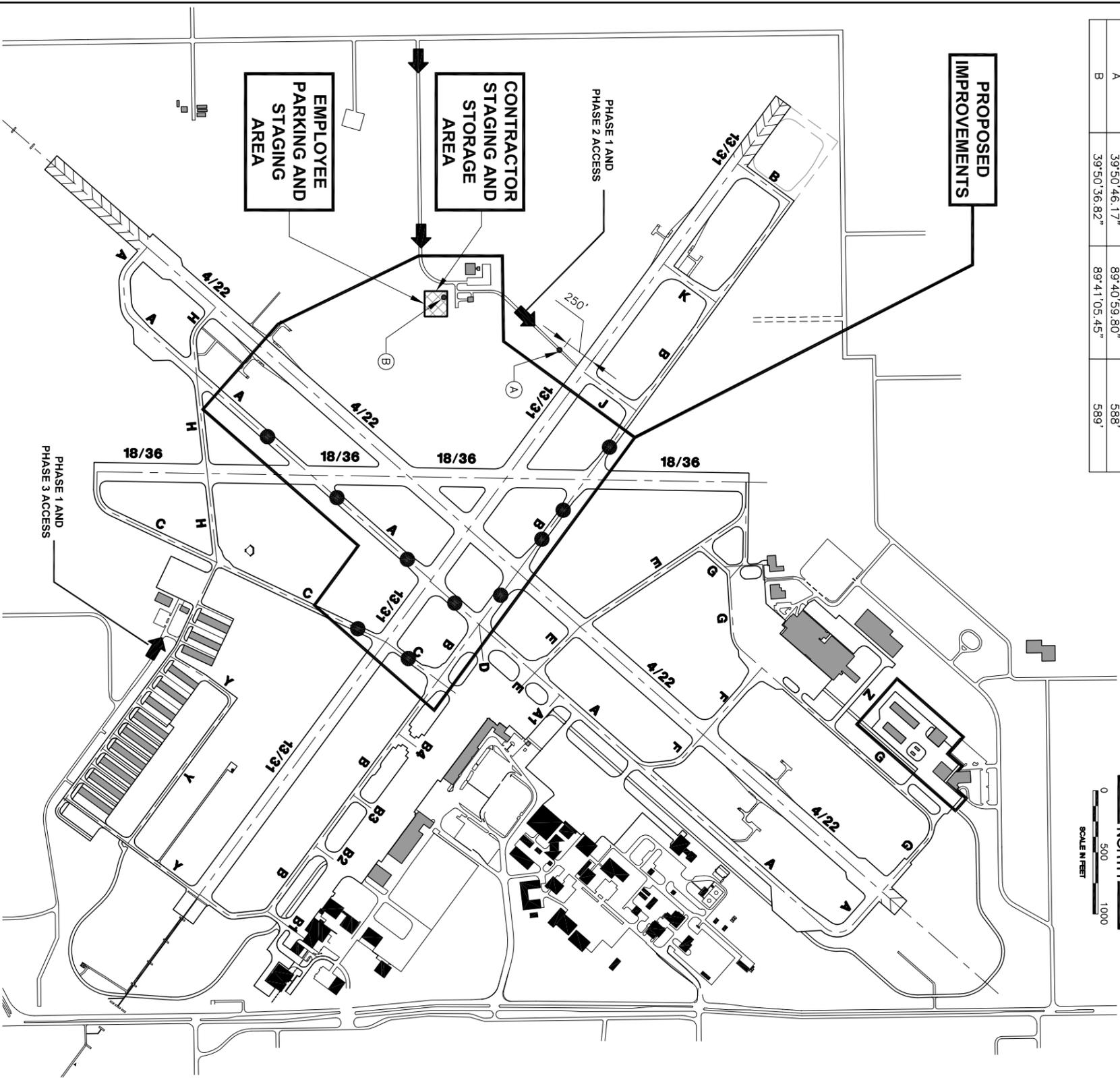
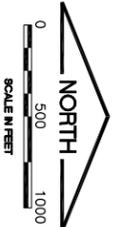


GROUND CONTROL FREQUENCY - 121.90
 AIR CONTROL FREQUENCY - 121.30
 MAXIMUM HEIGHT OF EQUIPMENT - 25'

CRITICAL POINTS			
POINT	LATITUDE	LONGITUDE	ELEVATION
A	39°50'46.17"	89°40'59.80"	588'
B	39°50'36.82"	89°41'05.45"	589'

- LEGEND**
- NEW RUNWAY GUARD LIGHT (RGL) INSTALLATION
 - ➔ CONTRACTOR'S ACCESS



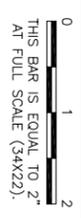
GENERAL NOTES

1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED SAA/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT DIRECTOR.
4. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
5. BROKEN ASPHALT OR CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
6. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 95' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS WITHOUT AIR TRAFFIC CONTROL TOWER (ATCT) APPROVAL.
7. ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
8. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
9. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC.
10. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
11. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - B. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
 - C. THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER, S.A.A. MAINTENANCE SUPERVISOR, AND S.A.A. SECURITY CHIEF. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE RESIDENT ENGINEER.
 - D. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACTOR. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
 - G. THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES.

