06-12-2015 LETTING ITEM 046

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN: THE VILLAGE OF GRAYSLAKE

THE VILLAGE OF GURNEE THE CITY OF HIGHLAND PARK THE CITY OF LAKE FOREST

THE VILLAGE OF METTAWA
THE CITY OF NORTH CHICAGO

THE CITY OF PARK CITY
THE VILLAGE OF VERNON HILLS

THE CITY OF WAUKEGAN

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

**DIVISION OF HIGHWAYS** 

PROPOSED HIGHWAY PLANS

VARIOUS ROUTES
SECTION: 2015–028RS
VARIOUS LOCATIONS IN EASTERN LAKE COUNTY
INTERMITTENT RESURFACING
LAKE COUNTY
C-91-334-15

FOR GENERAL LOCATION MAP, SEE SHEET NO. 4

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LOCATION OF SECTION INDICATED THUS: --

2015-028RS

D-91-334-15

LAKE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED

DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

20 15

ENGINEER OF DESIGN AND ENVIRONMENT

DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

0 100' 200' 300' — 1" = 100'
0 10' 20' 30' — 1" = 10'
0 50' 100' 1" = 50'
0 50' 100' 1" = 40'
0 50' 100' 1" = 30'

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DANIEL WILGREEN (847) 705–4240 PROJECT MANAGER: KEN ENG

CONTRACT NO. 62A88

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#### INDEX OF SHEETS

#### STATE STANDARDS

SHEET NO.	DESCRIPTION	STANDARD NO.	DESCRIPTION			
1	COVER SHEET	000001 - OG	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS			
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	701011 - 04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY			
3	SUMMARY OF QUANTITIES	701301 - 04	LANE CLOSURE, 2L. 2W, SHORT TIME OPERATIONS			
4	CENERAL LOCATION MAP	701306 - 03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS - DAY ONLY			
5	ROUTE INFORMATION	701311 - 03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY			
6	SUMMARY OF INTERMITTENT RESURFACING SCHEDULE	701336 - 06	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES			
7-30	INTERMITTENT RESURFACING SCHEDULE		LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR			
31	BUTT JOINT AND HMA TAPER DETAILS (BD-32)		SPEEDS > 45 MPH TO 55 MPH			
32	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)	701426 - 07	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS > 45 MPH			
33	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)	701427 - 03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS & 40 MPH			
34	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	701502 - <i>06</i>	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE			
35	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)	701601 - <i>09</i>	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH			
36	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC	.0.001 67	NONTRAVERSABLE MEDIAN			
	STAGING (TC-16)	701602 - 07	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL			
37	ARTERIAL ROAD INFORMATION SIGN (TC-22)		LEFT TURN LANE			
38	STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05, SHEET 2 OF 7)	701606 - 10	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN			
39	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING (TS-07)	701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION			
		701901 - <i>04</i>	TRAFFIC CONTROL DEVICES			

HOT-MIX ASPHALT MIXTURE RI	QUALITY MANAGEMENT			
MIXTURE TYPE	AIR VOIDS (%) @ NoEs.	PROGRAM (OMP)		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5MM), 2"	4% @ 70 GYR	OC / QA		
OMP DESIGNATION: QUALITY CONTROL/QUALITY	ASSUBANCE (OC/OA)	<del> </del>		

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SO YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PC 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS, FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

DUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

#### GENERAL NOTES

BEFORE STARTING ANY EXCAVATION. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA)

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL INTERMITTENT RESURFACING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER AT (847) 438-2300 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE EXISTING ROADWAY TYPICAL SECTION IS ASSUMED TO HAVE A 3 INCH HOT-MIX ASPHALT OVERLAY ON TOP OF A TEN INCH CONCRETE BASE.

ALL INTERMITTENT RESURFACING LOCATIONS SHOWN IN THE PLANS ARE TWO (2) INCH MILL AND RESURFACE ONLY. THE MINIMUM WIDTH FOR INTERMITTENT RESURFACING SHALL BE THREE (3) FEET.

NO PATCHING OR RESURFACING IS TO BE DONE WITHIN FIFTY (50) FEET OF ANY RAILROAD CROSSING.

THE COST OF ANY PARTIAL OR FULL DEPTH PATCHING REQUIRED AFTER THE REMOVAL OF THE EXISTING 2 INCH HOT-MIX ASPHALT SURFACE SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

ANY DETECTOR LOOPS DAMAGED BY MILLING SHALL BE REPLACED IN KIND.

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO QUANTIFY LOOP REPLACEMENTS NEEDED AND PROVIDE THE RESIDENT ENGINEER THIS INFORMATION PRIOR TO GRINDING OR REMOVAL.

ALL LOOP DETECTOR LOCATIONS SHALL BE CURB MARKED BY THE CONTRACTOR PRIOR TO MILLING FOR THE PURPOSE OF REESTABLISHING DETECTOR LOOP LAYOUT AFTER THE RESURFACING IS COMPLETED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).

OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.

ANY MILLED PAVEMENT IS TO BE RESURFACED BY THE END OF EACH DAY AND OPEN TO TRAFFIC.

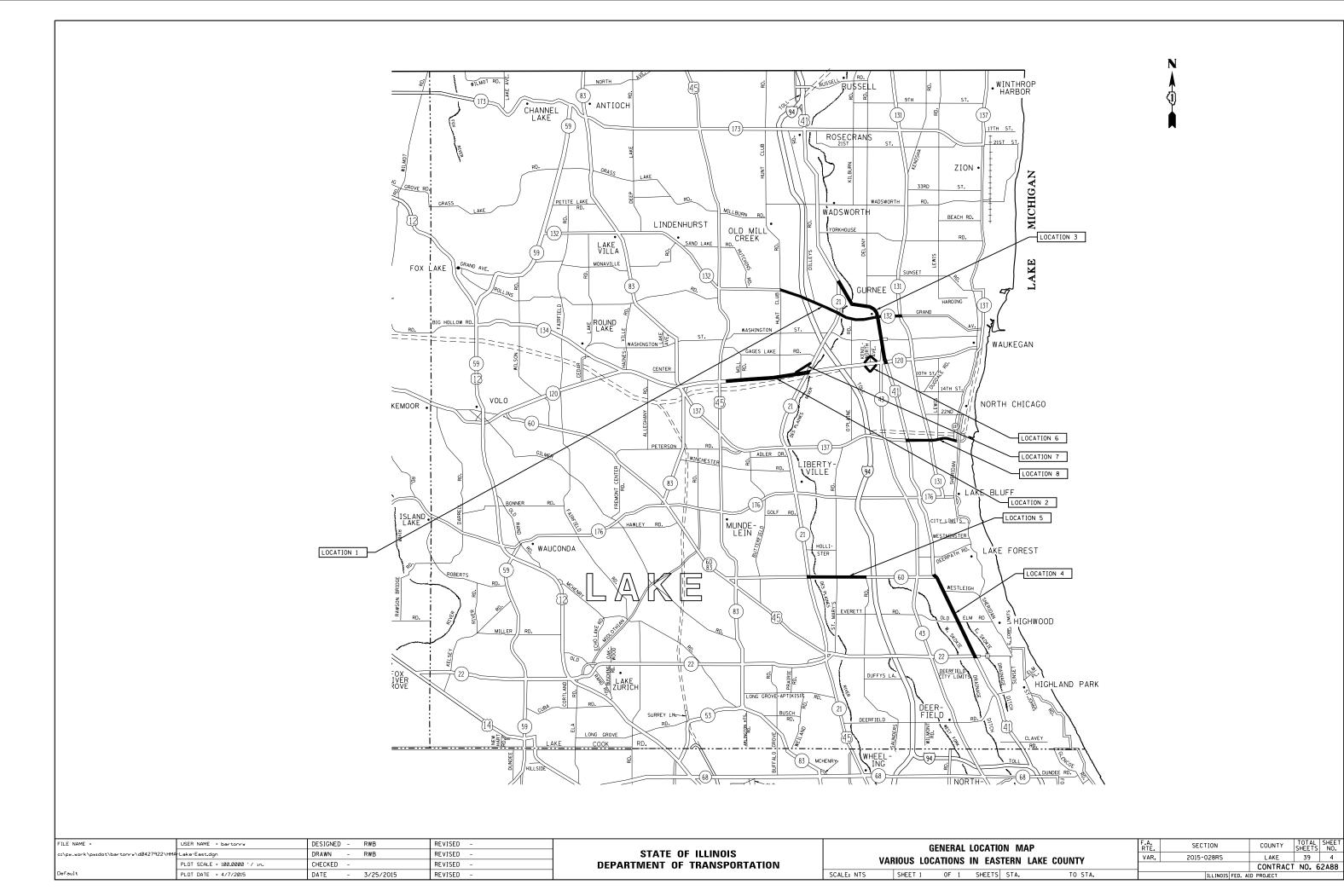
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STAT	E OI	ILLINOIS
DEPARTMENT	0F	TRANSPORTATION

SCALE

NACY OF AUGUS ATATE ATAMBABA AND ATAMBABA	F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES	VAR,	2015-028RS	LAKE	39	2
			CONTRACT	NO. (	2A88
SHEET 1 OF 1 SHEETS STA. TO STA.		INCINOIS FED. A	O PROJECT		

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	SUMMARY - EASTERN LAKE COUNTY ARTERIAL ROUTES	CITIES/VILLAGES	TOWNSHIPS	SPEED LIMIT	EXISTING ADT (YEAR)
LOC.1	IL 132 (IL 131 TO HUNT CLUB RD.)	GURNEE, WAUKEGAN	WARREN, WAUKEGAN	35-45 MPH	41,300 (2013)
LOC.2	IL 120 (IL 21 TO WEST OF SEARS BLVD.)	GRAYSLAKE, GURNEE	WARREN	35-55 MPH	37,500 (2013)
LOC.3	US 41 (IL 120 TO IL 21)	GURNEE, PARK CITY	WARREN	45-55 MPH	37,900 (2013)
LOC.4	US 41 (IL 22 TO IL 60)	HIGHLAND PARK, LAKE FOREST	WEST DEERFIELD	45-55 MPH	49,400 (2013)
LOC.5	IL 60 (RIVERWOODS RD. TO IL 21)	METTAWA, VERNON HILLS	LIBERTYVILLE, VERNON	40-45 MPH	39,300 (2013)
LOC.6	IL 120 (GREENLEAF AVE., ALL RAMPS)	PARK CITY, WAUKEGAN	WARREN	N/A	5,000 (2011)
LOC.7	IL 120 (ENTRANCE RAMP FROM IL 21 TO WB IL 120)	GURNEE	WARREN	25 MPH	1,750 (2011)
LOC.8	IL 137 (US 41 TO SHERIDAN RD.)	NORTH CHICAGO	SHIELDS	45 MPH	18,400 (2013)

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SCALE:

		ROUTE	INFORM	ATION		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
VΔR	VARIOUS LOCATIONS IN EASTERN LAKE COUNTY						2015-028RS	LAKE	39	5	
771	VARIOUS LUCATIONS IN LASTERIN LAKE COUNTY					CONTRACT NO. 62A					
	SHEET 1	OF 1	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT					

		HMA 2" MILL
	SUMMARY - EASTERN LAKE COUNTY ARTERIAL ROUTES	& RESURFACE
		(SY)
LOC.1	IL 132 (IL 131 TO HUNT CLUB RD.)	3,962
LOC.2	IL 120 (IL 21 TO WEST OF SEARS BLVD.)	5,868
LOC.3	US 41 (IL 120 TO IL 21)	2,701
LOC.4	US 41 (IL 22 TO IL 60)	3,991
LOC.5	IL 60 (RIVERWOODS RD. TO IL 21)	1,039
LOC.6	IL 120 (GREENLEAF AVE., ALL RAMPS)	4,829
LOC.7	IL 120 (ENTRANCE RAMP FROM IL 21 TO WB IL 120)	392
LOC.8	IL 137 (US 41 TO SHERIDAN RD.)	1,870
	EASTERN LAKE COUNTY ARTERIAL TOTAL =	24,652
		SY

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SCALE:

SUMMARY OF INTERMITTENT RESURFACING SCHEDULE VARIOUS LOCATIONS IN EASTERN LAKE COUNTY							
SHEET 1 OF 1 SHEETS STA.	TO STA.	_					

## ROUTE: IL 132 (IL 131 to Hunt Club Road)

CROSS STREE	ΕT	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
IL 131		WB	1	12	3	36	4
12 101		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
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		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	6	72	8
		WB	1	12	6	72	8
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		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	3	25	75	8
		WB	1	3	25	75	8
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		WB WB	1		25		
		WB	1	3	25	75	8
		WB WB	1	3	25 25	75 75	8 8
		WB	1	3	25	75	8

### ROUTE: IL 132 (IL 131 to Hunt Club Road)

(Continued)

