

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(50-7B)I-2	LASALLE	29	1
FED. ROAD DIST. NO. -		ILLINOIS	CONTRACT NO. 66890	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

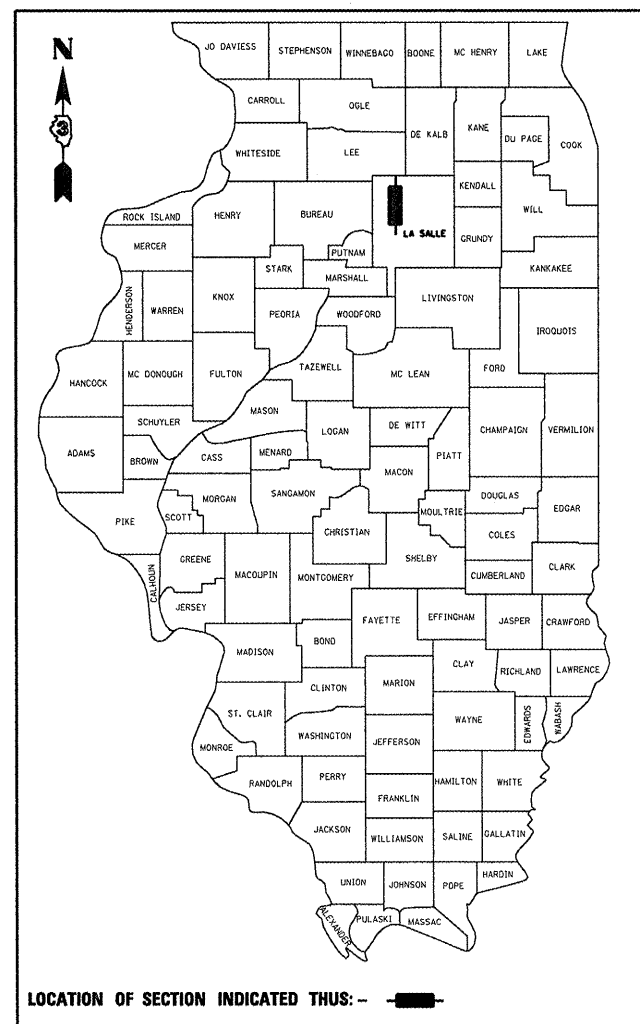
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

FAI ROUTE 39 (I-39)  
SECTION (50-7B)I-2

**BRIDGE REPAIR  
LASALLE COUNTY**  
C-93-040-09

D-93-002-09



LOCATION OF SECTION INDICATED THUS: - ■ -

FUNCTIONAL CLASSIFICATION

RURAL - INTERSTATE

F.A.I. ROUTE 39 (I-39)

2007 ADT = 19500

P.V = 56.4% M.U. = 38.5% S.U. = 5.1%

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED 4/12 2009  
*George Ryan*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER  
May 8, 2009  
*Charles J. Ingersoll*  
ENGINEER OF DESIGN AND ENVIRONMENT  
May 8, 2009  
*Christine M. Reed*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

**INDEX OF SHEETS**

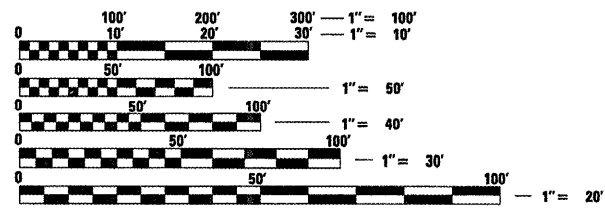
1. COVER SHEET
2. GENERAL NOTES
3. SUMMARY OF QUANTITIES
4. TYPICAL SECTION
- 6-7. TRAFFIC CONTROL STAGING
8. TEMPORARY CONCRETE BARRIER
9. TYPICAL PAVEMENT MARKINGS
10. ROADWAY DETAILS
11. GENERAL PLAN & ELEVATION
12. SLOPEWALL DETAILS
- 13-19. BRIDGE PLANS - SN 050-0169 (SB)
- 20-27. BRIDGE PLANS - SN 050-0170 (NB)
28. STRIP SEAL EXPANSION JOINT DETAILS
29. BAR SPLICER ASSEMBLY DETAILS

**STANDARDS**

- |           |   |
|-----------|---|
| 000001-05 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS                                  |
| 001001-02 | AREAS OF REINFORCEMENT BARS   |
| 001006    | DECIMAL OF AN INCH AND OF A FOOT  |
| 482006-03 | HMA SHOULDER ADJACENT TO RIGID PAVEMENT                                       |
| 701101-02 | OFF-ROAD OPERATIONS MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE |
| 701106-02 | OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY                    |
| 701400-03 | APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY                                  |
| 701402-07 | LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH BARRIER (INCLUDED FOR REFERENCE ONLY)  |
| 701406-05 | LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY                         |
| 701411-05 | LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP FOR SPEEDS ≥ 45 MPH         |
| 701901-01 | TRAFFIC CONTROL DEVICES   |
| 704001-05 | TEMPORARY CONCRETE BARRIER  |

**PROJECT LOCATION**

S.N. 050-0169 (SB) & S.N. 050-0170 (NB)  
FAI 39 OVER THE VERMILION RIVER  
1.30 MILES NORTH OF US 52  
(NEAR TROY GROVE)

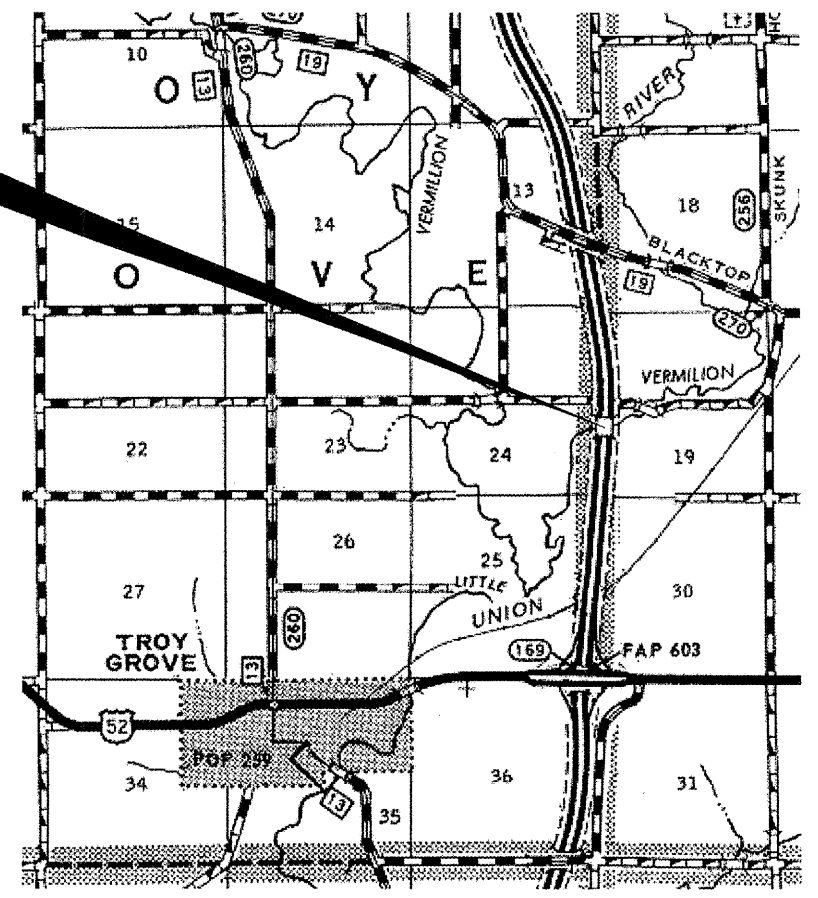


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: JOE KANNEL  
UNIT CHIEF: RON WOODSHANK  
TOWNSHIP: OPHIR

CONTRACT NO. 66890



POINT LOCATION

# GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY FURNISHED BASED UPON THE UNIT BID PRICE FOR THE WORK.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE "JULIE" NUMBER IS 1-800-892-0123. A MINIMUM OF FORTY-EIGHT (48) HOURS ADVANCE NOTICE IS REQUIRED.

THE COST OF ANY SAW CUTS MADE TO COMPLETE THE WORK AS DESCRIBED IN PLAN DETAILS, UNLESS OTHERWISE NOTED SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED WITH THE VARIOUS PAY REMOVAL PAY ITEMS INVOLVED.

FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.

SHORT TERM PAVEMENT MARKING SHALL BE USED TO OUTLINE EXIT AND ENTRANCE RAMPS FOR THE PRIME COAT APPLICATION AND EACH RESURFACING LIFT.

ALL ELEVATIONS REFERRING TO U.S.G.S. MEAN SEA LEVEL DATUM.

ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

<del>BITUMINOUS MAT PRIME COAT</del>	<del>0.08</del>	<del>GAL / SQ YD OR</del>
	<del>0.375</del>	<del>GAL / SQ YD</del>
<del>HMA RESURFACING</del>	<del>112</del>	<del>LBS / SQ YD / IN</del>
SHORT TERM PAVEMENT MARKING	10	FT /100 FT OF APPLICATION

	<b>HMA SHOULDER</b>
<b>PG GRADE</b>	<b>PG64-22</b>
<b>MAX % RAP ALLOWABLE</b>	<b>10%**</b>
<b>DESIGN AIR VOIDS</b>	<b>4.0% @ N90</b>
<b>MIXTURE COMPOSITION</b>	<b>IL 12.5 or IL 9.5</b>
<b>FRICTION AGGREGATE</b>	<b>MIXTURE C</b>
<b>DENSITY CONTROL METHOD</b>	<b>NUCLEAR OR CORES</b>

**\*\*IF RAP OPTION IS SELECTED, THE ASPHALT CEMENT GRADE MAY NEED TO BE ADJUSTED. THIS WILL BE DETERMINED BY THE ENGINEER.**

DATE: 4.1.09

PREPARED BY: Rick Powell  
DISTRICT STUDIES & PLANS ENGINEER

EXAMINED BY: Herbert K. Jung  
DISTRICT CONSTRUCTION ENGINEER

Wayne J. Phillips  
DISTRICT MATERIALS ENGINEER

Bruce A. Wheeler  
DISTRICT OPERATIONS ENGINEER

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -
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	PLOT DATE = Mar 25, 2009 - 06:57:13 AM	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

## GENERAL NOTES

SCALE: _____	SHEET NO. 1 OF 1 SHEETS	STA. _____ TO STA. _____	F.A.I. RTE. 39	SECTION (50-7B)I-2	COUNTY LASALLE	TOTAL SHEETS 29	SHEET NO. 2
			CONTRACT NO. 66890				
			FED. ROAD DIST. NO. - ILLINOIS				

SUMMARY OF QUANTITIES				
CONSTRUCTION CODE SFTY-2A				
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	RURAL 100% STATE
44000920	BITUMINOUS CONCRETE SHOULDER REMOVAL	SQ YD	2,232	2,232
44200541	CLASS A PATCHES, TYPE II, 9 INCH	SQ YD	100	100
48300400	PORTLAND CEMENT CONCRETE SHOULDERS 9"	SQ YD	1112	1112
50102400	CONCRETE REMOVAL	CU YD	56	56
50300225	CONCRETE STRUCTURES	CU YD	12	12
50300255	CONCRETE SUPERSTRUCTURE	CU YD	44	44
50300260	BRIDGE DECK GROOVING	SQ YD	1,134	1,134
50300300	PROTECTIVE COAT	SQ YD	1,200	1,200
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	10,344	10,344
50800515	BAR SPLICERS	EACH	100	100
52000110	PREFORMED JOINT STRIP SEAL	FOOT	170	170
59000200	EPOXY CRACK INJECTION	FOOT	29	29
67100100	MOBILIZATION	L SUM	1	1
70101605	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402 (SPECIAL)	EACH	2	2
70400100	TEMPORARY CONCRETE BARRIER	FOOT	800	800
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	750	750
X 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	2,820	2,820
X 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	520	520
78300100	PAVEMENT MARKING REMOVAL	SQ FT	1,190	1,190
X0321468	PLUG EXISTING DECK DRAINS	EACH	4	4
X0322194	POLYMER MODIFIED PORTLAND CEMENT MORTAR	SQ FT	73	73
X0322489	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/2 INCHES	SQ YD	2,177	2,177
X0323491	SLOPE WALL CRACK SEALING	FOOT	67	67
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	96	96
Z0006225	BRIDGE DECK HYDRO-SCARIFICATION 2 1/2"	SQ YD	2,177	2,177
50300530	FLOOR DRAIN EXTENSION	EACH	5	5
Z0016000	DECK SLAB REPAIR (FULL DEPTH)	SQ YD	40	40
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2
Z0032470	JOINT SEALER	FOOT	119	119
Z0065700	SLOPE WALL REPAIR	SQ YD	15	15

X SPECIALTY  
ITEM

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(50-7B)I-2	LASALLE	29	3
FED. ROAD DIST. NO. [ILLINOIS]			CONTRACT NO. 66890	

**SUMMARY OF QUANTITIES**

CONSTRUCTION CODE SFTY-2A

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	RURAL 100% STATE SN 050-0170 (NB) PPS# 0-01616-3002
44000915	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	600	600
44000920	BITUMINOUS CONCRETE SHOULDER REMOVAL	SQ YD	1120	1120
44200541	CLASS 1 PATCHES, TYPE II, 9"	SQ YD	50	50
48203033	HOT-MIX ASPHALT SHOULDERS 9"	SQ YD	1120	1120
50102400	CONCRETE REMOVAL	CU YD	28	28
50300225	CONCRETE STRUCTURES	CU YD	6	6
50300255	CONCRETE SUPERSTRUCTURE	CU YD	22	22
50300260	BRIDGE DECK GROOVING	SQ YD	567	567
50300300	PROTECTIVE COAT	SQ YD	600	600
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	5172	5172
50800515	BAR SPLICERS	EACH	50	50
52000110	PREFORMED JOINT STRIP SEAL	FOOT	85	85
59000210	EPOXY CRACK SEALING	FOOT	15	15
67100100	MOBILIZATION	L SUM	0.5	0.5
70101605	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402 (SPECIAL)	EACH	1	1
70400100	TEMPORARY CONCRETE BARRIER	FOOT	400	400
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	375	375
78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	1410	1410
78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	260	260
78300100	PAVEMENT MARKING REMOVAL	SQ FT	595	595
X0321468	PLUG EXISTING DECK DRAINS	EACH	4	4
X0322194	POLYMER MODIFIED PORTLAND CEMENT MORTAR	SQ FT	40	40
X0322489	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/2"	SQ YD	822	822
X0323491	SLOPE WALL CRACK SEALING	FOOT	29	29
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	54	54
Z0006225	BRIDGE DECK HYDRO-SCARIFICATION 2 1/2"	SQ YD	822	822
Z0015595	DECK DRAIN EXTENSIONS	EACH	5	5
Z0016000	DECK SLAB REPAIR (FULL DEPTH)	SQ YD	20	20
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	1	1
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	1	1
Z0032470	JOINT SEALER	FOOT	76	76
Z0065700	SLOPE WALL REPAIR	SQ YD	15	15

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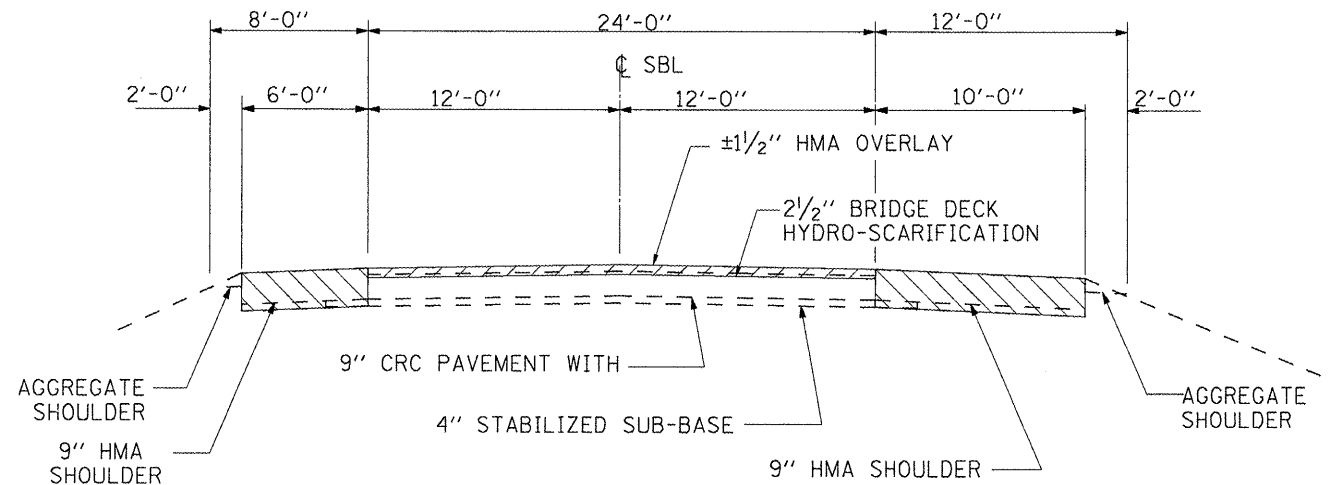
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

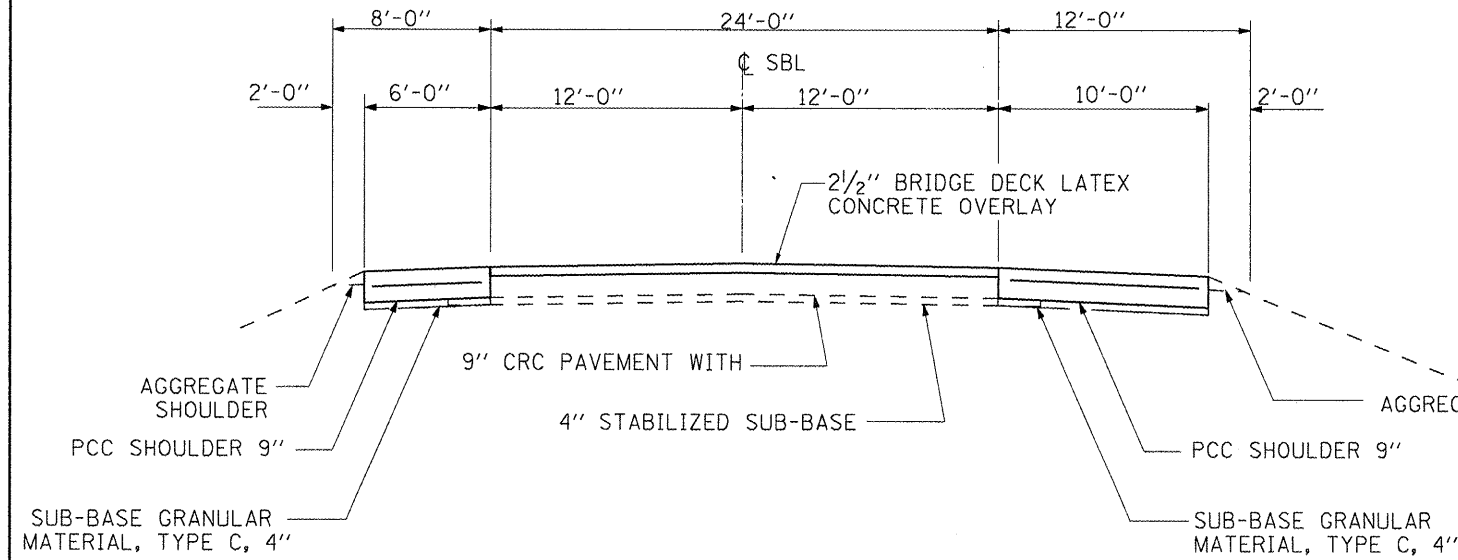
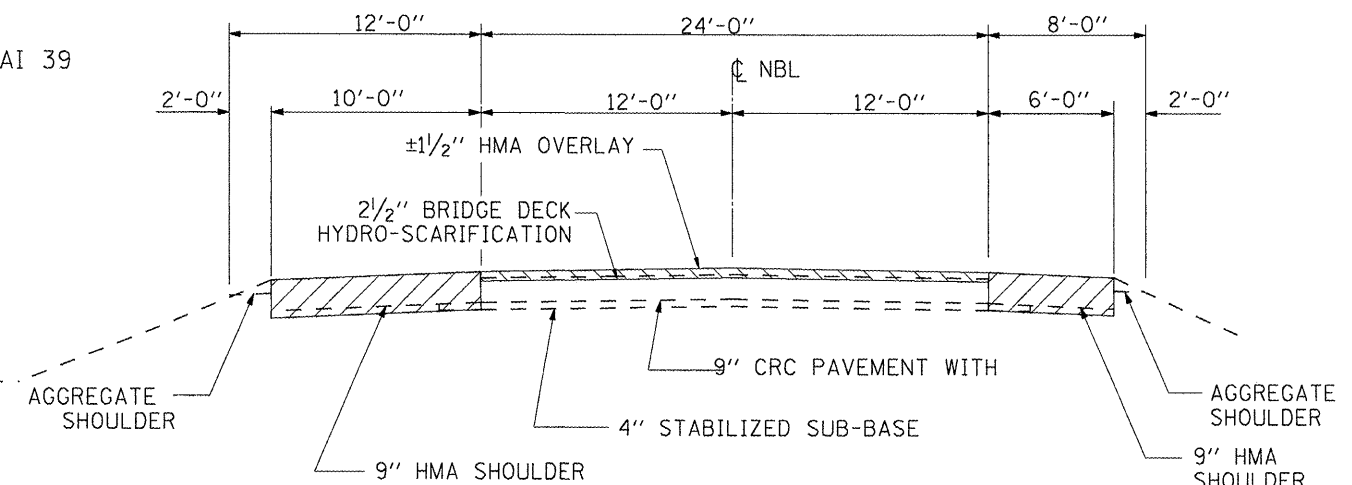
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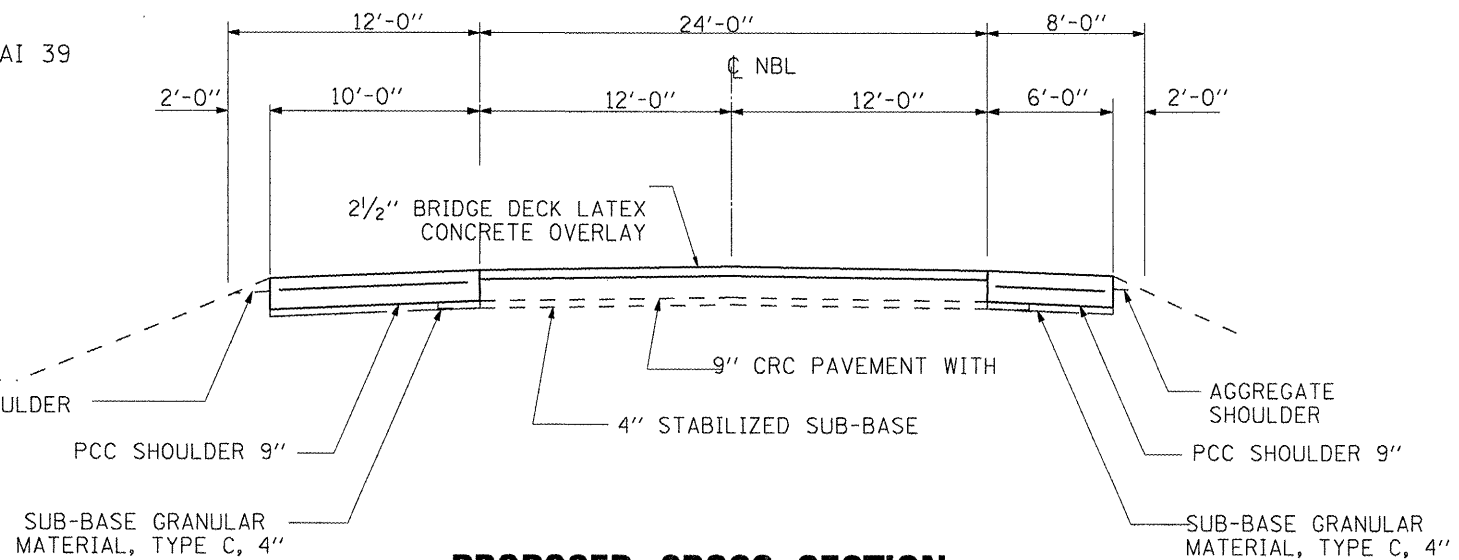
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39	(50-7B)I-2	LASALLE	29	4
FED. ROAD DIST. NO. [ILLINOIS]			CONTRACT NO. 66890	



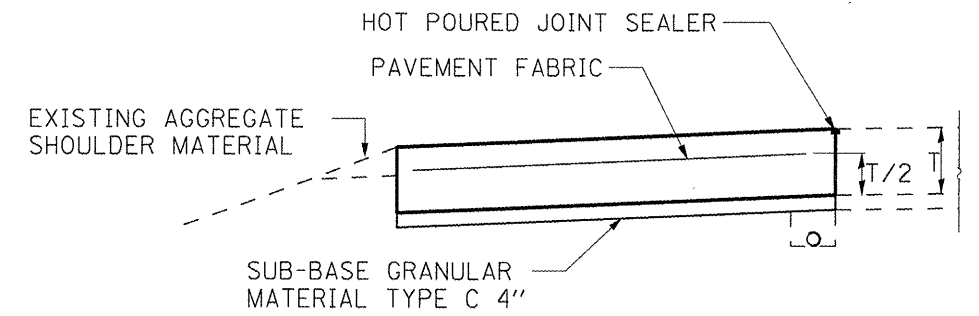
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LOOKING AHEAD



**PROPOSED  
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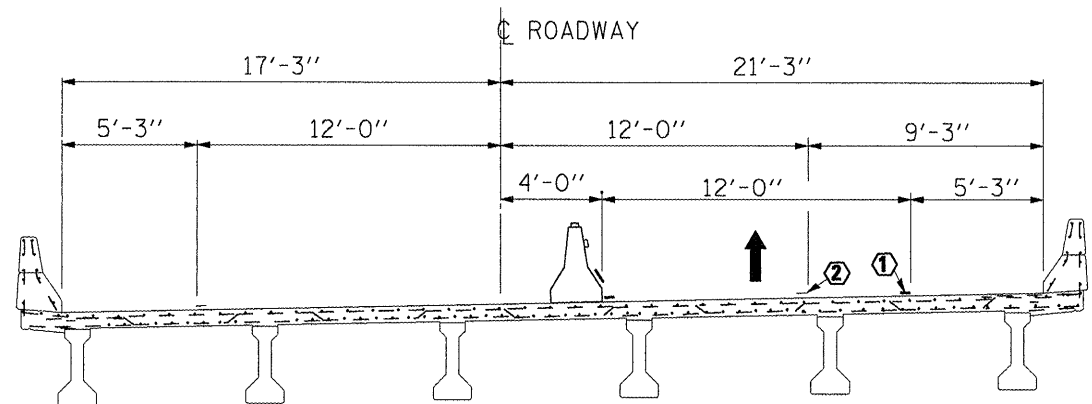


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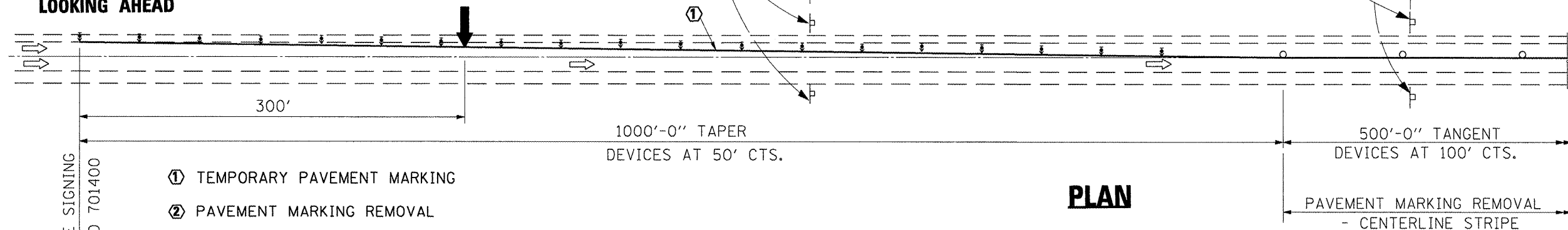
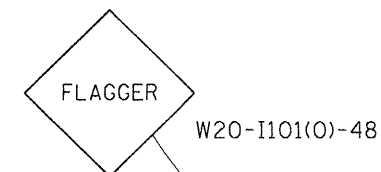
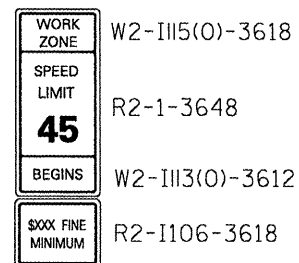


**SHOULDER DETAIL**

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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**CROSS SECTION AT STRUCTURE  
LOOKING AHEAD**



**PLAN**

- SYMBOLS**
- ↑ Arrow board
  - ▨ Work area
  - ⊥ Sign
  - ⬆ Direction indicator barricade with steady burn monodirectional light
  - ⬆ Type II barricade, drum, or vertical barricade with steady burn monodirectional light
  - ▬ Temporary concrete barrier
  - ◁ Monodirectional barrier wall/guardrail marker
  - ▨ Impact attenuator
  - Flagger with traffic control sign

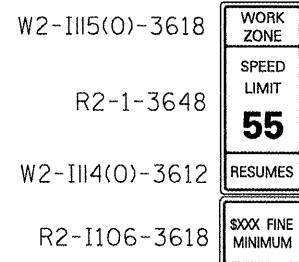
FOR ADVANCE SIGNING  
SEE STANDARD 701400

- ① TEMPORARY PAVEMENT MARKING
- ② PAVEMENT MARKING REMOVAL

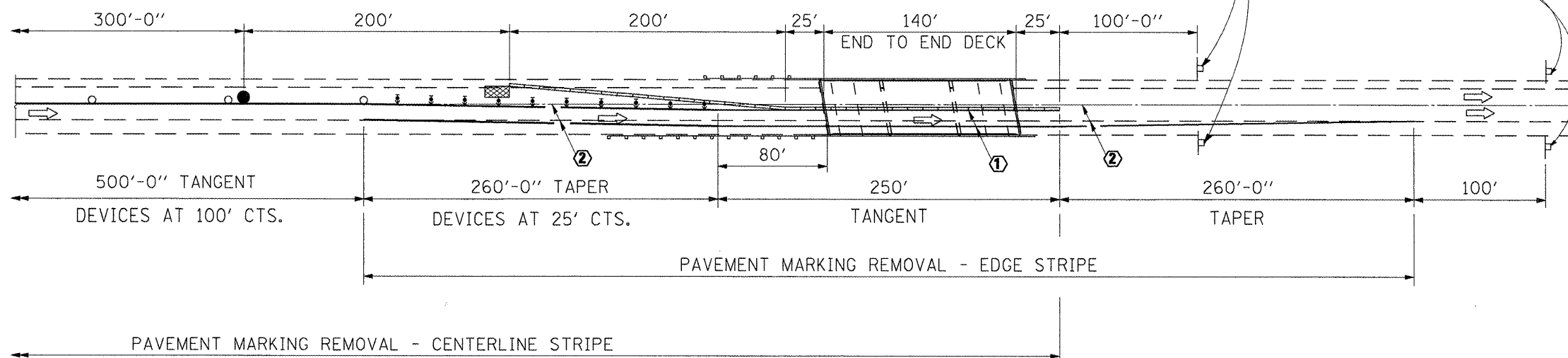
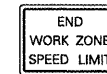
1000'-0" TAPER  
DEVICES AT 50' CTS.

