			R+	STAGE 1											
SUBTOTALS					167.8	76	0	0	216	0	0	0	0	0	0
<i>FAP 310</i>															
STRUCTURE (3-1)	to	STRUCTURE (3-2)	L+	STAGE 1	15.7	46									
STRUCTURE (3-2)	to	STRUCTURE (3-3)	L+	STAGE 1	44.4	116									
STRUCTURE (3-3)	to	STRUCTURE (3-4)	L+	STAGE 1	21.9	102									
STRUCTURE (3-4)			R+	STAGE 1											
<i>FAP 310 (Logeman Road Intersection)</i>															
STRUCTURE (5-1)	to	STRUCTURE (5-2)	L+	STAGE 1	8.0	22									
STRUCTURE (5-2)	to	STRUCTURE (5-3)	L+	STAGE 1	6.0						54				
STRUCTURE (5-3)	to	STRUCTURE (5-4)	L/R	STAGE 1	17.3	26									
STRUCTURE (5-4)	to	STRUCTURE (5-5)	R+	STAGE 1	8.0	22									
STRUCTURE (5-5)	to	STRUCTURE (5-6)	R+	STAGE 1	8.0	22									
STRUCTURE (5-4)	to	STRUCTURE (5-12)	R+	STAGE 1	13.9	22									

STORM SEWER SCHEDULE																
LOCATION	STAGE CONST	TRENCH BACKFILL	CLASS A, TYPE 1					CLASS A, TYPE 2					STORM SEWERS, TY. 1 RC ELLIPTICAL PIPE, SPAN 30, RISE 19			
			12"	15"	18"	21"	24"	30"	12"	15"	18"	24"		30"		
			CU YD	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT		FOOT	FOOT	
STRUCTURE (5-12)	to	STRUCTURE (5-13)	R+	STAGE 1	8.0	22										
STRUCTURE (5-13)	to	STRUCTURE (5-14)	R+	STAGE 1	8.0	22										
STRUCTURE (5-15)	to	STRUCTURE (5-7)	L+	STAGE 1	6.0						52					
STRUCTURE (5-15)	to	STRUCTURE (5-8)	L+	STAGE 1	8.8	22										
STRUCTURE (5-8)	to	STRUCTURE (5-9)	L/R	STAGE 1	12.4	58										
STRUCTURE (5-9)	to	STRUCTURE (5-16)	R+	STAGE 1	7.8	11										
STRUCTURE (5-16)	to	STRUCTURE (5-17)	R+	STAGE 1	5.0	7										
STRUCTURE (5-17)	to	STRUCTURE (5-10)	R+	STAGE 1	6.4	9										
STRUCTURE (5-10)	to	STRUCTURE (5-18)	R+	STAGE 1	11.4	16										
STRUCTURE (5-18)	to	STRUCTURE (5-11)	R+	STAGE 1	8.0	12										
STRUCTURE (5-11)	to	STRUCTURE (5-19)	R+	STAGE 1	16.4	48										
<i>Access Road 1</i>																
STRUCTURE (4-2)	to	STRUCTURE (4-1)	L+	STAGE 1	3.4			32								
STRUCTURE (4-2)	to	STRUCTURE (4-3)	L+	STAGE 1	123.9			346								
STRUCTURE (4-3)	to	STRUCTURE (4-4)	L+	STAGE 1	212.0			310								
STRUCTURE (4-4)	to	STRUCTURE (4-5)	L+	STAGE 1	3.4			32								
STRUCTURE (4-4)	to	STRUCTURE (4-6)	R/L	STAGE 1	8.5						44					
<i>Access Road 3</i>																
STRUCTURE (2-2)	to	STRUCTURE (2-3)	L+	STAGE 1							72					
STRUCTURE (2-3)	to	STRUCTURE (2-1)	L+	STAGE 1											58	
STRUCTURE (2-3)	to	STRUCTURE (2-4)	L+	STAGE 1							60					
<i>Benjamin Drive</i>																
STRUCTURE (1-11)	to	STRUCTURE (1-10)	L+	STAGE 1	16.9	44										
STRUCTURE (1-10)	to	STRUCTURE (1-9)	L+	STAGE 1	2.9	8										
STRUCTURE (1-13)	to	STRUCTURE (1-14)	R+	STAGE 1	2.7	8										
STRUCTURE (1-10)	to	STRUCTURE (1-14)	L/R	STAGE 1	7.7	30										
SUBTOTALS					622.4	572	123	720	44	52	132	0	0	0	54	0
<i>FAP 310</i>																
222+00.00	to	222+50.00	R+	STAGE 2											45	
222+50.00	to	222+50.00	R+	STAGE 2											6	
222+50.00	to	226+00.00	R+	STAGE 2											345	
SUBTOTALS					0	0	0	0	0	0	0	0	0	0	396	0
<i>FAP 310</i>																
STRUCTURE (6-2)	to	STRUCTURE (6-3)	R+	STAGE 3	21.8									68		
STRUCTURE (6-3)	to	STRUCTURE (6-1)	R+	STAGE 3	24.3									67		
STRUCTURE (6-3)	to	STRUCTURE (6-4)	R+	STAGE 3	116.1										131	
STRUCTURE (6-4)	to	STRUCTURE (6-5)	R+	STAGE 3	80.7										96	
STRUCTURE (6-5)	to	STRUCTURE (6-6)	R+	STAGE 3	38.7										46	
STRUCTURE (6-6)	to	STRUCTURE (6-7)	R+	STAGE 3	218.0										246	
STRUCTURE (6-7)	to	STRUCTURE (6-8)	R+	STAGE 3	195.1										246	
STRUCTURE (6-8)	to	STRUCTURE (6-9)	R+	STAGE 3	195.1										212	
STRUCTURE (6-9)	to	STRUCTURE (6-10)	R+	STAGE 3	84.4											
STRUCTURE (6-10)	to	STRUCTURE (6-11)	R+	STAGE 3	3.9	32										
SUBTOTALS					977.9	0	32	0	0	0	67	68	212	492	246	273
TOTALS					2864	1033	408	781	821	314	199	68	212	492	696	273

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

# SCHEDULES OF QUANTITIES

SCALE: NO SCALE  
DATE: 02-06-09

DRAWN BY: C.J.C.  
CHECKED BY: J.J.C.

## PAVING SUMMARY

STATION	AGGREGATE (PRIME COAT)	BITUMINOUS MATERIALS (PRIME COAT)	INCIDENTAL HOT-MIX ASPHALT SURFACING	PROCESSING MODIFIED SOILS 12"	LIME	SLAG-MODIFIED PORTLAND CEMENT CONCRETE	AGGREGATE BASE COURSE TYPE A	AGGREGATE BASE COURSE TYPE B 4"	SUB-BASE GRANULAR MATERIAL TYPE A 12"	SUB-BASE GRANULAR MATERIAL TYPE B 12"	SUB-BASE GRANULAR MATERIAL TYPE C	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	LEVELING BINDER (MACHINE METHOD), N90	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH) 9 1/2"	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 14 1/4"	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15 3/4"	HOT-MIX ASPHALT SHOULDERS 8"	AGGREGATE SHOULDERS, TYPE B	CLASS D PATCHES, TYPE II, 11"	STRIP REFLECTIVE CRACK CONTROL TREATMENT	BRIDGE APPROACH PAVEMENT	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)			
																										(TON)	(GALLON)	(TON)
<i>US 67 (Godfrey Road)</i>																												
165+45.00	to	167+88.10	4.5	417.7					109				153			571										729		
167+88.10	to	174+75.00	13.3	1935.5					1742				447			710										2061		
174+75.00	to	177+10.39	4.2	866.2					1062				141			240										942		
45+00.00	to	48+86.00	2.6						1980							399										1544		
48+86.00	to	56+33.49	4.7	4598.3					4067		216		661			3332						13.1				2990		
SUB-TOTAL			29.3	7817.7	0	0	0	0	8959	0	216	0	1402	0	5422	2238	0	0	0	664	333	13		8266	0	0		
<i>IL 111</i>																												
1172+38.13	to	1174+86.83	3.3	609.1					697				109			196										62.1		
1174+86.83	to	1179+40.00	6.0	1377.3					1794		1185		202			132					426			390		15.6		
Rt Turn Lane from U.S. 67			1.3	715.1					1220				44			29												
Rt Turn Lane from Bus. 67			0.6	172.7					248				20			14												
Temporary Pavement - Medlar				113.2									48			23												
SUB-TOTAL			11.2	2987.4	0	0	0	0	3960	0	1185	0	423	0	2210	394	0	0	0	426	390	78		0	0	0		
<i>Ramp A</i>																												
00+00.00	to	03+96.24		47.4																								
03+96.24	to	19+84.00		282.3							604																	
Temporary Pavement				19.1									16		134	11												
<i>Ramp B</i>																												
08+42.90	to	15+33.50		123.3																								
21+63.40	to	37+55.18		278.8							925								997							110	368	
37+55.18	to	47+03.00		106.6														2741		1066					110	356		
<i>FAP Route 310</i>																												
91+50.00	to	160+07.07		3617.9					60800		2437																	
161+29.93	to	165+43.79		223.1					4197																	21		
166+61.56	to	228+25.00		3206.0		108			53879																	13.4		
Buettel Road Turn Lanes				211.6					1570																	49.2		
Lagemann Road Turn Lanes				330.1					3187																			
Davis Lane Turn Lanes				216.1					1626																			
Intersection Median (221+80)				42.3					251																			
<i>Access Road 1</i>																												
10+87.05	to	73+00.00		41.4																								
73+00.00	to	73+22.00		119.9					1605																			
<i>Access Road 2</i>																												
10+19.00	to	10+70.00		0.7																								
10+70.00	to	17+34.60		5.5																								
17+34.60	to	18+99.60		1.1																								
18+99.60	to	41+07.65		9.8																								
Turnaround																												
<i>Access Road 3</i>																												
10+43.00	to	10+94.00		0.7																								
10+94.00	to	21+00.00		8.4																								
21+00.00	to	21+35.00		0.6																								
<i>Benjamin Drive</i>																												
10+24.00	to	10+84.15		0.8																								
10+84.15	to	11+50.00		0.5																								
<i>Buettel Road</i>																												
05+20.00	to	06+00.00		0.4					32				12			63												
06+00.00	to	07+91.32		1.1					94				36			185												
07+91.32	to	09+51.00		1.9					170				65			329												
<i>Lagemann Road</i>																												
10+50.69	to	10+85.20		1.7					152				57			293												
10+85.20	to	24+00.00		8.0					713				249			1290												

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

## SCHEDULES OF QUANTITIES

SCALE: NO SCALE  
DATE: 09-04-09

DRAWN BY: C.J.C.  
CHECKED BY: J.J.C.

CONTRACT NO. 76311

## PAVING SUMMARY

STATION	AGGREGATE (PRIME COAT)	BITUMINOUS MATERIALS (PRIME COAT)	INCIDENTAL HOT-MIX ASPHALT SURFACING	PROCESSING MODIFIED SOILS 12"	LIME	SLAG-MODIFIED PORTLAND CEMENT CONCRETE	AGGREGATE BASE COURSE TYPE A	AGGREGATE BASE COURSE TYPE B 4"	SUB-BASE GRANULAR MATERIAL TYPE A 12"	SUB-BASE GRANULAR MATERIAL TYPE B 12"	SUB-BASE GRANULAR MATERIAL TYPE C	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	LEVELING BINDER (MACHINE METHOD), N90	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH) 9 1/2"	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 14 1/4"	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15 3/4"	HOT-MIX ASPHALT SHOULDERS 8"	AGGREGATE SHOULDERS, TYPE B	CLASS D PATCHES, TYPE II, 11"	STRIP REFLECTIVE CRACK CONTROL TREATMENT	BRIDGE APPROACH PAVEMENT	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)		
	(TON)	(GALLON)	(TON)	(SQ YD)	(TON)	(TON)	(TON)	(SQ YD)	(SQ YD)	(SQ YD)	(TON)	(TON)	(TON)	(TON)	(TON)	(TON)	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)	(TON)	(SQ YD)	(FOOT)	(SQ YD)	(SQ YD)		
<i>Davis Lane</i>																											
02+75.00	to	03+25.00	0.3	60.0			22					8		44													
03+25.00	to	08+09.32	2.7	645.8			239					90		469													
08+09.32	to	09+50.24	1.7	406.8			150					57		291													
<i>Ingham Lane</i>																											
08+10.00	to	09+25.17	0.6	154.8					264								227										
09+25.17	to	09+88.00	0.8	204.0					343								326				94						
10+12.00	to	10+77.09	0.9	224.0					377								356										
10+77.09	to	11+50.00	0.4	98.0					167								141										
<i>Connector Detail No. 1</i>																											
136+00.00	to	140+08.00		803.3						1363							1220										
<i>Connector Detail No. 2</i>																											
00+00.00	to	03+32.00		632.6						1074							958										
<i>Cross Over Detail No. 1</i>																											
01+12.00	to	05+90.00		343.3						596							454										
<i>Cross Over Detail No. 2</i>																											
02+59.00	to	11+59.00		1564.8						2650							2396										
<i>Cross Over Detail No. 3</i>																											
03+50.00	to	09+08.00		1047.1						1780							1573										
<i>Cross Over Detail No. 4</i>																											
00+00.00	to	11+56.14		1711.0						3048	118						1870				713	164					
<i>Entrances</i>																											
(See Entrance Schedule)							2965	309																			
SUB-TOTAL			90.5	38021.3	1277	140076	2549.5	148	6413	2746	32393	10510	19117	575	16	2963	134	11	37738	6560	80011	46343	15255	84	0	220	723
TOTAL			131.0	48826.4 *	1277	140076	2549.5	148.4	6413	2746	45312	10510	20518	575	1841	2963	7766	2643	37738	6560	80011	47433	15978	175	8266	220	723

\* GALLONS CONVERTED TO TONS (0.004172 TONS/GALLON) = 203.7 TONS

TEMPORARY ENTRANCE SCHEDULE				
TYPE	SIDE / STATION	PHASE	AGGREGATE SURFACE COURSE, TYPE A	
			6"	TON
<i>FAP Route 310</i>				
FE	L+	111+94.00	Stage 1	26.1
FE	L+	114+50.00	Stage 1	48.4
FE	L+	119+00.00	Stage 1	48.7
FE	L+	121+50.00	Stage 1	49.1
SERVICE DR	L+	125+86.00	Stage 1	48.4
FE	R+	128+50.00	Stage 2	48.4
FE	L+	130+80.00	Stage 1	48.7
PE	L+	141+97.00	Stage 2	46.1
PE	L+	143+08.00	Stage 2	92.3
PE	L+	144+28.00	Stage 2	111.0
PE	R+	148+46.82	Stage 1	76.3
Beuttel Road	L+	149+00.00	Stage 3	216.4
FE	L+	179+50.00	Stage 3	22.2
FE	R+	184+50.00	Stage 3	37.8
PE	R+	189+21.50	Stage 3	36.7
PE	R+	191+75.00	Stage 3	36.7
PE	R+	193+79.00	Stage 3	36.7
PE	R+	197+82.00	Stage 3	46.8
PE	R+	199+33.00	Stage 3	29.6
PE	R+	200+80.00	Stage 3	26.8
FE	R+	207+00.00	Stage 3	29.3
PE	L+	227+89.00	Stage 1	33.6
TOTAL				1196

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

## SCHEDULES OF QUANTITIES

SCALE: NO SCALE DRAWN BY: C.J.C.

DATE: 09-04-09 CHECKED BY: J.J.C.

PROTECTIVE COAT		SO YD
FAP 310, SIDEROADS		1909.7
PORTLAND CEMENT CONCRETE DRIVEWAY		723.0
CONCRETE GUTTER, TYPE A		129.2
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24		0.0
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06		1254.9
CONCRETE MEDIAN SURFACE, 4 INCH		1006.8
CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT		300.2
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24		129.4
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06		220.0
BRIDGE APPROACH PAVEMENT		147.1
CLASS SI CONCRETE (OUTLET)		
	Subtotal	5820
U.S. 67		
PORTLAND CEMENT CONCRETE DRIVEWAY		226.0
CONCRETE GUTTER, TYPE A		0.0
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24		979.5
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06		14.3
CONCRETE MEDIAN SURFACE, 4 INCH		0.0
CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT		0.0
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24		0.0
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06		0.0
BRIDGE APPROACH PAVEMENT		0.0
CLASS SI CONCRETE (OUTLET)		0.0
	Subtotal	1220
IL III		
PORTLAND CEMENT CONCRETE DRIVEWAY		0.0
CONCRETE GUTTER, TYPE A		0.0
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24		377.2
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06		0.0
CONCRETE MEDIAN SURFACE, 4 INCH		930.7
CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT		0.0
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24		0.0
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06		0.0
BRIDGE APPROACH PAVEMENT		0.0
CLASS SI CONCRETE (OUTLET)		22.3
	Subtotal	1330
	Total	8370

NITROGEN FERTILIZER NUTRIENT				POUND
FAP Route 310				
Stage 1				
97+50	to	228+25		1789.20
Stage 2				
105+50	to	228+50		310.50
Stage 3				
91+50	to	225+00		1282.35
RAMP A				
08+00	to	18+50		170.10
RAMP B				
Stage 2				
10+00	to	33+00		427.50
Stage 3				
10+00	to	15+50		72.90
IL III				
Stage 1				
1172+50	to	1179+40		69.30
Stage 2				
1172+50	to	1179+40		65.70
US 67				
Stage 1				
165+50	to	57+00		216.90
Stage 2				
54+50	to	57+50		3.60
Stage 3				
56+50	to	57+50		4.50
Access Road 1				
10+87	to	61+50		307.80
Access Road 2				
10+18	to	41+00		331.20
Access Road 3				
10+43	to	17+00		46.80
Benjamin Road				
10+50	to	11+50		4.50
Beuttel Road				
05+20	to	08+50		42.30
Davis Lane				
02+75	to	09+00		66.60
Ingham Road				
07+40	to	11+50		22.50
Lagemann Road				
10+51	to	24+00		247.50
Special Ditch				
10+65	to	14+30		30.60
			TOTAL	5512

PHOSPHORUS FERTILIZER NUTRIENT				POUND
FAP Route 310				
Stage 1				
97+50	to	228+25		1789.20
Stage 2				
105+50	to	228+50		310.50
Stage 3				
91+50	to	225+00		1282.35
RAMP A				
08+00	to	18+50		170.10
RAMP B				
Stage 2				
10+00	to	33+00		427.50
Stage 3				
10+00	to	15+50		72.90
IL III				
Stage 1				
1172+50	to	1179+40		69.30
Stage 2				
1172+50	to	1179+40		65.70
US 67				
Stage 1				
165+50	to	57+00		216.90
Stage 2				
54+50	to	57+50		3.60
Stage 3				
56+50	to	57+50		4.50
Access Road 1				
10+87	to	61+50		307.80
Access Road 2				
10+18	to	41+00		331.20
Access Road 3				
10+43	to	17+00		46.80
Benjamin Road				
10+50	to	11+50		4.50
Beuttel Road				
05+20	to	08+50		42.30
Davis Lane				
02+75	to	09+00		66.60
Ingham Road				
07+40	to	11+50		22.50
Lagemann Road				
10+51	to	24+00		247.50
Special Ditch				
10+65	to	14+30		30.60
			TOTAL	5512

POTASSIUM FERTILIZER NUTRIENT				POUND
FAP Route 310				
Stage 1				
97+50	to	228+25		1789.20
Stage 2				
105+50	to	228+50		310.50
Stage 3				
91+50	to	225+00		1282.35
RAMP A				
08+00	to	18+50		170.10
RAMP B				
Stage 2				
10+00	to	33+00		427.50
Stage 3				
10+00	to	15+50		72.90
IL III				
Stage 1				
1172+50	to	1179+40		69.30
Stage 2				
1172+50	to	1179+40		65.70
US 67				
Stage 1				
165+50	to	57+00		216.90
Stage 2				
54+50	to	57+50		3.60
Stage 3				
56+50	to	57+50		4.50
Access Road 1				
10+87	to	61+50		307.80
Access Road 2				
10+18	to	41+00		331.20
Access Road 3				
10+43	to	17+00		46.80
Benjamin Road				
10+50	to	11+50		4.50
Beuttel Road				
05+20	to	08+50		42.30
Davis Lane				
02+75	to	09+00		66.60
Ingham Road				
07+40	to	11+50		22.50
Lagemann Road				
10+51	to	24+00		247.50
Special Ditch				
10+65	to	14+30		30.60
			TOTAL	5512

TEMPORARY EROSION CONTROL SEEDING				POUND
FAP Route 310				
Stage 1				
97+50	to	228+25		3976.0
Stage 2				
105+50	to	228+50		690.0
Stage 3				
91+50	to	225+00		2849.7
RAMP A				
08+00	to	18+50		378.0
RAMP B				
Stage 2				
10+00	to	33+00		950.0
Stage 3				
10+00	to	15+50		162.0
IL III				
Stage 1				
1172+50	to	1179+40		154.0
Stage 2				
1172+50	to	1179+40		146.0
US 67				
Stage 1				
165+50	to	57+00		482.0
Stage 2				
54+50	to	57+50		8.0
Stage 3				
56+50	to	57+50		10.0
Access Road 1				
10+87	to	61+50		684.0
Access Road 2				
10+18	to	41+00		736.0
Access Road 3				
10+43	to	17+00		104.0
Benjamin Road				
10+50	to	11+50		10.0
Beuttel Road				
05+20	to	08+50		94.0
Davis Lane				
02+75	to	09+00		148.0
Ingham Road				
07+40	to	11+50		50.0
Lagemann Road				
10+51	to	24+00		550.0
Special Ditch				
10+65	to	14+30		68.0
			Total	12250

SHOULDER RUMBLE STRIP				
Station	Side	FOOT		
FAP 310				
91+50.00	to	98+16.50	Lt	666.5
91+50.00	to	145+55.00	Lt	5405
92+50.00	to	145+55.00	Rt	5305
92+50.00	to	108+83.00	Rt	1633
104+76.50	to	111+78.00	Lt	701.5
112+10.00	to	114+28.00	Lt	218
113+50.00	to	128+28.00	Rt	1478
114+72.00	to	118+78.00	Lt	406
119+22.00	to	121+28.00	Lt	206
121+72.00	to	125+43.00	Lt	371
126+29.00	to	130+58.00	Lt	429
128+72.00	to	136+58.00	Rt	786
131+02.00	to	141+75.00	Lt	1073
137+03.00	to	148+20.00	Rt	1117
142+19.00	to	142+71.00	Lt	52
143+26.00	to	143+89.00	Lt	63
144+44.00	to	147+78.00	Lt	334
148+74.00	to	159+55.00	Rt	1081
149+39.00	to	160+00.00	Lt	1061
149+39.00	to	159+68.00	Rt	1029
149+50.00	to	160+13.00	Lt	1063
160+81.00	to	164+99.00	Rt	418
160+94.00	to	165+18.00	Rt	424
161+21.00	to	165+42.00	Lt	421
161+36.00	to	165+46.00	Lt	410
166+26.00	to	169+71.00	Rt	345
166+56.00	to	167+03.00	Rt	47
166+59.00	to	169+60.00	Lt	301
166+63.00	to	170+17.00	Lt	354
170+62.00	to	179+28.00	Lt	866
171+39.00	to	201+19.00	Rt	2980
171+39.00	to	184+33.00	Rt	1294
174+50.00	to	201+42.00	Lt	2692
179+72.00	to	201+36.00	Lt	2164
184+69.00	to	189+08.00	Rt	439
189+34.00	to	191+61.00	Rt	227
191+87.00	to	193+65.00	Rt	178
193+91.00	to	197+65.00	Rt	374
197+99.00	to	199+20.00	Rt	121
199+46.00	to	200+68.00	Rt	122
200+92.00	to	202+21.00	Rt	129
202+47.00	to	206+82.00	Rt	435
203+07.00	to	212+15.00	Lt	908
203+08.00	to	221+09.00	Rt	1801
203+26.00	to	221+03.00	Lt	1777
207+18.00	to	221+08.00	Rt	1390
212+57.00	to	227+77.00	Lt	1520
222+45.00	to	225+00.00	Rt	255
222+55.00	to	225+00.00	Rt	245
225+08.00	to	228+25.00	Lt	317
228+20.00	to	228+25.00	Lt	5
			Total	47437

\* Adjusted for Shrinkage (25% Shrinkage Assumed)  
 \*\* Waste (+) or Shortage (-)

EARTH EXCAVATION						
Stage	Location		Earth Excavation	Excavation to be used in Embankment*	Embankment	Earthwork Balance**
	From	To				
FAP 310						
Stage 1	97+50	228+50	353920	265440	13887	251553
Stage 2	105+50	228+50	21686	16265	2330	13935
Stage 3	91+50	225+00	242381	181786	3932	177854
Stage 3-6	218+50	225+00	441	331	2	329
US 67 (Godfrey Rd)						
Stage 1	165+45	57+50	15251	11438	1208	10230
Stage 2	56+50	57+50	101	76	69	7
Stage 3	56+33.49	57+50	177	133	24	109
RAMP A	7+23.56	18+67.57	16256	12192	801	11391
RAMP B						
Stage 2	9+60.71	33+07.23	13096	9822	33204	-23382
Stage 3	9+60.71	15+50	1457	1093	1399	-306
IL III						
Stage 1	1173+00	1179+40	4780	3585	240	3345
Stage 2	1173+00	1179+40	5023	3767	136	3631
Turn Lane Removal	--	--	0	0	444	-444
ACCESS ROAD 1	10+87.05	61+58.50	8855	6641	5641	1000
ACCESS ROAD 2	11+00	41+07.65	16506	12380	3642	8738
ACCESS ROAD 2 -						
Drive Sta. 41+07.65	--	--	90	68	0	68
ACCESS ROAD 3	11+00	17+31.50	3429	2572	129	2443
BENJAMIN DRIVE	10+50	11+60	1184	888	0	888
BEUTTEL ROAD	5+00					

PRECAST FLARED END SECTIONS

STATION	STAGE CONSTRUCTION	12"	15"	18"	21"	24"	30"	36"	24" ERS	36" ERS
		EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH
<i>US 67</i>										
168+00.00	STAGE 1	1								
169+25.00	STAGE 1	1								
170+54.00	STAGE 1	1								
48+42.00	STAGE 1					2				
SUBTOTALS	STAGE 1	3	0	0	0	2	0	0	0	0
<i>IL III</i>										
1173+20.00	STAGE 1					1				
1173+98.00	STAGE 1	1								
1174+60.00	STAGE 1									2
SUBTOTALS	STAGE 1	1	0	0	0	1	0	0	0	2
<i>FAP 310</i>										
102+00.00	STAGE 1					1				
112+25.00	STAGE 1	1								
113+00.00	STAGE 1					1				
117+50.00	STAGE 1					1				
126+00.00	STAGE 1				1					
127+00.00	STAGE 1					1				
143+00.00	STAGE 1					1				
147+50.00	STAGE 1					1				
163+50.00	STAGE 1					1				
167+50.00	STAGE 1					1				
171+50.00	STAGE 1					1				
175+00.00	STAGE 1					1				
187+00.00	STAGE 1					2				
190+00.00	STAGE 1			2						
190+65.00	STAGE 1						1			
193+00.00	STAGE 1					1				
203+50.00	STAGE 1					1				
219+50.00	STAGE 1					1				
222+50.00	STAGE 1					1				
227+50.00	STAGE 1					1				
<i>Access Rd 1</i>										
42+00.00	STAGE 1			2						
44+50.00	STAGE 1			2						
46+00.00	STAGE 1			2						
51+50.00	STAGE 1			2						
60+25.00	STAGE 1							2		
66+50.00	STAGE 1			1						
73+14.00	STAGE 1			1						
<i>Access Rd 2</i>										
13+30.00	STAGE 1			2						
23+75.00	STAGE 1			2						
40+50.00	STAGE 1			2						
<i>Access Rd 3</i>										
12+00.00	STAGE 1			2						
20+24.00	STAGE 1						1			
21+00.00	STAGE 1								1	
21+62.00	STAGE 1						1			
<i>Ramp A</i>										
04+22.00	STAGE 1					1				
10+15.00	STAGE 1									1
<i>Lagemann Rd</i>										
10+92.00	STAGE 1						2			
19+20.13	STAGE 1					2				
SUBTOTALS	STAGE 1	1	0	20	1	20	5	2	1	1
<i>FAP 310</i>										
226+00.00	STAGE 2					1				
<i>Ramp B</i>										
30+50.00	STAGE 2					1				
SUBTOTALS	STAGE 2	0	0	0	0	2	0	0	0	0
<i>FAP 310</i>										
135+50.00	STAGE 3					1				
177+50.00	STAGE 3					1				
187+00.00	STAGE 3					2				
<i>Beuttel Rd</i>										
09+00.00	STAGE 3									
SUBTOTALS	STAGE 3	0	0	0	0	4	0	0	0	0
TOTALS		5	0	20	1	29	5	2	1	3

FOR INFORMATION ONLY  
HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH

STATION	SQ. FT	DISTANCE	CU YD
<i>US 67 - Stage 1</i>			
167+50.00	0.00	50.00	0.00
168+00.00	0.00	50.00	0.00
168+50.00	0.00	50.00	0.20
169+00.00	0.22	50.00	0.82
169+50.00	0.67	50.00	1.45
170+00.00	0.90	50.00	3.19
170+50.00	2.54	50.00	3.63
171+00.00	1.38	50.00	1.97
171+50.00	0.76	50.00	0.70
172+00.00	0.00	100.00	3.06
173+00.00	1.65	50.00	1.93
173+50.00	0.44	50.00	0.40
174+00.00	0.00	50.00	0.00
174+50.00	0.00	50.00	0.00
175+00.00	0.00	50.00	0.00
175+50.00	0.00	50.00	0.00
176+00.00	0.00	50.00	0.00
176+50.00	0.00	50.00	0.00
177+00.00	0.00	10.39	0.00
45+00.00	0.00	50.00	0.00
45+50.00	0.00	50.00	0.00
46+00.00	0.00	50.00	0.00
46+50.00	0.00	50.00	0.00
47+00.00	0.00	50.00	0.00
47+50.00	0.00	50.00	0.00
48+00.00	0.00	50.00	0.00
48+50.00	0.00	50.00	0.04
49+00.00	0.05	100.00	0.08
50+00.00	0.00	50.00	0.00
50+50.00	0.00	50.00	1.98
51+00.00	2.14	50.00	3.61
51+50.00	1.77	50.00	3.88
52+00.00	2.43	50.00	4.79
52+50.00	2.75	50.00	5.25
53+00.00	2.92	50.00	6.59
53+50.00	4.20	50.00	8.46
54+00.00	4.94	50.00	9.84
54+50.00	5.70	50.00	11.69
55+00.00	6.93	50.00	12.92
55+50.00	7.03	50.00	12.83
56+00.00	6.84		
Total			99.3

FOR INFORMATION ONLY  
HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH

STATION	SQ. FT	DISTANCE	CU YD
<i>IL III - Stage 1</i>			
1172+56.10	0.00	43.90	0.00
1173+00.00	0.00	50.00	0.00
1173+50.00	0.00	50.00	0.00
1174+00.00	0.00	50.00	1.31
1174+50.00	1.41	50.00	3.98
1175+00.00	2.89	50.00	5.52
1175+50.00	3.08	50.00	4.37
1176+00.00	1.65	50.00	2.96
1176+50.00	1.56	50.00	5.09
1177+00.00	3.95	50.00	6.61
1177+50.00	3.20	50.00	6.74
1178+00.00	4.09	50.00	6.90
1178+50.00	3.37	50.00	6.12
1179+00.00	3.24	40.00	5.53
1179+40.00	4.23		
Total			55.1
<i>FOR INFORMATION ONLY</i>			
<i>HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH</i>			
STATION	SQ. FT	DISTANCE	CU YD
<i>IL III - Stage 2</i>			
1172+56.10	0.00	43.90	0.00
1173+00.00	0.00	50.00	0.00
1173+50.00	0.00	50.00	0.00
1174+00.00	0.00	50.00	2.84
1174+50.00	3.07	50.00	6.96
1175+00.00	4.45	50.00	8.53
1175+50.00	4.77	50.00	7.54
1176+00.00	3.38	50.00	4.62
1176+50.00	1.61	50.00	4.74
1177+00.00	3.51	50.00	7.17
1177+50.00	4.24	50.00	8.51
1178+00.00	4.96	50.00	10.19
1178+50.00	6.06	50.00	11.34
1179+00.00	6.20	40.00	8.95
1179+40.00	5.89		
Total			81.4

HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH

<i>US 67 (Godfrey Road)</i>		CADD (sq ft)	SO YD
169+00.00	to 171+75.00	3144.39	349.4
172+25.00	to 173+75.00	2767.48	307.5
50+75.00	to 56+34.00	12550.74	1394.5
		SubTotal	2052
<i>Business US 67 (IL III)</i>			
1174+25.00	to 1179+40.00	11683	1298.1
		SubTotal	1299
		Total	3351

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

## SCHEDULES OF QUANTITIES

SCALE: NO SCALE  
DATE: 09-04-09

DRAWN BY: C.J.C.  
CHECKED BY: J.J.C.

CONDUIT IN TRENCH, 1" DIA., PVC		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
NORTHEAST QUAD		
Advance Loops 6'x6'	EOP TO HANDHOLE	20
Loops 6' x 50'	EOP TO HANDHOLE	45
SOUTHWEST QUAD		
	EOP TO HANDHOLE	5
SOUTHEAST QUAD		
Advance Loops 6'x6'	EOP TO HANDHOLE	20
Loops 6' x 50'	EOP TO HANDHOLE	30
NORTHWEST QUAD		
Advance Loops 6'x6'	EOP TO HANDHOLE	20
Loops 6' x 50'	EOP TO HANDHOLE	50
		Total
		190

CONDUIT IN TRENCH, 3 1/2" DIA., PVC		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
HANDHOLE (NE QUAD) TO EOP		
		5
EOP TO DBL. HANDHOLE (NW QUAD)		
		5
		Total
		10

ELECTRIC CABLE IN CONDUIT SIGNAL NO. 14 7C		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
SOUTHWEST QUAD		
7C TO MAST ARM MOUNTED SIGNAL HEAD OVER US 67		215
7C TO MAST ARM MOUNTED SIGNAL HEAD OVER BENJAMIN DRIVE		200
NORTHEAST QUAD		
7C TO MAST ARM MOUNTED SIGNAL HEAD		270
		Total
		685

FULLY ACTUATED CONTROLLER AND TYPE IV CABINET			
U.S. 67 (Godfrey Road)		Offset	Each
172+87.00	LT	37.0	1
		Total	1

SIGNAL HEAD, POLYCARB., LED 1-FACE, 5-SECT., MAST ARM MOUNTED			
U.S. 67 (Godfrey Road)		Offset	Each
171+77.50	LT		1
173+28.66	LT		1
		Subtotal	2
Benjamin Road			
10+36.56	LT		1
		Subtotal	1
		Total	3

CONDUIT IN TRENCH, 1 1/2" DIA., PVC		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
NORTHEAST QUAD		
HANDHOLE TO HANDHOLE		145
SOUTHEAST QUAD		
HANDHOLE TO HANDHOLE		270
NORTHWEST QUAD		
HANDHOLE TO N. EDGE OF ENTRANCE		10
S. EDGE OF ENTRANCE TO DBL. HANDHOLE		245
T.S. CONTROLLER TO POWER POLE		10
		Total
		680

CONDUIT IN TRENCH, 4" DIA., PVC		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
NORTHWEST QUAD		
CONTROLLER TO DBL. HANDHOLE		20
		Total
		20

ELECTRIC CABLE IN CONDUIT SIGNAL NO. 14 5C		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
NORTHWEST QUAD		
5C TO POST MOUNTED SIGNAL HEAD		60
SOUTHWEST QUAD		
5C TO MAST ARM MOUNTED SIGNAL HEAD OVER US 67		200
5C TO MAST ARM MOUNTED SIGNAL HEAD OVER BENJAMIN DRIVE		215
5C TO POST MOUNTED SIGNAL HEAD		160
SOUTHEAST QUAD		
5C TO POST MOUNTED SIGNAL HEADS (2 SIGNAL HEADS)		630
NORTHEAST QUAD		
5C TO MAST ARM MOUNTED SIGNAL HEAD		260
5C TO POST MOUNTED SIGNAL HEAD		220
		Total
		1745

ELECTRIC CABLE IN CONDUIT LEAD-IN, NO 14, 1 PAIR		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
CONTROLLER		
Advance Loops - N. Leg		660
Thru & LTL Loops - N. Leg		110
Single Lane Loop - W. Leg		135
Advance Loops - S. Leg		1195
Thru & LTL Loops - S. Leg		815
Advance Loops - E. Leg		710
LTL, Thru, RTL Loops		555
		Total
		4180

SIGNAL HEAD, POLYCARB., LED 1-FACE, 3-SECT., MAST ARM MOUNTED			
U.S. 67 (Godfrey Road)		Offset	Each
171+77.50	LT		1
173+28.66	LT		1
		Subtotal	2
Benjamin Road			
10+36.56	LT		1
		Subtotal	1
		Total	3

STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS 46 FT			
U.S. 67 (Godfrey Road)		Offset	Each
171+77.50	RT	43.0	1
		Total	1

SIGNAL HEAD, POLYCARB., LED 1-FACE, 3-SECTION, BRACKET MOUNTED			
U.S. 67 (Godfrey Road)		Offset	Each
171+77.50	LT		1
171+77.51	LT		1
171+77.52	RT		1
		Total	3

TRAFFIC SIGNAL BACKPLATE			
U.S. 67 (Godfrey Road)		Offset	Each
171+77.50	LT	43.0	2
173+28.66	RT	69.0	2
		Subtotal	4
Benjamin Drive			
10+36.56	RT	43.0	2
		Subtotal	2
		Total	6

CONDUIT IN TRENCH, 2" DIA., PVC		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
NORTHEAST QUAD		
MAST ARM POLE TO HANDHOLE		40
SIGNAL POST TO HANDHOLE		10
SOUTHEAST QUAD		
SIGNAL POST TO HANDHOLE		45
NORTHWEST QUAD		
SIGNAL POST TO DBL. HANDHOLE		20
		Total
		115

CONDUIT PUSHED, 2 1/2" DIA. GALVANIZED STEEL		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
NORTHEAST QUAD		
BETWEEN HANDHOLES, UNDER TURN LANE PVMT		35
NORTHWEST - SOUTHWEST QUADS		
UNDER BENJAMIN DRIVE		55
SOUTHEAST - NORTHEAST QUADS		
UNDER IL 111		80
		Total
		170

TRENCH AND BACKFILL FOR ELECTRICAL WORK		
PAY ITEM		FOOT
81012300		5
81012500		680
81012600		115
81012700		145
81012900		10
81013000		20
		Total
		975

DETECTOR LOOP, TYPE 1		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
SOUTH LEG OF US 67/IL 111		
Adv. Det.		105
Thru Lanes		354
LTL		207
WEST LEG - BENJAMIN DRIVE		
SINGLE LANE		180
NORTH LEG US 67		
LTL		201
THRU		361
Adv Loops		84
EAST LEG - IL 111		
LTL		201
THRU		188
RTL		175
Adv Loops		82
		Total
		2138

CONCRETE FOUNDATION TYPE E, 36 - INCH DIAMETER			
U.S. 67 (Godfrey Road)		Offset	Foot
171+77.50	LT	43.0	13
173+28.67	RT	69.0	13
		Total	26

CONDUIT IN TRENCH, 2 1/2" DIA., PVC		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
NORTHEAST QUAD		
HANDHOLE TO EOP		10
EDGE OF ISLAND (NE) TO HANDHOLE		15
SOUTHEAST QUAD		
HANDHOLE TO EDGE OF ISLAND (SE)		60
NORTHWEST QUAD		
EOP TO DBL. HANDHOLE		45
SOUTHWEST QUAD		
MAST ARM POLE TO HANDHOLE		15
		Total
		145

HANDHOLE, PORTLAND CEMENT CONCRETE			
U.S. 67 (Godfrey Road)		Offset	Each
168+52.02	RT	67.9	1
171+14.86	RT	93.3	1
171+39.00	RT	56.0	1
172+78.99	RT	50.2	1
175+79.01	LT	32.0	1
		Subtotal	5
IL 111			
1173+27.47	LT	67.0	1
1174+87.00	LT	42.0	1
		Subtotal	2
Benjamin Drive			
10+48.23	LT	23.9	1
		Subtotal	1
		Total	8

ELECTRIC CABLE IN CONDUIT SERVICE NO. 6 2C		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
CONTROLLER		
Power Supply to Cabinet		35
		Total
		35

TRAFFIC SIGNAL POST, ALUMINUM 14 FT			
U.S. 67 (Godfrey Road)		Offset	Each
171+89.00	RT	50.6	1
172+64.00	LT	32.5	1
172+67.57	RT	50.2	1
		Total	3

INDUCTIVE LOOP DETECTOR		
U.S. 67 (Godfrey Road)		Each
SOUTH LEG		
		5
NORTH LEG		
		5
		Sub total
		10
IL 111		
		Each
		5
		Subtotal
		5
Benjamin Drive		
		Each
		1
		Sub total
		1
		Total
		16

SIGNAL HEAD, POLYCARB., LED 2-FACE, 3-SECT., BRACKET MOUNTED			
U.S. 67 (Godfrey Road)		Offset	Each
171+80.99	RT		1
172+67.57	RT		1
		Total	2

CONCRETE FOUNDATION TYPE A			
U.S. 67 (Godfrey Road)		Offset	Foot
171+89.00	RT	50.6	3
172+64.00	LT	32.5	3
172+67.57	RT	50.2	3
		Total	9

STEEL MAST ARM ASSEMBLY AND POLE, 46 FT			
U.S. 67 (Godfrey Road)		Offset	Each
173+28.67	RT	69.0	1
		Total	1

CONCRETE FOUNDATION TYPE D			
U.S. 67 (Godfrey Road)		Offset	Foot
172+87.00	LT	37.0	3
		Total	3

CONDUIT PUSHED, 3 1/2" DIA. GALVANIZED STEEL		
US 67-IL 111 / BENJAMIN DRIVE		FOOT
NORTHEAST-NORTHWEST QUADS		
UNDER US 67 PAVEMENT		75
		Total
		75

TRENCH AND BACKFILL FOR ELECTRICAL WORK, SPECIAL			
IL 111		Offset	Foot
1174+87		LT	10
		Total	10

REVISIONS	
NAME	DATE



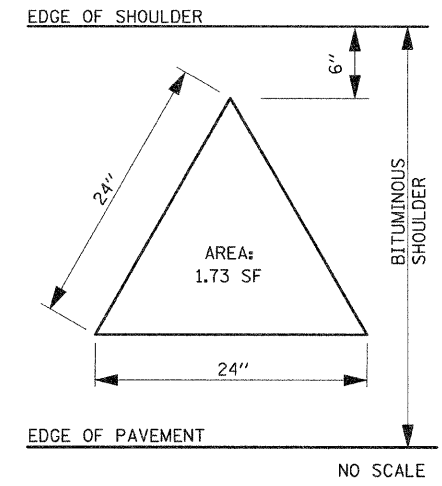
PIPE UNDERDRAINS 4"					PIPE UNDERDRAIN 4" SPECIAL	CONC HEADWALL FOR PIPE DRAIN EACH
LOCATION FAP 310	SOUTHBOUND LT.		NORTHBOUND RT.			
	OUTSIDE	MEDIAN	MEDIAN	OUTSIDE		
91+50 TO 94+49	299'				27'	1
91+50 TO 98+35		685'			31'	2
92+50 TO 98+37				587'	14'	1
92+50 TO 102+37			987'		30'	2
94+49 TO 98+17	367'				27'	1
102+39 TO 106+38			399'		16'	1
104+77 TO 106+40	173'				15'	1
106+30 TO 108+83				253'		
106+40 TO 107+93		153'				
107+42 TO 112+95			553'			
107+96 TO 112+91		495'			15'	1
109+52 TO 111+94	242'					
112+95 TO 117+45		450'			46'	3
113+05 TO 117+45			440'		15'	1
117+55 TO 122+50		495'	495'		14'	1
120+28 TO 122+50	222'				15'	1
122+50 TO 126+95	440'	440'	440'		44'	3
126+50 TO 131+50				500'	15'	1
127+06 TO 131+50	444'	444'	444'		45'	3
131+57 TO 135+47	390'	390'	390'	390'	60'	4
135+57 TO 139+50	393'	393'	393'	393'	60'	4
139+56 TO 142+95		339'	339'	339'	45'	3
139+56 TO 143+51	395'				14'	1
143+04 TO 147+45		441'	441'	441'	45'	3
143+56 TO 147+45	389'				15'	1
147+55 TO 149+45				190'	15'	1
149+50 TO 154+50	500'	500'	500'	500'	60'	4
154+55 TO 159+45	490'	490'	490'	490'	60'	4
160+99 TO 164+98			399'	399'	30'	2

LOCATION FAP 310	SOUTHBOUND LT.		NORTHBOUND RT.		PIPE UNDERDRAIN 4" SPECIAL	CONC HEADWALL FOR PIPE DRAIN EACH
	OUTSIDE	MEDIAN	MEDIAN	OUTSIDE		
161+42 TO 165+42	400'	400'			30'	2
177+55 TO 186+95	940'	940'	940'	940'	120'	8
187+06 TO 189+94	288'	288'			15'	1
187+06 TO 192+95			589'		15'	1
190+05 TO 192+95	290'	290'			15'	1
193+05 TO 196+70	365'	365'	365'		45'	3
196+75 TO 201+50	475'	475'	475'		45'	3
203+55 TO 209+34	579'	579'	579'	579'	60'	4
209+39 TO 214+45	506'	506'	506'	506'	60'	4
214+50 TO 219+45	495'	495'	495'	495'	60'	4
219+55 TO 224+00	445'	445'	445'	445'	60'	4
224+05 TO 225+00	95'	95'				
224+50 TO 228+25			420'	420'		
SUB TOTAL	9622'	10593'	11524'	7867'	1298'	85
LOCATION RAMP "B"	OUTSIDE RT	OUTSIDE LT			PIPE UNDERDRAIN 4" SPECIAL	CONC HEADWALL FOR PIPE DRAIN EACH
5+42 TO 6+00	68'					
8+44 TO 11+00	256'				15'	1
11+00 TO 15+00		400'			15'	1
22+00 TO 31+00		900'			30'	2
31+08 TO 40+65	957'				15'	1
41+08 TO 42+46	138'					
SUB TOTAL	1419'	1300'			75'	5
LOCATION RAMP "A"	OUTSIDE RT	OUTSIDE LT			PIPE UNDERDRAIN 4" SPECIAL	CONC HEADWALL FOR PIPE DRAIN EACH
0+00 TO 19+83	1983'				60'	4
LOCATION IL 111	OUTSIDE RT	OUTSIDE LT			PIPE UNDERDRAIN 4" SPECIAL	CONC HEADWALL FOR PIPE DRAIN EACH
1174+31 TO 1179+40	453'	509'			30'	2
LOCATION US 67	OUTSIDE RT	OUTSIDE LT			PIPE UNDERDRAIN 4" SPECIAL	CONC HEADWALL FOR PIPE DRAIN EACH
49+51 TO 56+34	683'	683'			30'	2
TOTAL	46636'				1493'	**

\* PIPE UNDERDRAINS CROSS ROAD 4"

LOCATION IL 111	LT FOOT	RT FOOT	GEO FABRIC SQ YD	CONC HEADWALL EACH	POROUS GRAN BACKFILL CU YD
1173+00 TO 1173+00	62'	72'	15.6	2	10
1175+00 TO 1175+00	37'	37'	8.6	2	6
TOTAL	208		24	102	16

\* SEE SHEET 214 FOR FURTHER DETAIL



UNDERDRAIN HEADWALL MARKER DETAIL  
24" WHITE THERMOPLASTIC TRIANGLE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

# UNDERDRAIN TABLES

SCALE: NO SCALE  
DATE: 09-04-09

DRAWN BY: U.J.J.  
CHECKED BY: J.J.C.