0, 1001	S STREET	DIRECTION	LANE	PAV/EMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
TROW	10	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		WB	1	3	25	75	8
	-	WB	1	3	25	75	8
	-	WB	1	3	25	75	8
	-	WB	1	3	25	75	8
	-	WB	1	3	25	75	8
	+	WB	1	3	25	75 75	8
	-	WB	1	3	25	75	8
	_	WB	1	3	25	75	8
		WB	1	3	25	75	8
		WB	1	3	100	300	33
	_	WB	1	3	100	300	33
		WB		3	100	300	33
		WB	1	3	100		33
	+	WB	1		100	300	33
		WB	1	3	100	300	33
	+	WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300 300	33
		WB	1	3	100	300	33
		WB WB	1	3	100	300	33 33
		WB	1		100 100	300	33
		WB	1	3	100	300 300	33
		WB WB	1	3	100 100	300	33 33
		WB	1	3	100	300 300	33
		WB	1	3	100	300	33
	Hunt Club Road	WB	1	3	100	300	33
IL 131	Hunt Club Road	WB	2	12	3	36	4
IL IJI	-	WB	2	12	3	36	4
	-	WB	2	12	3	36	4
	-	WB	2	12	3	36	4
				12	3	30	
	<del> </del>		2	12	2	36	
		WB	2	12	3	36	4
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		WB WB WB	2 2	12 12	3	36 36	4
		WB WB WB WB	2 2 2	12 12 12	3 3 3	36 36 36	4 4 4
		WB WB WB WB	2 2 2 2	12 12 12 12	3 3 3	36 36 36 36	4 4 4 4
		WB WB WB WB WB	2 2 2 2 2	12 12 12 12 12	3 3 3 3 3	36 36 36 36 36	4 4 4 4 4
		WB WB WB WB WB WB WB	2 2 2 2 2 2	12 12 12 12 12 12	3 3 3 3 3	36 36 36 36 36 36	4 4 4 4 4
		WB WB WB WB WB WB WB WB	2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12	3 3 3 3 3 3	36 36 36 36 36 36 36	4 4 4 4 4 4
		WB WB WB WB WB WB WB WB WB	2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12	3 3 3 3 3 3 3	36 36 36 36 36 36 36 36	4 4 4 4 4 4 4
		WB	2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12	3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36	4 4 4 4 4 4 4 4
		WB	2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12	3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36	4 4 4 4 4 4 4 4 4
		WB	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12	3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36	4 4 4 4 4 4 4 4 4 4
		WB	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12	3 3 3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36 36	4 4 4 4 4 4 4 4 4 4 4
		WB W	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	3 3 3 3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36 36 36	4 4 4 4 4 4 4 4 4 4 4 4
		WB W	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36 36 36	4 4 4 4 4 4 4 4 4 4 4 4 4
		WB W	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36 36 36 36	4 4 4 4 4 4 4 4 4 4 4 4 4 4
		WB W	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36 36 36 36 3	4 4 4 4 4 4 4 4 4 4 4 4 4 4 4
		WB W	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36 36 36 36 3	4 4 4 4 4 4 4 4 4 4 4 4 4 4 4
		WB W	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36 36 36 36 3	4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4
		WB W	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36 36 36 36 3	4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4
		WB W	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36	4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4
		WB W	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36	4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4
		WB W	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	36 36 36 36 36 36 36 36 36 36	4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMI	TTENT RESU	RFACING SCH	HEDUI F	F.A. RTF.	SECTION	COUNTY	TOTAL SHEET
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS			IL 1		III DOLL	VAR.	2015-028RS	LAKE	39 7
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			IL I	32				CONTRAC	T NO. 62A88
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 1	0F 24 SH	EETS STA.	TO STA.		ILLINOIS FED.	AID PROJECT	

ROUTE:	IL 132 (	IL 131 to Hunt Club Road	$\overline{\mathbf{d}}$ ) (d)	Continued

CROSS STREET   DIRECTION   LAW   PAKEMENT   PAKEMENT   REPAIR   REPAIR	00000	OTDEET	DIDECTION	LANG	I DAY (EN JENIE	DAY ENTENIE	DEDAID	L DEDAID
NISSB  (1.2.3)   WIDTH   LENGTH   (SQ TV)			DIRECTION	LANE			REPAIR	REPAIR
WB   2   12   3   36   4     WB   2   12   6   72   8     WB   2   13   100   300   33     WB   2   3   100   300   33	FROM	10		l	1			1
WB								
WB								
WB								
WB								4
WB			WB	2	12	3		4
WB			WB	2	12	3	36	4
WB			WB	2		3	36	4
WB			WB	2	12	3	36	4
WB			WB	2	12	3	36	4
WB			WB	2	12	3	36	4
WB			WB	2	12	3	36	4
WB			WB	2	12	3	36	4
WB   2   12   3   36   4     WB   2   12   6   72   8     WB   2   3   1000   300   33     WB			WB	2	12		36	4
WB				2				4
WB								
WB   2								
WB   2   12   6   72   8								
WB   2   12   6   72   8								
WB								
WB   2								
WB   2								
WB   2   12   6   72   8								
WB   2   12   6   72   8								
WB   2   12   6   72   8								
WB   2   12   6   72   8								
WB   2   12   6   72   8								
WB   2   12   6   72   8								
WB   2   12   6   72   8								
WB   2   12   6   72   8								
WB         2         12         6         72         8           WB         2         3         100         300         33           WB         2         3         100         300         33 </td <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td>								
WB         2         12         6         72         8           WB         2         3         100         300         33           WB         2         3         100         300 <t< td=""><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></t<>								
WB         2         12         6         72         8           WB         2         3         100         300         33           WB         2         3         100         300								
WB         2         12         6         72         8           WB         2         3         100         300         33           WB         2         3         100         300								
WB         2         12         6         72         8           WB         2         3         100         300         33           Hunt Club Road         WB         2         3 <td< td=""><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></td<>								
WB         2         12         6         72         8           WB         2         12         6         72         8           WB         2         3         100         300         33           Hunt Club RoadWB         2         3         10								
WB         2         12         6         72         8           WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           EB         1         12								l
WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         EB         1         12         3         36         4           EB         1         12         3         36         4           EB <td< td=""><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></td<>								
WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         EB         1         12         3         36         4           EB         1         12         3         36         4           EB <td< td=""><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></td<>								
WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         EB         1         12         3         36         4           EB         1 <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td>								
WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         EB         1         12         3         36         4								
WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         EB         1         12         3         36         4           EB         1								
WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         EB         1         12         3         36         4           EB <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td>								
WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         EB         1         12         3         36         4           EB         1         1								
WB         2         3         100         300         33           WB         2         3         100         300         33           WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         EB         1         12         3         36         4           EB         1         12 </td <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td>								
WB         2         3         100         300         33           WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         EB         1         12         3         36         4           EB         1         12								
WB         2         3         100         300         33           Hunt Club Road         WB         2         3         100         300         33           Hunt Club Road         EB         1         12         3         36         4           EB         1         12								
Hunt Club Road   WB   2   3   100   300   33     Hunt Club Road   EB   1   12   3   36   4								
Hunt Club Road								
EB     1     12     3     36     4		Hunt Club Road						
EB     1     12     3     36     4	Hunt Club Road							
EB     1     12     3     36     4								
EB     1     12     3     36     4				1				
EB     1     12     3     36     4       EB     1     12     3     36     4       EB     1     12     3     36     4				1	12		36	
EB         1         12         3         36         4           EB         1         12         3         36         4				1				4
EB 1 12 3 36 4				1				
EB 1 12 3 36 4			EB	1	12	3	36	4
			EB	1	12			
				1	12	3		4

ROUTE: IL 132 (IL 131 to Hunt Club Road	) (Continued)

FROM   TO   (EB/M8)   (N)   NO.   PATCH   AREA   AREA   (SQ YP)	CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
(NBSS) (12.3) WDTH (ENGTH (SD PT) (SD PD)  EB 1 1 12 3 3 36 4  EB 1 1 12 3 36 4  EB 1 1 12 3 3 36 4  EB 1 1 12 5 3 3 36 4  EB 1 1 12 6 72 8  EB 1 1					PATCH	PATCH		AREA
EB								
EB								
EB				1				
EB								
BB								4
EB								
EB 1 12 3 36 4 EB 1 12 6 72 8 EB 12 6								
EB								
EB								
BB								
BB		+						
BB								
BB		+						
EB		+						
EB		+						
EB		+						
EB								
EB								
EB								
EB 1 12 6 72 8  EB 1 13 55 75 8  EB 1 3 25 75 8  EB 1 3 3 50 150 17  EB 1 3 50 150 17								
EB 1 12 6 72 8 EB 1 13 25 75 8 EB 1 3 3 2								
EB		+						
EB		+						
EB         1         12         6         72         8           EB         1         3         25         75         8		+						
EB								
EB 1 12 6 72 8  EB 1 3 25 75 8  EB 1 3 3 50 150 17  EB 1 3 50 150 17								
EB         1         12         6         72         8           EB         1         3         25         75         8								
EB         1         12         6         72         8           EB         1         3         25         75         8								
EB         1         12         6         72         8           EB         1         12         6         72         8           EB         1         12         6         72         8           EB         1         3         25         75         8								
EB         1         12         6         72         8           EB         1         12         6         72         8           EB         1         3         25         75         8								
EB 1 12 6 72 8  EB 1 3 25 75 8  EB 1 3 3 50 150 17  EB 1 3 50 150 17								
EB 1 3 25 75 8  EB 1 3 50 150 17								
EB         1         3         25         75         8								
EB         1         3         25         75         8								
EB       1       3       25       75       8         EB       1       3       50       150       17         EB       1		+						
EB         1         3         25         75         8           EB         1         3         50         150         17								
EB         1         3         25         75         8           EB         1         3         50         150         17           EB         1         3         50         150         17           EB         1         3         50         150         17 <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td>								
EB         1         3         25         75         8           EB         1         3         50         150         17           EB         1         3         50         150         17           EB         1         3         50         150         17 <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td>								
EB         1         3         25         75         8           EB         1         3         50         150         17								
EB         1         3         25         75         8           EB         1         3         50         150         17								
EB         1         3         25         75         8           EB         1         3         50         150         17           EB         1         3         50         150         17 <td></td> <td></td> <td></td> <td>-</td> <td></td> <td></td> <td></td> <td></td>				-				
EB         1         3         25         75         8           EB         1         3         50         150         17		1						
EB       1       3       25       75       8         EB       1       3       50       150       17		1						
EB     1     3     25     75     8       EB     1     3     25     75     8       EB     1     3     25     75     8       EB     1     3     50     150     17		+						
EB     1     3     25     75     8       EB     1     3     25     75     8       EB     1     3     50     150     17		1						
EB     1     3     25     75     8       EB     1     3     50     150     17		1						
EB     1     3     50     150     17								
EB     1     3     50     150     17								
EB     1     3     50     150     17		1						
EB         1         3         50         150         17								
EB         1         3         50         150         17		1						
EB     1     3     50     150     17								
EB     1     3     50     150     17       EB     1     3     50     150     17       EB     1     3     50     150     17								
EB         1         3         50         150         17           EB         1         3         50         150         17		+						
EB 1 3 50 150 17								
EB   1   3   50   150   17								
			EB	1	3	50	150	17

F	ILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMIT	TTFNT RE	SURFA	CING SCH	HEDUJI E	F.A. RTF.	SECTION	COUNTY	TOTAL SHEET SHEETS NO.
٥	:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS				L 132	OII10 00II	ILDOLL	VAR.	2015-028RS	LAKE	39 8
		PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION				L IJZ					CONTRAC	T NO. 62A88
D	efault	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 2	OF 24	SHEETS	STA.	TO STA.		ILLINOIS FED.	AID PROJECT	

## ROUTE: IL 132 (IL 131 to Hunt Club Road) (Continued)

00000	OTDEET		=	I = =	[		
	STREET	DIRECTION	LANE		PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
	IL 131	EB	1	3	100	300	33
Hunt Club Road	IL 131	EB	2	12	3	36	4
Tidrit Glab Ttoad		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12		36	
		EB	2	12	3	36	4 4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
			2	12	3	36	4
		EB		12		36	
		EB EB	2	12	3	36	4 4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
	1	EB	2	12	3	36	4

(Continued)

CROSS STREET FROM TO	DIRECTION (EB/WB)	LANE		PAVEMENT	REPAIR	
	(ED/VVD)	NO.	PATCH	PATCH	AREA	AREA
	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	3	36	4
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	12	6	72	8
	EB	2	3	100	300	33
	EB	2	3	100	300	33
	EB	2	3	100	300	33
	EB	2	3	100	300	33
	EB	2	3	100	300	33
	EB	2	3	100	300	33
	EB	2	3	100	300	33
	EB	2	3	100	300	33
	EB	2	3	100	300	33
	EB	2	3	100	300	33
IL 131	EB	2	3	100	300	33

TOTALS: 9384 3962 FT SY

FILE NAME =	USER NAME = bartonrw	DESIGNED -	RWB	REVISED -	
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN -	RWB	REVISED -	
	PLOT SCALE = 100.0000 '/ in.	CHECKED -		REVISED -	
Default	PLOT DATE = 4/7/2015	DATE -	4/3/2015	REVISED -	

STATE	: OF	FILLINOIS
DEPARTMENT	OF	TRANSPORTATION

INTERMITTENT RESURFACING SCHEDULE							F.A. RTE.		
					IL 132			VAR.	
	SCALE:	SHEET 3	OF	24	SHEETS	STA.	TO STA.		

