

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY  
FAU ROUTE 2637 (FAIRVIEW AVENUE)  
FAP 0369 (75TH STREET) TO FAU 1504 (55TH STREET)  
RESURFACING**  
SECTION: 14-00108-00-RS  
PROJECT: M-4003(441)  
VILLAGE OF DOWNERS GROVE  
DU PAGE COUNTY  
C-91-162-15

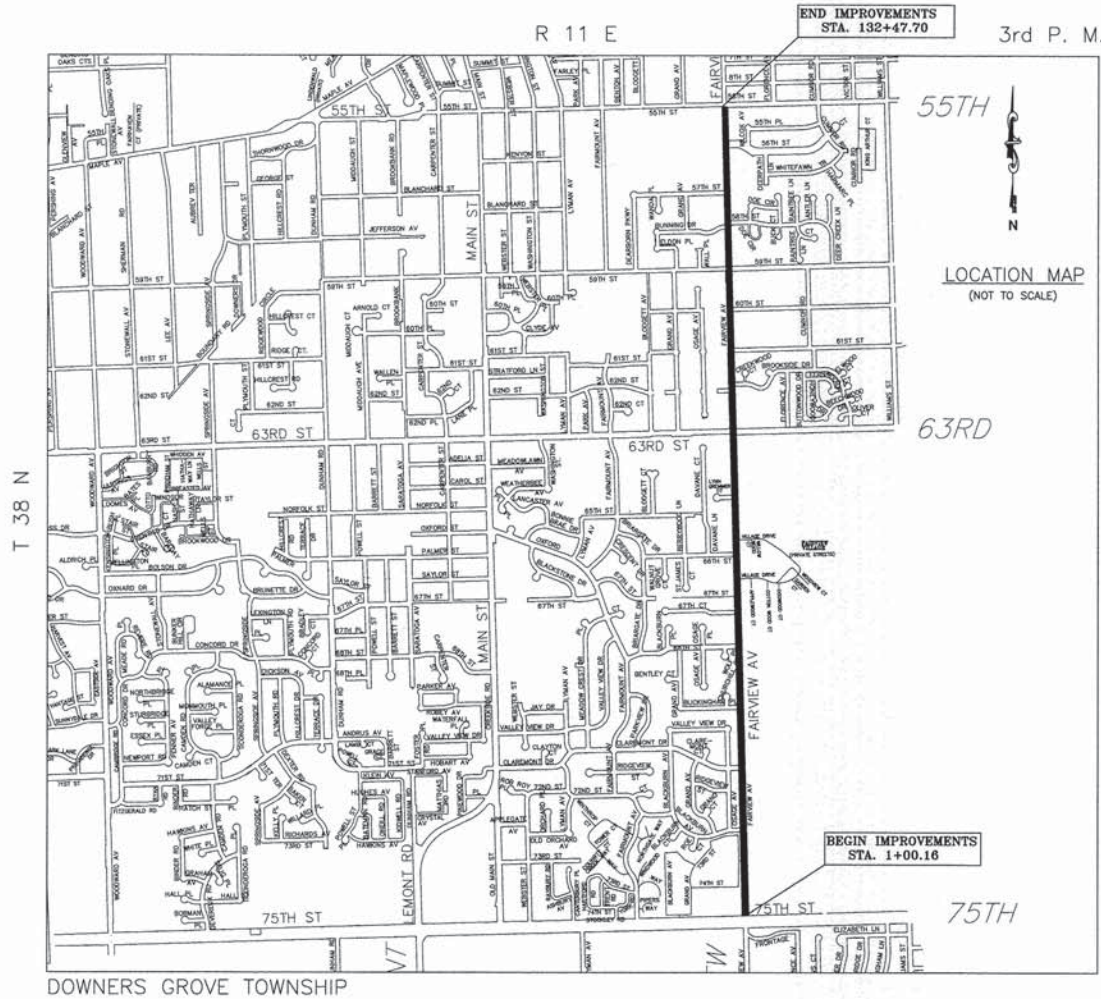
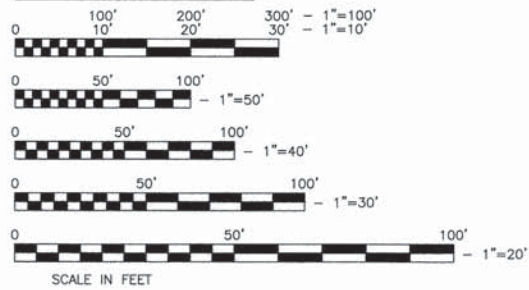
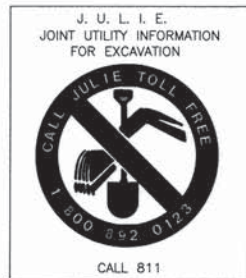
F.A.H. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	1
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 61B37				

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FAIRVIEW AVENUE DESIGN DESIGNATION  
MINOR ARTERIAL  
DESIGN SPEED: 30 MPH  
ADT: 14,000



GROSS LENGTH: 13,148 FT = 2.49 MILE

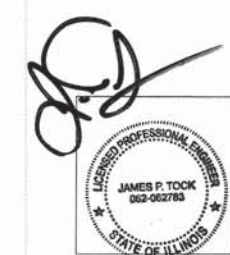
NET LENGTH: 13,036 FT = 2.47 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED April 7 2015  
*Nancy Newton*  
DIRECTOR OF PUBLIC WORKS, VILLAGE OF DOWNERS GROVE

PASSED April 22 2015  
*Christopher Holt*  
DIVISION ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW April 22 2015  
*John F. ...*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER



James P. Tock, P.E.  
#062-062783  
My License Expires 11/30/15

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OF THE STATE OF ILLINOIS

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P. E. 847-705-4021 SCHAMBOURG, IL

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZE PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES CAN BE USED

Contract No. 61B37



## IDOT STANDARDS

1. ALL REFERENCES TO THE 'VILLAGE' IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE VILLAGE OF DOWNERS GROVE.

2. ALL REFERENCES TO THE 'STANDARD SPECIFICATIONS' IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE 'STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION' ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) ON JANUARY 1, 2012, ALONG WITH SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS AS ADOPTED JANUARY 1, 2015.

3. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE, DEPARTMENT, AND THE ENGINEERS DO NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING THE CONSTRUCTION OPERATION SO AS NOT TO DAMAGE THEM. IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.20 OF THE 'STANDARD SPECIFICATIONS' THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS.

4. THOSE EXISTING TRAFFIC SIGNS WHICH ARE SO DESIGNATED BY THE ENGINEER SHALL BE REMOVED, STORED AND SUBSEQUENTLY RELOCATED BY THE CONTRACTOR AT NO ADDITIONAL COST. ANY SIGNS WHICH ARE DAMAGED BY THE CONTRACTOR AS DETERMINED BY THE ENGINEER SHALL BE REPLACED IN KIND BY THE CONTRACTOR AND TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST.

5. ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE PLANS AND IDOT STANDARDS FOR TRAFFIC CONTROL AND PROTECTION.

6. SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.

7. DEBRIS REMOVAL - MATERIALS RESULTING FROM THE VARIOUS CONSTRUCTION OPERATIONS SHALL BE REMOVED AT THE END OF EACH WORK DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE ENGINEER WILL REMOVE MATERIALS AND THE CONTRACTOR SHALL BE BILLED ACCORDINGLY.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THE WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

9. WHENEVER, DURING CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

10. THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS WITHOUT PRIOR AUTHORIZATION FROM THE VILLAGE WATER DEPARTMENT.

11. QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. UPON COMPLETION OF SURFACE REMOVAL (MILLING), CONTRACTOR SHALL PER THE DIRECTION OF THE ENGINEER, PERFORM A PROOF ROLL OF ALL EXPOSED PAVEMENT. THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD PRIOR TO REMOVAL.

12. HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDE DIN THE PLANS, UNLESS OTHERWISE SPECIFIED.

13. THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NORMAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASE ON WHICH THEY ARE PLACED. PLAN THICKNESS SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

14. MAILBOXES WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED, TEMPORARILY RELOCATED, AND REPLACED UPON COMPLETION OF THE PROPOSED IMPROVEMENTS IN ACCORDANCE WITH ARTICLE 107.20 AND AS DIRECTED BY THE ENGINEER.

15. THE CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80 DEGREES OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80 DEGREES OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.

16. SPECIAL ATTENTION IS CALLED TO ARTICLE 107.30 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DEFAACEMENT OF ANY CONCRETE POURS BEFORE THEY HAVE SET UP. CONCRETE SIDEWALK, DRIVEWAY, CURB, AND CURB AND GUTTER THAT HAVE BEEN DEFACED, IN THE OPINION OF THE ENGINEER, SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT HIS EXPENSE. THIS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

17. FOR WORK OUTSIDE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.

### DUPAGE COUNTY DIVISION OF TRANSPORTATION GENERAL NOTES AND SPECIFICATIONS

1. ALL CONSTRUCTION WITHIN THE COUNTY'S RIGHT-OF-WAY SHALL BE PERFORMED ACCORDING TO IDOT'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (LATEST EDITION) AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" (LATEST EDITION).

2. DAILY LANE CLOSURES ARE PERMITTED BETWEEN 9:00 A.M. AND 4:00 P.M. ONLY. TRAFFIC CONTROL SHALL CONFORM TO IDOT'S HIGHWAY STANDARDS THE FHWA'S 'MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES' AND IDOT'S SUPPLEMENT TO THE MUTCD AT ALL TIMES DURING CONSTRUCTION.

3. LANE CLOSURES ARE NOT PERMITTED ON COUNTY ROADWAYS DURING SNOWFALL OR WITHIN 2 HOURS PRIOR TO PREDICTED SNOWFALL OR PRECIPITATION CONDITIONS BETWEEN NOVEMBER 15 AND APRIL 15 FOR MAINTENANCE OF THE ROADWAY PAVEMENT BY COUNTY HIGHWAY MAINTENANCE DEPARTMENT STAFF AND EQUIPMENT.

4. DISTURBED AREAS OF THE RIGHT-OF-WAY SHALL BE DRESSED WITH A MINIMUM OF 6" TOPSOIL AND CLASS 2A SALT TOLERANT SEED (WITH EROSION CONTROL BLANKET) OR SOD (SALT TOLERANT AND STAKED IN PLACE).

5. THE DUPAGE COUNTY DIVISION OF TRANSPORTATION OPERATES/MAINTAINS TRAFFIC SIGNALS AND RELATED EQUIPMENT WITHIN THE VICINITY OF THE PROJECT. CONTACT THE DIVISION OF TRANSPORTATION A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION WITHIN THE COUNTY'S RIGHT OF WAY AND WITHIN 300' OF ANY COUNTY MAINTAINED SIGNAL TO LOCATE SAID EQUIPMENT. TRAFFIC SIGNALS AND RELATED EQUIPMENT ARE NOT ON THE J.U.L.I.E. SYSTEM.

6. EROSION CONTROL MEASURES SHALL COMPLY WITH THE MINIMUM REQUIREMENTS OF THE DUPAGE COUNTY STORMWATER AND FLOODPLAIN ORDINANCE SPECIFICATIONS AT ALL TIMES.

7. EQUIPMENT AND MATERIALS SHALL NOT BE STORED WITHIN THE COUNTY'S RIGHT-OF-WAY AT ANY TIME WITHOUT THE EXPRESS WRITTEN PERMISSION OF THE COUNTY ENGINEER, OR HIS DULY AUTHORIZED ASSIGN.

8. PAVEMENT, CURB/GUTTER AND STORM STRUCTURES WITHIN THE COUNTY'S RIGHT-OF-WAY SHALL BE MAINTAINED FREE OF MUD/DEBRIS AT ALL TIMES AND SHALL BE CLEANED AS IS REQUIRED AND/OR AS DIRECTED BY DUPAGE COUNTY.

9. CONTACT DUPAGE COUNTY (630/407-6900) A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION TO ARRANGE FOR INSPECTIONS OF AND AT THE COMPLETION OF THE DESCRIBED WORK WITHIN THE COUNTY'S RIGHT-OF-WAY.

10. TRENCH BACKFILL FOR NON-PAVED AREAS SHALL BE INSTALLED WITHIN THE COUNTY'S RIGHT-OF-WAY PER DUPAGE COUNTY'S STANDARD.

11. TRENCH BACKFILL BELOW EXISTING OR PROPOSED PAVEMENT, CURB/GUTTER AND/OR SIDEWALK SHALL BE INSTALLED WITHIN THE COUNTY'S RIGHT OF WAY PER DUPAGE COUNTY'S STANDARD.

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424016-02	MID-BLOCK CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C & D PATCHES
606001-06	CONCRETE CURB TYPE B & COMBINATION CONCRETE CURB AND GUTTER
701427-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS < 40 MPH
701606-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

FILE NAME =	USER NAME - USER	DESIGNED - NRH	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAIRVIEW AVENUE IMPROVEMENTS GENERAL NOTES AND INDEX OF STANDARDS	F.A.S. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
FILE NAME =		DRAWN - NRH	REVISED			2637	14-00108-00-RS	DU PAGE	26	2	
	PLOT SCALE -	CHECKED - JPT	REVISED			CONTRACT NO. 61B37					
	PLOT DATE - 04/06/15	DATE - 04/06/15	REVISED			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
						NOT TO SCALE	SHEET NO. 2 OF 26 SHEETS	STA.	TO STA.		



# SUMMARY OF QUANTITIES

FAIRVIEW AVENUE				
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE 0005
20200100	EARTH EXCAVATION	CU YD	50	50
21101625	TOPSOIL FURNISH AND PLACE, 6'	SQ YD	585	585
25200110	SODDING, SALT TOLERANT	SQ YD	585	585
25200200	SUPPLEMENTAL WATERING	UNIT	5	5
28000510	INLET FILTERS	EACH	150	150
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	51,828	51,828
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	80	80
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50	TON	2,373	2,373
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	535	535
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX 'D', N70	TON	4,745	4,745
42001300	PROTECTIVE COAT	SQ YD	845	845
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	40	40
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	125	125
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	6,175	6,175
42400800	DETECTABLE WARNINGS	SQ FT	600	600
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	54,845	54,845
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	260	260
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1,315	1,315
44000600	SIDEWALK REMOVAL	SQ FT	6,960	6,960
44201773	CLASS D PATCHES, TYPE I, 11 INCH	SQ YD	200	200
44201777	CLASS D PATCHES, TYPE II, 11 INCH	SQ YD	200	200
44201781	CLASS D PATCHES, TYPE III, 11 INCH	SQ YD	300	300
44201783	CLASS D PATCHES, TYPE IV, 11 INCH	SQ YD	450	450
60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	5	5
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	5	5
60260300	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	5	5

FAIRVIEW AVENUE				
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE 0005
60260400	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	5	5
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	17	17
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1,315	1,315
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	40	40
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1
67100100	MOBILIZATION	LSUM	1	1
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	LSUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1
70300100	SHORT TERM PAVEMENT MARKINGS	FOOT	9,090	9,090
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	762	762
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	3,792	3,792
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	2,262	2,262
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	27,610	27,610
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,850	3,850
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	580	580
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	535	535
* 88600100	DETECTOR LOOP, TYPE 1	FOOT	1,730	1,730
Z0004510	HMA DRIVEWAY PAVEMENT, 3"	SQ YD	10	10
Z0004530	HMA DRIVEWAY PAVEMENT, 8"	SQ YD	85	85
X2800510	INLET FILTER CLEANING	EACH	150	150
X6030310	FRAMES AND LIDS TO BE ADJUSTED, (SPECIAL)	EACH	10	10
Z0013798	CONSTRUCTION LAYOUT	LSUM	1	1
# Z0076600	TRAINEES	HR	500	500
# Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HR	500	500

\* DENOTES SPECIALTY ITEM # 0042

FILE NAME =	USER NAME - USER	DESIGNED - NRH	REVISED
FILE NAME =		DRAWN - NRH	REVISED
	PLOT SCALE -	CHECKED - JPT	REVISED
	PLOT DATE - 04/06/15	DATE - 04/06/15	REVISED

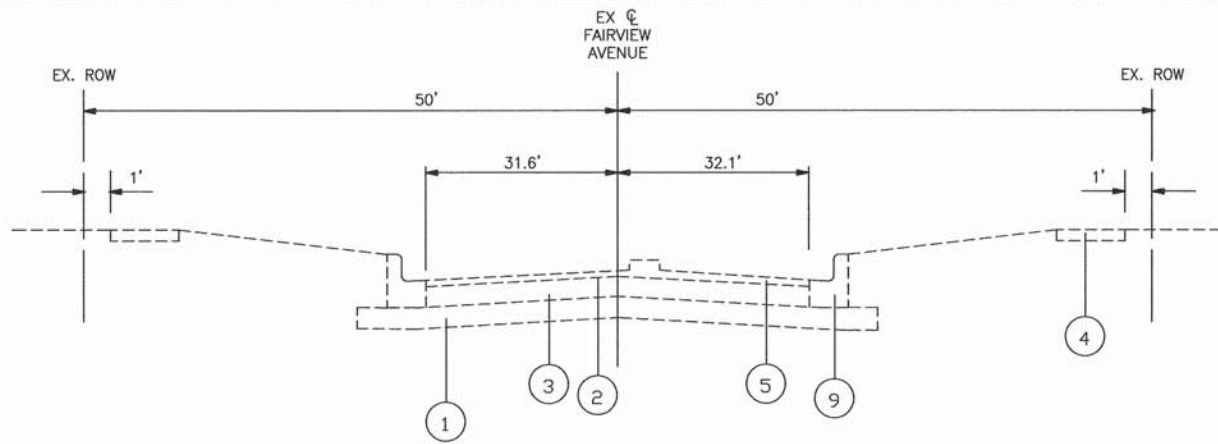
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
SUMMARY OF QUANTITIES

NOT TO SCALE      SHEET NO. 3 OF 26 SHEETS      STA.      TO STA.