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SPRINGFIELD, ILLINOIS

INSTALL RUNWAY GUARD LIGHTS (RGL)

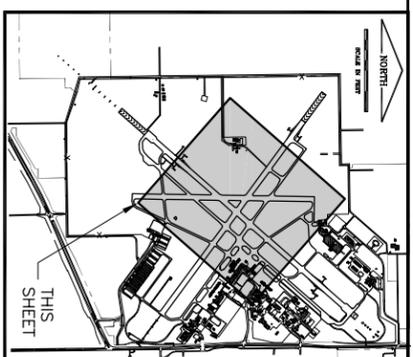
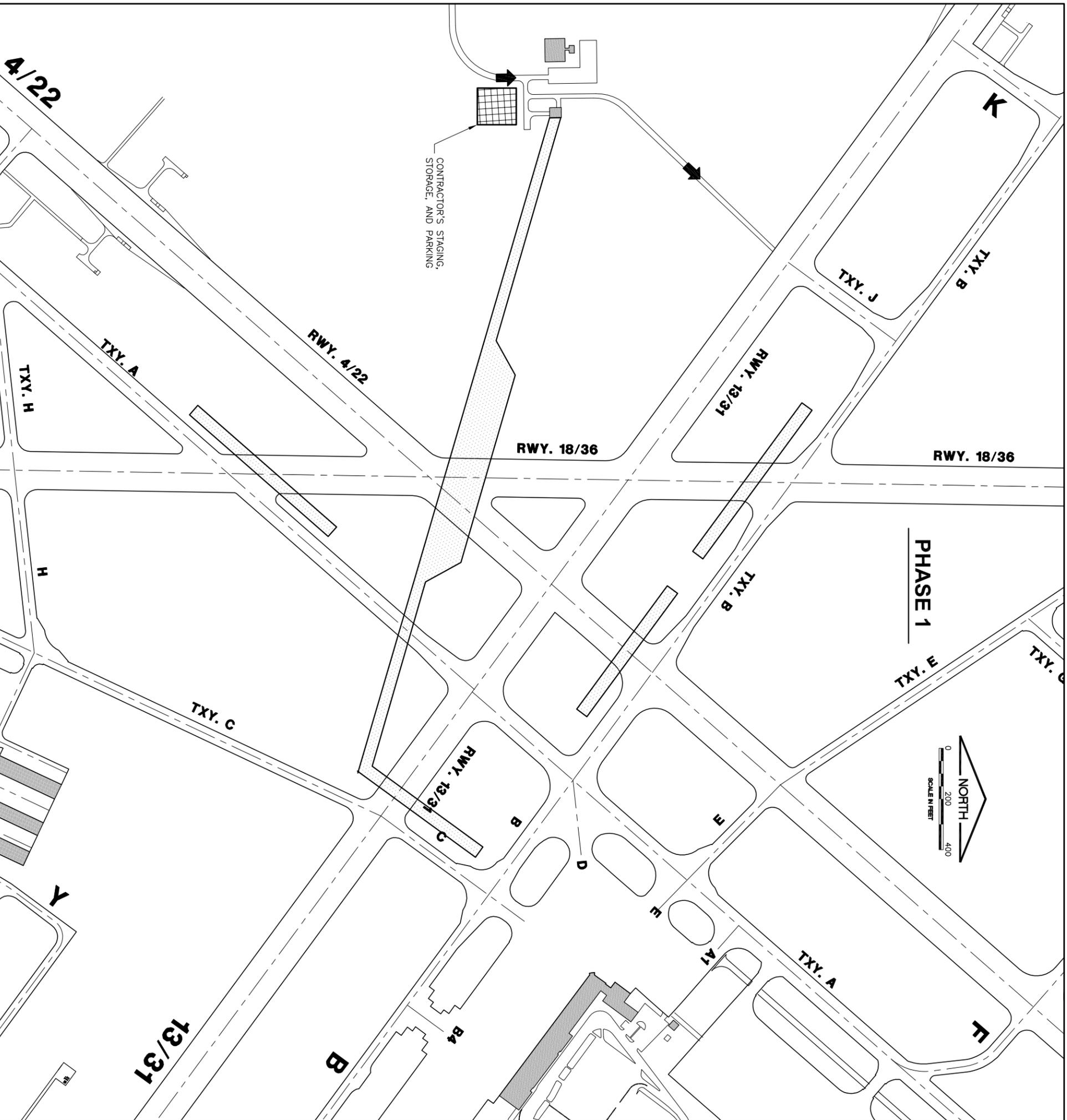
AIRPORT SITE PLAN