	THE INDIC EED AT	D DDO IECT		
		CONTRACT	NO. 6	2A88
VAR.	2015-028RS	LAKE	39	9
F.A. RTE.	SECTION	COUNTY	SHEETS	SHEET NO.

## ROUTE: IL 120 (IL 21 to West of Sears Boulevard)

CROSS S	TREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
IL 21		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
+		WB	1	12	3	36	4
+		WB	1	12	3	36	4
		WB		12		36	
			1		3		4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
+		WB	1	12	3	36	4
+		WB	1	12	3	36	4
		WB	1	12	3	36	4
<u>_</u>							
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
+		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB		12			
			1		3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
+		WB		12	3	36	4
			1				
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		, a					
		WB	1	12	3	36	4
		WB WB WB	1 1 1	12 12 12	3 3 3	36 36 36	4 4 4

## ROUTE: IL 120 (IL 21 to West of Sears Boulevard) (Continued)

CROSS S	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	3	25	75	8
		WB	1	3	25	75 75	8
		WB	1	3	25	75 75	8
		WB	1	3	25	75	8
		WB	1	3	25	75 75	8
		WB	1	3	25	75	8
		WB	1	3	25	75 75	8
		WB	1	3	25	75 75	8
		WB	1	3	50	150	17
		WB	1	3	50	150	17
							17
		WB	1	3	50	150	
		WB WB	1	3	50 100	150 300	17 33
			1				
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
	<del> </del>	WB	1	3	100	300	33
	<u> </u>	WB	1	3	100	300	33
	<del> </del>	WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
	<del> </del>	WB	1	3	100	300	33
	<del> </del>	WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
	<del> </del>	WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
	<del> </del>	WB	1	3	100	300	33
	<u> </u>	WB	1	3	100	300	33
	<u> </u>	WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB WB	1	3	100	300 300	33 33

F	FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMIT	TTENT RE	SURFA	CING SCHI	FNIJI F	F.A.	SECTION	COUNTY	TOTAL S	SHEET
٠	c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS					Jii 4 00111	EDOLL	VAR.	2015-028RS	LAKE	39	10
		PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION				L 120					CONTRAC	T NO. 6	2A88
10	Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 4	OF 24	SHEETS	STA.	TO STA.		ILLINOIS FED.	AID PROJECT		

CROSS S	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB		3	100	300	33
			1				
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
		WB	1	3	100	300	33
	Mill Road	WB	1	3	100	300	33
Mill Road		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB				36	
			1	12	3		4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	12	3	36	4
		WB	1	3	50	150	17
		WB	1	3	50	150	17
		WB	1	3	50	150	17
		WB	1	3	50	150	17
						150	17
		WB	1	3	50		
		WB WB	1	3	50 50	150	17
		WB WB	1	3	50 50	150 150	17 17
		WB WB WB	1	3 3 3	50 50 50	150 150 150	17 17 17
		WB WB WB	1 1 1	3 3 3 3	50 50 50 50	150 150 150 150	17 17 17 17
		WB WB WB WB	1 1 1 1	3 3 3 3 3	50 50 50 50 50	150 150 150 150 150	17 17 17 17 17
		WB WB WB WB WB	1 1 1 1 1	3 3 3 3 3 3	50 50 50 50 50 50	150 150 150 150 150 150	17 17 17 17 17 17
		WB WB WB WB WB WB	1 1 1 1 1 1	3 3 3 3 3 3 3	50 50 50 50 50 50 50	150 150 150 150 150 150 150 300	17 17 17 17 17 17 17 33
		WB WB WB WB WB WB WB	1 1 1 1 1 1 1	3 3 3 3 3 3 3 3	50 50 50 50 50 50 50 100	150 150 150 150 150 150 150 300 300	17 17 17 17 17 17 17 33 33
		WB WB WB WB WB WB WB WB	1 1 1 1 1 1 1 1	3 3 3 3 3 3 3 3 3	50 50 50 50 50 50 50 100 100	150 150 150 150 150 150 150 300 300 300	17 17 17 17 17 17 17 33 33 33
		WB WB WB WB WB WB WB WB WB	1 1 1 1 1 1 1 1 1	3 3 3 3 3 3 3 3 3 3	50 50 50 50 50 50 50 100 100 100	150 150 150 150 150 150 150 300 300 300 300	17 17 17 17 17 17 17 33 33 33 33
		WB	1 1 1 1 1 1 1 1 1 1	3 3 3 3 3 3 3 3 3 3 3 3	50 50 50 50 50 50 100 100 100 100	150 150 150 150 150 150 150 300 300 300 300 300	17 17 17 17 17 17 17 17 33 33 33 33 33
		WB	1 1 1 1 1 1 1 1 1 1 1	3 3 3 3 3 3 3 3 3 3 3 3 3	50 50 50 50 50 50 100 100 100 100 100	150 150 150 150 150 150 150 300 300 300 300 300 300	17 17 17 17 17 17 17 33 33 33 33 33 33
		WB	1 1 1 1 1 1 1 1 1 1 1 1 1	3 3 3 3 3 3 3 3 3 3 3 3 3	50 50 50 50 50 50 100 100 100 100 100	150 150 150 150 150 150 150 300 300 300 300 300 300 300	17 17 17 17 17 17 17 17 33 33 33 33 33 33 33
		WB W	1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	50 50 50 50 50 50 100 100 100 100 100 10	150 150 150 150 150 150 150 300 300 300 300 300 300 300 300	17 17 17 17 17 17 17 17 33 33 33 33 33 33 33 33
		WB W	1 1 1 1 1 1 1 1 1 1 1 1 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	50 50 50 50 50 50 100 100 100 100 100 10	150 150 150 150 150 150 150 300 300 300 300 300 300 300 300	17 17 17 17 17 17 17 33 33 33 33 33 33 33 33 67
		WB W	1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3	50 50 50 50 50 50 100 100 100 100 100 10	150 150 150 150 150 150 150 300 300 300 300 300 300 300 300	17 17 17 17 17 17 17 33 33 33 33 33 33 33 33

## ROUTE: IL 120 (IL 21 to West of Sears Boulevard) (Continued)

CROSS S	TREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
IL 21		WB	2	12	3	36	4
12 2 1		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
+		WB	2	12	3	36	4
+		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2				
				12	3	36	4
		WB	2	12 12	3	36	4
<del> </del>		WB				36	
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
1		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
+		WB	2	12	3	36	4
+		WB	2	12	3	36	4
					3	36	
		WB WB	2	12 12	3	36	4
				12		36	
		WB	2		3		4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4

FILE NAM	ME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMITTENT RESURFACING SCHEDULE	F.A.	SECTION	COUNTY	TOTAL SHEET SHEETS NO.
c:\pw_wor	rk\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS			VAR.	2015-028RS	LAKE	39 11
		PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	IL 120				CONTRAC	T NO. 62A88
Default		PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 5 OF 24 SHEETS STA. TO STA.		ILLINOIS FED.	AID PROJECT	

00000	OTDEET	DIDECTION	LANE	I DAY (EN JENIE	DAY CACALIT	DEDAID	L DEDAID
CROSS S		DIRECTION	LANE		PAVEMENT		REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	3	36	4
		WB	2	12	6	72	8
		WB	2	12	6	72	8
		WB	2	12	6	72	8
	Mill Road	WB	2	12	6	72	8
West of Sears Boulevard	IVIIII TOUG	EB	1	12	3	36	4
vvest of ceals bedievard		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB		12	3	36	
			1	12		36	4
		EB	1		3		
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	3	50	150	17
		EB	1	3	50	150	17
		EB	1	3	50	150	17
		EB	1	3	50	150	17
		EB	1	3	50	150	17
		EB	1	3	50	150	17
		EB	1	3	50	150	17
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
	l	L CD		ر ا	100	300	

ROUTE: IIL 120 (IL 21 1	o West of Sears Boulevard	(Continued)

(Continued)	

CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		EB	1	12	25	300	33
		EB	1	12	25	300	33
		EB	1	12	25	300	33
		EB	1	12	25	300	33
		EB	1	12	25	300	33
		EB	CL	3	100	300	33
		EB	CL	3	100	300	33
		EB	CL	3	100	300	33
		EB	CL	3	100	300	33
		EB	CL	3	100	300	33
		EB	CL	3	100	300	33
	Mill Road	EB	CL	3	100	300	33
Mill Road		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		LD	<u>'</u>	14	3	50	7

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMITTENT RESURFACING SCHEDULE	F.A.	SECTION	COUNTY	TOTAL S	SHEET NO.
c:\pw_work\pwidot\bartonrw\d0427922\HM	A-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS			VAR.	2015-028RS	LAKE	39	12
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION		IL 120			CONTRACT	T NO. 62	A88
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 6 OF 24 SHEETS STA. TO STA.		ILLINOIS FED. AI	D PROJECT		

CROSS S	TREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
				12	3		
		EB	1			36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	3	36	4
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	3	25	75	8
		EB	1	3	25	75	8
		EB	1	3	25	75	8
		EB	1	3	25	75	8
		EB	1		25	75 75	
		EB		3	25	75 75	8
			1	3			
		EB	1		25	75 75	8
		EB	1	3	25	75	8
		EB	1	3	50	150	17
		EB	1	3	50	150	17
		EB	1	3	50	150	17
		EB	1	3	50	150	17
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33

DOLUTE III 400 (II 04 ( )AV ( ) 60 D ( )	/O (' I)
ROUTE: IL 120 (IL 21 to West of Sears Boulevard)	(Continued)
NOUTE. HE 120 HE 21 to West of Seals Doulevald	(Continued)

CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
	<u> </u>	EB	1	3	100	300	33
	1	EB	1	3	100	300	33
		EB	1	3	100	300	33
	+	EB	1	3	100	300	33
	-	EB	1	3	100	300	33
		EB	1	3	100	300	33
				3	100	300	33
		EB EB	1	3	100	300	33
	-		1				
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
		EB	1	3	100	300	33
	IL 21	EB	1	3	100	300	33
Mill Road		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
<u> </u>		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
	•						

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMIT	TTENT RES	SURFA	CING SCH	4FNIJI F	F.A.	SECTION	COUNTY	TOTAL SH SHEETS 1	IEET NO.
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS					01110 0011	ILDOLL	VAR.	2015-028RS	LAKE	39	13
	PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			11	L 120					CONTRAC	T NO. 624	488
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 7	0F 24	SHEETS	STA.	TO STA.		ILLINOIS FED.	AID PROJECT		

CPOSS	STREET	DIRECTION	LANE	IDAN/EMENIT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
PROW	10	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
	+	EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB		12		36	
		EB	2	12	3	36	4
		EB		12	3	36	4
			2				
	-	EB	2	12	3	36	4
		EB EB	2	12 12	3	36 36	4
			2				4
		EB	2	12	3	36 36	4
		EB EB	2	12 12	3	36 36	4
			2				4
		EB	2	12 12	3	36	4
		EB	2		3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36 36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	3	36	4
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
	11.04	EB	2	12	6	72	8
	IL 21	EB	2	12	6	72	8

TOTALS:	13701	5868
	FT	SY

## ROUTE: US 41 (IL 120 to IL 21)

FROM  (NBS)	CROSS STREET		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
NBSS   (1,2,3)   WDTH   LENGTH   (SO FT)   (SO YC)				l				
IL 120	T TOW		<b>I</b>	l		1		
NB	II 120							
NB	12 120							
NB								
NB								
NB								
NB								
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NB								ļ
NB         1         12         3         36         4           NB         1         12         6         72         8								
NB								
NB								
NB								
NB								
NB								
NB     1     12     6     72     8								
NB								
NB     1     12     6     72     8								
NB     1     12     6     72     8				_				
NB         1         12         6         72         8								
NB         1         12         6         72         8								
NB         1         12         6         72         8								
NB         1         12         6         72         8								
NB     1     12     6     72     8		+						
NB     1     12     6     72     8								
NB     1     12     6     72     8								
NB         1         12         6         72         8		+						
NB     1     12     6     72     8								
NB     1     12     6     72     8								
NB         1         12         6         72         8								
NB     1     12     6     72     8								
NB     1     12     6     72     8								
NB         1         12         6         72         8           NB         1         12         6         72         8           NB         1         12         6         72         8		+						
NB 1 12 6 72 8 NB 1 12 6 72 8								
NB 1 12 6 72 8								
		1	ן ואט	'	12	J	12	

