

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AR108812	12 PAIR CONTROL CABLE	LF	5,750
AR110014	4" DIRECTIONAL BORE	LF	520
AR125565	SPLICE CAN	EA	3
AR125942	ADJUST BASE MOUNTED LIGHT	EA	84
AR125951	ADJUST RUNWAY GUARD LIGHT	EA	4
AR150510	ENGINEER'S FIELD OFFICE	LS	1
AR150520	MOBILIZATION	LS	1
AR152410	UNCLASSIFIED EXCAVATION	CY	12,900
AR156500	TEMPORARY EROSION CONTROL	LS	1
AR201670	CRACK CONTROL FABRIC	SY	1,450
AR209614	CRUSHED AGG. BASE COURSE - 14"	SY	29,234
AR401610	BITUMINOUS SURFACE COURSE	TON	8,250
AR401630	BITUMINOUS SURFACE TEST SECTION	EA	1
AR401650	BITUMINOUS PAVEMENT MILLING	SY	585
AR401665	BITUMINOUS PAVEMENT SAWING	LF	5,050
AR401921	REMOVE PAVEMENT	SY	200
AR602510	BITUMINOUS PRIME COAT	GAL	14,620
AR603510	BITUMINOUS TACK COAT	GAL	4,700
AR620510	PAVEMENT MARKING	SF	30,400
AR901510	SEEDING	AC	6
AR904510	SODDING	SY	3,800
AR908510	MULCHING	AC	6
AR908525	KNITTED STRAW MAT	SY	3,800
AR910975	RELOCATE ROADWAY SIGN	EA	1

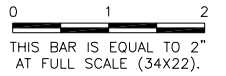
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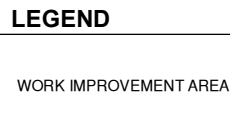
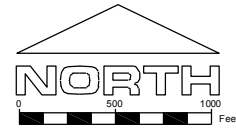
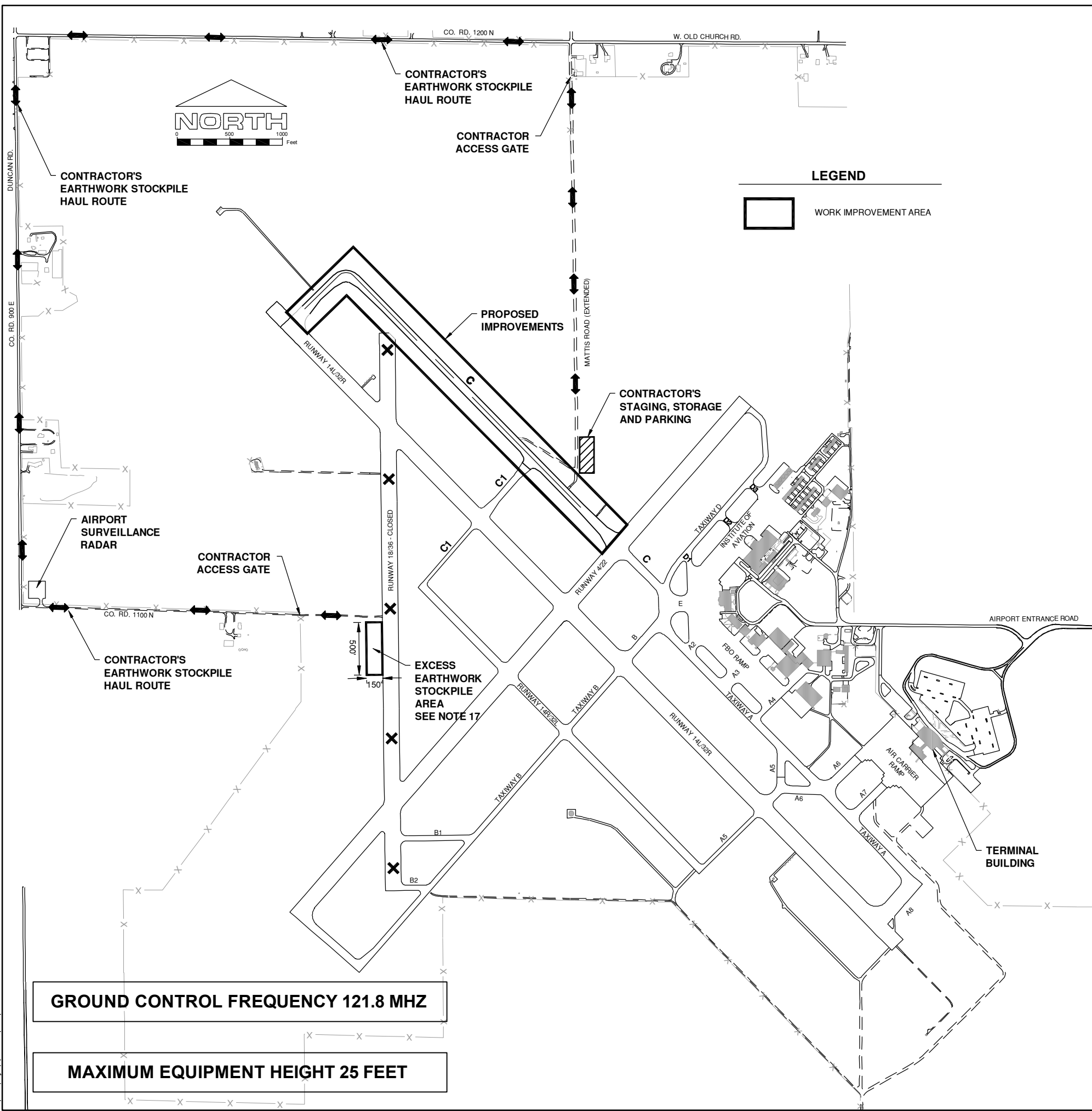
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 WILLARD AIRPORT
 SAVOY, ILLINOIS

CONSTRUCT TAXIWAY C PAVED SHOULDERS
 INDEX TO SHEETS & SUMMARY OF
 QUANTITIES



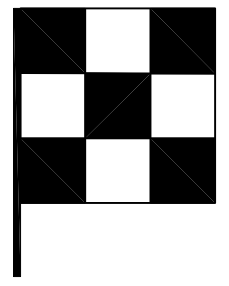
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SHEET	02 OF 52 SHEETS



GENERAL NOTES

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLANS OR AS DIRECTED BY THE AIRPORT MANAGER.
2. ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING ACTIVE RUNWAYS, TAXIWAYS AND APRONS SHALL BE UNDER CONTROL OF A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CLOSING AND OPENING PAVEMENTS AND CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT MANAGER.
4. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS AT THE LOCATION SHOWN FOR THE "CONTRACTOR'S STAGING, STORAGE, PARKING SITE AND FIELD OFFICE".
5. BROKEN OR WASTE CONCRETE AND ASPHALT SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY, UNLESS DIRECTED BY THE AIRPORT MANAGER.
6. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN AREAS 130' (ADV IV - OFA) FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
7. ALL PAVEMENTS, DRIVES AND OTHER AREAS USED BY THE CONTRACTOR FOR HAUL ROADS AND STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF AIRPORT MANAGER. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
8. EXISTING TURF & AGRICULTURAL AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS OUTSIDE OF THE TURFING LIMITS SHALL BE COMPLETELY RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT MANAGER.
9. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS OF ANY LOOSE ITEMS OR DEBRIS PRIOR TO OPENING TO AIR TRAFFIC.
10. REFER TO THE CONSTRUCTION ACTIVITY PLANS AND THE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS CONCERNING COORDINATION OF CONSTRUCTION ACTIVITIES.
11. THE ARFF VEHICLES SHALL HAVE COMPLETE ACCESS TO THE ENTIRE AIRFIELD INCLUDING THE CLOSURE AREAS.
12. THE CONTRACTOR IS REQUIRED TO GIVE SEVEN (7) FULL WORKING DAYS NOTICE TO THE AIRPORT MANAGER PRIOR TO CLOSING WORK AREAS TO AIRCRAFT.
13. AT THE PRECONSTRUCTION MEETING, CONTRACTOR SHALL SUPPLY THE AIRPORT MANAGER WITH PROPOSED CLOSURE AND PHASING DATES FOR HIS REVIEW AND APPROVAL. THE RESIDENT ENGINEER SHALL KEEP THE AIRPORT MANAGER ADVISED OF ANY PROPOSED CHANGES IN CLOSURE AND PHASING DATES.
14. ALL CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL DISPLAY AN ORANGE AND WHITE CHECKED AVIATION VEHICLE SIGNAL FLAG, EXCEPT HAUL VEHICLES.
15. ANY VEHICLE OPERATING WITHIN A MOVEMENT AREA DURING THE HOURS OF DARKNESS SHOULD BE EQUIPPED WITH AN AMBER REVOLVING OR FLASHING DOME-TYPE LIGHT AS SPECIFIED IN THE SPECIAL PROVISIONS.
16. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
17. EXCESS SOIL SHALL BE STOCKPILED AT THE LOCATION SHOWN AND WITHIN THE APPROXIMATE DIMENSIONS. THE STOCKPILE SHALL BE SMOOTH-GRADED AND NOT SIGNIFICANTLY ALTER THE EXISTING DRAINAGE PATTERNS. THE STOCKPILE SHALL NOT EXCEED FOUR (4') FEET ABOVE THE EXISTING, SURROUNDING GROUND SURFACE.
18. ACCESS TO THE STOCKPILE AREA FROM THE WORK IMPROVEMENT AREA SHALL ONLY BE VIA OUTER, PUBLIC ACCESS COUNTY ROADS AS SHOWN. EARTHWORK HAULING OPERATIONS WILL NOT BE ALLOWED ACROSS AIRFIELD PAVEMENT.



VEHICLE SIGNAL FLAG
(ORANGE / WHITE)
N.T.S.

GROUND CONTROL FREQUENCY 121.8 MHZ

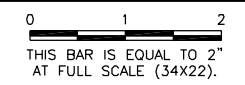
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UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, ILLINOIS

CONSTRUCT TAXIWAY C PAVED SHOULDERS
AIRPORT SITE PLAN



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SHEET	03 OF 52 SHEETS

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GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP IS SPECIALIZED TO AIRPORT OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEANS AND METHODS, THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

A. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

B. PHASING

- 1. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEET.

C. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

D. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATE SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) SECURED AT ALL TIMES INCLUDING WORK HOURS. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE OPEN, HE SHALL POST A COMPETENT, FULL-TIME SECURITY GUARD TO CONTROL ACCESS AT THE GATE AND PREVENT UNAUTHORIZED ENTRIES. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS IF SO DIRECTED BY THE AIRPORT MANAGER OR ENGINEER.
3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE VILLAGE, UNIVERSITY, COUNTY, TOWNSHIP, OR I.D.O.T.
4. A SUFFICIENT NUMBER OF CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE IN ORDER TO CONTROL ALL WORKERS. THIS WILL CONSTITUTE A MINIMUM OF THREE (3) PERSONNEL OBTAINING BADGES. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
5. ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
6. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
7. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES OR HAULING (AGGREGATE, CONCRETE, EARTHWORK, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL THAT HAS OBTAINED PROPER DRIVING PRIVILEGES.
8. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED CMI/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
9. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
10. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
11. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
12. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
13. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
14. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
15. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.

D. CONTRACTOR ACCESS (CONTINUED)

- 16. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
17. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
18. THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

E. WILDLIFE MANAGEMENT

- 1. THE CONTRACTOR SHALL NOTIFY PUBLIC SAFETY OR THE ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED AND LOCKED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

F. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 40' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

PUBLIC SAFETY

JOHN RIEGEL - FIRE CHIEF
OFFICE (217) 244-8764
CELL (217) 202-8213

AIRPORT MAINTENANCE

RON DECKARD - MAINTENANCE CONTACT
OFFICE (217) 369-0099
CELL (217) 714-4600

ENGINEER

CHRIS GROTH P.E. - PROJECT ENGINEER
(217) 787-8050
RESIDENT ENGINEER TO BE DETERMINED
OFFICE (217) 787-8050

G. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. PUBLIC SAFETY WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

H. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

I. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

J. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

K. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2F.

L. MARKING AND SIGNS FOR ACCESS ROUTES

- 1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

M. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
5. THE AIRPORT WILL PROVIDE TWO PORTABLE CLOSED RUNWAY MARKERS FOR USE DURING THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTENANCE OF THE RUNWAY CLOSURE MARKERS INCLUDING FUEL, OIL CHANGES AND REPLACEMENT OF THE LIGHTS. UPON COMPLETION OF THE PROJECT, THE PORTABLE CLOSED RUNWAY MARKERS SHALL BE TURNED OVER TO THE AIRPORT.

N. PROTECTION

- 1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 4-22 OR RUNWAY 14L-32R SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF SEVEN (7) CALENDAR DAYS PRIOR TO THE REQUESTED CLOSURE TIME.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY CENTERLINE OF 50' TAXIWAYS AND 130' FROM THE CENTERLINE OF 75' TAXIWAYS, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
3. ALL WORK REQUIRED ON AN ACTIVE APRON OR INSIDE OF AN ACTIVE SAFETY AREA, WHICH EXTENDS 70' FROM THE APRON'S EDGE OF PAVEMENT, WILL REQUIRE A PORTION OF THAT APRON TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

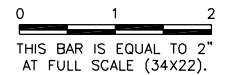
O. OTHER LIMITATIONS ON CONSTRUCTION

- 1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.

UN055

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Table with 3 columns: NUMBER, BY, DATE



UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, ILLINOIS

CONSTRUCT TAXIWAY C PAVED SHOULDERS

CONSTRUCTION SAFETY PHASING NOTES



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Table with 2 columns: Field Name, Value. Fields include DESIGN BY: AJH, DRAWN BY: JLR, CHECKED BY: CBG, APPROVED BY: CET, DATE: MAY 1, 2015, JOB No: 14059-02-00, IL PROJ. NO. CMI-4382, AIP PROJ. NO. 3-17-0016-XX, SHEET 04 OF 52 SHEETS

SEQUENCE OF CONSTRUCTION NOTES

THE GENERAL PROGRESSION OF THE WORK SHALL BE AS FOLLOWS:

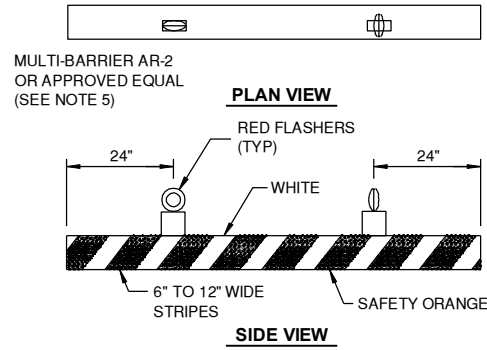
- SUBMIT EQUIPMENT AND BUILDING SHOP, PLAN AND WORKING DRAWINGS FOR REVIEW. INCLUDE WITH THE SUBMITTALS ALL BUY AMERICAN CERTIFICATIONS FOR ALL MATERIALS.
- SUBMIT NOTICE OF OBSTRUCTION EVALUATION- AIRPORT AIRSPACE ANALYSIS (OE/AAA) INFORMATION FOR ANTICIPATED EQUIPMENT HEIGHTS IF IN EXCESS OF 25'. NOTE THAT THIS PROCESS MAY REQUIRE UP TO 90 DAYS FOR FAA APPROVAL. EQUIPMENT ABOVE 25' HEIGHT SHALL NOT BE UTILIZED UNTIL FAA APPROVAL HAS BEEN PROVIDED.
- SUBMIT PROJECT SCHEDULE SHOWING RELATIONSHIP BETWEEN CONSTRUCTION DURATION FOR PAY ITEMS IN RELATION TO THE PHASES OF WORK WHERE THEY ARE BEING PERFORMED. CLEARLY IDENTIFY DATES OF RUNWAY CLOSURES AND WHAT PHASES WILL BE WORKED IN DURING THAT CLOSURE.
- SUBMIT PRELIMINARY MATERIALS CERTIFICATIONS INCLUDING BUY AMERICAN CERTIFICATIONS AND WAIVER REQUEST FOR MATERIALS THAT DO NOT MEET THE CONTRACT REQUIREMENTS.
- INITIATE CONSTRUCTION OF TAXIWAY PAVED SHOULDERS. INSTALL BARRICADES AS OUTLINED ON THE CONSTRUCTION ACTIVITY PLANS. FIELD VERIFY LOCATION OF EXISTING CIRCUITS, PERFORM TESTING ON EXISTING AIRFIELD CIRCUITS TO VERIFY CONDITION OF CIRCUIT CABLES. THE RESIDENT ENGINEER SHALL BE PRESENT AT THE TIME OF TESTING AND SHALL BE GIVEN A COPY OF THE TEST RESULTS.
- INITIATE CONSTRUCTION WITHIN THE VARIOUS PHASES OF THE PROJECT. THE ONLY WORK AREA SHALL BE THE TAXIWAY PAVED SHOULDERS. WORK IN THIS AREA SHALL INCLUDE EARTH EMBANKMENT EXCAVATION, PAVEMENT CONSTRUCTION, DRAINAGE IMPROVEMENTS, ELECTRICAL IMPROVEMENTS, PAVEMENT MARKING AND TURFING.
- UPON COMPLETION OF ALL PHASES, THE CONTRACTOR SHALL REQUEST A FINAL INSPECTION OF THE PROJECT.

