

CONSTRUCTION PLANS FOR TAYLORVILLE MUNICIPAL AIRPORT

CITY OF TAYLORVILLE TAYLORVILLE, ILLINOIS

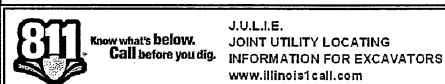
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ILLINOIS PROJECT TAZ - 3976 A. I. P. PROJECT 3-17-0100-B12

REHABILITATE T-HANGAR PAVEMENT PHASE 1

JUNE 4, 2010

ITEM NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
AR152480	SHOULDER ADJUSTMENT	S.Y.	1200
AR152511	SUBGRADE REPAIR	S.Y.	2200
AR152540	SOIL STABILIZATION FABRIC	S.Y.	2200
AR156540	RIPRAP	S.Y.	35
AR209608	CRUSHED AGG. BASE COURSE - 8"	S.Y.	2200
AR401611	BIT. SURFACE COURSE-METHOD I	TON	1450
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	10550
AR510900	REMOVE TIE DOWN	EACH	49
AR602510	BITUMINOUS PRIME COAT	GAL	1000
AR603510	BITUMINOUS TACK COAT	GAL	1050
AR620510	PAVEMENT MARKING	S.F.	2900
AR901510	SEEDING	ACRE	0.25
AR908520	EXCELSIOR BLANKET	S.Y.	900



THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH UTILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

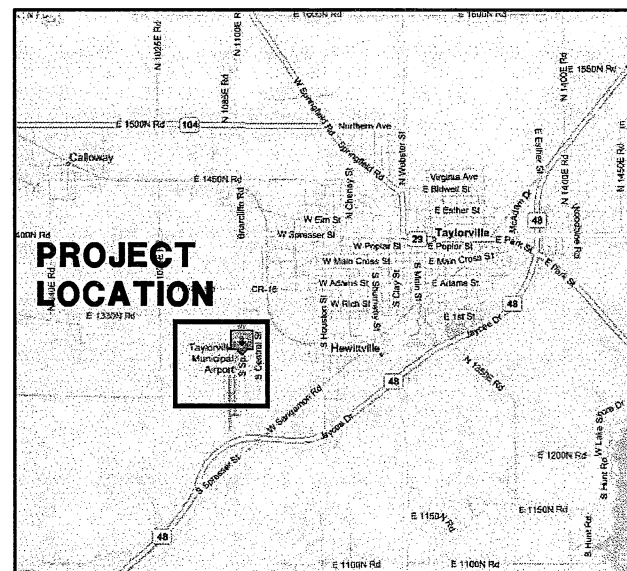
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DESIGN INFORMATION
GEOMETRIC CRITERIA
AIRPLANE DESIGN GROUP II

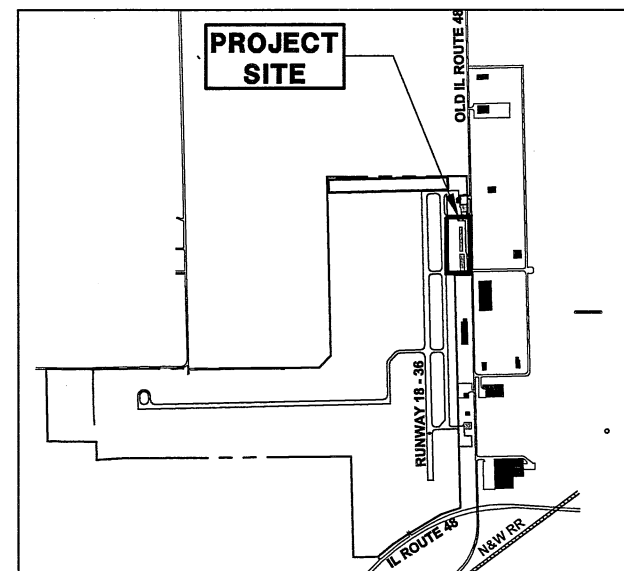
PAVEMENT DESIGN CRITERIA
AIRCRAFT SINGLE WHEEL GEAR
DEPARTURE WEIGHT = 12,500 LBS.
100 ANNUAL DEPARTURES

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 13 NORTH
RANGE: 2 WEST OF THE 4TH P.M.
SECTION: 32
COUNTY: CHRISTIAN
CIVIL TOWNSHIP: TAYLORVILLE



LOCATION MAP



SITE PLAN

4-16-10

 Christopher B. Grate
 Exp 11-30-11

**CITY OF TAYLORVILLE
TAYLORVILLE MUNICIPAL AIRPORT
TAYLORVILLE, ILLINOIS**

APPROVED *Greg Britherton*
MAYOR
DATE 4/15/10
APPROVED *Pamela J. Peabody*
CITY CLERK
DATE 4/15/10

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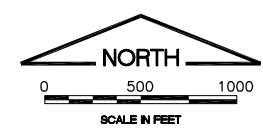
SUBMITTED BY *Christopher B. Grate*
DATE 4-16-10

CMT JOB NUMBER 09050-01

GENERAL NOTES

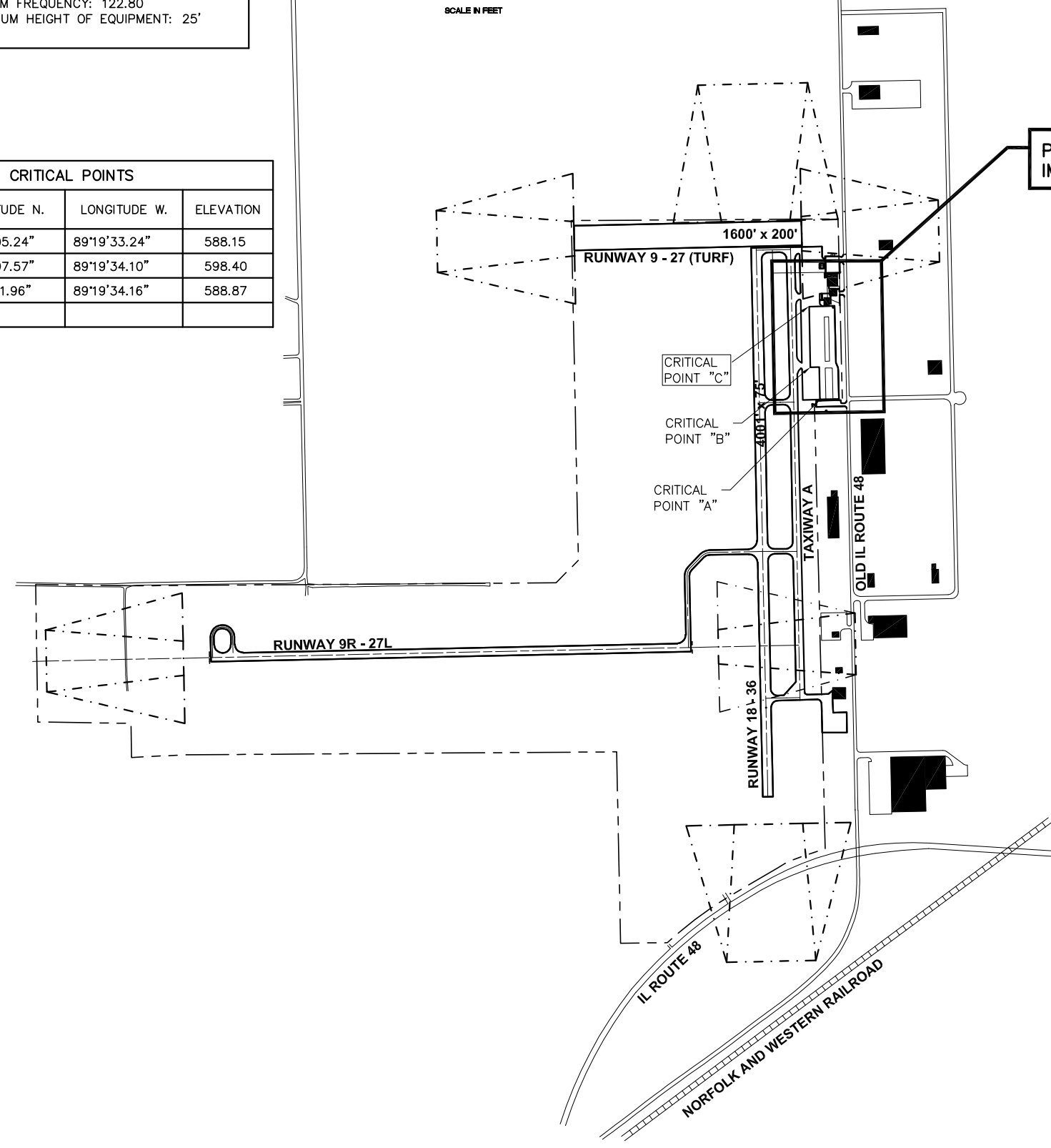
- 1.) ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- 2.) ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF FLAGMAN IN RADIO CONTACT WITH AIR TRAFFIC. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO ARE FAMILIAR WITH AIRCRAFT OPERATIONS.
- 3.) WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT MANAGER.
- 4.) THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- 5.) BROKEN CONCRETE AND ASPHALT SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY.
- 6.) VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 40' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 200' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- 7.) ALL PAVEMENTS, DRIVES, OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT MANAGER. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- 8.) EXISTING TURF AREAS OUTSIDE THE GRADING LIMITS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT MANAGER.
- 9.) THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
- 10.) IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES WITHIN THE PROJECT SITE SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- 11.) CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A.) THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN.
 - B.) THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
 - C.) THE CONTRACTOR SHALL USE AN EXISTING GATE FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER AND AIRPORT MANAGEMENT. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE RESIDENT ENGINEER.
 - D.) CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL TRAFFIC CONTROL SIGNAGE REQUIRED BY THE COUNTY, TOWNSHIP, OR I.D.O.T. .
 - E.) DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F.) THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS.
 - G.) THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H.) ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I.) THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY, TAXIWAY SAFETY AREAS, AND OBJECT FREE AREAS.
 - J.) THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
 - K.) THE CONTRACTOR MAY USE ALTERNATE AIRPORT GATES TO DELIVER LARGER EQUIPMENT TO THE JOB SITE. THE CONTRACTOR SHALL COORDINATE ARRIVAL AND DEPARTURE OF THESE DELIVERIES WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER.