\*\* SEE NEXT COLUMN LABELED CONC HEADWALL FOR TOTAL

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	39
STA. 92+50 TO STA. 123+00			FED. AID PROJECT	
CONTRACT NO. 76311				

FAP 310 310-1 PI STA. = 80+34.80 Δ = 23° 43' 39" (LT) D = 1° 45' 19" R = 3,264.43' T = 685.77' L = 1,351.88' E = 71.25' e = 4.80% T.R. = 40.00' S.E. RUN = 216.00' P.C. STA. = 73+49.03 P.T. STA. = 87+00.91	FAP 310 310-2 PI STA. = 102+83.08 Δ = 4° 10' 45" (LT) D = 0° 49' 17" R = 6,976.16' T = 254.54' L = 508.86' E = 4.64' e = 2.70% T.R. = 48.00' S.E. RUN = 123.00' P.C. STA. = 100+28.54 P.T. STA. = 105+37.40	FAP 310 310-3 PI STA. = 111+57.88 Δ = 4° 41' 50" (RT) D = 0° 33' 53" R = 10,148.10' T = 416.21' L = 831.96' E = 8.53' e = 1.50% T.R. = 69.00' S.E. RUN = 69.00' P.C. STA. = 107+41.67 P.T. STA. = 115+73.63	ACCESS ROAD A2-1 PI STA. = 15+42.00 Δ = 138° 47' 00" (RT) D = 34° 43' 29" R = 165.00' T = 438.78' L = 399.67' E = 303.78' e = 4.00% T.R. = 33.00' S.E. RUN = 87.00' P.C. STA. = 11+03.22 P.T. STA. = 15+02.89	ACCESS ROAD A2-2 PI STA. = 16+62.76 Δ = 54° 22' 57" (LT) D = 34° 43' 29" R = 165.00' T = 84.77' L = 156.61' E = 20.50' e = 4.00% T.R. = 33.00' S.E. RUN = 87.00' P.C. STA. = 15+77.99 P.T. STA. = 17+34.60	ACCESS ROAD A2-3 PI STA. = 29+19.79 Δ = 34° 26' 03" (LT) D = 5° 32' 48" R = 1,033.00' T = 320.10' L = 620.82' E = 48.46' e = 3.50% T.R. = 21.00' S.E. RUN = 51.00' P.C. STA. = 25+99.68 P.T. STA. = 32+20.51	ACCESS ROAD A3-1 PI STA. = 15+53.00 Δ = 138° 47' 00" (LT) D = 34° 43' 29" R = 165.00' T = 438.78' L = 399.67' E = 303.78' e = 4.00% T.R. = 33.00' S.E. RUN = 87.00' P.C. STA. = 11+14.22 P.T. STA. = 15+13.89	ACCESS ROAD A3-2 PI STA. = 16+69.98 Δ = 51° 04' 23" (RT) D = 34° 43' 29" R = 165.00' T = 78.83' L = 147.08' E = 17.86' e = 4.00% T.R. = 33.00' S.E. RUN = 87.00' P.C. STA. = 15+91.15 P.T. STA. = 17+38.23	RAMP A RA-1 PI STA. = 2+30.43 Δ = 3° 30' 37" (RT) D = 0° 49' 48" R = 6,904.00' T = 211.55' L = 422.97' E = 3.24' e = 2.70% T.R. = NA S.E. RUN = NA P.C. STA. = 0+18.88 P.T. STA. = 4+41.85	RAMP A RA-2 PI STA. = 11+21.41 Δ = 34° 20' 18" (RT) D = 5° 39' 42" R = 1,012.00' T = 312.67' L = 606.51' E = 47.20' e = 8.00% T.R. = NA S.E. RUN = 249 P.C. STA. = 8+08.74 P.T. STA. = 14+15.25
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ACCESS ROAD A1-1 PI STA. = 14+25.00 Δ = 32° 40' 07" (RT) D = 21° 37' 16" R = 265.00' T = 77.66' L = 151.10' E = 11.15' e = 8.00% T.R. = 30.00' S.E. RUN = 147.00' P.C. STA. = 13+47.34 P.T. STA. = 14+98.43	ACCESS ROAD A1-2 PI STA. = 17+91.29 Δ = 32° 40' 07" (LT) D = 21° 37' 16" R = 265.00' T = 77.66' L = 151.10' E = 11.15' e = 8.00% T.R. = 30.00' S.E. RUN = 147.00' P.C. STA. = 17+13.63 P.T. STA. = 18+64.72	ACCESS ROAD A1-3 PI STA. = 39+11.97 Δ = 47° 48' 17" (LT) D = 8° 40' 52" R = 660.00' T = 292.50' L = 109.03' E = 61.91' e = 5.50% T.R. = 30.00' S.E. RUN = 102.00' P.C. STA. = 36+19.46 P.T. STA. = 41+70.13	ACCESS ROAD A1-4 PI STA. = 47+13.63 Δ = 23° 45' 34" (RT) D = 6° 59' 14" R = 820.00' T = 172.50' L = 340.04' E = 17.95' e = 5.50% T.R. = 30.00' S.E. RUN = 93.00' P.C. STA. = 45+41.13 P.T. STA. = 48+81.17	ACCESS ROAD A1-5 PI STA. = 61+00.00 Δ = 9° 27' 54" (RT) D = 8° 40' 52" R = 660.00' T = 54.64' L = 109.03' E = 2.26' e = 5.50% T.R. = 30.00' S.E. RUN = 102.00' P.C. STA. = 60+45.36 P.T. STA. = 61+54.39	ACCESS ROAD A1-6 PI STA. = 65+08.06 Δ = 5° 22' 05" (RT) D = 8° 40' 52" R = 660.00' T = 30.94' L = 61.84' E = 0.72' e = 5.50% T.R. = 30.00' S.E. RUN = 102.00' P.C. STA. = 64+77.12 P.T. STA. = 65+38.96	RAMP A RA-3 PI STA. = 20+00.75 Δ = 2° 52' 21" (RT) D = 0° 46' 34" R = 7,381.89' T = 185.09' L = 370.09' E = 2.32' e = N.C. T.R. = NA S.E. RUN = NA P.C. STA. = 18+15.66 P.T. STA. = 21+85.76	RAMP B RB-1 PI STA. = 8+79.54 Δ = 4° 17' 57" (RT) D = 0° 46' 34" R = 7,381.89' T = 277.08' L = 553.89' E = 5.20' e = N.C. T.R. = NA S.E. RUN = NA P.C. STA. = 6+02.46 P.T. STA. = 11+56.35
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RAMP B RB-3 PI STA. = 33+73.93 Δ = 12° 15' 57" (RT) D = 3° 21' 54" R = 1,702.76' T = 182.96' L = 364.52' E = 9.80' e = 6.50% T.R. = 54 S.E. RUN = 204 P.C. STA. = 31+90.97 P.T. STA. = 35+55.49	RAMP B RB-4 PI STA. = 38+16.03 Δ = 2° 57' 42" (RT) D = 0° 34' 07" R = 10,078.55' T = 260.53' L = 520.95' E = 3.37' e = 3.20% T.R. = NA S.E. RUN = NA P.C. STA. = 35+55.50 P.T. STA. = 40+76.45
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U.S. 67 (GODFREY ROAD)		
DESCRIPTION	NORTH	EAST
P.O.T. Station 160+00.00	837099.0108	2289389.2339
Sta 177+10.39(BK) = Sta 45+00.00(AH)	838775.9468	2289052.6158
P.C. Station 61+68.11	840411.4300	2288724.3186

F.A.P. ROUTE 310		
DESCRIPTION	NORTH	EAST
P.O.T. Station 56+27.91	837868.7856	2290838.8640
P.C. Station 73+49.03	839389.0977	2290032.0701
P.L. Station 80+34.80	839994.8527	2289710.6101
P.T. Station 87+00.91	840420.0517	2289172.5751
P.C. Station 100+28.54	841243.2278	2288130.9509
P.L. Station 102+83.08	841401.0523	2287931.2442
P.T. Station 105+37.40	841543.9030	2287720.5666
P.C. Station 107+41.67	841658.5433	2287551.4940
P.L. Station 111+57.88	841892.1258	2287207.0042
P.T. Station 115+73.63	842153.1341	2286882.7996
P.C. Station 121+55.12	842517.7847	2286429.8584

BENJAMIN DRIVE		
DESCRIPTION	NORTH	EAST
P.O.T. Station 10+00.00	838281.5220	2289151.8630
P.O.T. Station 13+00.00	838280.4220	2288851.8640

BUSINESS U.S. 67 - ILLINOIS ROUTE 111		
DESCRIPTION	NORTH	EAST
P.O.T. Station 1172+06.10	838281.5220	2289151.8640
P.O.T. Station 1202+06.10	839693.5580	2291798.7780

ACCESS ROAD #1			
NODE	DESCRIPTION	NORTH	EAST
P.O.T.	Station 10+00.00	839273.1481	2291010.7040
P.C.	Station 13+47.34	839531.4672	2290778.5107
P.L.	Station 14+25.00	839589.2268	2290726.5927
P.T.	Station 14+98.43	839665.8735	2290714.0653
P.C.	Station 17+13.63	839878.2500	2290679.3537
P.L.	Station 17+91.29	839954.8967	2290666.8262
P.T.	Station 18+64.72	840012.6563	2290614.9083
P.C.	Station 36+19.46	841317.6812	2289441.8705
P.L.	Station 39+11.97	841535.2204	2289246.3327
P.T.	Station 41+70.13	841536.4670	2288953.8317
P.C.	Station 45+41.13	841538.0482	2288582.8367
P.L.	Station 47+13.63	841538.7834	2288410.3403
P.T.	Station 48+81.17	841608.9547	2288252.7600
P.C.	Station 60+45.36	842082.5423	2287189.2462
P.L.	Station 61+00.00	842104.7688	2287139.3331
P.T.	Station 61+54.39	842134.9007	2287093.7545
P.C.	Station 64+77.12	842312.8820	2286824.5329
P.L.	Station 65+08.06	842329.9450	2286798.7228
P.T.	Station 65+38.96	842349.3478	2286714.6221

ACCESS ROAD #2			
NODE	DESCRIPTION	NORTH	EAST
P.O.T.	Station 10+00.00	839154.3970	2288976.6480
P.C.	Station 11+03.22	839134.0832	2288875.4482
P.L.	Station 15+42.00	839047.7274	2288445.2483
P.T.	Station 15+02.89	839396.1485	2288711.9541
P.C.	Station 15+77.99	839455.7890	2288757.6071
P.L.	Station 16+62.76	839523.0993	2288809.1311
P.T.	Station 17+34.60	839604.1839	2288784.4191
P.C.	Station 25+99.68	840431.6871	2288532.2219
P.L.	Station 29+19.79	840737.8860	2288438.9020
P.T.	Station 32+20.51	840937.6634	2288188.7913
P.O.T.	Station 41+32.06	841506.5640	2287476.5580

RAMP A			
NODE	DESCRIPTION	NORTH	EAST
P.O.T.	Station 0+00.00	841468.3713	2287742.3978
P.C.	Station 0+18.88	841456.7828	2287757.3002
P.L.	Station 2+30.43	841326.9189	2287924.3012
P.T.	Station 4+41.85	841187.0738	2283519.1781
P.C.	Station 8+08.74	840944.5416	2288358.3334
P.L.	Station 11+21.41	840737.8536	2288592.9427
P.T.	Station 14+15.25	840434.8490	2288670.0769
P.C.	Station 18+15.66	840046.8067	2288768.8588
P.L.	Station 20+00.75	839867.4414	2288814.5188
P.T.	Station 21+85.76	839686.0132	2288851.1328
P.O.T.	Station 22+86.36	839587.4003	2288871.0338
P.O.T.	Station 28+28.22	839056.2030	2288977.9997

RAMP B			
NODE	DESCRIPTION	NORTH	EAST
P.O.T.	Station 0+00.00	839068.0115	2289036.8263
P.O.T.	Station 2+19.76	839283.4734	2288993.5757
P.O.T.	Station 5+41.86	839599.4233	2288930.9290
P.C.	Station 6+02.46	839658.8704	2288919.1719
P.L.	Station 8+79.54	839930.6822	2288865.4144
P.T.	Station 11+56.35	840205.7590	2288832.1842
P.C.	Station 12+89.52	840337.9647	2288816.2133
P.L.	Station 22+49.35	841290.8645	2288701.0999
P.T.	Station 30+26.40	841657.3561	2287813.9962
P.C.	Station 31+90.97	841720.1954	2287661.8919
P.L.	Station 33+73.93	841790.0554	2287492.7937
P.T.	Station 35+55.49	841894.2450	2287342.3970
P.C.	Station 35+55.50	841894.2473	2287342.3937
P.L.	Station 38+16.03	842042.6119	2287128.2308
P.T.	Station 40+76.45	842201.8434	2286922.0193
P.O.T.	Station 47+03.34	842584.9858	2286425.8336

INGHAM LANE		
DESCRIPTION	NORTH	EAST
P.O.T. Station 8+00.00	841338.0560	2288582.9680
P.O.T. Station 12+00.00	841738.0180	2288588.4970

SPECIAL DITCH NO. 1		
DESCRIPTION	NORTH	EAST
P.O.T. Station 10+00.00	842035.9060	228722.4990
P.O.T. Station 15+00.00	842535.9060	228722.5750

ACCESS ROAD #3			
NODE	DESCRIPTION	NORTH	EAST
P.O.T.	Station 10+00.00	839154.3974	2288976.6478
P.C.	Station 11+4.22	839176.8765	2289088.6324
P.L.	Station 15+53.00	839263.2323	2289518.8323
P.T.	Station 15+13.89	839481.7358	2289138.3254
P.C.	Station 15+91.15	839520.2114	2289071.3230
P.L.	Station 16+69.98	839559.4670	2289002.9624
P.T.	Station 17+38.23	839637.3134	2288990.5482
P.O.T.	Station 21+48.06	840042.0277	2288926.0083

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NAME	DATE

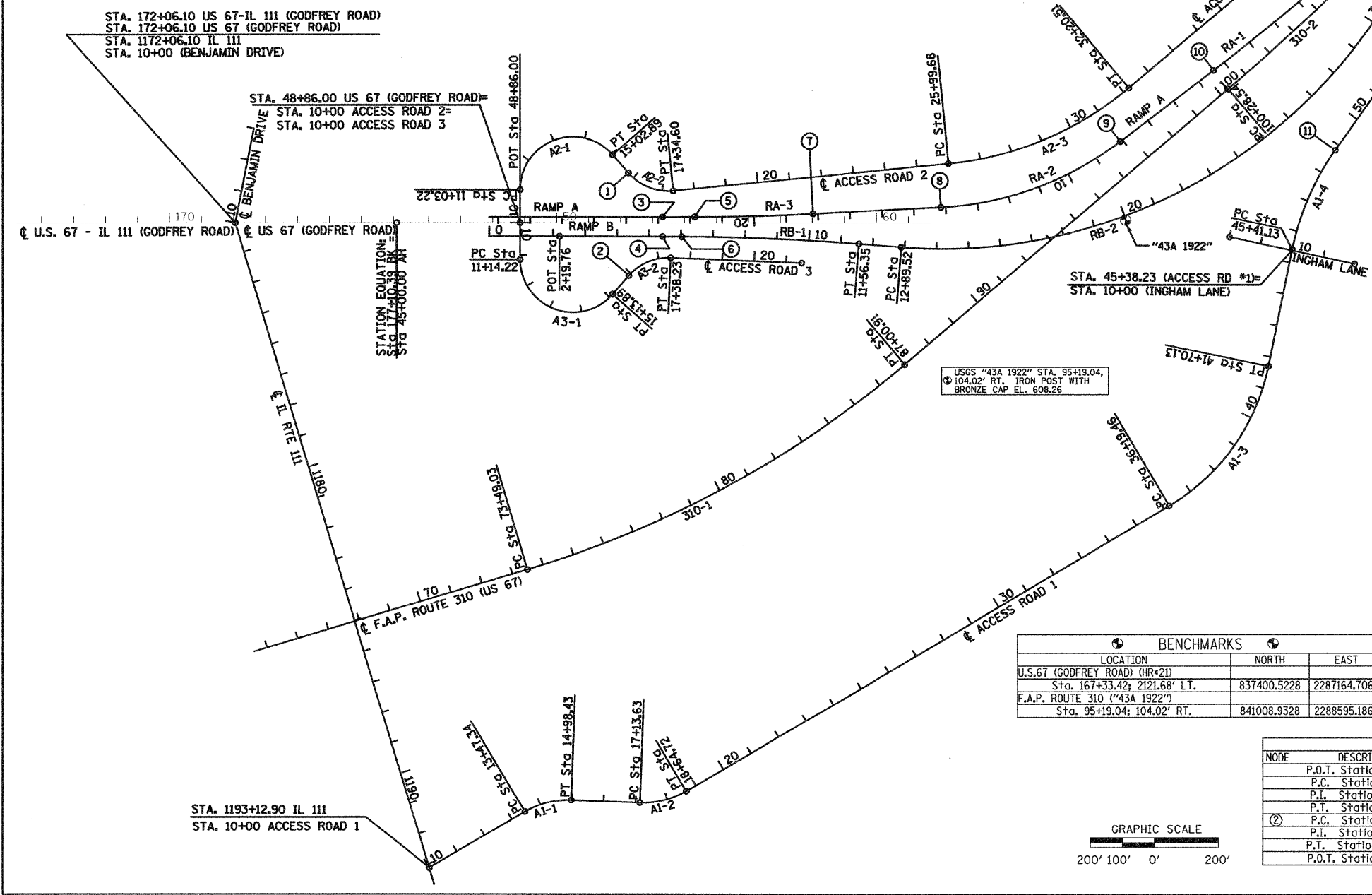
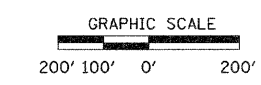
ILLINOIS DEPARTMENT OF TRANSPORTATION

# HORIZONTAL CONTROL

SCALE: VERT. 1"=200'  
HORIZ. 1"=200'  
DATE: 09-04-09

DRAWN BY: IWC  
CHECKED BY: MAR

BENCHMARKS		
LOCATION	NORTH	EAST
U.S. 67 (GODFREY ROAD) (HR#21)		
Sta. 167+33.42; 2121.68' L.T.	837400.5228	2287164.7066
F.A.P. ROUTE 310 ("43A 1922")		
Sta. 95+19.04; 104.02' RT.	841008.9328	2288595.1862



STA. 172+06.10 US 67-IL 111 (GODFREY ROAD)  
STA. 172+06.10 US 67 (GODFREY ROAD)  
STA. 1172+06.10 IL 111  
STA. 10+00 (BENJAMIN DRIVE)

STA. 48+86.00 US 67 (GODFREY ROAD)=  
STA. 10+00 ACCESS ROAD 2=  
STA. 10+00 ACCESS ROAD 3

STATION EQUATION:  
Sta 45+00.00 AH  
Sta 45+00.00 AH

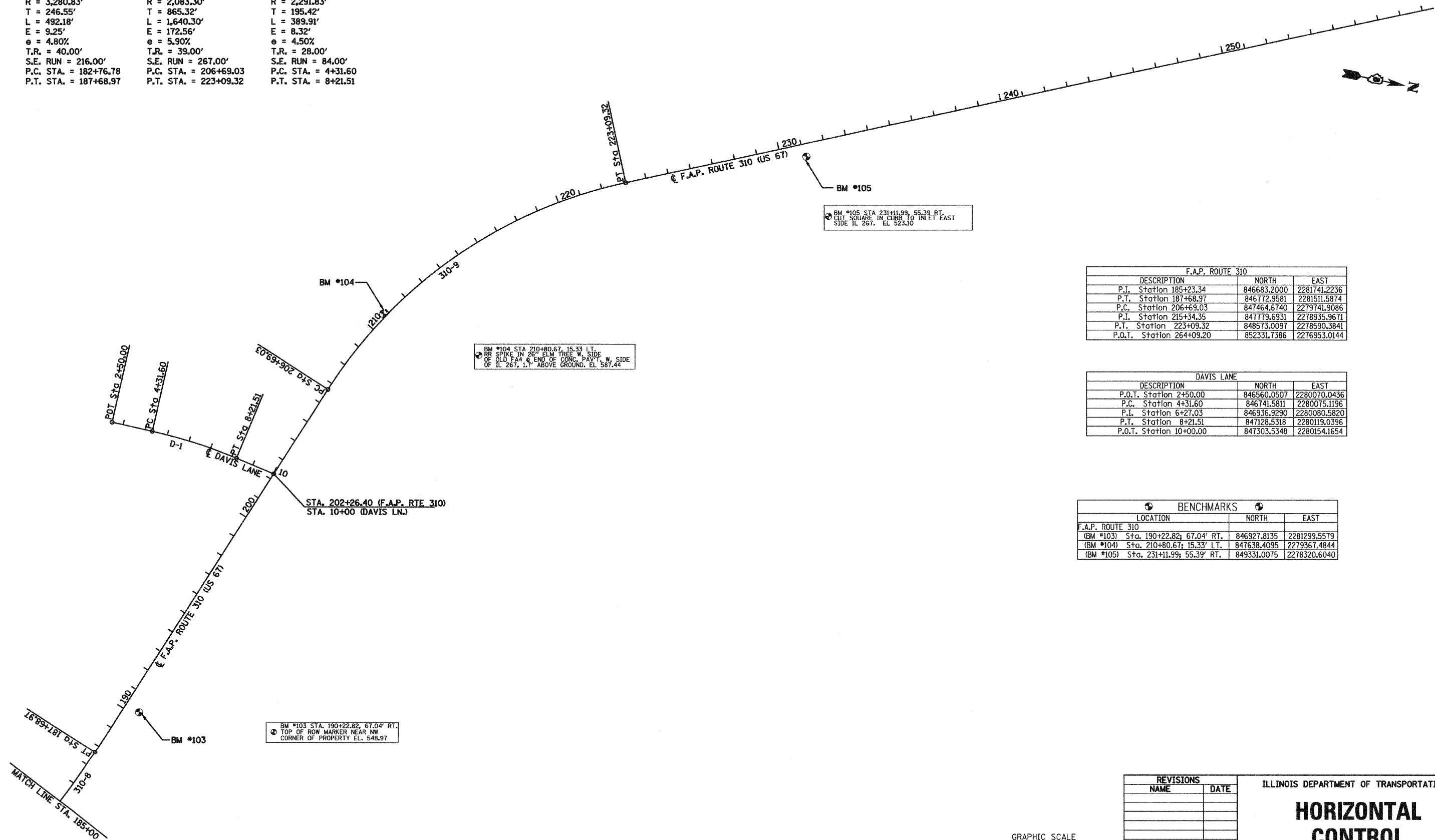
USGS "43A 1922" STA. 95+19.04,  
104.02' RT. IRON POST WITH  
BRONZE CAP EL. 608.26

STA. 1193+12.90 IL 111  
STA. 10+00 ACCESS ROAD 1



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	41
STA. 185+00		TO STA. 225+00		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				

F.A.P. 310 310-8 PI STA. = 185+23.34 $\Delta = 8^\circ 35' 43''$ (LT) D = 1° 44' 47" R = 3,280.83' T = 246.55' L = 492.18' E = 9.25' e = 4.80% T.R. = 40.00' S.E. RUN = 216.00' P.C. STA. = 182+76.78 P.T. STA. = 187+68.97	F.A.P. 310 310-9 PI STA. = 215+34.35 $\Delta = 45^\circ 06' 44''$ (RT) D = 2° 45' 01" R = 2,083.30' T = 865.32' L = 1,640.30' E = 172.56' e = 5.90% T.R. = 39.00' S.E. RUN = 267.00' P.C. STA. = 206+69.03 P.T. STA. = 223+09.32	DAVIS LANE D-1 PI STA. = 6+27.03 $\Delta = 9^\circ 44' 51''$ (RT) D = 2° 30' 00" R = 2,291.83' T = 195.42' L = 389.91' E = 8.32' e = 4.50% T.R. = 28.00' S.E. RUN = 84.00' P.C. STA. = 4+31.60 P.T. STA. = 8+21.51
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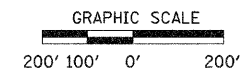
F.A.P. ROUTE 310		
DESCRIPTION	NORTH	EAST
P.I. Station 185+23.34	846683.2000	2281741.2236
P.T. Station 187+68.97	846772.9581	2281511.5874
P.C. Station 206+69.03	847464.6740	2279741.9086
P.I. Station 215+34.35	847779.6931	2278935.9671
P.T. Station 223+09.32	848573.0097	2278590.3841
P.O.T. Station 264+09.20	852331.7386	2276953.0144

DAVIS LANE		
DESCRIPTION	NORTH	EAST
P.O.T. Station 2+50.00	846560.0507	2280070.0436
P.C. Station 4+31.60	846741.5811	2280075.1196
P.I. Station 6+27.03	846936.9290	2280080.5820
P.T. Station 8+21.51	847128.5318	2280119.0396
P.O.T. Station 10+00.00	847303.5348	2280154.1654

BENCHMARKS		
LOCATION	NORTH	EAST
F.A.P. ROUTE 310 (BM #103) Sta. 190+22.82; 67.04' RT.	846927.8135	2281299.5579
(BM #104) Sta. 210+80.67; 15.33' LT.	847638.4095	2279367.4844
(BM #105) Sta. 231+11.99; 55.39' RT.	849331.0075	2278320.6040

BM #104 STA 210+80.67, 15.33 LT.  
BR SPIKE IN 26" ELM TREE W. SIDE  
OF OLD FA4 @ END OF CONC. PAVT. W. SIDE  
OF IL 267, 1.7' ABOVE GROUND. EL. 587.44

BM #103 STA. 190+22.82, 67.04' RT.  
TOP OF ROW MARKER NEAR NW  
CORNER OF PROPERTY EL. 548.97

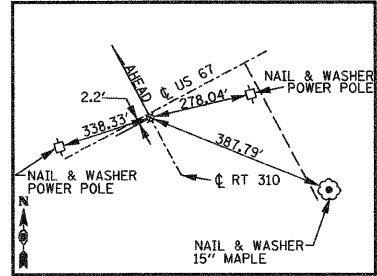


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NAME	DATE

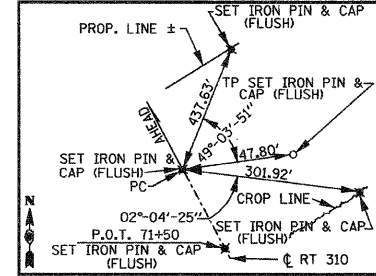
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**HORIZONTAL CONTROL**  
SCALE: VERT. 1"=200'  
HORIZ. 1"=200'  
DATE: 09-04-09  
DRAWN BY: TWC  
CHECKED BY: MAR

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	42
STA.		TO STA.		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		

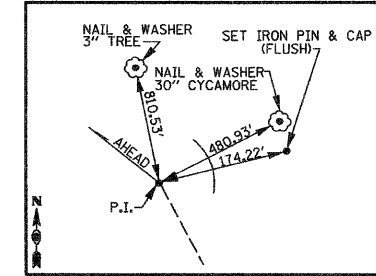
CONTRACT NO. 76311



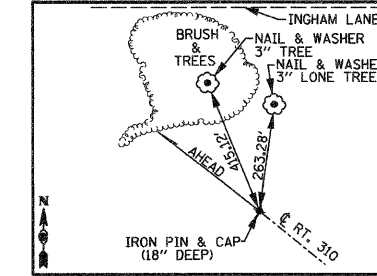
P.O.T. STA. 67+83.55 F.A.P. RTE. 310  
PK NAIL SET



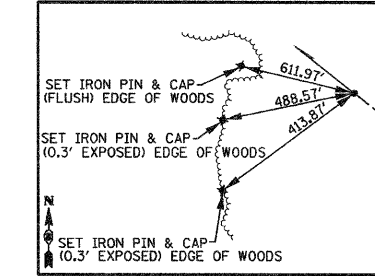
PC STA. 73+49.03 F.A.P. RTE. 310  
IRON PIN & CAP (FLUSH)



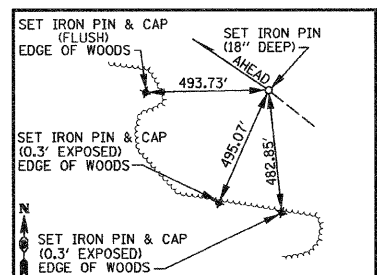
PI STA. 80+34.80 F.A.P. RTE. 310  
IRON PIN & CAP (FLUSH)



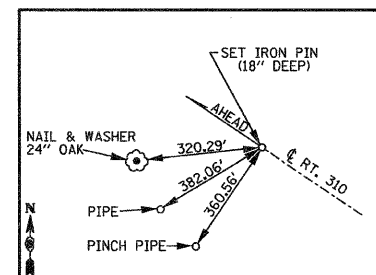
PT STA. 87+00.91 F.A.P. RTE. 310



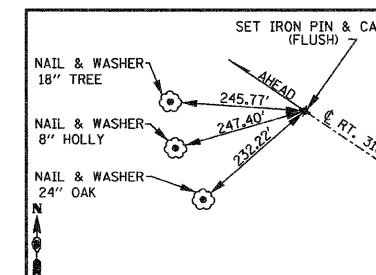
PC STA. 100+28.54 F.A.P. RTE. 310  
60 SPIKE SET OVER IRON PIN (18"DEEP)



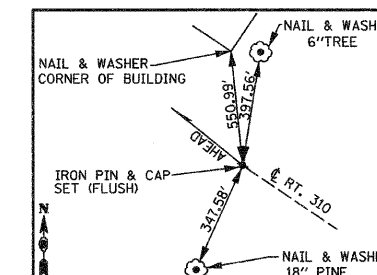
P.I. STA. 102+83.08 F.A.P. RTE. 310  
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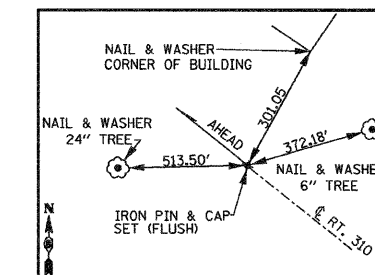
P.T. STA. 105+37.40 F.A.P. RTE. 310  
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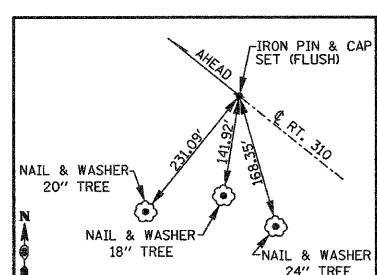
PC STA. 107+41.67 F.A.P. RTE. 310  
IRON PIN & CAP SET (FLUSH)



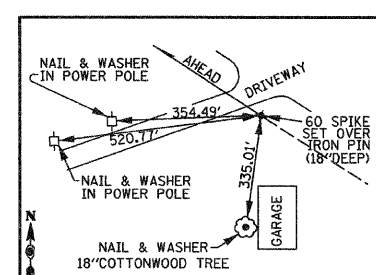
PI STA. 111+57.88 F.A.P. RTE. 310  
IRON PIN & CAP SET (FLUSH)



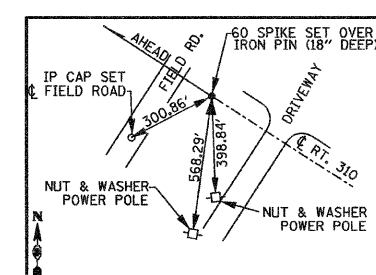
P.T. STA. 115+73.63 F.A.P. RTE. 310  
IRON PIN & CAP SET (FLUSH)



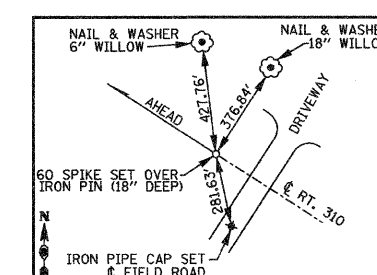
PC STA. 121+55.12 F.A.P. RTE. 310  
IRON PIN & CAP SET (FLUSH)



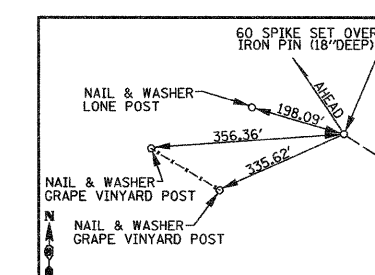
PI STA. 124+63.18 F.A.P. RTE. 310  
60 SPIKE SET OVER IRON PIN (18"DEEP)



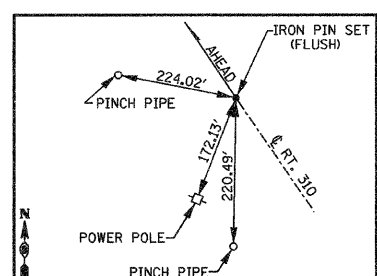
PT STA. 127+70.65 F.A.P. RTE. 310  
60 SPIKE SET OVER IRON PIN (18"DEEP)



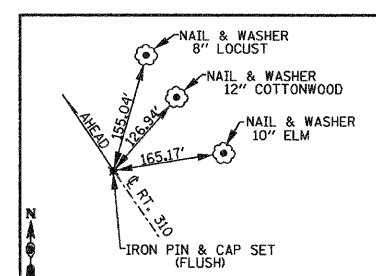
PC STA. 133+14.00 F.A.P. RTE. 310  
60 SPIKE SET OVER IRON PIN (18"DEEP)



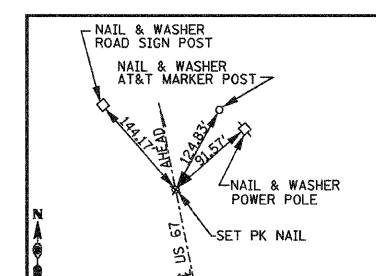
PI STA. 139+88.75 F.A.P. RTE. 310  
IRON PIN & CAP SET (FLUSH)



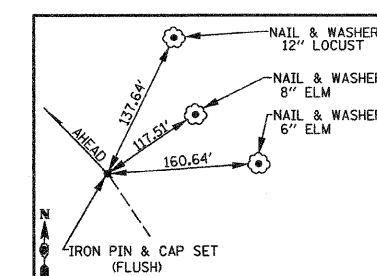
PT STA. 146+44.95 F.A.P. RTE. 310  
IRON PIN SET (FLUSH)



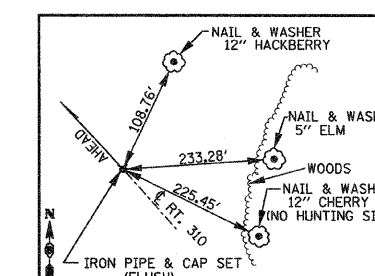
PC STA. 158+11.69 F.A.P. RTE. 310  
IRON PIN & CAP SET (FLUSH)



POT STA. 160+00 F.A.P. RTE. 310  
SET PK NAIL



PI STA. 160+96.28 F.A.P. RTE. 310  
IRON PIN & CAP SET (FLUSH)



PCC STA. 163+79.84 F.A.P. RTE. 310  
IRON PIN & CAP SET (FLUSH)

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NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

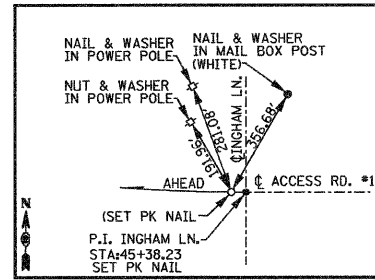
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DATE: 09-04-09

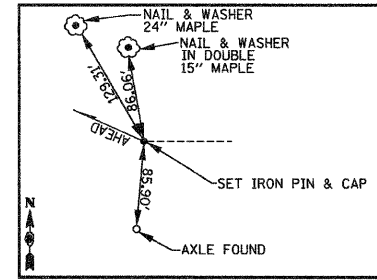
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CHECKED BY: J.J.C.

