

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS PLANS FOR PROPOSED FEDERAL AID HIGHWAY LOCAL AGENCY PAVEMENT PRESERVATION (LAPP) FAU ROUTE 2733 (9th AVENUE) HARRISON STREET TO MADISON STREET SECTION NO. 08-00128-00-RS PROJECT M-9003 (177) VILLAGE OF MAYWOOD COOK COUNTY JOB NO. C-91-210-09

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	1
ILLINOIS PROJECT M-9003 (177)				
CONTRACT NO. 63172				



LOCATION OF SECTION INDICATED THUS:

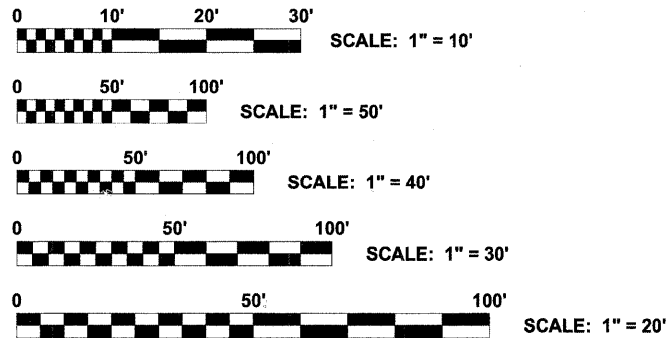
TRAFFIC DATA

2030 ADT = 7,500
POSTED SPEED LIMIT: 25 MPH
DESIGN SPEED: 30 MPH

DESIGN DESIGNATION

COLLECTOR

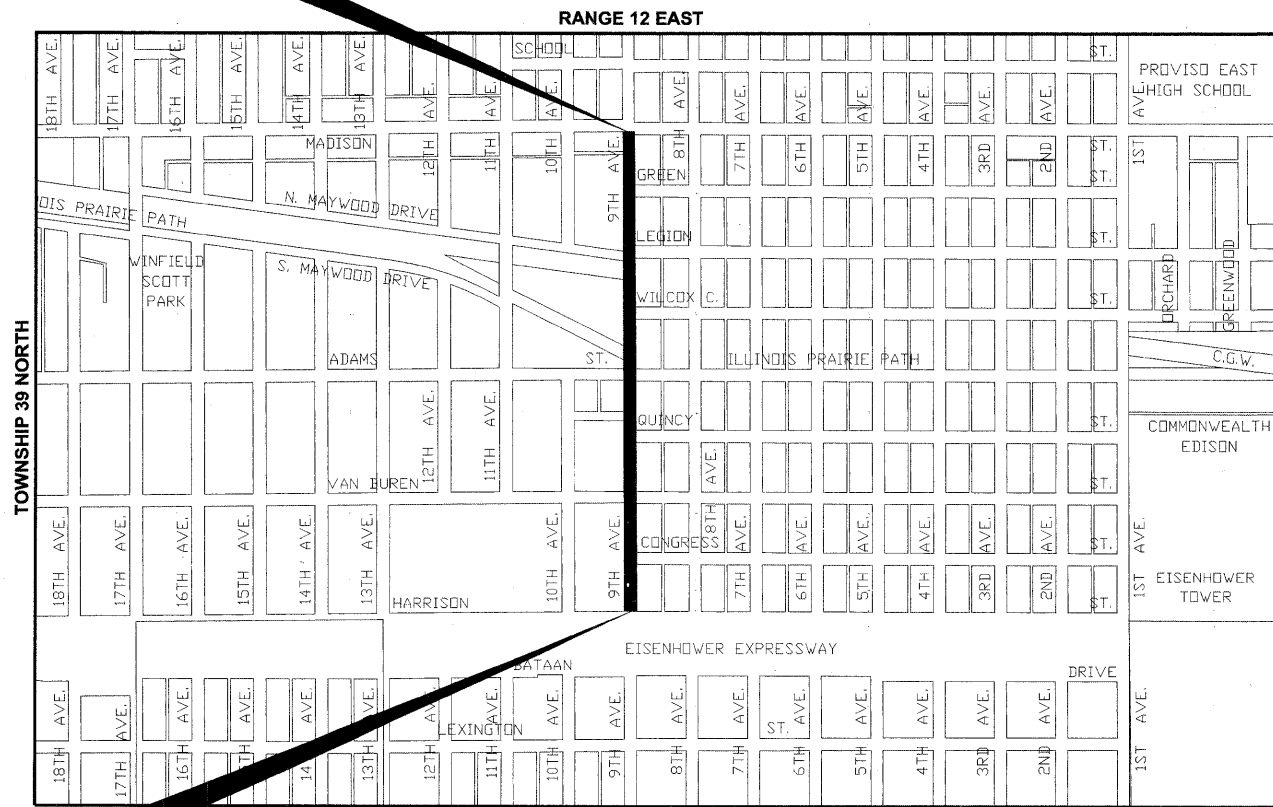
PROJECT LOCATED IN THE VILLAGE OF MAYWOOD



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

PROJECT ENDS STATION 26+70
9th AVENUE

LOCATION MAP



PROJECT BEGINS STATION 0+86
9th AVENUE

- AREA OF IMPROVEMENT
NOT TO SCALE

GROSS AND NET LENGTH OF IMPROVEMENT = 2,584 FT. = 0.489 MI.



STATE OF ILLINOIS
ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

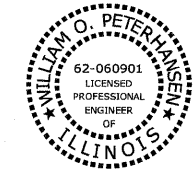
APPROVED MARCH 31 2009
Hanserson M. O'Keefe
VILLAGE OF MAYWOOD, PRESIDENT

PASSED APRIL 06 2009
Chris H. Henschler
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASED FOR BID
BASED ON LIMITED
REVIEW APRIL 06, 2009
Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS,
REGION 1 ENGINEER

(PRINTED BY AUTHORITY OF THE STATE OF ILLINOIS)

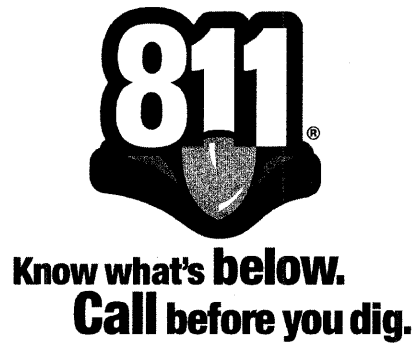
William O. Peterhansin



DATE SIGNED: 03-23-09

LICENSE EXPIRES: 11-30-09

EDWIN HANCOCK ENGINEERING COMPANY
9933 ROOSEVELT ROAD PHONE : (708) 865-0300
WESTCHESTER, ILLINOIS 60154



CONTRACT NO. 63172

INDEX OF SHEETS



























SHEET NO.	DESCRIPTION
1	COVER SHEET, LOCATION MAP
2	INDEX OF SHEETS, LEGEND OF SYMBOLS, AND I.D.O.T. STANDARD DRAWINGS
3	GENERAL NOTES
4	SUMMARY OF QUANTITIES
5	EXISTING AND PROPOSED TYPICAL CROSS SECTIONS
6	DETAILS & NOTES
7-8	PAVING PLANS
9	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD 08)
10	CURB AND GUTTER REMOVAL AND REPLACEMENT (BD 24)
11	BUTT JOINT AND HMA TAPER DETAILS (BD 32)
12	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC 10)
13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC 13)
14	TRAFFIC CONTROL AND PROTECTIONS AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC 14)

I.D.O.T. STANDARD DRAWINGS

STANDARD NO.	TITLE OR DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C&D PATCHES
604001-03	FRAMES AND LIDS, TYPE 1
701501-05	URBAN LANE CLOSURE, 2-LANE, 2-WAY, UNDIVIDED
701606-06	URBAN LANE CLOSURE, MULTILANE, 2-WAY, WITH MOUNTABLE MEDIAN
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

LEGEND OF SYMBOLS

(TO BE USED IN CONJUNCTION WITH I.D.O.T. STANDARD 000001-05)

SYMBOL	DESCRIPTION
	EXISTING HOT-MIX ASPHALT AREA
	EXISTING CONCRETE AREA
	EXISTING GRASS AREA
	PROPOSED HOT-MIX ASPHALT BUTT JOINT
	EXISTING CONCRETE SIDEWALK OR DRIVEWAY REMOVAL
	PROPOSED CONCRETE AREA, 5" SIDEWALK, 7" DRIVEWAY
	PROPOSED HOT-MIX ASPHALT PAVING AREA
	PROPOSED CLASS C PATCHES
	PROPOSED DETECTABLE WARNINGS
A	STRUCTURE TO BE ADJUSTED
A*	STRUCTURE TO BE ADJUSTED (SPECIAL)
1C	NEW FRAME AND LID, TYPE 1, CLOSED LID
1P	NEW FRAME AND LID, TYPE 1, OPEN LID
RC	STRUCTURE TO BE RECONSTRUCTED
F	FILLING MANHOLES
	EXISTING DOMESTIC WATER SERVICE BOX
	EXISTING FIRE HYDRANT
	EXISTING WATER VALVE BOX
	EXISTING WATER MAIN VALVE VAULT
	EXISTING STORM SEWER INLET
	EXISTING STORM SEWER CATCH BASIN
	EXISTING SEWER MANHOLE
	EXISTING STREET LIGHT POLE
	EXISTING POWER POLE
	EXISTING TRAFFIC SIGNAL POLE
	EXISTING TRAFFIC SIGNAL MAST ARM
	EXISTING HANDHOLE
	DOUBLE HANDHOLE
	EXISTING TRAFFIC SIGNAL OR STREET LIGHT CONTROLLER
	EXISTING TRAFFIC SIGNAL MANHOLE
	EXISTING CURB AND GUTTER
	PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

Drawing file: W:\Projects\56509236 - 8th Avenue LAPP\INDEX.dwg Mar 16, 2009 - 3:49pm

HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911

1023 Riverside Blvd
 Westchester, Illinois 60594-2700
 Phone: 708.965.4300
 Fax: 708.965.1332

USER NAME	DESIGNED	WOP	REVISED
	DRAWN	MK/LEV	
PLOT SCALE	CHECKED	JCG	
PLOT DATE	DATE	03-16-09	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, I.D.O.T. STANDARD
 DRAWINGS, AND LEGEND OF SYMBOLS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	2
CONTRACT NO.			63172	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 565-08-23601

GENERAL NOTES

STANDARDS

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION AS SHOWN ON THE INDEX OF SHEETS IN THE PLANS. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2007, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2009, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS," "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 FIFTH EDITION, AND THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

UNDERGROUND UTILITIES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 811 FOR FIELD LOCATIONS OF BURIED ELECTRICAL, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).