PROPOSED IMPROVEMENTS



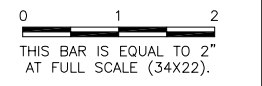
UNICOM FREQUENCY: 122.80
 MAXIMUM HEIGHT OF EQUIPMENT: 25'

CRITICAL POINTS			
POINT	LATITUDE N.	LONGITUDE W.	ELEVATION
A	39°32'05.24"	89°19'33.24"	588.15
B	39°32'07.57"	89°19'34.10"	598.40
C	39°32'11.96"	89°19'34.16"	588.87



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TAYLORVILLE MUNICIPAL AIRPORT
 TAYLORVILLE, ILLINOIS
 REHABILITATE T-HANGAR PAVEMENT
 SITE PLAN

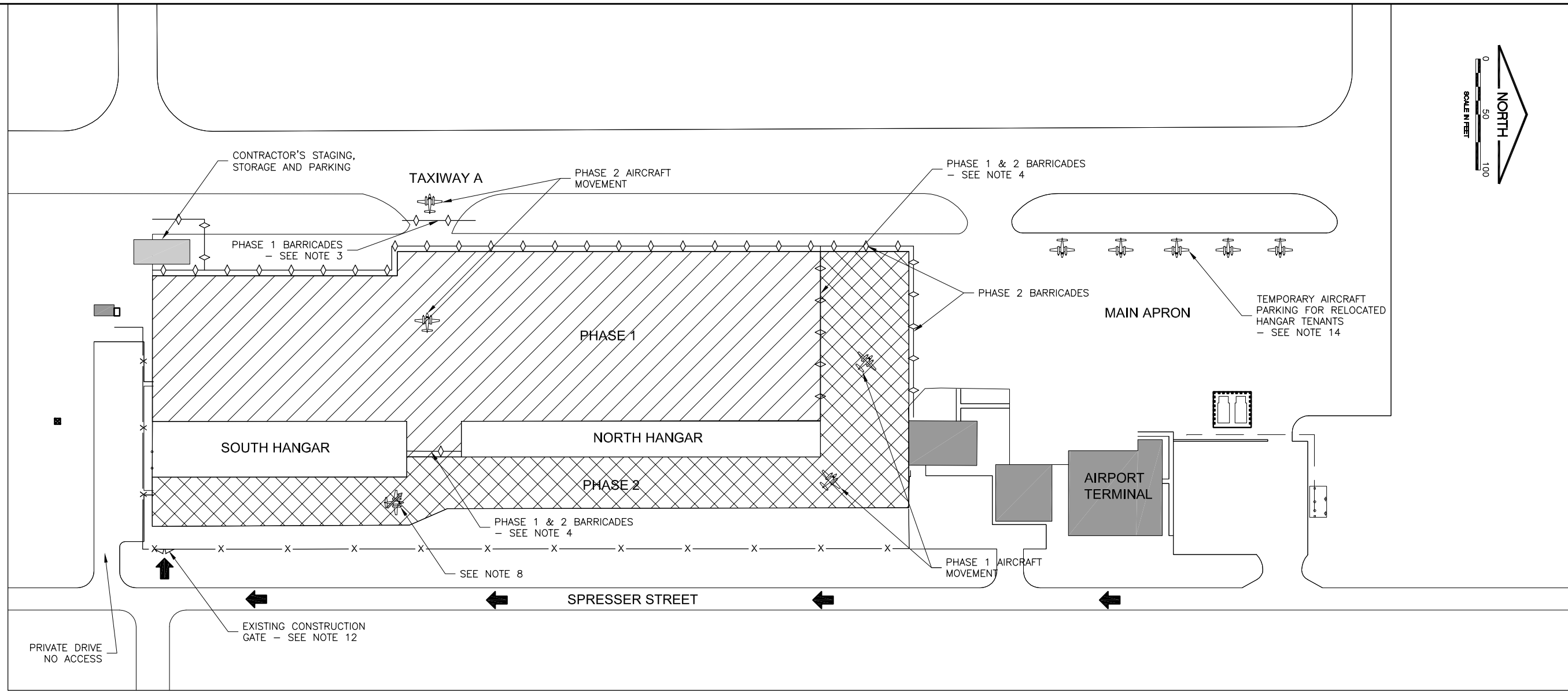
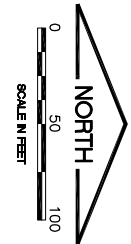
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DATE:	06-04-2010
JOB No:	09050-01
ILL. PROJECT TAZ-3976	
A.I.P. PROJECT 3-17-0100-B12	
SHEET 02 OF 13 SHEETS	

TA003

REVISIONS		
NUMBER	BY	DATE

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 AT FULL SCALE (34X22).

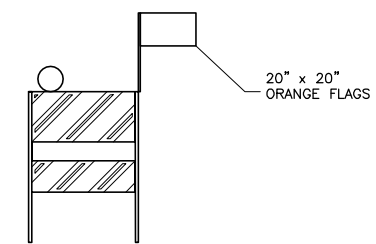


NOTES

1. THE AIRPORT WILL REQUIRE 7 CALENDAR DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING ANY PHASE TO COORDINATE WITH THE TENANTS.
2. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON THESE PHASES.
3. PHASE 1 BARRICADES SHALL BE PLACED FOR THE DURATION OF WORK IN PHASE 1.
4. PHASE 1 & 2 BARRICADES SHALL BE PLACED FOR THE DURATION OF THE PROJECT. THE CONTRACTOR MAY ADJUST THE LOCATION OF THE BARRICADES 20' TO FACILITATE WORK BEING DONE AT THE LOCATION OF THE BARRICADES.
5. PHASE 1 & 2 SHALL NOT BE CONSTRUCTED CONCURRENTLY.
6. THE CONTRACTOR WILL BE ALLOWED (14) CALENDAR DAYS TO COMPLETE ALL REMOVALS AND PAVING OPERATIONS IN PHASE 1
7. THE CONTRACTOR WILL BE ALLOWED (7) CALENDAR DAYS TO COMPLETE ALL REMOVALS AND PAVING OPERATIONS IN PHASE 2
8. THE CONTRACTOR'S HAULING OPERATIONS FOR PHASE 1 & 2 WILL CROSS PAVEMENTS THAT WILL REMAIN OPEN TO AIRCRAFT. THE CONTRACTOR SHALL SUSPEND HAULING OPERATIONS AND CLEAN THE PAVEMENT TO ALLOW AIRCRAFT TO TAXI.
9. ALL RUNWAYS, TAXIWAYS, AND MAIN APRON SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION.
10. CONTRACTOR SHALL WORK WITH THE AIRPORT AND THE AIRPORT TENANTS SUCH THAT CONSTRUCTION OPERATIONS TIME WILL NOT EXCESSIVELY DELAY HANGAR ACCESS.
11. IF DURING CONSTRUCTION IT IS FOUND THAT FULLY LOADED TRUCKS ARE EXCESSIVELY DAMAGING THE EXISTING SUBGRADE OR PAVEMENT, THE CONTRACTOR MAY BE REQUIRED TO LIMIT THE WEIGHT OF DELIVERY TRUCKS. THE CONTRACTOR SHALL REPAIR DAMAGE TO THE EXISTING SUBGRADE OR PAVEMENT CAUSED BY FULLY LOADED TRUCKS. COST TO REPAIR THE DAMAGED AREAS SHALL BE INCIDENTAL TO THE PROJECT.
12. THE CONTRACTOR SHALL HAUL ROAD TO SUPPORT ALL WEATHER HAULING OPERATIONS AND RESTORE TO ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT.
13. CONTRACTOR SHALL PROVIDE TEMPORARY TIE DOWNS FOR RELOCATED TENANTS. TEMPORARY TIE DOWNS SHALL MEET THE APPROVAL OF THE AIRPORT AND RESIDENT ENGINEER PRIOR TO BEING INSTALLED.

LEGEND

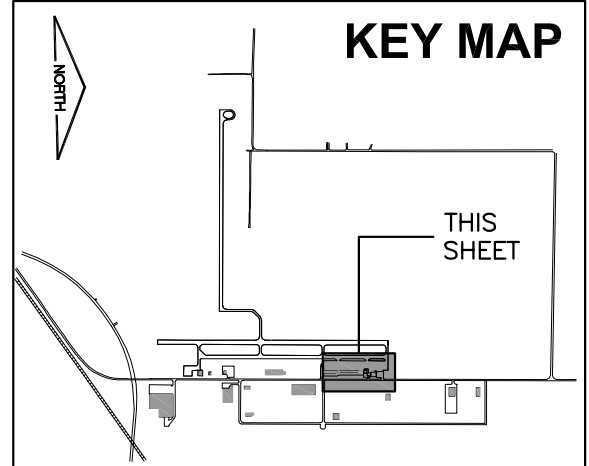
- WORK LIMITS - PHASE 1
- WORK LIMITS - PHASE 2
- CONTRACTOR'S STAGING AND STORAGE
- BARRICADES IDOT TYPE 1 WITH RED LIGHTS & 20"x 20" ORANGE FLAGS AT 20' SPACING
- CONTRACTOR ACCESS ROAD
- AIR TRAFFIC ACCESS
- CONTRACTOR AND AIRCRAFT ACCESS
- EXISTING GATE
- EXISTING FENCE



FLASHER BARRICADE DETAIL
 N.T.S.

FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 20' INTERVALS.