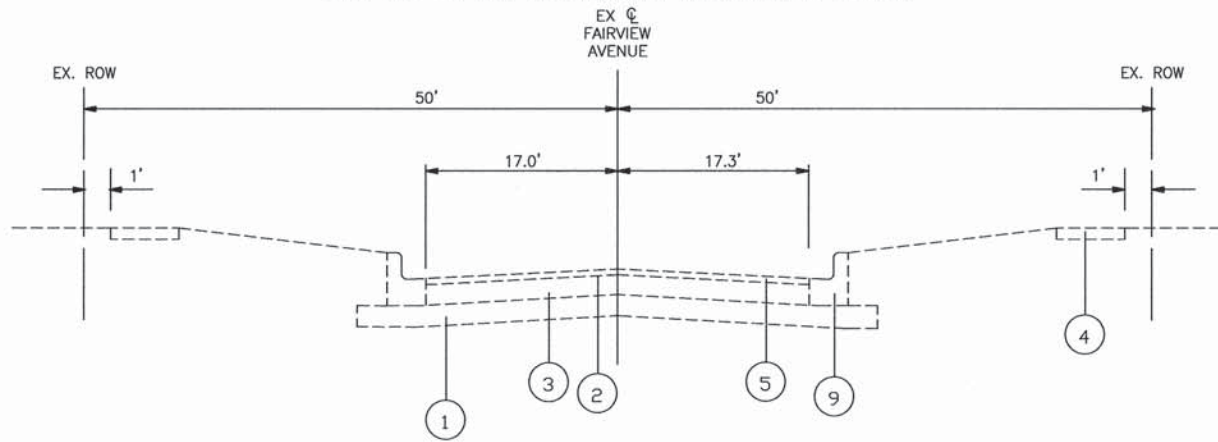
F. & U. SHEET	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	3
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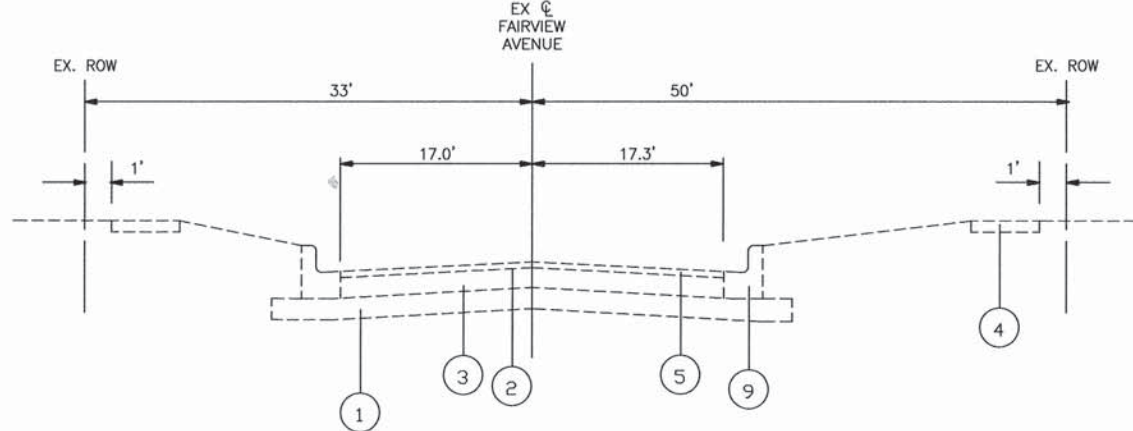
**EXISTING TYPICAL SECTION**

STA. 1+00 TO STA. 4+27, FAIRVIEW AVENUE  
 STA. 4+27 TO STA. 9+00, FAIRVIEW AVENUE TAPER SECTION



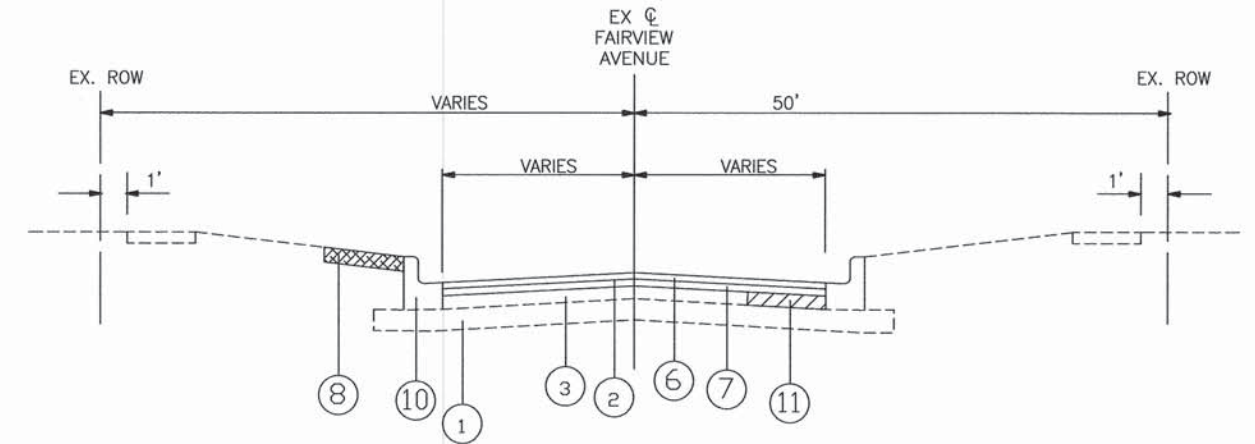
**EXISTING TYPICAL SECTION**

STA. 9+00 TO STA. 74+00, FAIRVIEW AVENUE  
 STA. 74+00 TO STA. 83+00, FAIRVIEW AVENUE TAPER SECTION  
 STA. 83+00 TO STA. 92+85, FAIRVIEW AVENUE  
 STA. 101+44 TO STA. 106+43, FAIRVIEW AVENUE



**EXISTING TYPICAL SECTION**

STA. 92+85 TO STA. 101+44, FAIRVIEW AVENUE  
 STA. 106+43 TO STA. 132+47.70, FAIRVIEW AVENUE



**PROPOSED TYPICAL SECTION**

STA. 1+00 TO STA. 132+47.70, FAIRVIEW AVENUE

**LEGEND**

- ① EXISTING SUBGRADE
- ② EXISTING BITUMINOUS PAVEMENT, 2" - 4", VARIES
- ③ EXISTING CONCRETE, BRICK BASE, 5 1/2" - 10", VARIES
- ④ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- ⑤ PROPOSED HMA SURFACE REMOVAL, 2"
- ⑥ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50, 3/4"
- ⑧ PROPOSED SODDING, SALT TOLERANT & TOP SOIL FURNISH AND PLACE, 6" (LOCATIONS DETERMINED BY ENGINEER)
- ⑨ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.12/B6.18 REMOVAL WHERE SHOWN ON PLANS
- ⑩ PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B6.12/B6.18 REPLACEMENT WHERE SHOWN ON PLANS
- ⑪ CLASS D PATCH (LOCATION AND DIMENSIONS DETERMINED BY ENGINEER), 11"

CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS @ Ndes
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 1 1/2"	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50, 3/4"	3.5% @ 50 GYR
DRIVEWAY: HOT MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 3"	4% @ 50 GYR
CLASS D PATCHES, (HMA BINDER) IL-19mm, N70, 13" (IN 4 LIFTS)	4% @ 70 GYR

-THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.

-THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 - 22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 - 22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

FILE NAME =	USER NAME -- USER	DESIGNED -- NRH	REVISED
FILE NAME =		DRAWN -- NRH	REVISED
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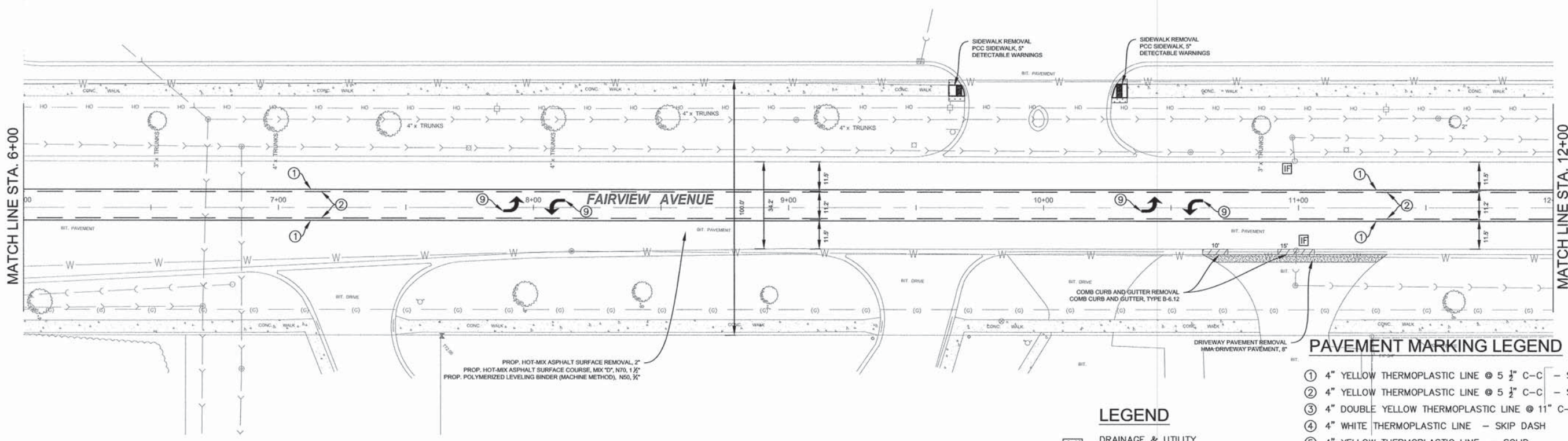
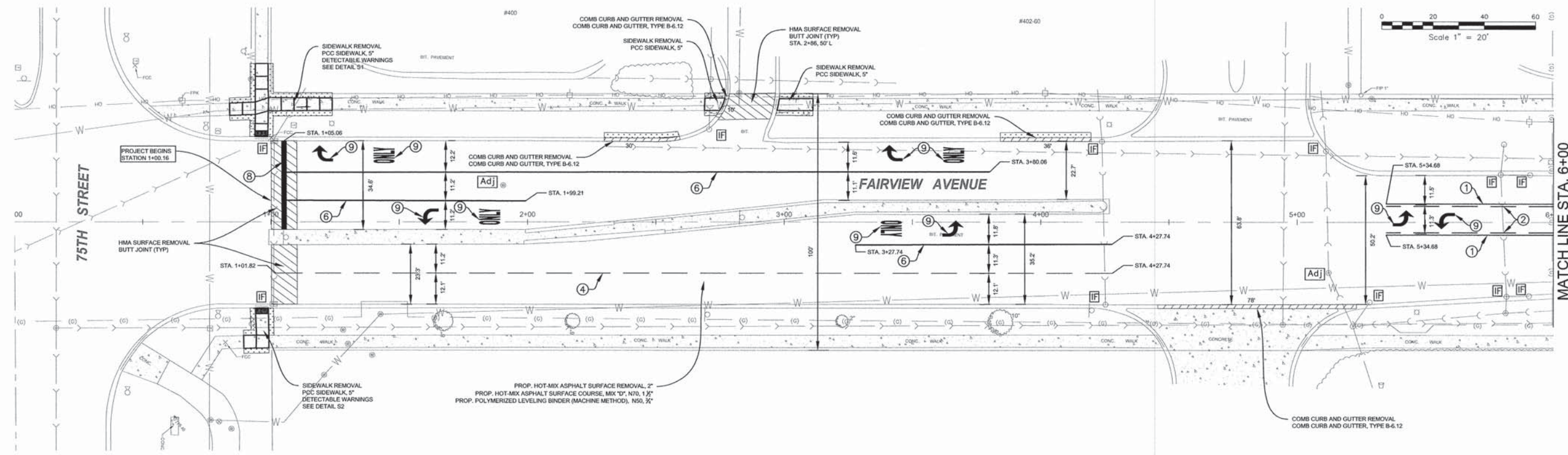
STATE OF ILLINOIS  
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FAIRVIEW AVENUE IMPROVEMENTS  
 TYPICAL SECTIONS

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2637	14-00108-00-RS	DU PAGE	26	4
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

NOT TO SCALE SHEET NO. 4 OF 26 SHEETS STA. TO STA.





**PAVEMENT MARKING LEGEND**

- ① 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SOLID
- ② 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SKIP DASH
- ③ 4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11" C-C - SOLID
- ④ 4" WHITE THERMOPLASTIC LINE - SKIP DASH
- ⑤ 4" YELLOW THERMOPLASTIC LINE - SOLID
- ⑥ 6" WHITE THERMOPLASTIC LINE - SOLID
- ⑦ 12" YELLOW THERMOPLASTIC LINE - SOLID
- ⑧ 24" WHITE THERMOPLASTIC LINE - SOLID
- ⑨ LETTERS AND SYMBOLS - FULL SIZE
- ⑩ 12" WHITE THERMOPLASTIC LINE - SOLID

**LEGEND**

- Adj** DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
- Adj-S** SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
- IF** INLET FILTERS
- TOPSOIL, SOD RESTORATION

- NOTES**
1. CLASS D PATCHES, 11" AS DIRECTED BY THE ENGINEER.
  2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
  3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

FILE NAME =	USER NAME - USER	DESIGNED - NRH	REVISED
FILE NAME =		DRAWN - NRH	REVISED
	PLOT SCALE -	CHECKED - JPT	REVISED
	PLOT DATE - 04/06/15	DATE - 04/06/15	REVISED

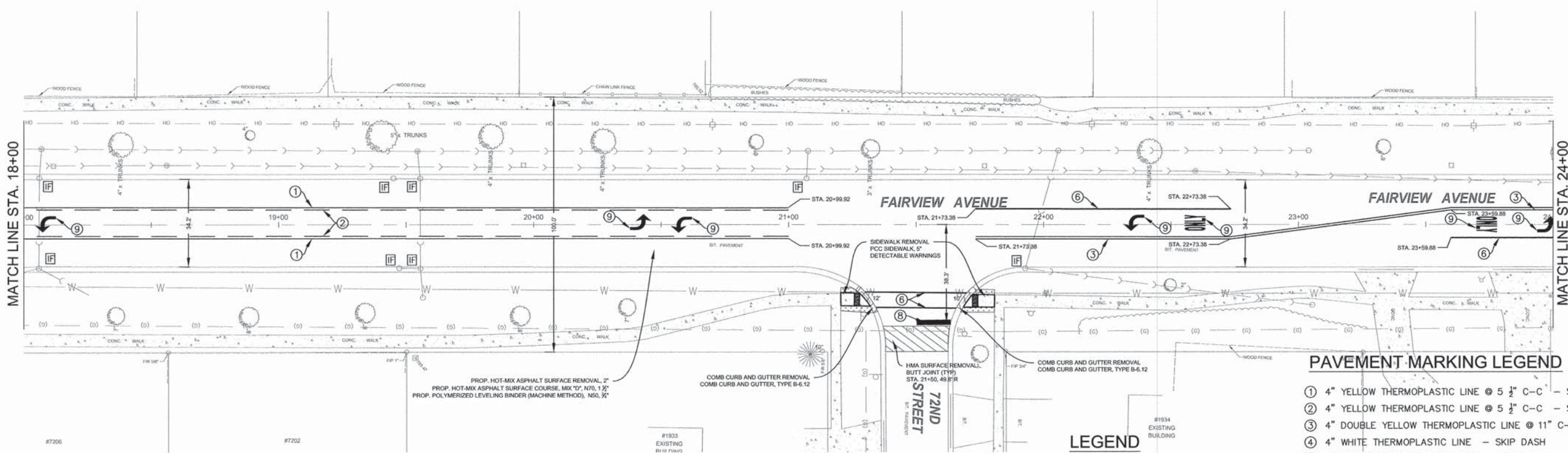
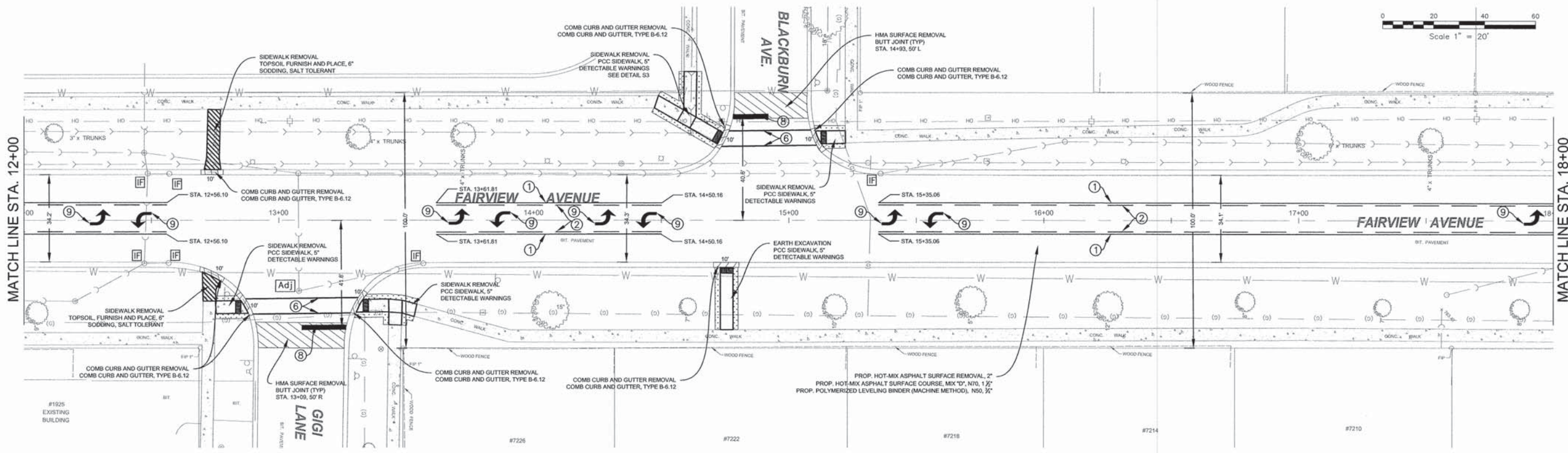
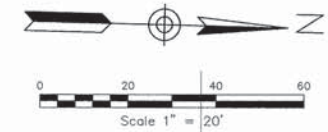
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
IMPROVEMENT PLAN

SCALE: 1"=20' SHEET NO. 5 OF 26 SHEETS STA. 0+00 TO STA. 12+00

F.A.R. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	5
CONTRACT NO. 61B37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





- NOTES**
1. CLASS D PATCHES, 11" AS DIRECTED BY THE ENGINEER.
  2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
  3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

- LEGEND**
- [Adj] DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
  - [Adj-S] SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
  - [IF] INLET FILTERS
  - [Topsoil] TOPSOIL, SOD RESTORATION

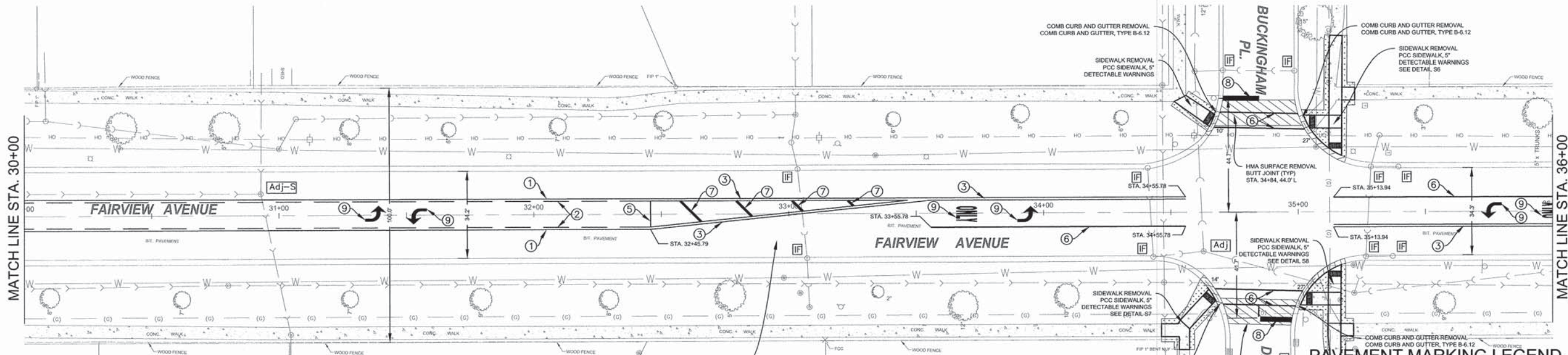
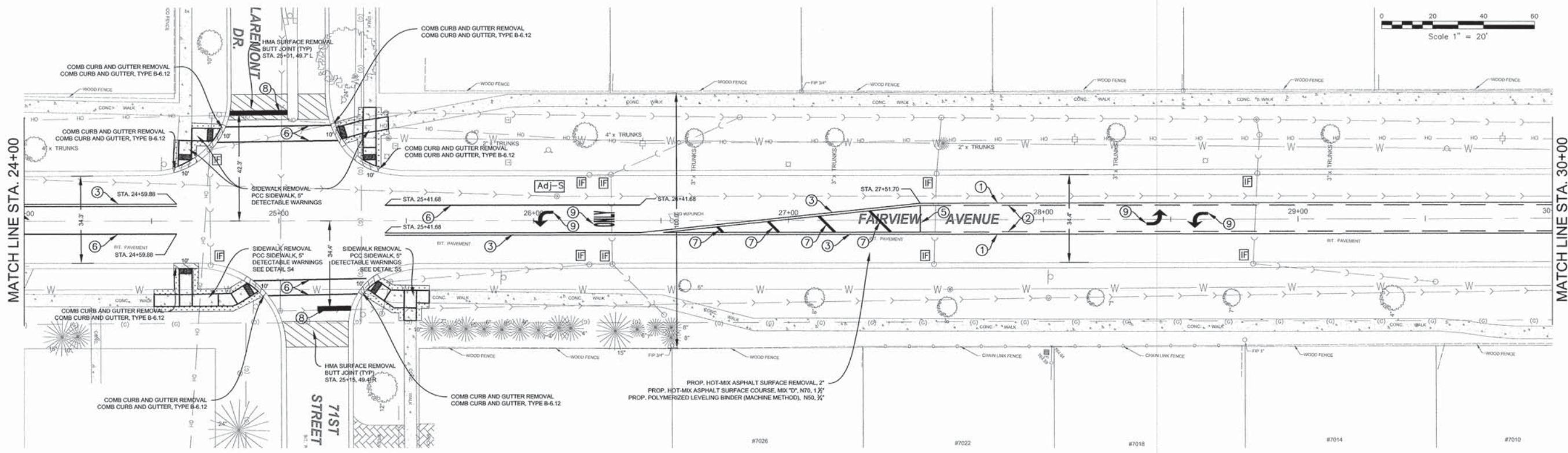
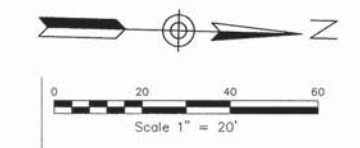
- PAVEMENT MARKING LEGEND**
- ① 4" YELLOW THERMOPLASTIC LINE @ 5 1/2' C-C - SOLID
  - ② 4" YELLOW THERMOPLASTIC LINE @ 5 1/2' C-C - SKIP DASH
  - ③ 4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11' C-C - SOLID
  - ④ 4" WHITE THERMOPLASTIC LINE - SKIP DASH
  - ⑤ 4" YELLOW THERMOPLASTIC LINE - SOLID
  - ⑥ 6" WHITE THERMOPLASTIC LINE - SOLID
  - ⑦ 12" YELLOW THERMOPLASTIC LINE - SOLID
  - ⑧ 24" WHITE THERMOPLASTIC LINE - SOLID
  - ⑨ LETTERS AND SYMBOLS - FULL SIZE
  - ⑩ 12" WHITE THERMOPLASTIC LINE - SOLID