THE LOCATIONS OF THE UNDERGROUND UTILITIES IF SHOWN ON THE PLANS HAVE BEEN OBTAINED BY FIELD SURVEYS AND SEARCHES OF AVAILABLE RECORDS. IT IS BELIEVED THAT DATA IS ESSENTIALLY CORRECT, BUT THE VILLAGE OF MAYWOOD, THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND/OR OTHER OFFICES AND AGENCIES ASSOCIATED WITH THE DEVELOPMENT OF THESE PLANS DO NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE EXACT LOCATION OF EACH FACILITY WITH THE UTILITY COMPANY, AND SHALL TAKE DUE CARE IN ALL PHASES OF THE CONSTRUCTION TO PROTECT ANY SUCH FACILITIES WHICH MAY BE AFFECTED BY THE WORK. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF MAYWOOD.

FRAMES AND LIDS

THE TYPE OF FRAMES AND LIDS REQUIRED FOR ALL MANHOLES AND VALVE VAULTS LISTED IN THE SUMMARY OF QUANTITIES MAY BE FOUND ON THE PLANS AT THEIR RESPECTIVE LOCATIONS. WHERE LIDS ARE CALLED FOR ON THE PLANS, THEY SHALL BE IN ACCORDANCE WITH ARTICLE 604.04 OF THE STANDARD SPECIFICATIONS AND THE TERM LID IS USED IN LIEU OF GRATE. ALL LIDS ON SANITARY MANHOLES, COMBINED SEWER MANHOLES, AND VALVE VAULTS SHALL BE OF THE SELF SEALING TYPE.

ON ALL IMPROVEMENTS, THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS WHICH ARE TO BE ABANDONED DUE TO CONSTRUCTION OF THIS IMPROVEMENT ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF MAYWOOD AND BE SALVAGED. THESE ITEMS SHALL BE DELIVERED TO THE VILLAGE OF MAYWOOD PUBLIC WORKS DEPARTMENT.

MANHOLE OR VALVE COVERS

THE WORD "WATER", "SANITARY", OR "STORM" SHALL BE CAST INTO THE LID OF EACH RESPECTIVE MANHOLE OR VALVE VAULT.

MAINTENANCE OF SEWER FLOWS

THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS AS TO MAINTAIN AT ALL TIMES FLOW THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE SHALL ALSO PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT IF NECESSARY AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES WHICH ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. EXISTING STRUCTURES ARE TO BE INSPECTED BEFORE CONSTRUCTION STARTS - ANY ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS SHALL BE REMOVED BY THE CONTRACTOR AT HIS EXPENSE.

MAINTENANCE OF EXISTING DRAINAGE STRUCTURES

WHEN DURING THE CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF ANY GUTTERS AND DRAINAGE STRUCTURE SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE FACILITIES SHALL BE CLEAN AND FREE OF ALL OBSTRUCTIONS DUE TO CONSTRUCTION OPERATIONS. THE COST OF THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.

SAW CUTTING

THE CONTRACTOR SHALL SAW CUT ASPHALT PAVEMENT AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING PAVEMENT TO BE REMOVED BY APPROVED MEANS OR AN APPROVED CONCRETE SAW TO A DEPTH AS DIRECTED BY THE ENGINEER. SUITABLE GUIDELINES OR DEVICES SHALL BE USED TO ASSURE CUTTING A NEAT, STRAIGHT LINE AS SHOWN ON THE PLANS. CARE SHALL BE TAKEN BY THE CONTRACTOR AS NOT TO DAMAGE THE REMAINING PAVEMENT DIRECTLY ADJACENT TO THE PAVEMENT TO BE REMOVED. ANY DAMAGE TO THE EXISTING PAVEMENT RESULTING FROM PAVEMENT REMOVAL OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE COST OF SAW CUTTING DESCRIBED ABOVE SHALL BE INCLUDED IN THE ITEM BEING REMOVED. SAW CUTS FOR PAVEMENT PATCHING WILL BE PAID FOR IN THE CONTRACT.

FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

THIS ITEM ONLY PERTAINS TO STRUCTURES LOCATED IN THE CONCRETE OR HOT-MIX ASPHALT ROADWAY PAVEMENT AREAS THAT WILL REQUIRE CONCRETE OR HOT-MIX SURFACE REMOVAL. THE ENGINEER WILL MARK IN THE FIELD ALL STRUCTURES TO BE DONE UNDER THIS ITEM. SEE DETAIL SHEET FOR "FRAMES AND LIDS ADJUSTMENT WITH MILLING."

PRIME COAT

PRIME COAT MUST BE INSTALLED NO EARLIER THAN TWENTY-FOUR (24) HOURS PRIOR TO PLACEMENT OF HOT-MIX ASPHALT.

FIELD OFFICE

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT OR THE VILLAGE.

BARRICADES

THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED, ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.

BUTT JOINTS

BUTT JOINT WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

MILLED PAVEMENT OPEN TO TRAFFIC

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

PAVING OPERATIONS

CONTRACTOR MUST PAVE 9TH AVENUE IN A MAXIMUM OF 2 PASSES. IF THE CONTRACTOR IS NOT ABLE TO COMPLETE ALL THE PAVING IN ONE (1) DAY, THE LONGITUDINAL JOINT SHALL BE SEALED.

PAVEMENT PATCHING

LOCATIONS OF CLASS C PATCHES ON PLANS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN FIELD BY ENGINEER.

USER NAME	DESIGNED	WOP	REVISED
	DRAWN	MK/LEV	
PLOT SCALE	CHECKED	JCG	
PLOT DATE	DATE	03-16-09	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	3
CONTRACT NO.			63172	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 565-08-23601

SUMMARY OF QUANTITIES

CODE	PAY ITEM	UNIT	1000	1000
			TOTAL QUANTITY	80% FEDERAL 20% LOCAL
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQYD	500	500
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	10	10
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	10	10
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	10	10
25200100	SODDING	SQYD	500	500
25200200	SUPPLEMENTAL WATERING	UNIT	20	20
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	60	60
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL	2,500	2,500
40600300	AGGREGATE (PRIME COAT)	TON	60	60
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	20	20
40600895	CONSTRUCTING TEST STRIP	EACH	1	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQYD	225	225
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50	TON	1,350	1,350
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	20	20
42101300	PROTECTIVE COAT	SQYD	600	600
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQYD	250	250
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQFT	1,100	1,100
~ 42400800	DETECTABLE WARNINGS	SQFT	128	128
~ 44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQYD	11,850	11,850
~ 44000200	DRIVEWAY PAVEMENT REMOVAL	SQYD	450	450
~ 44000600	SIDEWALK REMOVAL	SQFT	1,350	1,350
~ 44001700	COMBINATION CONC CURB AND GUTTER REMOVAL AND REPL	FOOT	1,000	1,000
~ 44201325	CLASS C PATCHES, TYPE I, 8 INCH	SQYD	45	45
~ 44201329	CLASS C PATCHES, TYPE II, 8 INCH	SQYD	135	135
~ 44201333	CLASS C PATCHES, TYPE III, 8 INCH	SQYD	45	45
~ 44201335	CLASS C PATCHES, TYPE IV, 8 INCH	SQYD	175	175
~ 60228110	MANHOLES, SANITARY, 4' DIA., TY 1 FRAME, CLOSED LID	EACH	2	2
~ 60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	3	3
~ 60257900	MANHOLES TO BE RECONSTRUCTED	EACH	20	20
~ 60266610	VALVE BOXES TO BE ADJUSTED (SPECIAL)	EACH	3	3
~ 60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	3	3
~ 60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	32	32
~ 60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	3	3
~ 60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	32	32
~ 60500105	FILLING MANHOLES	EACH	1	1
67100100	MOBILIZATION	L SUM	1	1
~ 70101700	TRAFFIC CONTROL AND PROTECTION	L SUM	1	1
~ 70106800	CHANGEABLE MESSAGE SIGN	CAL-MO	1	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	750	750
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQFT	100	100
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	5,000	5,000
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	450	450
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	475	475
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	175	175
~ X0517100	STORM SEWERS, DUCTILE IRON PIPE 8"	FOOT	12	12
~ X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	575	575
~ XX104100	CONNECTION TO EXISTING MANHOLE	EACH	3	3
~ Z0004900	BITUMINOUS MIXTURE FOR PATCHING POTHOLES (HOT MIX)	TON	20	20
~ Δ Z0076600	TRAINEES	hour	500	500