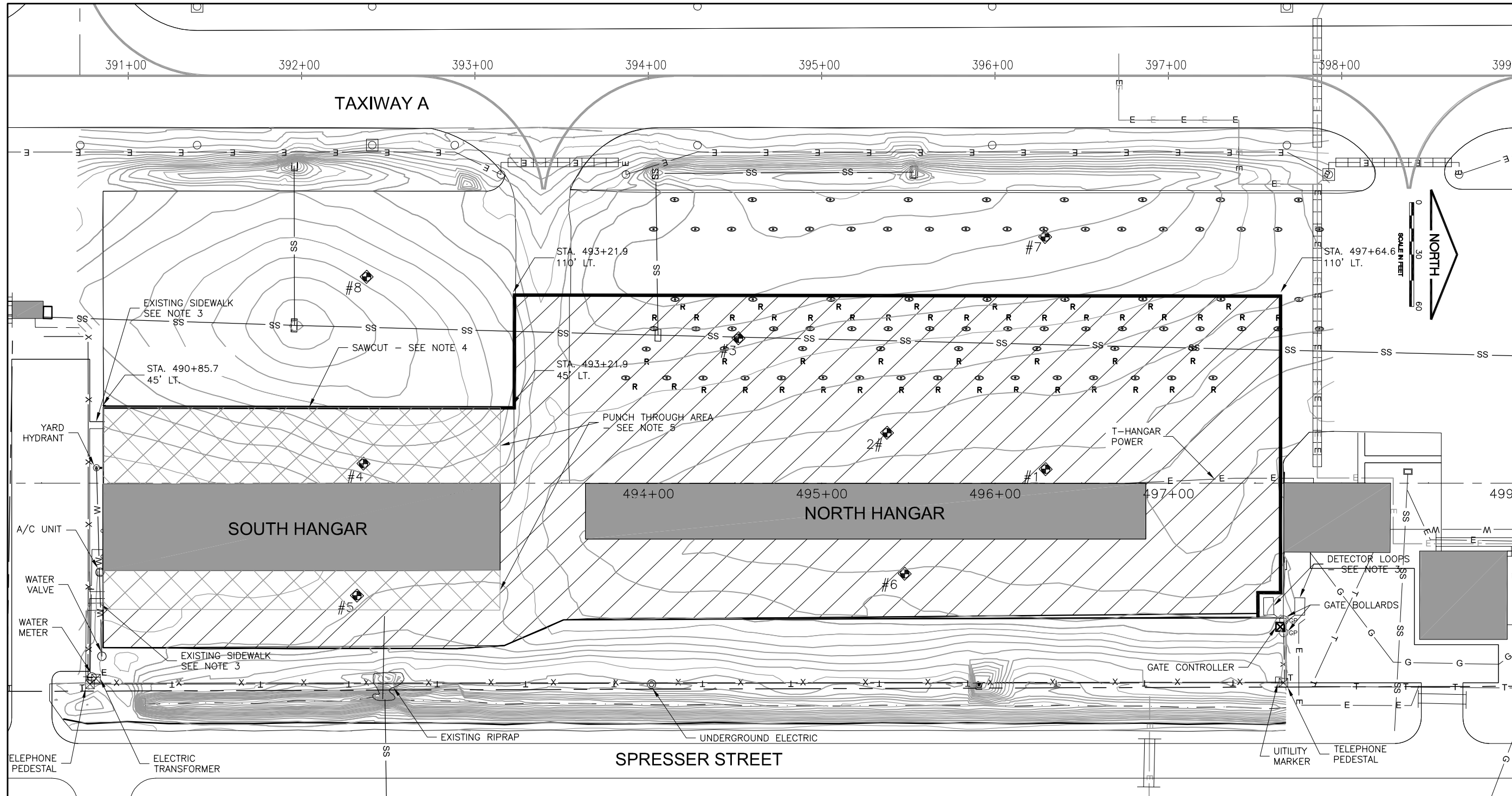
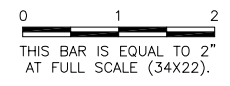
**TAYLORVILLE MUNICIPAL AIRPORT
 TAYLORVILLE, ILLINOIS**
**REHABILITATE T-HANGAR PAVEMENT
 CONSTRUCTION ACTIVITY PLAN**

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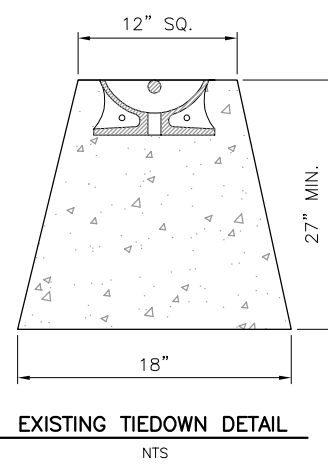


**TAYLORVILLE MUNICIPAL AIRPORT
 TAYLORVILLE, ILLINOIS**

**REHABILITATE T-HANGAR PAVEMENT
 EXISTING CONDITIONS AND REMOVALS**

NOTES

- EXISTING HANGAR BUILDINGS HAVE UTILITY CONNECTIONS, SLIDING DOOR HARDWARE AND DRAINAGE HARDWARE THAT EXTENDS BEYOND THE FOOTPRINT OF THE BUILDING (NOT SHOWN).
- THE MILLING OPERATIONS SHALL REMAIN A FIXED DISTANCE FROM THE FACE OF THE BUILDING WHERE THERE ARE NO BUILDING DOORS. THE RESIDENT ENGINEER WILL DETERMINE THE ACCEPTABLE OFFSET FOR THE MILLING EQUIPMENT. MILLING OPERATIONS AT THE BUILDING DOORS SHALL BE MILLED UP TO THE BUILDING FOUNDATION. MILLING ALONG THE BUILDING FOUNDATION MAY REQUIRE SPECIAL EQUIPMENT CAPABLE MILLING IN CLOSE PROXIMITY OF A VERTICAL FACE OR THE USE OF HAND TOOLS. COSTS ASSOCIATED WITH THIS MILLING SHALL BE INCIDENTAL TO THE MILLING PAY ITEM.
- CONTRACTOR SHALL NOT DAMAGE EXISTING SIDEWALK OR DETECTOR LOOPS. DAMAGE TO SIDEWALK OR DETECTOR LOOPS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
- SAWCUT SHOWN SHALL BE INCIDENTAL TO MILLING OPERATIONS. ADDITIONAL AREAS NOT SHOWN MAY REQUIRE SAWCUT. NO ADDITIONAL PAYMENTS WILL BE MADE FOR SAWCUT.
- PUNCH THROUGH AREA SHOWN HAVE BEEN APPROXIMATED BASED OFF OF PAVEMENT CORES AND PROPOSED PAVEMENT GRADES. ACTUAL PUNCH THROUGH LIMITS MAY VARY.

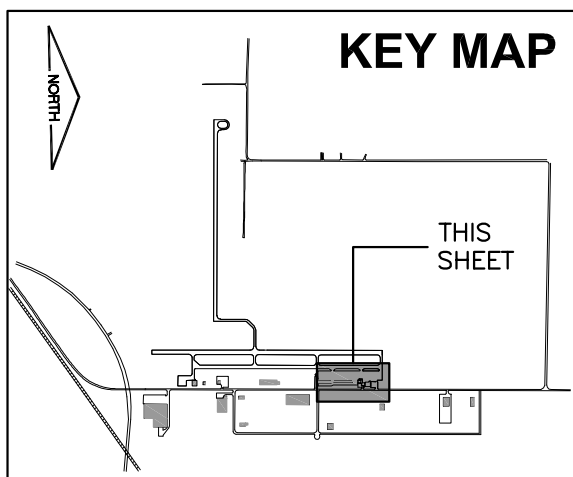


LEGEND

	PAVEMENT MILLING
	EXISTING PAVEMENT
	EXISTING ELECTRIC
	EXISTING GAS
	EXISTING WATER
	EXISTING STORM SEWER
	EXISTING TELEPHONE
	EXISTING DUCT
	EXISTING TIEDOWN
	REMOVAL
	SOIL BORING

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.



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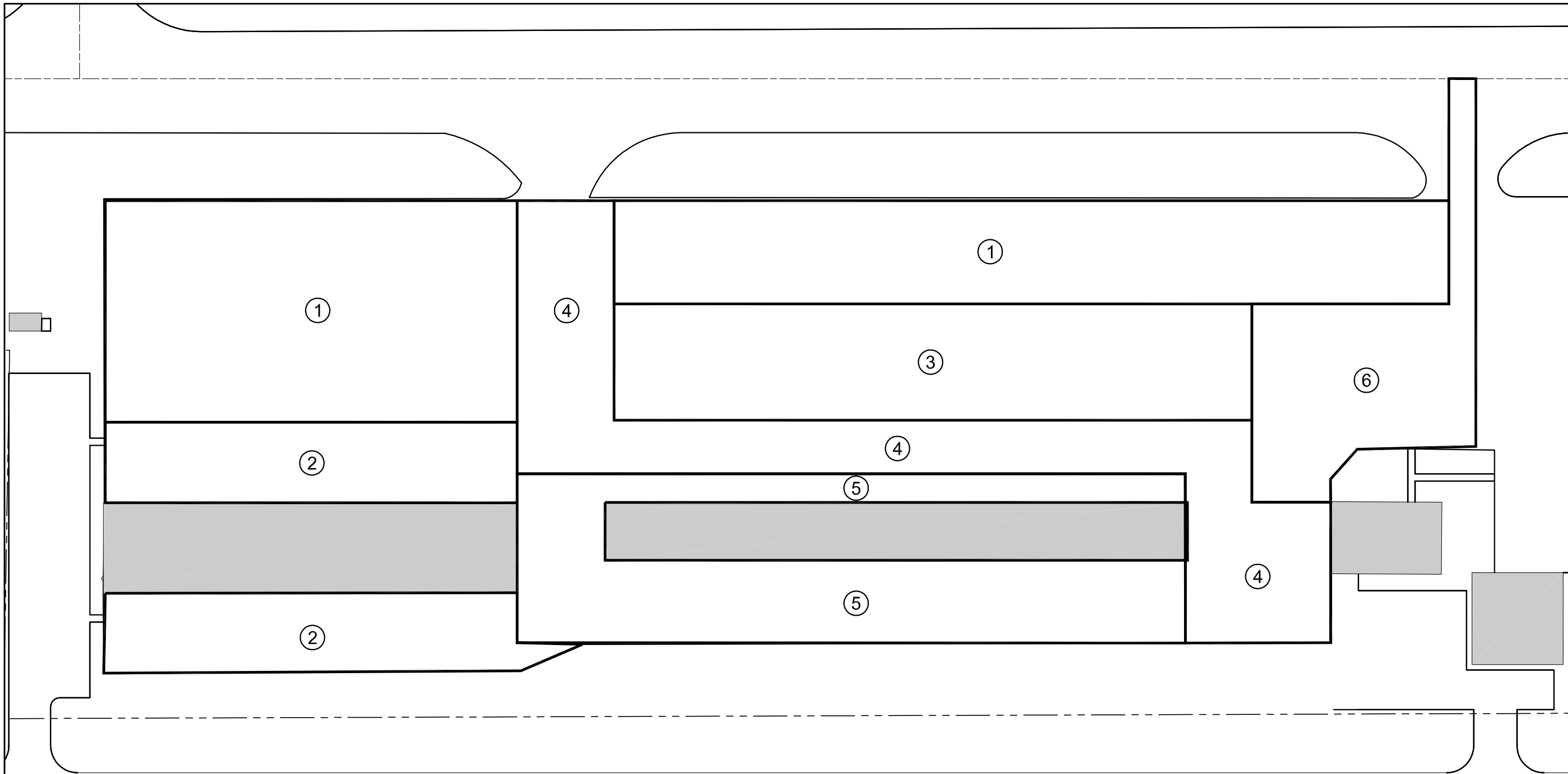


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SHEET 04 OF 13 SHEETS	

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TAYLORVILLE MUNICIPAL AIRPORT
 TAYLORVILLE, ILLINOIS

REHABILITATE T-HANGAR PAVEMENT
 EXISTING PAVEMENT STRUCTURE

EXISTING PAVEMENT

- | | |
|--|--|
| <p>① 4" BITUMINOUS SURFACE COURSE (401)
 6" CRUSHED AGGREGATE BASE COURSE (209)
 8" AGGREGATE BASE COURSE (208)</p> <p>② 4" AND VARIABLE BITUMINOUS SURFACE COURSE (401)
 4" AGGREGATE BASE COURSE (208)</p> <p>③ 4" AND VARIABLE BITUMINOUS SURFACE COURSE (401)
 2" BITUMINOUS BASE COURSE (201)
 6" AGGREGATE BASE COURSE (208)</p> | <p>④ 4" AND VARIABLE BITUMINOUS SURFACE COURSE (401)
 7" AGGREGATE BASE COURSE (208)</p> <p>⑤ 2" AND VARIABLE BITUMINOUS SURFACE COURSE (401)
 7" AGGREGATE BASE COURSE (208)</p> <p>⑥ 6" AND VARIABLE BITUMINOUS SURFACE COURSE (401)
 6" AND VARIABLE SOIL CEMENT BASE</p> |
|--|--|

