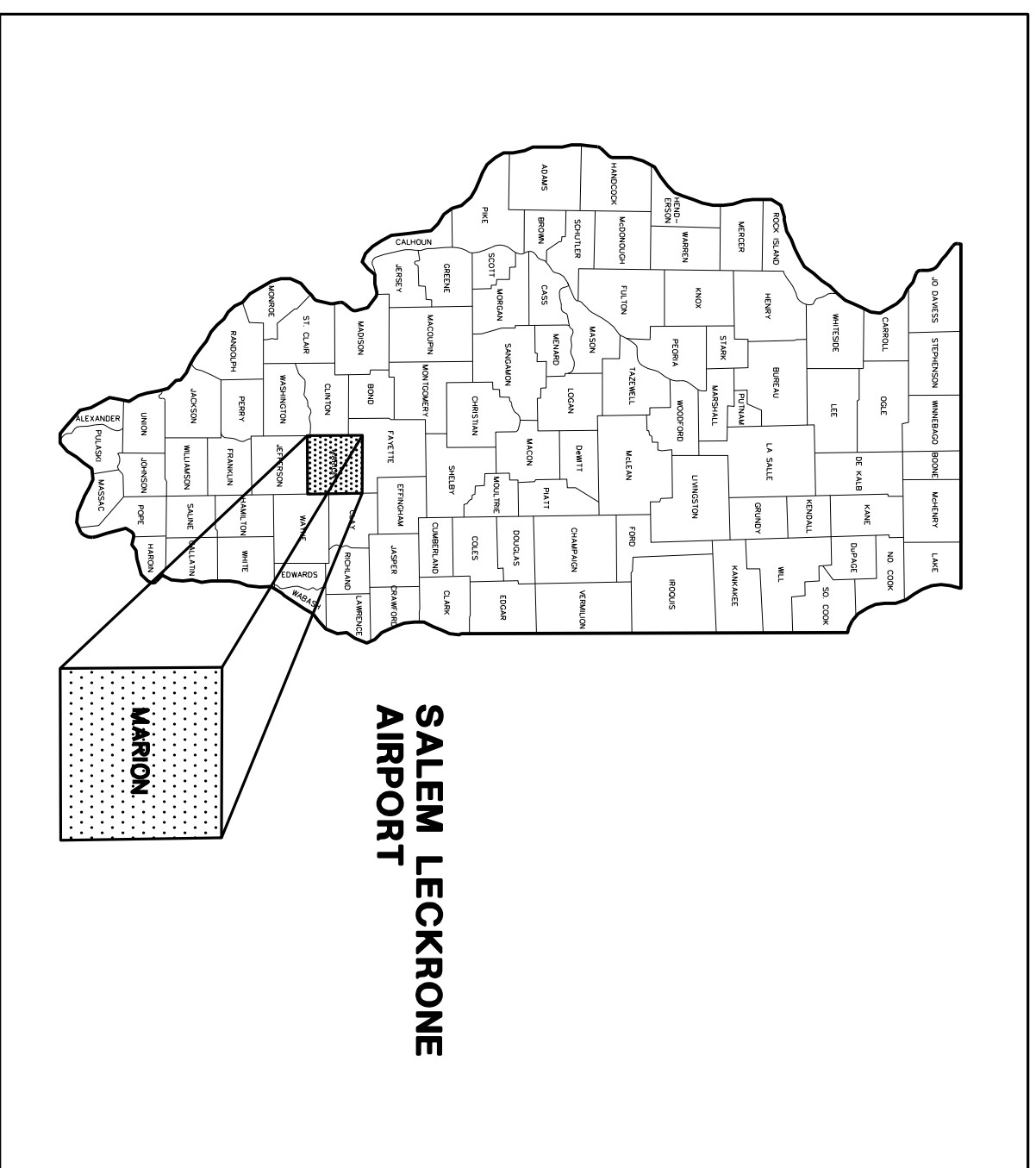


CONSTRUCTION PLANS FOR SALEM LECKRONE AIRPORT SALEM, MARION COUNTY, ILLINOIS

REHABILITATE AIRFIELD EDGE LIGHTING SYSTEM AND REPLACE REILS

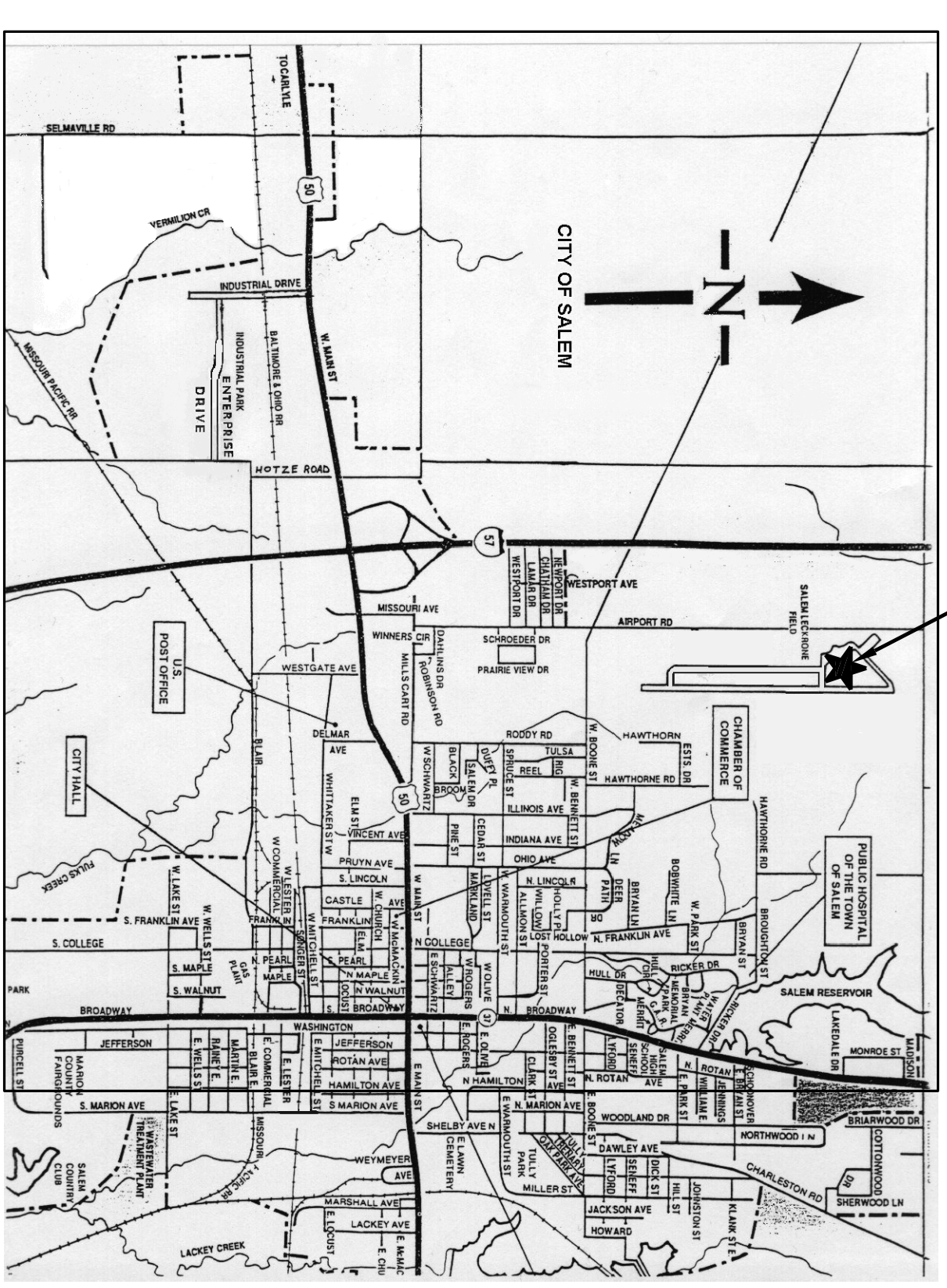
THIS PROJECT SHALL CONSIST OF THE REMOVAL AND REPLACEMENT OF THE MEDIUM INTENSITY LIGHTING SYSTEMS ON RUNWAY 18-36 AND ASSOCIATED TAXIWAYS



LOCATION MAP

ILLINOIS PROJECT - SLO-3929
AIP PROJECT NO - 3-17-0089-B18
DATE: MAY 4, 2010

LATITUDE: 38°38'34"
LONGITUDE: 88°57'51"
ELEVATION: 573 M.S.L.



VICINITY MAP

AECOM
2524 South Broadway
P.O. Box 850
Salem, Illinois 62881
T: 618.548.3500
F: 618.548.5246
www.aecom.com
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IL Design Firm Reg. Nos.
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184-003706
AECOM Technical Services
184-000178