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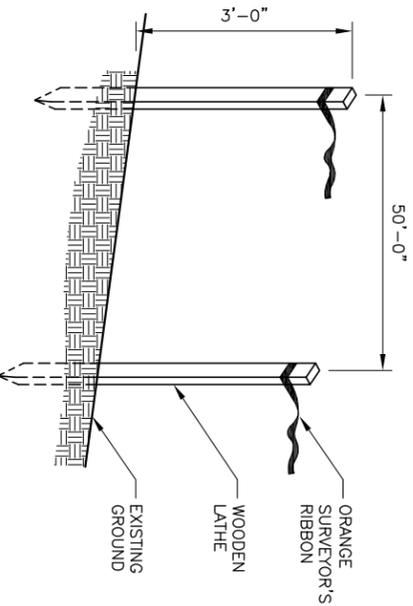


LEGEND

- WORK LIMITS
- CONTRACTOR'S STAGING AND STORAGE
- CONSTRUCTION SET BACK LINE
- CONTRACTOR ACCESS
- FLAGMAN - IN CONTACT WITH AIR TRAFFIC CONTROL TOWER (ATCT)
- AIR TRAFFIC ACCESS
- CONTRACTOR AND AIRCRAFT ACCESS

CONSTRUCTION ACTIVITY PLAN PHASING NOTES - PHASE 1

1. THE AIRPORT WILL REQUIRE 3 CALENDAR DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING THIS PHASE.
2. PHASE 1 WORK SHALL INCLUDE THE INSTALLATION OF THE DUCTS AND CABLE RUNS UNDER THE RUNWAYS.
3. IN ORDER TO COMPLETE THE WORK IN THIS PHASE, THE CONTRACTOR SHALL OBTAIN A SECURITY BADGE AND SHALL MAINTAIN CONSTANT COMMUNICATIONS WITH THE TOWER.
4. THE WORK SHALL BE COMPLETED SUCH THAT ONE (1) RUNWAY REMAINS OPEN AT ALL TIMES. WORK ON RUNWAY 13/31 AND RUNWAY 4/22 MAY NOT BE COMPLETED AT THE SAME TIME.
5. THE CONTRACTOR SHALL PROMINENTLY MARK OPEN TRENCHES AND EXCAVATIONS AT THE CONSTRUCTION SITE, AS APPROVED BY THE AIRPORT OPERATOR, AND LIGHT THEM WITH RED LIGHTS DURING HOURS OF VISIBILITY OR DARKNESS.
6. THE CONTRACTOR SHALL COMPLETE THE DIRECTIONAL BORE, MARK, AND CLEAN THE AREAS AS REQUIRED BY THE AIRPORT OR RESIDENT ENGINEER, PRIOR TO INITIATING WORK AT THE NEXT LOCATION.



CONSTRUCTION SETBACK LINE DETAIL

N.T.S.

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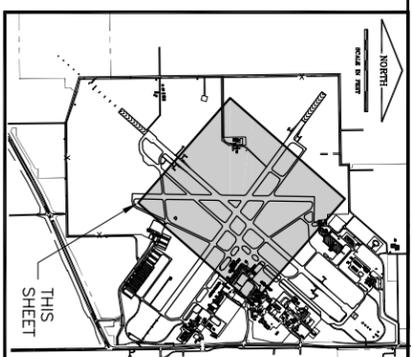
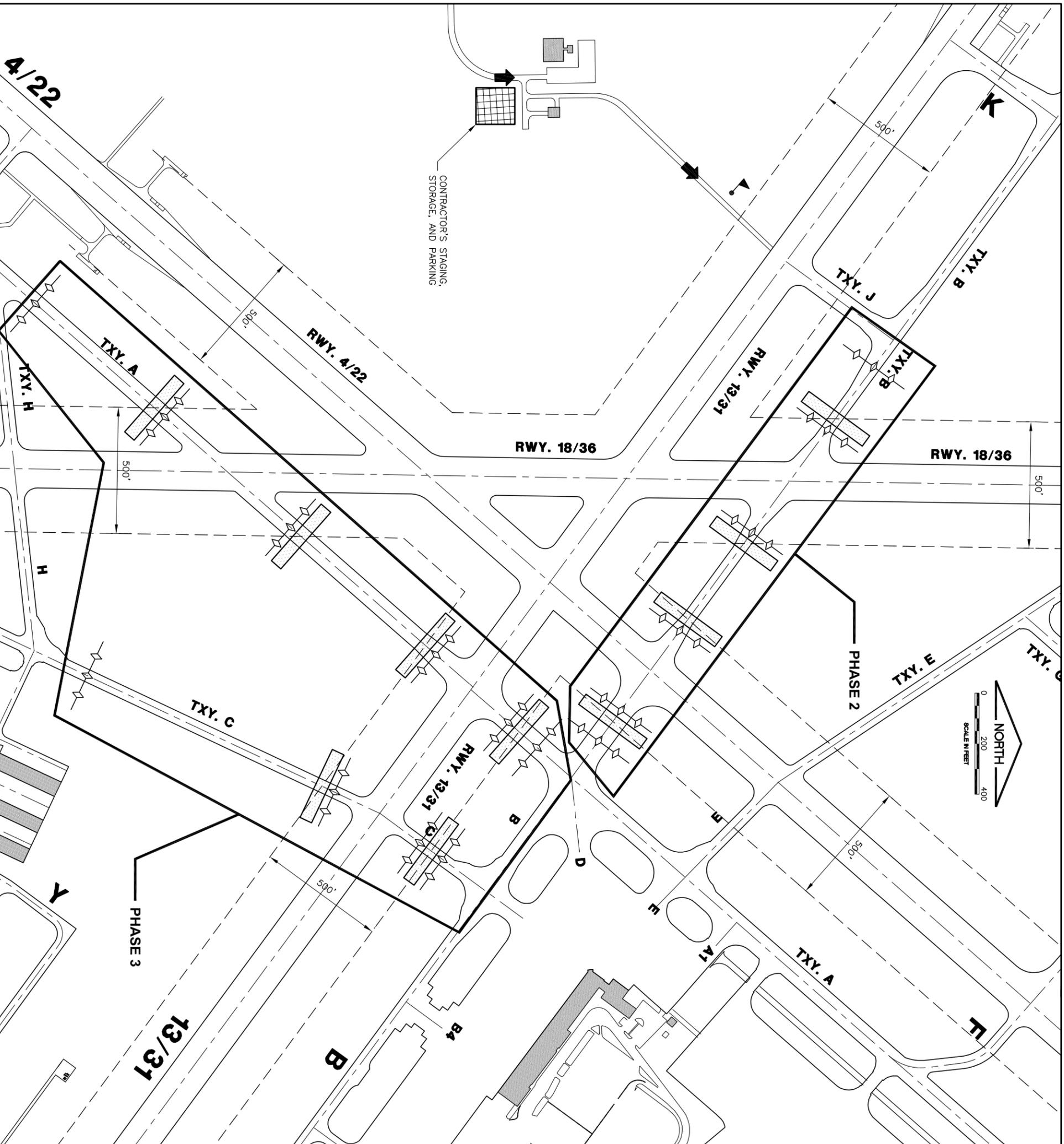
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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