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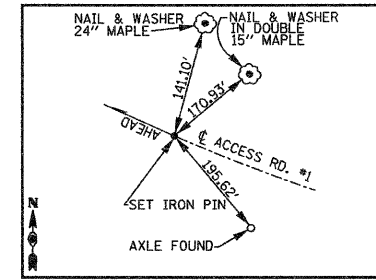




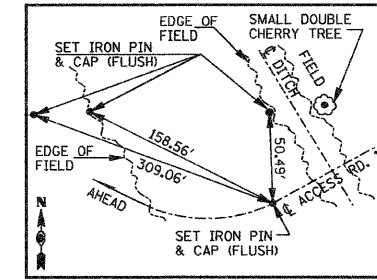
PC STA. 45+41.13 ACCESS ROAD #1  
SET PK NAIL



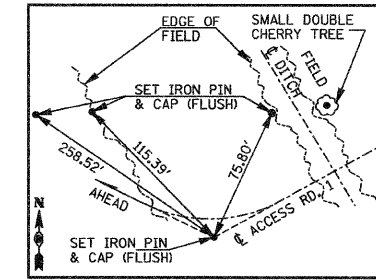
PI STA. 47+13.63 ACCESS ROAD #1  
SET IRON PIN & CAP (FLUSH)



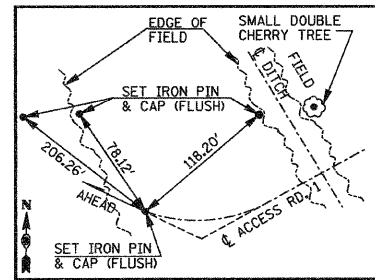
PT STA. 48+81.17 ACCESS ROAD #1  
SET IRON PIN (18\"/>



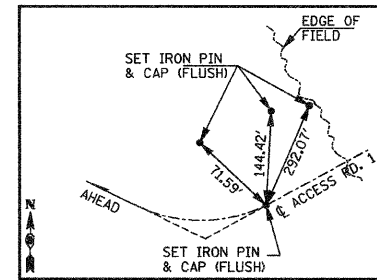
PC STA. 60+45.36 ACCESS ROAD #1  
SET IRON PIN & CAP (FLUSH)



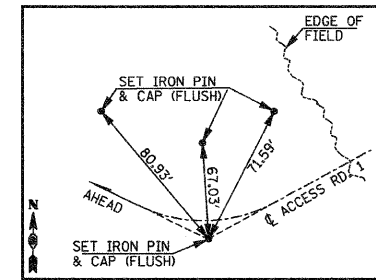
PI STA. 61+00.00 ACCESS ROAD #1  
SET IRON PIN & CAP (FLUSH)



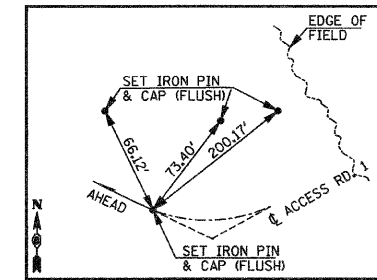
PT STA. 61+54.39 ACCESS ROAD #1



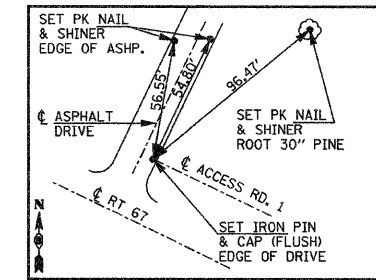
PC STA. 64+77.12 ACCESS ROAD #1  
SET IRON PIN & CAP (FLUSH)



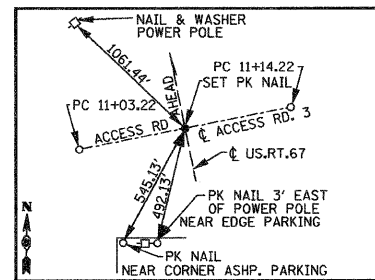
PI STA. 65+08.06 ACCESS ROAD #1  
SET IRON PIN & CAP (FLUSH)



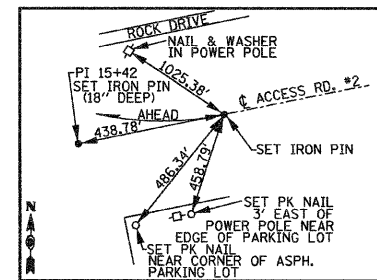
PT STA. 65+38.96 ACCESS ROAD #1  
SET IRON PIN & CAP (FLUSH)



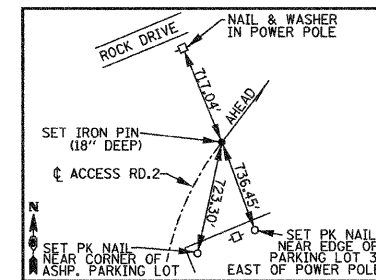
P.O.T. STA. 73+22.00 ACCESS ROAD #1  
SET PK NAIL IN ASPHALT DRIVE  
END CONSTRUCTION



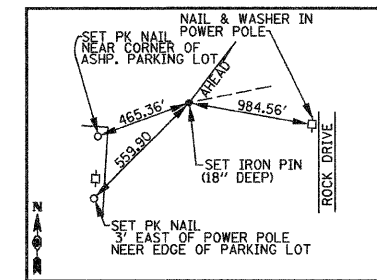
PI STA. 10+00 ACCESS ROAD #2 &  
PI STA. 10+00 ACCESS ROAD #3 =  
STA. 48+86.00 US. 67 (GODFREY RD.)



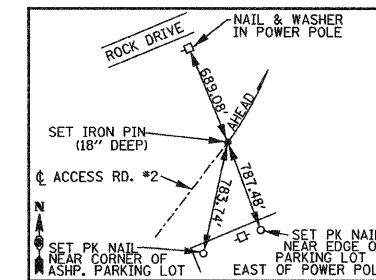
PC STA. 11+03.22 ACCESS ROAD #2  
SET IRON PIN (18\"/>



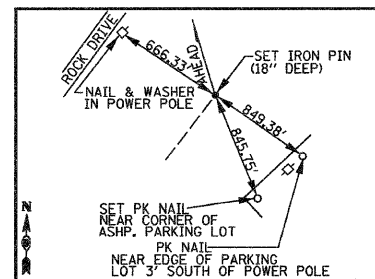
PT STA. 15+02.89 ACCESS ROAD #2  
SET IRON PIN (18\"/>



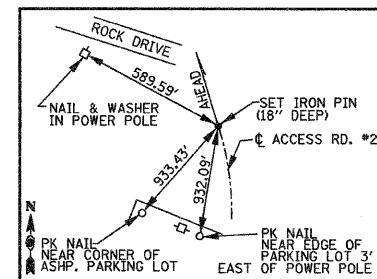
PI STA. 15+42 ACCESS ROAD #2  
IRON PIN (18\"/>



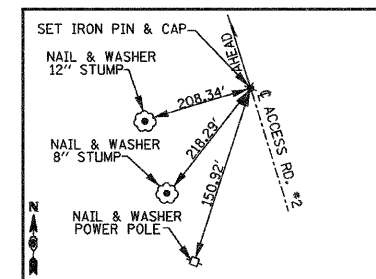
PC STA. 15+77.99 ACCESS ROAD #2  
SET IRON PIN (18\"/>



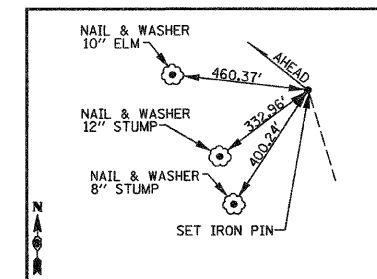
PI STA. 16+62.76 ACCESS ROAD #2  
SET IRON PIN (18\"/>



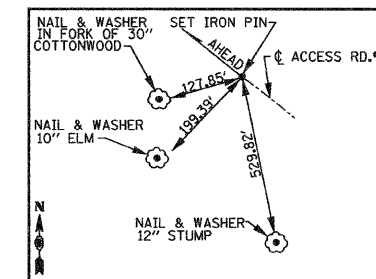
PT STA. 17+34.60 ACCESS ROAD #2  
SET IRON PIN (18\"/>



PC STA. 25+99.68 ACCESS ROAD #2  
IRON PIN & CAP (FLUSH)



PI STA. 29+19.79 ACCESS ROAD #2  
SET IRON PIN (18\"/>



PT STA. 32+20.51 ACCESS ROAD #2  
SET IRON PIN (18\"/>

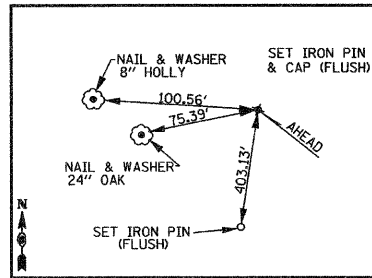
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ILLINOIS DEPARTMENT OF TRANSPORTATION

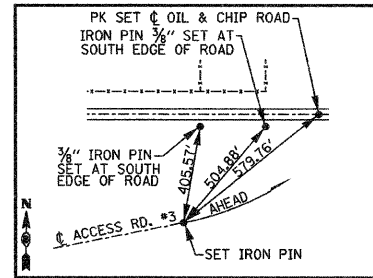
# REFERENCE TIES

SCALE: NO SCALE  
DATE: 09-04-09

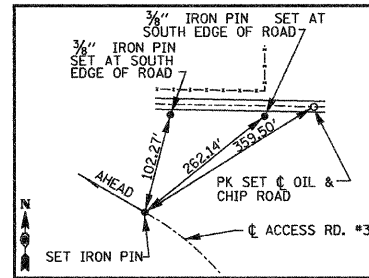
DRAWN BY: U.J.  
CHECKED BY: J.J.C.



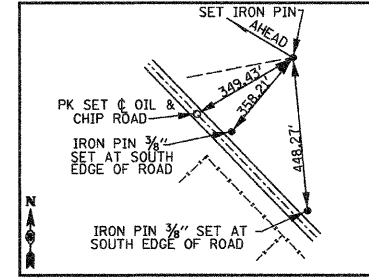
P.O.T. STA. 41+32.06 ACCESS ROAD #2  
SET IRON PIN & CAP (FLUSH)



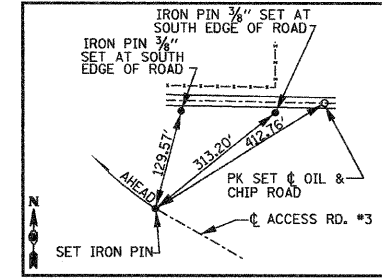
PC STA. 11+14.22 ACCESS ROAD #3  
SET IRON PIN (18" DEEP)



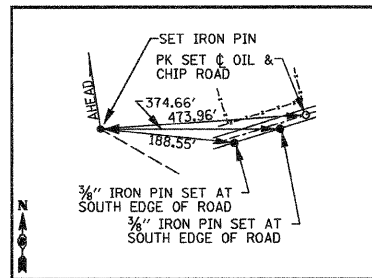
PT STA. 15+13.89 ACCESS ROAD #3  
SET IRON PIN (18" DEEP)



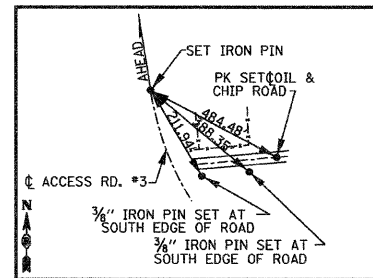
PI STA. 15+53 ACCESS ROAD #3  
IRON PIN (18" DEEP)



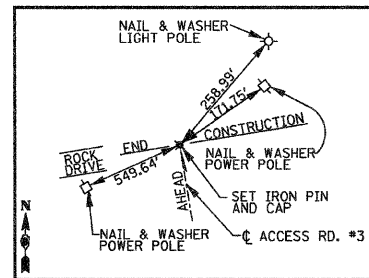
PC STA. 15+91.15 ACCESS ROAD #3  
SET IRON PIN (18" DEEP)



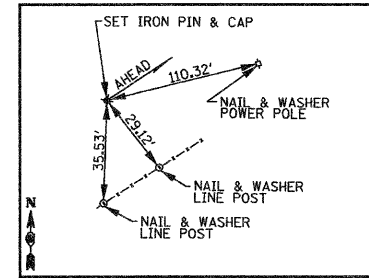
PI STA. 16+69.98 ACCESS ROAD #3  
SET IRON PIN (18" DEEP)



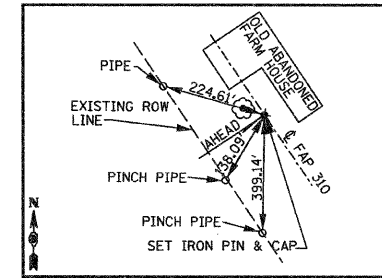
PT STA. 17+38.23 ACCESS ROAD #3  
SET IRON PIN (18" DEEP)



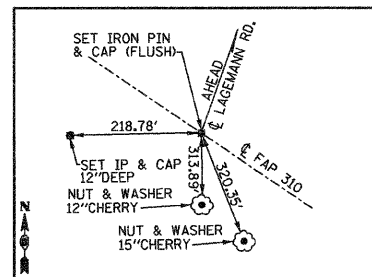
P.O.T. STA. 21+48.06 ACCESS ROAD #3  
SET IRON PIN & CAP (FLUSH)



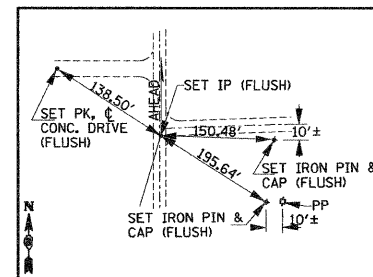
POT STA. 5+00.00 BEUTTEL ROAD  
SET IRON PIN & CAP 1' RIGHT OF C.R. ROAD (FLUSH)



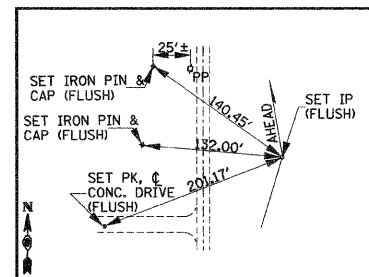
PI STA. 10+00 BEUTTEL ROAD=  
STA. 148+46.71 STA. F.A.P. 310  
SET IRON PIN & CAP (10.4' EXPOSED)



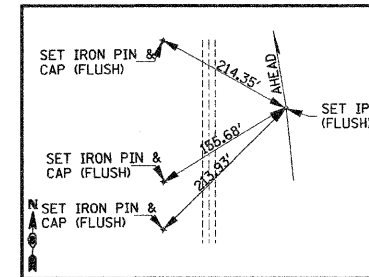
PI STA. 10+00.00 LAGEMANN RD. =  
STA. 170+40.00 RT. F.A.P. 310  
SET IRON PIN & CAP (FLUSH)



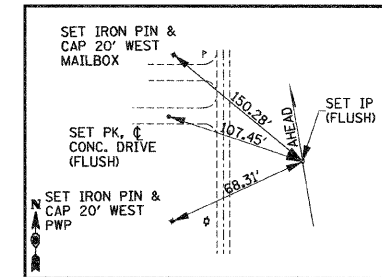
PC STA. 14+32.48 LAGEMANN ROAD  
SET IRON PIN (FLUSH)



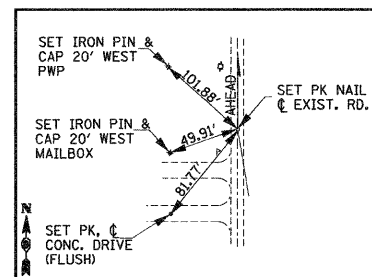
PI STA. 16+23.15 LAGEMANN ROAD  
SET IRON PIN (FLUSH)



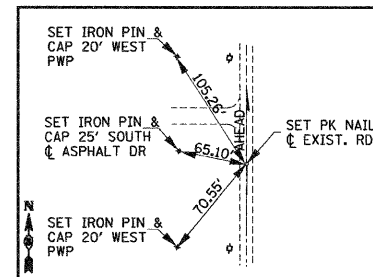
PT STA. 18+06.84 LAGEMANN ROAD  
SET IRON PIN (FLUSH)



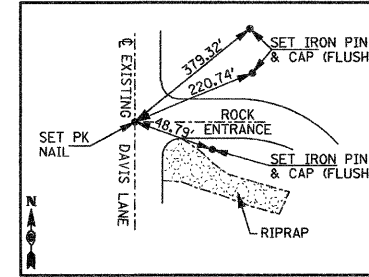
PC STA. 20+16.61 LAGEMANN ROAD  
SET IRON PIN (FLUSH)



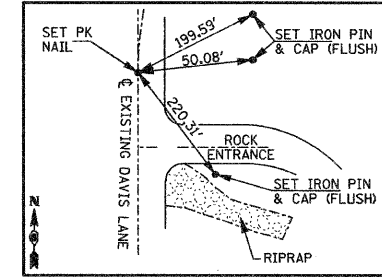
PI STA. 21+68.22 LAGEMANN ROAD  
SET PK NAIL



PT STA. 23+19.66 LAGEMANN ROAD  
SET PK NAIL



POT STA. 2+75 DAVIS LANE  
SET PK NAIL



PC STA. 4+31.60 DAVIS LANE  
SET PK NAIL

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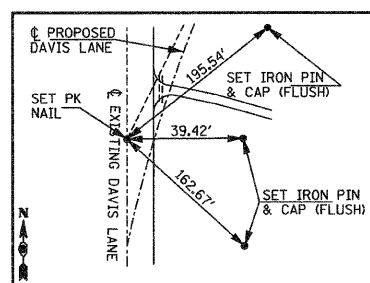
ILLINOIS DEPARTMENT OF TRANSPORTATION

# REFERENCE TIES

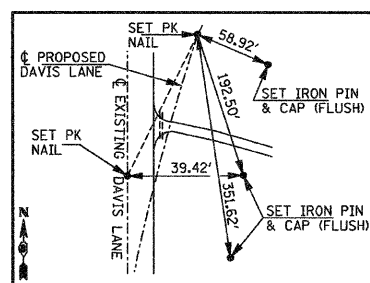
SCALE: NO SCALE  
DATE: 09-04-09

DRAWN BY: U.J.  
CHECKED BY: J.J.C.

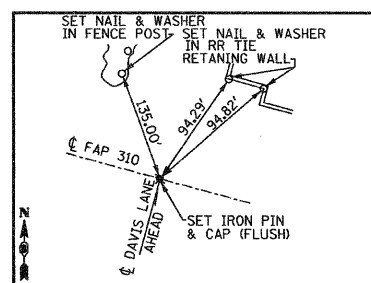




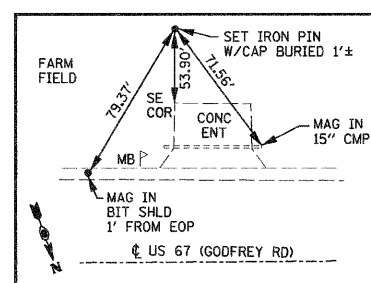
PI 6+27.03 DAVIS LANE  
SET PK NAIL



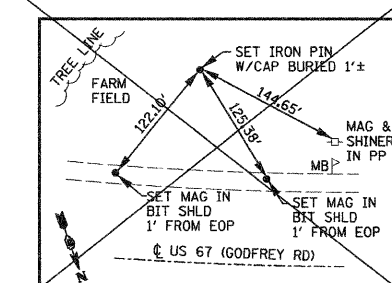
PT STA. 8+21.51 DAVIS LANE  
SET IRON PIN & CAP (FLUSH)



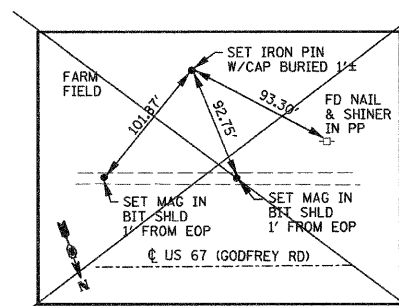
PI STA. 10+00.00 DAVIS LANE=  
F.A.P. 310 STA. 202+26.40  
SET IRON PIN & CAP (FLUSH)



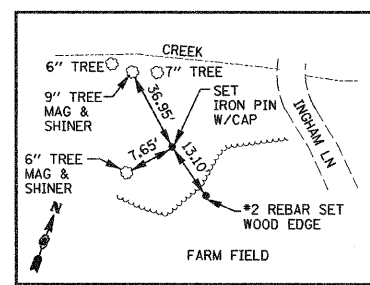
STA 0+00.00 RAMP "A"=  
50' LT STA 104+76.50 FAP RTE 310  
SET IRON PIN W/CAP BURIED



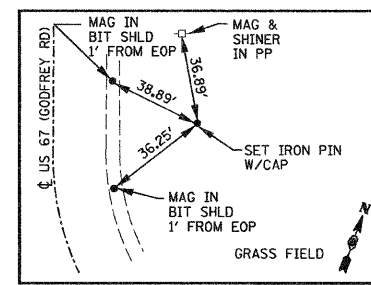
PC STA 1+86.11 RAMP "A"  
SET IRON PIN W/CAP BURIED



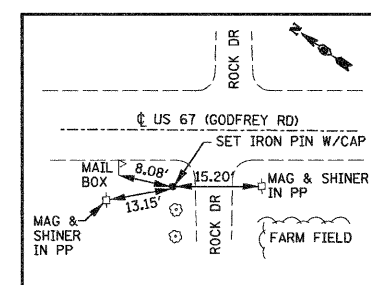
PT STA 4+79.52 RAMP "A"  
SET IRON PIN W/CAP BURIED



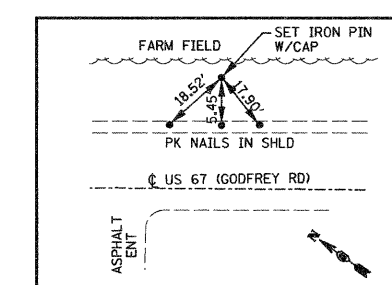
PC STA 8+08.74 RAMP "A"  
SET IRON PIN W/CAP



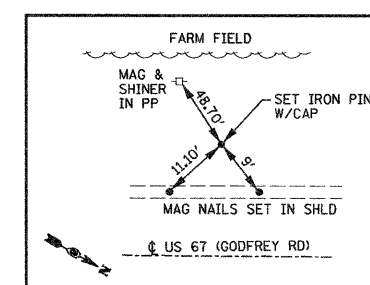
PT STA 14+15.25 RAMP "A"  
SET IRON PIN W/CAP



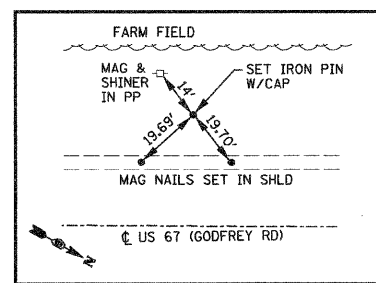
PC STA 18+15.66 RAMP "A"  
SET IRON PIN W/CAP



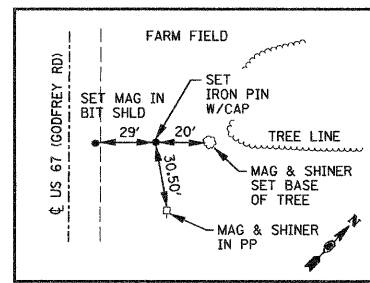
PT STA 21+85.76 RAMP "A"  
SET IRON PIN W/CAP



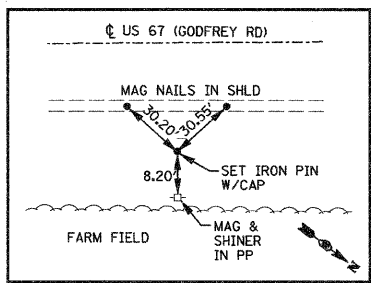
POT STA 22+86.36 RAMP "A"  
SET IRON PIN W/CAP



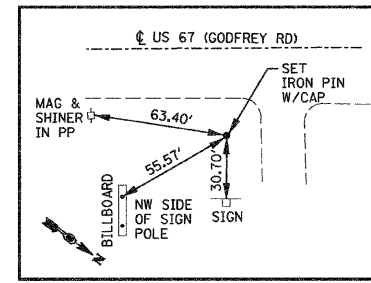
STA 28+28.22 RAMP "A"=  
18' LT STA 47+89.46 US 67  
SET IRON PIN W/CAP



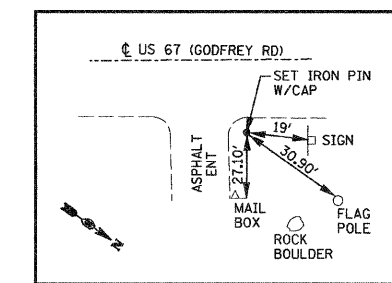
STA 0+00.00 RAMP "B"=  
42' RT STA 47+89.46 US 67  
SET IRON PIN W/CAP



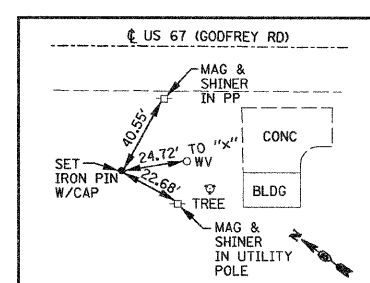
PI STA 2+19.76 RAMP "B"  
SET IRON PIN W/CAP



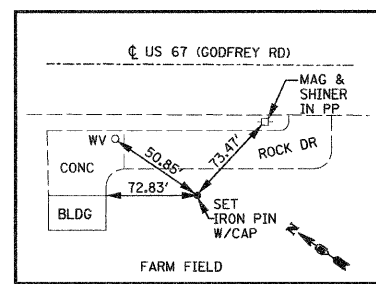
PI STA 5+41.86 RAMP "B"  
SET IRON PIN W/CAP



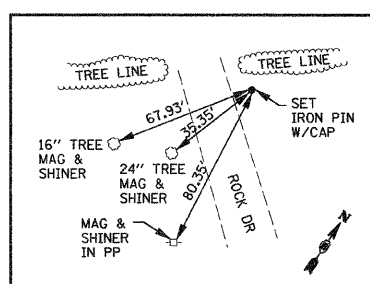
PC STA 6+02.46 RAMP "B"  
SET IRON PIN W/CAP



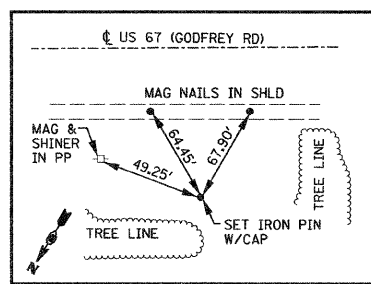
PT STA 11+56.35 RAMP "B"  
SET IRON PIN W/CAP



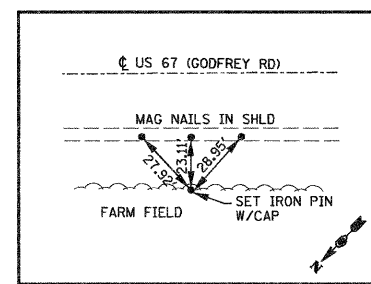
PC STA 12+89.52 RAMP "B"  
SET IRON PIN W/CAP



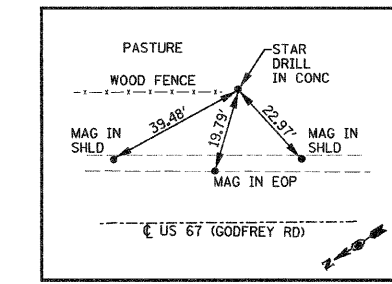
PT STA 30+26.40 RAMP "B"  
SET IRON PIN W/CAP



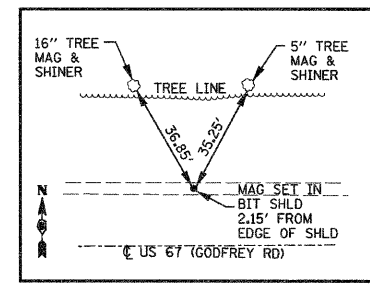
PC STA 31+90.97 RAMP "B"  
SET IRON PIN W/CAP



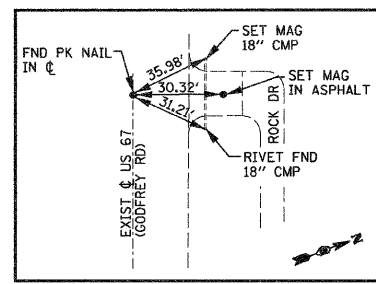
PCC STA 35+55.50 RAMP "B"  
SET IRON PIN W/CAP



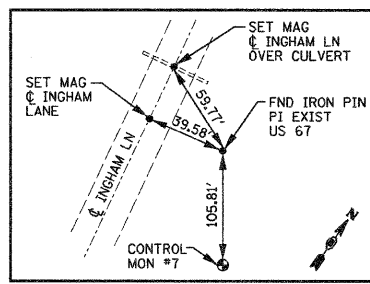
PT STA 40+76.45 RAMP "B"  
CUT "X" IN CONCRETE



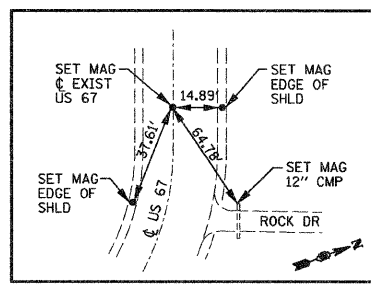
POT STA 47+03.34 RAMP "B"=  
50' RT STA 122+00.00 FAP RTE 310  
MAG SET IN BITUMINOUS SHOULDER



PC STA 61+68.11 EXIST US 67  
FOUND PK NAIL IN CENTERLINE



PI STA 67+54.39 EXIST US 67  
FOUND IRON PIN



PT STA 72+81.10 EXIST US 67  
SET MAG NAIL IN CENTERLINE

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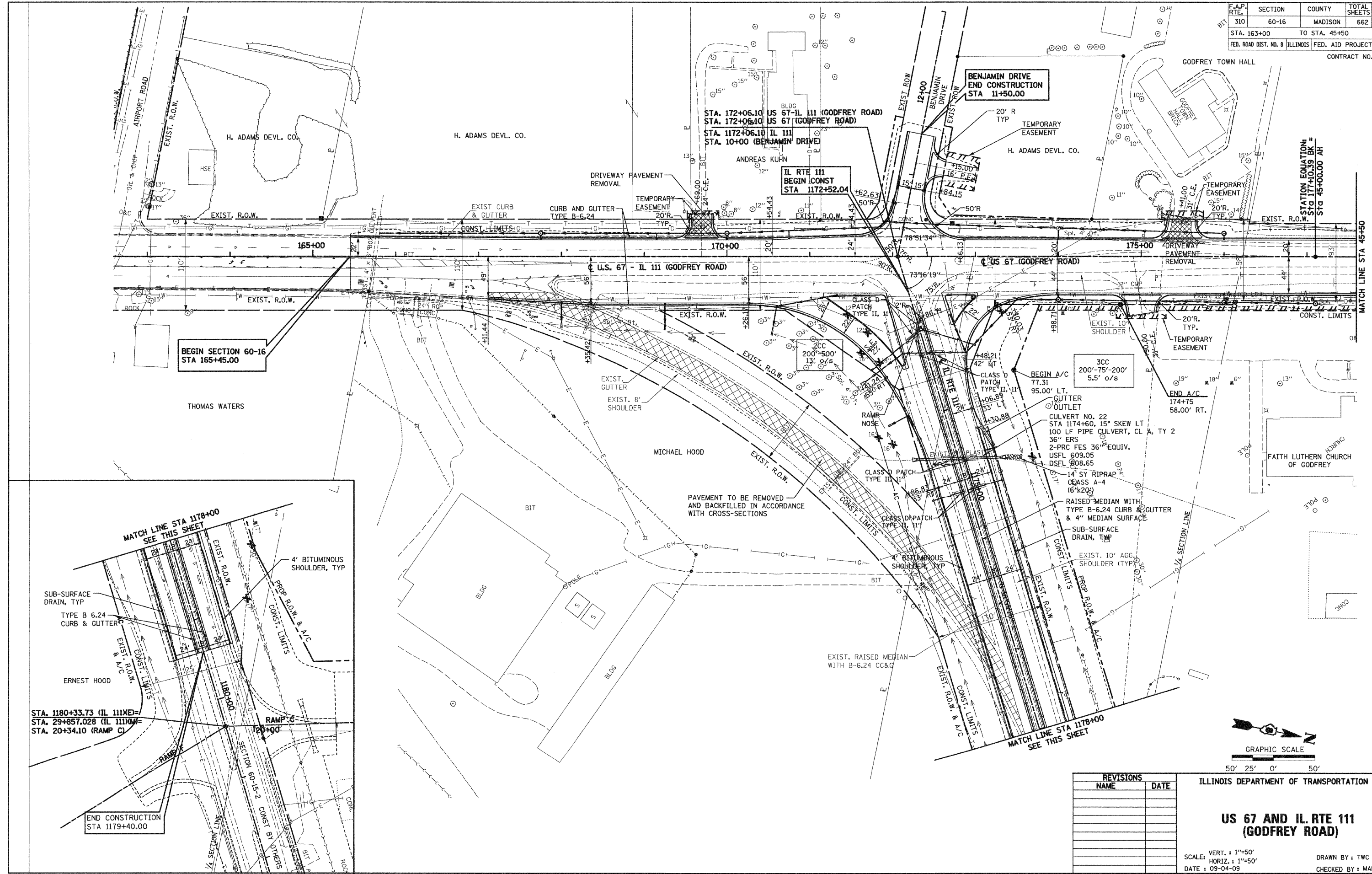
ILLINOIS DEPARTMENT OF TRANSPORTATION

## REFERENCE TIES

SCALE: NO SCALE  
DATE: 09-04-09

DRAWN BY: U.J.  
CHECKED BY: J.J.C.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	47
STA. 163+00		TO STA. 45+50		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				



BEGIN SECTION 60-16  
STA 165+45.00

BENJAMIN DRIVE  
END CONSTRUCTION  
STA 11+50.00

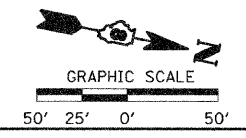
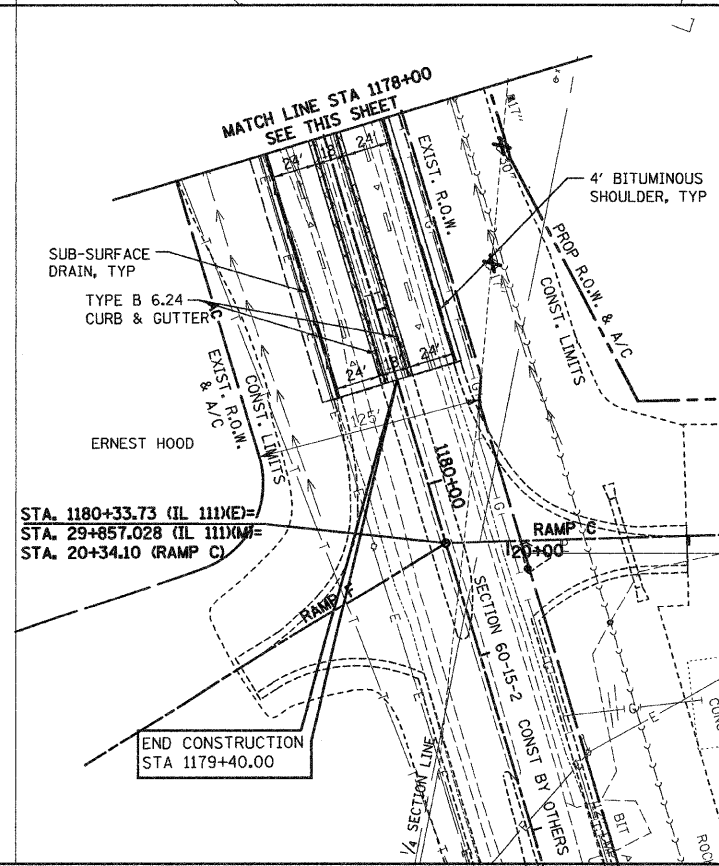
IL RTE 111  
BEGIN CONST  
STA 1172+52.04

STATION EQUATION  
Sta 177+10.39 BK =  
Sta 45+00.00 AH

MATCH LINE STA 45+50

MATCH LINE STA 1178+00  
SEE THIS SHEET

MATCH LINE STA 1178+00  
SEE THIS SHEET



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

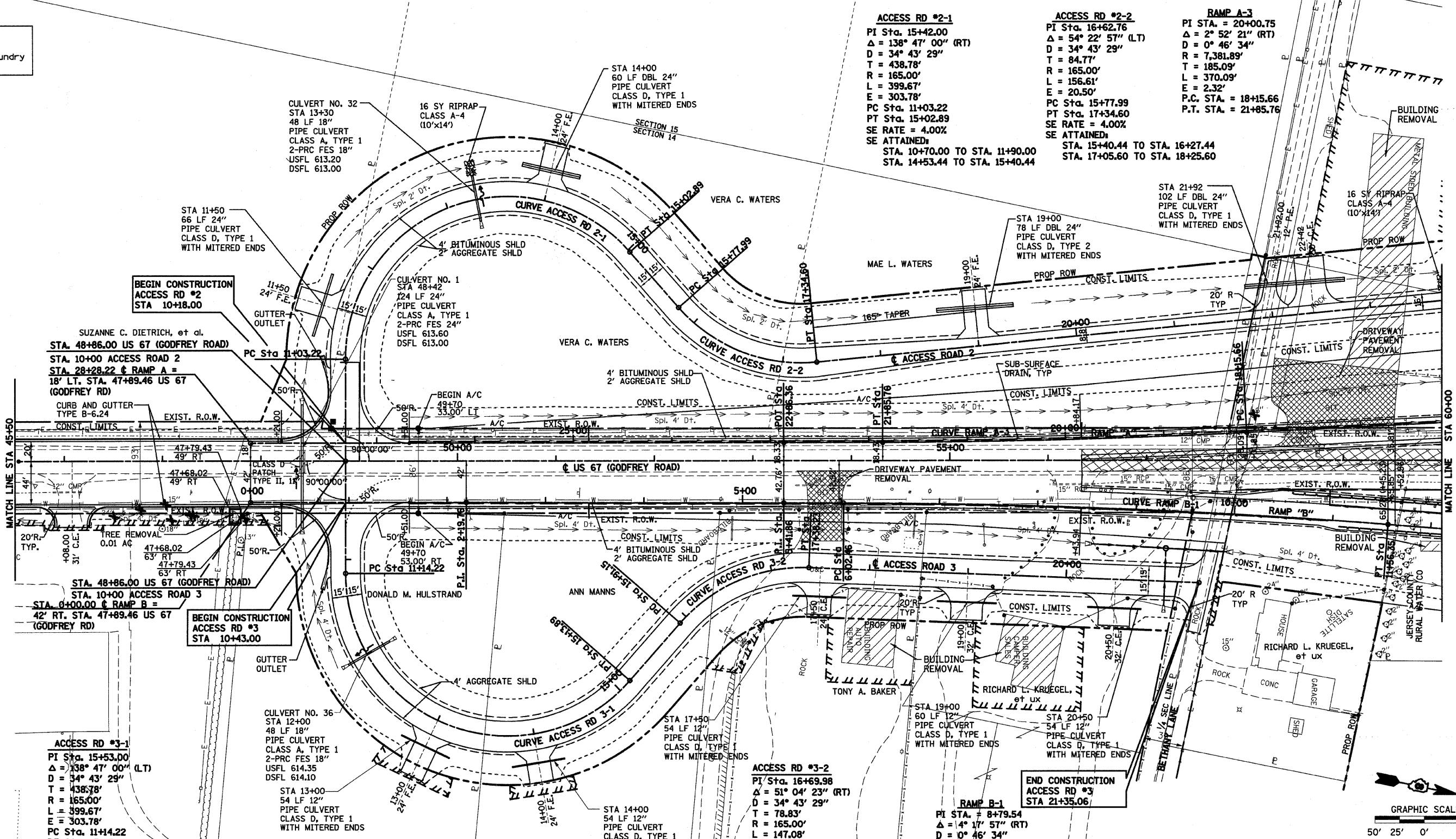
**US 67 AND IL RTE 111  
(GODFREY ROAD)**

SCALE: VERT. : 1"=50'  
HORIZ. : 1"=50'  
DATE : 09-04-09

DRAWN BY : TWC  
CHECKED BY : MAR

**LEGEND**

Tree Removal Boundary



**ACCESS RD #2-1**  
 PI Sta. 15+42.00  
 $\Delta = 138^\circ 47' 00''$  (RT)  
 $D = 34^\circ 43' 29''$   
 $T = 438.78'$   
 $R = 165.00'$   
 $L = 399.67'$   
 $E = 303.78'$   
 PC Sta. 11+03.22  
 PT Sta. 15+02.89  
 SE RATE = 4.00%  
 SE ATTAINED:  
 STA. 10+70.00 TO STA. 11+90.00  
 STA. 14+53.44 TO STA. 15+40.44

**ACCESS RD #2-2**  
 PI Sta. 16+62.76  
 $\Delta = 54^\circ 22' 57''$  (LT)  
 $D = 34^\circ 43' 29''$   
 $T = 84.77'$   
 $R = 165.00'$   
 $L = 156.61'$   
 $E = 20.50'$   
 PC Sta. 15+77.99  
 PT Sta. 17+34.60  
 SE RATE = 4.00%  
 SE ATTAINED:  
 STA. 15+40.44 TO STA. 16+27.44  
 STA. 17+05.60 TO STA. 18+25.60

**RAMP A-3**  
 PI STA. = 20+00.75  
 $\Delta = 2^\circ 52' 21''$  (RT)  
 $D = 0^\circ 46' 34''$   
 $R = 7,381.89'$   
 $L = 185.09'$   
 $E = 2.32'$   
 P.C. STA. = 18+15.66  
 P.T. STA. = 21+85.76

SUZANNE C. DIETRICH, et al.  
 STA. 48+86.00 US 67 (GODFREY ROAD)  
 STA. 10+00 ACCESS ROAD 2  
 STA. 28+28.22 RAMP A =  
 18' LT. STA. 47+89.46 US 67  
 (GODFREY RD)

STA. 48+86.00 US 67 (GODFREY ROAD)  
 STA. 10+00 ACCESS ROAD 3  
 STA. 0+00.00 RAMP B =  
 42' RT. STA. 47+89.46 US 67  
 (GODFREY RD)

**ACCESS RD #3-1**  
 PI Sta. 15+53.00  
 $\Delta = 138^\circ 47' 00''$  (LT)  
 $D = 34^\circ 43' 29''$   
 $T = 438.78'$   
 $R = 165.00'$   
 $L = 399.67'$   
 $E = 303.78'$   
 PC Sta. 11+14.22  
 PT Sta. 15+13.89  
 SE RATE = 4.00%  
 SE ATTAINED:  
 STA. 10+94.00 TO STA. 12+14.00  
 STA. 14+65.52 TO STA. 15+52.52

STA 13+00  
 54 LF 12"  
 PIPE CULVERT  
 CLASS D, TYPE 1  
 WITH MITERED ENDS

STA 14+00  
 54 LF 12"  
 PIPE CULVERT  
 CLASS D, TYPE 1  
 WITH MITERED ENDS

**ACCESS RD #3-2**  
 PI Sta. 16+69.98  
 $\Delta = 51^\circ 04' 23''$  (RT)  
 $D = 34^\circ 43' 29''$   
 $T = 78.83'$   
 $R = 165.00'$   
 $L = 147.08'$   
 $E = 17.86'$   
 PC Sta. 15+91.15  
 PT Sta. 17+38.23  
 SE RATE = 4.00%  
 SE ATTAINED:  
 STA. 15+52.52 TO STA. 16+39.52  
 STA. 17+09.23 TO STA. 18+29.23

**RAMP B-1**  
 PI STA. = 8+79.54  
 $\Delta = 14^\circ 17' 57''$  (RT)  
 $D = 0^\circ 46' 34''$   
 $R = 7,381.89'$   
 $L = 277.08'$   
 $E = 5.20'$   
 S.E. = N.C.  
 P.C. STA. = 6+02.46  
 P.T. STA. = 11+56.35