Δ Y070
 * DENOTES SPECIALTY ITEM
 ~ DENOTES THAT A SPECIAL PROVISION HAS BEEN PROVIDED

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HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911

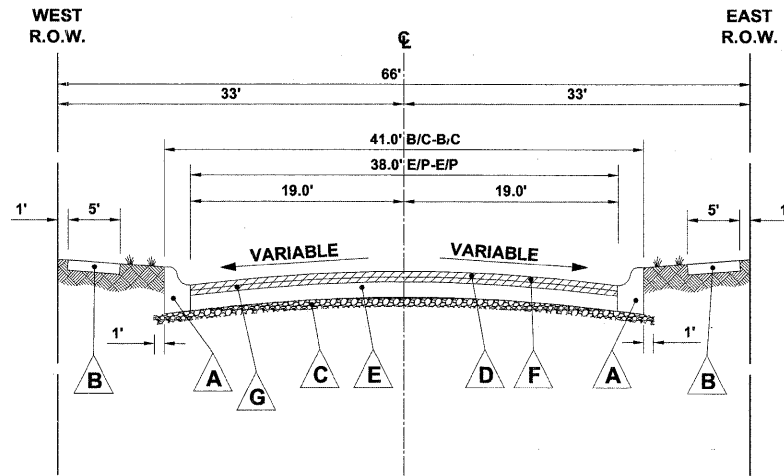
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	DRAWN	MK/LEV	
PLOT SCALE	CHECKED	JCG	
PLOT DATE	DATE	3-16-09	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	4
CONTRACT NO. 63172			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



**EXISTING TYPICAL CROSS SECTION
9th AVENUE**

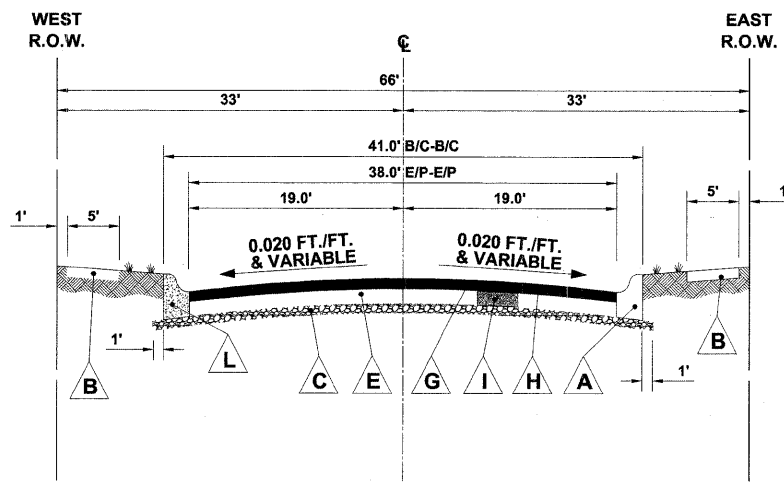
LEGEND OF SYMBOLS

SYMBOL	DESCRIPTION
A	EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
C	EXISTING SUB-BASE GRANULAR MATERIAL, 4" AND VARIABLE
D	EXISTING HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
E	EXISTING 5" HOT-MIX ASPHALT BASE COURSE
F	PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
G	EXISTING HOT-MIX ASPHALT BINDER COURSE, 1 1/2"

HOT-MIX ASPHALT (HMA) MIXTURE REQUIREMENTS

ITEM	A C TYPE	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50, (IL - 9.5 mm)	PG 64 -22	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	SBS/SBR PG 76 -28/ -22	4% @ 50 GYR.
INCIDENTAL HOT-MIX ASPHALT SURFACING, MIX "C", N50	PG 64 -22	4% @ 50 GYR.
BITUMINOUS MIXTURE FOR PATCHING POTHOLES (HOT MIX) MIX "C" N50	SBS/SBR PG 76 -28/ -22	4% @ 50 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE COURSE IS 112 LBS/SQYD/IN.



**PROPOSED TYPICAL CROSS SECTION
9th AVENUE**

LEGEND OF SYMBOLS

SYMBOL	DESCRIPTION
A	EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 5"
C	EXISTING SUB-BASE GRANULAR MATERIAL, 4" AND VARIABLE
E	EXISTING HOT-MIX ASPHALT BASE COURSE
G	PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, MINIMUM 3/4"
H	PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50, 1-3/4"
I	PROPOSED CLASS C PATCHES, 8"
L	PROPOSED INTERMITTENT COMBINATION CONCRETE CURB & GUTTER REMOVAL & REPLACEMENT

Drawing file: W:\Projects\06500236 - 9th Avenue LRP\YIPSEC.dwg Mar 17, 2009 - 11:26am

HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911

USER NAME	DESIGNED	WOP	REVISED
	DRAWN	MK/LEV	
PLOT SCALE NONE	CHECKED	JCG	
PLOT DATE	DATE	03-16-09	

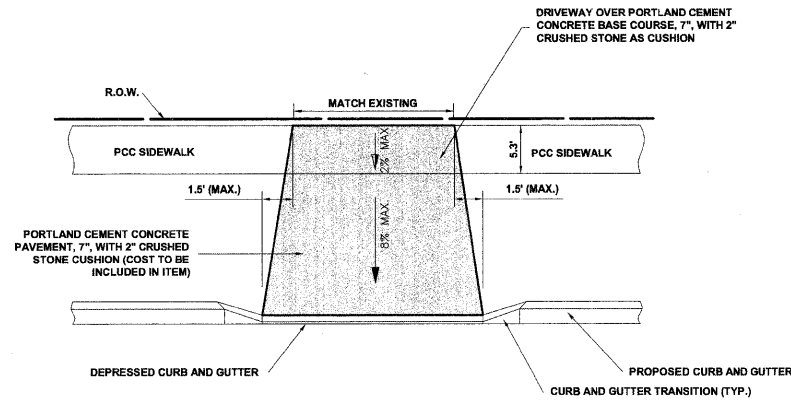
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED
TYPICAL CROSS SECTIONS**

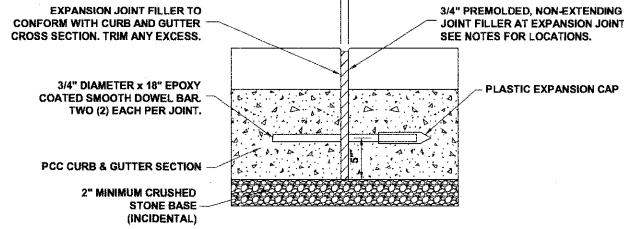
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63172	

E.H.E. PROJECT NO. 565-08-23601

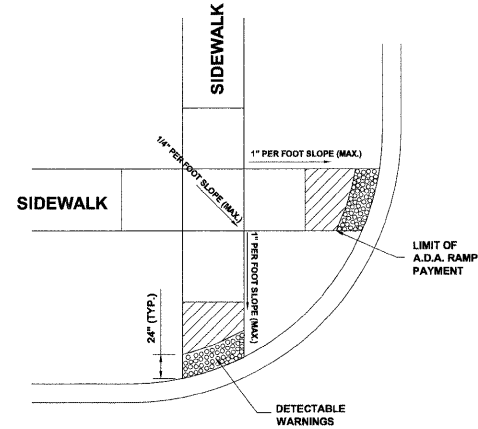


DRIVEWAY DETAIL

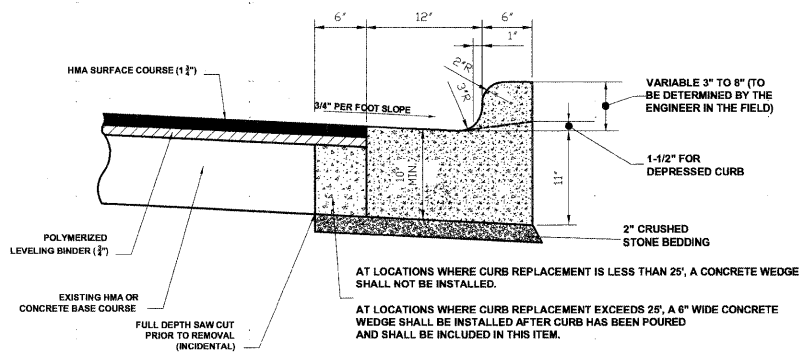


NOTE:
 1. EXPANSION JOINTS ARE TO BE CONSTRUCTED AT ALL P.C.'S & P.T.'S OF INTERSECTION RETURNS AND ALL OTHER SHORT RADIUS SECTIONS, CONSTRUCTION JOINTS, EVERY 65' ON TANGENT SECTIONS, AND AS DIRECTED BY THE ENGINEER.