RUNWAY SAFETY AREAS

- WORK IN THE RUNWAY 14L/32R SAFETY AREA (250' FROM THE CENTERLINE) SHALL BE LIMITED TO THAT WORK NECESSARY TO CONSTRUCT THE CONNECTING TAXIWAY C PAVED SHOULDERS.
- WORK IN THE RUNWAY 4/22 SAFETY AREA SHALL BE LIMITED TO THAT WORK NECESSARY TO CONSTRUCT THE PARALLEL TAXIWAY C PAVED SHOULDERS. PAVEMENT IMPROVEMENTS OF TAXIWAY C WILL BE AT THE INTERSECTION OF TAXIWAY C AND THE RUNWAY 4/22 PAVEMENT SURFACE.
- RUNWAY 4/22 AND RUNWAY 14L/32R SHALL NOT BE CLOSED AT THE SAME TIME.
- EQUIPMENT AND PERSONNEL SHALL REMAIN CLEAR OF THE RUNWAY PAVEMENTS AT ALL TIMES.
- NO EQUIPMENT, STOCKPILES OR EXCAVATIONS SHALL REMAIN INSIDE THE RUNWAY SAFETY AREAS AFTER WORKING HOURS.

APRON / TAXIWAY OBJECT FREE AREAS

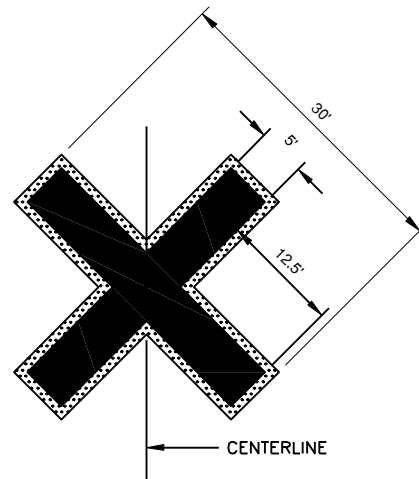
- WORK IN THE TAXIWAY OBJECT FREE AREAS (130' FROM THE CENTERLINE FOR ADG IV) SHALL REQUIRE THAT TAXIWAY TO BE CLOSED.
- NO EQUIPMENT, OPEN TRENCHES OR EXCAVATIONS SHALL REMAIN INSIDE THE TAXIWAY OBJECT FREE AREAS AFTER WORKING HOURS.
- THE TAXIWAYS SHALL BE CLOSED WITH BARRICADES AT 15' MAXIMUM SPACING PRIOR TO WORKING IN THE CRITICAL WORK AREAS.



LOW PROFILE LIGHTED BARRICADE DETAIL
N.T.S.

NOTES

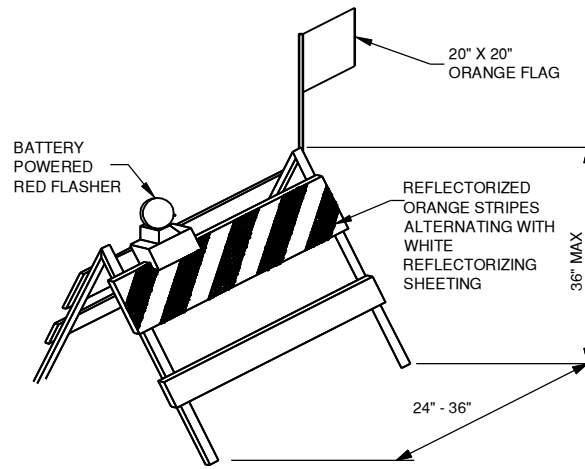
- FLASHERS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 15' SPACING BETWEEN ENDS OF BARRICADES ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION OR AS DIRECTED BY THE ENGINEER. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- FLASHERS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE ENGINEER.
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR LOW AS POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.



CLOSED TAXIWAY MARKER DETAIL
N.T.S.

NOTES

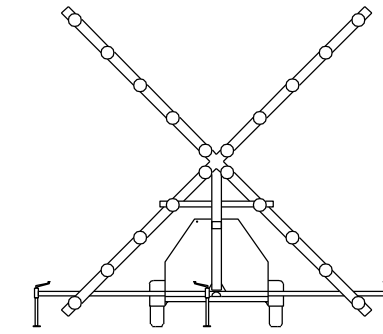
- CLOSURE MARKERS SHALL BE SOLID YELLOW.
- MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY AREA.
- MARKERS SHALL BE PAINTED ON THE TAXIWAY USING TEMPORARY PAINT OR CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
- NON PAINTED MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT STRUCTURE



IDOT TYPE 1 BARRICADE DETAIL
N.T.S.

NOTES

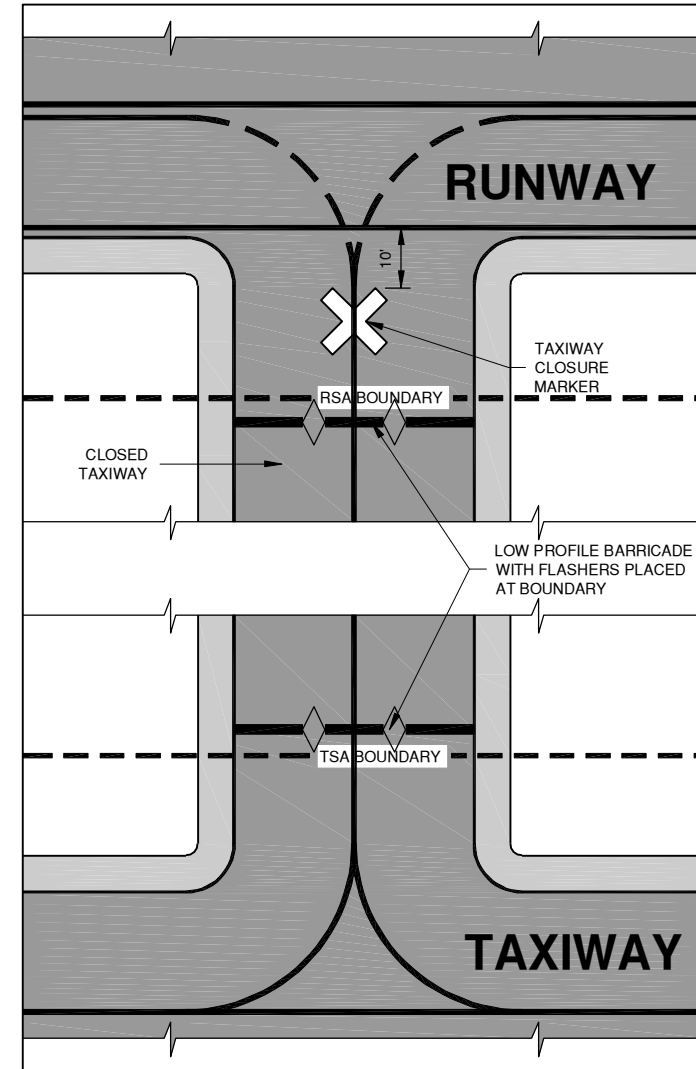
- BARRICADES SHALL BE PLACED AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS 15' ON CENTER AT DESIGNATED LOCATIONS. BARRICADE SHALL BE WEIGHTED WITH A MINIMUM OF 6 SAND BAGS TO PREVENT THEM FROM BEING BLOWN OVER.
- WHERE NOT SPECIFIED, THE CONTRACTOR SHALL HAVE THE OPTION AS TO WHICH TYPE OF BARRICADE IS USED.



LIGHTED RUNWAY CLOSURE MARKER
N.T.S.

NOTES

- PAIR OF LIGHTED 'X'S TO BE PROVIDED BY THE AIRPORT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE (FUEL, OIL, LIGHT BULBS) WHEN USED DURING CONSTRUCTION CLOSURES.



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UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, ILLINOIS

CONSTRUCT TAXIWAY C PAVED SHOULDERS
CONSTRUCTION ACTIVITY PLAN NOTES &
DETAILS



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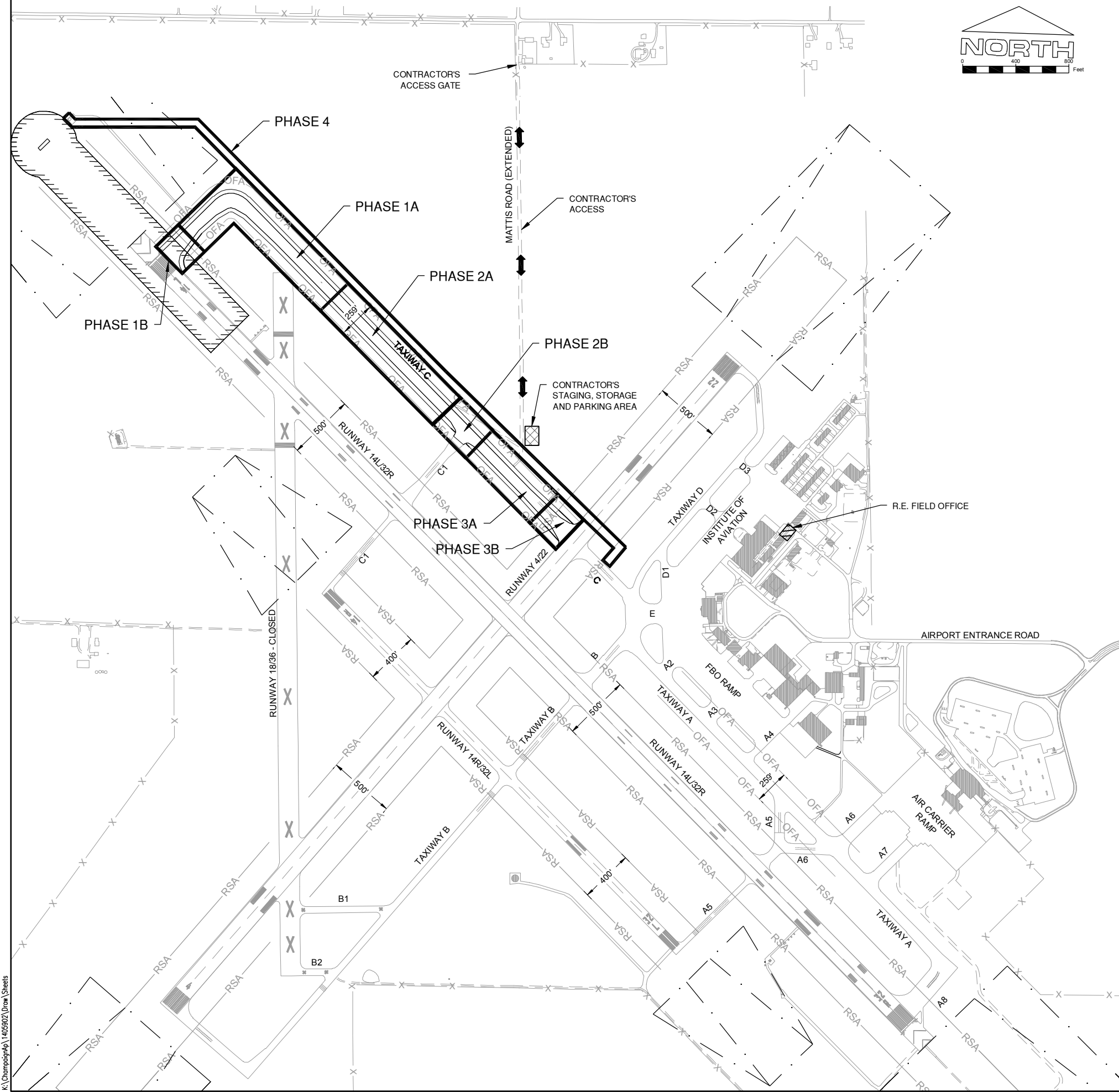
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DRAWN BY:	JLR
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO. CMI-4382 AIP PROJ. NO. 3-17-0016-XX	
SHEET 05 OF 52 SHEETS	

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LEGEND

- PHASE WORK AREAS
- R.E. FIELD OFFICE
- CONTRACTOR'S STAGING, STORAGE & PARKING AREA
- CONTRACTOR'S ACCESS/HAUL ROUTE
- RSA - RUNWAY SAFETY AREA
- OFA - TAXIWAY OBJECT FREE AREA
- RUNWAY APPROACH SURFACE
- LOCALIZER CRITICAL AREA

CONTRACTOR'S ACCESS

1. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTORS ACCESS TO THE WORK SHALL BE AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLANS.
 - B. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK SITE AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
 - C. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR THIS PADLOCK TO THE RESIDENT ENGINEER, AIRPORT SECURITY AND MAINTENANCE SUPERVISOR. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE AIRPORT MANAGER.
 - D. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED & SECURED AT ALL TIMES INCLUDING WORK HOURS. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE OPEN, HE SHALL POST A COMPETENT, **FULL TIME SECURITY GUARD TO PREVENT UNAUTHORIZED ENTRIES**. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS IF SO DIRECTED BY THE AIRPORT MANAGER OR ENGINEER.
 - E. THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATES UPON LEAVING THE SITE.
 - F. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGE TO THE ACCESS GATES OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE AIRPORT MANAGER.
 - G. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - H. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS TO NOT DISTURB AGRICULTURAL AREAS OR VIOLATE THE PART 77 APPROACH AND PRIMARY SURFACES.
 - I. EMPLOYEE PERSONAL VEHICLES SHALL NOT BE ALLOWED BEYOND THE CONTRACTOR'S PARKING AREA. CONTRACTOR PERSONNEL SHALL PARK IN THE CONTRACTOR'S STORAGE AREA. PERSONNEL SHALL BE TRANSPORTED TO THE WORK SITE BY COMPANY OWNED VEHICLES.
 - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL ACCESS ROADS WITH THE APPROPRIATE AGENCY RESPONSIBLE FOR THE ROADWAY.
 - K. THE CONTRACTOR SHALL HAVE A VACUUM TYPE SWEEPER AVAILABLE AT ALL TIMES.
2. AN AREA WILL BE PROVIDED BY THE AIRPORT TO THE CONTRACTOR AS THE STAGING, STORAGE AND EMPLOYEE PARKING SITE, AT THE LOCATION SHOWN.
3. A LIST OF AUTHORIZED PERSONNEL PERMITTED TO USE THE GATE SHALL BE PROVIDED BY THE CONTRACTOR TO THE RESIDENT ENGINEER.
4. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL & CONSTRUCTION ACCESS ONLY".
5. SUPERINTENDENT, SUPERVISING FOREMEN, ESCORT PERSONNEL AND SECURITY GUARDS ON THE AIRFIELD WILL BE REQUIRED TO SUBMIT A TEN YEAR BACKGROUND CHECK TO AIRPORT SECURITY AND WILL BE REQUIRED TO BE TRAINED BY THE AIRPORT TO GAIN ACCESS PER SECTION 80-13 OF THE SPECIFICATIONS. CONTRACTOR SHALL COORDINATE THE BADGING PROCESS A MINIMUM OF 10 DAYS PRIOR TO REQUESTING ACCESS TO THE FIELD.

UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, ILLINOIS

**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 CONSTRUCTION ACTIVITY PLAN - INDEX**



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DESIGN BY:	AJH
DRAWN BY:	JLR
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL. PROJ. NO. CMI-4382 AIP PROJ. NO. 3-17-0016-XX	
SHEET	06 OF 52 SHEETS

PHASE 1A		
RUNWAY 14L-32R	OPEN	TAXIWAY C1 TO RUNWAY 14L-32R
RUNWAY 4-22	OPEN	
TAXIWAY C	CLOSED	
TAXIWAY C1	OPEN	

LEGEND

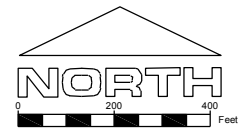
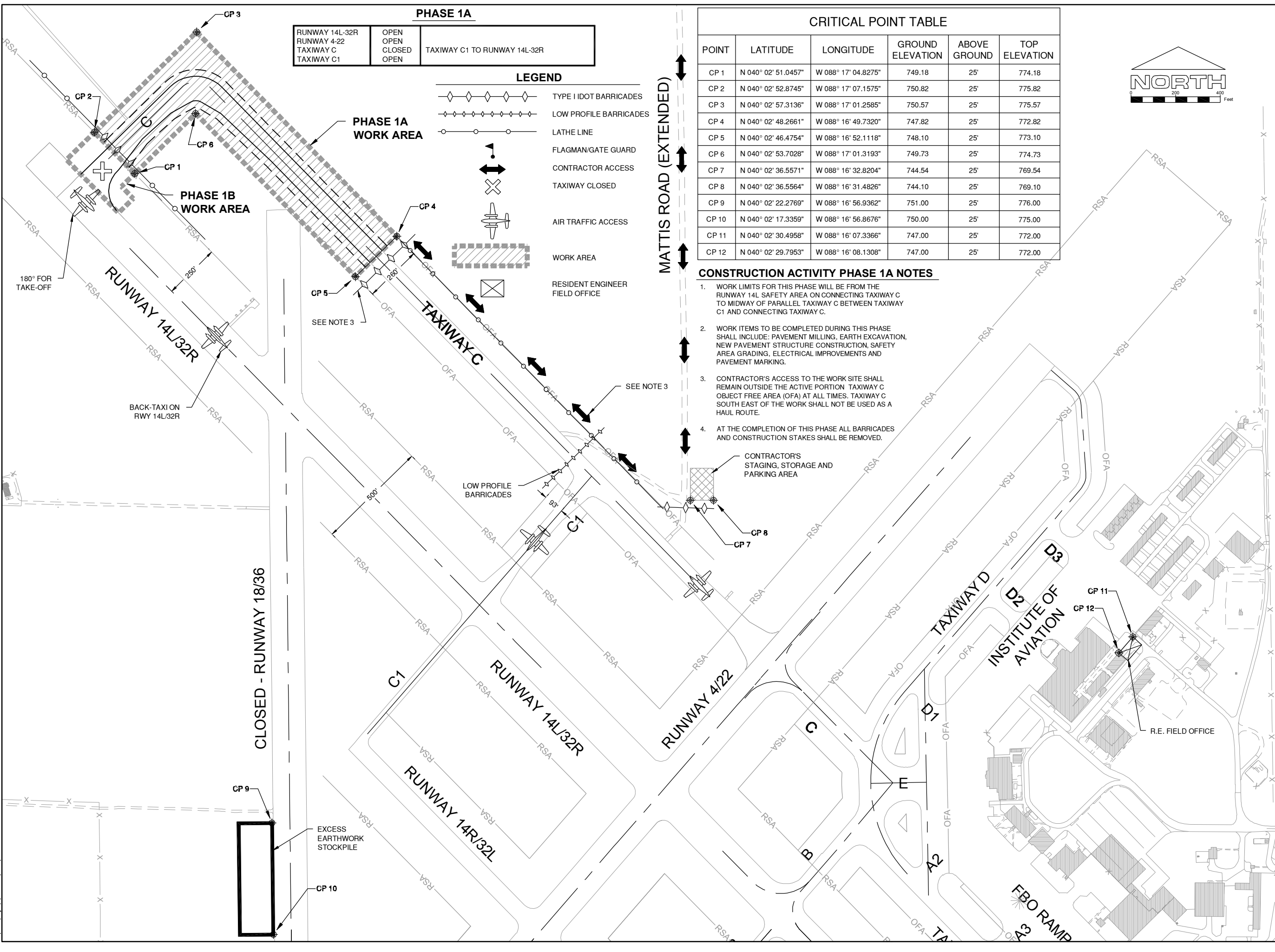
- TYPE I DOT BARRICADES
- LOW PROFILE BARRICADES
- LATHE LINE
- FLAGMAN/GATE GUARD
- CONTRACTOR ACCESS
- TAXIWAY CLOSED
- AIR TRAFFIC ACCESS
- WORK AREA
- RESIDENT ENGINEER FIELD OFFICE

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 51.0457"	W 088° 17' 04.8275"	749.18	25'	774.18
CP 2	N 040° 02' 52.8745"	W 088° 17' 07.1575"	750.82	25'	775.82
CP 3	N 040° 02' 57.3136"	W 088° 17' 01.2585"	750.57	25'	775.57
CP 4	N 040° 02' 48.2661"	W 088° 16' 49.7320"	747.82	25'	772.82
CP 5	N 040° 02' 46.4754"	W 088° 16' 52.1118"	748.10	25'	773.10
CP 6	N 040° 02' 53.7028"	W 088° 17' 01.3193"	749.73	25'	774.73
CP 7	N 040° 02' 36.5571"	W 088° 16' 32.8204"	744.54	25'	769.54
CP 8	N 040° 02' 36.5564"	W 088° 16' 31.4826"	744.10	25'	769.10
CP 9	N 040° 02' 22.2769"	W 088° 16' 56.9362"	751.00	25'	776.00
CP 10	N 040° 02' 17.3359"	W 088° 16' 07.3366"	750.00	25'	775.00
CP 11	N 040° 02' 30.4958"	W 088° 16' 07.3366"	747.00	25'	772.00
CP 12	N 040° 02' 29.7953"	W 088° 16' 08.1308"	747.00	25'	772.00

CONSTRUCTION ACTIVITY PHASE 1A NOTES

1. WORK LIMITS FOR THIS PHASE WILL BE FROM THE RUNWAY 14L SAFETY AREA ON CONNECTING TAXIWAY C TO MIDWAY OF PARALLEL TAXIWAY C BETWEEN TAXIWAY C1 AND CONNECTING TAXIWAY C.
2. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT MILLING, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, SAFETY AREA GRADING, ELECTRICAL IMPROVEMENTS AND PAVEMENT MARKING.
3. CONTRACTOR'S ACCESS TO THE WORK SITE SHALL REMAIN OUTSIDE THE ACTIVE PORTION TAXIWAY C OBJECT FREE AREA (OFA) AT ALL TIMES. TAXIWAY C SOUTH EAST OF THE WORK SHALL NOT BE USED AS A HAUL ROUTE.
4. AT THE COMPLETION OF THIS PHASE ALL BARRICADES AND CONSTRUCTION STAKES SHALL BE REMOVED.



FILE: CMI4382-1405902-GP401.dwg
 UPDATE BY: Andrew Huebner
 PLOT DATE: 5/13/2015 8:33 AM

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 CMI 1305902-C-SPAL
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UNIVERSITY OF ILLINOIS
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 SAVOY, ILLINOIS

CONSTRUCT TAXIWAY C PAVED SHOULDERS
 CONSTRUCTION ACTIVITY PLAN - PHASE 1A



DESIGN BY:	AJH
DRAWN BY:	JLR
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET	07 OF 52 SHEETS

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PHASE 1B (RUNWAY CLOSED)

RUNWAY 14L-32R	CLOSED	TAXIWAY C1 TO RUNWAY 14L-32R SE OF TAXIWAY A7 COORDINATE CLOSURE WITH FAA
RUNWAY 4-22	OPEN	
TAXIWAY C	CLOSED	
TAXIWAY C1	OPEN	
TAXIWAY A	CLOSED	
PAPI 14L & 32R ILS - 14L/32R	OFF OFF	

LEGEND

- TYPE I IDOT BARRICADES
- LOW PROFILE BARRICADES
- LATHE LINE
- FLAGMAN
- CONTRACTOR ACCESS
- RUNWAY CLOSED - YELLOW "X" OR LIGHTED "X" EACH END OF RUNWAY
- TAXIWAY CLOSED
- AIR TRAFFIC ACCESS
- COMPLETED/ONGOING WORK AREA
- WORK AREA

CONSTRUCTION ACTIVITY PHASE 1B NOTES

- WORK LIMITS FOR THIS PHASE WILL BE WITHIN THE RUNWAY 14L SAFETY AREA AT THE CONNECTING TAXIWAY C.
- WORK IN THIS PHASE WILL REQUIRE THE CLOSURE OF RUNWAY 14L/32R. CONTRACTOR SHALL MAKE CLOSURE REQUEST A MINIMUM OF 7 DAYS PRIOR TO THE DESIRED CLOSURE DATE.
- THIS IS A HIGH PRIORITY WORK AREA. IT WILL REQUIRE THE CLOSURE OF RUNWAY 14L/32R, THE AIRPORT'S PRIMARY RUNWAY.
- CONTRACTOR'S SCHEDULE FOR THIS AREA SHALL CONSIDER WEATHER FORECAST AND THE POSSIBLE NEED FOR ADDITIONAL MANPOWER.
- CONTRACTOR SHALL CLOSE RUNWAY 14L/32R NO LONGER THAN 10 CONSECUTIVE CALENDAR DAYS.
- WORK DURING PHASE 1B CAN BE CONCURRENT WITH WORK IN PHASE 1A, BUT SHALL NOT NEGATIVELY IMPACT THE COMPLETION OF THE WORK IN THE HIGH PRIORITY AREA OF PHASE 1B.
- WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT MILLING, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, SAFETY AREA GRADING, PAVEMENT MARKING AND ELECTRICAL WORK.
- PLACE BARRICADES ACROSS TAXIWAY A SOUTH EAST OF TAXIWAY A7, ACROSS RUNWAY 32R NORTH AND SOUTH OF TAXIWAY A5 OFA, AND ACROSS RUNWAY 32R NORTH AND SOUTH OF TAXIWAY B DURING RUNWAY 14L/32R CLOSURE (NOT SHOWN). THESE BARRICADES SHALL FOLLOW THE SETUP SHOWN ON THIS SHEET AT TAXIWAY C1 AND RUNWAY 14L/32R INTERSECTION.
- CONTRACTOR'S ACCESS TO THE WORK SITE SHALL REMAIN OUTSIDE THE ACTIVE PORTION TAXIWAY C OBJECT FREE AREA (OFA) AT ALL TIMES. TAXIWAY C SOUTH EAST OF THE WORK SHALL NOT BE USED AS A HAUL ROUTE.

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 49.3104"	W 088° 17' 05.7615"	753.24	25'	778.24
CP 2	N 040° 02' 49.9419"	W 088° 17' 04.9158"	749.99	25'	774.99
CP 3	N 040° 02' 50.4715"	W 088° 17' 05.5905"	750.55	25'	775.55
CP 4	N 040° 02' 51.0457"	W 088° 17' 04.8275"	749.18	25'	774.18
CP 5	N 040° 02' 52.8745"	W 088° 17' 07.1575"	750.82	25'	775.82
CP 6	N 040° 02' 51.6628"	W 088° 17' 08.7678"	752.83	25'	777.83

FILE: CMI4382-1405902-GP402.dwg
 UPDATE BY: Andrew Huebner
 PLOT DATE: 5/13/2015 8:33 AM

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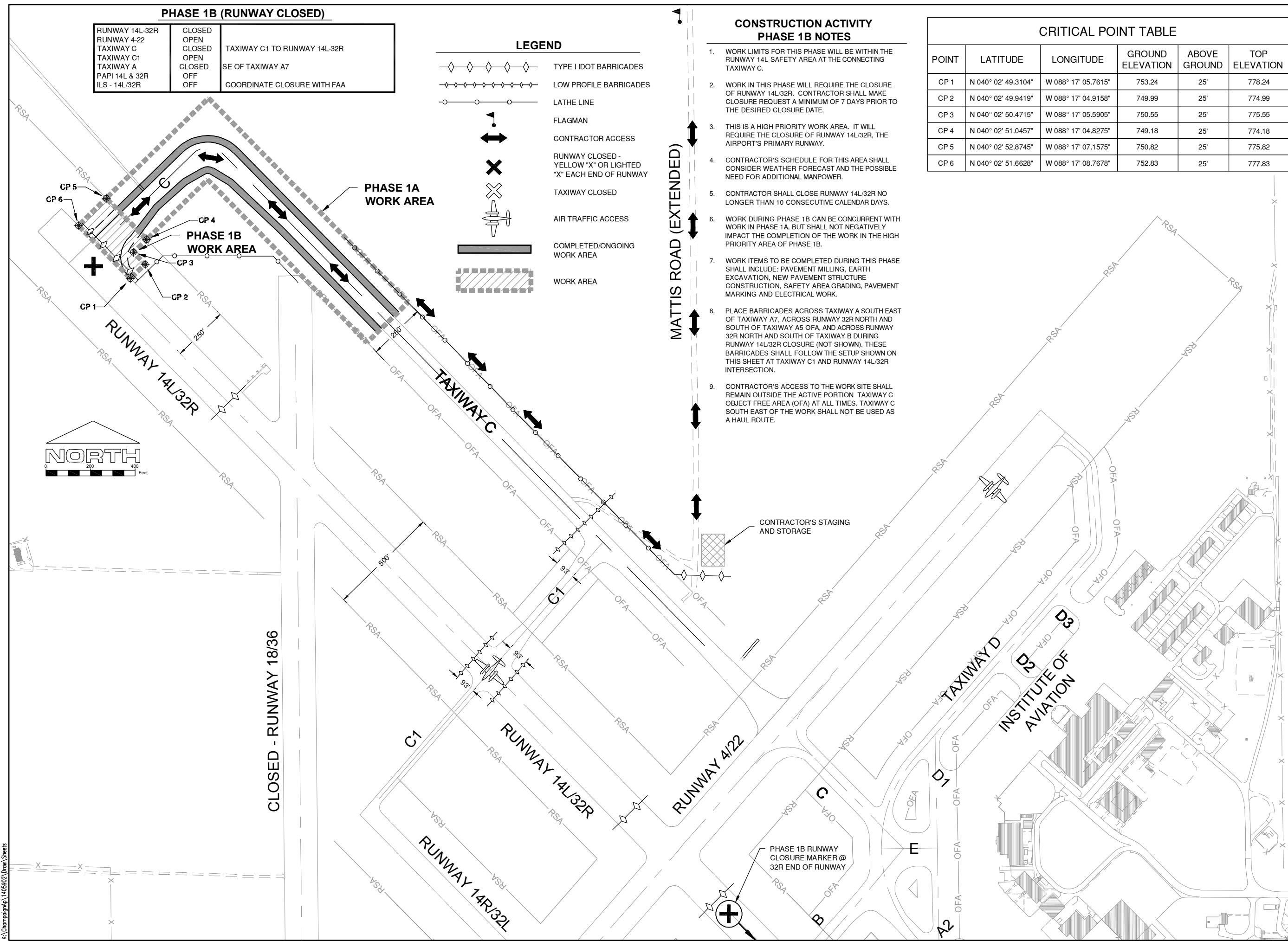
CONSTRUCT TAXIWAY C PAVED SHOULDERS