FILE NAME =	USER NAME -- USER	DESIGNED -- NRH	REVISED
FILE NAME =		DRAWN -- NRH	REVISED
	PLOT SCALE --	CHECKED -- JPT	REVISED
	PLOT DATE -- 04/06/15	DATE -- 04/06/15	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
IMPROVEMENT PLAN





**PAVEMENT MARKING LEGEND**

- ① 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SOLID
- ② 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SKIP DASH
- ③ 4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11" C-C - SOLID
- ④ 4" WHITE THERMOPLASTIC LINE - SKIP DASH
- ⑤ 4" YELLOW THERMOPLASTIC LINE - SOLID
- ⑥ 6" WHITE THERMOPLASTIC LINE - SOLID
- ⑦ 12" YELLOW THERMOPLASTIC LINE - SOLID
- ⑧ 24" WHITE THERMOPLASTIC LINE - SOLID
- ⑨ LETTERS AND SYMBOLS - FULL SIZE
- ⑩ 12" WHITE THERMOPLASTIC LINE - SOLID

**LEGEND**

- [Adj] DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
- [Adj-S] SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
- [IF] INLET FILTERS
- [Hatched Box] TOPSOIL, SOD RESTORATION

**NOTES**

1. CLASS D PATCHES, 11" AS DIRECTED BY THE ENGINEER.
2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

FILE NAME =	USER NAME -- USER	DESIGNED -- NRH	REVISED
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	PLOT SCALE --	CHECKED -- JPT	REVISED
	PLOT DATE -- 04/06/15	DATE -- 04/06/15	REVISED

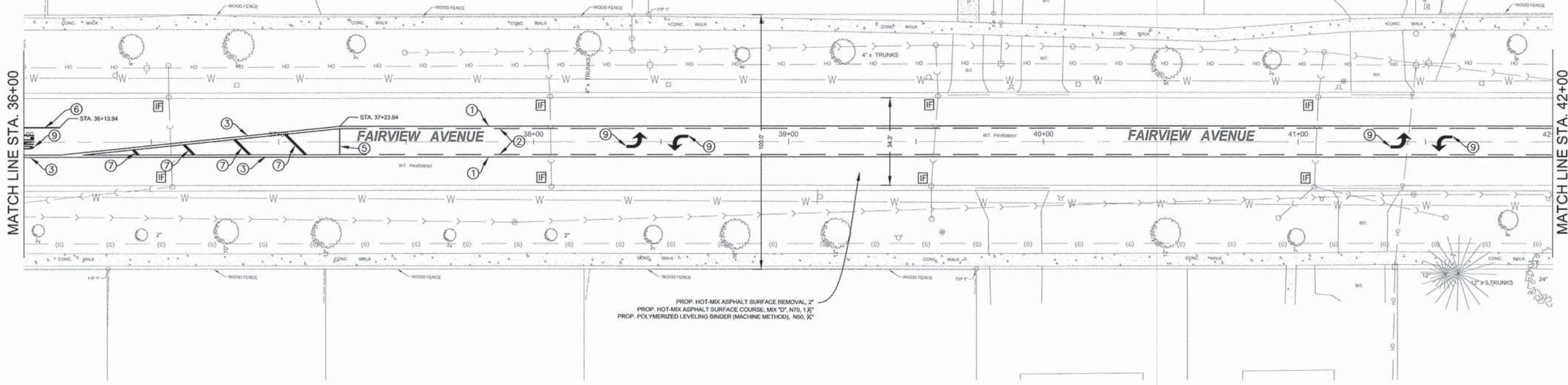
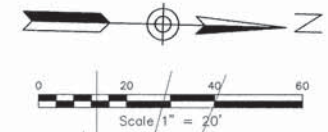
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
IMPROVEMENT PLAN

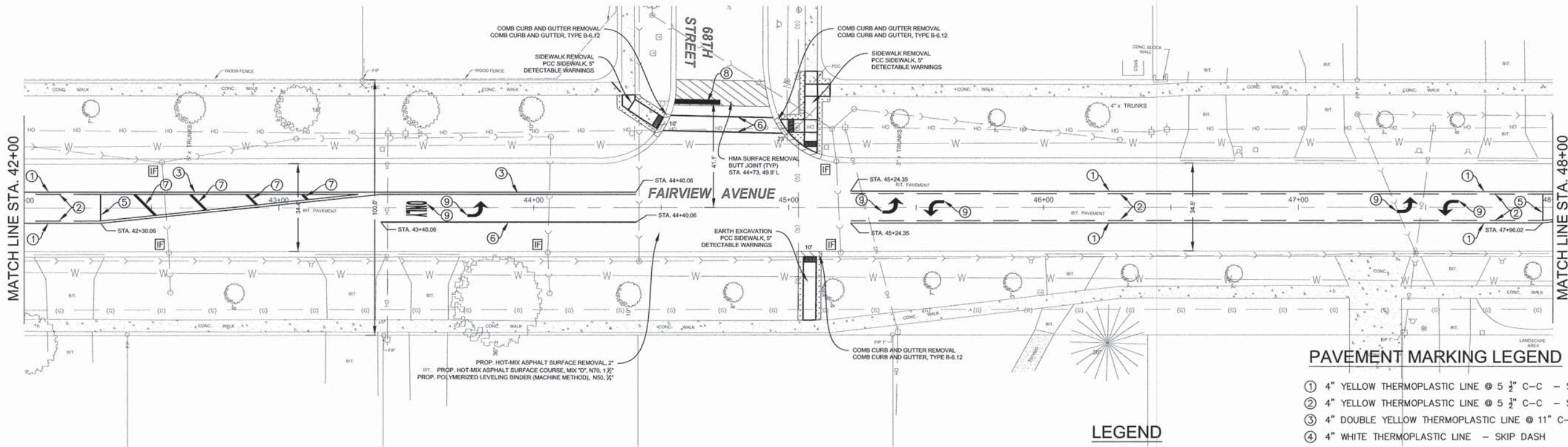
SCALE: 1"=20' SHEET NO. 7 OF 26 SHEETS STA. 24+00 TO STA. 36+00

F. A. N. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	7
CONTRACT NO. 61B37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2"  
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), NS0, 3/4"



PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2"  
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), NS0, 3/4"

**PAVEMENT MARKING LEGEND**

- ① 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SOLID
- ② 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SKIP DASH
- ③ 4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11" C-C - SOLID
- ④ 4" WHITE THERMOPLASTIC LINE - SKIP DASH
- ⑤ 4" YELLOW THERMOPLASTIC LINE - SOLID
- ⑥ 6" WHITE THERMOPLASTIC LINE - SOLID
- ⑦ 12" YELLOW THERMOPLASTIC LINE - SOLID
- ⑧ 24" WHITE THERMOPLASTIC LINE - SOLID
- ⑨ LETTERS AND SYMBOLS - FULL SIZE
- ⓐ 12" WHITE THERMOPLASTIC LINE - SOLID

**LEGEND**

- [Adj] DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
- [Adj-S] SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
- [IF] INLET FILTERS
- [TOPSOIL] TOPSOIL, SOD RESTORATION

**NOTES**

1. CLASS D PATCHES, 11" AS DIRECTED BY THE ENGINEER.
2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

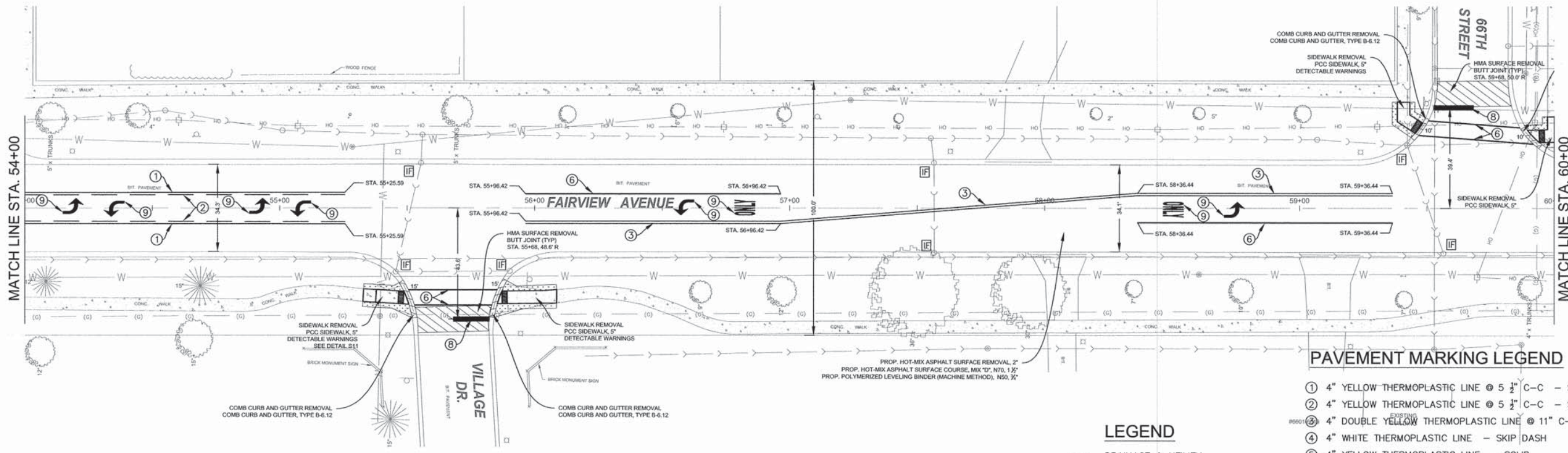
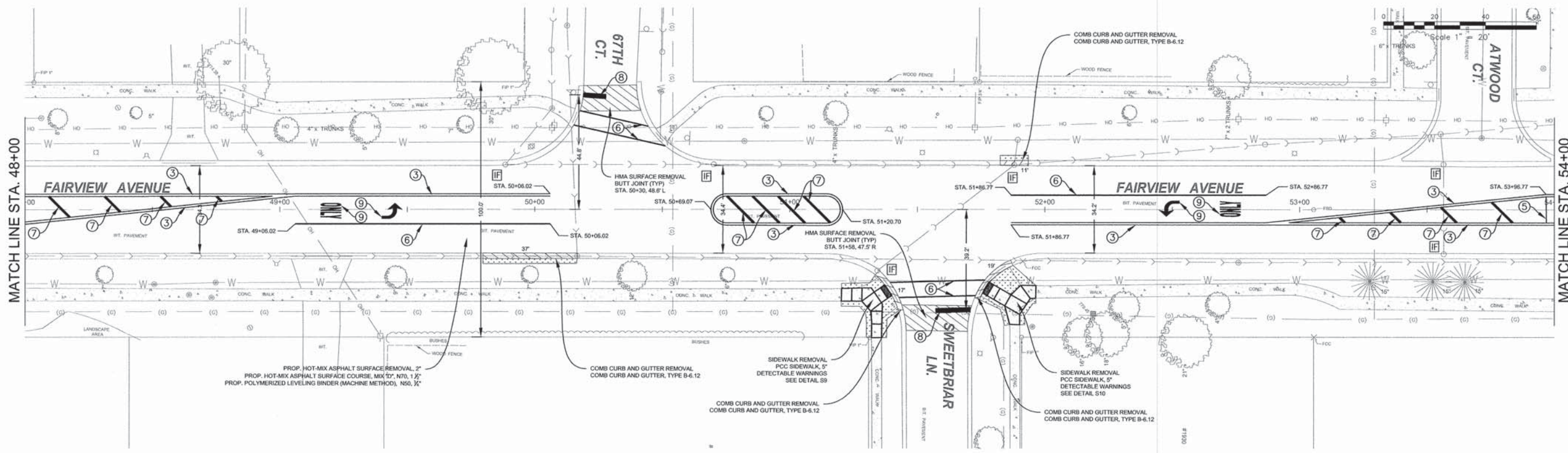
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
 IMPROVEMENT PLAN

FILE NAME =	USER NAME - USER	DESIGNED - NRH	REVISED
FILE NAME =		DRAWN - NRH	REVISED
PLOT SCALE -		CHECKED - JPT	REVISED
PLOT DATE - 04/06/15		DATE - 04/06/15	REVISED

SCALE: 1"=20'	SHEET NO. 8 OF 26 SHEETS	STA. 36+00 TO STA. 48+00	F. A. S. REC. 2637	SECTION 14-00108-00-RS	COUNTY DU PAGE	TOTAL SHEETS 26	SHEET NO. B
			CONTRACT NO. 61B37				
			FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT		





- NOTES**
1. CLASS D PATCHES, 11" AS DIRECTED BY THE ENGINEER.
  2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
  3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

- LEGEND**
- [Adj] DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
  - [Adj-S] SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
  - [IF] INLET FILTERS
  - [Pattern] TOPSOIL, SOD RESTORATION

- PAVEMENT MARKING LEGEND**
- ① 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SOLID
  - ② 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SKIP DASH
  - ③ 4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11" C-C - SOLID
  - ④ 4" WHITE THERMOPLASTIC LINE - SKIP DASH
  - ⑤ 4" YELLOW THERMOPLASTIC LINE - SOLID
  - ⑥ 6" WHITE THERMOPLASTIC LINE - SOLID
  - ⑦ 12" YELLOW THERMOPLASTIC LINE - SOLID
  - ⑧ 24" WHITE THERMOPLASTIC LINE - SOLID
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FILE NAME =	USER NAME -- USER	DESIGNED -- NRH	REVISED
FILE NAME =		DRAWN -- NRH	REVISED
	PLOT SCALE --	CHECKED -- JPT	REVISED
	PLOT DATE -- 04/06/15	DATE -- 04/06/15	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
IMPROVEMENT PLAN

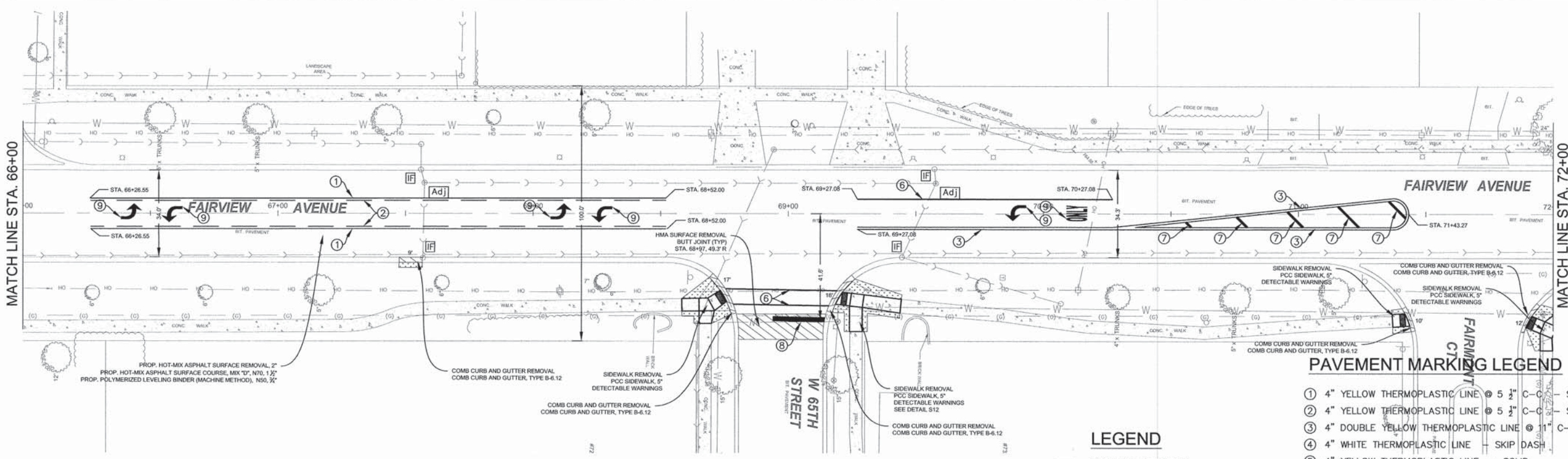
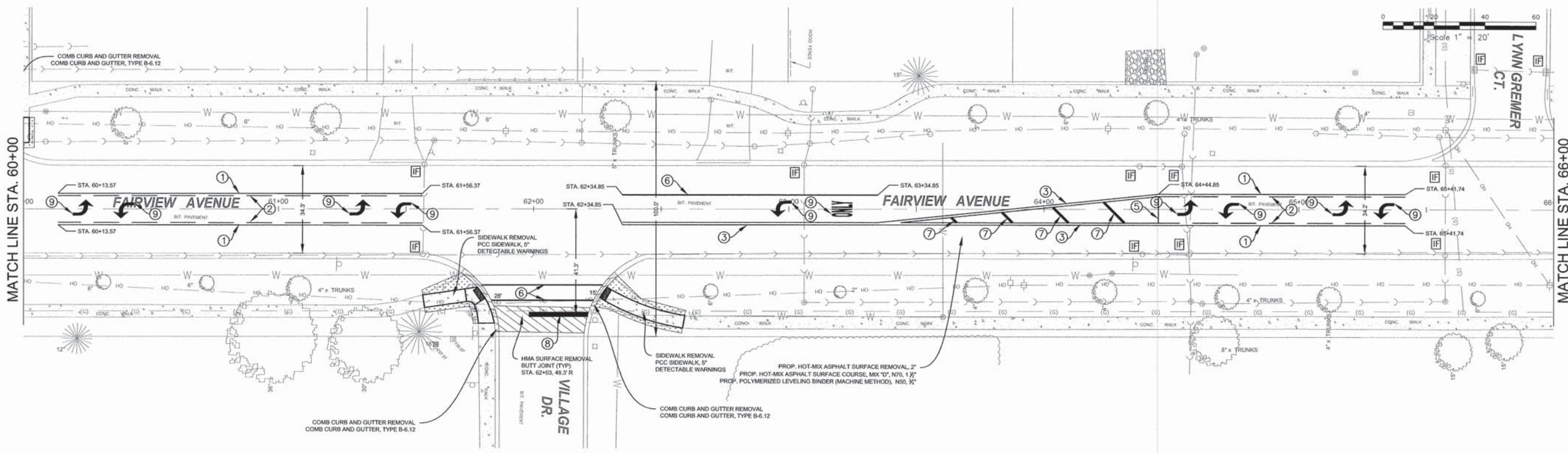
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F.A.S. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	9
CONTRACT NO. 61B37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





Scale 1" = 20'



- NOTES**
1. CLASS D PATCHES, 11" AS DIRECTED BY THE ENGINEER.
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  3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

- LEGEND**
- [Adj] DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
  - [Adj-S] SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
  - [IF] INLET FILTERS
  - [Pattern] TOPSOIL, SOD RESTORATION

