

**INDEX OF SHEETS**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

DAN RYAN EXPRESSWAY:	ADT (2010)	DESIGN SPEED	POSTED SPEED
SB I-94 (DAN RYAN)	140,500	60 MPH	55 MPH
SB 87TH STREET EXIT RAMP	12,600	45 MPH	40 MPH
SB 87TH STREET ENTRANCE RAMP	13,400	45 MPH	40 MPH

**CROSS STREETS AND FRONTAGE ROADS:**

	ADT	POSTED SPEED
87TH STREET	24,900	30 MPH
SOUTH LAFAYETTE AVENUE	6,800	30 MPH

**DESCRIPTION OF PROJECT**

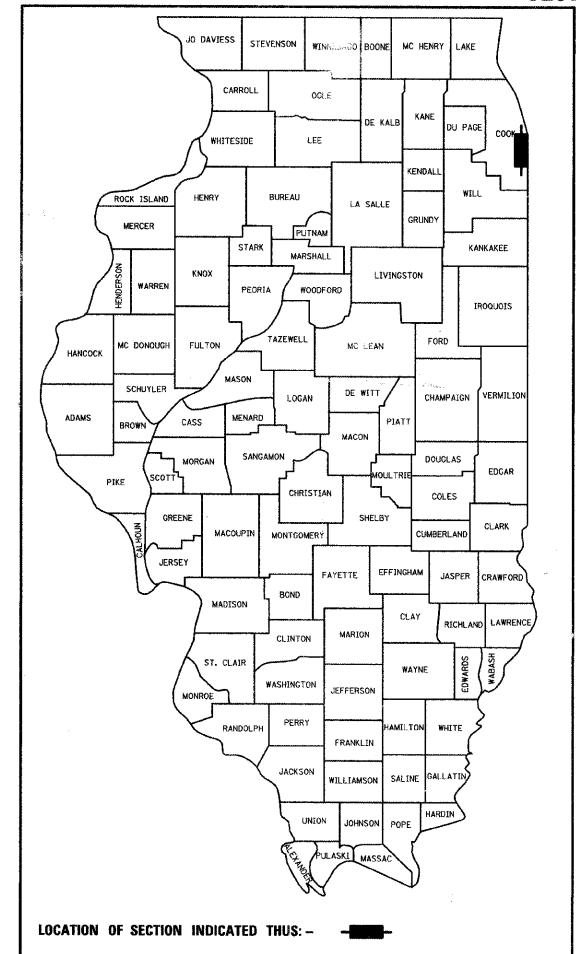
CONSTRUCTION OF FLUTED KNEE WALLS AND HIGH MAST LIGHT TOWER MINI RETAINING WALL LOCATED WITHIN THE PROJECT LIMITS

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

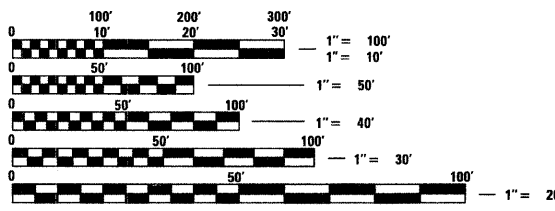
**PROPOSED HIGHWAY PLANS**

**F.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)  
SECTION 1717 I-7  
FLUTED KNEE WALL CONSTRUCTION  
ALONG LAFAYETTE AVE. FROM 87TH STREET TO 85TH STREET  
COOK COUNTY  
C-91-237-05**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	1
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
D-91-237-05			62979	



**PROJECT LOCATED IN THE CITY OF CHICAGO**



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES; REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED

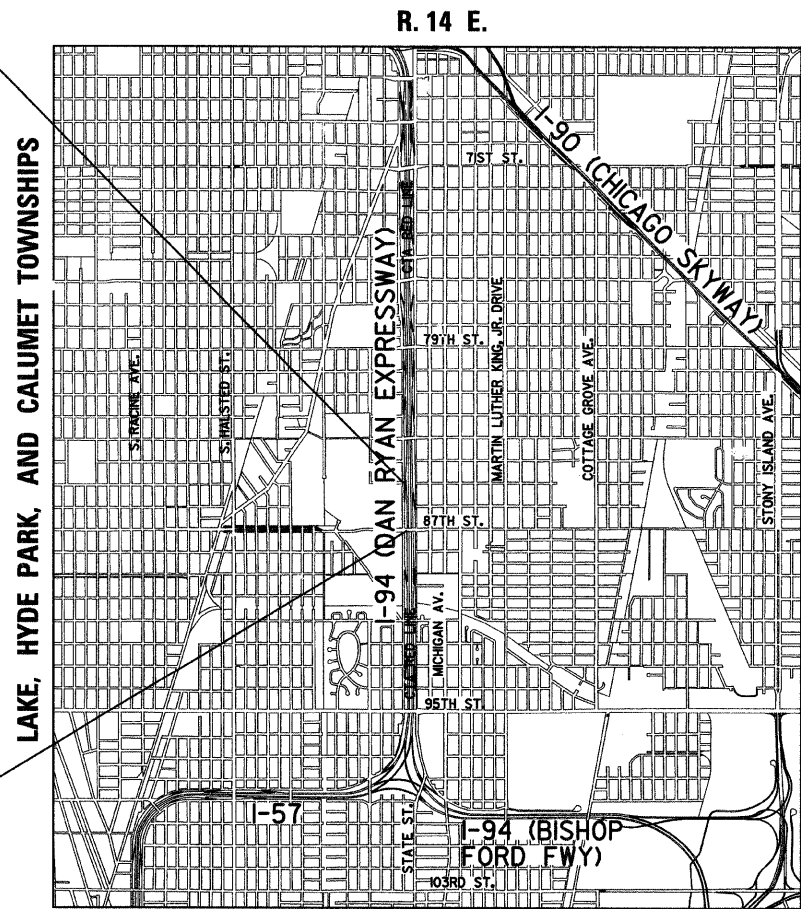
DIGGER:  
CHICAGO UTILITY ALERT NETWORK  
(312) 744-7000

CTA CONTACT:  
DAVID HEARD,  
MANAGER, CONSTRUCTION  
MANAGEMENT OVERSIGHT  
(312) 681-3862

**CONTRACT NO. 62979**

F.A.I. 94 (RYAN) PROJECT LIMIT  
STA. 1270+24.39 (SB I-94)

F.A.I. 94 (RYAN) PROJECT LIMIT  
STA. 1259+91.12 (SB I-94)



**LOCATION MAP**

0 1/2 MILE 1 MILE  
GROSS LENGTH OF PROJECT = 1033.27 FT. = 0.20 MI.  
MAP SCALE: 1" = 1/2 MILE  
NET LENGTH OF PROJECT = 1033.27 FT. = 0.20 MI.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED October 9, 2008

Diane M. O'Keefe  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 8, 2009  
Charles J. Ingersoll  
ENGINEER OF DESIGN AND ENVIRONMENT

May 8, 2009  
Christine M. Reed  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**CONTRACT 26C**

32957  
REGISTERED  
PROFESSIONAL  
ENGINEER  
OF  
ILLINOIS

Signed Dean A. Krasling  
Dean A. Krasling, P.E., Ill. Lic. No. 32957  
Expires 11-30-2009

Date October 7, 2008

**TYLIN INTERNATIONAL**  
PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

DISTRICT ONE DESIGN / PROJECT MANAGER:  
BRIAN KUTTAB (847) 705 - 4431

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	2
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

62979

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001006	DECIMAL OF AN INCH AND A FOOT
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606001	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
664001	CHAIN LINK FENCE
701456	PARTIAL EXIT RAMP CLOSURE FREEWAY/EXPRESSWAY
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701801	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901	TRAFFIC CONTROL DEVICES

## GENERAL NOTES:

- UTILITY LOCATIONS SHOWN ON THESE PLANS MAY NOT BE CORRECT OR COMPLETE. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE RESPECTIVE UTILITIES OF THE CITY OF CHICAGO AND THOSE PRIVATELY OWNED. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL THE CHICAGO UTILITY ALERT NETWORK AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, CABLE, AND GAS FACILITIES (48 HOURS NOTIFICATION IS REQUIRED). CONTACT THE CHICAGO DEPARTMENT OF WATER MANAGEMENT PERMIT SECTION AT (312) 747-7893 FOR WATER AND SEWER LOCATIONS.
- WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS. THE ENGINEER OR AN AUTHORIZED SURVEYOR AGENT WILL WITNESS OR OTHERWISE REFERENCE AND RESET MONUMENTS AS NECESSARY. ALL PROPERTY CORNERS EXCEPT THOSE WITHIN AREAS WHERE THE SCHEDULE, IF PROVIDED, SHOWS PLACEMENT OF R.O.W. MARKERS SHALL REMAIN UNDISTURBED.
- THE CONTRACTOR SHALL NOT SET UP A YARD OR FIELD OFFICE ON IDOT PROPERTY WITHOUT WRITTEN PERMISSION FROM IDOT.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY SAFETY PRECAUTIONS TO PROTECT AND PROVIDE ACCESS TO ADJUTING PROPERTY, UTILITIES, PEDESTRIANS, AND VEHICULAR TRAFFIC.
- ALL ELEVATIONS SHOWN ARE BASED ON THE CHICAGO CITY DATUM OF 0.00, WHICH IS 579.19 FEET ABOVE MEAN TIDE NEW YORK. (NAVD 88)
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT NO GAP REMAINS BETWEEN PROPOSED RELOCATED FENCING OR WHERE PROPOSED RELOCATED FENCING TERMINATES AND EXISTING FENCE REMAINS IN PLACE.
- ALL EXISTING FENCE THAT IS REQUIRED TO BE REMOVED AND RE-ERECTED OR TEMPORARY FENCE SHALL BE RELOCATED OR PLACED PRIOR TO THE START OF INSTALLATION OF THE PROPOSED KNEE WALL OR AS DIRECTED BY THE ENGINEER.
- THE LOCATIONS OF VARIOUS ITEMS SUCH AS PAVEMENT, RETAINING WALLS, AND DRAINAGE STRUCTURES BUILT IN CONTRACTS UNDER CONSTRUCTION ARE BASED ON THE PUBLISHED CONTRACT PLAN DRAWINGS AVAILABLE DURING DESIGN. THE CONTRACTOR SHALL VERIFY AS-BUILT CONDITIONS.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
- THE INTENT OF THE CONTRACT DOCUMENTS IS TO CONSTRUCT THE FLUTED KNEE WALL BEHIND THE EXISTING CURB AND GUTTER. ANY DAMAGE TO THE EXISTING CURB AND GUTTER SHALL BE REPAIRED TO THE ORIGINAL CONDITION OF THE CURB AND GUTTER OR REPLACED AT THE CONTRACTOR'S OWN EXPENSE.
- THE ENGINEER SHALL BE ALLOWED ACCESS TO THE FIELD OFFICE IN USE FOR CONTRACT #62981 (26E).
- THE CONTRACTOR SHALL PROVIDE ALL NECESSARY TRAFFIC CONTROL DEVICES NEEDED TO COMPLETE THE WORK. THIS WORK SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).
- THE CONTRACTOR SHALL NOT MAINTAIN A PERMANENT PARTIAL RAMP OR SHOULDER CLOSURE ALONG THE MAINLINE I-94 (DAN RYAN) FOR THE DURATION OF THIS PROJECT. ONLY TEMPORARY PARTIAL RAMP CLOSURES SHALL BE PERMITTED.
- ALL CATCH BASINS IN THE CITY OF CHICAGO SHALL MEET THE CHICAGO DEPARTMENT OF WATER MANAGEMENT'S STANDARDS.
- PERMITS FROM THE DEPARTMENT OF WATER MANAGEMENT ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY AND COMBINED SEWER SYSTEM CONSTRUCTION, AND FOR ALL WORK INVOLVING ADJUSTMENT OF SEWER STRUCTURES. THE DEPARTMENT OF WATER MANAGEMENT'S PERMIT SHALL BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF CONSTRUCTION. THE LICENSED SEWER CONTRACTOR/ SUBCONTRACTOR SHALL SUBMIT TWO SETS OF PLANS APPROVED BY THE DEPARTMENT OF WATER MANAGEMENT FOR THE ISSUANCE OF THE SEWER PERMIT TO BUREAU OF ENGINEERING SERVICES-SEWER SECTION, JARDINE PURIFICATION PLANT, EL+51, ROOM 313, 1000 E. OHIO ST., CHICAGO, IL 60611. INSPECTION WILL BE PROVIDED BY THE DEPARTMENT OF WATER MANAGEMENT.
- THE RESIDENT ENGINEER AND THE CONTRACTOR SHALL FIELD VERIFY THE CITY'S EXISTING SEWER FACILITIES IN THE LIMITS OF THE REFERENCED PROJECT FOR ANY CONFLICTS DUE TO THE PROPOSED IMPROVEMENT. ANY CONFLICT SHALL BE RESOLVED WITH THE DEPARTMENT OF WATER MANAGEMENT PRIOR TO START OF CONSTRUCTION.
- IN CASE OF DAMAGE TO CITY OF CHICAGO SEWERS, PRIVATE AND PUBLIC DRAINS, SEWER STRUCTURES AND/OR BENCH MONUMENTS, THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE DEPARTMENT OF WATER MANAGEMENT AT (312) 744-0409 OR (312) 744-0408.
- PERFORATED LIDS SHALL BE PLACED ON ALL SEWER MANHOLES AND CATCH BASINS.
- NIGHT OPERATIONS SHALL NOT BE ALLOWED
- LOCATION OF THE ELECTRICAL EQUIPMENT, INSTALLED BY CONTRACT 62583, IS APPROXIMATE. THE RESIDENT ENGINEER AND THE CONTRACTOR SHALL FIELD VERIFY THE EXACT LOCATION OF ALL ELECTRICAL ITEMS WITHIN THE PROJECT LIMITS WITH THE IDOT ELECTRICAL REPRESENTATIVE PRIOR TO THE START OF CONSTRUCTION. THE COST SHALL BE INCLUDED IN THE COST OF MOBILIZATION.

## GENERAL NOTES (CONT.):

- CITY OF CHICAGO WATER VALVE VAULTS AND SEWER STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION WITHOUT PRIOR WRITTEN PERMISSION FROM THE CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT.
- IN LOCATIONS WHERE THE MAIN SEWER IS NOT BEING REPLACED AND THE EXISTING DRAINAGE FACILITIES ARE DISTURBED OR DAMAGED DURING CONSTRUCTION BY THE CONTRACTOR, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE AND REPLACE THE DAMAGED FACILITIES AT HIS/HER OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.
- IT IS CALLED TO THE CONTRACTOR'S ATTENTION THAT HE/SHE SHALL BE REQUIRED TO OBTAIN AND PAY FOR ANY INSPECTION OR PERMIT FEES TO THE VARIOUS DEPARTMENTS OF THE CITY OF CHICAGO AND THE STATE OF ILLINOIS. THE COST FOR THESE FEES SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF MOBILIZATION. PERMIT FEES SHALL BE PAID PRIOR TO COMMENCING CONSTRUCTION.
- A PERMIT SHALL BE SECURED FROM THE BUREAU OF TRAFFIC PERMIT DIVISION, ROOM 804, CITY HALL AND BE INCLUDED IN THE COST OF MOBILIZATION. ADDITIONALLY, SEVEN (7) DAYS NOTICE SHALL BE GIVEN TO THE BUREAU OF TRAFFIC (312-744-0330) PRIOR TO STARTING ANY WORK OR MAKING ANY CHANGES IN THE TRAFFIC PATTERNS. ANY LANE OR ROADWAY CLOSURE INCLUDING DETOUR SHALL BE REVIEWED AND APPROVED BY THE BUREAU OF TRAFFIC.
- A "BOXED" NOTE INDICATES AN ITEM OF WORK THAT IS NOT PAID FOR SEPARATELY, BUT IS PAID FOR AS PART OF ANOTHER ITEM LISTED IN THE SUMMARY OF QUANTITIES.
- THE REMOVED AND RE-ERECTED EXISTING FENCE OR TEMPORARY FENCE SHALL REMAIN IN ITS POSITION, AS SHOWN ON PLANS, AT THE END OF THIS CONTRACT. FUTURE CONTRACT #60A02 SHALL INSTALL THE PERMANENT FENCING ON THE KNEE WALL AND THEN REMOVE THE FENCE LEFT IN PLACE BY THIS CONTRACT.
- THE CONTRACTOR SHALL PROTECT ALL BLUNT ENDS OF THE KNEE WALL FOR TRAFFIC SAFETY. ALL TEMPORARY PROTECTION DEVICES USED FOR THIS PURPOSE SHALL BE NCHRP 350 COMPLIANT AND AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).

## CONTRACTOR ACCESS ASSUMPTIONS:

- FOR HMLT 5PAB3, ACCESS IS ASSUMED TO BE FROM THE FRONTAGE ROAD (S. LAFAYETTE AVE.). THE LIGHTING CONTRACTOR (#62583) HAS COMPLETED INSTALLATION OF THIS TOWER AS OF MARCH 2006.
- FOR FLUTED KNEE WALL AND FLUTED KNEE WALL, SPECIAL CONSTRUCTION, ACCESS IS ASSUMED TO BE FROM THE FRONTAGE ROAD.

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)INDEX OF SHEETS, INDEX OF STATE  
STANDARDS, GENERAL NOTES, AND  
CONTRACTOR ACCESS ASSUMPTIONS

SCALE: NONE

DRAWN BY: JDF

DATE: OCTOBER 17, 2008

CHECKED BY: JPM

PROJECT: KNEE WALLS ALONG LAFAYETTE AVENUE  
FROM 87TH STREET TO 85TH STREET  
COOK COUNTY/CITY OF CHICAGO

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	3
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

62979

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	QUANTITY	100% STATE SFTY-4A
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	14	14
20101000	TEMPORARY FENCE	FOOT	590	590
20200100	EARTH EXCAVATION	CU YD	100	100
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	275	275
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	473	473
21101645	TOPSOIL FURNISH AND PLACE, 12"	SQ YD	437	437
21101825	COMPOST FURNISH AND PLACE, 6"	SQ YD	437	437
25000210	SEEDING, CLASS 2A	ACRE	0.25	0.25
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	23	23
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	23	23
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	23	23
25001820	SEEDING, CLASS 5 (MODIFIED)	ACRE	0.25	0.25
25002014	SEEDING, CLASS 4A (MODIFIED)	ACRE	0.25	0.25
25100630	EROSION CONTROL BLANKET	SQ YD	910	910
25200200	SUPPLEMENTAL WATERING	UNIT	27	27
28000510	INLET FILTERS	EACH	11	11
42001300	PROTECTIVE COAT	SQ YD	175	175
42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	SQ FT	63	63
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	48	48
44004250	PAVED SHOULDER REMOVAL	SQ YD	105	105
50200100	STRUCTURE EXCAVATION	CU YD	12	12
50300225	CONCRETE STRUCTURES	CU YD	9	9
50300300	PROTECTIVE COAT	SQ YD	11	11
50300510	RUSTICATION FINISH	SQ FT	43	43
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1030	1030
60255500	MANHOLES TO BE ADJUSTED	EACH	1	1
60608521	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24	FOOT	48.0	48.0

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	QUANTITY	100% STATE SFTY-4A
66410300	CHAIN LINK FENCE REMOVAL	FOOT	547	547
66410400	CHAIN LINK FENCE TO BE REMOVED AND RE-ERECTED	FOOT	474	474
67100100	MOBILIZATION	L SUM	1	1
* 70101800	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1
* 81400215	HEAVY-DUTY HANDHOLE TO BE ADJUSTED	EACH	2	2
X0326143	FLUTED KNEE WALL (SPECIAL)	FOOT	270	270
X0326311	RELOCATE EXISTING SIGN PANEL (SPECIAL)	EACH	2	2
* X0321750	REMOVE TEMPORARY CONCRETE BARRIER, STATE OWNED	FOOT	40	40
* X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	52	52
X0323973	SEDIMENT CONTROL, SILT FENCE	FOOT	70	70
X0324640	FLUTED KNEE WALL	FOOT	751	751
* X0325891	IMPACT ATTENUATORS (FULLY REDIRECTIVE, RESETTABLE), TEST LEVEL 2	EACH	1	1
X6065740	CONCRETE MEDIAN SURFACE, 5" (MODIFIED)	SQ FT	1336	1336
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1

\*SPECIALTY ITEM

EARTHWORK SCHEDULE

LOCATION			EARTH EXCAVATION (CU YD)	REM. & DISP. UNSUITABLE (CU YD)
ALIGNMENT	STATION	TO STATION		
SB I-94 (DAN RYAN EXPY)	1259+91.12	1267+50.00	60.0	195.0
SB I-94 (DAN RYAN EXPY)	1268+00.00	1270+24.39	40.0	80.0
TOTAL =			100.0	275.0

TREE REMOVAL (6 TO 15 UNITS)

LOCATION			TREE REMOV 6-15 (UNIT)
ALIGNMENT	STATION	OFFSET	
SB I-94 (DAN RYAN EXPY)	1262+40.78	116.9' LT	14
TOTAL =			14

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

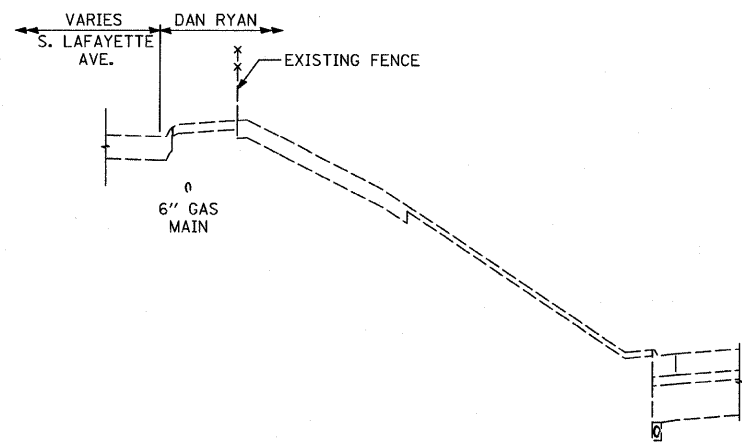
SUMMARY OF QUANTITIES AND SCHEDULE OF QUANTITIES

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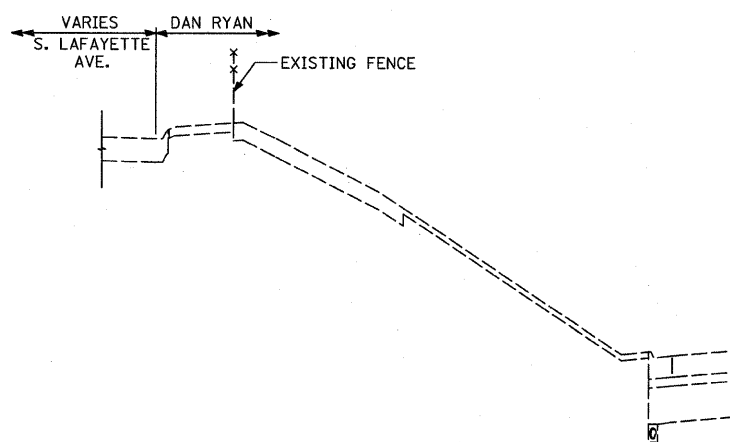
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	4
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

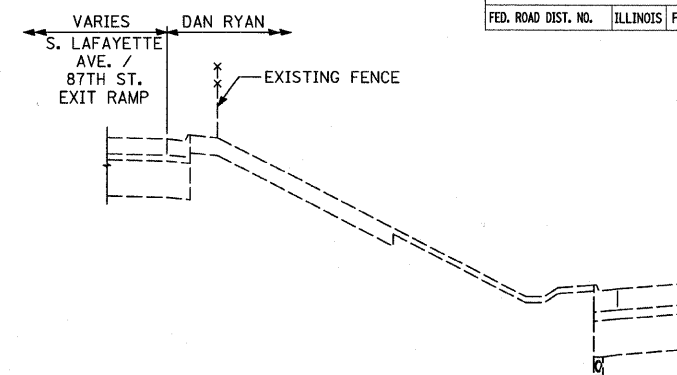
62979



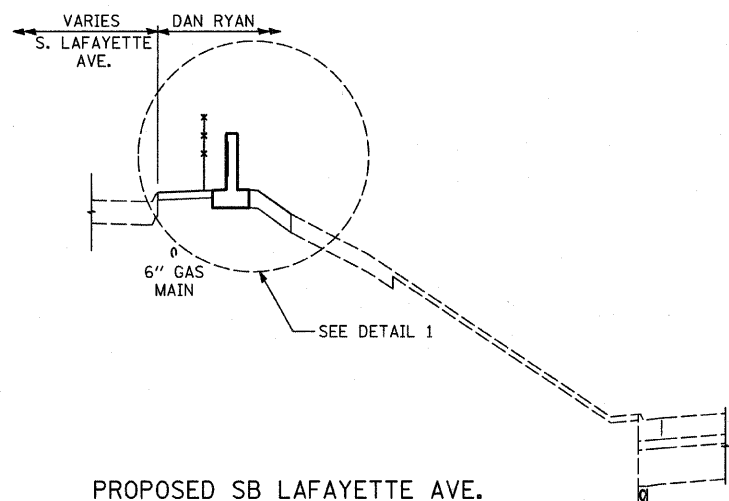
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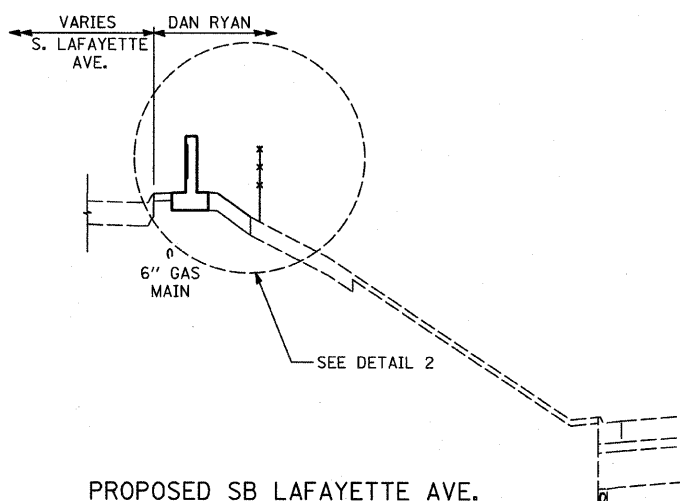
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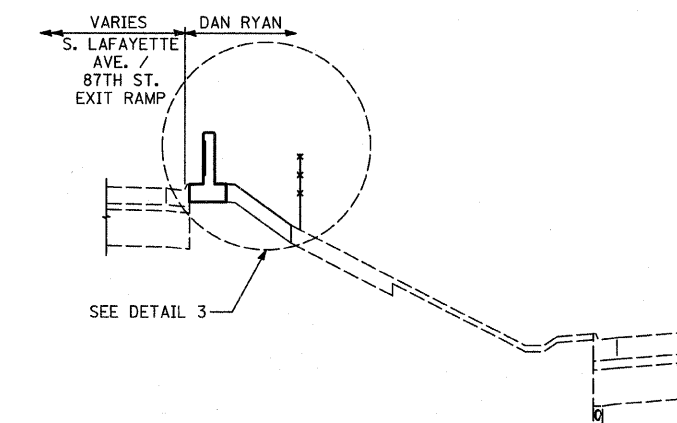
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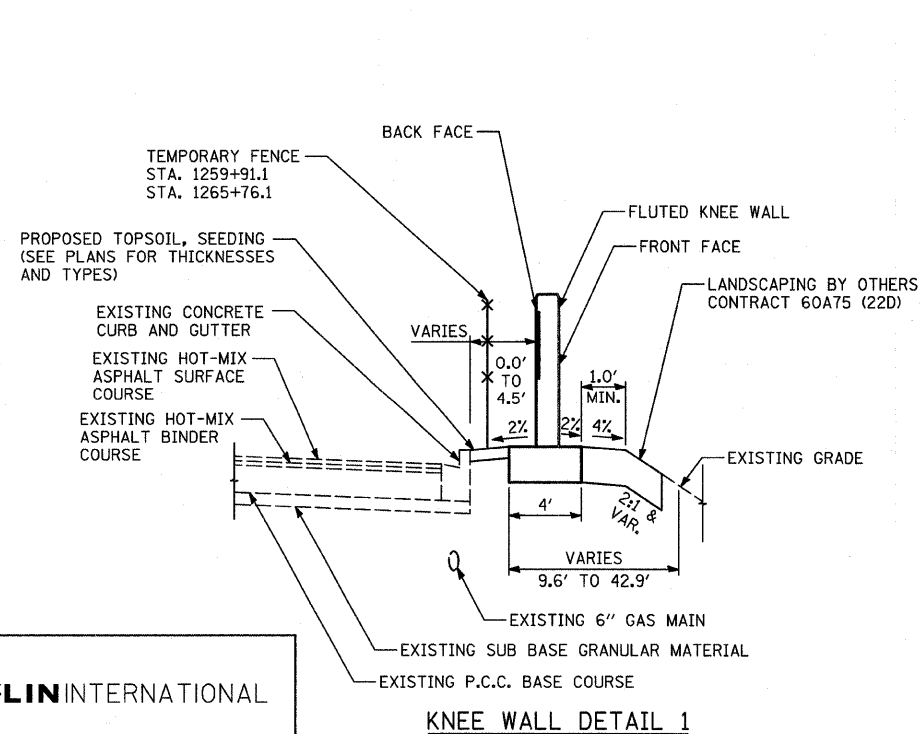
PROPOSED SB LAFAYETTE AVE.  
STA. 1259+91.1 TO STA. 1265+76.1  
STA. 1265+99.4 TO STA. 1266+73.4



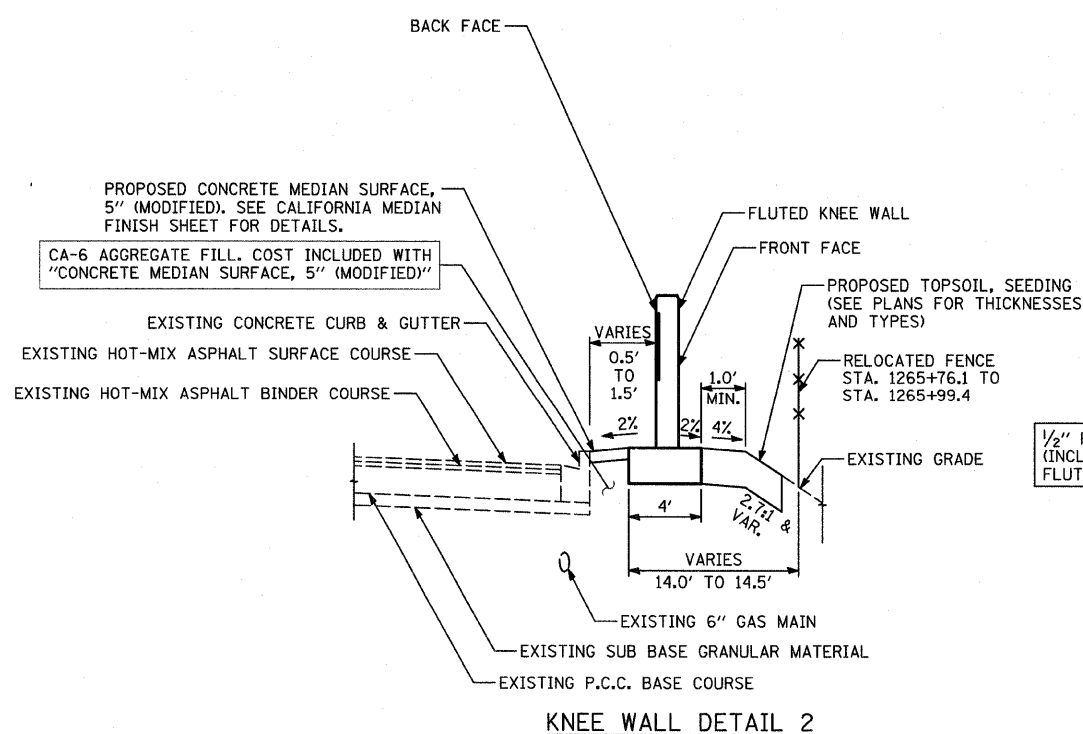
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STA. 1265+84.7 TO STA. 1265+99.4



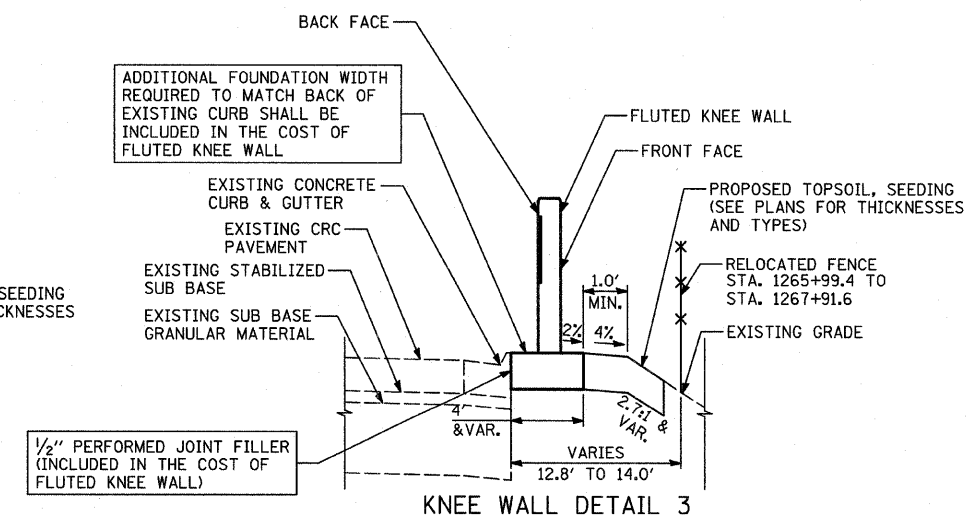
PROPOSED 87TH ST. EXIT RAMP  
STA. 1266+73.4 TO STA. 1267+91.6



KNEE WALL DETAIL 1



KNEE WALL DETAIL 2



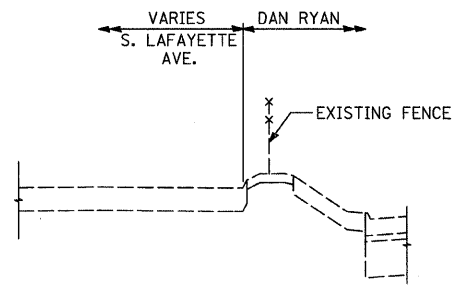
KNEE WALL DETAIL 3

REVISIONS	
NAME	DATE

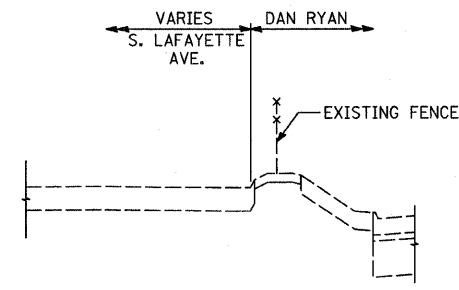
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED TYPICAL SECTIONS  
SB LAFAYETTE AVE  
1 OF 2

SCALE: NONE  
DATE: OCTOBER 17, 2008

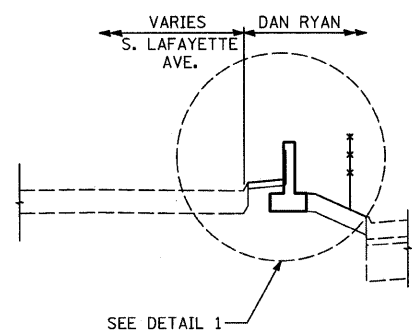
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CHECKED BY: JPM



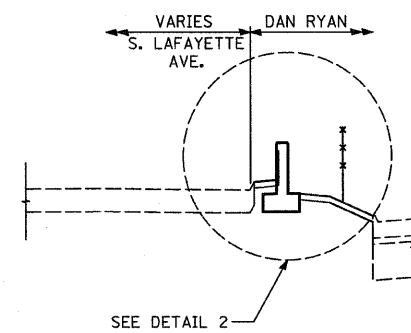
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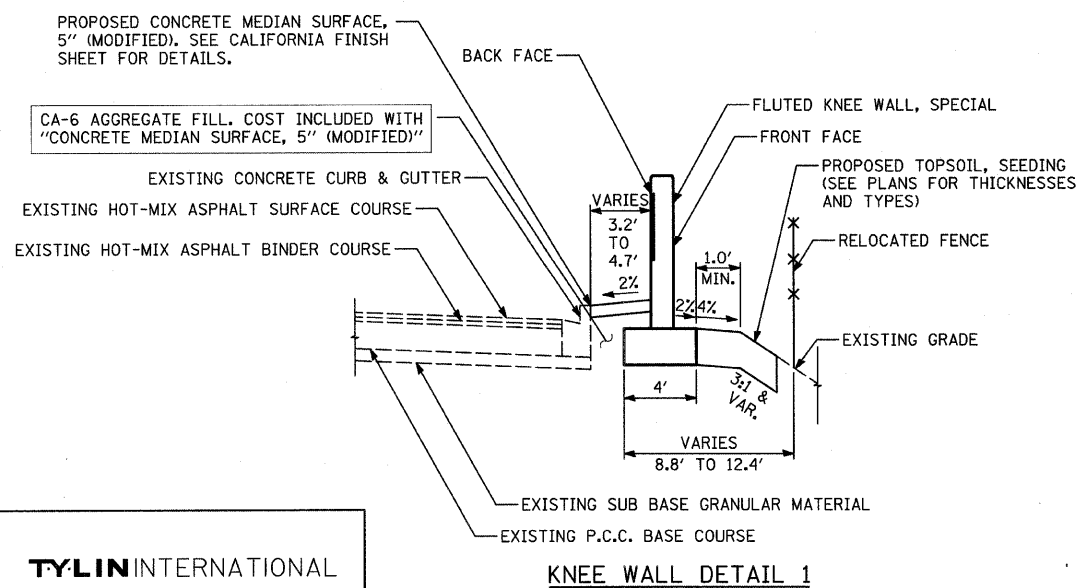
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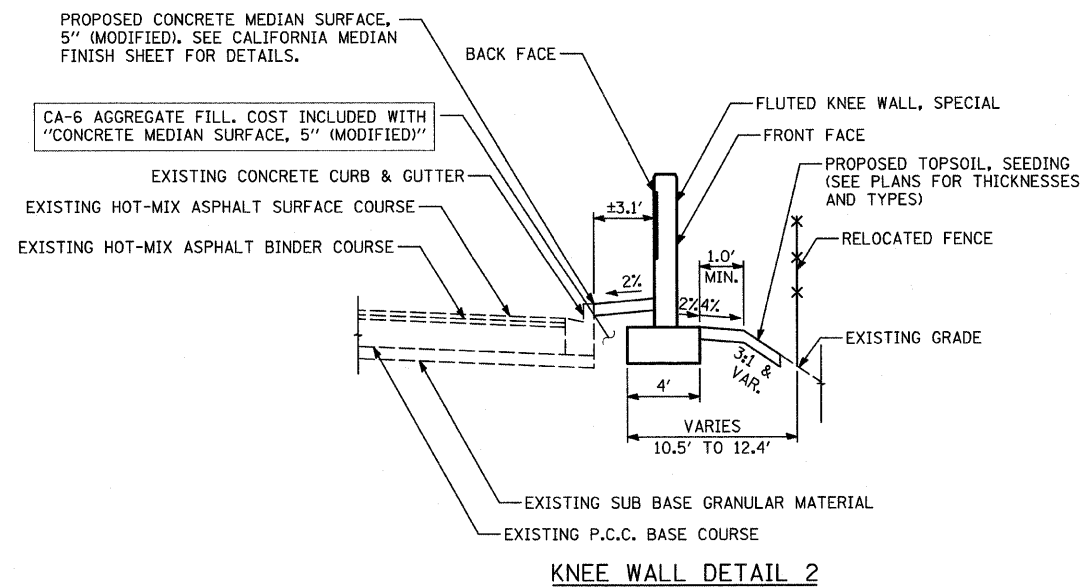
PROPOSED SB LAFAYETTE AVE.  
STA. 1267+39.4 TO STA. 1269+51.9



PROPOSED SB LAFAYETTE AVE.  
STA. 1269+51.9 TO STA. 1270+14.4



KNEE WALL DETAIL 1



KNEE WALL DETAIL 2

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
EXISTING AND PROPOSED TYPICAL SECTIONS  
SB LAFAYETTE AVE  
2 OF 2

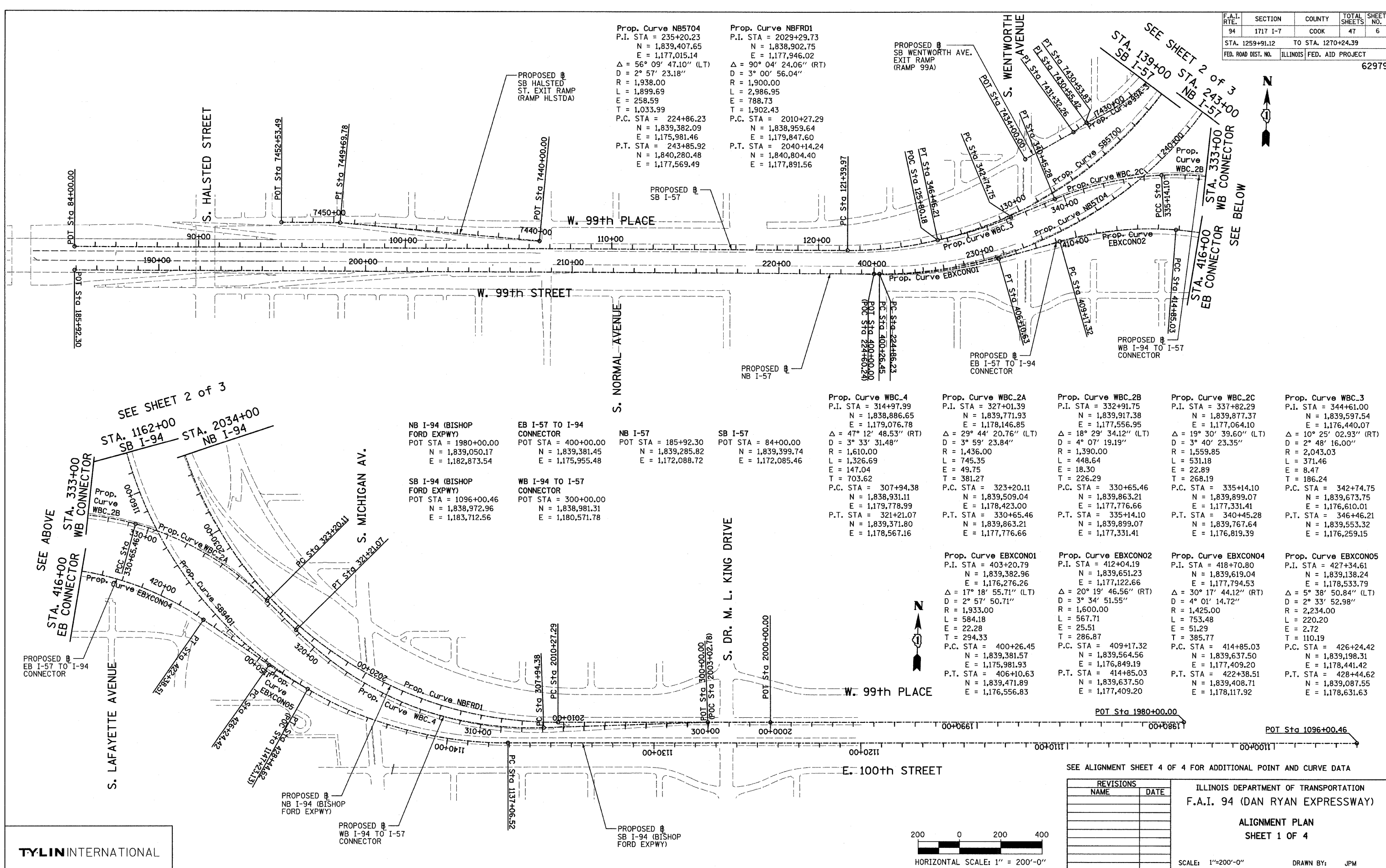
SCALE: NONE DRAWN BY: JDF  
DATE: OCTOBER 17, 2008 CHECKED BY: JPM

