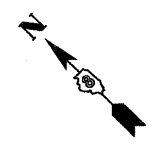
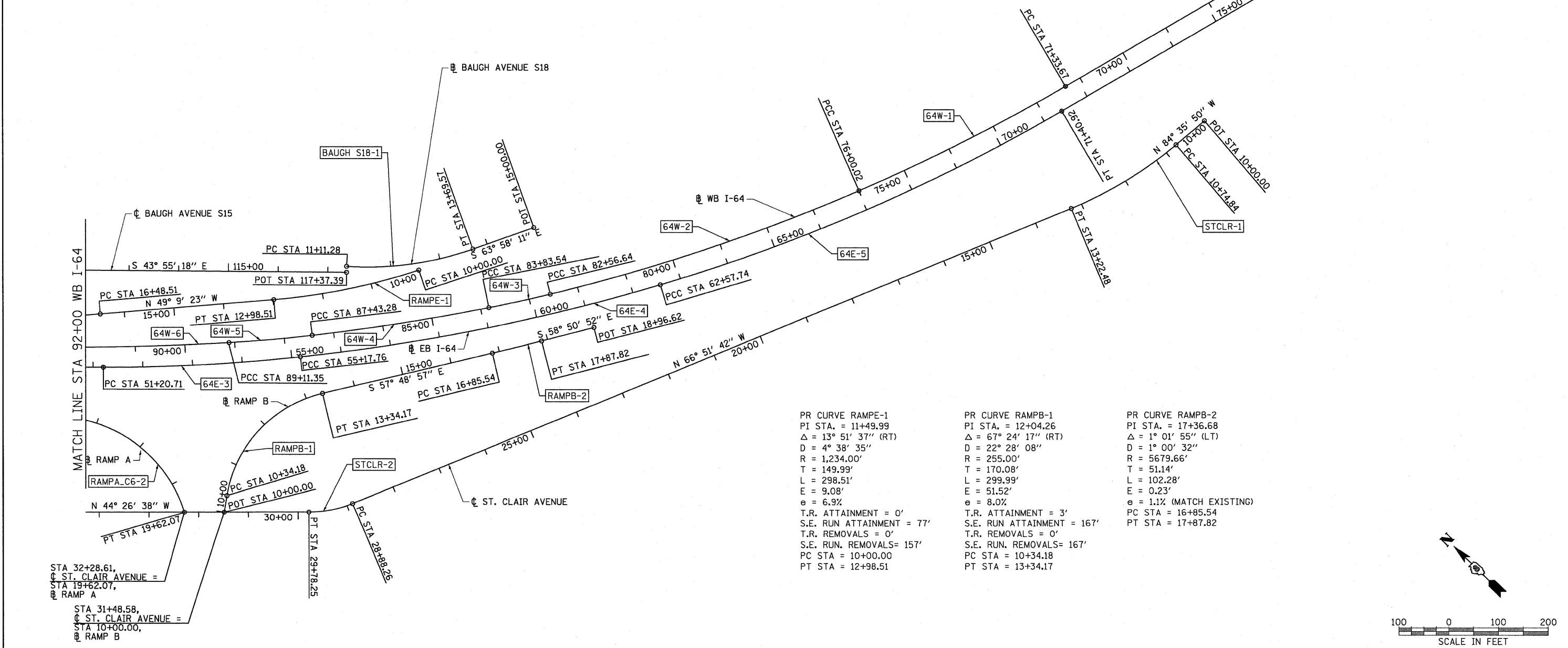


PR CURVE ST_CLR-1 PI STA. = 11+99.66 Δ = 17° 44' 08" (RT) D = 7° 09' 43" R = 800.00' T = 124.82' L = 247.64' E = 9.68' PC STA = 10+74.84 PT STA = 13+22.48	PR CURVE ST_CLR-2 PI STA. = 29+33.84 Δ = 22° 25' 05" (RT) D = 24° 54' 40" R = 230.00' T = 45.58' L = 89.99' E = 4.47' PC STA = 28+88.26 PT STA = 29+78.25	PR CURVE 64E-3 PI STA. = 53+19.44 Δ = 6° 19' 10" (LT) D = 1° 35' 30" R = 3,600.00' T = 198.73' L = 397.06' E = 5.48' PC STA = 51+20.71 PCC STA = 55+17.76	PR CURVE 64E-4 PI STA. = 58+88.81 Δ = 10° 35' 58" (LT) D = 1° 25' 57" R = 4,000.00' T = 371.05' L = 739.97' E = 17.17' PCC STA = 55+17.76 PCC STA = 62+57.74	PR CURVE 64E-5 PI STA. = 67+01.39 Δ = 13° 31' 16" (LT) D = 1° 31' 51" R = 3,742.50' T = 443.65' L = 883.18' E = 26.20' PCC STA = 62+57.74 PT STA = 71+40.92	PR CURVE BAUGH_S18-1 PI STA. = 12+42.25 Δ = 23° 23' 50" (LT) D = 9° 03' 31" R = 632.49' T = 130.97' L = 258.29' E = 13.42' PC STA = 11+11.28 PT STA = 13+69.57	PR CURVE 64W-1 PI STA. = 73+67.12 Δ = 6° 45' 48" (RT) D = 1° 27' 01" R = 3,950.66' T = 233.44' L = 466.34' E = 6.89' PC STA = 71+33.67 PCC STA = 76+00.02	PR CURVE 64W-2 PI STA. = 79+29.11 Δ = 9° 39' 54" (RT) D = 1° 28' 19" R = 3,892.58' T = 329.09' L = 656.62' E = 13.89' PCC STA = 76+00.02 PCC STA = 82+56.64	PR CURVE 64W-3 PI STA. = 83+20.10 Δ = 2° 30' 26" (RT) D = 1° 58' 33" R = 2,900.00' T = 63.46' L = 126.91' E = 0.69' PCC STA = 82+56.64 PCC STA = 83+83.54			
