

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
DIVISION STREET
37TH AVE. TO 31ST AVE.
RESURFACING
SECTION NO. 14-00129-00-RS
PROJECT M-4003(489)
VILLAGE OF MELROSE PARK
COOK COUNTY
C-91-223-15

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	14-00129-00-RS	COOK	12	1
ILLINOIS PROJECT -				
VILLAGE SECTION -				

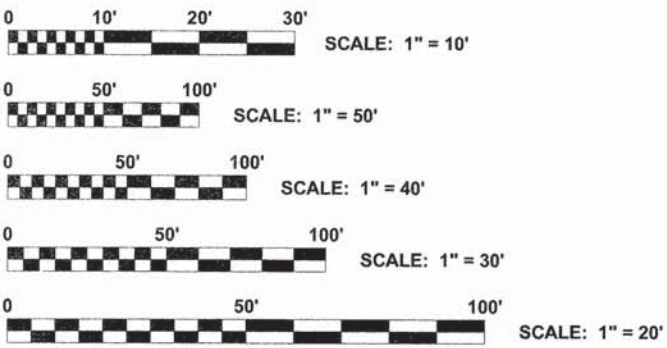


LOCATION OF SECTION INDICATED THUS:

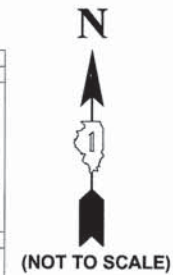
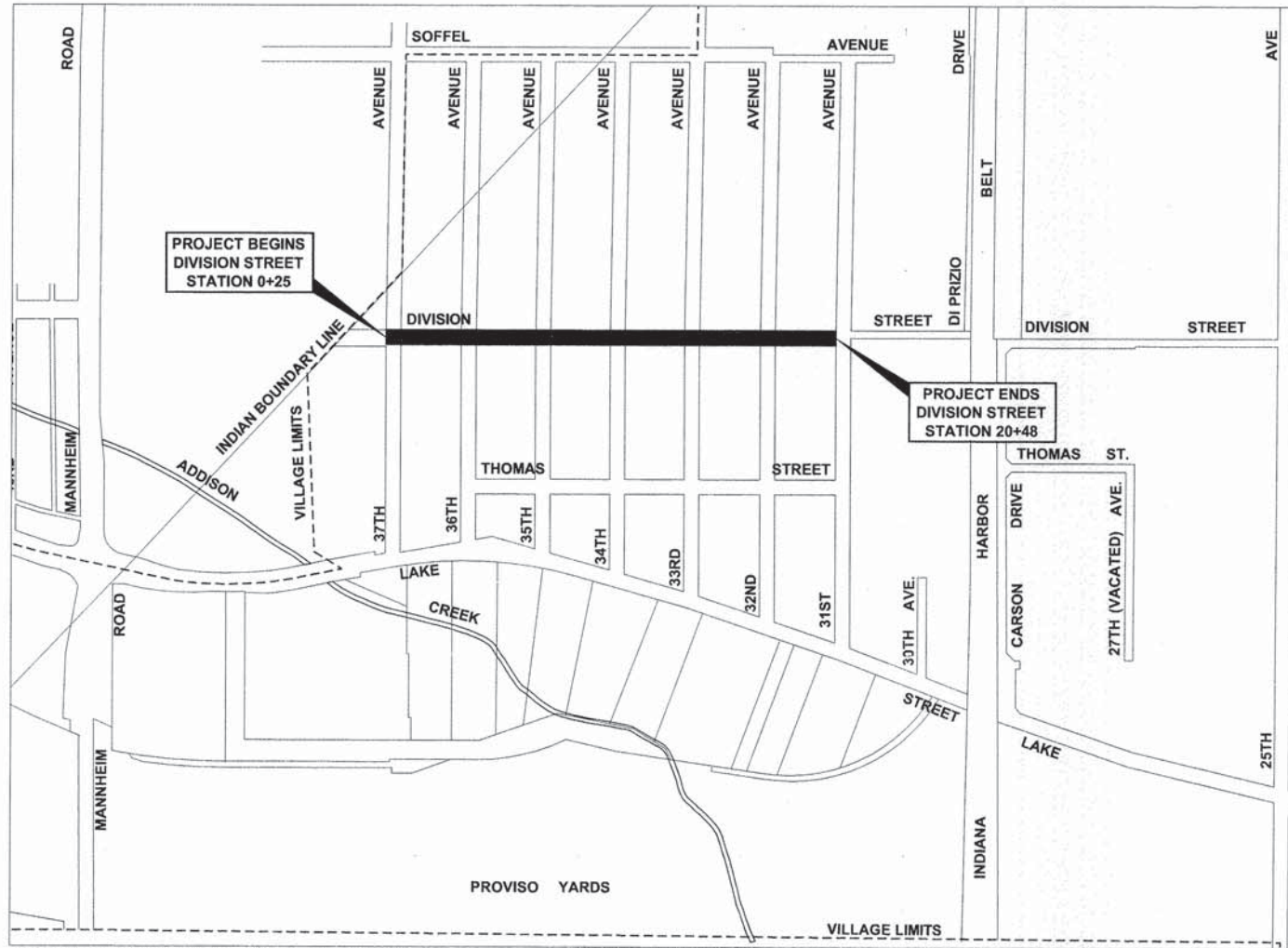
TRAFFIC DATA
 ADT (2014) = 3,200
 POSTED SPEED LIMIT = 25 MPH
 DESIGN SPEED LIMIT = 25 MPH

DESIGN DESIGNATION
 COLLECTOR

PROJECT LOCATED IN THE VILLAGE OF MELROSE PARK



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



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STATE OF ILLINOIS
ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED: *[Signature]* 2015
 VILLAGE OF MELROSE PARK, PRESIDENT

PASSED: *[Signature]* APRIL 10 2015
 DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASED FOR BID BASED ON LIMITED REVIEW: *[Signature]* APR 10 2015
 DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER



SIGNED: *[Signature]*
 DATE: 3/10/15 LICENSE EXPIRES: 11/30/15



CONTRACT NO. 61B52

- AREA OF IMPROVEMENT
 GROSS LENGTH OF IMPROVEMENT = 2,023 FT = 0.383 MI
 NET LENGTH OF IMPROVEMENT = 2,023 FT = 0.383 MI

HANCOCK ENGINEERING 100+ Years of Excellence

- Civil Engineers 9933 Roosevelt Road Westchester, IL 60154-2780
- Municipal Consultants Phone: 708-865-0300
- Established 1911 www.hancock.com

INDEX OF SHEETS





















SHEET NO.	DESCRIPTION
1	COVER SHEET, LOCATION MAP
2	INDEX OF SHEETS, LEGEND OF SYMBOLS, AND I.D.O.T. STANDARD DRAWINGS
3	GENERAL NOTES
4	SUMMARY OF QUANTITIES
5	EXISTING AND PROPOSED TYPICAL SECTIONS
6-7	PAVING / PAVEMENT MARKING PLAN
8	DETAILS
9	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)
10	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
11	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
12	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)

I.D.O.T. STANDARD DRAWINGS

STANDARD NO.	TITLE OR DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C&D PATCHES
604001-04	FRAMES AND LIDS, TYPE 1
701301-04	LANE CLOSURE, 2-LANE, 2-WAY, SHORT-TIME OPERATIONS
701311-03	LANE CLOSURE, 2-LANE, 2-WAY, MOVING OPERATIONS, DAY ONLY
701501-06	URBAN LANE CLOSURE, 2-LANE, 2-WAY, UNDIVIDED
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

LEGEND OF SYMBOLS

(TO BE USED IN CONJUNCTION WITH I.D.O.T. STANDARD 000001-06)

SYMBOL	DESCRIPTION
	EXISTING HOT-MIX ASPHALT AREA
	EXISTING CONCRETE AREA
	EXISTING GRASS AREA
	PROPOSED HOT-MIX ASPHALT BUTT JOINT
	REMOVE AND REINSTALL BRICK PAVERS
	PROPOSED CONCRETE AREA, 5" SIDEWALK, 7" DRIVEWAY
	PROPOSED HOT-MIX ASPHALT PAVING AREA
A	STRUCTURE TO BE ADJUSTED
A*	STRUCTURE TO BE ADJUSTED (SPECIAL)
AH	HANDHOLE TO BE ADJUSTED
1C	NEW FRAME AND LID, TYPE 1, CLOSED LID
1P	NEW FRAME AND LID, TYPE 1, OPEN LID
RC	STRUCTURE TO BE RECONSTRUCTED
	EXISTING FIRE HYDRANT
	EXISTING WATER VALVE BOX
	EXISTING WATER MAIN VALVE VAULT
	EXISTING STORM SEWER INLET
	EXISTING STORM SEWER CATCH BASIN
	PROPOSED STORM SEWER CATCH BASIN
	EXISTING SEWER MANHOLE
	EXISTING STREET LIGHT POLE
	EXISTING POWER POLE
	EXISTING HANDHOLE
	EXISTING CURB AND GUTTER
	PROPOSED CONCRETE CURB, TYPE B
	PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

Drawing file: W:\Projects_by_Village\Melrose Park\60014276 - Division Street L&O\Index (Division).dwg Feb 12, 2015 - 9:27am



◆ Civil Engineers
◆ Municipal Consultants
◆ Established 1911

9933 Roosevelt Road
Westchester, Illinois 60154-2700
Phone: 708/965-0500
Fax: 708/965-1212

DESIGNED - -	REVISED - -
DRAWN - MK, DMM	REVISED - -
CHECKED - -	REVISED - -
DATE - 02/12/15	REVISED - -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, LEGEND OF SYMBOLS, AND I.D.O.T. STANDARD DRAWINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. - TO STA. -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	14-00129-00-RS	COOK	12	2
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61B52	
FED. AID PROJECT				

GENERAL NOTES

STANDARDS

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION AS SHOWN ON THE INDEX OF SHEETS IN THE PLANS. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2012, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2015, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS," "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JULY 2014 7TH EDITION, AND THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

UNDERGROUND UTILITIES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 811 FOR FIELD LOCATIONS OF BURIED ELECTRICAL, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).