10/16/2008 4:06:18 PM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	6
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		62979

Prop. Curve NB5704  
P.I. STA = 235+20.23  
N = 1,839,407.65  
E = 1,177,015.14  
Δ = 56° 09' 47.10" (LT)  
D = 2° 57' 23.18"  
R = 1,938.00  
L = 1,899.69  
E = 258.59  
T = 1,033.99  
P.C. STA = 224+86.23  
N = 1,839,382.09  
E = 1,175,981.46  
P.T. STA = 243+85.92  
N = 1,840,280.48  
E = 1,177,569.49

Prop. Curve NBFDR1  
P.I. STA = 2029+29.73  
N = 1,838,902.75  
E = 1,177,946.02  
Δ = 90° 04' 24.06" (RT)  
D = 3° 00' 56.04"  
R = 1,900.00  
L = 2,986.95  
E = 788.73  
T = 1,902.43  
P.C. STA = 2010+27.29  
N = 1,838,959.64  
E = 1,179,847.60  
P.T. STA = 2040+14.24  
N = 1,840,804.40  
E = 1,177,891.56



NB I-94 (BISHOP FORD EXPWY)  
POT STA = 1980+00.00  
N = 1,839,050.17  
E = 1,182,873.54

EB I-57 TO I-94 CONNECTOR  
POT STA = 400+00.00  
N = 1,839,381.45  
E = 1,175,955.48

NB I-57  
POT STA = 185+92.30  
N = 1,839,285.82  
E = 1,172,088.72

SB I-57  
POT STA = 84+00.00  
N = 1,839,399.74  
E = 1,172,085.46

SB I-94 (BISHOP FORD EXPWY)  
POT STA = 1096+00.46  
N = 1,838,972.96  
E = 1,183,712.56

WB I-94 TO I-57 CONNECTOR  
POT STA = 300+00.00  
N = 1,838,981.31  
E = 1,180,571.78

Prop. Curve WBC.4  
P.I. STA = 314+97.99  
N = 1,838,886.65  
E = 1,179,076.78  
Δ = 47° 12' 48.53" (RT)  
D = 3° 33' 31.48"  
R = 1,610.00  
L = 1,326.69  
E = 147.04  
T = 703.62  
P.C. STA = 307+94.38  
N = 1,838,931.11  
E = 1,179,778.99  
P.T. STA = 321+21.07  
N = 1,839,371.80  
E = 1,178,567.16

Prop. Curve WBC.2A  
P.I. STA = 327+01.39  
N = 1,839,771.93  
E = 1,178,146.85  
Δ = 29° 44' 20.76" (LT)  
D = 3° 59' 23.84"  
R = 1,436.00  
L = 745.35  
E = 49.75  
T = 381.27  
P.C. STA = 323+20.11  
N = 1,839,509.04  
E = 1,178,423.00  
P.T. STA = 330+65.46  
N = 1,839,863.21  
E = 1,177,776.66

Prop. Curve WBC.2B  
P.I. STA = 332+91.75  
N = 1,839,917.38  
E = 1,177,556.95  
Δ = 18° 29' 34.12" (LT)  
D = 4° 07' 19.19"  
R = 1,390.00  
L = 448.64  
E = 18.30  
T = 226.29  
P.C. STA = 330+65.46  
N = 1,839,863.21  
E = 1,177,776.66  
P.T. STA = 335+14.10  
N = 1,839,899.07  
E = 1,177,331.41

Prop. Curve WBC.2C  
P.I. STA = 337+82.29  
N = 1,839,877.37  
E = 1,177,064.10  
Δ = 19° 30' 39.60" (LT)  
D = 3° 40' 23.35"  
R = 1,559.85  
L = 531.18  
E = 22.89  
T = 268.19  
P.C. STA = 335+14.10  
N = 1,839,899.07  
E = 1,177,331.41  
P.T. STA = 340+45.28  
N = 1,839,767.64  
E = 1,176,819.39

Prop. Curve WBC.3  
P.I. STA = 344+61.00  
N = 1,839,597.54  
E = 1,176,440.07  
Δ = 10° 25' 02.93" (RT)  
D = 2° 48' 16.00"  
R = 2,043.03  
L = 371.46  
E = 8.47  
T = 186.24  
P.C. STA = 342+74.75  
N = 1,839,673.75  
E = 1,176,610.01  
P.T. STA = 346+46.21  
N = 1,839,553.32  
E = 1,176,259.15

Prop. Curve EBXC01  
P.I. STA = 403+20.79  
N = 1,839,651.96  
E = 1,176,276.26  
Δ = 17° 18' 55.71" (LT)  
D = 2° 57' 50.71"  
R = 1,933.00  
L = 584.18  
E = 22.28  
T = 294.33  
P.C. STA = 400+26.45  
N = 1,839,381.57  
E = 1,175,981.93  
P.T. STA = 406+10.63  
N = 1,839,471.89  
E = 1,176,556.83

Prop. Curve EBXC02  
P.I. STA = 412+04.19  
N = 1,839,619.04  
E = 1,177,122.66  
Δ = 20° 19' 46.56" (RT)  
D = 3° 34' 51.55"  
R = 1,600.00  
L = 567.71  
E = 25.51  
T = 286.87  
P.C. STA = 409+17.32  
N = 1,839,564.56  
E = 1,176,849.19  
P.T. STA = 414+85.03  
N = 1,839,637.50  
E = 1,177,409.20

Prop. Curve EBXC04  
P.I. STA = 418+70.80  
N = 1,839,819.04  
E = 1,177,794.53  
Δ = 30° 17' 44.12" (RT)  
D = 4° 01' 14.72"  
R = 1,425.00  
L = 753.48  
E = 51.29  
T = 385.77  
P.C. STA = 414+85.03  
N = 1,839,637.50  
E = 1,177,409.20  
P.T. STA = 422+38.51  
N = 1,839,408.71  
E = 1,178,117.92

Prop. Curve EBXC05  
P.I. STA = 427+34.61  
N = 1,839,138.24  
E = 1,178,533.79  
Δ = 5° 38' 50.84" (LT)  
D = 2° 33' 52.98"  
R = 2,234.00  
L = 220.20  
E = 2.72  
T = 110.19  
P.C. STA = 426+24.42  
N = 1,839,198.31  
E = 1,178,441.42  
P.T. STA = 428+44.62  
N = 1,839,087.55  
E = 1,178,631.63

SEE SHEET 2 of 3  
STA. 1162+00  
SB I-94  
STA. 2034+00  
NB I-94

SEE ABOVE  
STA. 416+00  
EB CONNECTOR  
STA. 333+00  
WB CONNECTOR

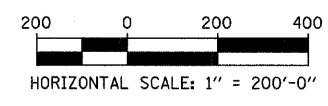
SEE ALIGNMENT SHEET 4 OF 4 FOR ADDITIONAL POINT AND CURVE DATA

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
ALIGNMENT PLAN  
SHEET 1 OF 4

SCALE: 1"=200'-0"  
DATE: OCTOBER 17, 2008

DRAWN BY: JPM  
CHECKED BY: DAK

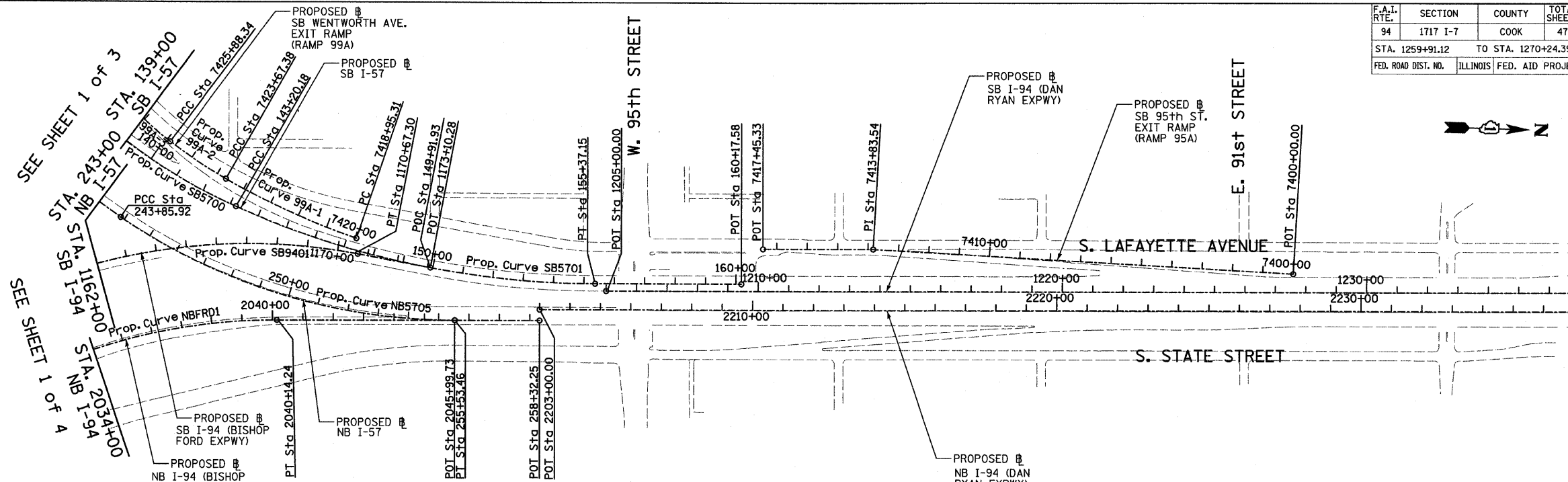


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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	7
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

62979



Prop. Curve SB5700  
P.I. STA = 133+70.06  
N = 1,839,521.70  
E = 1,177,054.03  
 $\Delta = 66^\circ 11' 55.15''$  (LT)  
D =  $3^\circ 02' 10.83''$   
R = 1,887.00  
L = 2,180.21  
E = 365.53  
T = 1,230.09  
P.C. STA = 121+39.97  
N = 1,839,491.52  
E = 1,175,824.31  
P.T. STA = 143+20.18  
N = 1,840,659.02  
E = 1,177,522.68

Prop. Curve SB5701  
P.I. STA = 149+37.75  
N = 1,841,230.00  
E = 1,177,757.97  
 $\Delta = 24^\circ 02' 07.49''$  (LT)  
D =  $1^\circ 58' 30.13''$   
R = 2,901.00  
L = 1,216.96  
E = 65.01  
T = 617.56  
P.C. STA = 143+20.18  
N = 1,840,659.02  
E = 1,177,522.68  
P.T. STA = 155+37.15  
N = 1,841,847.31  
E = 1,177,740.29

Prop. Curve NB5705  
P.I. STA = 249+87.51  
N = 1,840,788.31  
E = 1,177,892.02  
 $\Delta = 34^\circ 03' 37.98''$  (LT)  
D =  $2^\circ 55' 02.28''$   
R = 1,964.00  
L = 1,167.54  
E = 90.07  
T = 601.59  
P.C. STA = 243+85.92  
N = 1,840,280.48  
E = 1,177,569.49  
P.T. STA = 255+53.46  
N = 1,841,389.65  
E = 1,177,874.80

Prop. Curve SB9401  
P.I. STA = 1160+15.97  
N = 1,838,783.95  
E = 1,177,299.83  
 $\Delta = 100^\circ 48' 58.07''$  (RT)  
D =  $2^\circ 59' 59.20''$   
R = 1,910.00  
L = 3,360.79  
E = 1,086.95  
T = 2,309.46  
P.C. STA = 1137+06.52  
N = 1,838,851.99  
E = 1,179,608.28  
P.T. STA = 1170+67.30  
N = 1,841,064.16  
E = 1,177,666.20

SB I-94 (DAN RYAN EXPWY)  
POT STA = 1205+00.00  
N = 1,841,884.04  
E = 1,177,763.25

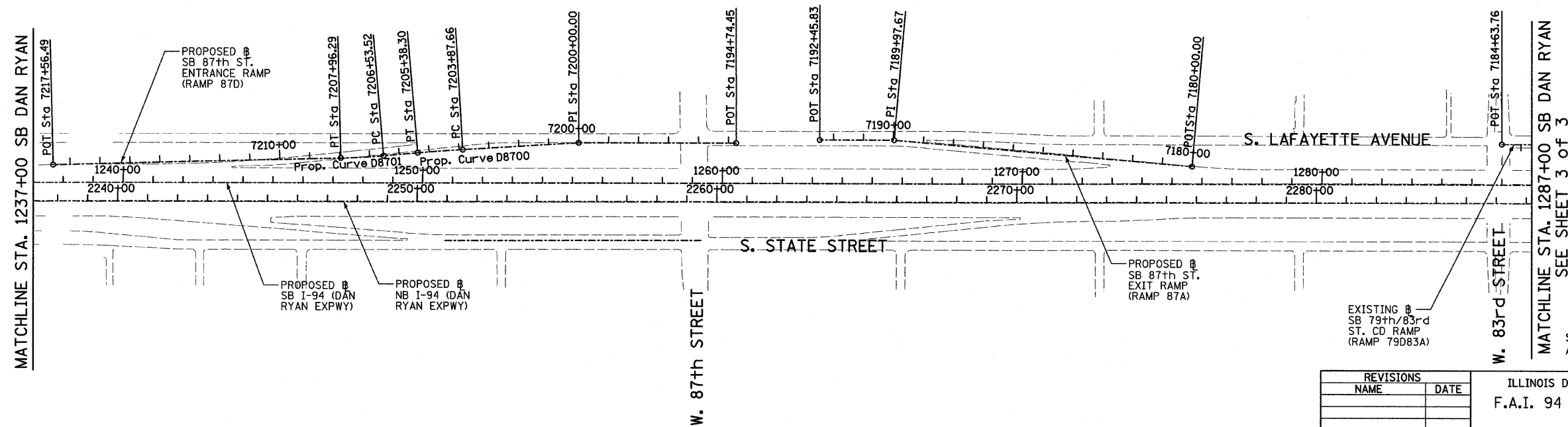
SB I-57  
POT STA = 160+17.58  
N = 1,842,327.55  
E = 1,177,726.54

SB I-94 (BISHOP FORD EXPWY)  
POT STA = 1173+10.28  
N = 1,841,304.06  
E = 1,177,704.74

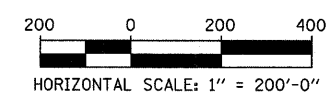
NB I-57  
POT STA = 258+32.25  
N = 1,841,668.33  
E = 1,177,866.82

NB I-94 (BISHOP FORD EXPWY)  
POT STA = 2045+99.73  
N = 1,841,389.65  
E = 1,177,874.80

NB I-94 (DAN RYAN EXPWY)  
POT STA = 2203+00.00  
N = 1,841,667.31  
E = 1,177,830.83



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REVISIONS	
NAME	DATE

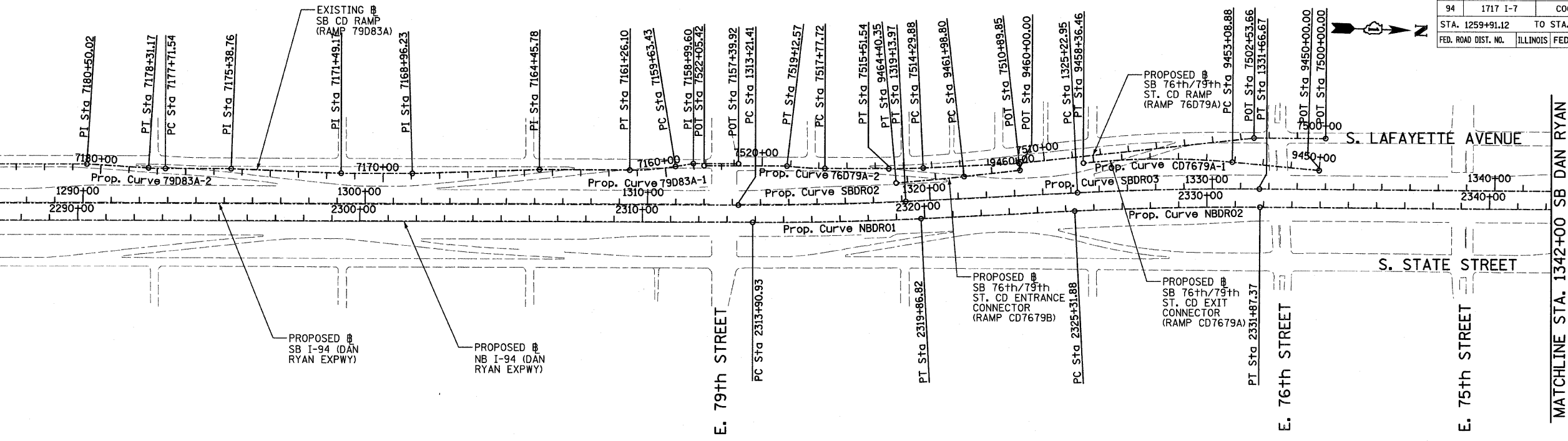
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
ALIGNMENT PLAN  
SHEET 2 OF 4

SCALE: 1"=200'  
DATE: OCTOBER 17, 2008

DRAWN BY: JPM  
CHECKED BY: DAK

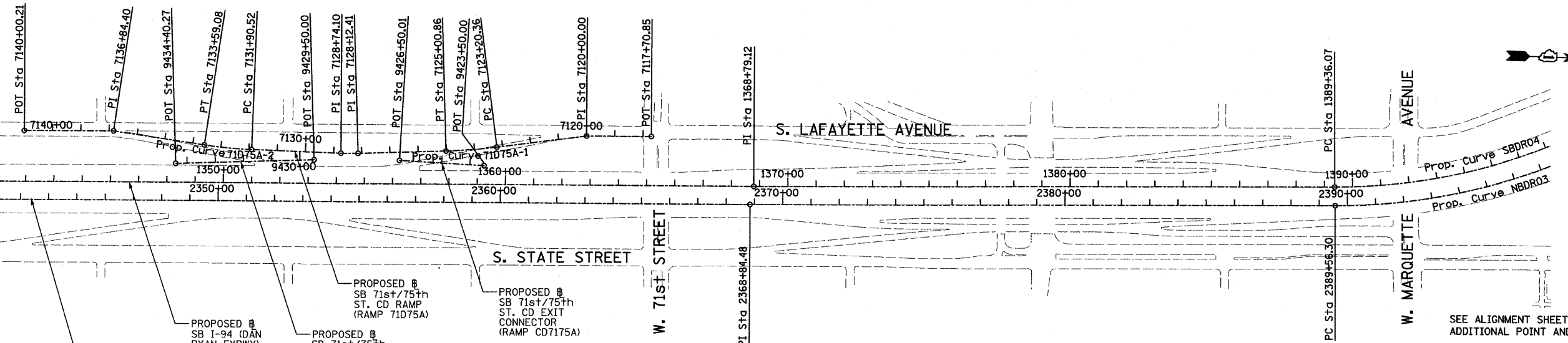
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SEE SHEET 2 OF 3  
MATCHLINE STA. 1287+00 SB DAN RYAN



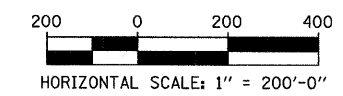
<b>Prop. Curve NBDR01</b> P.I. STA = 2316+88.94 N = 1,853,051.58 E = 1,177,504.84 $\Delta = 2^\circ 58' 38.60''$ (LT) D = $0^\circ 29' 58.77''$ R = 11,467.00 L = 595.89 E = 3.87 T = 298.01 P.C. STA = 2313+90.93 N = 1,852,753.69 E = 1,177,513.37 P.T. STA = 2319+86.82 N = 1,853,348.62 E = 1,177,480.85	<b>Prop. Curve NBDR02</b> P.I. STA = 2328+59.71 N = 1,854,218.69 E = 1,177,410.58 $\Delta = 3^\circ 16' 48.34''$ (RT) D = $0^\circ 30' 01.44''$ R = 11,450.00 L = 655.49 E = 4.69 T = 327.84 P.C. STA = 2325+31.88 N = 1,853,891.91 E = 1,177,436.97 P.T. STA = 2331+87.37 N = 1,854,546.43 E = 1,177,402.92	<b>Prop. Curve NBDR03</b> P.I. STA = 2398+13.58 N = 1,861,170.38 E = 1,177,231.19 $\Delta = 44^\circ 36' 17.12''$ (LT) D = $2^\circ 44' 29.13''$ R = 2,090.00 L = 1,627.06 E = 168.99 T = 857.27 P.C. STA = 2389+56.30 N = 1,860,313.48 E = 1,177,256.19 P.T. STA = 2405+83.37 N = 1,861,762.92 E = 1,176,611.65	<b>Prop. Curve SBDRO2</b> P.I. STA = 1316+17.76 N = 1,852,997.25 E = 1,177,445.02 $\Delta = 2^\circ 58' 38.60''$ (LT) D = $0^\circ 30' 08.86''$ R = 11,403.00 L = 592.56 E = 3.85 T = 296.35 P.C. STA = 1313+21.41 N = 1,852,701.02 E = 1,177,453.50 P.T. STA = 1319+13.97 N = 1,853,292.63 E = 1,177,421.16	<b>Prop. Curve SBDRO3</b> P.I. STA = 1328+44.90 N = 1,854,220.54 E = 1,177,346.22 $\Delta = 3^\circ 16' 11.04''$ (RT) D = $0^\circ 30' 28.59''$ R = 11,280.00 L = 643.72 E = 4.59 T = 321.95 P.C. STA = 1325+22.95 N = 1,853,899.63 E = 1,177,372.14 P.T. STA = 1331+66.67 N = 1,854,542.40 E = 1,177,338.64	<b>Prop. Curve SBDRO4</b> P.I. STA = 1397+75.78 N = 1,861,149.16 E = 1,177,163.97 $\Delta = 44^\circ 32' 58.56''$ (LT) D = $2^\circ 47' 41.70''$ R = 2,050.00 L = 1,593.95 E = 165.31 T = 839.71 P.C. STA = 1389+36.07 N = 1,860,309.83 E = 1,177,189.29 P.T. STA = 1405+30.02 N = 1,861,729.54 E = 1,176,557.11	<b>NB I-94 (DAN RYAN EXPRESSWAY)</b> P.I. STA = 2368+84.48 N = 1,858,242.53 E = 1,177,316.62  <b>SB I-94 (DAN RYAN EXPRESSWAY)</b> P.I. STA = 1368+79.12 N = 1,858,253.81 E = 1,177,251.32
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MATCHLINE STA. 1342+00 SB DAN RYAN



PROPOSED # NB I-94 (DAN RYAN EXPWY)  
 PROPOSED # SB I-94 (DAN RYAN EXPWY)  
 PROPOSED # SB 71st/75th ST. CD RAMP (RAMP 71D75A)  
 PROPOSED # SB 71st/75th ST. CD ENTRANCE CONNECTOR (RAMP CD7175B)  
 PROPOSED # SB 71st/75th ST. CD EXIT CONNECTOR (RAMP CD7175A)

TYLIN INTERNATIONAL



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 ALIGNMENT PLAN  
 SHEET 3 OF 4  
 SCALE: 1"=200'  
 DATE: OCTOBER 17, 2008  
 DRAWN BY: JPM  
 CHECKED BY: DAK

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SB HALSTED ST.  
EXIT RAMP  
(RAMP HLSTDA)  
POT STA = 7440+00.00  
N = 1,839,490.94  
E = 1,174,333.70  
P.I. STA = 7449+69.78  
N = 1,839,551.73  
E = 1,173,365.83  
POT STA = 7452+53.49  
N = 1,839,544.76  
E = 1,173,082.20

Prop. Curve 79D83A-1  
P.I. STA = 7160+44.94  
N = 1,852,394.55  
E = 1,177,340.32  
 $\Delta = 9^\circ 19' 12.90''$  (RT)  
D = 5° 43' 46.48"  
R = 1,000.00  
L = 162.67  
E = 3.32  
T = 81.51  
P.C. STA = 7159+63.43  
N = 1,852,474.58  
E = 1,177,324.82  
P.T. STA = 7161+26.10  
N = 1,852,313.07  
E = 1,177,342.65

Prop. Curve 79D83A-2  
P.I. STA = 7178+01.36  
N = 1,850,638.94  
E = 1,177,390.59  
 $\Delta = 3^\circ 24' 57.69''$  (RT)  
D = 5° 43' 46.48"  
R = 1,000.00  
L = 59.62  
E = 0.44  
T = 29.82  
P.C. STA = 7177+71.54  
N = 1,850,668.74  
E = 1,177,389.73  
P.T. STA = 7178+31.17  
N = 1,850,609.13  
E = 1,177,389.66

SB C-D RAMP  
(RAMP 79D83A)  
POT STA = 7157+39.92  
N = 1,852,696.86  
E = 1,177,308.11  
P.I. STA = 7158+99.60  
N = 1,852,537.24  
E = 1,177,312.68  
P.I. STA = 7164+45.78  
N = 1,851,993.52  
E = 1,177,351.80  
P.I. STA = 7168+96.23  
N = 1,851,543.85  
E = 1,177,378.34  
P.I. STA = 7171+49.17  
N = 1,851,291.02  
E = 1,177,385.58  
P.I. STA = 7175+38.76  
N = 1,850,901.43  
E = 1,177,383.07  
P.I. STA = 7180+50.02  
N = 1,850,390.38  
E = 1,177,382.88  
POT STA = 7184+63.76  
N = 1,849,976.81  
E = 1,177,394.72

SB WENTWORTH AVE.  
EXIT RAMP  
(RAMP 99A)  
POT STA = 7430+55.42  
N = 1,840,141.17  
E = 1,176,961.02  
POT STA = 7431+32.26  
N = 1,840,094.48  
E = 1,176,899.99  
POT STA = 7434+00.00  
N = 1,839,955.89  
E = 1,176,670.92

Prop. Curve 99A-1  
P.I. STA = 7421+32.05  
N = 1,840,832.05  
E = 1,177,544.75  
 $\Delta = 10^\circ 49' 08.53''$  (RT)  
D = 2° 17' 30.59"  
R = 2,500.00  
L = 472.07  
E = 11.18  
T = 236.74  
P.C. STA = 7418+95.31  
N = 1,841,058.12  
E = 1,177,615.03  
P.T. STA = 7423+67.38  
N = 1,840,623.20  
E = 1,177,433.29

Prop. Curve 99A-2  
P.I. STA = 7424+78.02  
N = 1,840,525.59  
E = 1,177,381.20  
 $\Delta = 7^\circ 26' 49.68''$  (RT)  
D = 3° 22' 13.22"  
R = 1,700.00  
L = 220.96  
E = 3.60  
T = 110.64  
P.C. STA = 7423+67.38  
N = 1,840,623.20  
E = 1,177,433.29  
P.T. STA = 7425+88.34  
N = 1,840,435.56  
E = 1,177,316.90

Prop. Curve 99A-3  
P.I. STA = 7428+26.42  
N = 1,840,241.82  
E = 1,177,178.53  
 $\Delta = 29^\circ 38' 02.27''$  (RT)  
D = 6° 21' 58.31"  
R = 900.00  
L = 465.49  
E = 30.96  
T = 238.08  
P.C. STA = 7425+88.34  
N = 1,840,435.56  
E = 1,177,316.90  
P.T. STA = 7430+53.83  
N = 1,840,141.84  
E = 1,176,962.46

SB 76th/79th ST.  
CD RAMP  
(RAMP 76D79A)  
POT STA = 7500+00.00  
N = 1,854,771.42  
E = 1,177,153.21  
P.I. STA = 7502+53.66  
N = 1,854,517.84  
E = 1,177,159.17  
P.I. STA = 7510+89.85  
N = 1,853,689.73  
E = 1,177,275.17  
POT STA = 7522+05.42  
N = 1,852,575.71  
E = 1,177,319.03

Prop. Curve 76D79A-1  
P.I. STA = 7514+90.72  
N = 1,853,290.06  
E = 1,177,306.25  
 $\Delta = 2^\circ 48' 22.72''$  (RT)  
D = 2° 18' 23.74"  
R = 2,484.00  
L = 121.66  
E = 0.75  
T = 60.84  
P.C. STA = 7514+29.88  
N = 1,853,350.72  
E = 1,177,301.54  
P.T. STA = 7515+51.54  
N = 1,853,229.24  
E = 1,177,307.99

Prop. Curve 76D79A-2  
P.I. STA = 7518+45.22  
N = 1,852,935.69  
E = 1,177,316.40  
 $\Delta = 6^\circ 31' 33.29''$  (RT)  
D = 4° 50' 21.01"  
R = 1,184.00  
L = 134.86  
E = 1.92  
T = 67.50  
P.C. STA = 7517+77.72  
N = 1,853,003.16  
E = 1,177,314.47  
P.T. STA = 7519+12.57  
N = 1,852,868.44  
E = 1,177,310.65

SB 76th/79th ST.  
CD EXIT CONNECTOR  
(CD7679A)  
POT STA = 9450+00.00  
N = 1,854,751.70  
E = 1,177,267.37  
Prop. Curve CD7679A-1  
P.I. STA = 9455+73.58  
N = 1,854,179.28  
E = 1,177,230.83  
 $\Delta = 11^\circ 37' 34.33''$  (LT)  
D = 2° 12' 13.26"  
R = 2,600.00  
L = 527.58  
E = 13.44  
T = 264.70  
P.C. STA = 9453+08.88  
N = 1,854,443.44  
E = 1,177,247.69  
P.T. STA = 9458+36.46  
N = 1,853,917.14  
E = 1,177,267.55

SB 76th/79th ST.  
CD ENTRANCE CONNECTOR  
(CD7679B)  
POT STA = 9460+00.00  
N = 1,853,693.06  
E = 1,177,298.94  
Prop. Curve CD7679B-1  
P.I. STA = 9463+19.58  
N = 1,853,376.57  
E = 1,177,343.27  
 $\Delta = 1^\circ 13' 13.54''$  (RT)  
D = 0° 30' 18.91"  
R = 11,340.00  
L = 241.55  
E = 0.64  
T = 120.78  
P.C. STA = 9461+98.80  
N = 1,853,496.18  
E = 1,177,326.52  
P.T. STA = 9464+40.35  
N = 1,853,256.63  
E = 1,177,357.48

SB 95th ST. EXIT RAMP  
(RAMP 95A)  
POT STA = 7400+00.00  
N = 1,844,139.73  
E = 1,177,638.63  
P.I. STA = 7413+83.54  
N = 1,842,756.77  
E = 1,177,598.60  
POT STA = 7417+45.33  
N = 1,842,395.13  
E = 1,177,608.95

Prop. Curve D8700  
P.I. STA = 7204+62.99  
N = 1,846,437.54  
E = 1,177,524.69  
 $\Delta = 1^\circ 30' 03.72''$  (LT)  
D = 0° 59' 47.21"  
R = 5,750.00  
L = 150.64  
E = 0.49  
T = 75.32  
P.C. STA = 7203+87.66  
N = 1,846,512.58  
E = 1,177,518.09  
P.T. STA = 7205+38.30  
N = 1,846,362.71  
E = 1,177,533.26

Prop. Curve D8701  
P.I. STA = 7207+24.93  
N = 1,846,177.29  
E = 1,177,554.49  
 $\Delta = 3^\circ 27' 36.43''$  (RT)  
D = 2° 25' 25.25"  
R = 2,364.00  
L = 142.76  
E = 1.08  
T = 71.40  
P.C. STA = 7206+53.52  
N = 1,846,248.23  
E = 1,177,546.37  
P.T. STA = 7207+96.29  
N = 1,846,105.99  
E = 1,177,558.32

SB 87th ST.  
ENTRANCE RAMP  
(RAMP 87D)  
POT STA = 7194+74.45  
N = 1,847,424.06  
E = 1,177,468.58  
P.I. STA = 7200+00.00  
N = 1,846,898.74  
E = 1,177,484.09  
POT STA = 7217+56.49  
N = 1,845,147.17  
E = 1,177,609.78

Prop. Curve D8700  
P.I. STA = 7204+62.99  
N = 1,846,437.54  
E = 1,177,524.69  
 $\Delta = 1^\circ 30' 03.72''$  (LT)  
D = 0° 59' 47.21"  
R = 5,750.00  
L = 150.64  
E = 0.49  
T = 75.32  
P.C. STA = 7203+87.66  
N = 1,846,512.58  
E = 1,177,518.09  
P.T. STA = 7205+38.30  
N = 1,846,362.71  
E = 1,177,533.26

Prop. Curve D8701  
P.I. STA = 7207+24.93  
N = 1,846,177.29  
E = 1,177,554.49  
 $\Delta = 3^\circ 27' 36.43''$  (RT)  
D = 2° 25' 25.25"  
R = 2,364.00  
L = 142.76  
E = 1.08  
T = 71.40  
P.C. STA = 7206+53.52  
N = 1,846,248.23  
E = 1,177,546.37  
P.T. STA = 7207+96.29  
N = 1,846,105.99  
E = 1,177,558.32

SB 87th ST. EXIT RAMP  
(RAMP 87A)  
POT STA = 7180+00.00  
N = 1,848,946.30  
E = 1,177,501.00  
P.I. STA = 7189+97.67  
N = 1,847,950.35  
E = 1,177,442.53  
POT STA = 7192+45.83  
N = 1,847,702.29  
E = 1,177,449.63

SB 71st/75th ST.  
CD RAMP  
(RAMP 71D75A)  
POT STA = 7117+70.85  
N = 1,857,886.40  
E = 1,177,086.26  
P.I. STA = 7120+00.00  
N = 1,857,657.31  
E = 1,177,091.65  
P.I. STA = 7128+12.41  
N = 1,856,850.29  
E = 1,177,177.64  
P.I. STA = 7128+74.10  
N = 1,856,788.62  
E = 1,177,179.10  
P.I. STA = 7136+84.40  
N = 1,855,981.27  
E = 1,177,124.75  
POT = 7140+00.21  
N = 1,855,665.56  
E = 1,177,132.18

Prop. Curve 71D75A-1  
P.I. STA = 7124+10.68  
N = 1,857,251.52  
E = 1,177,154.81  
 $\Delta = 5^\circ 35' 23.99''$  (RT)  
D = 3° 05' 49.45"  
R = 1,850.00  
L = 180.49  
E = 2.20  
T = 90.32  
P.C. STA = 7123+20.36  
N = 1,857,340.76  
E = 1,177,140.92  
P.T. STA = 7125+00.86  
N = 1,857,161.34  
E = 1,177,159.94

Prop. Curve 71D75A-2  
P.I. STA = 7132+74.89  
N = 1,856,387.85  
E = 1,177,175.17  
 $\Delta = 6^\circ 30' 28.18''$  (RT)  
D = 3° 51' 39.25"  
R = 1,484.00  
L = 168.56  
E = 2.40  
T = 84.37  
P.C. STA = 7131+90.52  
N = 1,856,472.22  
E = 1,177,176.00  
P.T. STA = 7133+59.08  
N = 1,856,304.13  
E = 1,177,164.79

SB 71st/75th ST.  
CD EXIT CONNECTOR  
(RAMP CD7175A)  
POT STA = 9423+50.00  
N = 1,857,297.2866  
E = 1,177,207.4713  
POT STA = 9426+50.01  
N = 1,856,997.4225  
E = 1,177,198.1891  
CD ENTRANCE CONNECTOR  
(RAMP CD7175B)  
POT STA = 9429+50.00  
N = 1,856,694.2083  
E = 1,177,205.3235  
POT STA = 9434+40.27  
N = 1,856,204.7281  
E = 1,177,233.1786

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
  
ALIGNMENT PLAN  
SHEET 4 OF 4  
  
SCALE: NO SCALE  
DATE: OCTOBER 17, 2008  
  
DRAWN BY: JPA  
CHECKED BY: JPM

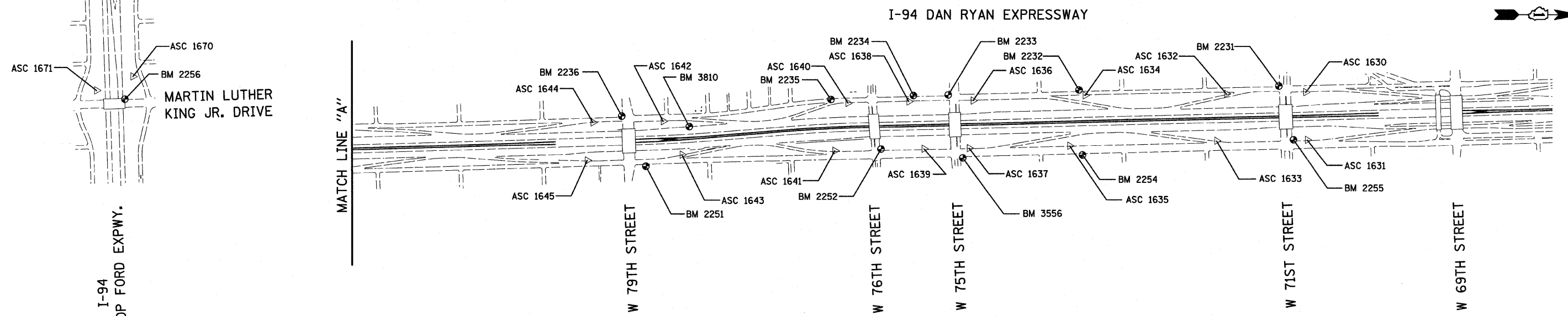
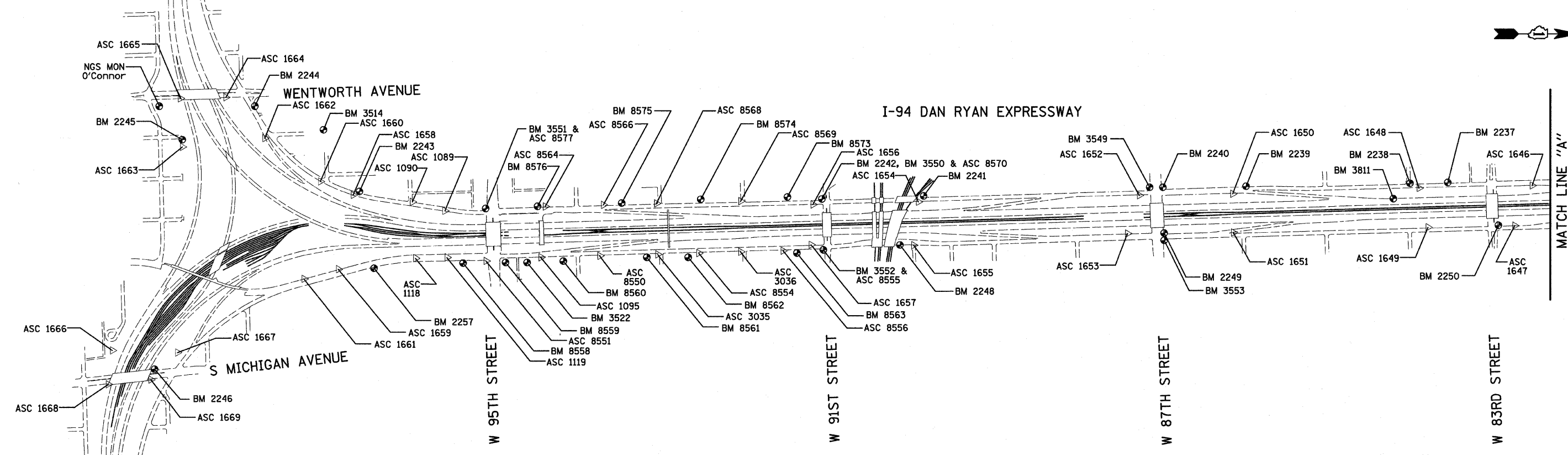


10/16/2008 4:06:20 PM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	10
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

62979

I-57 EXPWY  
SEE SHEET 2 OF 7  
MATCH LINE "B"



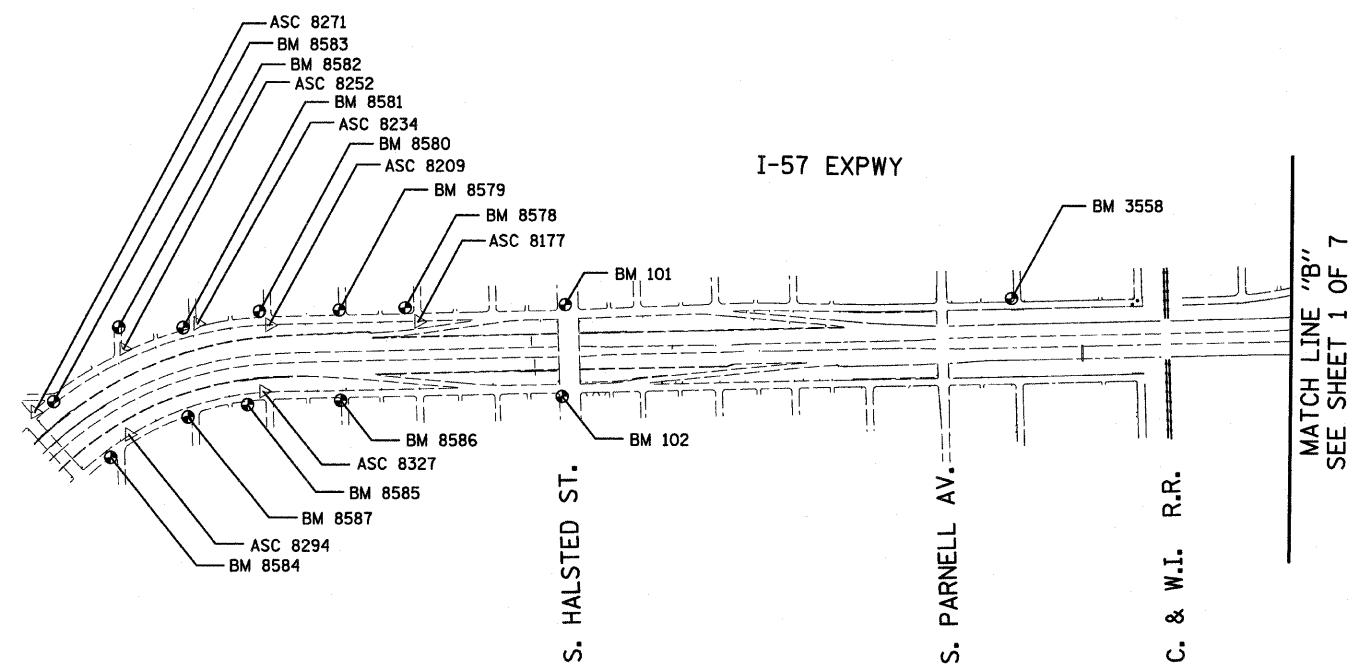
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
SURVEY TIES FOR CONTROL POINTS  
SHEET 1 OF 7  
SCALE: 1"=400'  
DATE: OCTOBER 17, 2008  
DRAWN BY: GSP  
CHECKED BY: MMW