PR CURVE 64W-4 PI STA. = 85+63.51 Δ = 4° 34' 49" (RT) D = 1° 16' 24" R = 4,500.00' T = 179.97' L = 359.74' E = 3.60' PCC STA = 83+83.54 PCC STA = 87+43.28	PR CURVE 64W-5 PI STA. = 88+27.34 Δ = 3° 30' 05" (RT) D = 2° 05' 01" R = 2,750.00' T = 84.06' L = 168.06' E = 1.28' PCC STA = 87+43.28 PCC STA = 89+11.35	PR CURVE 64W-6 PI STA. = 90+75.92 Δ = 3° 21' 59" (RT) D = 1° 01' 23" R = 5,600.16' T = 164.57' L = 329.05' E = 2.42' PCC STA = 89+11.35 PT STA = 92+40.39							PR CURVE RAMPE-1 PI STA. = 11+49.99 Δ = 13° 51' 37" (RT) D = 4° 38' 35" R = 1,234.00' T = 149.99' L = 298.51' E = 9.08' e = 6.9% T.R. ATTAINMENT = 0' S.E. RUN ATTAINMENT = 77' T.R. REMOVALS = 0' S.E. RUN. REMOVALS = 157' PC STA = 10+00.00 PT STA = 12+98.51	PR CURVE RAMPB-1 PI STA. = 12+04.26 Δ = 67° 24' 17" (RT) D = 22° 28' 08" R = 255.00' T = 170.08' L = 299.99' E = 51.52' e = 8.0% T.R. ATTAINMENT = 3' S.E. RUN ATTAINMENT = 167' T.R. REMOVALS = 0' S.E. RUN. REMOVALS = 167' PC STA = 10+34.18 PT STA = 13+34.17	PR CURVE RAMPB-2 PI STA. = 17+36.68 Δ = 1° 01' 55" (LT) D = 1° 00' 32" R = 5679.66' T = 51.14' L = 102.28' E = 0.23' e = 1.1% (MATCH EXISTING) PC STA = 16+85.54 PT STA = 17+87.82



FILE NAME =	USER NAME = IDOT	DESIGNED - TTB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ALIGNMENT, TIES AND BENCHMARKS</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*FILE#	PLOT SCALE = 100.0000' / IN.	DRAWN - TTB	REVISED -					64	82-1-2HB	ST. CLAIR	345	38
	PLOT DATE = 3/17/2010	CHECKED - JAH	REVISED -		SCALE: 1"=100'			SHEET NO. 2 OF 8 SHEETS			CONTRACT NO. 76C49	
		DATE = 3/19/2010	REVISED -		STA. TO STA.			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				