500'-0" TANGENT  
DEVICES AT 100' CTS.

PAVEMENT MARKING REMOVAL  
- CENTERLINE STRIPE



G20-I103(O)-3660



**PLAN**

**GENERAL NOTES**

THIS DETAIL IS USED WHERE AT ANY TIME, ANY VEHICLE, EQUIPMENT, WORKERS OR THEIR ACTIVITIES WILL ENCROACH ON THE PAVEMENT OR ON THE SHOULDER WITHIN 24" OF THE EDGE OF PAVEMENT FOR WORK OPERATIONS EXCEEDING ONE DAY AND WHERE TEMPORARY CONCRETE BARRIER IS UTILIZED.

THIS DETAILS MUST ALWAYS BE USED IN COMBINATION WITH STANDARD 701400.

BARRIER WALL/GUARDRAIL MARKERS AT 25' CTS. MARKERS ON RIGHT SHALL BE CRYSTAL AND MARKERS ON LEFT SHALL BE AMBER.

VERTICAL BARRICADES SHALL NOT BE USED IN LANE SHIFT TAPERS.

SPEED LIMIT SIGNS SHALL BE REQUIRED WHENEVER WORK IS BEING PERFORMED WITHIN THE INTERSTATE RIGHT OF WAY.

FLAGGER AND FLAGGER SIGNS SHALL NOT BE REQUIRED WHEN TEMPORARY CONCRETE BARRIER IS IN PLACE.

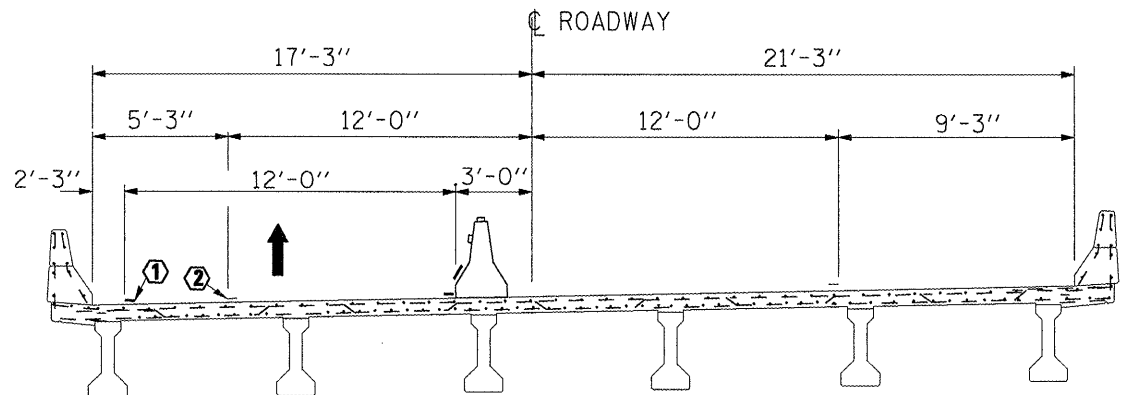
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	PLOT DATE = Mar 25, 2009 - 06:57:37 AM	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**STANDARD 701402 (SPECIAL)  
STAGE I**

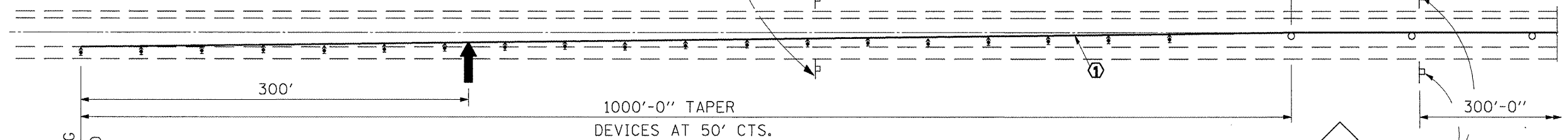
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(50-7B)I-2	LASALLE	29	6
FED. ROAD DIST. NO. - [ILLINOIS]			CONTRACT NO. 66890	



**CROSS SECTION AT STRUCTURE**  
LOOKING AHEAD

WORK ZONE W2-III5(0)-3618  
SPEED LIMIT R2-1-3648  
**45**  
BEGINS W2-III3(0)-3612  
\$XXX FINE MINIMUM R2-I106-3618



**PLAN**

**GENERAL NOTES**

THIS DETAIL IS USED WHERE AT ANY TIME, ANY VEHICLE, EQUIPMENT, WORKERS OR THEIR ACTIVITIES WILL ENCROACH ON THE PAVEMENT OR ON THE SHOULDER WITHIN 24" OF THE EDGE OF PAVEMENT FOR WORK OPERATIONS EXCEEDING ONE DAY AND WHERE TEMPORARY CONCRETE BARRIER IS UTILIZED.

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- SYMBOLS**
- ↑ Arrow board
  - ▨ Work area
  - ⊥ Sign
  - ↑ Direction indicator barricade with steady burn monodirectional light
  - ⊥ Type II barricade, drum, or vertical barricade with steady burn monodirectional light
  - ▬ Temporary concrete barrier
  - ◁ Monodirectional barrier wall/guardrail marker
  - ▨ Impact attenuator
  - Flagger with traffic control sign

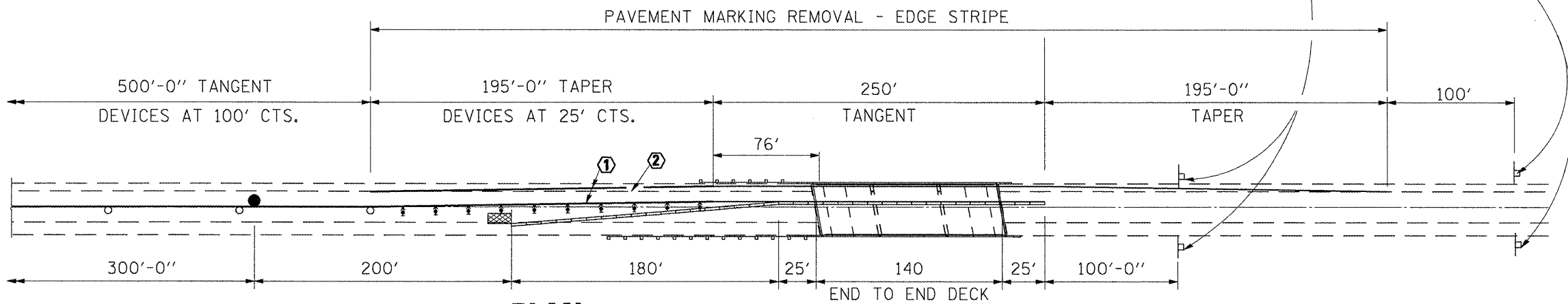
FOR ADVANCE SIGNING  
SEE STANDARD 701400

- ① TEMPORARY PAVEMENT MARKING
- ② PAVEMENT MARKING REMOVAL

W2-III5(0)-3618  
R2-1-3648  
W2-III4(0)-3612  
R2-I106-3618

WORK ZONE  
SPEED LIMIT  
**55**  
RESUMES  
\$XXX FINE MINIMUM

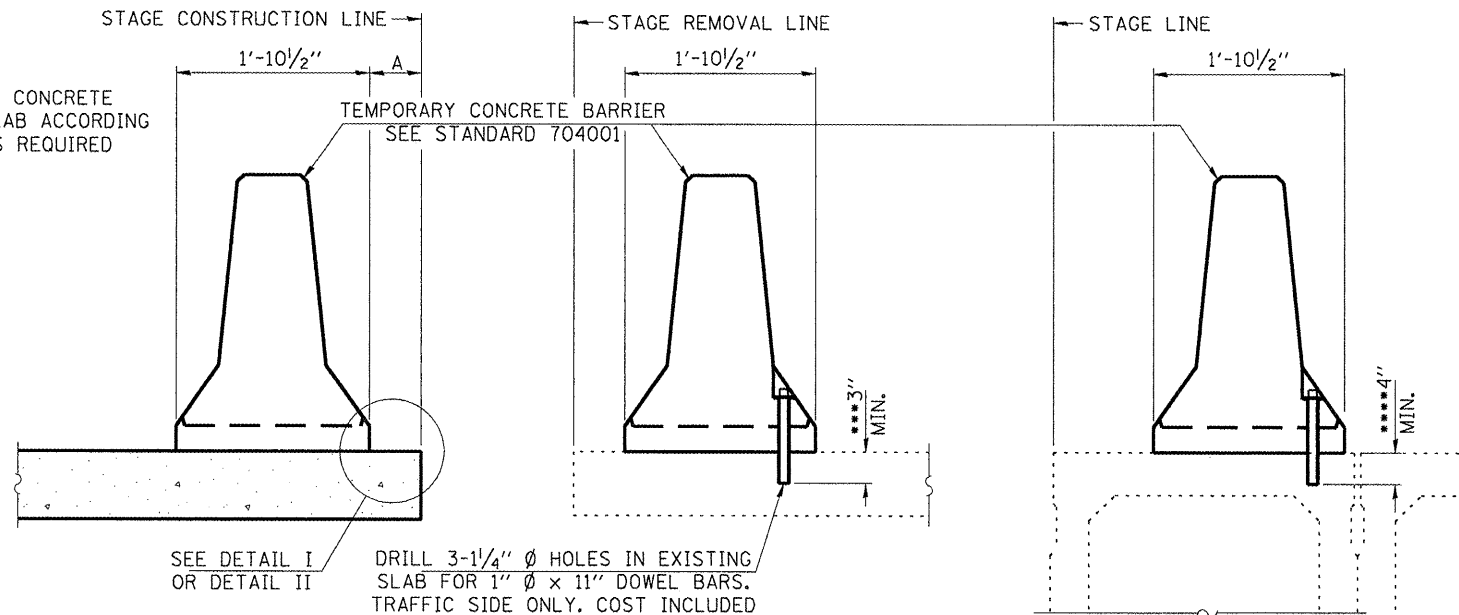
G20-I103(0)-3660  
END WORK ZONE SPEED LIMIT



**PLAN**



WHEN "A" IS 3'-6" OF LESS, THE TEMPORARY CONCRETE BARRIER SHALL BE ANCHORED TO THE NEW SLAB ACCORDING TO DETAIL I OR DETAIL II. NO ANCHORAGE IS REQUIRED WHEN "A" IS GREATER THAN 3'-6".

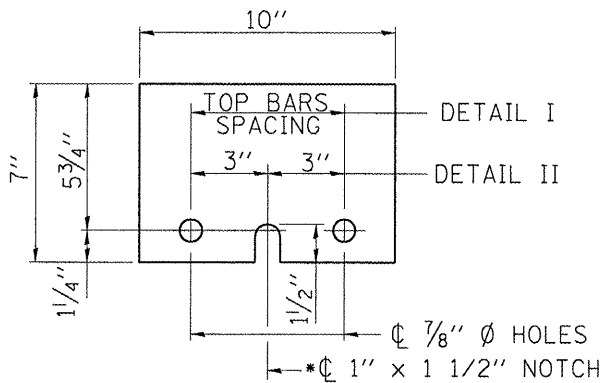


**NEW SLAB**

**EXISTING SLAB**

**EXISTING DECK BEAM**

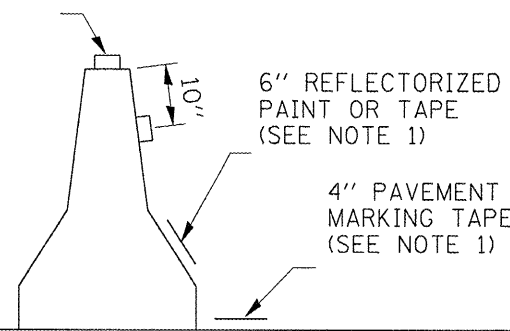
**SECTIONS THRU SLAB OR DECK BEAM**



**STEEL RETAINER PLATE 1" x 7" x 10"**

\*REQUIRED ONLY WITH DETAIL II

PRISMATIC BARRIER REFLECTOR @ 50' CENTERS



**TEMPORARY CONCRETE BARRIER**

NOTES:

1. THE CONTRACTOR HAS THE OPTION OF USING EITHER THE LINE ON THE TEMPORARY CONCRETE BARRIER OR ON THE PAVEMENT.
2. THE COLOR OF THE REFLECTORS AND PAVEMENT/BARRIER MARKING LINE WILL VARY WITH STAGING AND SHALL MATCH THE EXISTING LINE IN THE WORK AREA.
3. THE COST OF THE REFLECTORS AND THE PAVEMENT/BARRIER MARKING LINE IS INCLUDED IN THE COST OF THE TEMPORARY CONCRETE BARRIER.

- \*\* WOOD BLOCKS MAY BE OMITTED WHEN REQUIRED TO PROVIDE MINIMUM STAGE TRAFFIC LANE WIDTH. WHEN THE WOOD BLOCKS ARE OMITTED, THE CONCRETE BARRIER SHALL BE IN DIRECT CONTACT WITH THE STEEL RETAINER PLATE.
- \*\*\* DIMENSION SHOWN IS MINIMUM REQUIRED EMBEDMENT INTO CONCRETE. IF HOT-MIX ASPHALT WEARING SURFACE IS PRESENT, MINIMUM EMBEDMENT SHALL BE IN ADDITION TO THE WEARING SURFACE DEPTH.
- \*\*\*\* IF EXISTING DECK BEAM IS TO REMAIN IN PLACE AFTER STAGE CONSTRUCTION, EMBEDMENT SHALL ONLY BE INTO THE WEARING SURFACE AND NOT INTO EXISTING DECK BEAM CONCRETE.

**NOTES**

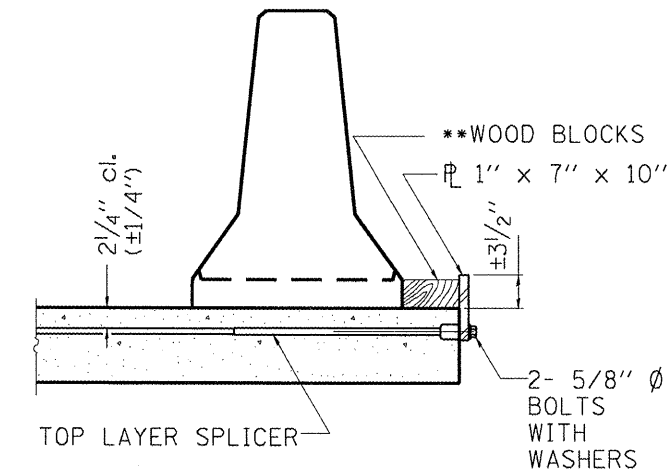
DETAIL I - WITH BAR SPLICER OR COUPLERS:

CONNECT ONE (1) 1" x 7" x 10" STEEL PLATE TO THE TOP LAYER OF COUPLERS WITH 2-5/8" Ø BOLTS SCREWED TO COUPLER AT APPROXIMATE C OF EACH BARRIER PANEL.

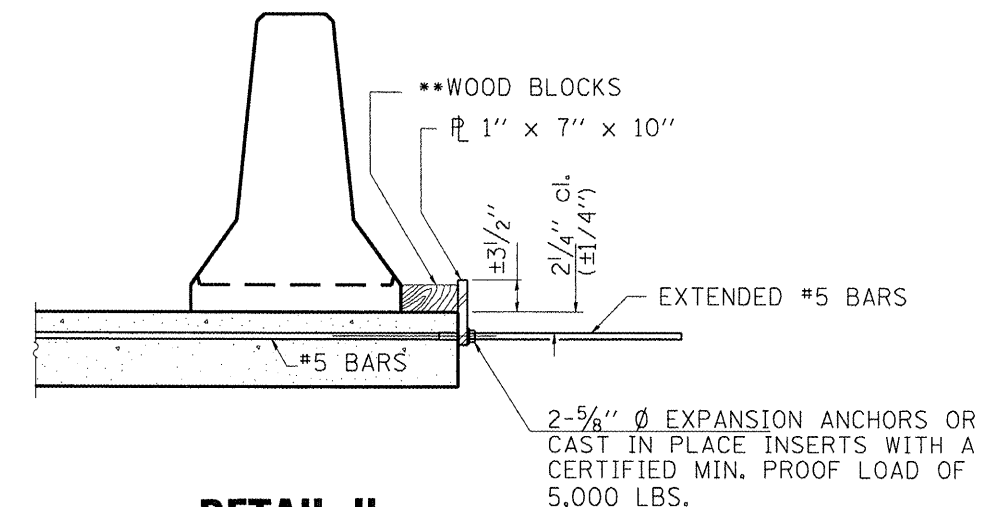
DETAIL II - WITH EXTENDED REINFORCEMENT BARS:

CONNECT ONE (1) 1" x 7" x 10" STEEL PLATE TO THE CONCRETE SLAB OR CONCRETE WEARING SURFACE WITH 2-5/8" Ø EXPANSION ANCHORS OR CAST IN PLACE INSERTS SPACED BETWEEN THE TOP LAYER OF REINFORCEMENT AT APPROXIMATE C OF EACH BARRIER PANEL

COST OF ANCHORAGE IS INCLUDED WITH TEMPORARY CONCRETE BARRIER. THE 1" x 7" x 10" PLATE SHALL NOT BE REMOVED UNTIL STAGE II CONSTRUCTION FORMS AND ALL REINFORCEMENT BARS ARE IN PLACE AND THE CONCRETE IS READY TO BE PLACED.



**DETAIL I**



**DETAIL II**

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -
ct:\pw_work\PIWIDOT\WOODSHANKRL\dms91655	D3-66890-detailed.dgn	DRAWN - RON WOODSHANK	REVISED -
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = Mar 25, 2009 - 06:57:50 AM	DATE -	REVISED -

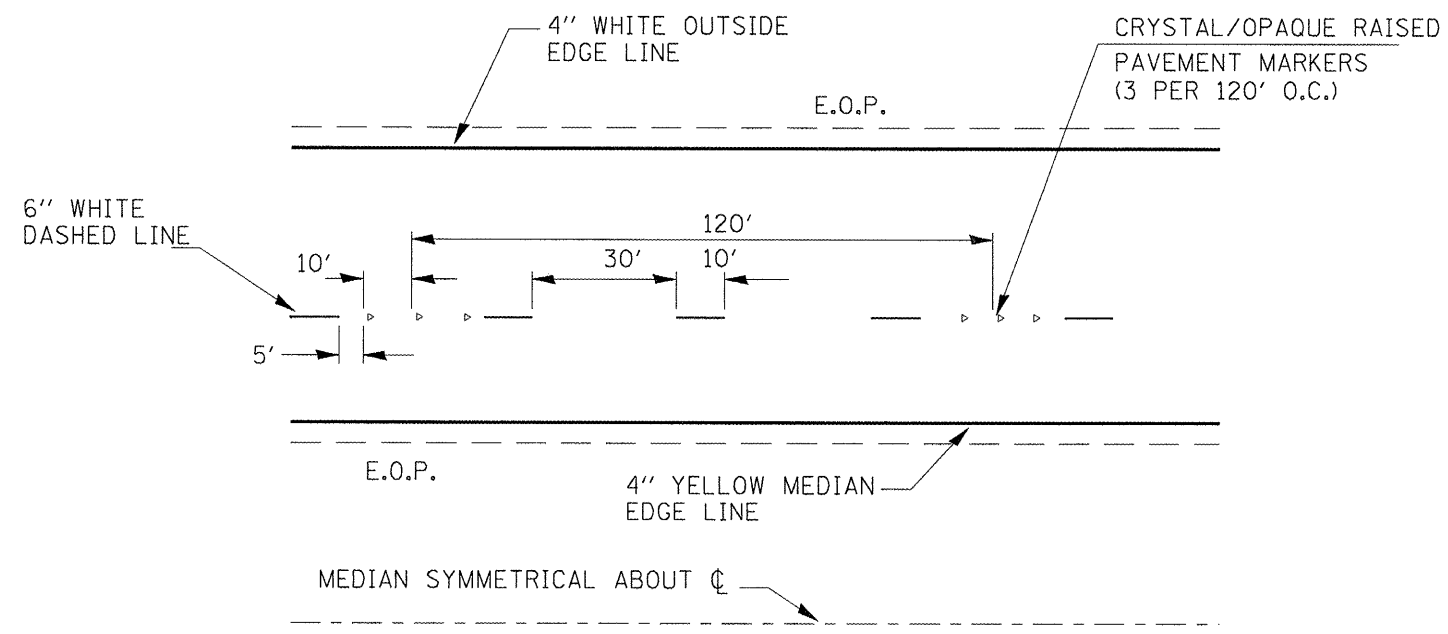
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**TEMPORARY CONCRETE BARRIER**

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

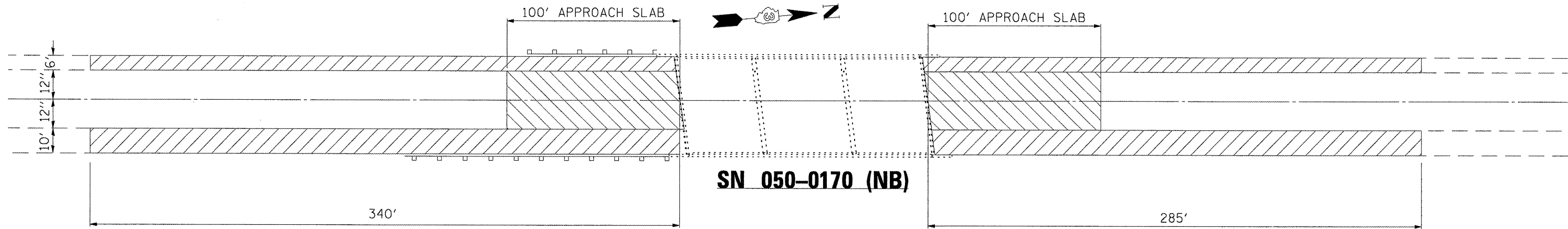
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(50-7B)I-2	LASALLE	29	8
FED. ROAD DIST. NO. ILLINOIS			CONTRACT NO. 66890	





**TYPICAL PAVEMENT MARKING**

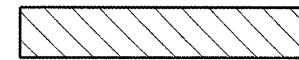
FILE NAME = c:\pr_work\PW\DOT\WOODSHANKRL\dms91655	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL PAVEMENT MARKING</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	03-66898-deta1.s.dgn	DRAWN - RON WOODSHANK	REVISED -			39	(50-7B)I-2	LASALLE	29	9
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 66890				
	PLOT DATE = Mar 25, 2009 - 06:57:56 AM	DATE -	REVISED -			FED. ROAD DIST. NO. [ILLINOIS]				
SCALE: _____ SHEET NO. 1 OF 1 SHEETS STA. _____ TO STA. _____										



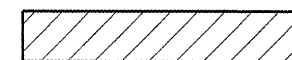
**SN 050-0170 (NB)**

**PLAN**

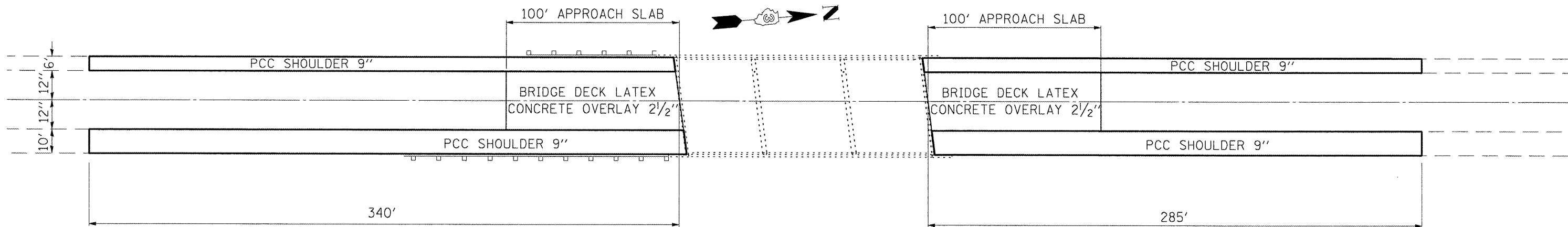
NBL SHOWN, SBL SIMILAR THRU 180° ROTATION



**BRIDGE DECK HYDRO-SCARIFICATION 2 1/2"**



**BITUMINOUS CONCRETE SHOULDER REMOVAL**



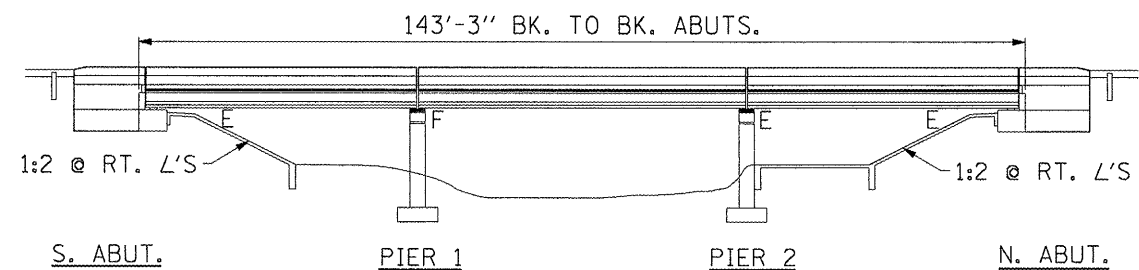
**SN 050-0170 (NB)**

NBL SHOWN, SBL SIMILAR THRU 180° ROTATION

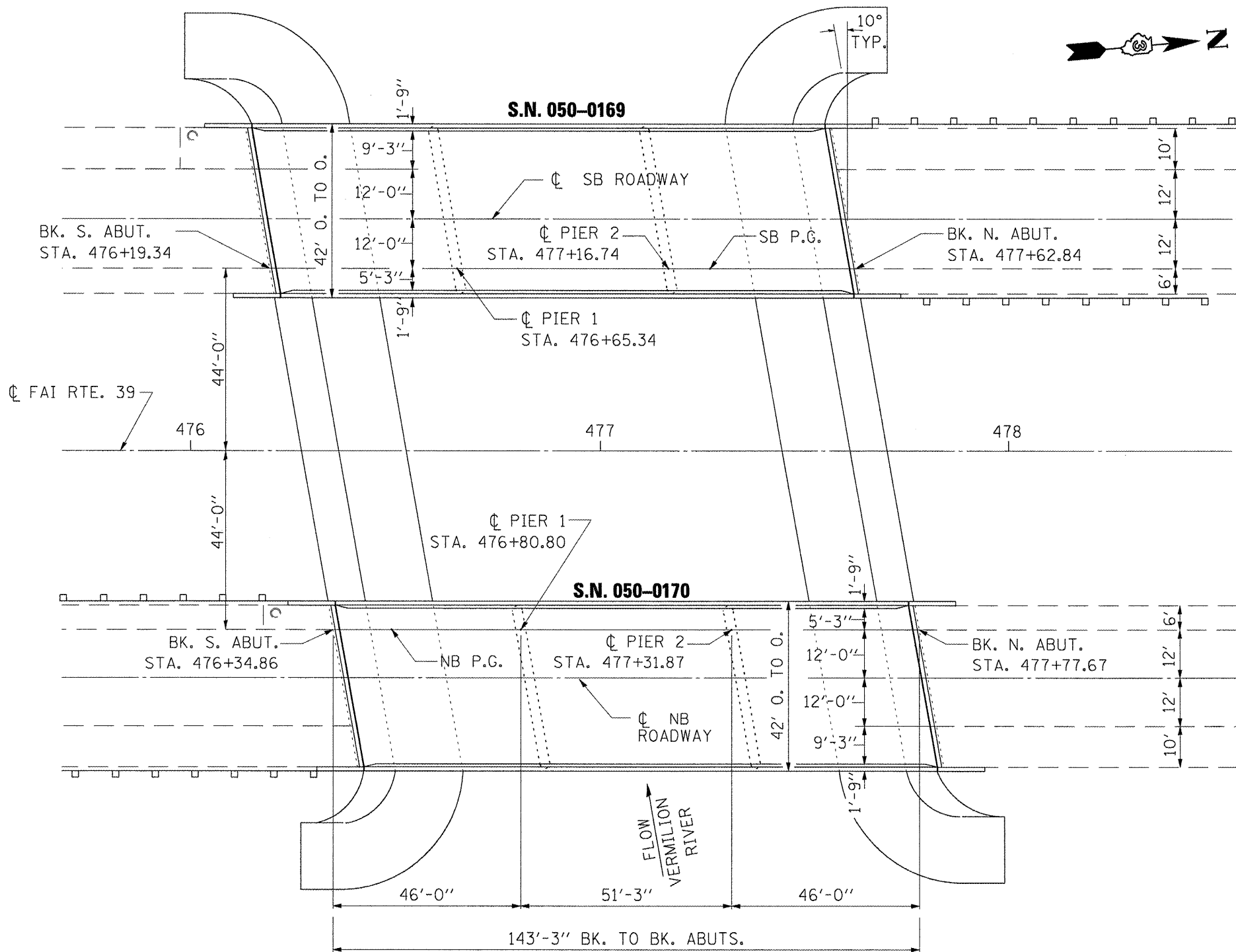
**NOTE:**

ALL SHOULDER WORK SHALL BE COMPLETED PRIOR TO BEGINNING WORK OPERATIONS ON THE STRUCTURE OR APPROACH PAVEMENTS FOR STAGE CONSTRUCTION.