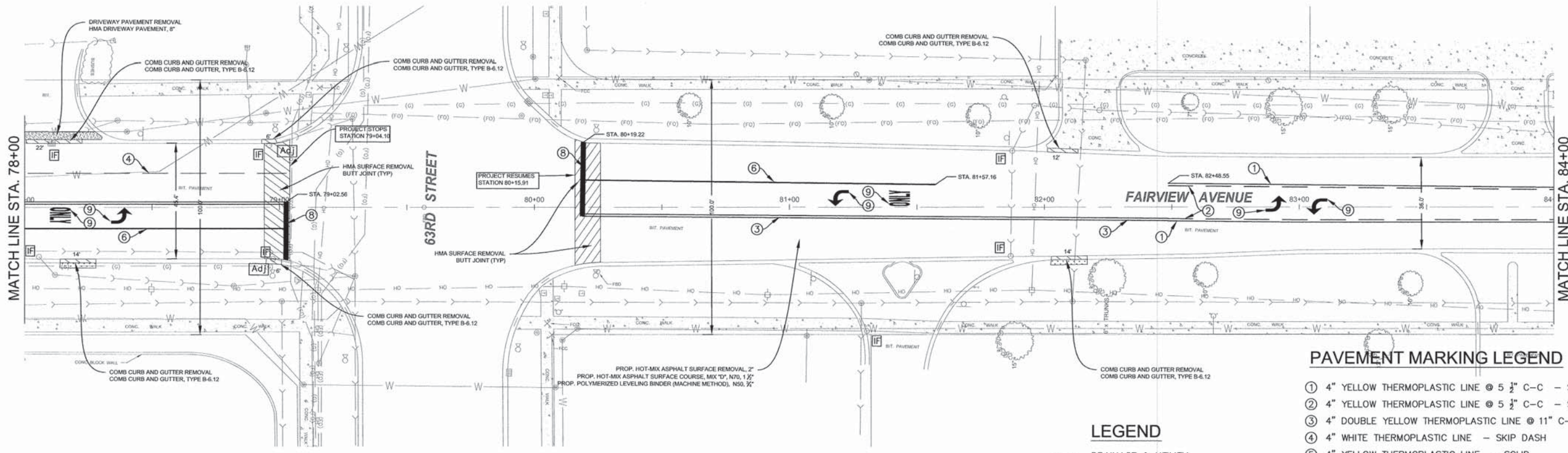
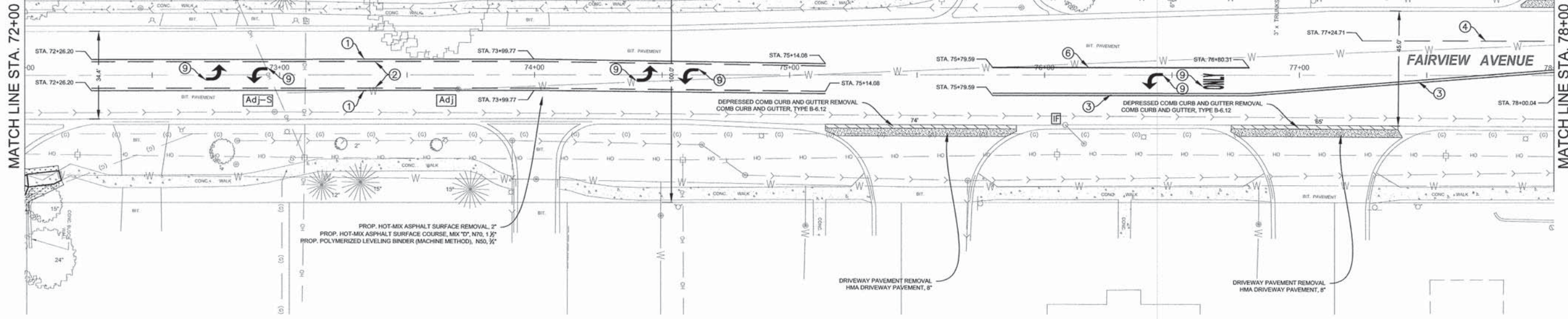
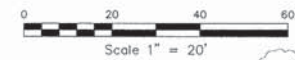
- PAVEMENT MARKING LEGEND**
- ① 4" YELLOW THERMOPLASTIC LINE @ 5 1/2' C-C - SOLID
  - ② 4" YELLOW THERMOPLASTIC LINE @ 5 1/2' C-C - SKIP DASH
  - ③ 4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11' C-C - SOLID
  - ④ 4" WHITE THERMOPLASTIC LINE - SKIP DASH
  - ⑤ 4" YELLOW THERMOPLASTIC LINE - SOLID
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  - ⑦ 12" YELLOW THERMOPLASTIC LINE - SOLID
  - ⑧ 24" WHITE THERMOPLASTIC LINE - SOLID
  - ⑨ LETTERS AND SYMBOLS - FULL SIZE
  - ⑩ 12" WHITE THERMOPLASTIC LINE - SOLID

FILE NAME =	USER NAME -- USER	DESIGNED -- NRH	REVISED
FILE NAME =		DRAWN -- NRH	REVISED
PLOT SCALE --		CHECKED -- JPT	REVISED
PLOT DATE -- 04/06/15		DATE -- 04/06/15	REVISED

STATE OF ILLINOIS  
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FAIRVIEW AVENUE IMPROVEMENTS  
IMPROVEMENT PLAN





**PAVEMENT MARKING LEGEND**

- ① 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SOLID
- ② 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SKIP DASH
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- ⑨ LETTERS AND SYMBOLS - FULL SIZE
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**LEGEND**

- [Adj] DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
- [Adj-S] SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
- [IF] INLET FILTERS
- [Topsoil] TOPSOIL, SOD RESTORATION

**NOTES**

1. CLASS D PATCHES, 11" AS DIRECTED BY THE ENGINEER.
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3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

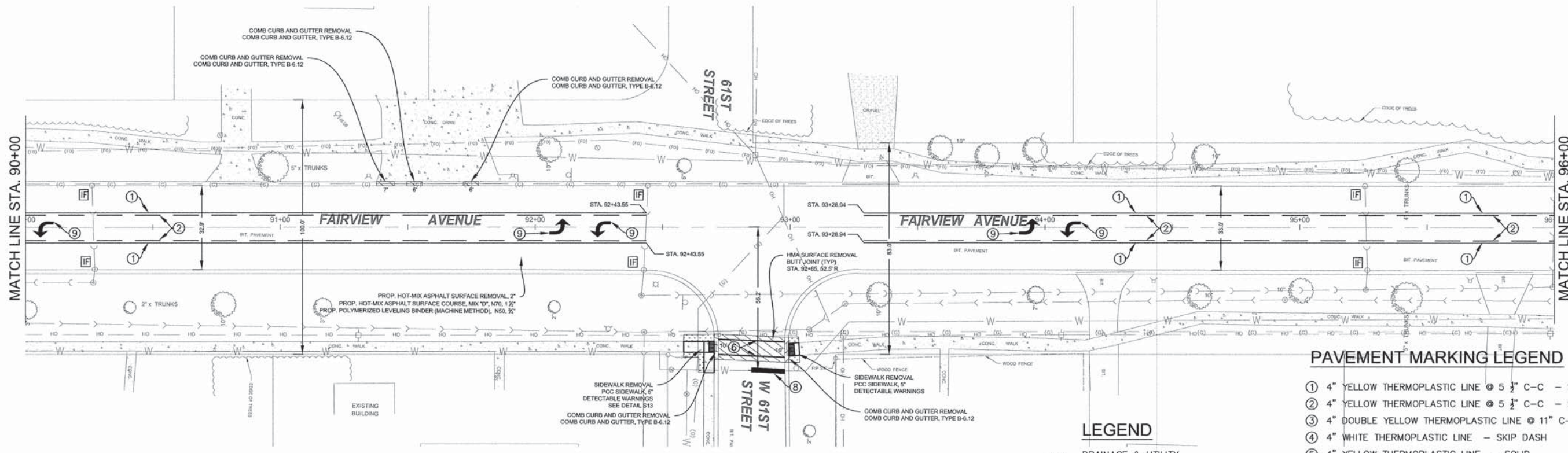
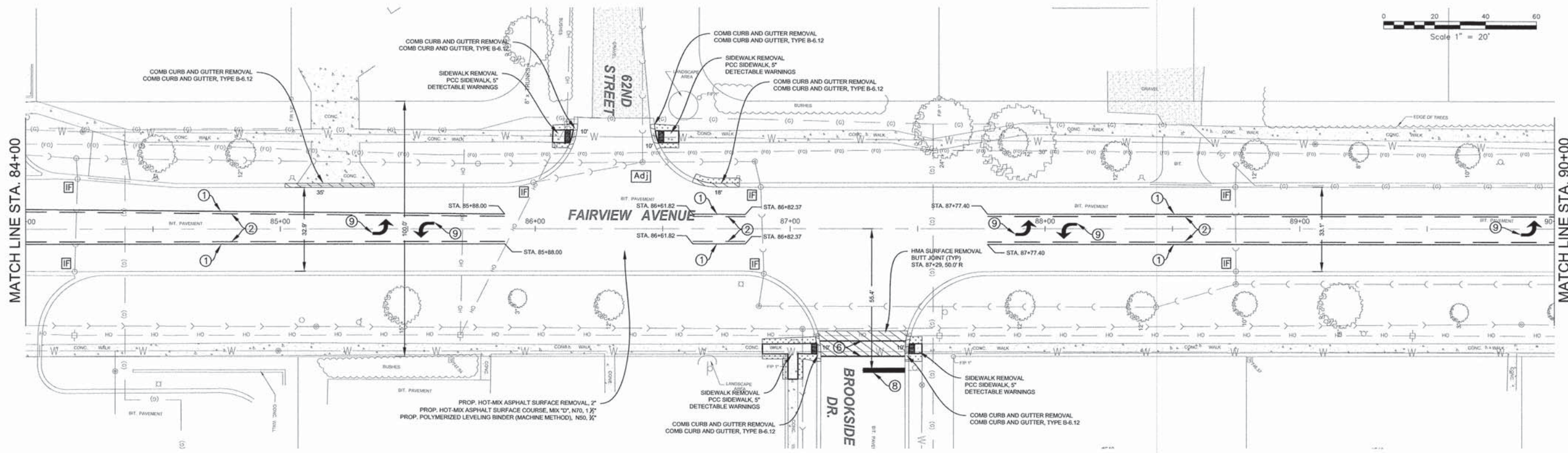
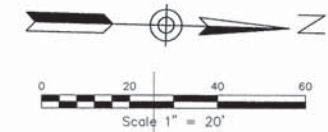
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
IMPROVEMENT PLAN

FILE NAME =	USER NAME - USER	DESIGNED - NRH	REVISED
FILE NAME =		DRAWN - NRH	REVISED
	PLOT SCALE -	CHECKED - JPT	REVISED
	PLOT DATE - 04/06/15	DATE - 04/06/15	REVISED

SCALE: 1"=20'	SHEET NO. 11 OF 26 SHEETS	STA. 72+00 TO STA. 84+00	F.A.S. NO. 2637	SECTION 14-00108-00-RS	COUNTY DU PAGE	TOTAL SHEETS 26	SHEET NO. 11
			CONTRACT NO. 61B37				
			FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		





- NOTES**
- CLASS D PATCHES, 11" AS DIRECTED BY THE ENGINEER.
  - ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
  - TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

- LEGEND**
- [Adj] DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
  - [Adj-S] SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
  - [IF] INLET FILTERS
  - [Pattern] TOPSOIL, SOD RESTORATION

- PAVEMENT MARKING LEGEND**
- ① 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SOLID
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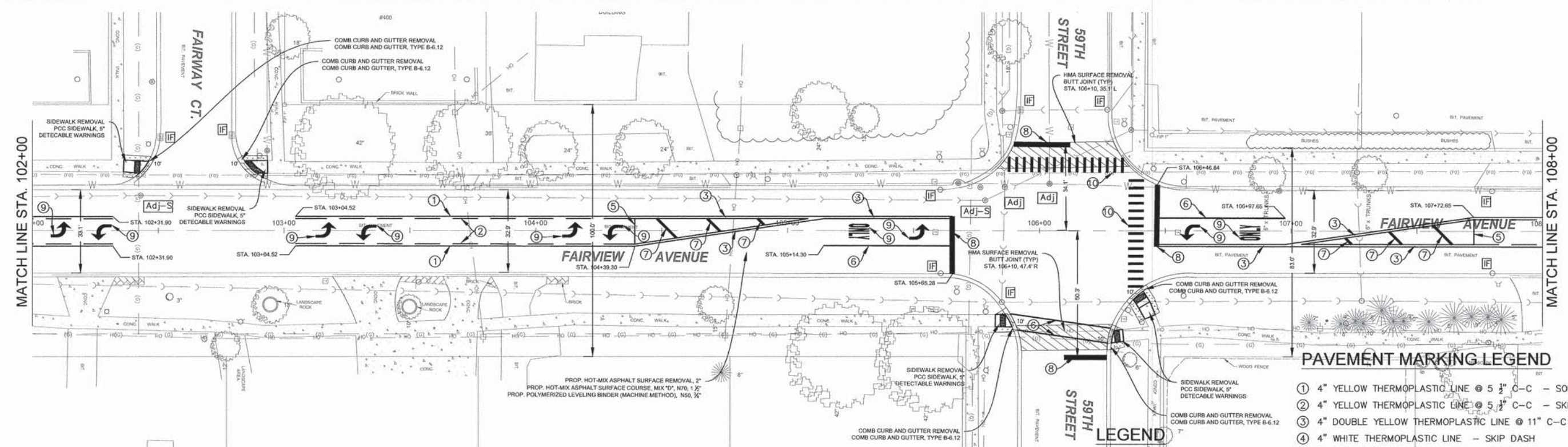
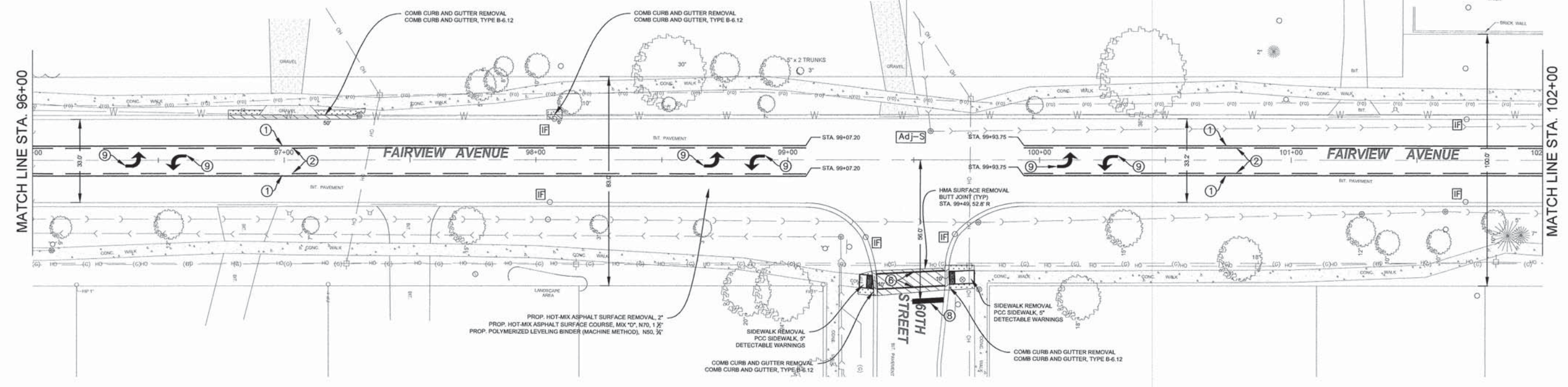
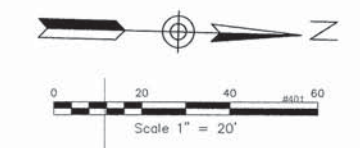
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	PLOT SCALE --	CHECKED -- JPT	REVISED
	PLOT DATE -- 04/06/15	DATE -- 04/06/15	REVISED

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FAIRVIEW AVENUE IMPROVEMENTS  
IMPROVEMENT PLAN

F.A.S. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	12
CONTRACT NO. 61B37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**PAVEMENT MARKING LEGEND**

①	4" YELLOW THERMOPLASTIC LINE @ 5 1/2' C-C - SOLID
②	4" YELLOW THERMOPLASTIC LINE @ 5 1/2' C-C - SKIP DASH
③	4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11' C-C - SOLID
④	4" WHITE THERMOPLASTIC LINE - SKIP DASH
⑤	4" YELLOW THERMOPLASTIC LINE - SOLID
⑥	6" WHITE THERMOPLASTIC LINE - SOLID
⑦	12" YELLOW THERMOPLASTIC LINE - SOLID
⑧	24" WHITE THERMOPLASTIC LINE - SOLID
⑨	LETTERS AND SYMBOLS - FULL SIZE
⑩	12" WHITE THERMOPLASTIC LINE - SOLID

**LEGEND**

Adj	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
Adj-S	SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
IF	INLET FILTERS
⊞	TOPSOIL, SOD RESTORATION

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	PLOT SCALE -	CHECKED - JPT	REVISED
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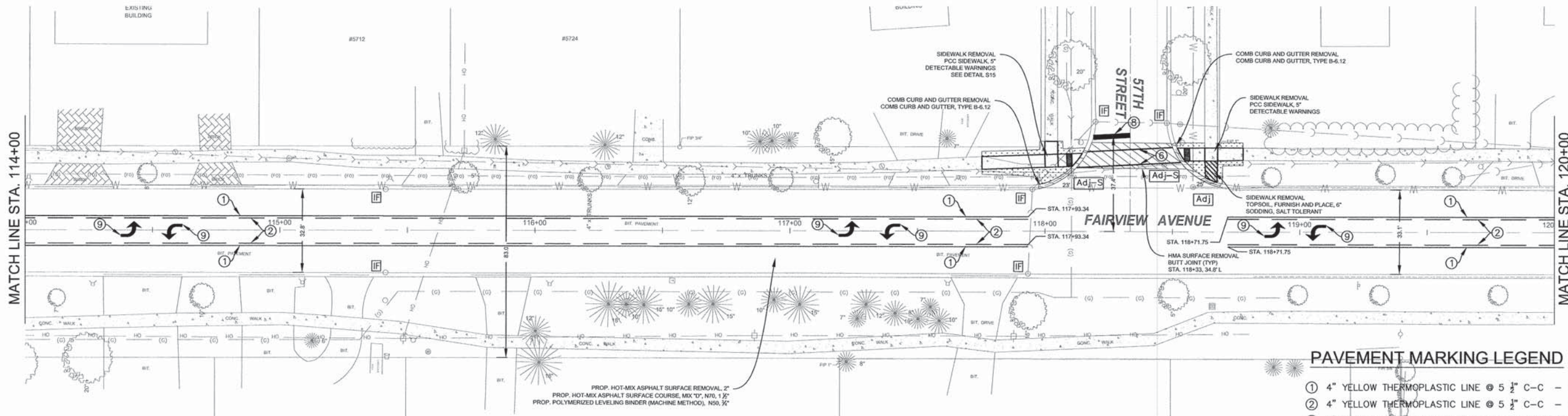
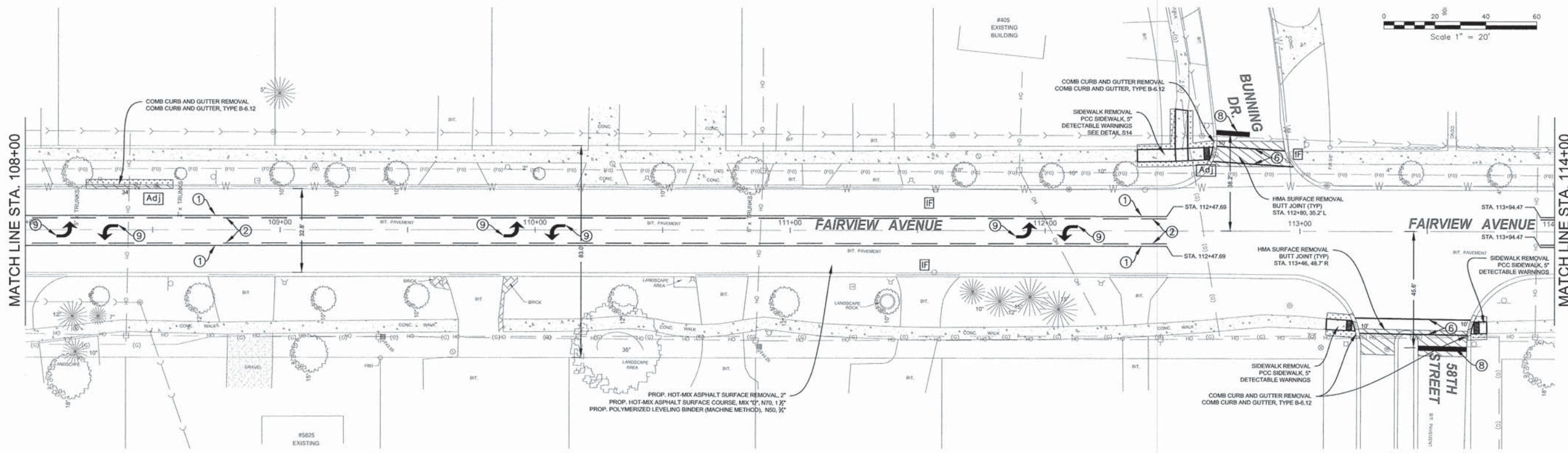
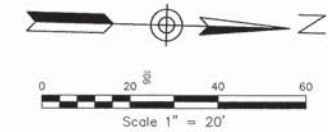
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
IMPROVEMENT PLAN