062-046824
LICENSED
PROFESSIONAL
ENGINEER
OF
ILLINOIS
EVP
11/28/11

SUBMITTED BY *[Signature]*
DATE *05/04/2010*

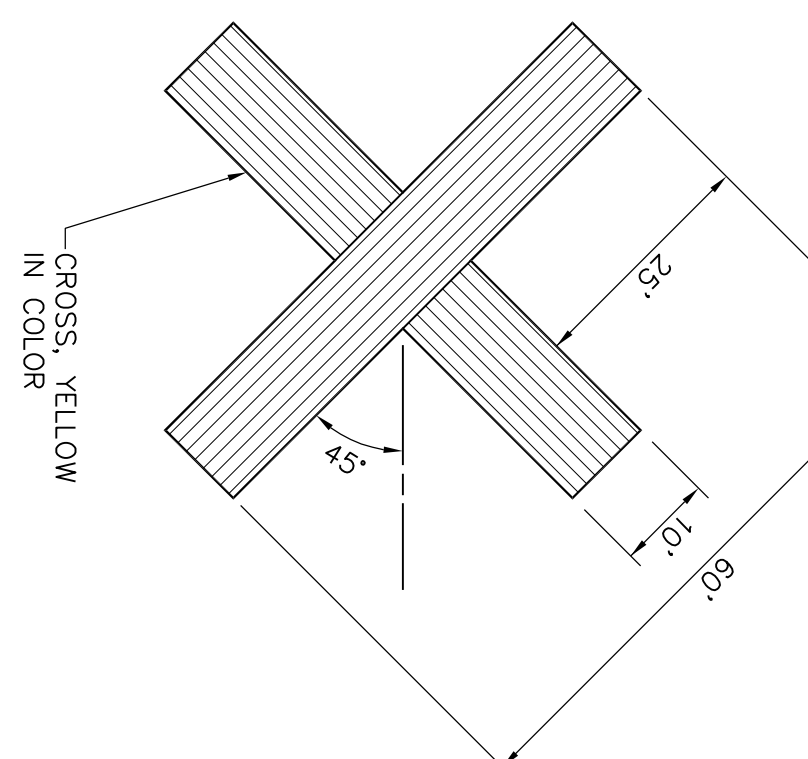
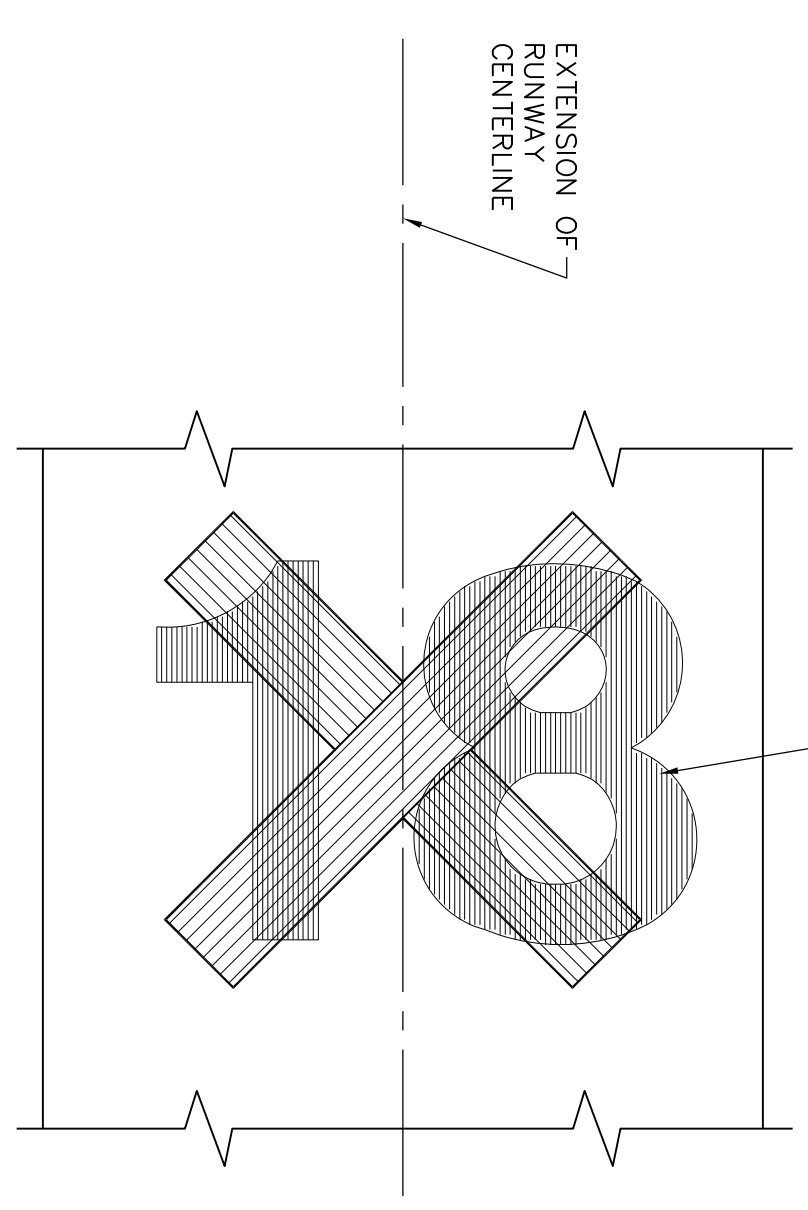
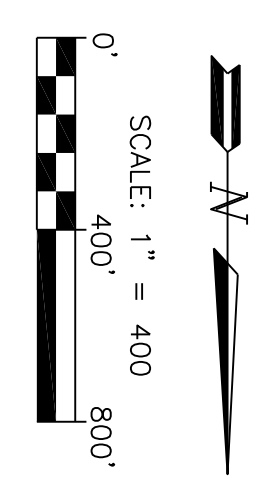
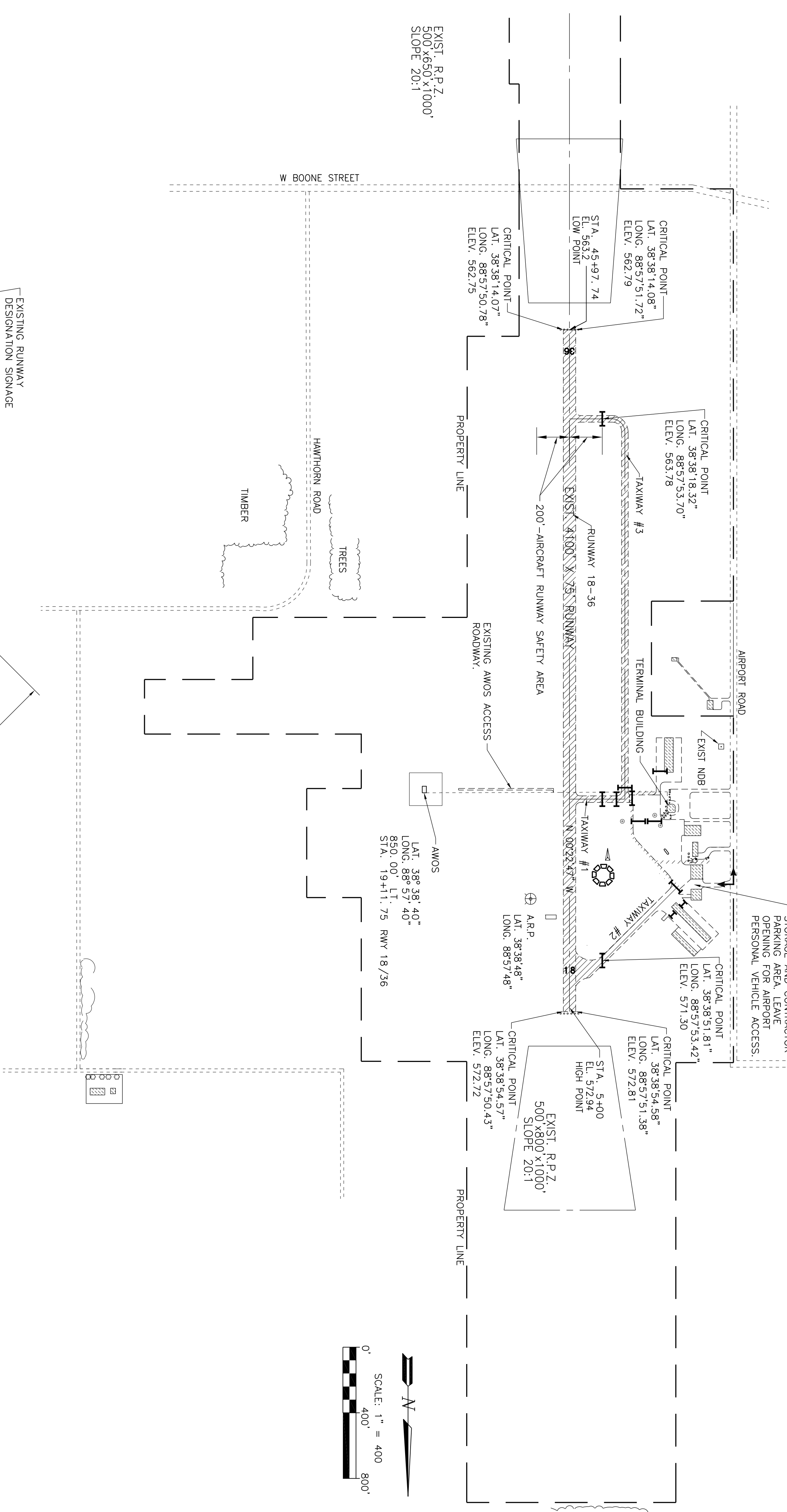
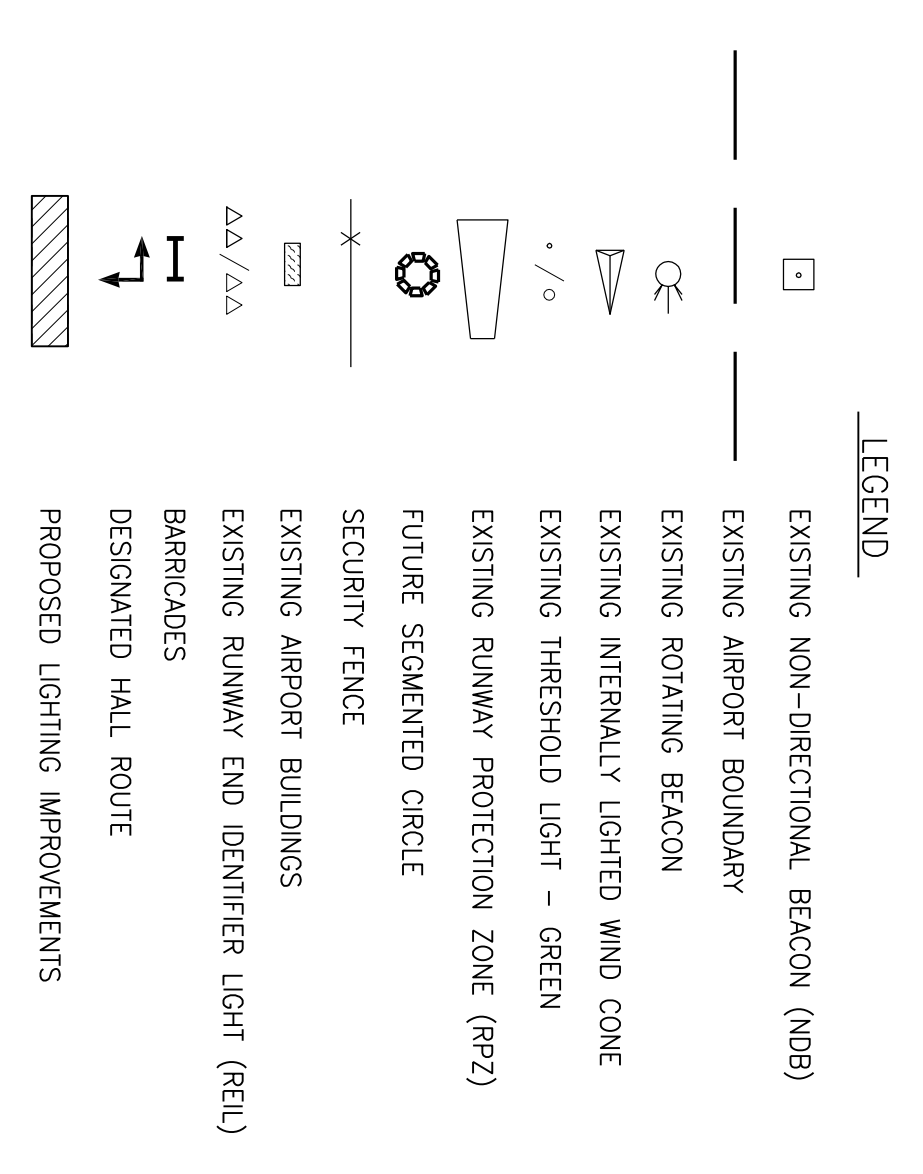
SALEM LECKRONE AIRPORT
SALEM, ILLINOIS

APPROVED *[Signature]*
CHAIRMAN SALEM AIRPORT AUTHORITY
DATE *05/04/2010*

SAFETY PLAN

Issued
Rev Date
Description

Designed: MRO 05/04/2010
Drawn: JMW 05/04/2010
Checked: MRO 05/04/2010
Approved: MRO 05/04/2010
PROJECT NUMBER
60144190
SHEET REFERENCE NUMBER



CONTRACTOR'S RESPONSIBILITIES

1. THE CONTRACTOR'S EMPLOYEES WILL PARK IN THE EQUIPMENT PARKING AREA.
2. THE CONTRACTOR WILL FURNISH ALL EMPLOYEES WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THEM AS BEING PART OF THE CONSTRUCTION CREW.
3. WHEN THE CONTRACTOR'S VEHICLES ARE ON THE AIRPORT SITE THEY WILL BE DISPLAYED IN FULL VIEW ABOVE THE VEHICLES.
4. CONTRACTOR MUST HAVE AERONAUTIC RADIO FOR EMERGENCY USE (122.8 MHz)
5. THE CONTRACTOR WILL NOT BE ALLOWED ACCESS TO THE AIRPORT TERMINAL BUILDING & WILL NEED TO SUPPLY PORTABLE REST ROOMS ON SITE FOR CONSTRUCTION CREWS.
6. CONTRACTOR PERSONNEL SHALL BE LIMITED TO AREA REQUIRED TO CONSTRUCT IMPROVEMENTS. ACCESS NOT ALLOWED ACROSS AIRPORT PROPERTY.
7. BARRICADES SHALL BE PLACED IN SUCH A WAY AS TO PREVENT AIRCRAFT ACCESS TO THE CONTRACTOR'S WORK AREA.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES AND THE COST WILL BE CONSIDERED INCIDENTAL TO THE PROJECT WITH NO ADDITIONAL COMPENSATION ALLOWED.
9. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPlicing NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT. CONTRACTOR SHALL VERIFY THAT ALL EXISTING RUNWAYS AND TAXIWAYS HAVE LIGHTING OPERATIONAL AT THE END OF EACH WORKING DAY.

AIRCRAFT SAFETY AREA - RUNWAY

THE CONTRACTOR OR HIS EMPLOYEES WILL NOT PROCEED INTO THE AIRCRAFT RUNWAY SAFETY AREA WITHOUT FIRST CLOSING THE AFFECTED RUNWAY. THE CONTRACTOR WILL NOTIFY THE AIRPORT MANAGER TWO DAYS IN ADVANCE OF A RUNWAY CLOSING. ANY RUNWAY CLOSURE MUST BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO ANY CONSTRUCTION. WHEN CONSTRUCTION ACTIVITIES ARE WITHIN 200' OF THE RUNWAY CENTERLINE, THE RUNWAY WILL BE CLOSED BY PLACING YELLOW CROSSES OVER THE NUMBERS AT THE END OF THE RUNWAY. COST OF CONSTRUCTING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE RUNWAY WILL BE CLOSED ONLY DURING THE CONSTRUCTION DAY. AT THE END OF EACH CONSTRUCTION DAY THE CONTRACTOR WILL SMOOTH GRADE ALL AREAS WITHIN THE SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER AND REOPEN THE RUNWAY. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE ONLY EXCEPTION TO THE REQUIREMENT OF HAVING THE RUNWAY OPENED EACH NIGHT WILL BE A TWO (2) DAY MAXIMUM PERIOD WHERE THE SWITCH OVER FROM USING THE EXISTING LIGHTING SYSTEM TO THE NEW LIGHTING SYSTEM OCCURS.

AIRCRAFT SAFETY AREA - TAXIWAYS / TAXIWAYS / APRONS

CONSTRUCTION WILL BE ALLOWED UP TO THE EDGE OF PAVEMENTS WITHOUT CLOSURE ON A LIMITED BASIS. ANY DROP OFF SHALL BE EQUIQUALLY SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN 66' OF THE 2' OF AN ACTIVE TAXIWAY OR TAXIWAY. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO AT NO ADDITIONAL COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

MISCELLANEOUS NOTES

THE LOCATIONS OF THE UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED.

LOCATION OF EXISTING AIRPORT UNDERGROUND CABLES MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE MADE IN ACCORDANCE WITH THE SPECIFICATIONS OR AS DIRECTED BY THE OWNER. DAMAGED CABLES SHALL BE DONE FROM POINT TO POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PERFORMANCE OF AN FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS, IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.

EROSION CONTROL

THE WORK REQUIRED FOR THE COMPLETION OF THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND, THEREFORE NO N.F.D.'S, PERMITS WILL BE REQUIRED.

SCOPE OF WORK

THIS PROJECT SHALL CONSIST OF THE REMOVAL AND REPLACEMENT OF THE MEDIUM INTENSITY LIGHTING SYSTEMS ON RUNWAY 18-36 AND ASSOCIATED TAXIWAYS. REMOVE AND REPLACE REILS.

HAUL ROAD AND EQUIPMENT PARKING AREA

CONTRACTOR WILL USE THE DESIGNATED HAIL ROUTE AND EQUIPMENT PARKING AREA AS SHOWN ON THIS SHEET. IT WILL BE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE AIRPORT SECURITY AT THE HAIL ROUTE ENTRANCE, AND RESTORE THESE AREAS TO THEIR ORIGINAL CONDITION AFTER CONSTRUCTION IS COMPLETED. ANY AREAS DAMAGED OUTSIDE THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE. THE COST OF THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

CONSTRUCTION EQUIPMENT HEIGHT

MAXIMUM HEIGHT OF CONSTRUCTION EQUIPMENT SHALL NOT EXCEED 25'.

CONSTRUCTION STAGING

THE CONTRACTOR SHALL PLACE BARRICADES AS SHOWN TO CLOSE INDIVIDUAL WORK AREAS AND TO ALLOW ACCESS FROM THE APRON AND/OR T-HANGARS TO THE RUNWAY AS DIRECTED BY THE RESIDENT ENGINEER. NO CONTRACTOR PERSONNEL OR EQUIPMENT WILL BE ALLOWED WITHIN 66' OF AN ACTIVE TAXIWAY OR TAXIWAY CENTERLINE, UNLESS OTHERWISE APPROVED.