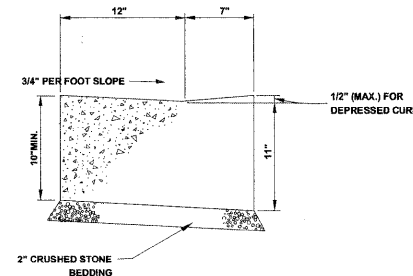
TYPICAL CURB AND GUTTER EXPANSION JOINT



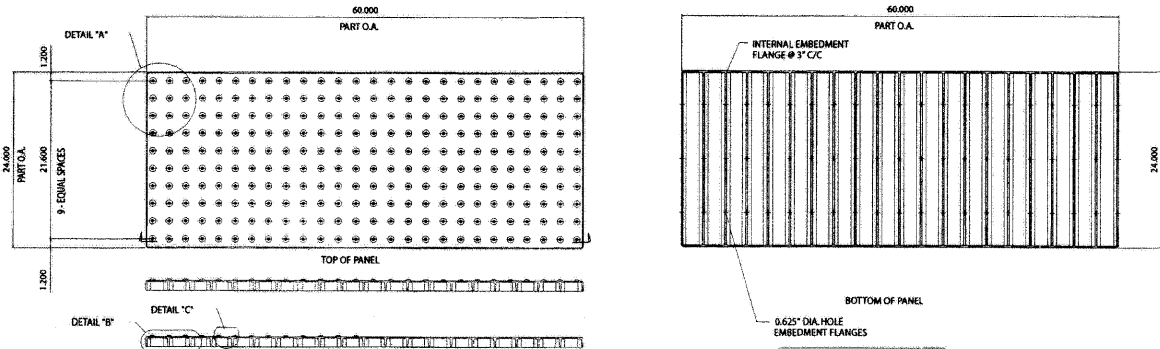
A.D.A. RAMP DETAIL



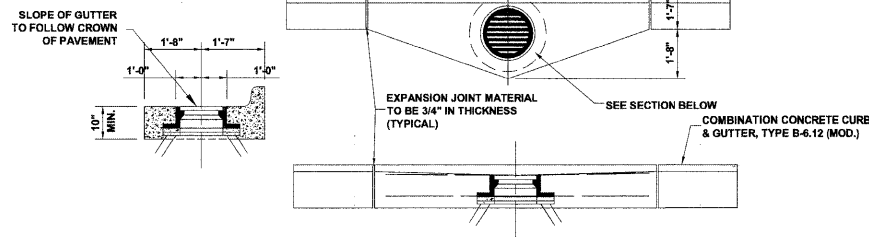
COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12 (MODIFIED)



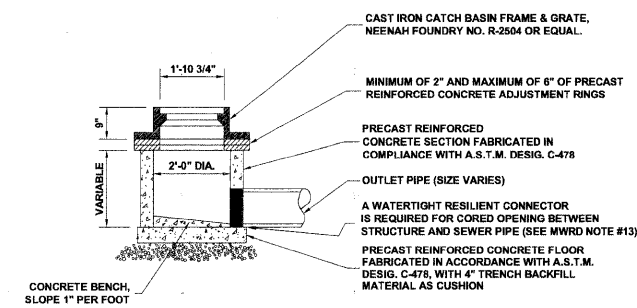
CURB AND GUTTER AT A.D.A. RAMPS



CAST-IN-PLACE COMPOSITE PAVER TILE FOR DETECTABLE WARNING



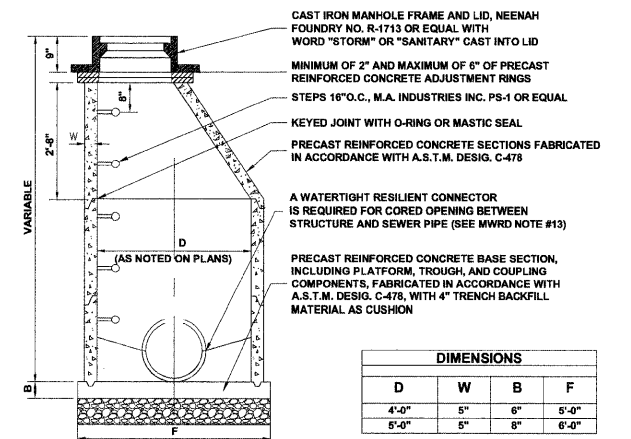
GUTTER DETAIL AT DRAINAGE STRUCTURE



INLET, TYPE "A"

M.W.R.D.G.C. GENERAL NOTES

- THE MWRD SEWER SYSTEMS SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF WORK AT (708) 588-4055.
- ELEVATION DATUM IS U.S.G.S.
- ALL FLOOR DRAINS SHALL DISCHARGE TO THE SANITARY SEWER SYSTEM.
- ALL DOWNSPOUTS AND FOOTING DRAINS SHALL DISCHARGE INTO THE STORM SEWER SYSTEM.
- ALL PVC SEWER PIPE SHALL BE SDR 26. ALL PVC SEWER PIPE JOINTS SHALL CONFORM TO ASTM D-3139 FOR PVC PIPE 12" IN DIAMETER OR LESS. ALL PVC SEWER PIPE JOINTS SHALL CONFORM TO ASTM D-3212 FOR PVC PIPE 15" IN DIAMETER OR MORE. ALL PVC SEWER PIPE 12" IN DIAMETER OR LESS SHALL CONFORM TO ASTM D-2241 (WATER QUALITY PIPE). ALL PVC SEWER PIPE 15" IN DIAMETER OR MORE SHALL CONFORM TO ASTM D-3034.
- ALL D.I.P. STORM, COMBINED AND SANITARY SEWER PIPE JOINTS SHALL CONFORM TO ANSI A-21.11. ALL D.I.P. SEWER PIPE SHALL CONFORM TO ASTM A-21.51. ALL D.I.P. SEWER PIPE SHALL BE CLASS 52.
- ALL SANITARY, COMBINED, AND STORM SEWER CONSTRUCTION IN COMBINED SEWER AREAS, REQUIRES STONE BEDDING 1/4" TO 1" IN SIZE, WITH A MINIMUM THICKNESS EQUAL TO 1/4 THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR INCHES (4") NOR MORE THAN EIGHT INCHES (8"). MATERIAL SHALL BE CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 12" ABOVE THE TOP OF THE PIPE.
- "BAND SEAL" OR SIMILAR FLEXIBLE-TYPE COUPLINGS SHALL BE USED IN THE CONNECTION OF SEWER PIPE OF DISSIMILAR MATERIALS.
- WHEN CONNECTING TO AN EXISTING SEWER MAIN BY MEANS OTHER THAN AN EXISTING WYE, TEE, OR AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:
 - CIRCULAR SAW-CUT OF SEWER MAIN BY MECHANICAL CORING MACHINE, AND PROPER INSTALLATION OF HUB-WYE SADDLE OR HUB-TEE SADDLE, IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
 - REMOVE AN ENTIRE SECTION OF PIPE (BREAKING ONLY THE TOP OF ONE BELL) AND REPLACE WITH A WYE OR TEE BRANCH SECTION. AFTER THE WYE OR TEE BRANCH IS INSERTED, CONCRETE SHALL BE PLACED OVER THE BROKEN AREA TO A MINIMUM THICKNESS OF 4" AND TO A DIMENSION OF 8" IN ALL DIRECTIONS.
 - USING PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING. USE "BAND SEAL" OR SIMILAR COUPLINGS TO HOLD FIRMLY IN PLACE. FOLLOW MANUFACTURER'S RECOMMENDATIONS FOR THE INSTALLATION.
- WHEREVER A SEWER CROSSES UNDER A WATER MAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATER MAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY/COMBINED SEWERS AND WATER MAINS SHALL BE MAINTAINED UNLESS: THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18" VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATER MAIN LOCATED AT THE OPPOSITE SIDE ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18" VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED ABOVE CANNOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATER MAIN, THE SEWER SHALL BE CONSTRUCTED TO WATER MAIN STANDARDS.
- ALL SANITARY MANHOLES, (AND STORM MANHOLES IN COMBINED SEWER AREAS), SHALL HAVE A MINIMUM INSIDE DIAMETER OF 48 INCHES, AND SHALL BE PRE-CAST REINFORCED CONCRETE.
- ALL ABANDONED SEWERS SHALL BE PLUGGED AT BOTH ENDS WITH A MINIMUM OF TWO (2) FEET LONG, NON-SHRINK CONCRETE/MORTAR PLUG.
- ALL INLET AND OUTLET PIPES OF SANITARY SEWER MANHOLES AND OTHER UNDERGROUND STRUCTURES (AND IN COMBINED SEWER AREAS, ALSO ALL COMBINED/STORM SEWER MANHOLES, CATCH BASINS, INLETS, AND UNDERGROUND DETENTION STORAGE STRUCTURES) SHALL BE JOINED WITH WATERTIGHT FLEXIBLE RUBBER CONNECTORS CONFORMING TO A.S.T.M. C-443 & C-923 WITH STAINLESS STEEL BANDS.
- THE MAXIMUM ALLOWABLE INFILTRATION OR EXFILTRATION IS 100 GAL/DAY/MILE/INCH DIA OF THE SEWER PIPE.