**END CONSTRUCTION  
 ACCESS RD #3  
 STA 21+35.06**

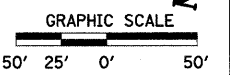
REVISIONS	
NAME	DATE

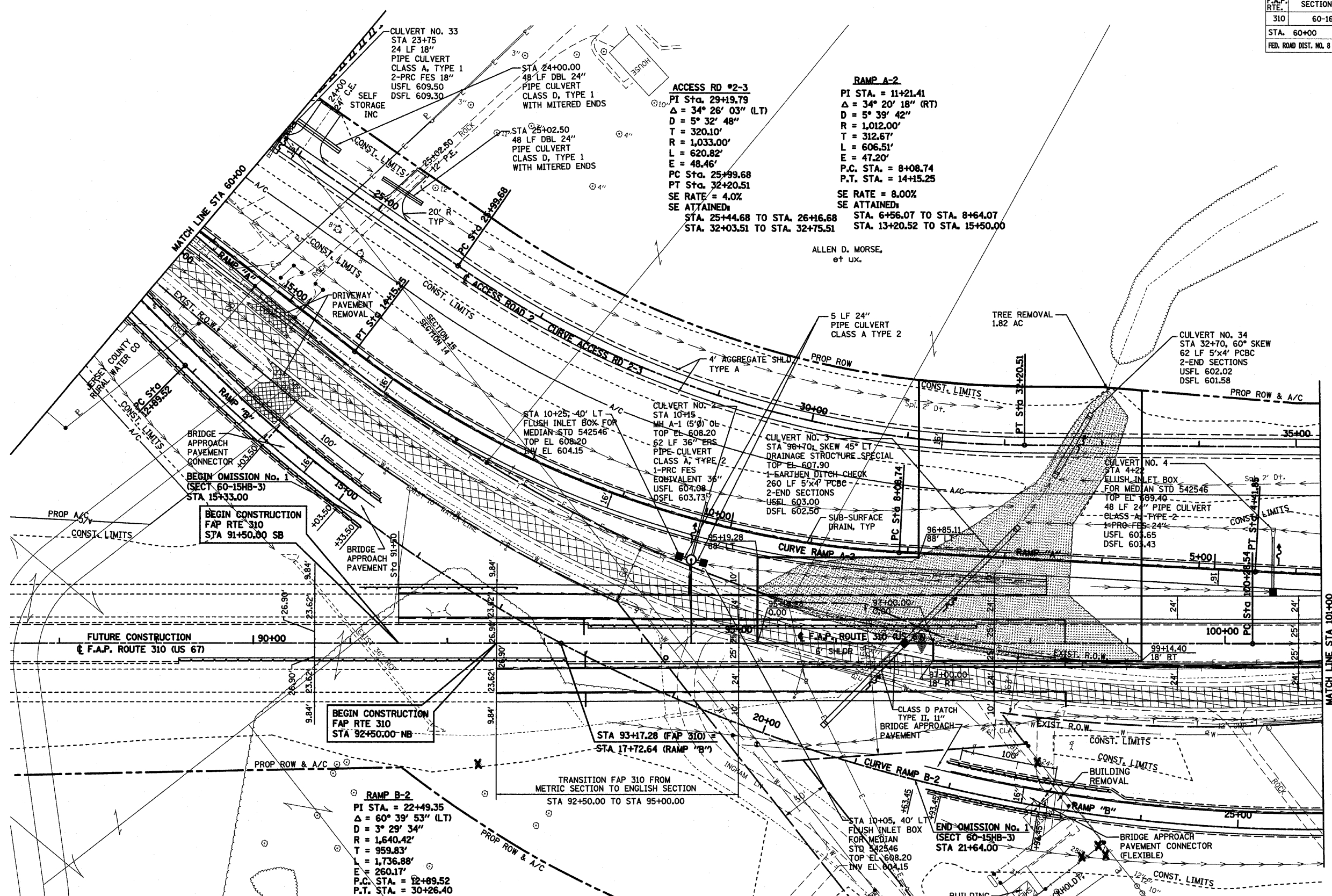
ILLINOIS DEPARTMENT OF TRANSPORTATION

**US 67 (GODFREY ROAD),  
 ACCESS RD 2  
 AND ACCESS RD 3**

SCALE: VERT. : 1"=50'  
 HORIZ. : 1"=50'  
 DATE : 09-04-09

DRAWN BY : TWC  
 CHECKED BY : MAR





**ACCESS RD #2-3**  
 PI STA. 29+19.79  
 $\Delta = 34^\circ 26' 03''$  (LT)  
 $D = 5^\circ 32' 48''$   
 $T = 320.10'$   
 $R = 1,033.00'$   
 $L = 620.82'$   
 $E = 48.46'$   
 SE RATE = 4.0%  
 SE ATTAINED:  
 STA. 25+44.68 TO STA. 26+16.68  
 STA. 32+03.51 TO STA. 32+75.51

**RAMP A-2**  
 PI STA. = 11+21.41  
 $\Delta = 34^\circ 20' 18''$  (RT)  
 $D = 5^\circ 39' 42''$   
 $R = 1,012.00'$   
 $T = 312.67'$   
 $L = 606.51'$   
 $E = 47.20'$   
 P.C. STA. = 8+08.74  
 P.T. STA. = 14+15.25  
 SE RATE = 8.00%  
 SE ATTAINED:  
 STA. 6+56.07 TO STA. 8+64.07  
 STA. 13+20.52 TO STA. 15+50.00

**RAMP B-2**  
 PI STA. = 22+49.35  
 $\Delta = 60^\circ 39' 53''$  (LT)  
 $D = 3^\circ 29' 34''$   
 $R = 1,640.42'$   
 $T = 959.83'$   
 $L = 1,736.88'$   
 $E = 260.17'$   
 P.C. STA. = 12+89.52  
 P.T. STA. = 30+26.40  
 SE RATE = 6.00%  
 SE ATTAINED:  
 STA. 12+34.98 TO STA. 13+68.98  
 STA. 29+18.89 TO STA. 31+04.89

BEGIN CONSTRUCTION  
 FAP RTE 310  
 STA 91+50.00 SB

BEGIN CONSTRUCTION  
 FAP RTE 310  
 STA 92+50.00 NB

STA 93+17.28 (FAP 310)  
 STA 17+72.64 (RAMP "B")

USGS "43A 1922" STA. 95+19.04,  
 104.02' RT. IRON POST WITH  
 BRONZE CAP EL. 608.26

REVISIONS	
NAME	DATE

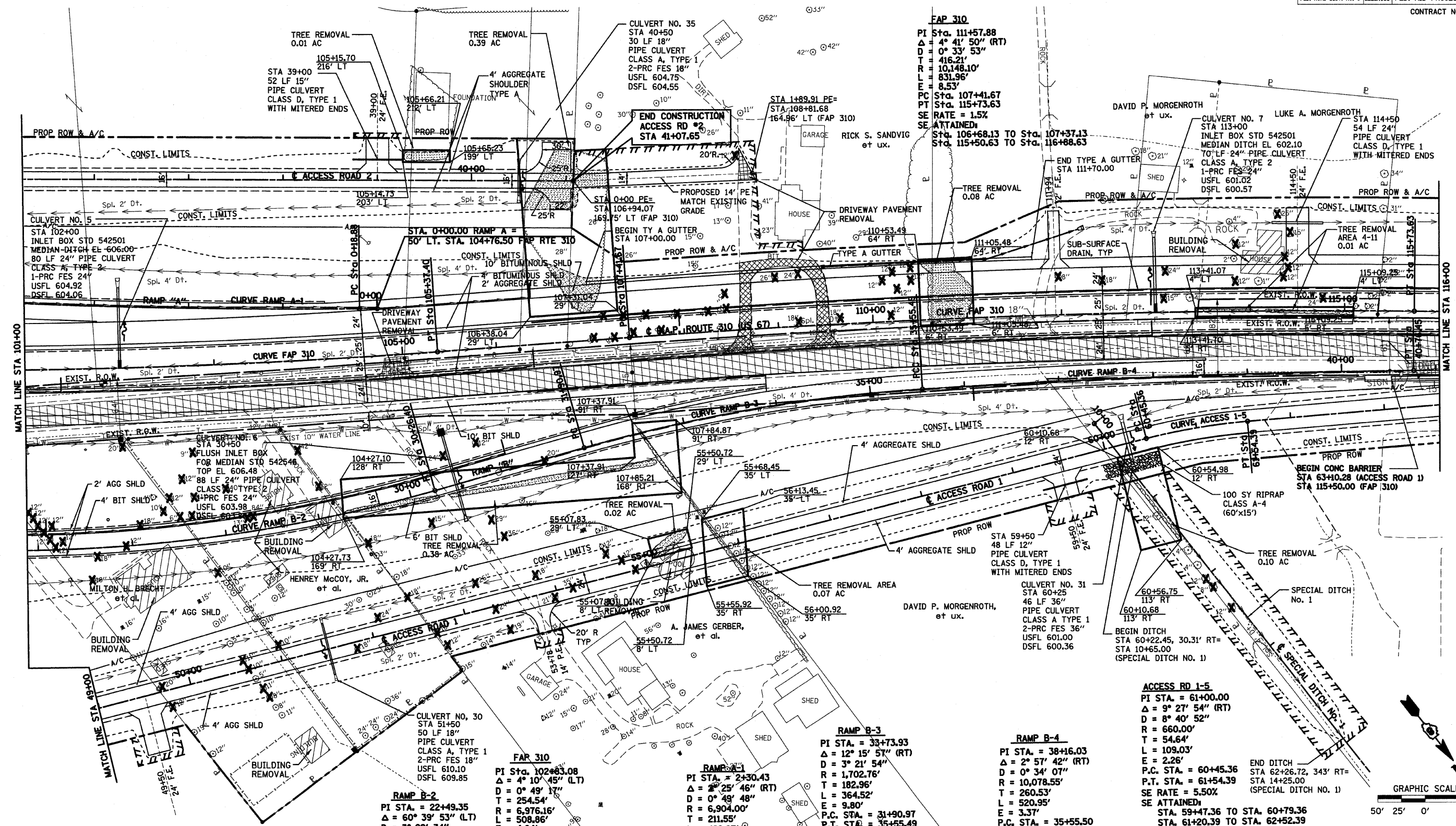
ILLINOIS DEPARTMENT OF TRANSPORTATION

**FAP RTE 310 (US 67)  
 AND ACCESS RD 2**

SCALE: VERT. : 1"=50'  
 HORIZ. : 1"=50'  
 DATE : 09-04-09

DRAWN BY : TWC  
 CHECKED BY : MAR

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	50
STA. 101+00		TO STA. 116+00		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				



**RAMP B-2**  
 PI STA. = 22+49.35  
 $\Delta$  = 60° 39' 53" (LT)  
 D = 3° 29' 34"  
 R = 1,640.42'  
 T = 959.83'  
 L = 1,736.88'  
 E = 260.17'  
 P.C. STA. = 12+89.52  
 P.T. STA. = 30+26.40  
 SE RATE = 6.00%  
 SE ATTAINED:  
 STA. 11+34.98 TO STA. 13+68.98  
 STA. 29+18.89 TO STA. 31+04.89

**FAP 310**  
 PI STA. 102+83.08  
 $\Delta$  = 4° 10' 45" (LT)  
 D = 0° 49' 17"  
 T = 254.54'  
 R = 6,976.16'  
 L = 508.86'  
 E = 4.64'  
 PC STA. 100+28.54  
 PT STA. 105+37.40  
 SE RATE = 2.7%  
 SE ATTAINED:  
 Sta. 98+98.54 To Sta. 100+69.54  
 Sta. 105+45.13 To Sta. 106+68.13

**RAMP B-1**  
 PI STA. = 2+30.43  
 $\Delta$  = 2° 25' 46" (RT)  
 D = 0° 49' 48"  
 R = 6,904.00'  
 T = 211.55'  
 L = 422.97'  
 E = 3.24'  
 P.C. STA. = 0+18.88  
 P.T. STA. = 4+41.85  
 SE RATE = 2.7%  
 SE TRANSITION  
 MATCH FAP 310

**RAMP B-3**  
 PI STA. = 33+73.93  
 $\Delta$  = 12° 15' 57" (RT)  
 D = 3° 21' 54"  
 R = 1,702.76'  
 T = 182.96'  
 L = 364.52'  
 E = 9.80'  
 P.C. STA. = 31+90.97  
 P.T. STA. = 35+55.49  
 SE RATE = 6.5%  
 SE ATTAINED:  
 Sta. 31+04.89 To Sta. 33+08.89  
 Sta. 34+25.49 To Sta. 37+55.18

**RAMP B-4**  
 PI STA. = 38+16.03  
 $\Delta$  = 2° 57' 42" (RT)  
 D = 0° 34' 07"  
 R = 10,078.55'  
 T = 260.53'  
 L = 520.95'  
 E = 3.37'  
 P.C. STA. = 35+55.50  
 P.T. STA. = 40+76.45  
 SE RATE = 3.2%  
 SE TRANSITION  
 MATCH FAP 310

**ACCESS RD 1-5**  
 PI STA. = 61+00.00  
 $\Delta$  = 9° 27' 54" (RT)  
 D = 8° 40' 52"  
 R = 660.00'  
 T = 54.64'  
 L = 109.03'  
 E = 2.26'  
 P.C. STA. = 60+45.36  
 P.T. STA. = 61+54.39  
 SE RATE = 5.50%  
 SE ATTAINED:  
 STA. 59+47.36 TO STA. 60+79.36  
 STA. 61+20.39 TO STA. 62+52.39

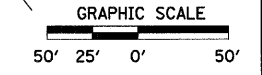
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

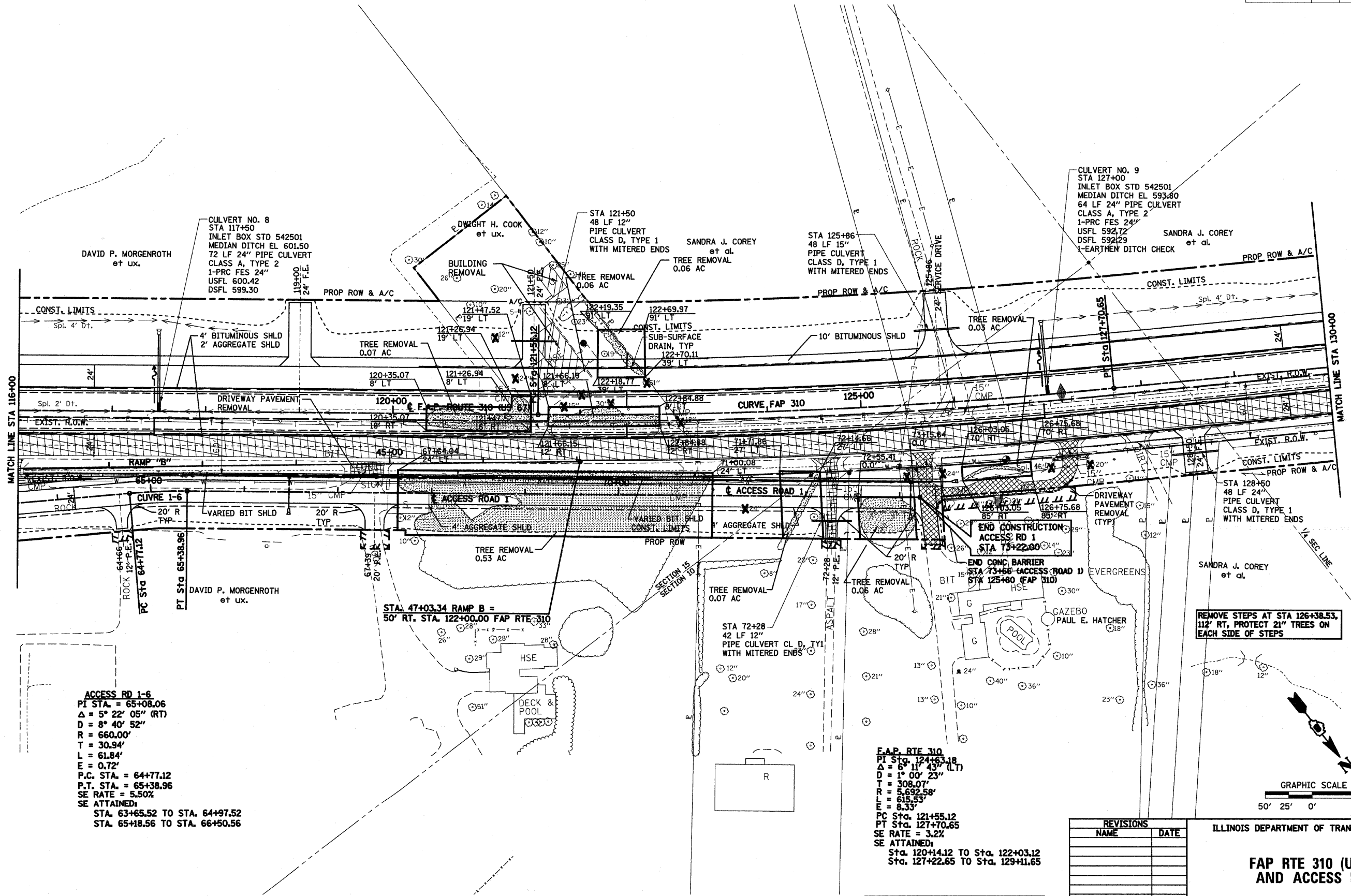
**FAP RTE 310 (US 67),  
 ACCESS RD 1 AND  
 ACCESS RD 2**

SCALE: VERT.: 1"=50'  
 HORIZ.: 1"=50'  
 DATE: 09-04-09

DRAWN BY: TWC  
 CHECKED BY: MAR



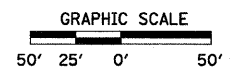
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	51
STA. 116+00		TO STA. 130+00		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				



**ACCESS RD 1-6**  
 PI STA. = 65+08.06  
 $\Delta = 5^{\circ} 22' 05''$  (RT)  
 $D = 8^{\circ} 40' 52''$   
 $R = 660.00'$   
 $T = 30.94'$   
 $L = 61.84'$   
 $E = 0.72'$   
 P.C. STA. = 64+77.12  
 P.T. STA. = 65+38.96  
 SE RATE = 5.50%  
 SE ATTAINED:  
 STA. 63+65.52 TO STA. 64+97.52  
 STA. 65+18.56 TO STA. 66+50.56

**F.A.P. RTE 310**  
 PI Sta. 124+63.18  
 $\Delta = 6^{\circ} 11' 43''$  (LT)  
 $D = 1^{\circ} 00' 23''$   
 $T = 308.07'$   
 $R = 5,692.58'$   
 $E = 615.53'$   
 $L = 8.33'$   
 PC Sta. 121+55.12  
 PT Sta. 127+70.65  
 SE RATE = 3.2%  
 SE ATTAINED:  
 Sta. 120+14.12 TO Sta. 122+03.12  
 Sta. 127+22.65 TO Sta. 129+11.65

REMOVE STEPS AT STA 126+38.53,  
 112' RT, PROTECT 21" TREES ON  
 EACH SIDE OF STEPS



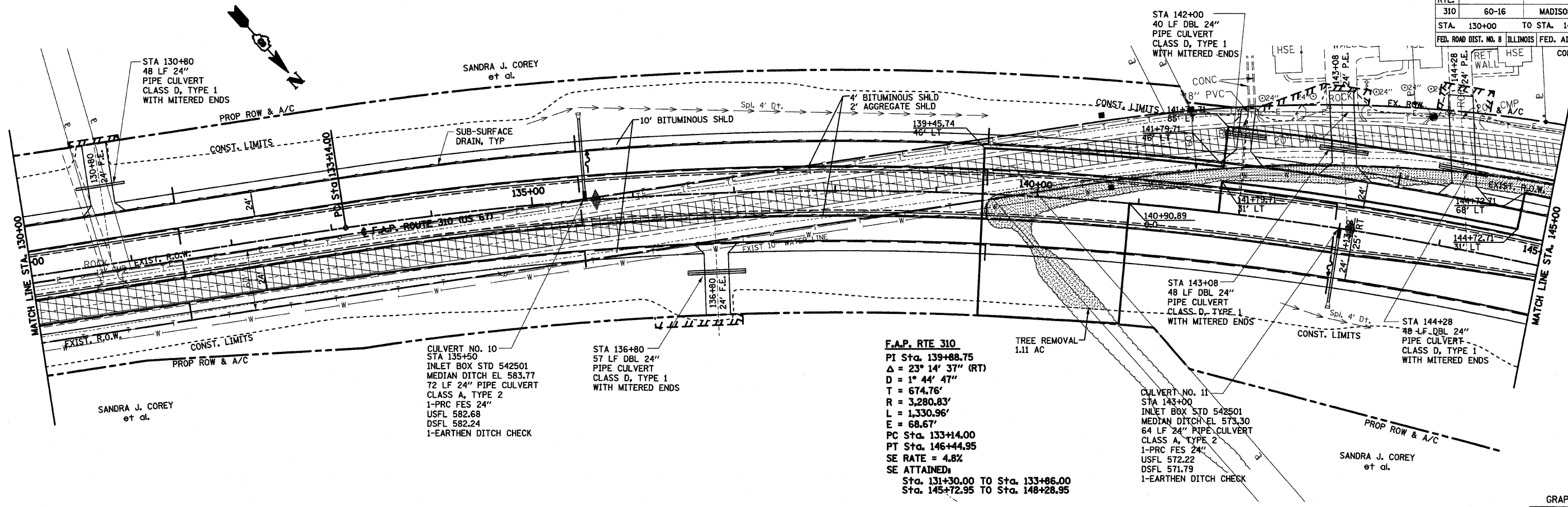
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**FAP RTE 310 (US 67)  
 AND ACCESS RD 1**  
 SCALE: VERT. : 1"=50'  
 HORIZ. : 1"=50'  
 DATE : 09-04-09  
 DRAWN BY : TWC  
 CHECKED BY : MAR

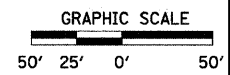
BM #28 STA. 125+37.23, 64.93' RT.  
 RR SPIKE IN POWER POLE, IN FRONT  
 OF HOUSE #7609 EL. 599.90

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	52
STA. 130+00		TO STA. 145+00		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				

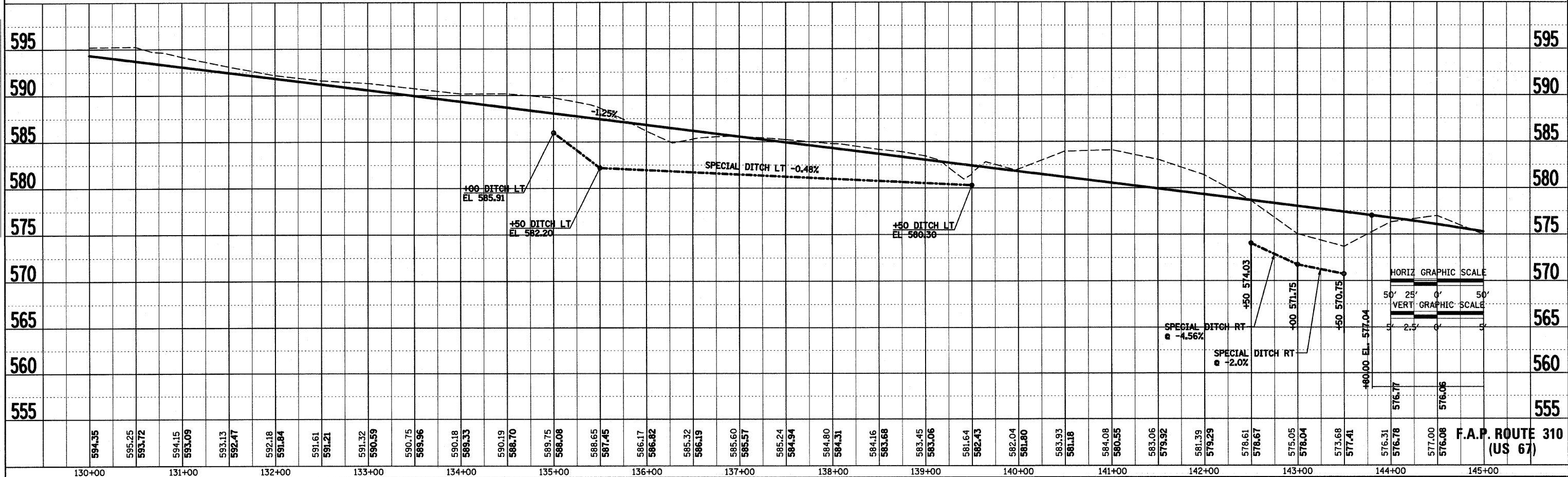
DATE	BY	DATE	BY
5/29	SJC	5/29	SJC
7/28	SJC	7/28	SJC
PLAN		PROFILE	
SURVEYED		SURVEYED	
PLOTTED		PLOTTED	
CHECKED		CHECKED	
NO. _____		NO. _____	
NOTE BOOK		NOTE BOOK	
NO. _____		NO. _____	
DRAWING FILE NAME		DRAWING FILE NAME	
NO. _____		NO. _____	



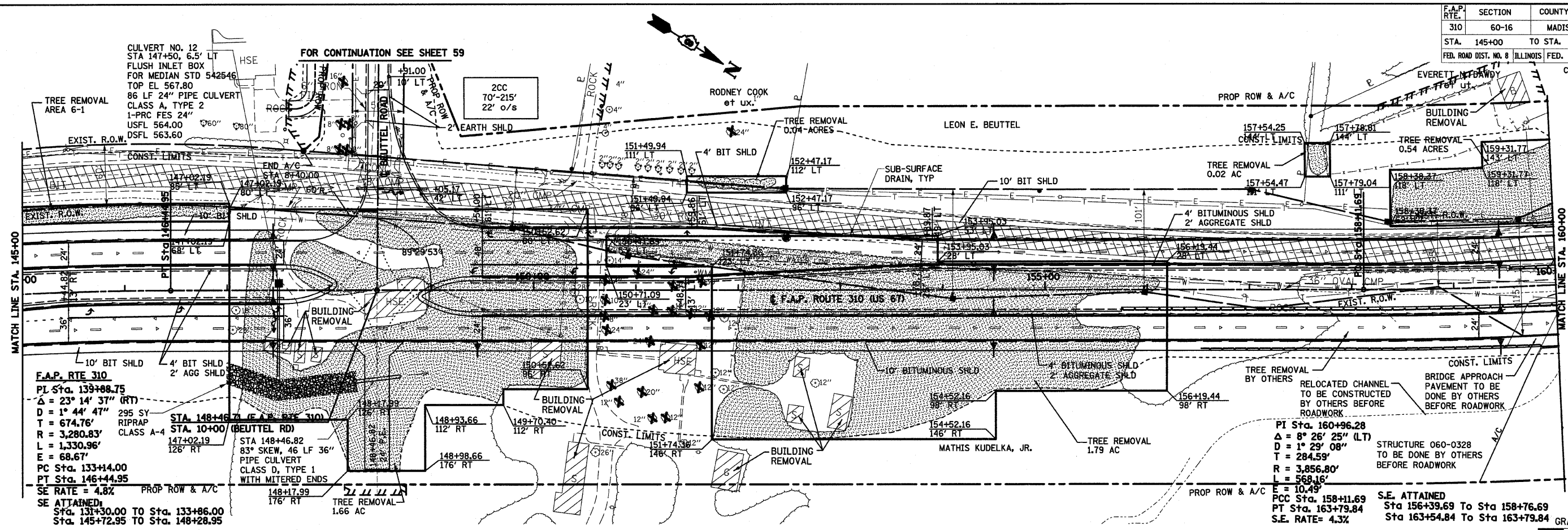
**F.A.P. RTE 310**  
 PI Sta. 139+88.75  
 $\Delta = 23^\circ 14' 37''$  (RT)  
 $D = 1^\circ 44' 47''$   
 $T = 674.76'$   
 $R = 3,280.83'$   
 $L = 1,330.96'$   
 $E = 68.67'$   
 PC Sta. 133+14.00  
 PT Sta. 146+44.95  
 SE RATE = 4.8%  
 SE ATTAINED:  
 Sta. 131+30.00 TO Sta. 133+86.00  
 Sta. 145+72.95 TO Sta. 148+28.95



DATE	BY	DATE	BY
PROFILE		PROFILE	
SURVEYED		SURVEYED	
PLOTTED		PLOTTED	
CHECKED		CHECKED	
NO. _____		NO. _____	
NOTE BOOK		NOTE BOOK	
NO. _____		NO. _____	
STRUCTURE NOTATIONS OK'D		STRUCTURE NOTATIONS OK'D	
NO. _____		NO. _____	

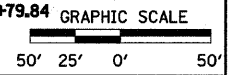


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	53
STA. 145+00		TO STA. 160+00		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				



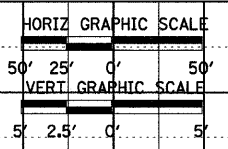
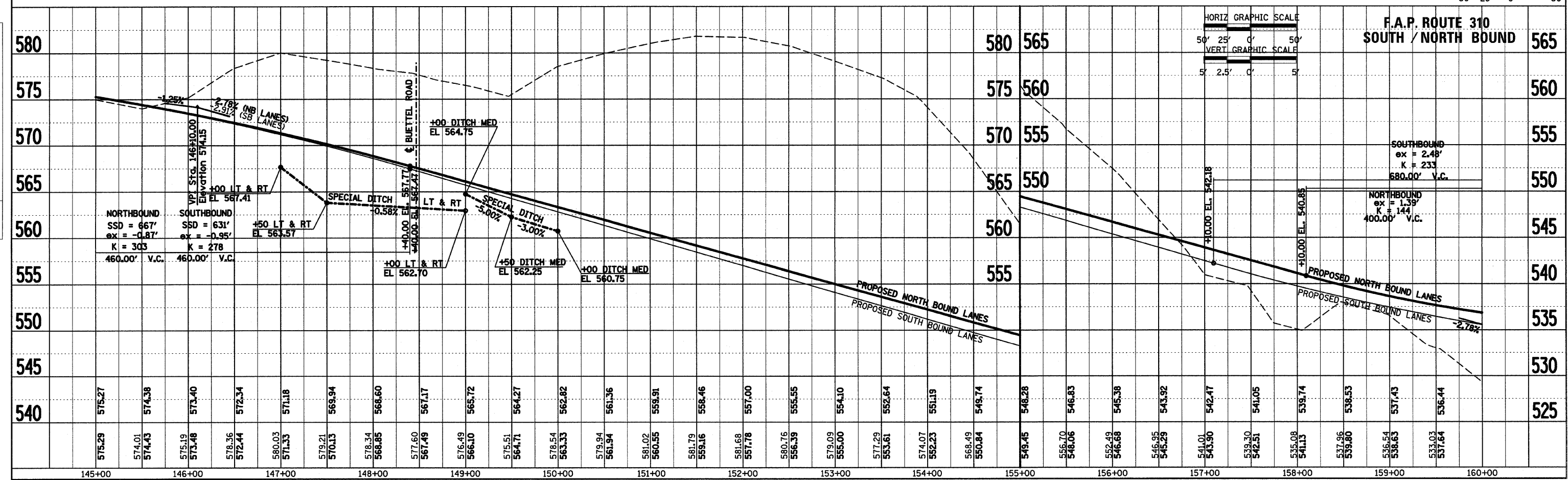
**F.A.P. RTE 310**  
 PL Sta. 139+88.75  
 $\Delta = 23^\circ 14' 37''$  (RT)  
 $D = 1^\circ 44' 47''$  295 SY  
 $T = 674.76'$  RIPRAP CLASS A-4  
 $R = 3,280.83'$   
 $L = 1,330.96'$   
 $E = 68.67'$   
 PC Sta. 133+44.00  
 PT Sta. 146+44.95  
 SE RATE = 4.8%  
 SE ATTAINED  
 Sta. 131+30.00 To Sta. 133+86.00  
 Sta. 145+72.95 To Sta. 148+28.95

PI Sta. 160+96.28  
 $\Delta = 8^\circ 26' 25''$  (LT)  
 $D = 1^\circ 29' 08''$   
 $T = 284.59'$   
 $R = 3,856.80'$   
 $L = 568.16'$   
 $E = 10.49'$   
 PCC Sta. 158+11.69  
 PT Sta. 163+79.84  
 S.E. RATE = 4.3%  
 S.E. ATTAINED  
 Sta 156+39.69 To Sta 158+76.69  
 Sta 163+54.84 To Sta 163+79.84



DATE	5/28/13
BY	JK
CHECKED	JK
DATE	7/25/13
BY	JK
CHECKED	JK
DATE	
BY	
CHECKED	
DATE	
BY	
CHECKED	

DATE	
BY	
CHECKED	
DATE	
BY	
CHECKED	
DATE	
BY	
CHECKED	



**F.A.P. ROUTE 310**  
**SOUTH / NORTH BOUND**

**SOUTHBOUND**  
 $ex = 2.48'$   
 $K = 233$   
 400.00' V.C.

**NORTHBOUND**  
 $ex = 1.39'$   
 $K = 144$   
 400.00' V.C.

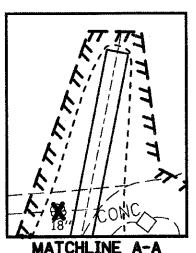




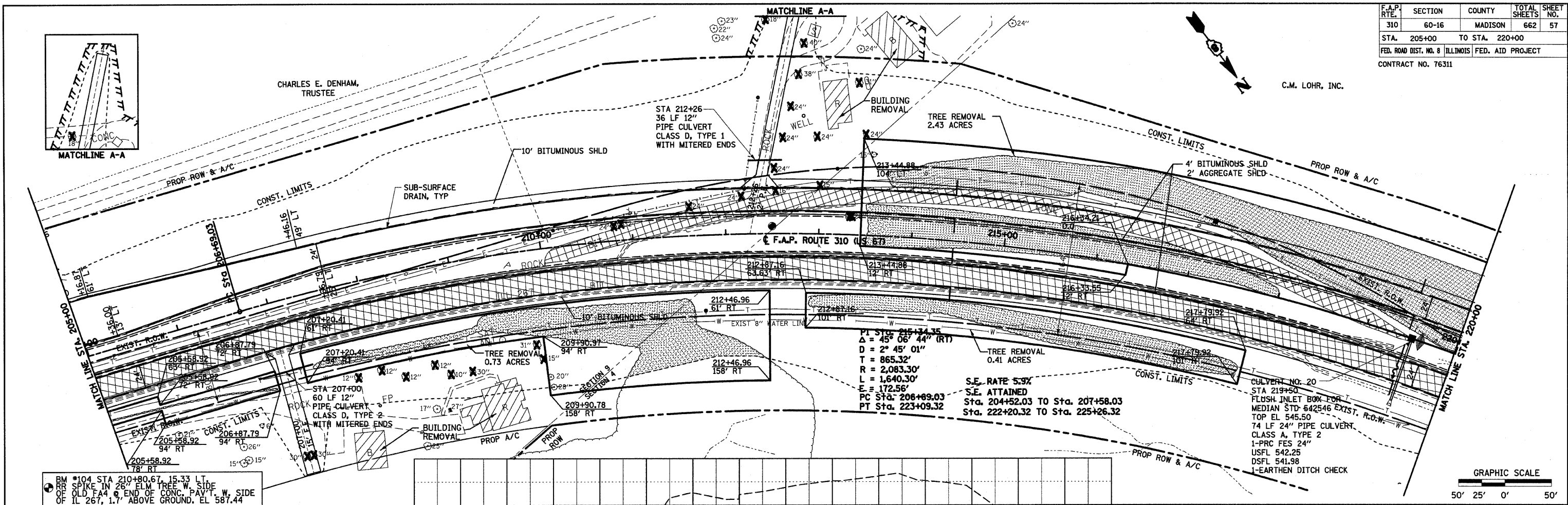




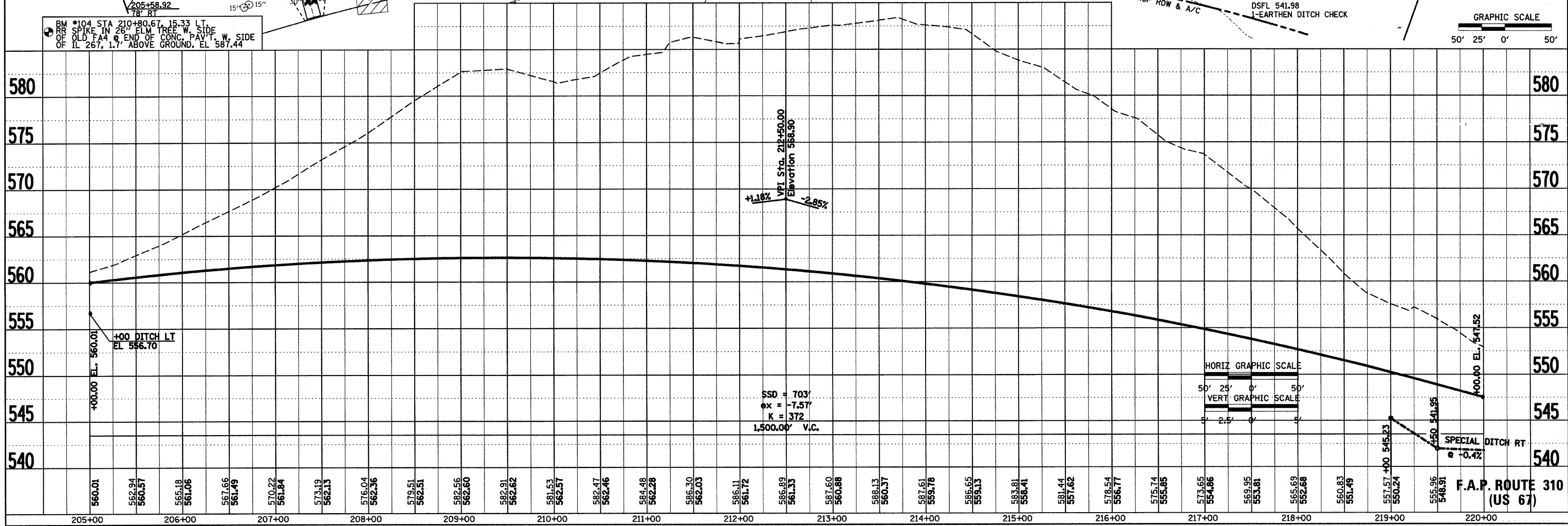
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	57
STA. 205+00		TO STA. 220+00		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				



DATE: 6/28/09  
 BY: J.M. [Signature]  
 SURVEYED: [Signature]  
 PLOTTED: [Signature]  
 NOTE BOOK: [Signature]  
 RT. OF WAY CHECKED: [Signature]  
 CADD FILE NAME: [Signature]



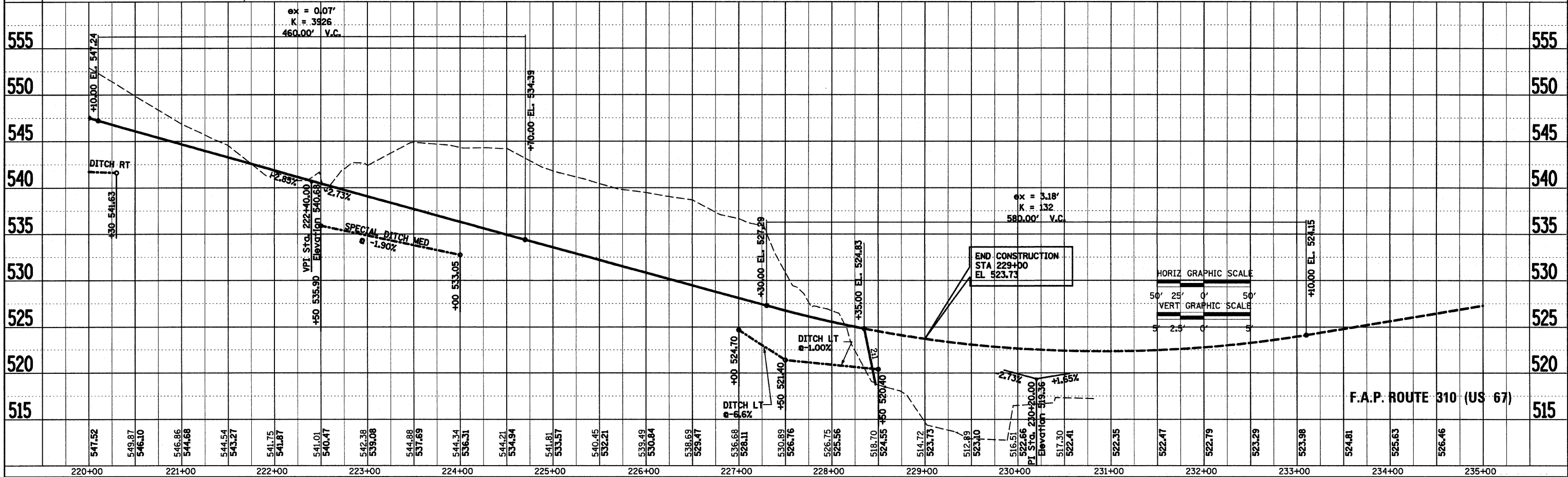
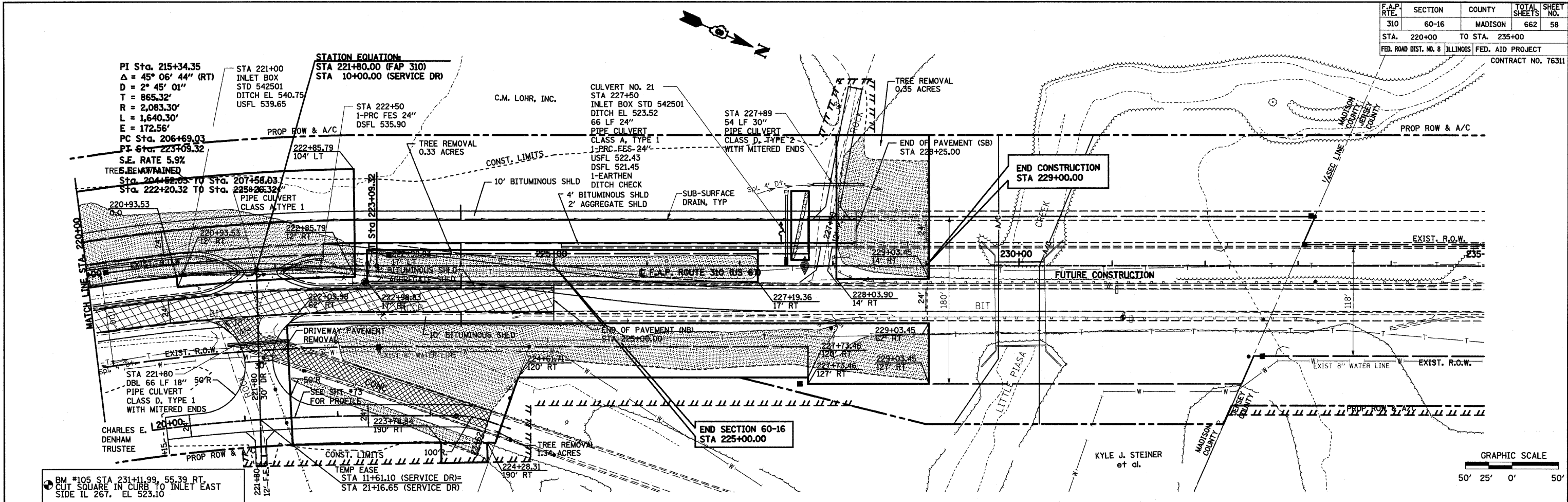
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 BY: [Blank]  
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 NOTE BOOK: [Blank]  
 RT. OF WAY CHECKED: [Blank]  
 STRUCTURE NOTATIONS CHECKED: [Blank]



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	58
STA. 220+00 TO STA. 235+00		FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				

DATE	BY	DATE	BY
8/28/98	W.M.	8/28/98	W.M.
7/98	W.M.	7/98	W.M.
NO. _____	NO. _____	NO. _____	NO. _____

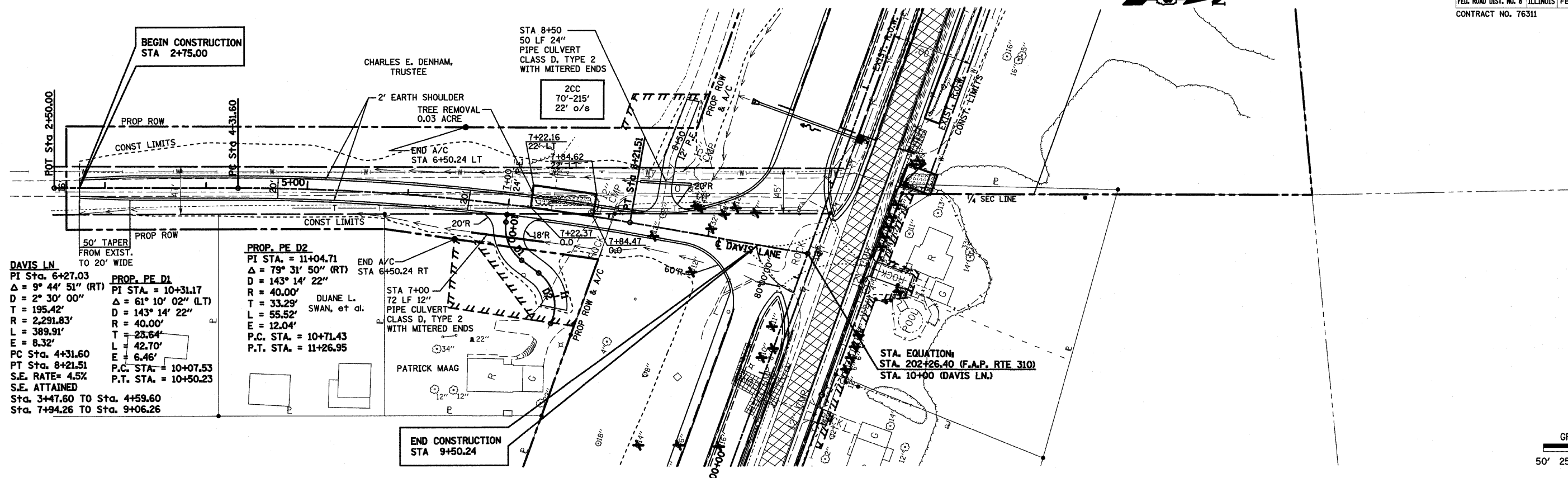
DATE	BY
NO. _____	NO. _____







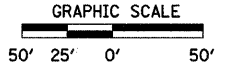
DATE	BY
12/18/09	...
REVISION	...
NO.	...