10/16/2008 4:06:21 PM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	11
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

62979



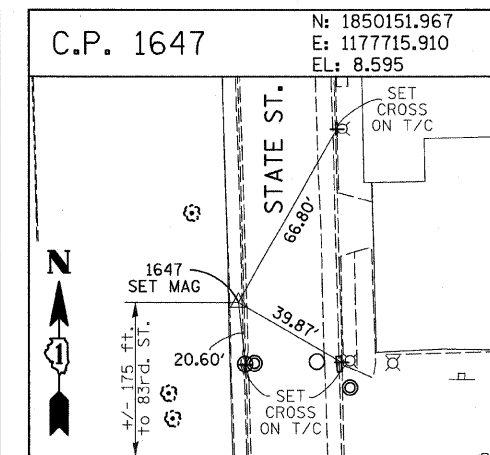
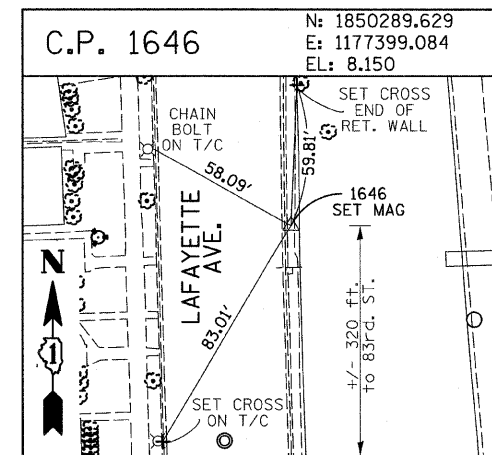
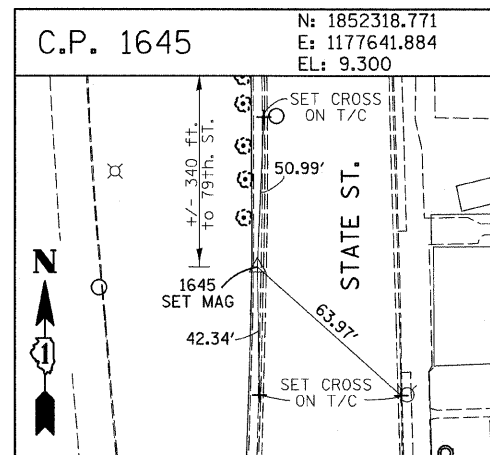
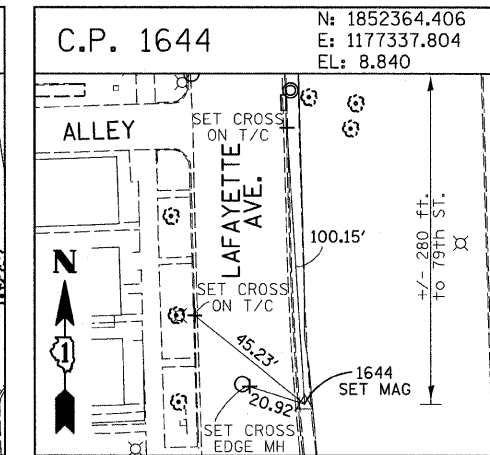
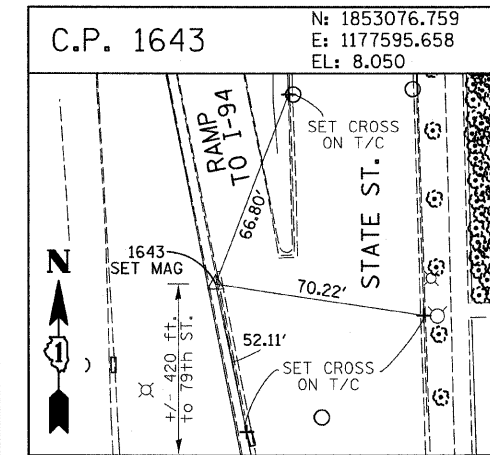
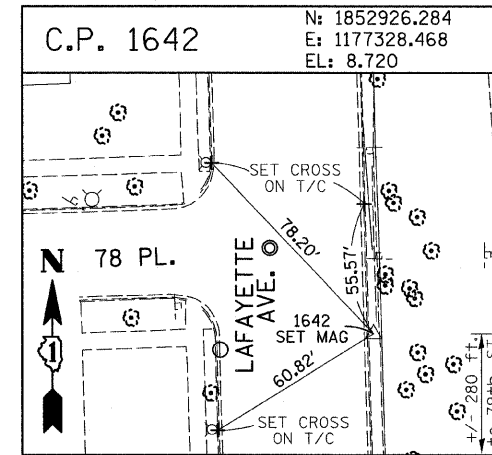
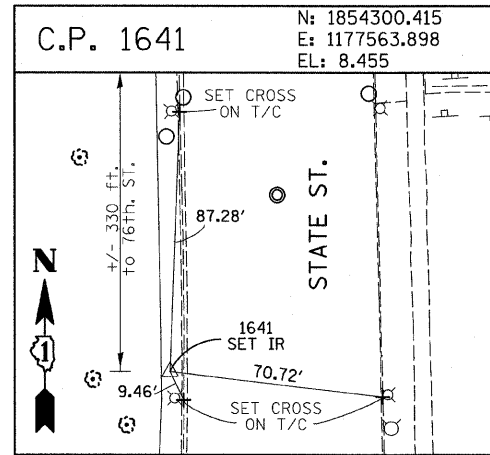
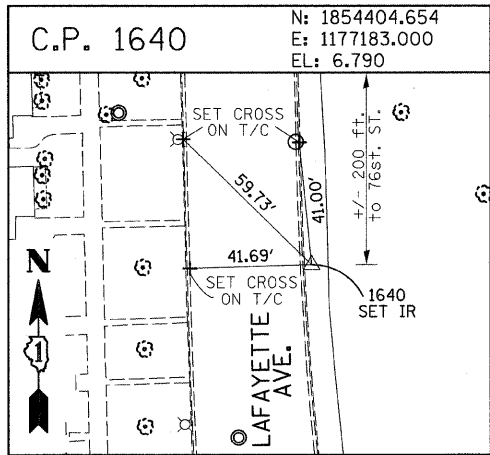
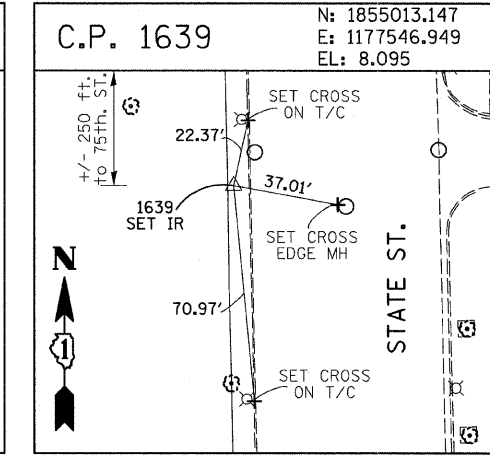
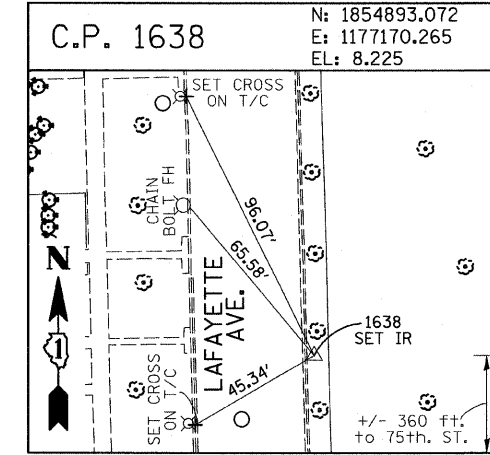
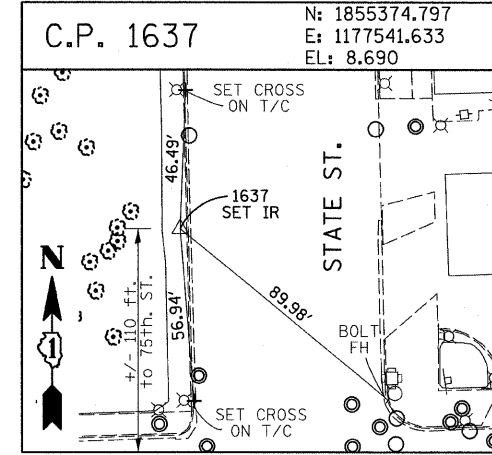
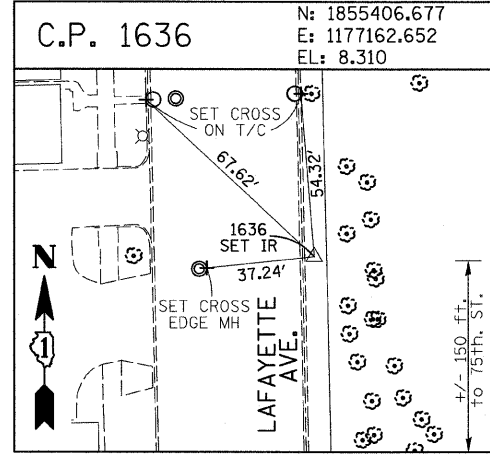
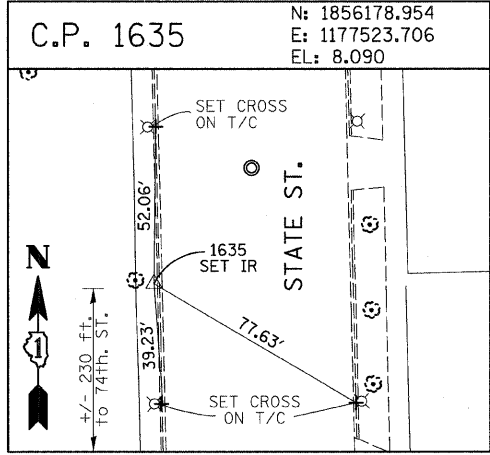
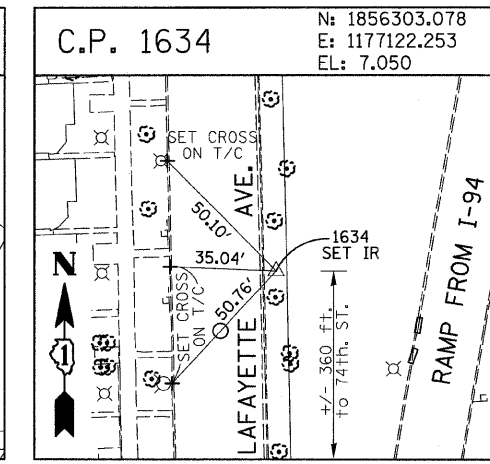
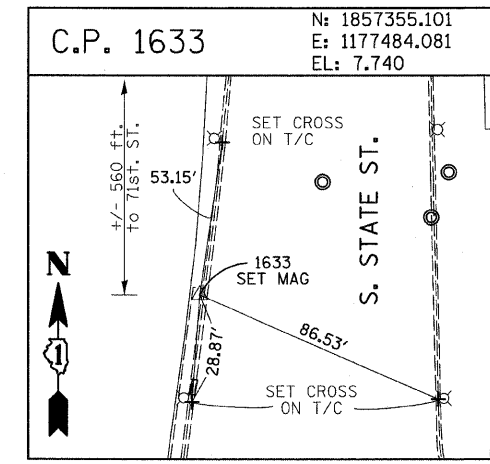
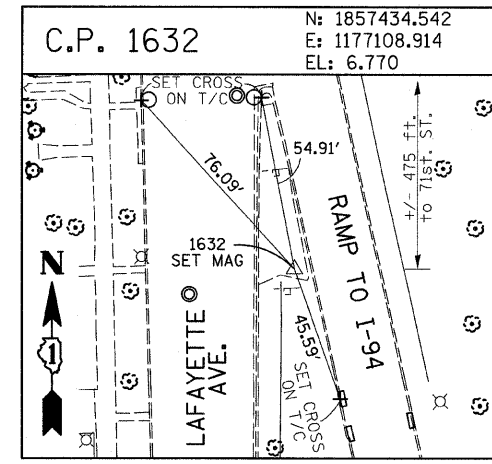
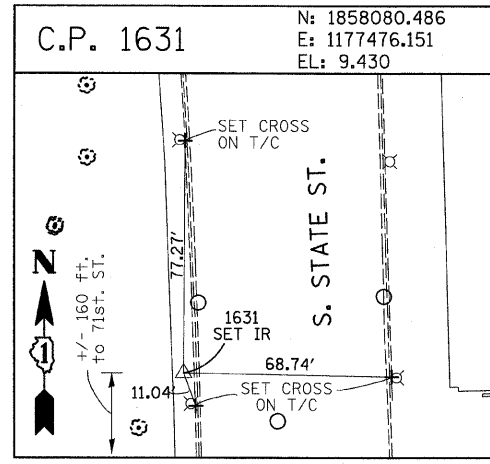
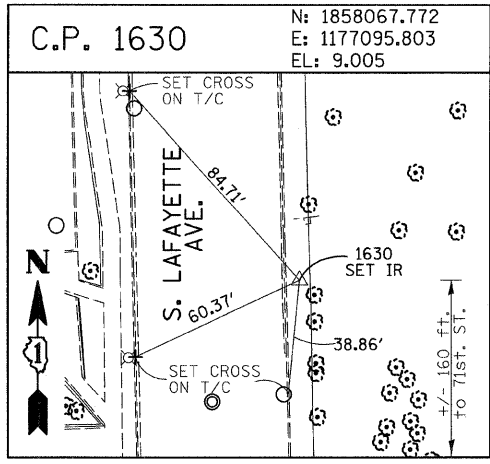
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 SURVEY TIES FOR CONTROL POINTS  
 SHEET 2 OF 7

SCALE: 1"=400'  
 DATE: OCTOBER 17, 2008  
 DRAWN BY: GSP  
 CHECKED BY: MMW

10/16/2008 4:08:22 PM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	12
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
62979				



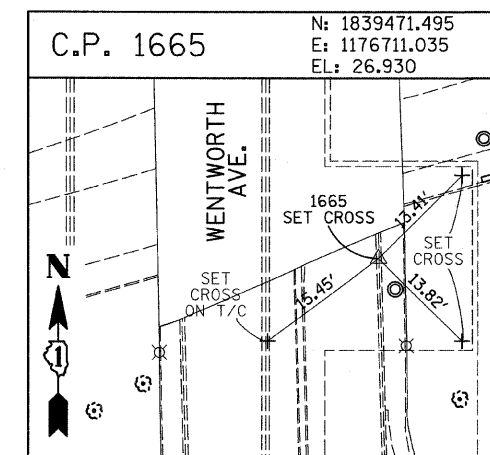
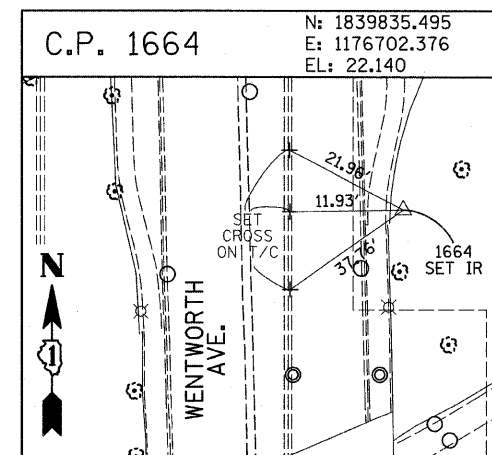
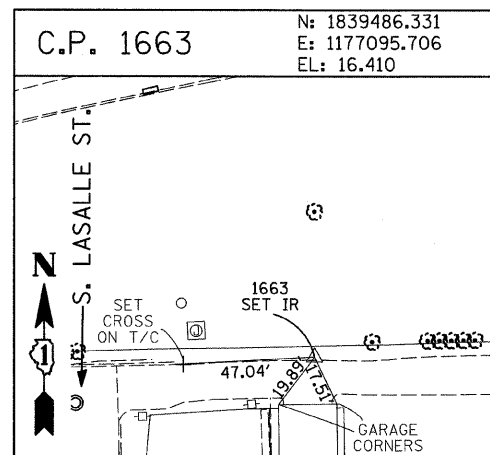
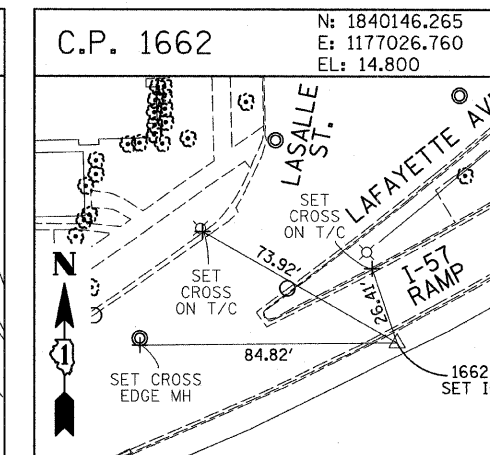
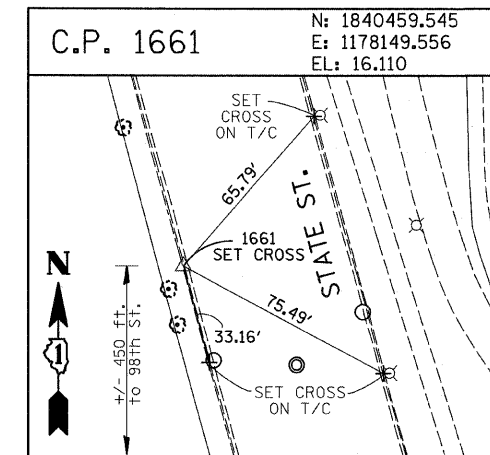
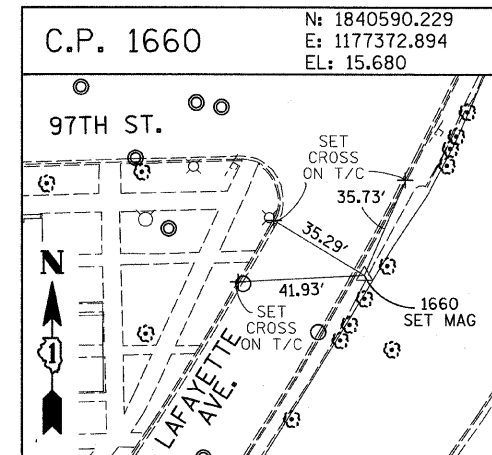
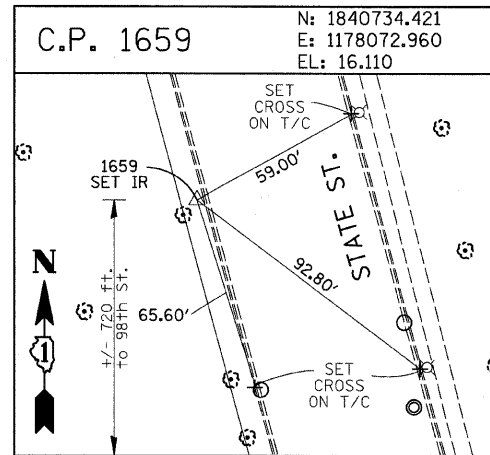
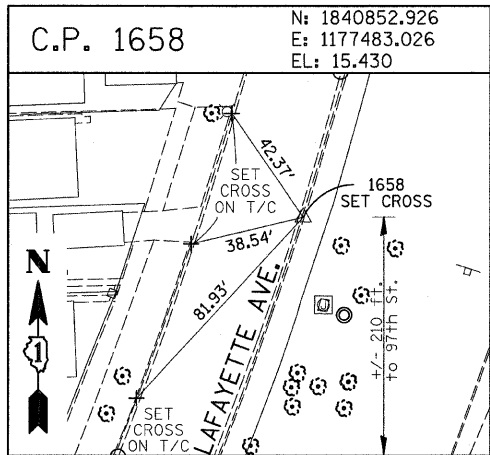
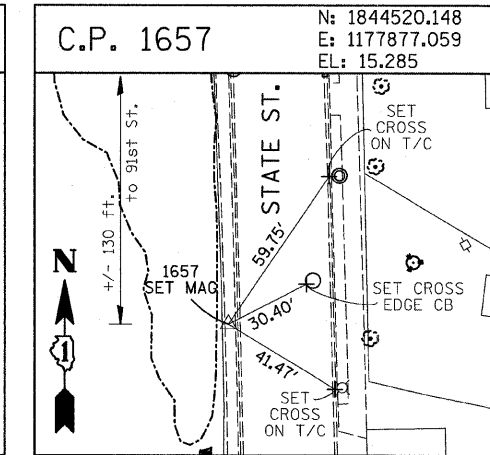
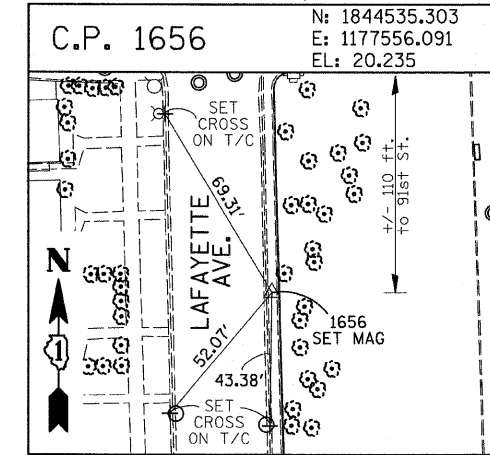
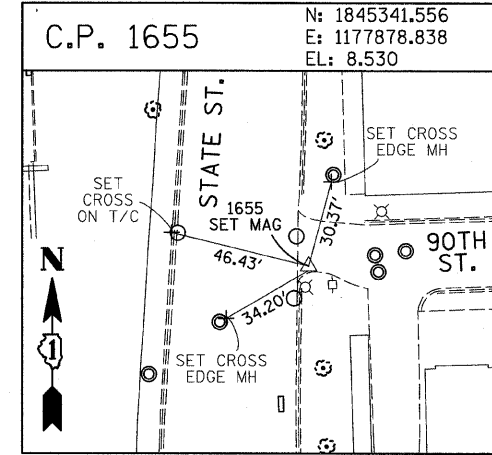
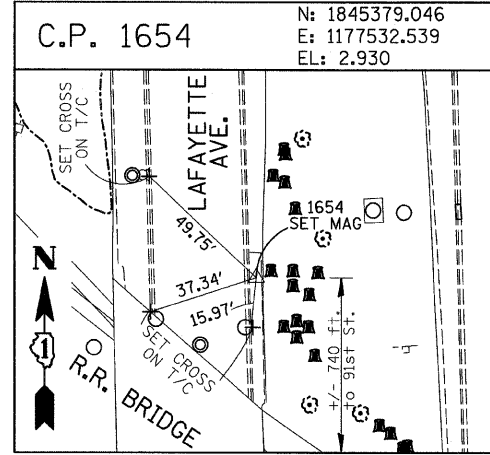
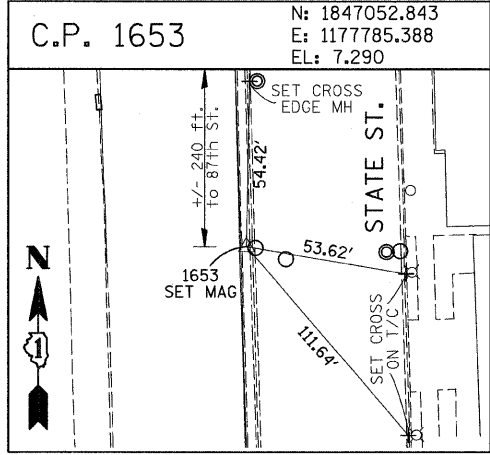
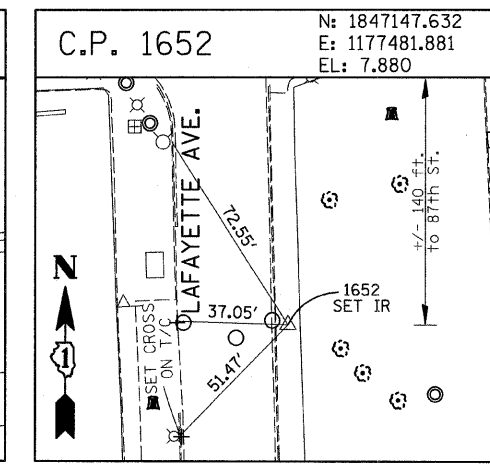
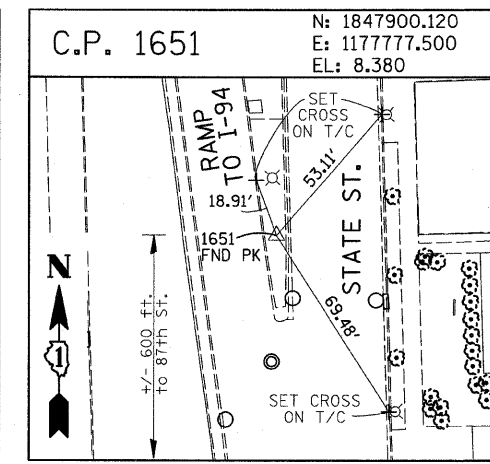
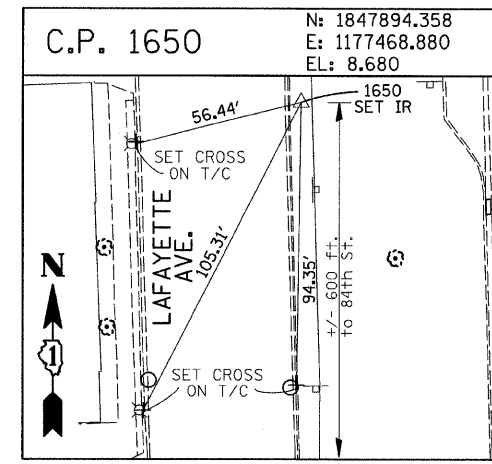
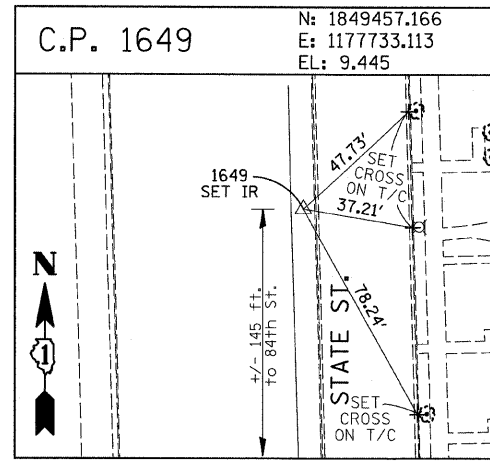
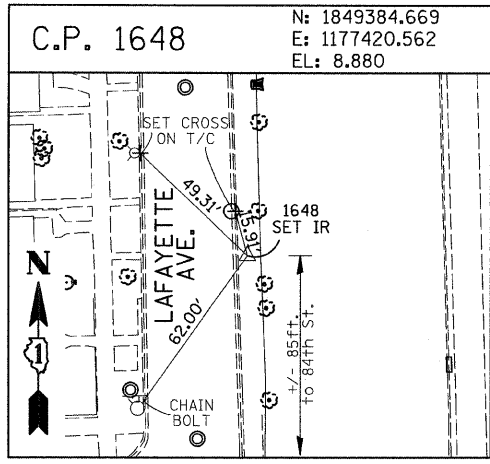
**American Surveying Consultants, P.C.**  
1877 Park St., Suite 200, Chicago, IL 60612  
Tel: 312-281-2222 Fax: 312-281-2223  
1877 Park St., Suite 200, Chicago, IL 60612  
Tel: 312-281-2222 Fax: 312-281-2223

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
SURVEY TIES FOR CONTROL POINTS  
SHEET 3 OF 7  
SCALE: 1"=30'  
DATE: OCTOBER 17, 2008  
DRAWN BY: GSP  
CHECKED BY: MMW

10/16/2008 4:06:23 PM P:\02373\road\62979\1265812\3b.dgn

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	13
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS		FED. AID PROJECT
				62979

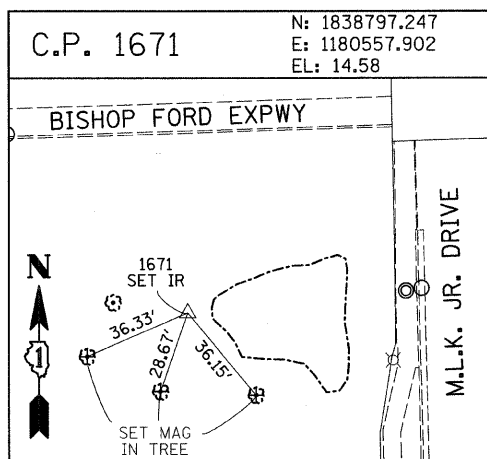
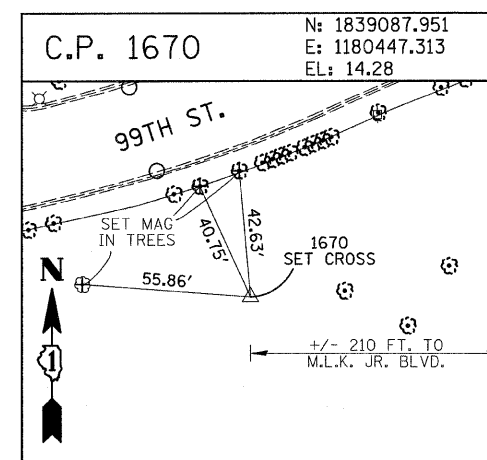
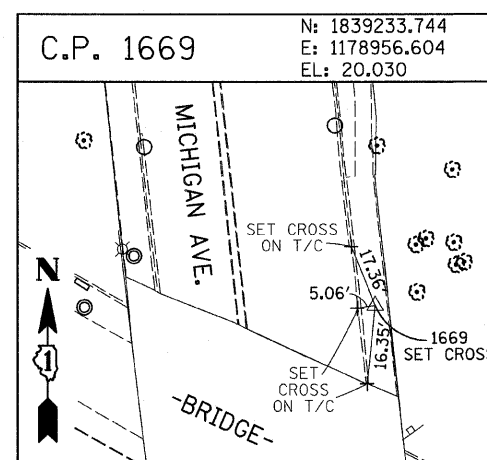
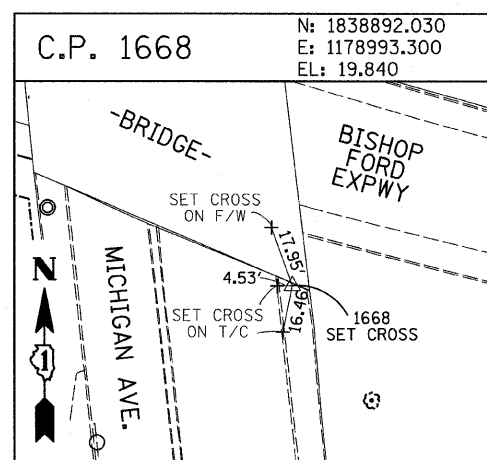
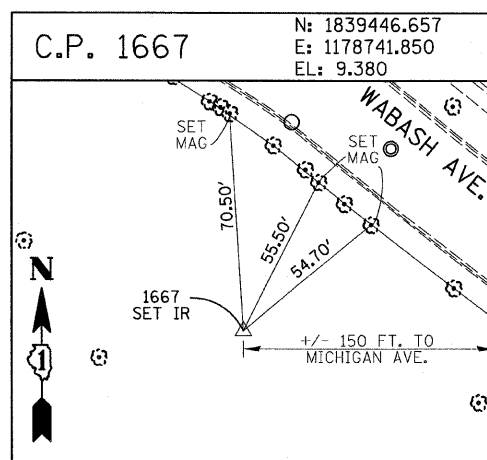
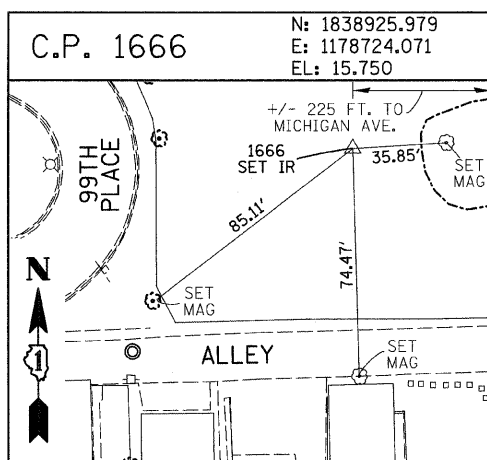


American Surveying Consultants, P.C.  
 1837 First Pl. N., Naperville, IL 60563

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 SURVEY TIES FOR CONTROL POINTS  
 SHEET 4 OF 7  
 SCALE: 1"=30'  
 DATE: OCTOBER 17, 2008  
 DRAWN BY: GSP  
 CHECKED BY: MMW

10/16/2008 4:06:24 PM  
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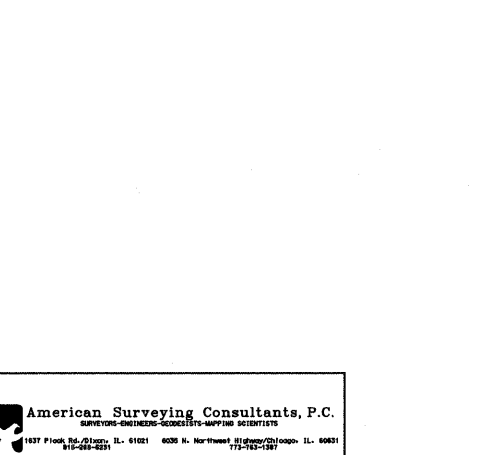
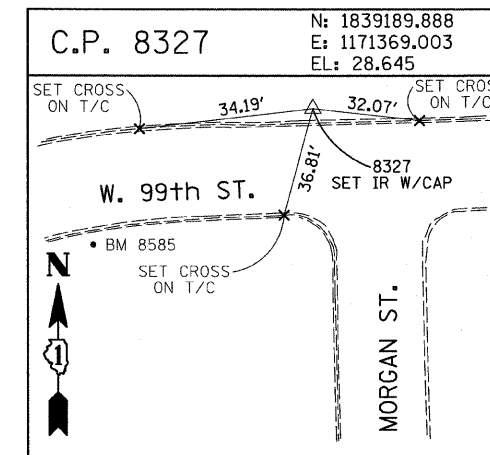
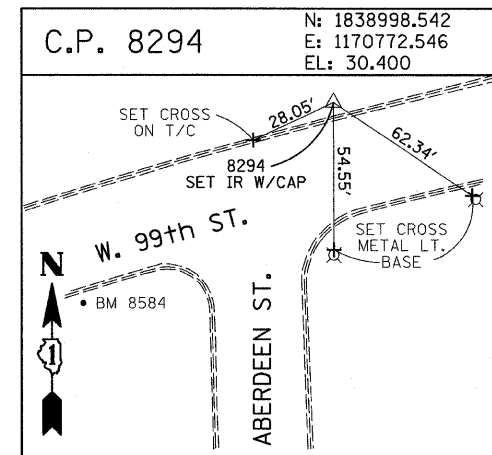
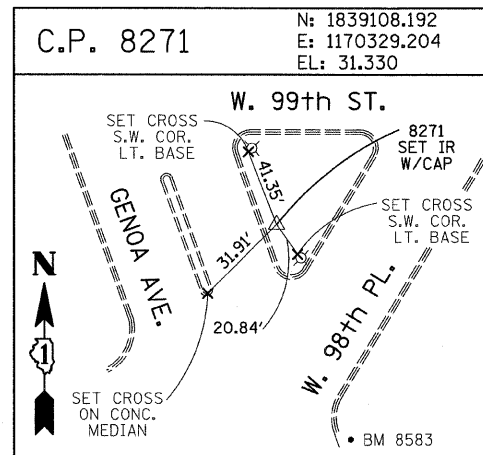
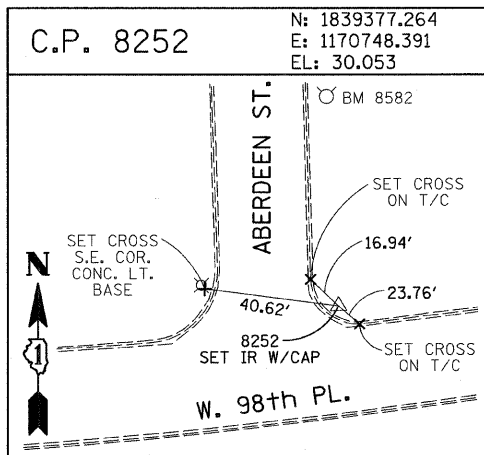
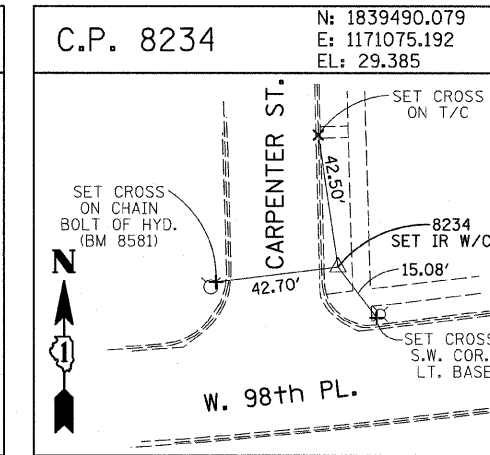
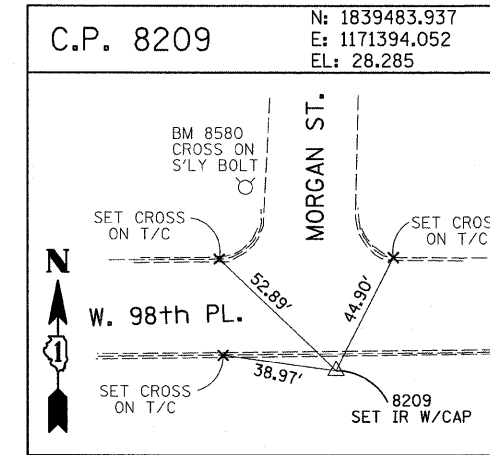
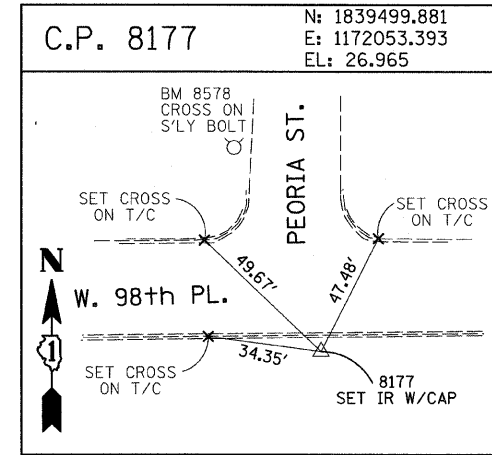
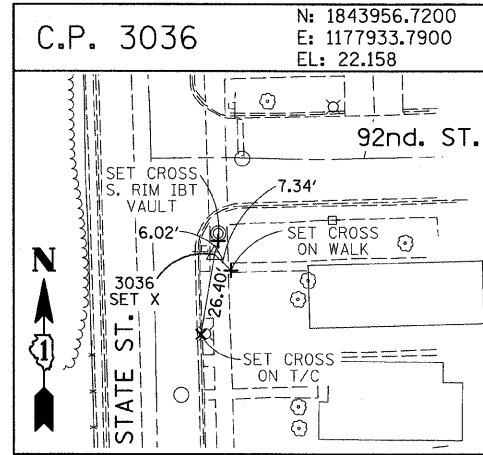
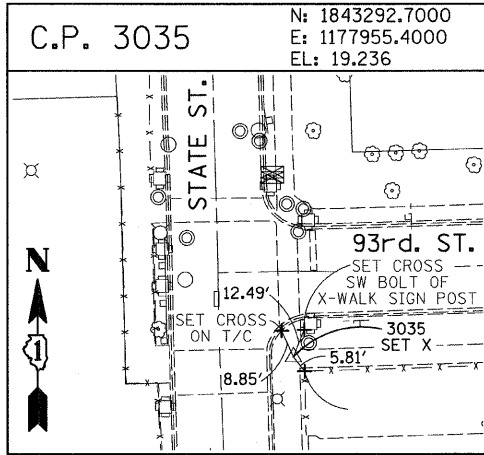
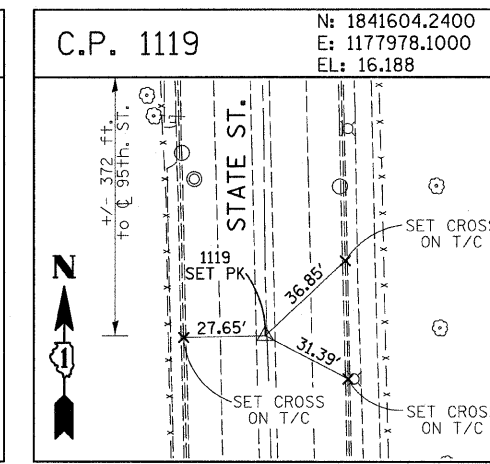
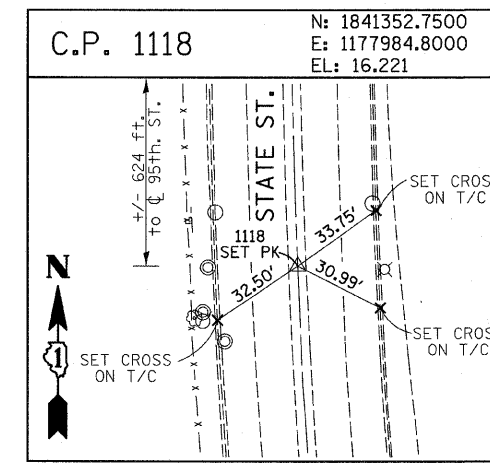
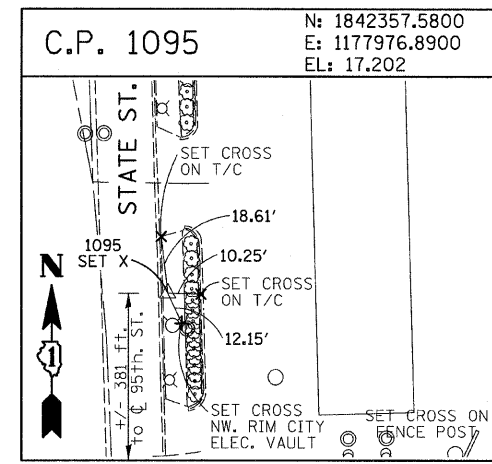
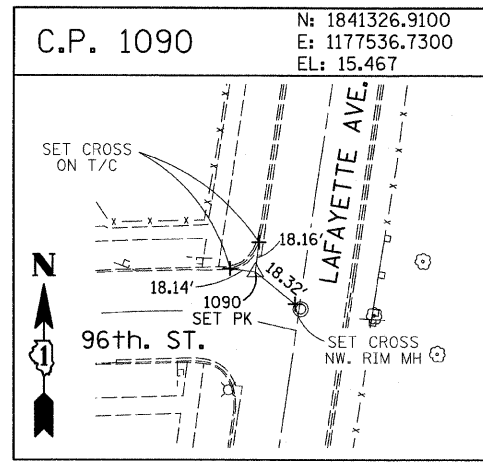
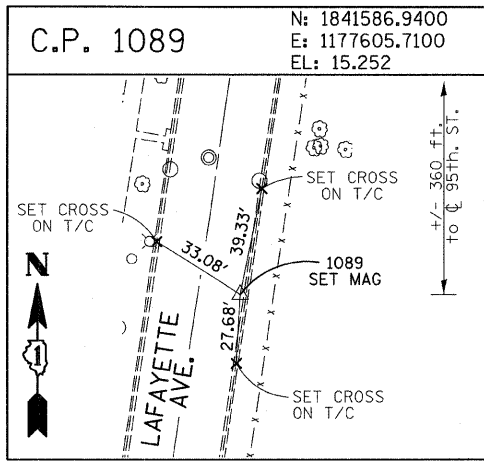
**PROJECT BENCHMARKS**

- BM 2231 ELEVATION= 10.24 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 71ST STREET AND LAFAYETTE AVENUE.
- BM 2232 ELEVATION= 8.46 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 73RD STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 330 FEET SOUTH OF 73RD STREET.
- BM 2233 ELEVATION= 9.90 FEET  
SET CROSS ON NORTHWEST BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 75TH STREET AND LAFAYETTE AVENUE.
- BM 2234 ELEVATION= 9.56 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 75TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 320 FEET SOUTH OF 75TH STREET.
- BM 2235 ELEVATION= 7.50 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 76TH STREET ON THE WEST SIDE OF LAFAYETTE. APPROXIMATELY 330 FEET SOUTH OF 76TH STREET.
- BM 2236 ELEVATION= 10.41 FEET  
SET CROSS ON WEST BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 79TH STREET AND LAFAYETTE AVENUE.
- BM 2237 ELEVATION= 9.20 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF 83RD STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 350 FEET NORTH OF 83RD STREET.
- BM 2238 ELEVATION= 10.04 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT OF THE NORTHWEST CORNER OF 4TH STREET AND LAFAYETTE AVENUE.
- BM 2239 ELEVATION= 9.59 FEET  
SET CROSS ON CHAIN BOLT OF SECOND FIRE HYDRANT NORTH OF 87TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 720 FEET NORTH OF 87TH STREET.
- BM 2240 ELEVATION= 9.49 FEET  
SET CROSS ON THE NORTHWEST BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 78TH STREET AND LAFAYETTE AVENUE.
- BM 2241 ELEVATION= 3.63 FEET  
SET SQUARE CUT ON TOP OF CURB NEXT TO LIGHT POLE ON THE NORTHWEST CORNER OF LAFAYETTE AVENUE AND RAILROAD BRIDGE. APPROXIMATELY 770 FEET NORTH OF 91ST STREET.
- BM 2242 ELEVATION= 22.82 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 91ST STREET AND LAFAYETTE AVENUE.
- BM 2243 ELEVATION= 15.68 FEET  
SET SQUARE CUT ON THE TOP OF CURB NEXT TO THIRD LIGHT POLE NORTH OF 97TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 260 FEET NORTH OF 97TH STREET.
- BM 2244 ELEVATION= 15.94 FEET  
SET SQUARE CUT ON TOP OF CURB NEXT TO LIGHT POLE ON THE NORTH SIDE OF 98TH STREET. APPROXIMATELY 120 FEET EAST OF WENTWORTH AVENUE.