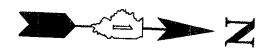
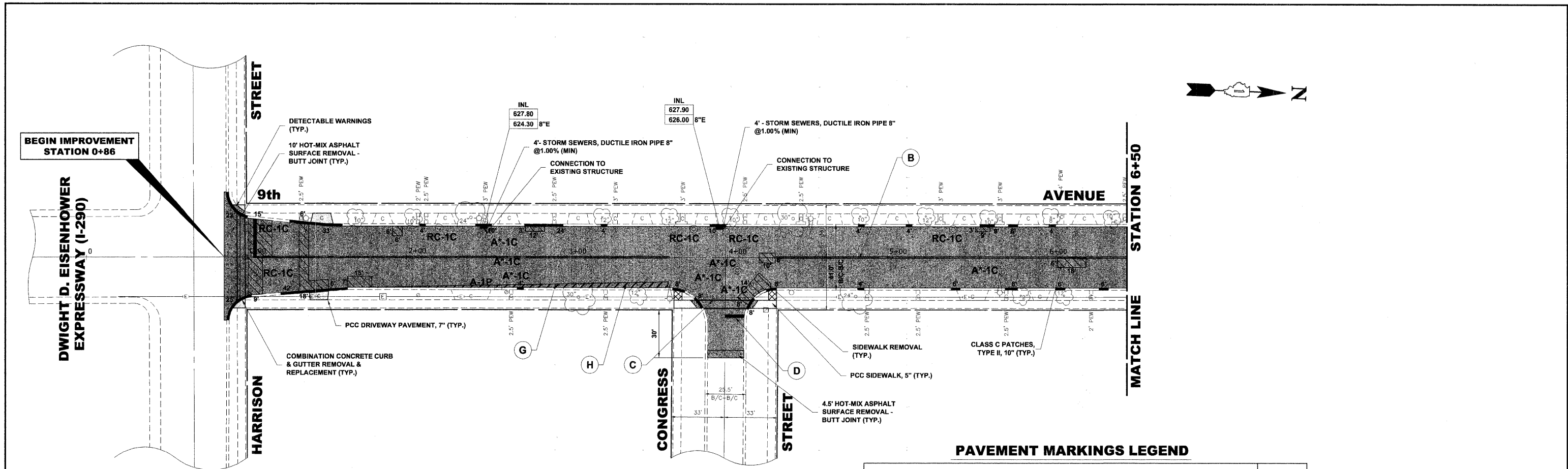


STANDARD SEWER MANHOLE

DIMENSIONS			
D	W	B	F
4'-0"	5'	6'	5'-0"
5'-0"	5'	8'	6'-0"

USER NAME	DESIGNED	WOP	REVISED
PLOT SCALE NONE	DRAWN	MK/LEV	
PLOT DATE	CHECKED	JCG	
	DATE	03-16-09	

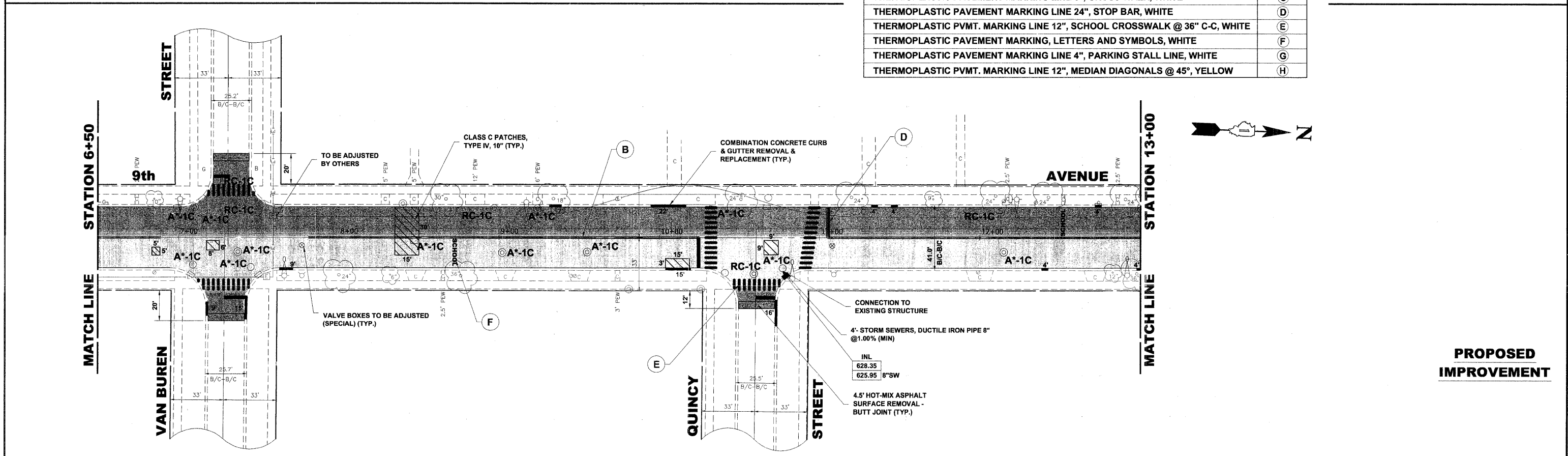
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	6
CONTRACT NO. 63172			ILLINOIS FED. AID PROJECT	



PAVEMENT MARKINGS LEGEND

ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE @ 11" C-C, YELLOW	(B)
THERMOPLASTIC PAVEMENT MARKING LINE 6", CROSS WALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE	(D)
THERMOPLASTIC PVMT. MARKING LINE 12", SCHOOL CROSSWALK @ 36" C-C, WHITE	(E)
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING LINE 4", PARKING STALL LINE, WHITE	(G)
THERMOPLASTIC PVMT. MARKING LINE 12", MEDIAN DIAGONALS @ 45°, YELLOW	(H)

PROPOSED IMPROVEMENT



PROPOSED IMPROVEMENT

Drawing file: W:\Projects\6508236 - 9th Avenue LAPP\9th Avenue\9th Avenue.dwg Mar 16, 2009 - 3:29pm

HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911

1913 Broadway Road
 Wheeling, Illinois 60091-2708
 Phone: 708-945-0300
 Fax: 708-945-1212

USER NAME	DESIGNED	WOP	REVISED
	DRAWN	MK/LEV	
PLOT SCALE	CHECKED	JCG	
PLOT DATE	DATE	03-16-09	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVING PLAN

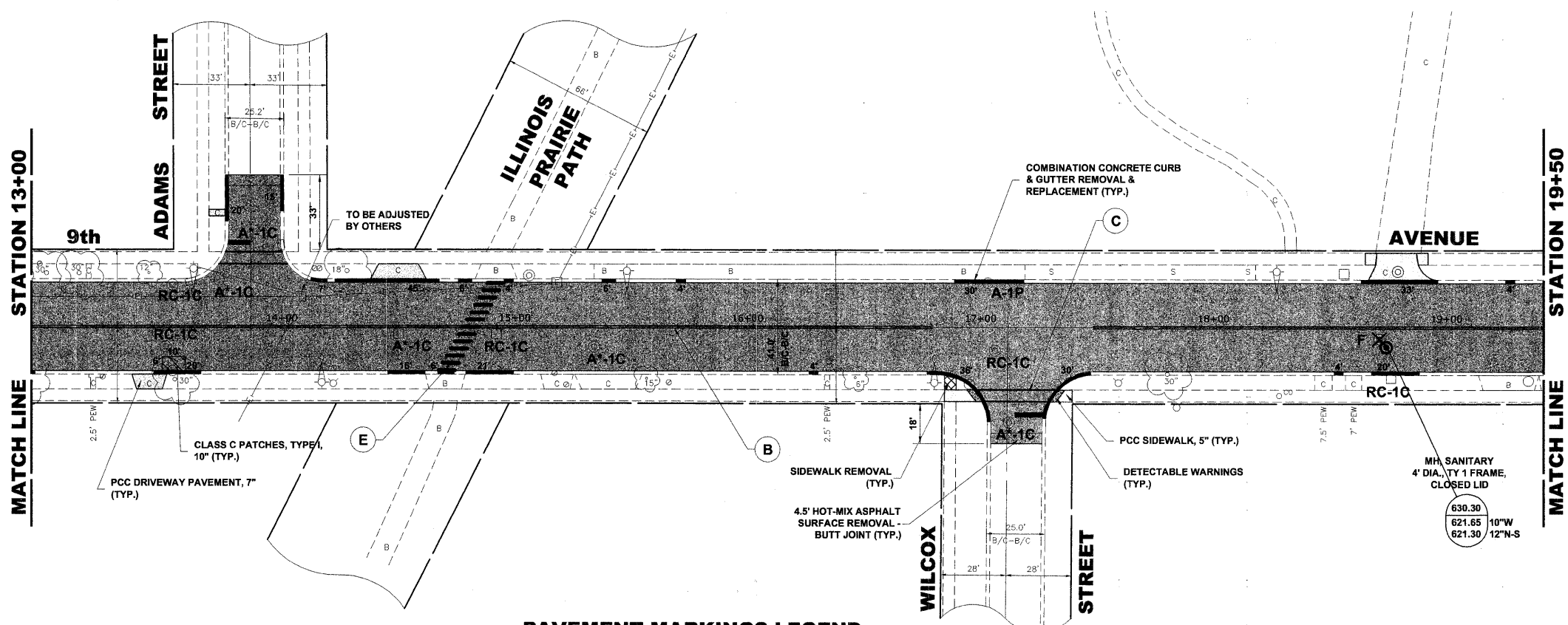
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SHEET NO. OF SHEETS STA. 0+86 TO STA. 13+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	7
CONTRACT NO. 63172				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 565-08-23601