**INSTALL RUNWAY GUARD LIGHTS (RGL)
 CONSTRUCTION ACTIVITY PLAN 1**

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LEGEND

- WORK LIMITS
- CONTRACTOR'S STAGING AND STORAGE
- CONTRACTOR ACCESS
- FLAG MAN - IN CONTACT WITH AIR TRAFFIC CONTROL TOWER (ATCT)
- AIR TRAFFIC ACCESS
- CONTRACTOR AND AIRCRAFT ACCESS
- RUNWAY SAFETY AREA/CONSTRUCT SET BACK LINE
- BARRICADES: TYPE 1 WITH RED LIGHTS AND 20" x 20" ORANGE FLAGS AT 10' SPACING

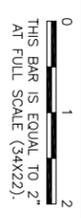
CONSTRUCTION ACTIVITY PLAN PHASING NOTES - PHASES 2 & 3

1. THE AIRPORT WILL REQUIRE 3 CALENDAR DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING THIS PHASE.
2. PHASE 2 SHALL INCLUDE THE INSTALLATION OF THE RUNWAY GUARD LIGHTS AND THE DIRECTIONAL BORE UNDER THE TAXIWAYS.
3. IN ORDER TO PERFORM THE WORK IN THIS PHASE, THE CONTRACTOR SHALL OBTAIN A SECURITY BADGE AND SHALL MAINTAIN CONSTANT COMMUNICATION WITH THE TOWER.
4. NO RUNWAYS SHALL BE CLOSED DURING THIS PHASE. HOWEVER, THE CONTRACTOR SHALL CLOSELY COMMUNICATE WITH AND FOLLOW THE DIRECTION OF THE TOWER WHEN WORKING NEAR THE RUNWAY SAFETY AREA.
5. CONSTRUCTION BARRICADES SHALL BE PLACED 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS AND 95' FROM THE CENTERLINE OF ACTIVE TAXIWAYS AT THE LOCATIONS SHOWN ON THIS SHEET WHEN WORK IS BEING PERFORMED.
6. THE CONTRACTOR SHALL PROMINENTLY MARK OPEN TRENCHES AND EXCAVATIONS AT THE CONSTRUCTION SITE, AS APPROVED BY THE AIRPORT OPERATOR, AND LIGHT THEM WITH RED LIGHTS DURING HOURS OF VISIBILITY OR DARKNESS.
7. TAXIWAYS SHALL BE REOPENED AT THE END OF EACH WORK DAY.