THE LOCATIONS OF THE UNDERGROUND UTILITIES IF SHOWN ON THE PLANS HAVE BEEN OBTAINED BY FIELD SURVEYS AND SEARCHES OF AVAILABLE RECORDS. IT IS BELIEVED THAT DATA IS ESSENTIALLY CORRECT, BUT THE VILLAGE OF LAGRANGE PARK, THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND/OR OTHER OFFICES AND AGENCIES ASSOCIATED WITH THE DEVELOPMENT OF THESE PLANS DO NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE EXACT LOCATION OF EACH FACILITY WITH THE UTILITY COMPANY, AND SHALL TAKE DUE CARE IN ALL PHASES OF THE CONSTRUCTION TO PROTECT ANY SUCH FACILITIES WHICH MAY BE AFFECTED BY THE WORK. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF MELROSE PARK.

FRAMES AND GRATES

THE TYPE OF FRAMES AND GRATES REQUIRED FOR ALL CATCH BASINS AND MANHOLES LISTED IN THE SUMMARY OF QUANTITIES MAY BE FOUND ON THE PLANS AT THEIR RESPECTIVE LOCATIONS. WHERE LIDS ARE CALLED FOR ON THE PLANS, THEY SHALL BE IN ACCORDANCE WITH ARTICLE 604.01 OF THE STANDARD SPECIFICATIONS AND THE TERM LID IS USED IN LIEU OF GRATE.

ON ALL IMPROVEMENTS, THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS WHICH ARE TO BE ABANDONED DUE TO CONSTRUCTION OF THIS IMPROVEMENT ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF MELROSE PARK AND BE SALVAGED. THE CONTRACTOR IS TO DELIVER FRAMES AND LIDS TO THE VILLAGE OF MELROSE PARK LOCATED AT 1000 N. 25TH AVENUE, MELROSE PARK, IL 60160

MANHOLE OR VALVE COVERS

THE WORD "WATER", "SANITARY", OR "STORM" SHALL BE CAST INTO THE LID OF EACH RESPECTIVE MANHOLE OR VALVE VAULT. THE WORDS "VILLAGE OF MELROSE PARK" SHALL BE CAST INTO ALL CLOSED LIDS.

MAINTENANCE OF SEWER FLOWS

THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS AS TO MAINTAIN AT ALL TIMES FLOW THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE SHALL ALSO PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT IF NECESSARY AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES WHICH ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. EXISTING STRUCTURES ARE TO BE INSPECTED BEFORE CONSTRUCTION STARTS - ANY ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS SHALL BE REMOVED BY THE CONTRACTOR AT HIS EXPENSE.

MAINTENANCE OF EXISTING DRAINAGE STRUCTURES

WHEN DURING THE CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF ANY GUTTERS AND DRAINAGE STRUCTURE SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE FACILITIES SHALL BE CLEAN AND FREE OF ALL OBSTRUCTIONS DUE TO CONSTRUCTION OPERATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

UNCONTAMINATED SOIL CERTIFICATION

SECTION 22.51 OF THE ENVIRONMENTAL PROTECTION ACT REQUIRES THAT UNCONTAMINATED SOIL, INCLUDING UNCONTAMINATED SOIL MIXED WITH OTHER CLEAN CONSTRUCTION OR DEMOLITION DEBRIS (CCDD) MATERIALS ACCEPTED AT A CCDD LANDFILL FACILITY, MUST BE CERTIFIED TO BE UNCONTAMINATED. DEPENDING ON THE LOCATION OF THE SOIL REMOVAL, THE CERTIFICATION MAY HAVE TO BE MADE BY A LICENSED PROFESSIONAL ENGINEER.

ANY COSTS FOR THE TESTING OF THE UNCONTAMINATED MATERIAL AND HAVING A LICENSED PROFESSIONAL ENGINEER COMPLETE THE REQUIRED PAPERWORK TO CERTIFY THAT THE MATERIAL IS UNCONTAMINATED AND CAN BE PROPERLY DISPOSED AT A CCDD FACILITY SHALL BE BORNE BY THE CONTRACTOR AND SHALL BE INCLUDED IN THE CONTRACT.

IF THE CONTRACTOR CHOOSES TO DISPOSE OF THE EXCAVATED MATERIAL AT MORE THAN ONE CCDD FACILITY, THEN ANY ADDITIONAL COSTS SHALL ALSO BE BORNE BY THE CONTRACTOR. SHOULD ANY CONTAMINATED MATERIAL BE DISCOVERED ON THE PROJECT, THE ADDITIONAL COSTS FOR TESTING AND PROPER DISPOSAL OF THE CONTAMINATED MATERIAL SHALL EITHER BE PAID BY THE VILLAGE UNDER SEPARATE CONTRACT OR AS PART OF THIS CONTRACT AS ADDITIONAL WORK AT A PRICE TO BE AGREED UPON OR ON A TIME AND MATERIAL BASIS.

OPEN EXCAVATIONS

THE CONTRACTOR WILL NOT BE ALLOWED TO LEAVE ANY EXCAVATION NECESSARY FOR PAVEMENT PATCHES OR STRUCTURE ADJUSTMENTS OPEN OVERNIGHT. THE CONTRACTOR WILL BE RESPONSIBLE FOR COMPLETELY BACKFILLING OR INSTALLING A PLATE OVER ALL EXCAVATIONS AT THE END OF EACH DAY.

CONCRETE BREAKERS

WHEN REMOVING PAVEMENT AND/OR OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS SUCH AS DROP HAMMERS, WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES, WILL NOT BE PERMITTED.

SAW CUTTING

THE CONTRACTOR SHALL SAW CUT ASPHALT PAVEMENT AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING PAVEMENT TO BE REMOVED BY APPROVED MEANS OR AN APPROVED CONCRETE SAW TO A DEPTH AS DIRECTED BY THE ENGINEER. SUITABLE GUIDELINES OR DEVICES SHALL BE USED TO ASSURE CUTTING A NEAT, STRAIGHT LINE AS SHOWN ON THE PLANS. CARE SHALL BE TAKEN BY THE CONTRACTOR AS NOT TO DAMAGE THE REMAINING PAVEMENT DIRECTLY ADJACENT TO THE PAVEMENT TO BE REMOVED. ANY DAMAGE TO THE EXISTING PAVEMENT RESULTING FROM PAVEMENT REMOVAL OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE COST OF SAW CUTTING DESCRIBED ABOVE SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

THIS ITEM ONLY PERTAINS TO STRUCTURES LOCATED IN THE CONCRETE OR HOT-MIX ASPHALT ROADWAY PAVEMENT AREAS THAT WILL REQUIRE CONCRETE OR HOT-MIX SURFACE REMOVAL. THE ENGINEER WILL MARK IN THE FIELD ALL STRUCTURES TO BE ADJUSTED UNDER THIS ITEM. SEE DETAIL SHEET FOR "FRAMES AND LIDS ADJUSTMENT WITH MILLING."

FIELD OFFICE

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.

BUTT JOINTS

A BUTT JOINT WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

MILLED PAVEMENT OPEN TO TRAFFIC

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H)

PAVING OPERATIONS

THE CONTRACTOR WILL BE REQUIRED TO SCHEDULE HIS OPERATIONS SO THAT NO SECTIONS OF PAVEMENT ALONG THE CENTERLINE WILL HAVE A COLD JOINT OVERNIGHT. THE CONTRACTOR WILL BE REQUIRED TO PAVE THE ROADWAY SURFACE IN TWO (2) PASSES.

PAVEMENT PATCHING

LOCATIONS OF CLASS D PATCHES ON PLANS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN FIELD BY ENGINEER. CLASS D PATCHES LOCATED WITHIN THE THROUGH LANES SHALL BE MADE ACCESSIBLE TO TRAFFIC AT THE END OF EACH WORK DAY.

Drawing file: W:\Projects\By_Village\Melrose Park\60014276 - Division Street LIFO\Index (Division).dwg Feb 12, 2015 - 9:27am



◆ Civil Engineers
◆ Municipal Consultants
◆ Established 1911

5933 Roosevelt Road
Wheaton, Illinois 60154-2780
Phone: 708/965-0390
Fax: 708/965-1212

DESIGNED --	-	REVISED --	-
DRAWN --	MK, DMM	REVISED --	-
CHECKED --	-	REVISED --	-
DATE --	02/12/15	REVISED --	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. - TO STA. -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	14-00129-00-RS	COOK	12	3
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61B52	
FED. AID PROJECT			E.U.E. PROJECT NO. 60014276	

SUMMARY OF QUANTITIES

S.P.	S.I.	Code No.	Item	Unit	Total Quantity	Const. Type Code Resurfacing 0005 80% Federal 20% Local
	*	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	750	750
	*	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	15	15
	*	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	15	15
	*	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	15	15
	*	25200100	SODDING	SQ YD	750	750
		25200200	SUPPLEMENTAL WATERING	UNIT	10	10
#		28000510	INLET FILTERS	EACH	18	18
		35800100	PREPARATION OF BASE	SQ YD	1200	1200
		35800200	AGGREGATE BASE REPAIR	TON	240	240
#		40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	17000	17000
		40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	20	20
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	225	225
		40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	925	925
		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	625	625
#		40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	5	5
		42101300	PROTECTIVE COAT	SQ YD	1200	1200
		42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	275	275
		42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	6700	6700
#		42400800	DETECTABLE WARNINGS	SQ FT	490	490
#		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	275	275
#		44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1200	1200
#		44000600	SIDEWALK REMOVAL	SQ FT	6700	6700