- BM 2245 ELEVATION= 16.35 FEET  
SET SQUARE CUT ON THE EAST END OF CURB AT THE NORTHEAST CORNER OF FIRST ALLEY NORTH OF 99TH STREET. APPROXIMATELY 35 FEET EAST OF LASALLE STREET.
- BM 2246 ELEVATION= 20.00 FEET  
SET SQUARE CUT WITH CROSS ON TOP OF ABUTMENT WALL ON THE NORTHWEST CORNER OF STATE STREET BRIDGE OVER THE BISHOP FORD EXPRESSWAY. APPROXIMATELY 20 FEET NORTH OF THE NORTH EDGE OF THE BRIDGE.
- BM 2248 ELEVATION= 9.77 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF RAILROAD BRIDGE ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 110 FEET SOUTH OF 90TH STREET.
- BM 2249 ELEVATION= 9.67 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 87TH AND STATE STREET.
- BM 2250 ELEVATION= 10.03 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 83RD ST. AND STATE STREET.
- BM 2251 ELEVATION= 10.95 FEET  
FOUND CROSS ON EAST BOLT OF FIRE HYDRANT ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 135 FEET NORTH OF 79TH STREET.
- BM 2252 ELEVATION= 10.02 FEET  
SET CROSS ON WEST BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF STATE STREET AND 76TH STREET.
- BM 2254 ELEVATION= 9.17 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 73RD STREET ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 330 FEET SOUTH OF 73RD STREET.
- BM 2255 ELEVATION= 11.25 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 71ST STREET AND STATE STREET.
- BM 2256 ELEVATION= 16.36 FEET  
SET SQUARE CUT ON TOP OF ABUTMENT WALL ON THE NORTHWEST CORNER OF MARTIN LUTHER KING DRIVE BRIDGE OVER THE BISHOP FORD EXPRESSWAY. APPROXIMATELY 1 FOOT NORTH OF THE NORTH END OF THE BRIDGE.
- BM 2257 ELEVATION= 18.25 FEET  
SET CROSS ON NORTHEAST BOLT OF LIGHT POLE BASE ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 970 FEET SOUTH OF 95TH STREET.
- BM 2258 ELEVATION= 10.53 FEET  
SET CROSS ON NORTHWEST BOLT OF LIGHT BASE ON NORTHWEST CORNER OF 71ST STREET AND LAFAYETTE AVENUE.

REVISIONS	
NAME	DATE

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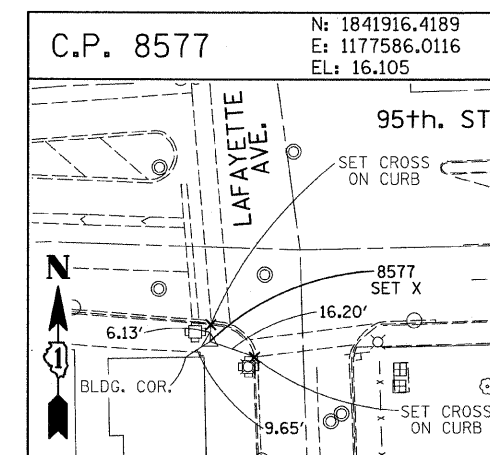
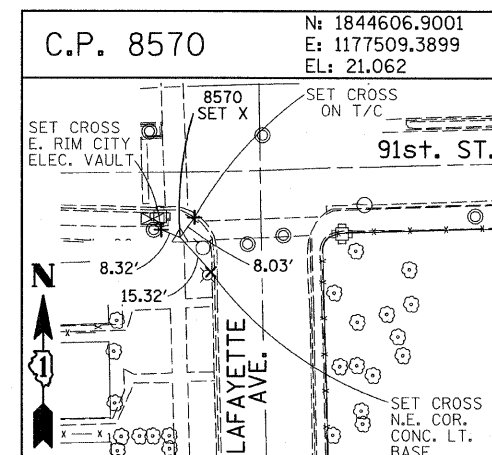
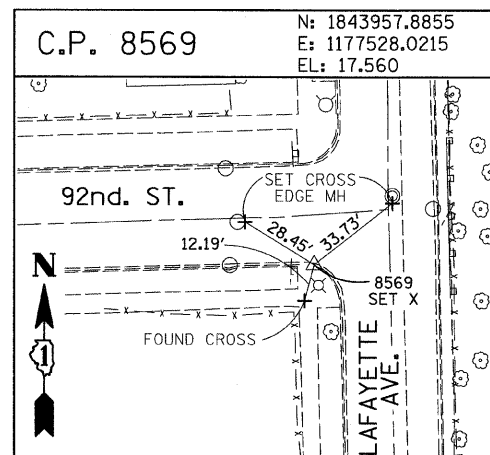
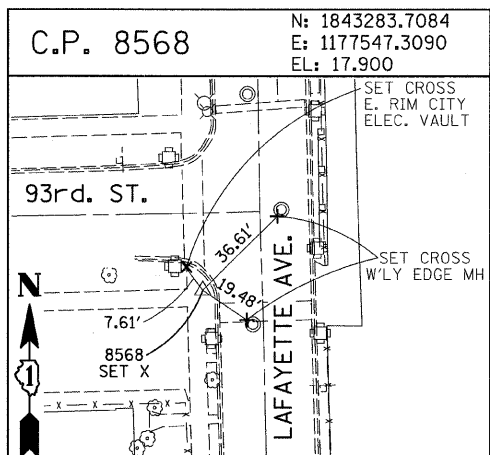
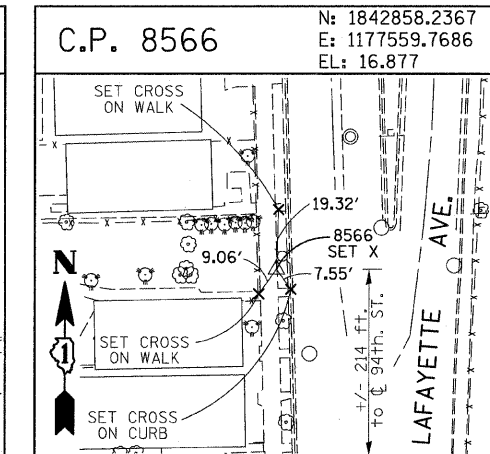
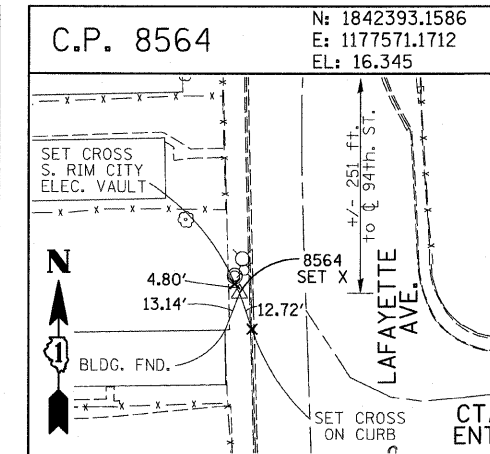
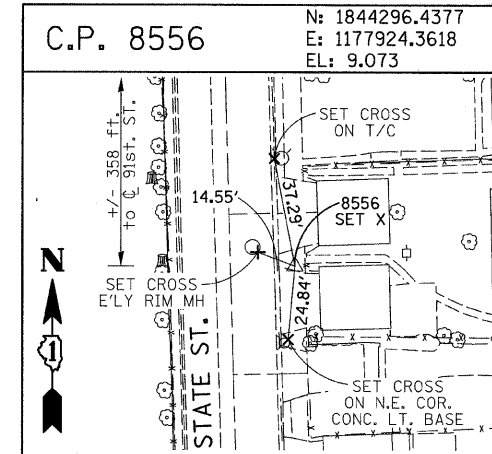
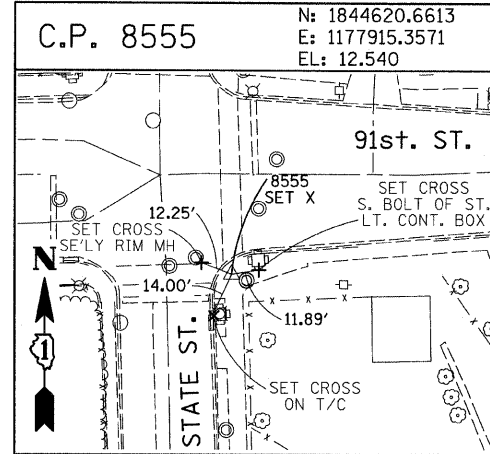
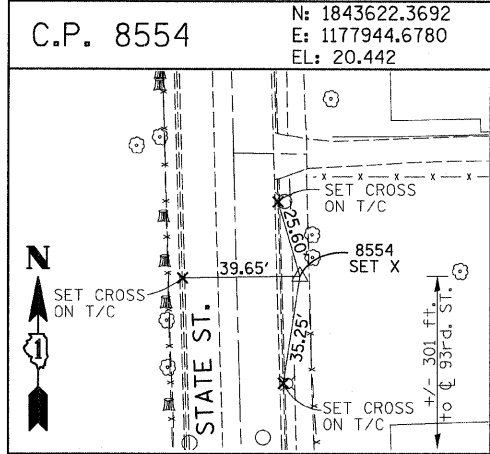


REVISIONS	
NAME	DATE

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	16
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

62979



**PROJECT BENCHMARKS**

BM 101 ELEVATION= 24.89 FEET  
SET SQUARE CUT ON SOUTHERLY CURB OF WEST 98TH PLACE. APPROXIMATELY 70 FEET EAST OF THE CENTERLINE OF GREEN STREET.

BM 102 ELEVATION= 27.36 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF WEST 99TH STREET AND GREEN STREET.

BM 8558 ELEVATION= 17.73 FEET  
SET SQUARE CUT ON SOUTHEAST CORNER OF FOURTH LIGHT BASE SOUTH OF 95TH STREET, ON THE EAST SIDE OF STATE STREET.

BM 8559 ELEVATION= 17.10 FEET  
SET SQUARE CUT ON SOUTHWEST CORNER OF CONCRETE SIGN BASE AT THE NORTHWEST CORNER OF THE MOBIL GAS STATION PROPERTY AT THE NORTHEAST CORNER OF 95TH STREET AND STATE STREET.

BM 8560 ELEVATION= 17.50 FEET  
SET SQUARE CUT ON SOUTHEAST CORNER OF CONCRETE BUS SLAB ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 50 FEET SOUTH OF THE CENTERLINE OF 94TH STREET.

BM 8561 ELEVATION= 19.77 FEET  
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT SOUTH OF 93RD STREET, ON THE EAST SIDE OF STATE STREET.

BM 8562 ELEVATION= 22.29 FEET  
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT NORTH OF 93RD STREET, ON THE EAST SIDE OF STATE STREET.

BM 8563 ELEVATION= 20.47 FEET  
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT SOUTH OF 91ST STREET, ON THE EAST SIDE OF STATE STREET.

BM 8573 ELEVATION= 19.89 FEET  
SET SQUARE CUT ON EAST SIDE OF SIDEWALK OPPOSITE TO THE SECOND FIRE HYDRANT SOUTH OF 91ST STREET, ON THE WEST SIDE OF LAFAYETTE AVENUE.

BM 8574 ELEVATION= 18.90 FEET  
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT APPROXIMATELY AT 9224 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.

BM 8575 ELEVATION= 17.77 FEET  
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT APPROXIMATELY AT 9326 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.

BM 8576 ELEVATION= 17.43 FEET  
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT AT THE VACANT LOT BETWEEN 9416 AND 9422 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.

BM 8578 ELEVATION= 27.92 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND PEORIA STREET.

BM 8579 ELEVATION= 28.59 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND SANGAMON STREET.

BM 8580 ELEVATION= 29.24 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND MORGAN STREET.

BM 8581 ELEVATION= 30.82 FEET  
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF CARPENTER STREET AND WEST 98TH PLACE.

BM 8582 ELEVATION= 30.69 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF WEST 98TH PLACE, ON THE EAST SIDE OF ABERDEEN STREET.

BM 8583 ELEVATION= 33.14 FEET  
SET SQUARE CUT ON CONCRETE BRIDGE WALL AT THE SOUTHEASTERLY CORNER OF GENOA AVENUE AND 99TH STREET.

BM 8584 ELEVATION= 32.36 FEET  
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT WEST OF ABERDEEN STREET, ON THE SOUTHERLY SIDE OF 99TH STREET.

BM 8585 ELEVATION= 30.28 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRST FIRE HYDRANT WEST OF MORGAN STREET, ON THE SOUTH SIDE OF 99TH STREET.

BM 8586 ELEVATION= 28.75 FEET  
SET CROSS ON NORTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF 99TH STREET AND SANGAMON STREET.

BM 8587 ELEVATION= 31.19 FEET  
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF 99TH STREET AND CARPENTER STREET.

BM 3514 ELEVATION= 17.56 FEET  
SET CROSS ON CHAIN BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF LASALLE ST. AND 97TH ST.

BM 3522 ELEVATION= 18.06 FEET  
SET CROSS ON SOUTH SOUTHEAST FLANGE BOLT OF HYDRANT ± HALFWAY BETWEEN NORTH AND SOUTH ENTRANCE TO CITGO, EAST SIDE OF STATE ST. AND NORTH OF 95TH ST.

BM 3549 ELEVATION= 9.49 FEET  
SET CROSS WESTERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 87TH ST. AND LAFAYETTE AVE.

BM 3550 ELEVATION= 22.88 FEET  
SET CROSS WESTERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 91ST ST. AND LAFAYETTE AVE.

BM 3551 ELEVATION= 17.50 FEET  
SET CROSS NORTHERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 95TH ST. AND LAFAYETTE AVE.

BM 3552 ELEVATION= 14.81 FEET  
SET CROSS NORTHEAST BOLT OF TRAFFIC SIGNAL/LIGHT BASE AT SOUTHEAST INTERSECTION OF 91ST ST. AND STATE ST.

BM 3553 ELEVATION= 8.47 FEET  
SET CROSS NORTHWEST BOLT OF TRAFFIC SIGNAL/LIGHT BASE AT NORTHEAST INTERSECTION OF 87TH ST. AND STATE ST.

BM 3556 ELEVATION= 9.06 FEET  
SET CROSS EASTERLY FLANGE BOLT OF HYDRANT AT NORTHEAST CORNER OF 75TH ST. AND STATE ST.

BM 3558 ELEVATION= 18.15 FEET  
SET CROSS SOUTHWESTERLY FLANGE BOLT OF HYDRANT AT NORTHWEST INTERSECTION OF NORMAL ST. AND 98TH PLACE.

BM 3810 ELEVATION= 0.29 FEET  
CUT SQUARE ON SOUTHERLY CORNER OF METAL BASE FOR OVERHEAD SIGN, ON THE WESTERLY SIDE OF SOUTHBOUND DAN RYAN, OPPOSITE OF WEST 77TH PLACE, APPROXIMATELY 1000 FEET NORTH OF 79TH STREET BRIDGE.

BM 3811 ELEVATION= 1.95 FEET  
CUT SQUARE ON SOUTHEASTERLY CORNER OF METAL BASE FOR OVERHEAD SIGN, ON THE WESTERLY SIDE OF SOUTHBOUND DAN RYAN APPROXIMATELY 840 FEET SOUTHERLY OF 83RD STREET.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)

**SURVEY TIES FOR CONTROL POINTS**  
SHEET 7 OF 7

SCALE: 1"=30'  
DATE: OCTOBER 17, 2008

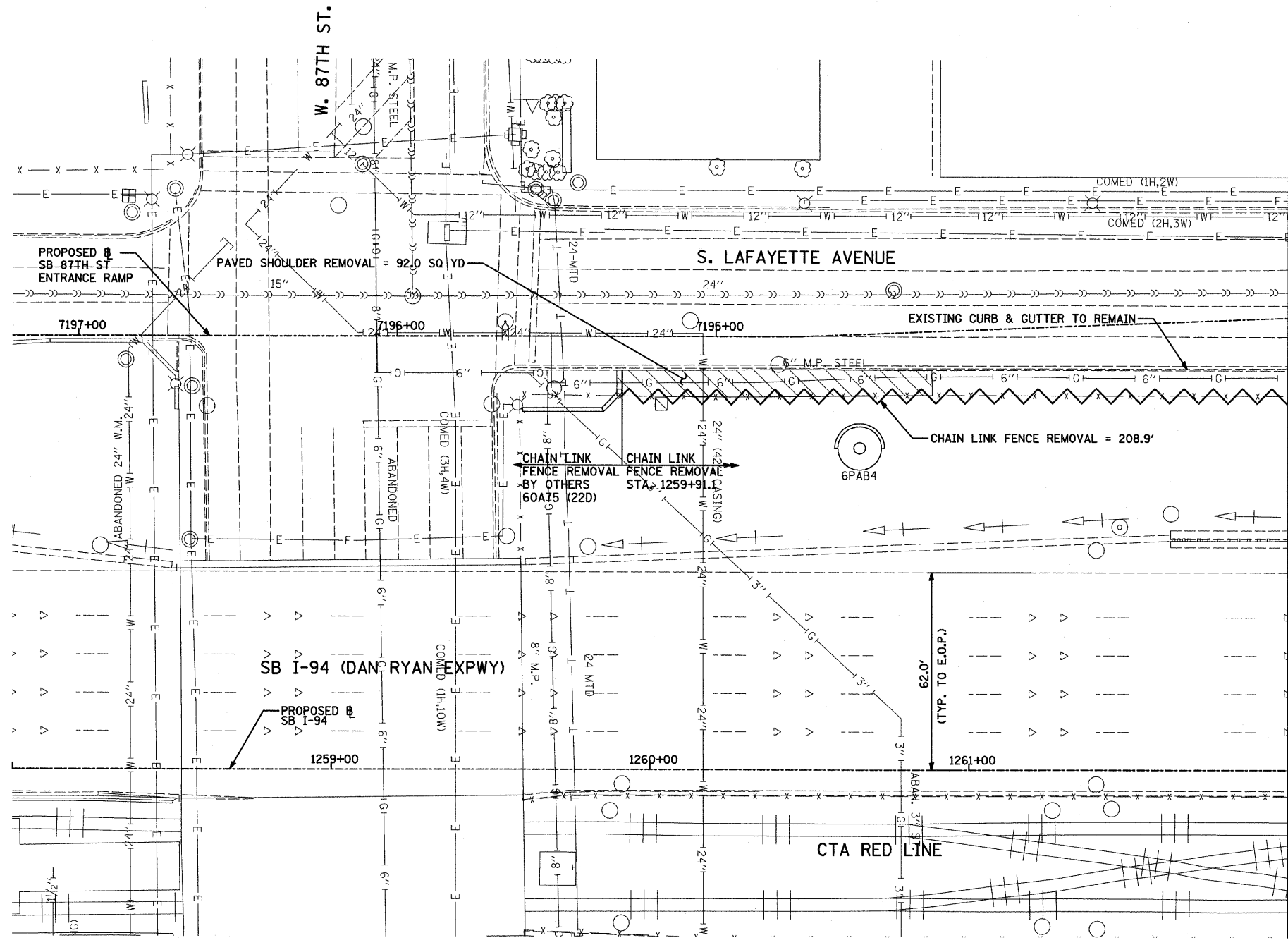
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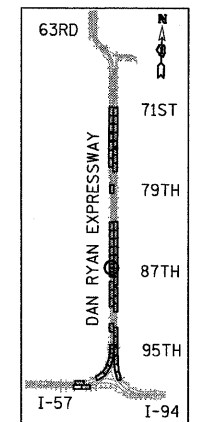


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	17
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

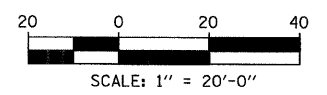
62979



MATCHLINE STA. 1262+00.00



LOCATION MAP



- LEGEND:**
- PAVED SHOULDER REMOVAL
  - COMB CONC CURB & GUTTER REMOVAL
  - CHAIN LINK FENCE REMOVAL
  - CONCRETE BARRIER/GUARDRAIL REMOVAL
  - TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)

**NOTE:**  
SIGNS SHALL BE RELOCATED AT A MINIMUM OF 3 FEET FROM THE FRONT FACE OF THE PROPOSED KNEE WALL. EXACT PLACEMENT SHALL BE DETERMINED BY THE ENGINEER. PAID FOR AS "RELOCATE EXISTING SIGN PANEL, SPECIAL".

**TYLIN** INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**REMOVAL PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**STA. 1258+00.00 TO STA. 1262+00.00**

SCALE: 1"=20'  
 DATE: OCTOBER 17, 2008

DRAWN BY: JDF  
 CHECKED BY: JPM





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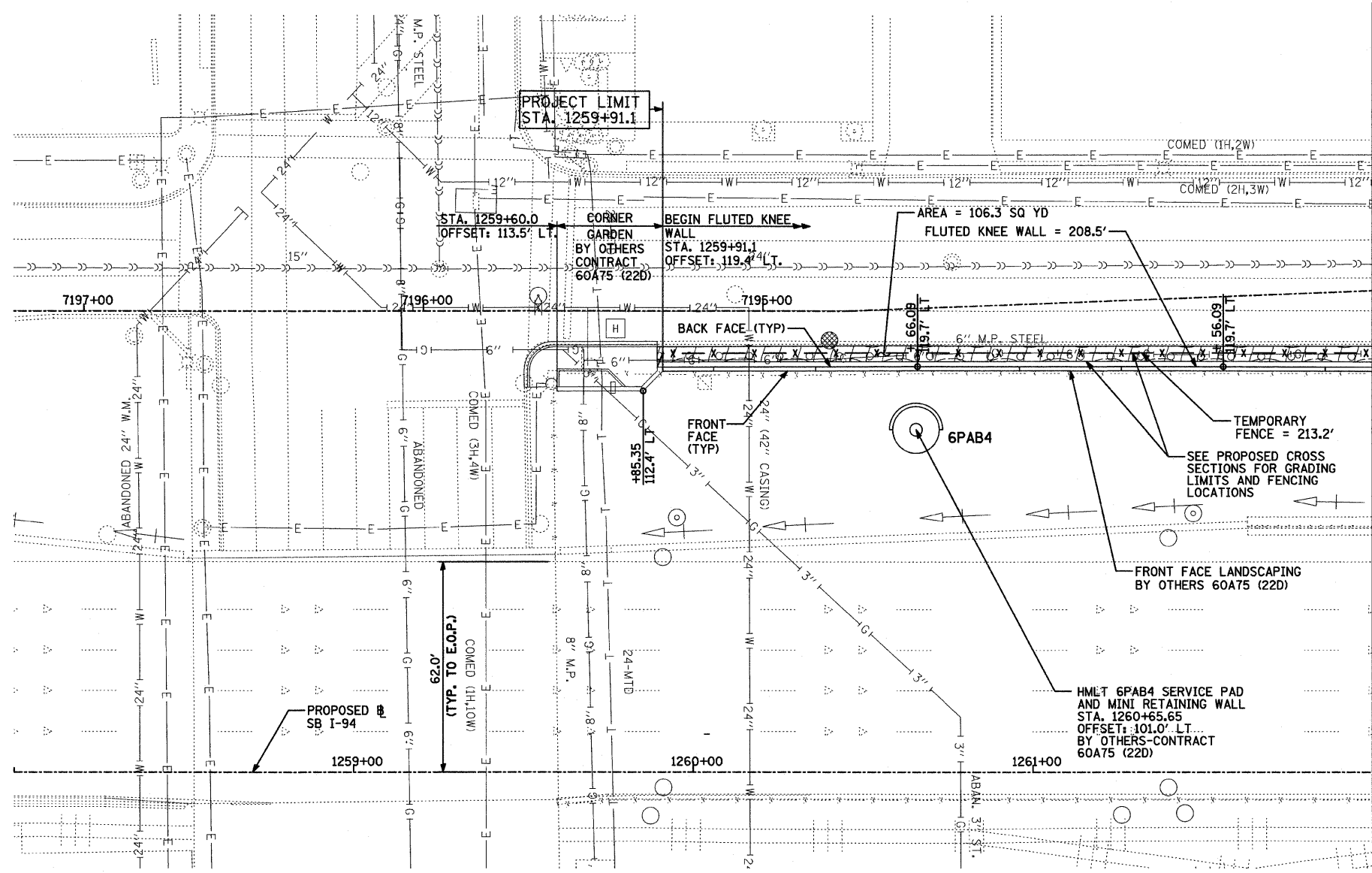
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	18
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

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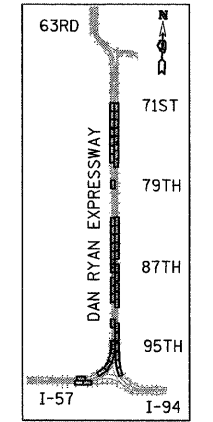


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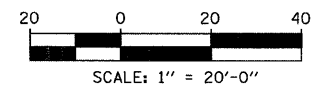
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-  EROSION CONTROL BLANKET, SEEDING, CLASS 2A, NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENTS, TOPSOIL FURNISH AND PLACE, 12" COMPOST FURNISH AND PLACE, 6"
-  EROSION CONTROL BLANKET, SEEDING, CLASS 4A (MODIFIED), SEEDING, CLASS 5 (MODIFIED), TOPSOIL FURNISH AND PLACE, 12" COMPOST FURNISH AND PLACE, 6"
-  INLET FILTER, TO BE INSTALLED IN OFF PROJECT DRAINAGE STRUCTURES ACCEPTING STORMWATER RUNOFF



MATCHLINE STA. 1262+00.00



LOCATION MAP



**NOTES:**

1. STATION AND OFFSET CALLOUTS ALONG THE LENGTH OF EITHER FLUTED KNEE WALL OR FLUTED KNEE WALL, SPECIAL ARE TAKEN FROM THE BACK FACE SIDE AT PROPOSED EXPANSION JOINT LOCATIONS.
2. THE CONTRACTOR SHALL PROTECT AND USE CAUTION DURING CONSTRUCTION OPERATIONS AROUND ALL EXISTING PUBLIC OR PRIVATE UTILITY FACILITIES.
3. CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH CONTRACT 60A75 (22D).

**TYLIN**INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 PROPOSED PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1258+00.00 TO STA. 1262+00.00

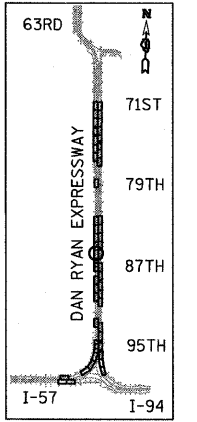
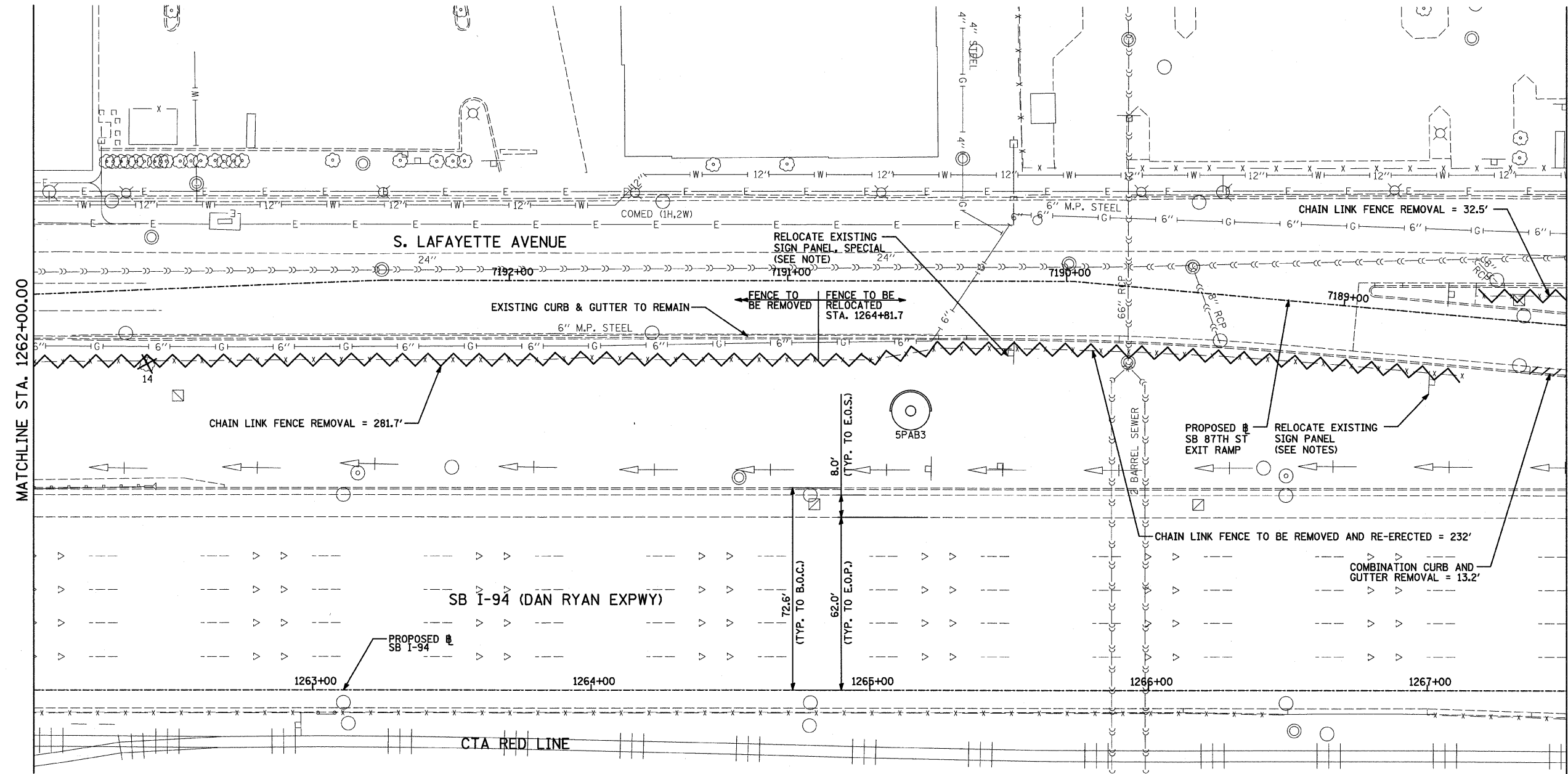
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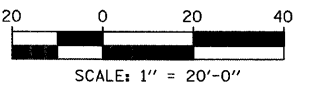
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	19
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

62979



LOCATION MAP



- LEGEND:**
- PAVED SHOULDER REMOVAL
  - COMB CONC CURB & GUTTER REMOVAL
  - CHAIN LINK FENCE REMOVAL
  - CONCRETE BARRIER/GUARDRAIL REMOVAL
  - TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)

**NOTE:**  
 SIGNS SHALL BE RELOCATED AT A MINIMUM OF 3 FEET FROM THE FRONT FACE OF THE PROPOSED KNEE WALL. EXACT PLACEMENT SHALL BE DETERMINED BY THE ENGINEER. PAID FOR AS "RELOCATE EXISTING SIGN PANEL, SPECIAL".

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 REMOVAL PLAN  
 SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1262+00.00 TO STA. 1267+50.00

SCALE: 1"=20'  
 DATE: OCTOBER 17, 2008

DRAWN BY: JDF  
 CHECKED BY: JPM

**TYLIN** INTERNATIONAL

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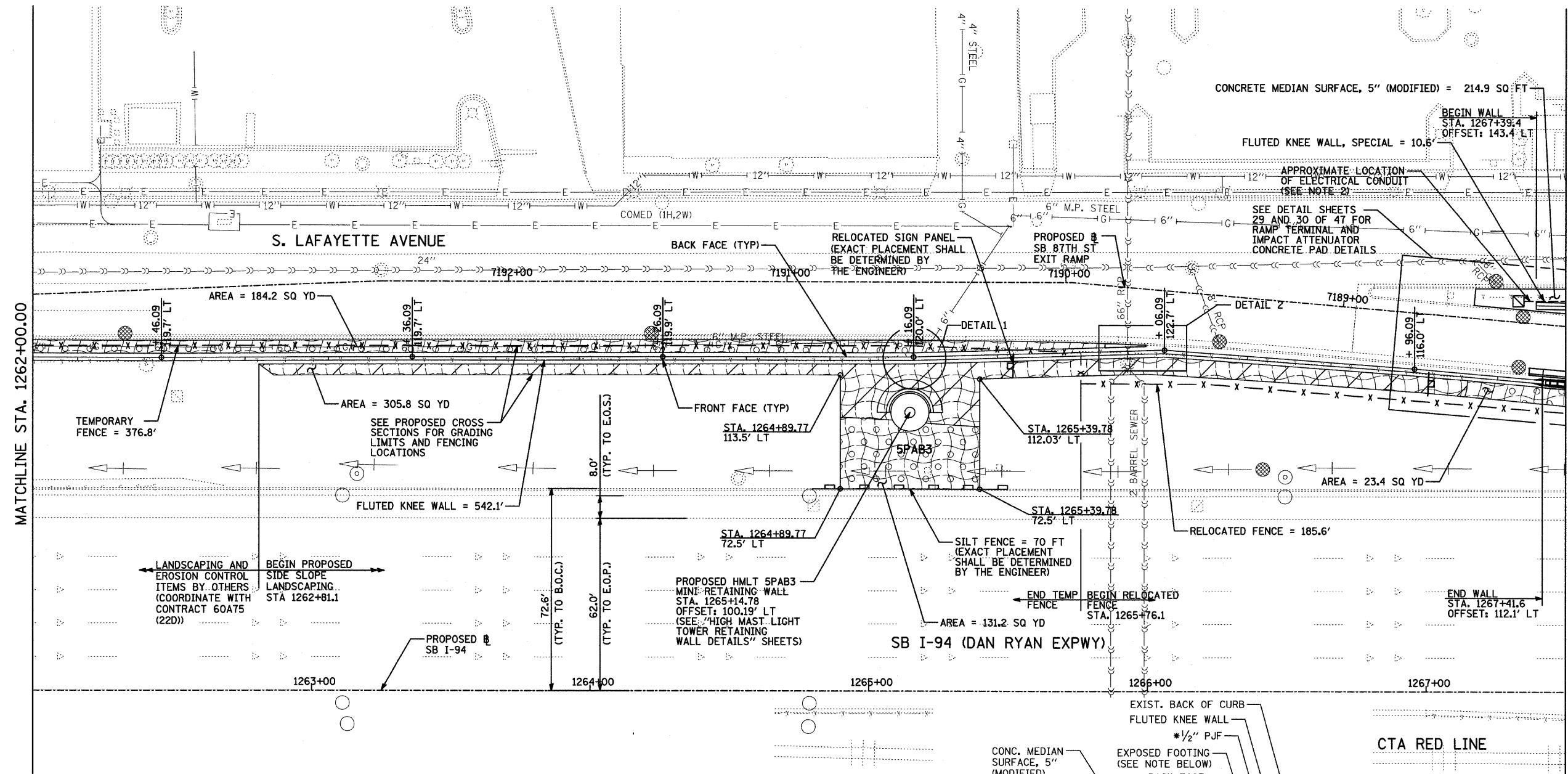
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	20
STA. 1259+91.12 TO STA. 1270+24.39		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

62979



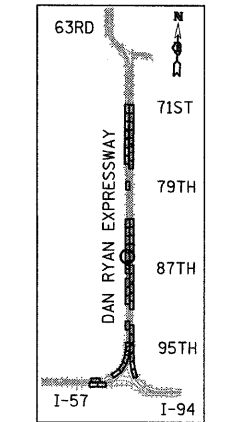
**LEGEND:**

- EROSION CONTROL BLANKET, SEEDING, CLASS 2A, NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENTS, TOPSOIL FURNISH AND PLACE, 4"
- EROSION CONTROL BLANKET, SEEDING, CLASS 2A, NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENTS, TOPSOIL FURNISH AND PLACE, 12"
- EROSION CONTROL BLANKET, SEEDING, CLASS 4A (MODIFIED) SEEDING, CLASS 5 (MODIFIED), TOPSOIL FURNISH AND PLACE, 12"
- EROSION CONTROL BLANKET, SEEDING, CLASS 5 (MODIFIED) SEEDING, CLASS 4A (MODIFIED) SEEDING, CLASS 5 (MODIFIED), TOPSOIL FURNISH AND PLACE, 6"
- INLET FILTER, TO BE INSTALLED IN OFF PROJECT DRAINAGE STRUCTURES ACCEPTING STORMWATER RUNOFF

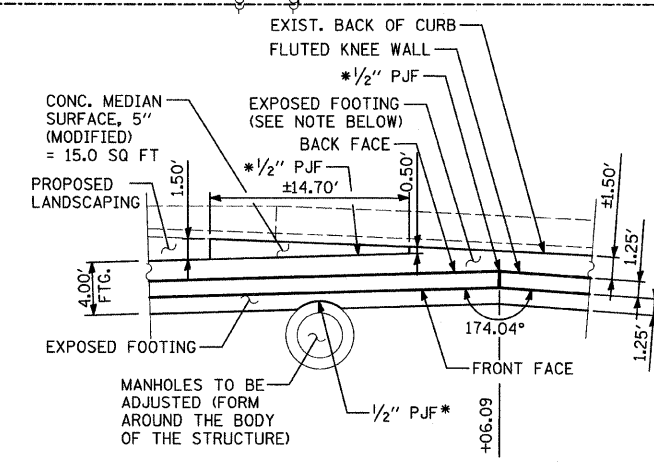
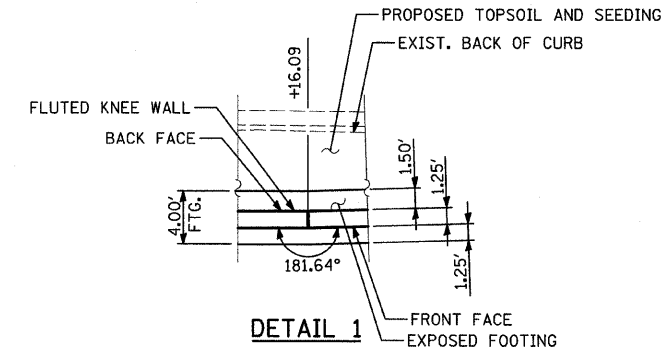
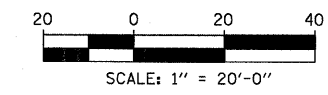


MATCHLINE STA. 1262+00.00

MATCHLINE STA. 1267+50.00



LOCATION MAP



- NOTES:**
- STATION AND OFFSET CALLOUTS ALONG THE LENGTH OF EITHER FLUTED KNEE WALL OR FLUTED KNEE WALL, SPECIAL ARE TAKEN FROM THE BACK FACE SIDE AT PROPOSED EXPANSION JOINT LOCATIONS.
  - THE CONTRACTOR SHALL PROTECT AND USE CAUTION DURING CONSTRUCTION OPERATIONS AROUND ALL EXISTING PUBLIC OR PRIVATE UTILITY FACILITIES.
  - CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH CONTRACT 60A75 (22D).