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMIT	TTENT RESURF	ACING SCHE	NIII F	F.A. RTF	SECTION	COUNTY	TOTAL SHEET
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS			IL 120 / U		DOLL	VAR.	2015-028RS	LAKE	39 14
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			IL 120 / U	3 41				CONTRAC	T NO. 62A88
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 8	OF 24 SHEE	TS STA.	TO STA.		ILLINOIS FED.	AID PROJECT	

ROUTE: US 4	1 (IL 120 to IL 21)	(Continued)
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CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
I I KOWI	"	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		NB	1	12	10	120	13
		NB	1	12	10	120	13
		NB	1	12	10	120	13
		NB	1	3	25	75	8
		NB	1	3	25	75	8
		NB	1	3	25	75	8
		NB	1	3	25	75	8
		NB	1	3	25	75	8
		NB	1	3	25	75	8
		NB	1	3	25	75	8
		NB	1	3	25	75	8
		NB	1	3	50	150	17
		NB	1	3	50	150	17
		NB	1	3	100	300	33
		NB	1	3	100	300	33
		NB	1	3	100	300	33
		NB	1	3	100	300	33
		NB	1	3	100	300	33
		NB	1	3	100	300	33
		NB	1	3	100	300	33
		NB	1	3	100	300	33
	IL 21	NB	1	3	100	300	33
IL 120		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36	4
		NB	2	12	3	36 36	4
		NB	2	12	3	36 36	4
		NB NB	2	12	3	36 36	4
		NB NB	2	12 12	3	36	4
		NB NB	2	12	3	36	4
		NB NB	2	12 12	6	72	8
		NB	2	12	6	72	8

ROUTE: US 41 (IL 120 to IL 21)	(Continued)

TO	DIRECTION (EB/WB) (NB/SB) NB	LANE NO. (1, 2, 3) 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	PATCH WIDTH  12 12 12 12 12 12 12 12 12 12 12 12 12	PAVEMENT PATCH LENGTH  6 6 6 6 6 6 6 6 6 6 6 6 6 6 10 10 10	REPAIR AREA (SQ FT) 72 72 72 72 72 72 72 72 72 72 72 72 72	REPAIR AREA (SQ YD)  8  8  8  8  8  8  8  8  8  8  8  8  8
	(NB/SB)  NB	(1, 2, 3) 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	WIDTH  12  12  12  12  12  12  12  12  12  1	ENGTH  6  6  6  6  6  6  6  6  6  6  6  6  6	(SQ FT)  72  72  72  72  72  72  72  72  72  7	(SQ YD)  8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 13
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	72 72 72 72 72 72 72 72 72 72 72 72 72 7	8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	72 72 72 72 72 72 72 72 72 72 72 72 72 7	8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	72 72 72 72 72 72 72 72 72 72 72 72 72 7	8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	6 6 6 6 6 6 6 6 6 6 6 6 6 6	72 72 72 72 72 72 72 72 72 72 72 72 72 7	8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	6 6 6 6 6 6 6 6 6 6 6 6 6	72 72 72 72 72 72 72 72 72 72 72 72 72 7	8 8 8 8 8 8 8 8 8 8 8 8 8 8 8
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	6 6 6 6 6 6 6 6 6 6 6 6	72 72 72 72 72 72 72 72 72 72 72 72 72 7	8 8 8 8 8 8 8 8 8 8 8 8 8 13
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	6 6 6 6 6 6 6 6 6 6 6 6	72 72 72 72 72 72 72 72 72 72 72 72 72 7	8 8 8 8 8 8 8 8 8 8 8 8 13
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	6 6 6 6 6 6 6 6 6 6 6	72 72 72 72 72 72 72 72 72 72 72 72 72 7	8 8 8 8 8 8 8 8 8 8 8 13
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	6 6 6 6 6 6 6 6 6 6 6	72 72 72 72 72 72 72 72 72 72 72 72 72 7	8 8 8 8 8 8 8 8 8 8 8 13
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12	6 6 6 6 6 6 6 6 6 6	72 72 72 72 72 72 72 72 72 72 72 72 72 120	8 8 8 8 8 8 8 8 8 8 13
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12	6 6 6 6 6 6 6 6 6	72 72 72 72 72 72 72 72 72 72 72 120	8 8 8 8 8 8 8 8 8 13
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12	6 6 6 6 6 6 6 6 10	72 72 72 72 72 72 72 72 72 72 120	8 8 8 8 8 8 8 8 13
	NB N	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12	6 6 6 6 6 6 6 10	72 72 72 72 72 72 72 72 72 120	8 8 8 8 8 8 8 13
	NB	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12	6 6 6 6 6 10	72 72 72 72 72 72 72 72 120	8 8 8 8 8 8 13
	NB	2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12	6 6 6 6 6 10	72 72 72 72 72 72 72 72 120	8 8 8 8 8 8 13
	NB	2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12	6 6 6 6 10	72 72 72 72 72 72 120	8 8 8 8 8 13
	NB	2 2 2 2 2 2 2 2 2	12 12 12 12 12 12	6 6 6 6 10	72 72 72 72 72 120	8 8 8 8 13
	NB NB NB NB NB NB NB NB	2 2 2 2 2 2 2 2	12 12 12 12 12	6 6 6 10	72 72 72 72 120	8 8 8 13 13
	NB NB NB NB NB NB	2 2 2 2 2 2	12 12 12 12	6 6 10 10	72 72 120 120	8 8 13 13
	NB NB NB NB	2 2 2 2 2	12 12 12	6 10 10	72 120 120	8 13 13
	NB NB NB NB	2 2 2 2	12 12	10 10	120 120	13 13
	NB NB NB	2 2 2	12	10	120	13
	NB NB	2				
	NB	2	12	10	120	1 12
	NB		3	25	75	8
		2	3	25	75	8
	NB	2	3	25	75	8
	NB	2	3	25	75	8
	NB	2	3	50	150	17
	NB			50		17
						33
						33
						33
						33
						33
II 04		2				33
IL Z I						
						4
						4
						4
						4
	SB	1	12	3	36	4
	SB	1	12	3	36	4
	SB	1	12	3	36	4
	SB	1	12	3	36	4
	SB	1				4
						4
						4
						4
						4
						4
						4
						4
						4
						4
						4
	SB	1			36	4
	SB	1	12	3	36	4
	SB	1	12	3	36	4
	IL 21	NB	NB	NB         2         3           NB         1         12           SB         1         12           SB         1         12           SB         1         12           SB         1         12           SB	NB         2         3         25           NB         2         3         50           NB         2         3         50           NB         2         3         100           SB         1         12         3           SB         1         12         3 <td>  NB</td>	NB

FILE NAME	[ e	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMIT	TTENT RE	SURFA	CING SCH	4FNIII F	F.A.	SECTION	COUNTY	TOTAL S SHEETS	SHEET NO.
c:\pw_work	<pre></pre> <pre><td>-Lake-East.dgn</td><td>DRAWN - RWB</td><td>REVISED -</td><td>STATE OF ILLINOIS US 44</td><td>VAR.</td><td>2015-028RS</td><td>LAKE</td><td>39</td><td>15</td></pre>	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS US 44	VAR.	2015-028RS	LAKE	39	15						
		PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	US 41							CONTRAC	CT NO. 62	2A88	
Default		PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 9	0F 24	SHEETS	STA.	TO STA.		ILLINOIS FED	. AID PROJECT		

ROUTE: US 41 (IL 120 to IL 21)	(Continued)
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CROSS ST	REET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	3	36	4
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	10	120	13
		SB	1	12	10	120	13
		SB	1	12	10	120	13
		SB	1	3	25	75	8
		SB	1	3	25	75	8
		SB	1	3	25	75	8
		SB	1	3	25	75	8
		SB	1	3	25	75	8
		SB	1	3	25	75	8
		SB	1	3	25	75	8
		SB	1	3	25	75	8
		SB	1	3	50	150	17
		SB	1	3	50	150	17
		SB	1	3	100	300	33
		SB	1	3	100	300	33
		SB	1	3	100	300	33
		SB	1	3	100	300	33
		SB	1	3	100	300	33
		SB	1	3	100	300	33
		SB	1	3	100	300	33
		SB	1	3	100	300	33
	IL 120	SB	1	3	100	300	33

ROUTE: US 41 (IL 120 to IL 21)	(Continued)

CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
11(0)(1		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
IL 21	+	SB	2	12	3	36	4
IL Z I	+	SB	2	12	3	36	4
	+	SB	2	12	3	36	4
		SB	2	12	3	36	
							4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
	†	SB	2	12	3	36	4
		SB	2	12	3	36	4
	+	SB	2	12	3	36	4
	+	SB	2	12	3	36	4
	+	SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	3	36	4
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
	+	SB	2	12	6	72	8
		SB	2	12	6	72	8
	+	SB					8
	+	SB	2	12 12	6 6	72 72	8
	+						
	1	SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
		SB	2	12	6	72	8
	†	SB	2	12	6	72	8
	+	SB	2	12	6	72	8
	+	SB	2	12	10	120	13
	+						
		SB	2	12	10	120	13
		SB	2	12	10	120	13

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMITTENT RESURFACING SCHEDULE					F.A.	SECTION	COUNTY	TOTAL SHI	ĒΤ
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS		US 41				<b>/LL</b>	VAR.	2015-028RS	LAKE	39 1	6
	PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	US 41						CONTRAC	T NO. 62A	88		
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 10	OF 24 S	HEETS S	TA.	TO STA.		ILLINOIS FED.	AID PROJECT		

## ROUTE: US 41 (IL 120 to IL 21) (Continued)

CROSS :	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		SB	2	3	25	75	8
		SB	2	3	25	75	8
		SB	2	3	25	75	8
		SB	2	3	25	75	8
		SB	2	3	50	150	17
		SB	2	3	50	150	17
		SB	2	3	100	300	33
		SB	2	3	100	300	33
		SB	2	3	100	300	33
		SB	2	3	100	300	33
		SB	2	3	100	300	33
	IL 120	SB	2	3	100	300	33

TOTALS: 5026 2701 FT SY

## ROUTE: US 41 (IL 22 to IL 60)

CROSS S	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
IL 60		SB	2	6	12	72	8
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	CL	3	20	60	7
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	CL	3	15	45	5
		SB	2	3	12	36	4
	_	SB	2	3	12	36	4
		SB	CL	3	15	45	5
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	2	3	12	36	4

## ROUTE: US 41 (IL 22 to IL 60) (Continued)

CROSS S	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		SB	CL	3	20	60	7
		SB	CL	3	30	90	10
		SB	2	12	12	144	16
		SB	1	3	12	36	4
		SB	2	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	2	3	40	120	13
		SB	1	3	12	36	4
		SB	2	3	30	90	10
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	2	3	30	90	10
		SB	CL	3	30	90	10
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	
		SB			12	72	8
		SB	2	6	12	36	8 4
		SB	2	3	12	36	4
		SB			12	72	8
			1	6	12	72	
		SB	2	6			8 17
		SB SB	CL	3	50	150 72	
			1	6	12		8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	2	6	30	180	20
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	1	3	12	36	4
		SB	2	12	12	144	16
		SB	1	6	12	72	8

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMIT	TENT RESI	IRFACIN	IG SCHEDU	II F	F.A.	SECTION	COUNTY	TOTAL SHE	ĒΤ
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS		IIS 41				'LL	VAR.	2015-028RS	LAKE	39 1	7
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	US 41						CONTRAC	T NO. 62A	38		
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 11	OF 24 S	HEETS ST	TA.	TO STA.		ILLINOIS FED.	AID PROJECT		$\neg$

ROUTE:	US 41	(IL 22 to IL 60)		(Continued)	
	100	10 10 00	,	Continuaca	

CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
TROW		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		SB	2	6	12	72	8
		SB	CL	3	20	60	7
		SB	2	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	3	30	90	10
		SB	1	3	12	36	4
		SB	2	3	12	36	
		SB	2	3	25	75	4 8
		SB			12	73	
		SB	2	6	12	72	8 8
				3	12		
		SB	2		12	36 36	4
		SB		3			4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
	Westleigh Road	SB	LT	6	12	72	8
Westleigh Road		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	CL	3	30	90	10
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	1	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8

ROUTE: US 41 (IL 22 to IL 60	) (Co	ntinued)

CROSS STRE	ET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
1		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB			50	150	17
		SB	CL	3	12	72	
		SB	2	6	12	72	8
				6			8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	1	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	1	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
<del> </del>		SB	1	6	12	72	8
<u> </u>		SB	1	6	12	72	8
+		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
<del>                                     </del>		SB	1	3	12	36	4
<del>                                     </del>		SB	2	3	12	36	4
		SB		3		36	
		SB	2	3	12 12	36	4
		SB		6	12	72	
			1				8
ļ		SB	1	6	12	72	8
<u> </u>		SB	1	6	12	72	8
1		SB	2	6	12 12	72	8
					i 12	72	8
		SB	2				
		SB SB SB	1	3	12	36 36	4

ILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMITTENT RESURFA	CING SCHEDULE	RTE.	SECTION	COUNTY	TOTAL SE
\pw_work\pwidot\bartonrw\d0427922\HMA-	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS		US 41			2015-028RS	LAKE	39
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION						CONTRACT	NO. 62
efault	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 12 OF 24 SHEETS	STA. TO STA.		ILLINOIS FED. A		

ROUTE: US 41 (IL 22 to IL 60)	(Continued)
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CDOCC	CTDEET	DIDECTION	LANG	DAY EMENT	DAY/EMENT	DEDAID	DEDAID
CROSS	_	DIRECTION	LANE NO.	PATCH	PAVEMENT PATCH	REPAIR AREA	REPAIR AREA
FROM	ТО	(EB/WB)		WIDTH		(SQ FT)	l
		(NB/SB)	(1, 2, 3)		LENGTH	,	(SQ YD)
		SB SB	2	3	12 12	36 36	4
		SB	1	6	12	72	8
		SB			12		
			2	6		72	8
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	50	150	17
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	1	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	CL	3	55	165	18
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	1	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	50	150	17
		SB	1	3	50	150	17
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	CL	3	50	150	17
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	2	3	12	36	4
		SB	CL	3	30	90	10
		SB	1	6	12	72	8
		SB	2	6	12	72	8
	Old Elm Road	SB	RT	3	75	225	25
	Old Ellif Modd		1.11		, 0		

	(0 1)
ROUTE: US 41 (IL 22 to IL 60)	(Continued)
110012. 00 11 (12 22 10 12 00)	(5511111454)

CROSS S	TREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
	. •	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Old Elm Road		SB	1	3	12	36	4
Old Ellii road		SB	2	3	12	36	4
		SB	CL	3	25	75	8
		SB	1	3	12	36	4
			2	3	12		4
		SB				36	
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
					12	72	
		SB	1	6			8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	30	180	20
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	2		12	36	4
				3			
ı		SB	CL	3	75	225	25

Γ	FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -		INTERMITTENT RESURFACING SCHEDULE			4FNIJI F	F.A.	SECTION	COUNTY	TOTAL S SHEETS	HEET		
	c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS				10 //	01110 0011	ILDOLL	VAR.	2015-028RS	LAKE	39	19
		PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION				JS 41				CONTRACT NO. 62A		288A	
	Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 13	0F 24	SHEETS	STA.	TO STA.		ILLINOIS FED.	AID PROJECT		

ROUTE:	US 41 (IL 22 to IL 60)	(Continued)
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CROSS S	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
I I COIVI	10	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2		12	72	8
		SB		6 3	12	36	
			1				4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	1	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	CL	3	75	225	25
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	30	90	10
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36 36	4
		SB	2	3	12	36	4

ROUTE:	US 41 (IL 22 to IL 60)	(Continued)

CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
1110111		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
	-		2	3	12		
		SB				36	4
		SB	CL	3	25	75	8
		SB	1	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	CL	3	75	225	25
		SB	2	6	12	72	8
		SB	1	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	2	3	70	210	23
		SB	1	3	12	36	
							4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	75	225	25
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	30	90	10
		SB	1	3	12	36	4
		SB	2	3	12	36	4
					12		
		SB	1	3		36	4
		SB	2	3	12	36	4
	i	SB	1	3	12	36	4
		SB	2	3	12	36	4

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -	CTATE OF HIMMOR	INTERMI	II F	F.A. RTF	SECTION	COUNTY	TOTAL	SHEE		
c:\pw_work\pwidot\bartonrw\dØ427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS	INTERMITTENT RESURFACING SCHEDULE					2015-028RS	LAKE	39	20
	PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	US 41						CONTRACT	T NO. F	2A88
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE: SHEET 14	OF 24 SHEETS	STA.	TO STA.		ILLINOIS FED. A			

ROUTE: US 41 (IL 22 to IL 60) (Continued)

CROSS STREE	ΕT	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		SB	2	3	12	36	4
		SB	2	6	12	72	8
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	25	75	8
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	6	12	72	8
		SB	2	6	12	72	8
		SB	CL	3	25	75	8
		SB	2	3	50	150	17
		SB	1	6	12	72	8
		SB	2	3	12	36	4
		SB	2	3	12	36	4
		SB	CL	3	50	150	17
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	2	3	50	150	17
		SB	2	3	25	75	8
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	2	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	12	36	4
		SB	1	3	25	75	8
		SB	2	3	50	150	17
		SB	1	3	12	36	4
		SB	2	3	12	36	4
		SB	1	3	12	36	4
	IL 22	SB	2	3	50	150	17
IL 22	IL ZZ	NB	1	6	12	72	8
IL ZZ		NB	2	6	12	72	8
		NB	1	6	12	72	8
		NB NB	2	6	12	72	8
		NB NB	1	6	12	72	8
		NB NB	2	6	12	72	8
		NB NB	1	3	12	36	4
		NB NB	2	3	12	36	4
		NB NB	1	3	12	36	4
		NB NB	2	3	12	36	4
		NB NB		3	12	36	4
		NB NB	2	3	12	36	4
		NB NB			25		
		NB NB	CL	3	12	75 26	8
			1	3	12	36 36	4
		NB NB	2			36	
		NB	1	3	12	36	4
		NB NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
	Old Elm Road	NB	2	3	12	36	4

ROUTE: US 41 (IL 22 to IL 60)	(0)	ontinued)
ROUTE. JUS 41 (IL 22 to IL 60)	(00	munuea)

CROSS ST	REET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Old Elm Road		NB	1	3	12	36	4
Old Ellii rodd		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
					12		
		NB	1	3		36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	6	12	72	8
		NB	2	6	12	72	8
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
			2	3	12		
		NB				36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	6	12	72	8
		NB	2	6	12	72	8
		NB	1	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB		3	12	36	

FILE NAM	ME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMITTENT RESURFACING SCHEDULE	F.A.	SECTION	COUNTY	TOTAL	SHEET NO.
c:\pw_wo	rk\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS			VAR.	2015-028RS	LAKE	39	21
		PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	US 41				CONTRAC	T NO. 6	2A88
Default		PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 15 OF 24 SHEETS STA. TO STA.		ILLINOIS FED. AI	D PROJECT		

ROUTE:	US 41 (IL 22 to IL 60)	(Continued)
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CROSS	STREET	DIRECTION	LANE	PAVEMENT.	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
FROM		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	6	12	72	8
		NB	2	6	12	72	8
		NB					
			1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	1	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB NB	2	3	12	36	4
		NB NB	1	3	12	36	4
		NB NB	2	3	12	36	
							4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4

ROUTE: US 41 (IL 22 to IL 60	) (Co	ntinued)

CROSS	STREET	DIRECTION		PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		NB	1	3	12	36	4
	+	NB	2	3	12	36	4
	+	NB	1	3	12	36	4
	+	NB	2	3	12	36	4
	+	NB	CL	3	25	75	8
	+				12		
		NB	1	6		72	8
		NB	2	6	12	72	8
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
	1	NB	1	3	12	36	4
	+	NB	2	3	12	36	4
	+	NB	1	3	12	36	4
	+	NB	1	3	12	36	4
	+	NB	2	3	12	36	
	_						4
		NB	1	3	12	36	4
		NB	1	3	12	36	4
		NB	1	6	12	72	8
		NB	2	6	12	72	8
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	6	12	72	8
		NB	2	6	12	72	8
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
	+	NB	1	3	12	36	4
	+	NB	2	3	12	36	4
	_					36	
		NB	1	3	12		4
		NB	2		12	36	4
	<del> </del>	NB	1	6	12	72	8
	<del> </del>	NB	2	6	12	72	8
	<u> </u>	NB	2	3	12	36	4
		NB	1	3	12	36	4
	1	NB	2	3	12	36	4
		NB	RT	3	75	225	25
	Westleigh Road	NB	LT	3	12	36	4
Westleigh Road		NB	1	6	12	72	8
		NB	2	6	12	72	8
		NB	1	6	12	72	8
		NB	2	6	12	72	8
		NB	1	3	12	36	4
	<del> </del>	NB	2	3	12	36	4
	+	NB	1	3	12	36	4
	+	NB	2	3	12	36	
	+						4
		NB NB	2	3	12 12	36 36	4

FILE N	IAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMITTENT RESURFACING SCHEDULE	F.A.	SECTION	COUNTY	TOTAL	SHEET
c:\pw_w	work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS			VAR.	2015-028RS	LAKE	39	22
		PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	US 41				CONTRAC	T NO. F	52A88
Default	t	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 16 OF 24 SHEETS STA. TO STA.		ILLINOIS FED. AI	D PROJECT		

## ROUTE: US 41 (IL 22 to IL 60) (Continued)

CROSS S	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	то	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	25	75	8
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	2	3	100	300	33
		NB	1	6	12	72	8
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
		NB	2	3	12	36	4
		NB	1	3	12	36	4
	IL 60	NB	2	3	175	525	58

TOTALS: 9598 3991 FT SY

## ROUTE: IL 60 (Riverwoods Road to IL 21)

CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Riverwoods Road		WB	1,2	3	100	300	33
		WB	1,2	3	100	300	33
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	1	6	12	72	8
		WB	2	6	12	72	8
		WB	1	6	12	72	8
		WB	2	6	12	72	8
	St. Mary Road	WB	CL	3	50	150	17
St. Mary Road		WB	CL	3	100	300	33
	IL 21	WB	RT	12	12	144	16
IL 21		EB	CL	3	250	750	83
		EB	1	3	350	1050	117
		EB	1	6	12	72	8
		EB	2	6	12	72	8
		EB	1	12	30	360	40
		EB	2	12	30	360	40
		EB	CL	3	45	135	15

## ROUTE: IL 60 (Riverwoods Road to IL 21) (Continued)

CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		EB	1	6	12	72	8
		EB	2	6	12	72	8
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	CL	3	200	600	67
		EB	CL	6	200	1200	133
		EB	LT	3	200	600	67
		EB	CL	3	50	150	17
	St. Mary Road	EB	CL	3	50	150	17
St. Mary Road		EB	CL	3	100	300	33
		EB	2,3	3	250	750	83
		EB	2	6	12	72	8
		EB	RT	3	200	600	67
		EB	2	9	12	108	12
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1	6	12	72	8
		EB	2	6	12	72	8
	Riverwoods Road	EB	1	3	12	36	4

TOTALS: 2545 1039 FT SY

## ROUTE: IL 120 (Greenleaf Ave., All Ramps)

CROSS S	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMITTENT RESURFACING SCHEDULE	F.A.	SECTION	COUNTY	TOTAL	S SHEE	Ŧ
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS		US 41 / IL 60 / IL 120	VAR.	2015-028RS	LAKE	39	23	_
	PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION		US 41 / IL 00 / IL 120			CONTRAC	T NO. F	62A8	<u>,</u>
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 17 OF 24 SHEETS STA. TO STA.		ILLINOIS FED. AI				Π

ROUTE:	IL 120 (Greenleaf Ave.,	All Ramps)	(Continued)
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CROSS S	STREET	DIRECTION	LANE	DAVEMENT.	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
FROM	10	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
			1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	ļ		3		5
		Ramp	1	16	3	48	
		Ramp	1	16		48	5
		Ramp	1	16 16	3	48 48	5
		Ramp	1				5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16 16	3	48	5 5
		Ramp	1	16	3	48 48	5
		Ramp			3		5
		Ramp Ramp	1	16	3	48	5
			1	16 16	3	48	5
		Ramp	1	16 16	3	48 48	5
		Ramp Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
			1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
			1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp					
		Ramp	1	16	3	48	5
		Ramp	1	16	ع ا	48	5

ROUTE: IL 120	(Greenleaf Ave., All Ramps)	Continued)

CROSS S	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
			1	16	3	48	5
		Ramp			3		5
		Ramp	1	16	3	48	5
		Ramp	1	16		48	
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
	•						<u> </u>

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMITTENT RESURFACING SCHEDULE	F.A. RTF	SECTION	COUNTY	TOTAL	SHEET NO.
c:\pw_work\pwidot\bartonrw\d0427922\HM	A-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS			VAR.	2015-028RS	LAKE	39	24
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION		IL 120			CONTRACT	T NO. 6	2A88
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 18 OF 24 SHEETS STA. TO STA.		ILLINOIS FED. AI	D PROJECT		

ROUTE:	IL 120 (Greenleaf Ave.	All Ramps)	Continued)
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CROSS S	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
			1	16	3	48	5
		Ramp					
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
					3		
		Ramp	1	16		48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
					3		5
		Ramp	1	16 16	3	48 48	5
		Ramp	1	16	3		5
		Ramp	1			48	
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
			1	16	3	48	5
		Ramp					
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5

ROUTE: IL 1	20 (Greenleaf Ave.	All Ramps)	Continued)

CROSS STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5
	Ramp	1	16	3	48	5

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMITTENT RESURFACING SCHEDULE	F.A.	SECTION	COUNTY	TOTAL	SHEET
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS	1		VAR.	2015-028RS	LAKE	39	25
	PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION		IL 120			CONTRACT	T NO. 6	2A88
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 19 OF 24 SHEETS STA. TO STA.		ILLINOIS FED. /	AID PROJECT		