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**INSTALL RUNWAY GUARD LIGHTS (RGL)
 CONSTRUCTION ACTIVITY PLAN 2**

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4/22

NOTES:

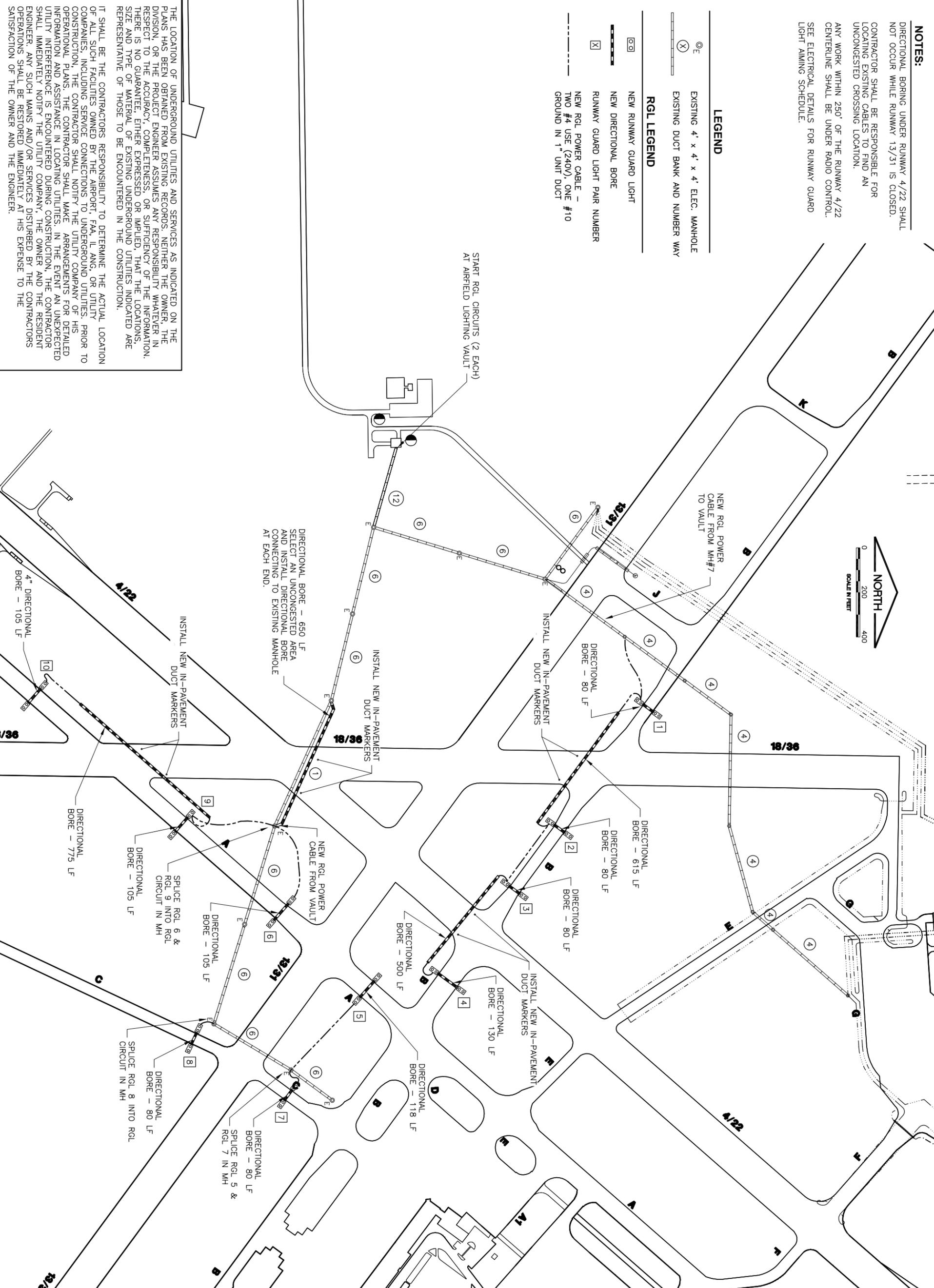
DIRECTIONAL BORING UNDER RUNWAY 4/22 SHALL NOT OCCUR WHILE RUNWAY 13/31 IS CLOSED.
 CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING EXISTING CABLES TO FIND AN UNCONGESTED CROSSING LOCATION.
 ANY WORK WITHIN 250' OF THE RUNWAY 4/22 CENTERLINE SHALL BE UNDER RADIO CONTROL. SEE ELECTRICAL DETAILS FOR RUNWAY GUARD LIGHT AIMING SCHEDULE.

LEGEND

⊙ E EXISTING 4' x 4' x 4' ELEC. MANHOLE
 ⊗ EXISTING DUCT BANK AND NUMBER WAY

RGL LEGEND

⊠ NEW RUNWAY GUARD LIGHT
 ⊠ NEW DIRECTIONAL BORE
 ⊠ RUNWAY GUARD LIGHT PAIR NUMBER
 --- NEW RGL POWER CABLE - TWO #4 USE (240V), ONE #10 GROUND IN 1" UNIT DUCT



THE LOCATION OF UNDERGROUND UTILITIES AND SERVICES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, OR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY, HOWEVER, IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES OWNED BY THE AIRPORT, FAA, IL, ANG, OR UTILITY COMPANIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTORS OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