**NOTE:** EXTEND FOUNDATION WIDTH UP TO 0.5 FT IN ORDER TO MEET CONCRETE MEDIAN SURFACE, 5" (MODIFIED) OR EXISTING CONCRETE CURB AND GUTTER. ADDITIONAL FOUNDATION WIDTH SHALL BE INCLUDED IN THE COST OF FLUTED KNEE WALL.

\* INCLUDED IN THE COST OF FLUTED KNEE WALL.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**PROPOSED PLAN**  
 SB I-94 (I-94 DAN RYAN EXPWY)  
 STA. 1262+00.00 TO STA. 1267+50.00

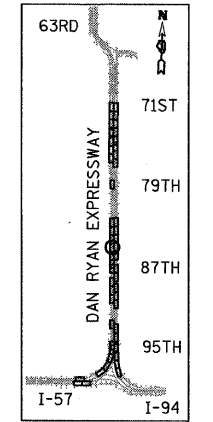
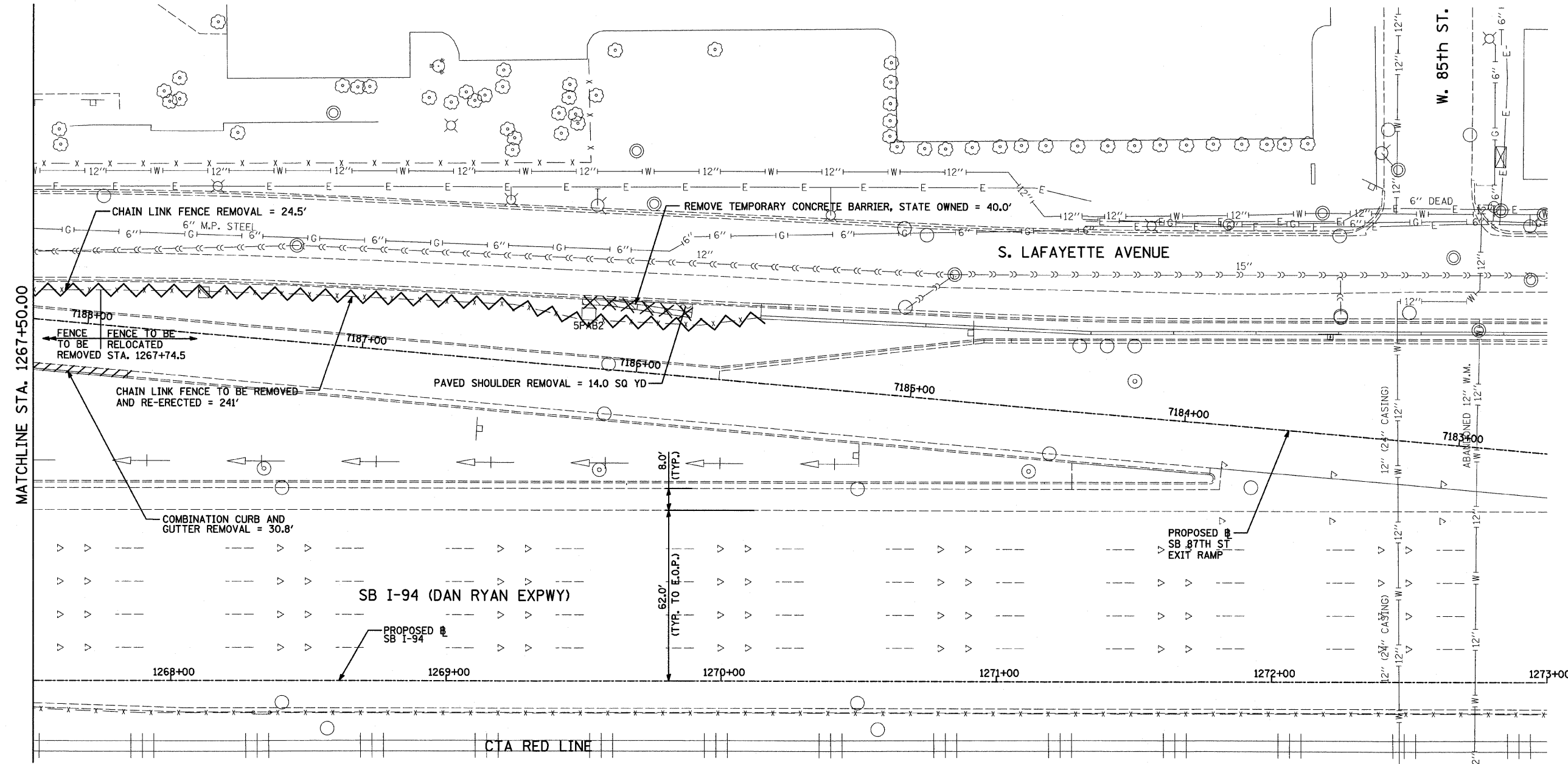
SCALE: 1"=20'  
 DATE: OCTOBER 17, 2008  
 DRAWN BY: JDF  
 CHECKED BY: JPM

**TYLIN INTERNATIONAL**

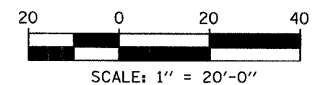
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	21
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

62979



LOCATION MAP



- LEGEND:**
- PAVED SHOULDER REMOVAL
  - COMB CONC CURB & GUTTER REMOVAL
  - CHAIN LINK FENCE REMOVAL
  - CONCRETE BARRIER/GUARDRAIL REMOVAL
  - TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)

**NOTE:**  
SIGNS SHALL BE RELOCATED AT A MINIMUM OF 3 FEET FROM THE FRONT FACE OF THE PROPOSED KNEE WALL. EXACT PLACEMENT SHALL BE DETERMINED BY THE ENGINEER. PAID FOR AS "RELOCATE EXISTING SIGN PANEL, SPECIAL".

REVISIONS	
NAME	DATE

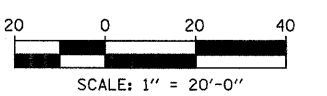
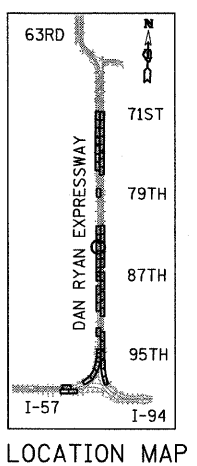
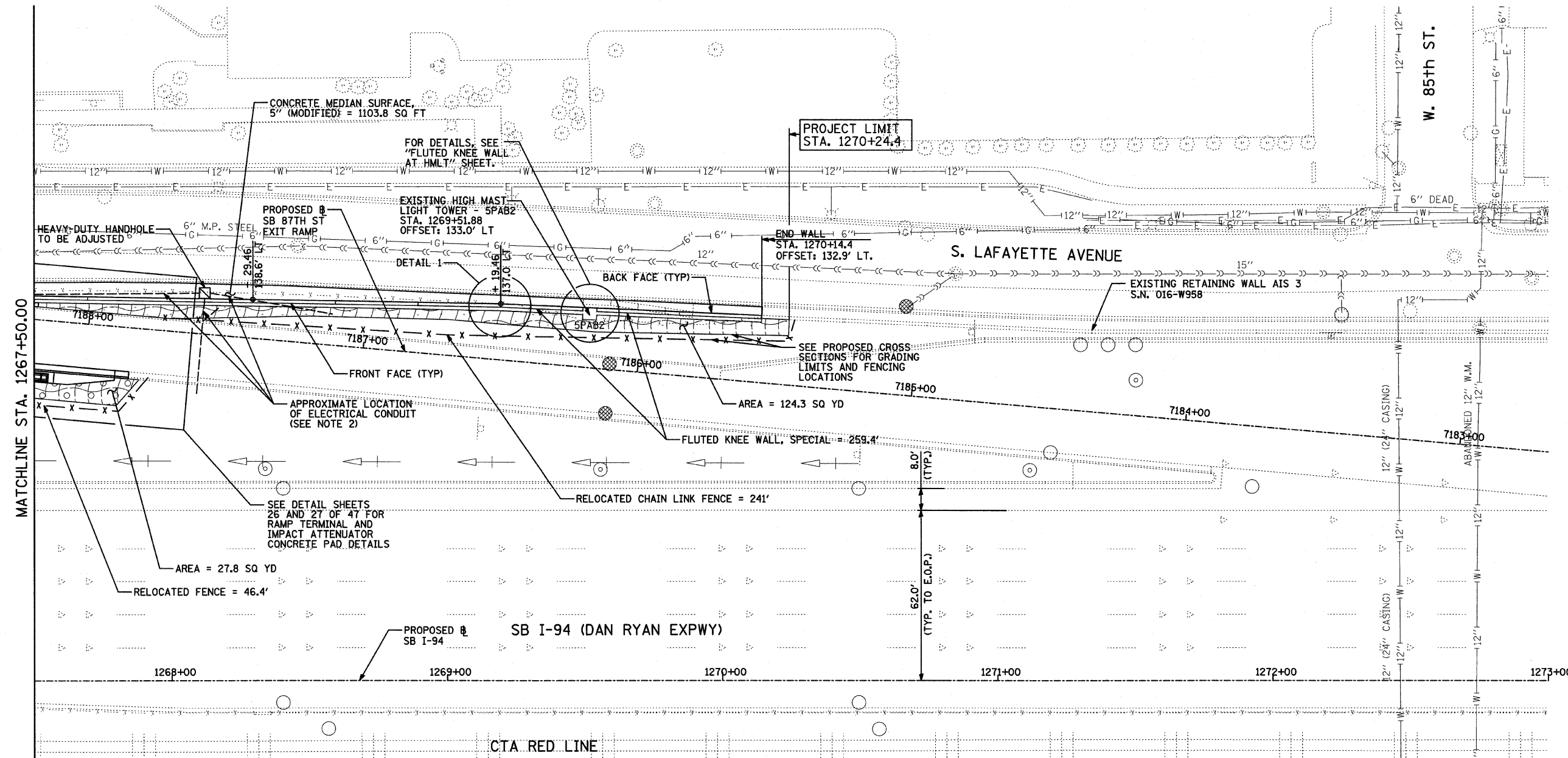
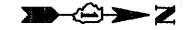
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**REMOVAL PLAN**  
**SB I-94 (DAN RYAN EXPRESSWAY)**  
**STA. 1267+50.00 TO STA. 1273+00.00**

SCALE: 1"=20'  
DATE: OCTOBER 17, 2008





DRAWN BY: JDF  
CHECKED BY: JPM

**TYLIN** INTERNATIONAL

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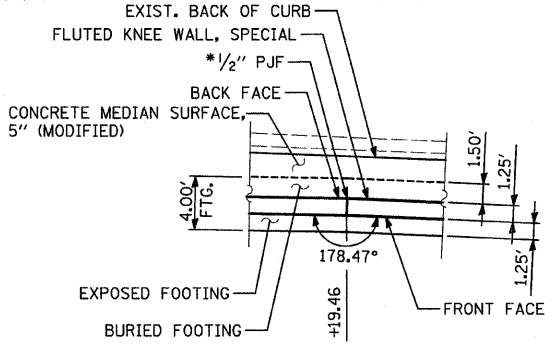


**LEGEND:**

-  EROSION CONTROL BLANKET, SEEDING, CLASS 2A, NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENTS, TOPSOIL FURNISH AND PLACE, 4"
-  EROSION CONTROL BLANKET, SEEDING, CLASS 2A, NITROGEN, PHOSPHORUS, AND POTASSIUM FERTILIZER NUTRIENTS, TOPSOIL FURNISH AND PLACE, 12" COMPOST FURNISH AND PLACE, 6"
-  EROSION CONTROL BLANKET, SEEDING, CLASS 4A (MODIFIED) SEEDING, CLASS 5 (MODIFIED), TOPSOIL FURNISH AND PLACE, 12" COMPOST FURNISH AND PLACE, 6"
-  INLET FILTER, TO BE INSTALLED IN OFF PROJECT DRAINAGE STRUCTURES ACCEPTING STORMWATER RUNOFF

**NOTES:**

1. STATION AND OFFSET CALLOUTS ALONG THE LENGTH OF EITHER FLUTED KNEE WALL OR FLUTED KNEE WALL, SPECIAL ARE TAKEN FROM THE BACK FACE SIDE AT PROPOSED EXPANSION JOINT LOCATIONS.
2. THE CONTRACTOR SHALL PROTECT AND USE CAUTION DURING CONSTRUCTION OPERATIONS AROUND ALL EXISTING PUBLIC OR PRIVATE UTILITY FACILITIES.



**DETAIL 1**  
\*INCLUDED IN THE COST OF FLUTED KNEE WALL, SPECIAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**  
**PROPOSED PLAN**  
**SB I-94 (I-94 DAN RYAN EXPWY)**  
**STA. 1267+50.00 TO STA. 1273+00.00**

SCALE: 1"=20'  
 DATE: OCTOBER 17, 2008

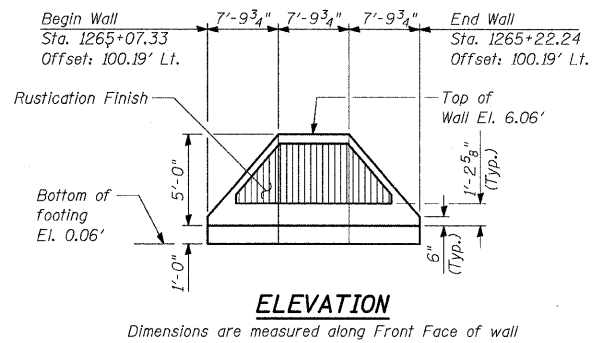
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	23
STA. 1259+91.12 TO STA. 1270+24.39		ILLINOIS FED. AID PROJECT		
<b>62979</b>				

**GENERAL NOTES:**

- Reinforcement bars shall conform to the requirements of AASHTO M31 or M322 Grade 60.
- Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work. However, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- All exposed concrete edges shall be chamfered 3/4" except as noted.
- Protective Coat shall be applied according to Section 503.19 of the Standard Specifications to all concrete surfaces not in contact with soil for the typical section of the retaining wall.
- The exposed top of the footing shall be broom finished.
- All construction joints shall be bonded.
- All elevations shown are based on the Chicago City Datum of 0.00, which is 579.19 feet above mean tide New York. (NAVD 88)
- The Contractor is responsible to coordinate all electrical crossings with this Contract and with Contract 62583.



**DESIGN SPECIFICATION**

AASHTO 2002 Standard Specifications for Highway Bridges

**DESIGN STRESSES  
FIELD UNITS**

$f'_c = 3,500$  psi  
 $f_y = 60,000$  psi (Reinforcement)

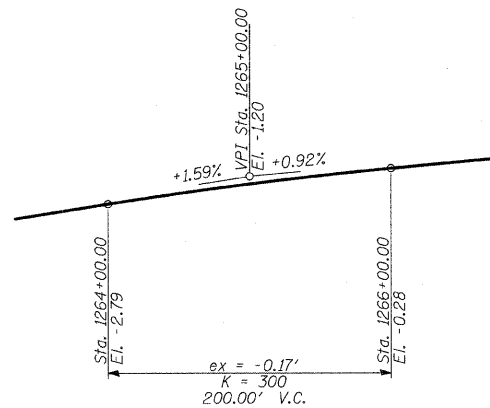
**LOADING**

Maximum soil pressure under knee wall is 1,550 psf.

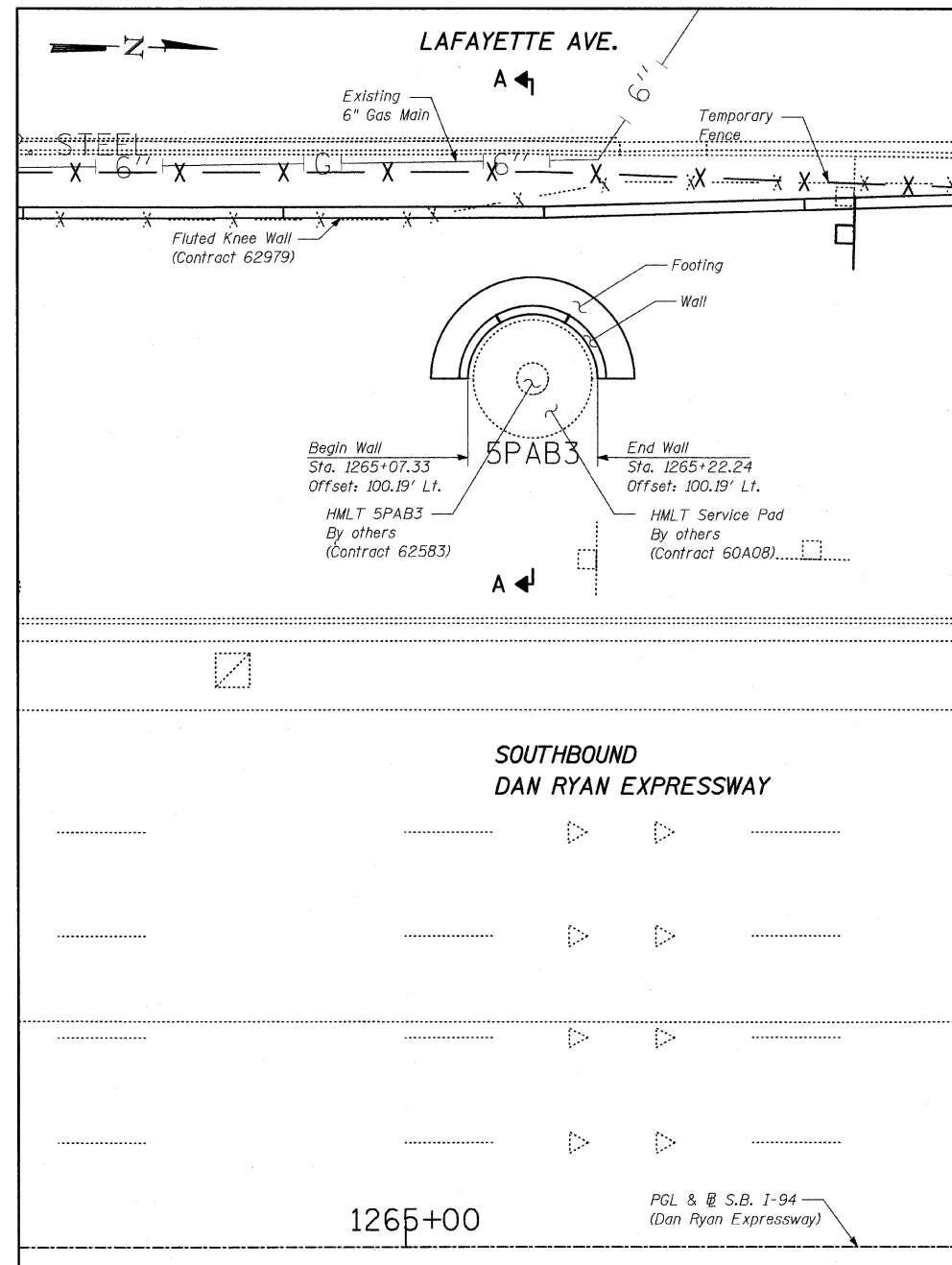
**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Porous Granular Embankment	CU YD	41
Structure Excavation	CU YD	12
Concrete Structures	CU YD	9
Protective Coat	SQ YD	11
Rustication Finish	SQ FT	43
Reinforcement Bars, Epoxy Coated	POUND	1,030

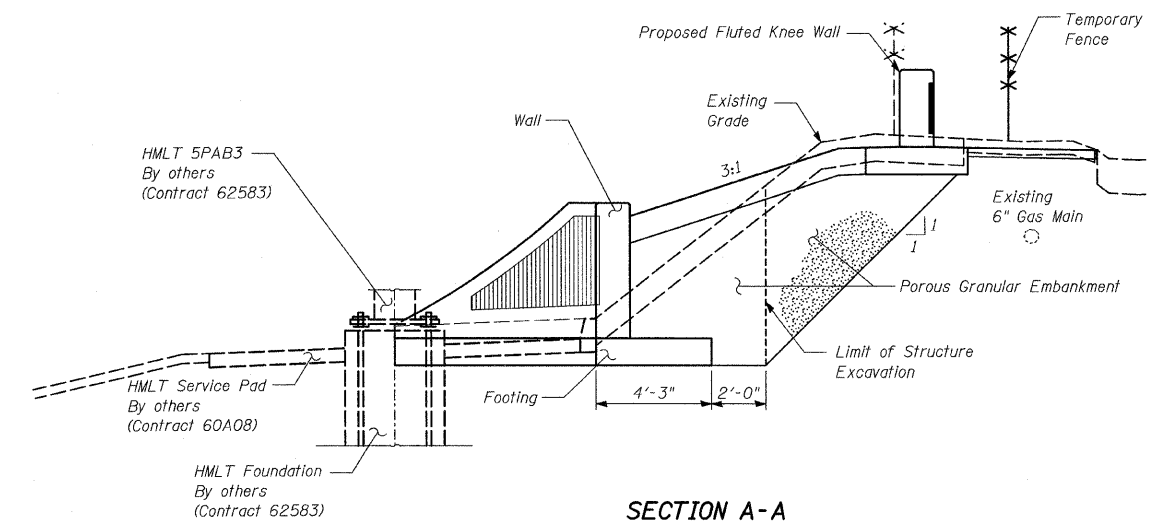
\* See Sheet 2 of 2 for Bar Diagram



**PROFILE GRADE**  
(Southbound I-94)



**PLAN**



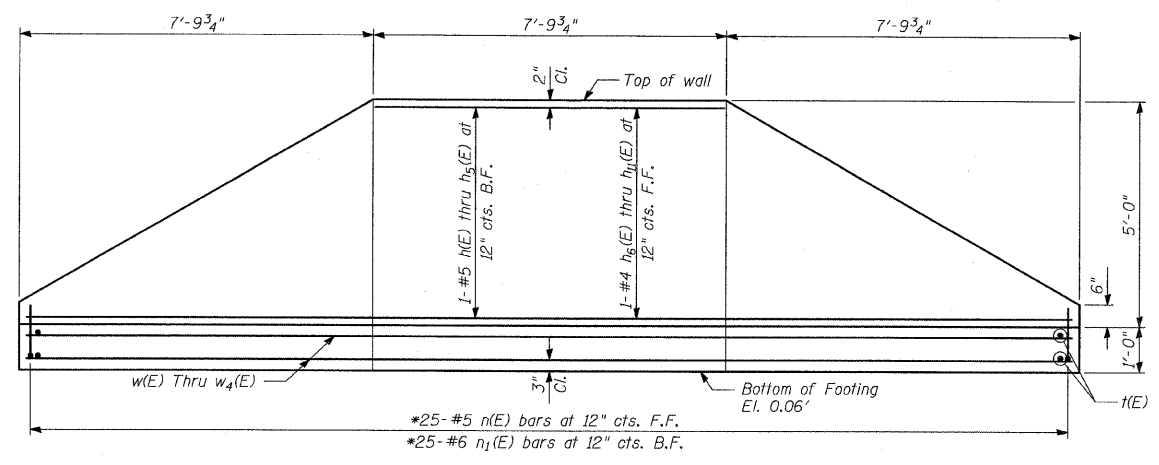
**SECTION A-A**

Dimensions are measured along Front Face of wall at H.M.L.T. 5PAB3

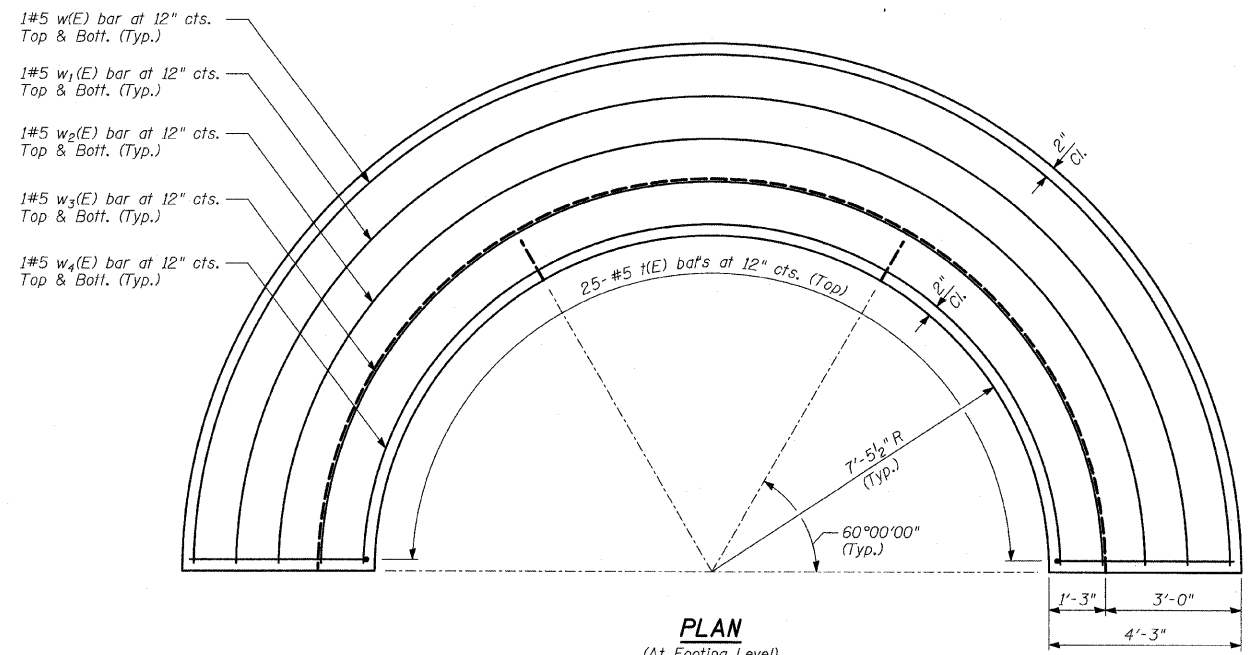
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**HIGH MAST LIGHT TOWER RETAINING WALL,  
SPECIAL DETAILS (1 OF 2)**

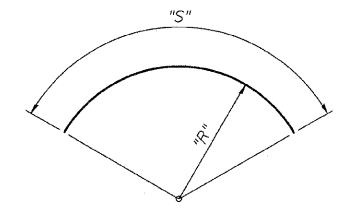
S.N. DESIGNED BY: MAF  
SCALE: DRAWN BY: MAF  
DATE: OCTOBER 17, 2008 CHECKED BY: PF



**ELEVATION**  
(Dimensions are measured along the Front Face of wall)



**PLAN**  
(At Footing Level)



Bar	"R"	"S"
h(E)	7'-8 1/2"	23'-11"
h <sub>1</sub> (E)	7'-8 1/2"	21'-10"
h <sub>2</sub> (E)	7'-8 1/2"	18'-7"
h <sub>3</sub> (E)	7'-8 1/2"	15'-4"
h <sub>4</sub> (E)	7'-8 1/2"	12'-1"
h <sub>5</sub> (E)	7'-8 1/2"	8'-10"
h <sub>6</sub> (E)	8'-5 3/4"	26'-4"
h <sub>7</sub> (E)	8'-5 3/4"	24'-0"
h <sub>8</sub> (E)	8'-5 3/4"	20'-5"
h <sub>9</sub> (E)	8'-5 3/4"	16'-10"
h <sub>10</sub> (E)	8'-5 3/4"	13'-3"
h <sub>11</sub> (E)	8'-5 3/4"	9'-9"
w(E)	11'-6 1/2"	35'-11"
w <sub>1</sub> (E)	10'-7 1/8"	32'-11"
w <sub>2</sub> (E)	9'-7 1/2"	29'-11"
w <sub>3</sub> (E)	8'-7 1/8"	26'-10"
w <sub>4</sub> (E)	7'-8 1/4"	23'-10"

Note: "R" and "S" measured along bar @

**BARS h(E) thru h<sub>11</sub>(E) & w(E) thru w<sub>4</sub>(E)**

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h(E)	1	#5	23'-11"	( )
h <sub>1</sub> (E)	1	#5	21'-10"	( )
h <sub>2</sub> (E)	1	#5	18'-7"	( )
h <sub>3</sub> (E)	1	#5	15'-4"	( )
h <sub>4</sub> (E)	1	#5	12'-1"	( )
h <sub>5</sub> (E)	1	#5	8'-10"	( )
h <sub>6</sub> (E)	1	#4	26'-4"	( )
h <sub>7</sub> (E)	1	#4	24'-0"	( )
h <sub>8</sub> (E)	1	#4	20'-5"	( )
h <sub>9</sub> (E)	1	#4	16'-10"	( )
h <sub>10</sub> (E)	1	#4	13'-3"	( )
h <sub>11</sub> (E)	1	#4	9'-9"	( )
n(E)	25	#5	8'-3"	( )
n <sub>1</sub> (E)	25	#6	5'-5"	( )
k(E)	25	#5	4'-6"	( )
w(E)	2	#5	35'-11"	( )
w <sub>1</sub> (E)	2	#5	32'-11"	( )
w <sub>2</sub> (E)	2	#5	29'-11"	( )
w <sub>3</sub> (E)	2	#5	26'-10"	( )
w <sub>4</sub> (E)	2	#5	23'-10"	( )

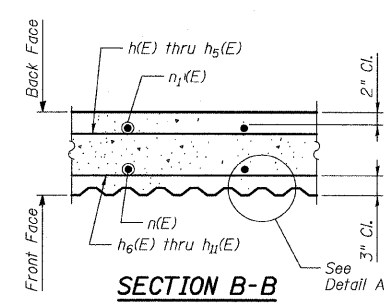
Reinforcement bars designated (E) shall be epoxy coated.

**NOTES:**

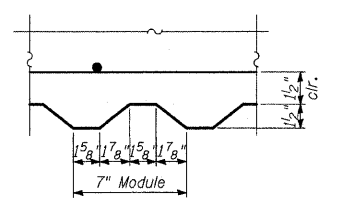
1. T/Wall slope is constant along the arc of any given radius.
2. Place horizontal tails of n(E) & n<sub>1</sub>(E) bars radially.

**LEGEND**

- \* Cut bars to fit in field.
- F.F. - Denotes Front Face.
- B.F. - Denotes Back Face.



**SECTION B-B**

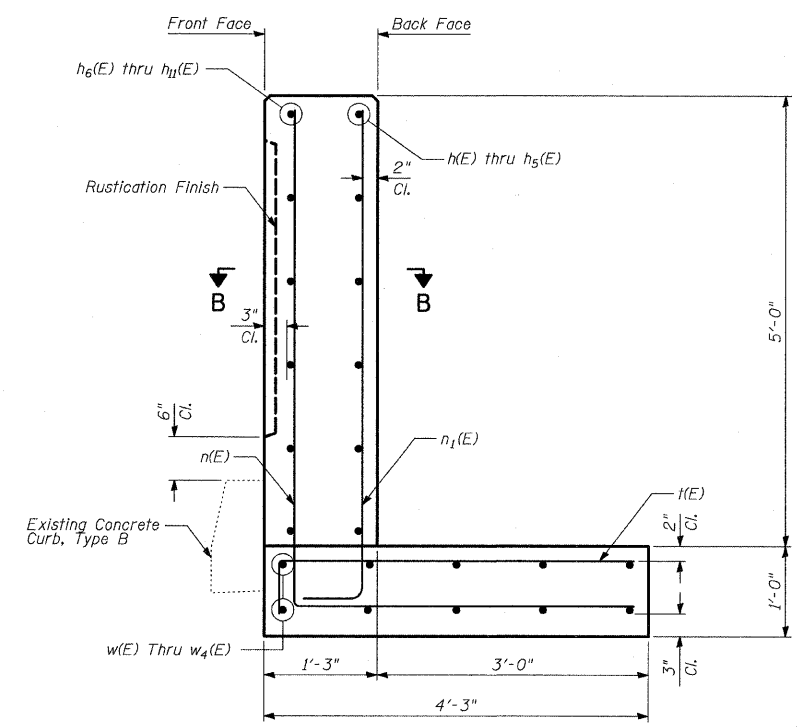


**DETAIL A**

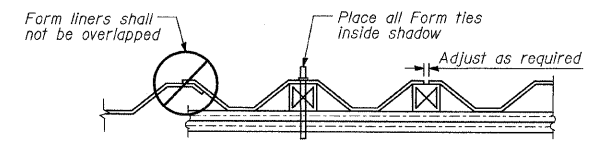
**MARK TABLE**

Bar	A	B
n(E)	4'-6"	3'-9"
n <sub>1</sub> (E)	4'-6"	11"
k(E)	3'-11"	7"

**BARS n(E), n<sub>1</sub>(E) & k(E)**



**TYPICAL SECTION**



**SUGGESTED FORMWORK DETAIL**

**TYLIN INTERNATIONAL**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
HIGH MAST LIGHT TOWER RETAINING WALL,  
SPECIAL DETAILS (2 OF 2)

S.N. DESIGNED BY: MAF  
SCALE: DRAWN BY: MAF  
DATE: OCTOBER 17, 2008 CHECKED BY: PF

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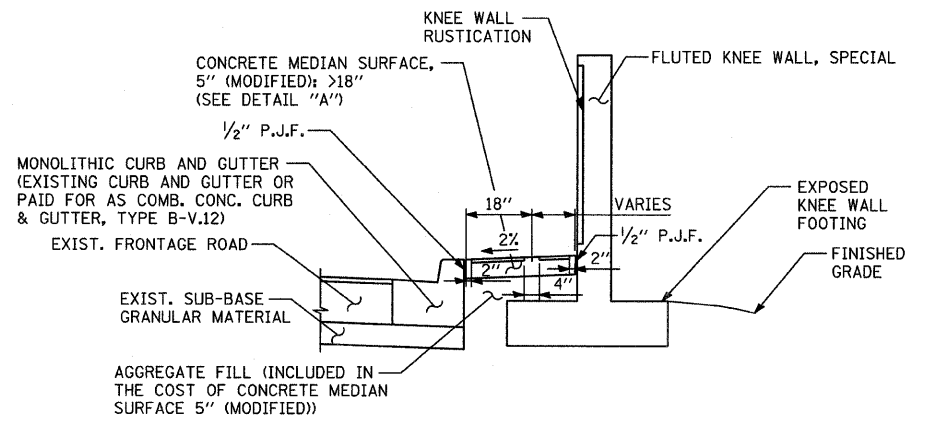
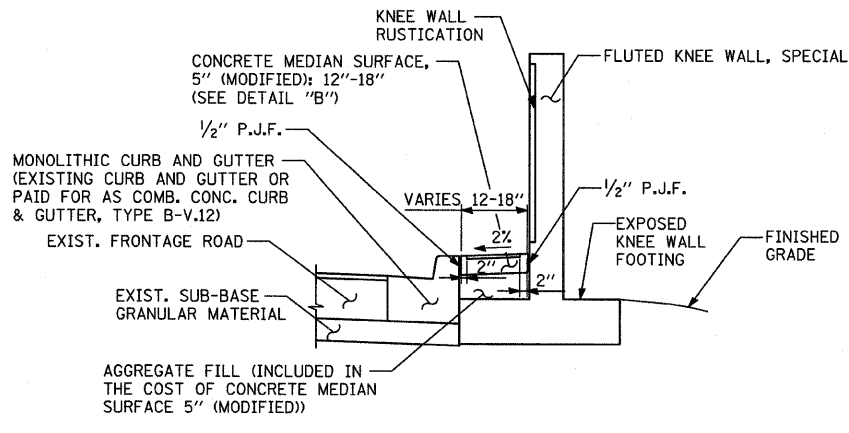
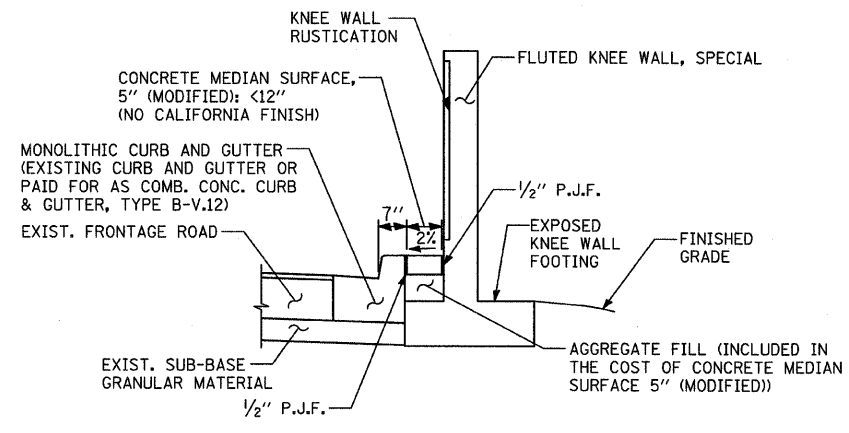


**NOTE:**  
THIS OPTION SHALL BE USED WHEN THE CARRIAGE WALK AREA MEASURES LESS THAN 12" IN WIDTH

**NOTE:**  
THIS OPTION SHALL BE USED WHEN THE CARRIAGE WALK AREA MEASURES 12" TO 18" IN WIDTH

**NOTE:**  
THIS OPTION SHALL BE USED WHEN THE CARRIAGE WALK AREA MEASURES 18" OR GREATER IN WIDTH

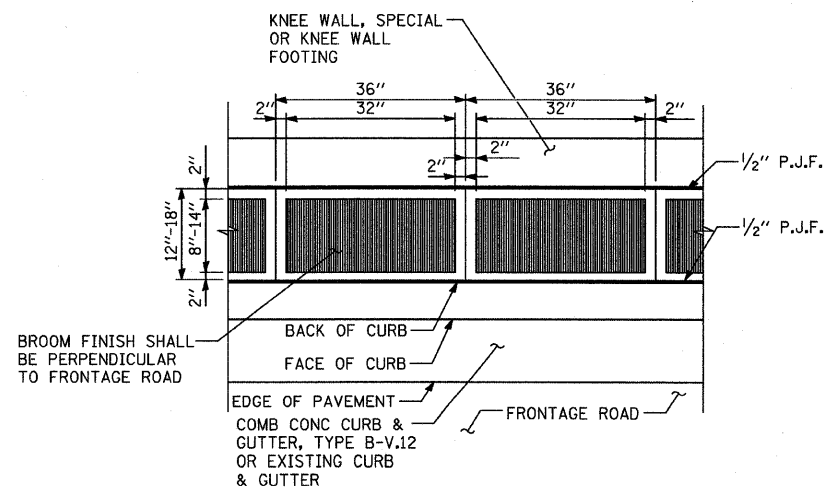
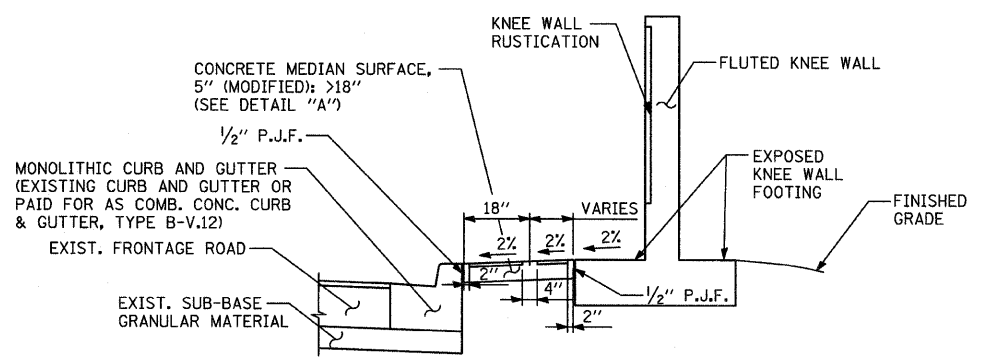
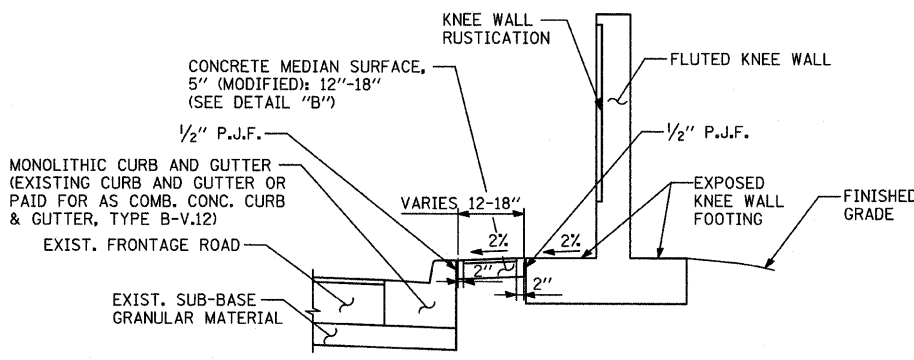
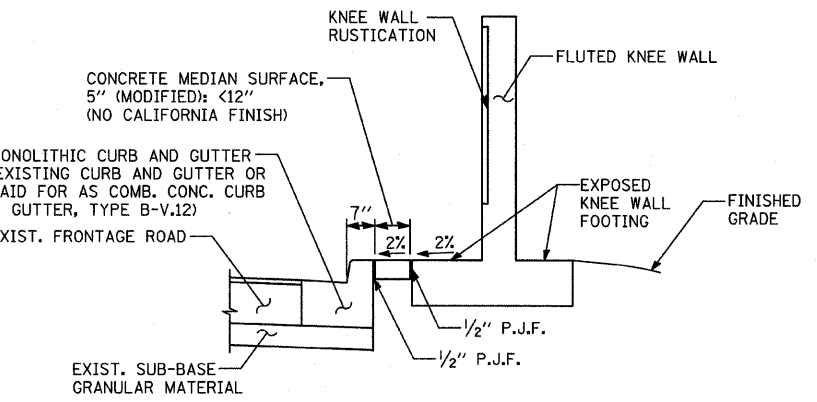
**VARIES PANEL:**  
• IF THE WIDTH IS  $\geq 4"$  THEN APPLY BROOM FINISH FLUSH TO THE KNEE WALL WITH 4" CALIFORNIA JOINT  
• IF THE WIDTH IS  $< 4"$  THEN SURFACE TO BE HARD TROWELLED (SEE DETAIL A)