SCALE: 1"=20'	SHEET NO. 13 OF 26 SHEETS	STA. 96+00 TO STA. 108+00	F.A.D. DIST. NO. 1	ILLINOIS	FED. AID PROJECT
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F.A.D. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	13
CONTRACT NO. 61B37				





**PAVEMENT MARKING LEGEND**

- ① 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SOLID
- ② 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SKIP DASH
- ③ 4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11" C-C - SOLID
- ④ 4" WHITE THERMOPLASTIC LINE - SKIP DASH
- ⑤ 4" YELLOW THERMOPLASTIC LINE - SOLID
- ⑥ 6" WHITE THERMOPLASTIC LINE - SOLID
- ⑦ 12" YELLOW THERMOPLASTIC LINE - SOLID
- ⑧ 24" WHITE THERMOPLASTIC LINE - SOLID
- ⑨ LETTERS AND SYMBOLS - FULL SIZE
- ⑩ 12" WHITE THERMOPLASTIC LINE - SOLID

**NOTES**

1. CLASS D PATCHES, 11" AS DIRECTED BY THE ENGINEER.
2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

**LEGEND**

- [Adj] DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
- [Adj-S] SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
- [IF] INLET FILTERS
- [Topsoil] TOPSOIL, SOD RESTORATION

FILE NAME =	USER NAME -- USER	DESIGNED -- NRH	REVISED
FILE NAME =		DRAWN -- NRH	REVISED
	PLOT SCALE --	CHECKED -- JPT	REVISED
	PLOT DATE -- 04/04/15	DATE -- 04/06/15	REVISED

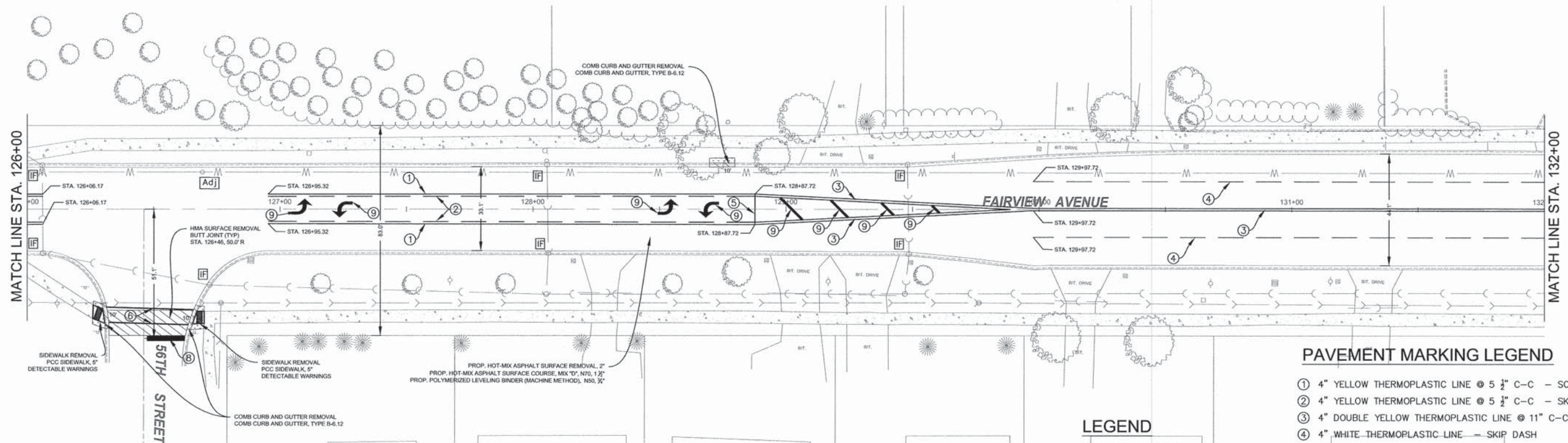
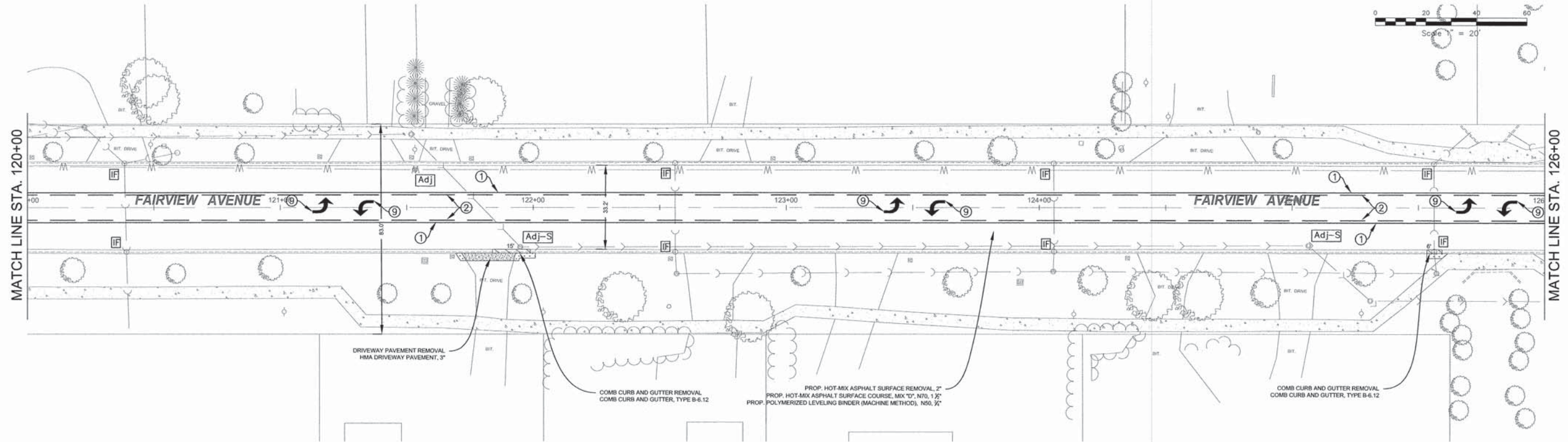
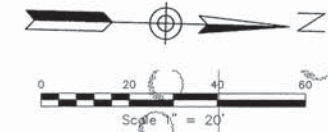
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
IMPROVEMENT PLAN

SCALE: 1"=20' SHEET NO. 14 OF 26 SHEETS STA. 108+00 TO STA. 120+00

F.A.A. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	14
CONTRACT NO. 61B37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**PAVEMENT MARKING LEGEND**

- ① 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SOLID
- ② 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SKIP DASH
- ③ 4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11" C-C - SOLID
- ④ 4" WHITE THERMOPLASTIC LINE - SKIP DASH
- ⑤ 4" YELLOW THERMOPLASTIC LINE - SOLID
- ⑥ 6" WHITE THERMOPLASTIC LINE - SOLID
- ⑦ 12" YELLOW THERMOPLASTIC LINE - SOLID
- ⑧ 24" WHITE THERMOPLASTIC LINE - SOLID
- ⑨ LETTERS AND SYMBOLS - FULL SIZE
- ⑩ 12" WHITE THERMOPLASTIC LINE - SOLID

**LEGEND**

- [Adj] DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
- [Adj-S] SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
- [IF] INLET FILTERS
- [Topsoil] TOPSOIL, SOD RESTORATION

- NOTES**
1. CLASS D PATCHES, 11" AS DIRECTED BY THE ENGINEER.
  2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
  3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

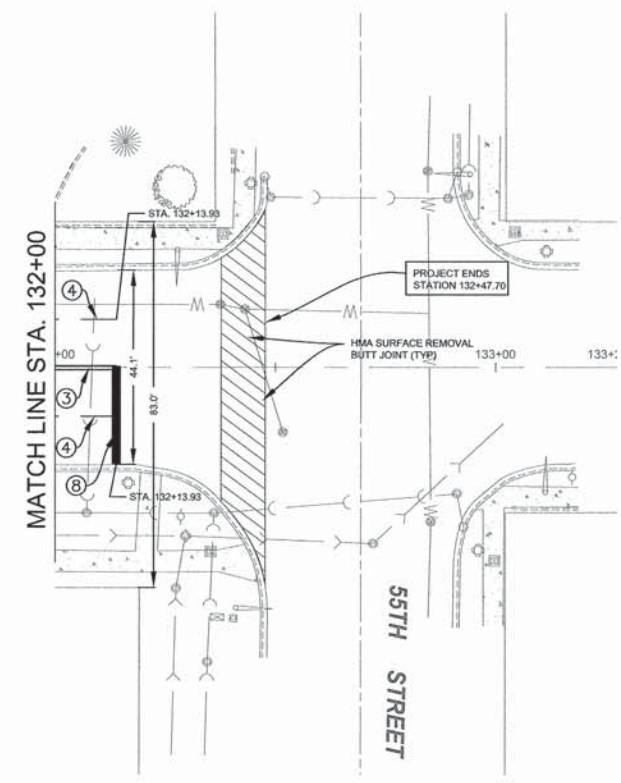
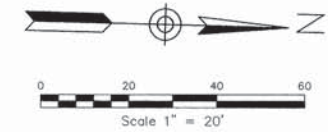
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
IMPROVEMENT PLAN

FILE NAME =	USER NAME -- USER	DESIGNED -- NRH	REVISED
FILE NAME =		DRAWN -- NRH	REVISED
	PLOT SCALE --	CHECKED -- JPT	REVISED
	PLOT DATE -- 04/06/15	DATE -- 04/06/15	REVISED

SCALE: 1"=20'	SHEET NO. 15 OF 26 SHEETS	STA. 120+00 TO STA. 132+00	F.A.S. DIST. 2637	SECTION 14-00108-00-RS	COUNTY DU PAGE	TOWN SHEETS 26	SHEET NO. 15
CONTRACT NO. 61B37							
FED. ROAD DIST. NO. 1							ILLINOIS FED. AID PROJECT





**NOTES**

1. CLASS D PATCHES, 11" AS DIRECTED BY THE ENGINEER.
2. ALL EXISTING DRAINAGE STRUCTURES WHICH ARE TO BE ADJUSTED SHALL BE CLEANED IN ACCORDANCE WITH ARTICLE 602.15 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ADJUSTMENT ITEM.
3. TOPSOIL, SODDING LOCATION/PLACEMENT AS DIRECTED BY THE ENGINEER.

**LEGEND**

- Adj DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
- Adj-S SANITARY STRUCTURE TO BE ADJUSTED SEE DETAIL ON SHEET 19
- IF INLET FILTERS
- TOPSOIL, SOD RESTORATION

**PAVEMENT MARKING LEGEND**

- ① 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SOLID
- ② 4" YELLOW THERMOPLASTIC LINE @ 5 1/2" C-C - SKIP DASH
- ③ 4" DOUBLE YELLOW THERMOPLASTIC LINE @ 11" C-C - SOLID
- ④ 4" WHITE THERMOPLASTIC LINE - SKIP DASH
- ⑤ 4" YELLOW THERMOPLASTIC LINE - SOLID
- ⑥ 6" WHITE THERMOPLASTIC LINE - SOLID
- ⑦ 12" YELLOW THERMOPLASTIC LINE - SOLID
- ⑧ 24" WHITE THERMOPLASTIC LINE - SOLID
- ⑨ LETTERS AND SYMBOLS - FULL SIZE
- ⑩ 12" WHITE THERMOPLASTIC LINE - SOLID

FILE NAME =	USER NAME -- USER	DESIGNED -- NRH	REVISED
FILE NAME =		DRAWN -- NRH	REVISED
	PLOT SCALE --	CHECKED -- JPT	REVISED
	PLOT DATE -- 04/06/15	DATE -- 04/06/15	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
IMPROVEMENT PLAN

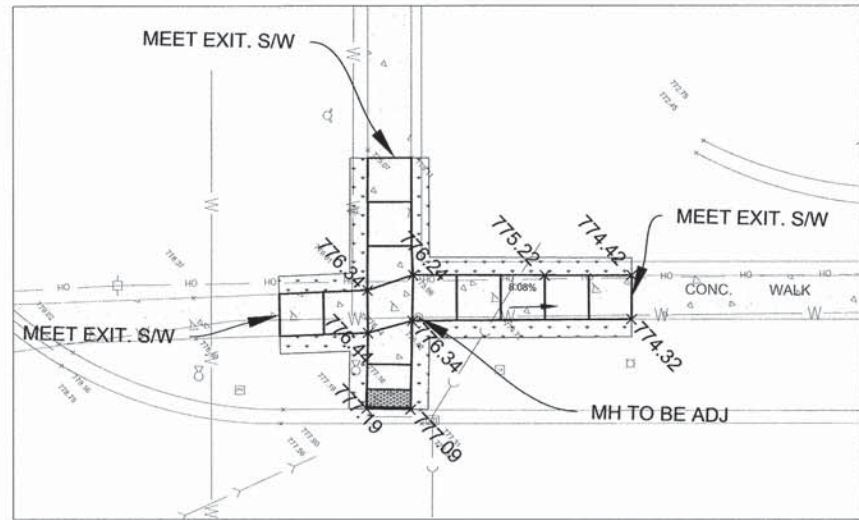
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SHEET NO. 16 OF 26 SHEETS

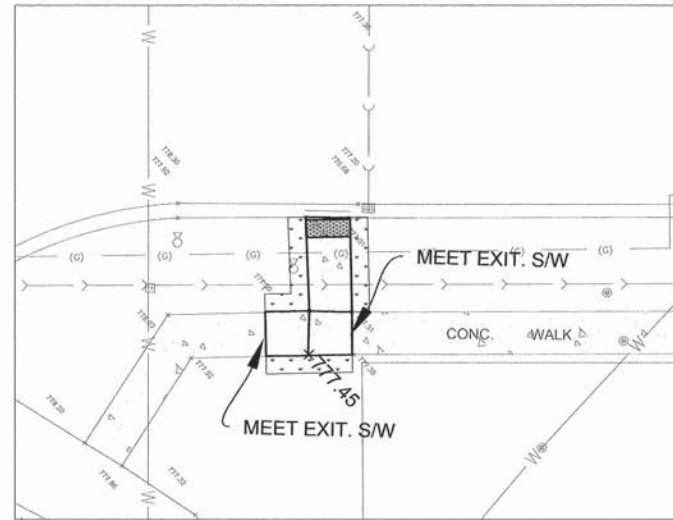
STA. 132+00 TO STA. 133+27.74

F. A. D. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	16
CONTRACT NO. 61B37				
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

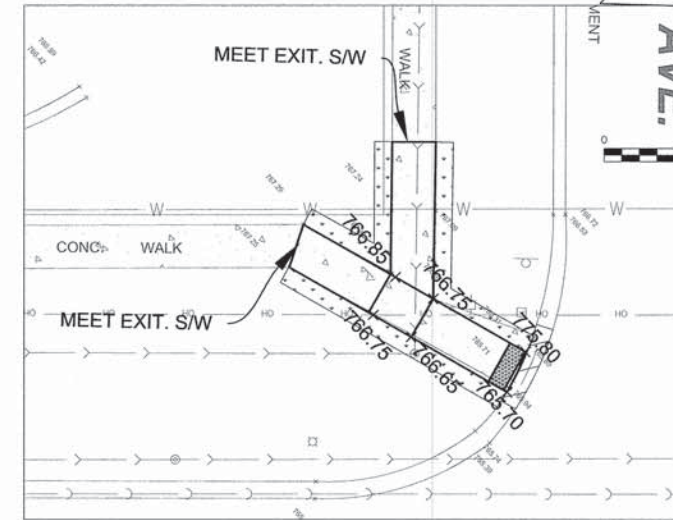




**SIDEWALK DETAIL S1**



**SIDEWALK DETAIL S2**



**SIDEWALK DETAIL S3**

**BENCHMARK**

SOURCE: NGS MONUMENT  
 STATION DESIGNATION: MF1251  
 ESTABLISHED BY: MARCHESI AND SONS  
 DATE: 1983

ELEVATION: 745.60 (PUBLISHED)  
 DATUM: NAVD 88

DESCRIPTION: IN DOWNERS GROVE, AT THE NORTHEAST CORNER OF DOWNERS GROVE SOUTH HIGH SCHOOL, 3.8 MILES WEST ALONG 63RD STREET FROM THE JUNCTION OF STATE HIGHWAY 83 AND 63RD STREET, SOUTHWEST OF THE JUNCTION OF 63RD STREET AND DUNHAM AVENUE, 95.0 FEET SOUTH OF THE SOUTH EDGE OF THE SIDEWALK ALONG 63RD STREET, 62.1 FEET SOUTHWEST OF THE SOUTHWEST CORNER OF THE MOST EASTERLY BRICK PILLAR SUPPORTING AN ENTRANCE SIGN FOR DOWNERS GROVE COMMUNITY HIGH SCHOOL, 9.7 FEET WEST OF THE EDGE OF A SIDEWALK ALONG DUNHAM AVENUE, A STAINLESS STEEL ROD IN SLEEVE ENCASED IN A CAST ALUMINUM CYLINDER, COVER BEING FLUSH WITH GROUND.

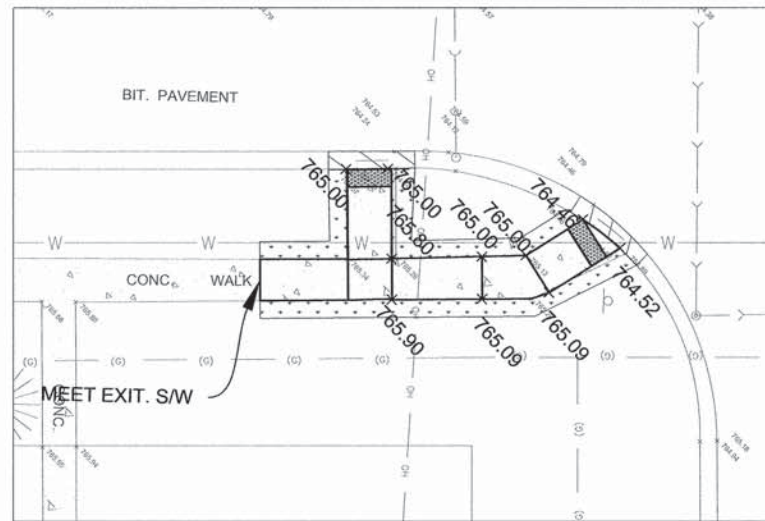
**SITE**

STATION DESIGNATION: SBM#1  
 ESTABLISHED BY: V3 COMPANIES  
 DATE: 3-21-14  
 ELEVATION: 771.34 (MEASURED)  
 DATUM: NAVD 88

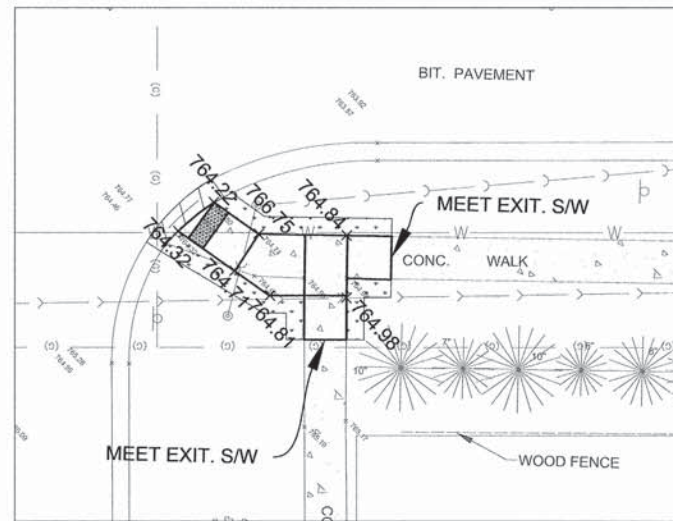
DESCRIPTION: SE TAG BOLT ON FIRE HYDRANT ON WEST SIDE OF FAIRVIEW AVENUE NORTH OF 75TH STREET, NORTH OF 1ST ENTRANCE TO WEST.