CONSTRUCTION ACTIVITY PLAN - PHASE 1B



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DATE:	MAY 1, 2015
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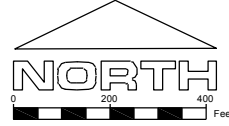
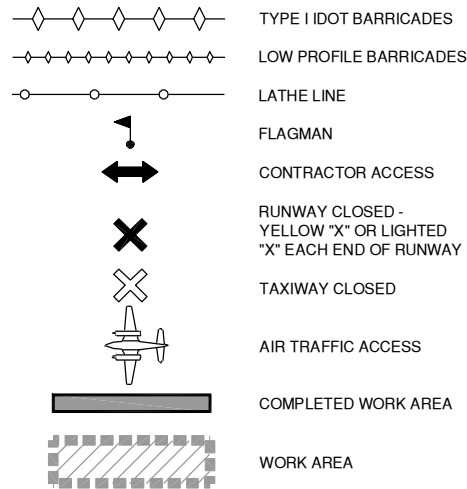


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PHASE 2A

RUNWAY 14L-32R RUNWAY 4-22 TAXIWAY C	OPEN OPEN CLOSED	FROM TAXIWAY C/C1 INTERSECTION TO TAXIWAY C / RUNWAY 14L INTERSECTION
TAXIWAY C1	OPEN	

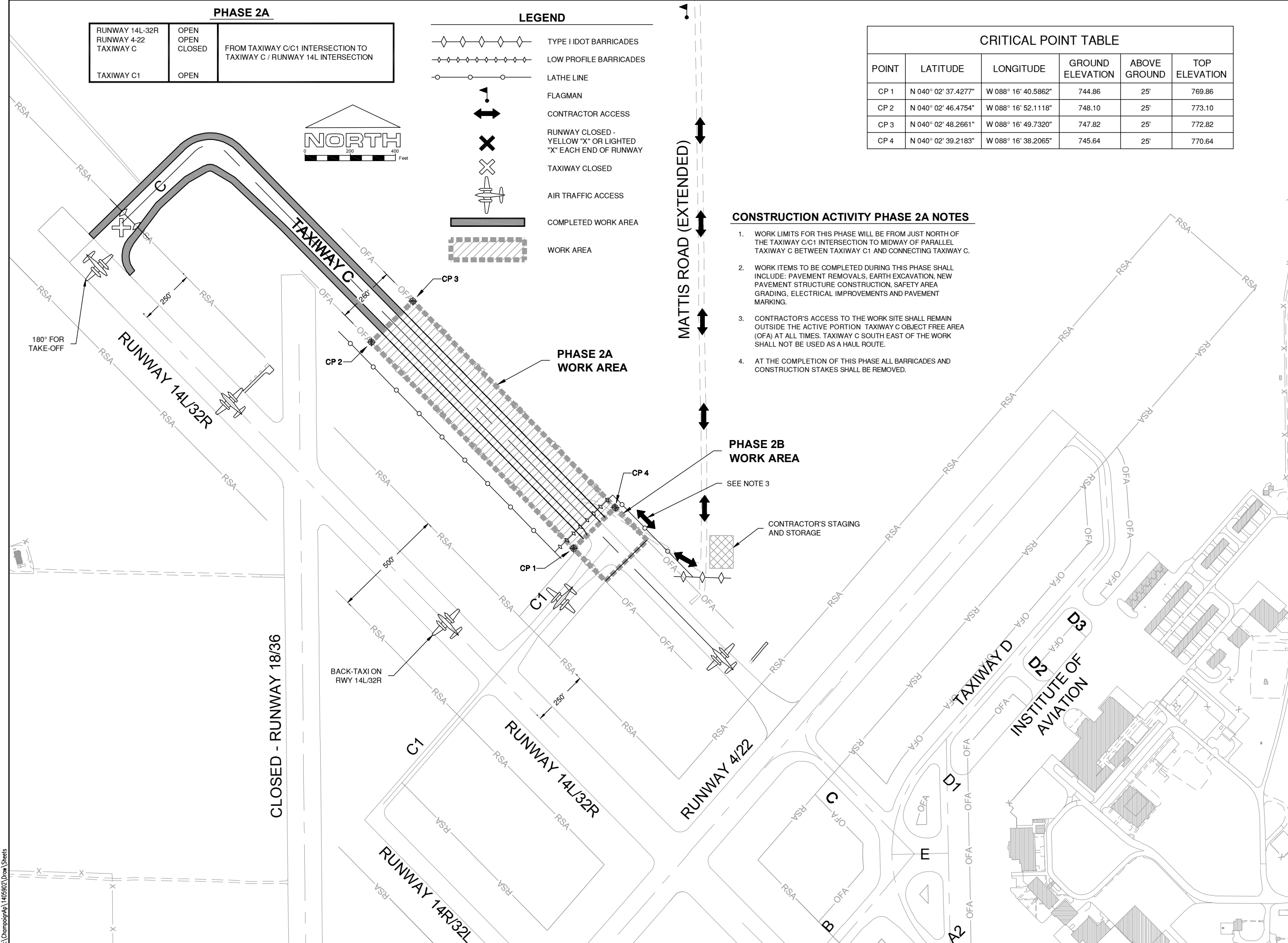
LEGEND



CRITICAL POINT TABLE					
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 37.4277"	W 088° 16' 40.5862"	744.86	25'	769.86
CP 2	N 040° 02' 46.4754"	W 088° 16' 52.1118"	748.10	25'	773.10
CP 3	N 040° 02' 48.2661"	W 088° 16' 49.7320"	747.82	25'	772.82
CP 4	N 040° 02' 39.2183"	W 088° 16' 38.2065"	745.64	25'	770.64

CONSTRUCTION ACTIVITY PHASE 2A NOTES

1. WORK LIMITS FOR THIS PHASE WILL BE FROM JUST NORTH OF THE TAXIWAY C/C1 INTERSECTION TO MIDWAY OF PARALLEL TAXIWAY C BETWEEN TAXIWAY C1 AND CONNECTING TAXIWAY C.
2. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT REMOVALS, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, SAFETY AREA GRADING, ELECTRICAL IMPROVEMENTS AND PAVEMENT MARKING.
3. CONTRACTOR'S ACCESS TO THE WORK SITE SHALL REMAIN OUTSIDE THE ACTIVE PORTION TAXIWAY C OBJECT FREE AREA (OFA) AT ALL TIMES. TAXIWAY C SOUTH EAST OF THE WORK SHALL NOT BE USED AS A HAUL ROUTE.
4. AT THE COMPLETION OF THIS PHASE ALL BARRICADES AND CONSTRUCTION STAKES SHALL BE REMOVED.

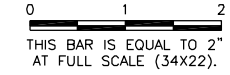


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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 CONSTRUCTION ACTIVITY PLAN - PHASE 2A



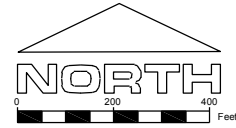
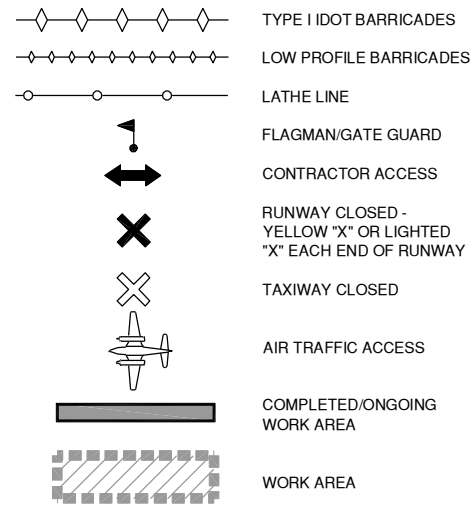
DESIGN BY:	AJH
DRAWN BY:	JLR
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET 09 OF 52 SHEETS	

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PHASE 2B

RUNWAY 14L-32R RUNWAY 4-22 TAXIWAY C	OPEN OPEN CLOSED	FROM TAXIWAY C/RUNWAY 14L INTERSECTION TO TAXIWAY C/RUNWAY 4-22 INTERSECTION
TAXIWAY C1	CLOSED	BETWEEN TAXIWAY C AND RUNWAY 14L/32R

LEGEND



POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 36.0001"	W 088° 16' 38.7677"	744.50	25'	769.50
CP 2	N 040° 02' 37.7907"	W 088° 16' 36.3881"	745.00	25'	770.00
CP 3	N 040° 02' 39.2183"	W 088° 16' 38.2065"	745.64	25'	770.64
CP 4	N 040° 02' 37.4277"	W 088° 16' 40.5863"	744.86	25'	779.86

CONSTRUCTION ACTIVITY PHASE 2B NOTES

1. WORK LIMITS FOR THIS PHASE WILL BE AT THE INTERSECTION OF TAXIWAY C/C1.
2. THIS IS A HIGH PRIORITY WORK AREA. IT WILL REQUIRE THE CLOSURE OF THE TAXIWAY C/C1 INTERSECTION.
3. WORK IN PHASE 2B CAN BE CONCURRENT WITH WORK IN PHASE 2A, BUT CANNOT NEGATIVELY IMPACT THE COMPLETION OF THE WORK IN THE HIGH PRIORITY AREA OF PHASE 2B.
4. CONTRACTOR SHALL CLOSE BOTH TAXIWAY C AND TAXIWAY C1 AS SHOWN NO LONGER THAN 10 CONSECUTIVE CALENDAR DAYS.
5. CONTRACTOR'S SCHEDULE FOR THIS AREA SHALL CONSIDER WEATHER FORECAST AND THE POSSIBLE NEED FOR ADDITIONAL MANPOWER.
6. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT REMOVALS, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, SAFETY AREA GRADING, ELECTRICAL IMPROVEMENTS AND PAVEMENT MARKING.
7. CONTRACTOR'S ACCESS TO THE WORK SITE SHALL REMAIN OUTSIDE THE ACTIVE PORTION TAXIWAY C OBJECT FREE AREA (OFA) AT ALL TIMES. TAXIWAY C SOUTH EAST OF THE WORK SHALL NOT BE USED AS A HAUL ROUTE.
8. AT THE COMPLETION OF THIS PHASE ALL BARRICADES AND CONSTRUCTION STAKES SHALL BE REMOVED.

MATTIS ROAD (EXTENDED)

PHASE 2A
WORK AREA

PHASE 2B
WORK AREA

CONTRACTOR'S STAGING
AND STORAGE

CLOSED - RUNWAY 18/36

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REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34x22).

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CONSTRUCT TAXIWAY C PAVED SHOULDERS

CONSTRUCTION ACTIVITY PLAN - PHASE 2B

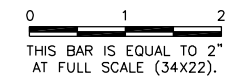


DESIGN BY:	AJH
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IL PROJ. NO. CMI-4382
AIP PROJ. NO. 3-17-0016-XX

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NUMBER	BY	DATE

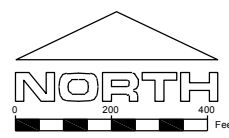
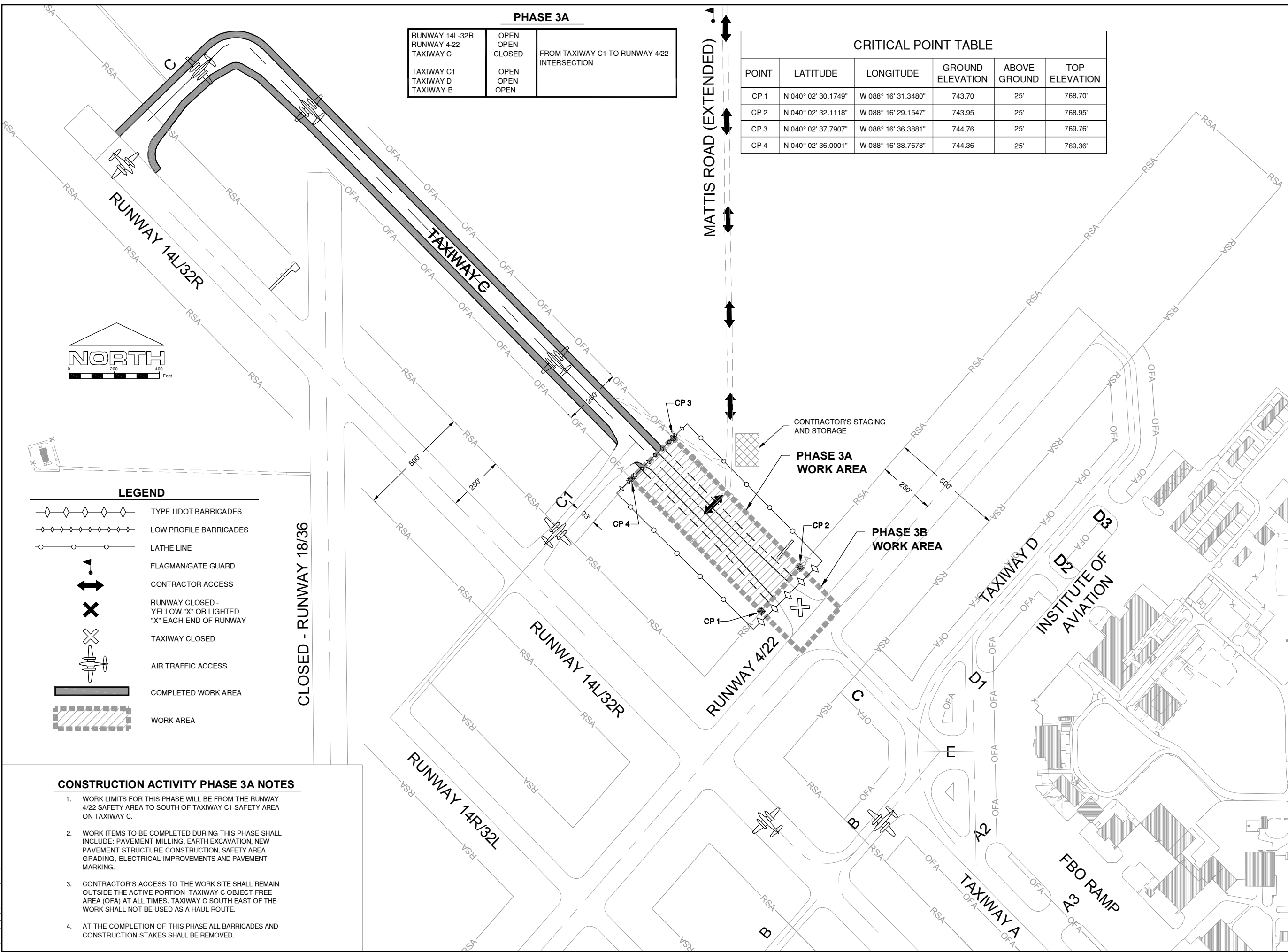


PHASE 3A

RUNWAY 14L-32R	OPEN	FROM TAXIWAY C1 TO RUNWAY 4/22 INTERSECTION
RUNWAY 4-22	OPEN	
TAXIWAY C	CLOSED	
TAXIWAY C1	OPEN	
TAXIWAY D	OPEN	
TAXIWAY B	OPEN	

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 30.1749"	W 088° 16' 31.3480"	743.70	25'	768.70'
CP 2	N 040° 02' 32.1118"	W 088° 16' 29.1547"	743.95	25'	768.95'
CP 3	N 040° 02' 37.7907"	W 088° 16' 36.3881"	744.76	25'	769.76'
CP 4	N 040° 02' 36.0001"	W 088° 16' 38.7678"	744.36	25'	769.36'



LEGEND

	TYPE I DOT BARRICADES
	LOW PROFILE BARRICADES
	LATHE LINE
	FLAGMAN/GATE GUARD
	CONTRACTOR ACCESS
	RUNWAY CLOSED - YELLOW "X" OR LIGHTED "X" EACH END OF RUNWAY
	TAXIWAY CLOSED
	AIR TRAFFIC ACCESS
	COMPLETED WORK AREA
	WORK AREA

CONSTRUCTION ACTIVITY PHASE 3A NOTES

1. WORK LIMITS FOR THIS PHASE WILL BE FROM THE RUNWAY 4/22 SAFETY AREA TO SOUTH OF TAXIWAY C1 SAFETY AREA ON TAXIWAY C.
2. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT MILLING, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, SAFETY AREA GRADING, ELECTRICAL IMPROVEMENTS AND PAVEMENT MARKING.
3. CONTRACTOR'S ACCESS TO THE WORK SITE SHALL REMAIN OUTSIDE THE ACTIVE PORTION TAXIWAY C OBJECT FREE AREA (OFA) AT ALL TIMES. TAXIWAY C SOUTH EAST OF THE WORK SHALL NOT BE USED AS A HAUL ROUTE.
4. AT THE COMPLETION OF THIS PHASE ALL BARRICADES AND CONSTRUCTION STAKES SHALL BE REMOVED.

UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, ILLINOIS

**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 CONSTRUCTION ACTIVITY PLAN - PHASE 3A**



DESIGN BY:	AJH
DRAWN BY:	JLR
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO. CMI-4382	
AIP PROJ. NO. 3-17-0016-XX	
SHEET 11 OF 52 SHEETS	

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NUMBER	BY	DATE

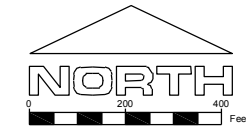
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 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34x22).

PHASE 3B (RUNWAY CLOSED)

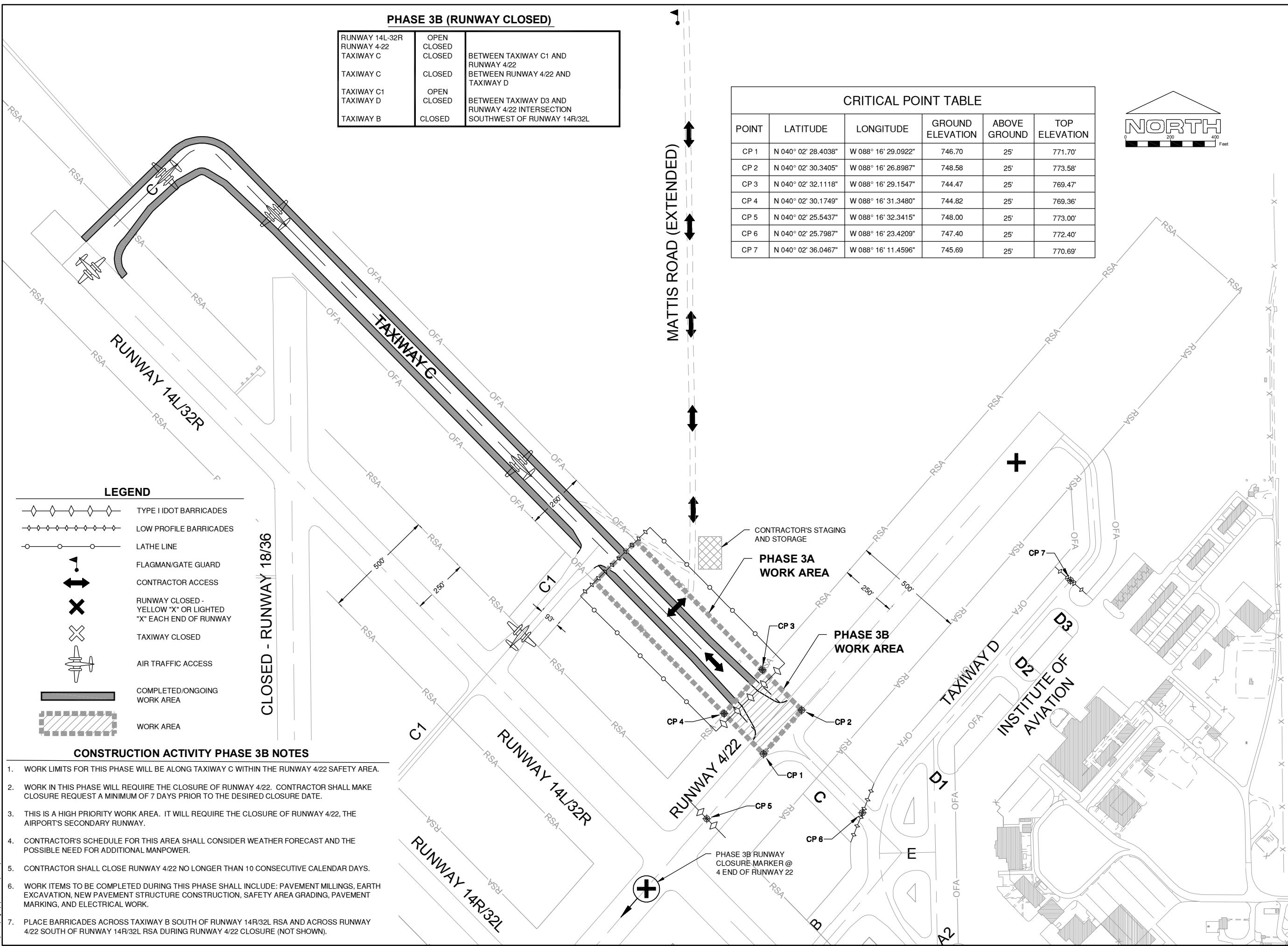
RUNWAY 14L-32R	OPEN	BETWEEN TAXIWAY C1 AND RUNWAY 4/22
RUNWAY 4-22	CLOSED	
TAXIWAY C	CLOSED	BETWEEN RUNWAY 4/22 AND TAXIWAY D
TAXIWAY C1	OPEN	BETWEEN TAXIWAY D3 AND RUNWAY 4/22 INTERSECTION SOUTHWEST OF RUNWAY 14R/32L
TAXIWAY D	CLOSED	
TAXIWAY B	CLOSED	

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 28.4038"	W 088° 16' 29.0922"	746.70	25'	771.70'
CP 2	N 040° 02' 30.3405"	W 088° 16' 26.8987"	748.58	25'	773.58'
CP 3	N 040° 02' 32.1118"	W 088° 16' 29.1547"	744.47	25'	769.47'
CP 4	N 040° 02' 30.1749"	W 088° 16' 31.3480"	744.82	25'	769.36'
CP 5	N 040° 02' 25.5437"	W 088° 16' 32.3415"	748.00	25'	773.00'
CP 6	N 040° 02' 25.7987"	W 088° 16' 23.4209"	747.40	25'	772.40'
CP 7	N 040° 02' 36.0467"	W 088° 16' 11.4596"	745.69	25'	770.69'



MATTIS ROAD (EXTENDED)



LEGEND

- TYPE I DOT BARRICADES
- LOW PROFILE BARRICADES
- LATHE LINE
- FLAGMAN/GATE GUARD
- CONTRACTOR ACCESS
- RUNWAY CLOSED - YELLOW "X" OR LIGHTED "X" EACH END OF RUNWAY
- TAXIWAY CLOSED
- AIR TRAFFIC ACCESS
- COMPLETED/ONGOING WORK AREA
- WORK AREA

CONSTRUCTION ACTIVITY PHASE 3B NOTES

1. WORK LIMITS FOR THIS PHASE WILL BE ALONG TAXIWAY C WITHIN THE RUNWAY 4/22 SAFETY AREA.
2. WORK IN THIS PHASE WILL REQUIRE THE CLOSURE OF RUNWAY 4/22. CONTRACTOR SHALL MAKE CLOSURE REQUEST A MINIMUM OF 7 DAYS PRIOR TO THE DESIRED CLOSURE DATE.
3. THIS IS A HIGH PRIORITY WORK AREA. IT WILL REQUIRE THE CLOSURE OF RUNWAY 4/22, THE AIRPORT'S SECONDARY RUNWAY.
4. CONTRACTOR'S SCHEDULE FOR THIS AREA SHALL CONSIDER WEATHER FORECAST AND THE POSSIBLE NEED FOR ADDITIONAL MANPOWER.
5. CONTRACTOR SHALL CLOSE RUNWAY 4/22 NO LONGER THAN 10 CONSECUTIVE CALENDAR DAYS.
6. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT MILLINGS, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, SAFETY AREA GRADING, PAVEMENT MARKING, AND ELECTRICAL WORK.
7. PLACE BARRICADES ACROSS TAXIWAY B SOUTH OF RUNWAY 14R/32L RSA AND ACROSS RUNWAY 4/22 SOUTH OF RUNWAY 14R/32L RSA DURING RUNWAY 4/22 CLOSURE (NOT SHOWN).

UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, ILLINOIS

CONSTRUCT TAXIWAY C PAVED SHOULDERS
 CONSTRUCTION ACTIVITY PLAN - PHASE 3B



DESIGN BY:	AJH
DRAWN BY:	JLR
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SHEET 12 OF 52 SHEETS	

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UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, ILLINOIS

**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 CONSTRUCTION ACTIVITY PLAN - PHASE 4**



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DRAWN BY:	JLR
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET	13 OF 52 SHEETS

LEGEND

- LOC ——— NEW LOCALIZER CABLE
- ◆◆◆◆◆ TYPE I IDOT BARRICADES
- ◆◆◆◆◆ LOW PROFILE BARRICADES
- ↔ FLAGMAN/GATE GUARD
- ✕ CONTRACTOR ACCESS
- ✕ AIR TRAFFIC ACCESS
- ▨ SHOULDER WORK AREA - OTHER PHASES
- ▨ PHASE 4A DIRECTIONAL BORE CRITICAL AREA
- ▨ PHASE 4B CABLING WORK AREA
- ▨ PHASE 4C FAA SHELTER CONNECTION CRITICAL AREA

PHASE 4A

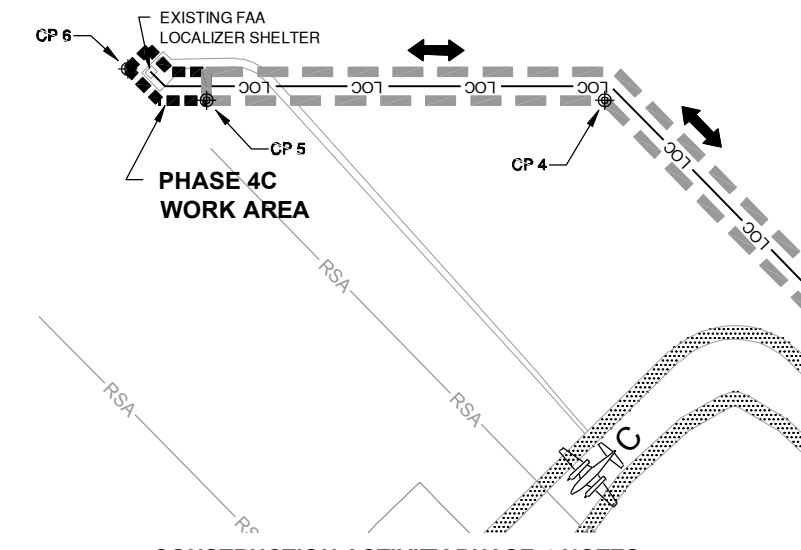
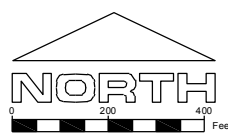
RUNWAY 4-22	CLOSED	BETWEEN TAXIWAY D AND TAXIWAY C1 SOUTHWEST OF RUNWAY 14R/32L
TAXIWAY C	CLOSED	
TAXIWAY B	CLOSED	