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY DETAILS</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pr_work\PWIDOT\WOODSHANKR1\dms91655	03-66898-details.dgn	DRAWN - RON WOODSHANK	REVISED -			39	(50-7B)-2	LASALLE	29	10
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -					CONTRACT NO. 66890		
	PLOT DATE = Mar 25, 2009 - 08:58:01 AM	DATE -	REVISED -			SCALE: _____ SHEET NO. 1 OF 1 SHEETS STA. _____ TO STA. _____		FED. ROAD DIST. NO. ILLINOIS		



**ELEVATION**



**PLAN**

**GENERAL NOTES**

PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY FURNISHED BASED UPON THE UNIT BID PRICE FOR THE WORK.

REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 706 GR 60. SEE SPECIAL PROVISIONS.

JOINT OPENINGS SHALL BE ADJUSTED ACCORDING TO ARTICLE 520.04 OF THE STANDARD SPECIFICATIONS WHEN THE DECK IS POURED AT AN AMBIENT TEMPERATURE OTHER THAN 50° F.

ALL STRUCTURAL STEEL SHALL CONFORM TO AASHTO CLASSIFICATION M-270 GR. 36, UNLESS OTHERWISE NOTED.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

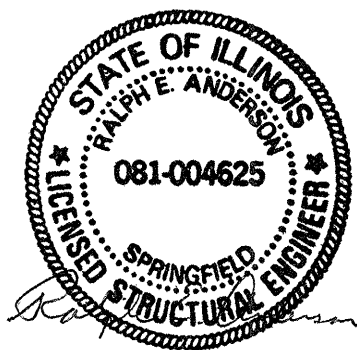
EXISTING REINFORCEMENT BARS EXTENDING INTO REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.

THE CONTRACTOR SHALL USE EXTREME CARE DURING CONCRETE REMOVAL SO AS NOT TO DAMAGE THE PCC I-BEAM.

THE DECK SURFACE SHALL HAVE ITS FINAL FINISH ACCORDING TO ARTICLE 503.16(3)(b) OF THE STANDARD SPECIFICATIONS.

THE BRIDGE APPROACH SLABS SHALL HAVE ITS FINAL FINISH ACCORDING TO ARTICLE 420.09(e)(1) OF THE STANDARD SPECIFICATIONS.

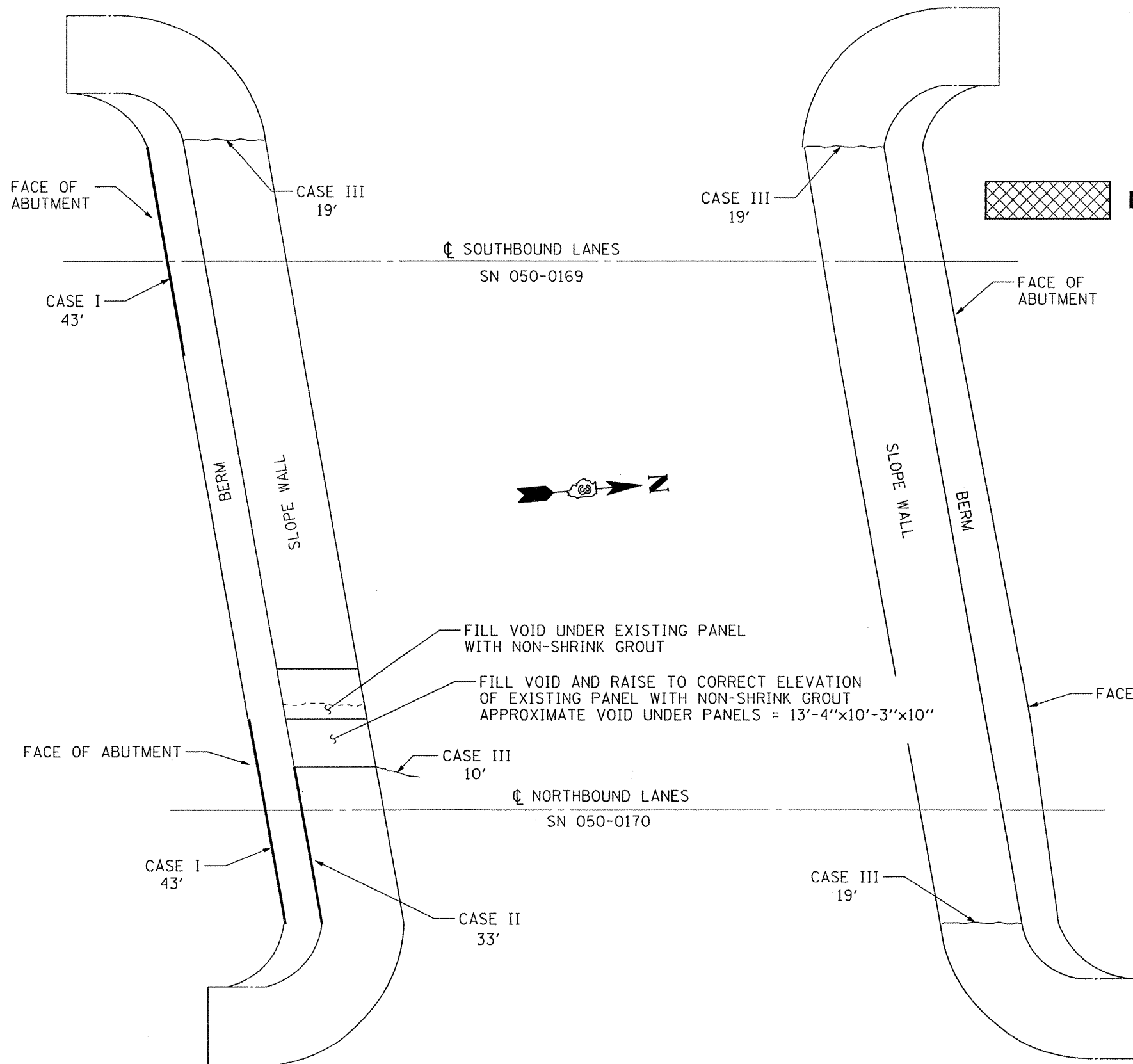
LABEL DESIGNATIONS: (BF) BACK FACE  
(FF) FRONT FACE  
(EF) EACH FACE



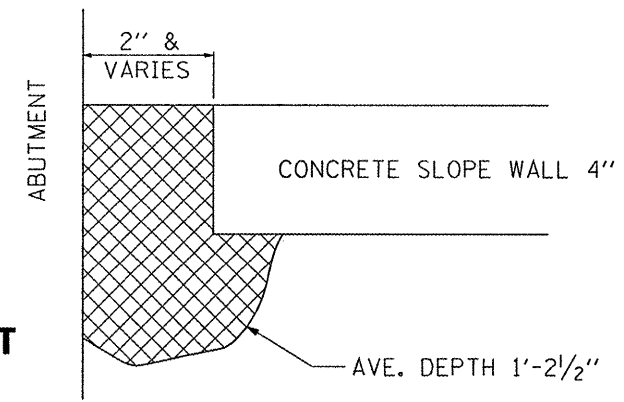
*Expires 11-30-2010*

**I-39 (FAI RTE 39) OVER  
THE LITTLE VERMILION RIVER  
SN 050-0169 (SB)  
SN 050-0170 (NB)**

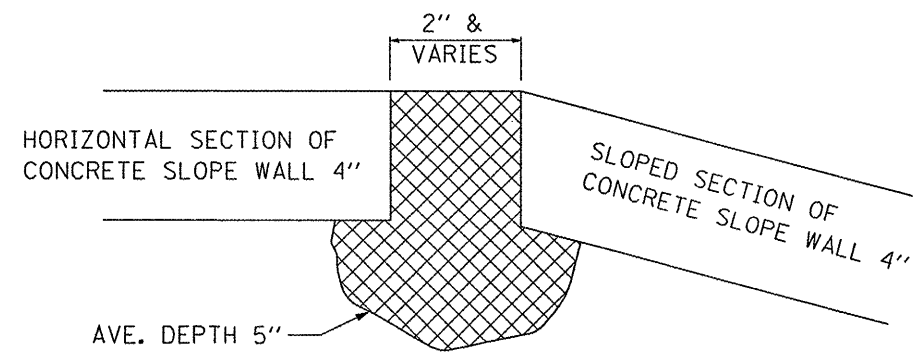
FILE NAME *	USER NAME * woodshankr1	DESIGNED - RON WOODSHANK	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL PLAN AND ELEVATION</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw_work\PW1001\WOODSHANKR1\dms91655	03-66898-details.dgn	DRAWN - RON WOODSHANK	REVISED -			39	(50-7B)-2	LASALLE	29	11	
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 66890					
	PLOT DATE = Mar 25, 2009 - 06:58:06 AM	DATE -	REVISED -			SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA. FED. ROAD DIST. NO. ILLINOIS					



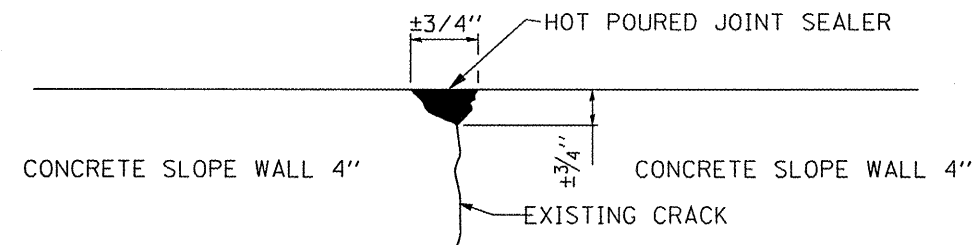
**NON-SHRINK GROUT**



**CASE I  
JOINT SEALER**



**CASE II  
JOINT SEALER**



**CASE III  
SLOPE WALL CRACK SEAL**

*BILL OF MATERIALS*

ITEM	UNIT	TOTAL QUANTITY	SN 050-169	
			(SB)	(NB)
SLOPE WALL CRACK SEALING	FOOT	67	38	29
JOINT SEALER	SQ YD	119	43	76
SLOPE WALL REPAIR	SQ YD	15		15

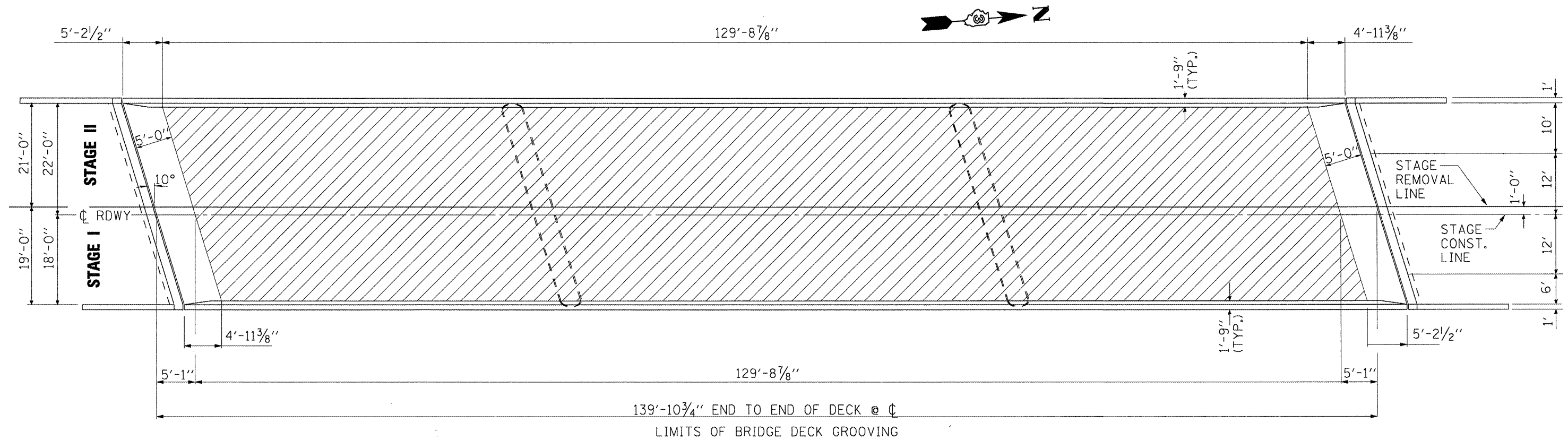
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c:\pwwork\PIWIDOT\WOODSHANKR1\dms91655	D3-66890-detail.dgn	DRAWN - RON WOODSHANK	REVISED -
	PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED -
	PLOT DATE = Apr 28, 2009 10:04:02 AM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SLOPEWALL REPAIR DETAILS**

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

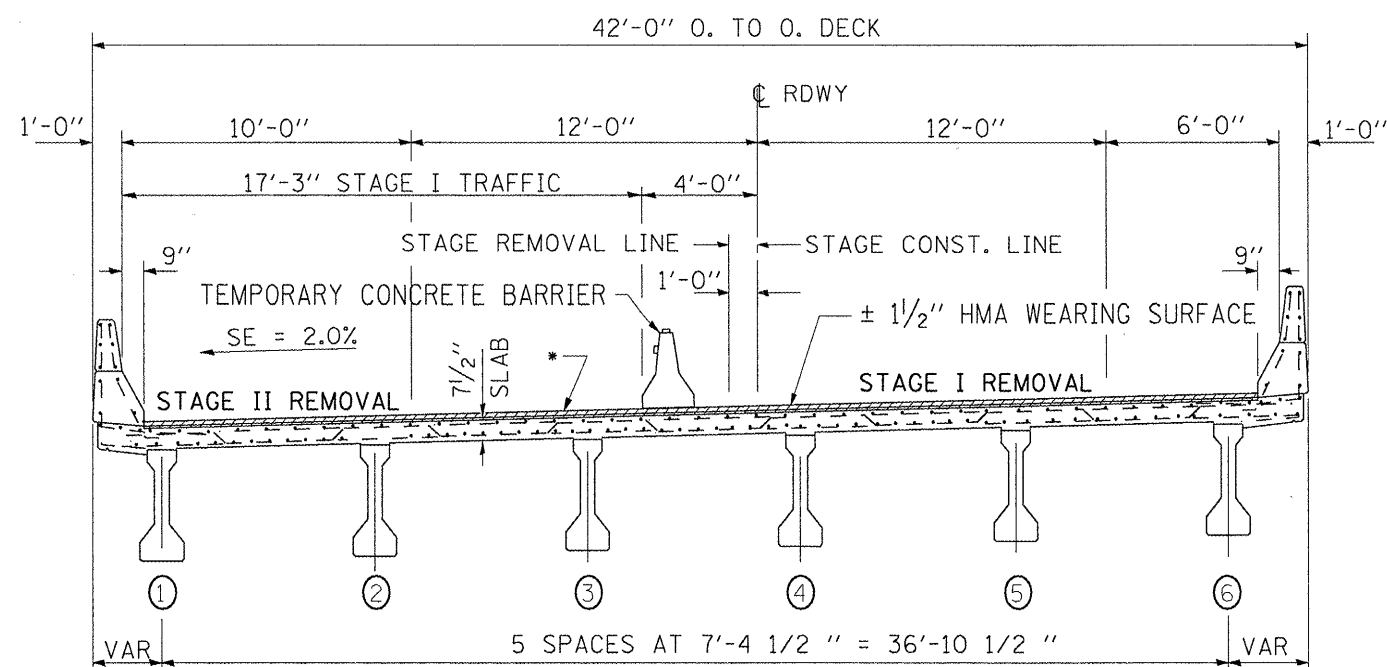
F.A.I. RITE:	SECTION	COUNTY	TOTAL SHEETS NO.
39	(50-7B)I-2	LASALLE	29 12
FED. ROAD DIST. NO. - ILLINOIS			CONTRACT NO. 66890



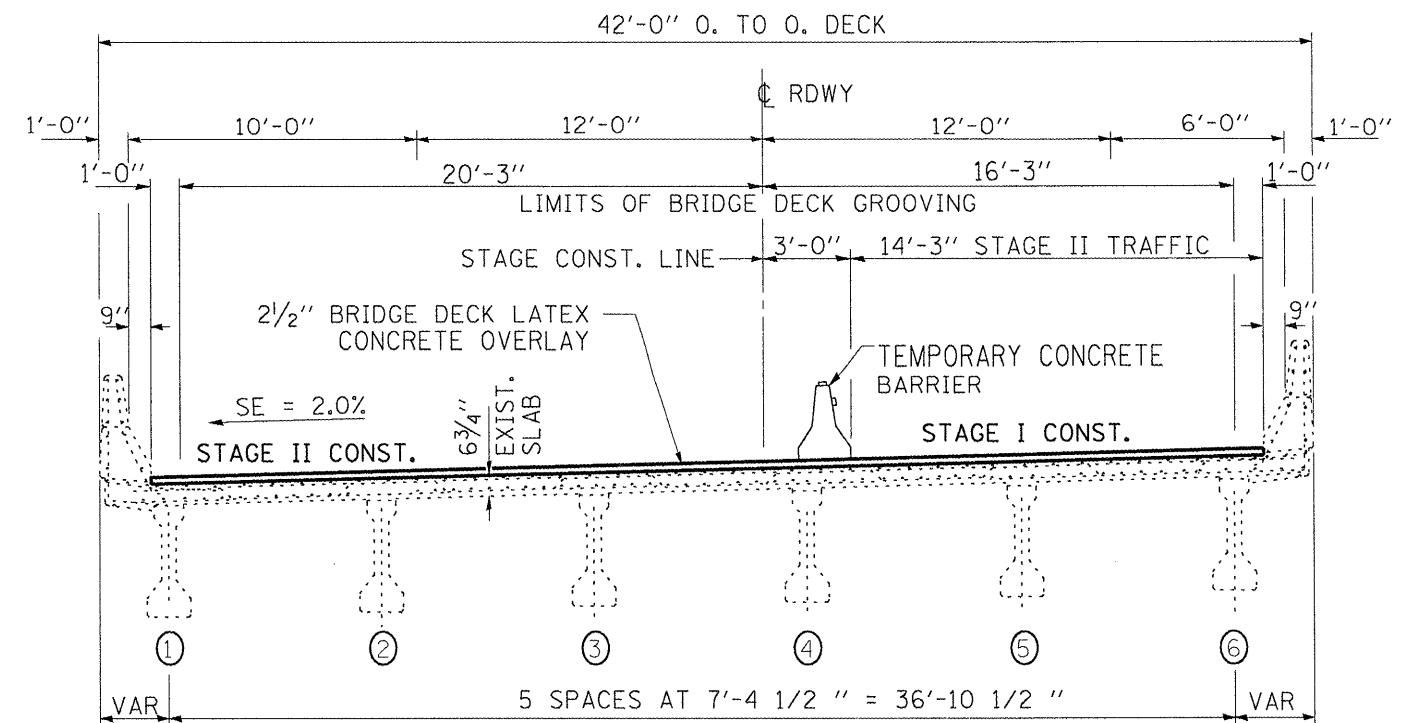
\* BRIDGE DECK HYDRO-SCARIFICATION DEPTH SHALL INCLUDE HMA OVERLAY AND CONCRETE DECK (1 1/2" HMA & 1" DECK)

**PLAN**  
**SN 050-0169 (SB)**  
LOOKING AHEAD

 **BRIDGE DECK HYDRO-SCARIFICATION 2 1/2"**

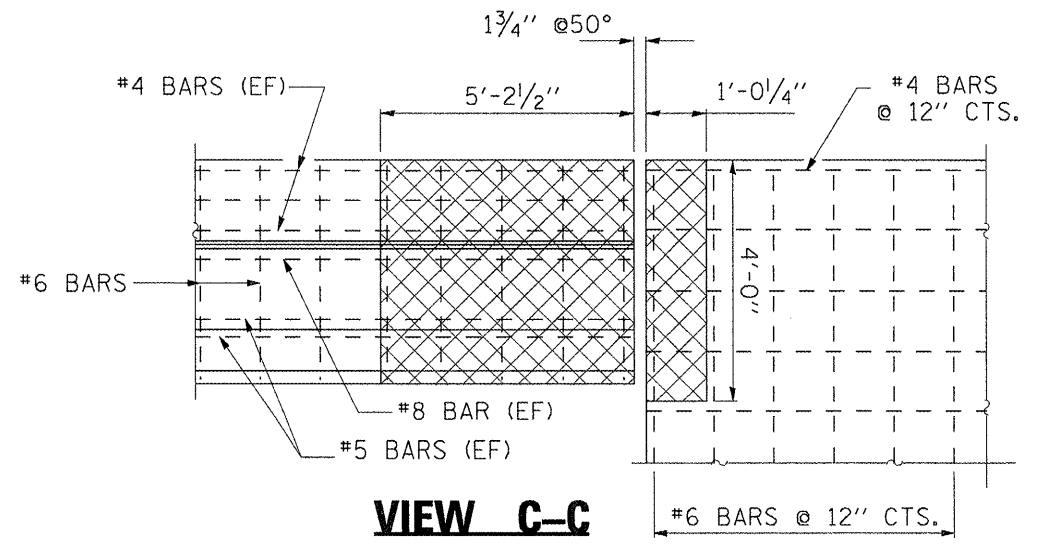
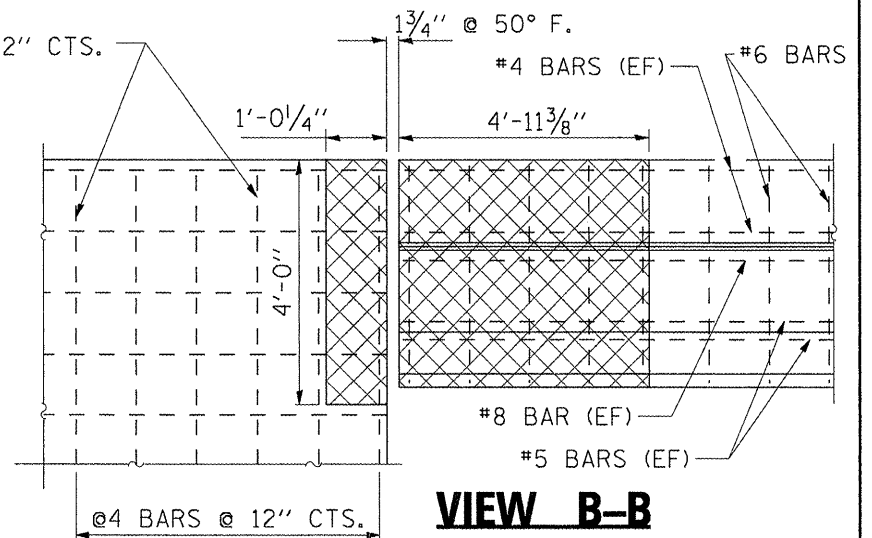
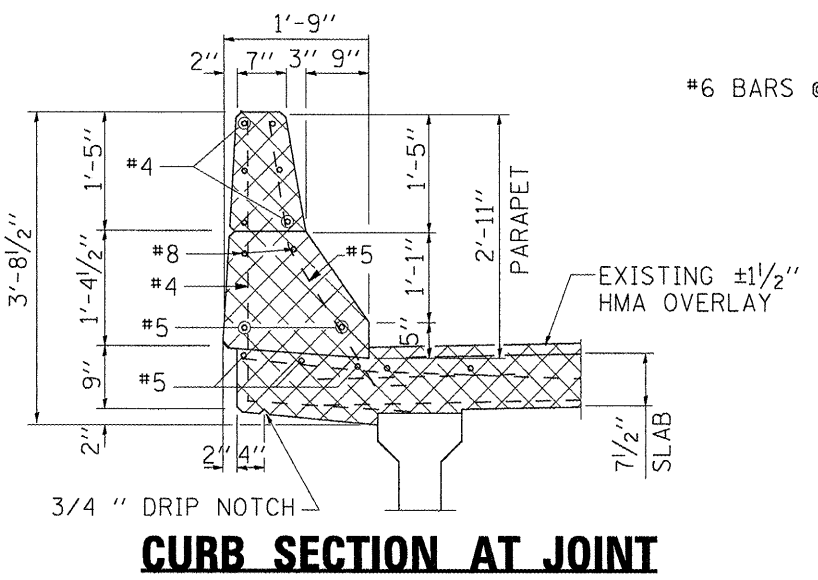
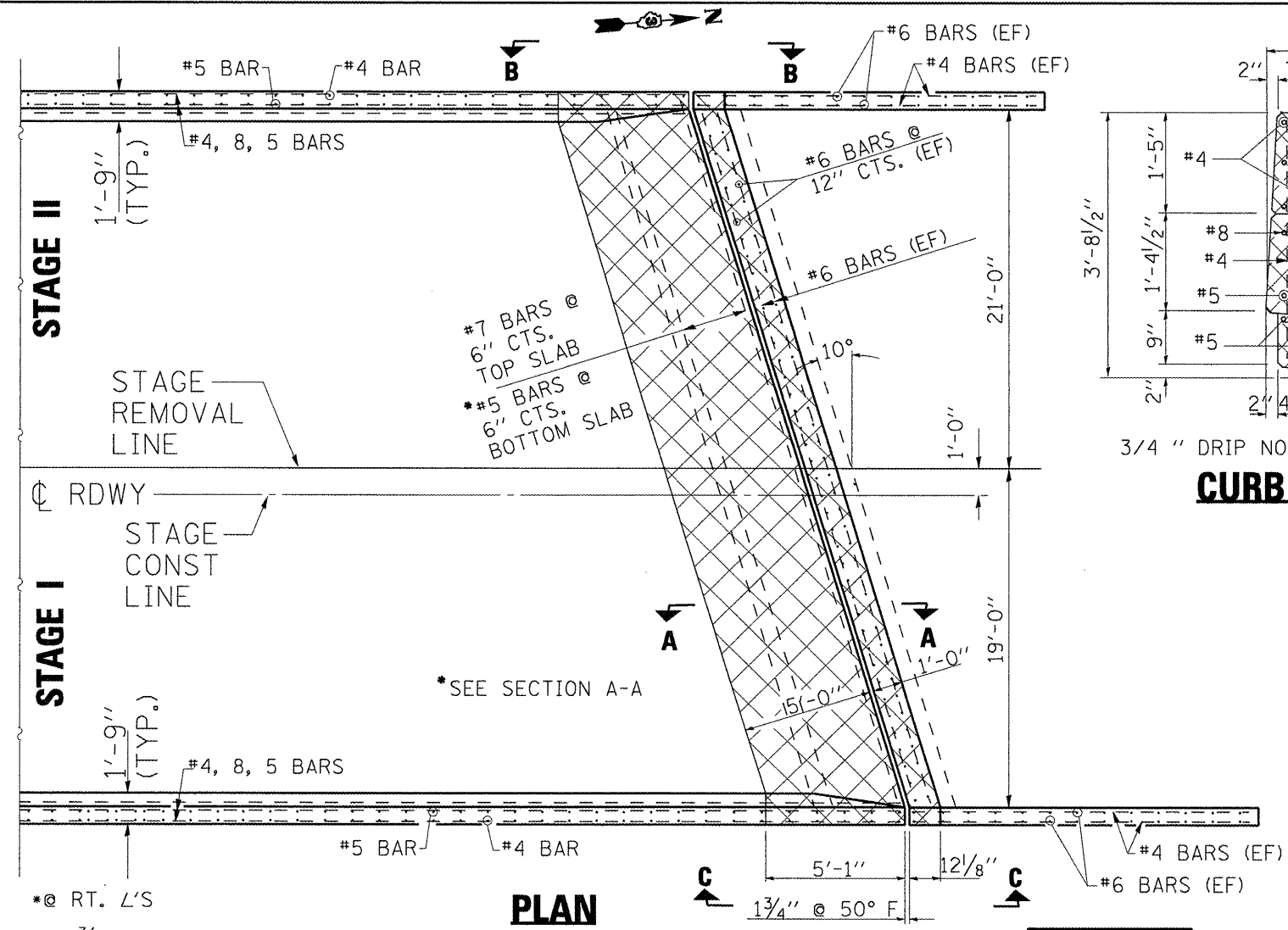