STATION DESIGNATION: SBM #2  
 ESTABLISHED BY: V3 COMPANIES  
 DATE: 4-18-14  
 ELEVATION: 727.08 (MEASURED)  
 DATUM: NAVD 88

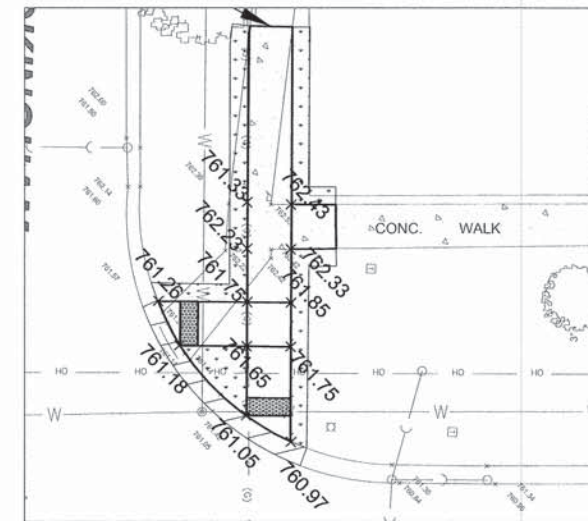
DESCRIPTION: NW FLANGE BOLT ON FIRE HYDRANT AT NORTHWEST CORNER OF 57TH STREET AND FAIRVIEW AVENUE.



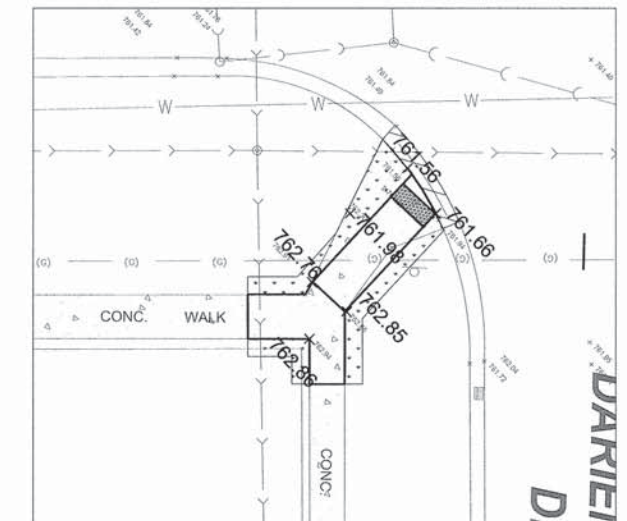
**SIDEWALK DETAIL S4**



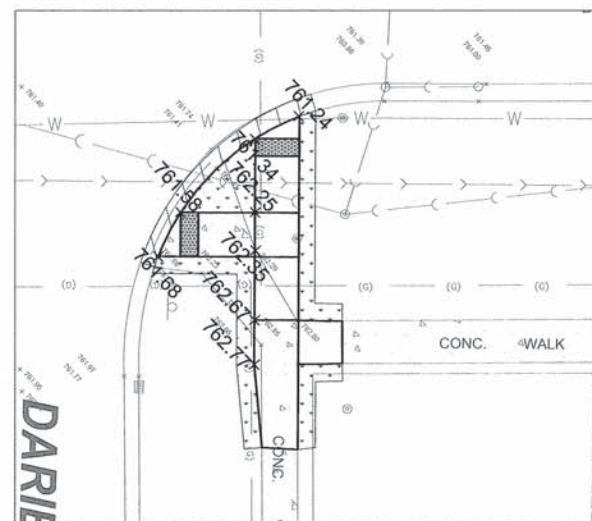
**SIDEWALK DETAIL S5**



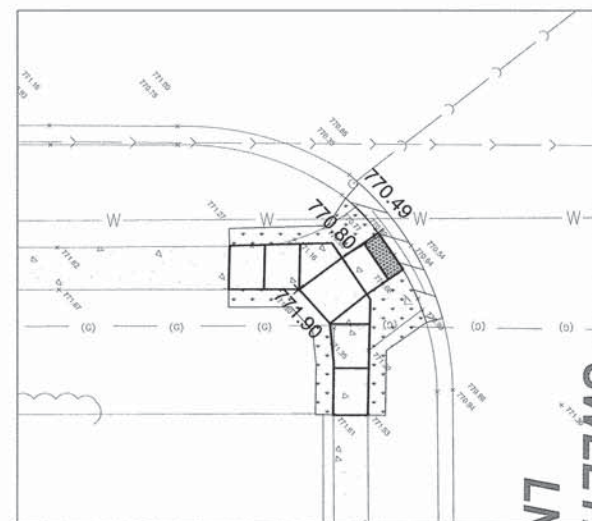
**SIDEWALK DETAIL S6**



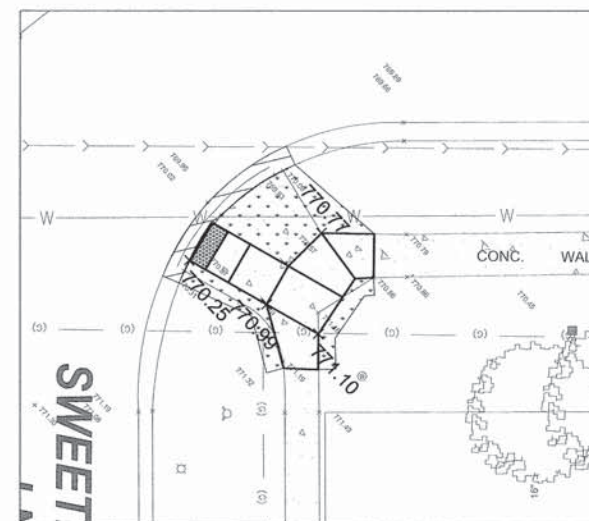
**SIDEWALK DETAIL S7**



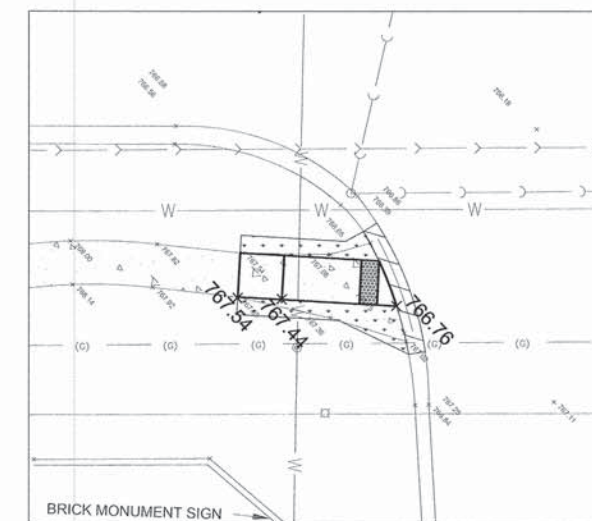
**SIDEWALK DETAIL S8**



**SIDEWALK DETAIL S9**



**SIDEWALK DETAIL S10**



**SIDEWALK DETAIL S11**

FILE NAME =	USER NAME - USER	DESIGNED - NRH	REVISED
FILE NAME =		DRAWN - NRH	REVISED
	PLOT SCALE -	CHECKED - JPT	REVISED
	PLOT DATE - 04/06/15	DATE - 04/06/15	REVISED

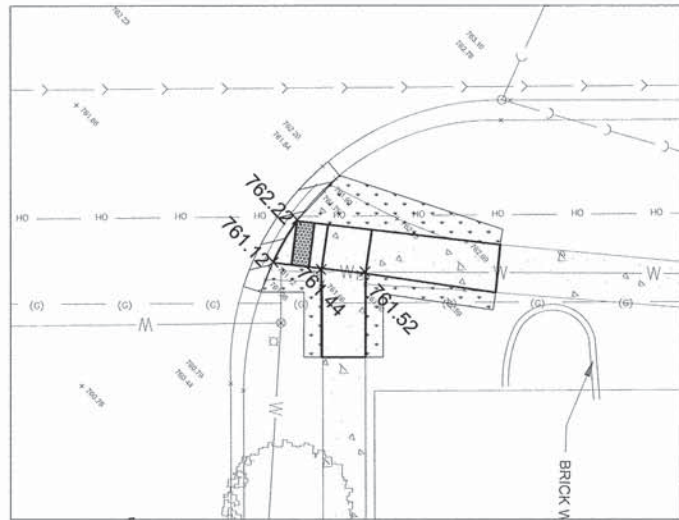
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
 SIDEWALK DETAILS

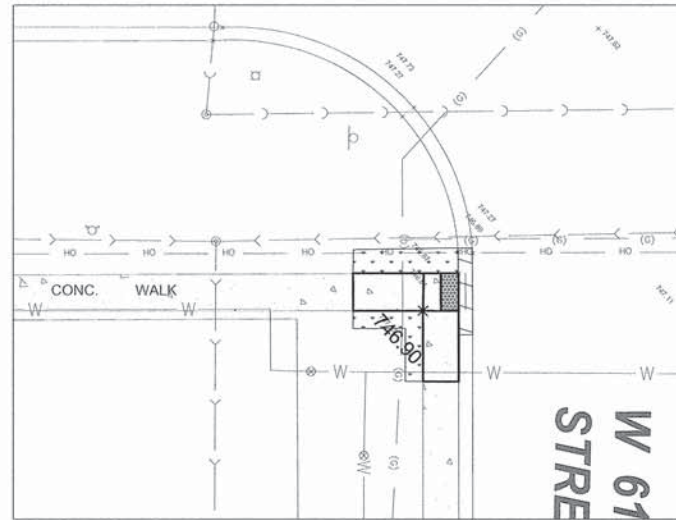
SCALE: 1"=20' SHEET NO. 17 OF 26 SHEETS STA. TO STA.

F.A.A. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	17
CONTRACT NO. 61B37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

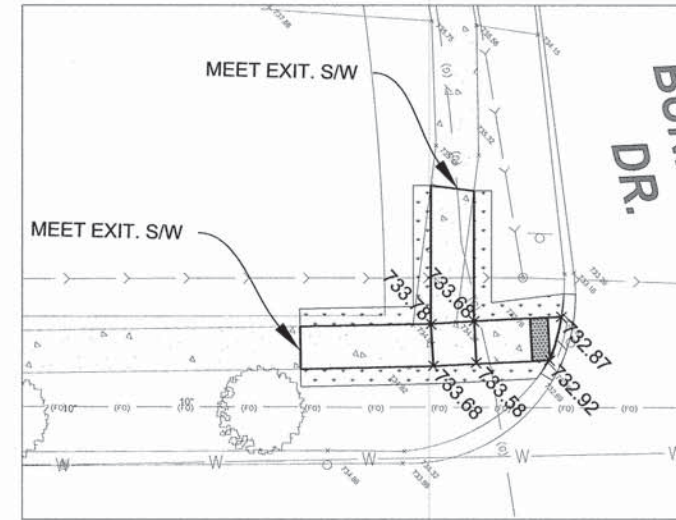




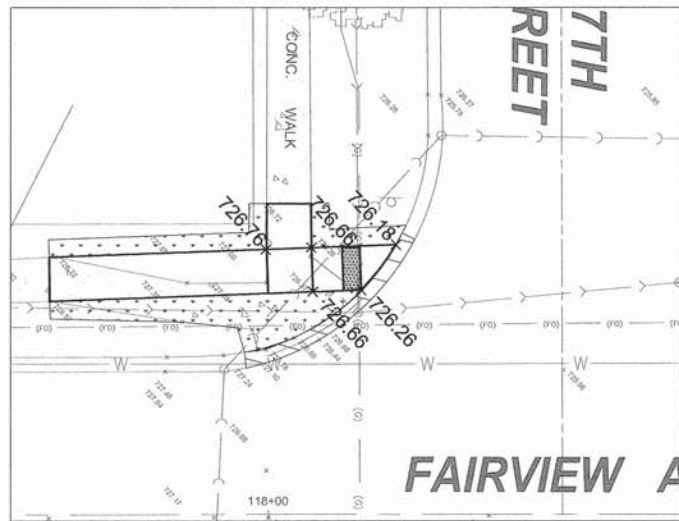
**SIDEWALK DETAIL S12**



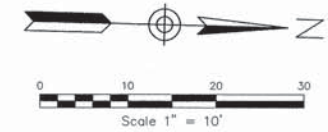
**SIDEWALK DETAIL S13**



**SIDEWALK DETAIL S14**



**SIDEWALK DETAIL S15**



**BENCHMARK**

**SOURCE:** NGS MONUMENT  
**STATION DESIGNATION:** MF1251  
**ESTABLISHED BY:** MARCHESI AND SONS  
**DATE:** 1993  
**ELEVATION:** 745.60 (PUBLISHED)  
**DATUM:** NAVD 88  
**DESCRIPTION:** IN DOWNERS GROVE, AT THE NORTHEAST CORNER OF DOWNERS GROVE SOUTH HIGH SCHOOL, 3.8 MILES WEST ALONG 63RD STREET FROM THE JUNCTION OF STATE HIGHWAY 83 AND 63RD STREET, SOUTHWEST OF THE JUNCTION OF 63RD STREET AND DUNHAM AVENUE, 95.0 FEET SOUTH OF THE SOUTH EDGE OF THE SIDEWALK ALONG 63RD STREET, 62.1 FEET SOUTHEAST OF THE SOUTHEAST CORNER OF THE MOST EASTERLY BRICK PILLAR SUPPORTING AN ENTRANCE SIGN FOR DOWNERS GROVE COMMUNITY HIGH SCHOOL, 9.7 FEET WEST OF THE EDGE OF A SIDEWALK ALONG DUNHAM AVENUE, A STAINLESS STEEL ROD IN SLEEVE ENCASED IN A CAST ALUMINUM CYLINDER, COVER BEING FLUSH WITH GROUND.

**SITE:**  
**STATION DESIGNATION:** SBM#1  
**ESTABLISHED BY:** V3 COMPANIES  
**DATE:** 3-21-14  
**ELEVATION:** 771.34 (MEASURED)  
**DATUM:** NAVD 88  
**DESCRIPTION:** SE TAG BOLT ON FIRE HYDRANT ON WEST SIDE OF FAIRVIEW AVENUE NORTH OF 75TH STREET, NORTH OF 1ST ENTRANCE TO WEST.

**STATION DESIGNATION:** SBM #2  
**ESTABLISHED BY:** V3 COMPANIES  
**DATE:** 4-18-14  
**ELEVATION:** 727.08 (MEASURED)  
**DATUM:** NAVD 88  
**DESCRIPTION:** NW FLANGE BOLT ON FIRE HYDRANT AT NORTHWEST CORNER OF 57TH STREET AND FAIRVIEW AVENUE.

FILE NAME =	USER NAME -- USER	DESIGNED -- NRH	REVISED
FILE NAME =		DRAWN -- NRH	REVISED
	PLOT SCALE --	CHECKED -- JPT	REVISED
	PLOT DATE -- 04/06/15	DATE -- 04/06/15	REVISED

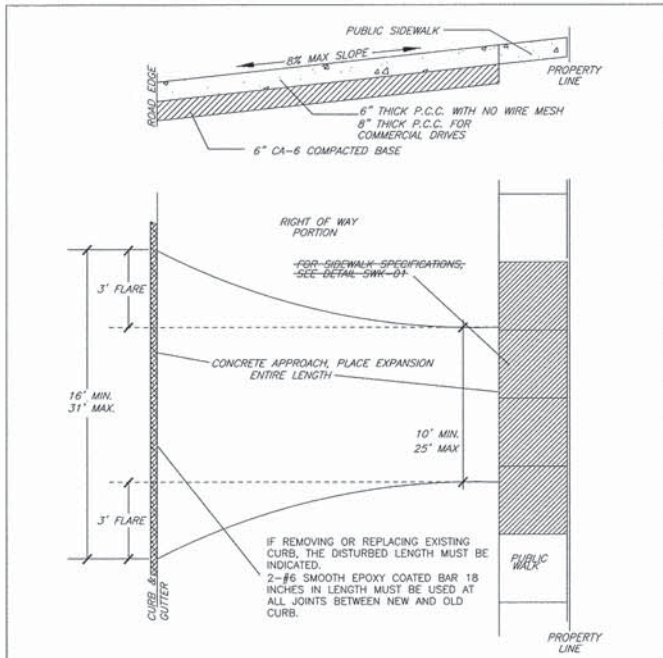
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

FAIRVIEW AVENUE IMPROVEMENTS  
 SIDEWALK DETAILS

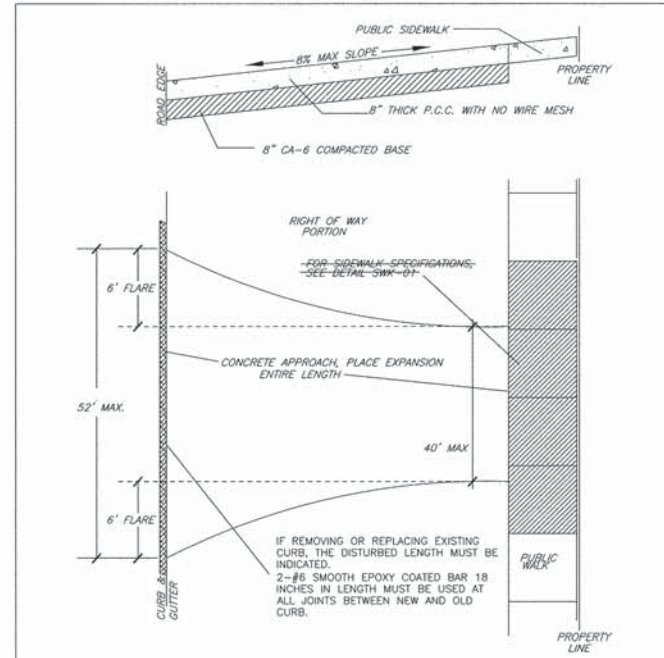
SCALE: 1"=10' SHEET NO. 18 OF 26 SHEETS STA. TO STA.