**INSTALL RUNWAY GUARD LIGHTS (RGL)
 ELECTRICAL PLAN**

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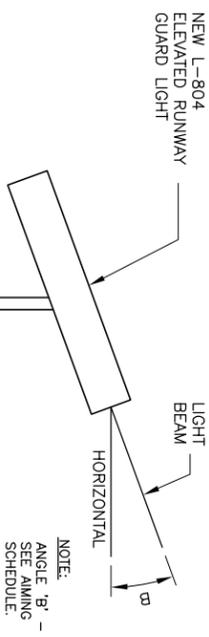
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RUNWAY GUARD LIGHT AIMING DETAIL

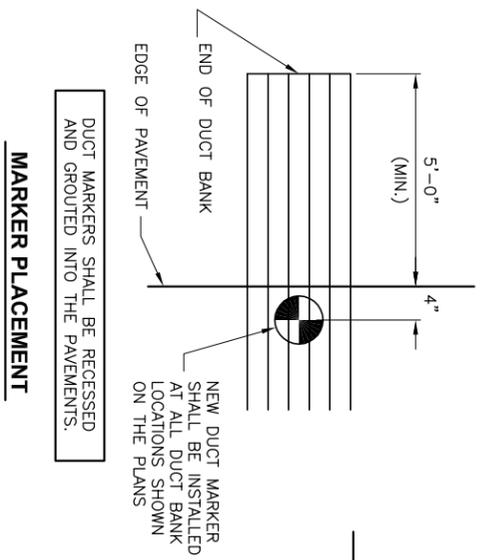
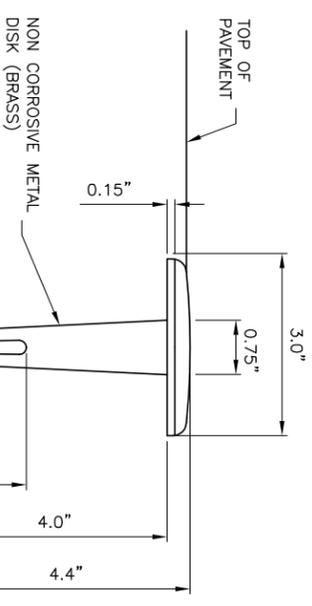
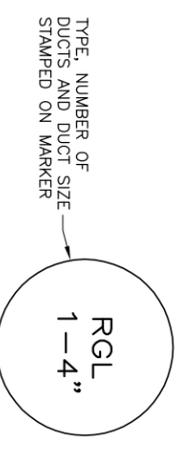
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RUNWAY GUARD LIGHT AIMING SCHEDULE

PAIR NUMBER	L	A	R	A	B
1	13.2'	13.2'		5'	
2	14.6'	13.1'		5'	
3	13.1'	13.4'		5'	
4	20.4'	20.4'		5'	
5	18.6'	18.6'		5'	
6	17.6'	17.1'		5'	
7	12.9'	12.9'		5'	
8	12.9'	12.9'		5'	
9	16.7'	16.7'		5'	
10	16.7'	16.7'		5'	

NOTES:

- A IS ANGLE UNIT SHOULD BE AIMED TOWARD TAXIWAY CENTERLINE AND IS SYMMETRICAL ABOUT THE TAXIWAY CENTERLINE.
- A, B IS ANGLE UNIT SHOULD BE AIMED UPWARD FROM THE HORIZONTAL AND IS THE SAME FOR EACH UNIT IN EVERY PAIR OF RUNWAY GUARD LIGHTS.