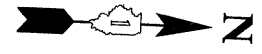
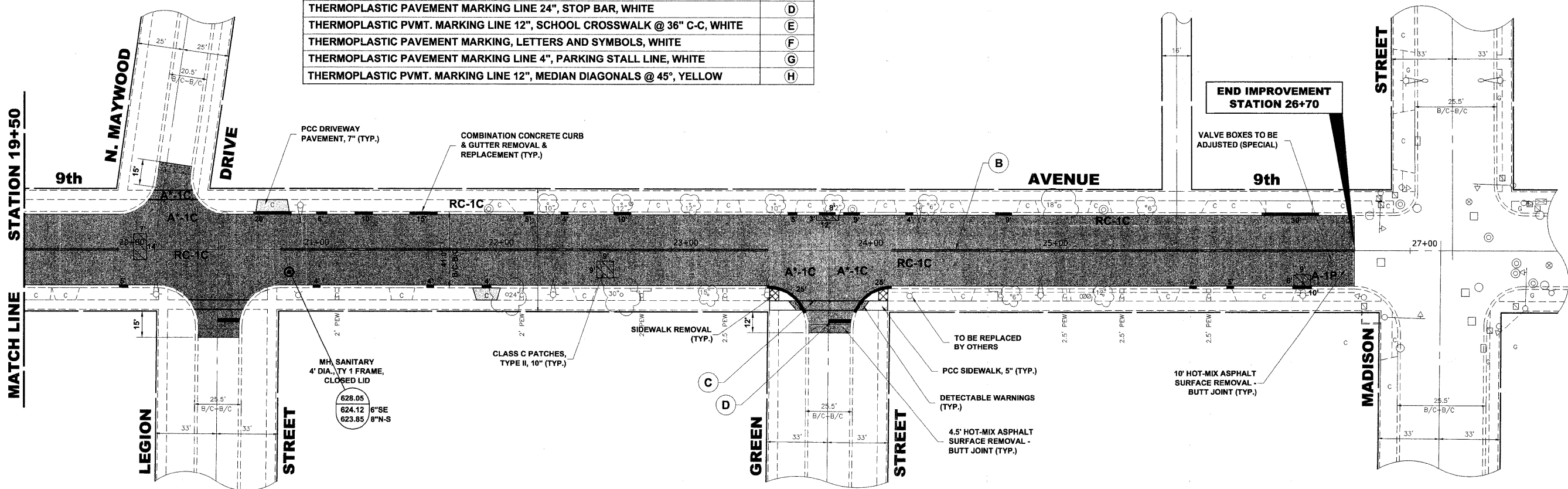
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PAVEMENT MARKINGS LEGEND

ITEM DESCRIPTION	SYMBOL
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THERMOPLASTIC PAVEMENT MARKING LINE 6", CROSS WALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP BAR, WHITE	(D)
THERMOPLASTIC PVMT. MARKING LINE 12", SCHOOL CROSSWALK @ 36" C-C, WHITE	(E)
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING LINE 4", PARKING STALL LINE, WHITE	(G)
THERMOPLASTIC PVMT. MARKING LINE 12", MEDIAN DIAGONALS @ 45°, YELLOW	(H)

PROPOSED IMPROVEMENT



PROPOSED IMPROVEMENT

HANCOCK ENGINEERING
 Civil Engineers
 Municipal Consultants
 Established 1911

USER NAME	DESIGNED	WOP	REVISED
	DRAWN	MK/LEV	
PLOT SCALE 1"=30'	CHECKED	JCG	
PLOT DATE	DATE	03-16-09	

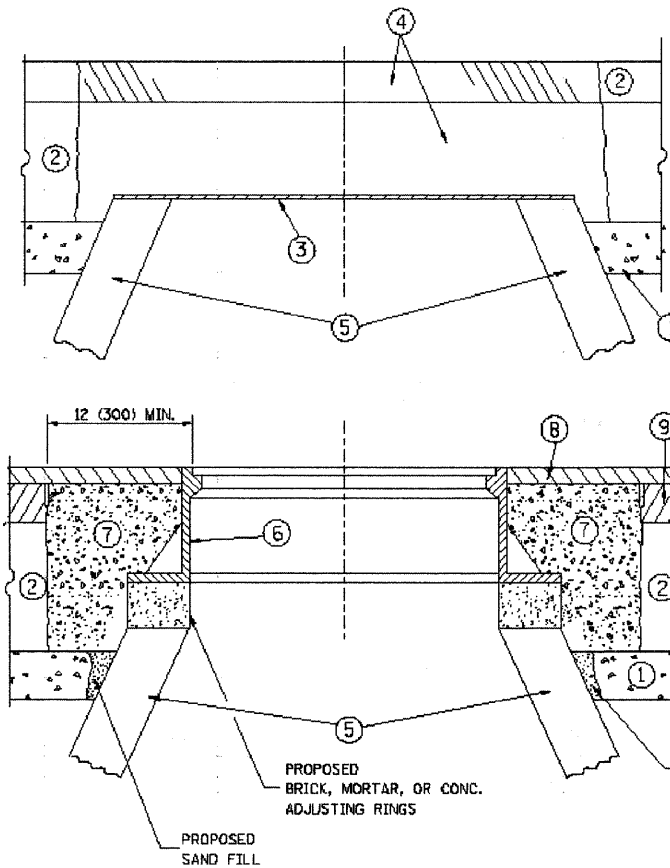
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVING PLAN

SCALE: 1" = 30' SHEET NO. OF SHEETS STA. 13+00 TO STA. 26+70

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	8
CONTRACT NO. 63172				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 565-08-23601



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

Drawing file: W:\Projects\56508236 - 9th Avenue LAPP\MEK\KINS.dwg Mar 16, 2009 - 3:40pm

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		DRAWN -	REVISED - A. ABBAS 03-21-97
		CHECKED -	REVISED - R. WIEDEMAN 05-14-04
		DATE - 10-25-94	REVISED - R. BDRO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	9
BD08-03 (BD-3)			CONTRACT NO. 63172	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 565-08-20601

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = geglendrt

DESIGNED - A. HOUSEH

REVISED - R. SHAH 10-03-96

DRAWN -

REVISED - A. ABBAS 03-21-97

PLOT SCALE = 68.000 1/4 IN.

REVISED - M. GOMEZ 01-22-01

PLOT DATE = 1/4/2008

REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE

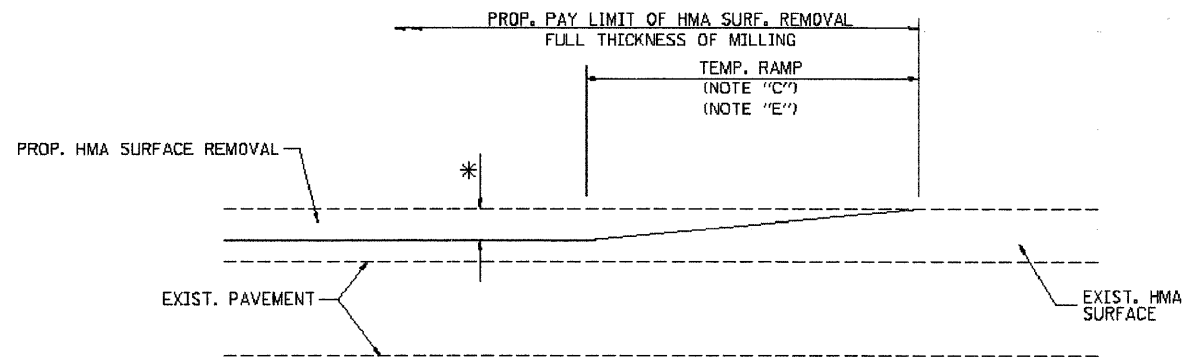
SHEET NO. 1 OF 1 SHEETS

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TO STA.

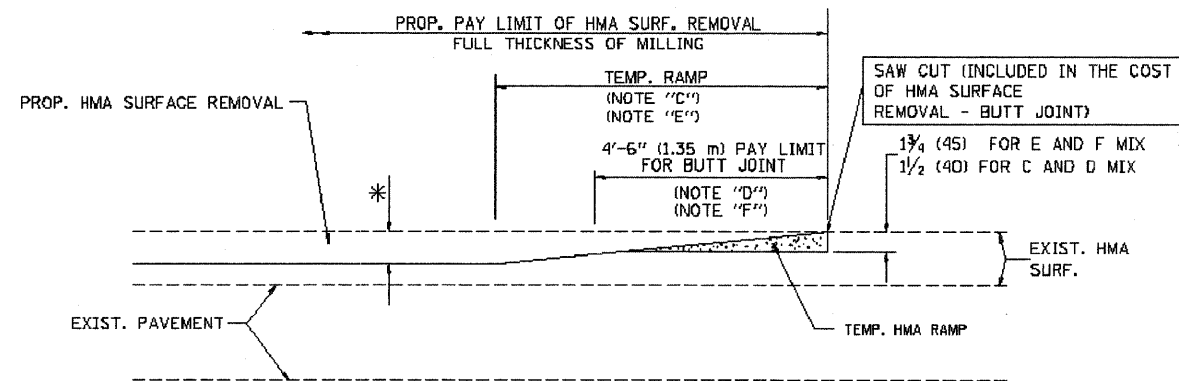
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	10
BD000-06 (BD-24)			CONTRACT NO. 63172	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 565-08-20601



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

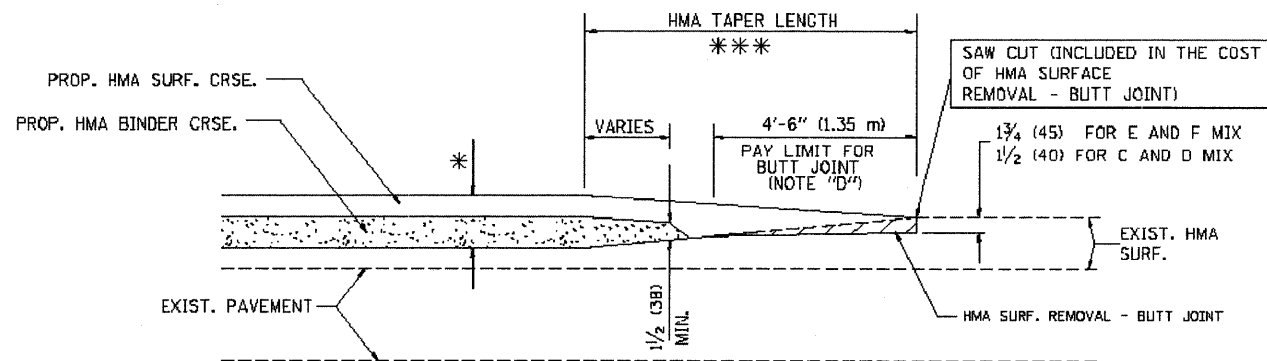
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