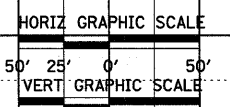
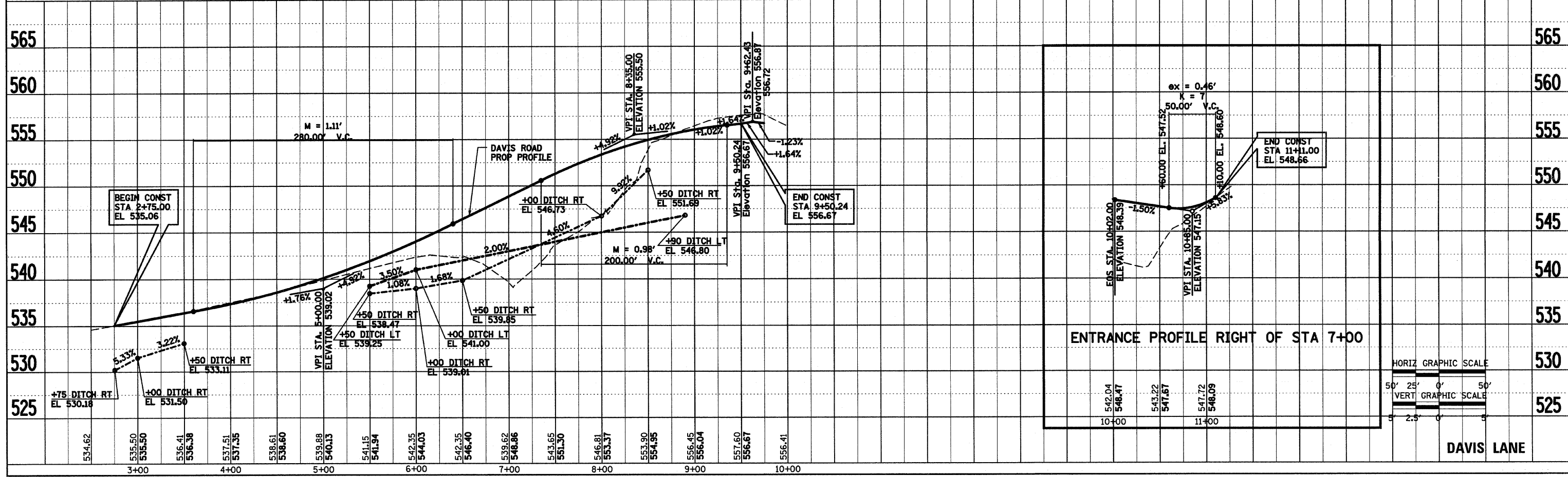
**DAVIS LN**  
 PI Sta. 6+27.03  
 $\Delta = 9^\circ 44' 51''$  (RT)  
 $D = 2^\circ 30' 00''$   
 $T = 195.42'$   
 $L = 2,291.83'$   
 $E = 8.32'$   
 PC Sta. 4+31.60  
 PT Sta. 8+21.51  
 S.E. RATE = 4.5%  
 S.E. ATTAINED  
 Sta. 3+47.60 TO Sta. 4+59.60  
 Sta. 7+94.26 TO Sta. 9+06.26

**PROP. PE D1**  
 PI STA. = 10+31.17  
 $\Delta = 61^\circ 10' 02''$  (LT)  
 $D = 143^\circ 14' 22''$   
 $R = 40.00'$   
 $T = 23.64'$   
 $L = 42.70'$   
 $E = 6.46'$   
 P.C. STA. = 10+07.53  
 P.T. STA. = 10+50.23

**PROP. PE D2**  
 PI STA. = 11+04.71  
 $\Delta = 79^\circ 31' 50''$  (RT)  
 $D = 143^\circ 14' 22''$   
 $R = 40.00'$   
 $T = 33.29'$   
 $L = 55.52'$   
 $E = 12.04'$   
 P.C. STA. = 10+71.43  
 P.T. STA. = 11+26.95



DATE	BY
12/18/09	...
REVISION	...
NO.	...





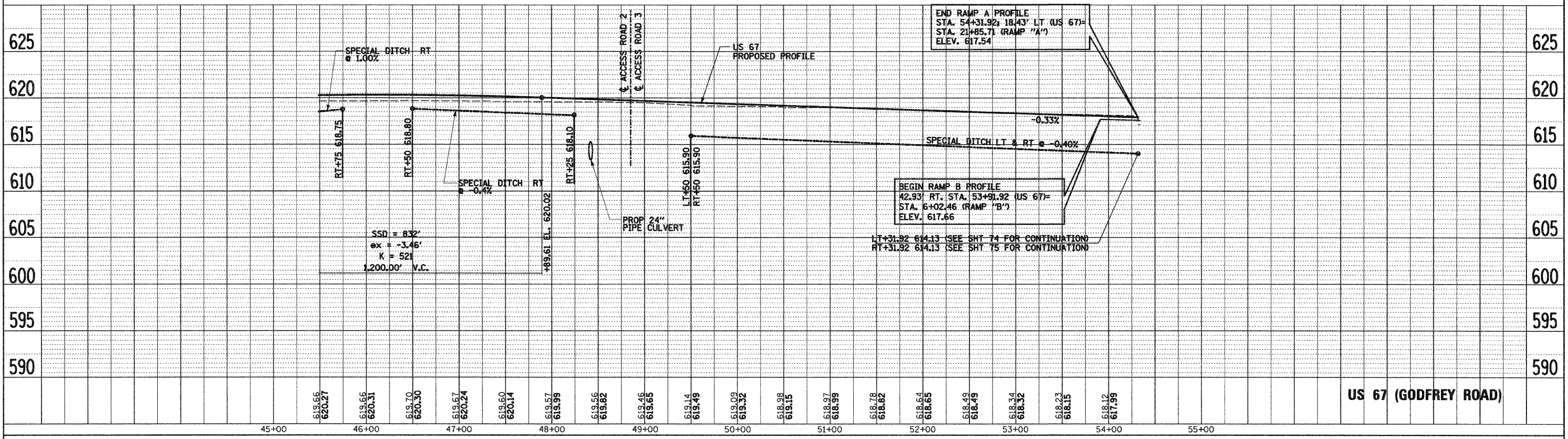






F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	65
STA. 163+00 TO STA. 54+31.92		FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT		

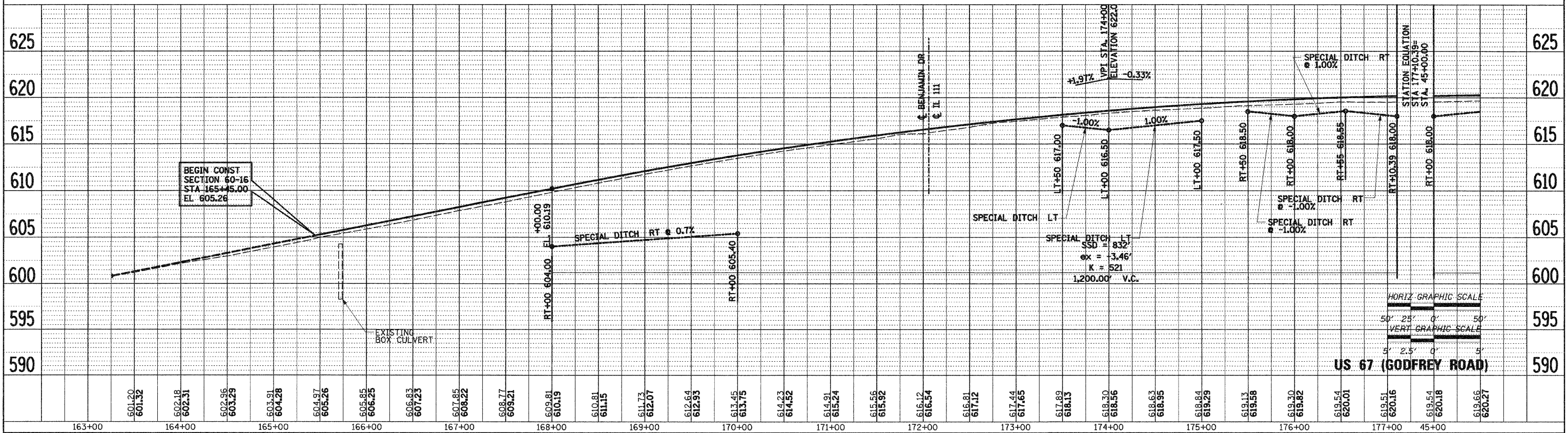
PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	REVISIONS		
	NO.		
	NOTE BOOK		
	CADD FILE NAME		
	NO.		



US 67 (GODFREY ROAD)

STA 45+50 TO STA 54+31.92

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	REVISIONS		
	NO.		
	NOTE BOOK		
	STRUCTURE NOTATIONS		
	NO.		



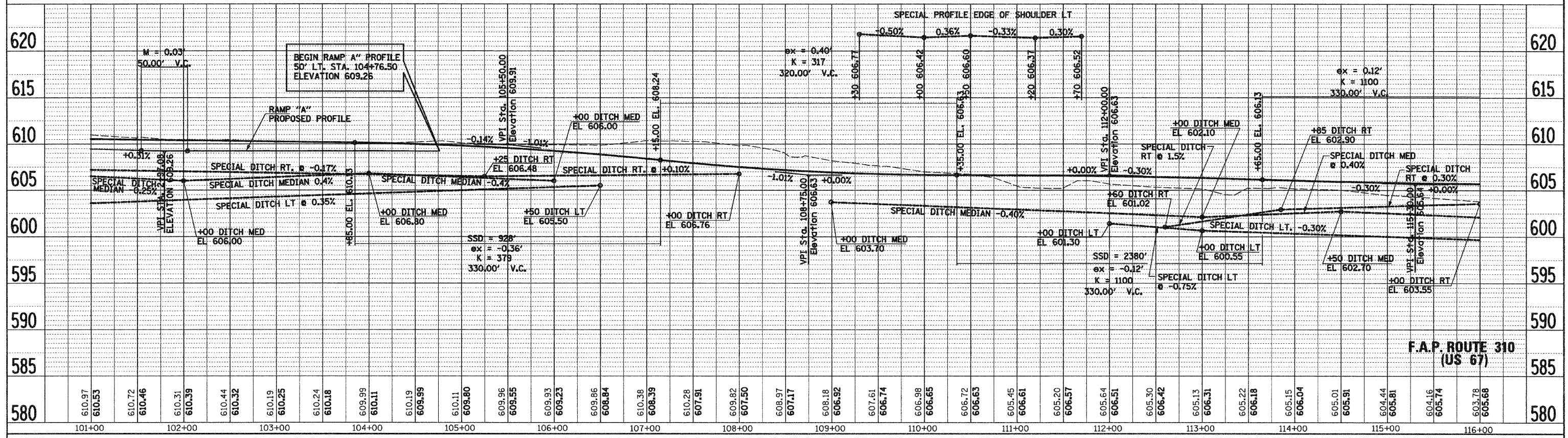
US 67 (GODFREY ROAD)

STA 163+00 TO STA 45+50

PLOT DATE = 04/26/04	FILE NAME = 07FILE6
PLOT SCALE = 1/4"=10'	SCALE = 1/4"=10'
USER NAME = 41589R	

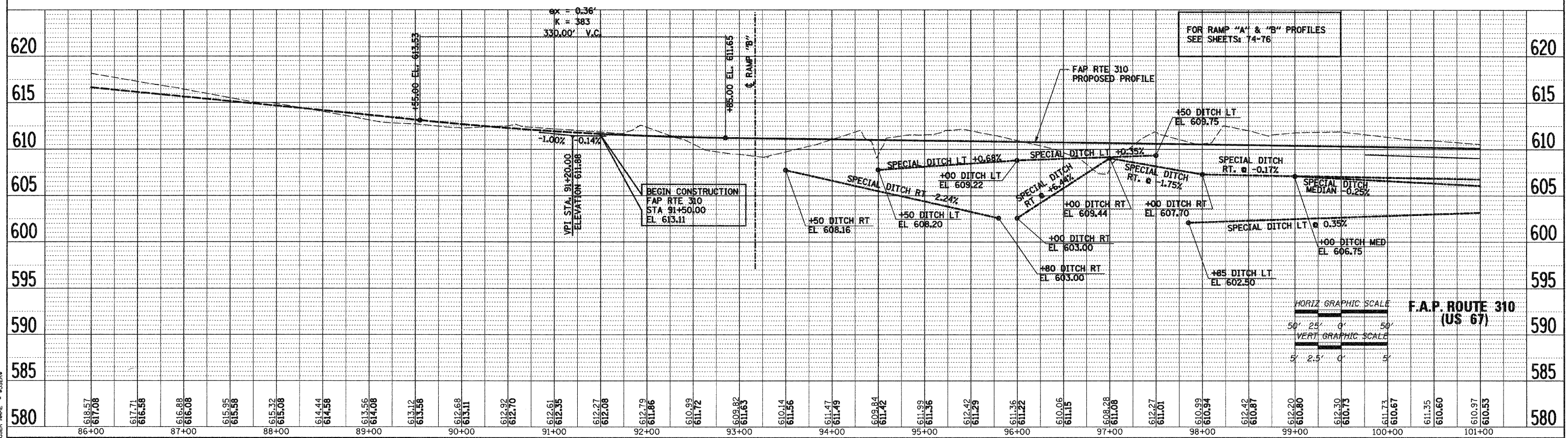
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	66
STA. 84+00		TO STA. 116+00		
FED. ROAD DIST. NO. 9 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				

DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
 SURVEYED: \_\_\_\_\_ PLOTTED: \_\_\_\_\_  
 CHECKED: \_\_\_\_\_  
 PLAN: \_\_\_\_\_  
 NOTE BOOK NO.: \_\_\_\_\_  
 CADD FILE NAME: \_\_\_\_\_



STA 101+00 TO STA 116+00

DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
 SURVEYED: \_\_\_\_\_ PLOTTED: \_\_\_\_\_  
 CHECKED: \_\_\_\_\_  
 PROFILE: \_\_\_\_\_  
 NOTE BOOK NO.: \_\_\_\_\_  
 STRUCTURE NOTATIONS: \_\_\_\_\_

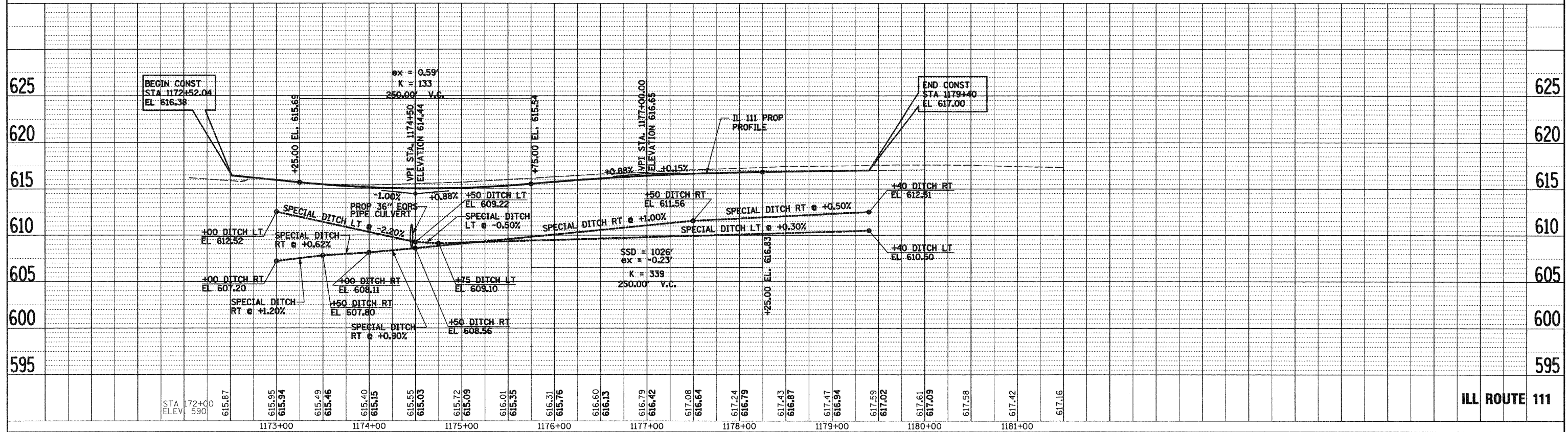


STA 86+00 TO STA 101+00

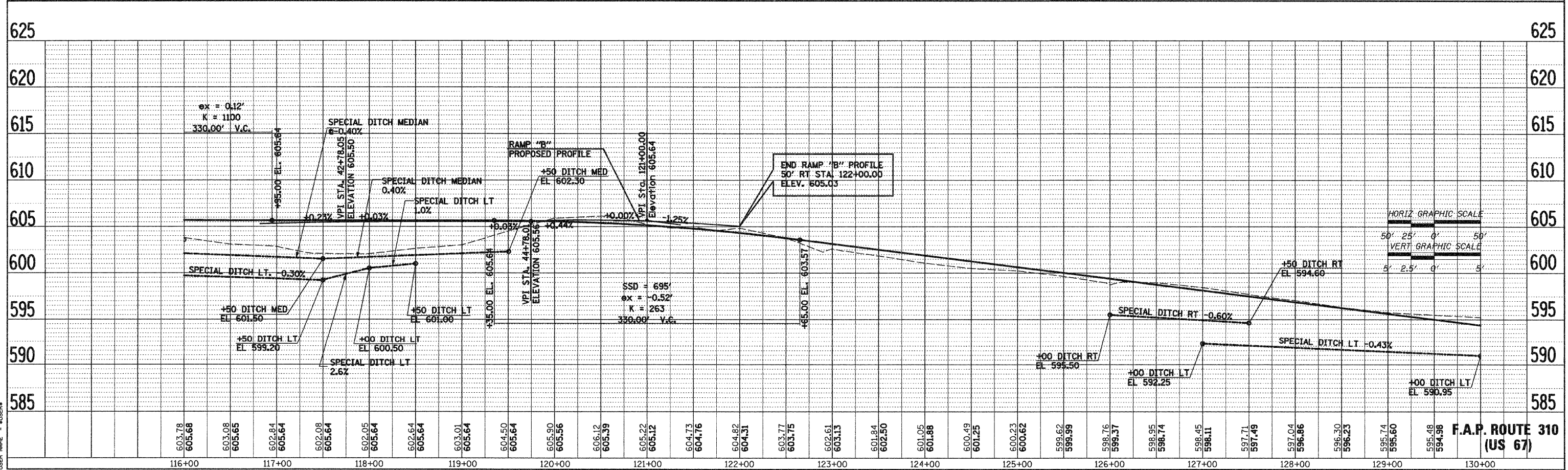
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	67
STA. 115+00		TO STA. 130+00		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				

PLAN SURVEYED BY DATE  
 PLOTTED BY  
 CHECKED BY  
 RT. OF WAY CHECKED BY  
 CADD FILE NAME NO.



PROFILE SURVEYED BY DATE  
 PLOTTED BY  
 CHECKED BY  
 STRUCTURE NOTATION CIP/D  
 NO.

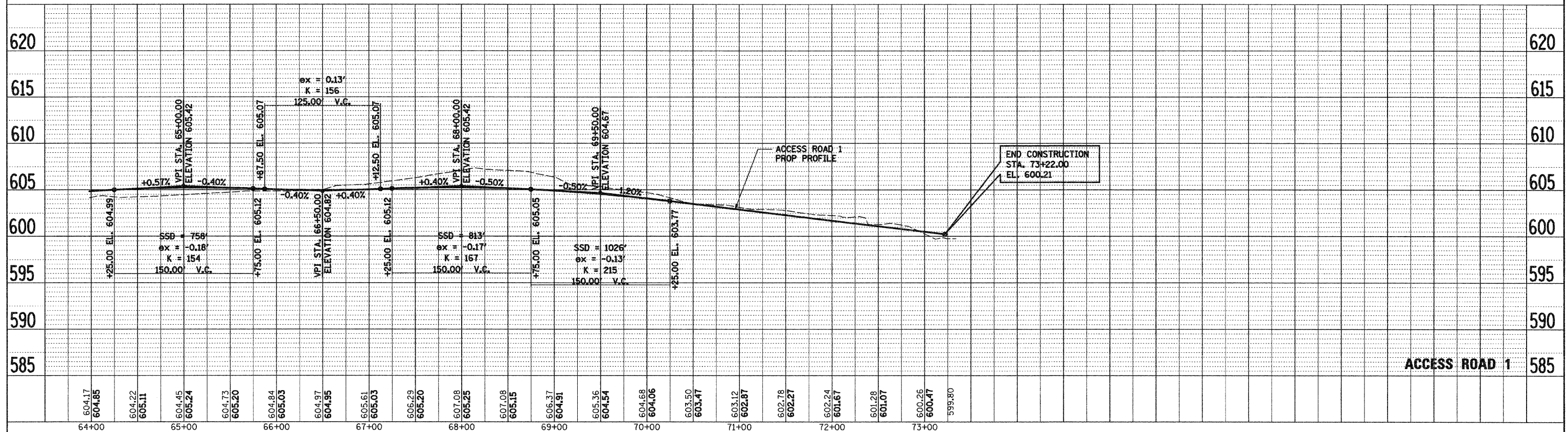


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 FILE NAME = #FILE#  
 PLOT SCALE = #SCALE#  
 USER NAME = #USER#

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	68
STA. 49+00 TO STA. 73+22.00		FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				

PLAN

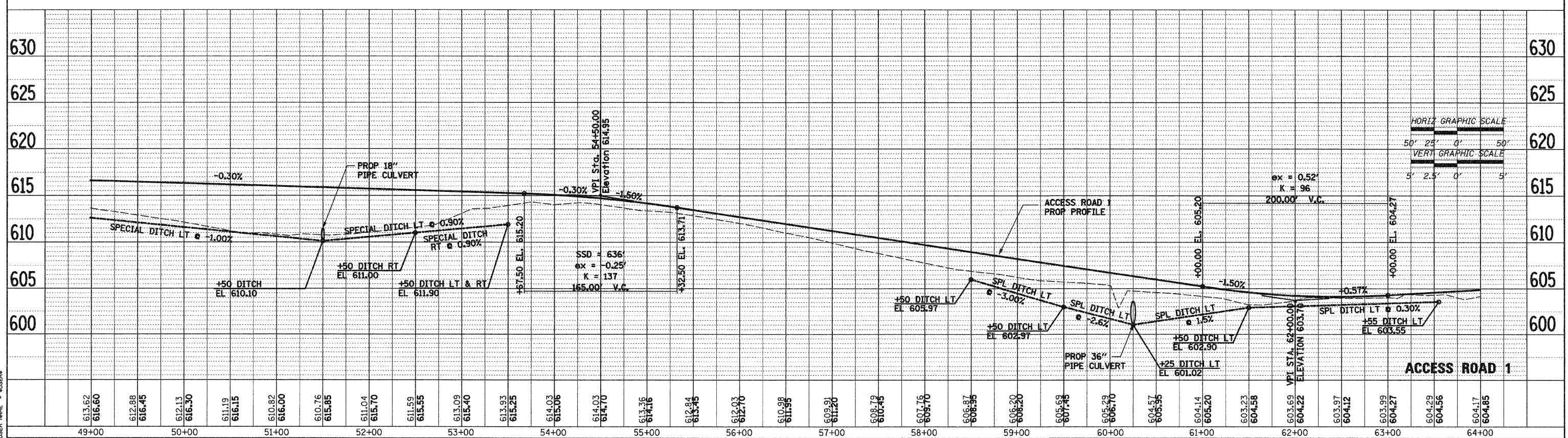
SURVEYED	BY	DATE
PLOTTED		
CHECKED		
REVISIONS		
NO. OF REVISIONS		
NOTE BOOK NO.		
ADD. FILE NAME		



STA 64+00 TO STA 74+00

PROFILE

SURVEYED	BY	DATE
PLOTTED		
CHECKED		
REVISIONS		
NO. OF REVISIONS		
NOTE BOOK NO.		
ADD. FILE NAME		



STA 49+00 TO STA 64+00

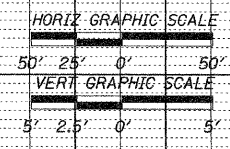
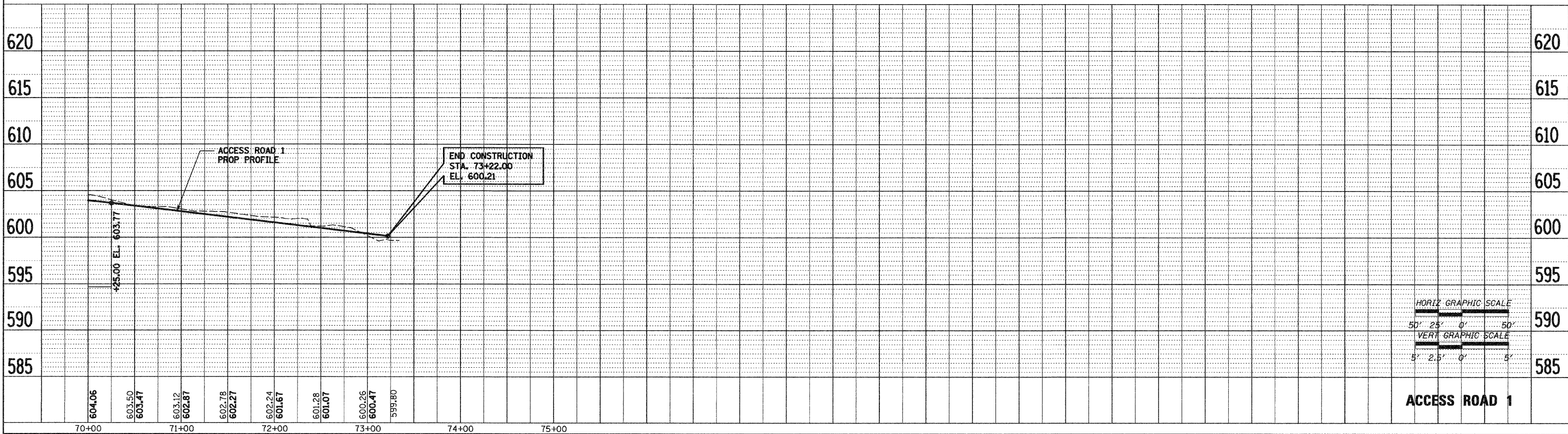
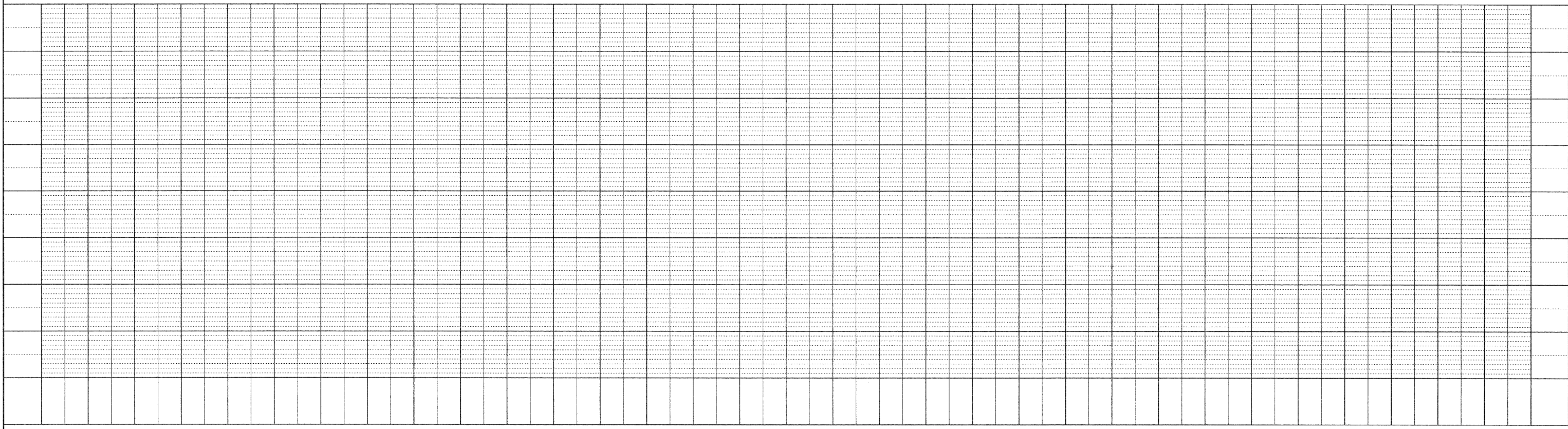
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	69
STA. 70+00		TO STA. 73+00		
FED. ROAD DIST. NO. 8		ILLINOIS		FED. AID PROJECT
CONTRACT NO. 76311				

PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
	CHECKED	
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
	CHECKED	
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	

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**ACCESS ROAD 1**

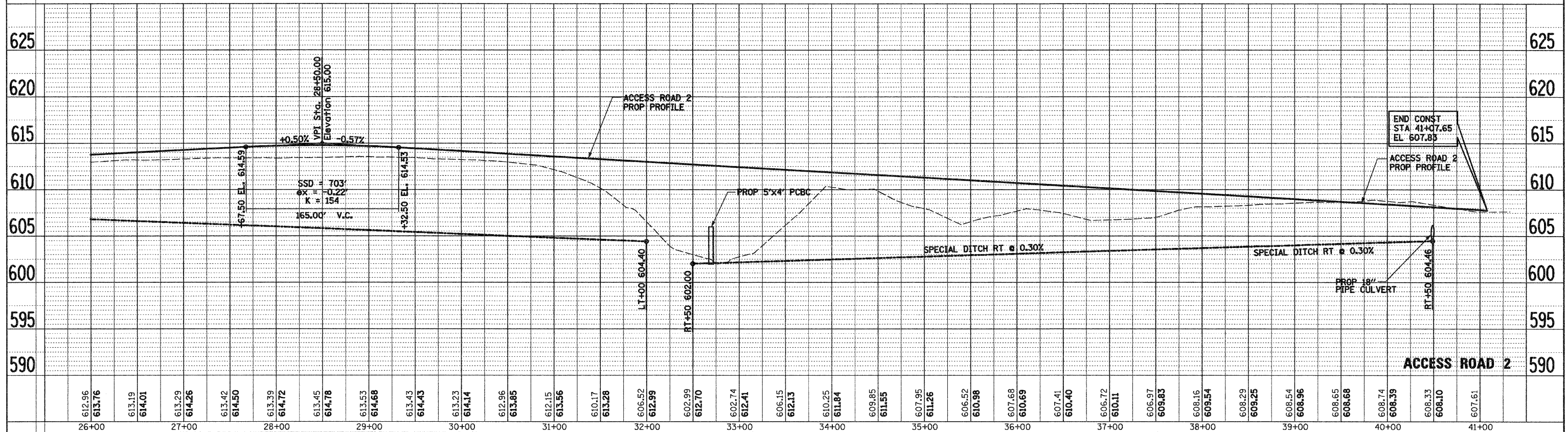
STA 70+00 TO STA 75+00



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	70
STA. 10+00		TO STA. 41+07.65		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				

PLAN

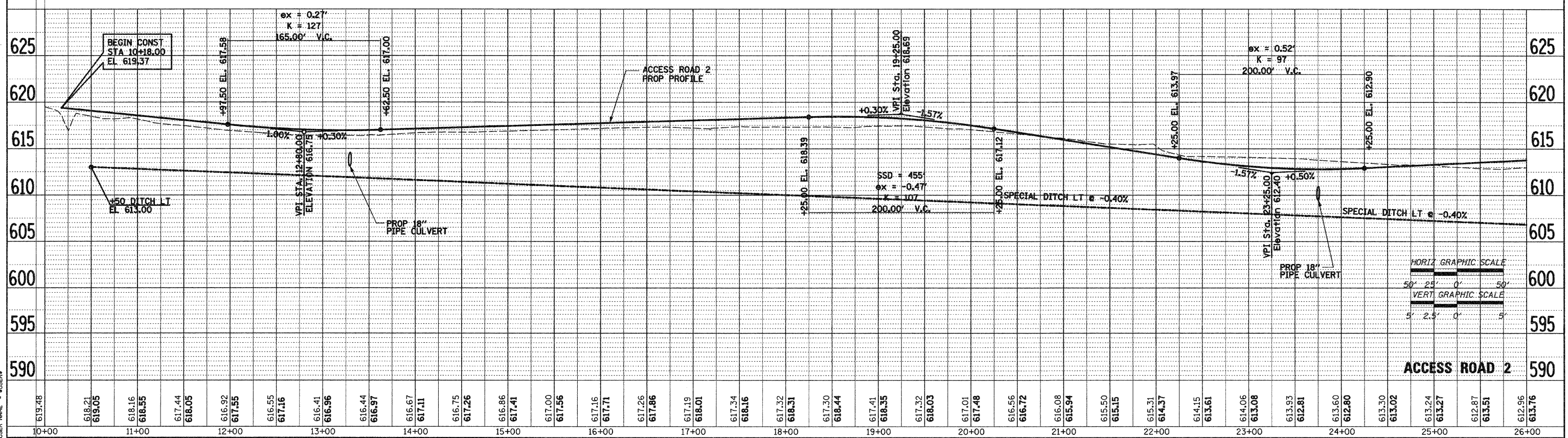
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PLOTTED		
CHECKED		
BY		
NO. OF WAYS CHECKED		
CADD FILE NAME		



STA 26+00 TO STA 41+08

PROFILE

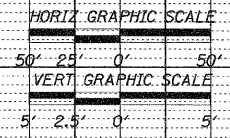
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PLOTTED		
CHECKED		
BY		
NO. OF WAYS CHECKED		
STRUCTURE NOTATION CHNO		



STA 10+00 TO STA 26+00

PLAN

SURVEYED	BY	DATE
PLOTTED		
CHECKED		
BY		
NO. OF WAYS CHECKED		
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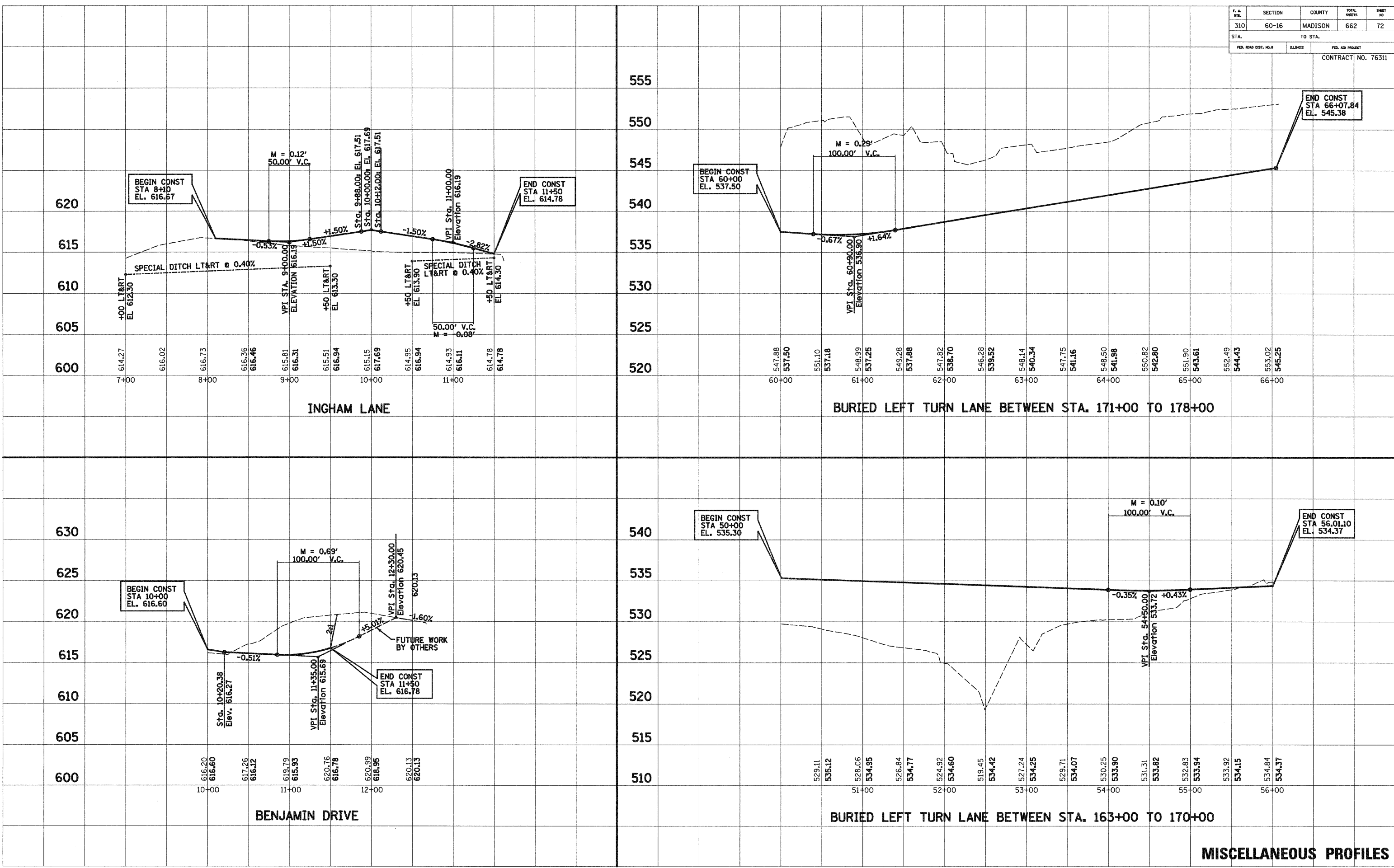




F. A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	72
STA.		TO STA.		
FED. ROAD DIST. NO. 9		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 76311				

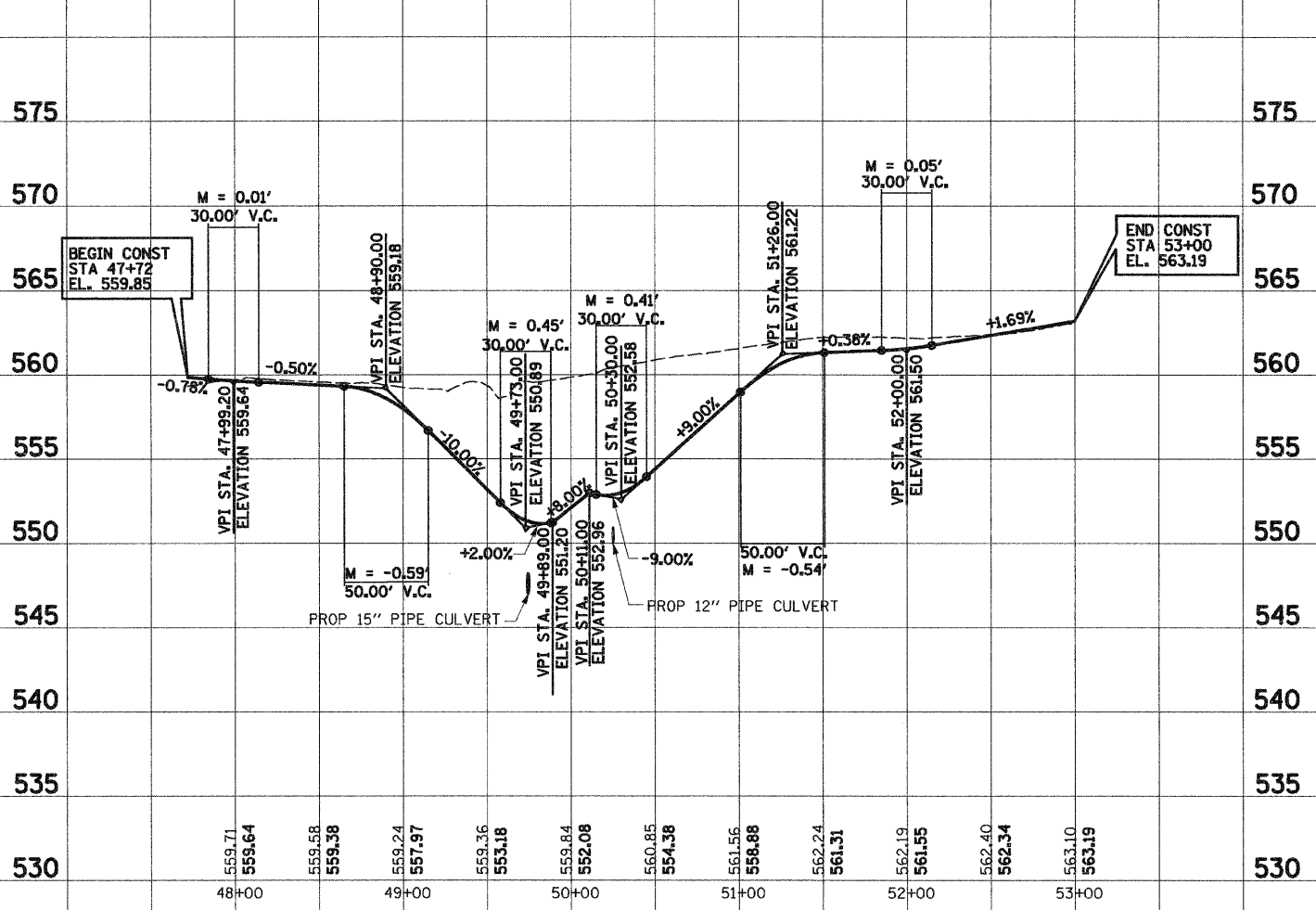
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 AREAS CHECKED