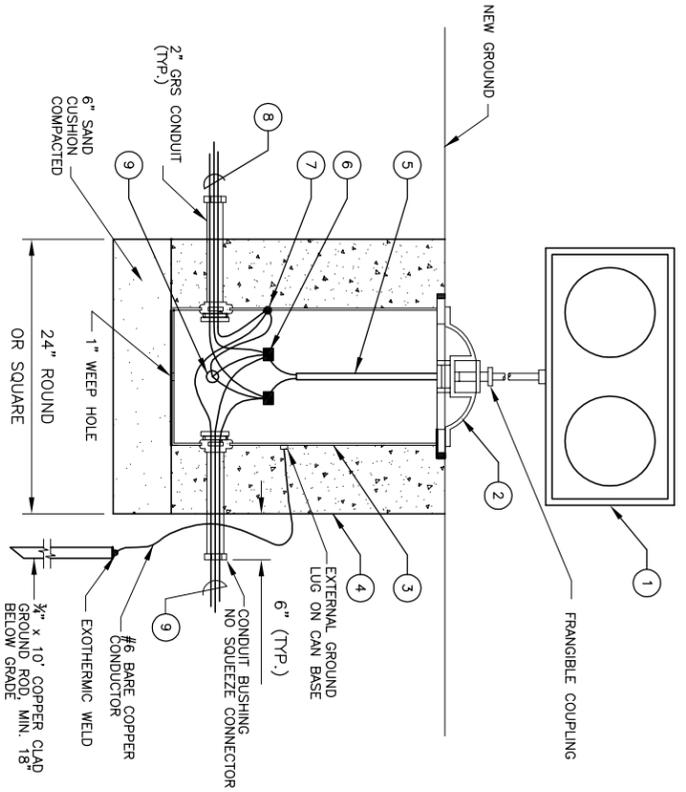


DUCT MARKER DETAILS

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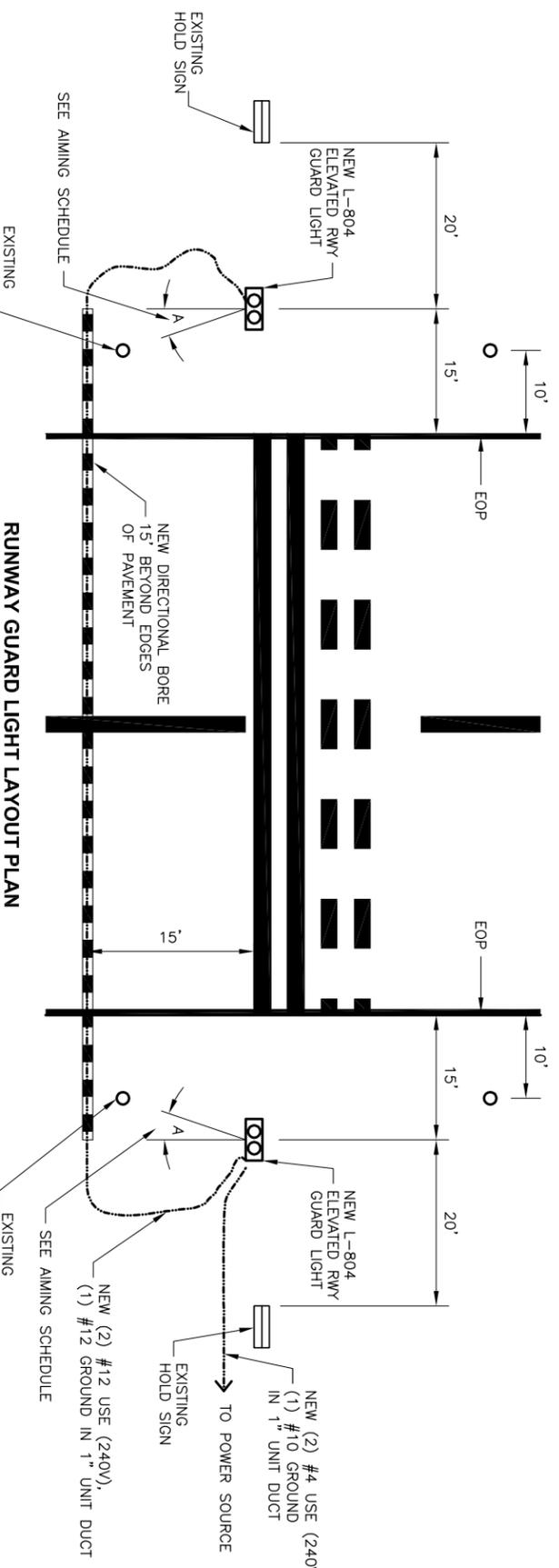
RUNWAY GUARD LIGHT INSTALLATION (VOLTAGE POWERED)

N.T.S.



NOTES:

- L-804 RUNWAY GUARD LIGHT (RGL), MODE 2 (240V) WITH PHOTOCELL, NOT MONITORED, 100W/5.6A INCANDESCENT LAMPS, TRAFFIC SIGNAL YELLOW.
- SPECIAL BASE PLATE.
- L-867B LIGHT BASE.
- CONCRETE.
- CONNECTOR AND PIGTAIL.
- WATERPROOF TAPED SPLIT BOLT SPLICE (TRP OF TWO).
- LIGHT BASE GROUND TERMINAL, CONNECT ALL GROUND WIRES TO HERE.
- TWO #4 USE (240V), ONE #10 GROUND IN 1" UNIT DUCT TO VAULT OR TO NEXT PAIR OF RUNWAY GUARD LIGHTS.
- TWO #12 USE (240V), ONE #12 GROUND, IN 1" UNIT DUCT TO RUNWAY GUARD LIGHT ON OPPOSITE SIDE OF TAXIWAY.



RUNWAY GUARD LIGHT LAYOUT PLAN

N.T.S.

NOTES:

- LOCATION OF DIRECTIONAL BORE MAY VARY TO AVOID EXISTING UTILITIES.
- ANGLE 'A' - SEE AIMING SCHEDULE.

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INSTALL RUNWAY GUARD LIGHTS (RGL)