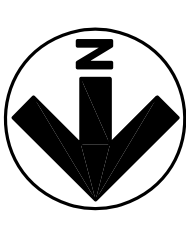
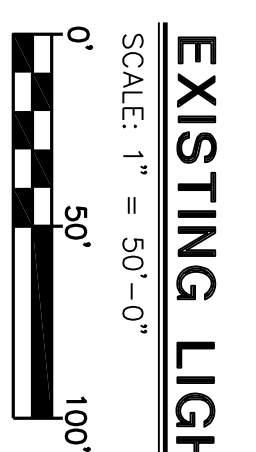
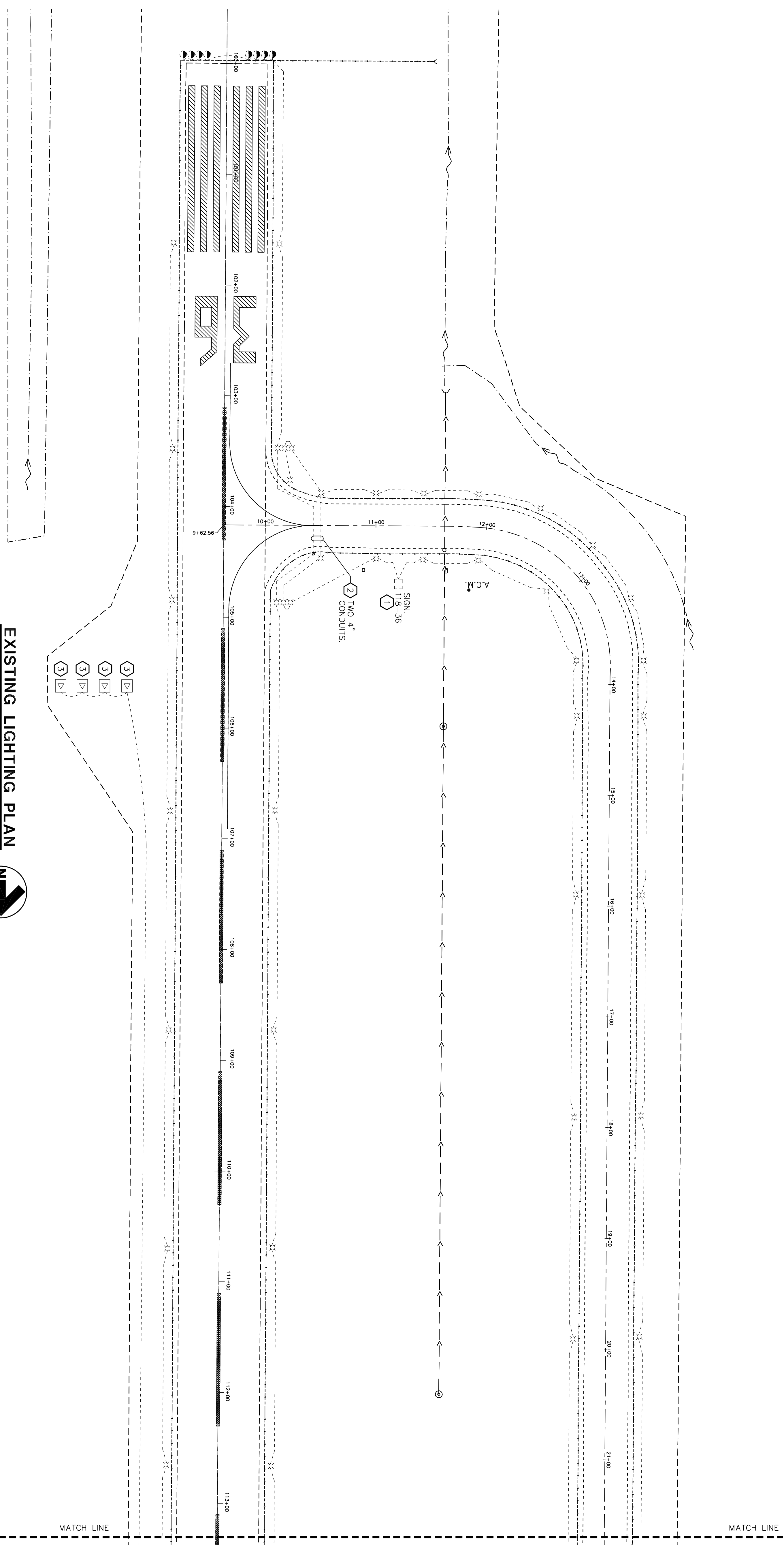
EXISTING UTILITY LOCATIONS

THE CONTRACTOR SHALL CONTACT UTILITE AND HAVE ALL UTILITIES LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. 811 OR 1-800-982-0123. UTILITE WILL ONLY MARK THE UTILITIES TO THE METER LOCATION. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL ON-SITE UNDERGROUND UTILITIES, AND THIS COST WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

EXISTING
 LIGHTING PLAN

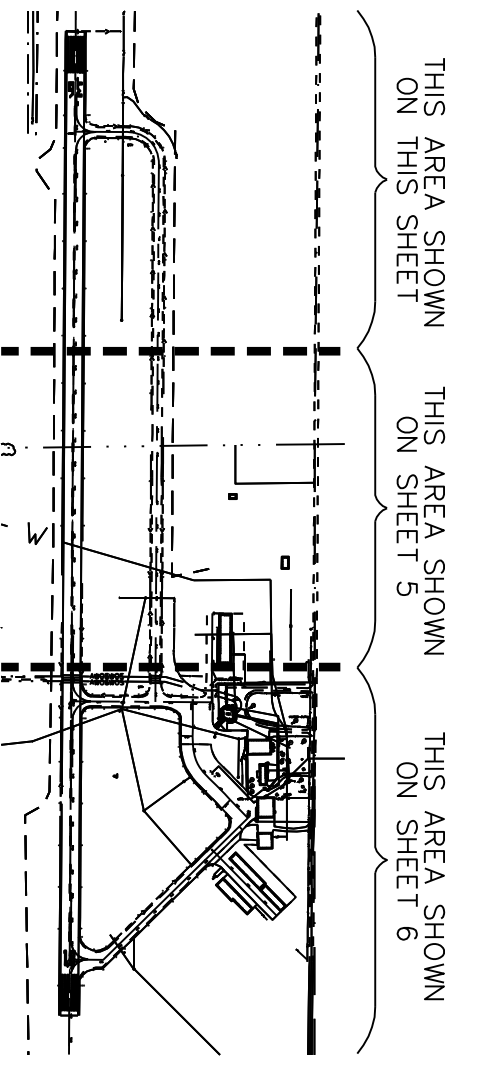
Rev	Date	Description
Issued		

Designed: JJE 5/04/2010
Drawn: GJS 5/04/2010
Checked: DBH 5/04/2010
Approved: JJE 5/04/2010
PROJECT NUMBER 60144190
SHEET REFERENCE NUMBER 4



- GENERAL NOTES:**
- ALL DASHED ELECTRICAL WORK IS TO BE REMOVED, UNLESS OTHERWISE NOTED.
- DEMOLITION KEYED NOTES:** (4)
- EXISTING SIGN TO REMAIN. DISCONNECT ELECTRICAL CABLE AND PREPARE FOR NEW CONDUCTORS. SEE PROPOSED WORK PLANS.
 - EXISTING CONDUIT UNDER TAXIWAY TO REMAIN AND BE REUSED. REMOVE CABLE.
 - EXISTING PAPI TO REMAIN. DISCONNECT ELECTRICAL CABLE AND PREPARE FOR NEW CONDUCTORS. SEE PROPOSED LIGHTING PLAN.

- DEMOLITION NOTES:**
- THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT, OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITIES COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
 - IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL ONSITE UNDERGROUND UTILITIES, AND THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- LIGHTING REMOVAL NOTES:**
- ALL EXISTING RUNWAY AND TAXIWAY LIGHTS THAT ARE DESIGNATED FOR REMOVAL SHALL BE REMOVED. THE LIGHTS AND THEIR ISOLATING TRANSFORMER SHALL BE TURNED OVER TO THE AIRPORT MANAGER. THE CONCRETE LIGHT BASES SHALL BE REMOVED AND DISPOSED OF LEGALLY OFF THE AIRPORT SITE.
 - REMAINING EXISTING LIGHTING CIRCUIT IN ORDER THAT THE LIGHTING CIRCUIT REMAINS OPERATIONAL, EACH NIGHT OR DURING NON-CONSTRUCTION PERIODS, THE CONTRACTOR SHALL FURNISH JUMPER CABLES OR ADDITIONAL CONNECTOR KITS AS REQUIRED TO PLACE THE LIGHTING CIRCUIT BACK INTO OPERATION. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THIS WORK.
 - REMOVAL OF THE EXISTING LIGHTS, AND ISOLATING TRANSFORMERS SHALL BE PAID FOR UNDER ITEMS #AR125901 REMOVE STAKE MOUNTED LIGHT PER EACH AND #AR125902 REMOVE BASE MOUNTED LIGHT PER EACH.



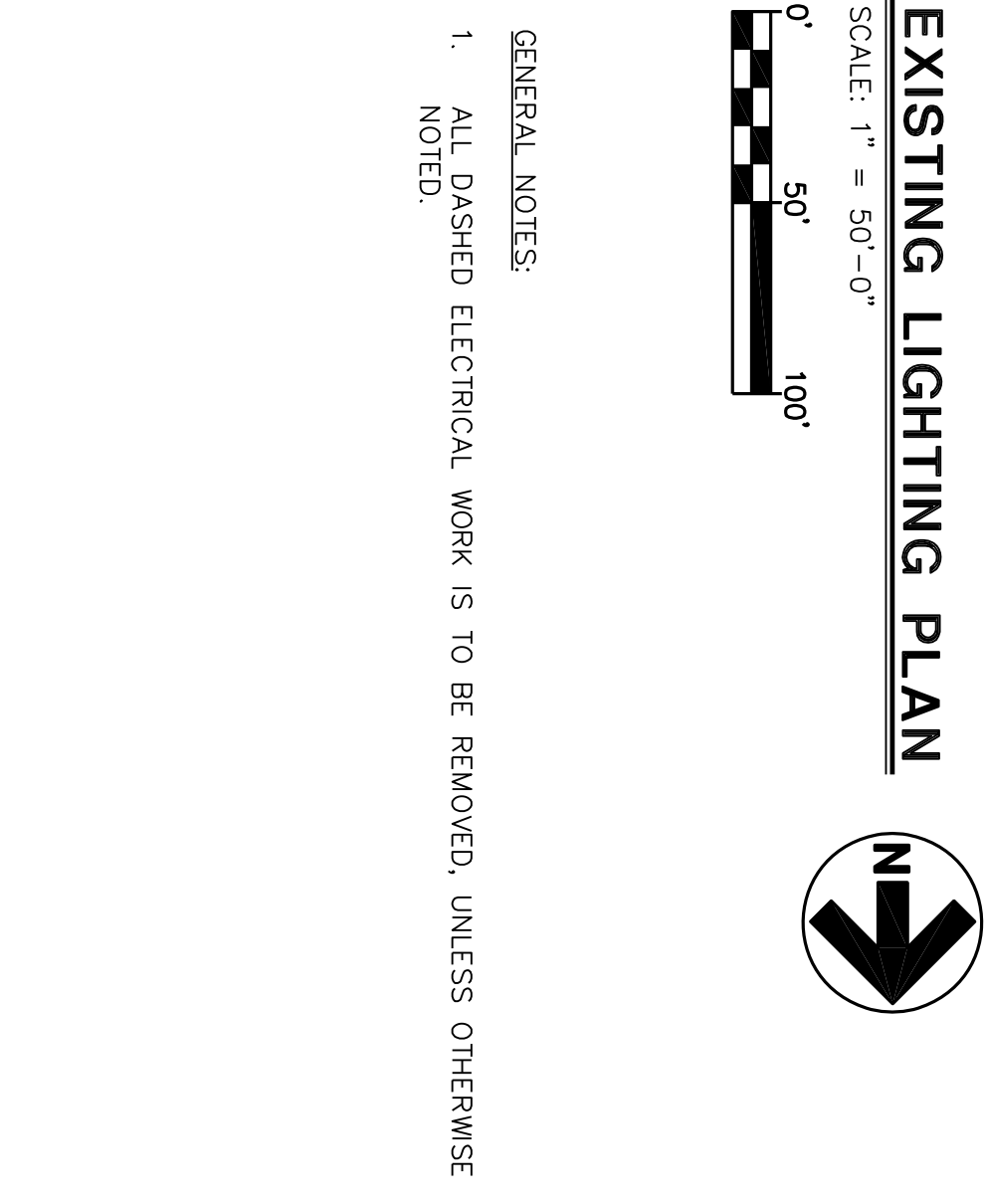
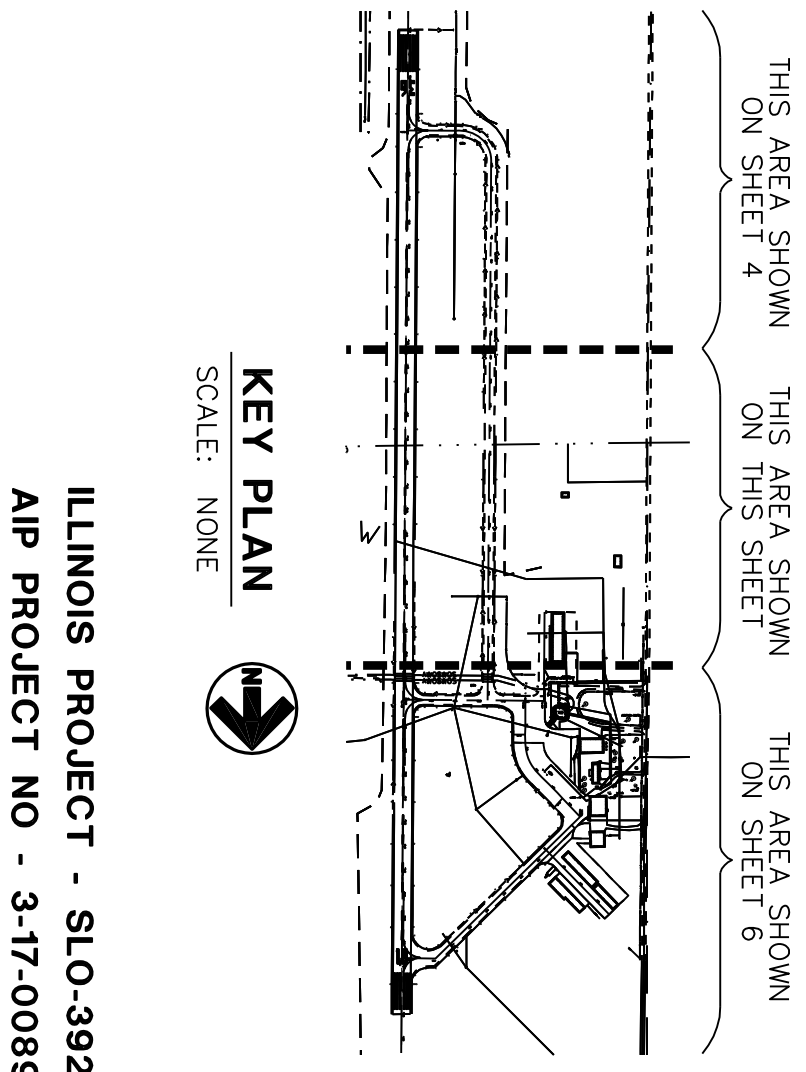
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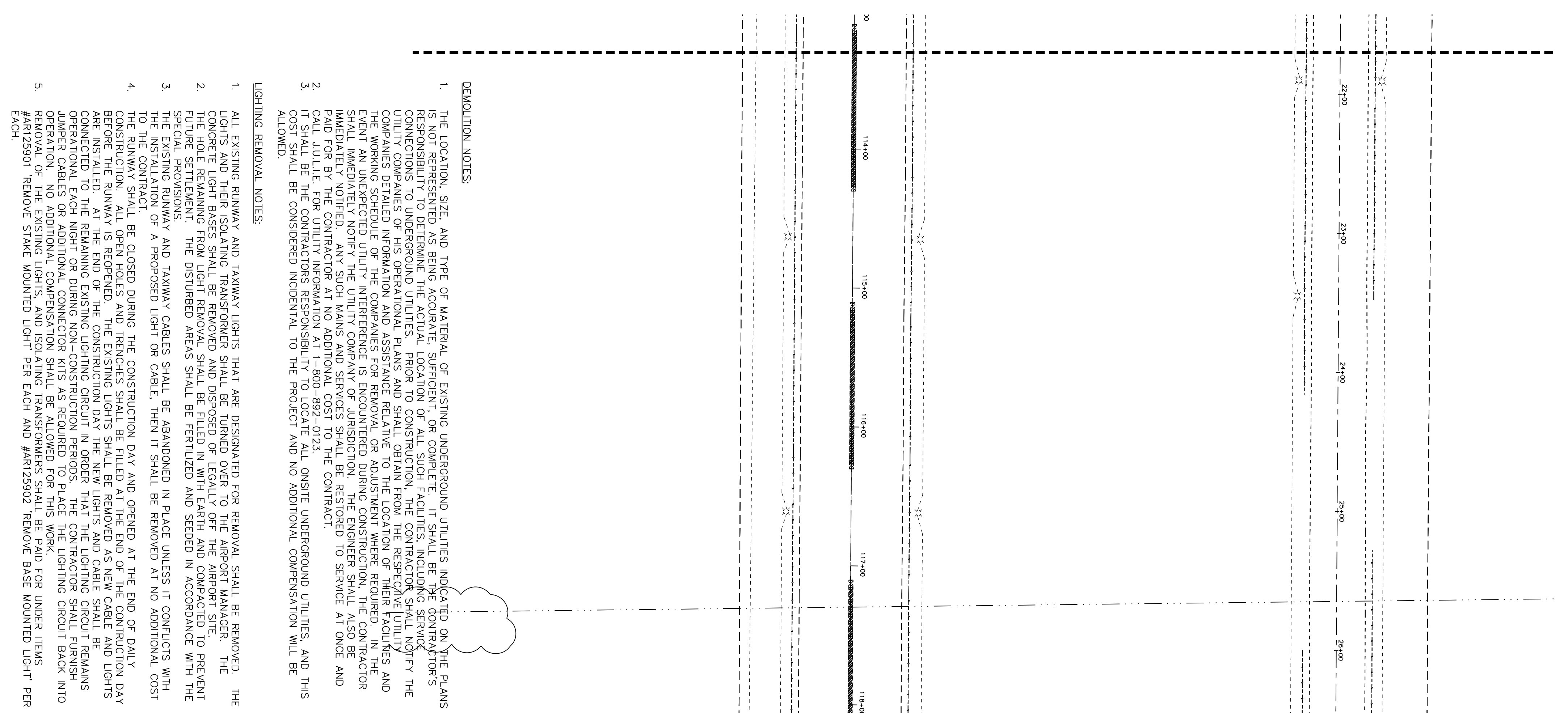
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EXISTING
 LIGHTING PLAN