**EXISTING CROSS SECTION**  
LOOKING AHEAD

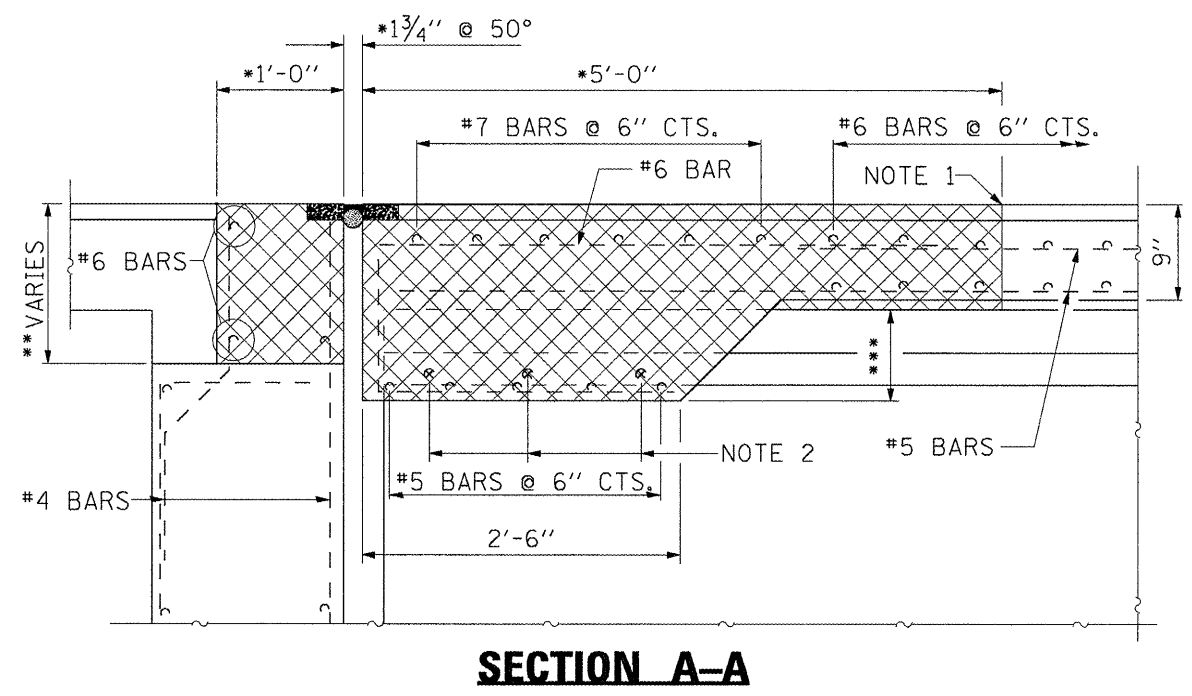
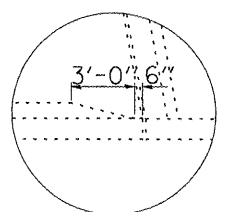


**PROPOSED CROSS SECTION**  
LOOKING AHEAD

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>SLAB DETAILS</b> <b>SN 050-0169 (SB)</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
cd:\pw\work\PIWIDOT\WOODSHANKR1\dms91655	03-66898-details.dgn	DRAWN - RON WOODSHANK	REVISED -			39	(50-78)I-2	LASALLE	29	13	
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 66890					
	PLOT DATE = Mar 25, 2009 - 06:58:17 AM	DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS					



NOTE 1: SAW CUT 3/4" MAX. INTO CONCRETE DECK  
NOTE 2: EXISTING THREADED RODS, SHALL BE STRAIGHTENED, CLEANED AND INCORPORATED INTO THE NEW CONCRETE



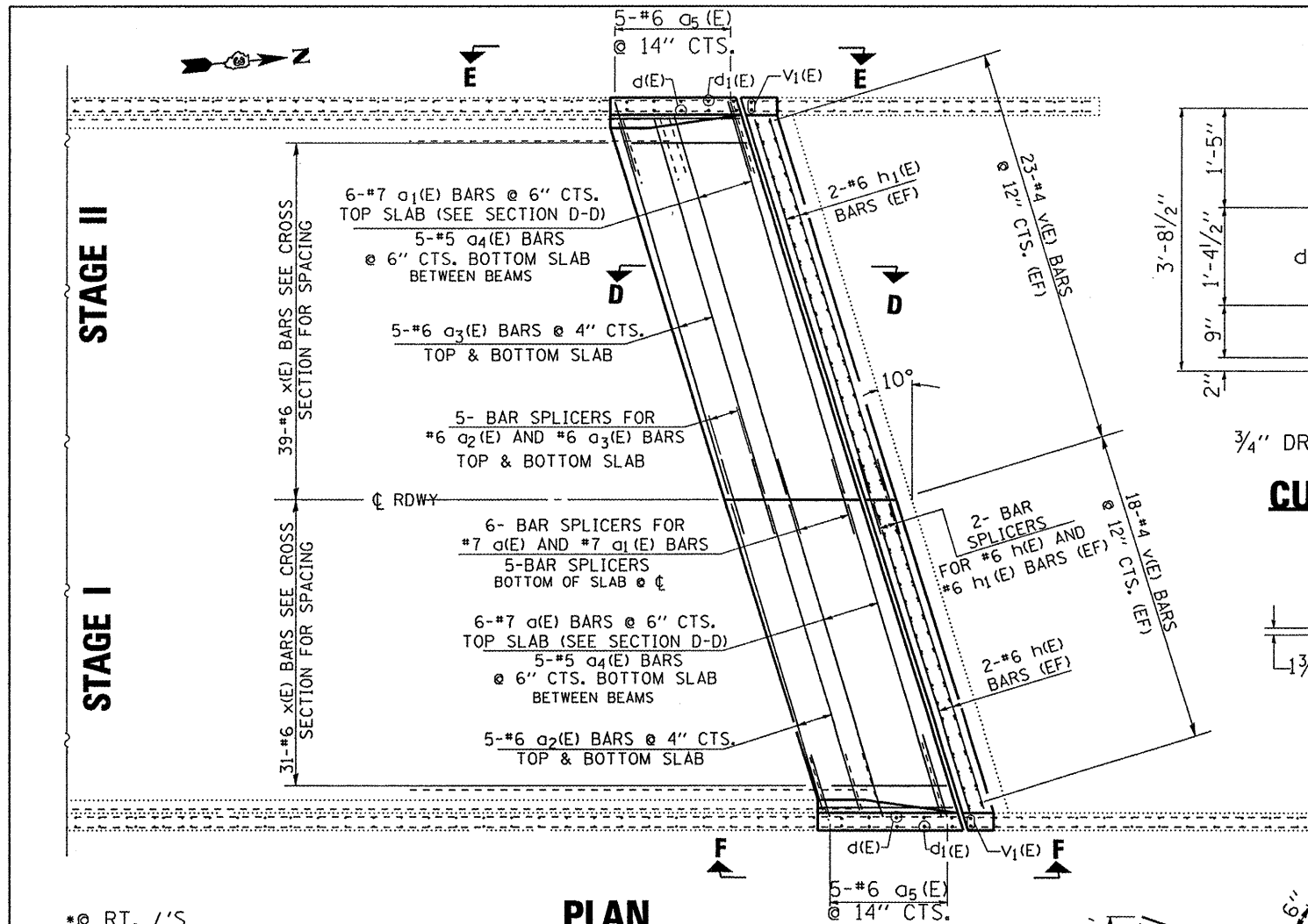
**BASIC BAR LAP**

BAR SIZE	LENGTH
#4	1'-4"
#5	1'-8"
#6	2'-0"

(EF) DENOTES "EACH FACE".

**BILL OF MATERIALS FOR ONE ABUTMENT**

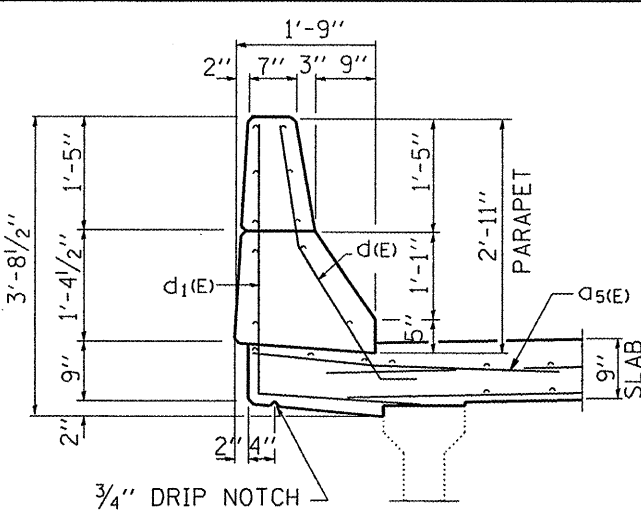
CONCRETE REMOVAL	CU YD	14
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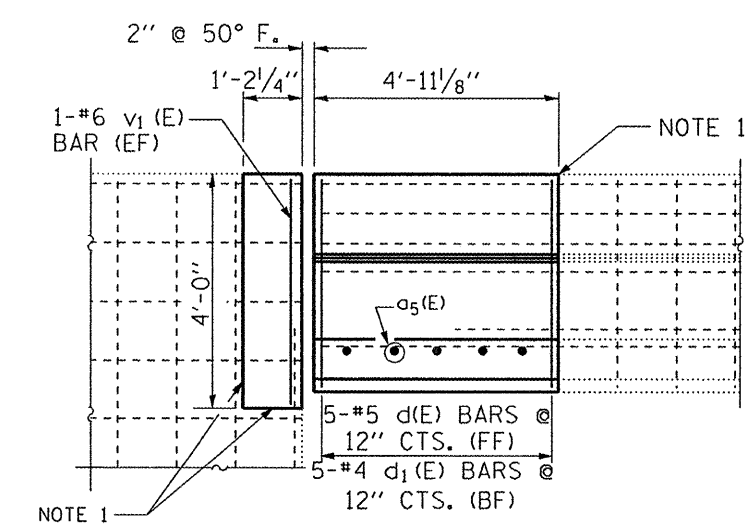
**PLAN**  
NORTH ABUTMENT SHOWN  
SOUTH ABUTMENT SIMILAR

- \* @ RT. L'S
- \*\* 1'-9 3/4" TO 1'-3" S. ABUT.  
1'-10" TO 1'-3" N. ABUT.
- \*\*\* 11" @ FASCIA BEAMS

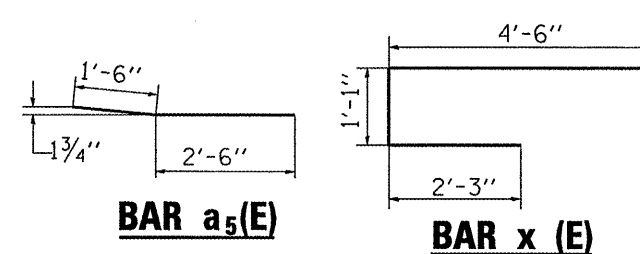
NOTE 1: BONDED CONSTRUCTION JOINT



**CURB SECTION AT JOINT**

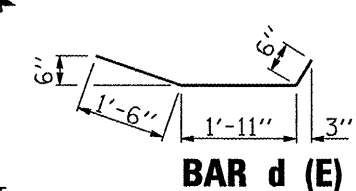


**VIEW E-E**



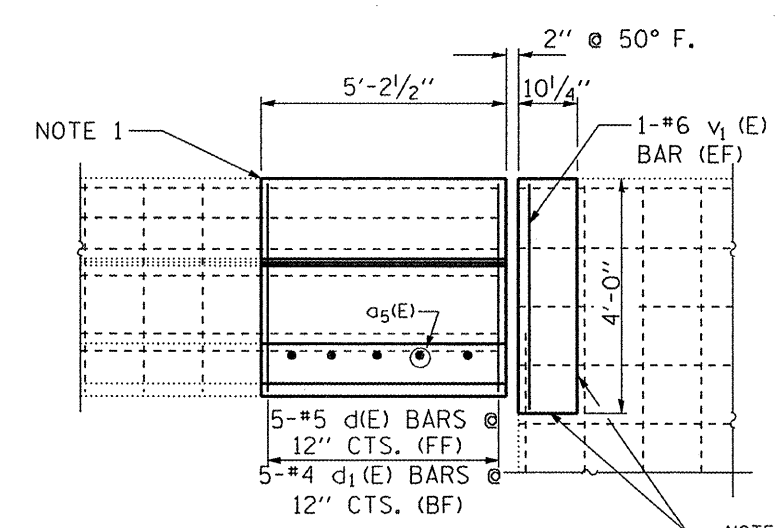
**BAR a5(E)**

**BAR x(E)**



**BAR d1(E)**

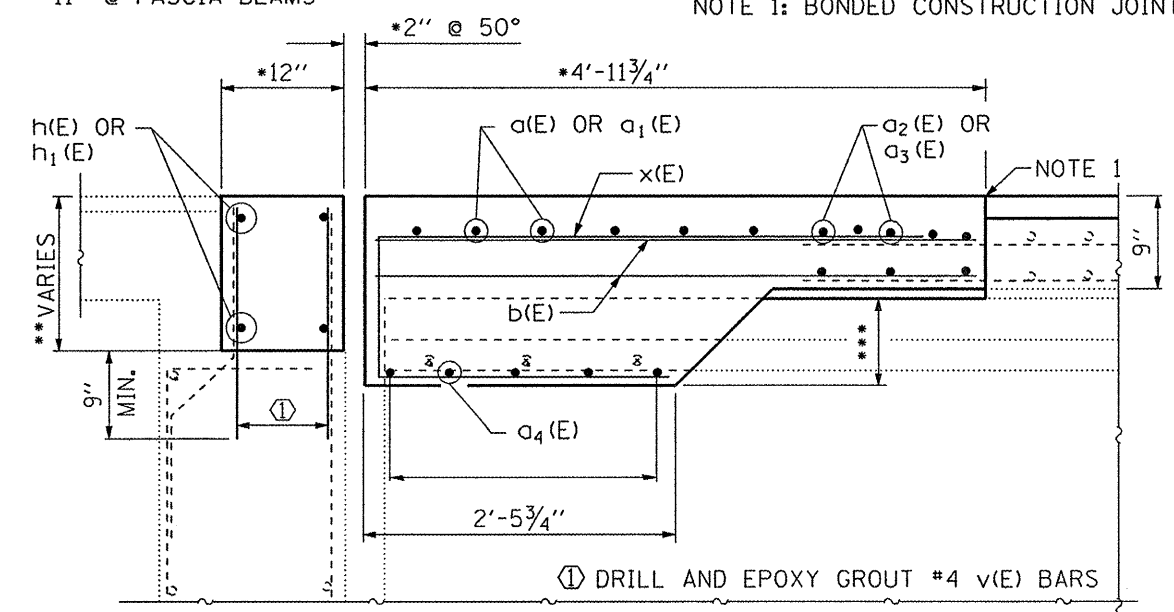
**BAR d(E)**



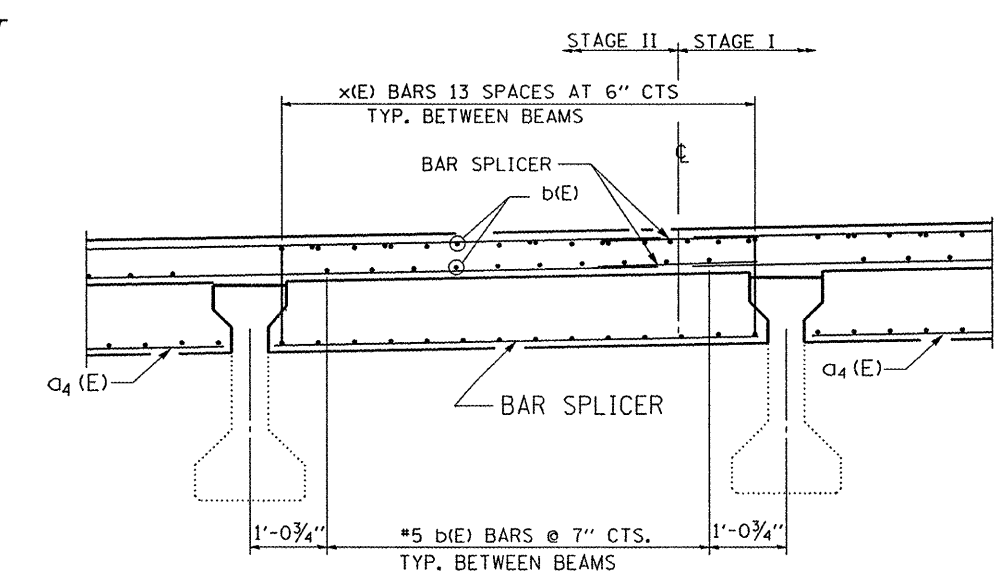
**VIEW F-F**

**BILL OF MATERIALS FOR ONE ABUTMENT**

BAR	NO. BARS	SIZE	LENGTH	SHAPE
a(E)	6	7	18'-0"	—
a1(E)	6	7	22'-1"	—
a2(E)	10	6	18'-0"	—
a3(E)	10	6	22'-1"	—
a4(E)	20	5	6'-7"	—
a5(E)	10	6	4'-0"	—
d(E)	10	5	3'-11"	—
d1(E)	10	4	5'-4"	—
h(E)	4	6	18'-0"	—
h1(E)	4	6	22'-1"	—
v(E)	82	4	2'-5"	—
v1(E)	4	6	3'-9"	—
x(E)	70	6	7'-10"	—
REINFORCEMENT BARS EPOXY COATED			POUNDS	2586
CONCRETE STRUCTURE (ABUTS.)			CU YD	3
CONCRETE SUPERSTRUCTURE			CU YD	11

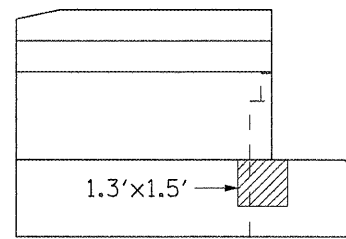


**SECTION D-D**

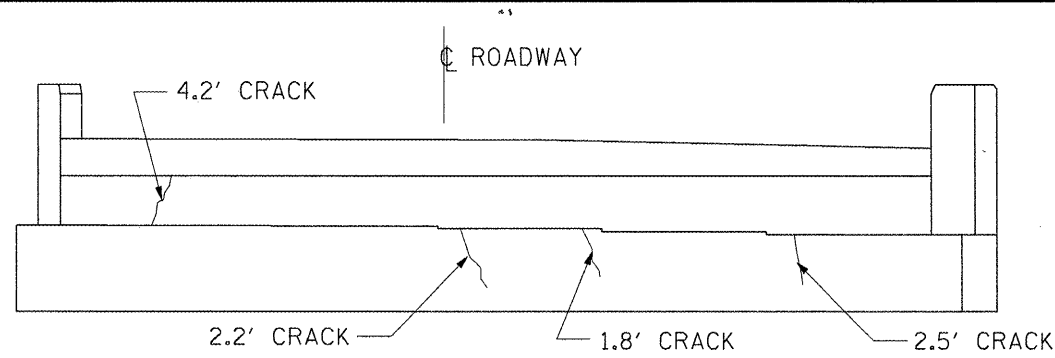


**CROSS SECTION AT END OF DECK**

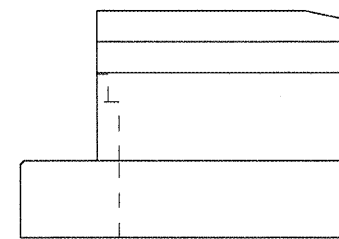




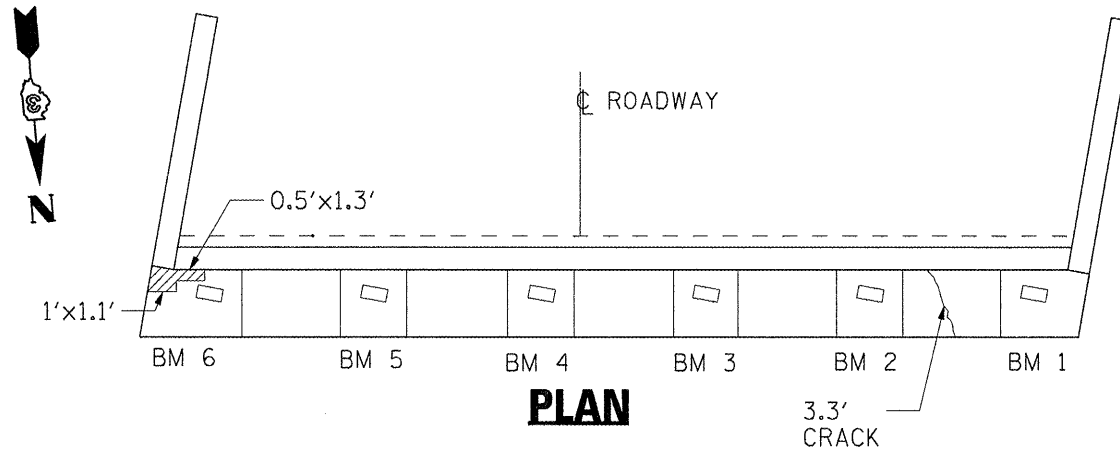
**EAST ELEVATION**



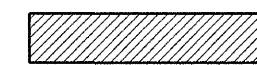
**ELEVATION**



**WEST ELEVATION**

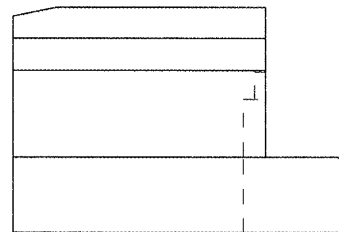


**PLAN**

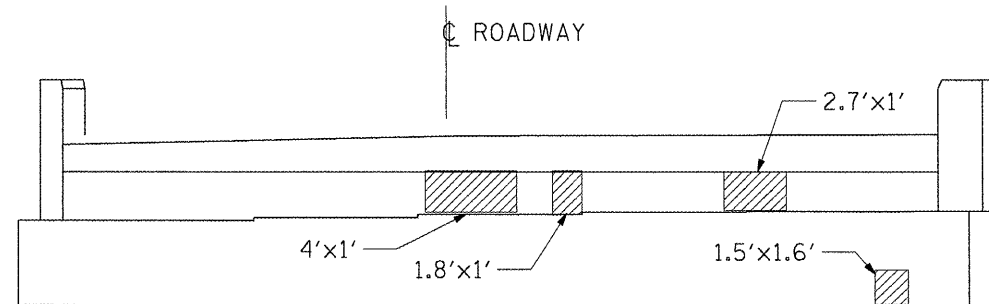


**STRUCTURAL REPAIR OF CONCRETE  
(DEPTH EQUAL TO OR LESS THEN 5")**

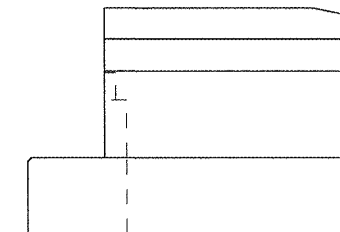
**S. ABUTMENT DETAIL**



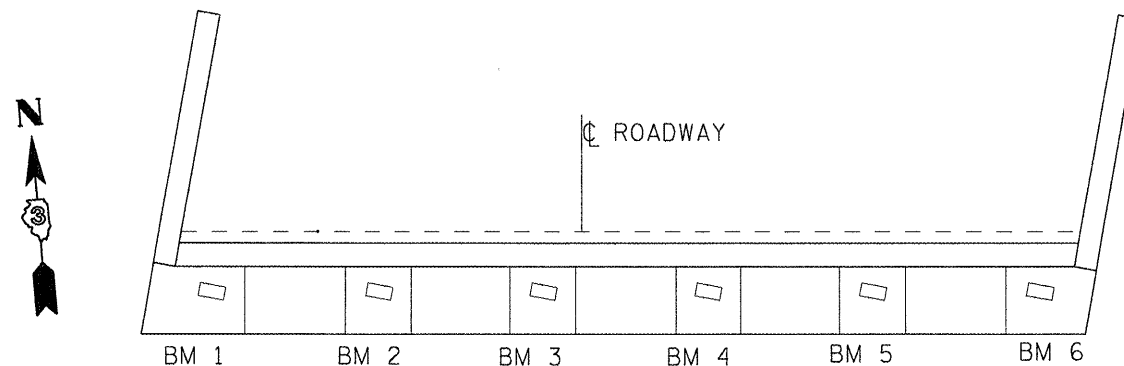
**WEST ELEVATION**



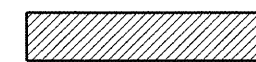
**ELEVATION**



**EAST ELEVATION**



**PLAN**



**STRUCTURAL REPAIR OF CONCRETE  
(DEPTH EQUAL TO OR LESS THEN 5")**

**EPOXY CRACK INJECTION**

**N. ABUTMENT DETAILS**

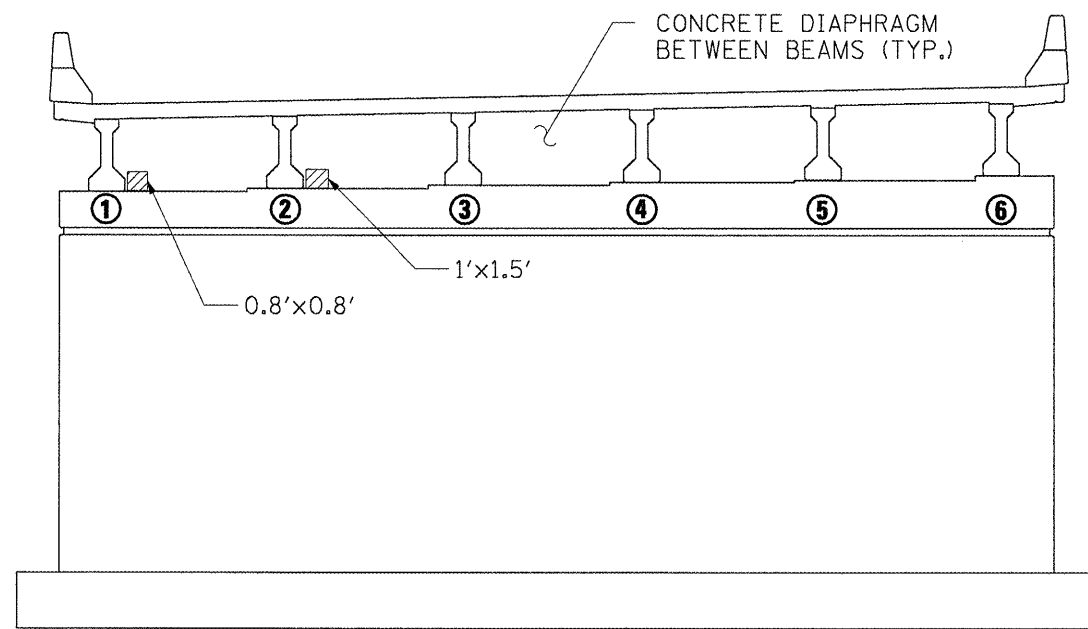
FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -
c:\pwork\pwwid\WOODSHANKRL\dms91655	03-66890-details.dgn	DRAWN - RON WOODSHANK	REVISED -
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = Mar 25, 2009 - 06:58:33 AM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