NOTES

1. PAVEMENT STRUCTURE AND AREAS SHOWN HAVE BEEN TAKEN FROM RECORD DRAWINGS.

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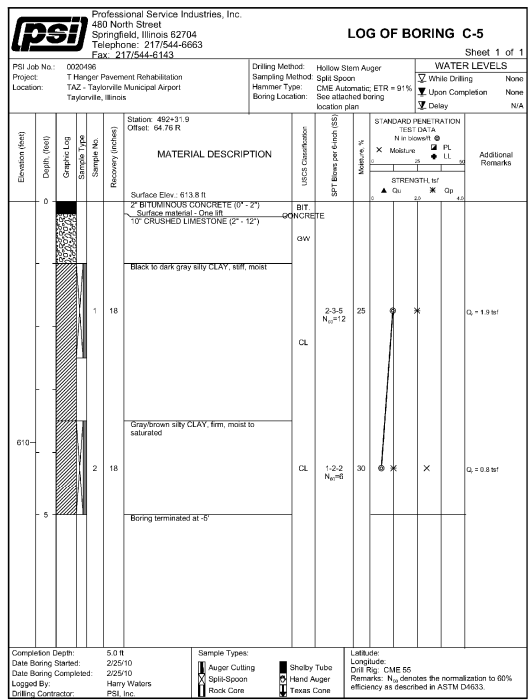
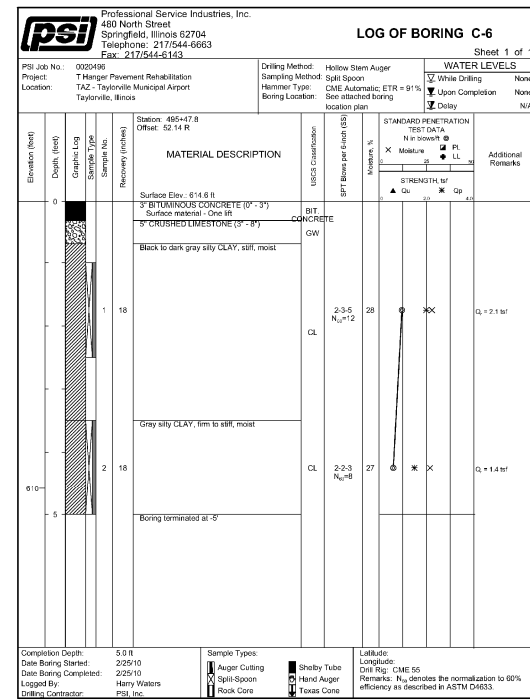
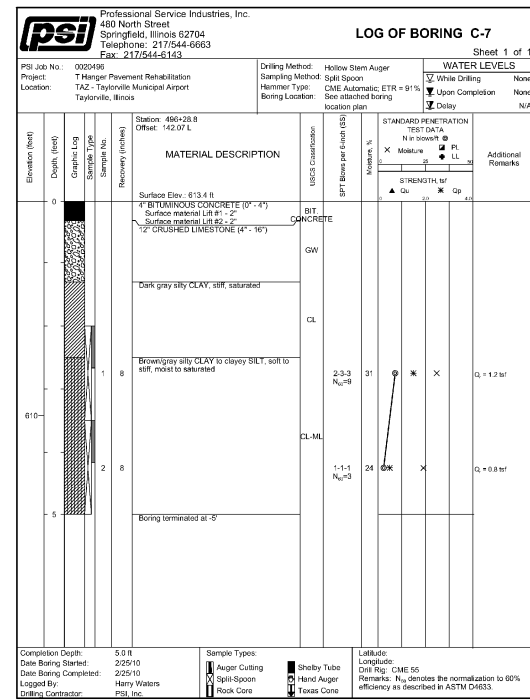
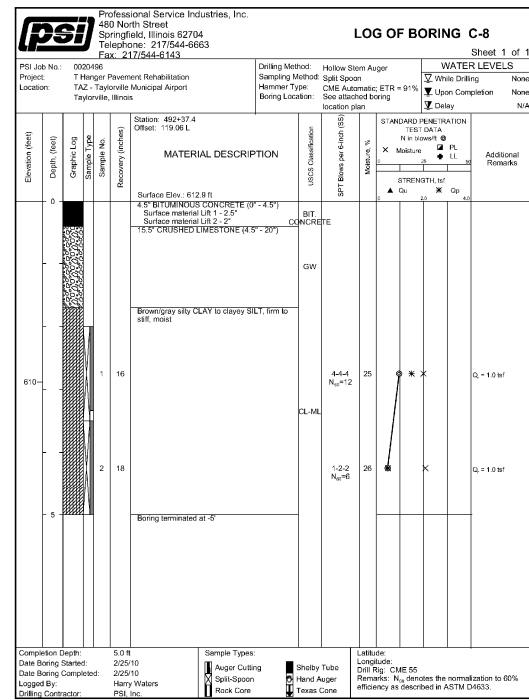
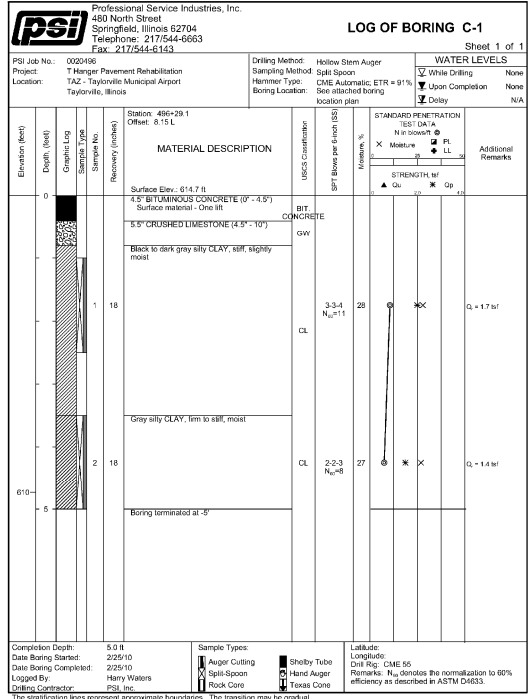
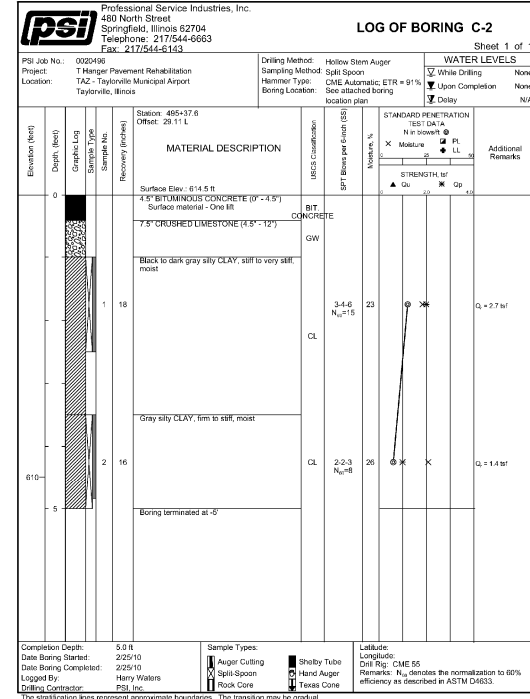
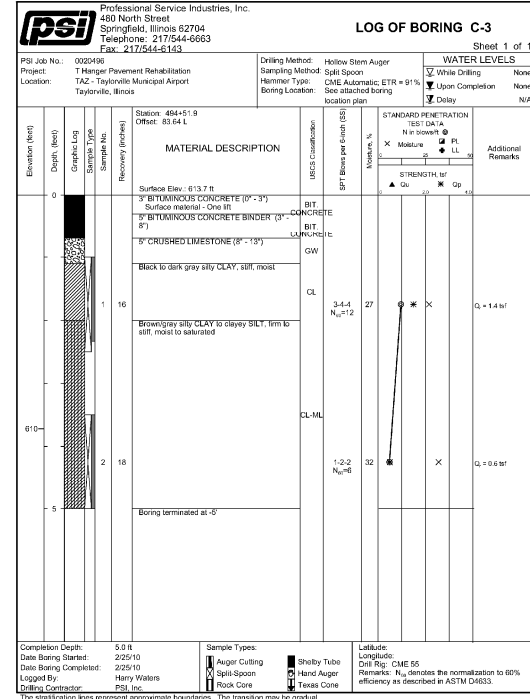
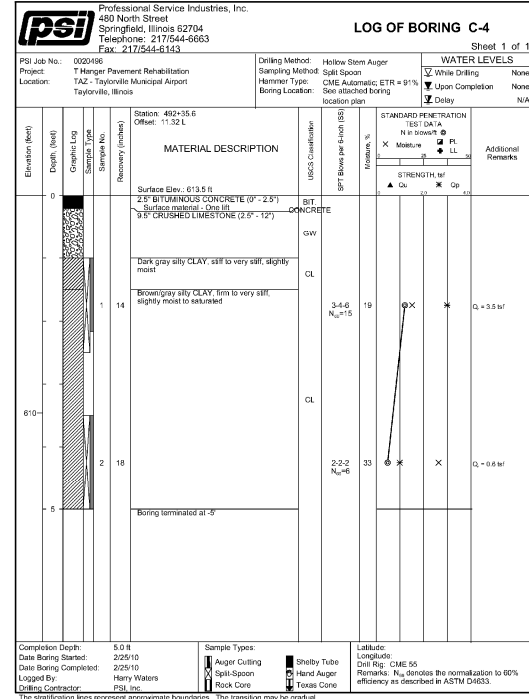
REHABILITATE T-HANGAR PAVEMENT
 BORING LOGS

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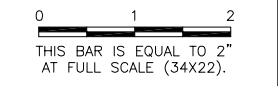