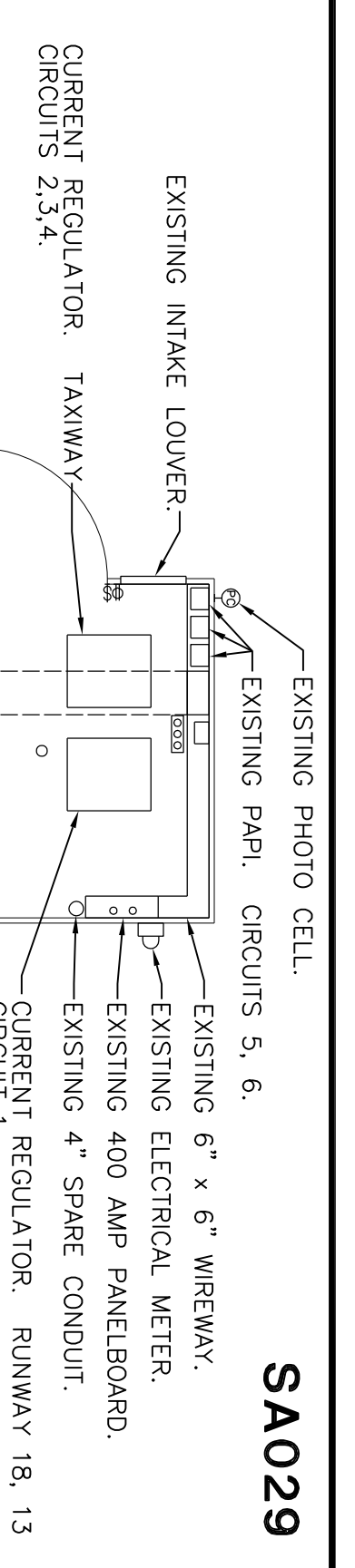
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Rev. Date	
Description	
Design: JJE 5/04/2010	
Drawn: GJS 5/04/2010	
Checked: DBH 5/04/2010	
Approved: JJE 5/04/2010	
PROJECT NUMBER	60144190
SHEET REFERENCE NUMBER	5
SHEET 5 OF 12	



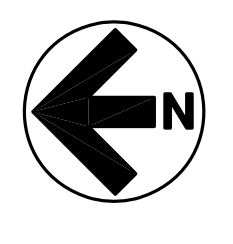
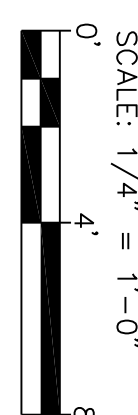
- DEMOLITION NOTES:**
1. THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT, OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH UTILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR UTILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED IN THE COURSE OF CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING THE NECESSARY PERMITS AND SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF ANY DISRUPTION. THE ENGINEER SHALL ALSO BE PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
 2. CALL JULIE FOR UTILITY INFORMATION AT 1-800-892-0123.
 3. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL ON-SITE UNDERGROUND UTILITIES, AND THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- LIGHTING REMOVAL NOTES:**
1. ALL EXISTING RUNWAY AND TAXIWAY LIGHTS THAT ARE DESIGNATED FOR REMOVAL SHALL BE REMOVED. THE LIGHTS AND THEIR ISOLATING TRANSFORMER SHALL BE TURNED OVER TO THE AIRPORT MANAGER. THE CONCRETE LIGHT BASES SHALL BE REMOVED AND DISPOSED OF LEGALLY OFF THE AIRPORT SITE.
 2. THE HOLE REMAINING FROM LIGHT REMOVAL SHALL BE FILLED IN WITH EARTH AND COMPACTED TO PREVENT FUTURE SETTLEMENT. THE DISTURBED AREAS SHALL BE FERTILIZED AND SEEDED IN ACCORDANCE WITH THE SPECIAL PROVISIONS.
 3. THE EXISTING RUNWAY AND TAXIWAY CABLES SHALL BE ABANDONED IN PLACE UNLESS IT CONFLICTS WITH THE CONTRACT.
 4. THE RUNWAY SHALL BE CLOSED DURING THE CONSTRUCTION DAY AND OPENED AT THE END OF DAILY CONSTRUCTION. ALL OPEN HOLES AND TRENCHES SHALL BE FILLED AT THE END OF THE CONSTRUCTION DAY BEFORE THE RUNWAY IS REOPENED. THE EXISTING LIGHTS SHALL BE REMOVED AS NEW CABLE AND LIGHTS ARE INSTALLED. AT THE END OF THE CONSTRUCTION DAY THE NEW LIGHTS AND CABLE SHALL BE CONNECTED TO THE REMAINING EXISTING LIGHTING CIRCUIT IN ORDER THAT THE LIGHTING CIRCUIT REMAINS OPERATIONAL EACH NIGHT OR DURING NON-CONSTRUCTION PERIODS. THE CONTRACTOR SHALL FURNISH UNDER CABLES OR ADDITIONAL CONNECTOR KITS AS REQUIRED TO PLACE THE LIGHTING CIRCUIT BACK INTO OPERATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF THE EXISTING LIGHTS AND ISOLATING TRANSFORMERS. SHALL BE PAID FOR UNDER ITEMS #AR725901 REMOVE STAKE MOUNTED LIGHT PER EACH AND #AR125902 REMOVE BASE MOUNTED LIGHT PER EACH.



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EXISTING ELECTRICAL BUILDING PLAN

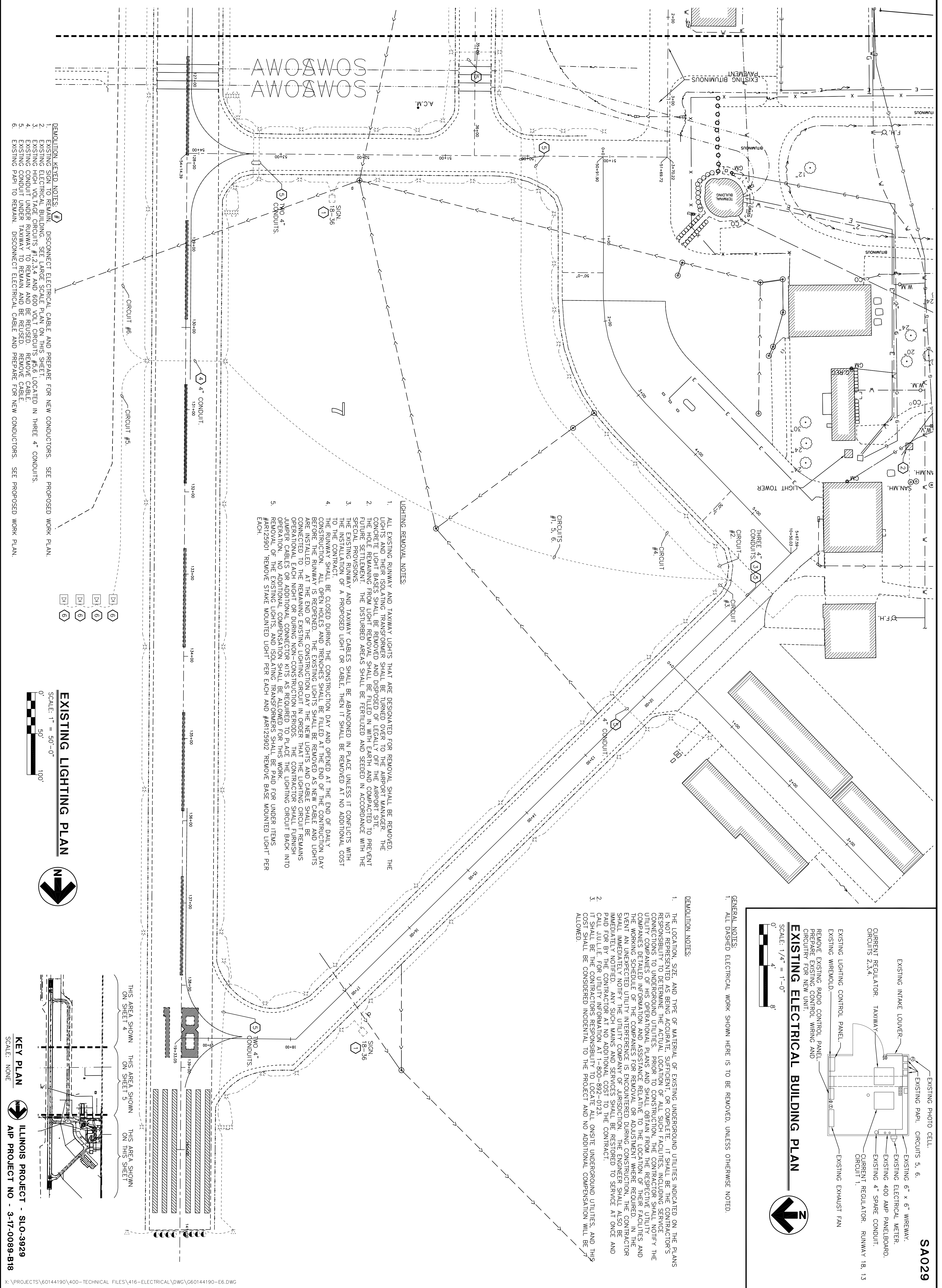


GENERAL NOTES
1. ALL DASHED ELECTRICAL WORK SHOWN HERE IS TO BE REMOVED, UNLESS OTHERWISE NOTED.

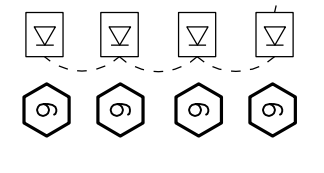
DEMOLITION NOTES:
1. THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE SUFFICIENT FOR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH UTILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE NOTIFIED IMMEDIATELY BY ANY MEANS AVAILABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACTOR.
2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL ONSITE UNDERGROUND UTILITIES, AND THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

LIGHTING REMOVAL NOTES:

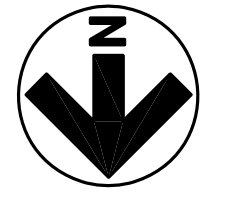
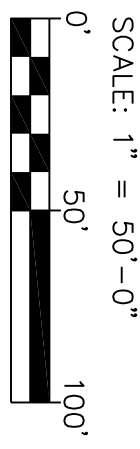
1. ALL EXISTING RUNWAY AND TAXIWAY LIGHTS THAT ARE DESIGNATED FOR REMOVAL SHALL BE REMOVED. THE LIGHTS AND THEIR ISOLATING TRANSFORMER SHALL BE TURNED OVER TO THE AIRPORT MANAGER. THE CONCRETE LIGHT BASES SHALL BE REMOVED AND DISPOSED OF LEGALLY OFF THE AIRPORT SITE.
2. THE HOLE REMAINING FROM LIGHT REMOVAL SHALL BE FILLED IN WITH EARTH AND COMPACTED TO PREVENT FUTURE SETTLEMENT. THE DISTURBED AREAS SHALL BE FERTILIZED AND SEEDED IN ACCORDANCE WITH THE SPECIAL PROVISIONS.
3. THE EXISTING RUNWAY AND TAXIWAY CABLES SHALL BE ABANDONED IN PLACE UNLESS IT CONFLICTS WITH THE INSTALLATION OF A PROPOSED LIGHT OR CABLE. THEN IT SHALL BE REMOVED AT NO ADDITIONAL COST TO THE RUNWAY.
4. THE RUNWAY SHALL BE CLOSED DURING THE CONSTRUCTION DAY AND OPENED AT THE END OF DAILY CONSTRUCTION. ALL OPEN HOLES AND TRENCHES SHALL BE FILLED AT THE END OF THE CONSTRUCTION DAY BEFORE THE RUNWAY IS REOPENED. THE EXISTING LIGHTS SHALL BE REMOVED AS NEW CABLE AND LIGHTS ARE INSTALLED. AT THE END OF THE CONSTRUCTION DAY THE REMAINING EXISTING LIGHTING CIRCUIT REMAINS CONNECTED TO THE REMAINING EXISTING LIGHTING CIRCUIT IN ORDER THAT THE LIGHTING SHALL FURNISH JUMPER CABLES OR ADDITIONAL CONNECTOR KITS AS REQUIRED TO PLACE THE LIGHTING CIRCUIT BACK INTO OPERATION. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THIS WORK.
5. REMOVAL OF THE EXISTING LIGHTS AND ISOLATING TRANSFORMERS SHALL BE PAID FOR UNDER ITEMS #18, #36, #18, #36, #18, #36 REMOVE STAKE MOUNTED LIGHT PER EACH AND #18, #36 REMOVE STAKE MOUNTED LIGHT PER EACH.