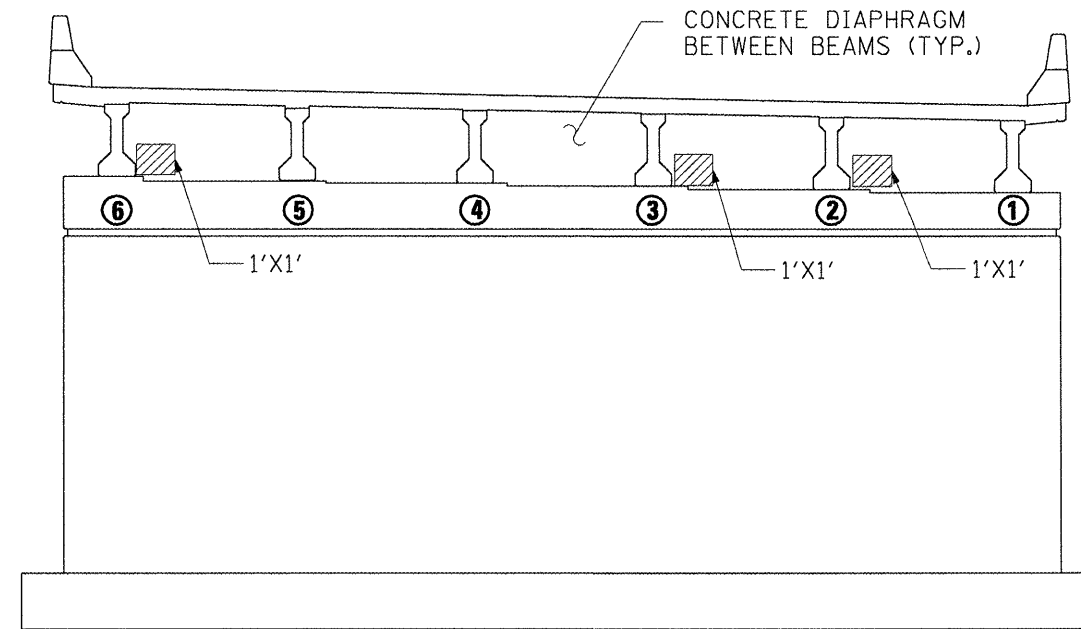
**ABUTMENT DETAILS  
SN 050-0169 (SB)**

SCALE: SHEET NO. 4 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(50-7B)I-2	LASALLE	29	16
FED. ROAD DIST. NO. ILLINOIS			CONTRACT NO. 66890	



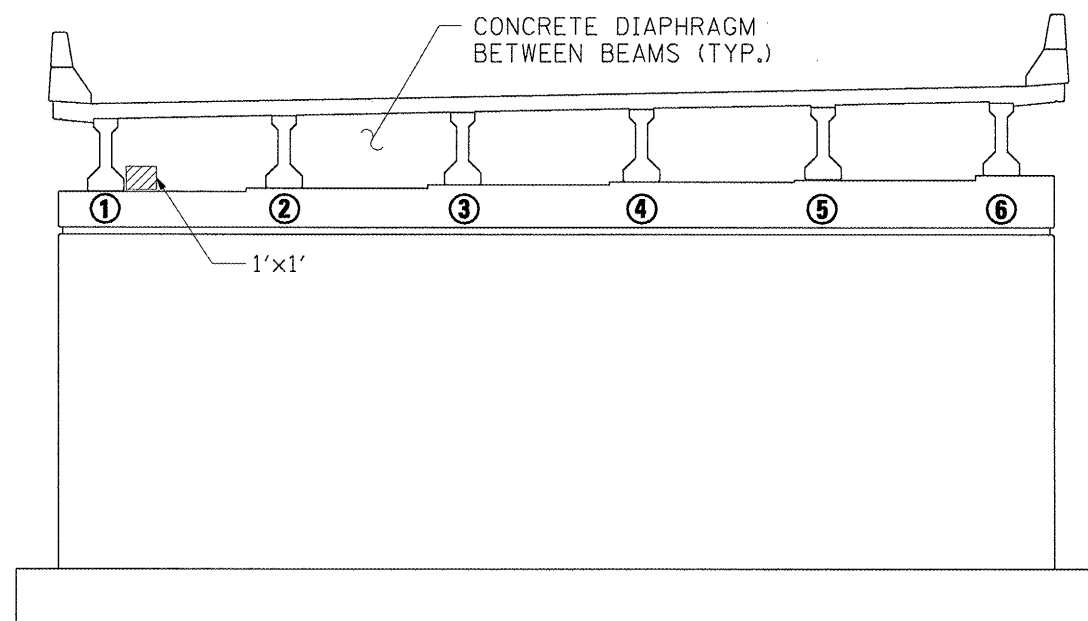
**LOOKING AHEAD**  
**SOUTH FACE**



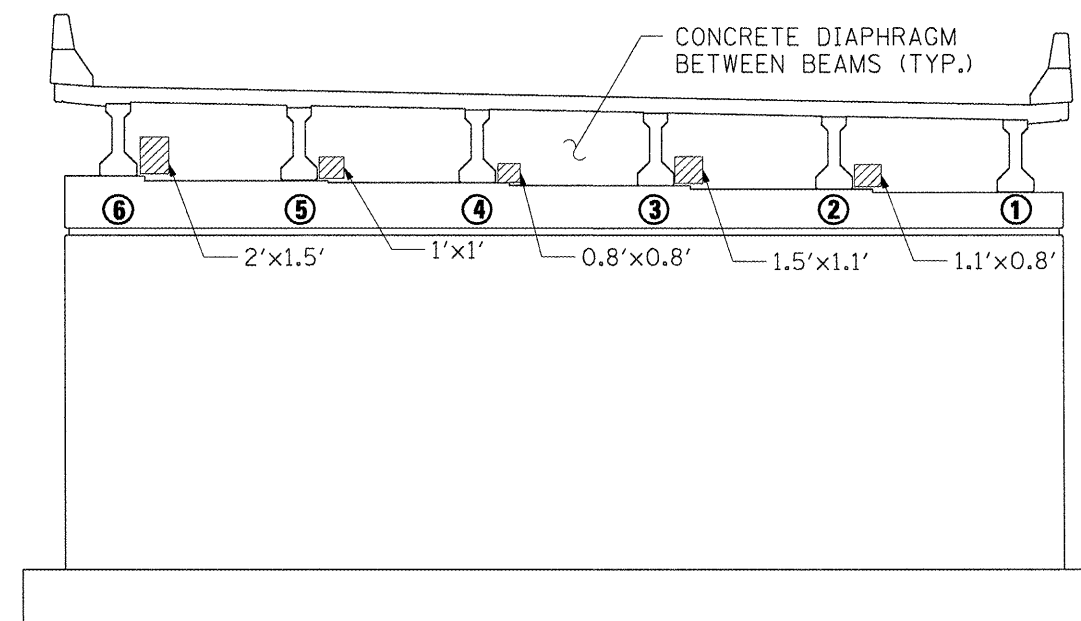
**LOOKING BACK**  
**NORTH FACE**

 **STRUCTURAL REPAIR OF CONCRETE**  
**(DEPTH EQUAL TO OR LESS THEN 5")**

**PIER 1**



**LOOKING AHEAD**  
**SOUTH FACE**



**LOOKING BACK**  
**NORTH FACE**

 **STRUCTURAL REPAIR OF CONCRETE**  
**(DEPTH EQUAL TO OR LESS THEN 5")**

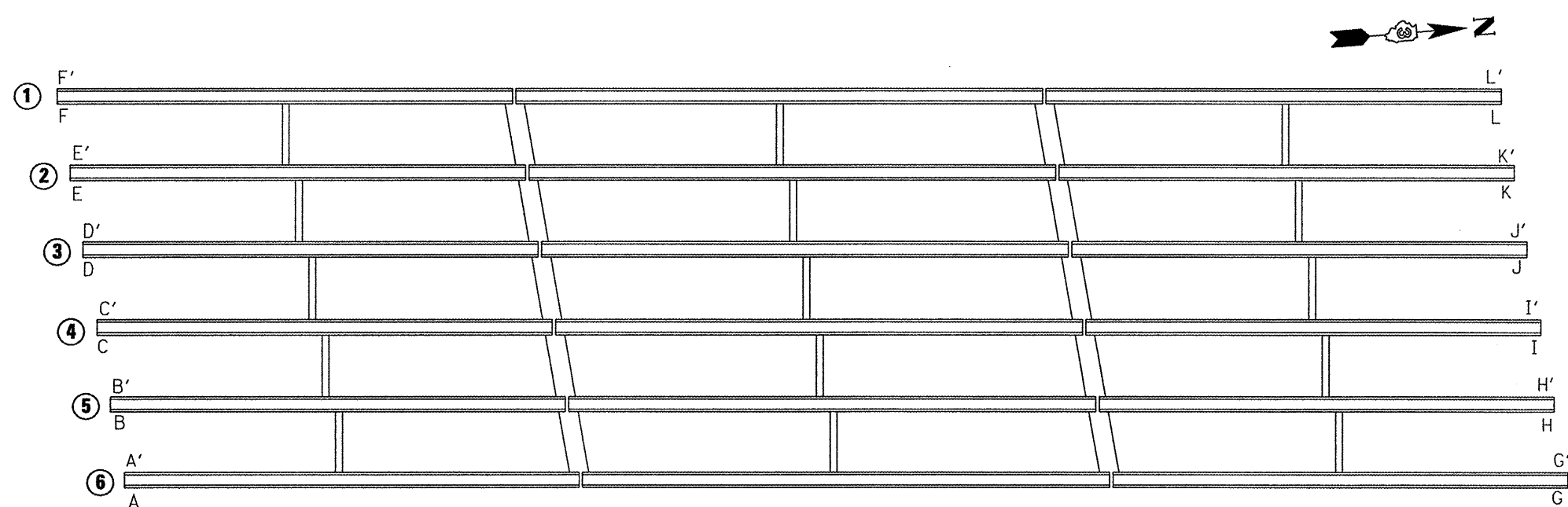
**PIER 2**

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	PLOT DATE = Mar 25, 2009 - 06:58:39 AM	DATE -	REVISED -

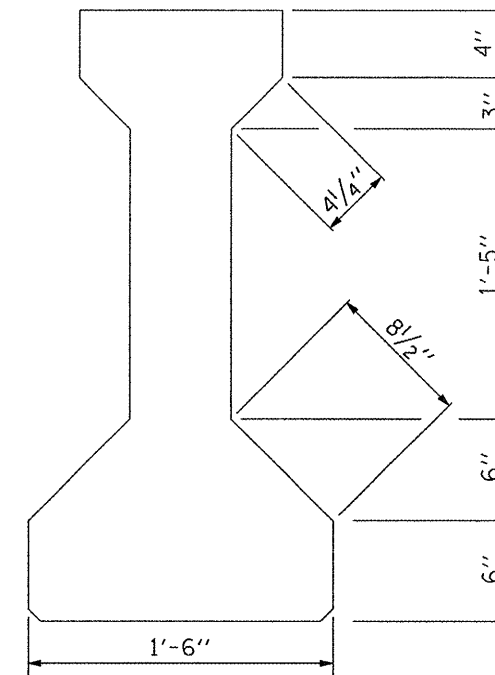
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PIER DETAILS**  
**SN 050-0169 (SB)**

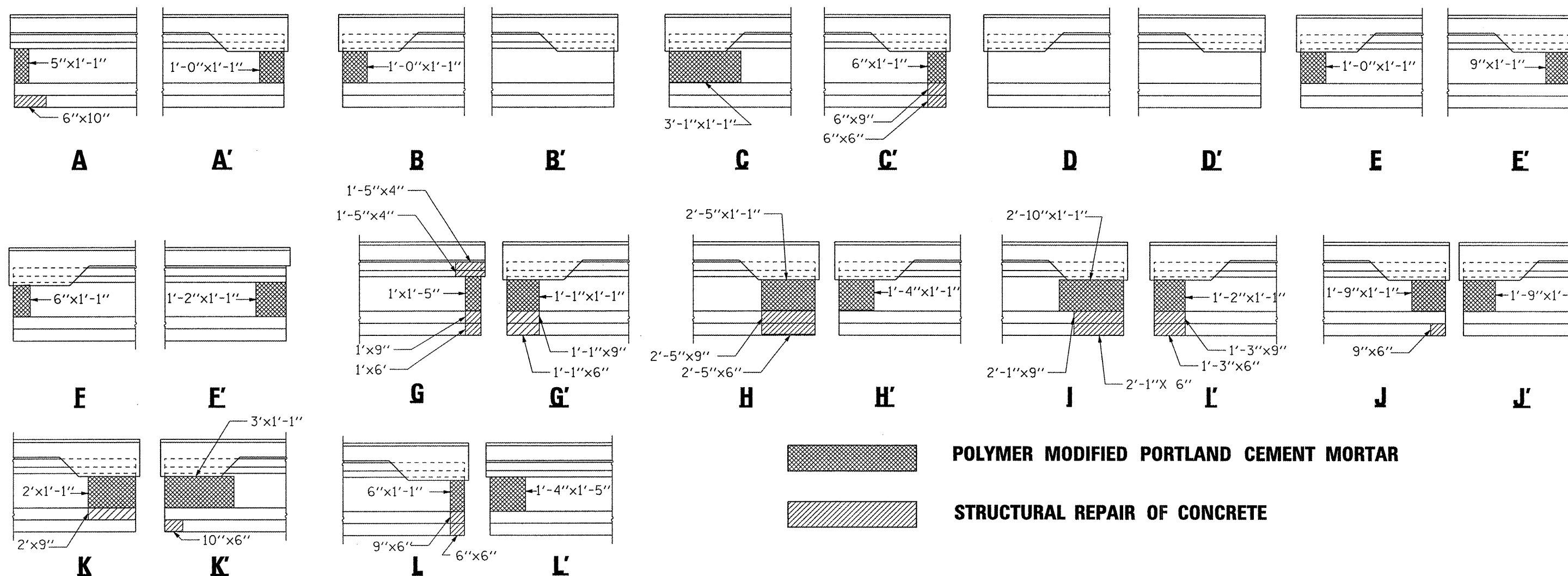
SCALE: _____	SHEET NO. 5 OF 7 SHEETS	STA. _____ TO STA. _____	F.A.I. RTE. 39	SECTION (50-7B)I-2	COUNTY LASALLE	TOTAL SHEETS 29	SHEET NO. 17
			CONTRACT NO. 66890				
			FED. ROAD DIST. NO. ILLINOIS				



**FRAMING PLAN  
SN 0169 (SB)**



**BEAM DETAIL**



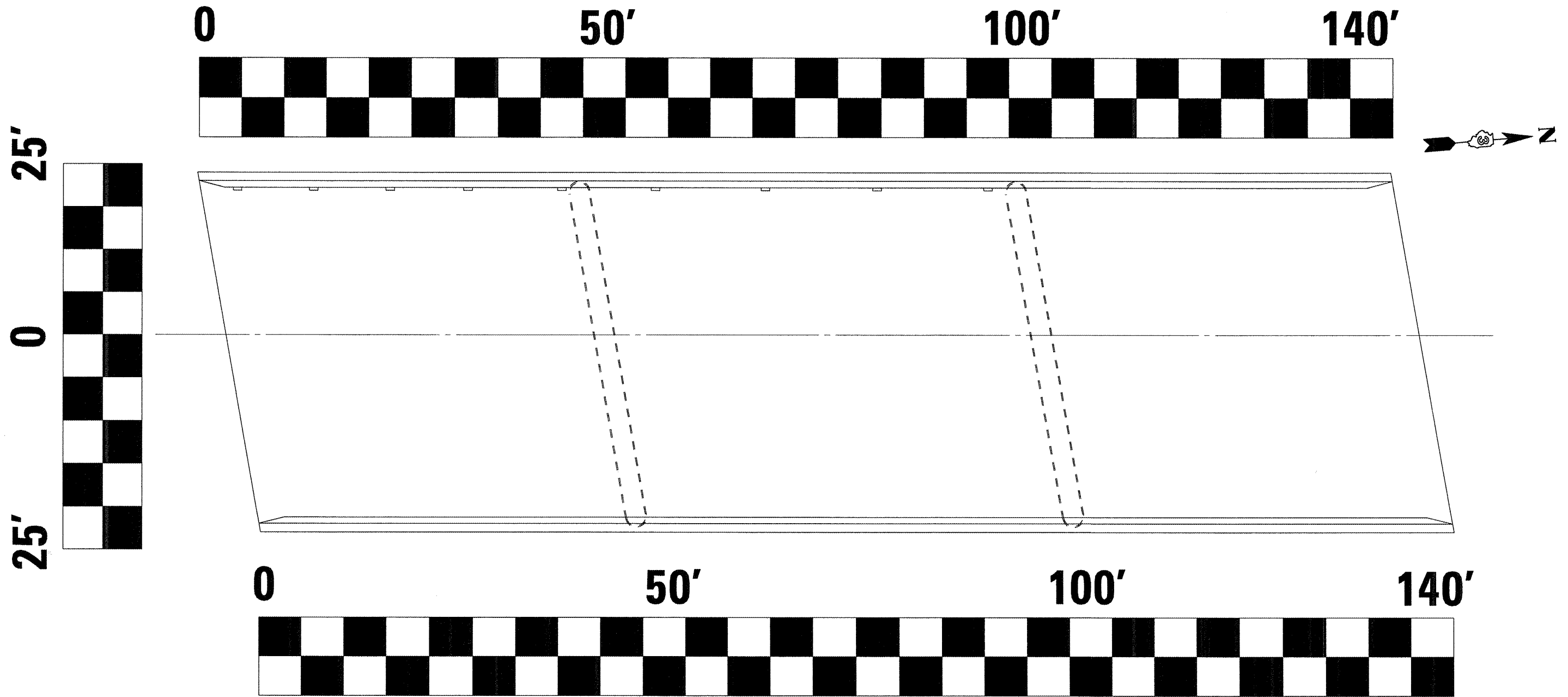
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	PLOT DATE = Apr 28, 2009 - 10:12:29 AM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FRAMING PLAN  
SN 050-0169 (SB)**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(50-7B)-2	LASALLE	29	18
FED. ROAD DIST. NO. - ILLINOIS			CONTRACT NO. 66890	

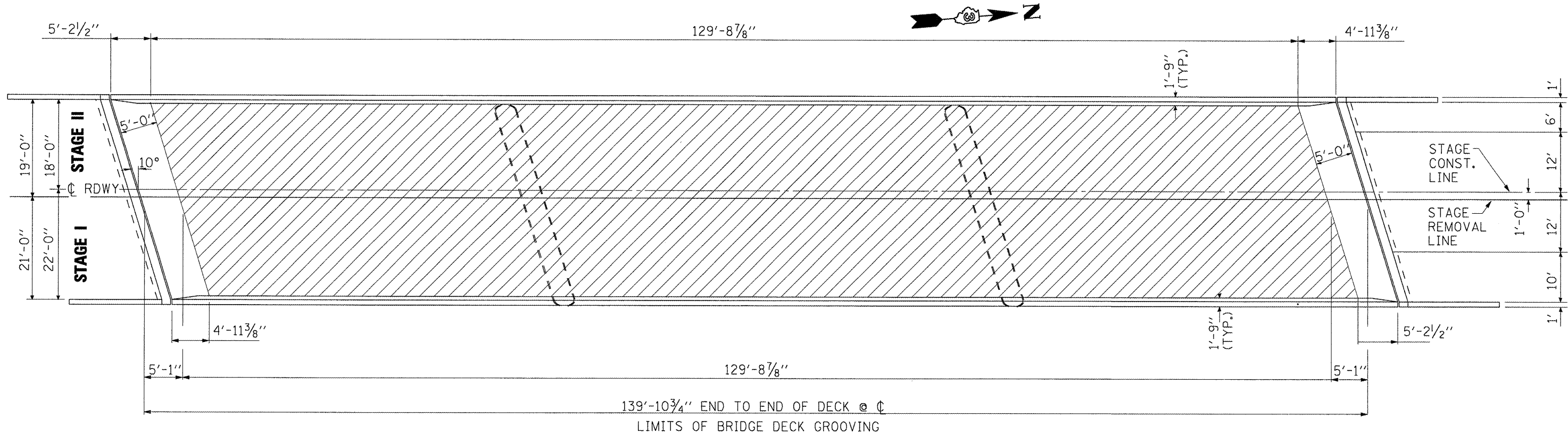
SCALE: \_\_\_\_\_ SHEET NO. 6 OF 7 SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_



**SN 050-0169 SB**

**SCALE 1" = 50'**

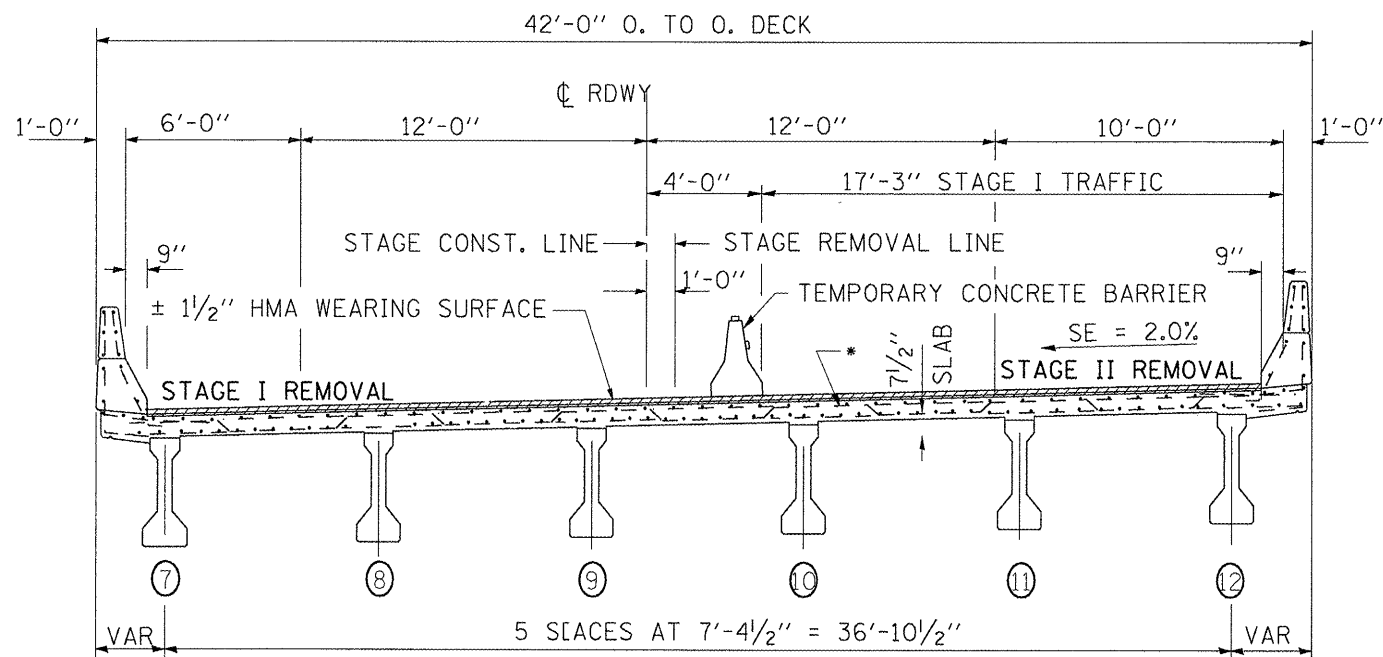
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	03-66898-details.dgn	DRAWN - RON WOODSHANK	REVISED -			39	(50-78)-2	LASALLE	29	19
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 66890				
	PLOT DATE = Mar 25, 2009 - 06:58:49 AM	DATE -	REVISED -			FED. ROAD DIST. NO. - [ILLINOIS]				



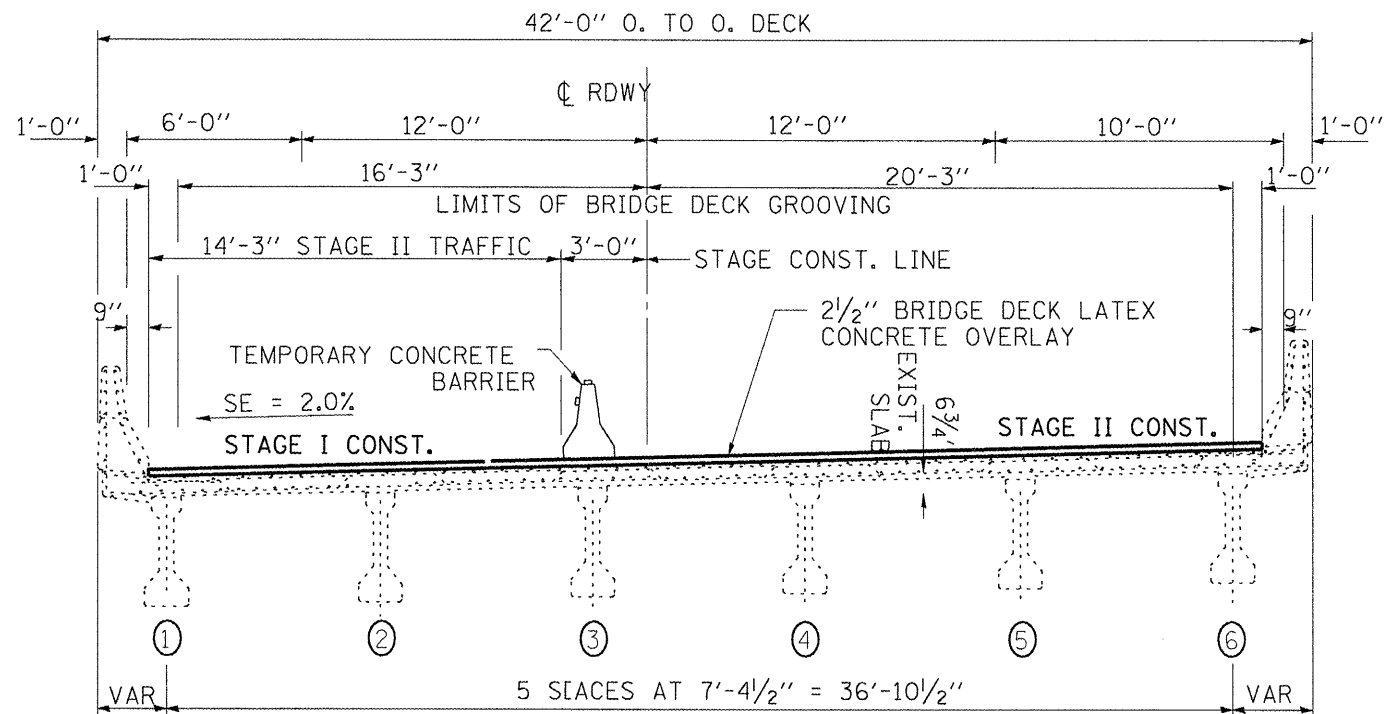
\* BRIDGE DECK HYDRO-SCARIFICATION DEPTH SHALL INCLUDE HMA OVERLAY AND CONCRETE DECK (1 1/2" HMA & 1" DECK)