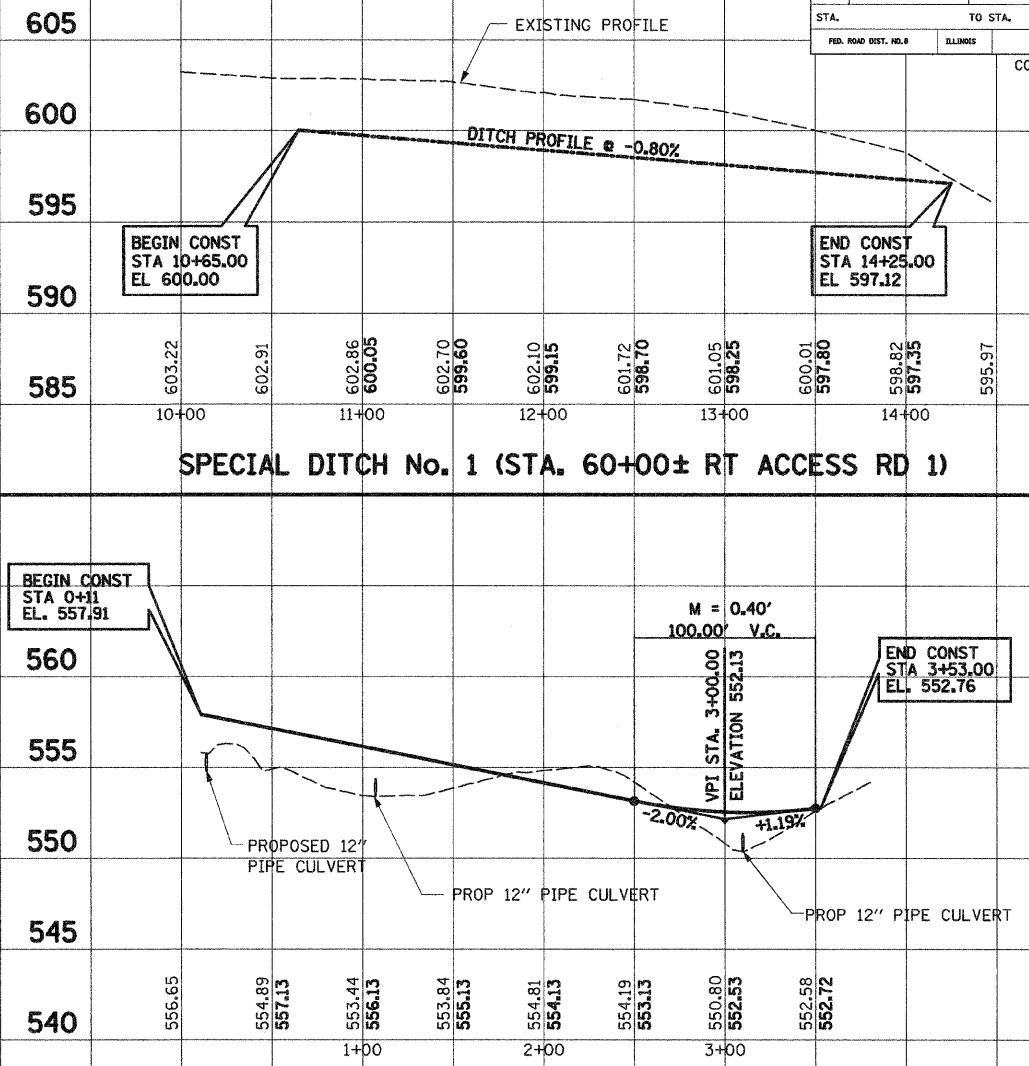


MISCELLANEOUS PROFILES

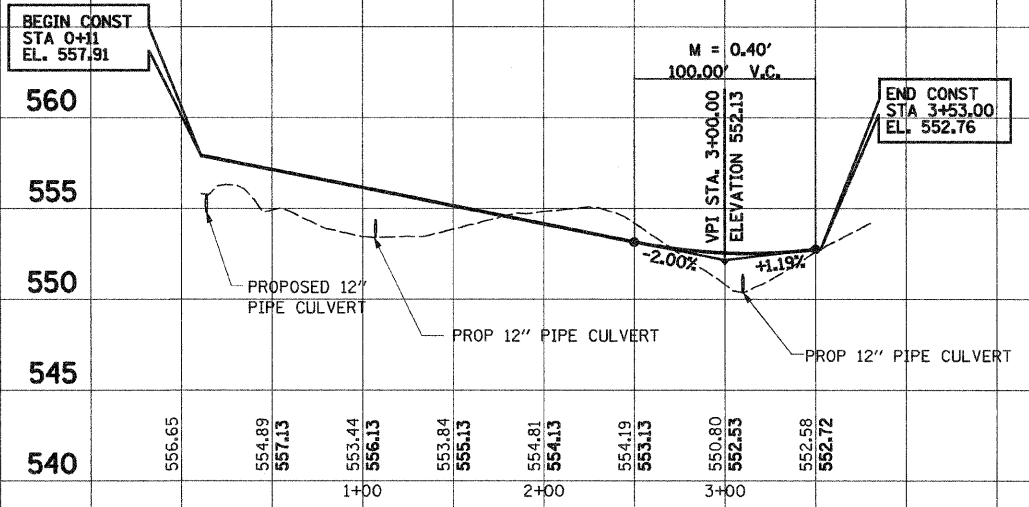
F. A. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	73
STA.	TO STA.			
FED. ROAD DIST. NO. 8	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 76311				



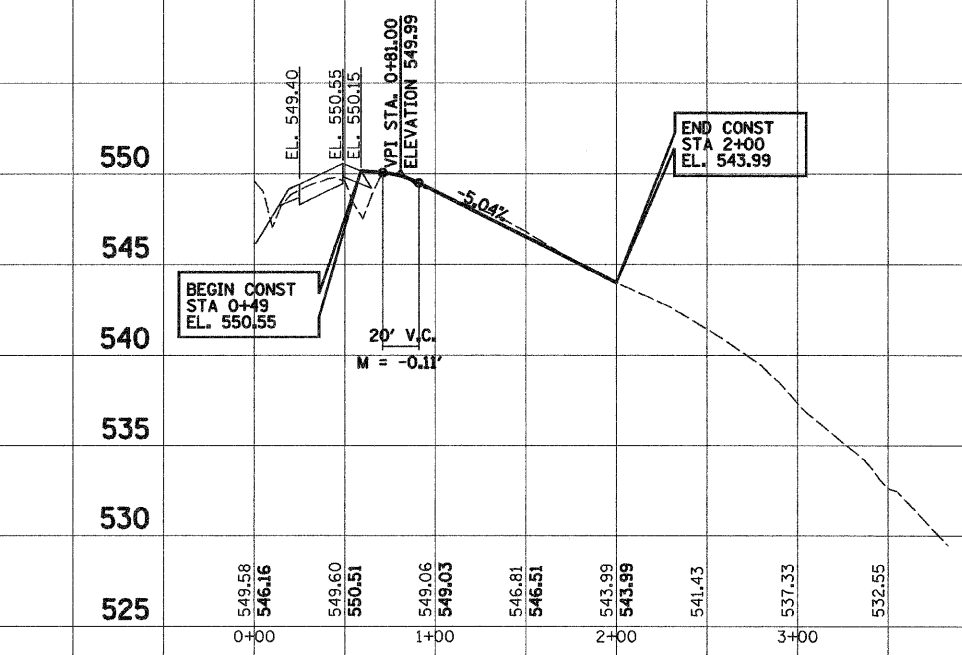
PRIVATE ENT LT & RT  
STA 16+50 OF LAGEMANN RD



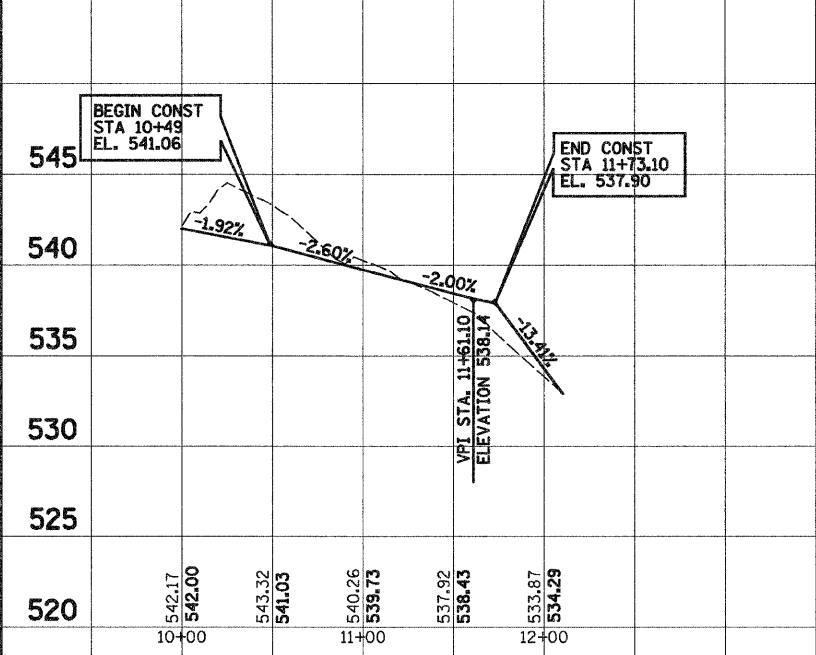
SPECIAL DITCH No. 1 (STA. 60+00± RT ACCESS RD 1)



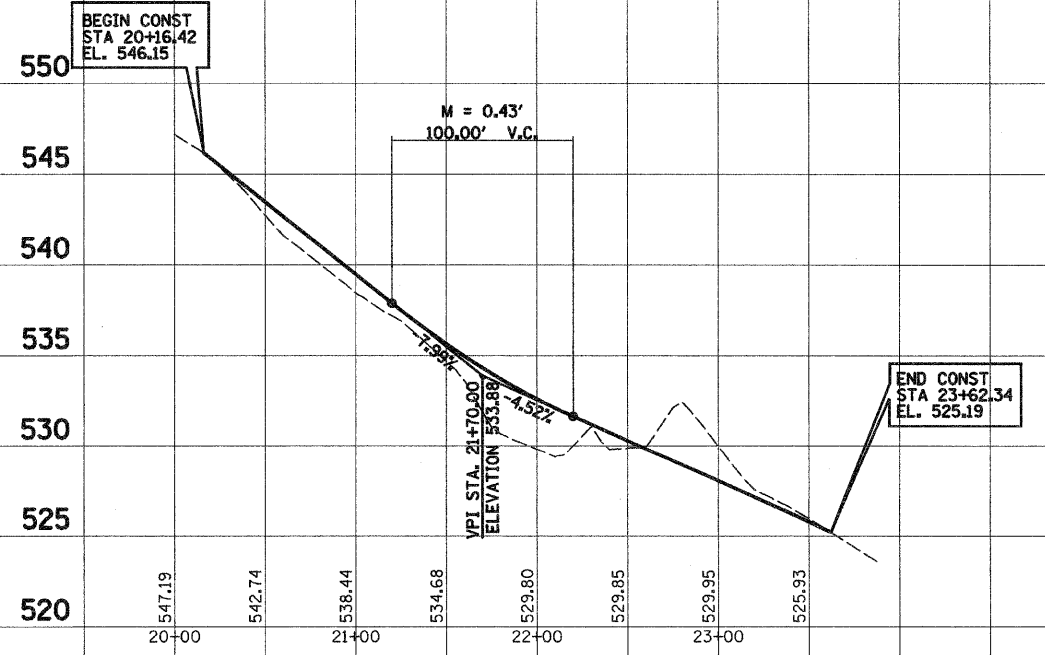
SERVICE DRIVE LT STA 19+61.50 (LAGEMANN RD)



PRIVATE ENTRANCE RT STA 184+50.00 (FAP 310)



SERVICE DRIVE RT STA 221+80.00 (FAP 310)  
PROFILE 1



SERVICE DRIVE RT STA 221+80.00 (FAP 310)  
PROFILE 2

MISCELLANEOUS PROFILES

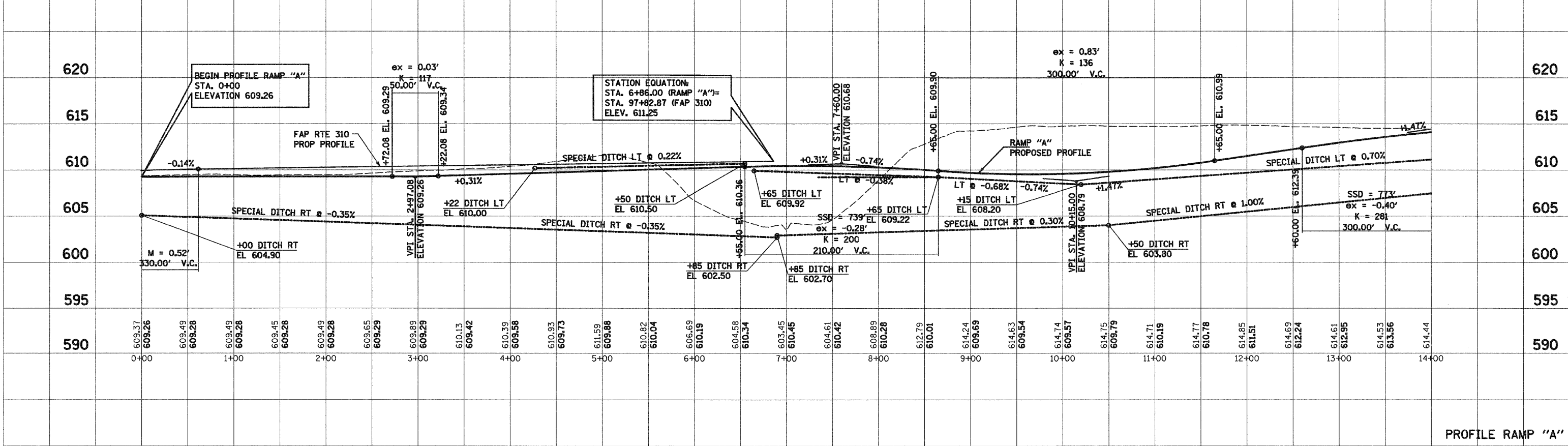
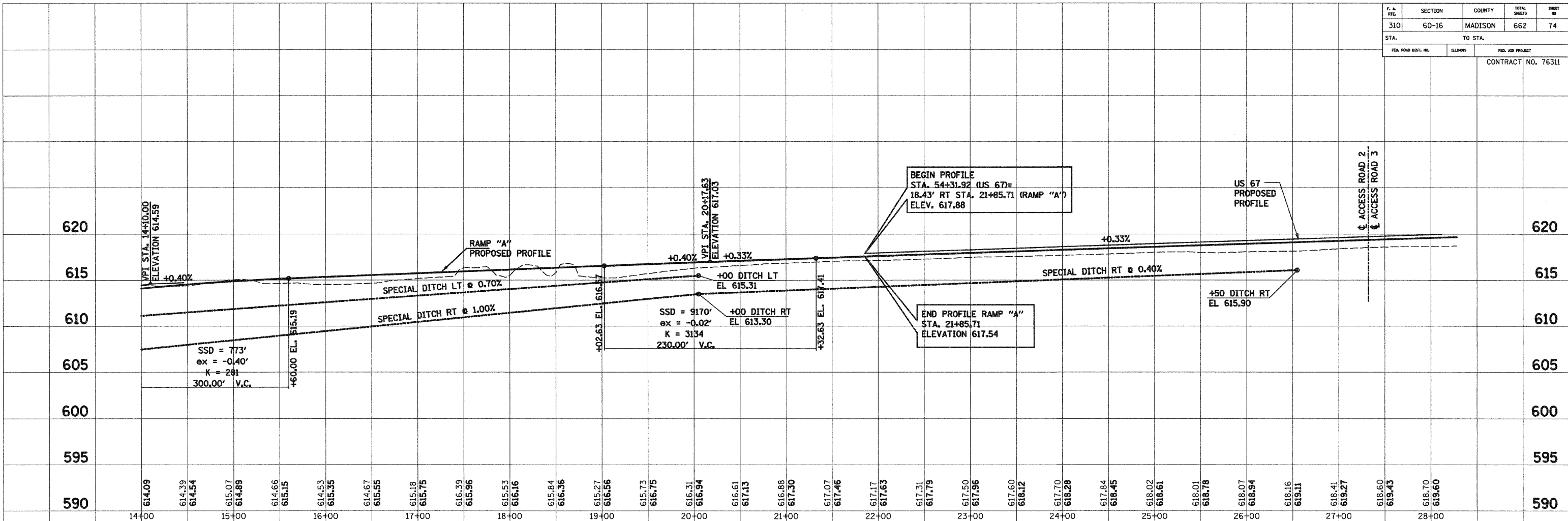
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F.A. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	74
STA. TO STA.		FED. AID PROJECT		
		CONTRACT NO. 76311		

DATE PLOTTED: \_\_\_\_\_  
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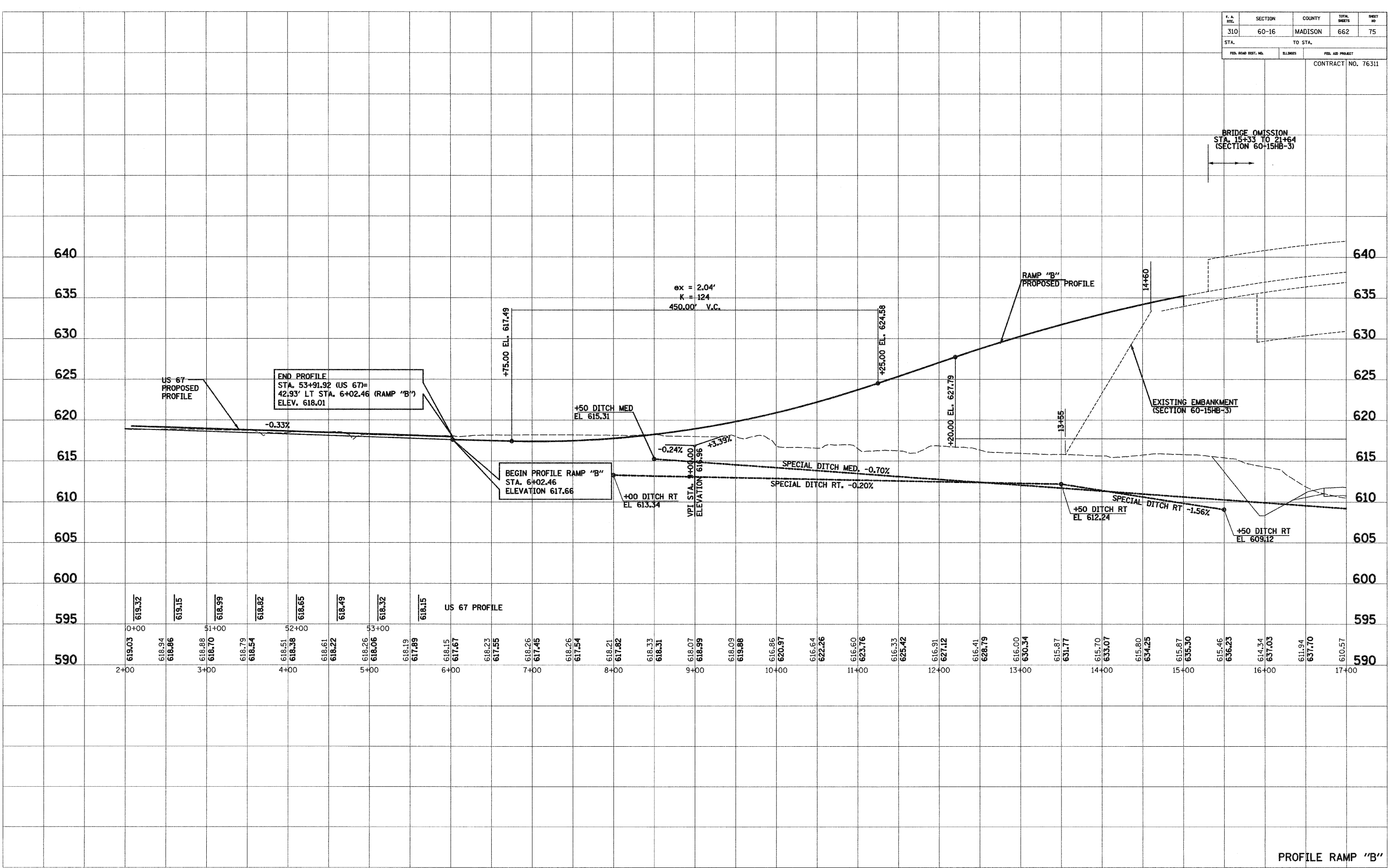
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PROFILE RAMP "A"

F. A. RTG.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	75
STA.		TO STA.		
FED. ROAD DIST. NO.		ELLINGS	FED. AID PROJECT	
CONTRACT NO. 76311				

BRIDGE OMISSION  
STA. 15+33 TO 21+64  
(SECTION 60-15HB-3)



PROFILE RAMP "B"

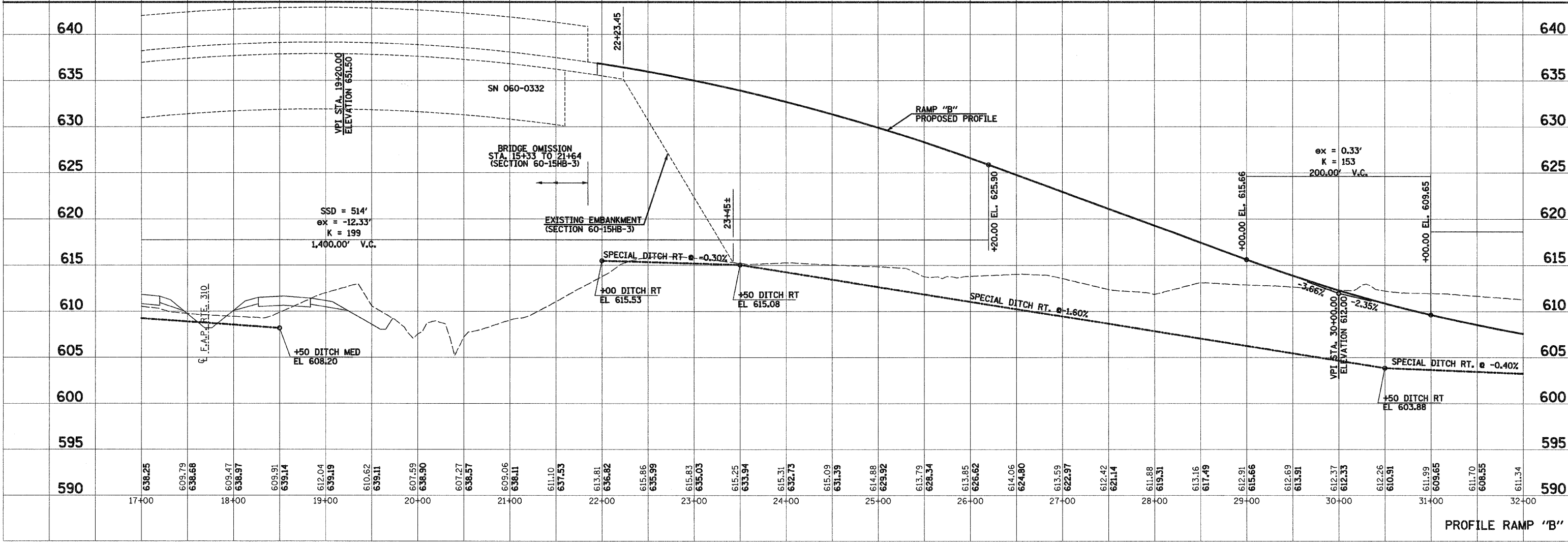
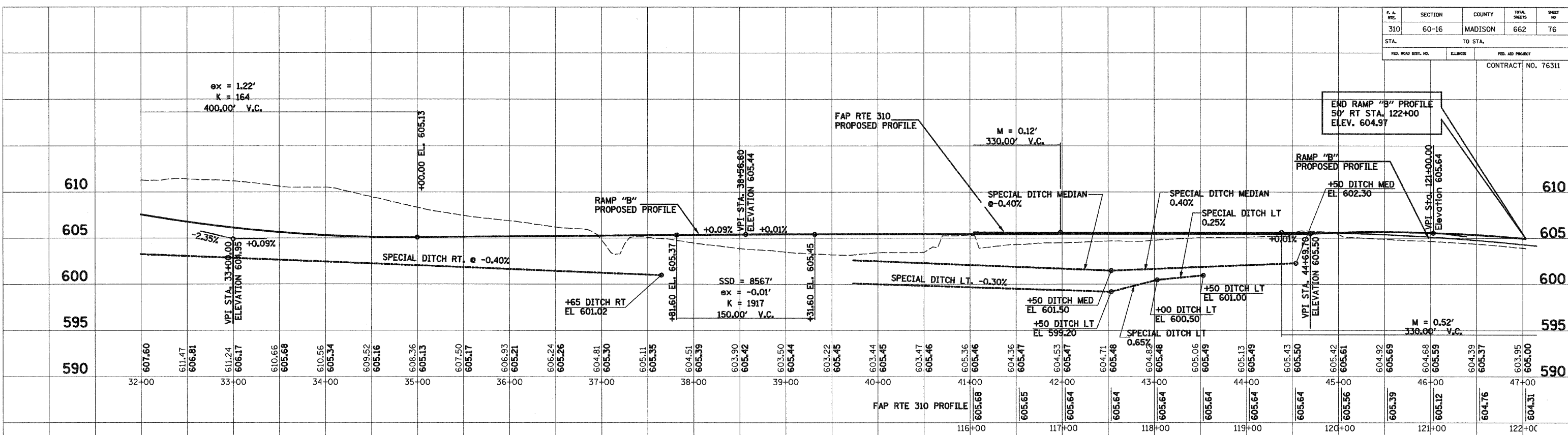
SWR/ELI  
PLOTTED  
NOTE BOOK  
NO. \_\_\_\_\_  
TEMPLATE  
AREAS CHECKED

SWR/ELI  
PLOTTED  
NOTE BOOK  
NO. \_\_\_\_\_  
TEMPLATE  
AREAS CHECKED

F.A. Rte.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	76
STA.	TO STA.			
FED. ROAD DIST. NO.	ELLINES	FED. AID PROJECT		
CONTRACT NO. 76311				

SURVEY PLOTTED  
 NOTE BOOK TEMPLATE  
 AREAS CHECKED

SURVEY PLOTTED  
 NOTE BOOK TEMPLATE  
 AREAS CHECKED



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	77
STA.	TO STA.			
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				

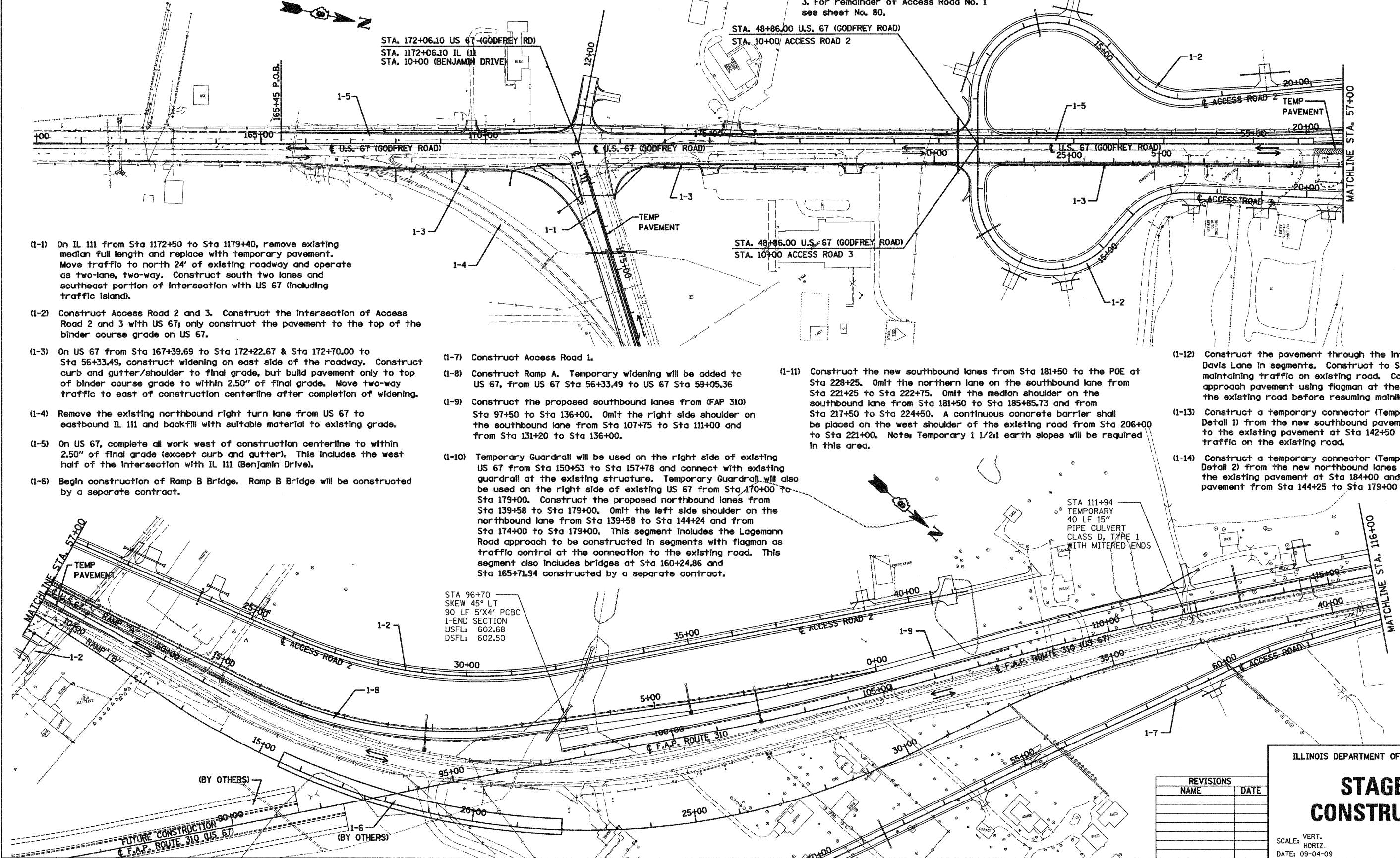
**STAGE CONSTRUCTION:**

**STAGE 1**

Two-way traffic will be maintained on existing pavements.

**NOTES:**

1. Ingham Lane can not be closed until Access Road No. 1 construction is complete.
2. Lagemann Road and N. Ingham Lane can not be constructed simultaneously.
3. For remainder of Access Road No. 1 see sheet No. 80.
4. See Pavement Marking Schedules for information on temporary striping.
5. See Typical Section and Schedules for temporary pavement information.
6. See IL-111 and US 67 Temporary Traffic Plans for additional information.



- (1-1) On IL 111 from Sta 1172+50 to Sta 1179+40, remove existing median full length and replace with temporary pavement. Move traffic to north 24' of existing roadway and operate as two-lane, two-way. Construct south two lanes and southeast portion of intersection with US 67 (including traffic island).
- (1-2) Construct Access Road 2 and 3. Construct the intersection of Access Road 2 and 3 with US 67; only construct the pavement to the top of the binder course grade on US 67.
- (1-3) On US 67 from Sta 167+39.69 to Sta 172+22.67 & Sta 172+70.00 to Sta 56+33.49, construct widening on east side of the roadway. Construct curb and gutter/shoulder to final grade, but build pavement only to top of binder course grade to within 2.50" of final grade. Move two-way traffic to east of construction centerline after completion of widening.
- (1-4) Remove the existing northbound right turn lane from US 67 to eastbound IL 111 and backfill with suitable material to existing grade.
- (1-5) On US 67, complete all work west of construction centerline to within 2.50" of final grade (except curb and gutter). This includes the west half of the intersection with IL 111 (Benjamin Drive).
- (1-6) Begin construction of Ramp B Bridge. Ramp B Bridge will be constructed by a separate contract.

- (1-7) Construct Access Road 1.
- (1-8) Construct Ramp A. Temporary widening will be added to US 67, from US 67 Sta 56+33.49 to US 67 Sta 59+05.36
- (1-9) Construct the proposed southbound lanes from (FAP 310) Sta 97+50 to Sta 136+00. Omit the right side shoulder on the southbound lane from Sta 107+75 to Sta 111+00 and from Sta 131+20 to Sta 136+00.
- (1-10) Temporary Guardrail will be used on the right side of existing US 67 from Sta 150+53 to Sta 157+78 and connect with existing guardrail at the existing structure. Temporary Guardrail will also be used on the right side of existing US 67 from Sta 170+00 to Sta 179+00. Construct the proposed northbound lanes from Sta 139+58 to Sta 179+00. Omit the left side shoulder on the northbound lane from Sta 139+58 to Sta 144+24 and from Sta 174+00 to Sta 179+00. This segment includes the Lagemann Road approach to be constructed in segments with flagman as traffic control at the connection to the existing road. This segment also includes bridges at Sta 160+24.86 and Sta 165+71.94 constructed by a separate contract.

- (1-11) Construct the new southbound lanes from Sta 181+50 to the POE at Sta 228+25. Omit the northern lane on the southbound lane from Sta 221+25 to Sta 222+75. Omit the median shoulder on the southbound lane from Sta 181+50 to Sta 185+85.73 and from Sta 217+50 to Sta 224+50. A continuous concrete barrier shall be placed on the west shoulder of the existing road from Sta 206+00 to Sta 221+00. Note Temporary 1 1/2:1 earth slopes will be required in this area.

- (1-12) Construct the pavement through the intersection at Davis Lane in segments. Construct to Sta 202+00 while maintaining traffic on existing road. Construct the approach pavement using flagman at the connection to the existing road before resuming mainline construction.
- (1-13) Construct a temporary connector (Temporary Connector Detail 1) from the new southbound pavement at Sta 136+00 to the existing pavement at Sta 142+50 while maintaining traffic on the existing road.
- (1-14) Construct a temporary connector (Temporary Connector Detail 2) from the new northbound lanes at Sta 179+00 to the existing pavement at Sta 184+00 and open the new pavement from Sta 144+25 to Sta 179+00 to two-way traffic.

STA 96+70  
SKEW 45° LT  
90 LF 5'X4' PCBC  
1-END SECTION  
USFL: 602.68  
DSFL: 602.50

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

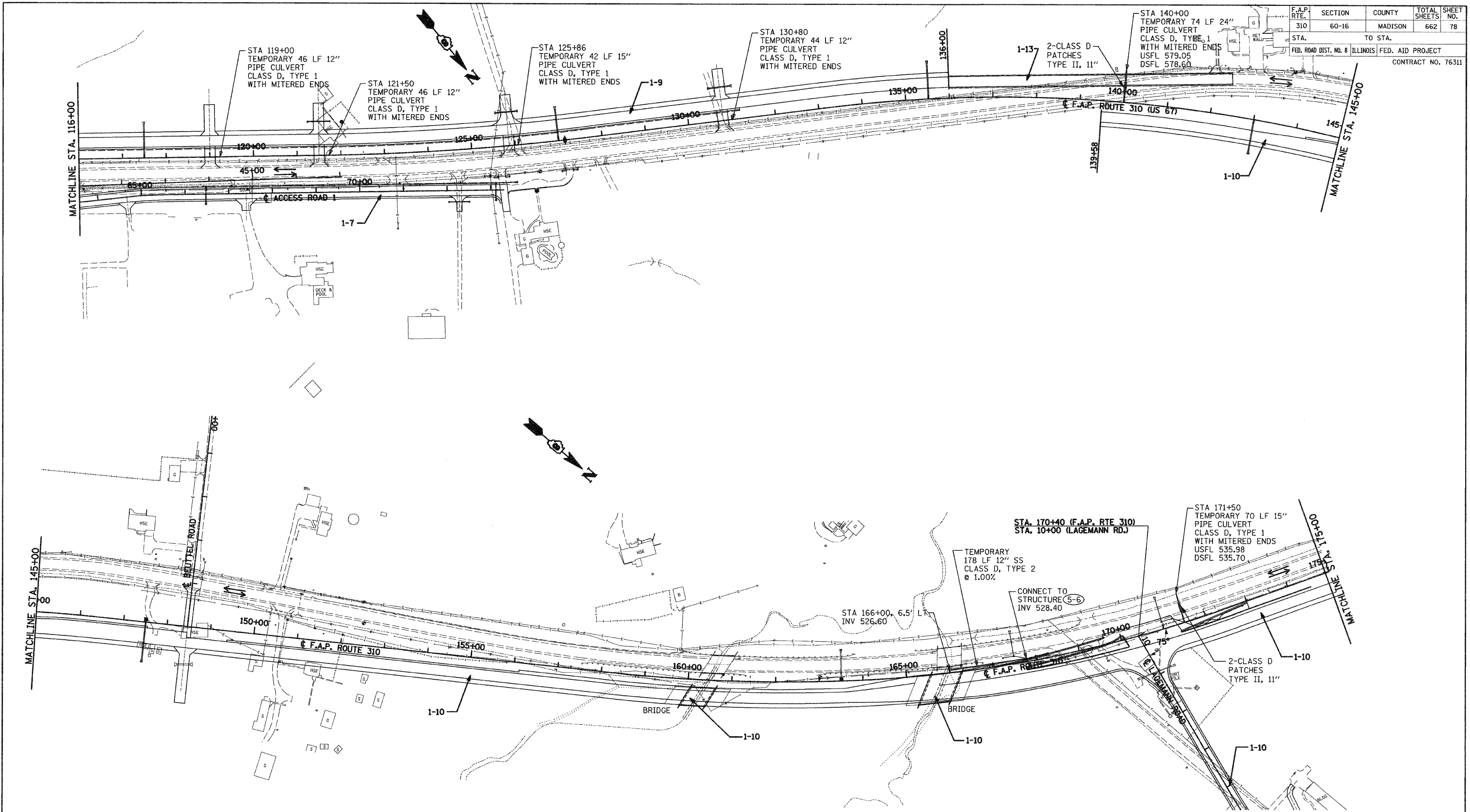
**STAGE 1  
CONSTRUCTION**

SCALE: VERT.  
HORIZ.  
DATE: 09-04-09

DRAWN BY: U.J.  
CHECKED BY: J.J.C.



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	78
STA.	TO STA.		FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 76311	



STA. 170+40 (F.A.P. RTE 310)  
STA. 10+00 (LAGEMANN RD.)

TEMPORARY  
178 LF 12" SS  
CLASS D, TYPE 2  
@ 1.00%

CONNECT TO  
STRUCTURE (5-6)  
INV 528.40

STA 171+50  
TEMPORARY 70 LF 15"  
PIPE CULVERT  
CLASS D, TYPE 1  
WITH MITERED ENDS  
USFL 535.98  
DSFL 535.70

2-CLASS D  
PATCHES  
TYPE II, 11"

REVISIONS	
NAME	DATE

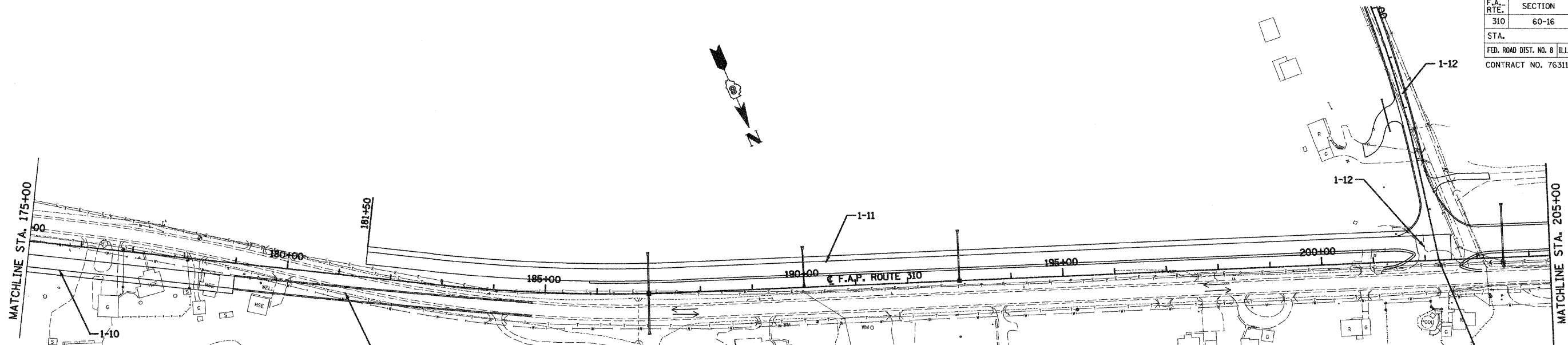
ILLINOIS DEPARTMENT OF TRANSPORTATION

# STAGE 1 CONSTRUCTION

SCALE: VERT.  
HORIZ.  
DATE: 09-04-09

DRAWN BY: U.J.  
CHECKED BY: J.J.C.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	79
STA.		TO STA.		
FED. ROAD DIST. NO. 8		ILLINOIS		FED. AID PROJECT
CONTRACT NO. 76311				

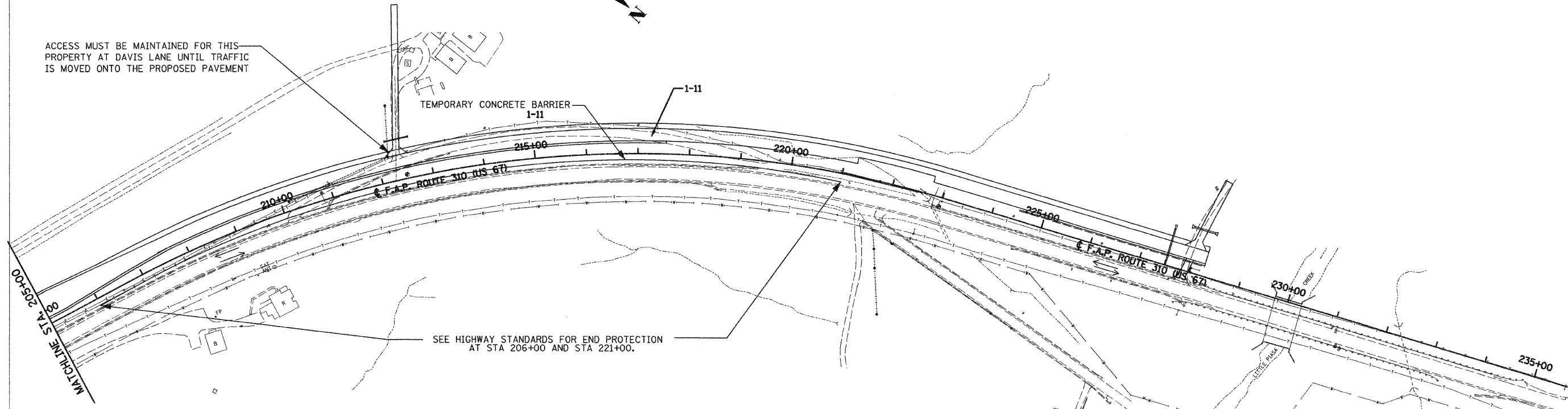


STA. EQUATION:  
 STA. 202+26.40 (F.A.P. RTE 310)  
 STA. 10+00 (DAVIS LN.)