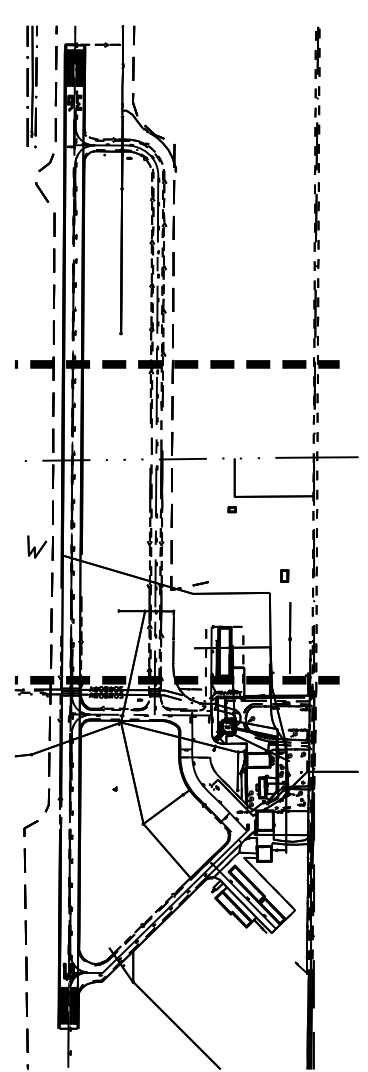
DEMOLITION KEYED NOTES:
1. EXISTING SIGN TO REMAIN.
2. EXISTING ELECTRICAL BUILDING, SEE LARGE SCALE PLAN ON THIS SHEET.
3. EXISTING CONDUIT UNDER RUNWAY TO REMAIN AND BE REUSED. REMOVE CABLE.
4. EXISTING CONDUIT UNDER TAXIWAY TO REMAIN AND BE REUSED. REMOVE CABLE.
5. EXISTING PAPI TO REMAIN. DISCONNECT ELECTRICAL CABLE AND PREPARE FOR NEW CONDUCTORS. SEE PROPOSED WORK PLAN.
6. DISCONNECT ELECTRICAL CABLE AND PREPARE FOR NEW CONDUCTORS. SEE PROPOSED WORK PLAN.
7. EXISTING SIGN TO REMAIN.



EXISTING LIGHTING PLAN



THIS AREA SHOWN ON SHEET 4
THIS AREA SHOWN ON SHEET 5
THIS AREA SHOWN ON THIS SHEET



KEY PLAN
ILLINOIS PROJECT - SLO-3929
AIP PROJECT NO. 3-17-0089-B18
SCALE: NONE

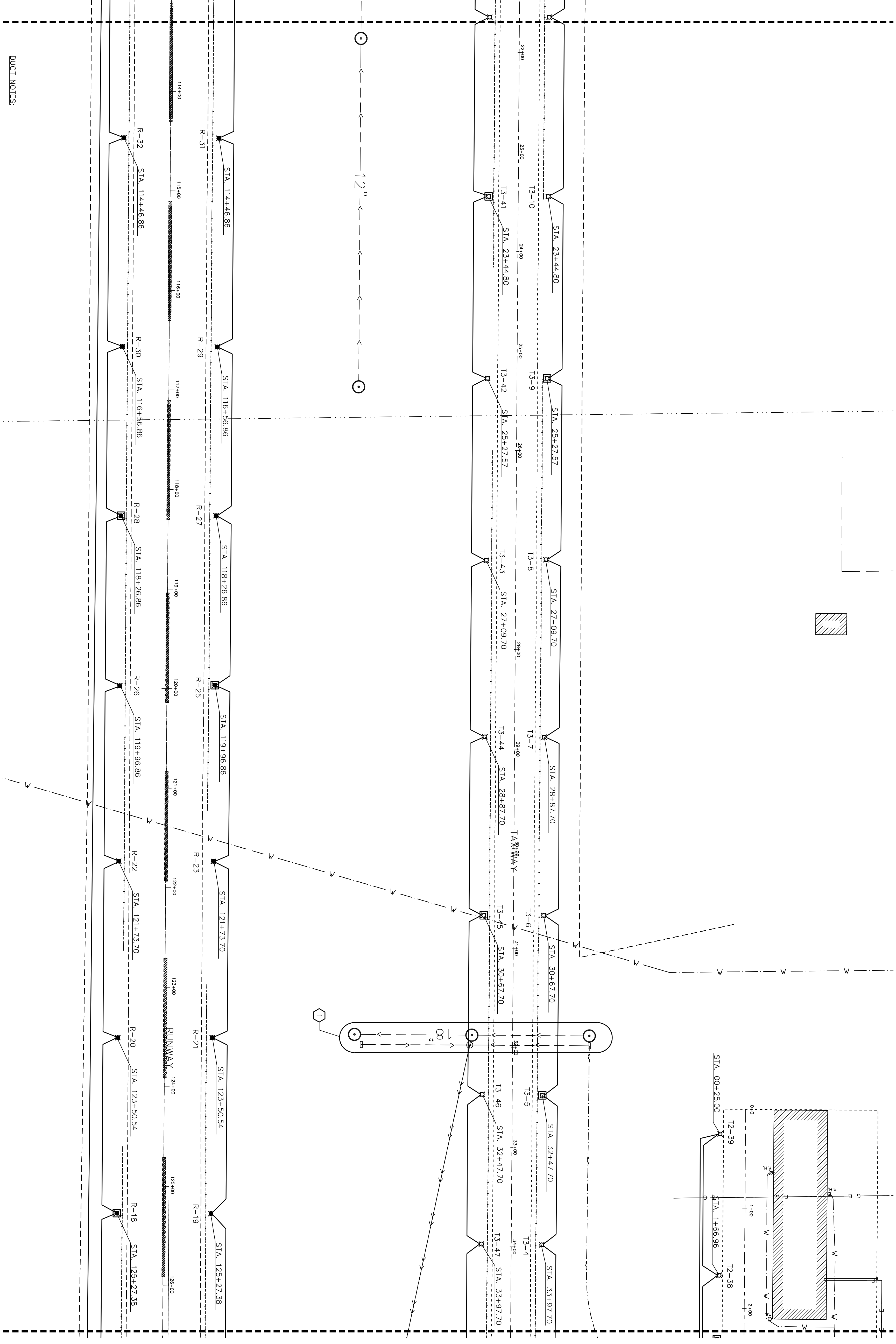
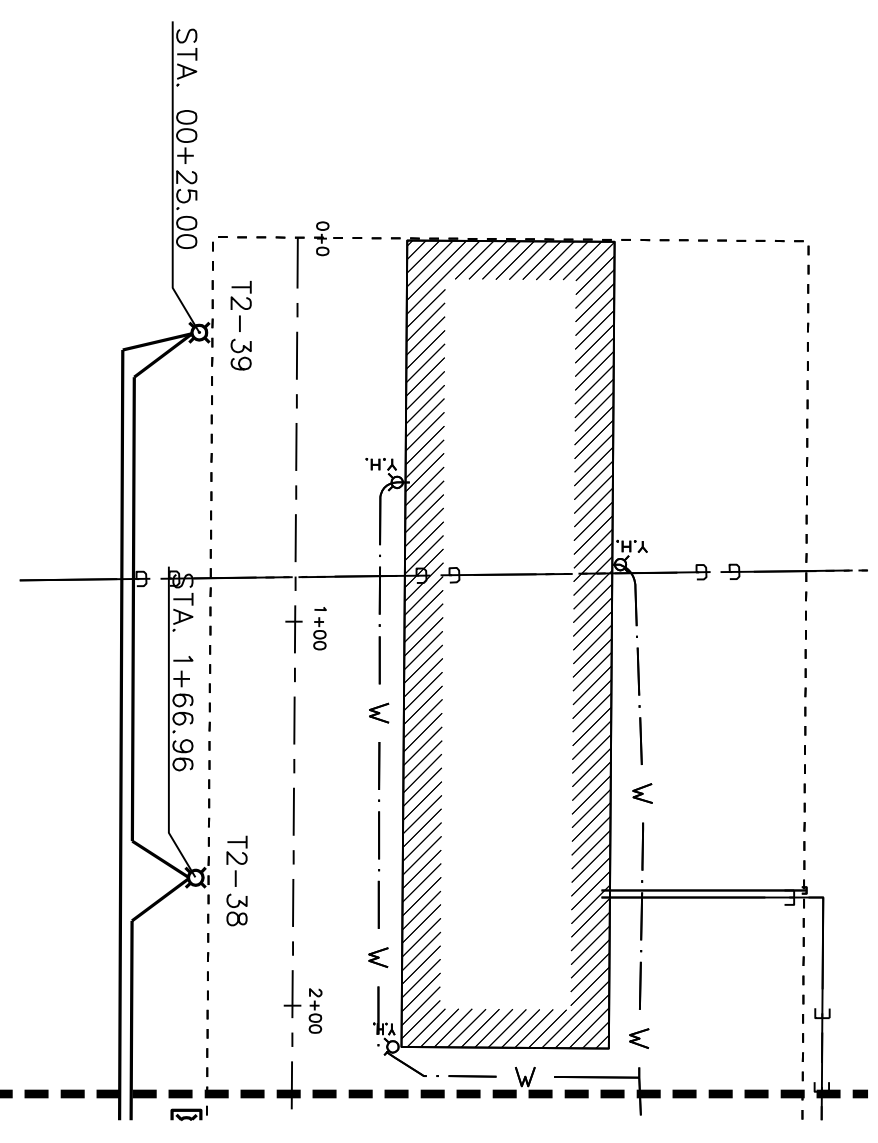
<p>AECOM 2524 South Broadway P. O. Box 850 Salem, Illinois 62881 815-548-3500 www.aecom.com</p>		<p>Copyright © 2011 by AECOM LE Design Firm Reg. No. AECOM USA, Inc. 194-003706 AECOM Technical Services 194-003715</p>		<p>REHABILITATE AIRFIELD EDGE LIGHTING SYSTEM AND REPLACE REILS SALEM LECKRONE AIRPORT SALEM, ILLINOIS ILLINOIS PROJECT: SLO-3929 AIP PROJECT NO: 3-17-0089-B18</p>	
<p>Issued</p>		<p>6</p>		<p>SHEET 6 OF 12</p>	

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**REHABILITATE AIRFIELD EDGE LIGHTING SYSTEM
AND REPLACE REILS
SALEM LECKRONE AIRPORT
SALEM, ILLINOIS
ILLINOIS PROJECT: SLO-3929
AIP PROJECT NO: 3-17-0089-B18**

**PROPOSED
LIGHTING PLAN**

Issued	
Rev. Date	
Description	
Designed: JJE 5/04/2010	
Drawn: GJS 5/04/2010	
Checked: DBH 5/04/2010	
Approved: JJE 5/04/2010	
PROJECT NUMBER	60144190
SHEET REFERENCE NUMBER	8
SHEET 8 OF 12	

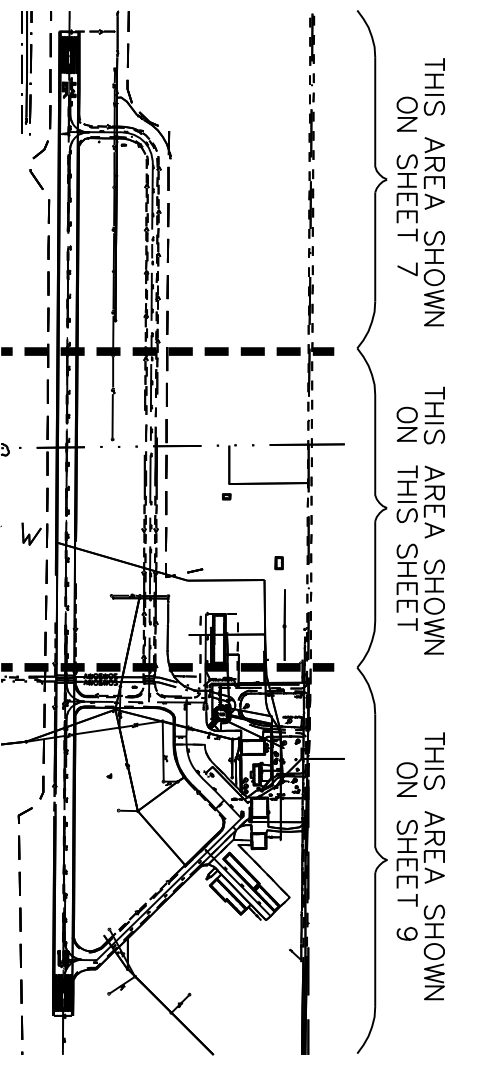


- DUCT NOTES:**
1. THE CONTRACTOR SHALL INSTALL THE PROPOSED DUCT AT THE LOCATIONS SHOWN ON THIS SHEET AND IN ACCORDANCE WITH THE DETAILS AND SPECIFICATIONS.
 2. THE CONTRACTOR SHALL REPAIR THE DISTURBED AREAS TO THEIR ORIGINAL STATE AND SEEDED IN ACCORDANCE WITH THE SPECIFICATIONS.
 3. THE PROPOSED DUCTS SHALL BE PAID FOR UNDER ITEM #A110014 4" DIRECTIONAL BORE PER LF.



- GENERAL NOTES:**
1. ALL WORK SHOWN IS NEW, UNLESS OTHERWISE NOTED.

- KEYED NOTES:**
1. IN ADDITION TO MEETING THE MINIMUM COVER REQUIREMENTS NOTED IN THESE PLANS AND SPECIFICATIONS, THE DEPTH OF THE ELECTRICAL CABLING IN UNDUCT SHALL PROVIDE A MINIMUM VERTICAL CLEARANCE OF 12" ABOVE OR BELOW EXISTING STORM SEWER, AND SHARP PROVIDE CONTINUATION OF THIS DEPTH TO 10' MINIMUM HORIZONTALLY BEYOND THE FUTURE STORM SEWER, AS APPLICABLE (TYPICAL).