S.P.	S.I.	Code No.	Item	Unit	Total Quantity	Const. Type Code Resurfacing 0005 80% Federal 20% Local
		44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	125	125
		44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	125	125
		44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	200	200
		44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	200	200
#		60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE I FRAME, OPEN LID	EACH	12	12
#		60200205	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE I FRAME, CLOSED LID	EACH	1	1
#		60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	2	2
#		60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	10	10
#		60406100	FRAMES AND LIDS, TYPE I, CLOSED LID	EACH	1	1
		60600605	CONCRETE CURB, TYPE B	FOOT	120	120
#		60604100	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (MODIFIED)	FOOT	1200	1200
		67100100	MOBILIZATION	L SUM	1	1
		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
		70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
		70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
		70300100	SHORT TERM PAVEMENT MARKING	FOOT	325	325
	*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1300	1300
	*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	200	200
	*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	325	325
#		X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	6750	6750
#		X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	21	21
#		X8140115	HANDHOLE TO BE ADJUSTED	EACH	7	7

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HANCOCK ENGINEERING
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 ♦ Municipal Consultants
 ♦ Established 1911
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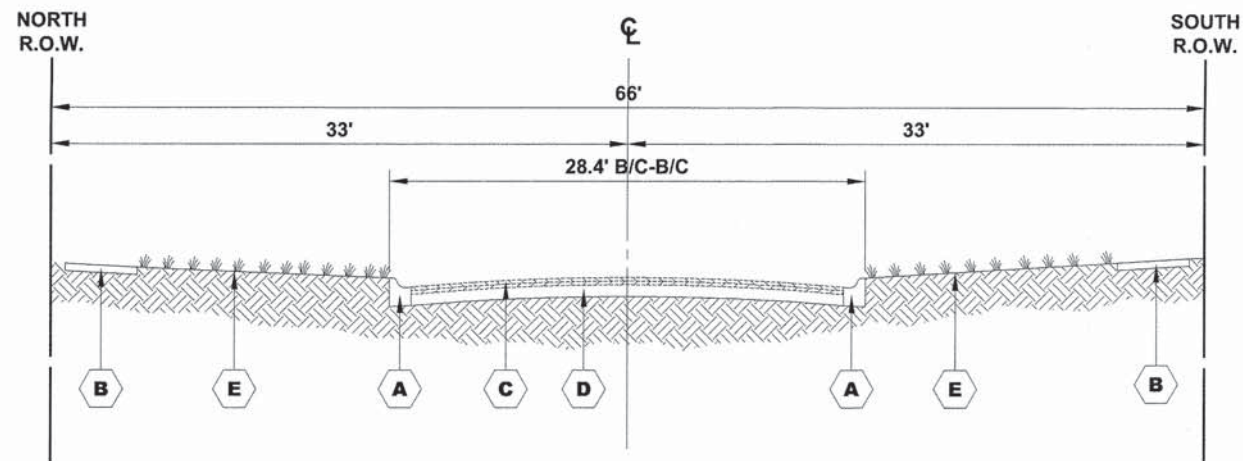
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DATE -- 02/12/15	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

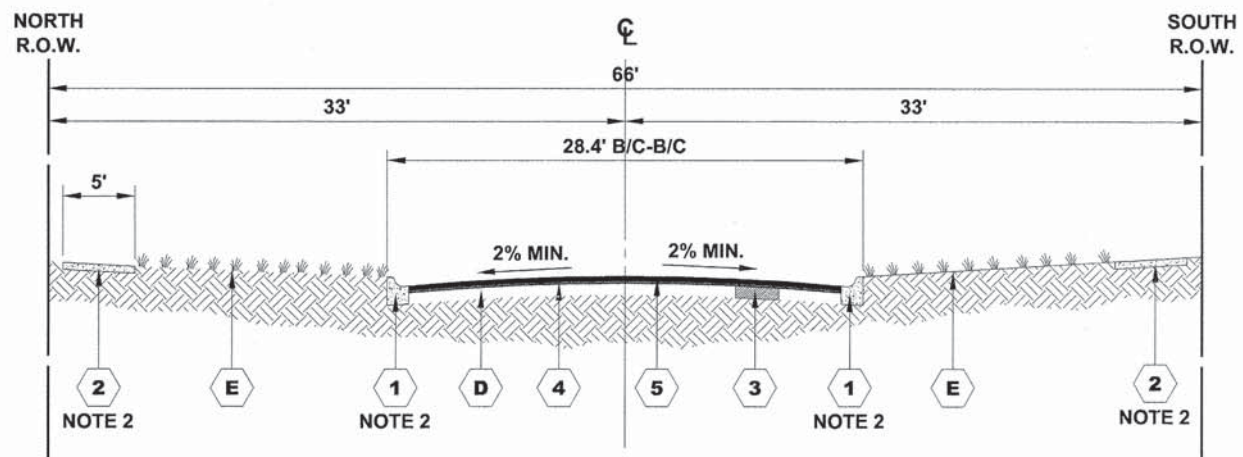
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	14-00129-00-RS	COOK	12	4
FED. ROAD DIST. NO. 1 ILLINOIS		CONTRACT NO. 61B52		
FED. AID PROJECT		E.H.E. PROJECT NO. 600-14-27601		



EXISTING TYPICAL SECTION
DIVISION STREET
STATION 0+25 TO STATION 20+48

LEGEND OF SYMBOLS	
SYMBOL	DESCRIPTION
EXISTING	A EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
	B EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
	C EXISTING HOT-MIX ASPHALT BINDER AND SURFACE COURSES, THICKNESS 4"
	D EXISTING AGGREGATE BASE COURSE, THICKNESS 12"
	E EXISTING LANDSCAPED PARKWAY
PROPOSED	1 PROPOSED INTERMITTENT COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, TYPE B-6.12
	2 PROPOSED INTERMITTENT PORTLAND CEMENT CONCRETE SIDEWALK, 5"
	3 PROPOSED CLASS D PATCHES, 6" (AS LOCATED IN FIELD)
	4 PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/4"
	5 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50, 1 1/2"



PROPOSED TYPICAL SECTION
DIVISION STREET
STATION 0+25 TO STATION 20+48

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5mm), 2"	4% @ 50 Gyr
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2-1/4"	4% @ 50 Gyr
CLASS D PATCHES (HMA BINDER IL-19mm), 6" (2 LIFTS)	4% @ 70 Gyr
INCIDENTAL HOT-MIX ASPHALT SURFACING (HMA SURFACE, MIX "D", N50 IL 9.5mm), 3"	4% @ 50 Gyr

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS

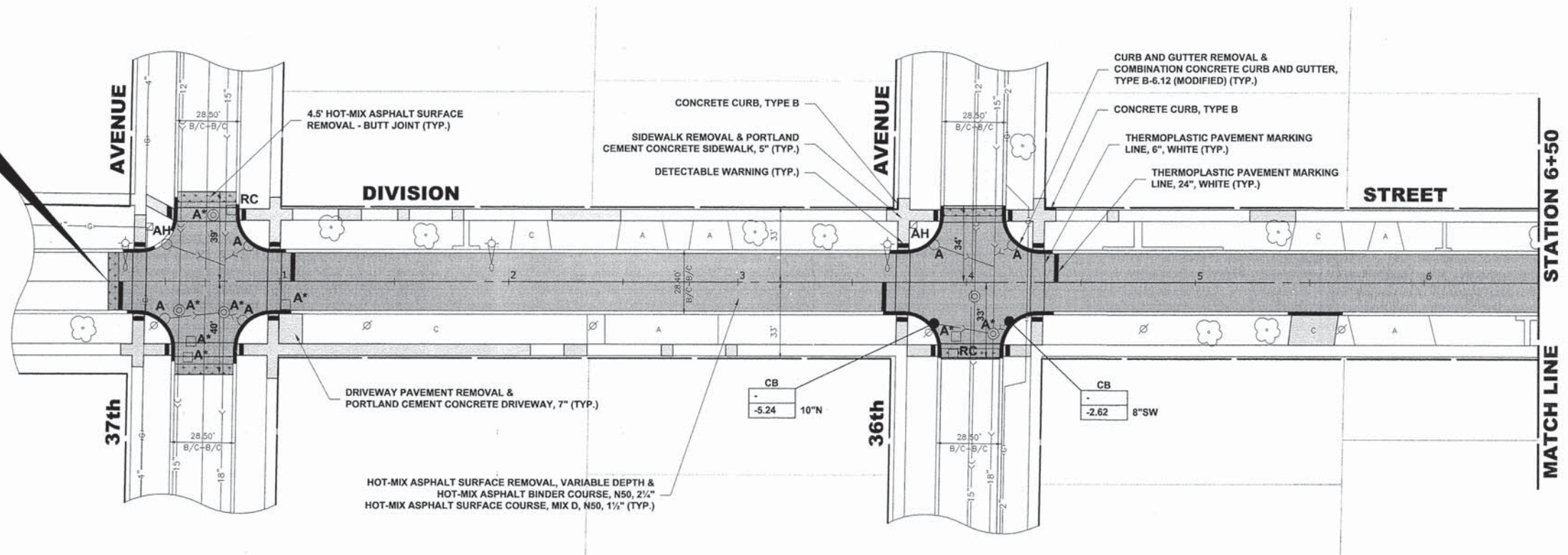
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS

- NOTE**
- CONTRACTOR SHALL MILL BEFORE PATCHING
 - SIDEWALK AND CURB & GUTTER LIMITS AS SHOWN ON PLANS

Drawing file: W:\Projects\by_Milrose_Park\60014276 - Division Street_LAFD\TypSec (Division).dwg Feb 12, 2015 - 9:32am