**CONCRETE MEDIAN SURFACE, 5" (MODIFIED) "A"**  
AREA <12" WIDE

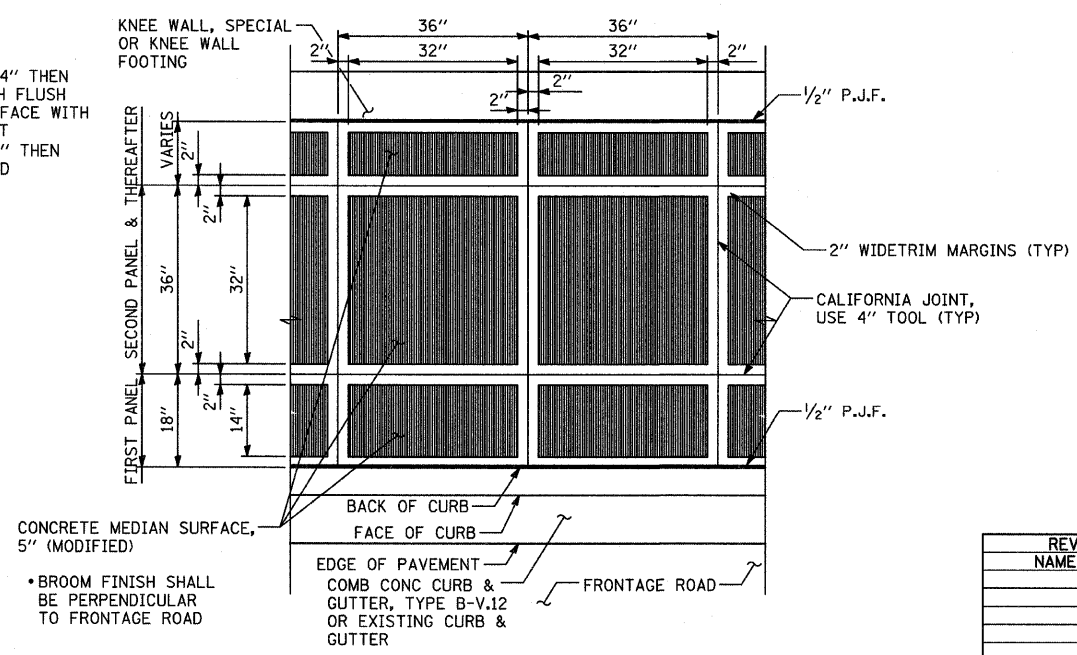
**CONCRETE MEDIAN SURFACE, 5" (MODIFIED) "B"**  
AREA 12"-18" WIDE

**CONCRETE MEDIAN SURFACE, 5" (MODIFIED) "C"**  
AREA >18" WIDE



**DETAIL "B": COORDINATE WITH CONC. MED. SURFACE, 5" (MODIFIED) "B"**  
CALIFORNIA FINISH

**VARIES PANEL:**  
• IF THE WIDTH IS  $\geq 4"$  THEN APPLY BROOM FINISH FLUSH TO THE KNEE WALL FACE WITH 4" CALIFORNIA JOINT  
• IF THE WIDTH IS  $< 4"$  THEN SURFACE TO BE HARD TROWELLED



**DETAIL "A": COORDINATE WITH CONC. MED. SURFACE, 5" (MODIFIED) "C"**  
CALIFORNIA FINISH

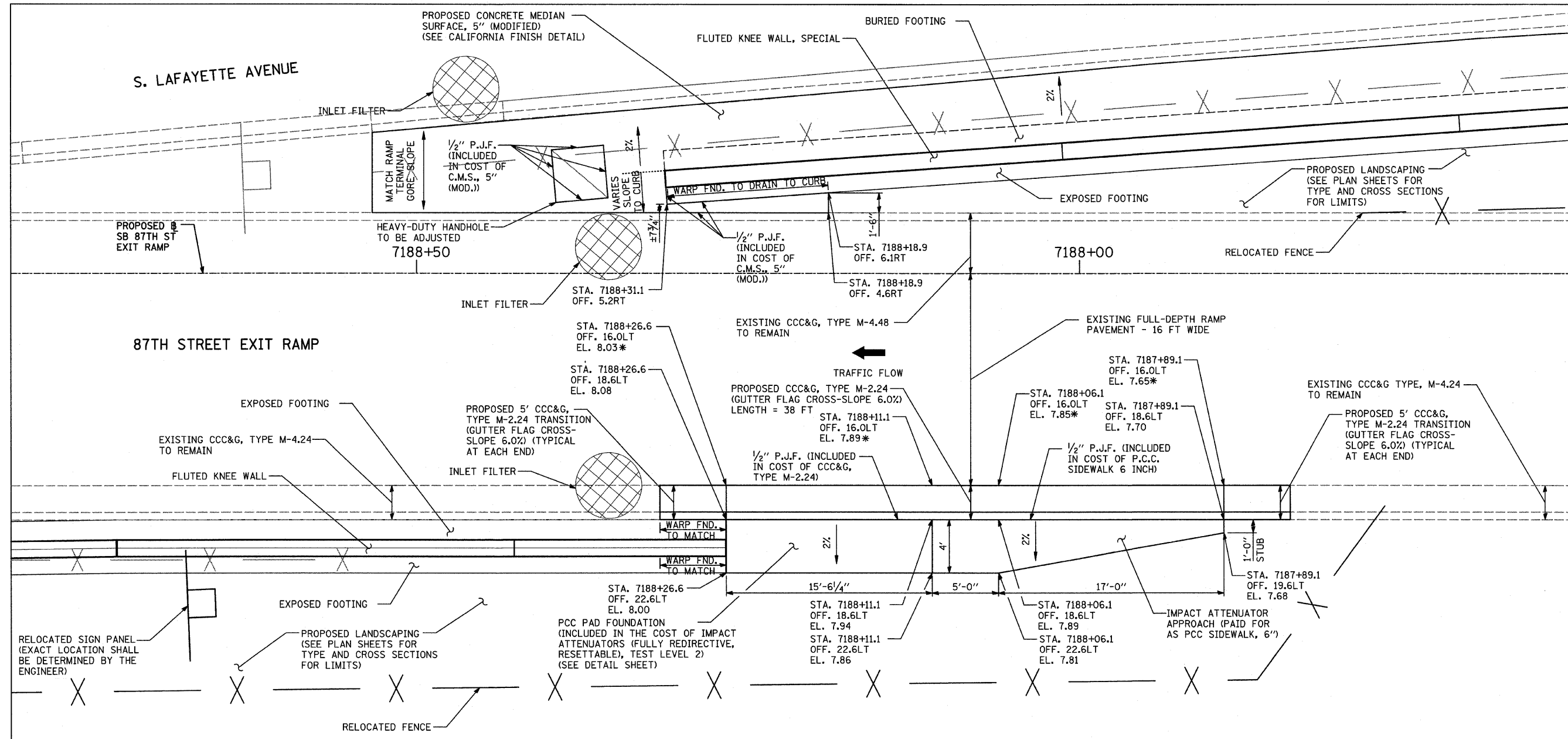
**NOTE:**  
NOT ALL ITEMS SHOWN ON THIS SHEET ARE PROPOSED. SEE PLANS FOR PROPOSED ITEMS.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
**CONCRETE MEDIAN SURFACE, 5" (MODIFIED) WITH CALIFORNIA FINISH DETAIL**  
SCALE: NONE  
DATE: OCTOBER 17, 2008  
DRAWN BY: JDF  
CHECKED BY: JPM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	26
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

62979



**87TH STREET EXIT RAMP TERMINAL DETAILS**

\* - ELEVATIONS ARE AS PROPOSED IN CONTRACT 62695. PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24 SHALL MATCH THE TOP OF THE EXISTING FULL-DEPTH RAMP PAVEMENT.

**TYLIN** INTERNATIONAL

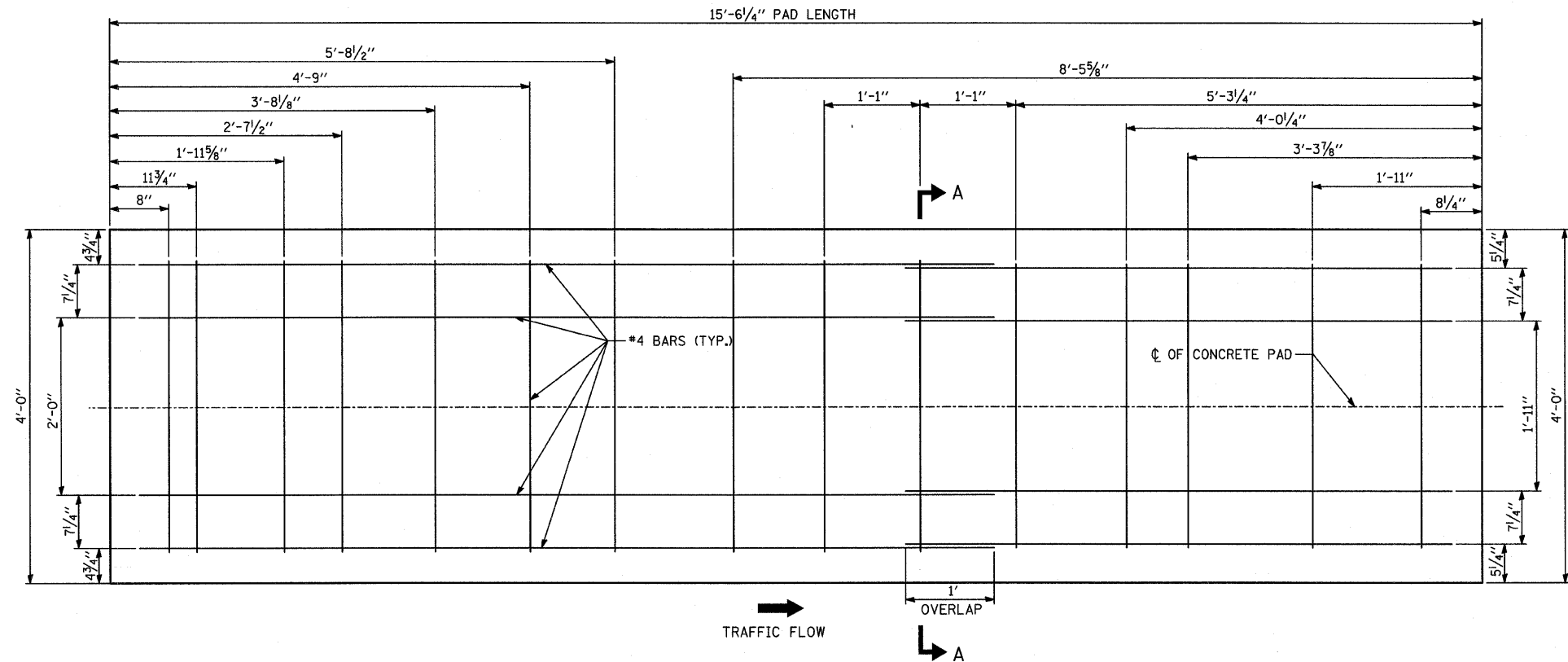
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**F.A.I. 94 (DAN RYAN EXPRESSWAY)**

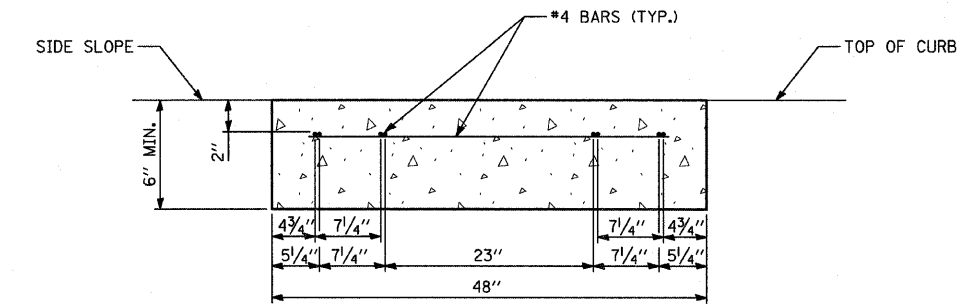
**87TH STREET EXIT RAMP  
 TERMINAL DETAIL**

SCALE: NONE      DRAWN BY: JDF  
 DATE: OCTOBER 17, 2008      CHECKED BY: JPM

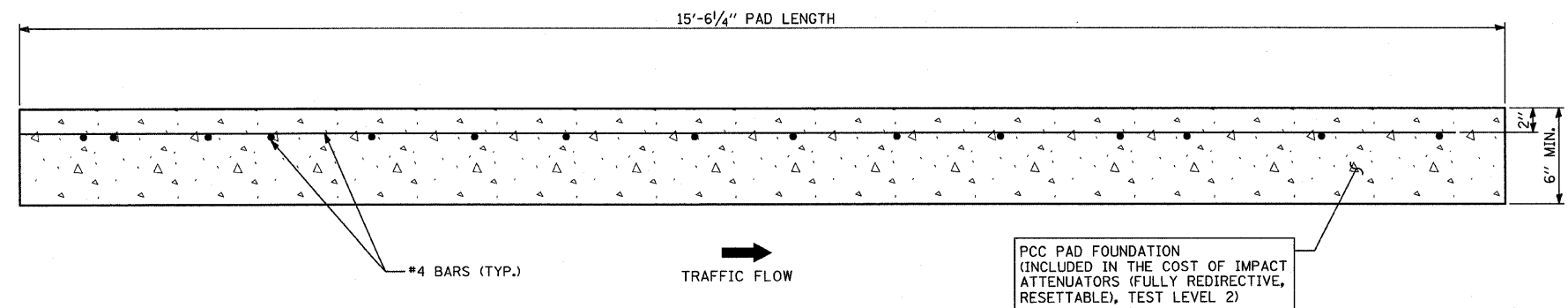
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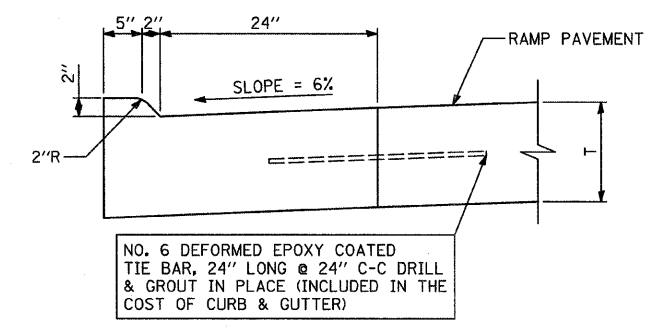
TYPICAL CONCRETE PAD PLAN VIEW



SECTION A-A



TYPICAL CONCRETE PAD ELEVATION VIEW



COMBINATION CONCRETE CURB & GUTTER, TYPE M-2.24

NOTE: CONCRETE PAD DRAWING INCLUDED FOR INFORMATIONAL PURPOSES ONLY. THE PAD FOR A SMART CUSHION INNOVATIONS TOGM SYSTEM IS DEPICTED. EXACT DIMENSIONS SHALL COMPLY WITH THE MANUFACTURER'S REQUIREMENTS FOR SPECIFIED PAY ITEM: IMPACT ATTENUATOR (FULLY REDIRECTIVE, RESETTABLE), TEST LEVEL 2

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

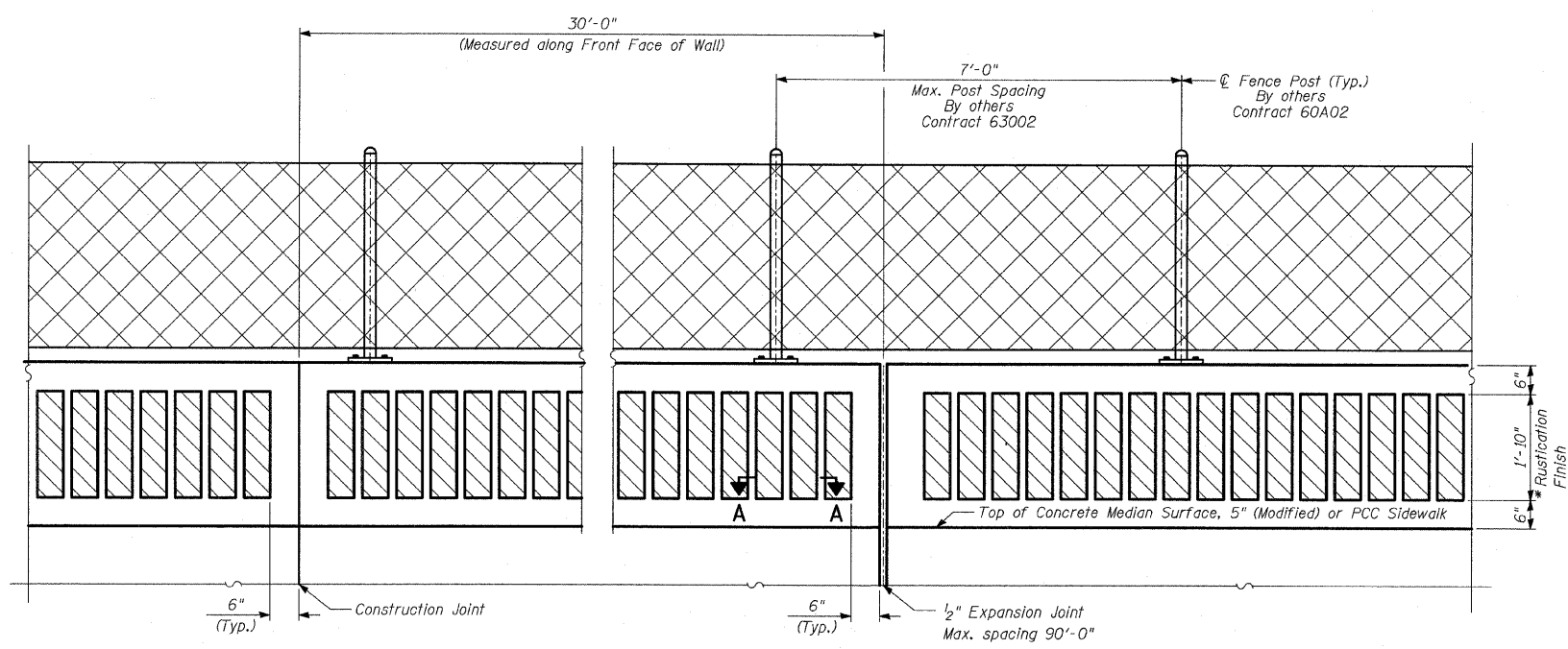
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 DETAIL OF CONCRETE PAD  
 FOR IMPACT ATTENUATOR (FULLY  
 REDIRECTIVE, RESETTABLE), TEST LEVEL 2

SCALE: NONE  
 DATE: OCTOBER 17, 2008

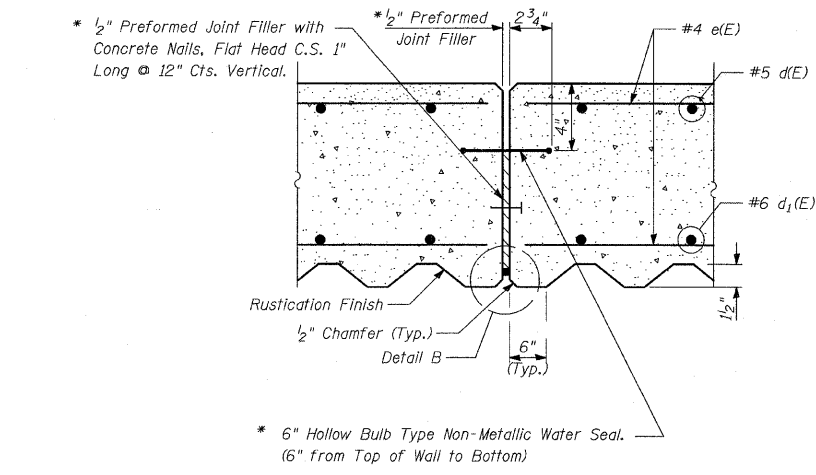
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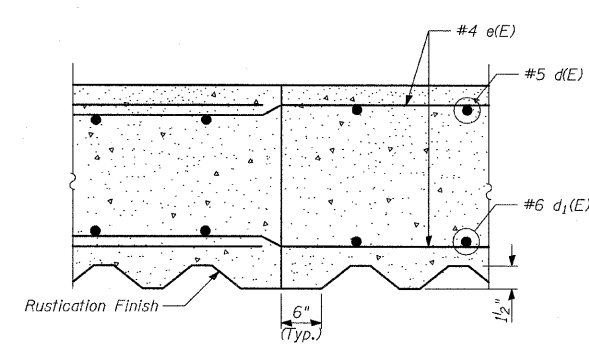




**BACK FACE ELEVATION - FLUTED KNEE WALL, SPECIAL AND CHAIN LINK FENCE**



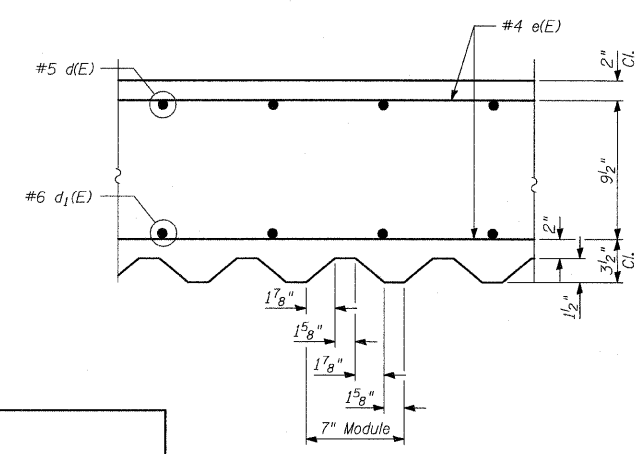
**EXPANSION JOINT DETAILS**



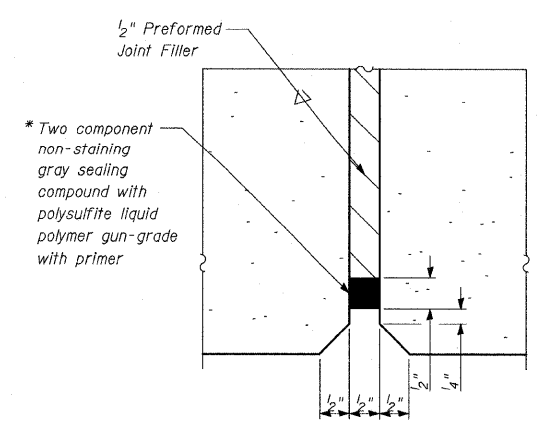
**CONSTRUCTION JOINT DETAILS**

**LAP SPLICES**

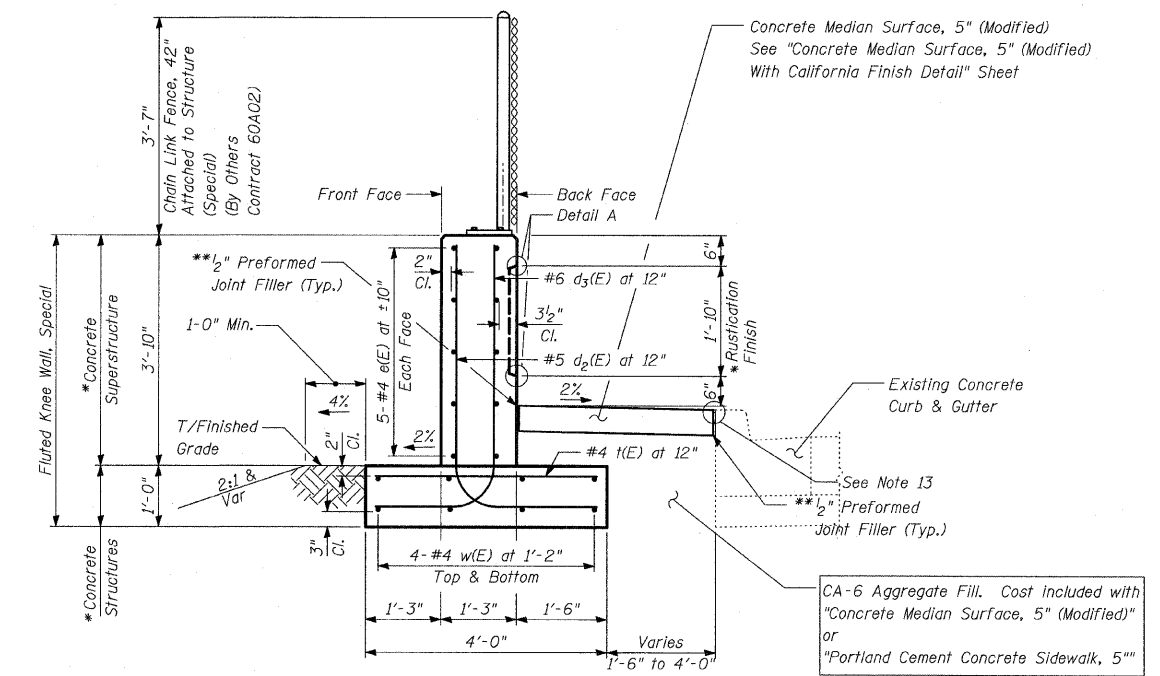
Bar	Lap
#4	1'-8"
#5	2'-2"
#6	2'-7"



**SECTION A-A**

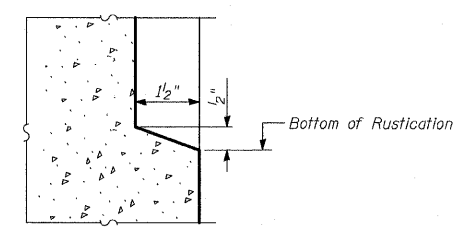
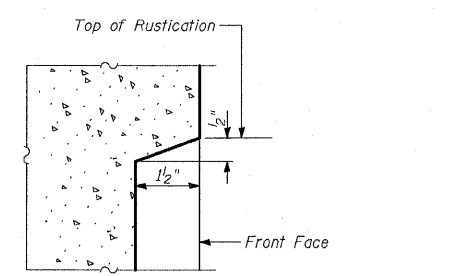


**DETAIL B**



**TYPICAL SECTION FLUTED KNEE WALL, SPECIAL**

Sta. 1267+39.4 to Sta. 1270+14.4



**DETAIL A**

**BILL OF MATERIAL**

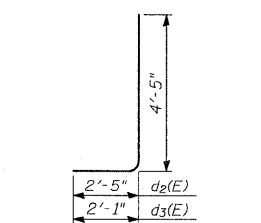
**QUANTITIES PER FOOT OF FLUTED KNEE WALL, SPECIAL**

ITEM	UNIT	TOTAL
Concrete Structures	CU YD	0.15
Concrete Superstructure	CU YD	0.18
Protective Coat	SQ YD	1.01
Rustication Finish	SQ FT	1.84
Reinforcement Bars, Epoxy Coated	POUND	31.8

**NOTES:**

1. For notes, see "Fluted Knee Wall Standard Details" sheet.

\* Cost included with "Fluted Knee Wall, Special."  
 \*\* Cost included with "Concrete Median Surface, 5" (Modified)"



**BARS d<sub>2</sub>(E) & d<sub>3</sub>(E)**

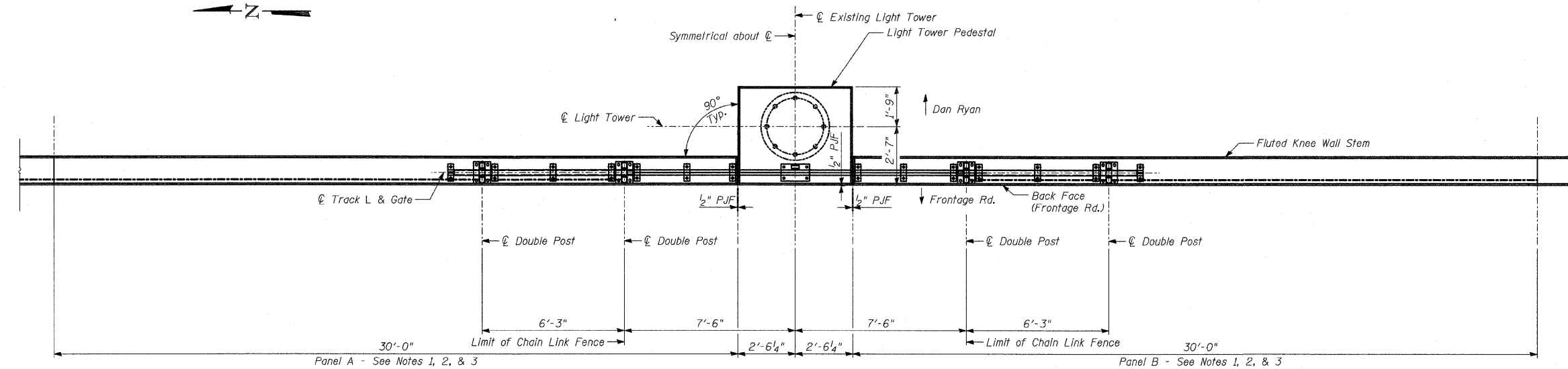
**REVISIONS**

NAME	DATE

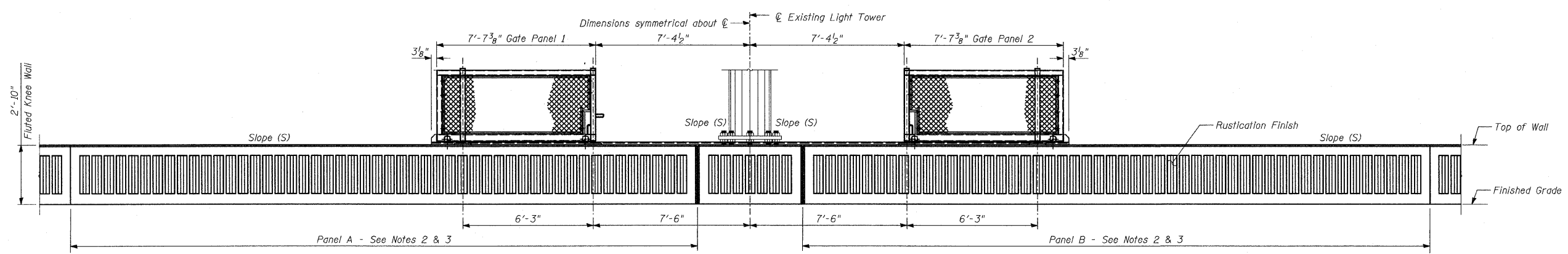
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 FLUTED KNEE WALL, SPECIAL  
 ALONG LAFAYETTE AVE.  
 S. 87TH ST. TO S. OF 85TH ST.  
 FLUTED KNEE WALL, SPECIAL STANDARD DETAILS  
 S.N. DESIGNED BY: MAF  
 SCALE: N.T.S. DRAWN BY: MAF, TB  
 DATE: OCTOBER 17, 2008 CHECKED BY: PF

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	30
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

62979



PLAN - LIMITS OF CHAIN LINK GATE ASSEMBLY IN OPEN POSITION



BACK FACE ELEVATION - LIMITS OF CHAIN LINK GATE ASSEMBLY IN OPEN POSITION

**NOTES:**

1. The Chain Link Gate Assembly (Special), and the Chain Link Gate (Special) are not included in this contract, but shall be furnished and installed by others in Contract 60A02.
2. Panels A & B shall be constructed to be aligned with the Back Face of existing High Mast Light Tower (HMLT).
3. The Slope (S) at the top of panels A & B and the HMLT foundation shall be the same.

**TYLIN**INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 F.A.I. 94 (DAN RYAN EXPRESSWAY)  
 FLUTED KNEE WALL, SPECIAL  
 ALONG LAFAYETTE AVE.  
 S. 87TH ST. TO S. OF 85TH ST.  
 FLUTED KNEE WALL AT HMLT

S.N. DESIGNED BY: MAF  
 SCALE: N.T.S. DRAWN BY: MAF, TB  
 DATE: OCTOBER 17, 2008 CHECKED BY: PF

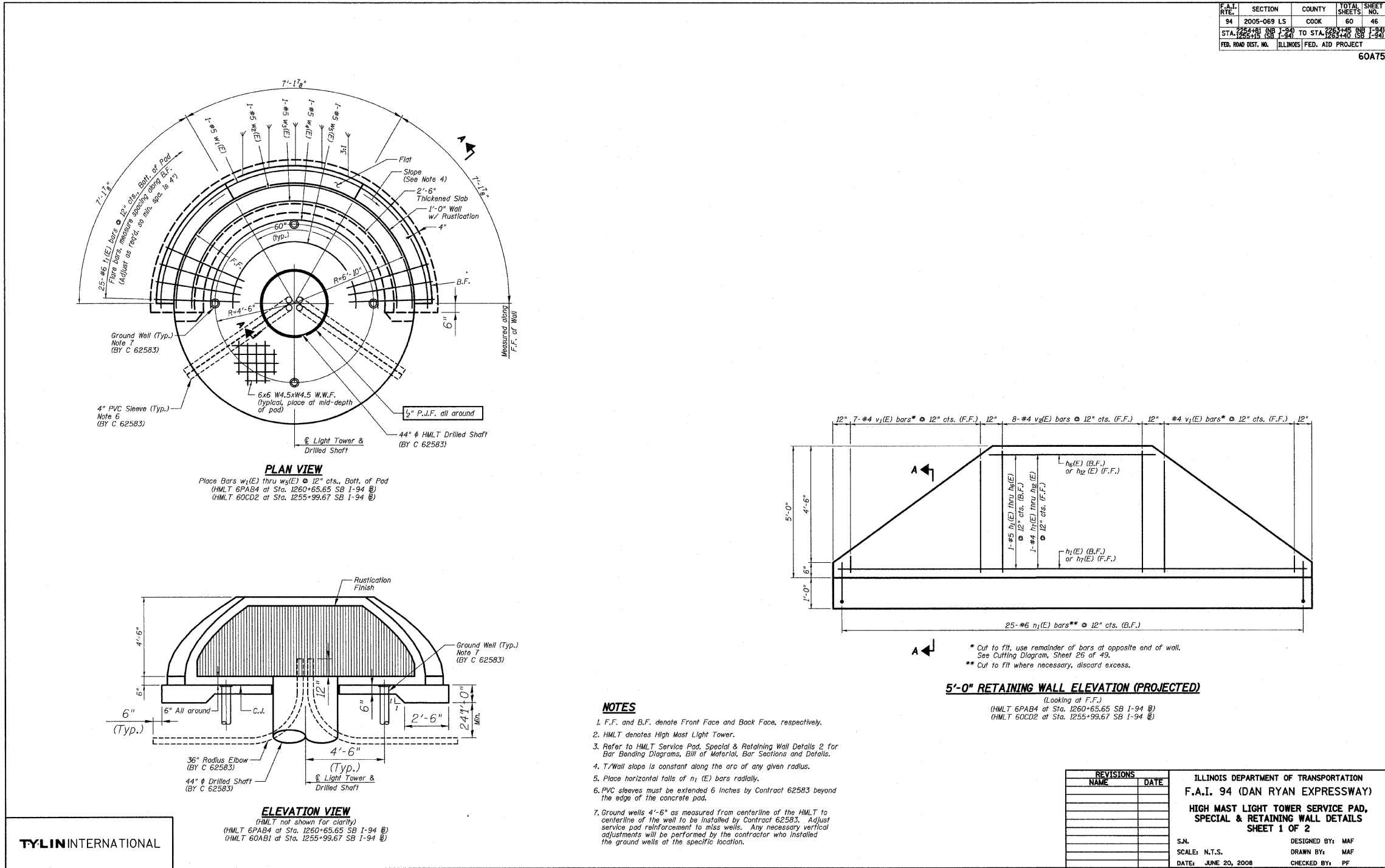
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	31
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

62979

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	2005-069 LS	COOK	60	46
STA. 1254+81.00 TO STA. 1261+45.00		SB I-94		
STA. 1255+15.00 TO STA. 1263+40.00		SB I-94		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

60A75



- NOTES**
1. F.F. and B.F. denote Front Face and Back Face, respectively.
  2. HMLT denotes High Mast Light Tower.
  3. Refer to HMLT Service Pad, Special & Retaining Wall Details 2 for Bar Bending Diagrams, Bill of Material, Bar Sections and Details.
  4. T/Wall slope is constant along the arc of any given radius.
  5. Place horizontal tails of n<sub>1</sub>(E) bars radially.
  6. PVC sleeves must be extended 6 inches by Contract 62583 beyond the edge of the concrete pad.
  7. Ground wells 4'-6" as measured from centerline of the HMLT to centerline of the wall to be installed by Contract 62583. Adjust service pad reinforcement to miss wells. Any necessary vertical adjustments will be performed by the contractor who installed the ground wells at the specific location.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE	F.A.I. 94 (DAN RYAN EXPRESSWAY)	
		HIGH MAST LIGHT TOWER SERVICE PAD, SPECIAL & RETAINING WALL DETAILS SHEET 1 OF 2	
		S.N.	DESIGNED BY: MAF
		SCALE: N.T.S.	DRAWN BY: MAF
		DATE: JUNE 20, 2008	CHECKED BY: PF

TYLIN INTERNATIONAL

**FOR INFORMATION ONLY**

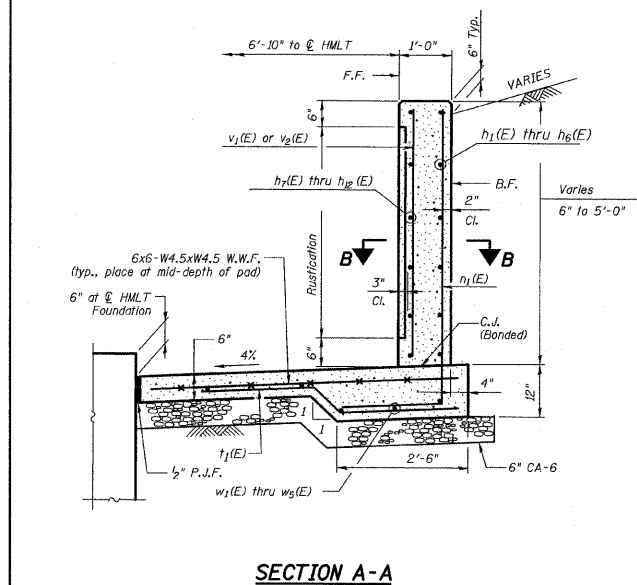
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

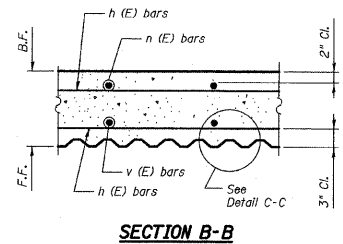
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
HIGH MAST LIGHT TOWER SERVICE PAD,  
SPECIAL & RETAINING WALL DETAILS  
SHEET 1 OF 2  
FOR INFORMATION ONLY

SCALE: N.T.S.      DRAWN BY: JDF  
DATE: OCTOBER 17, 2008      CHECKED BY: JPM

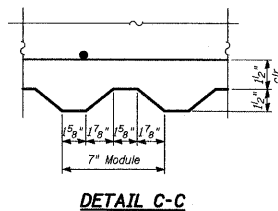
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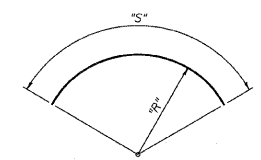
SECTION A-A



SECTION B-B



DETAIL C-C



Bar	"R"	"S"
h1(E)	7'-7"	23'-6"
h2(E)	7'-7"	20'-10"
h3(E)	7'-7"	17'-8"
h4(E)	7'-7"	14'-6"
h5(E)	7'-7"	11'-4"
h6(E)	7'-7"	8'-2"
h7(E)	7'-16"	22'-1"
h8(E)	7'-16"	19'-7"
h9(E)	7'-16"	16'-7"
h10(E)	7'-16"	13'-7"
h11(E)	7'-16"	10'-8"
h12(E)	7'-16"	7'-6"
w1(E)	7'-7"	24'-4"
w2(E)	6'-8"	21'-7"
w3(E)	5'-10"	18'-10"
w4(E)	4'-9"	15'-5"
w5(E)	3'-6"	11'-6"

NOTE: "R" and "S" measured along bar & BARS w1(E) thru w5(E) & h1(E) thru h12(E)

**BILL OF MATERIAL**  
Light Tower Service Pad, Special with 5'-0" Wall (Note 4)

Bar	No.	Size	Length	Shape
h1(E)	1	#5	23'-6"	
h2(E)	1	#5	20'-10"	
h3(E)	1	#5	17'-8"	
h4(E)	1	#5	14'-6"	
h5(E)	1	#5	11'-4"	
h6(E)	1	#5	8'-2"	
h7(E)	1	#4	22'-1"	
h8(E)	1	#4	19'-7"	
h9(E)	1	#4	16'-7"	
h10(E)	1	#4	13'-7"	
h11(E)	1	#4	10'-8"	
h12(E)	1	#4	7'-6"	
n1(E)	25	#6	7'-4"	
v1(E)	25	#6	5'-0"	
v2(E)	7	#4	5'-0"	
v3(E)	8	#4	4'-8"	
w1(E)	1	#5	24'-4"	
w2(E)	1	#5	21'-7"	
w3(E)	1	#5	18'-10"	
w4(E)	1	#5	15'-5"	
w5(E)	1	#5	11'-6"	

**NOTES**

- F.F. and B.F. denote Front Face and Back Face, respectively.
- HMLT denotes High Mast Light Tower.
- See IDOT Standard Drawing BE501 for additional High Mast Light Tower foundation and ground well details.
- Reinforcement Bars designated (E) shall be epoxy coated.
- Refer to High Mast Light Tower Service Pad, Special & Retaining Wall Details 1 for location and orientation of conduit sleeves and grounding wells.
- Contractor shall maintain integrity of Frontage Roads as may be required in excavating for HMLT service pad walls.
- Provide Protective Coat to top, front face, exposed portion of back face, and ends of walls, and top and edges of pad.