**PLAN**  
**SN 050-0170 (NB)**  
LOOKING AHEAD

**BRIDGE DECK HYDRO-SCARIFICATION 2 1/2"**



**EXISTING CROSS SECTION**  
LOOKING AHEAD



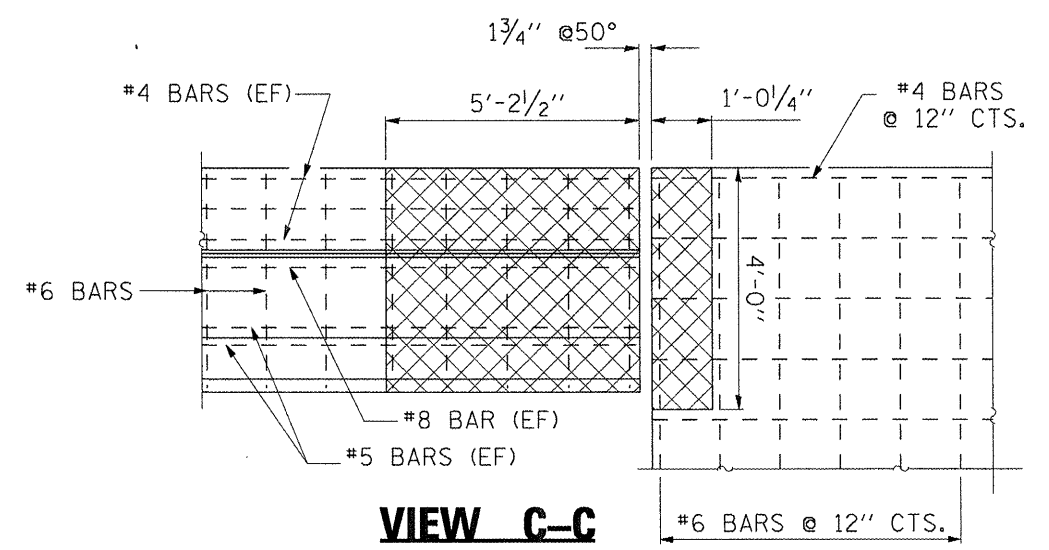
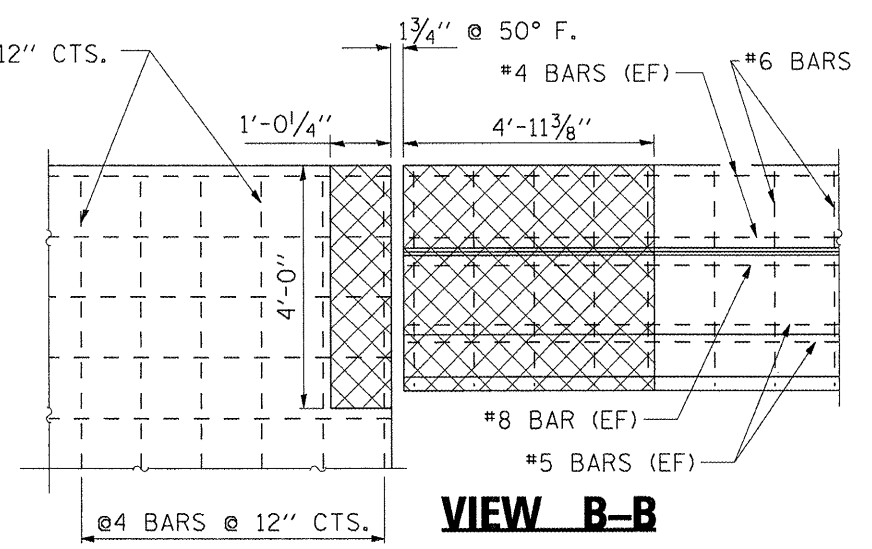
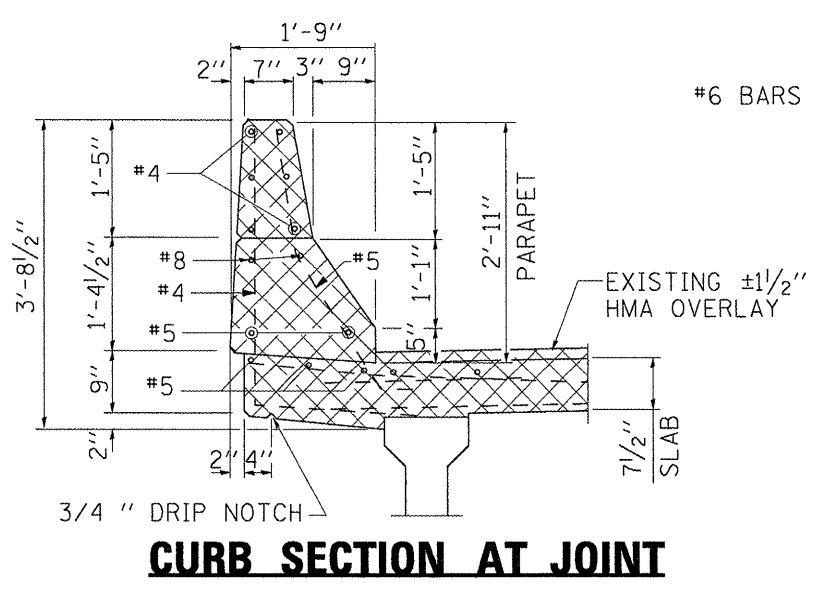
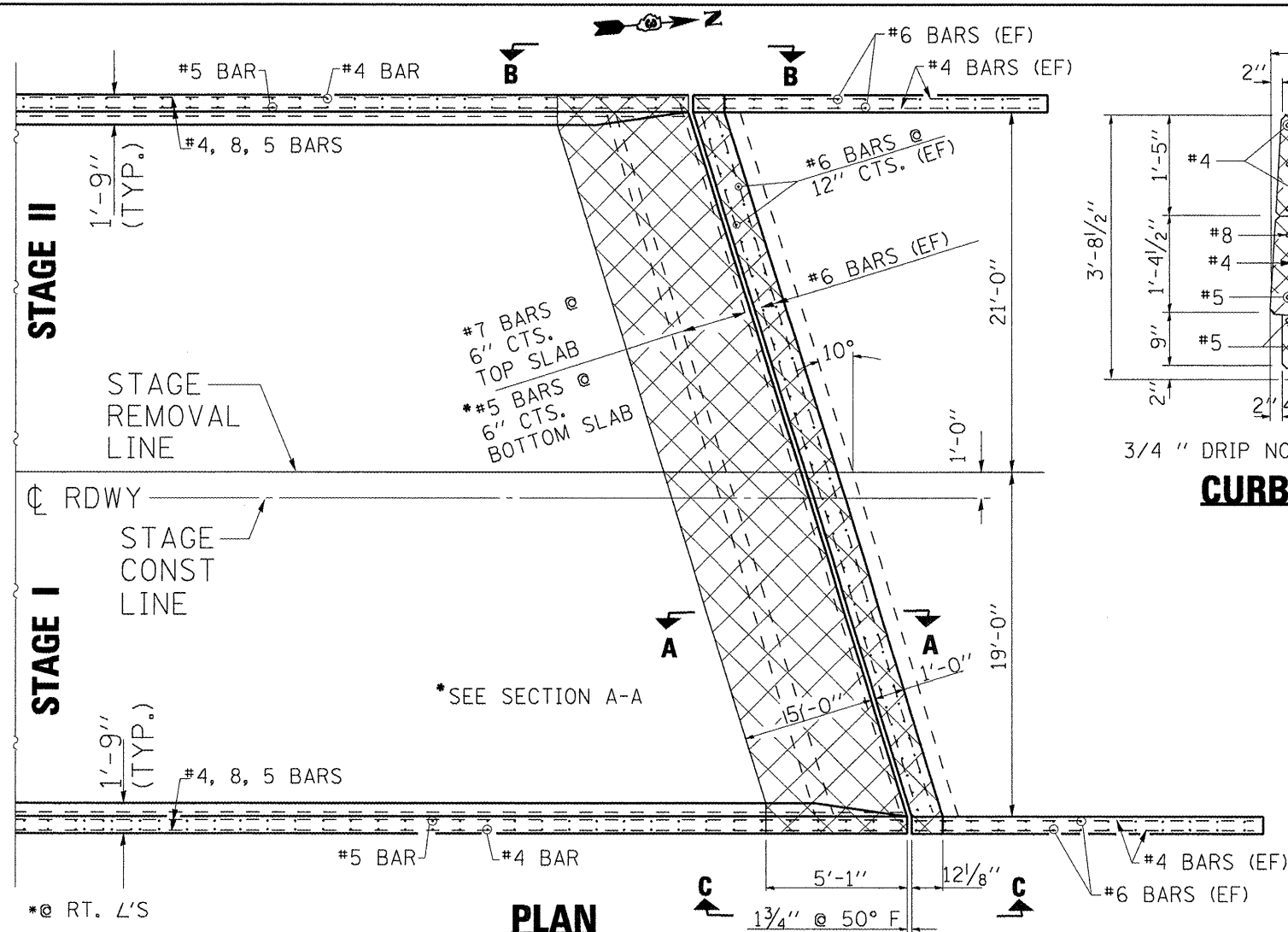
**EXISTING CROSS SECTION**  
LOOKING AHEAD

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -
c:\pwwork\FW100T\WOODSHANKR1\dms91655	03-66898-details.dgn	DRAWN - RON WOODSHANK	REVISED -
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

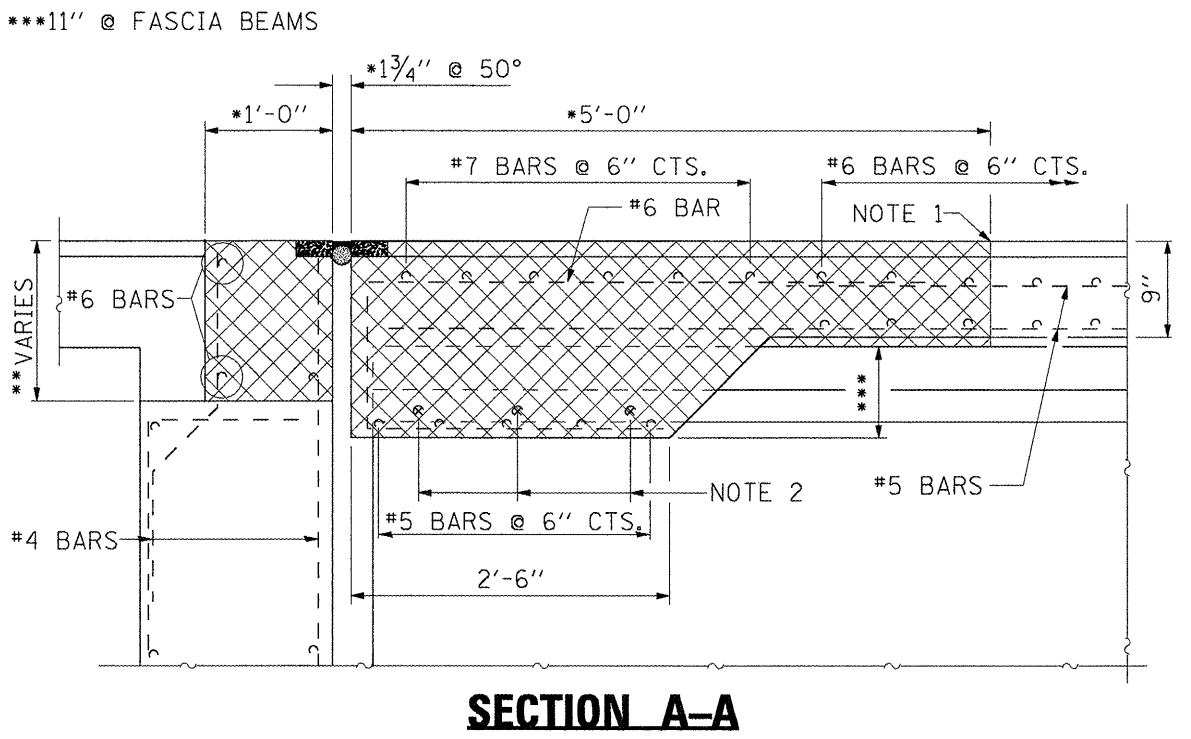
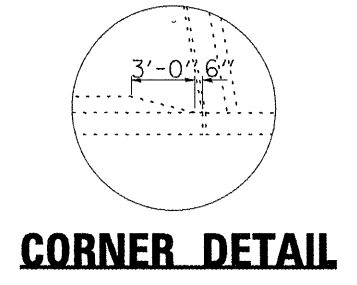
**SLAB DETAILS**  
**SN 050-0170 (NB)**

SCALE: _____	SHEET NO. 1 OF 8 SHEETS	STA. _____ TO STA. _____	F.A.I. RTE. 39	SECTION (50-7B)(-2)	COUNTY LASALLE	TOTAL SHEETS 29	SHEET NO. 20
			CONTRACT NO. 66890				



CONCRETE REMOVAL

NOTE 1: SAW CUT 3/4" MAX. INTO CONCRETE DECK  
 NOTE 2: EXISTING THREADED RODS, SHALL BE STRAIGHTENED, CLEANED AND INCORPORATED INTO THE NEW CONCRETE



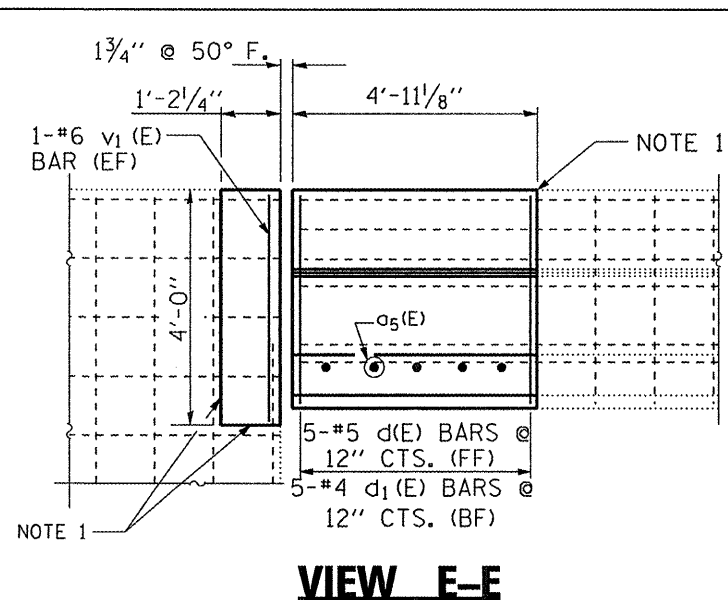
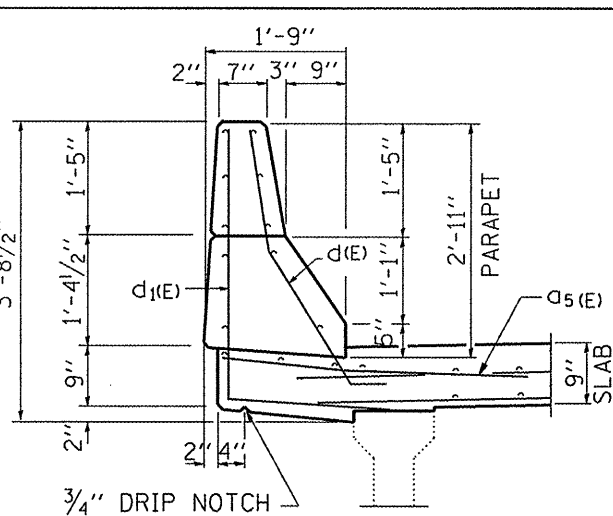
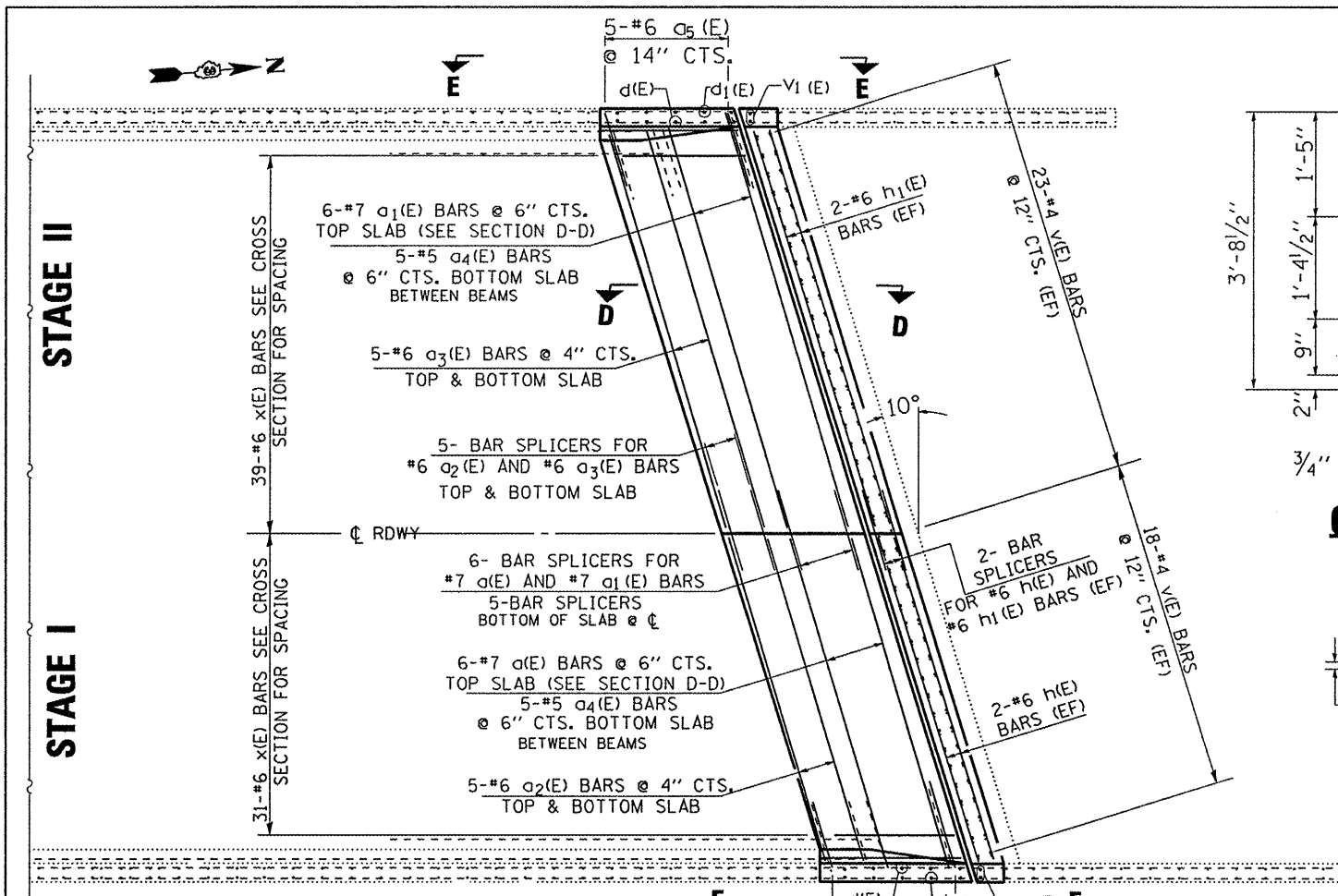
**BASIC BAR LAP**

BAR SIZE	LENGTH
#4	1'-4"
#5	1'-8"
#6	2'-0"

(EF) DENOTES "EACH FACE".

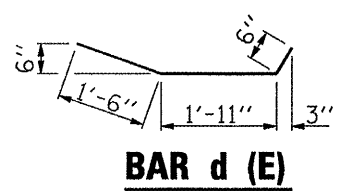
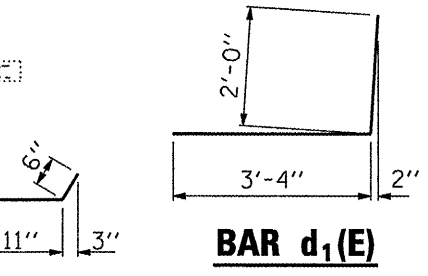
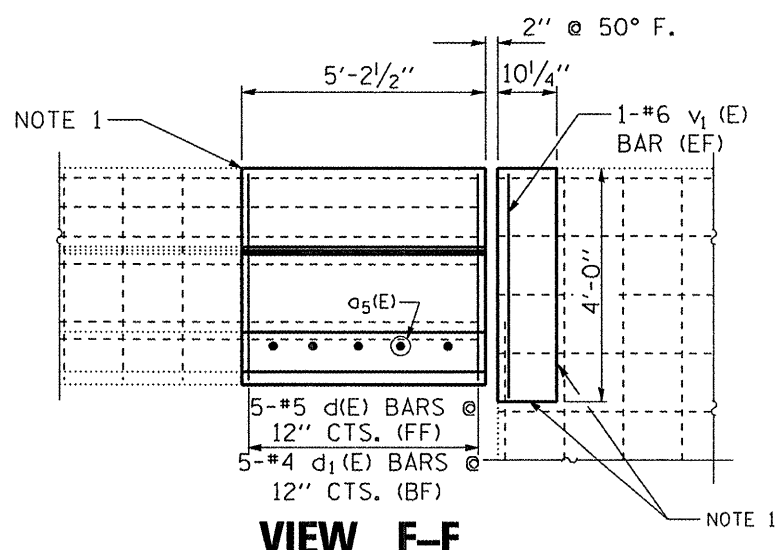
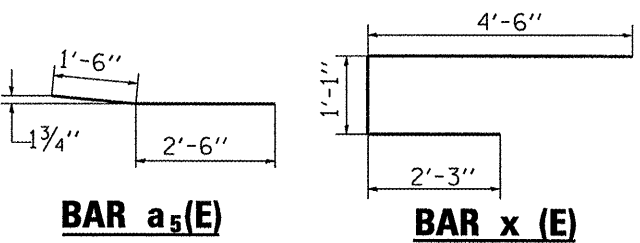
**BILL OF MATERIALS FOR ONE ABUTMENT**

CONCRETE REMOVAL	CU YD	14
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**CURB SECTION AT JOINT**

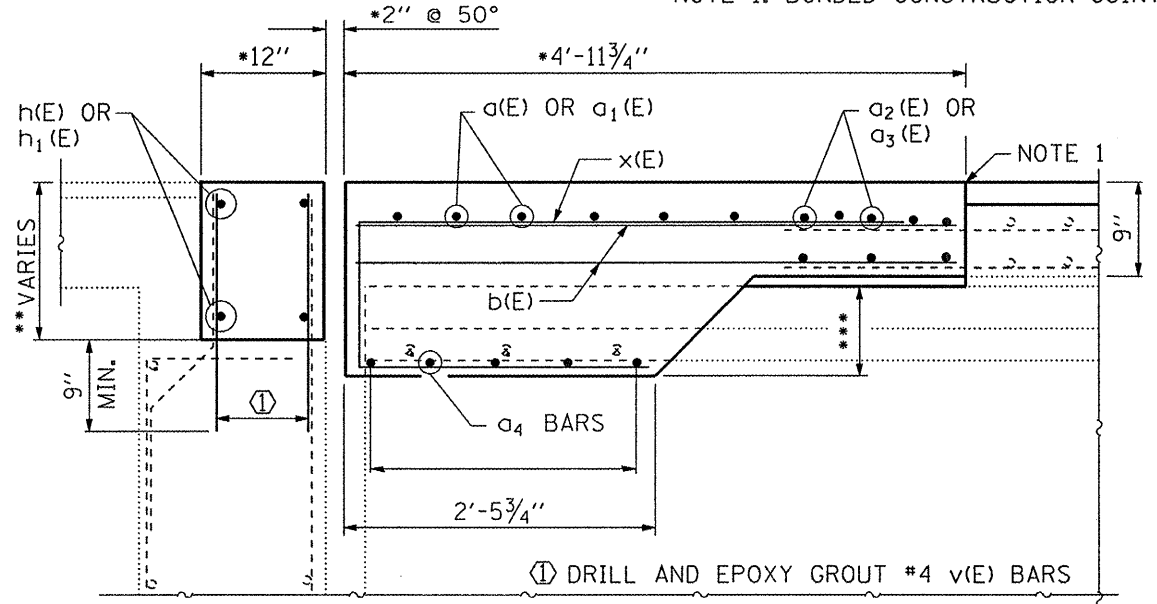
**VIEW E-E**



**PLAN**  
NORTH ABUTMENT SHOWN  
SOUTH ABUTMENT SIMILAR

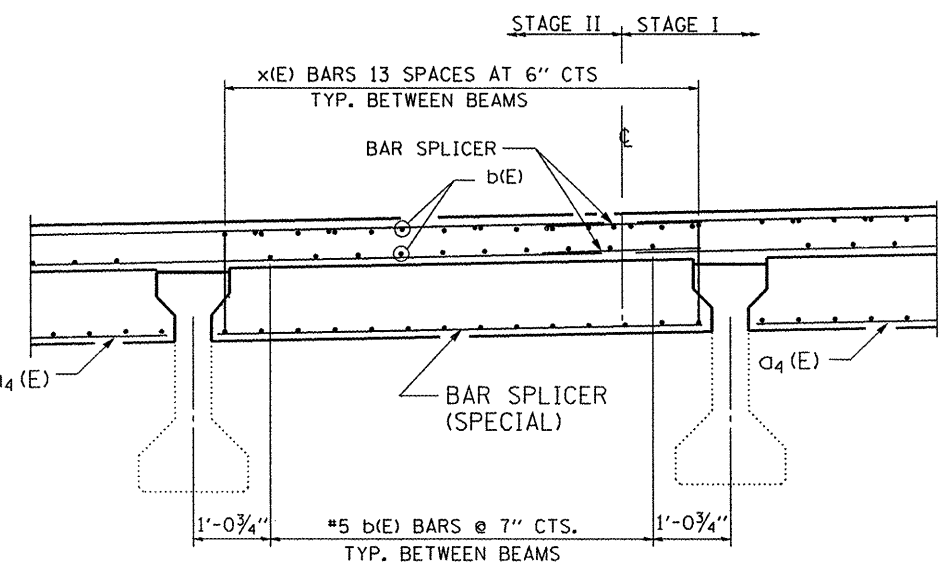
- \*@ RT. L'S
- \*\* 1'-8 1/2" TO 1'-3" S. ABUT.  
2'-0 1/2" TO 1'-3" N. ABUT.
- \*\*\* 11" @ FASCIA BEAMS

NOTE 1: BONDED CONSTRUCTION JOINT



**BILL OF MATERIALS FOR ONE ABUTMENT**

BAR	NO. BARS	SIZE	LENGTH	SHAPE
a (E)	6	7	18'-0"	
a <sub>1</sub> (E)	6	7	22'-1"	
a <sub>2</sub> (E)	10	6	18'-0"	
a <sub>3</sub> (E)	10	6	22'-1"	
a <sub>4</sub> (E)	20	5	6'-7"	
a <sub>5</sub> (E)	10	6	4'-0"	
d (E)	10	5	3'-11"	
d <sub>1</sub> (E)	10	4	5'-4"	
h (E)	4	6	18'-0"	
h <sub>1</sub> (E)	4	6	22'-1"	
v (E)	82	4	2'-5"	
v <sub>1</sub> (E)	4	6	3'-9"	
x (E)	70	6	7'-10"	
REINFORCEMENT BARS EPOXY COATED			POUNDS	2586
CONCRETE STRUCTURE (ABUTS.)			CU YD	3
CONCRETE SUPERSTRUCTURE			CU YD	11



**CROSS SECTION AT END OF DECK**

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -
c:\pwork\VPWIDT\WOODSHANKR1\dms91655	D3-66890-details.dgn	DRAWN - RON WOODSHANK	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = Apr 26, 2009 - 10:47:28 AM	DATE -	REVISED -

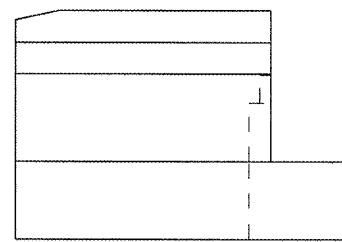
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT CONST.  
SN 050-0170 (NB)

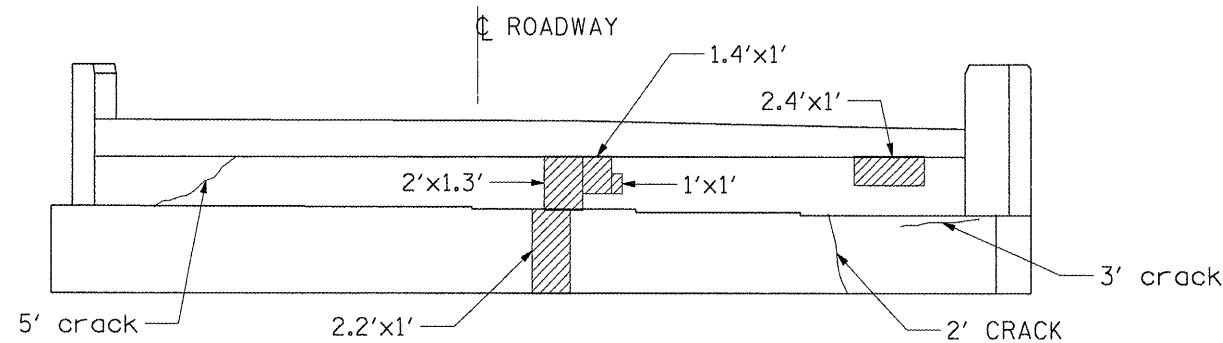
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(50-7B)1-2	LASALLE	29	22
FED. ROAD DIST. NO. - ILLINOIS			CONTRACT NO. 66890	

SCALE: SHEET NO. 3 OF 8 SHEETS STA. TO STA.