**IMPROVEMENTS BEGIN
DIVISION STREET
STATION 0+25**

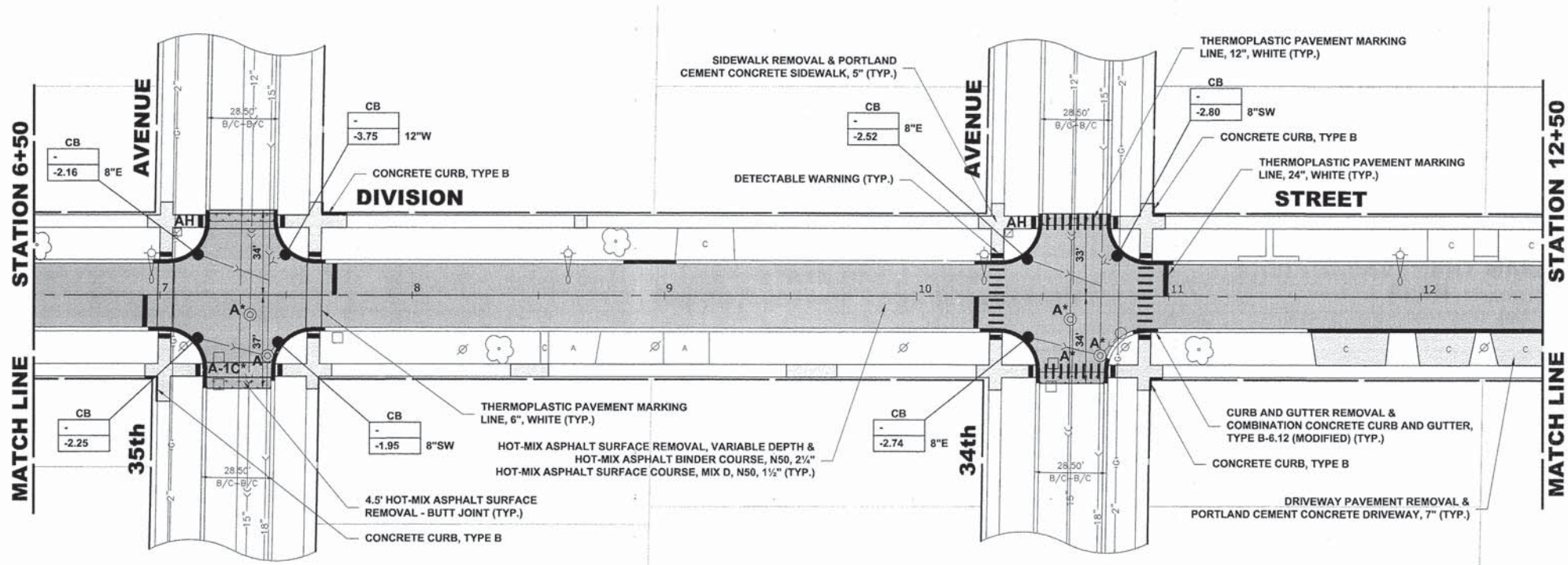
LOCATION OF STOP BARS
STA. 0+31
STA. 1+05
STA. 3+64
STA. 4+39



N

**PROPOSED
IMPROVEMENT**

LOCATION OF STOP BARS
STA. 6+95
STA. 7+19
STA. 10+24
STA. 10+99



N

**PROPOSED
IMPROVEMENT**

Drawing file: W:\Projects\by_Village\Melrose Park\60014276 - Division Street LAP\Site\Division St.dwg Feb 12, 2015 - 9:32am



♦ Civil Engineers
 ♦ Municipal Consultants
 ♦ Established 1911
 9933 Roosevelt Road
 Worthon, Illinois 60154-2780
 Phone: 708/965-0360
 Fax: 708/965-1313

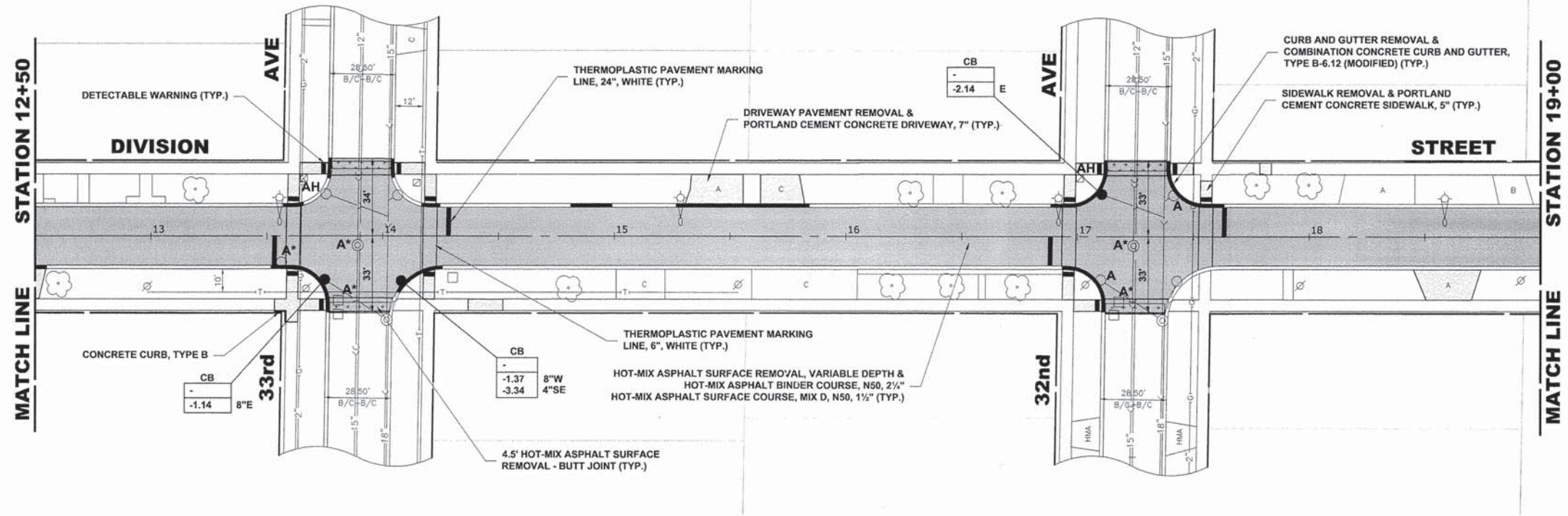
DESIGNED --	REVISION --
DRAWN -- MK, DMM	REVISION --
CHECKED --	REVISION --
DATE -- 02/12/15	REVISION --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVING / PAVEMENT MARKING PLAN	
SCALE: 1"=30'	SHEET NO. 1 OF 2 SHEETS
STA. 0+25	TO STA. 12+50

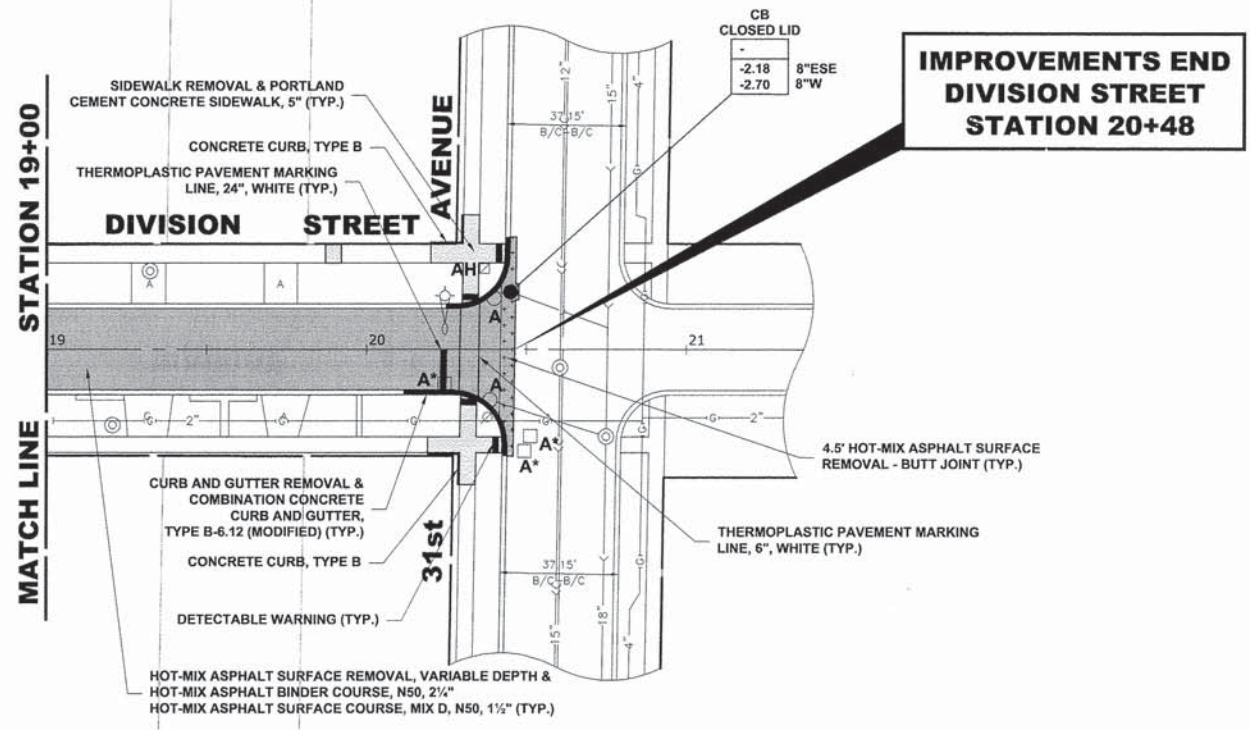
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	14-00129-00-RS	COOK	12	6
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61B52	
FED. AID PROJECT				