ROUTE:	IL 120 (Greenleaf Ave., All Ramps)	Continued)
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CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
			1	16	3	48	5
		Ramp		16	3	48	5
		Ramp	1				
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
					3		
		Ramp	1	16		48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
				16	3	48	5
		Ramp	1				
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
				16		48 48	
		Ramp	1		3		5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
L	1						

ROUTE: IL 120 (C	Prophloaf Ava	All Pampe)	Continued	١
ROUTE. JIL 120 (C	oleelileal Ave.,	All Kallips)	Continued	"

CROSS	S STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
	_	Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
	+	Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
	+	Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		Ramp	1	16	6	96	11
		<del></del>	1	16	6	96	11
	+	Ramp Ramp	1	16	6	96	11
	-	Ramp	1	16	6	96	11
				16	6	96	11
		Ramp	1			96	
		Ramp	1	16 16	6 6	96	11 11
		Ramp	1	16	10	160	18
		Ramp	1	16	10	160	18
		Ramp Ramp	1	16	10	160	18
		Ramp		16	10	160	18
	+	Ramp	1	16	10	160	18
			1	16	10	160	18
	+	Ramp	1	16	10	160	18
	+	Ramp					
	+	Ramp	1	16 16	10 10	160	18 18
		Ramp	1	16	10	160	18
		Ramp	1	16	10	160 160	18
	+	Ramp	1				
		Ramp	1	16	10	160	18
		Ramp	1	16	10	160	18
		Ramp	1	16	10	160	18
		Ramp	1	16	10	160	18
		Ramp	1	16	10	160	18
		Ramp Ramp	1	16 16	10 10	160 160	18 18
			1				

ELE MANE	LICED NAME - 1	DESIGNED DWB	DEVICED				ΕΛ			TOTAL	SHE
FILE NAME =	USER NAME = bartonrw	DESIGNED - KWB	REVISED -		INTERMITTENT RESURFACING SCHEDULE IL 120		RTÉ.	RTE. SECTION	COUNTY	SHEETS	NO
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS			VAR.	2015-028RS	LAKE	39	26
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			******	2010 020.10	CONTRAC	T NO. 62	2A8
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 20 OF 24 SHEETS STA. TO STA.		ILLINOIS FED. A			

### ROUTE: IL 120 (Greenleaf Ave., All Ramps) Continued)

CROSS	STREET	DIRECTION	LANE	PΔ\/EMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
PROM	10	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		Ramp	1	16	10	160	18
		Ramp	1	16	10	160	18
		Ramp	1	16	10	160	18
			1	16	10	160	18
		Ramp	1	16	10	160	18
		Ramp					
		Ramp	1	3	25 25	75 75	8
		Ramp	1	3	25	75 75	8
		Ramp	1	3	25	75 75	8
		Ramp Ramp	1	3	25	75 75	8
			1	3	25	75	8
		Ramp		3	25	75	8
		Ramp	1	3	25	75	8
		Ramp					
		Ramp	1	3	25 25	75 75	8 8
		Ramp	1	3	25 25	75 75	
		Ramp	1			75 75	8
		Ramp	1	3	25 25	75 75	8
		Ramp	1				
		Ramp	1	3	25 25	75 75	8
		Ramp	1	3	25 25	75 75	
		Ramp	1	3	25	75 75	8
		Ramp	1	3			8
		Ramp	1		25	75 75	8
		Ramp	1	3	25	75 75	8
		Ramp	1	3	25		8
		Ramp	1	3	50 50	150 150	17 17
		Ramp	1	3	50	150	17
		Ramp Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	50	150	17
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
L	l	Namp			100	500	

ROUTE: IL 120 (Greenleaf Ave., All Ramps)	Continued)

CROSS S	TREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
TROW	10	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
		Ramp	1	3	100	300	33
					.,,,		
		1	I .				1

TOTALS: 8485 4829 FT SY

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -

STATE OF ILLINOIS	
<b>DEPARTMENT OF TRANSPORTATION</b>	

INTERMITTENT RESURFACING SCHEDULE						F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
				IL 120			VAR. 2015-028RS LAKE 39 27				
	IL 120								CONTRACT	NO. 6	2A88
SCALE:	SHEET 21	OF	24	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

## ROUTE: IL 120 (Entrance Ramp from IL 21 to WB IL 120)

CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
	1	Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	1	16	3	48	5
		Ramp	2	16	3	48	5
		Ramp	3	16	3	48	5
		Ramp	4	16	3	48	5
		Ramp	5	16	3	48	5
		Ramp	6	16	3	48	5
		Ramp	7	16	3	48	5
		Ramp	8	16	3	48	5
		Ramp	9	16	3	48	5
		Ramp	10	16	3	48	5
		Ramp	11	16	3	48	5
		Ramp	12	16	3	48	5
		Ramp	13	16	3	48	5
		Ramp	14	16	6	96	11
		Ramp	15	16	6	96	11
		Ramp	16	16	6	96	11
		Ramp	17	16	6	96	11
		Ramp	18	16	6	96	11
		Ramp	19	16	6	96	11
		Ramp	20	16	6	96	11
		Ramp	21	16	6	96	11
		Ramp	22	16	6	96	11
		Ramp	23	16	6	96	11
		Ramp	24	16	6	96	11
		Ramp	25	16	6	96	11
		Ramp	26	16	6	96	11
		Ramp	27	16	6	96	11
		Ramp	28	16	6	96	11
		Ramp	29	16	6	96	11
		Ramp	30	3	50	150	17
		Ramp	31	3	50	150	17
		Ramp	32	3	50	150	17
		Ramp	33	3	50	150	17
		<u> </u>					

TOTALS: 383 392 FT SY

### ROUTE: IL 137 (US 41 to Sheridan Road)

CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
US 41		EB	1	6	12	72	8
		EB	2	6	12	72	8
		EB	1	6	12	72	8
		EB	2	6	12	72	8
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	2	3	50	150	17
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1	6	12	72	8
		EB	2	6	12	72	8
		EB	1	3	12	36	4
		EB	1	3	12	36	4
		EB	1	6	12	72	8
		EB	2	6	12	72	8
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1	6	12	72	8
		EB	2	6	12	72	8
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1	3	12	36	4
	Great Lakes Drive	EB	2	3	12	36	4
Great Lakes Drive		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	LT	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	LT	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	3	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	3	3	12	36	4
M 11 5 1	Meridian Drive	EB	1,2	3	12	36	4
Meridian Drive		EB	1	3	12	36	4
		EB	2	6	12	72	8
		EB	3	6	12	72	8
		EB	1	6	12	72	8
		EB	2	6	12	72	8
		EB	1,2	6	12	72	8
		EB	1	6	12	72	8
		EB EB	1	6	12	72	8
	1	. ⊢B I	1	6	12	72	8
	+	EB	2	6	12	72	8

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMITTENT RESURFACING SCHEDULE	F.A. RTF	SECTION	COUNTY	TOTAL SHEET SHEETS NO.
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS	IL 120 / IL 137			2015-028RS	LAKE	39 28
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION					CONTRAC	T NO. 62A88
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 22 OF 24 SHEETS STA. TO STA.		ILLINOIS FED.	AID PROJECT	

ROUTE: IL 137 (US 41 to Sheridan Road) (Continue
--

CROSS S	STREET	DIRECTION	LANE	TPAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
FROIVI	10	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		EB	3	6	12	72	8
	IL 131	EB	LT	6	12	72	8
IL 131	IL IST	EB	1	3	12	36	4
IL 131		EB	2	3	12	36	4
		EB	3	3	12	36	4
		EB	1	3	12	36	4
		EB	_	3	12	36	
			1			36	4
		EB	2	3	12		4
		EB	1,2	3	25	75 20	8
		EB EB	2	3	12 12	36 36	4
		EB	3	3	12	36	4
		EB	1	3	12	36 36	4
		EB EB	2	3	12 12	36	4
							4
		EB	3	3	12	36	4
		EB	1		12 12	36	4
		EB	2	3		36	4
		EB	3	3	12	36 36	4
		EB	1	3	12	36	4
		EB	2	3	12	36 36	4
		EB	3		12		4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	3	3	12	36	4
	Lewis Avenue	EB	LT	3	12	36	4
Lewis Avenue		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	3	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	3	3	12	36	4
		EB	1	3	12	36	4
		EB	1	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
	<u> </u>	EB	3	3	12	36	4
D 0:	Ray Street	EB	2	3	12	36	4
Ray Street		EB	1	3	12	36	4
		EB	1	3	12	36	4
		EB	1	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1,2	3	300	900	100
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1	3	12	36	4
		EB	2	3	12	36	4
		EB	1	3	12	36	4
	01 11 5 7	EB	2	3	12	36	4
01 11 5 1	Sheridan Road	EB	1,2	3	150	450	50
Sheridan Road		WB	1	18	18	324	36
		WB	2	18	18	324	36
		WB	1	18	18	324	36
		WB	2	18	18	324	36
		WB	1	18	18	324	36

ROUTE: IL 137 (US 41 to Sheridan Road)	(Continued)
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CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		WB	2	18	18	324	36
		WB	1	18	18	324	36
		WB	2	18	18	324	36
		WB	1	18	18	324	36
		WB	2	18	18	324	36
		WB	CL	3	100	300	33
	Dahlar E Thamas an Error						
	Bobby E Thompson Expy	WB	CL	3	100	300	33
Bobby E Thompson Expy.		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	6	12	72	8
		WB	2	6	12	72	8
		WB	3	6	12	72	8
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	CL	3	30	90	10
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
			3	3	12		4
	Lauria Arranta	WB				36	
1 ' 4	Lewis Avenue	WB	LT	3	12	36	4
Lewis Avenue		WB	1	3	50	150	17
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1,2	3	150	450	50
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
				3			
		WB WB	3		12 12	36 36	4
	i	· WH	1		. 1.,	36	. /
		WB	2	3	12	36	4

FILE NAME =	USER NAME = bartonrw	DESIGNED - RWB	REVISED -			INTERMITTENT RESURFACING SCHEDULE	F.A.	SECTION	COUNTY	TOTAL	SHEET NO.
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN - RWB	REVISED -	STATE OF ILLINOIS		II 427	VAR.	2015-028RS	LAKE	39	29
	PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION		IL 13/			CONTRACT	T NO. 6	2A88
Default	PLOT DATE = 4/7/2015	DATE - 4/3/2015	REVISED -		SCALE:	SHEET 23 OF 24 SHEETS STA. TO STA.		ILLINOIS FED. AI	D PROJECT		

### ROUTE: IL 137 (US 41 to Sheridan Road) (Continued)

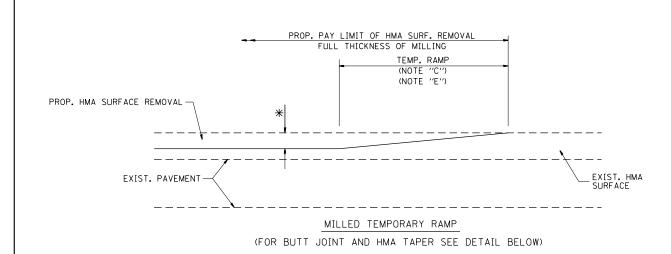
CROSS	STREET	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		WB	3	3	12	36	4
		WB	1,2	3	100	300	33
		WB	1,2	3	200	600	67
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
	IL 131	WB	1,2	3	200	600	67
IL 131		WB	1,2	3	50	150	17
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	3	3	12	36	4
		WB	LT	3	12	36	4
	Meridian Drive	WB	1,2	3	200	600	67
Meridian Drive		WB	1	3	10	30	3
		WB	2	3	20	60	7
		WB	3	3	9	27	3
		WB	1	3	20	60	7
		WB	2	3	3	9	1
		WB	3	3	3	9	1
		WB	1	3	3	9	1
		WB	2	3	6	18	2
		WB	3	3	3	9	1
		WB	LT	3	12	36	4
		WB	2,3	3	150	450	50
		WB	1	3	12	36	4
		WB	2	3	12	36	4
		WB	1,2	3	150	450	50
	Missississis Oto 1	WB	1	3	12	36	4
M: : : : 01 :	Mississippi Street	WB	2	3	12	36	4
Mississippi Street		WB	1	3	12	36	4
		WB	2	3	12	36	4
	110.44	WB	1	6	12	72	8
	US 41	WB	2	6	12	72	8

TOTALS: 4398 1870 FT SY

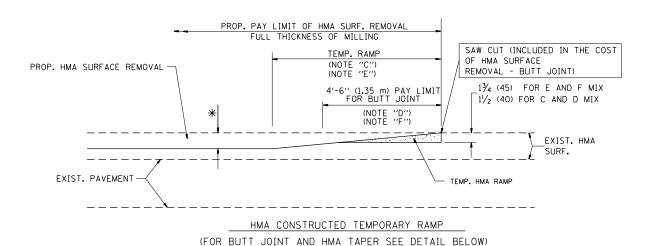
Default	PLOT DATE = 4/7/2015	DATE -	4/3/2015	REVISED -
	PLOT SCALE = 100.0000 '/ in.	CHECKED -		REVISED -
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East.dgn	DRAWN -	RWB	REVISED -
FILE NAME =	USER NAME = bartonrw	DESIGNED -	RWB	REVISED -