ELECTRICAL DETAILS

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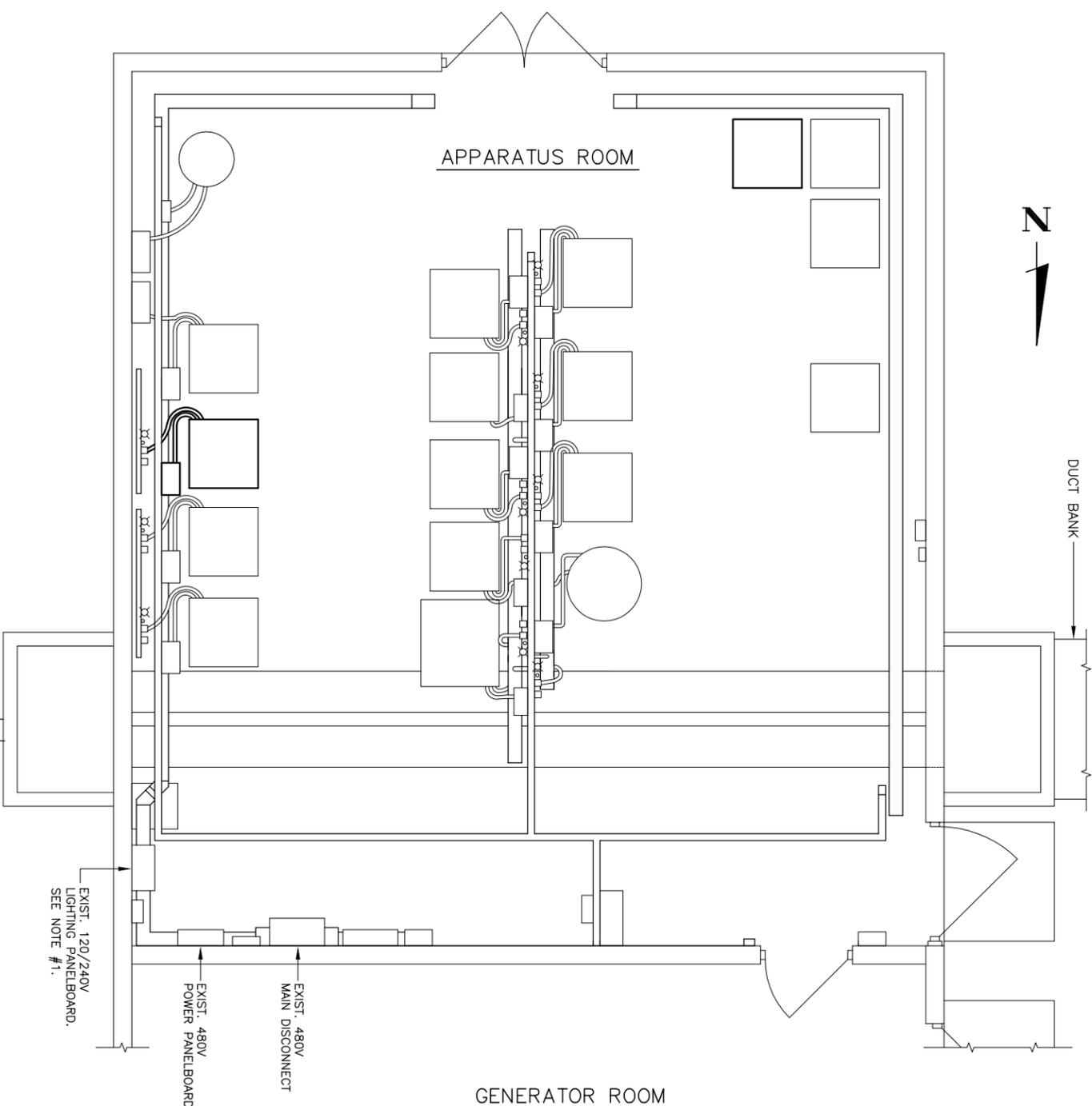
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NOTES

1. INSTALL TWO NEW 240V, 20A-2P CIRCUIT BREAKERS IN EXISTING 120/240V PANELBOARD. REVISE PANELBOARD SCHEDULE. NOTE THAT ALL MODIFICATIONS TO THIS PANELBOARD SHALL BE CONSIDERED INCIDENTAL TO THE INSTALLATION OF THE RUNWAY GUARD LIGHTS AND WILL NOT BE PAID FOR SEPARATELY.
2. ROUTE PROPOSED 240V RUNWAY GUARD LIGHT POWER (2 CIRCUITS) WIRING IN EXISTING WIREWAY AND IN-FLOOR TRENCH TO BUILDING EXTERIOR.
3. TWO SEPARATE CIRCUITS SHALL BE RUN FROM THE ELECTRICAL VAULT TO THE NEW RUNWAY GUARD LIGHTS. THE CABLES SHALL BE LABELED RGL CKT #1 (240V) AND RGL CKT #2 (240V). RGL CKT #1 SHALL SERVE RGL'S 1,2,3, AND 4 WHILE RGL CKT #2 WILL SERVE RGL'S 5,6,7,8,9, AND 10.



VAULT PLAN
1/4"=1'-0"

2-1/C #4 USE, 1 #10 GND IN UD, (RGL CKT #1 (240V))
2-1/C #4 USE, 1 #10 GND IN UD, (RGL CKT #2 (240V))
TO RUNWAY GUARD LIGHTS
SEE NOTE #2

EXIST. 120/240V
LIGHTING PANELBOARD.
SEE NOTE #1.

EXIST. 480V
POWER PANELBOARD

EXIST. 480V
MAIN DISCONNECT

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**INSTALL RUNWAY GUARD LIGHTS (RGL)
VAULT DETAILS**

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