ACCESS MUST BE MAINTAINED FOR THIS PROPERTY AT DAVIS LANE UNTIL TRAFFIC IS MOVED ONTO THE PROPOSED PAVEMENT

TEMPORARY CONCRETE BARRIER  
1-11

SEE HIGHWAY STANDARDS FOR END PROTECTION  
AT STA 206+00 AND STA 221+00.



REVISIONS	
NAME	DATE

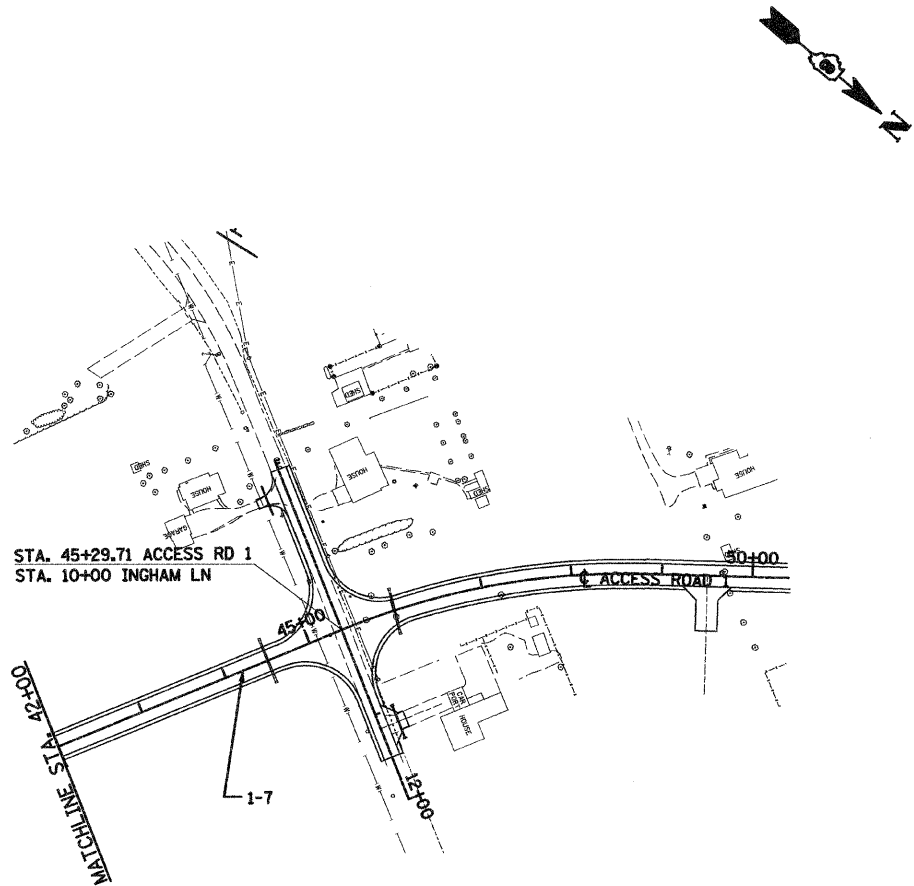
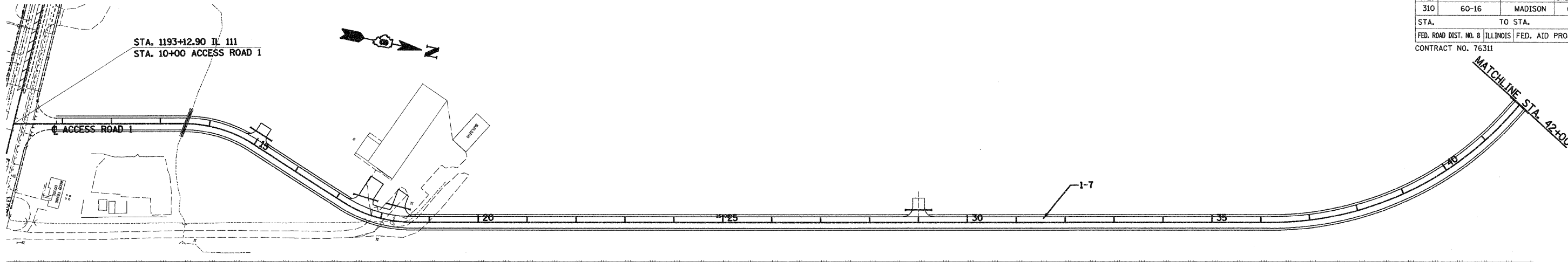
ILLINOIS DEPARTMENT OF TRANSPORTATION

## STAGE 1 CONSTRUCTION

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 09-04-09

DRAWN BY: U.J.  
 CHECKED BY: J.J.C.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	80
STA.		TO STA.		
FED. ROAD DIST. NO. 8 ILLINOIS		FED. AID PROJECT		
CONTRACT NO. 76311				



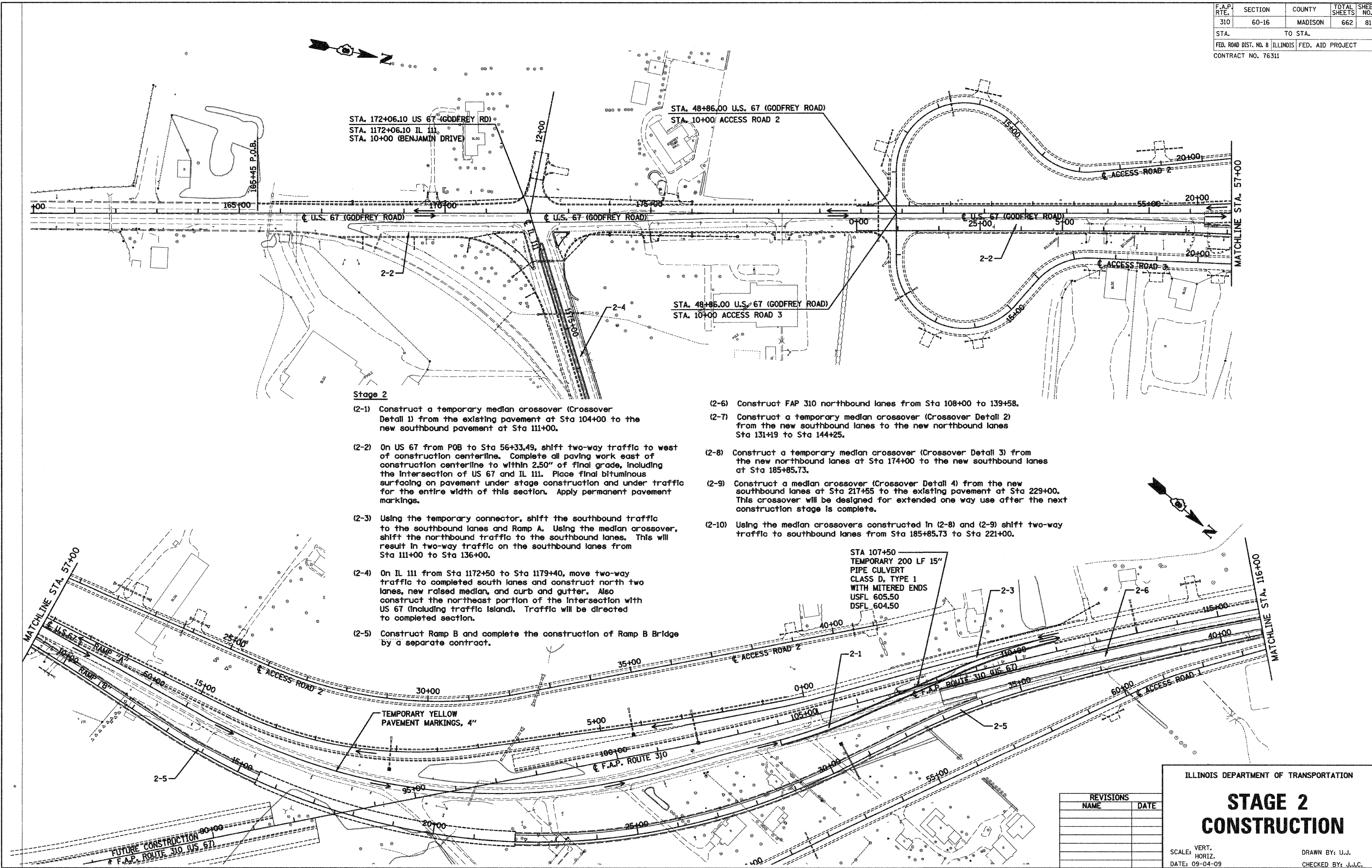
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

## STAGE 1 CONSTRUCTION

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 09-04-09

DRAWN BY: U.J.  
 CHECKED BY: J.J.C.



**Stage 2**

- (2-1) Construct a temporary median crossover (Crossover Detail 1) from the existing pavement at Sta 104+00 to the new southbound pavement at Sta 111+00.
- (2-2) On US 67 from POB to Sta 56+33.49, shift two-way traffic to west of construction centerline. Complete all paving work east of construction centerline to within 2.50" of final grade, including the intersection of US 67 and IL 111. Place final bituminous surfacing on pavement under stage construction and under traffic for the entire width of this section. Apply permanent pavement markings.
- (2-3) Using the temporary connector, shift the southbound traffic to the southbound lanes and Ramp A. Using the median crossover, shift the northbound traffic to the southbound lanes. This will result in two-way traffic on the southbound lanes from Sta 111+00 to Sta 136+00.
- (2-4) On IL 111 from Sta 1172+50 to Sta 1179+40, move two-way traffic to completed south lanes and construct north two lanes, new raised median, and curb and gutter. Also construct the northeast portion of the intersection with US 67 (including traffic island). Traffic will be directed to completed section.
- (2-5) Construct Ramp B and complete the construction of Ramp B Bridge by a separate contract.
- (2-6) Construct FAP 310 northbound lanes from Sta 108+00 to 139+58.
- (2-7) Construct a temporary median crossover (Crossover Detail 2) from the new southbound lanes to the new northbound lanes Sta 131+19 to Sta 144+25.
- (2-8) Construct a temporary median crossover (Crossover Detail 3) from the new northbound lanes at Sta 174+00 to the new southbound lanes at Sta 185+85.73.
- (2-9) Construct a median crossover (Crossover Detail 4) from the new southbound lanes at Sta 217+55 to the existing pavement at Sta 229+00. This crossover will be designed for extended one way use after the next construction stage is complete.
- (2-10) Using the median crossovers constructed in (2-8) and (2-9) shift two-way traffic to southbound lanes from Sta 185+85.73 to Sta 221+00.

STA 107+50  
 TEMPORARY 200 LF 15"  
 PIPE CULVERT  
 CLASS D, TYPE 1  
 WITH MITERED ENDS  
 USFL 605.50  
 DSFL 604.50

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

# STAGE 2 CONSTRUCTION

SCALE: VERT.  
 HORIZ.  
 DATE: 09-04-09

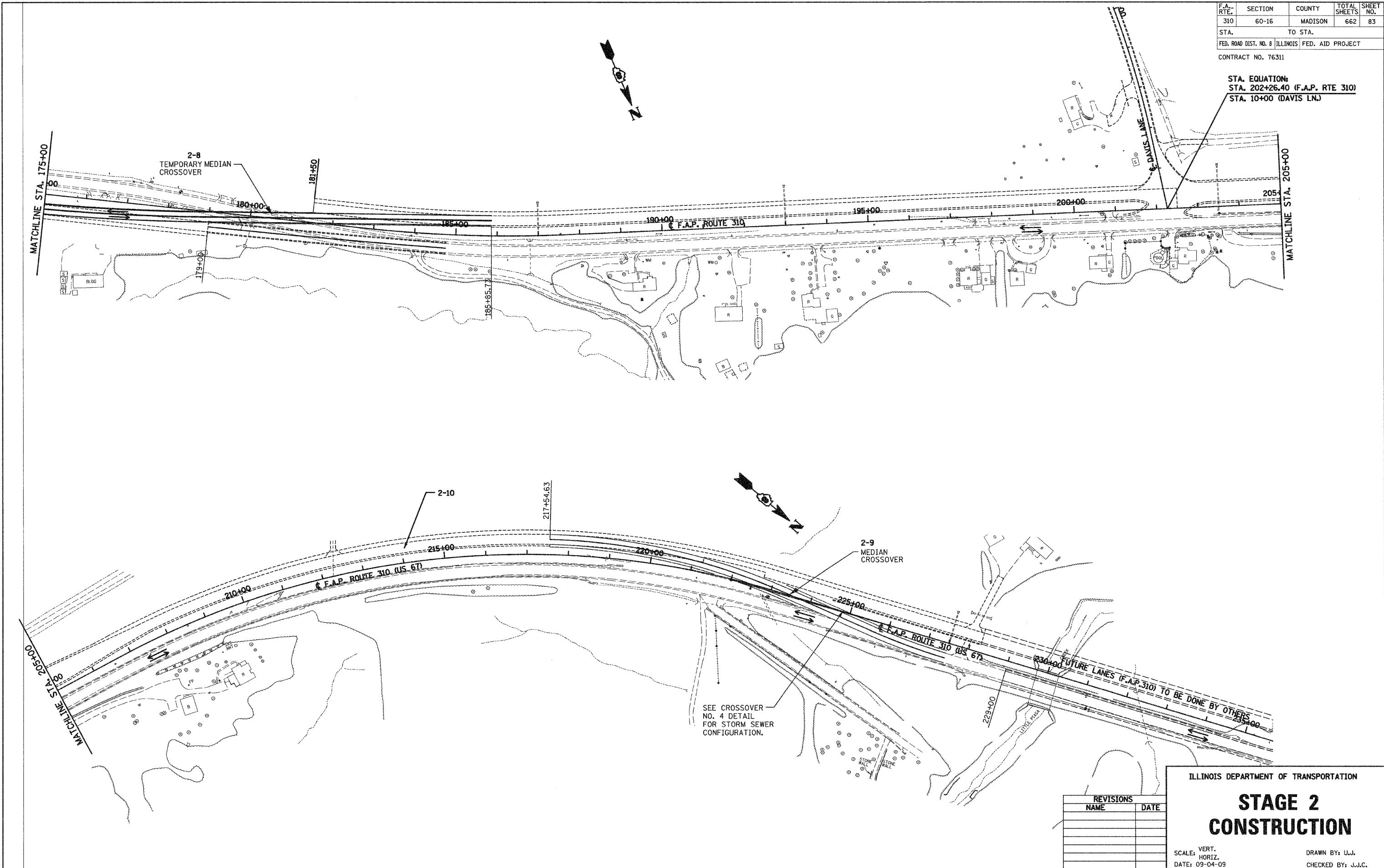
DRAWN BY: U.J.  
 CHECKED BY: J.J.C.

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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	83
STA.		TO STA.		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				

STA. EQUATION:  
 STA. 202+26.40 (F.A.P. RTE 310)  
 STA. 10+00 (DAVIS LN.)



SEE CROSSOVER  
 NO. 4 DETAIL  
 FOR STORM SEWER  
 CONFIGURATION.

REVISIONS	
NAME	DATE

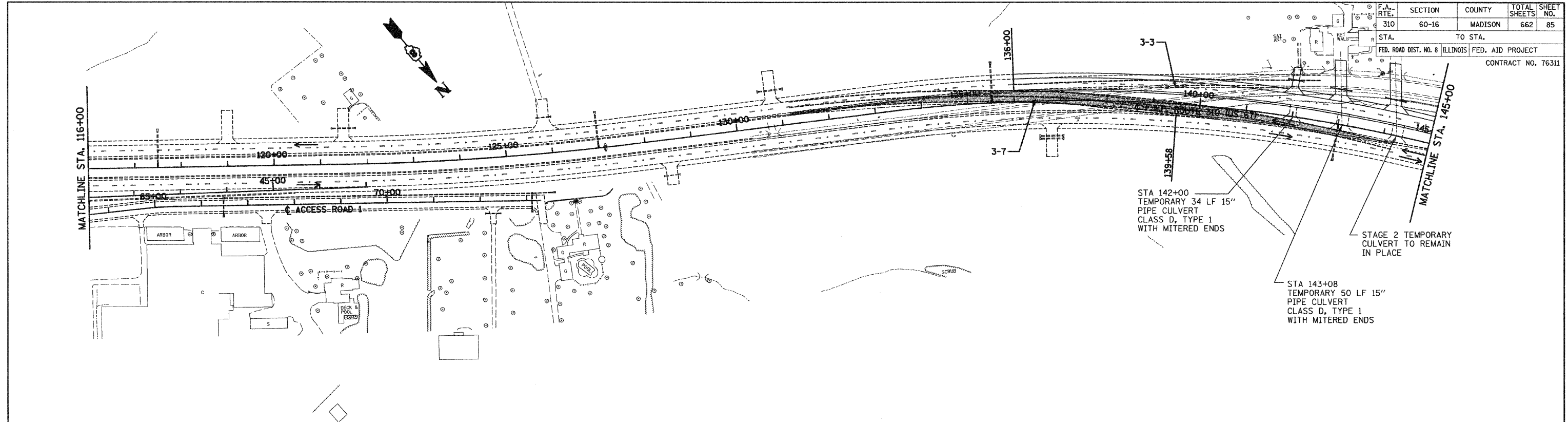
ILLINOIS DEPARTMENT OF TRANSPORTATION

## STAGE 2 CONSTRUCTION

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 09-04-09

DRAWN BY: U.J.  
 CHECKED BY: J.J.C.



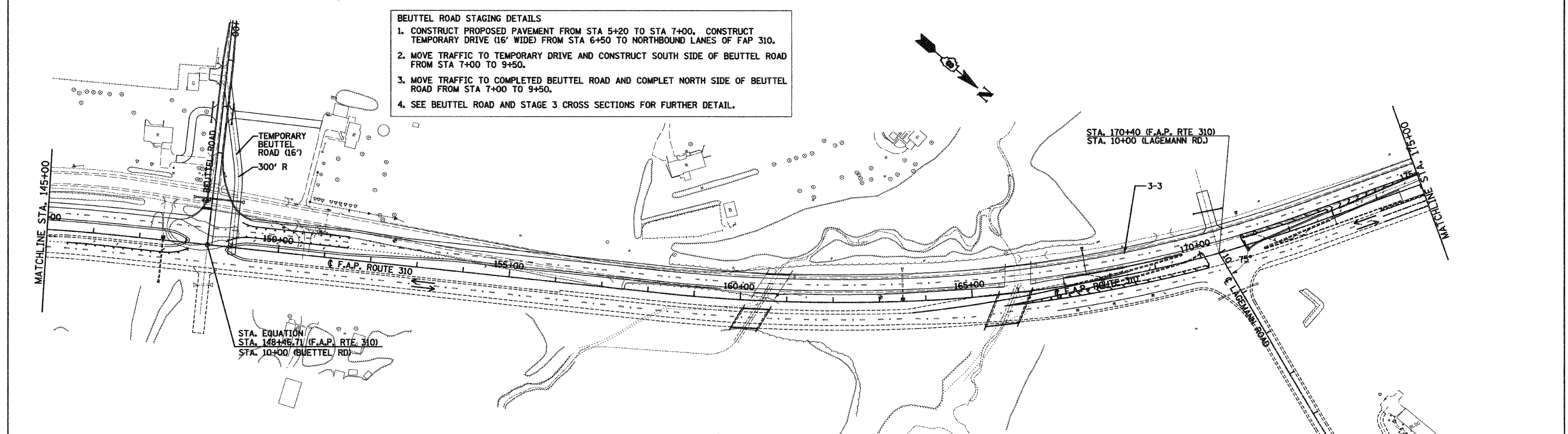


STA 142+00  
TEMPORARY 34 LF 15"  
PIPE CULVERT  
CLASS D, TYPE 1  
WITH MITERED ENDS

STAGE 2 TEMPORARY  
CULVERT TO REMAIN  
IN PLACE

STA 143+08  
TEMPORARY 50 LF 15"  
PIPE CULVERT  
CLASS D, TYPE 1  
WITH MITERED ENDS

- BEUTTEL ROAD STAGING DETAILS**
1. CONSTRUCT PROPOSED PAVEMENT FROM STA 5+20 TO STA 7+00. CONSTRUCT TEMPORARY DRIVE (16' WIDE) FROM STA 6+50 TO NORTHBOUND LANES OF FAP 310.
  2. MOVE TRAFFIC TO TEMPORARY DRIVE AND CONSTRUCT SOUTH SIDE OF BEUTTEL ROAD FROM STA 7+00 TO 9+50.
  3. MOVE TRAFFIC TO COMPLETED BEUTTEL ROAD AND COMPLETE NORTH SIDE OF BEUTTEL ROAD FROM STA 7+00 TO 9+50.
  4. SEE BEUTTEL ROAD AND STAGE 3 CROSS SECTIONS FOR FURTHER DETAIL.



STA. EQUATION  
STA. 148+46.71 (F.A.P. RTE. 310)  
STA. 10+00 (BEUTTEL RD.)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

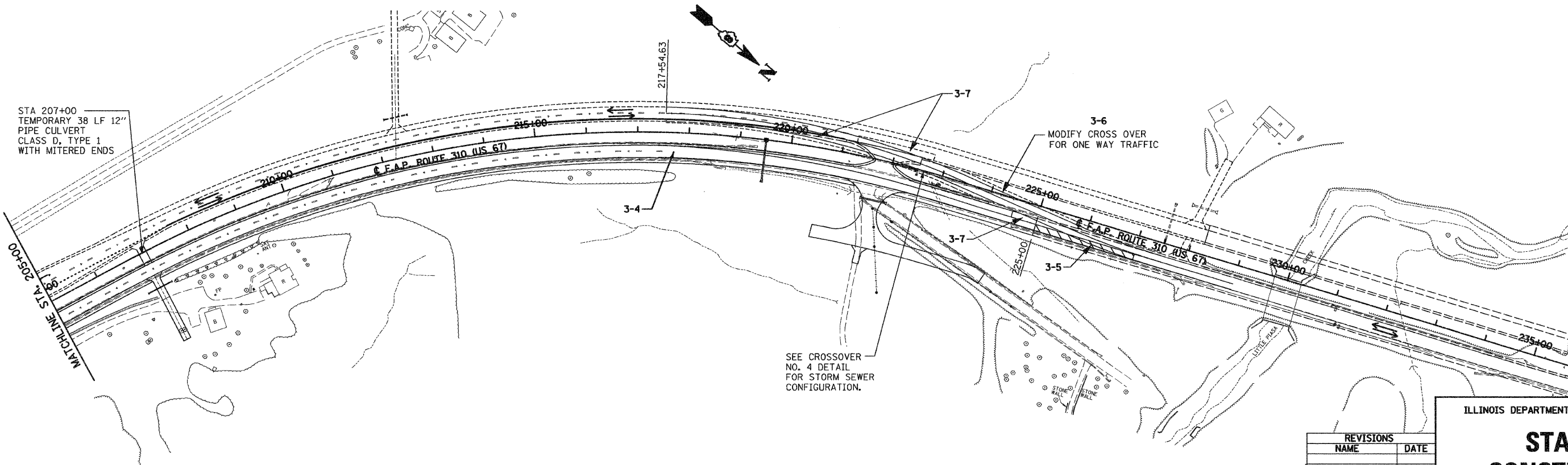
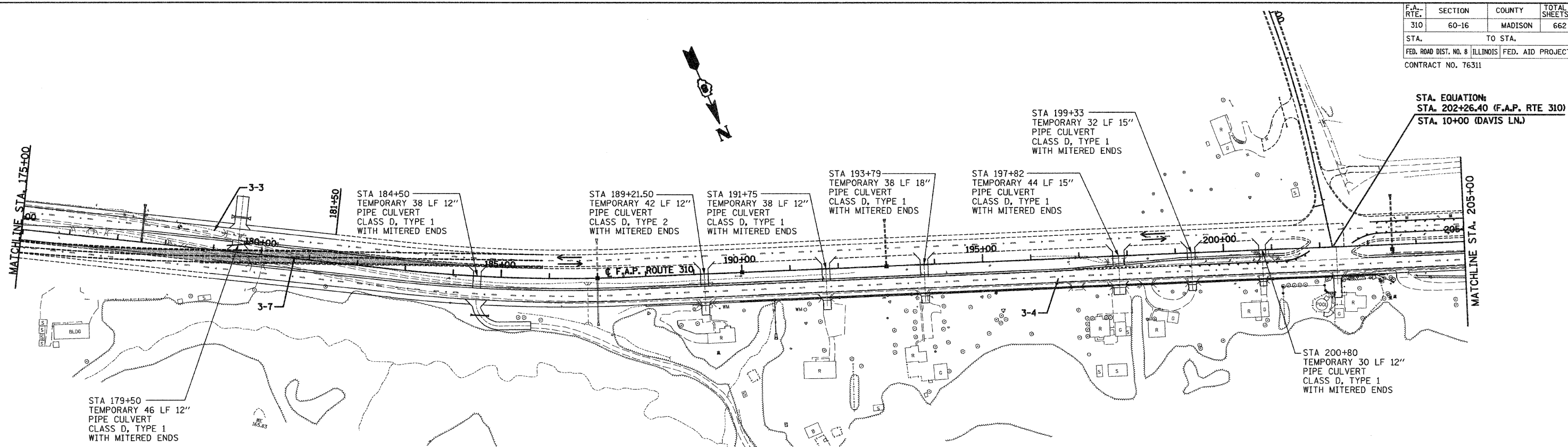
# STAGE 3 CONSTRUCTION

SCALE: VERT.  
HORIZ.  
DATE: 09-04-09

DRAWN BY: U.J.  
CHECKED BY: J.J.C.



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	86
STA.		TO STA.		
FED. ROAD DIST. NO. 8 ILLINOIS		FED. AID PROJECT		
CONTRACT NO. 76311				



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

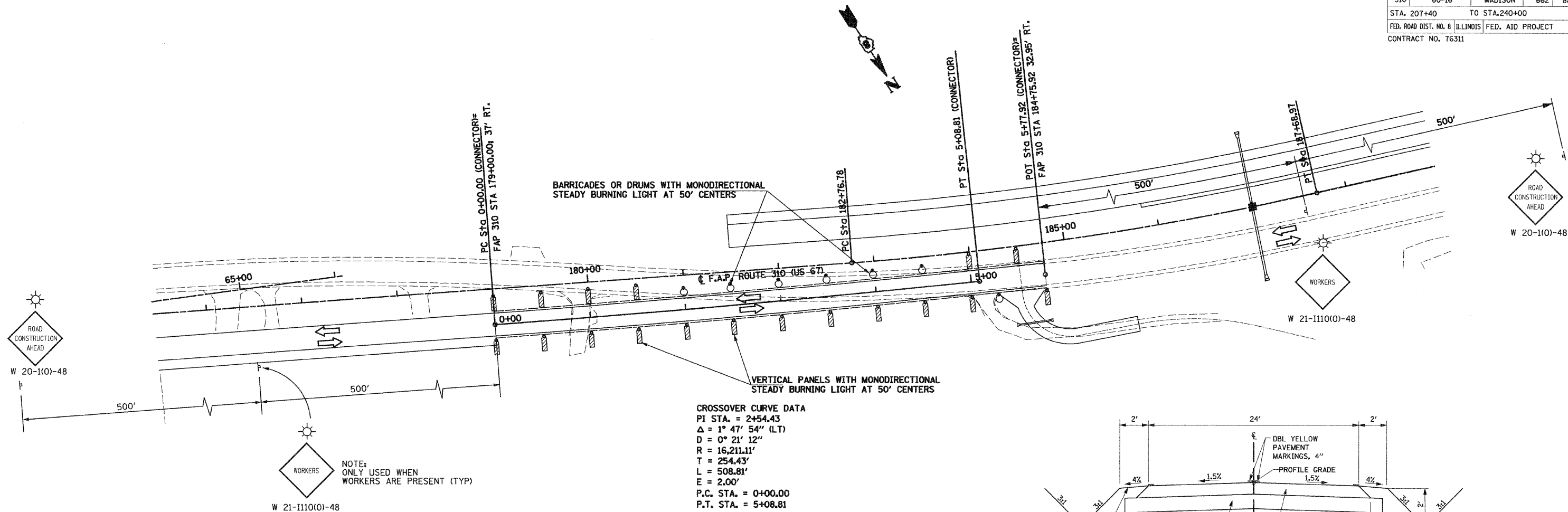
## STAGE 3 CONSTRUCTION

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 09-04-09

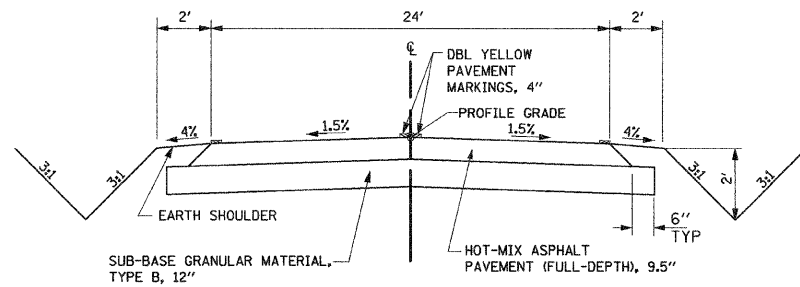
DRAWN BY: U.J.  
 CHECKED BY: J.J.C.



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	88
STA. 207+40		TO STA. 240+00		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				

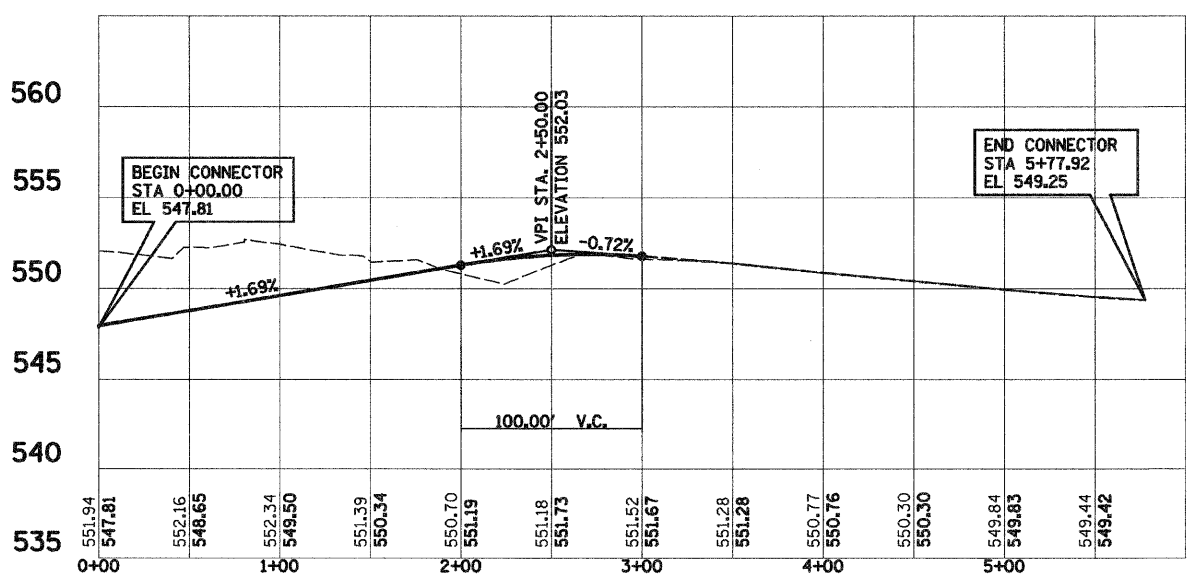


**CROSSOVER CURVE DATA**  
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 D = 0° 21' 12"  
 R = 16,211.11'  
 T = 254.43'  
 L = 508.81'  
 E = 2.00'  
 P.C. STA. = 0+00.00  
 P.T. STA. = 5+08.81

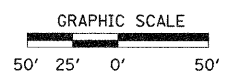


NOTE:  
 ONLY USED WHEN  
 WORKERS ARE PRESENT (TYP)

ADDITIONAL SIGNING AND TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION, SPECIAL I (SEE SHEET 94 FOR DETAILS)



- SYMBOLS**
- Sign
  - Barricade or drum with monodirectional steady burning light
  - Vertical panel with Monodirectional Steady Burning Lights
  - Type III barricade with Flashers
  - Flashing Light
  - Arrow Board
  - Directional Indicator Barricades



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

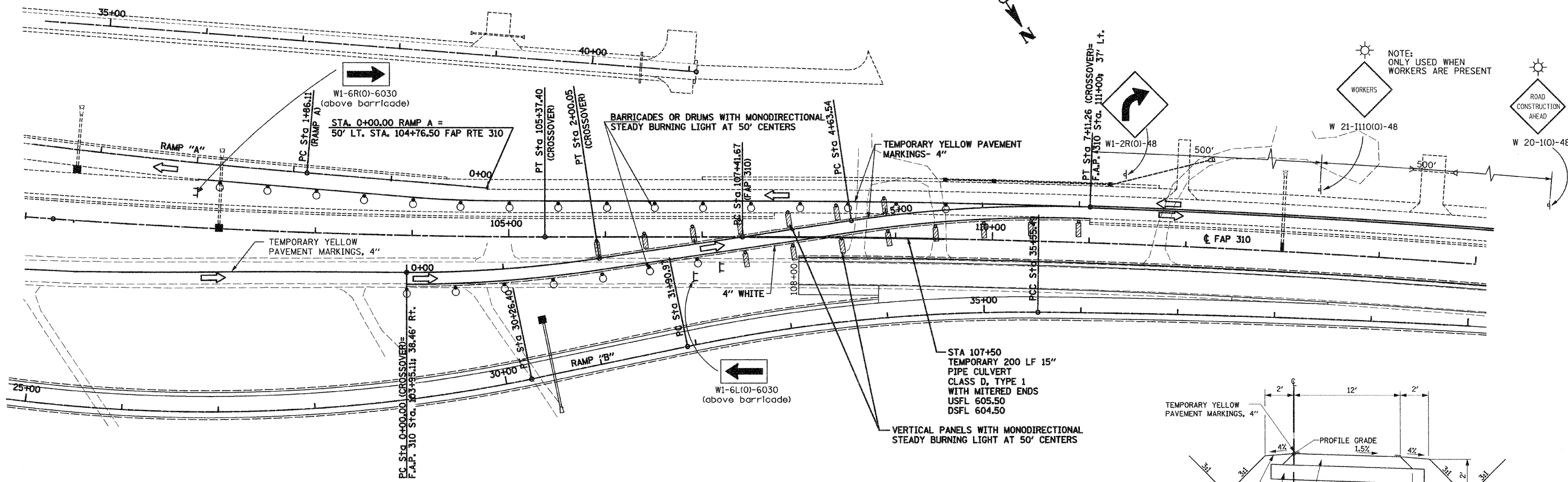
## CONNECTOR DETAIL NO. 2

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 09-04-09

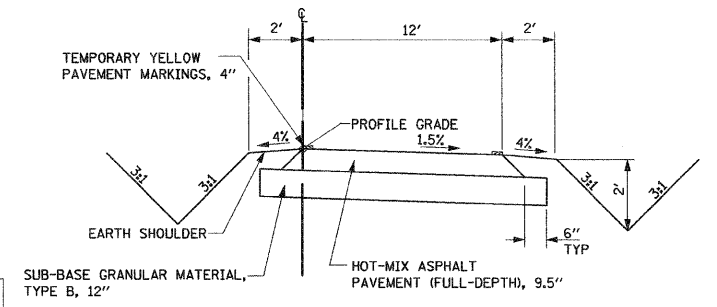
DRAWN BY: T.W.C.  
 CHECKED BY: J.J.C.

**CROSS OVER CURVE DATA**  
 PI STA. = 1+00.21  
 $\Delta = 8^\circ 29' 26''$  (LT)  
 $D = 4^\circ 14' 39''$   
 $R = 1,350.00'$   
 $T = 100.21'$   
 $L = 200.05'$   
 $E = 3.71'$   
 P.C. STA. = 0+00.00  
 P.T. STA. = 2+00.05

**CROSS OVER CURVE DATA**  
 PI STA. = 5+87.74  
 $\Delta = 10^\circ 30' 49''$  (RT)  
 $D = 4^\circ 14' 39''$   
 $R = 1,350.00'$   
 $T = 124.21'$   
 $L = 247.72'$   
 $E = 5.70'$   
 P.C. STA. = 4+63.54  
 P.T. STA. = 7+11.26

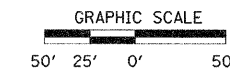
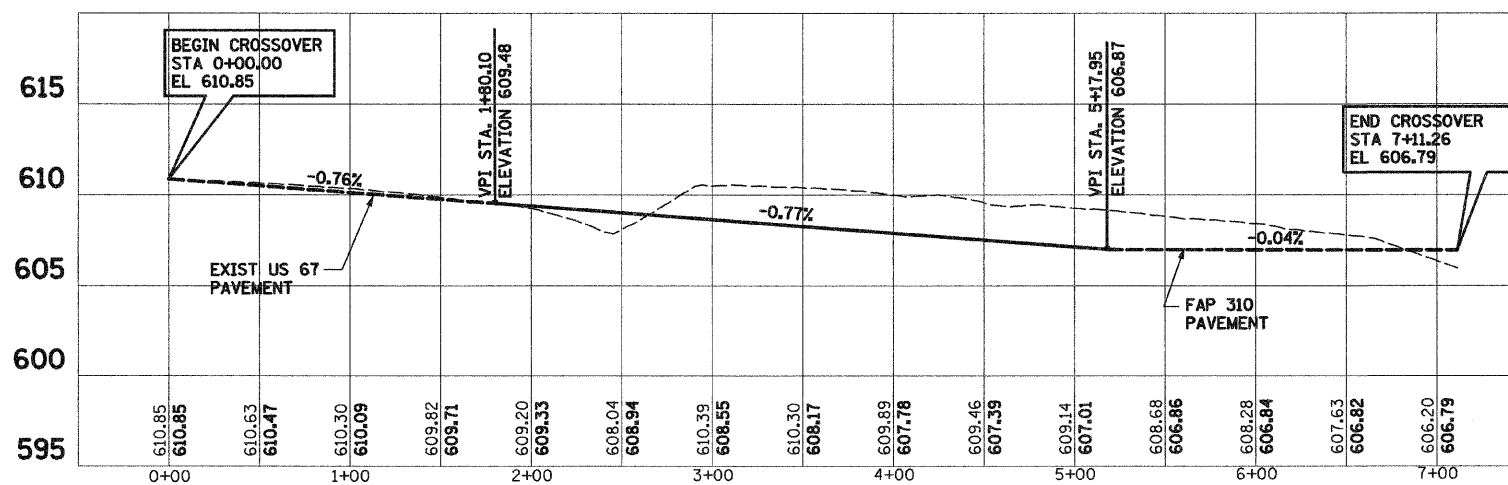


ADDITIONAL SIGNING AND TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION, SPECIAL I (SEE SHEET 94 FOR DETAILS)



TYPICAL SUPERELEVATED SECTION FOR CROSSOVER

- SYMBOLS**
- Sign
  - Barricade or drum with monodirectional steady burning light
  - Vertical panel with Monodirectional Steady Burning Lights
  - Type III barricade with Flashers
  - Flashing Light
  - Arrow Board
  - Directional Indicator Barricades



REVISIONS	
NAME	DATE

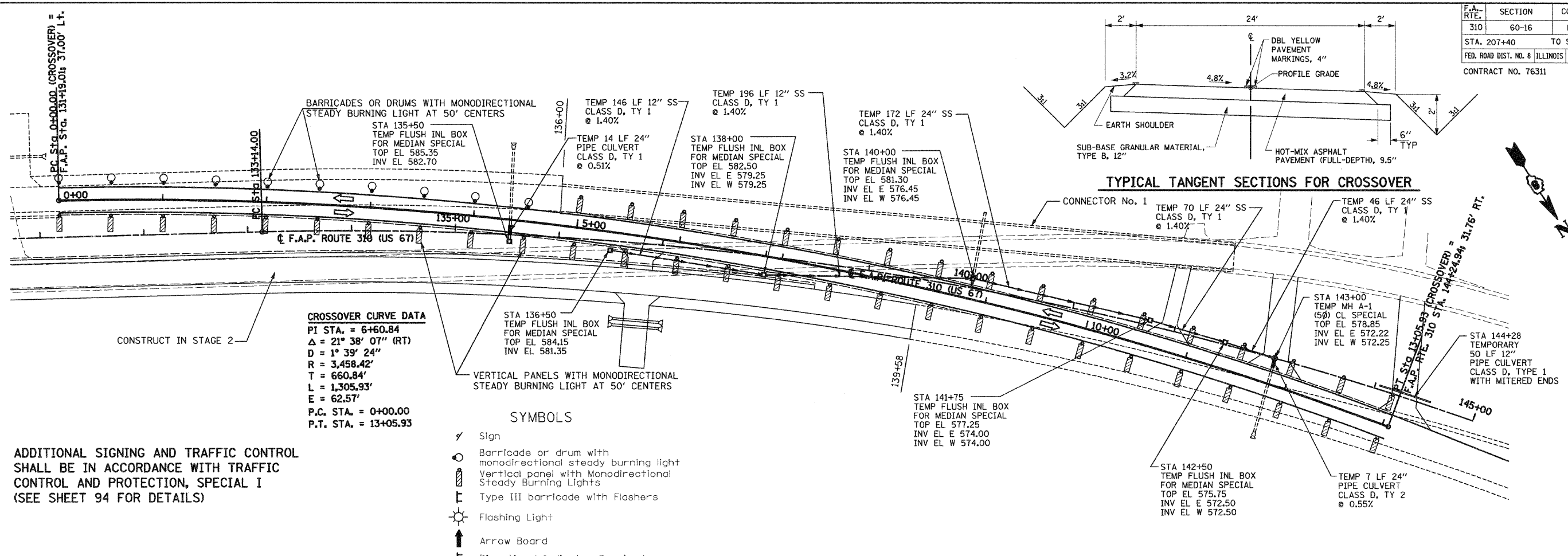
ILLINOIS DEPARTMENT OF TRANSPORTATION

# CROSS OVER DETAIL NO. 1

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 09-04-09

DRAWN BY: T.W.C.  
 CHECKED BY: J.J.C.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	90
STA. 207+40		TO STA. 240+00		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				