LOCATION OF STOP BARS	
STA. 13+53	
STA. 14+29	
STA. 16+88	
STA. 17+64	



PROPOSED IMPROVEMENT

LOCATION OF STOP BARS	
STA. 20+24	



PROPOSED IMPROVEMENT

Drawing file: W:\Projects\by_Village\Metrose Post\60014276 - Division Street L&P\Site\Division St.dwg Mar 11, 2015 - 9:44am



♦ Civil Engineers
 ♦ Municipal Consultants
 ♦ Established 1911
 9933 Roosevelt Road
 Waukegan, Illinois 60154-2780
 Phone: 708/945-0180
 Fax: 708/945-1212

DESIGNED	- -	REVISED	- -
DRAWN	- MK, DMM	REVISED	- -
CHECKED	- -	REVISED	- -
DATE	- 02/12/15	REVISED	- -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVING / PAVEMENT MARKING PLAN
 SCALE: 1"=30'
 SHEET NO. 2 OF 2 SHEETS
 STA. 12+50 TO STA. 21+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	14-00129-00-RS	COOK	12	7
CONTRACT NO. 61B52			FED. AID PROJECT	

E.H.E. PROJECT NO. 600-14-27601

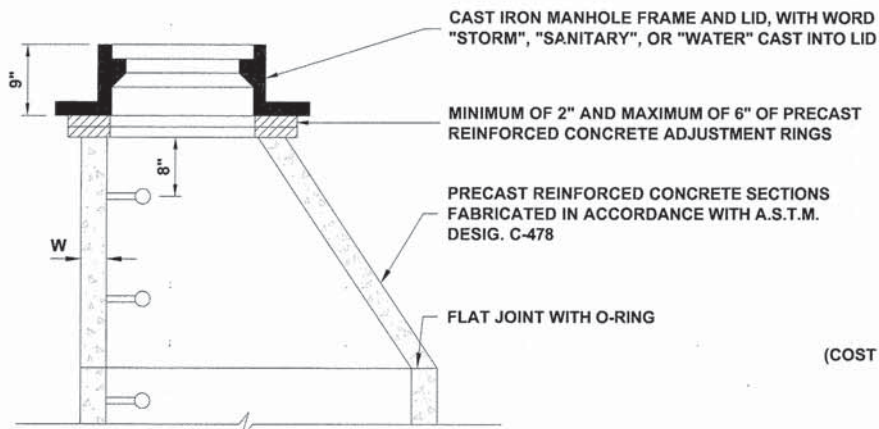
M.W.R.D.G.C. GENERAL NOTES

1. THE MWRD LOCAL SEWER SYSTEMS SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF ANY WORK. THE FIELD OFFICE PHONE NUMBER IS 708-588-4055.
2. ELEVATION DATUM IS USGS.
3. THE MWRD CONSIDERS 0.00 CHICAGO CITY DATUM (CCD) TO BE 579.48 MSL 1929 ADJUSTMENT.
4. ALL FLOOR DRAINS SHALL DISCHARGE TO THE SANITARY SEWER. N/A
5. ALL DOWNSPOUTS AND FOOTING DRAINS SHALL DISCHARGE TO THE STORM SEWER SYSTEM. N/A
6. ALL SANITARY SEWER CONSTRUCTION (AND STORM SEWER CONSTRUCTION IN COMBINED SEWER AREAS), REQUIRES STONE BEDDING WITH STONE 1/4 INCH TO 1 INCH IN SIZE, WITH MINIMUM BEDDING THICKNESS EQUAL 1/4 THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR (4) INCHES NOR MORE THAN EIGHT (8) INCHES. MATERIAL SHALL BE IDOT GRADATION CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 12 INCHES ABOVE THE TOP OF THE PIPE WHEN PVC PIPE IS USED. DUCTILE IRON DOES REQUIRE STONE BEDDING. IF A CONCRETE CRADLE OR ENCASUREMENT IS PROVIDED, BEDDING CAN BE ELIMINATED. <PROVIDE DETAIL>
7. A NON-SHEAR MISSION COUPLING SHALL BE USED FOR THE CONNECTION OF SEWER PIPES OF DISSIMILAR MATERIALS.
8. WHEN CONNECTING TO AN EXISTING SEWER MAIN BY OTHER THAN AN EXISTING WYE, TEE OR AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:

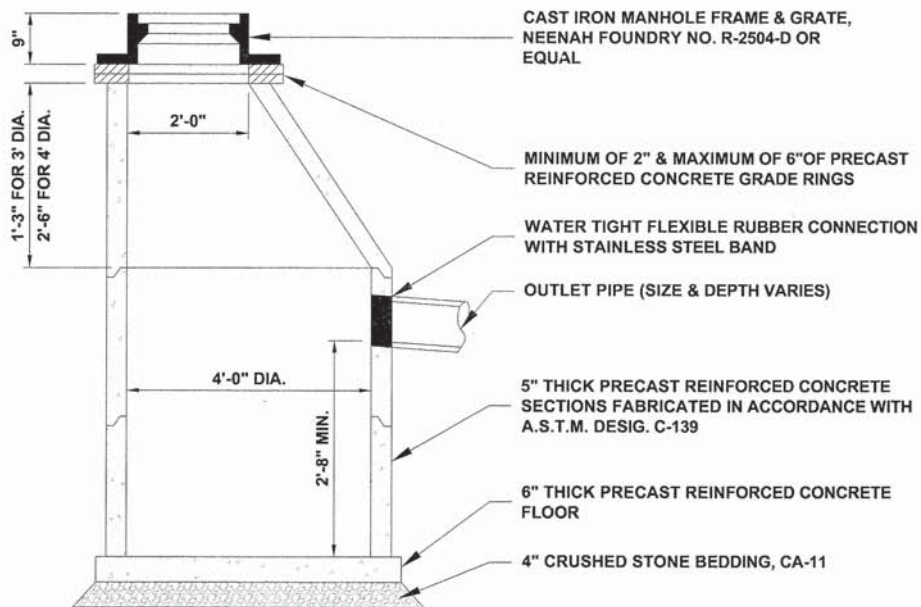
 USING A CIRCULAR CORING MACHINE, CORE DRILL AN OPENING INTO THE EXISTING PIPE AND INSTALL A SADDLE OR PREFABRICATED TEE.

 REMOVE AN ENTIRE SECTION OF THE PIPE BREAKING ONLY THE TOP OF ONE BELL AND REPLACE WITH A WYE OR TEE BRANCH SECTION.

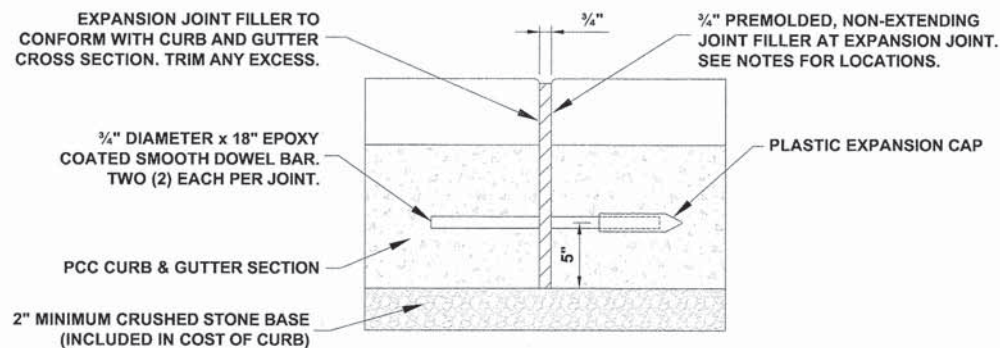
 WITH A PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING, USING A NON-SHEAR MISSION COUPLING TO HOLD IT FIRMLY ON PLACE.
9. WHENEVER A SANITARY/COMBINED SEWER CROSSES UNDER A WATER MAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATER MAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY/COMBINED SEWERS AND WATER MAINS SHALL BE MAINTAINED UNLESS: THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18-INCH VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATER MAIN LOCATED ON THE OPPOSITE SIDE ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18-INCH VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED ABOVE CANNOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATER MAIN, THE SEWER SHALL BE CONSTRUCTED TO WATER MAIN STANDARDS.
10. ALL EXISTING SEPTIC SYSTEMS SHALL BE ABANDONED. ABANDONED TANKS SHALL BE FILLED WITH ACCEPTABLE GRANULAR MATERIAL OR REMOVED.
11. ALL SANITARY MANHOLES AND STORM MANHOLES IN COMBINED SEWER AREAS SHALL HAVE A MINIMUM INSIDE DIAMETER OF 48 INCHES AND SHALL BE CAST IN PLACE CONCRETE OR PRE-CAST REINFORCED CONCRETE.
12. FOOTING DRAINS. EXCEPT FOR FOUNDATION/FOOTING DRAINS PROVIDED TO PROTECT BUILDINGS AND FOR UNDERDRAINS SERVING GREEN INFRASTRUCTURE, DRAIN TILES/FIELD TILES/UNDERDRAINS/PERFORATED PIPES ARE NOT ALLOWED TO BE CONNECTED TO OR TRIBUTARY TO COMBINED SEWERS, SANITARY SEWERS, OR STORM SEWERS TRIBUTARY TO COMBINED SEWERS IN COMBINED SEWER AREAS. CONSTRUCTION OF NEW FACILITIES OF THIS TYPE IS PROHIBITED; AND ALL EXISTING DRAIN TILES AND PERFORATED PIPES, OTHER THAN THOSE SERVING GREEN INFRASTRUCTURE, ENCOUNTERED WITHIN THE PROJECT AREA SHALL BE PLUGGED OR REMOVED, AND SHALL NOT BE CONNECTED TO COMBINED SEWERS, SANITARY SEWERS, OR STORM SEWERS TRIBUTARY TO COMBINED SEWERS.