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SHEET	06 OF 13 SHEETS



TA003

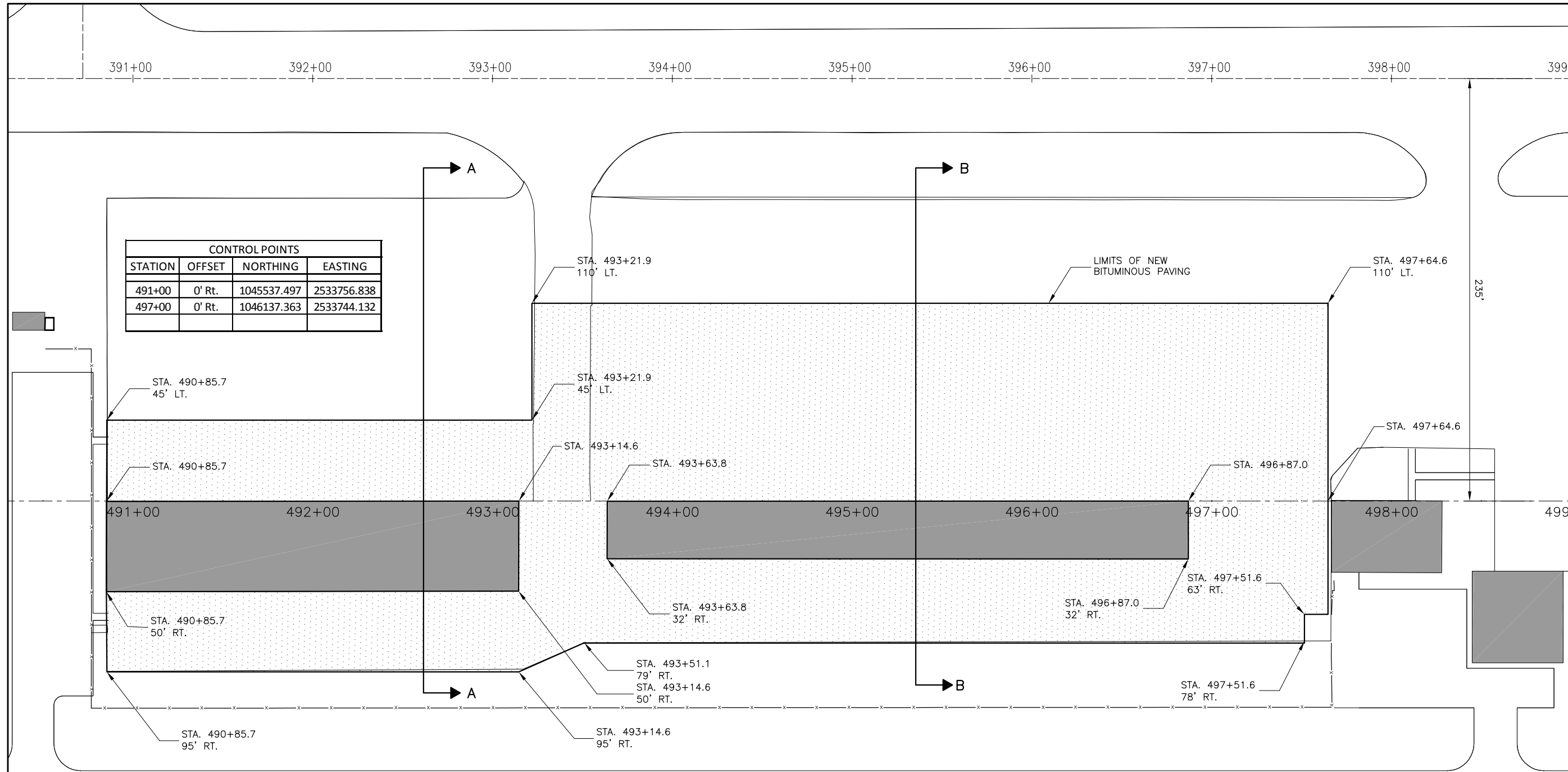
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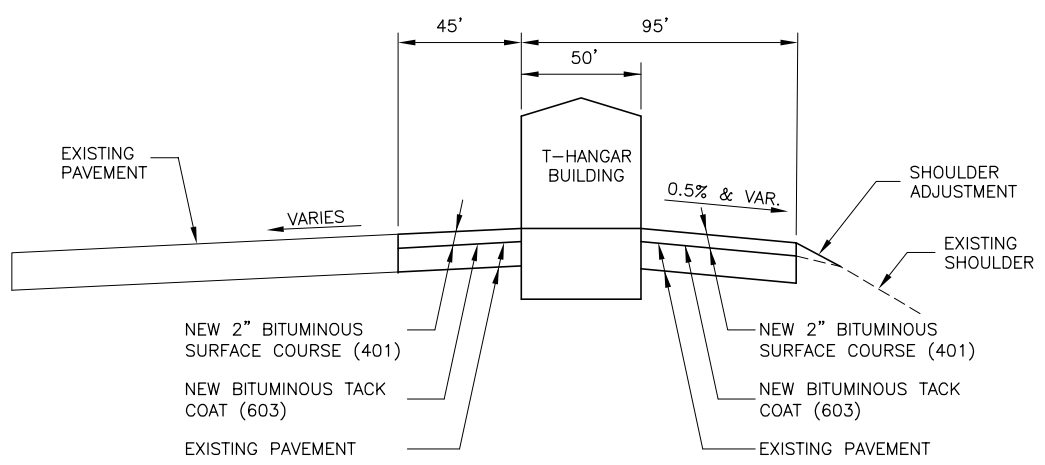
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 TAYLORVILLE, ILLINOIS**
**REHABILITATE T-HANGAR PAVEMENT
 PROPOSED IMPROVEMENTS**

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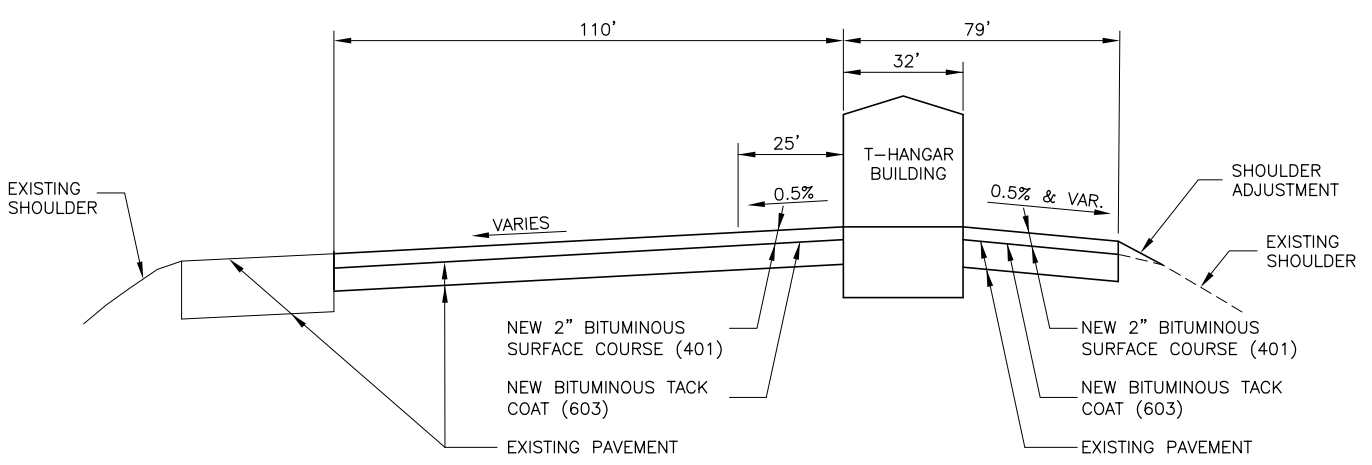
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SHEET 07 OF 13 SHEETS	



CONTROL POINTS			
STATION	OFFSET	NORTHING	EASTING
491+00	0' Rt.	1045537.497	2533756.838
497+00	0' Rt.	1046137.363	2533744.132



(A) HANGAR APRON PAVEMENT (A)
 NTS

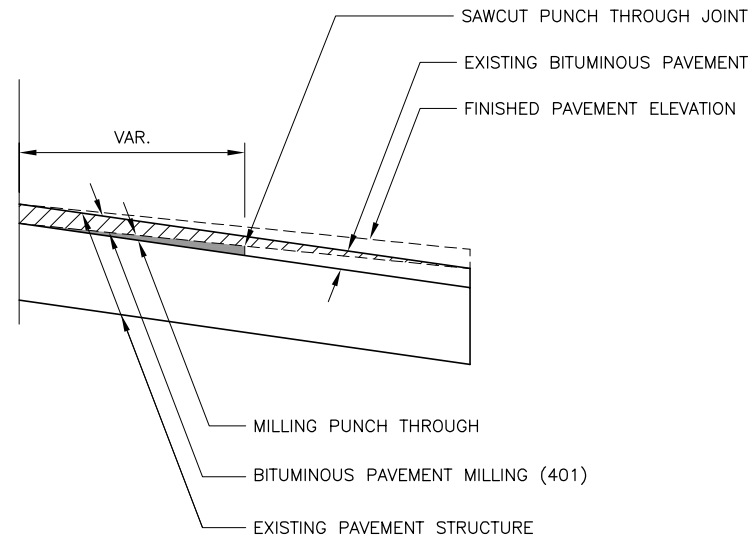


(B) HANGAR APRON PAVEMENT (B)
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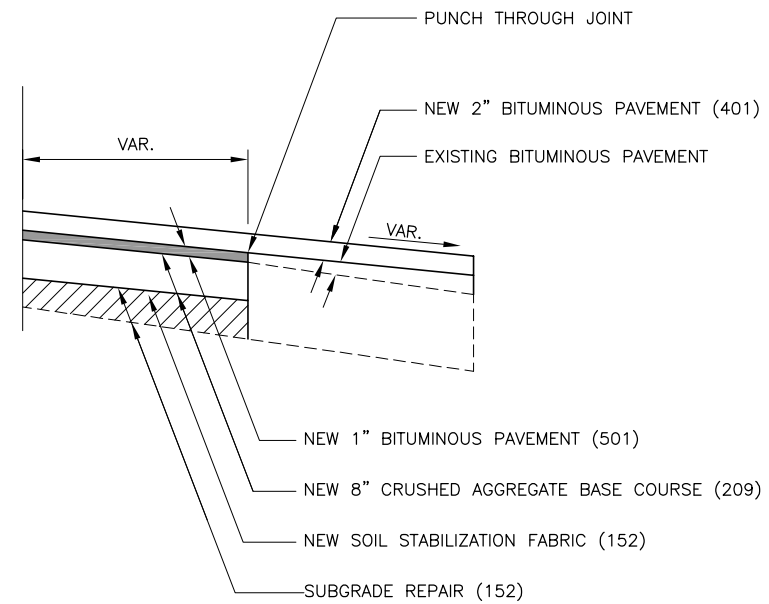


MILLING PUNCH THROUGH AREA

N.T.S.