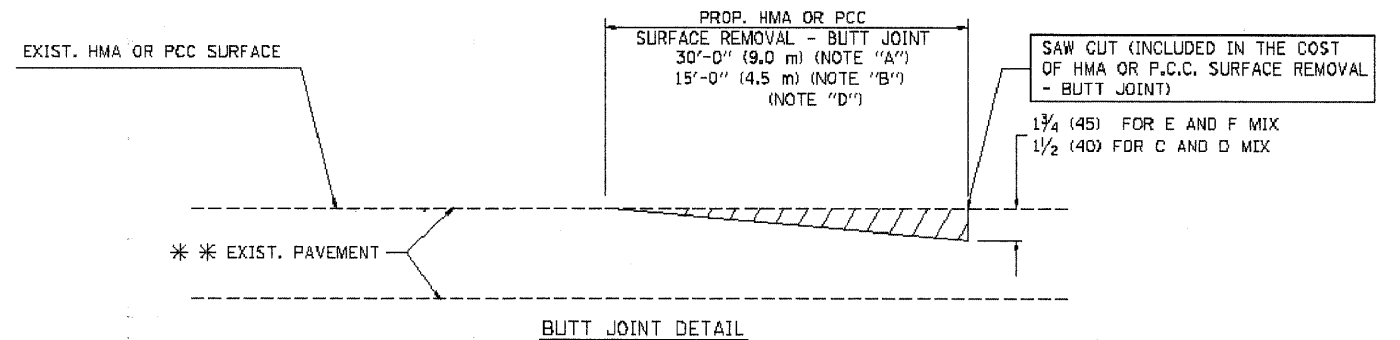
OPTION 2

TYPICAL TEMPORARY RAMP

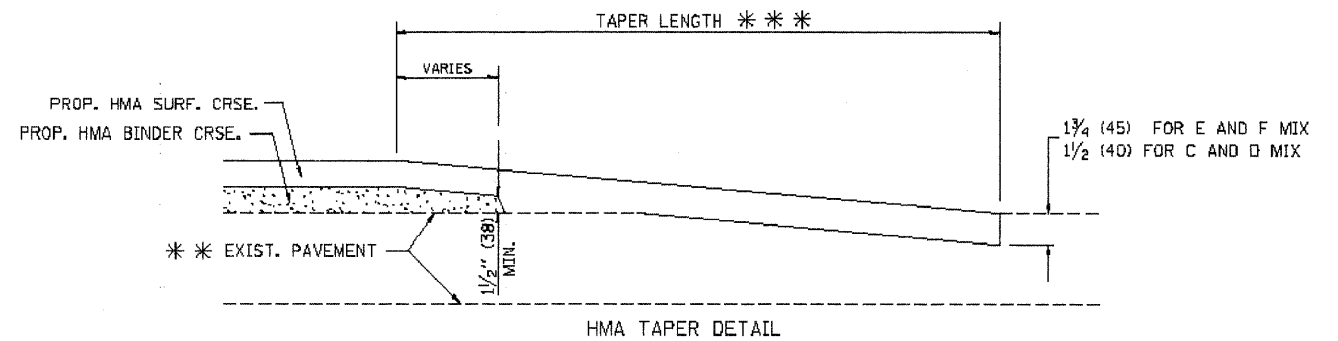


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- ** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90

REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

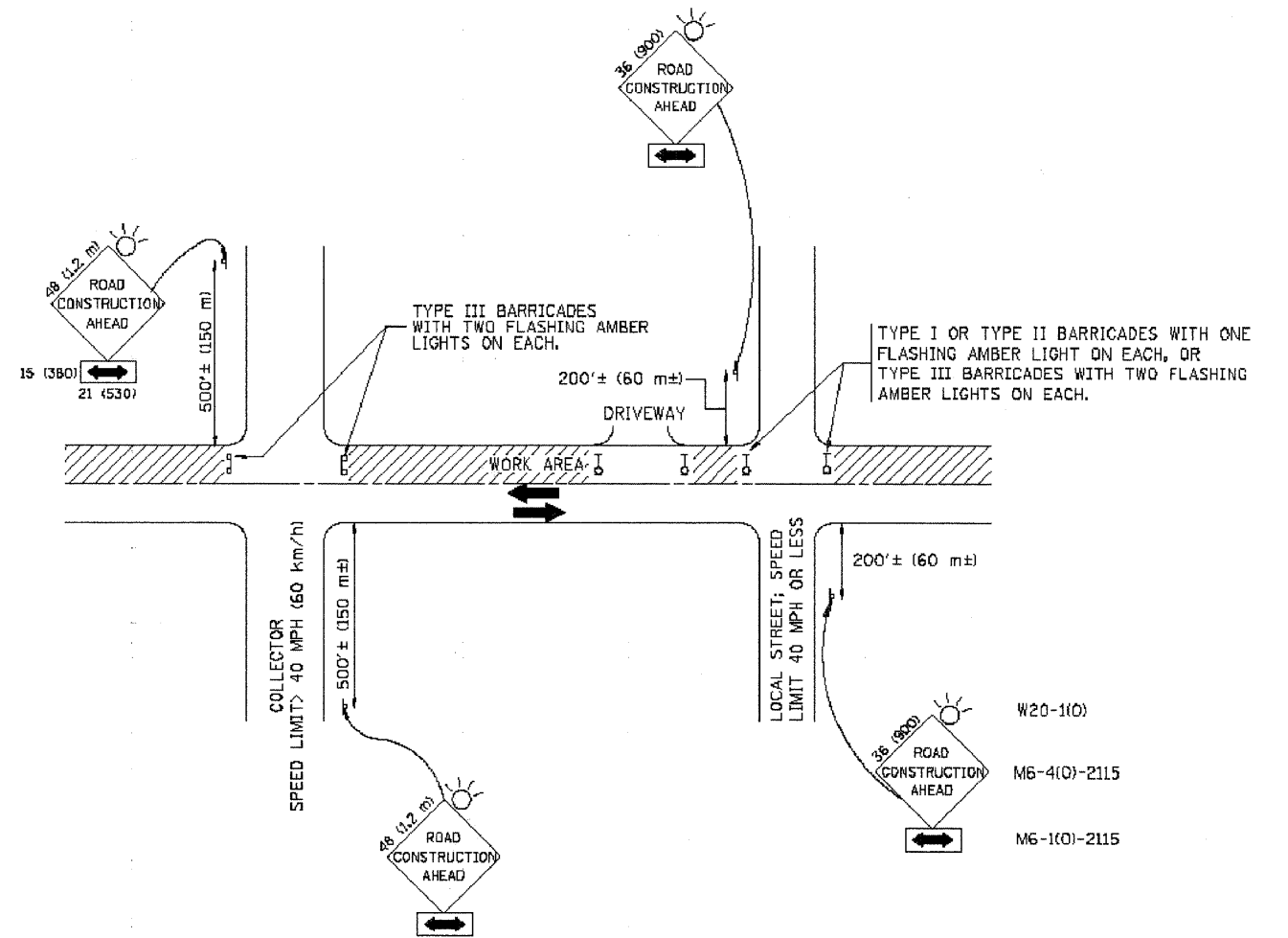
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	11
BD400-05 RD92		CONTRACT NO. 63172		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 565-08-20601



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

Drawing file: W:\Projects\56508236 - 9th Avenue LAPP\WASKINS.dwg Mar 16, 2009 - 3:50pm

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

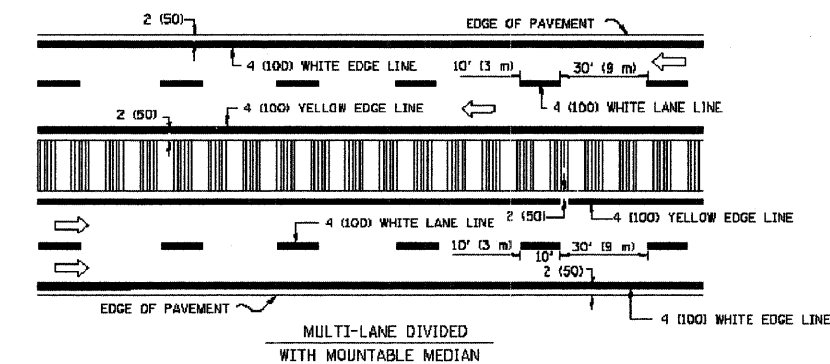
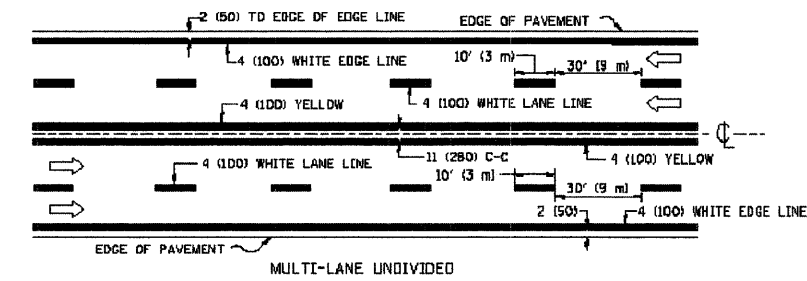
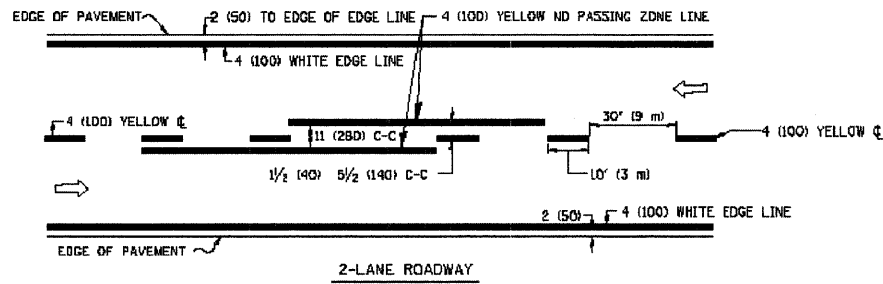
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