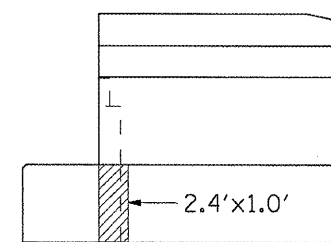




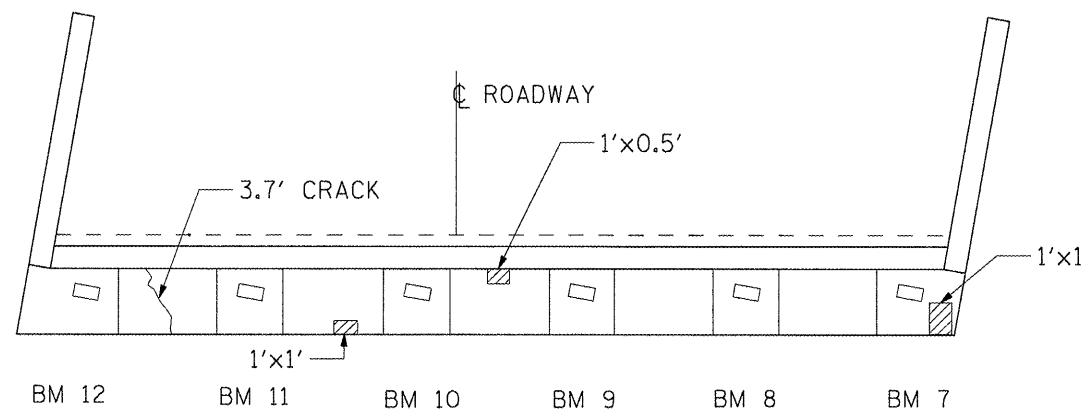
**EAST ELEVATION**



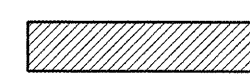
**ELEVATION**



**WEST ELEVATION**

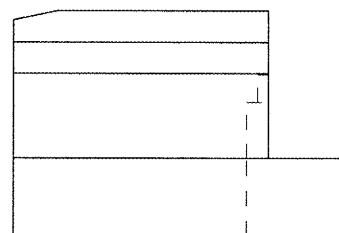


**PLAN**

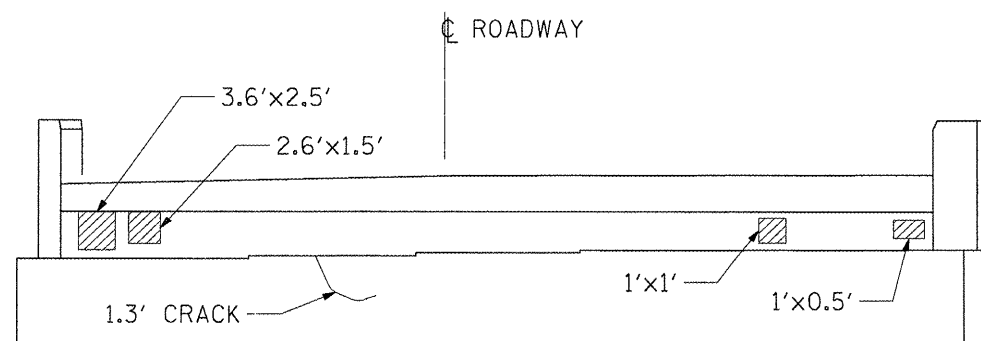


**STRUCTURAL REPAIR OF CONCRETE  
(DEPTH EQUAL TO OR LESS THEN 5")**

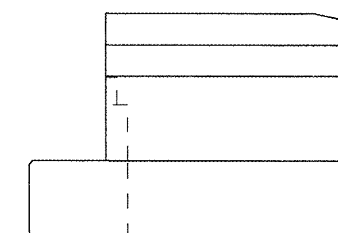
**S. ABUTMENT DETAIL - N.B.**



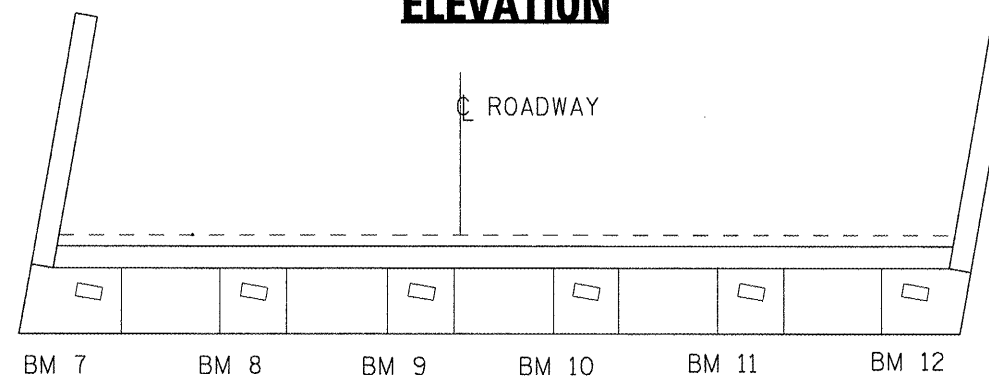
**WEST ELEVATION**



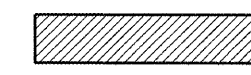
**ELEVATION**



**EAST ELEVATION**



**PLAN**



**STRUCTURAL REPAIR OF CONCRETE  
(DEPTH EQUAL TO OR LESS THEN 5")**



**EPOXY CRACK INJECTION**

**N. ABUTMENT DETAILS - N.B.**

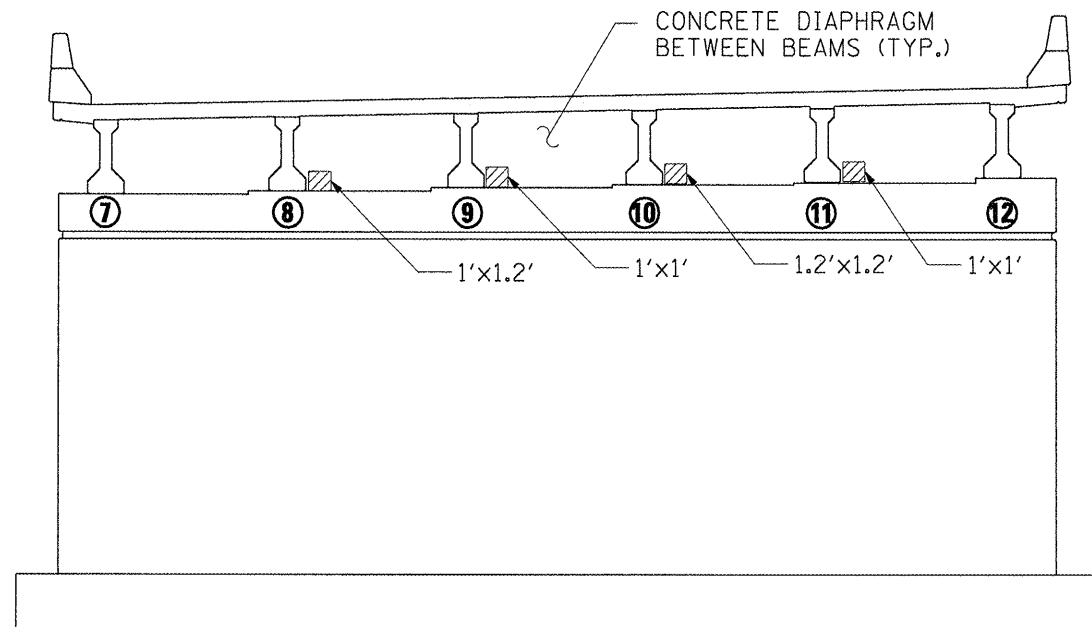
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c:\pwork\pwork\WOODSHANKR1\dms91655	D3-66898-details.dgn	DRAWN - RON WOODSHANK	REVISED -
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	REVISED -
PLOT DATE = Mar 25, 2009 - 06:59:12 AM	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

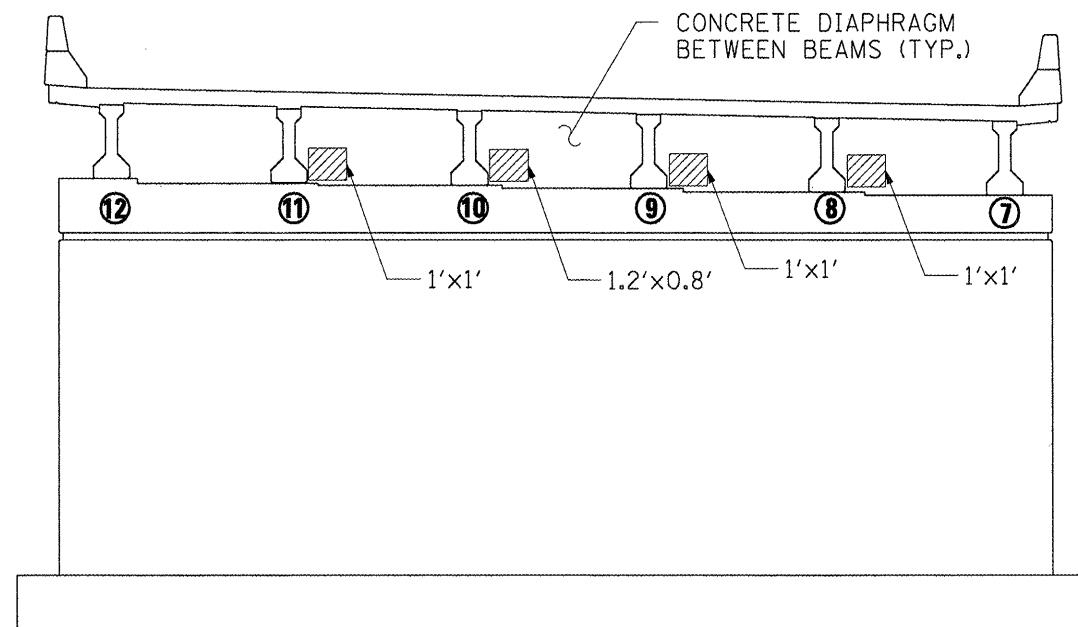
**ABUTMENT DETAILS  
SN 050-0170 (NB)**

SCALE: SHEET NO. 4 OF 8 SHEETS STA. TO STA.

F.A.I. RTE. 39	SECTION (50-7B)I-2	COUNTY LASALLE	TOTAL SHEETS 29	SHEET NO. 23
FED. ROAD DIST. NO. ILLINOIS			CONTRACT NO. 66890	



**LOOKING AHEAD  
SOUTH FACE**

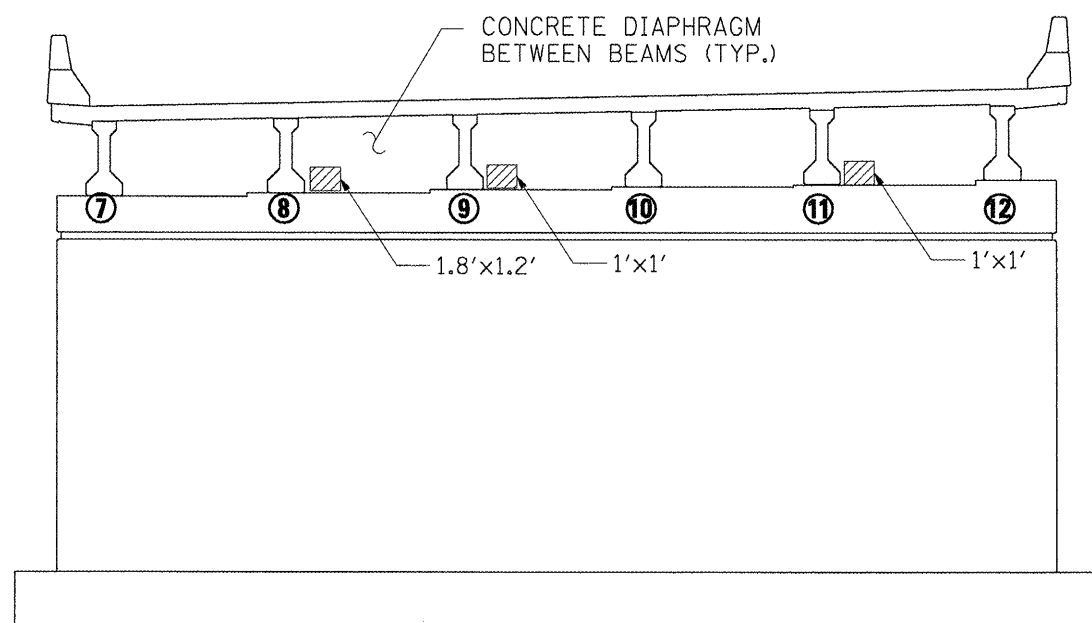


**LOOKING BACK  
NORTH FACE**

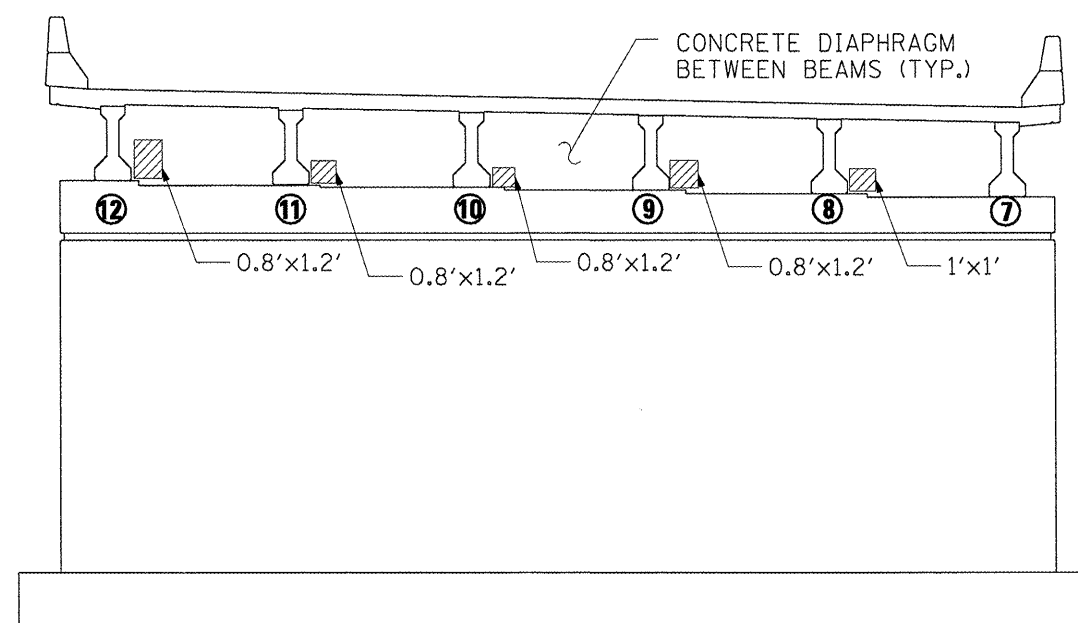


**STRUCTURAL REPAIR OF CONCRETE  
(DEPTH EQUAL TO OR LESS THEN 5")**

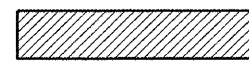
**PIER 1**



**LOOKING AHEAD  
SOUTH FACE**



**LOOKING BACK  
NORTH FACE**



**STRUCTURAL REPAIR OF CONCRETE  
(DEPTH EQUAL TO OR LESS THEN 5")**

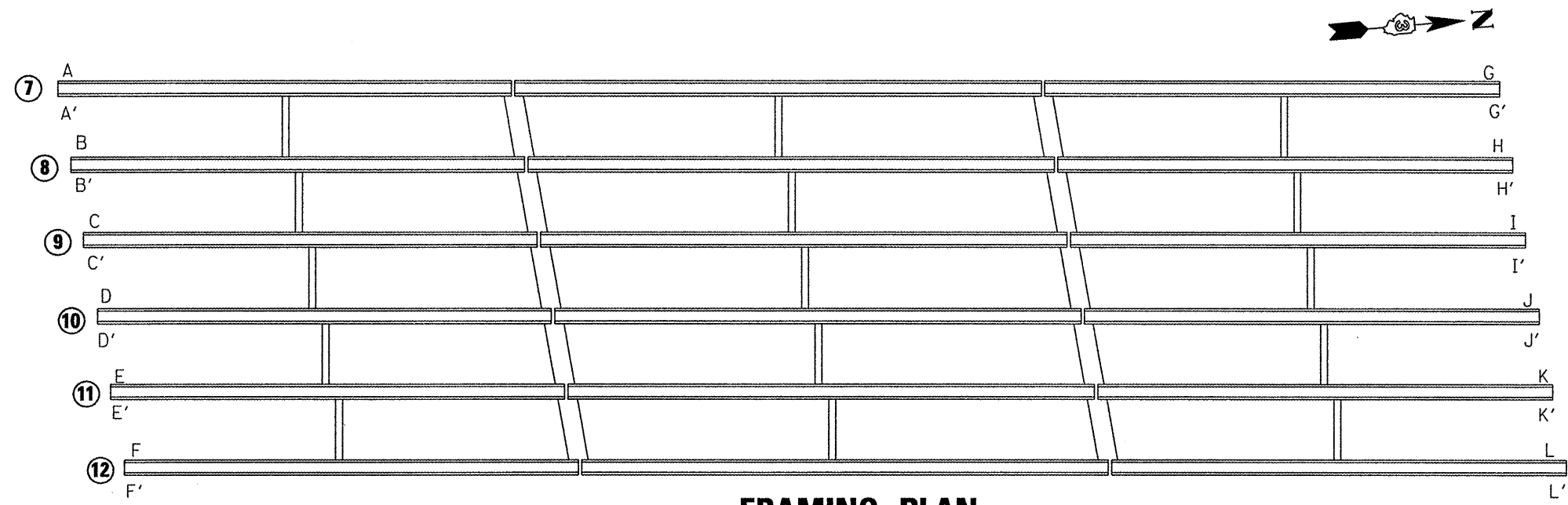
**PIER 2**

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PLOT DATE = Mar 25, 2009 - 06:59:17 AM	DATE -	REVISED -	REVISED -

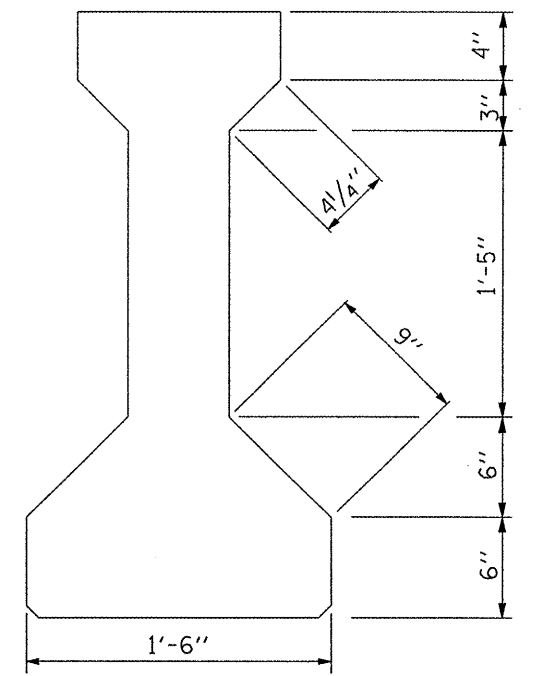
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PIER DETAILS  
SN 050-0170(NB)**

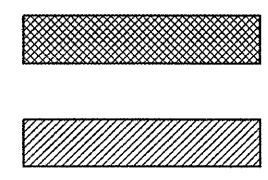
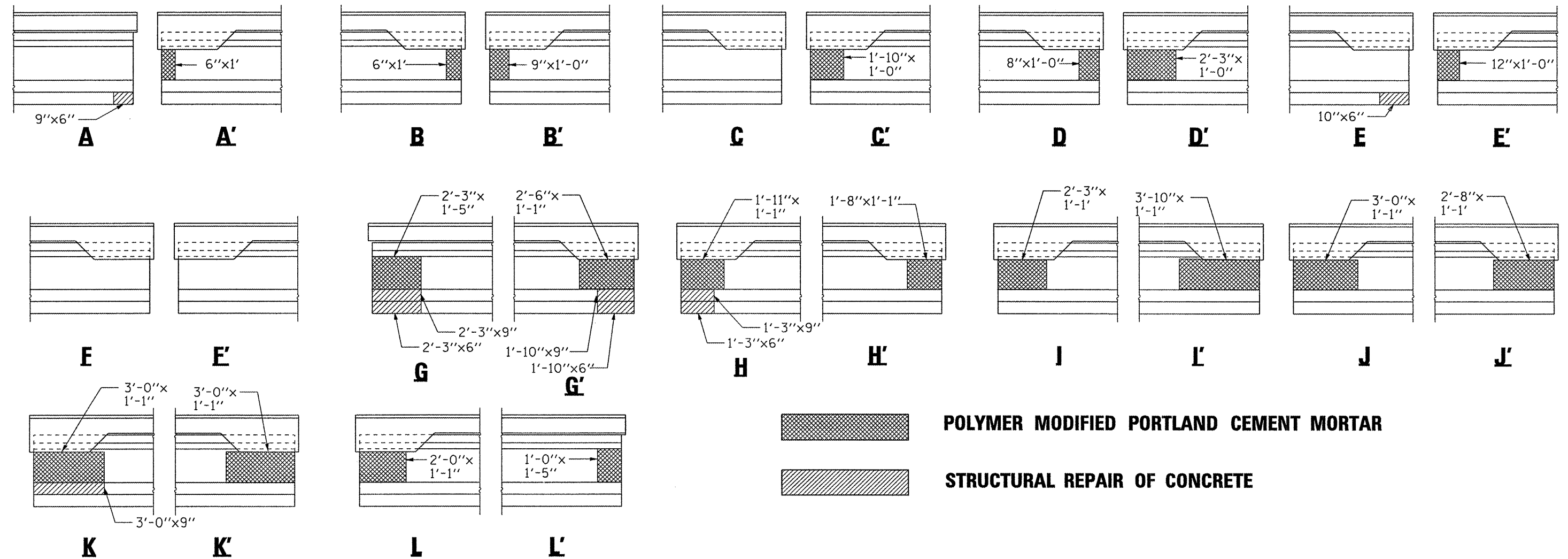
F.A.I. RTE. 39	SECTION (50-7B)I-2	COUNTY LASALLE	TOTAL SHEETS 29	SHEET NO. 24
SCALE: _____		SHEET NO. 5 OF 8 SHEETS		STA. _____ TO STA. _____
FED. ROAD DIST. NO. _____ ILLINOIS				
CONTRACT NO. 66890				



**FRAMING PLAN  
SN 0170 (NB)**



**BEAM DETAIL**



**POLYMER MODIFIED PORTLAND CEMENT MORTAR**

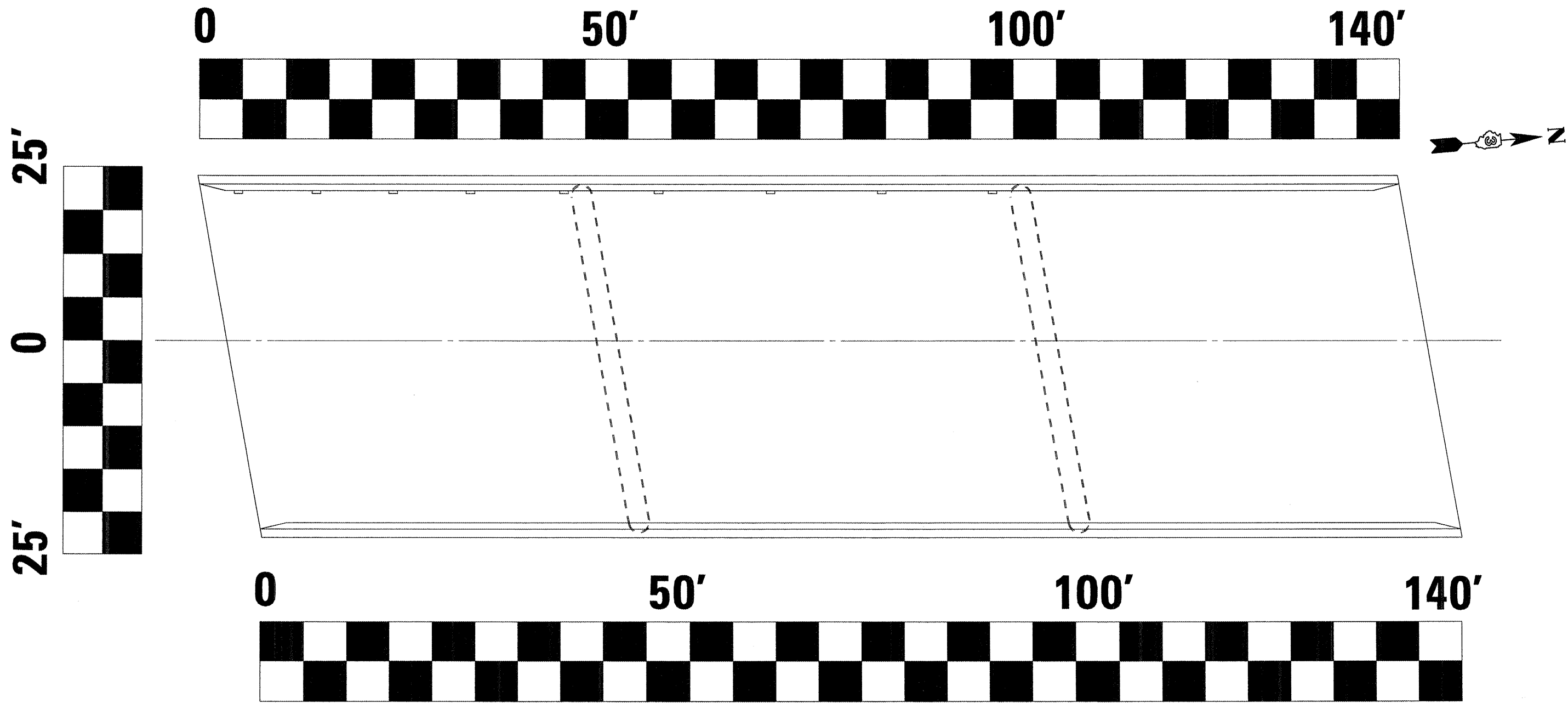
**STRUCTURAL REPAIR OF CONCRETE**

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -
c:\pwwork\pwwid01\WOODSHANKR1\dms91655	D3-66890-details.dgn	DRAWN - RON WOODSHANK	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = Apr 28, 2009 - 10:13:25 AM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>FRAMING PLAN SN 050-0170 (NB)</b>	
SCALE: _____	TO STA. _____
SHEET NO. 6 OF 8 SHEETS	

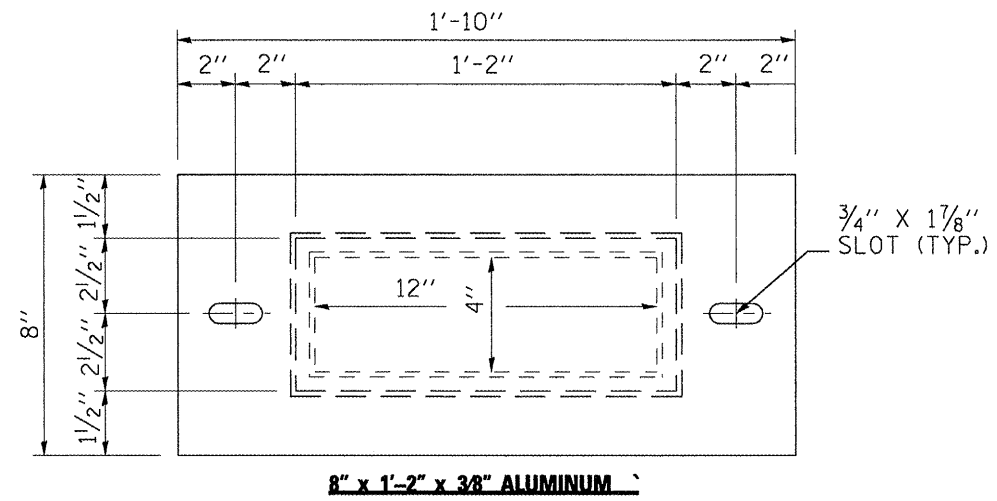
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(50-7B)I-2	LASALLE	29	25
FED. ROAD DIST. NO. - ILLINOIS			CONTRACT NO. 66890	