KEY PLAN



SCALE: NONE

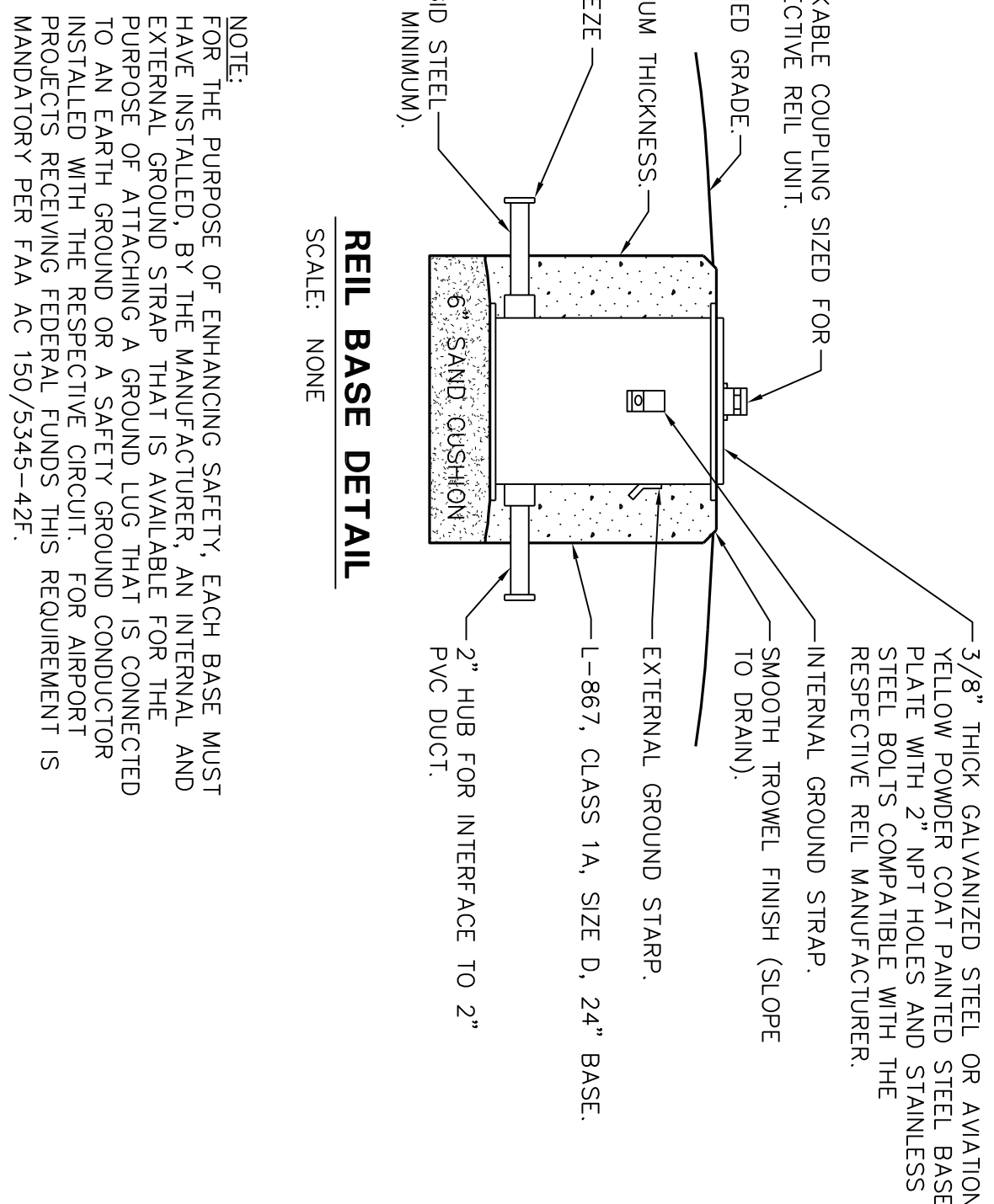
ILLINOIS PROJECT - SLO-3929
AIP PROJECT NO - 3-17-0089-B18

GENERAL NOTES:

1. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER, IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.
2. PER FAA AC 150/5340-300, DESIGN AND INSTALLATION DETAILS FOR CONDUIT SHALL BE AS SHOWN IN THE DETAILS. EACH STAKE MOUNTED LIGHT AND EACH TRANSFORMER BASE/LIGHT CAN ASSOCIATED WITH RUNWAY LIGHTS, TAXIWAY LIGHTS, AND LIGHTED TAXI GUIDANCE SIGNS, THE SAFETY GROUND SHALL BE A #6 AWG BARE COPPER CONDUCTOR CONNECTED TO THE GROUND LUG ON THE RESPECTIVE L-867 TRANSFORMER BASE/LIGHT CAN OR MOUNTING STAKE AND A 5/8" DIAMETER, 8'-0" LONG (MINIMUM) U.L. LISTED COPPER CLAD GROUND ROD.

REIL BASE DETAIL

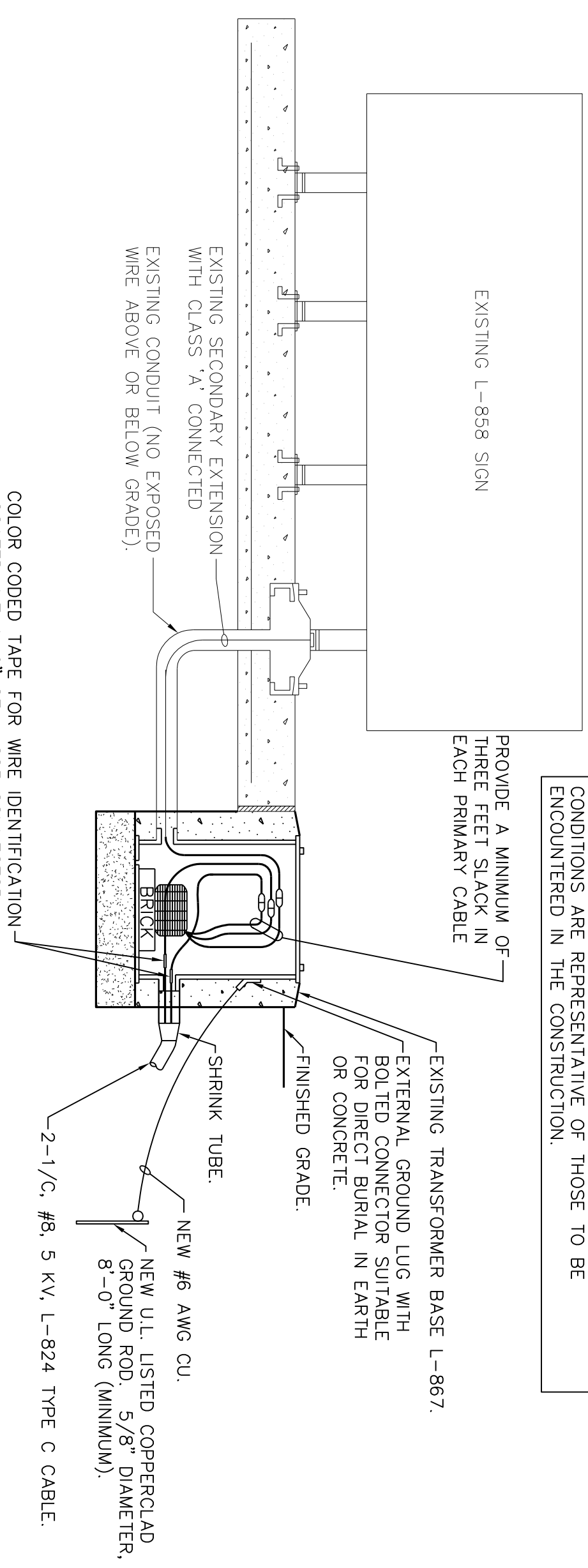
SCALE: NONE



NOTE:
FOR THE PURPOSE OF ENHANCING SAFETY, EACH BASE MUST HAVE INSTALLED, BY THE MANUFACTURER, AN INTERNAL AND EXTERNAL GROUND STRAP THAT IS AVAILABLE FOR THE PURPOSE OF ATTACHING A GROUND LUG THAT IS CONNECTED TO THE GROUND LUG ON THE RESPECTIVE L-867 TRANSFORMER BASE/LIGHT CAN OR MOUNTING STAKE AND A 5/8" DIAMETER, 8'-0" LONG (MINIMUM) U.L. LISTED COPPER CLAD GROUND ROD. THIS REQUIREMENT IS MANDATORY PER FAA AC 150/5345-42F.

SIGN AND TRANSFORMER DETAIL

SCALE: NONE



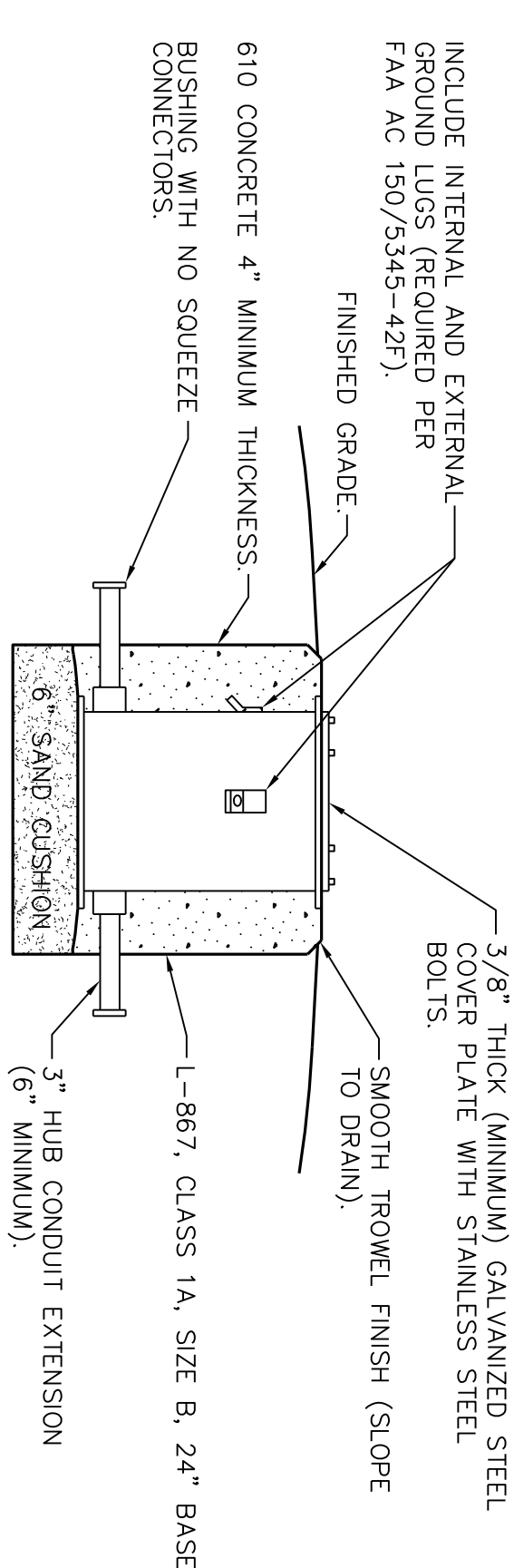
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GENERAL NOTES:

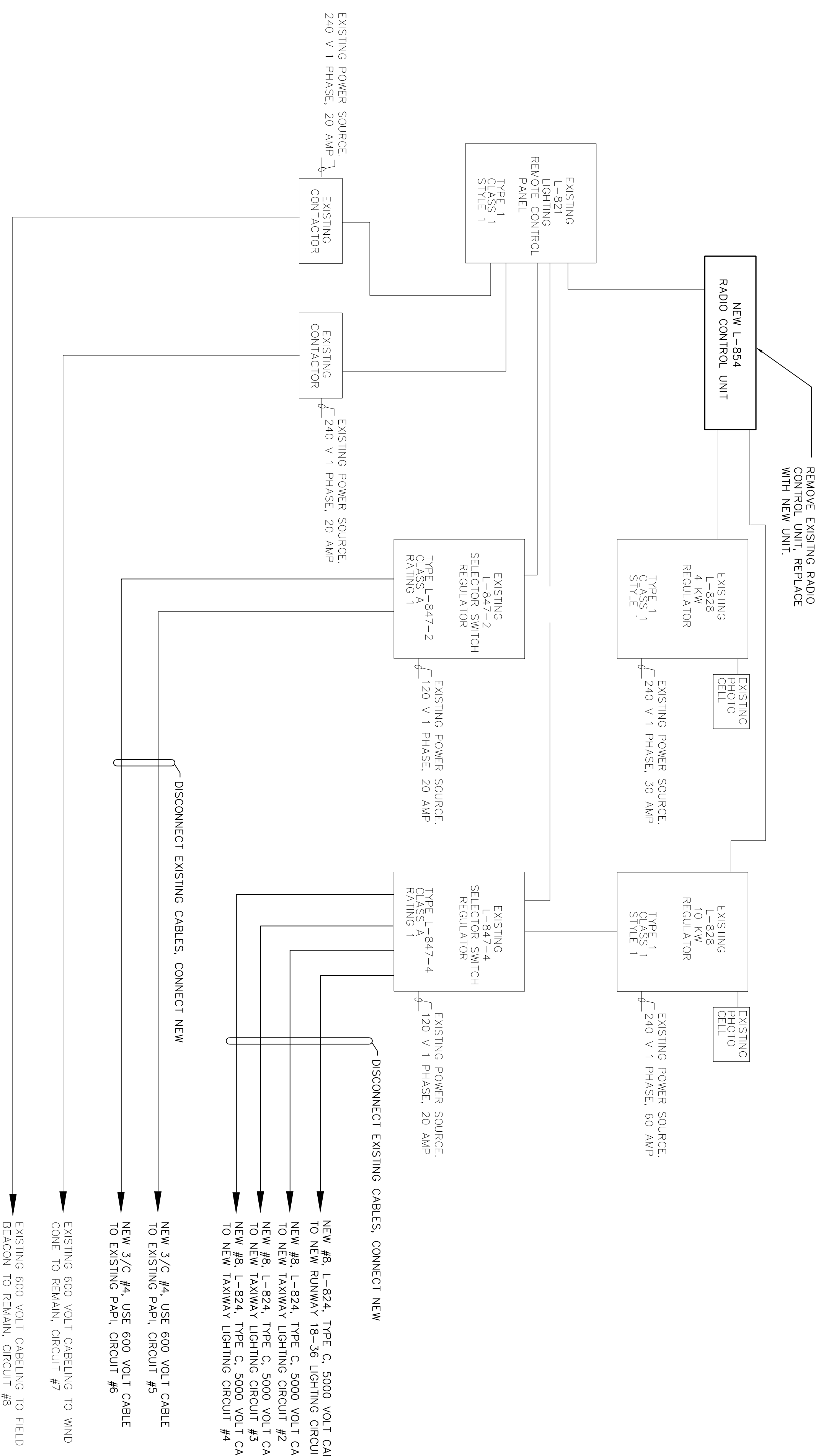
1. SEE LIGHTING LAYOUT SHEET FOR SIGN LEGEND, LOCATION, TYPE, SIZE, STYLE, AND GLASS.
2. SEE SHEET 2 FOR ELECTRICAL NOTES.

TRANSFORMER BASE/SPLICE CAN DETAIL

SCALE: NONE



INCLUDE INTERNAL AND EXTERNAL GROUND LUGS (REQUIRED PER FAA AC 150/5345-42F).



REMOVE EXISTING RADIO CONTROL UNIT, REPLACE WITH NEW UNIT.