PHASE 4B

ALL PAVEMENTS	OPEN
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PHASE 4C

RUNWAY 14L/32R	OPEN	VFR ONLY COORDINATE CLOSURE WITH FAA
ILS-14L/32R	OFF	



CONSTRUCTION ACTIVITY PHASE 4 NOTES

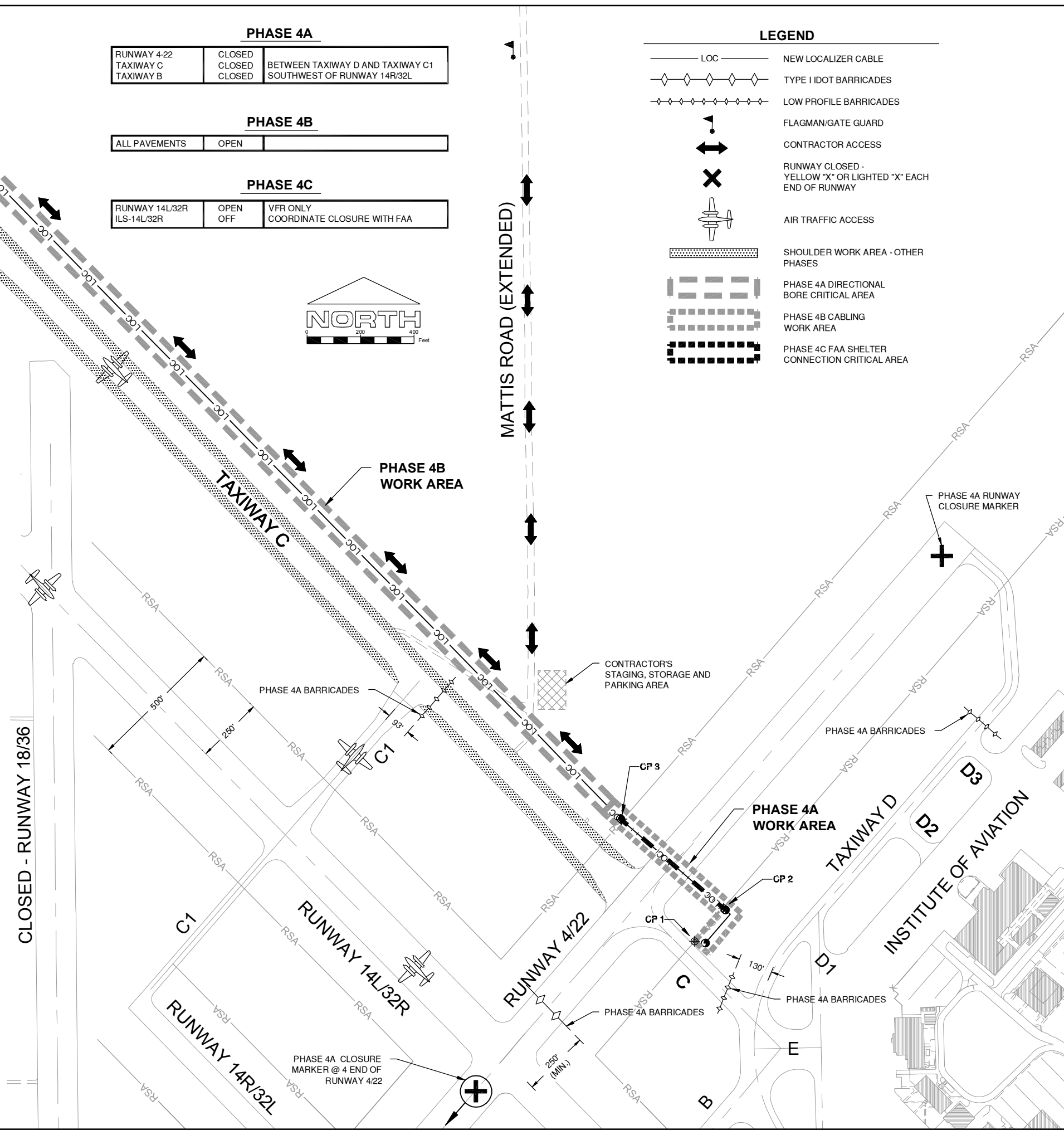
- THIS PHASE SHALL CONSIST OF INSTALLING, RECABLING AND RELOCATING THE ILS/LOCALIZER CABLE.
- IT SHOULD BE NOTED THAT THIS WORK IS BEING COMPLETED IN ORDER TO RELOCATE THE ILS/LOCALIZER CABLE FROM THE WORK AREAS OF THE OTHER PHASES INVOLVING THE CONSTRUCTION OF THE PAVED SHOULDERS. THE ILS/LOCALIZER CABLE WILL BE DAMAGED DURING EXCAVATION IN THOSE PHASES.
- THIS ENTIRE PHASE SHALL BE COMPLETED PRIOR TO BEGINNING EXCAVATION FOR THE PAVED SHOULDER WORK IN ORDER TO KEEP THE LOCALIZER FULLY-FUNCTIONAL DURING ALL OTHER PHASES.**
- WORK LIMITS OF THIS PHASE SHALL CONSIST OF 3 SUB-PHASES. PHASE 4A SHALL CONSIST OF WORK IN THE RSA OF RUNWAY 4/22. PHASE 4B SHALL CONSIST OF WORK ALONG AND OUTSIDE OF THE TAXIWAY C OFA. PHASE 4C SHALL CONSIST OF WORK AT THE FAA ILS/LOCALIZER SHELTER.
- WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: CABLING, SPLICE CAN INSTALLATION, DIRECTIONAL BORING AND CABLING CONNECTIONS.
- WORK IN ALL SUB-PHASES OF PHASE 4 MAY BE CONCURRENT WITH WORK IN ANY OF THE OTHER SUB-PHASES, BUT CANNOT NEGATIVELY IMPACT COMPLETION TIME FRAMES SPECIFIED IN ANY OF THE OTHER SUB-PHASES.
- PHASE 4A SHALL REQUIRE CLOSURE OF RUNWAY 4/22 AND A PORTION OF TAXIWAY C IN ORDER TO INSTALL THE DIRECTIONAL BORE UNDER THE RUNWAY PAVEMENT. ALL WORK REQUIRED WITHIN THE RSA OF RUNWAY 4/22 AND OFA OF TAXIWAY C TO COMPLETE THE CABLE RUN SHALL BE COMPLETED DURING THIS CLOSURE. THE CONTRACTOR SHALL MAKE CLOSURE REQUEST A MINIMUM OF 7 DAYS PRIOR TO THE DESIRED CLOSURE DATE.
- THE CONTRACTOR SHALL HAVE ONE (1) 8-HOUR CLOSURE PERIOD AT A MUTUALLY AGREED TIME TO COMPLETE THE WORK WITHIN PHASE 4A. THE CONTRACTOR WILL BE REQUIRED TO PLACE ALL BARRICADES AS SHOWN FOR THIS 8-HOUR CLOSURE.
- PHASE 4B SHALL CONSIST OF TRENCHING THE NEW CABLE OUTSIDE OF THE TAXIWAY C OFA AND CLEAR OF ALL FIELD LOCATED CABLES IN THE AREA.
- PHASE 4C WILL REQUIRE COORDINATION WITH THE FAA FOR TURNING OFF AND ON THE LOCALIZER AND ALL CONNECTIONS WITHIN THE ILS/LOCALIZER SHELTER. PHASE 4C WILL REQUIRE THE ILS/LOCALIZER TO BE DOWN FOR A PERIOD OF TIME WHILE THIS SWITCH-OVER IS CONDUCTED. THIS SHALL BE COORDINATED WITH THE AIRPORT AND THE FAA WITH AS MUCH NOTICE AS POSSIBLE, BUT A MINIMUM OF 7 DAYS PRIOR TO THE DESIRED SWITCH-OVER DATE.
- CONTRACTOR'S ACCESS TO THE WORK SITE SHALL REMAIN OUTSIDE THE ACTIVE PORTION TAXIWAY C OFA AT ALL TIMES. TAXIWAY C SHALL NOT BE USED AS A HAUL ROUTE.
- PLACE BARRICADES ACROSS TAXIWAY B SOUTH OF RUNWAY 14R/32L DURING PHASE 4A RUNWAY 4/22 CLOSURE (NOT SHOWN).
- AT THE COMPLETION OF THIS PHASE ALL BARRICADES AND CONSTRUCTION STAKES SHALL BE REMOVED.

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 27.9881"	W 088° 16' 25.3083"	747.00	25'	772.00'
CP 2	N 040° 02' 29.2123"	W 088° 16' 23.9203"	743.00	25'	768.00'
CP 3	N 040° 02' 32.4845"	W 088° 16' 28.8145"	743.50	25'	768.50'
CP 4	N 040° 03' 00.2724"	W 088° 17' 04.7754"	751.00	25'	776.00'
CP 5	N 040° 03' 00.2768"	W 088° 17' 15.4346"	750.00	25'	775.00'
CP 6	N 040° 03' 00.9219"	W 088° 17' 17.5419"	750.00	25'	775.00'

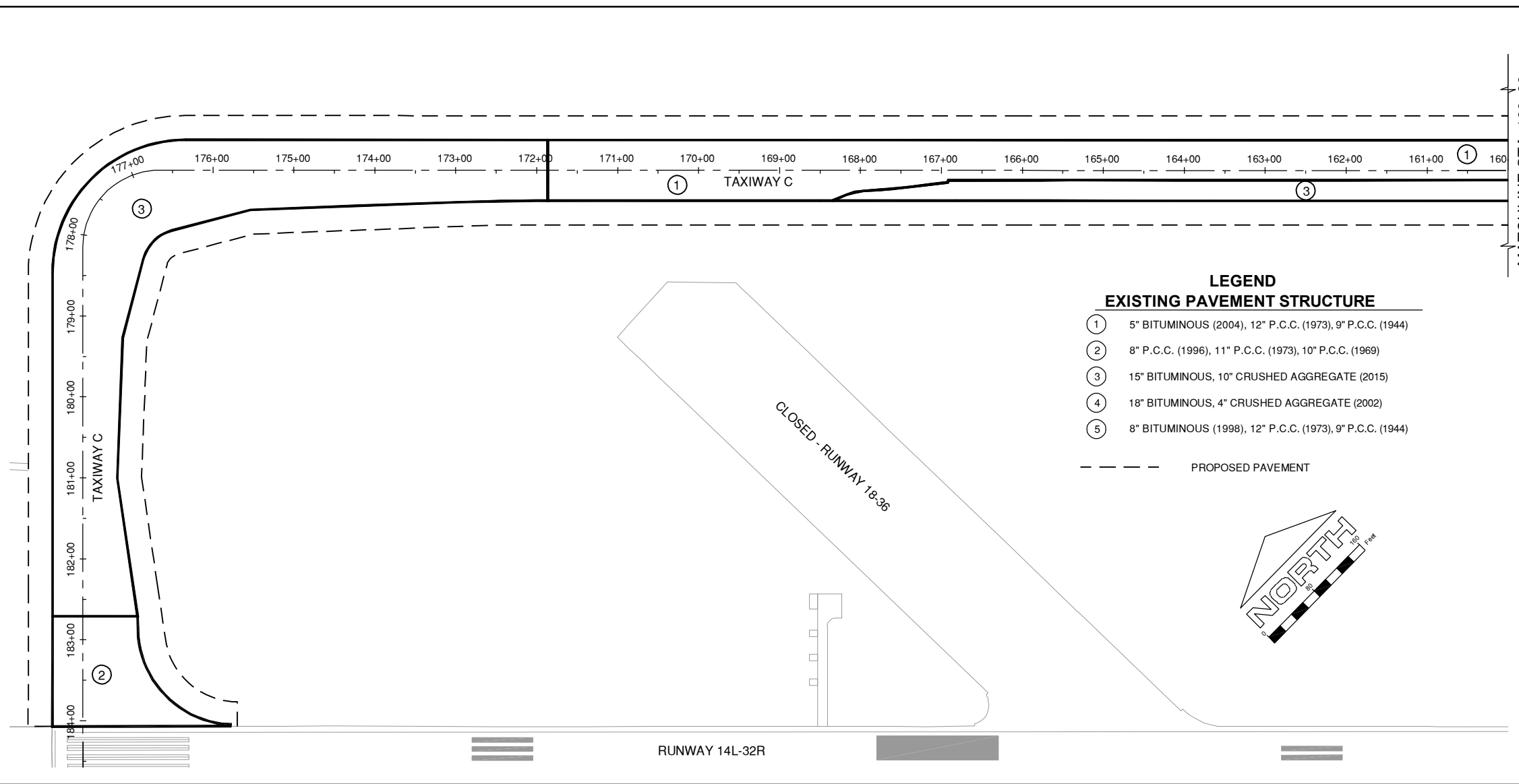
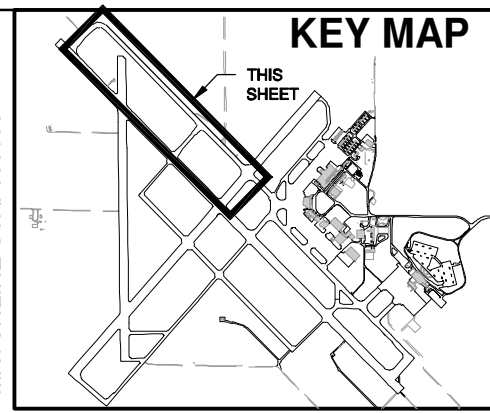
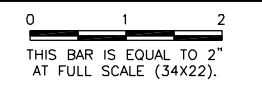
CLOSED - RUNWAY 18/36

MATTIS ROAD (EXTENDED)

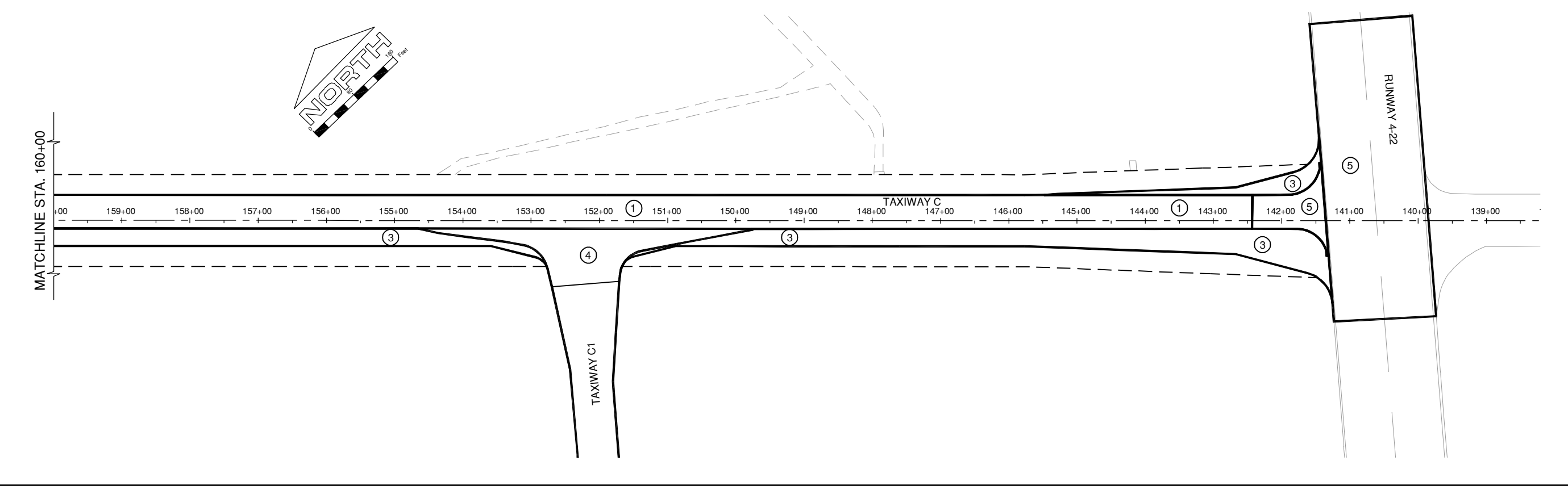


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- LEGEND**
EXISTING PAVEMENT STRUCTURE
- ① 5" BITUMINOUS (2004), 12" P.C.C. (1973), 9" P.C.C. (1944)
 - ② 8" P.C.C. (1996), 11" P.C.C. (1973), 10" P.C.C. (1969)
 - ③ 15" BITUMINOUS, 10" CRUSHED AGGREGATE (2015)
 - ④ 18" BITUMINOUS, 4" CRUSHED AGGREGATE (2002)
 - ⑤ 8" BITUMINOUS (1998), 12" P.C.C. (1973), 9" P.C.C. (1944)
- PROPOSED PAVEMENT



UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, ILLINOIS

CONSTRUCT TAXIWAY C PAVED SHOULDERS
 EXISTING PAVEMENT STRUCTURES



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SHEET	14 OF 52 SHEETS

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34x22).