STRUCTURE RECONSTRUCTION

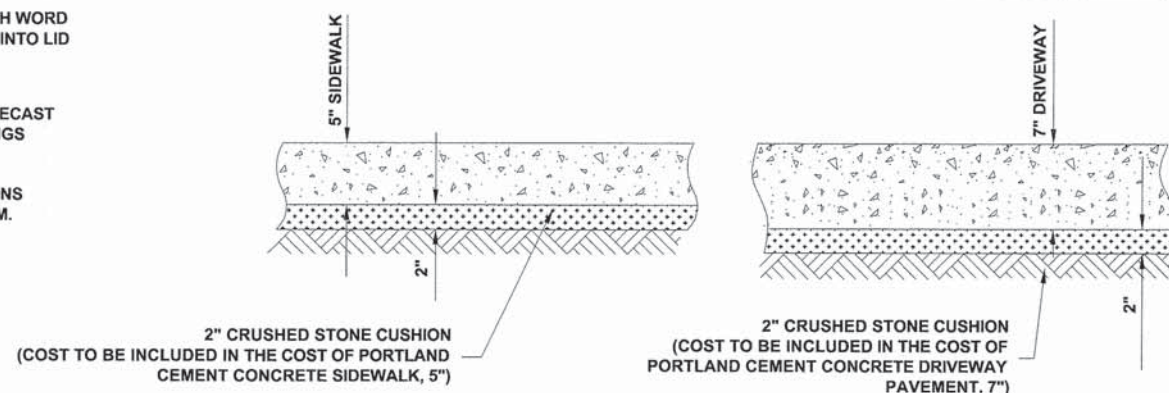


CATCH BASIN, TYPE A

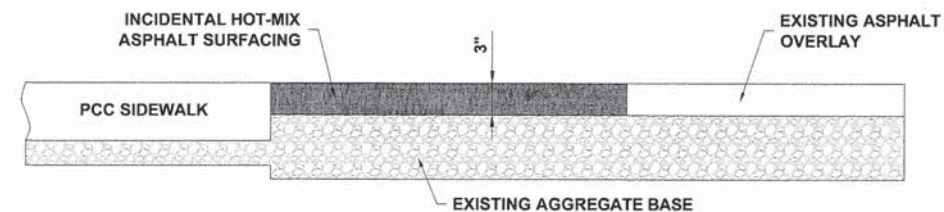


NOTE:
1. EXPANSION JOINTS ARE TO BE CONSTRUCTED AT ALL PC'S & PT'S OF INTERSECTION RETURNS AND ALL OTHER SHORT RADIUS SECTIONS, CONSTRUCTION JOINTS, EVERY 60' ON TANGENT SECTIONS, AND AS DIRECTED BY THE ENGINEER.

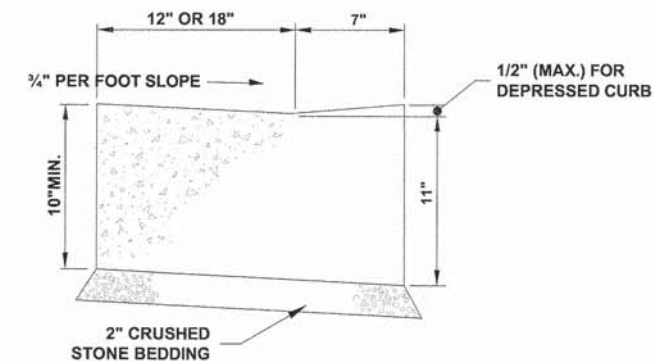
TYPICAL CURB AND GUTTER EXPANSION JOINT



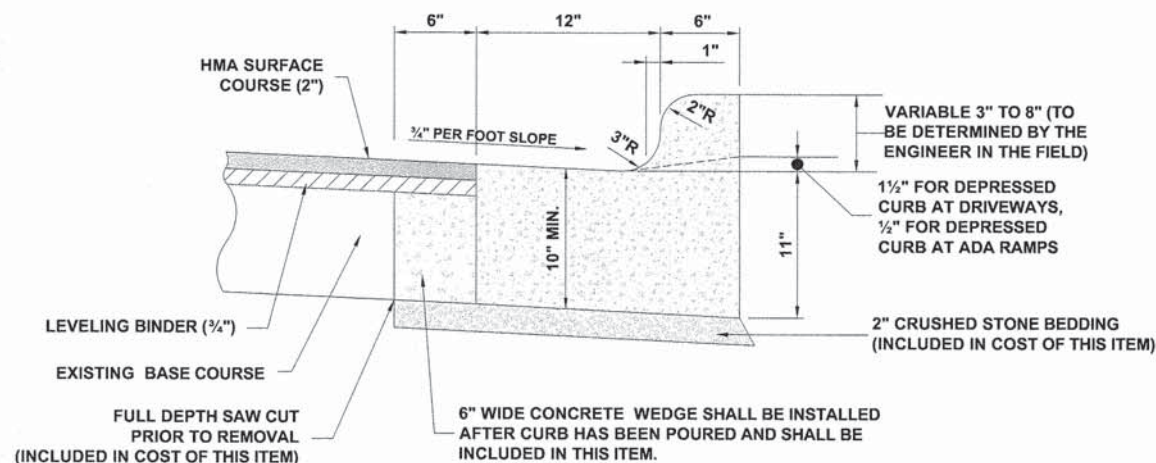
TYPICAL P.C.C. SIDEWALK & DRIVEWAY



INCIDENTAL HOT-MIX ASPHALT SURFACE

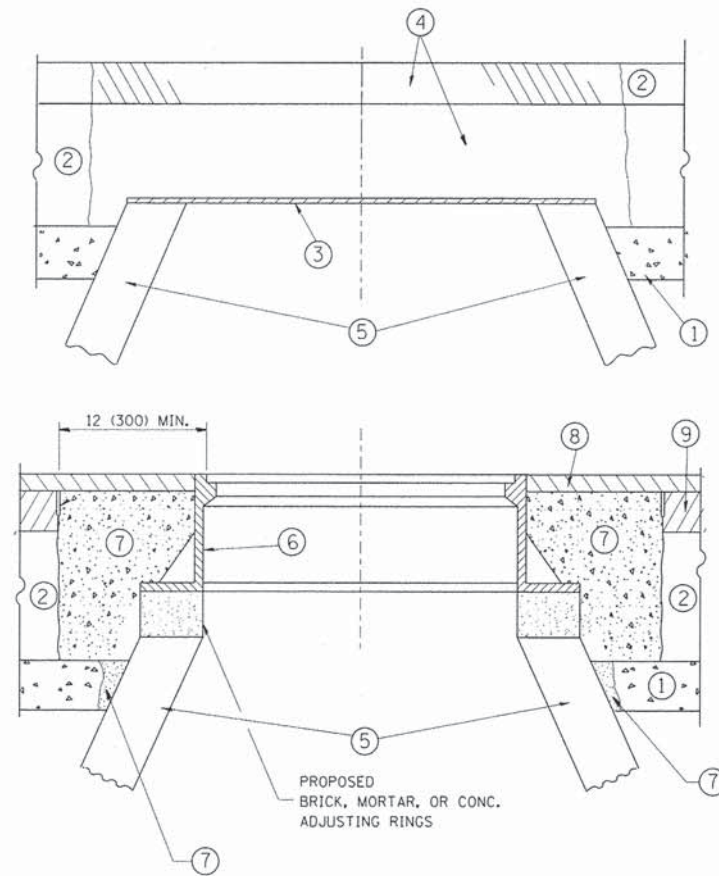


CURB AND GUTTER AT A.D.A. RAMPS



COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12 (MODIFIED)

Drawing file: W:\Projects_by_Village\Metrese Park\0014276 - Division Street LAD\DOT Standards (Division).dwg Feb 12, 2015 - 9:29am



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

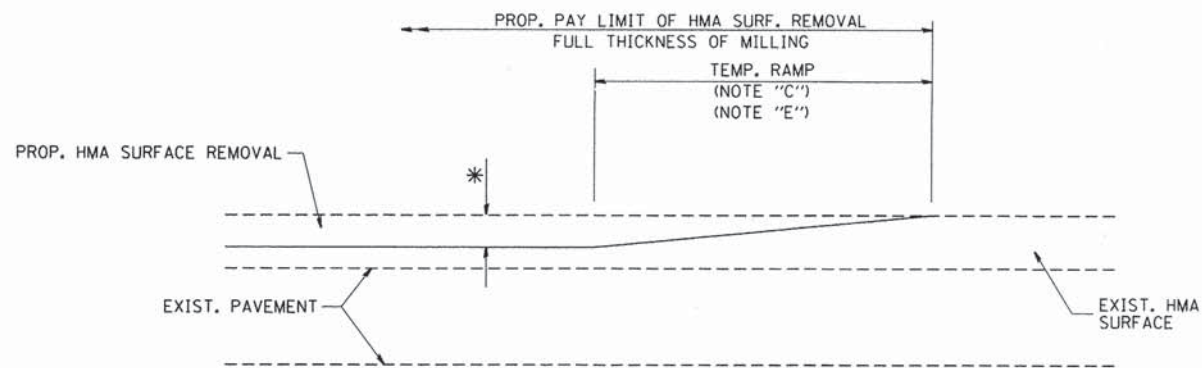
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