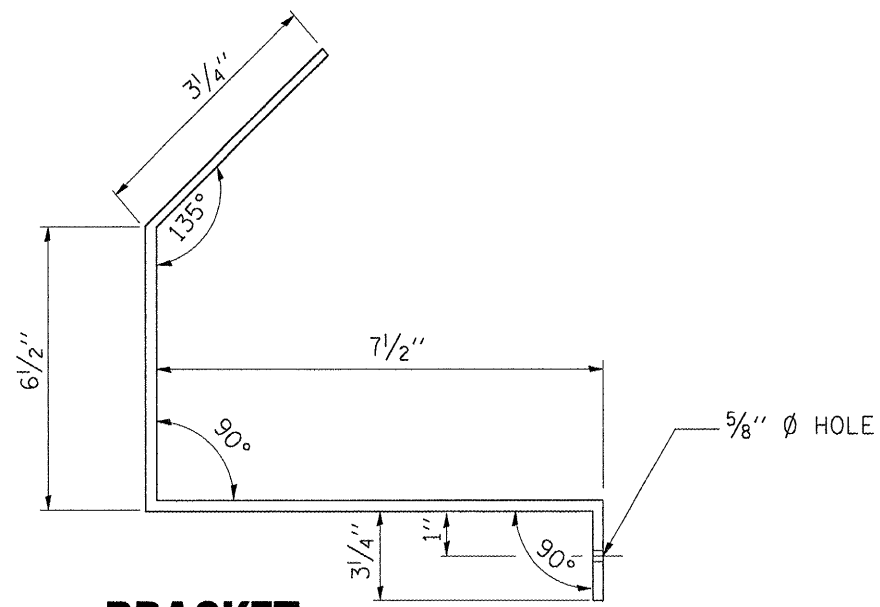
**SN 050-0170 NB**

**SCALE 1" = 50'**

FILE NAME = c:\pwwork\FW1DOT\WOODSHANKRL\dms91655	USER NAME = woodshankr1 03-66890-details.dgn	DESIGNED - RON WOODSHANK	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DECK SLAB REPAIR AS-BUILT SHEET</b>	F.A.I. RTE. 39	SECTION (50-7B)-2	COUNTY LASALLE	TOTAL SHEETS 29	SHEET NO. 26		
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -			SCALE: _____	SHEET NO. 7 OF 8 SHEETS	STA. _____ TO STA. _____	CONTRACT NO. 66890			
	PLOT DATE = Mar 25, 2009 - 06:59:27 AM	DATE -	REVISED -			FED. ROAD DIST. NO. _____ ILLINOIS						

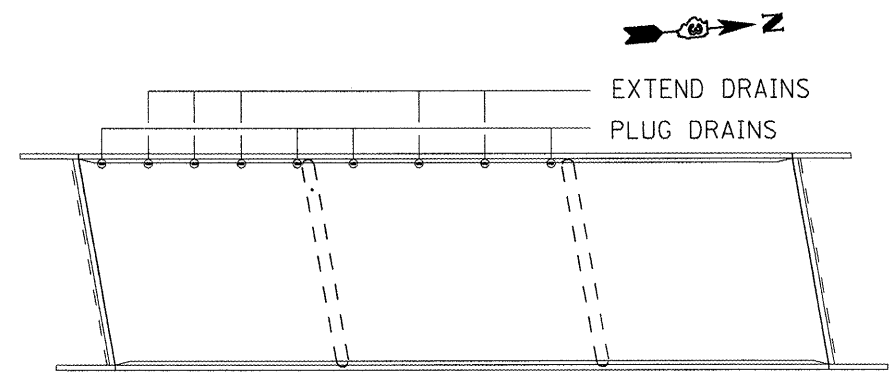


**8" x 1'-2" x 3/8" ALUMINUM**

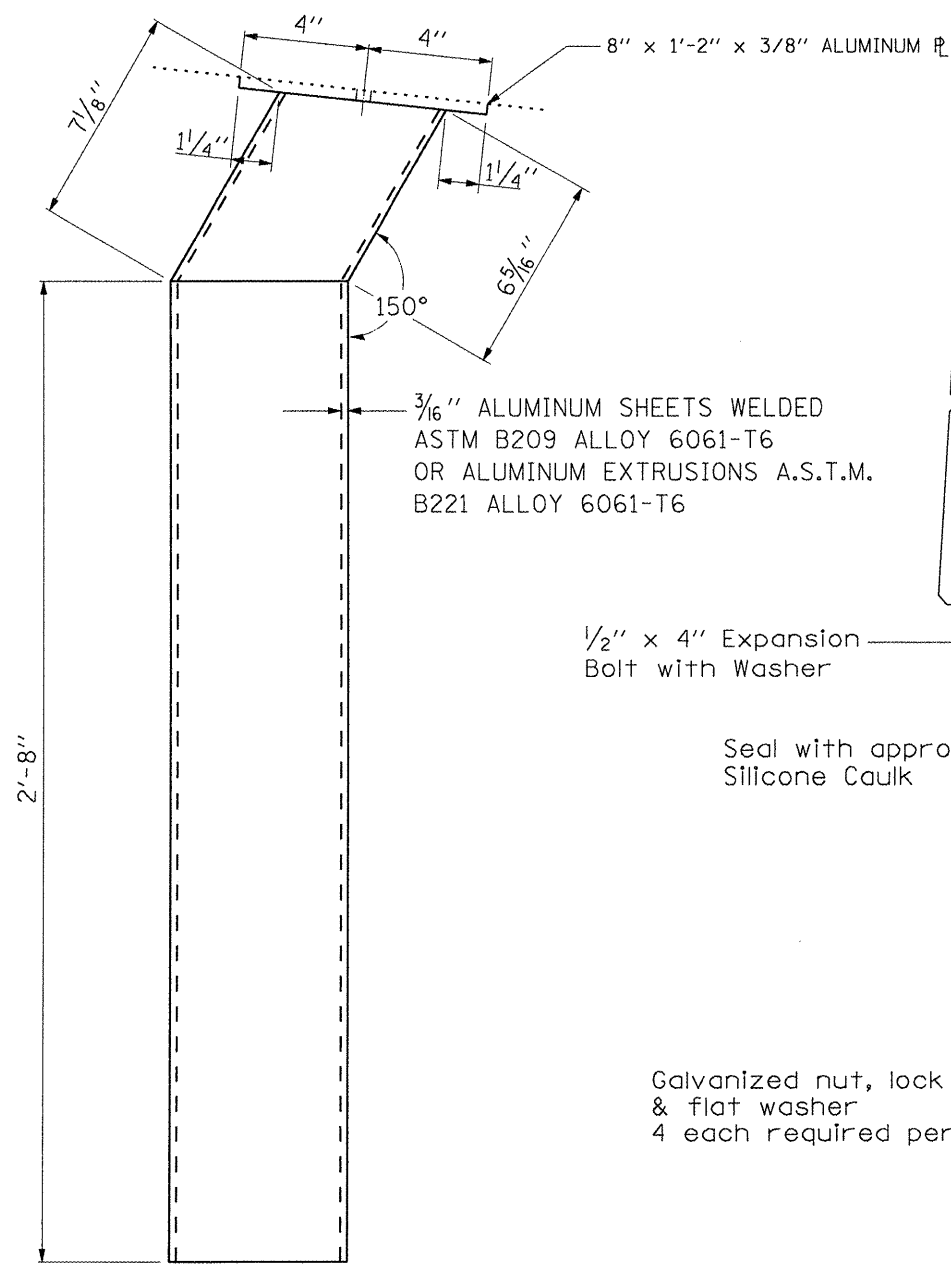


**BRACKET**

1/4" x 2" x 1'-8 1/2"  
ALUMINUM BRACKET  
(2 REQUIRED PER DRAIN)



**SN 050-0170 (NB)**

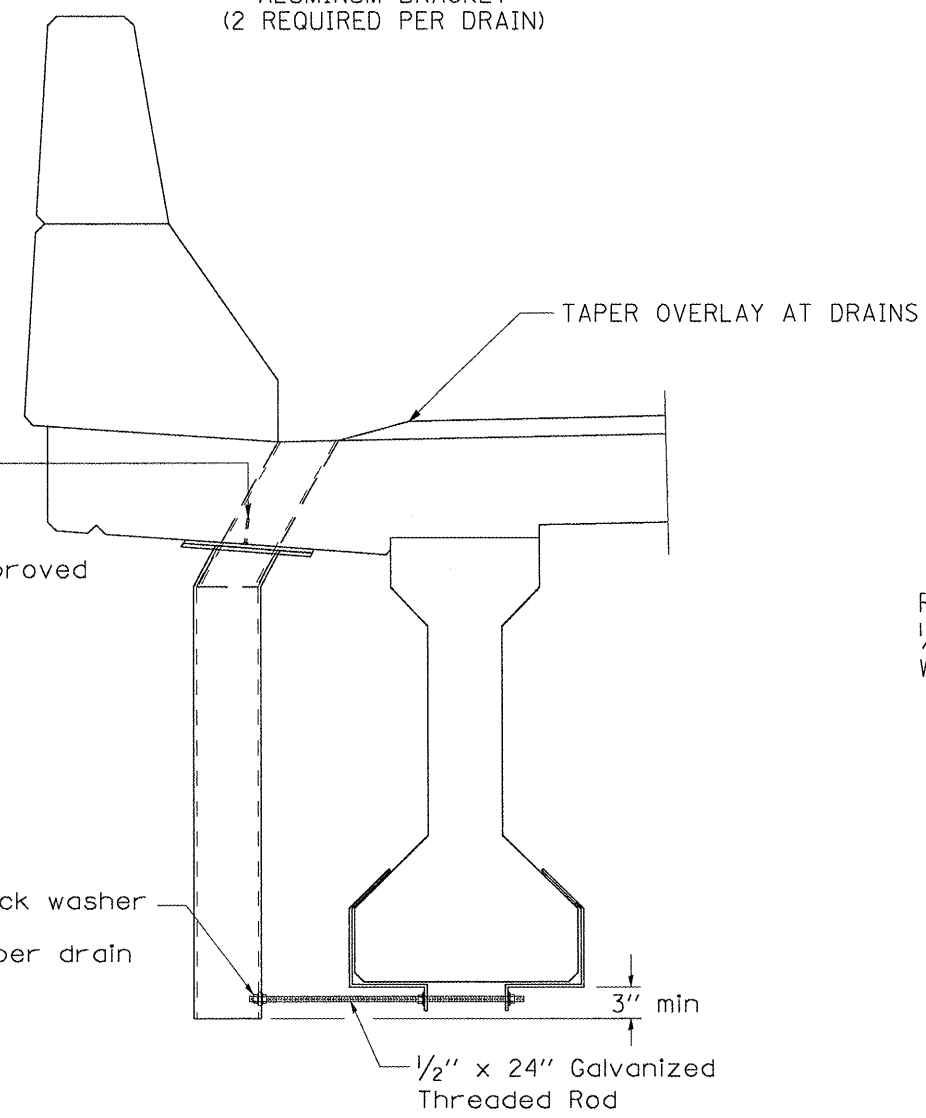


3/16" ALUMINUM SHEETS WELDED  
ASTM B209 ALLOY 6061-T6  
OR ALUMINUM EXTRUSIONS A.S.T.M.  
B221 ALLOY 6061-T6

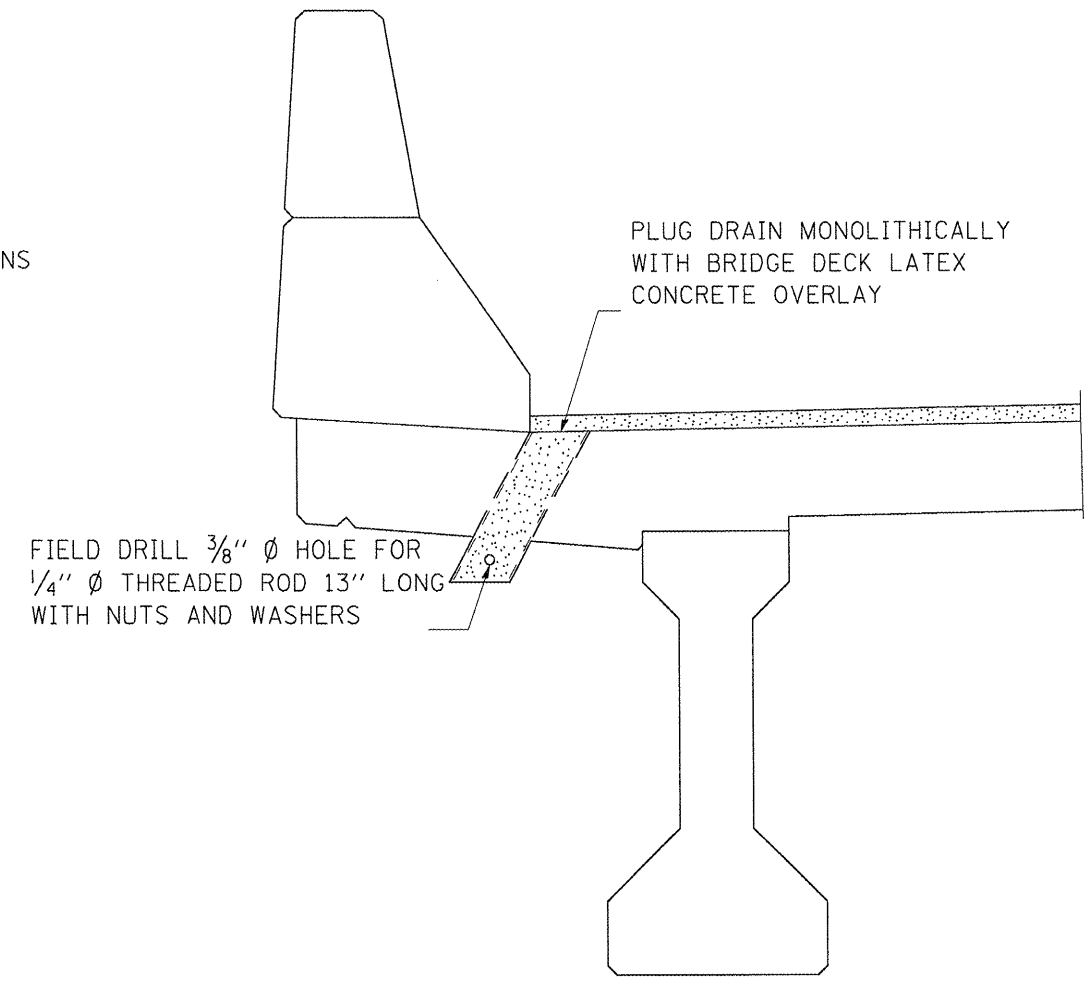
1/2" x 4" Expansion  
Bolt with Washer

Seal with approved  
Silicone Caulk

Galvanized nut, lock washer  
& flat washer  
4 each required per drain

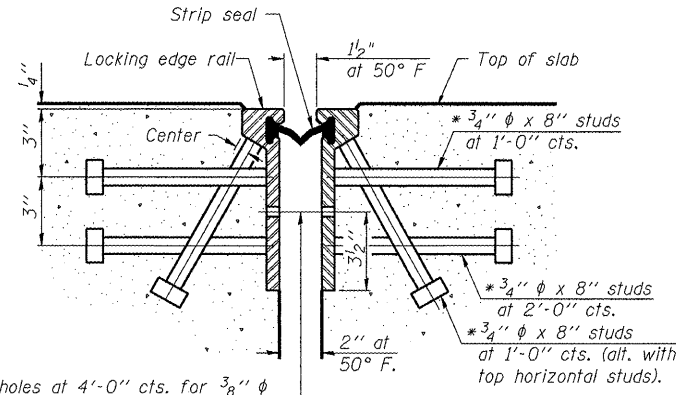


1/2" x 24" Galvanized  
Threaded Rod



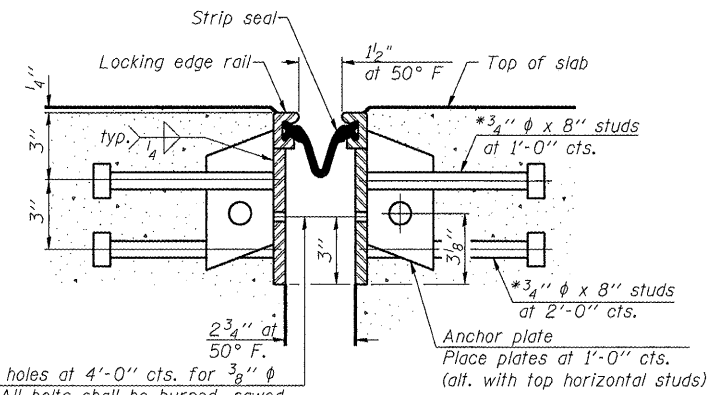
**PLUG DECK DRAIN**

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



7/16"  $\phi$  holes at 4'-0" cts. for 3/8"  $\phi$  bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

**SECTION THRU ROLLED RAIL JOINT**



7/16"  $\phi$  holes at 4'-0" cts. for 3/8"  $\phi$  bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

**SECTION THRU WELDED RAIL JOINT**

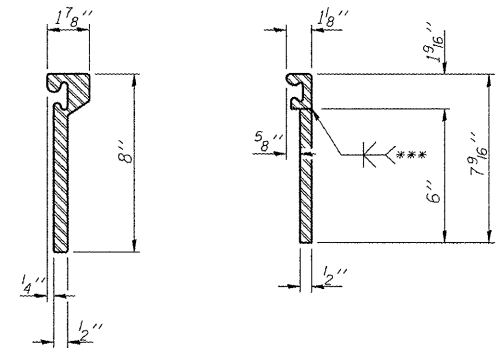
Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

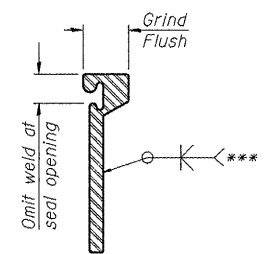
The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.



**ROLLED EXTRUDED RAIL WELDED RAIL**

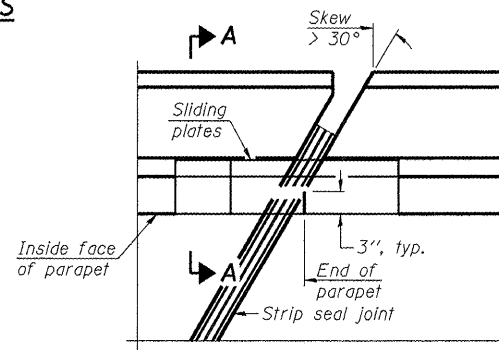


\*\*\*Back gouge not required if complete joint penetration is verified by mock-up.

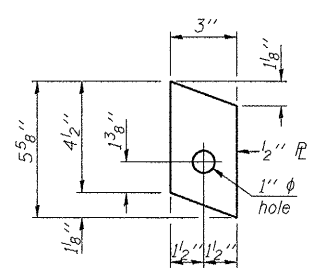
**LOCKING EDGE RAIL SPLICE**

The inside of the locking edge rail groove shall be free of weld residue.

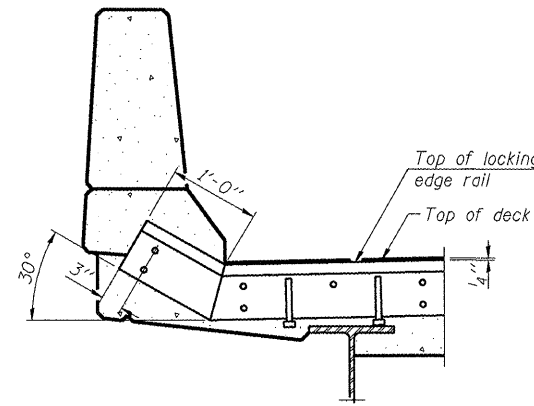
**LOCKING EDGE RAILS**



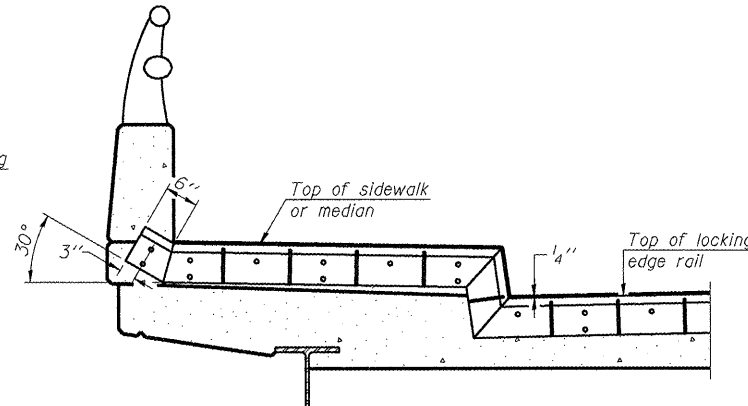
**PLAN**



**ANCHOR PLATE (for welded rail)**



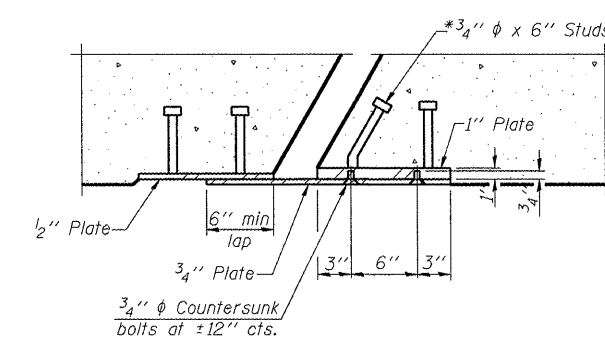
**AT PARAPET**



**AT SIDEWALK OR MEDIAN**

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

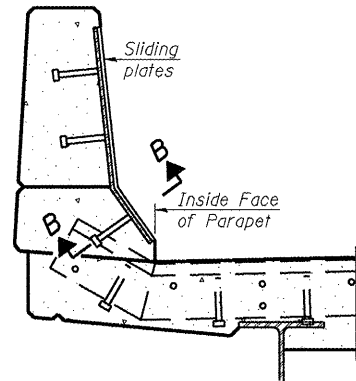
**TYPICAL END TREATMENTS**



**SECTION B-B**

**BILL OF MATERIAL**

Item	Unit	Total
Preformed Joint Strip Seal	Foot	170



**SECTION A-A**

**POINT BLOCK DETAILS (for skews > 30°)**

EJ-SSJ

10-1-08

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RON WOODSHANK	REVISED -
at:\pw\work\PIWIDOT\WOODSHANKR1\dms91655	03-66890-detail.dgn	DRAWN - RON WOODSHANK	REVISED -
	PLOT SCALE = 100.0000 / IN.	CHECKED -	REVISED -
	PLOT DATE = Mar 25, 2009 - 06:59:39 AM	DATE -	REVISED -

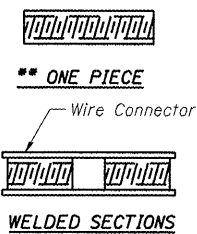
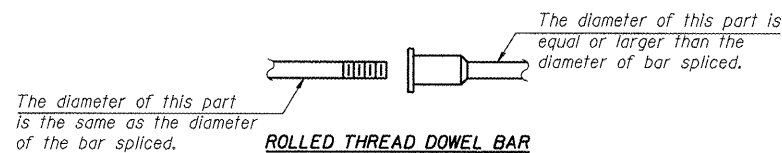
**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**STRIP SEAL EXPANSION JOINT DETAILS**

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

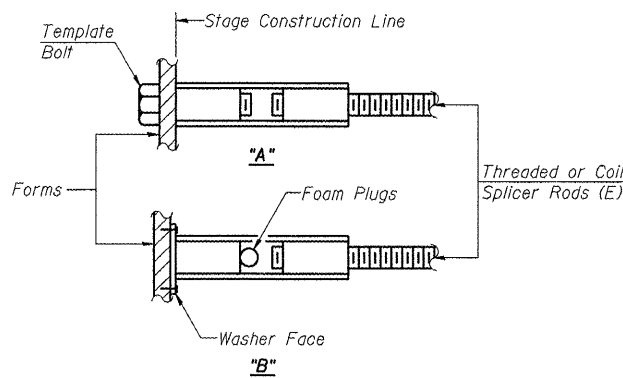
F.A.I. RTE. 39	SECTION (50-7B)-2	COUNTY LASALLE	TOTAL SHEETS 29	SHEET NO. 28
FED. ROAD DIST. NO. [ILLINOIS]			CONTRACT NO. 66890	

**PREFORMED JOINT STRIP SEAL STRUCTURE NO. 050-0169 (SB) STRUCTURE NO. 050-0170 (NB)**



**BAR SPLICER ASSEMBLY ALTERNATIVES**

\*\*Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



**INSTALLATION AND SETTING METHODS**

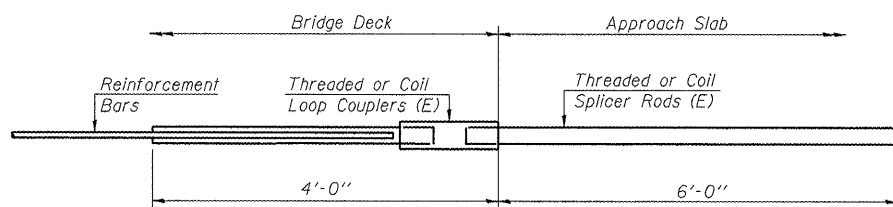
"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.

**NOTES**

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.  
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.  
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.  
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

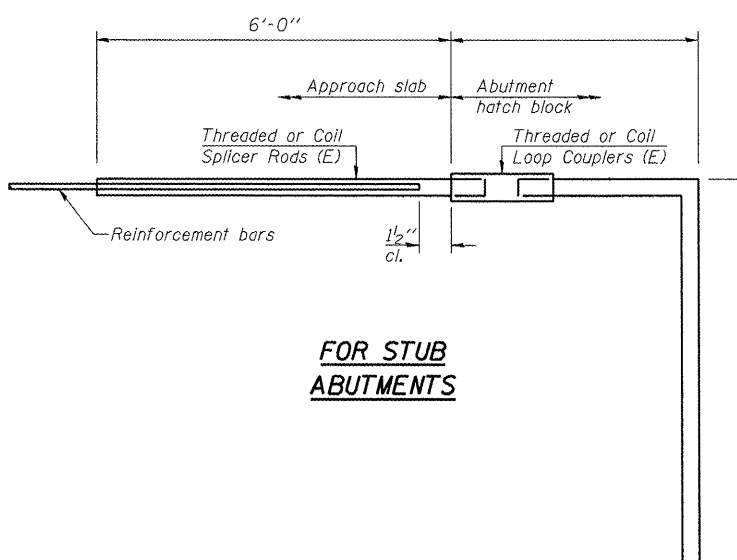
- ① Minimum Capacity =  $1.25 \times f_y \times A_t$   
 (Tension in kips)
  - ② Minimum \*Pull-out Strength =  $0.66 \times f_y \times A_t$   
 (Tension in kips)
- Where  $f_y$  = Yield strength of lapped reinforcement bars in ksi.  
 $A_t$  = Tensile stress area of lapped reinforcement bars.  
 \* = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-2"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



**FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

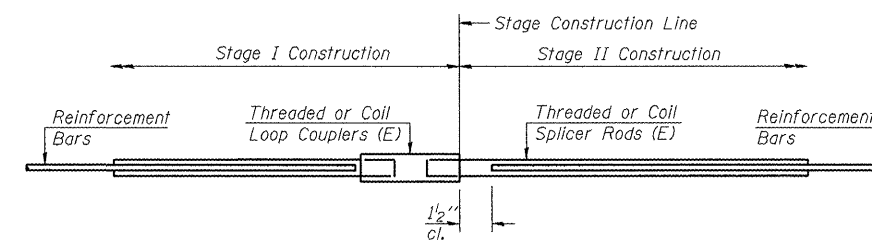
BAR SIZE	NO. ASSEMBLIES REQUIRED	LOCATION



**FOR STUB ABUTMENTS**

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =

BAR SIZE	NO. ASSEMBLIES REQUIRED	LOCATION



**STANDARD**

**STRUCTURE NO. 050-0169 (SB)**

BAR SIZE	NO. ASSEMBLIES REQUIRED	LOCATION
#6	4	N ABUT
#7	11	N END - DECK
#6	10	N END - DECK
#6	4	S ABUT
#7	11	S END - DECK
#6	10	S END - DECK

**STRUCTURE NO. 050-0170 (NB)**

BAR SIZE	NO. ASSEMBLIES REQUIRED	LOCATION
#6	4	N ABUT
#7	11	N END - DECK
#6	10	N END - DECK
#6	4	S ABUT
#7	11	S END - DECK
#6	10	S END - DECK

BSD-1 10-1-08