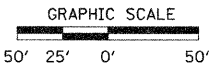
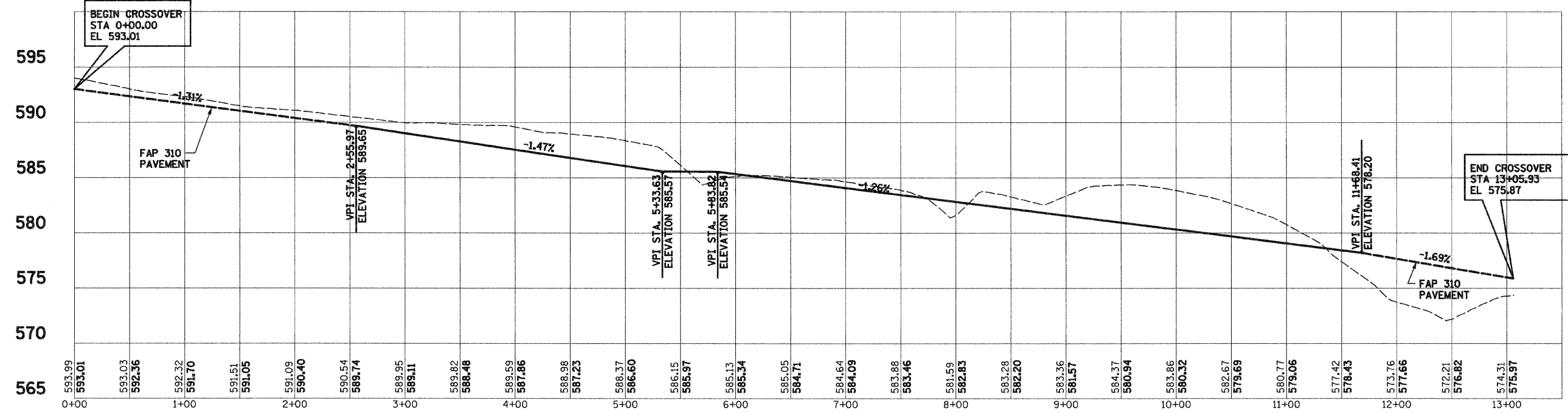
**CROSSOVER CURVE DATA**  
 PI STA. = 6+60.84  
 $\Delta = 21^\circ 38' 07''$  (RT)  
 $D = 1^\circ 39' 24''$   
 $R = 3,458.42'$   
 $T = 660.84'$   
 $L = 1,305.93'$   
 $E = 62.57'$   
 P.C. STA. = 0+00.00  
 P.T. STA. = 13+05.93

VERTICAL PANELS WITH MONODIRECTIONAL STEADY BURNING LIGHT AT 50' CENTERS

- SYMBOLS**
- Sign
  - Barricade or drum with monodirectional steady burning light
  - Vertical panel with Monodirectional Steady Burning Lights
  - Type III barricade with Flashers
  - Flashing Light
  - Arrow Board
  - Directional Indicator Barricades

ADDITIONAL SIGNING AND TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION, SPECIAL I (SEE SHEET 94 FOR DETAILS)

CONSTRUCT IN STAGE 2



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

# CROSS OVER DETAIL NO. 2

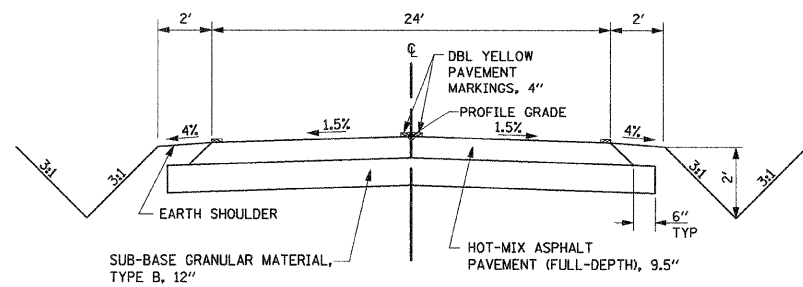
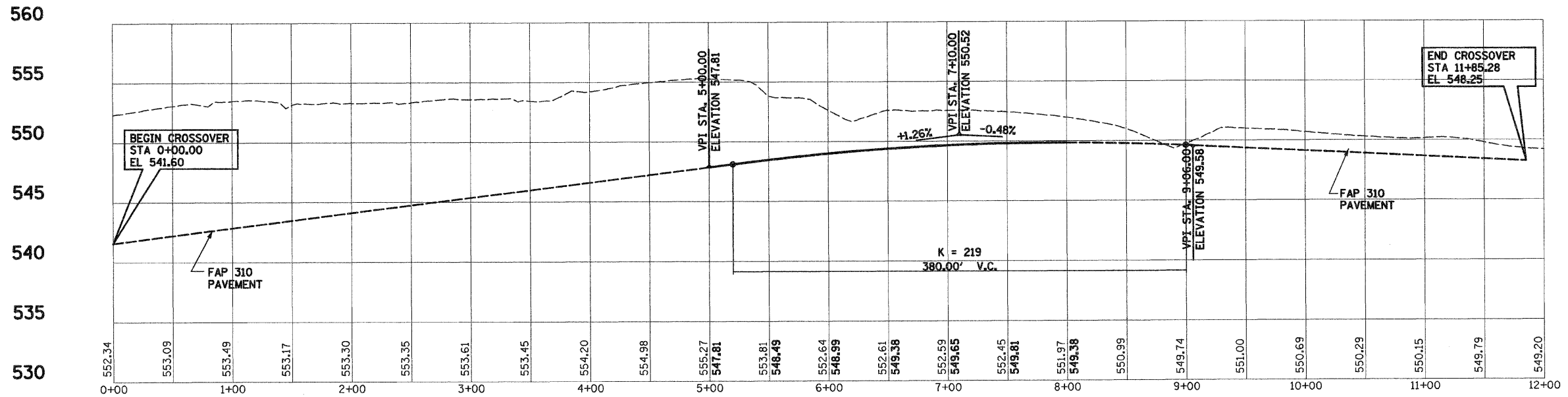
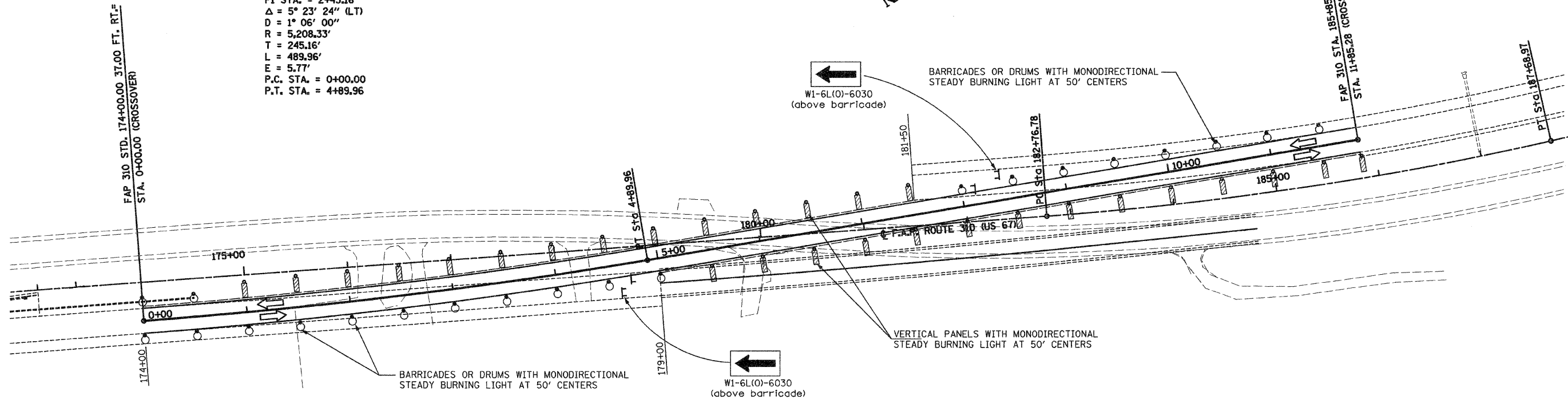
SCALE: VERT. HORIZ.  
DATE: 09-04-09

DRAWN BY: T.W.C.  
CHECKED BY: J.J.C.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	91
STA. 207+40		TO STA. 240+00		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				

**CROSSOVER CURVE DATA**  
 PI STA. = 2+45.16  
 $\Delta = 5^\circ 23' 24''$  (LT)  
 $D = 1^\circ 06' 00''$   
 $R = 5,208.33'$   
 $T = 245.16'$   
 $L = 489.96'$   
 $E = 5.77'$   
 P.C. STA. = 0+00.00  
 P.T. STA. = 4+89.96

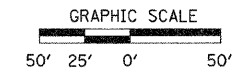
ADDITIONAL SIGNING AND TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION, SPECIAL I (SEE SHEET 94 FOR DETAILS)



**TYPICAL TANGENT SECTIONS FOR CROSSOVER**

**SYMBOLS**

- Sign
- Barricade or drum with monodirectional steady burning light
- Vertical panel with Monodirectional Steady Burning Lights
- Type III barricade with Flashers
- Flashing Light
- Arrow Board
- Directional Indicator Barricades



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**CROSS OVER  
DETAIL NO. 3**

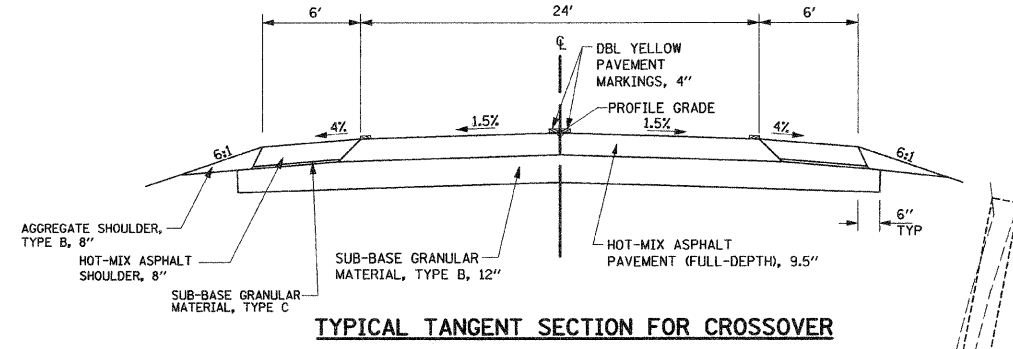
SCALE: VERT.  
HORIZ.  
DATE: 09-04-09

DRAWN BY: T.W.C.  
CHECKED BY: J.J.C.

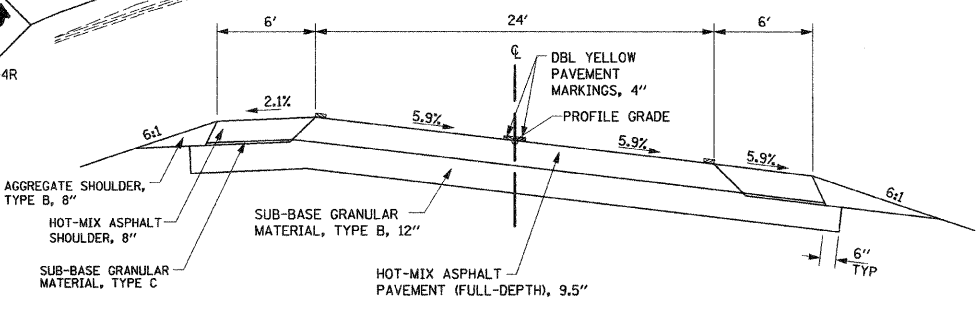
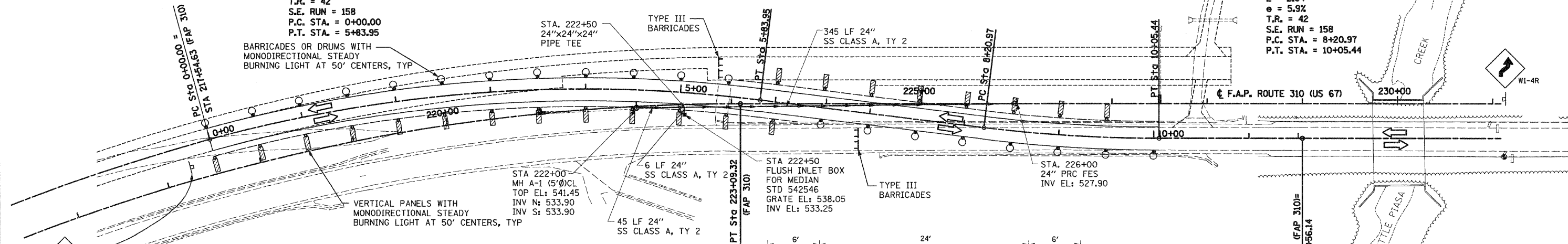
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	92
STA. 207+40		TO STA. 240+00		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				

**CROSS OVER CURVE DATA**  
 PI STA. = 2+95.72  
 $\Delta = 22^\circ 18' 19''$  (RT)  
 $D = 3^\circ 49' 11''$   
 $R = 1,500.00'$   
 $T = 295.72'$   
 $L = 583.95'$   
 $E = 28.87'$   
 $e = 5.9\%$   
 $T.R. = 42$   
 $S.E. RUN = 158$   
 $P.C. STA. = 0+00.00$   
 $P.T. STA. = 5+83.95$

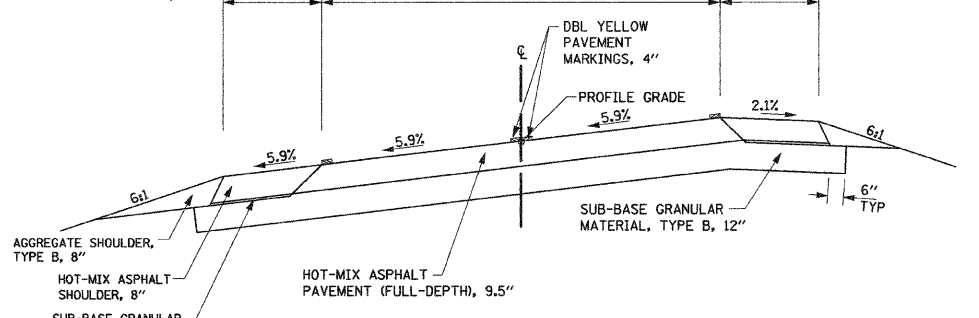
**CROSS OVER CURVE DATA**  
 PI STA. = 9+13.32  
 $\Delta = 7^\circ 02' 46''$  (LT)  
 $D = 3^\circ 49' 11''$   
 $R = 1,500.00'$   
 $T = 92.35'$   
 $L = 184.47'$   
 $E = 2.84'$   
 $e = 5.9\%$   
 $T.R. = 42$   
 $S.E. RUN = 158$   
 $P.C. STA. = 8+20.97$   
 $P.T. STA. = 10+05.44$



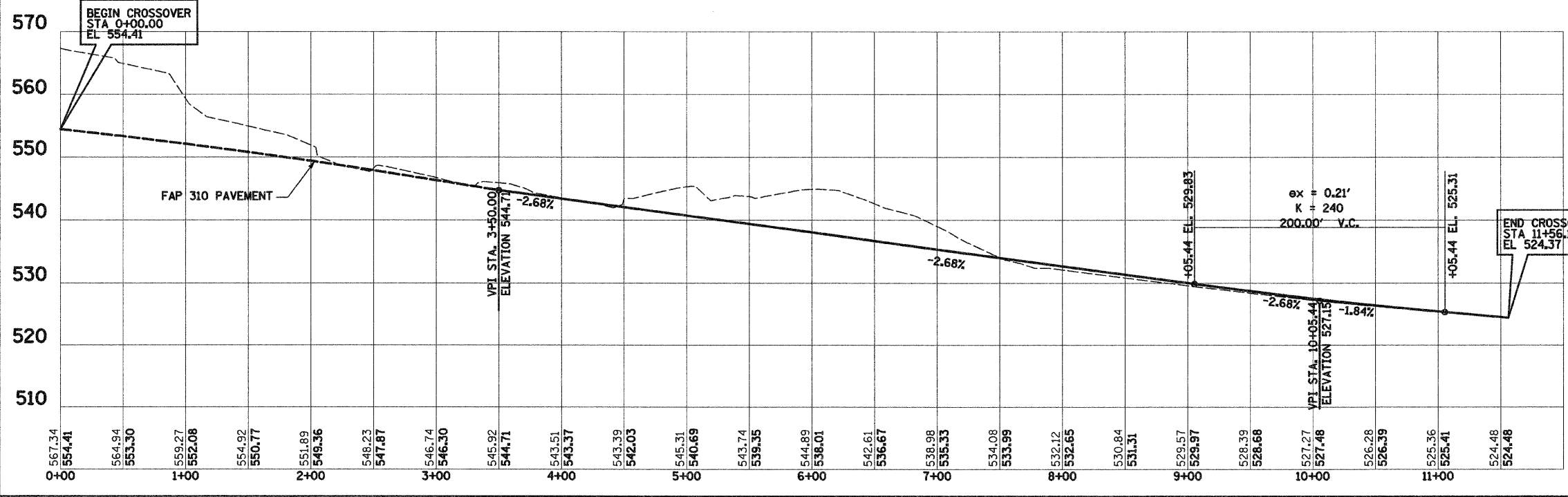
**TYPICAL TANGENT SECTION FOR CROSSOVER**



**TYPICAL SUPER ELEVATION SECTION FOR CROSSOVER**

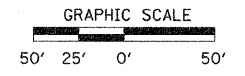


**TYPICAL SUPER ELEVATION SECTION FOR CROSSOVER**



ADDITIONAL SIGNING AND TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION, SPECIAL 1 (SEE SHEET 94 FOR DETAILS)

- ▲ Sign
- Barricade or drum with monodirectional steady burning light
- ▨ Vertical panel with Monodirectional Steady Burning Lights
- T Type III barricade with Flashers
- ⊙ Flashing Light
- ➔ Arrow Board
- TT Directional Indicator Barricades



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NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

# CROSS OVER DETAIL NO. 4 STAGE 2

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_

DATE : 09-04-09

DRAWN BY : T.W.C.  
 CHECKED BY : J.J.C.

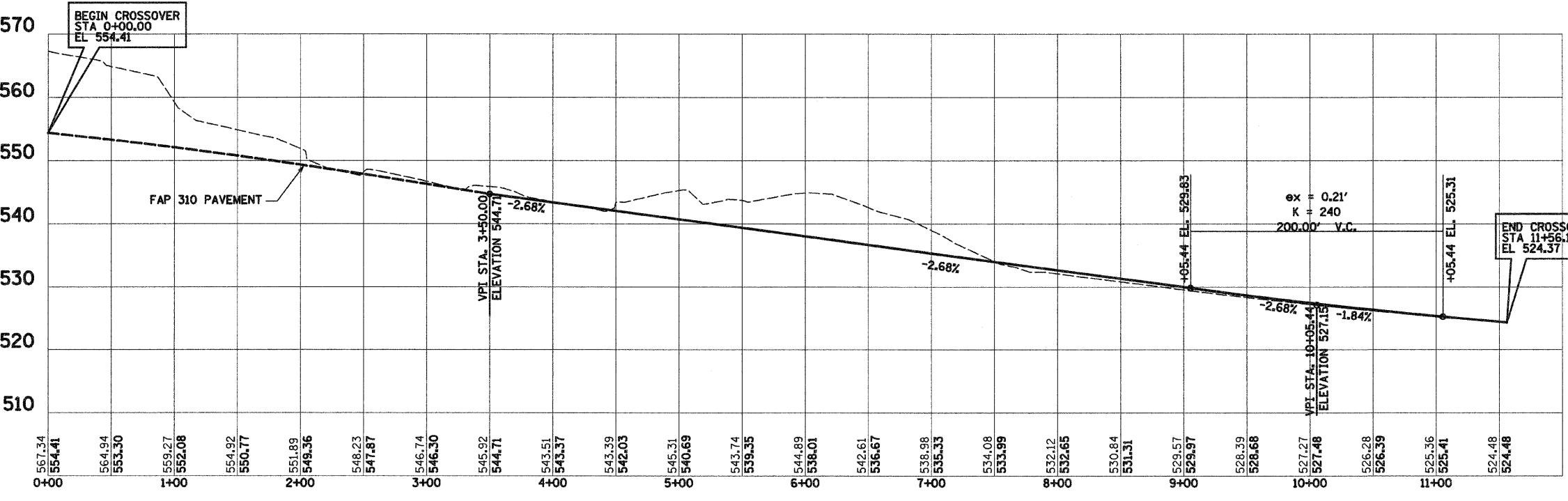
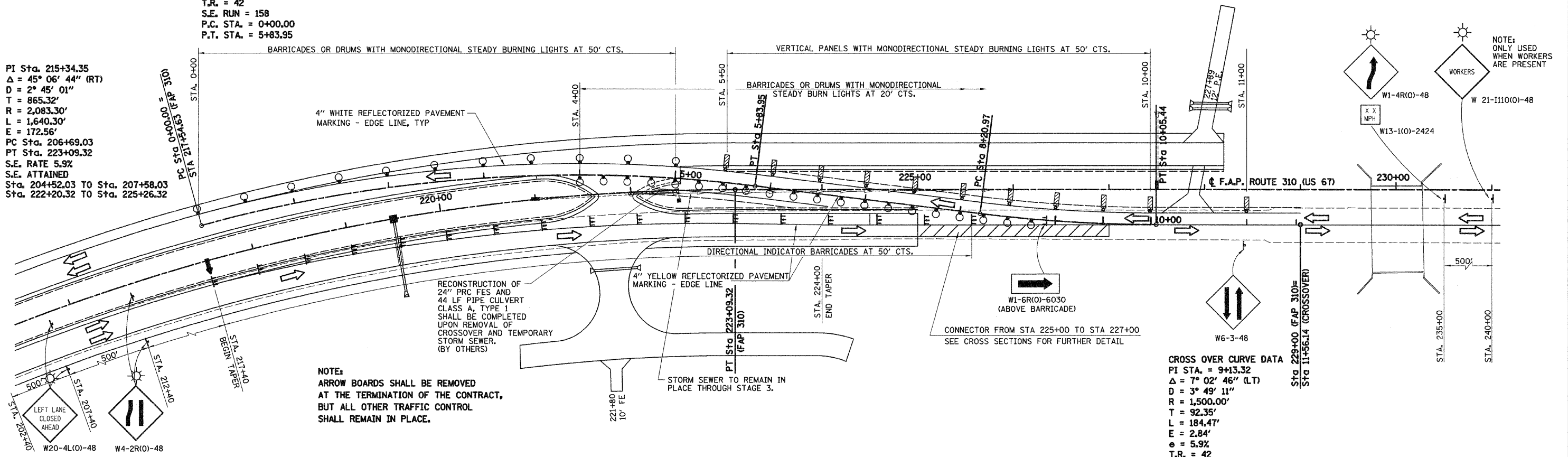
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	93

STA. 207+40 TO STA. 240+00  
 FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT  
 CONTRACT NO. 76311

**CROSS OVER CURVE DATA**  
 PI STA. = 2+95.72  
 $\Delta = 22^\circ 18' 19''$  (RT)  
 $D = 3^\circ 49' 11''$   
 $R = 1,500.00'$   
 $T = 295.72'$   
 $L = 583.95'$   
 $E = 28.87'$   
 $e = 5.9\%$   
 $T.R. = 42$   
 $S.E. RUN = 158$   
 P.C. STA. = 0+00.00  
 P.T. STA. = 5+83.95

PI Sta. 215+34.35  
 $\Delta = 45^\circ 06' 44''$  (RT)  
 $D = 2^\circ 45' 01''$   
 $T = 865.32'$   
 $R = 2,083.30'$   
 $L = 1,640.30'$   
 $E = 172.56'$   
 PC Sta. 206+69.03  
 PT Sta. 223+09.32  
 S.E. RATE 5.9%  
 S.E. ATTAINED  
 Sta. 204+52.03 TO Sta. 207+58.03  
 Sta. 222+20.32 TO Sta. 225+26.32

**CROSS OVER CURVE DATA**  
 PI STA. = 9+13.32  
 $\Delta = 7^\circ 02' 46''$  (LT)  
 $D = 3^\circ 49' 11''$   
 $R = 1,500.00'$   
 $T = 92.35'$   
 $L = 184.47'$   
 $E = 2.84'$   
 $e = 5.9\%$   
 $T.R. = 42$   
 $S.E. RUN = 158$   
 P.C. STA. = 8+20.97  
 P.T. STA. = 10+05.44



**TRAFFIC CONTROL AND PROTECTION (SPECIAL) LOCATION 2**

- ▲ Sign
- Barricade or drum with monodirectional steady burning light
- ▨ Vertical panel with Monodirectional Steady Burning Lights
- T Type III barricade with Flashers
- ⊙ Flashing Light
- ➔ Arrow Board
- ▬ Directional Indicator Barricades

REVISIONS	
NAME	DATE

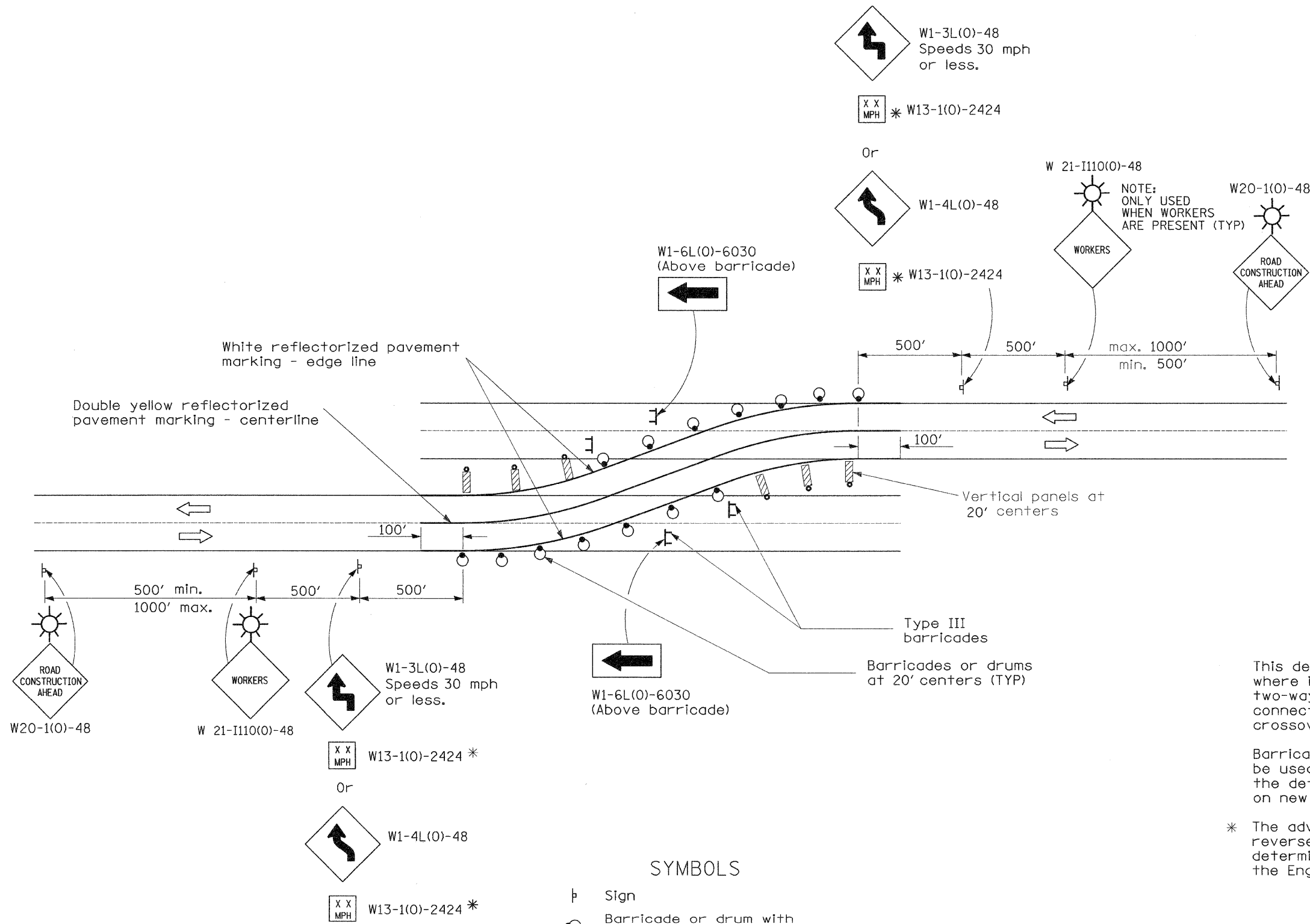
ILLINOIS DEPARTMENT OF TRANSPORTATION

# CROSS OVER DETAIL NO. 4

## STAGE 3

SCALE: VERT. DATE: 09-04-09  
 DRAWN BY: T.W.C.  
 CHECKED BY: J.J.C.





**GENERAL NOTES**

This detail is to be used at locations where it is necessary to shift two-lane, two-way traffic using a temporary connector or temporary median crossover.

Barricades or drums at 50' centers shall be used in lieu of vertical panels on the detour where they are to be placed on new or existing pavement.

\* The advisory speed to be shown below the reverse curve (turn) signs shall be determined at the site and approved by the Engineer.

**SYMBOLS**

- ⊥ Sign
- Barricade or drum with monodirectional steady burning light
- ▨ Vertical panel with Monodirectional Steady Burning Light
- ⊥ Type III barricade with Flashers
- ☀ Flashing Light

REVISIONS	
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ILLINOIS DEPARTMENT OF TRANSPORTATION

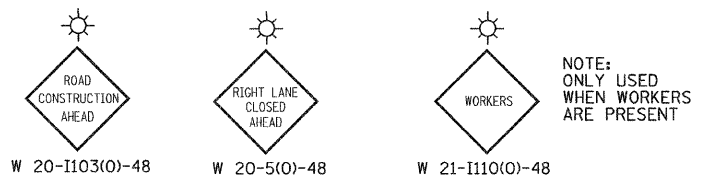
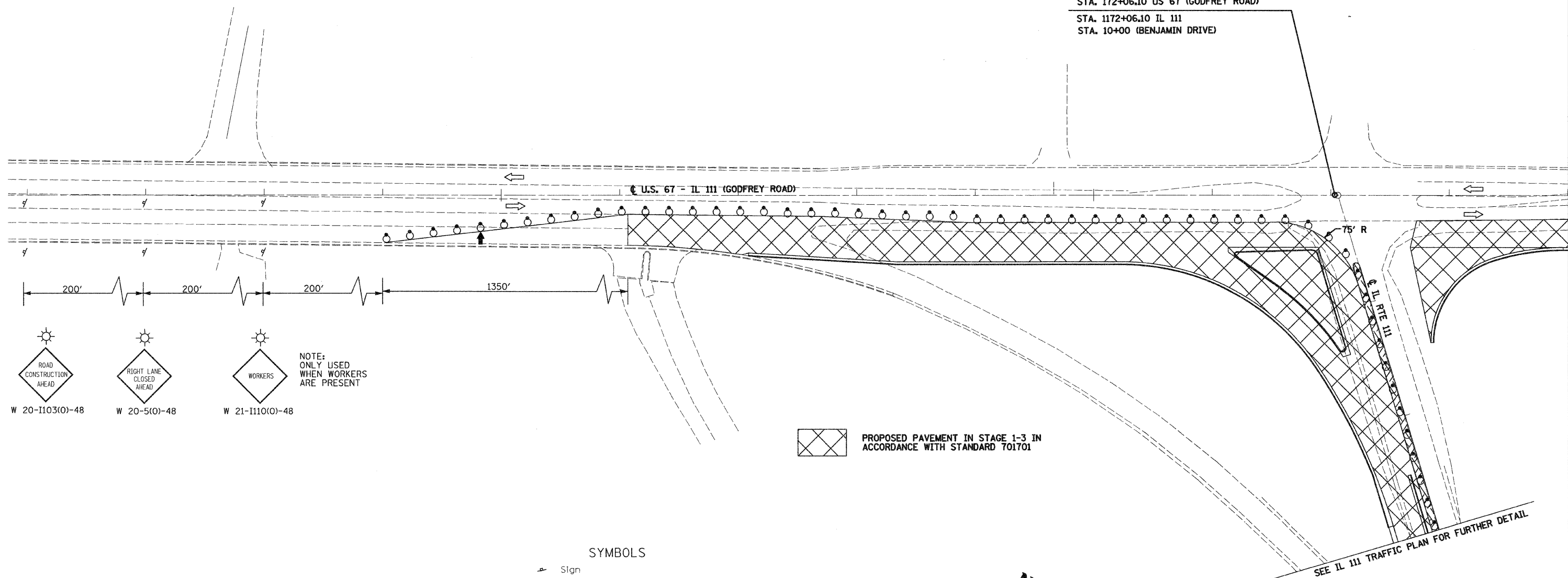
**TRAFFIC CONTROL AND PROTECTION (SPECIAL), LOCATION 1**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 09-04-09

DRAWN BY: T.W.C.  
 CHECKED BY: J.J.C.

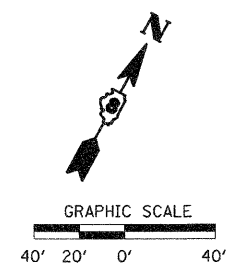
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	95
STA. 207+40		TO STA. 240+00		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				

STA. 172+06.10 US 67-IL 111 (GODFREY ROAD)  
 STA. 172+06.10 US 67 (GODFREY ROAD)  
 STA. 1172+06.10 IL 111  
 STA. 10+00 (BENJAMIN DRIVE)



PROPOSED PAVEMENT IN STAGE 1-3 IN ACCORDANCE WITH STANDARD 701701

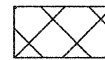
- SYMBOLS**
- Sign
  - Barricade or drum with monodirectional steady burning light
  - Vertical panel with Monodirectional Steady Burning Lights
  - Type III barricade with Flashers
  - Flashing Light
  - Arrow Board



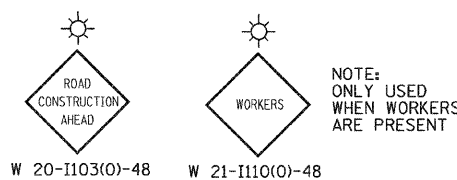
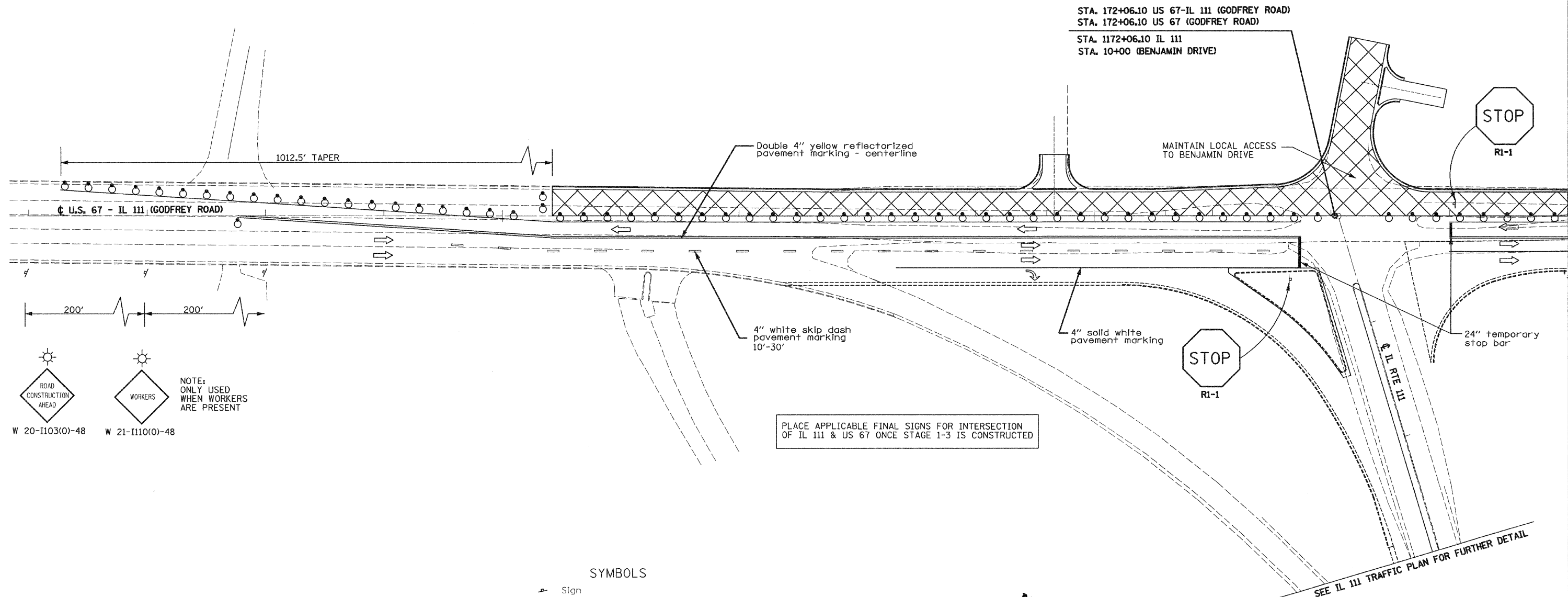
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**US 67 STAGE 1-3 TRAFFIC PLAN**  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 09-04-09  
 DRAWN BY: T.W.C.  
 CHECKED BY: J.J.C.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	96
STA. 207+40		TO STA. 240+00		
FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 76311				

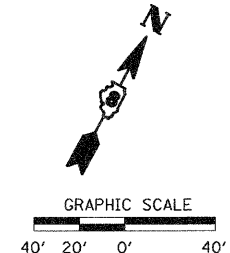


STAGE 1-5 IN ACCORDANCE WITH STANDARD 701701



PLACE APPLICABLE FINAL SIGNS FOR INTERSECTION OF IL 111 & US 67 ONCE STAGE 1-3 IS CONSTRUCTED

- SYMBOLS**
- ▲ Sign
  - Barricade or drum with monodirectional steady burning light
  - ▨ Vertical panel with Monodirectional Steady Burning Lights
  - ⊥ Type III barricade with Flashers
  - ⚡ Flashing Light
  - ➡ Arrow Board



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

## US 67 STAGE 1-5 TRAFFIC PLAN

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 09-04-09

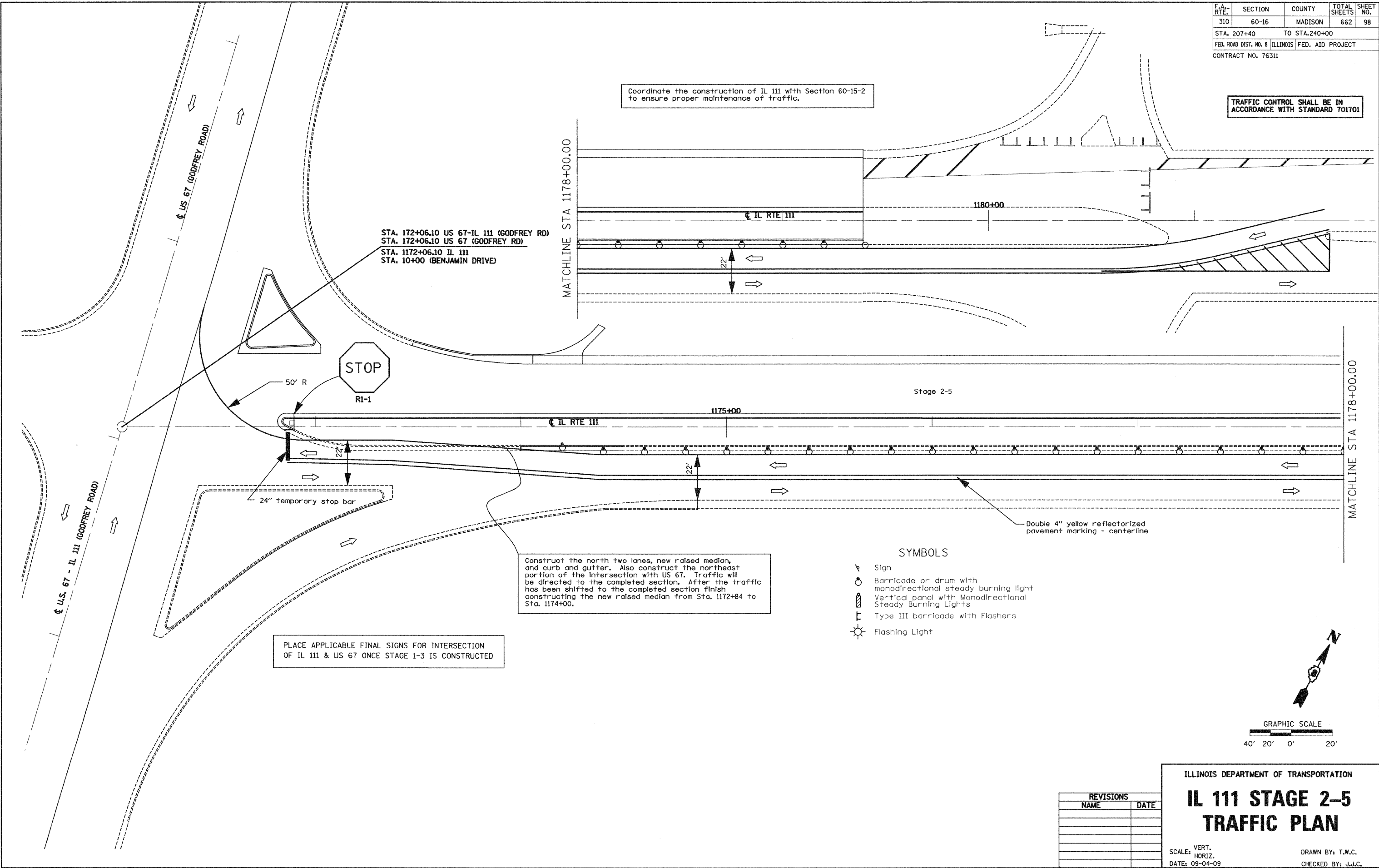
DRAWN BY: T.W.C.  
 CHECKED BY: J.J.C.



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	60-16	MADISON	662	98
STA. 207+40		TO STA. 240+00		
FED. ROAD DIST. NO. 8		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 76311				

Coordinate the construction of IL 111 with Section 60-15-2 to ensure proper maintenance of traffic.

TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH STANDARD 701701



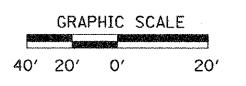
STA. 172+06.10 US 67-IL 111 (GODFREY RD)  
 STA. 172+06.10 US 67 (GODFREY RD)  
 STA. 1172+06.10 IL 111  
 STA. 10+00 (BENJAMIN DRIVE)

STOP  
R1-1

PLACE APPLICABLE FINAL SIGNS FOR INTERSECTION OF IL 111 & US 67 ONCE STAGE 1-3 IS CONSTRUCTED

Construct the north two lanes, new raised median, and curb and gutter. Also construct the northeast portion of the intersection with US 67. Traffic will be directed to the completed section. After the traffic has been shifted to the completed section finish constructing the new raised median from Sta. 1172+84 to Sta. 1174+00.

- SYMBOLS**
- Sign
  - Barricade or drum with monidirectional steady burning light
  - Vertical panel with Monidirectional Steady Burning Lights
  - Type III barricade with Flashers
  - Flashing Light



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

## IL 111 STAGE 2-5 TRAFFIC PLAN

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 09-04-09

DRAWN BY: T.W.C.  
 CHECKED BY: J.J.C.