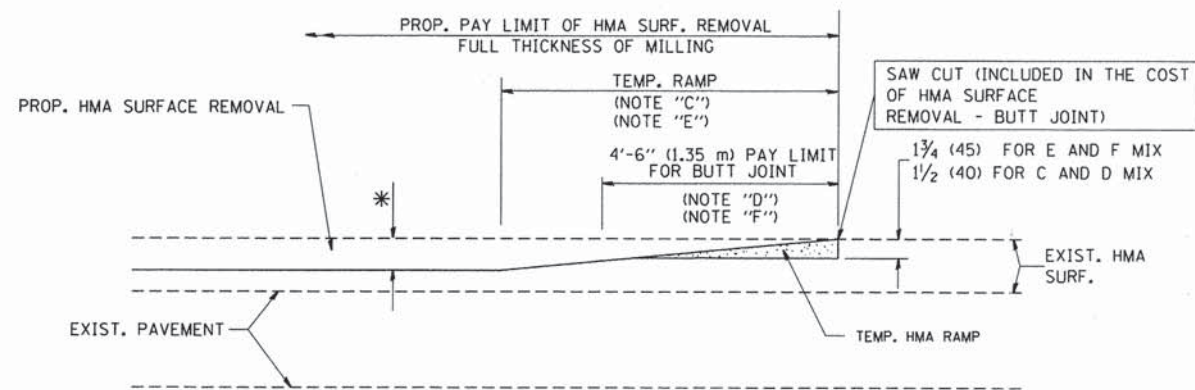
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\pw_work\puidot\bauerdl\0108315\bd08.sgn		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	14-00129-00-RS	COOK	12	9	
PLOT SCALE = 1/8" = 1'-0"		CHECKED -	REVISED - R. BORO 03-09-11					BD600-03 (BD-8)	CONTRACT NO. 61B52			
PLOT DATE = 12/6/2011		DATE - 10-25-94	REVISED - R. BORO 12-06-11					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



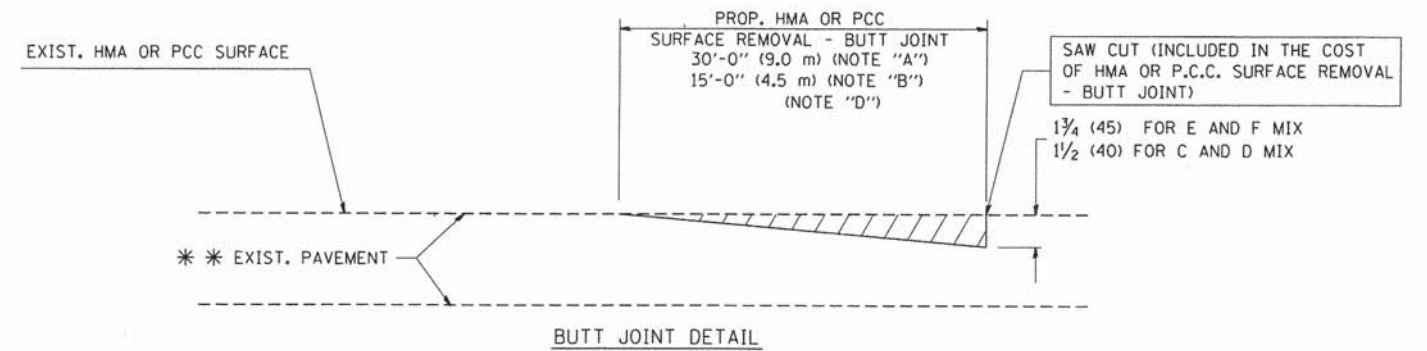
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

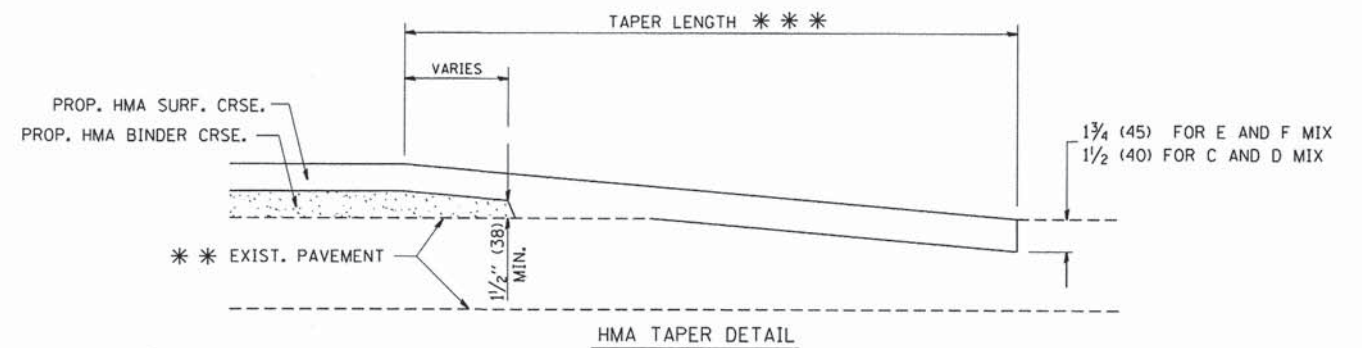


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

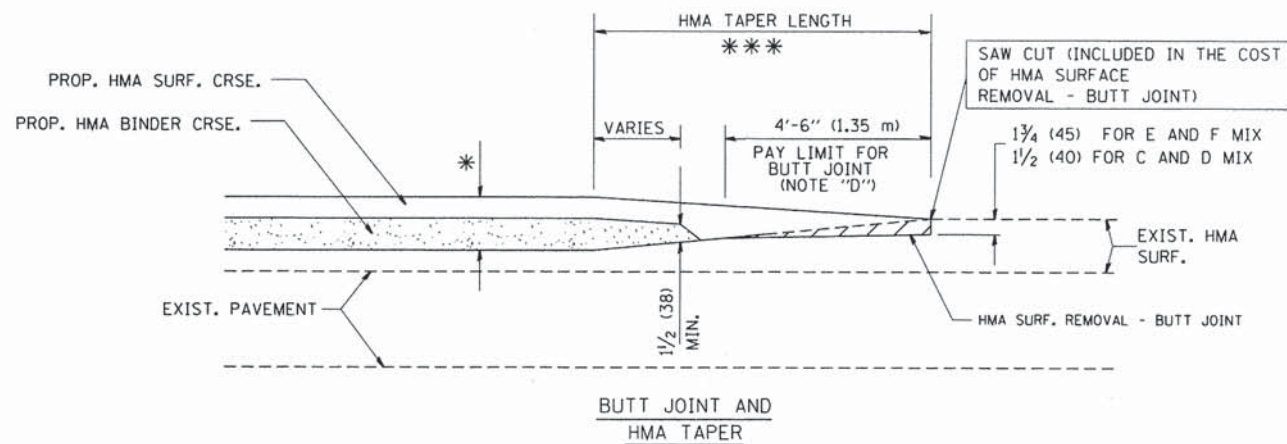
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING

FILE NAME =
W:\diststd\22x34\bd32.dgn

USER NAME = gegl1anabt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT DATE = 1/4/2008	CHECKED -	REVISED - M. GOMEZ 04-06-01
	DATE - 06-13-90	REVISED - R. BORO 01-01-07

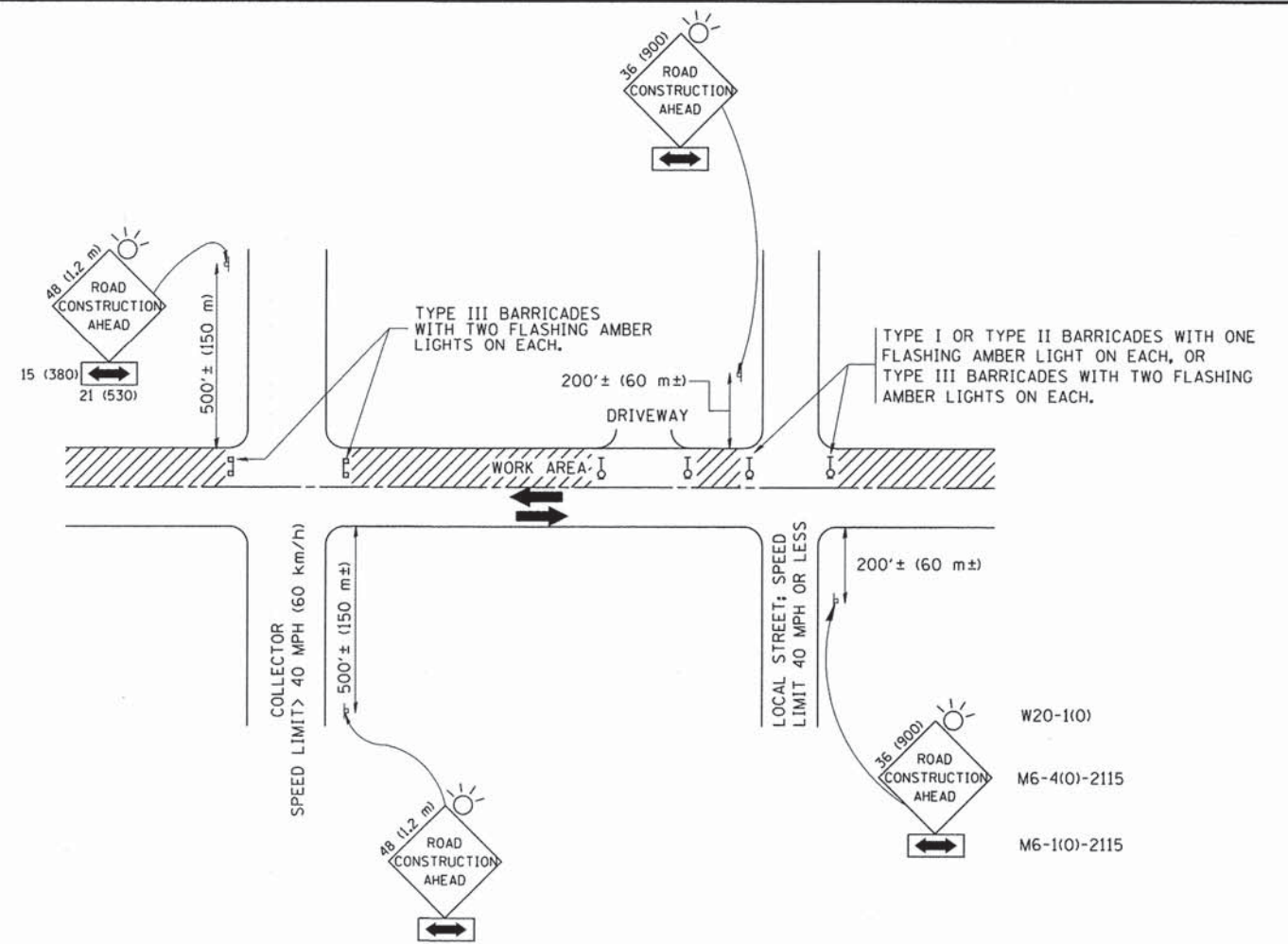
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	14-00129-00-RS	COOK	12	10
BD400-05 BD32		CONTRACT NO. 61B52		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Drawing file: W:\Projects_by_Village\Melrose Park\10014276 - Division Street LRF\001 Standards (Division).dgn Feb 12, 2015 - 9:30am