EXISTING L-828 REGULATOR (4 KW)

EXISTING L-828 REGULATOR (10 KW)

EXISTING L-847-2 SELECTOR SWITCH (RATING 1)

EXISTING L-847-4 SELECTOR SWITCH (RATING 1)

EXISTING POWER SOURCE (240 V 1 PHASE, 30 AMP)

EXISTING POWER SOURCE (240 V 1 PHASE, 20 AMP)

EXISTING POWER SOURCE (240 V 1 PHASE, 20 AMP)

EXISTING POWER SOURCE (240 V 1 PHASE, 20 AMP)

DISCONNECT EXISTING CABLES, CONNECT NEW

DISCONNECT EXISTING CABLES, CONNECT NEW

NEW #8, L-824, TYPE C, 5000 VOLT CABLE LOOPED TO NEW RUNWAY 18-36 LIGHTING CIRCUIT #1

NEW #8, L-824, TYPE C, 5000 VOLT CABLE LOOPED TO NEW TAXIWAY LIGHTING CIRCUIT #2

NEW #8, L-824, TYPE C, 5000 VOLT CABLE LOOPED TO NEW TAXIWAY LIGHTING CIRCUIT #3

NEW #8, L-824, TYPE C, 5000 VOLT CABLE LOOPED TO NEW TAXIWAY LIGHTING CIRCUIT #4

NEW 3/C #4, USE 600 VOLT CABLE TO EXISTING PAPI, CIRCUIT #5

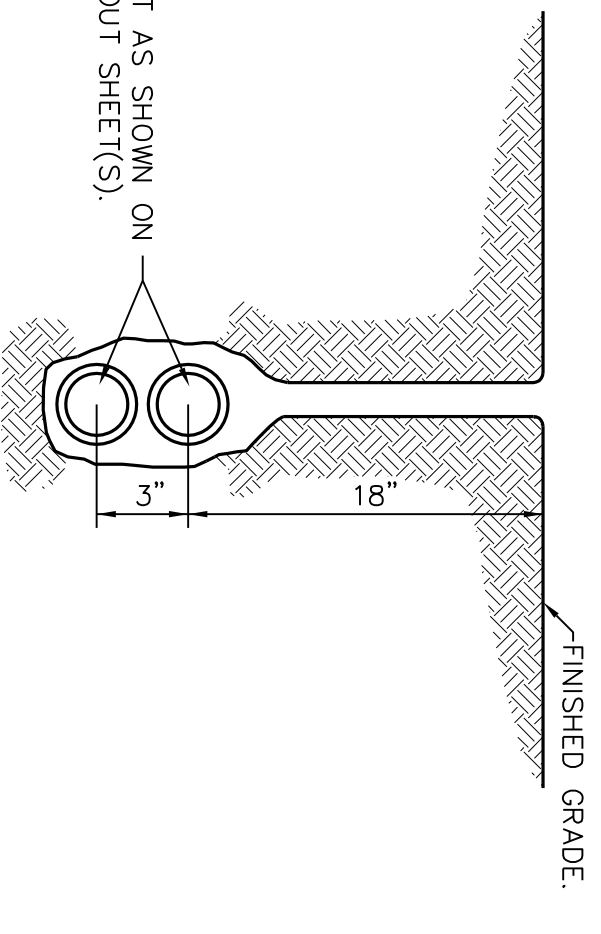
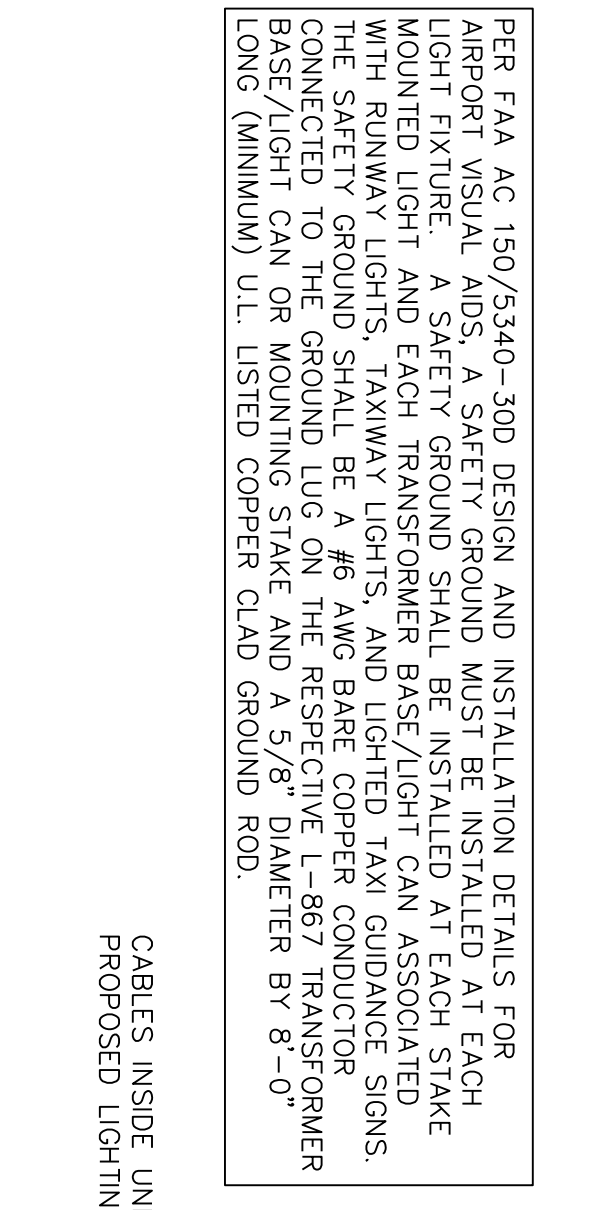
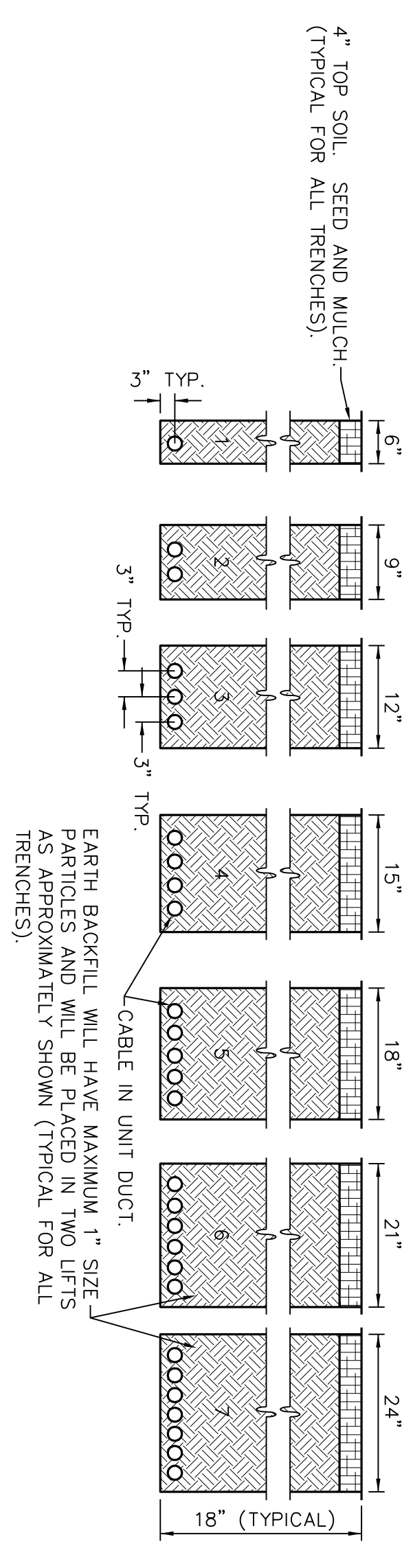
NEW 3/C #4, USE 600 VOLT CABLE TO EXISTING PAPI, CIRCUIT #6

EXISTING 600 VOLT CABELING TO WIND CONE TO REMAIN, CIRCUIT #7

EXISTING 600 VOLT CABELING TO FIELD BEACON TO REMAIN, CIRCUIT #8

LIGHTING CONTROL DIAGRAM

SCALE: NONE



NOTE:
AFFIX NON-CORROSIVE TAG TO FIXTURE FACING RUNWAY WITH SET SCREW, WIRE TIE, OR METAL BAND. NUMERALS SHALL BE ENGRAVED FOR PERMANENT READABILITY.

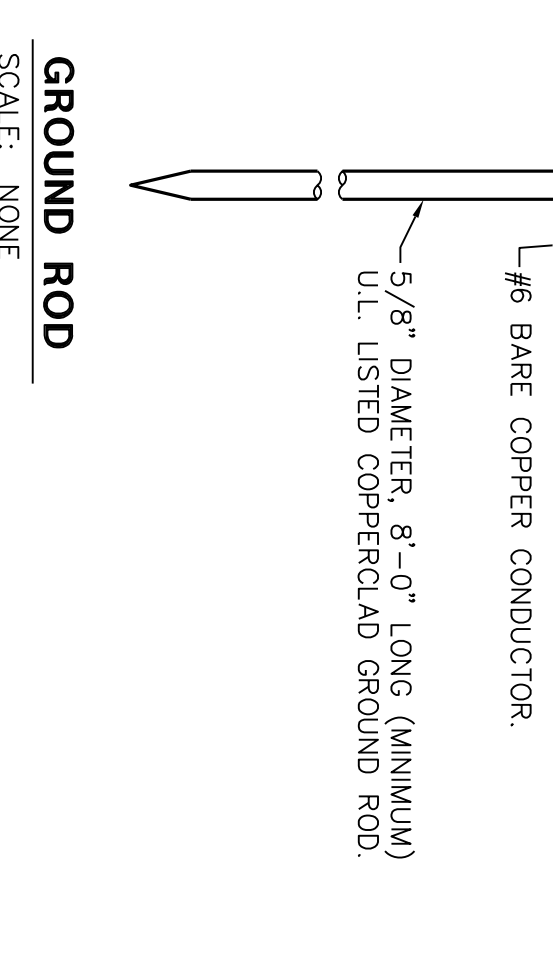
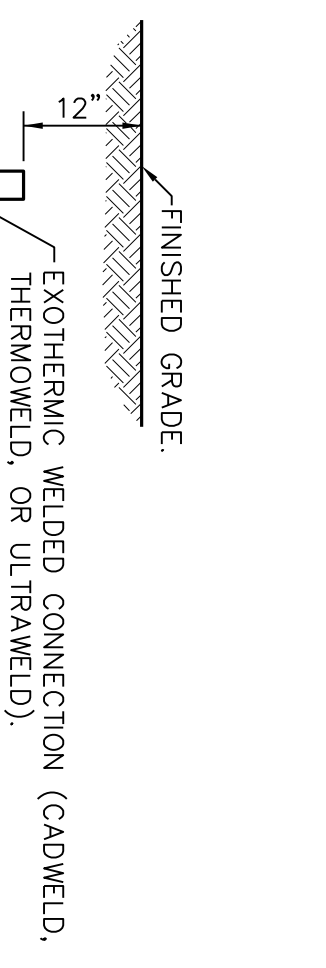
CABLE TRENCHES

SCALE: NONE

PLOWED CABLE

SCALE: NONE

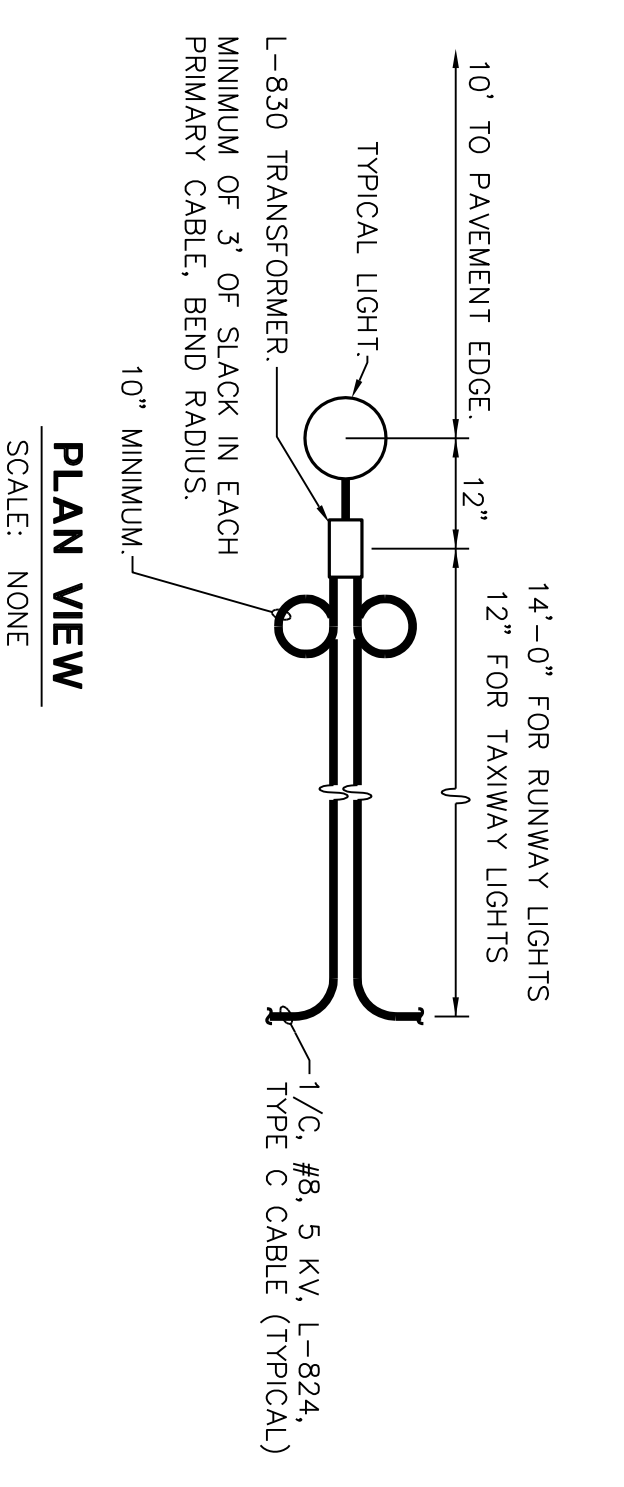
- NOTES:
1. DETAIL NUMBERS INDICATE NUMBER OF CABLES.
 2. TRENCHES WITH MORE THAN SEVEN CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
 3. DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
 4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COSTS INCIDENTAL TO TRENCH.



- NOTES:
1. TYPE AND MINIMUM NUMBER OF GROUND RODS SHALL BE AS SPECIFIED ON THE PLAN.
 2. THE RESISTANCE TO GROUND OF THE GROUNDING SYSTEM SHALL NOT EXCEED 25 OHMS.
- COST OF GROUND RODS IS INCIDENTAL TO THE ASSOCIATED ITEMS REQUIRING GROUNDING UNLESS OTHERWISE SPECIFIED.