NOTES

1. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER OF AREAS OF PAVEMENT MILLING THAT MAY REQUIRE REPAIR.
2. PRIOR TO BEGINNING SUBGRADE REPAIR, THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL AGREE ON THE LIMITS OF THE REPAIR.
3. THE CONTRACTOR SHALL SAW CUT THE FACE OF THE PUNCH THROUGH JOINT.

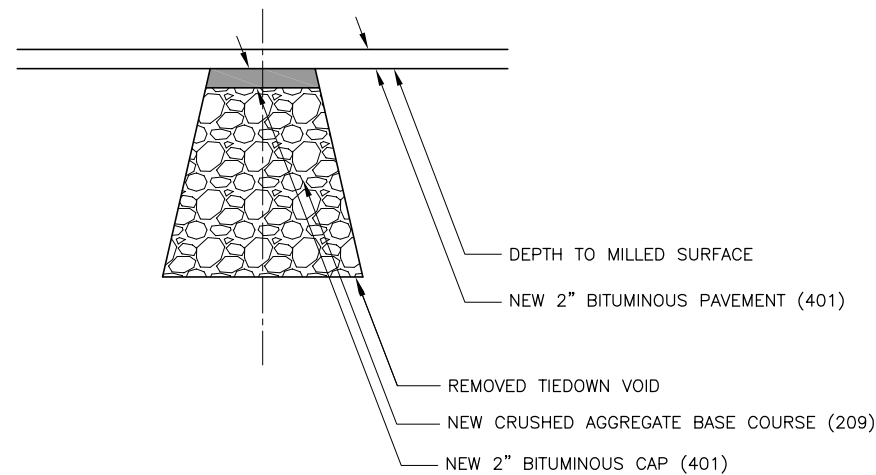


MILLING PUNCH THROUGH REPAIR

N.T.S.

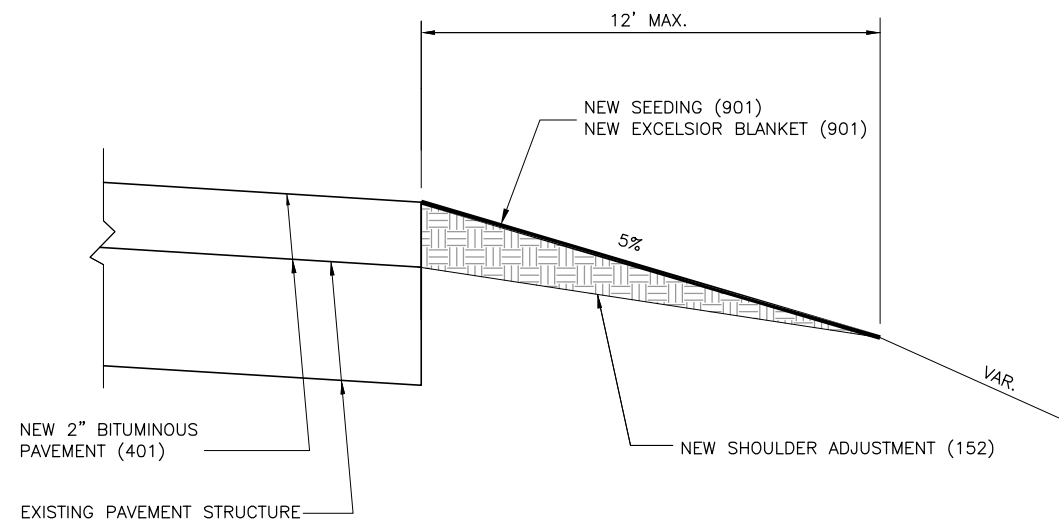
NOTES

1. CRUSHED AGGREGATE BASE COURSE (209) SHALL BE PLACED IN A MANNER APPROVED BY THE RESIDENT ENGINEER.
2. SUBGRADE REPAIR SHALL INCLUDE EXCAVATION REQUIRED TO CONSTRUCT THE AGGREGATE BASE COURSE.
3. SUBGRADE REPAIR SHALL INCLUDE COMPACTING THE SUBGRADE PER THE REQUIREMENTS OF ITEM 152.
4. CRUSHED AGGREGATE BASE COURSE (209) SHALL BE COMPACTED PER THE SPECIFICATIONS.
5. THE CONTRACTOR MAY REGRADE AND COMPACT THE EXISTING AGGREGATE SUBGRADE IN LIEU OF EXCAVATING AND ADDING NEW MATERIAL AT THE APPROVAL OF THE RESIDENT ENGINEER. COMPACTION SHALL BE TO THE SATISFACTION OF THE RESIDENT ENGINEER. PAYMENT FOR THIS WORK SHALL BE MADE UNDER SUBGRADE REPAIR.



TIEDOWN BACKFILL DETAIL

N.T.S.



SHOULDER ADJUSTMENT - TYPICAL SECTION

N.T.S.

TAYLORVILLE MUNICIPAL AIRPORT
 TAYLORVILLE, ILLINOIS

REHABILITATE T-HANGAR PAVEMENT
 TYPICAL SECTIONS

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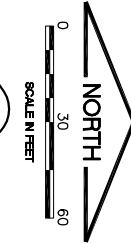


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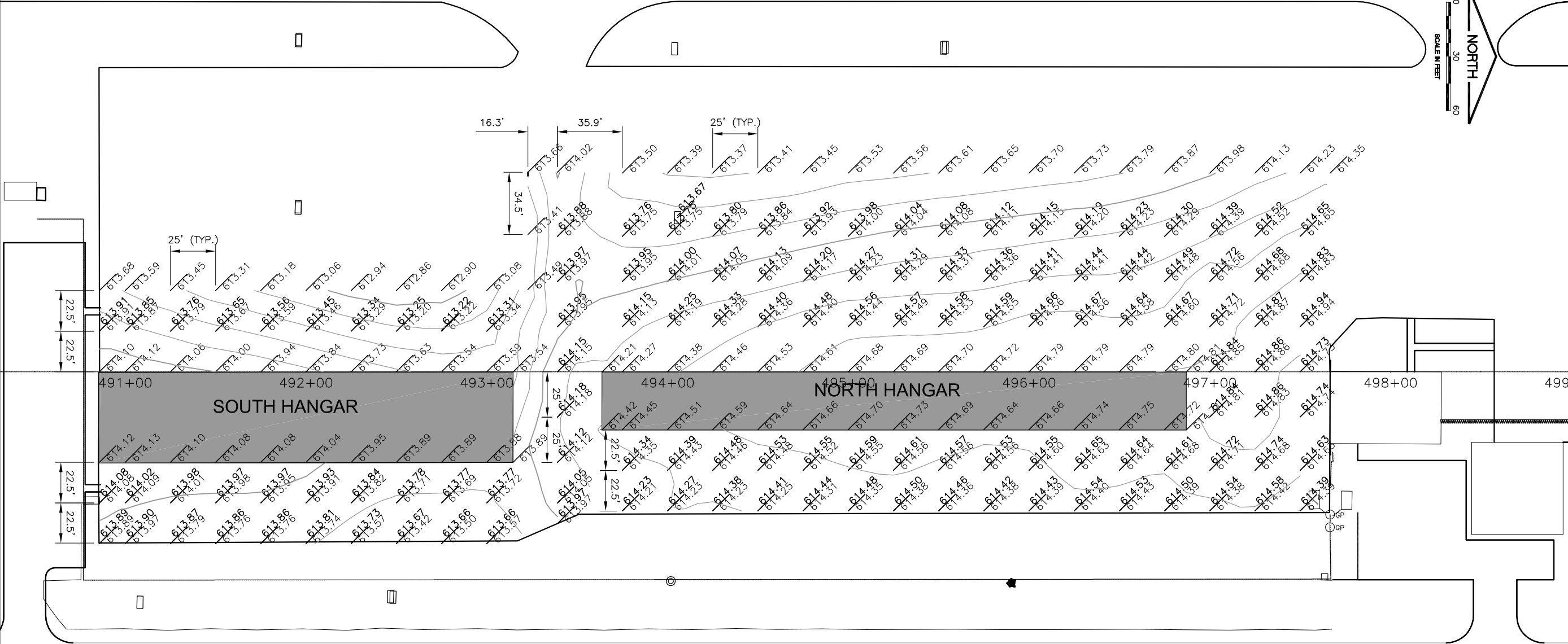
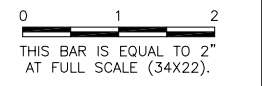
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TAXIWAY A