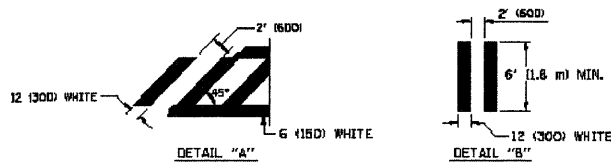
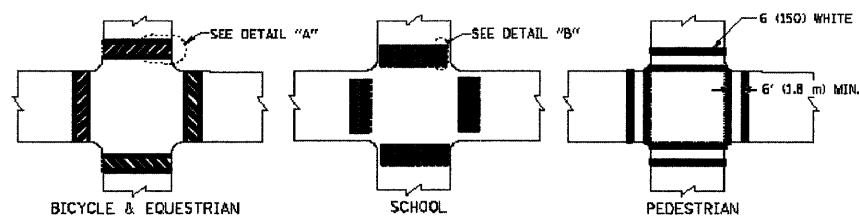
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	12
TC-10			CONTRACT NO. 63172	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 565-08-20601

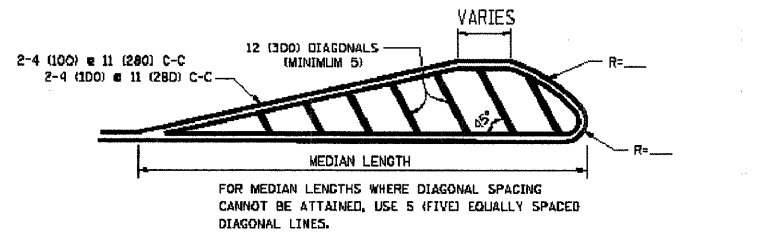
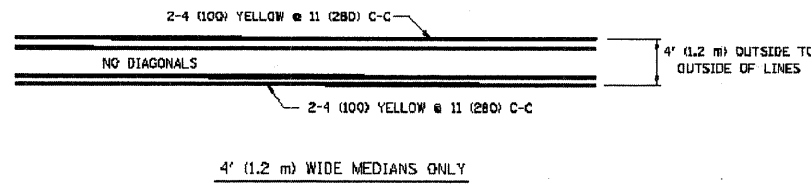


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

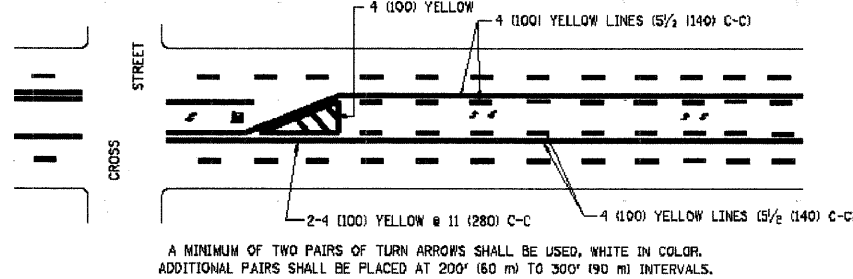


TYPICAL CROSSWALK MARKING



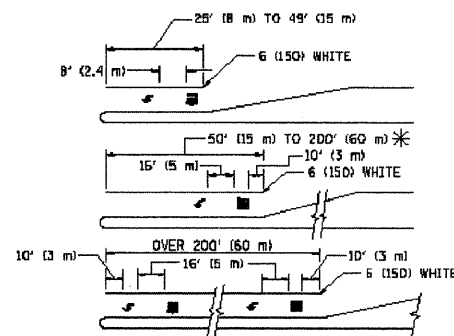
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

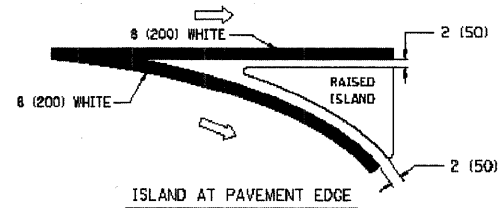
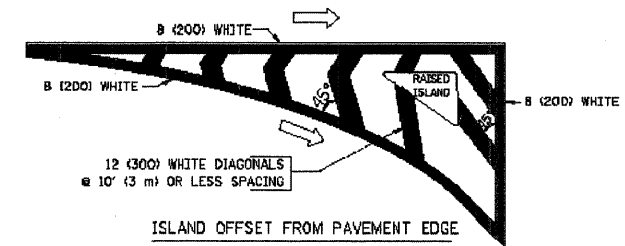


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	30' (9 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW, EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE, FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW, TWO WAY TRAFFIC WHITE, ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 5' (1.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 7800L AREA OF: "R" = 3.6 SQ. FT. (0.33 m ²) EACH "X" = 54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 7800L.

All dimensions are in inches (millimeters) unless otherwise shown.

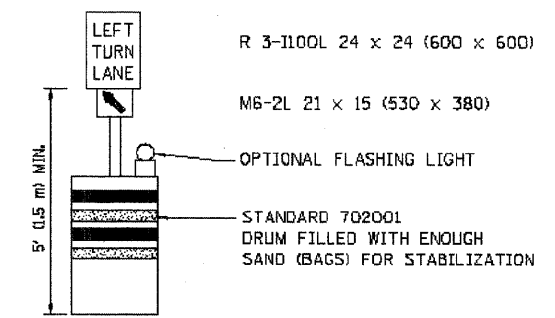
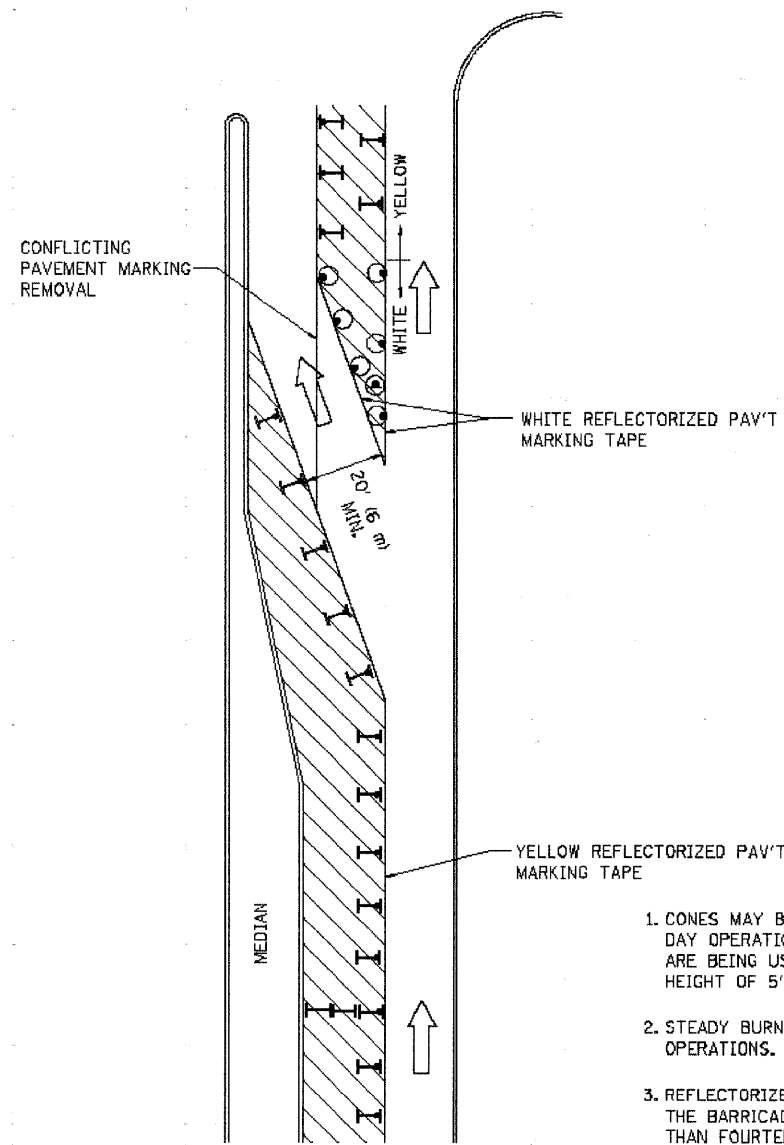
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PLOT DATE = 1/4/2008	DATE - 03-19-90	REVISOR -	REVISED - A. HOUSEH 10-17-96
			REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A. RTE. 2733	SECTION 08-00128-00-RS	COUNTY COOK	TOTAL SHEETS 14	SHEET NO. 13
			TC-13		CONTRACT NO. 63172		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

Drawing file: W:\Projects\65608236 - 9th Avenue LAPP\MARKINGS.dwg Mar 16, 2009 - 4:01pm



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

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		DATE -	REVISED -T. RAMMACHER 01-06-00
PLOT SCALE = 50.0000 / 1 IN.			
PLOT DATE = 1/4/2008			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2733	08-00128-00-RS	COOK	14	14
TC-14		CONTRACT NO. 63172		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

E.H.E. PROJECT NO. 565-08-20601