PROFILE VIEW

SCALE: NONE

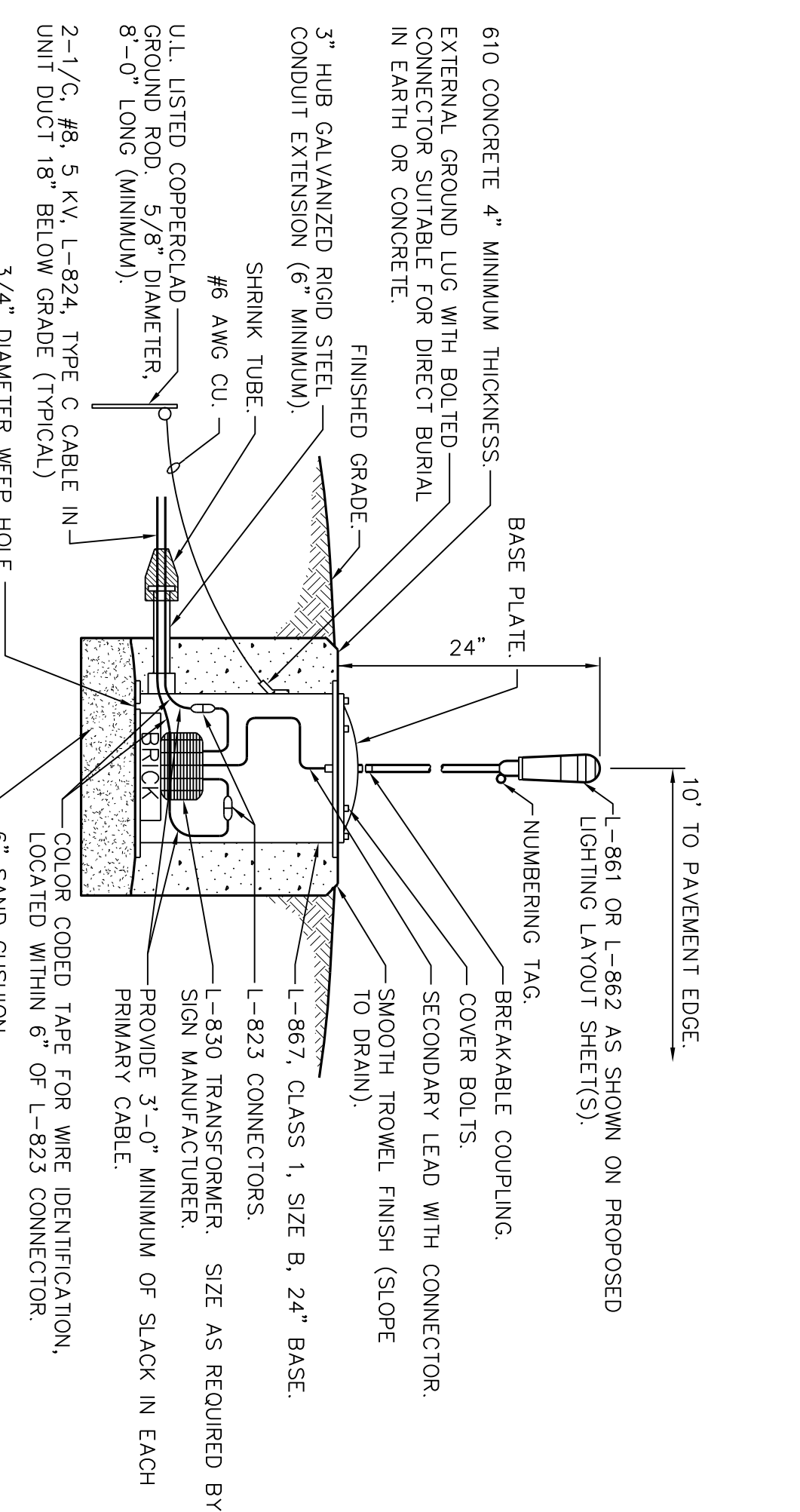
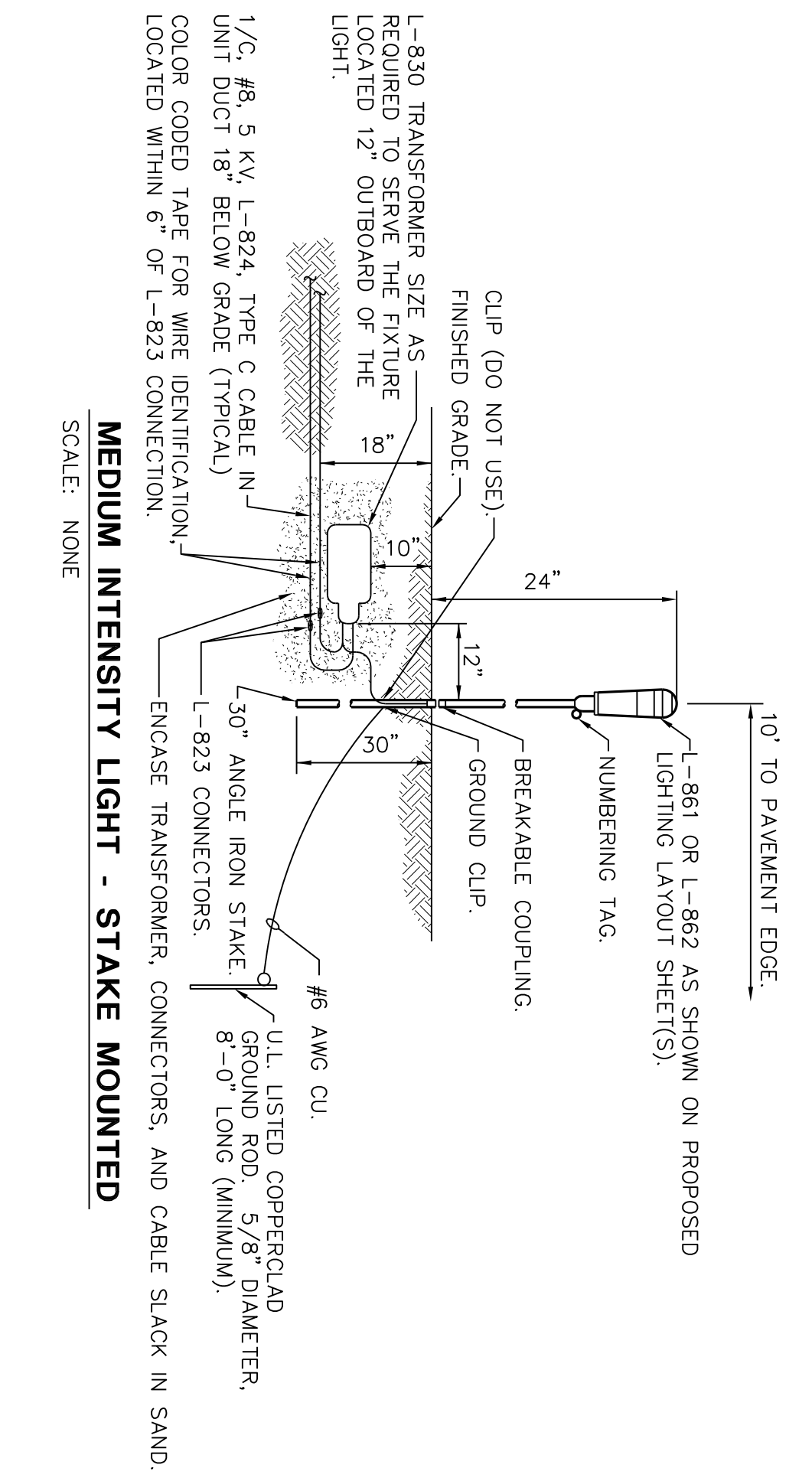


PLAN VIEW

SCALE: NONE

LIGHT AND CABLE INSTALLATION DETAIL

SCALE: NONE

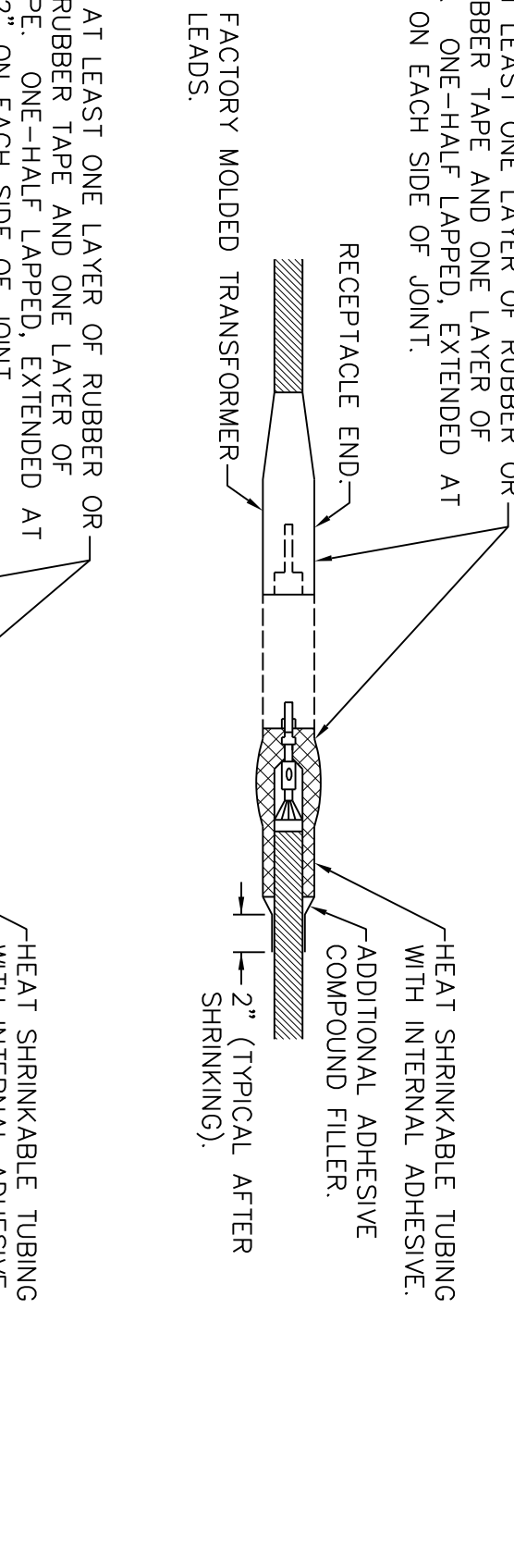
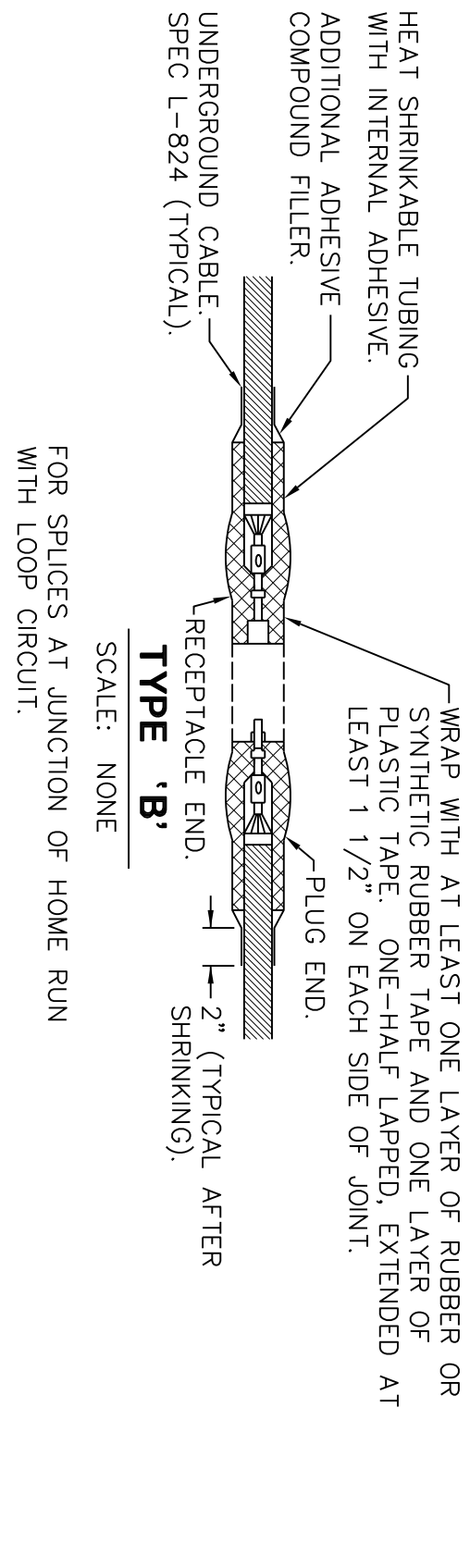
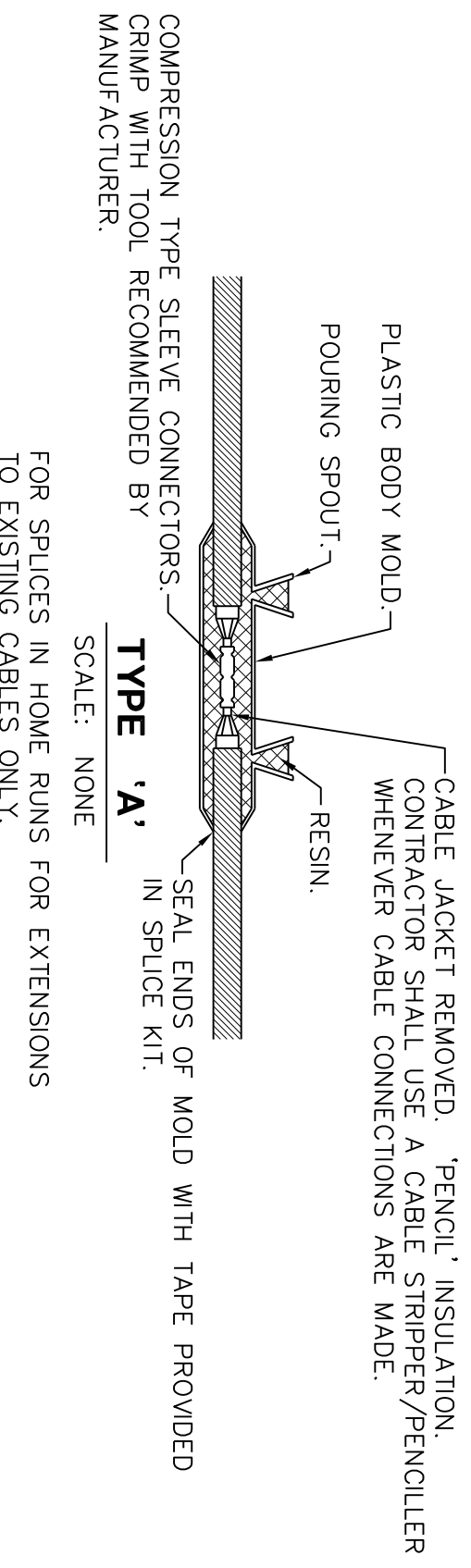


MEDIUM/HIGH INTENSITY LIGHT - BASE MOUNTED

SCALE: NONE

MEDIUM INTENSITY LIGHT - STAKE MOUNTED

SCALE: NONE



CABLE SPLICES

SCALE: NONE

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