TA003

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NUMBER	BY	DATE

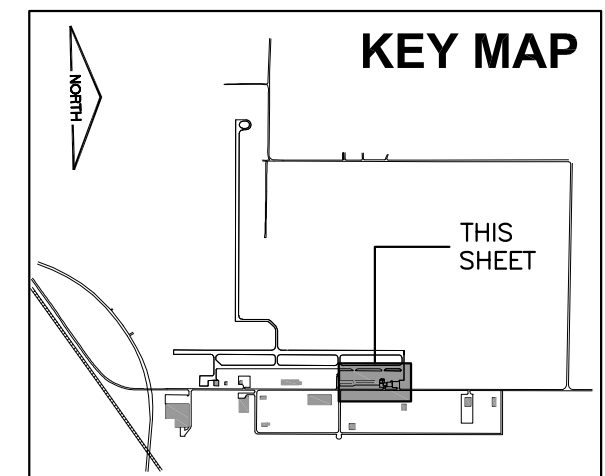


TAYLORVILLE MUNICIPAL AIRPORT
 TAYLORVILLE, ILLINOIS

REHABILITATE T-HANGAR PAVEMENT
 STAKING PLAN

LEGEND

	NEW ELEVATION
	EXISTING ELEVATION
	PROPOSED CONTOURS



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SHEET	09 OF 13 SHEETS

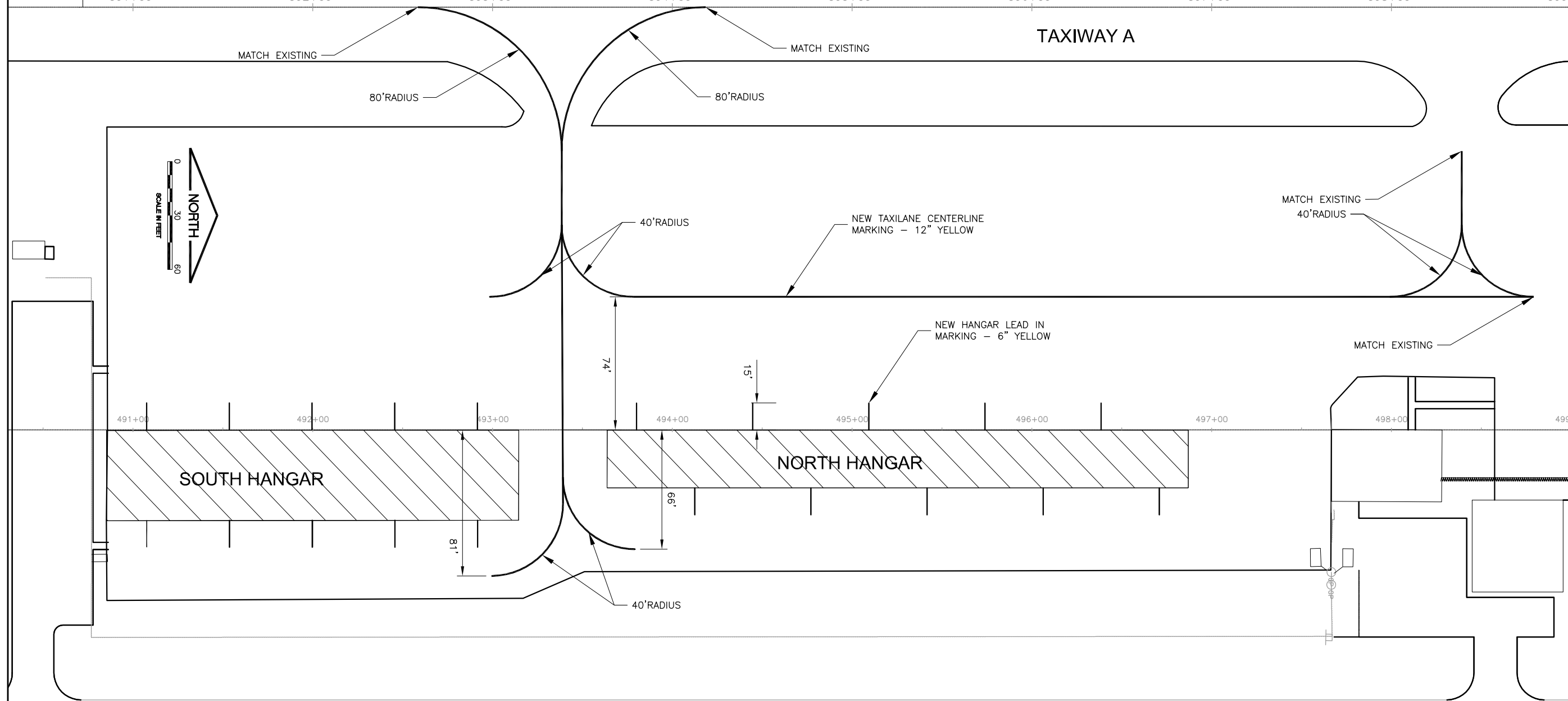
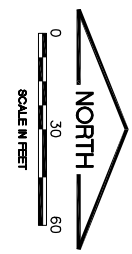
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TAXIWAY A

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REVISIONS		
NUMBER	BY	DATE

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**TAYLORVILLE MUNICIPAL AIRPORT
 TAYLORVILLE, ILLINOIS**
**REHABILITATE T-HANGAR PAVEMENT
 PAVEMENT MARKING**

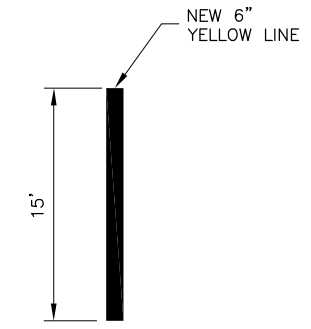
SPRESSER STREET

NOTES

1. ALL TAXIWAY AND TAXILANE CENTERLINE MARKING SHALL BE 12" YELLOW WITH 6" BLACK BORDER.
2. ALL HANGAR LEAD IN MARKING SHALL BE 6" YELLOW.
3. HANGAR LEAD IN MARKING SHALL BE CENTERED ABOUT THE HANGAR DOOR.



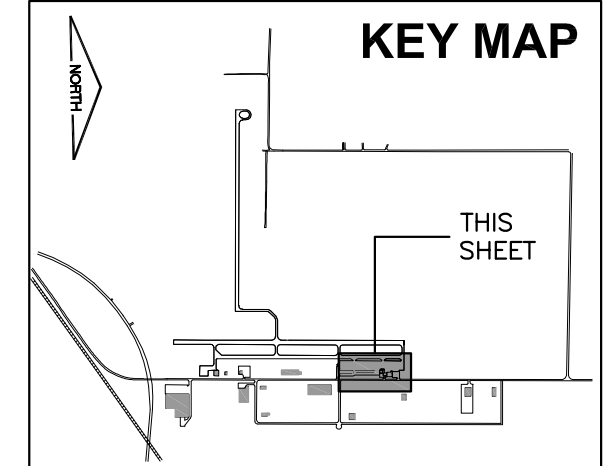
TAXIWAY CENTERLINE CONTINUOUS
 N.T.S.



HANGAR LEAN IN
 N.T.S.

LEGEND

- EXISTING PAVEMENT
- NEW PAVEMENT MARKING



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SHEET 10 OF 13 SHEETS	

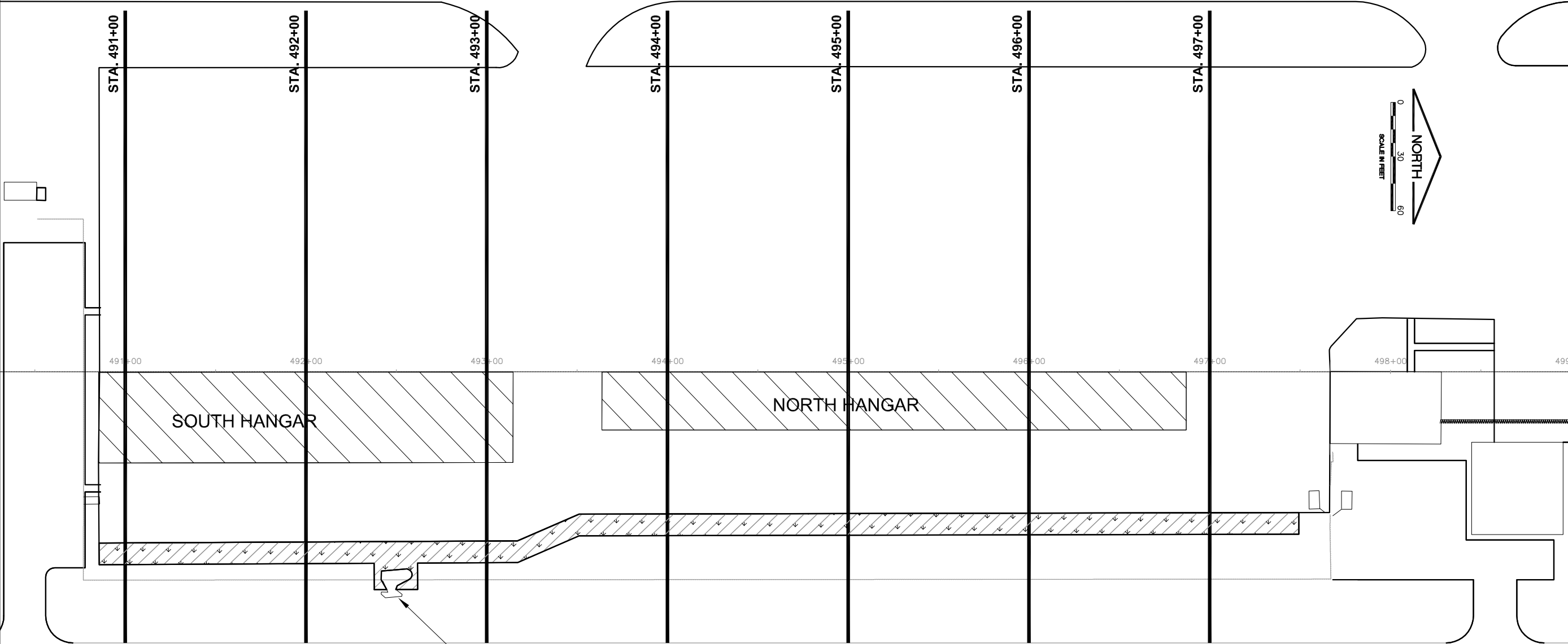
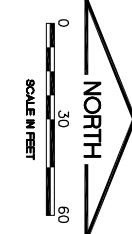
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TAXIWAY A

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NEW RIPRAP - SEE NOTE 2.
 SPRESSER STREET

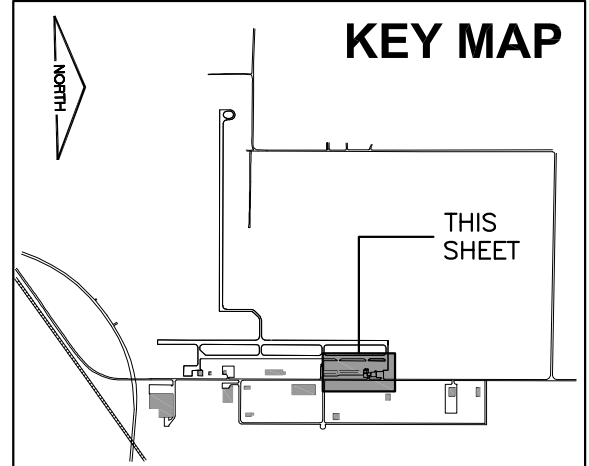
TAYLORVILLE MUNICIPAL AIRPORT
 TAYLORVILLE, ILLINOIS
 REHABILITATE T-HANGAR PAVEMENT
 TURFING PLAN AND INDEX TO CROSS SECTIONS

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LEGEND

	EXISTING PAVEMENT
STA. 497+00	CROSS SECTION STATION
	NEW SHOULDER EMBANKMENT & TURFING

- NOTES**
- EXCELSIOR BLANKET SHALL BE PLACED ALONG THE EDGE OF PAVEMENT AT THE SHOULDER ADJUSTMENT. EXCELSIOR BLANKET SHALL BE PLACED AT A WIDTH OF 6'.
 - REMOVAL OF THE EXISTING BROKEN CONCRETE AND LARGER AGGREGATE SHALL BE INCIDENTAL TO THE PLACEMENT OF THE RIPRAP AND FILTER FABRIC. EXISTING MATERIAL MAY BE REUSED BY THE CONTRACTOR IF THE RESIDENT ENGINEER APPROVES OF ITS USE.
 - AIRPORT FENCING SHALL BE REMOVED AND REPLACED TO ACCOMMODATE THE INSTALLATION OF THE RIPRAP. COSTS ASSOCIATED WITH FENCE REMOVAL AND REPLACEMENT SHALL BE INCIDENTAL THE RIPRAP INSTALLATION.