LEGEND

	EXISTING BASE MOUNTED TAXIWAY LIGHT		EXISTING UNDERDRAIN
	ADJUST EXISTING BASE MOUNTED TAXIWAY LIGHT		EXISTING STORM SEWER
	EXISTING STAKE MOUNTED TAXIWAY LIGHT		EXISTING VGSI CABLE
	EXISTING STAKE MOUNTED RUNWAY LIGHT		EXISTING LOCALIZER CABLE - TO BE RELOCATED
	EXISTING TAXIWAY GUIDANCE SIGN		EXISTING TAXIWAY C CIRCUIT
	EXISTING RUNWAY GUARD LIGHT		EXISTING RUNWAY GUARD LIGHT CABLE
	ADJUST EXISTING RUNWAY GUARD LIGHT		EXISTING RUNWAY 18-36 CIRCUIT
	EXISTING DUCT MARKER		EXISTING RUNWAY 14L-32R CIRCUIT
	EXISTING MANHOLE		EXISTING RUNWAY 4-22 CIRCUIT
	EXISTING CATCH BASIN		ABANDONED TAXIWAY C CIRCUIT
	EXISTING HANDHOLE		RUNWAY SAFETY AREA (NON UTILITY)
	EXISTING BASE MOUNTED THRESHOLD LIGHT		TAXIWAY OBJECT FREE AREA (NON UTILITY)
	EXISTING STAKE MOUNTED THRESHOLD LIGHT		EXISTING DUCT
	EXISTING CLEANOUT		EXISTING DIRECTIONAL BORE
	CONTROL POINTS		NEW BITUMINOUS PAVEMENT MILLING
	EXISTING VASI		NEW BITUMINOUS PAVEMENT SAWING
	EXISTING SPLICE CAN		REMOVE PAVEMENT
	NEW SPLICE CAN		NEW PAVEMENT

UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, ILLINOIS

**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 EXISTING CONDITIONS AND REMOVALS
 LEGEND**



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CONTRACTOR'S RESPONSIBILITY FOR UTILITY SERVICE

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

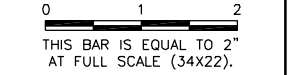
IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

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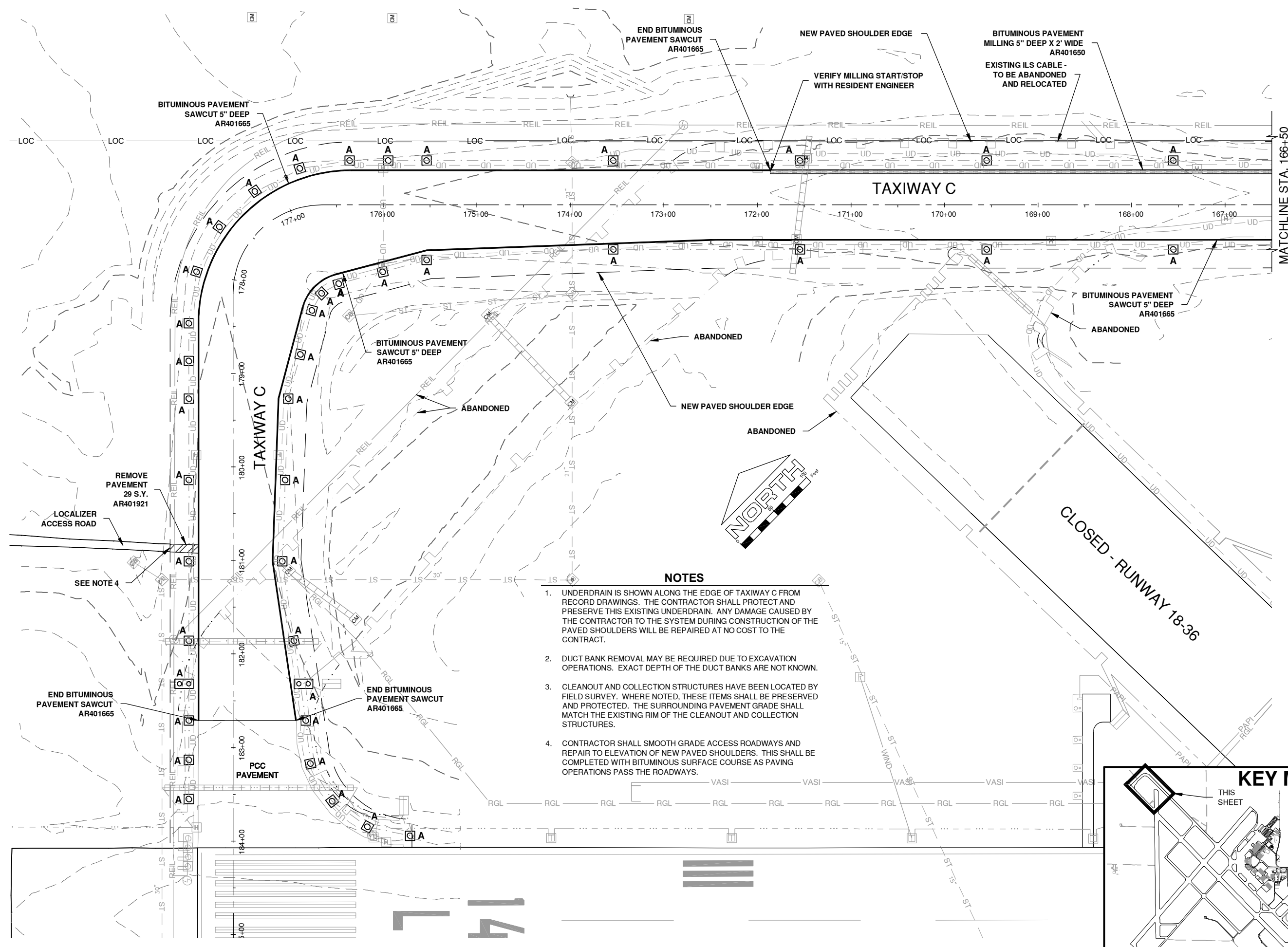


UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, ILLINOIS

**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 EXISTING CONDITIONS AND REMOVALS 1**

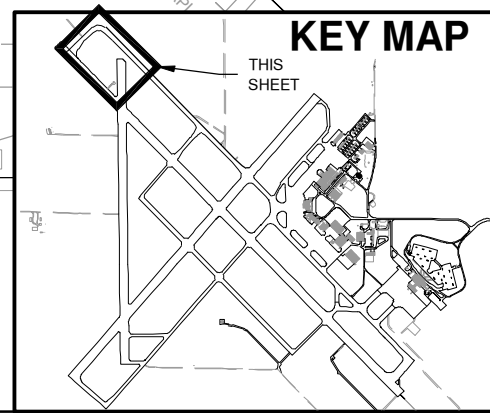


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SHEET 16 OF 52 SHEETS	

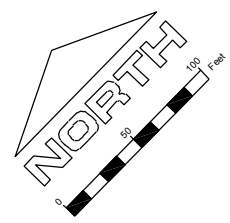
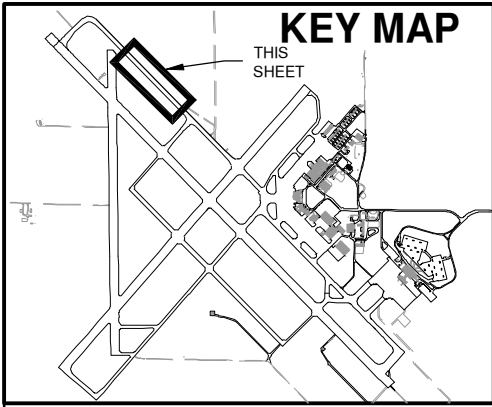


NOTES

1. UNDERDRAIN IS SHOWN ALONG THE EDGE OF TAXIWAY C FROM RECORD DRAWINGS. THE CONTRACTOR SHALL PROTECT AND PRESERVE THIS EXISTING UNDERDRAIN. ANY DAMAGE CAUSED BY THE CONTRACTOR TO THE SYSTEM DURING CONSTRUCTION OF THE PAVED SHOULDERS WILL BE REPAIRED AT NO COST TO THE CONTRACT.
2. DUCT BANK REMOVAL MAY BE REQUIRED DUE TO EXCAVATION OPERATIONS. EXACT DEPTH OF THE DUCT BANKS ARE NOT KNOWN.
3. CLEANOUT AND COLLECTION STRUCTURES HAVE BEEN LOCATED BY FIELD SURVEY. WHERE NOTED, THESE ITEMS SHALL BE PRESERVED AND PROTECTED. THE SURROUNDING PAVEMENT GRADE SHALL MATCH THE EXISTING RIM OF THE CLEANOUT AND COLLECTION STRUCTURES.
4. CONTRACTOR SHALL SMOOTH GRADE ACCESS ROADWAYS AND REPAIR TO ELEVATION OF NEW PAVED SHOULDERS. THIS SHALL BE COMPLETED WITH BITUMINOUS SURFACE COURSE AS PAVING OPERATIONS PASS THE ROADWAYS.



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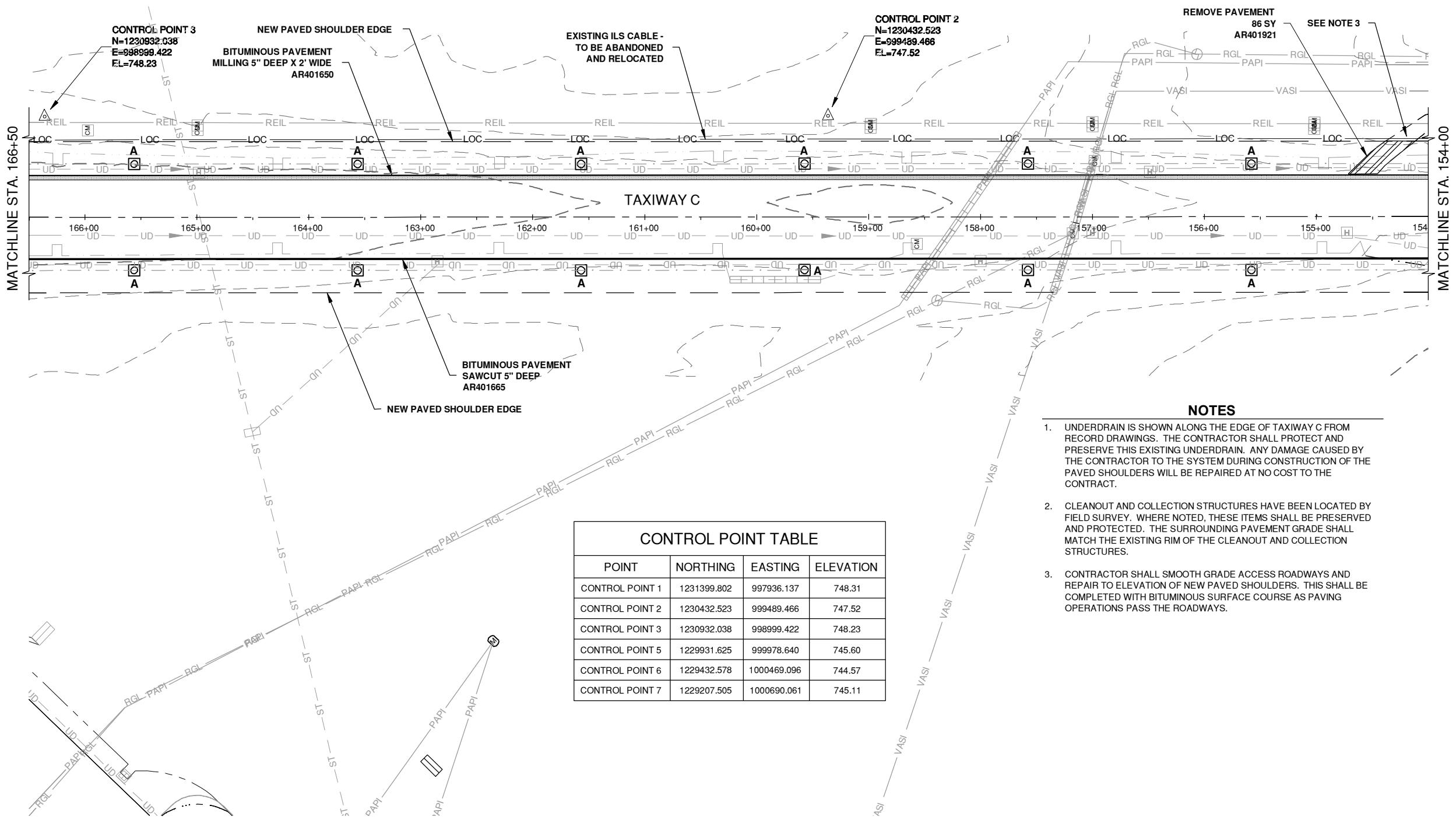
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 UPDATE BY: Andrew Huebner
 PLOT DATE: 5/13/2015 8:35 AM

CMI-V-AERL
 CMI 1305902-V-AF2D
 KEYMAP
 CMI PAVEMENT MARKING
 UTILITY_CMI
 CMI 1405902-C-SPAL

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0 1 2
 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34x22).



NOTES

1. UNDERDRAIN IS SHOWN ALONG THE EDGE OF TAXIWAY C FROM RECORD DRAWINGS. THE CONTRACTOR SHALL PROTECT AND PRESERVE THIS EXISTING UNDERDRAIN. ANY DAMAGE CAUSED BY THE CONTRACTOR TO THE SYSTEM DURING CONSTRUCTION OF THE PAVED SHOULDERS WILL BE REPAIRED AT NO COST TO THE CONTRACT.
2. CLEANOUT AND COLLECTION STRUCTURES HAVE BEEN LOCATED BY FIELD SURVEY. WHERE NOTED, THESE ITEMS SHALL BE PRESERVED AND PROTECTED. THE SURROUNDING PAVEMENT GRADE SHALL MATCH THE EXISTING RIM OF THE CLEANOUT AND COLLECTION STRUCTURES.
3. CONTRACTOR SHALL SMOOTH GRADE ACCESS ROADWAYS AND REPAIR TO ELEVATION OF NEW PAVED SHOULDERS. THIS SHALL BE COMPLETED WITH BITUMINOUS SURFACE COURSE AS PAVING OPERATIONS PASS THE ROADWAYS.

CONTROL POINT TABLE

POINT	NORTHING	EASTING	ELEVATION
CONTROL POINT 1	1231399.802	997936.137	748.31
CONTROL POINT 2	1230432.523	999489.466	747.52
CONTROL POINT 3	1230932.038	998999.422	748.23
CONTROL POINT 5	1229931.625	999978.640	745.60
CONTROL POINT 6	1229432.578	1000469.096	744.57
CONTROL POINT 7	1229207.505	1000690.061	745.11

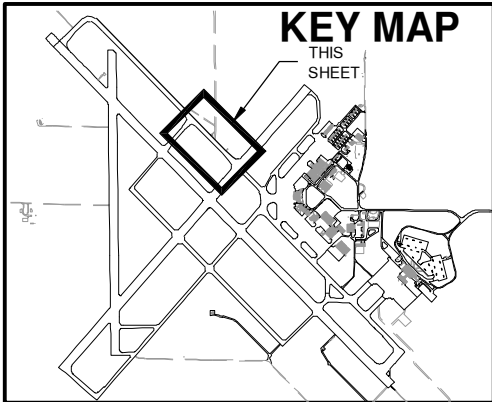
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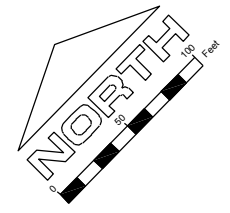
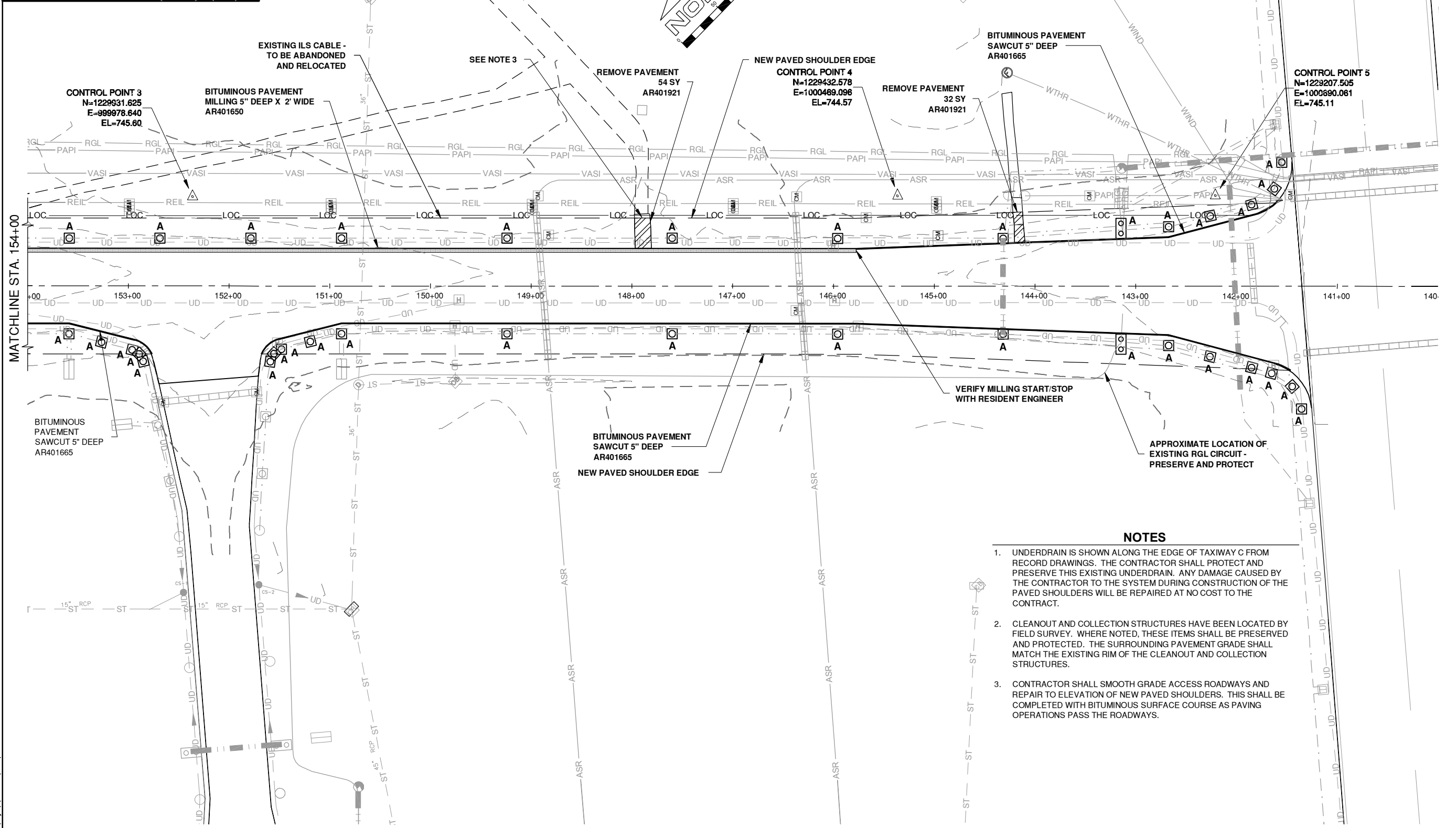
**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 EXISTING CONDITIONS AND REMOVALS 2**