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

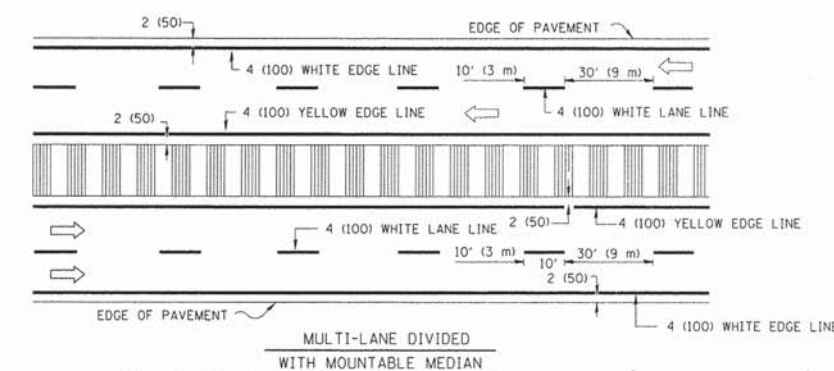
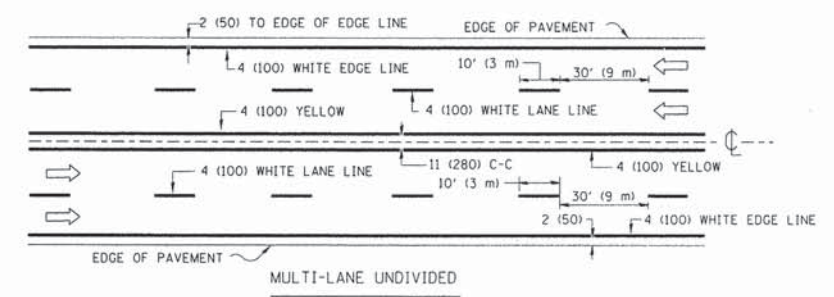
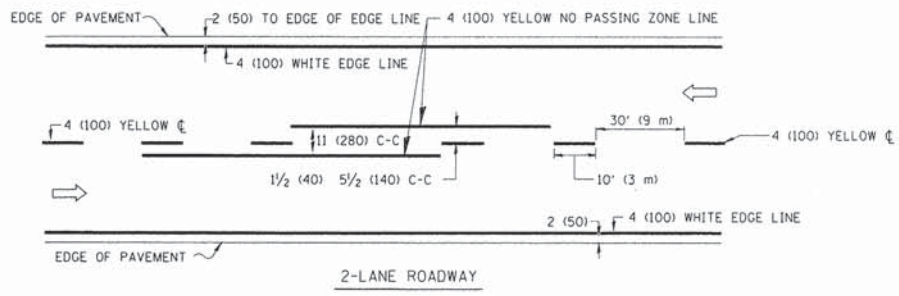
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

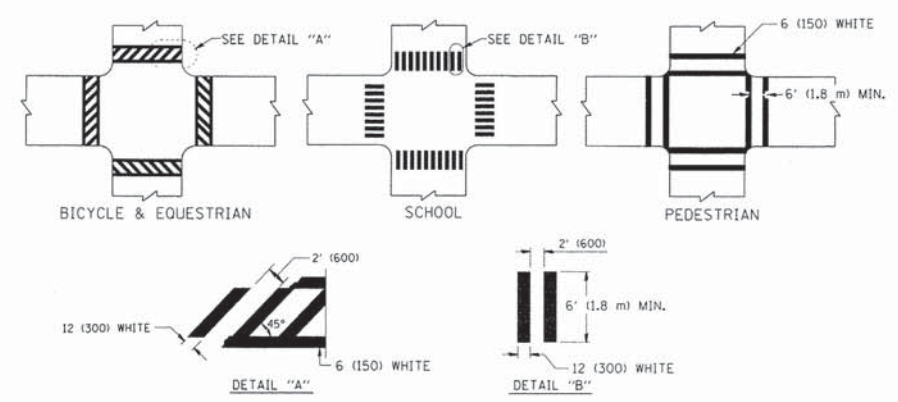
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	14-00129-00-RS	COOK	12	11
TC-10		CONTRACT NO. 61B52		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

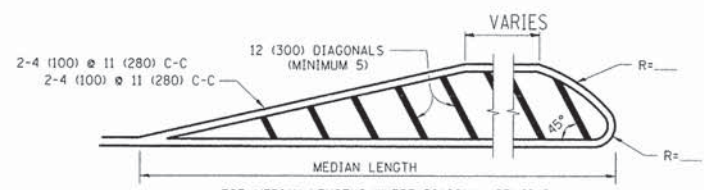
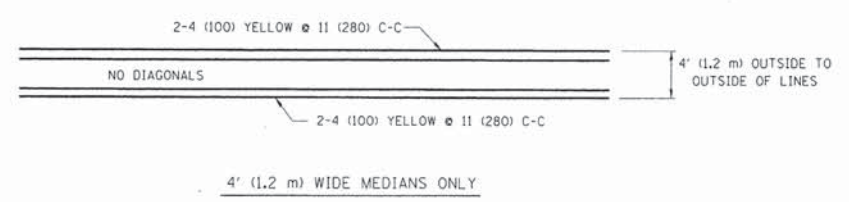


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

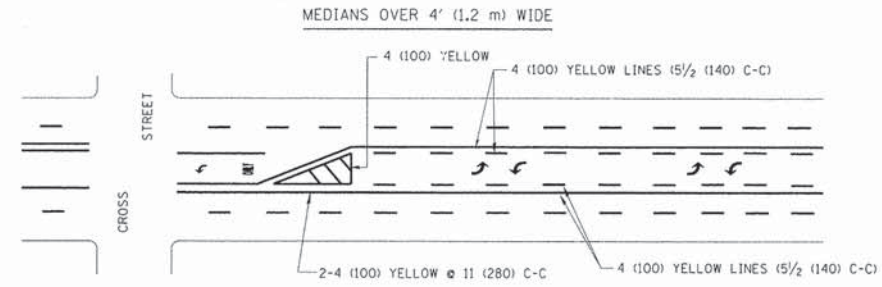
TYPICAL LANE AND EDGE LINE MARKING



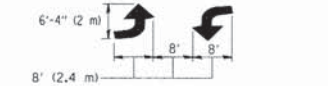
TYPICAL CROSSWALK MARKING



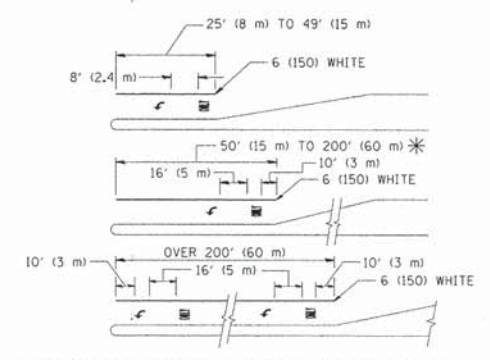
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



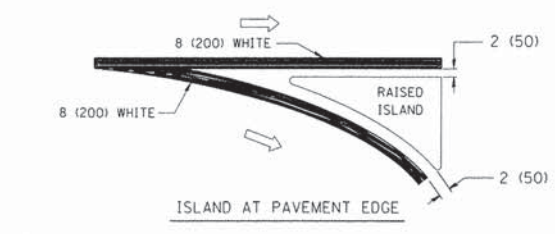
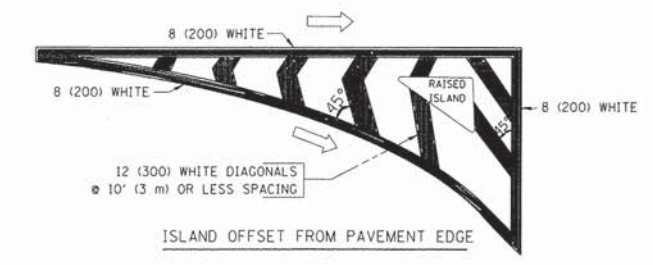
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (10.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

Drawing file: W:\Projects_by_Village\Melrose Park\50014276 - Division Street L&D\DOT Standards (Division).dgn Feb 12, 2015 - 9:30am

FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
c:\pwork\pwork\drivakosgn\d018315\td	3.dgn	DRAWN -	REVISED -C. JUCIUS 09-09-09
PLOT SCALE = 50.800 ' / IN.		CHECKED -	REVISED -
PLOT DATE = 9/9/2009		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	14-00129-00-RS	COOK	12	12
TC-13		CONTRACT NO. 61B52		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				