STATE OF ILLINOIS
<b>DEPARTMENT OF TRANSPORTATION</b>

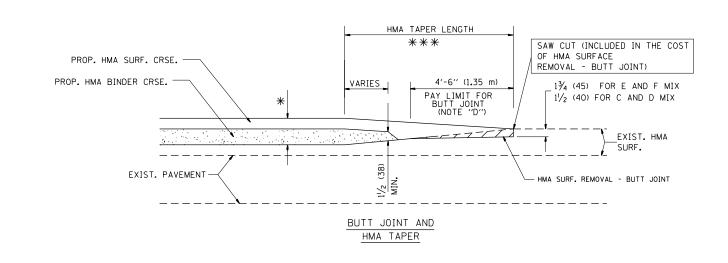
INTERMITTENT RESURFAC	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS				
IL 137	VAR.	2015-028RS	LAKE	39	30			
IL 137	IL 13/							
SCALE: SHEET 24 OF 24 SHEETS	STA. TO STA.		ILLINOIS FED. A	D PROJECT				



#### OPTION 1

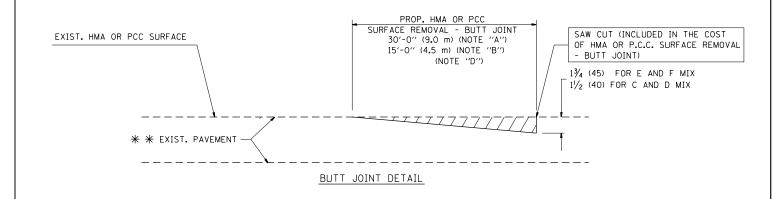


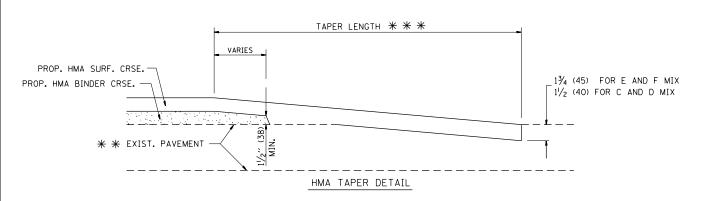
## OPTION 2 TYPICAL TEMPORARY RAMP



# TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION





## TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

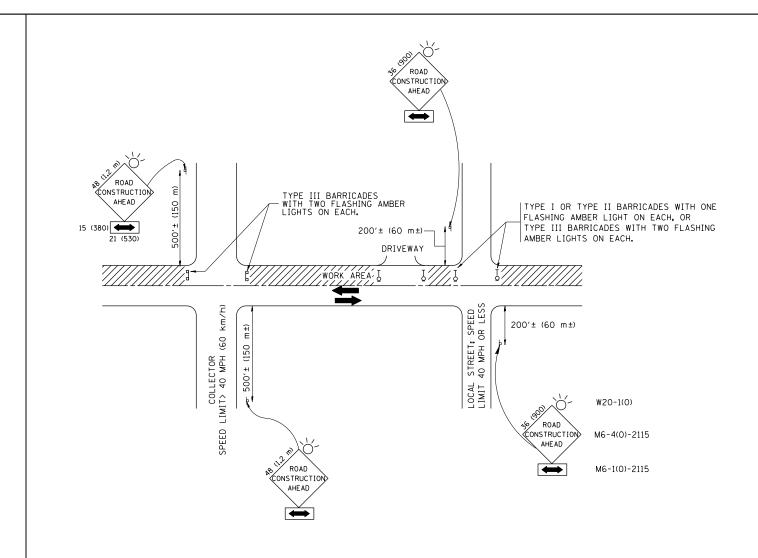
#### NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

### BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



#### TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

#### NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- 0) ONE ROAD CONSTRUCTION AHEAD SIGN  $36 \times 36 \ (900 \times 900)$  WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- d) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROLLTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (MG-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (MG-4).

SCALE: NONE

#### B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

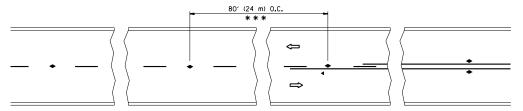
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

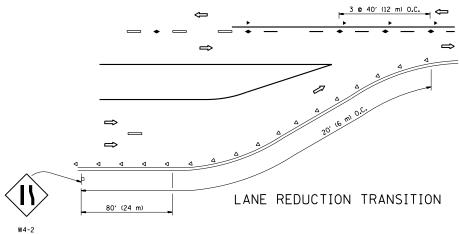
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

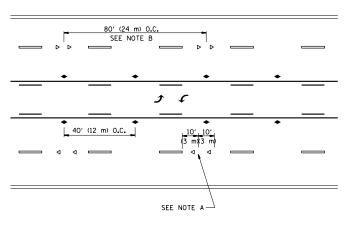
SHEET NO. 1 OF 1 SHEETS STA. TO



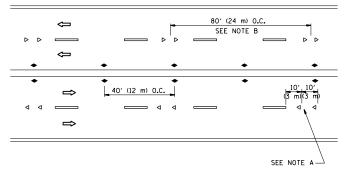
\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

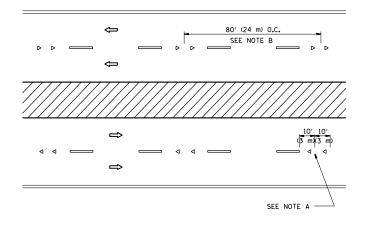




TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

#### GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- 2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- 3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

#### LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

#### SYMBOLS

---- YELLOW STRIPE

---- WHITE STRIPE

- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (₩/O)
- ◆ TWO-WAY AMBER MARKER

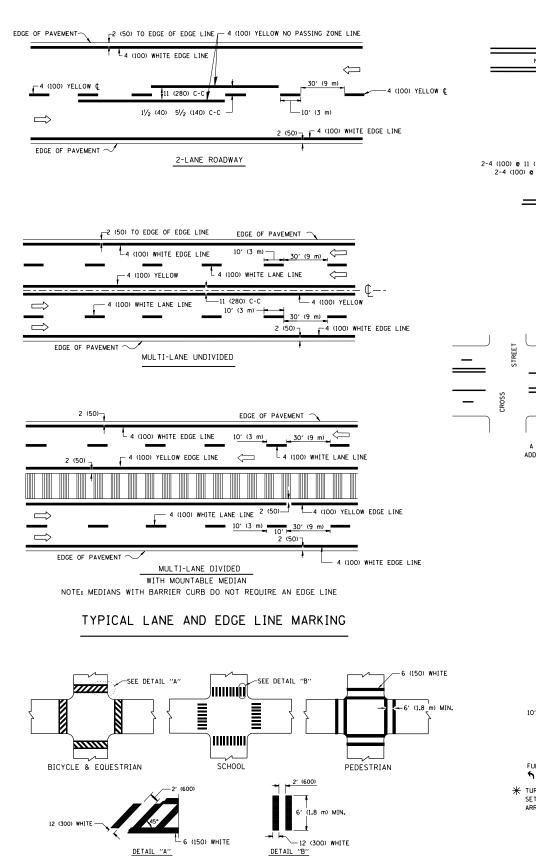
#### DESIGN NOTES

- 1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- 2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- 3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- 4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = bartonrw	DESIGNED -	REVISED -T. RAMMACHER 09-19-94		TYPICAL APPLICATIONS	F.A RTE.	SECTION	COUNTY TOTAL SHEET NO.
c:\pw_work\pwidot\bartonrw\d0427922\HM	-Lake-East-DistStd.dgn		REVISED -T. RAMMACHER 03-12-99	STATE OF ILLINOIS	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		2015-028RS	LAKE 39 33
	PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -T. RAMMACHER 01-06-00	DEPARTMENT OF TRANSPORTATION	KAISED REFLECTIVE PAVEINENT INARKERS (SNOW-PLOW RESISTANT)		TC-11	CONTRACT NO. 62A88
	PLOT DATE = 4/7/2015	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD	DIST. NO. 1 ILLINOIS FE	D. AID PROJECT



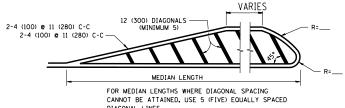
2-4 (100) YELLOW • 11 (280) C-C

NO DIAGONALS

4' (1.2 m) OUTSIDE TO OUTSIDE OF LINES

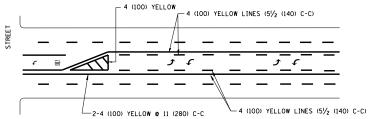
2-4 (100) YELLOW • 11 (280) C-C

#### 4' (1.2 m) WIDE MEDIANS ONLY

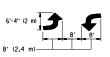


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) T0 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

#### MEDIANS OVER 4' (1.2 m) WIDE

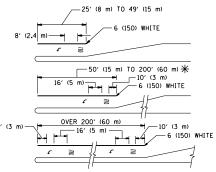


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

#### TYPICAL PAINTED MEDIAN MARKING

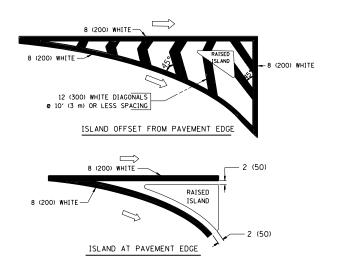


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  $\P$  AREA = 15.6 SO. FT. (1.5 m² )  $\P$  AREA = 20.8 SO. FT. (1.9 m²)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

#### TYPICAL TURN LANE MARKING



#### TYPICAL ISLAND MARKING

TURE OF MIRWING				DELENIE A DELUBYS
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 1280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW: EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART 5EE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1,2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SO. FT. (0.33 m²) EACH "X"=54.0 SO. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) <b>©</b> 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (0VER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

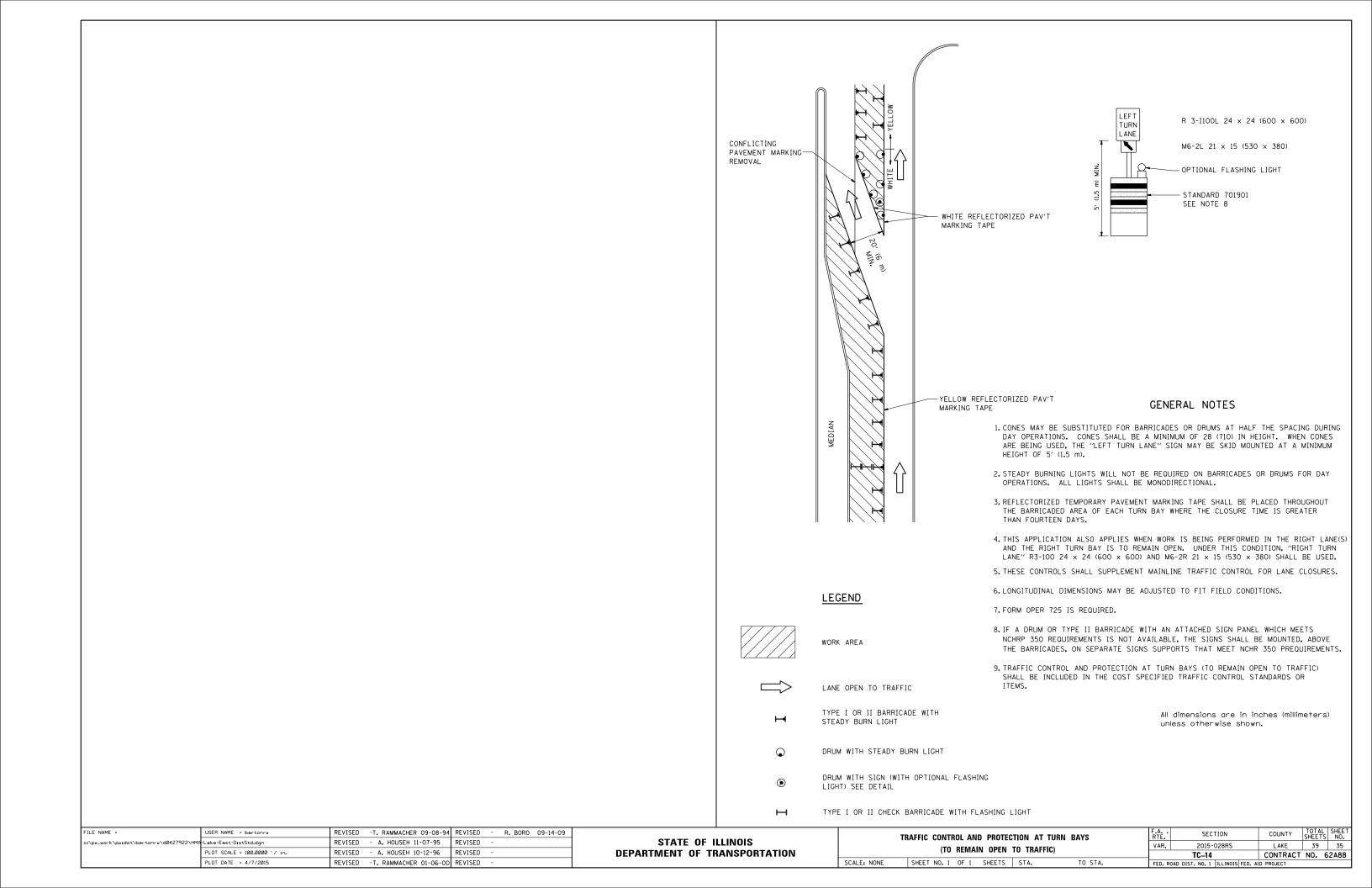
All dimensions are in inches (millimeters) unless otherwise shown.