F. A. I. D. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61B37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

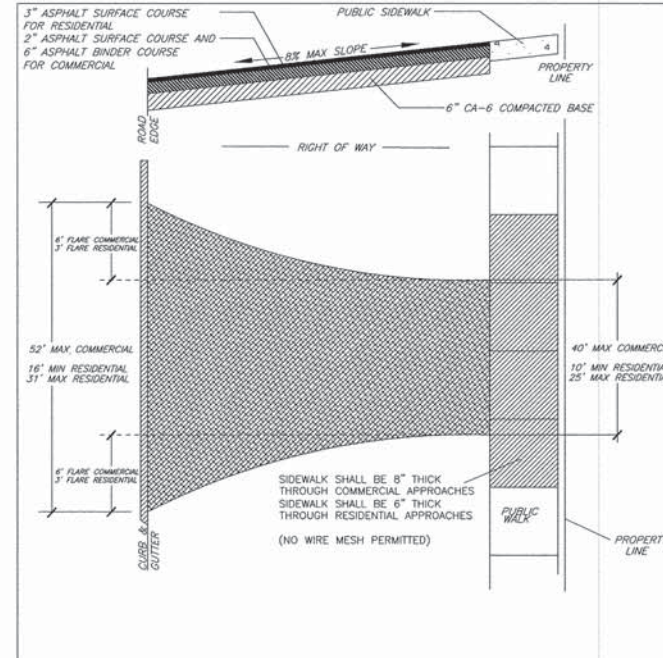




N.T.S.	DATE	REVISIONS	DRAWN BY	APPROV BY	STANDARD DETAIL
	02/20/07		J.M.L.	M.D.M.	CONCRETE APPROACH WITH CURB
	03/25/11		S.A.V.	A.J.S.	
DRAWING NO. PVT-09 I:\LIBRARY\DETAILS\PAVEMENT\PVT-09					



N.T.S.	DATE	REVISIONS	DRAWN BY	APPROV BY	STANDARD DETAIL
	02/20/07		J.M.L.	M.D.M.	COMMERCIAL CONCRETE APPROACH WITH CURB
	03/25/11		S.A.V.	A.J.S.	
DRAWING NO. PVT-09A I:\LIBRARY\DETAILS\PAVEMENT\PVT-09A					



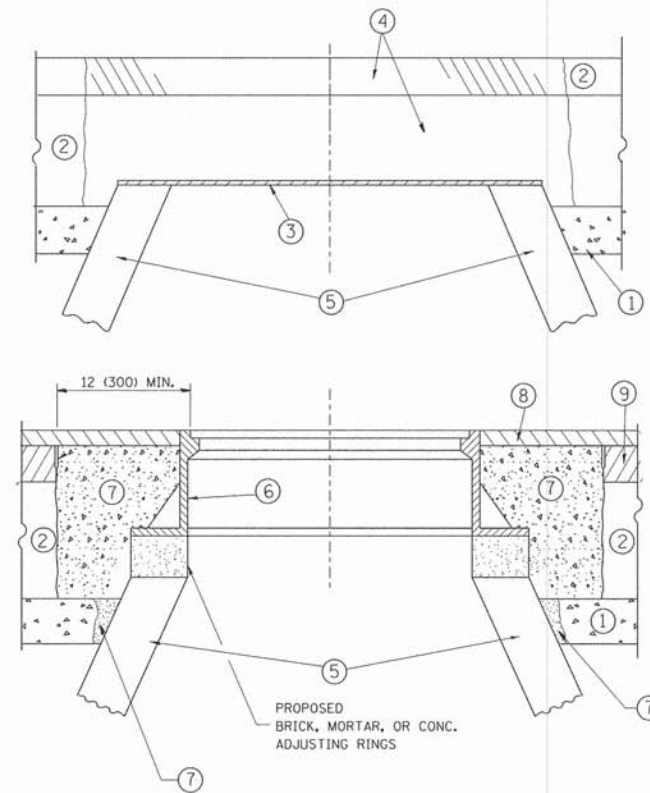
N.T.S.	DATE	REVISIONS	DRAWN BY	APPROV BY	STANDARD DETAIL
	02/20/07		J.M.L.	M.D.M.	ASPHALT APPROACH WITH CURB
	03/25/11		S.A.V.	A.J.S.	
	06/21/14		S.A.V.	A.J.S.	
	11/07/14		S.A.V.	A.J.S.	
DRAWING NO. PVT-02 I:\LIBRARY\DETAILS\PAVEMENT\PVT-02					

**SPECIFICATIONS FOR SANITARY SEWER MANHOLE ADJUSTMENT**

THIS METHOD SHALL BE USED FOR ALL MANHOLES WHICH WILL HAVE LESS THAN SEVEN AND ONE HALF (7 1/2) INCHES OF ADJUSTMENT BETWEEN THE TOP OF THE CONE AND THE BOTTOM OF THE FRAME WHEN SET AT FINISHED GRADE. TO RAISE THE FRAME, ADJUSTMENT SHALL BE MADE USING PRECAST REINFORCED CONCRETE RINGS, CONCRETE BLOCKS OR BRICKS SHALL NOT BE USED. THE SPACES BETWEEN THE CONE, RINGS, AND FRAME SHALL BE COMPLETELY SEALED WITH PREFORMED BITUMINOUS MASTIC (EASY STIK OR APPROVED EQUAL). MORTAR SHALL NOT BE USED. TO LOWER THE FRAME, EXISTING ADJUSTMENTS SHALL BE REMOVED AND THE SPACE BETWEEN THE FRAME AND THE CONE SHALL BE COMPLETELY SEALED WITH PREFORMED BITUMINOUS MASTIC GASKET. IF THE MANHOLE WOULD HAVE SEVEN AND ONE HALF (7 1/2) INCHES OR MORE OF ADJUSTMENT WHEN SET AT THE FINISHED GRADE OR IF THE FRAME MUST BE RECONSTRUCTED, THE AMOUNT OF THE EXISTING ADJUSTMENT, THE MANHOLE SHALL BE RECONSTRUCTED. REFER TO DOWNERS GROVE SANITARY DISTRICT SPECIFICATIONS FOR MANHOLE RECONSTRUCTION. THE DOWNERS GROVE SANITARY DISTRICT SHALL BE NOTIFIED OF MANHOLES TO BE ADJUSTED PRIOR TO BEGINNING CONSTRUCTION. ONCE COMPLETED, NO SUCH ADJUSTMENT SHALL BE BACKFILLED WITHOUT INSPECTION BY THE DOWNERS GROVE SANITARY DISTRICT.

**DOWNERS GROVE SANITARY DISTRICT  
DETAIL FOR SANITARY MANHOLE ADJUSTMENT**





**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
et\pav_work\pavidot\bauerdl\d0188315\bd00.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1/648,5000' / m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

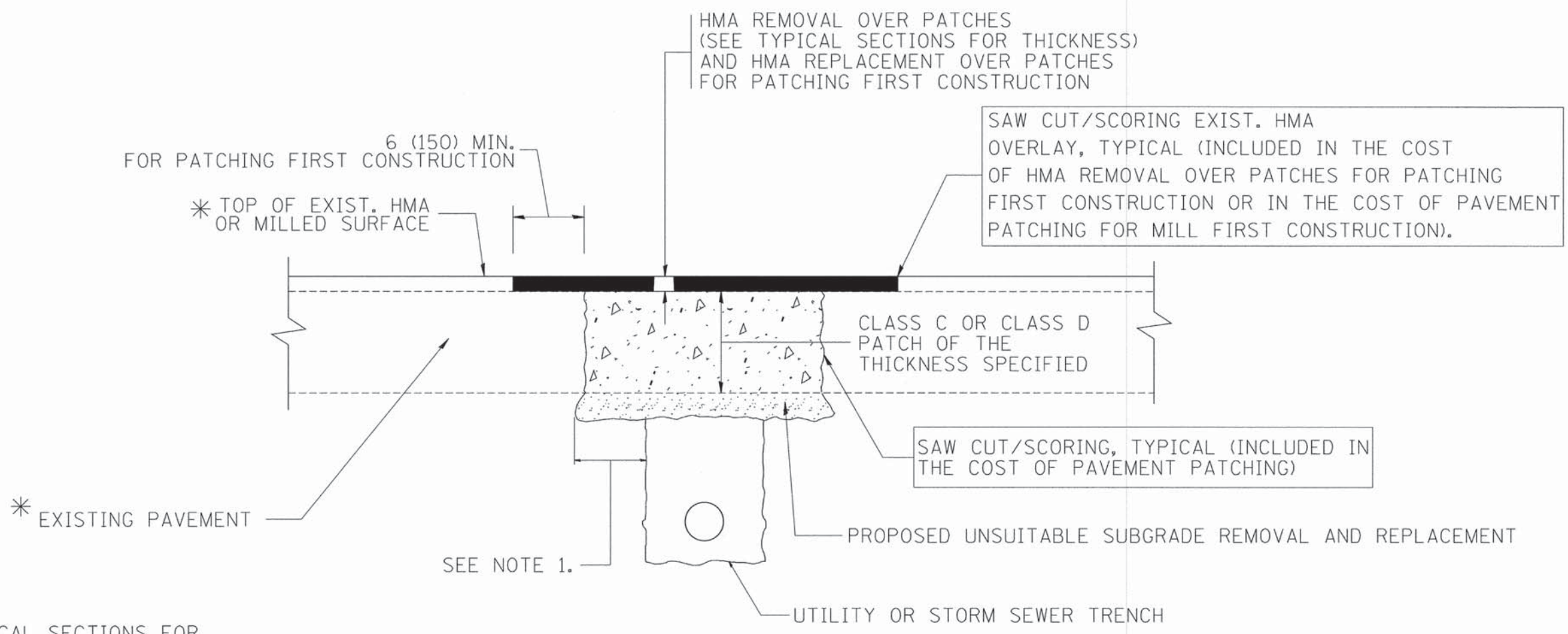
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	20
BD600-03 (BD-8)			CONTRACT NO. 61B37	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

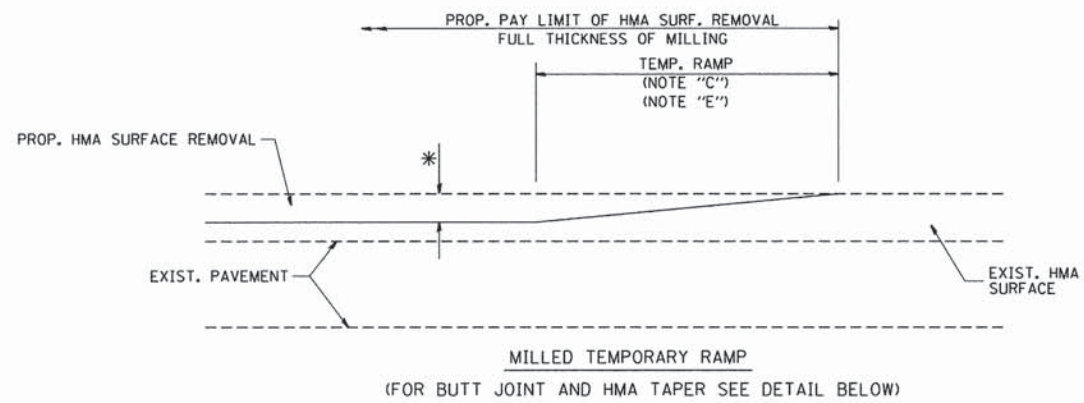
**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

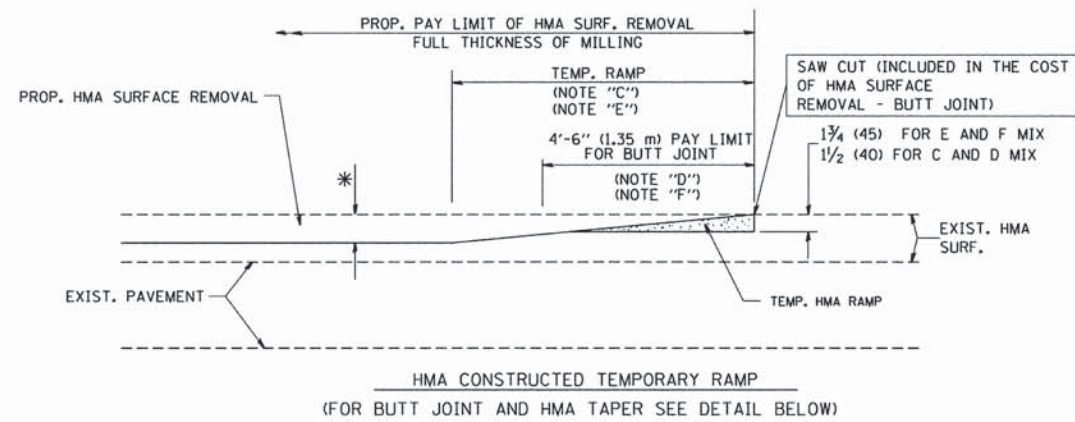
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bouerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.U. RTE. 2637	SECTION 14-00108-00-RS	COUNTY DU PAGE	TOTAL SHEETS 26	SHEET NO. 21
	PLOT SCALE = 58,000 ' / IN.	CHECKED -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	<b>BD400-04 (BD-22)</b>		CONTRACT NO.	61837	
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - R. BORO 09-04-07		FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT							
			REVISED - K. ENG 10-27-08									



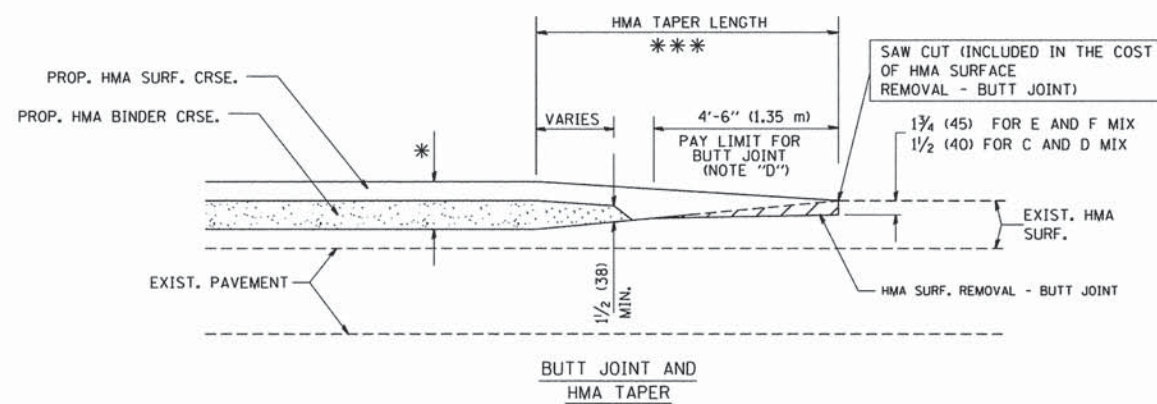


**OPTION 1**

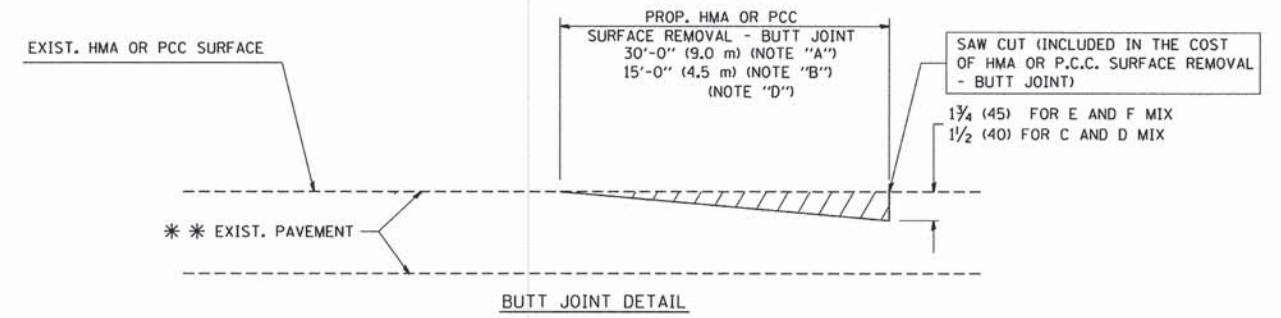


**OPTION 2**

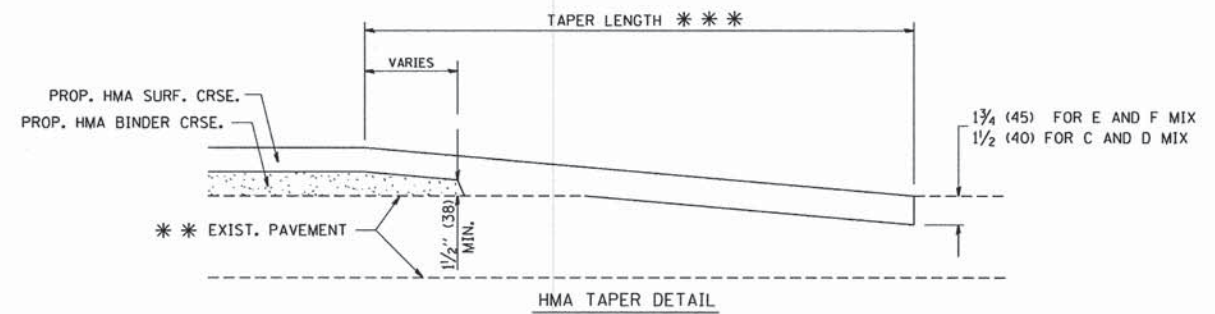
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL - BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

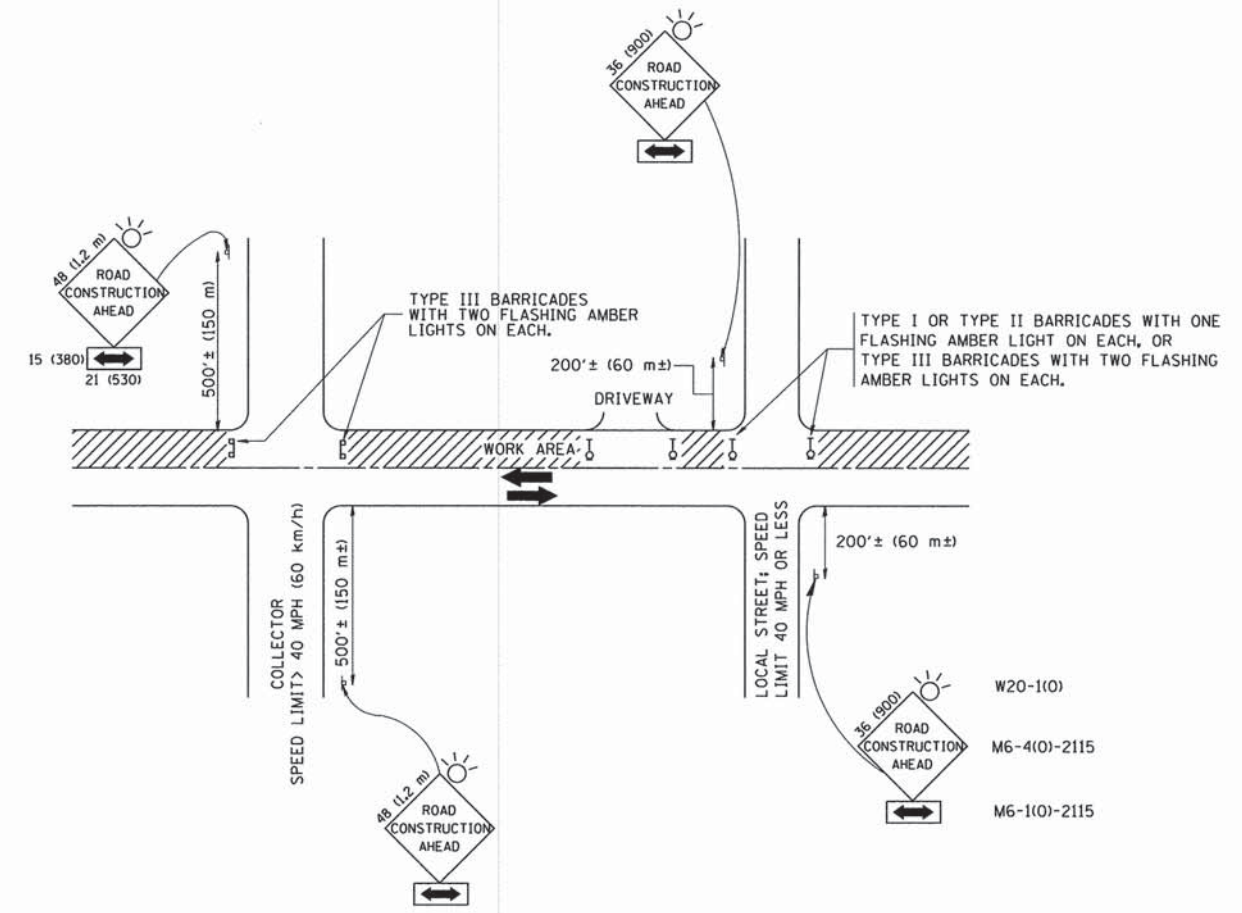
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		DRAWN -	REVISED - A. ABBAS 03-21-97
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	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A.I. RTE. 2637	SECTION 14-00108-00-RS	COUNTY DU PAGE	TOTAL SHEETS 26	SHEET NO. 22
			BD400-05 BD32		CONTRACT NO. 61B37		
			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50,000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