**TOTAL BILL OF MATERIAL**

Item	Unit	Total
Light Tower, Service Pad, Special	Each	2

**BILL OF MATERIAL**  
(For HMLT)

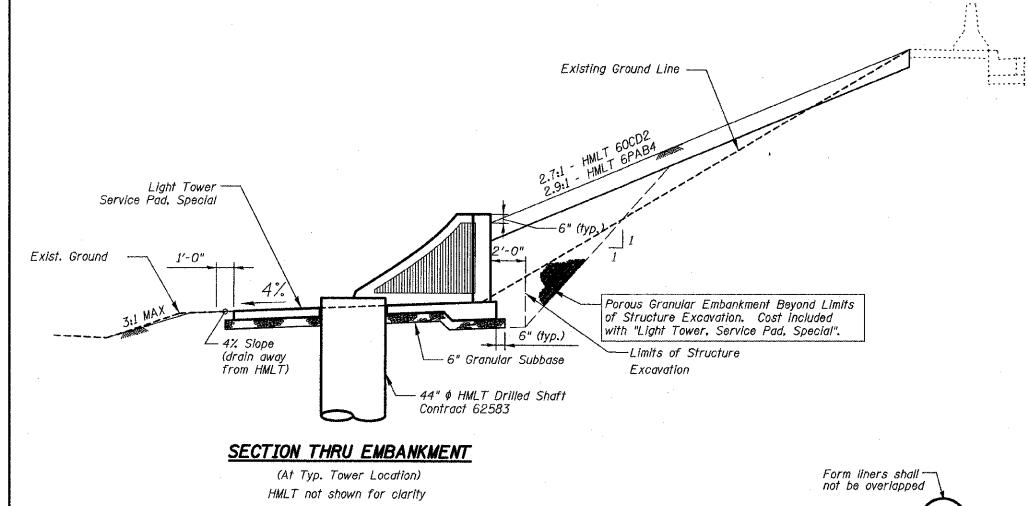
Item	Unit	Total
Welded Wire Fabric	Sq. Yd.	18
Structure Excavation	Cu. Yd.	11
Concrete Structures	Cu. Yd.	7.2
Reinforcement Bars, Epoxy Coated	Pound	775
Rustication Finish	Sq. Ft.	55
Protective Coat	Sq. Yd.	29
Sub-Base Granular Material, Type B 6"	Sq. Yd.	22
Porous Granular Embankment	Cu. Yd.	48

REVISIONS	
NAME	DATE

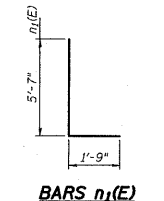
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
HIGH MAST LIGHT TOWER SERVICE PAD,  
SPECIAL & RETAINING WALL DETAILS  
SHEET 2 OF 2

DESIGNED BY: MAF  
DRAWN BY: MAF, TB  
CHECKED BY: PF

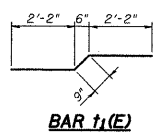
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DATE: JUNE 20, 2008



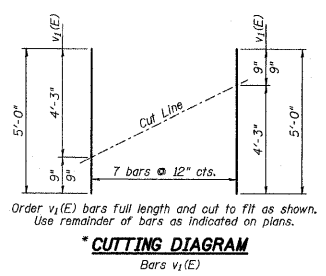
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(At Typ. Tower Location)  
HMLT not shown for clarity



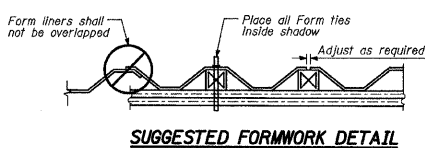
BAR n1(E)



BAR h1(E)



\* CUTTING DIAGRAM  
Bars v1(E)



SUGGESTED FORMWORK DETAIL

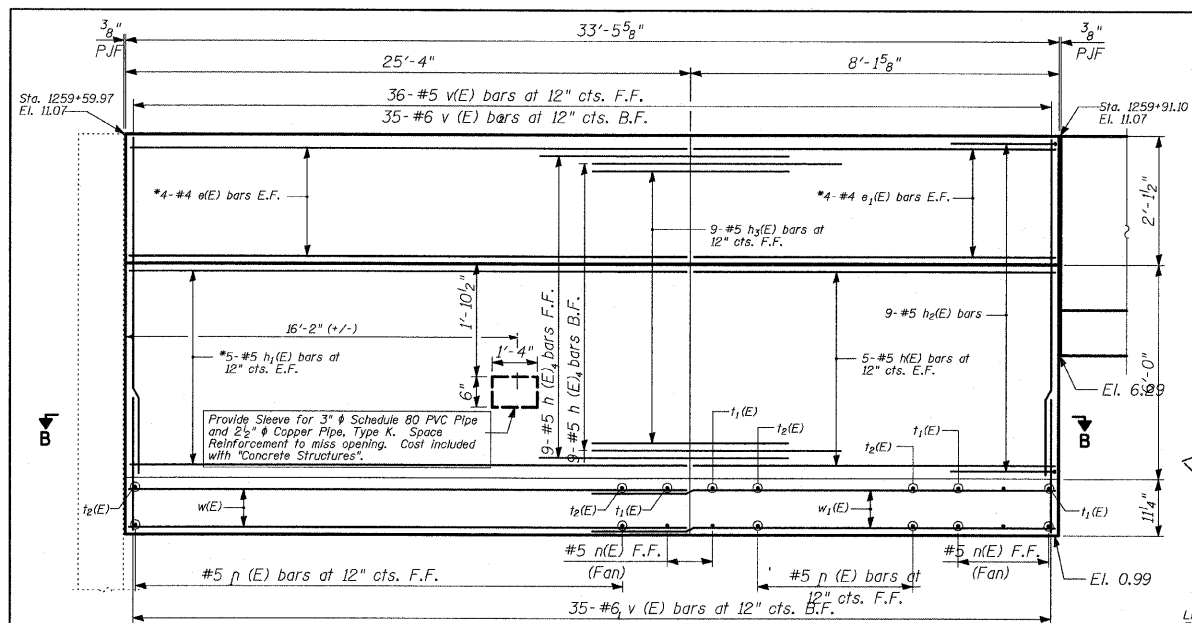
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
HIGH MAST LIGHT TOWER SERVICE PAD,  
SPECIAL & RETAINING WALL DETAILS  
SHEET 2 OF 2  
FOR INFORMATION ONLY

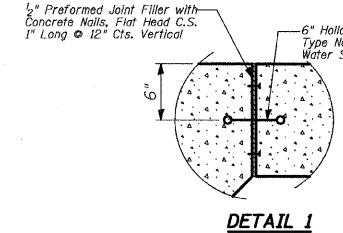
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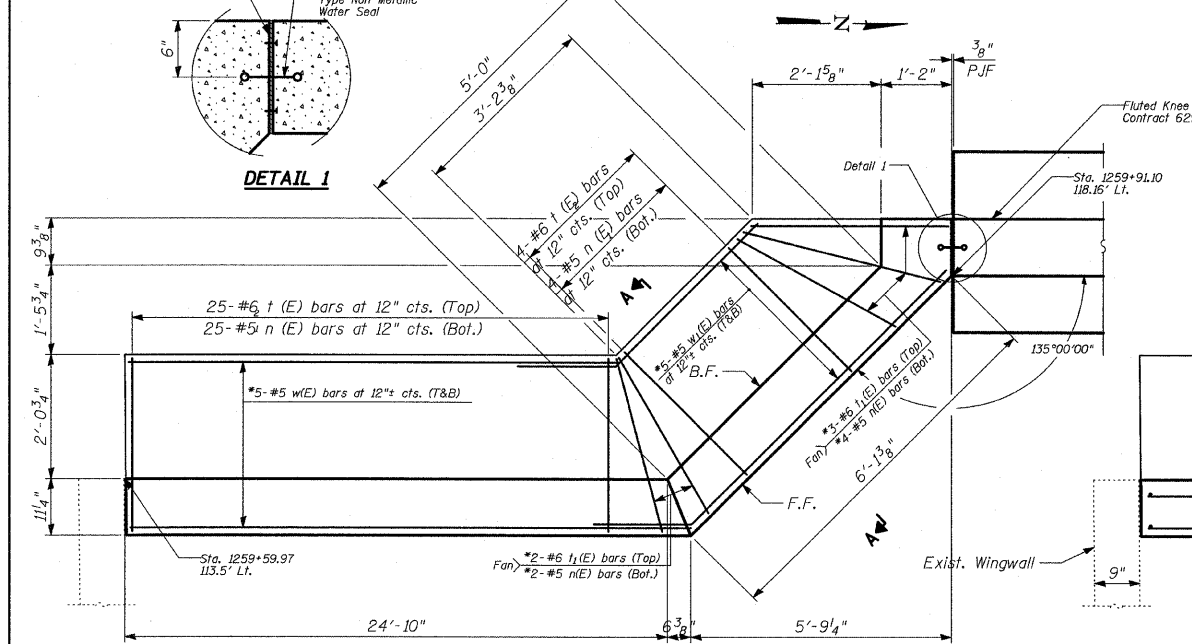




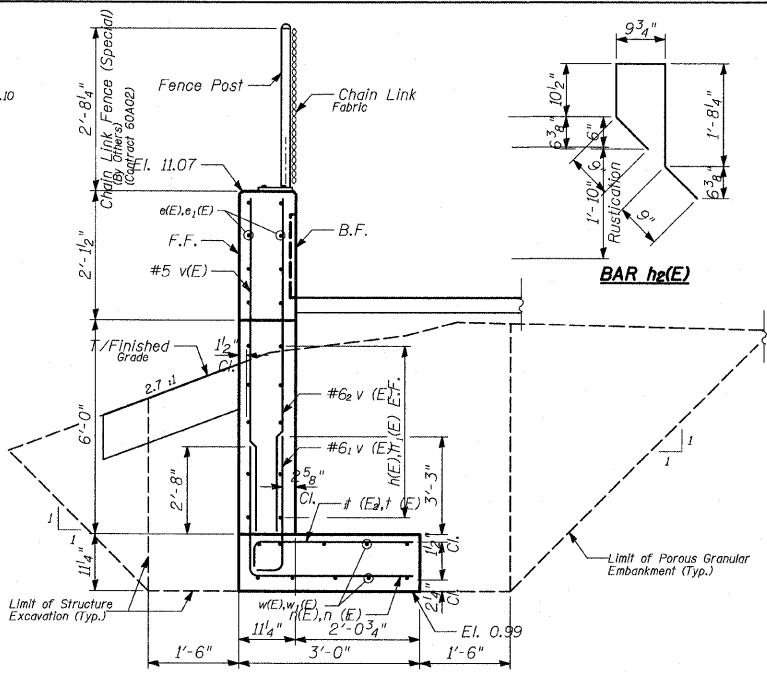
**ELEVATION AT FRONT FACE**



**DETAIL 1**



**FOUNDATION - PLAN**  
NW Quadrant at 87th Street

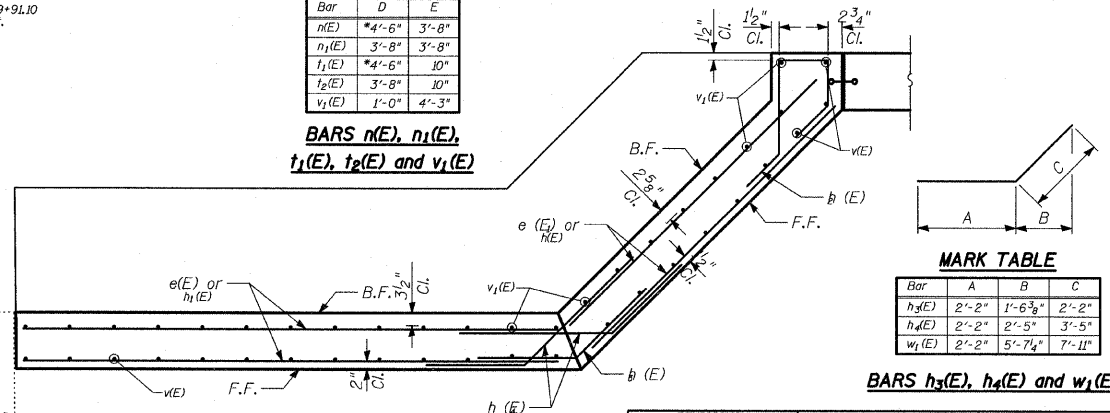


**SECTION A-A**  
Sta. 1259+59.97 to Sta. 1259+78.25

**MARK TABLE**

Bar	D	E
n(E)	*4'-6"	3'-8"
n1(E)	3'-8"	3'-8"
t1(E)	*4'-6"	10"
t2(E)	3'-8"	10"
v1(E)	1'-0"	4'-3"

**BARS n(E), n1(E), t1(E), t2(E) and v1(E)**



**SECTION B-B**

**MARK TABLE**

Bar	A	B	C
h3(E)	2'-2"	1'-6 3/8"	2'-2"
h4(E)	2'-2"	2'-5"	3'-5"
w1(E)	2'-2"	5'-7 1/4"	7'-11"

**BARS h3(E), h4(E) and w1(E)**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	2005-069 LS	COOK	60	45
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

60A75

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape	
e(E)	8	#4	25'-0"		
a1(E)	8	#4	7'-11"		
m(E)	10	#5	7'-11"		
h1(E)	10	#5	25'-0"		
h2(E)	9	#5	6'-6"		
h3(E)	9	#5	4'-4"		
h4(E)	18	#5	5'-7"		
n(E)	6	#5	8'-2"		
n1(E)	29	#5	7'-4"		
t1(E)	5	#6	5'-4"		
t2(E)	29	#6	4'-6"		
v(E)	36	#5	8'-8"		
v1(E)	35	#6	5'-3"		
v2(E)	35	#6	8'-8"		
w(E)	10	#5	25'-0"		
w1(E)	10	#5	10'-4"		
Reinforcement Bars, Epoxy Coated				POUND	2,670
Porous Granular Embankment				CU YD	89
Structure Excavation				CU YD	67
Concrete Structures				CU YD	20
Protective Coat				SQ YD	30
Rustlocation Finish				SQ FT	56

Reinforcement bars designated (E) shall be epoxy coated.

REVISIONS	NAME	DATE

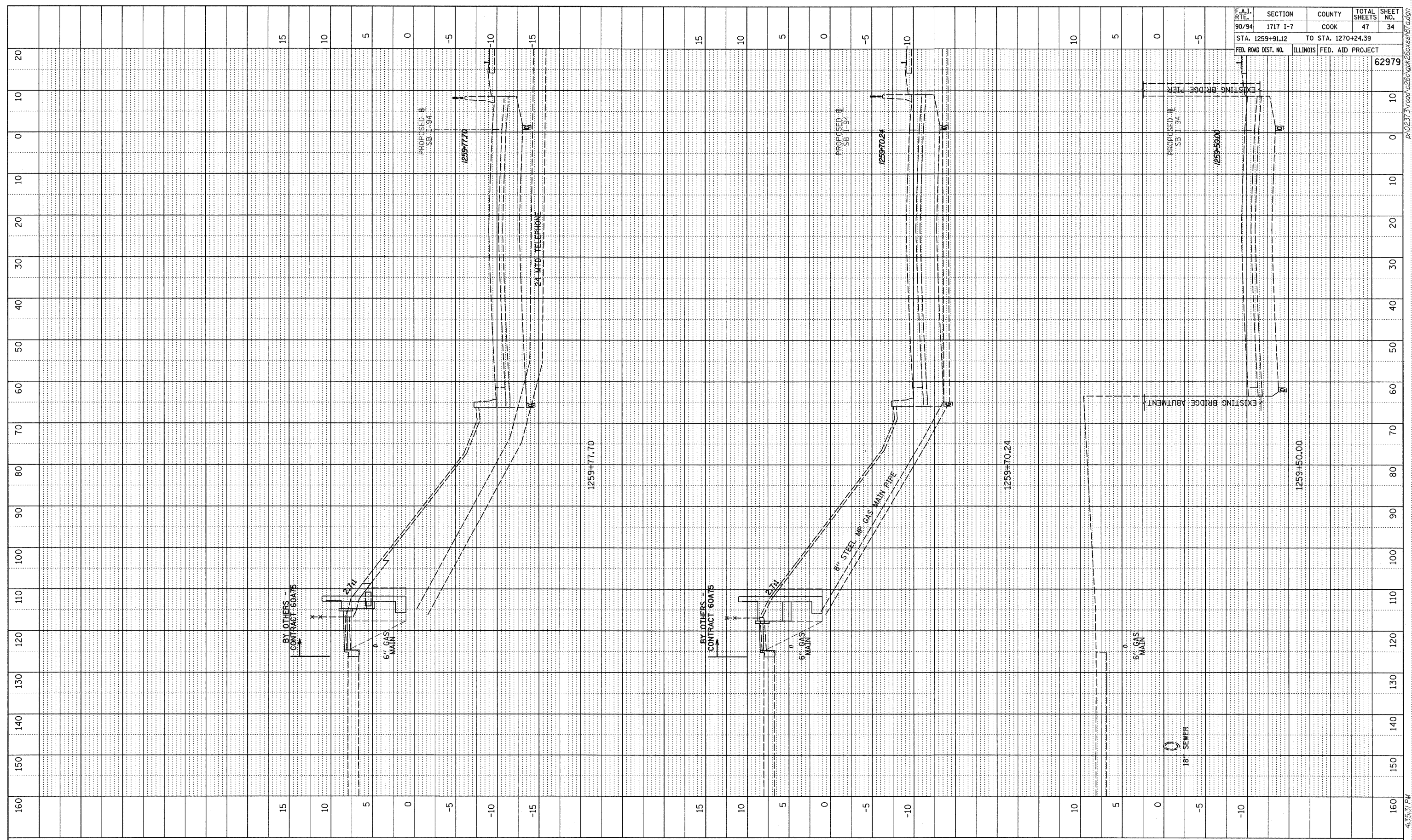
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
TYPICAL SECTIONS, ELEVATIONS AND DETAILS - CORNER GARDEN  
SCALE: N.T.S. DRAWN BY: MAF  
DATE: JUNE 20, 2008 CHECKED BY: PF

**LEGEND**  
E.F. - denotes Each Face  
F.F. - denotes Front Face  
B.F. - denotes Back Face  
T&B - denotes Top and Bottom

REVISIONS	NAME	DATE

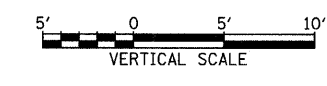
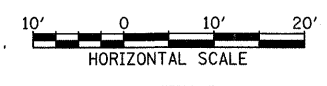
ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 94 (DAN RYAN EXPRESSWAY)  
TYPICAL SECTIONS, ELEVATIONS AND DETAILS - CORNER GARDEN  
FOR INFORMATION ONLY  
SCALE: N.T.S. DRAWN BY: MAF  
DATE: OCTOBER 17, 2008 CHECKED BY: PF

PROFILE	SURVEYED	BY	DATE
NOTE BOOK	CHECKED		
NO.	DATE		
	STRUCTURE	NOTATIONS	CHRD



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	1717 I-7	COOK	47	34
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		62979	

TYLIN INTERNATIONAL

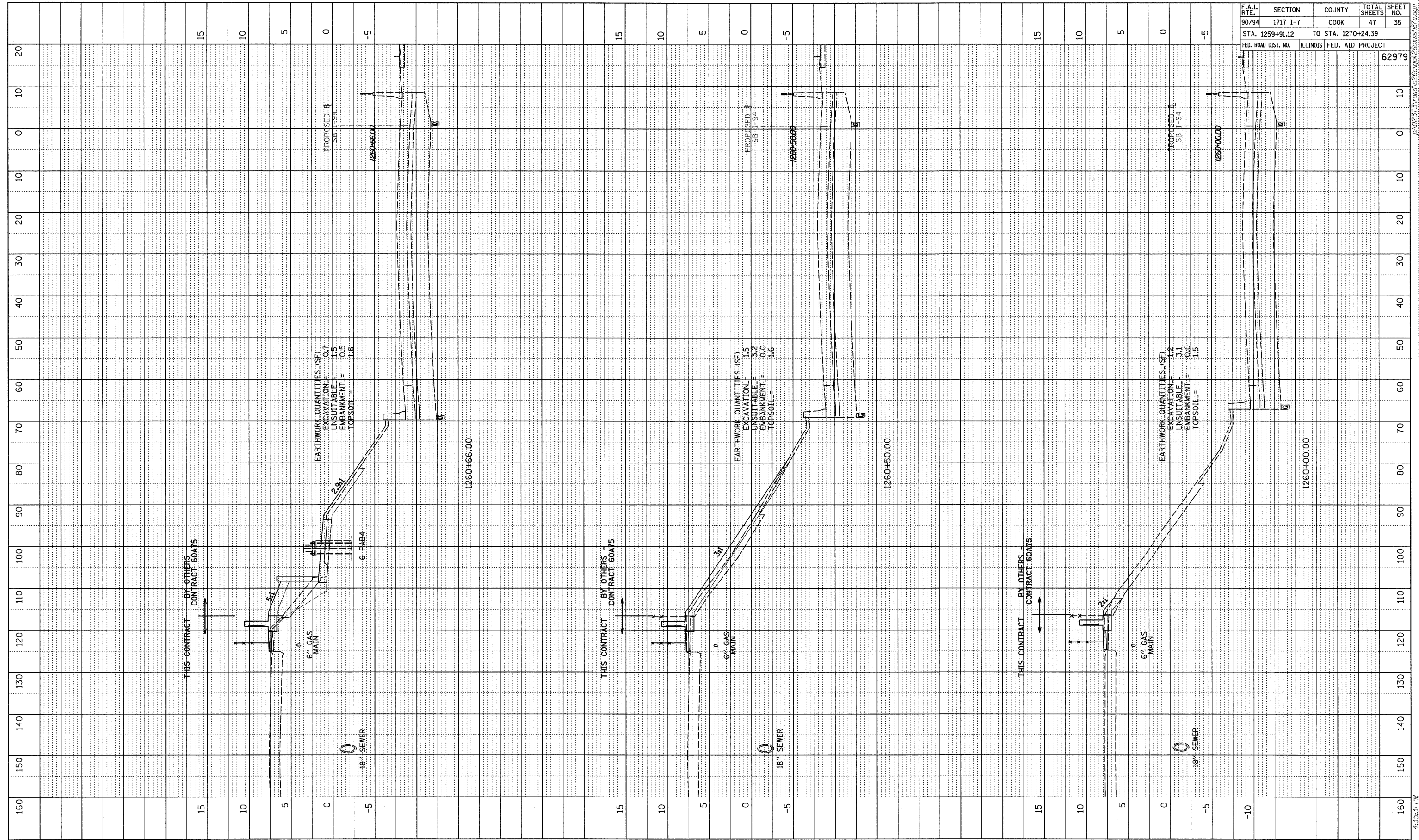


SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1259+50.00 TO STA. 1259+77.70

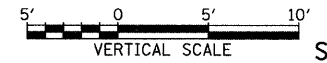
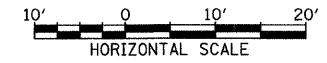
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PROFILE SURVEYED BY DATE  
 POINTS CHECKED  
 B.M. NOTED  
 NO. STRUCTURE NOTATING CHRD

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	1717 I-7	COOK	47	35
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
				62979



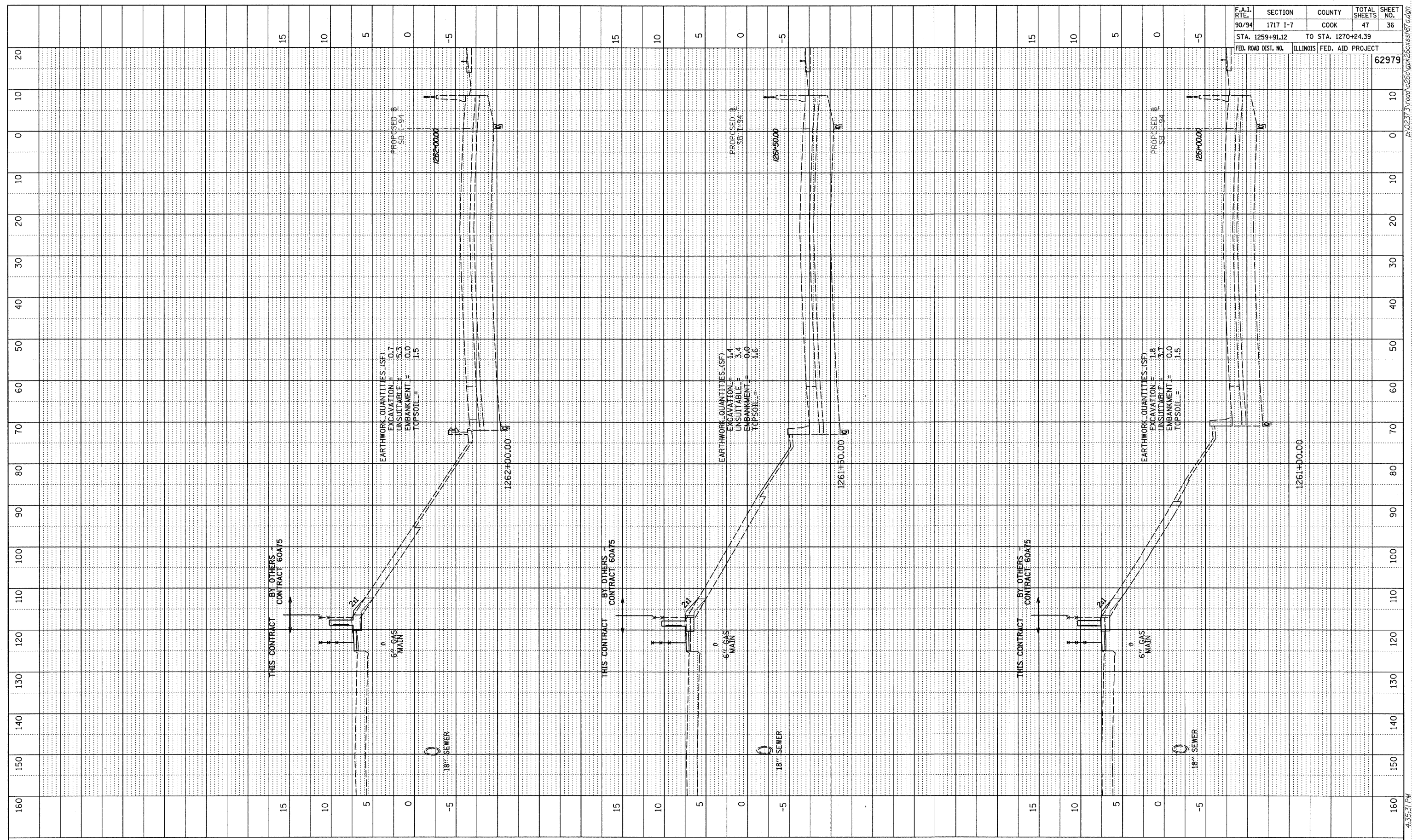
TYLIN INTERNATIONAL



SB I-94 (DAN RYAN EXPRESSWAY)  
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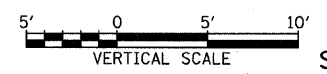
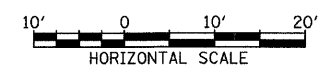
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PROFILE SURVEYED BY DATE  
 CHECKED BY  
 NOTE BOOK NO. NOTED BY  
 STRUCTURE NOTATIONS CHFD



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	1717 I-7	COOK	47	36
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		62979	

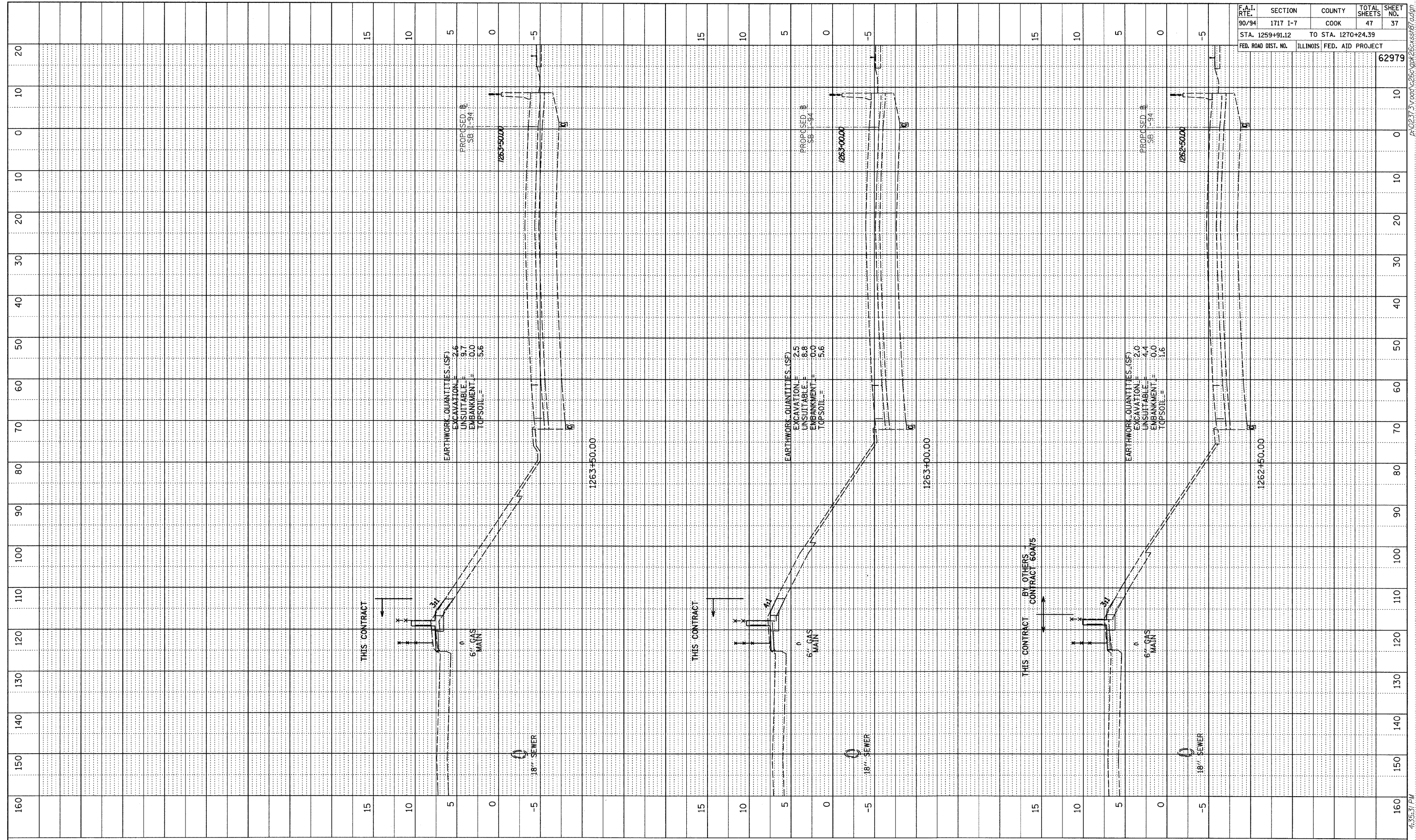
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SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1261+00.00 TO STA. 1262+00.00

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PROFILE SURVEYED BY DATE  
 GRADES CHECKED  
 NOTE BOOK NO. NOTED  
 STRUCTURE NOTATIONS CIP/ID



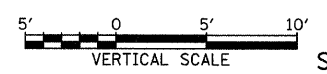
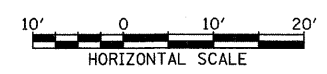
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	1717 I-7	COOK	47	37
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
				62979

EARTHWORK QUANTITIES (SF)  
 EXCAVATION = 2.6  
 UNSUITABLE = 9.7  
 EMBANKMENT = 0.0  
 TOPSOIL = 5.6

EARTHWORK QUANTITIES (SF)  
 EXCAVATION = 2.5  
 UNSUITABLE = 8.8  
 EMBANKMENT = 0.0  
 TOPSOIL = 5.6

EARTHWORK QUANTITIES (SF)  
 EXCAVATION = 2.0  
 UNSUITABLE = 4.4  
 EMBANKMENT = 0.0  
 TOPSOIL = 1.6

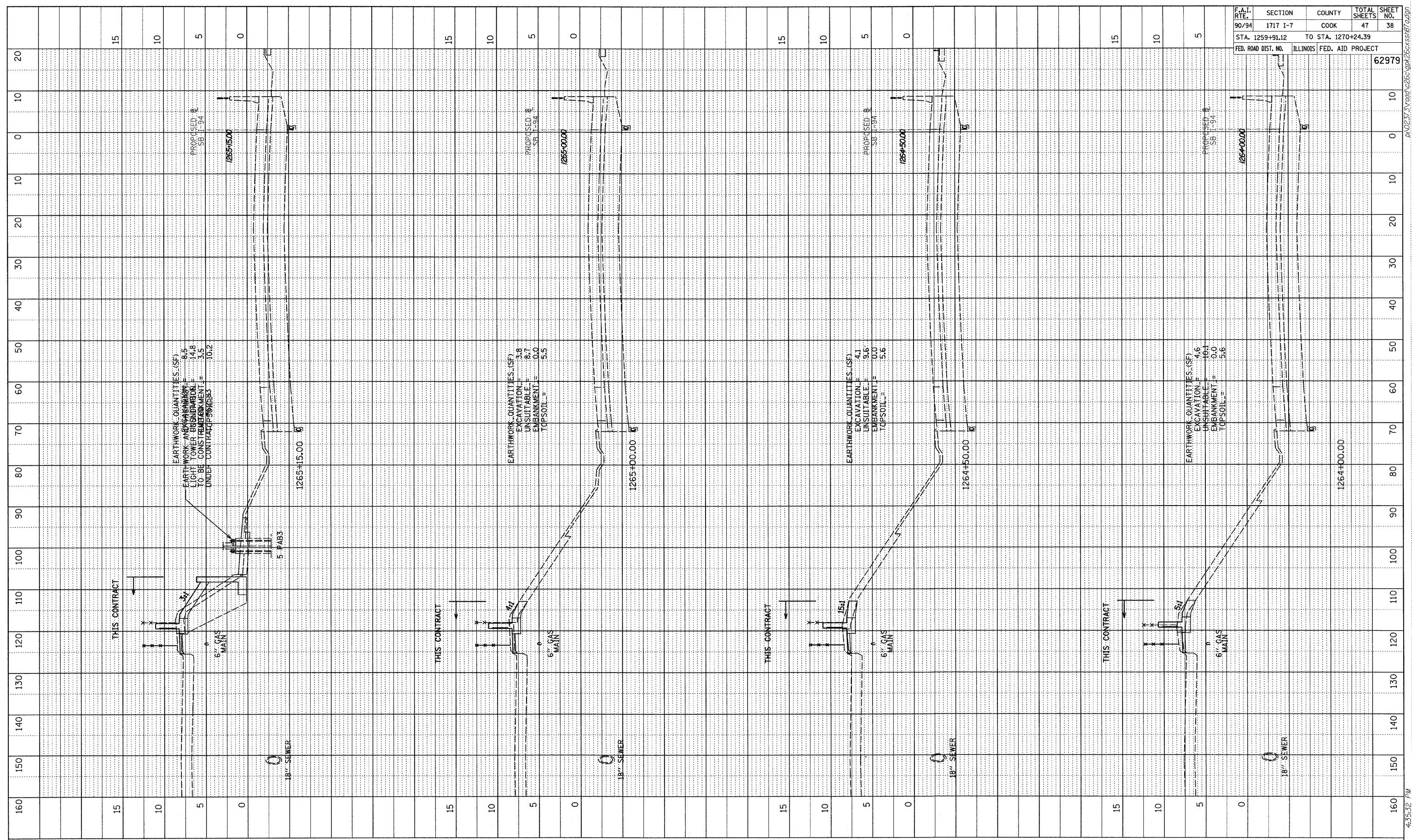
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SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1262+50.00 TO STA. 1263+50.00

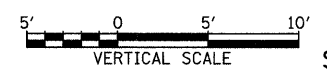
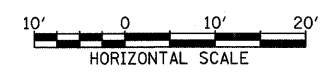
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PROFILE SURVEYED BY DATE  
 GRADES CHECKED  
 NOTE BOOK NO. NOTED  
 STRUCTURE NOTATIONS CHRD



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	1717 I-7	COOK	47	38
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
				62979

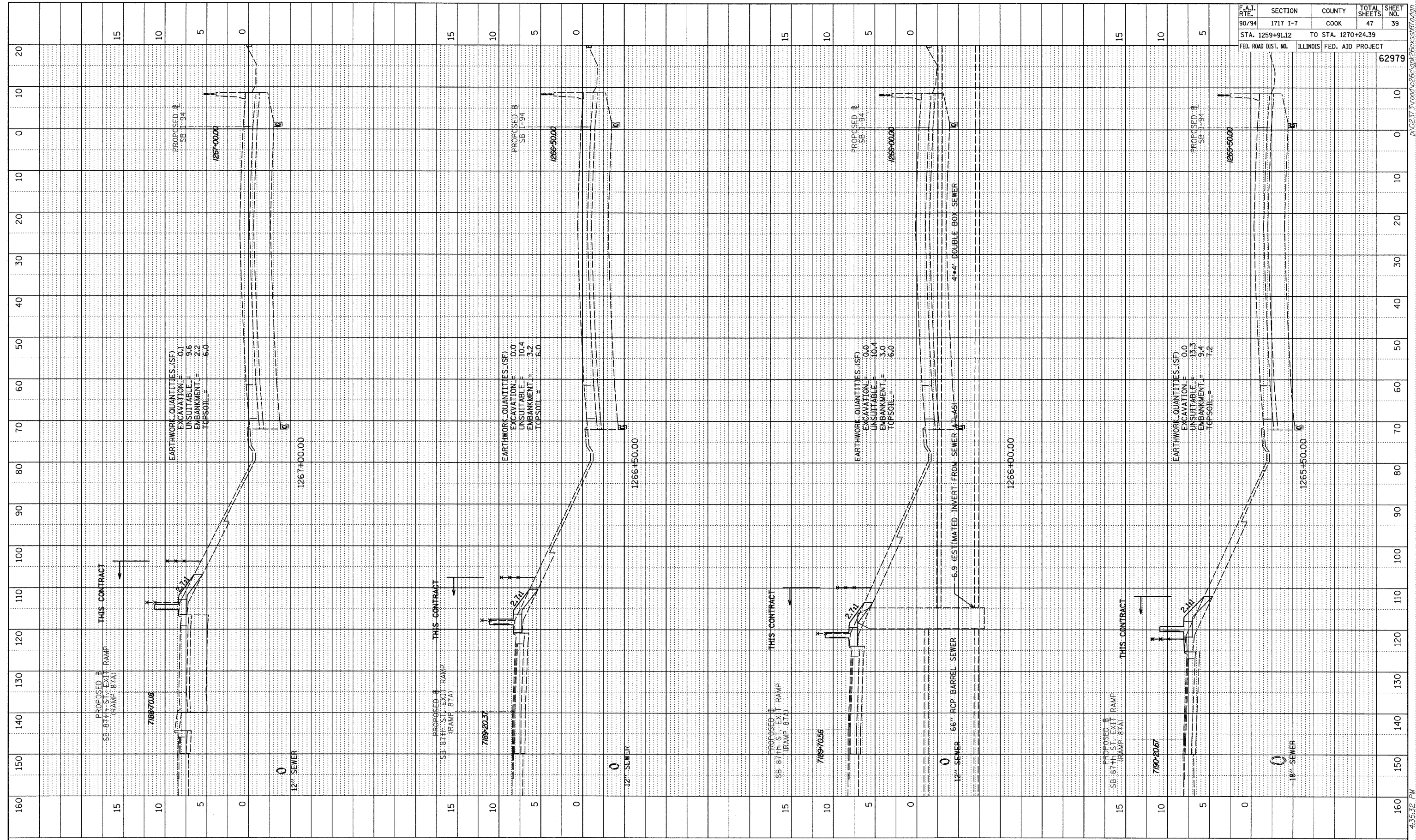
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SB I-94 (DAN RYAN EXPRESSWAY)  
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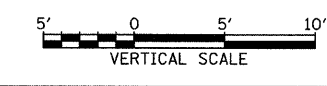
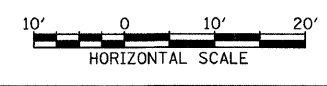
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PROFILE	SUBMITTED	BY	DATE
NOTE BOOK	DESIGNED		
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	DATE		
	BY		
	NO.		
	DATE		
	BY		
	NO.		
	DATE		



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	1717 I-7	COOK	47	39
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
				62979

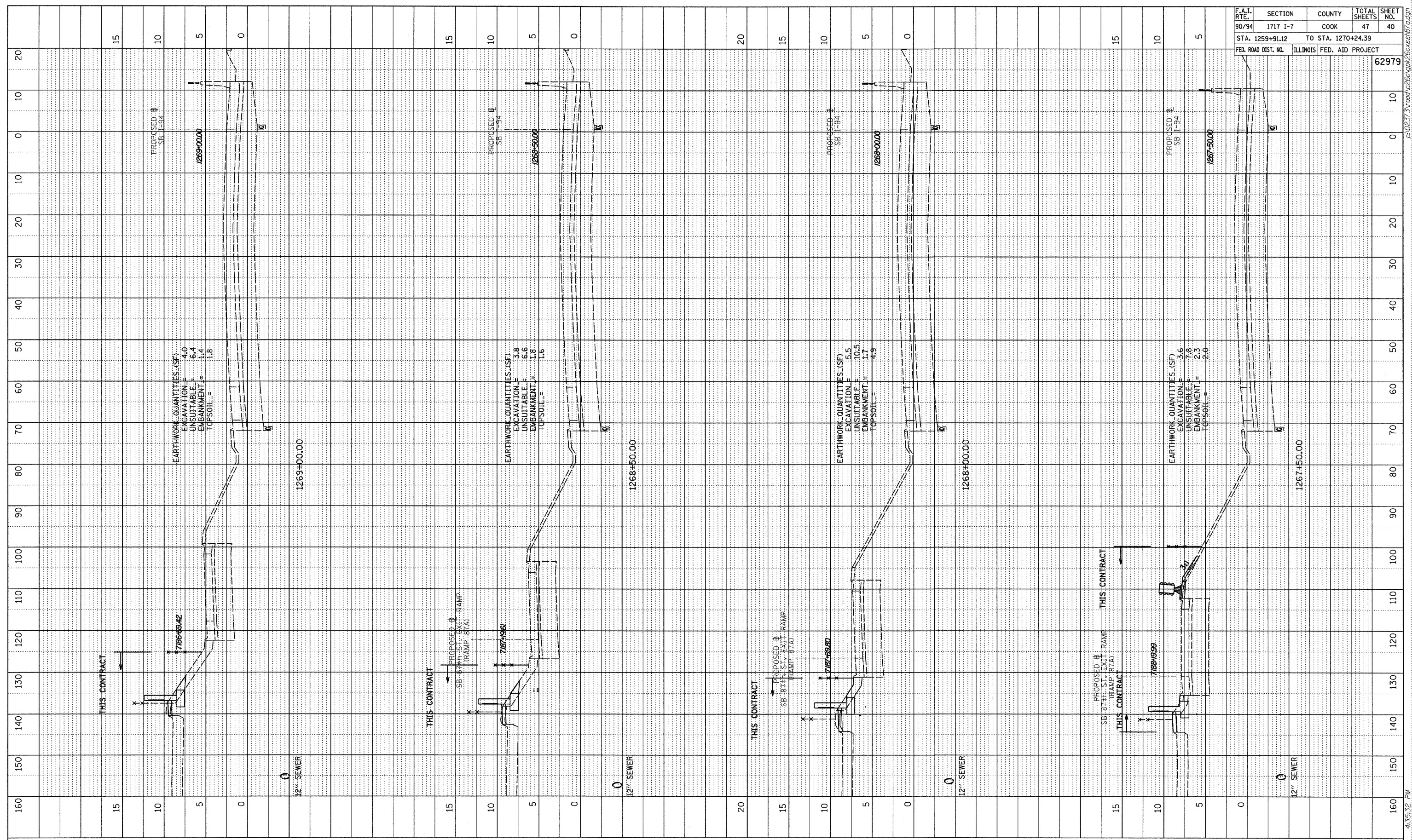
**TYLIN** INTERNATIONAL



SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1265+50.00 TO STA. 1267+00.00

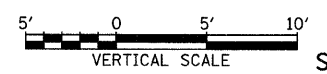
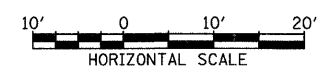
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PROFILE REVIEWED BY DATE  
 PLANS CHECKED BY DATE  
 NOTE BOOK NO. ELM. NOTED BY DATE  
 STRUCTURE NOTATIONS CHFD



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	1717 I-7	COOK	47	40
STA. 1259+91.12		TO STA. 1270+24.39		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
				62979