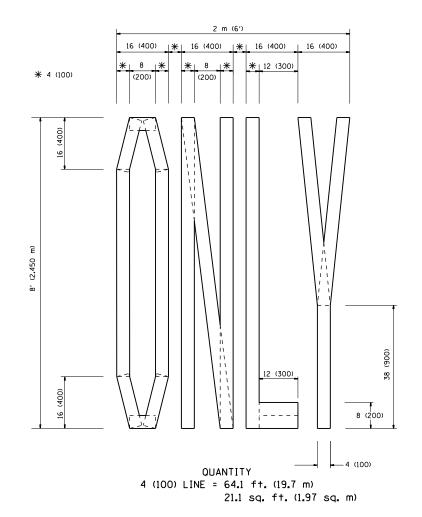
FILE NAME =	USER NAME = bartonrw	DESIGNED -	EVERS	REVISED	-T. RAMMACHER	10-27-94
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East-DistStd.dgn	DRAWN -		REVISED -C. JUCIUS		09-09-09
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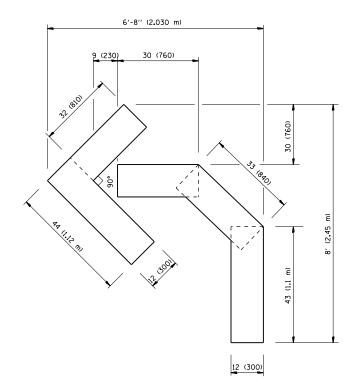
TYPICAL CROSSWALK MARKING

STATE OF ILLINOIS
<b>DEPARTMENT OF TRANSPORTATION</b>

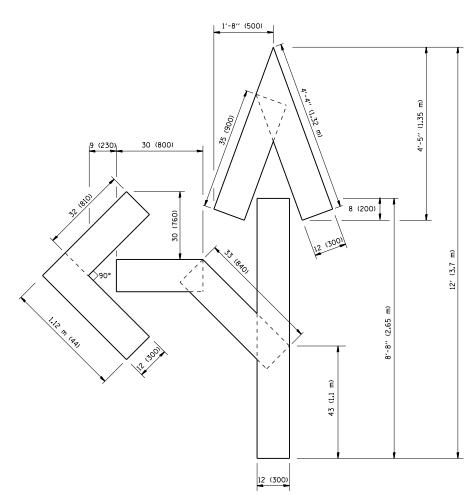
	DISTI	RICT ON	E		F.A RTE.	SECTION	COUNTY TOTAL SHE		
	TYPICAL PAVE	VAR.	2015-028RS	LAKE	39	34			
	TIFICAL FAVE		TC-13 CONTRACT NO.						
SCALE: NONE	SHEET NO. 1 OF 1 SI	HEETS	STA.	TO STA.	FED. RO	DAD DIST. NO. 1   ILLINOIS FED. AI	D PROJECT		







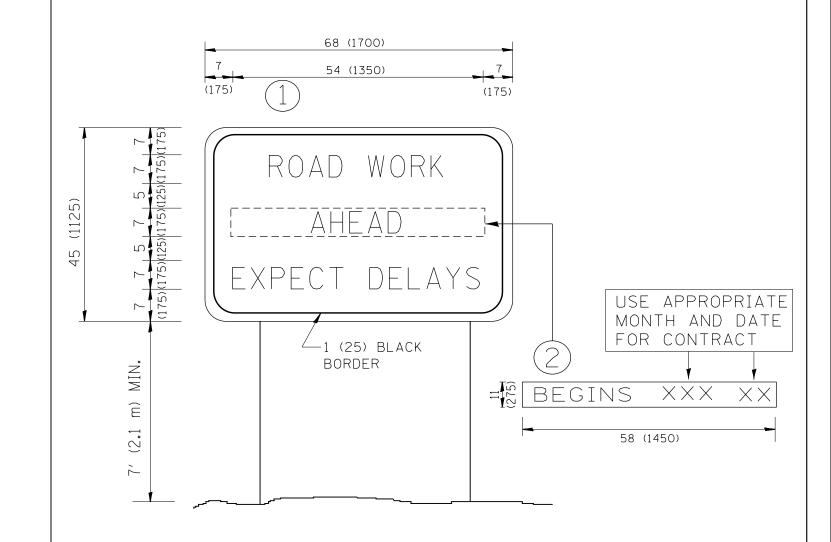
OUANTITY 4 (100) LINE = 45.5 ft. (13.9 m) 15.2 sq. ft. (1.39 sq. m)



QUANTITY 4 (100) LINE = 82.5 ft. (25.3 m) 27.5 sq. ft. (2.53 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = bartonrw		REVISED -T. RAMMACHER 06-05-96		PAVEMENT MARKING LETTERS AND SYMBOLS	F.A RTE.	SECTION	COUNTY TOTAL SHEET SHEETS NO.
c:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East-DistStd.dgn	DRAWN -	REVISED -T. RAMMACHER 11-04-97	STATE OF ILLINOIS		VAR.	2015-028RS	LAKE 39 36
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -T. RAMMACHER 03-02-98	DEPARTMENT OF TRANSPORTATION	FOR TRAFFIC STAGING		TC-16	CONTRACT NO. 62A88
	PLOT DATE = 4/7/2015	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. RO	DAD DIST. NO. 1   ILLINOIS   FED. A	



## NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

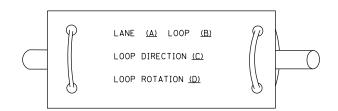
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

F	ILE NAME =	USER NAME = bartonrw	DESIGNED -	REVISED - R. MIRS 09-15-97	·		ARTERIAL ROAD		F.A	SECTION	COUNTY	TOTAL !	SHEET NO.
٠	:\pw_work\pwidot\bartonrw\d0427922\HMA	-Lake-East-DistStd.dgn	DRAWN -	REVISED - R. MIRS 12-11-97	STATE OF ILLINOIS	INFORMATION SIGN		VAR.	2015-028RS	LAKE	39	37	
		PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -T. RAMMACHER 02-02-99			INFURIMATION SIGN			TC-22	CONTRACT	NO. 61	2A88
		PLOT DATE = 4/7/2015	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA.	TO STA.	FED. ROAD	DIST. NO. 1 ILLINOIS FED. AI	PROJECT		

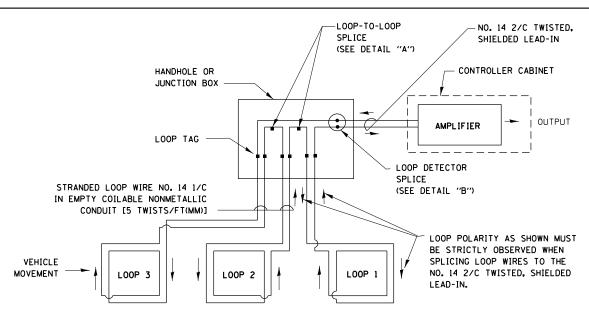
#### **LOOP DETECTOR NOTES**

- 1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- 2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- 3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- 4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- 5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- 6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- 7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

#### LOOP LEAD-IN CABLE TAG

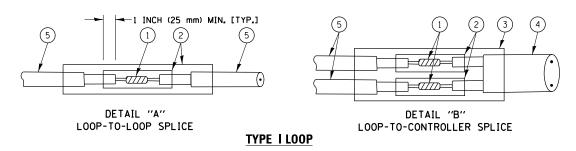


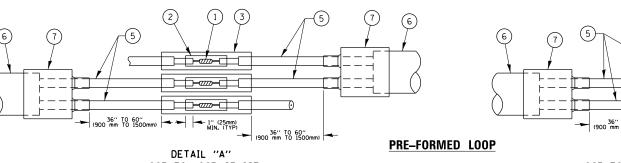
- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP \*1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



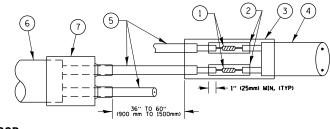
#### **DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE. THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.









DETAIL "B" LOOP-TO-CONTROLLER SPLICE

39 38 CONTRACT NO. 62A88

#### LOOP DETECTOR SPLICE

- (1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- (2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- (3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- (4) NO. 14 2/C TWISTED, SHIELDED CABLE.

- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR 7 BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = bartonrw	DESIGNED -	DAD	REVISED - DAG 1-1-14			nistr	CT ONE	F		F.A	SECTION	COUNTY
c:\pw_work\pwidot\bartonrw\d0427922\HMA		DRAWN -	BCK	REVISED -	STATE OF ILLINOIS	STANDARD TRAFFIC SIGNAL DESIGN DETAILS				TALLO	VAR.	2015-028RS	LAKE
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	PLOT DATE = 4/7/2015	DATE -	10-28-09	REVISED -		SCALE: NONE	SHEET NO. 2 OF 7 SI	IEETS	STA.	TO STA.	FED. ROAD	DIST. NO. 1 ILLINOIS FED	. AID PROJECT

## LOOPS NEXT TO SHOULDERS PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER. PAVED OR NON-PAVED SHOULDER Ê (1.5 m) (1.8 m) (1.5 m) 1" (25 mm) UNI DUCT-TRENCHED TO E/P •• (3.0 m) (3.0 m) \* = (600 mm)\* \* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS. ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)

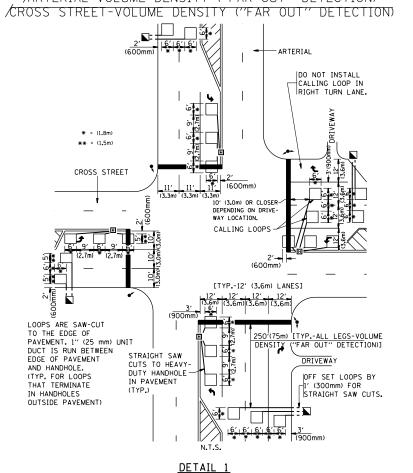
## LEFT TURN LANES WITH MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING) HANDHOLE LOCATION MAY HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD BLADOL TO ENESURE THAT WARRING! 814001 TO ENSURE THAT HANDHOLE TRENCHED 1" (25 mm) UNIT DUCT (3) \* \* \* = (600 mm) STRAIGHT SAW CUTS PERPENDICULAR TO MEDIAN (TYP.) (3.6 m) (900 mm) \*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO

PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING) \* = (600 mm) (900 m (1.8 m) (3.6 m |STRAIGHT SAW CUT TO HEAVY DUTY HANDHOLE (TYP.) PLACE HEAVY DUTY HANDHOLE BETWEEN FIRST AND SECOND LOOP AS SHOWN. NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



N.T.S.

DESIGNED

CHECKED

R.K.F.

DRAWN

DATE

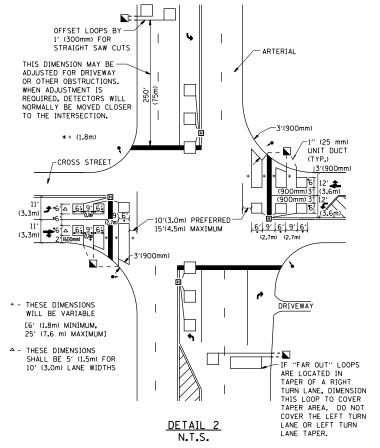
USER NAME = bartonrw

PLOT DATE = 4/7/2015

Lake-East-DistStd.dan

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SCALE: NONE

#### NOTES:

#### VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED,
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

#### PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

COUNTY

39 39

CONTRACT NO. 62A88

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STATE OF ILLINOIS EPARTMENT OF TRANSPORTATION

DISTRICT 1	1 – DE	TECTOR L	F.A RTE.	SECT	TION		COUNTY		
DETAIL	I C END	ROADWA	VAR.	2015-0	028RS		LAKE		
DLIAI	LOTON	NUMDVV		TS-07	'		CONTRA		
SHEET NO. 1 OF 1 SHEETS STA. TO STA.						DAD DIST. NO. 1	ILLINOIS F	FED. AI	D PROJECT