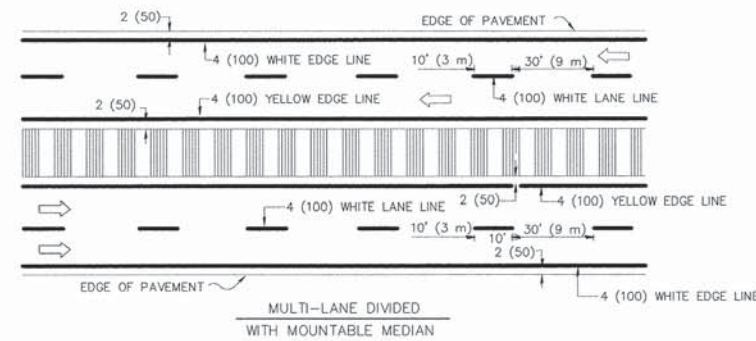
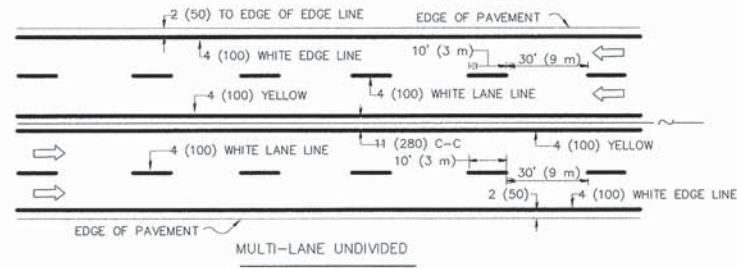
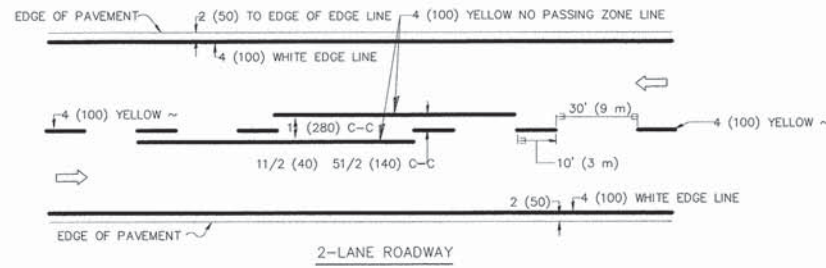
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

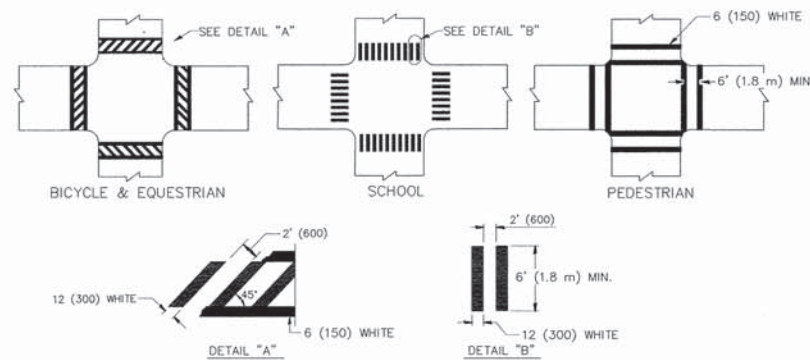
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 61837	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

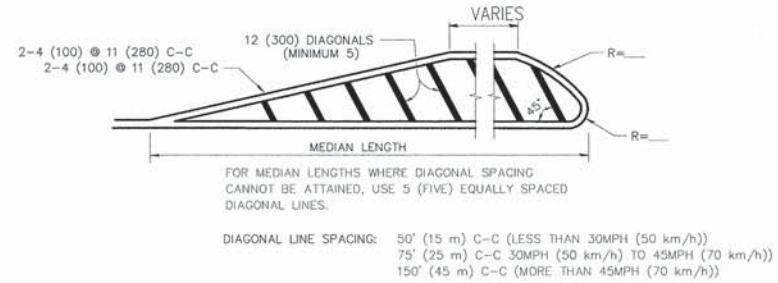
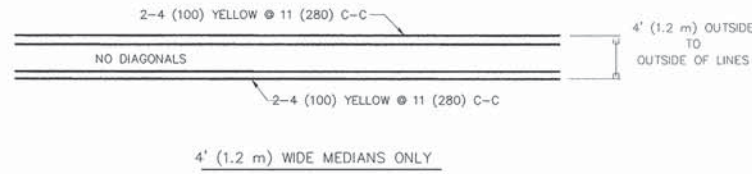




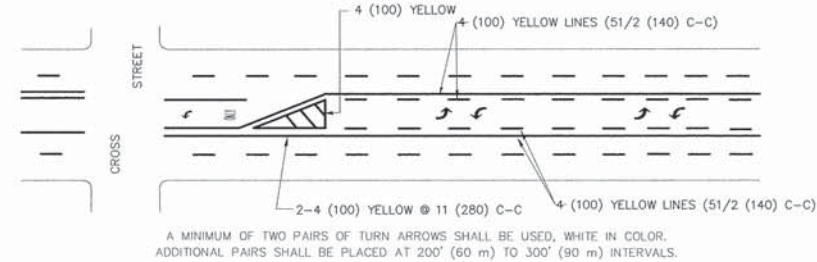
TYPICAL LANE AND EDGE LINE MARKING



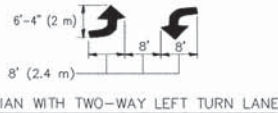
TYPICAL CROSSWALK MARKING



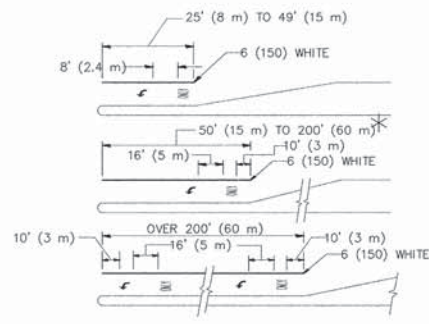
MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING



TYPICAL PAINTED MEDIAN MARKING

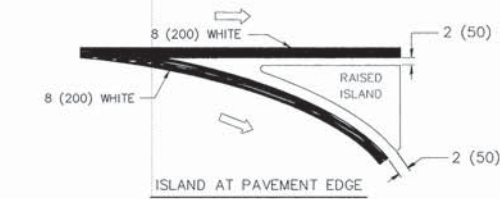
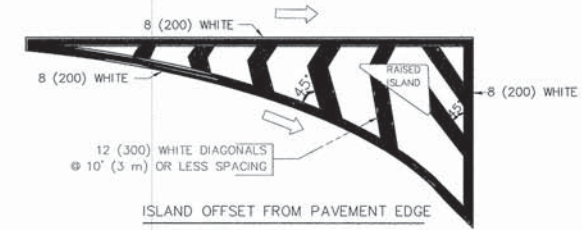


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	51/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125)	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 51/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT; OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R" = 3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X" = 54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

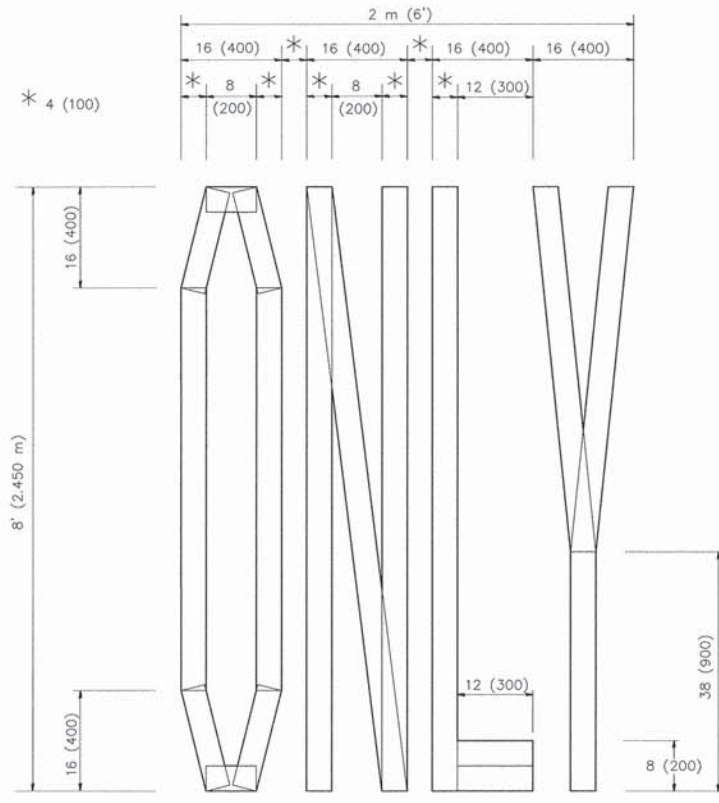
All dimensions are in inches (millimeters) unless otherwise shown.

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\$FILEL\$		DRAWN --	REVISED --C, JUCIUS 09-09-09
	PLOT SCALE = \$SCALE\$	CHECKED --	REVISED --
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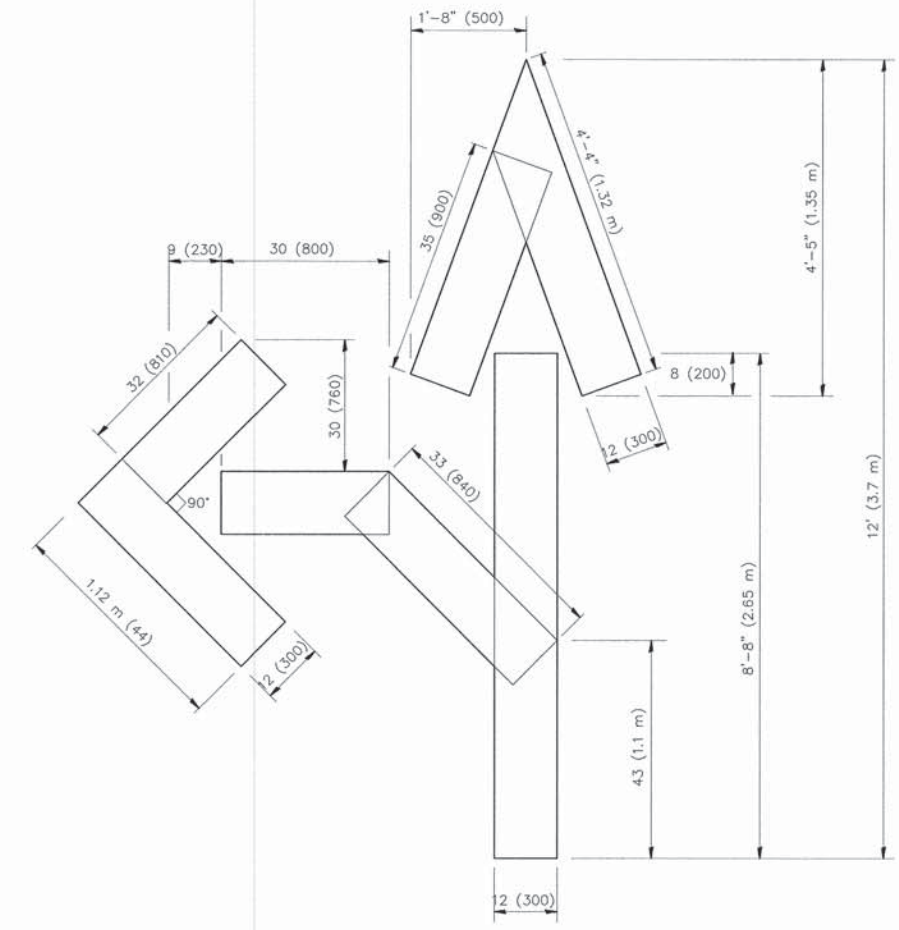
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		2637	14-00108-00-RS	DU PAGE	26	24
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
		CONTRACT NO. 61B37				

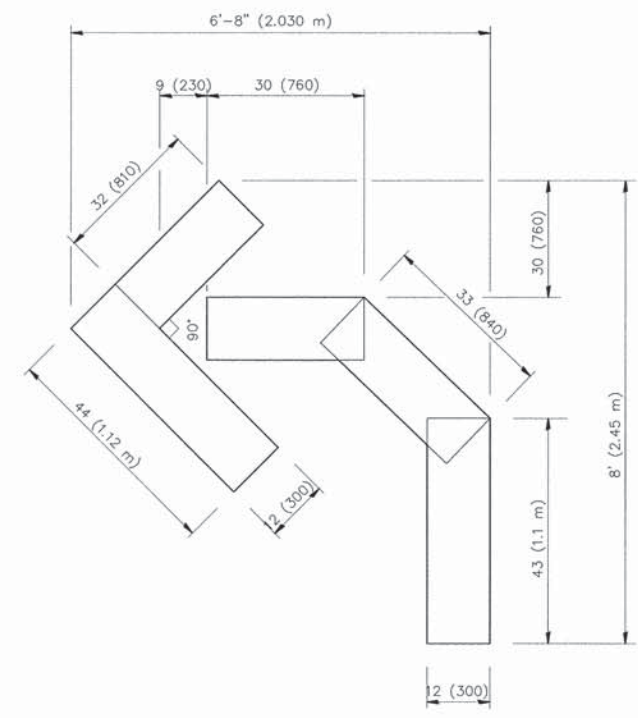




QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = \$FILES\$	USER NAME = \$USER\$	DESIGNED --	REVISED --T. RAMMACHER 06-05-96
		DRAWN --	REVISED --T. RAMMACHER 11-04-97
	PLOT SCALE = \$SCALE\$	CHECKED --	REVISED --T. RAMMACHER 03-02-98
	PLOT DATE = \$DATE\$	DATE -- 09-18-94	REVISED --E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

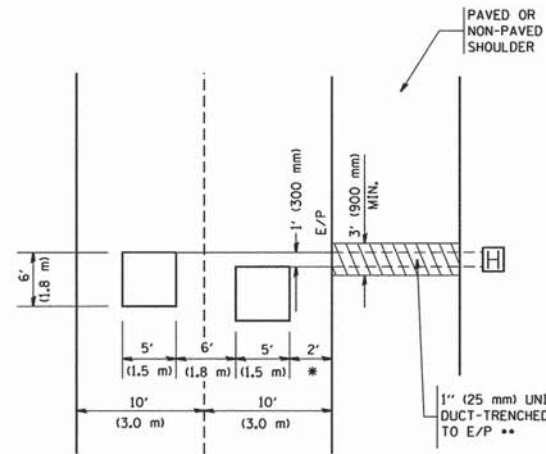
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2637	14-00108-00-RS	DU PAGE	26	25
TC-16			CONTRACT NO. 61B37	
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT  
NOTE WHICH SHOULD EQUAL  
3' (900 mm) X WIDTH OF  
PAVED SHOULDER.

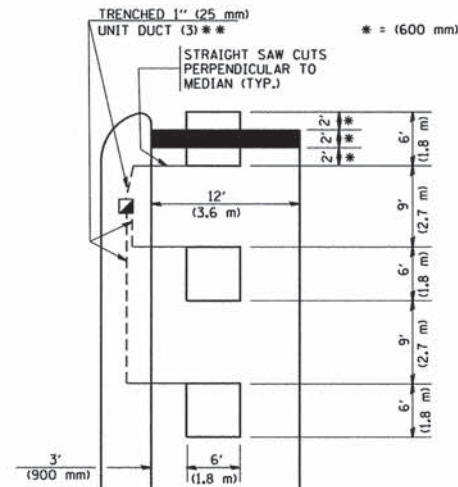


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS  
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**

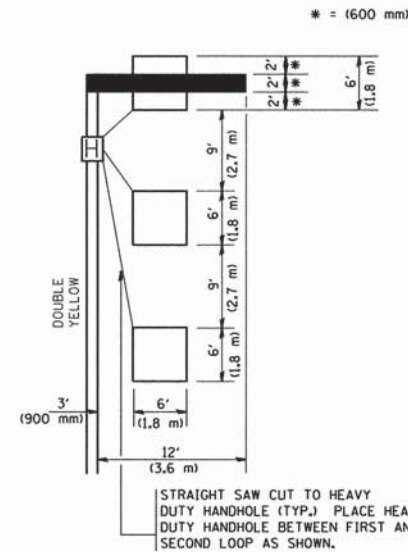
HANDHOLE LOCATION MAY  
VARY DEPENDING ON GEOMETRICS  
AND DESIGN OF TRAFFIC SIGNALS.  
HEAVY-DUTY HANDHOLES TO BE  
USED WHEN THE MEDIAN IS  
MOUNTABLE. REFER TO STANDARD  
814001 TO ENSURE THAT HANDHOLE  
FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS  
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

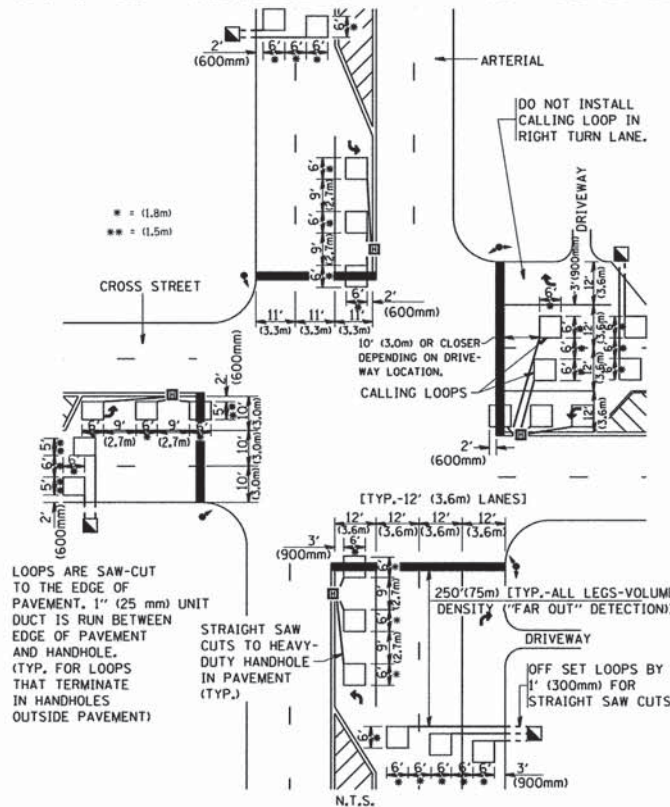
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO  
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**



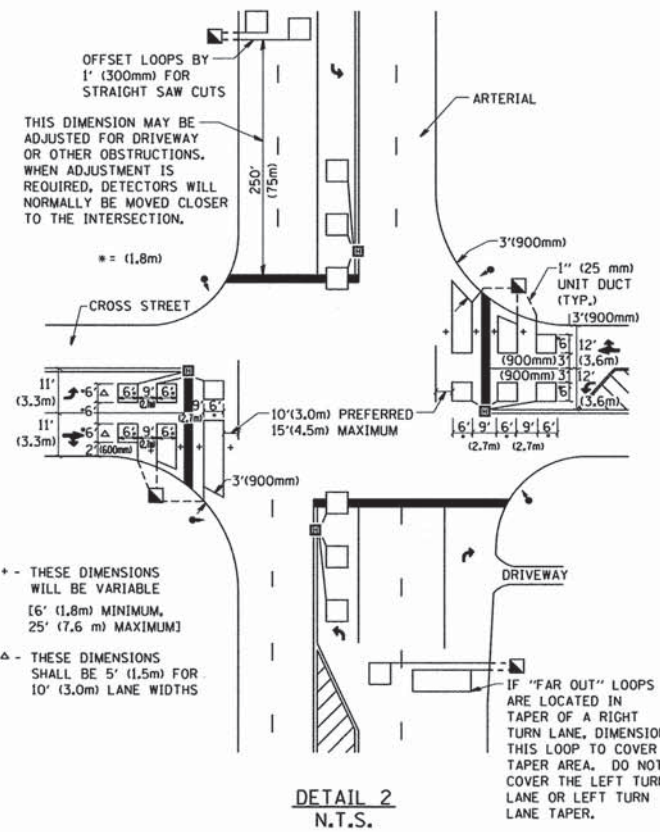
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO  
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2**  
N.T.S.

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\dststd\22\34\ts07.dgn	USER NAME = goglanobt	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 50.0000' / IN.	CHECKED - R.K.F.	DATE -	REVISED -				2637	14-00108-00-RS	DU PAGE	26	26
PLOT DATE = 1/4/2008	DATE -	REVISED -	REVISED -				<b>TS-07</b>			CONTRACT NO. 61B37	
							SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.