TYLIN INTERNATIONAL

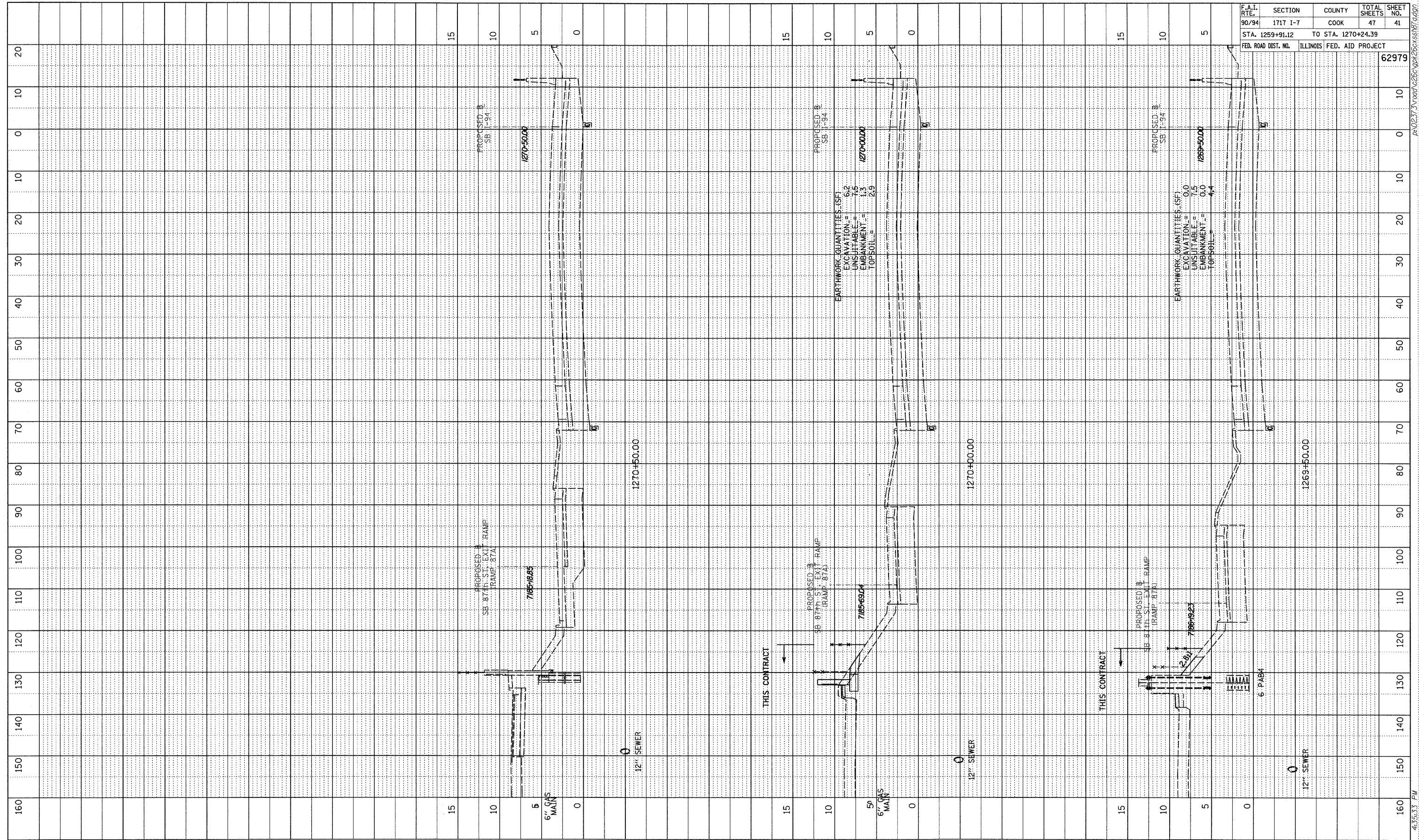


SB I-94 (DAN RYAN EXPRESSWAY)  
 STA. 1267+50.00 TO STA. 1269+00.00

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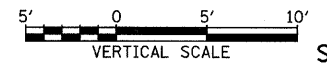
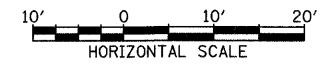


PROFILE	SURVEYED	BY	DATE
NOTE BOOK	GRADES CHECKED		
NO.	STRUCTURE NOTATIONS		



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	1717 I-7	COOK	47	41
STA. 1259+91.12	TO STA. 1270+24.39			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		62979	

TYLIN INTERNATIONAL

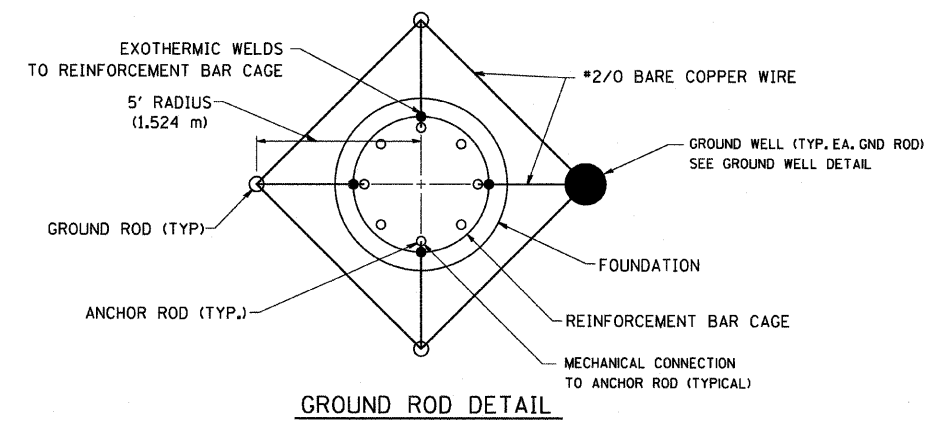
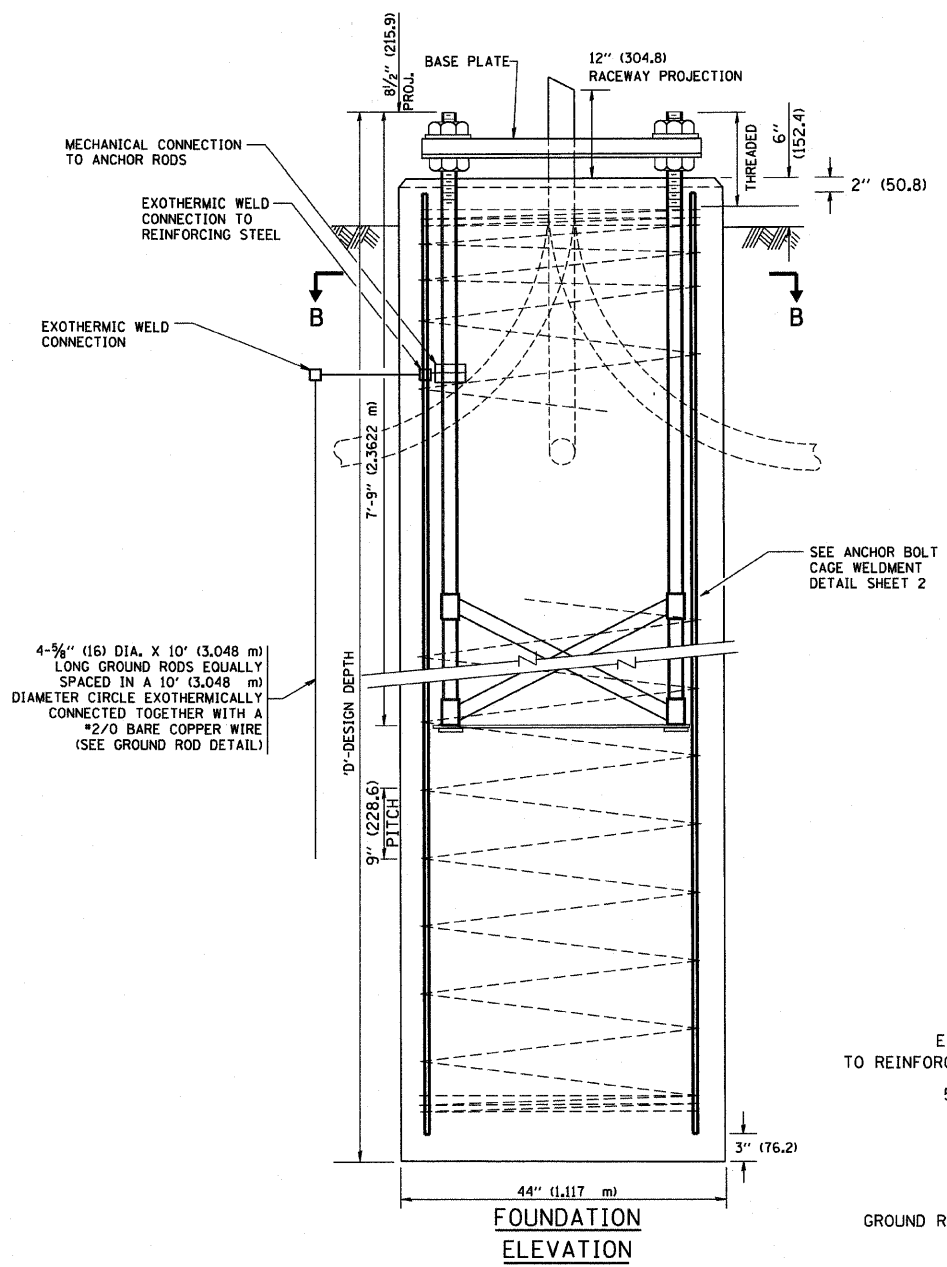
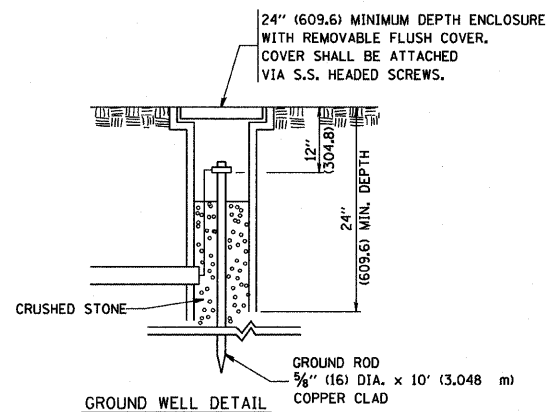
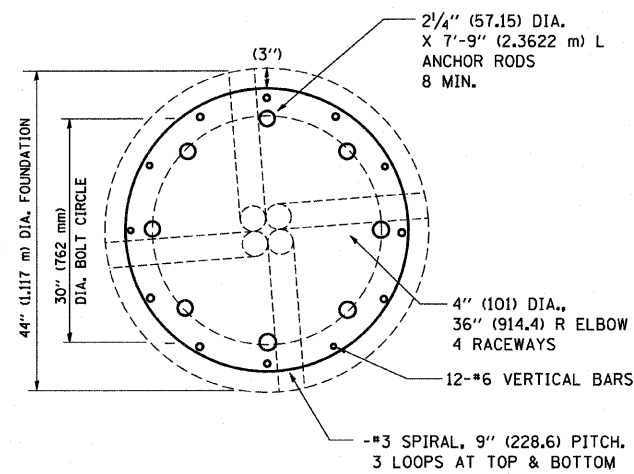


SB I-94 (DAN RYAN EXPRESSWAY)  
STA. 1269+50.00 TO STA. 1270+50.00

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**LIGHT TOWER FOUNDATION DEPTH "D"**

MOUNTING HEIGHT	SOIL CONDITIONS					
	SOFT CLAY Q <sub>u</sub> = 0.375 TON/SQ. FT	MEDIUM CLAY Q <sub>u</sub> = 0.75 TON/SQ. FT	STIFF CLAY Q <sub>u</sub> = 1.50 TON/SQ. FT	LOOSE SAND φ = 34°	MEDIUM SAND φ = 37.5°	DENSE SAND φ = 40°
90 FT (27 m)	29 FT (8.779 m)	20 FT (6.035 m)	15 FT (4.389 m)	15 FT (4.389 m)	13 FT (3,840 m)	12 FT (3,429 m)
100 FT (30 m)	32 FT (9.754 m)	22 FT (6.706 m)	16 FT (4.877 m)	16 FT (4.877 m)	14 FT (4,267 m)	13 FT (3.81 m)
110 FT (33 m)	35 FT (10.719 m)	24 FT (7.377 m)	18 FT (5.365 m)	18 FT (5.365 m)	15 FT (4,694 m)	14 FT (4.191 m)
120 FT (36 m)	38 FT (11.705 m)	26 FT (8.046 m)	19 FT (5.652 m)	19 FT (5.652 m)	17 FT (5.120 m)	16 FT (4.572 m)



**DESIGN NOTES**

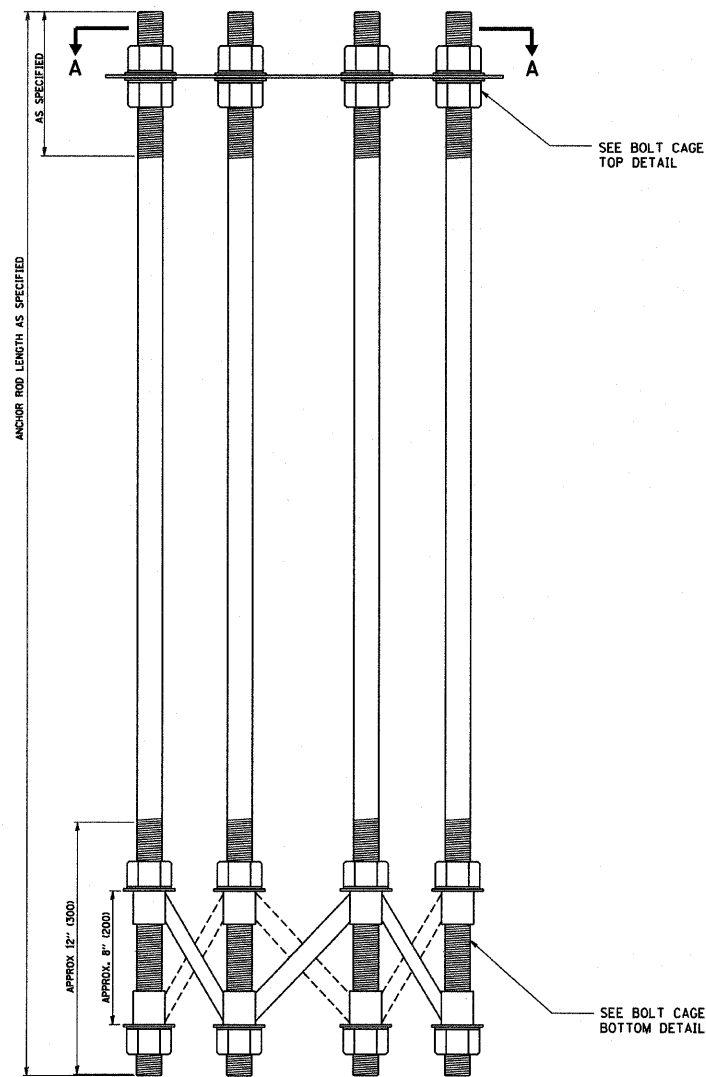
- (1) ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN
- (2) THE ANCHOR RODS SHALL BE VERTICAL NO ADJUSTMENT SHALL BE ALLOWED AFTER THE FOUNDATION IS PLACED.
- (3) THE GAP BETWEEN THE FOUNDATION AND THE BASE PLATE SHALL BE ENCLOSED WITH A STAINLESS STEEL SCREEN FASTENED WITH A STAINLESS STEEL BAND.
- (4) THE TOP OF THE FOUNDATION TO 18" (450) BELOW GRADE SHALL BE FORMED.
- (5) SURFACE WATER WILL NOT BE PERMITTED TO ENTER THE HOLE AND ALL WATER WHICH MAY HAVE INFILTRATED INTO THE HOLE SHALL BE REMOVED BEFORE PLACING CONCRETE.
- (6) THE LIGHT TOWER SHALL NOT BE ERECTED UNTIL AFTER THE CONCRETE HAS BEEN CURED ACCORDING TO ARTICLE 1020.13.
- (7) ANCHOR RODS SHALL BE STRAIGHT AND SHALL BE ACCORDING TO AASHTO M 314 OR ASTM F1554, GRADE 725 (GRADE 105) AND GALVANIZED ACCORDING TO ARTICLE 1006.9.
- (8) ANCHOR ROD INFORMATION SHALL BE SUBMITTED FOR APPROVAL AND SHALL BE FULLY COORDINATED FOR APPROVAL WITH TOWER MANUFACTURER REQUIREMENTS.
- (9) REINFORCEMENT BARS SHALL BE ACCORDING TO ARTICLE 1006.10
- (10) TWO ANCHOR RODS OPPOSITE EACH OTHER SHALL HAVE THE ANCHOR ROD THREADS PEENED AFTER NUTS ARE INSTALLED.

**FOR INFORMATION ONLY**

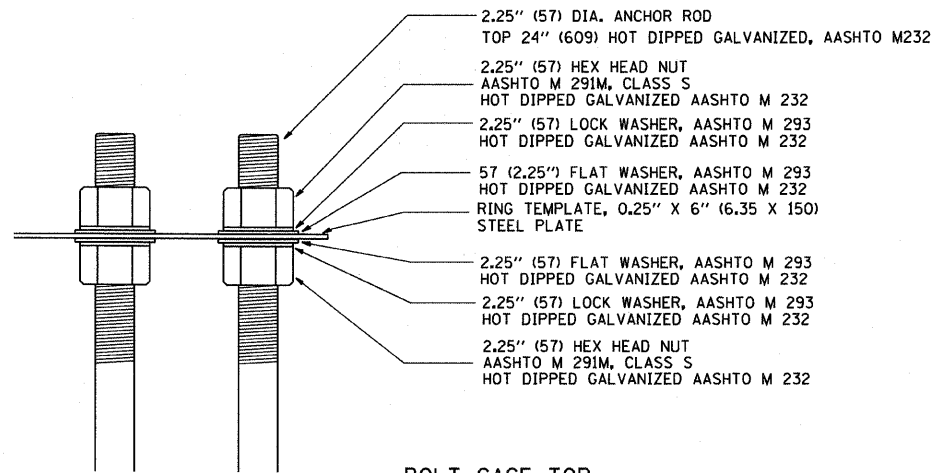
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

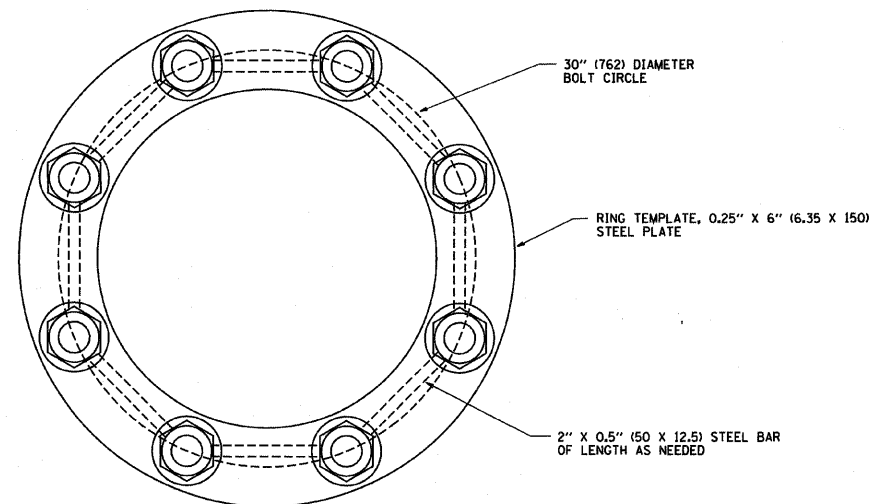
HIGH MAST LIGHT TOWER		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90 FT TO 120 FT (27 m TO 36 m) FOUNDATION DETAIL		94	1717 I-7	COOK	47	42
SCALE: NONE		SHEET NO. 1 OF 2 SHEETS		STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
		BE-501		CONTRACT NO. 62979		



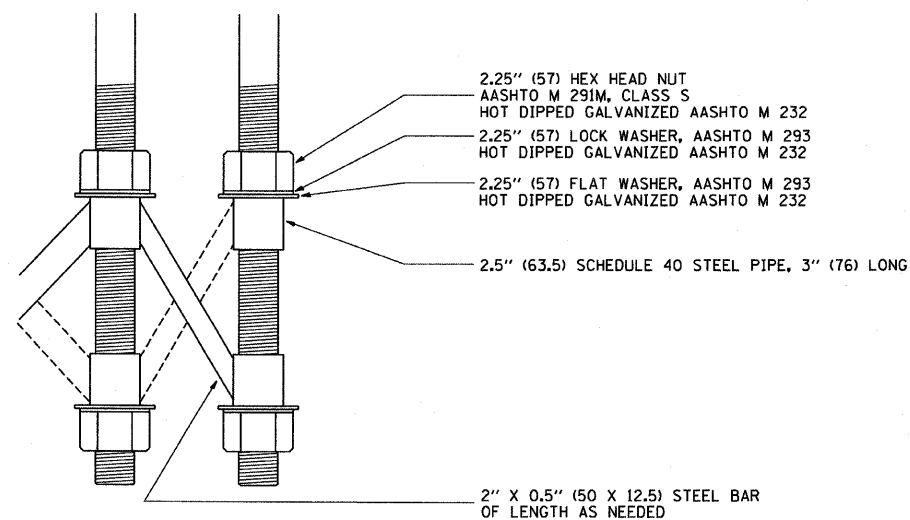
ANCHOR BOLT CAGE



BOLT CAGE TOP



SECTION A-A



BOLT CAGE BOTTOM

**NOTES**

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN
2. ANCHOR RODS SHALL BE STRAIGHT AND SHALL BE ACCORDING TO AASHTO M 314 OR ASTM F1554, GRADE 725 (GRADE 105) AND GALVANIZED ACCORDING TO ARTICLE 1006.09.
3. ANCHOR ROD INFORMATION SHALL BE SUBMITTED FOR APPROVAL AND SHALL BE FULLY COORDINATED WITH TOWER MANUFACTURERS REQUIREMENTS.

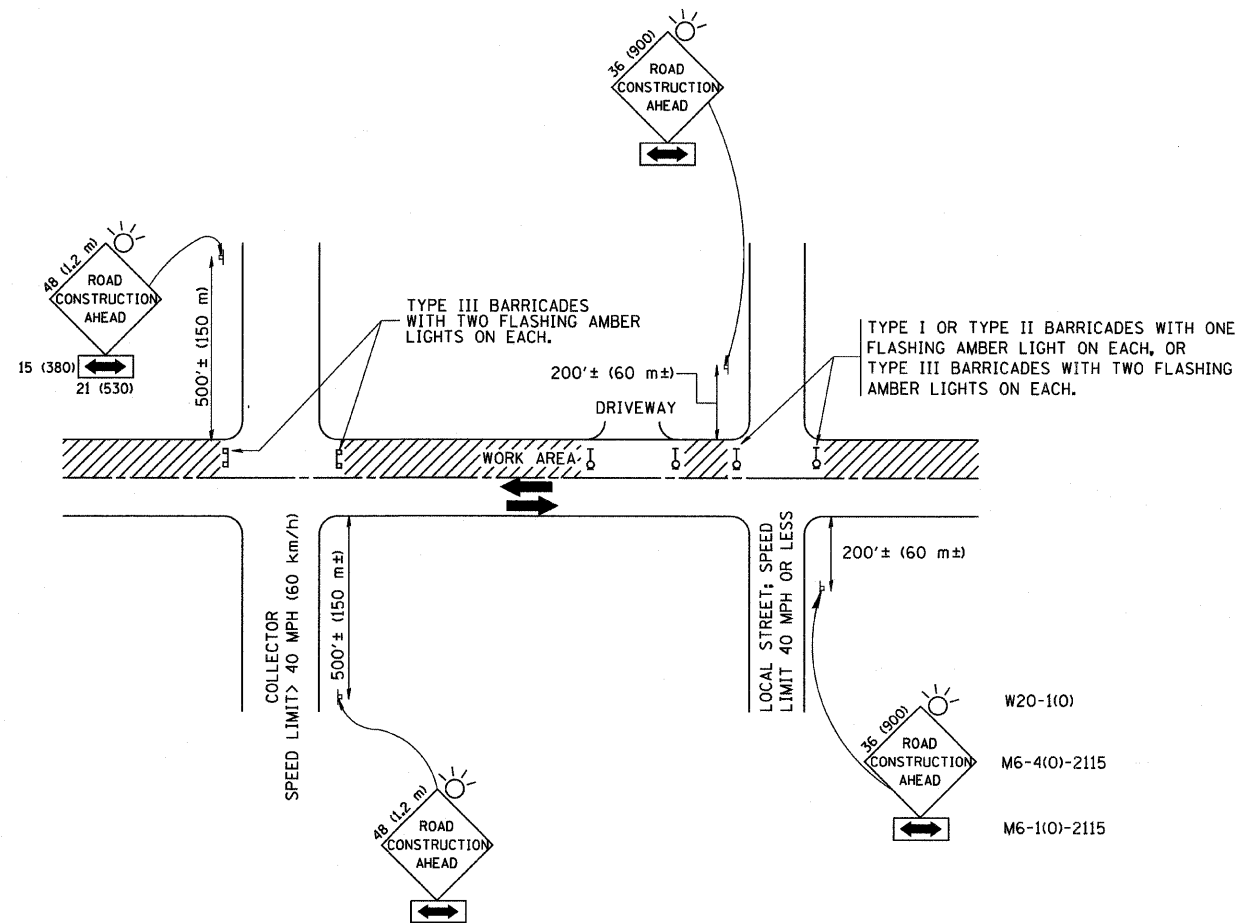
**FOR INFORMATION ONLY**

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

HIGH MAST LIGHT TOWER			
90 FT TO 120 FT (27 m TO 36 m) FOUNDATION DETAIL			
SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	I-7	COOK	47	43
BE-501			CONTRACT NO. 62979	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

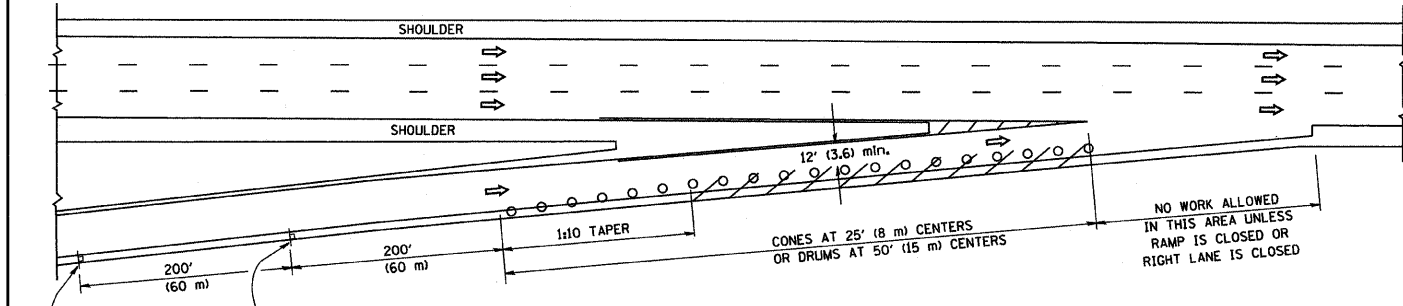
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

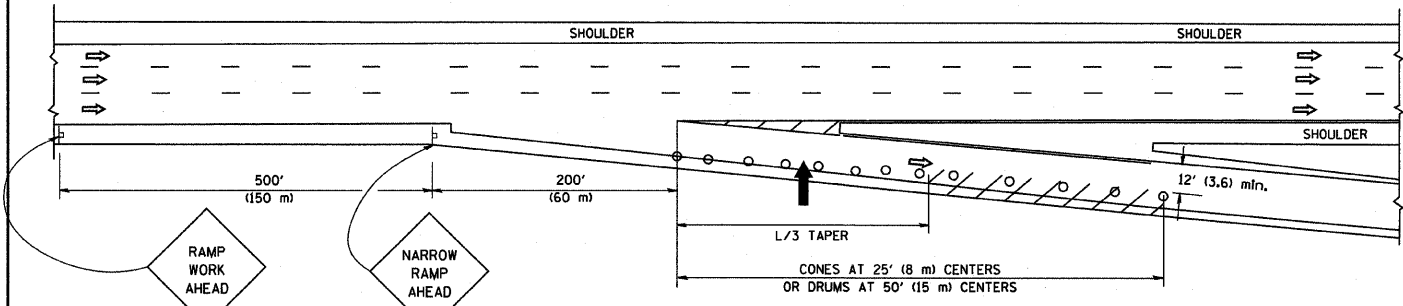
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 E-7	COOK	47	44
TC-10			CONTRACT NO. 62979	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				

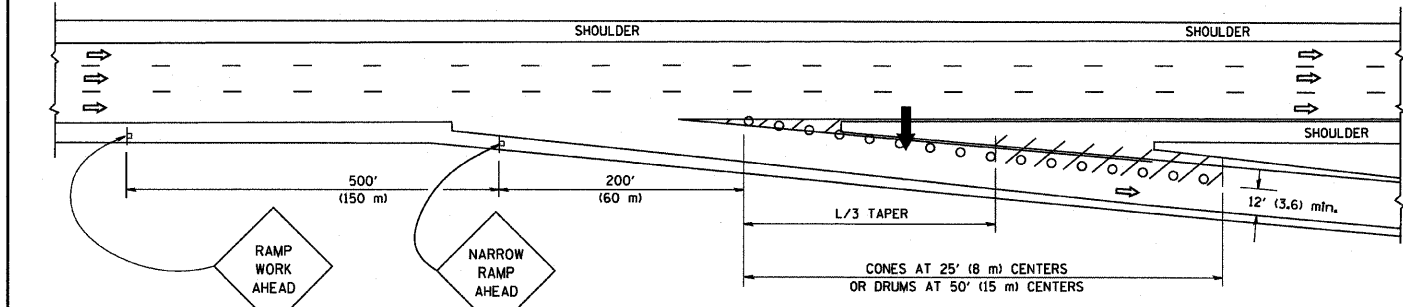
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

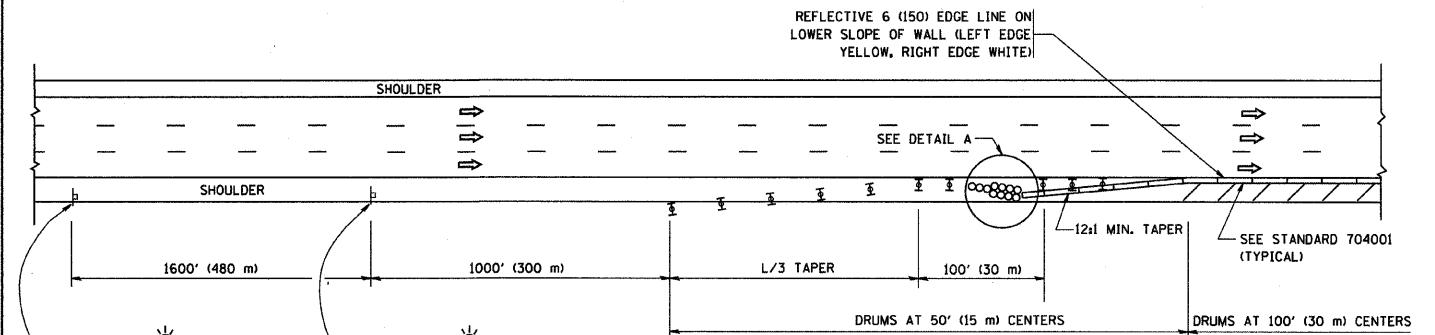
- ➔ ARROWBOARD
- ▨ WORK AREA
- ┌ SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- ⊠ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

GENERAL NOTES

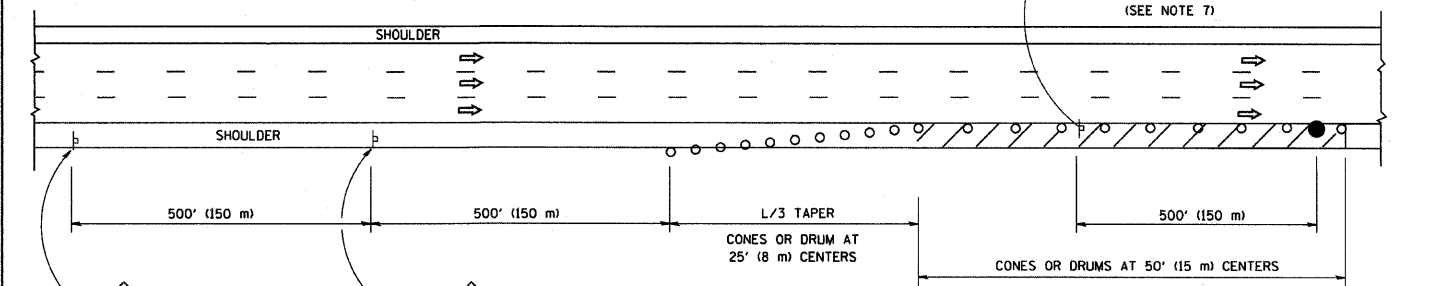
1. THE "L" DISTANCE EQUALS:  

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER	METRIC ENGLISH L=0.65(WXS) L=(WXS)
W = WIDTH OF OFFSET IN FEET (METERS)	
S = NORMAL POSTED SPEED MPH (KM/H)	
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS



PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:  
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCRUCH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350 COMPLIANT FOR POSTED SPEED.

DETAIL "A"  
 IMPACT ATTENUATOR, TEMPORARY  
 (SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

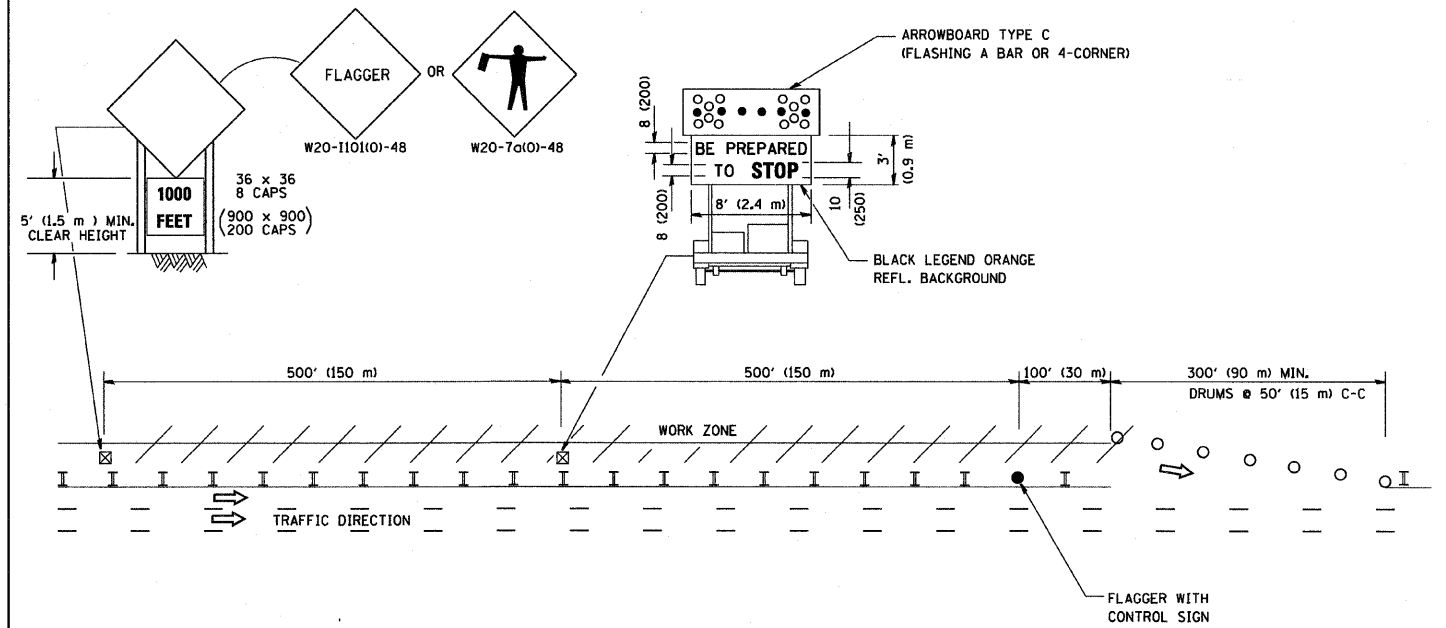
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		CHECKED -	REVISED - J.A.F. 12-06
		DATE - 11-96	REVISED - S.P.B. 01-07

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

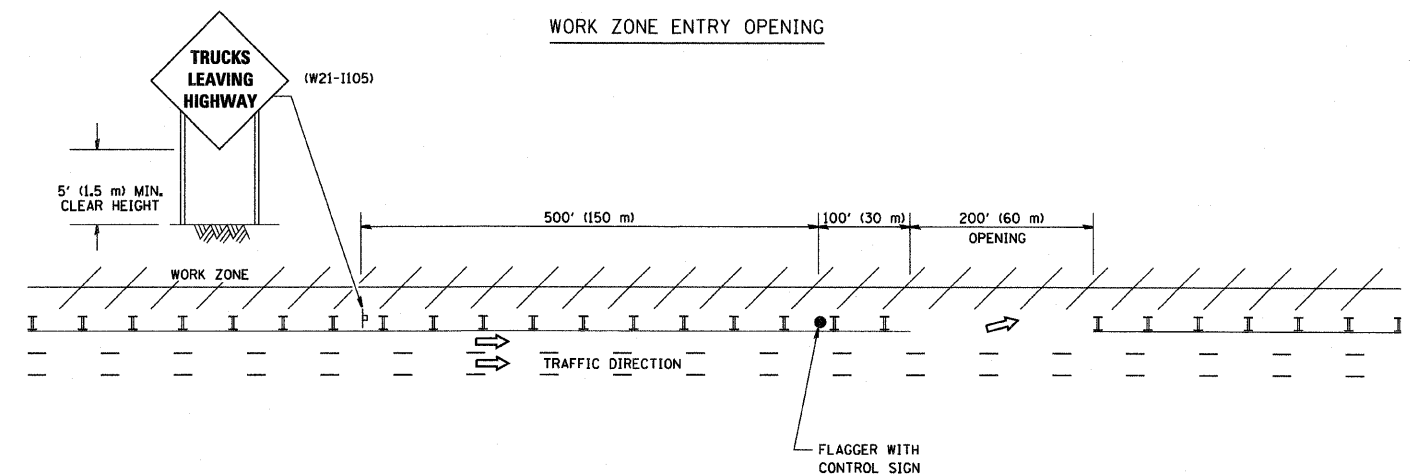
TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A. RTE. 94
			SECTION 1717 I-7
			COUNTY COOK
			TOTAL SHEETS 47
			SHEET NO. 45
			CONTRACT NO. 62979
			FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. The Arrowboard, the Flagger Ahead trailer mounted sign, and the Trucks Leaving Highway sign shall be removed or turned away from traffic and the exit and entry openings shall be closed when the flagging operation ceases.
2. Work Zone Exit Openings should be a minimum of one half mile apart.
3. Exiting the work zone at any place other than at a Work Zone Exit Opening will be prohibited.
4. All vehicles shall enter the work zone at entry openings, using their turn signals to warn motorists

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

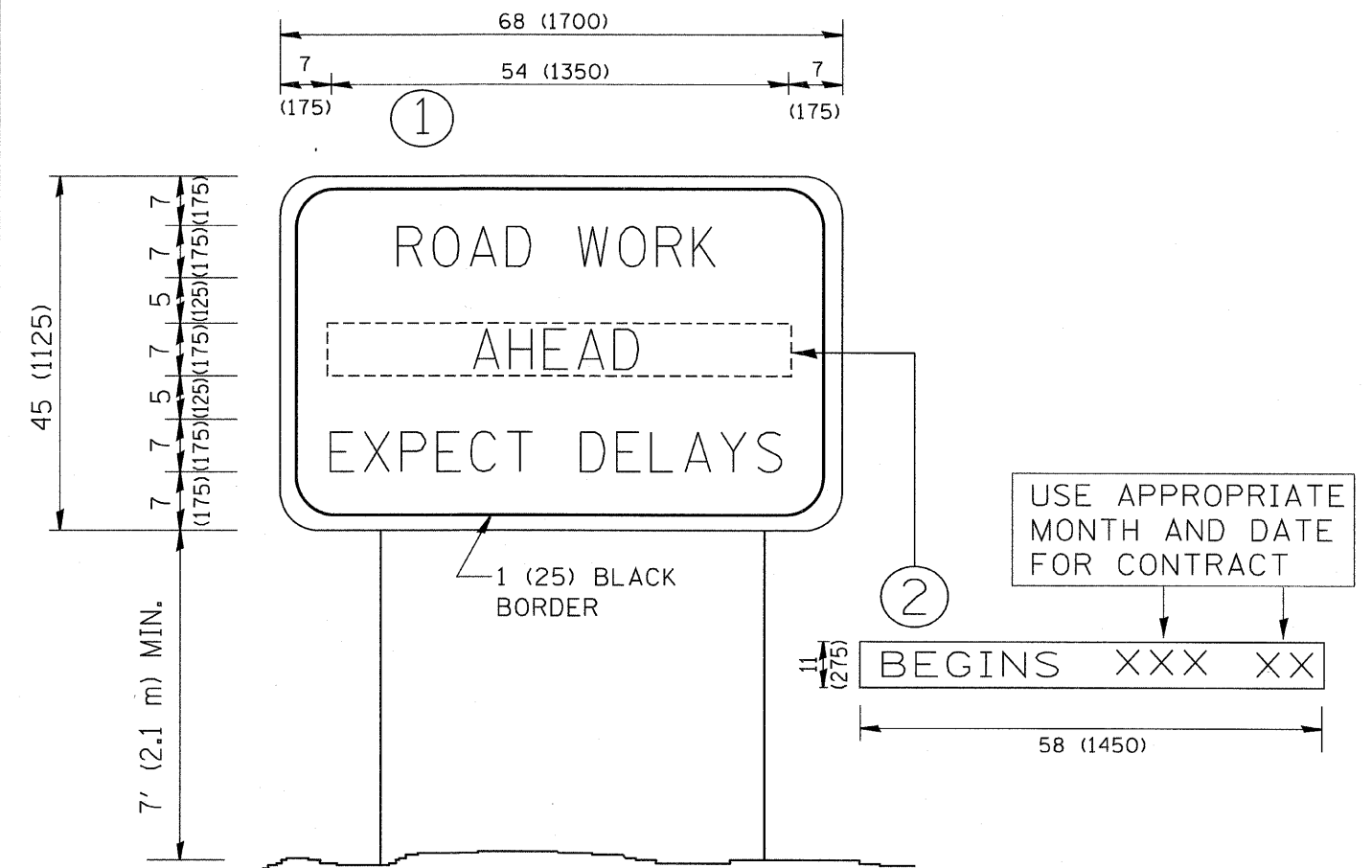
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	PLOT DATE = 1/4/2008	DATE -	REVISED - S.P.B. 01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS  
AT WORK ZONE OPENINGS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	46
TC-18			CONTRACT NO. 62979	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dstatd\22x34\tc22.dgn	USER NAME = geglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD  
INFORMATION SIGN

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1717 I-7	COOK	47	47
TC-22		CONTRACT NO. 62979		
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				