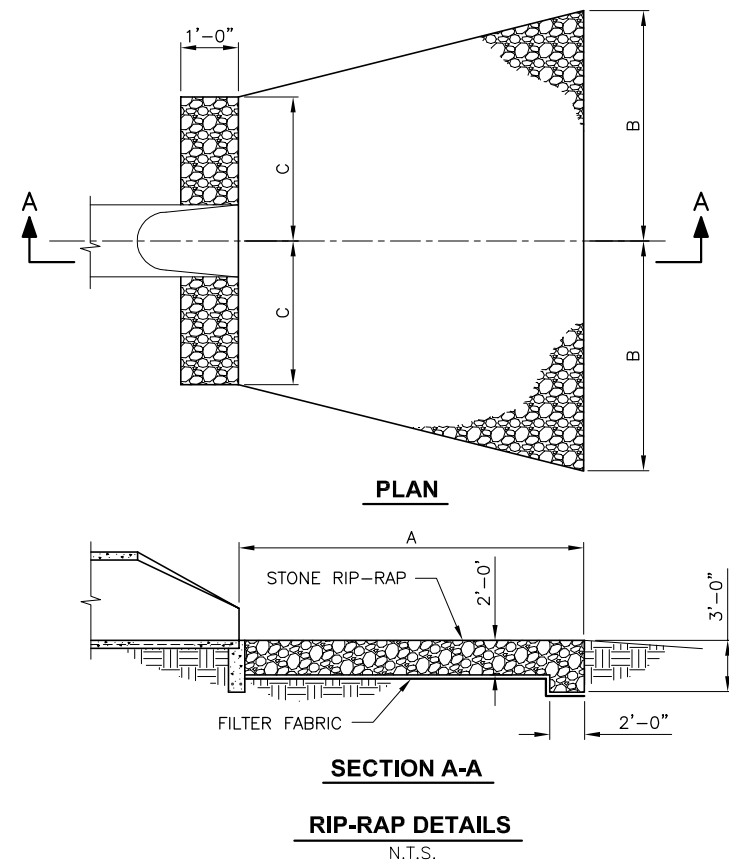


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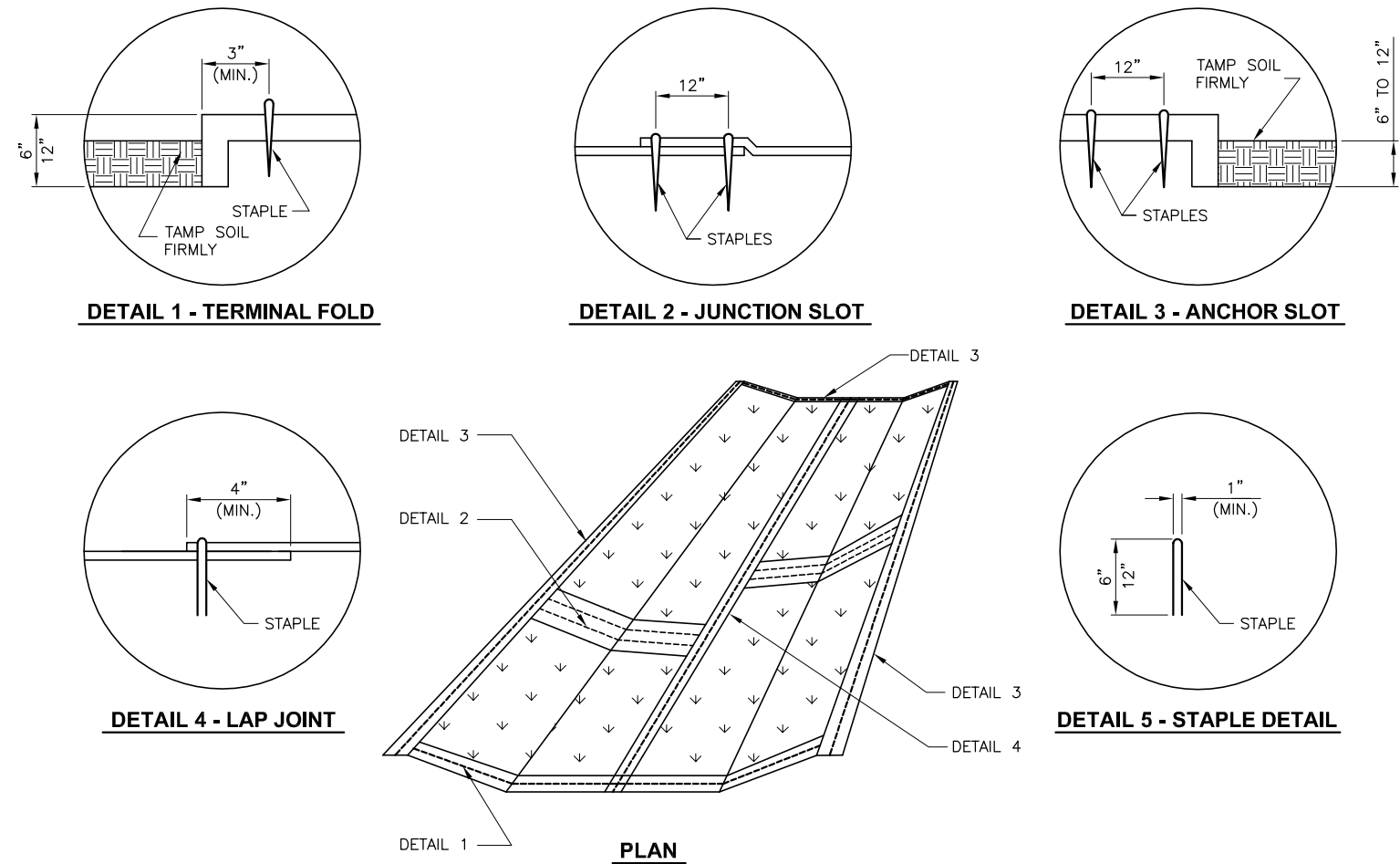


RIP-RAP NOTES

1. THE ENGINEER SHALL DETERMINE THE FINAL RIP-RAP CONFIGURATION IN THE FIELD.
2. PLACE AT EXISTING PIPE LOCATIONS AS DIRECTED BY THE ENGINEER.

DIMENSIONS - TABLE 1

INSIDE DIAMETER STORM SEWER (IN.)	OUTLET DIMENSION (FT.)			INLET DIMENSION (FT.)		
	A	B	C	A	B	C
12" THRU 24"	15	9	2	9	6	2



EXCELSIOR BLANKET NOTES

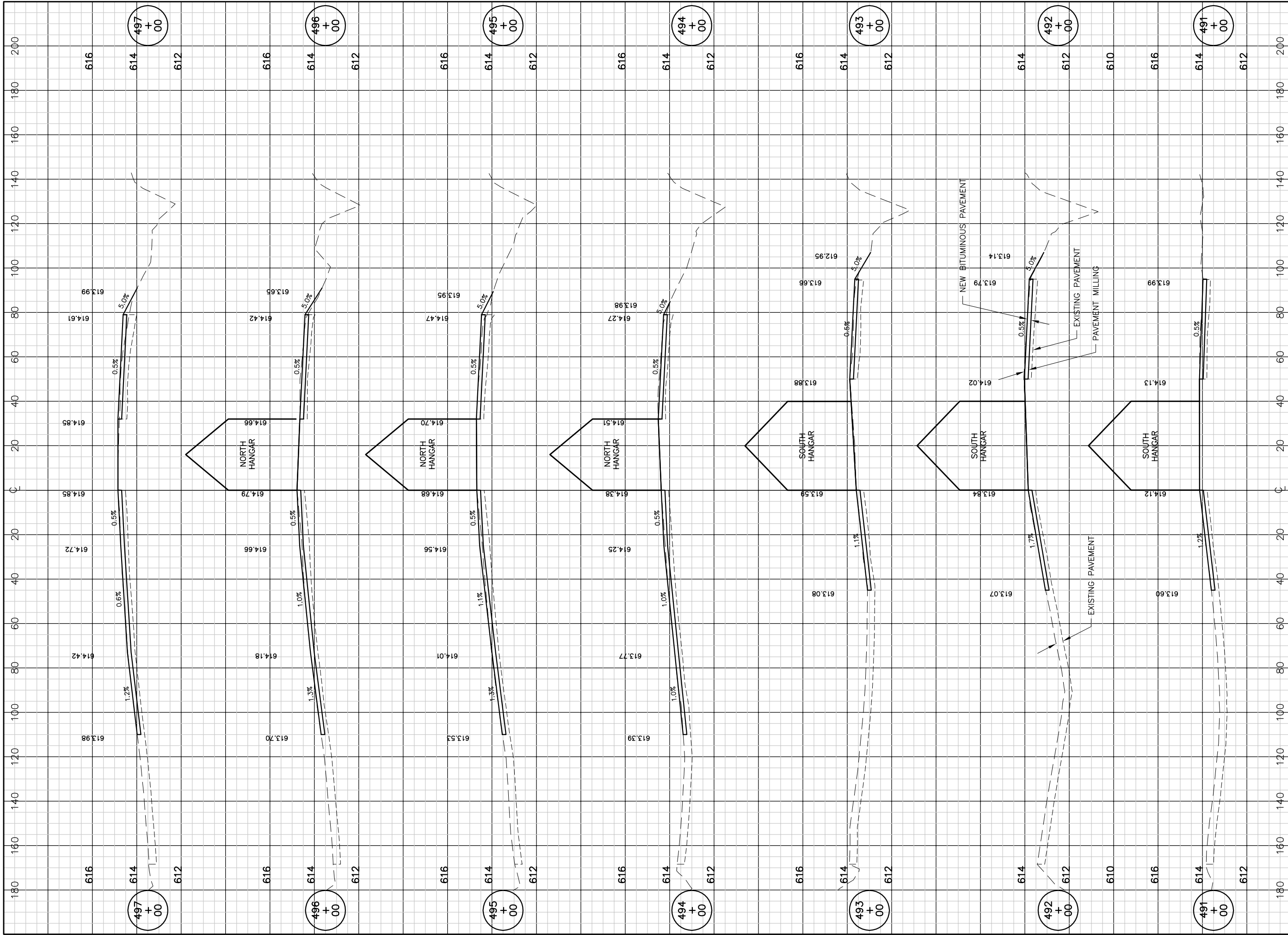
1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINALS ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.

**TAYLORVILLE MUNICIPAL AIRPORT
 TAYLORVILLE, ILLINOIS**

**REHABILITATE T-HANGAR PAVEMENT
 EROSION CONTROL DETAILS**

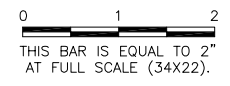
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 SHEET 12 OF 13 SHEETS



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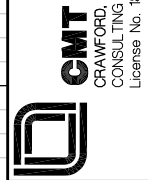
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 TAYLORVILLE, ILLINOIS**

**REHABILITATE T-HANGAR PAVEMENT
 CROSS SECTIONS**

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