DESIGN BY:	AJH
DRAWN BY:	JLR
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO. CMI-4382 AIP PROJ. NO. 3-17-0016-XX	
SHEET 17 OF 52 SHEETS	



KEY MAP
THIS SHEET

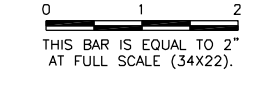


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 UPDATE BY: Andrew Huebner
 PLOT DATE: 5/13/2015 8:36 AM

CMI-V-AERL
 KEYMAP
 UTILITY_CMI

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**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 EXISTING CONDITIONS AND REMOVALS 3**

NOTES

1. UNDERDRAIN IS SHOWN ALONG THE EDGE OF TAXIWAY C FROM RECORD DRAWINGS. THE CONTRACTOR SHALL PROTECT AND PRESERVE THIS EXISTING UNDERDRAIN. ANY DAMAGE CAUSED BY THE CONTRACTOR TO THE SYSTEM DURING CONSTRUCTION OF THE PAVED SHOULDERS WILL BE REPAIRED AT NO COST TO THE CONTRACT.
2. CLEANOUT AND COLLECTION STRUCTURES HAVE BEEN LOCATED BY FIELD SURVEY. WHERE NOTED, THESE ITEMS SHALL BE PRESERVED AND PROTECTED. THE SURROUNDING PAVEMENT GRADE SHALL MATCH THE EXISTING RIM OF THE CLEANOUT AND COLLECTION STRUCTURES.
3. CONTRACTOR SHALL SMOOTH GRADE ACCESS ROADWAYS AND REPAIR TO ELEVATION OF NEW PAVED SHOULDERS. THIS SHALL BE COMPLETED WITH BITUMINOUS SURFACE COURSE AS PAVING OPERATIONS PASS THE ROADWAYS.



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DRAWN BY:	JLR
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APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00

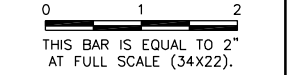
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 AIP PROJ. NO. 3-17-0016-XX

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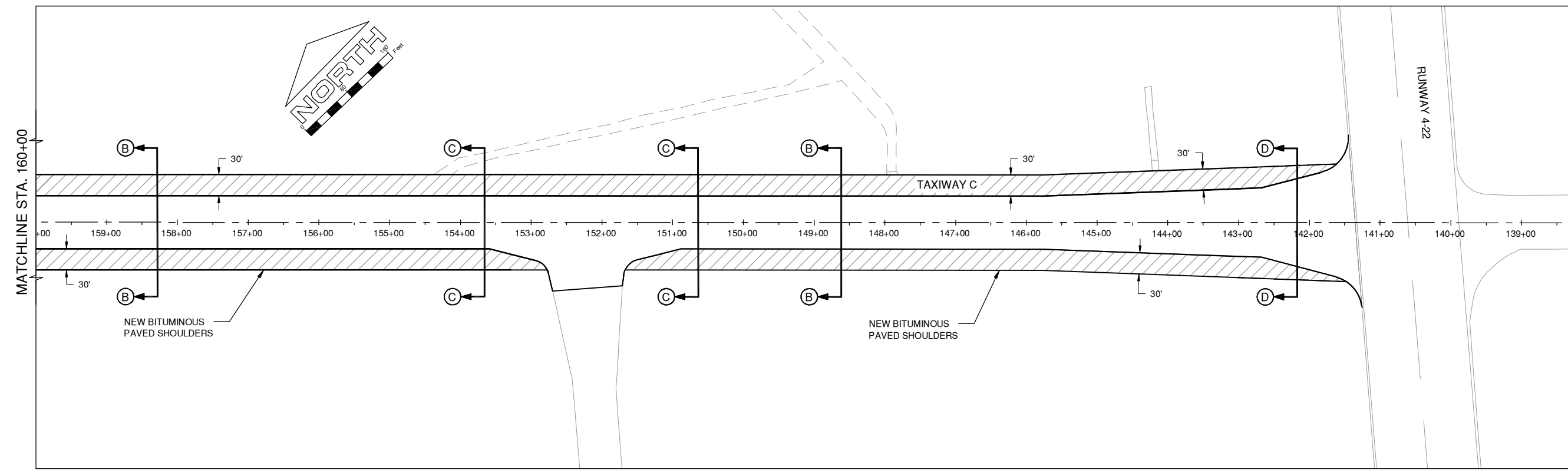
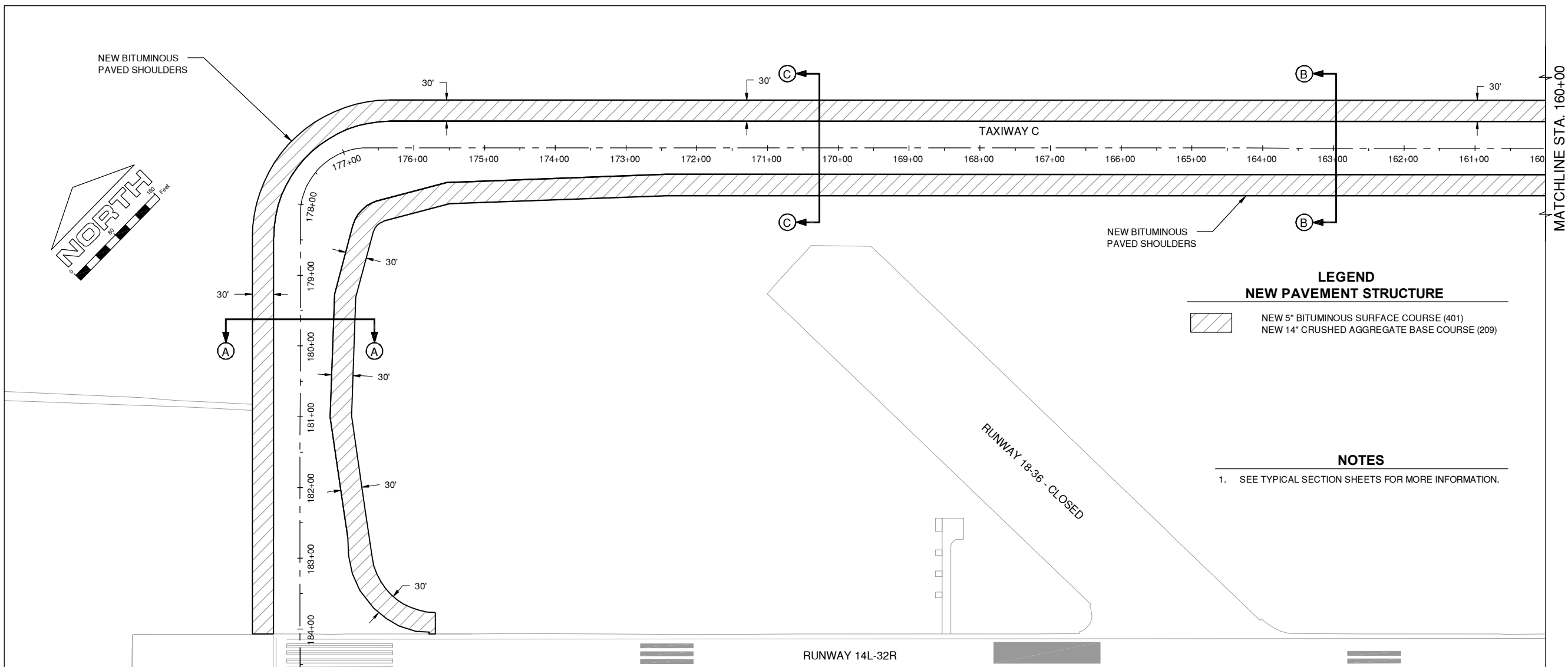
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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PROPOSED IMPROVEMENTS



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AIP PROJ. NO.	3-17-0016-XX
SHEET	19 OF 52 SHEETS

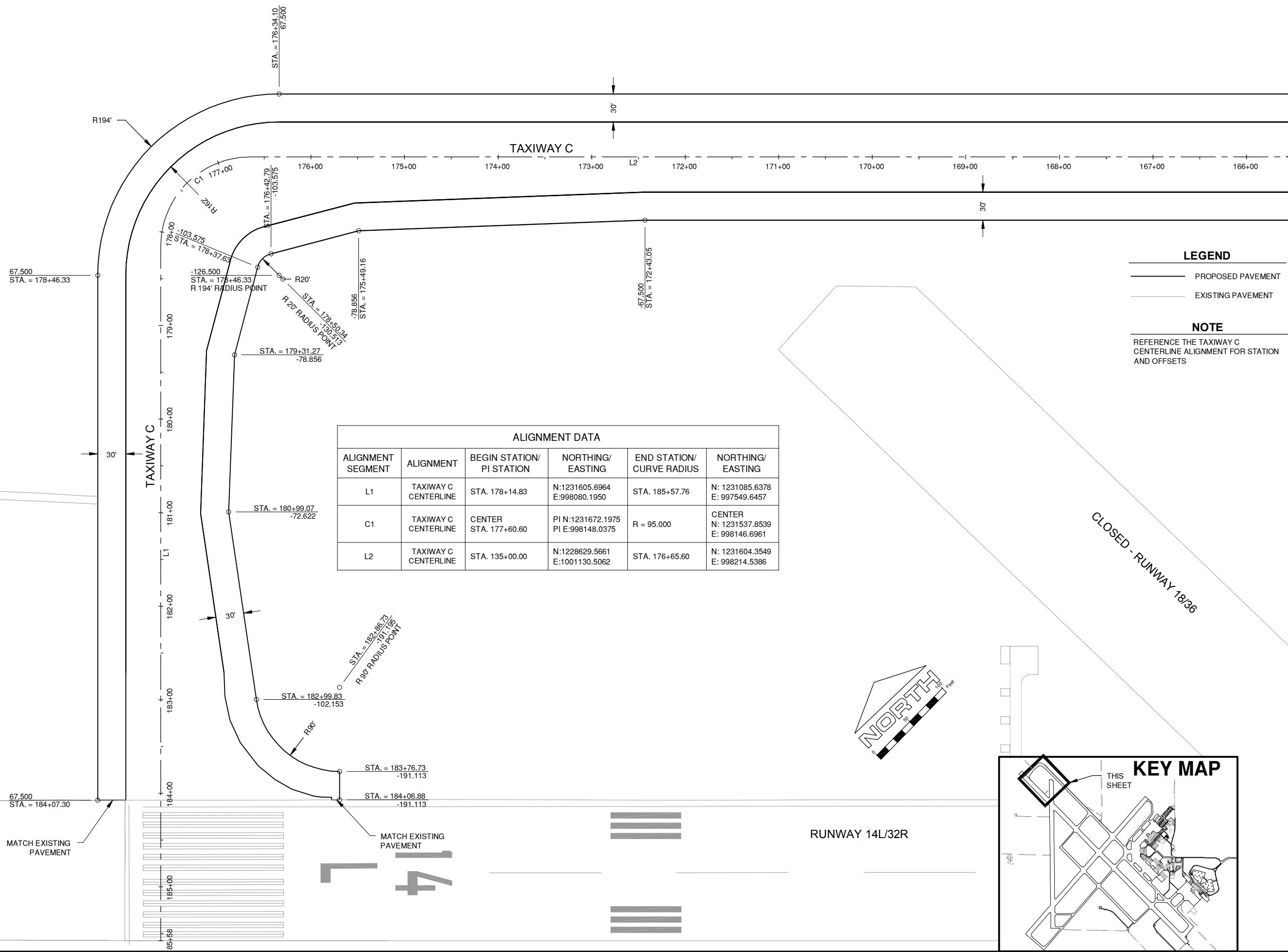
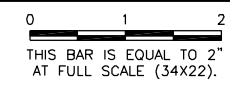


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ALIGNMENT DATA

ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	TAXIWAY C CENTERLINE	STA. 178+14.83	N:1231605.6964 E:998080.1950	STA. 185+57.76	N: 1231085.6378 E: 997549.6457
C1	TAXIWAY C CENTERLINE	CENTER STA. 177+60.60	PI N:1231672.1975 PI E:998148.0375	R = 95.000	CENTER N: 1231537.8539 E: 998146.6961
L2	TAXIWAY C CENTERLINE	STA. 135+00.00	N:1228629.5661 E:1001130.5062	STA. 176+65.60	N: 1231604.3549 E: 998214.5386

LEGEND

- PROPOSED PAVEMENT
- - - EXISTING PAVEMENT

NOTE

REFERENCE THE TAXIWAY C
 CENTERLINE ALIGNMENT FOR STATION
 AND OFFSETS

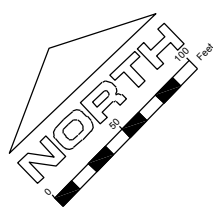
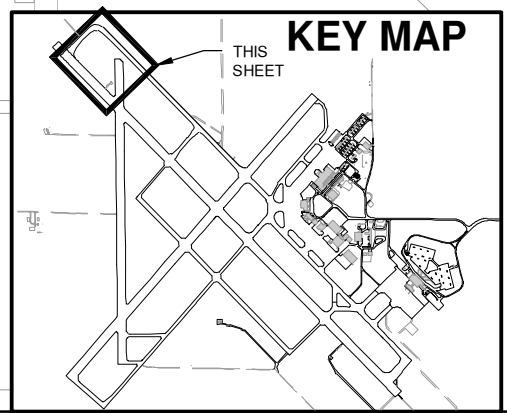
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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PROPOSED GEOMETRY 1



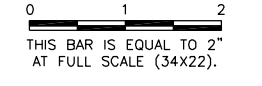
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CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET	20 OF 52 SHEETS



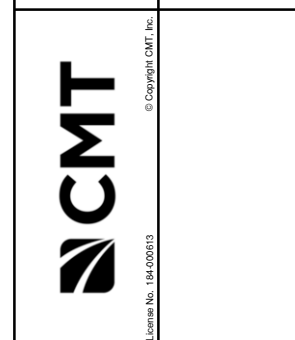
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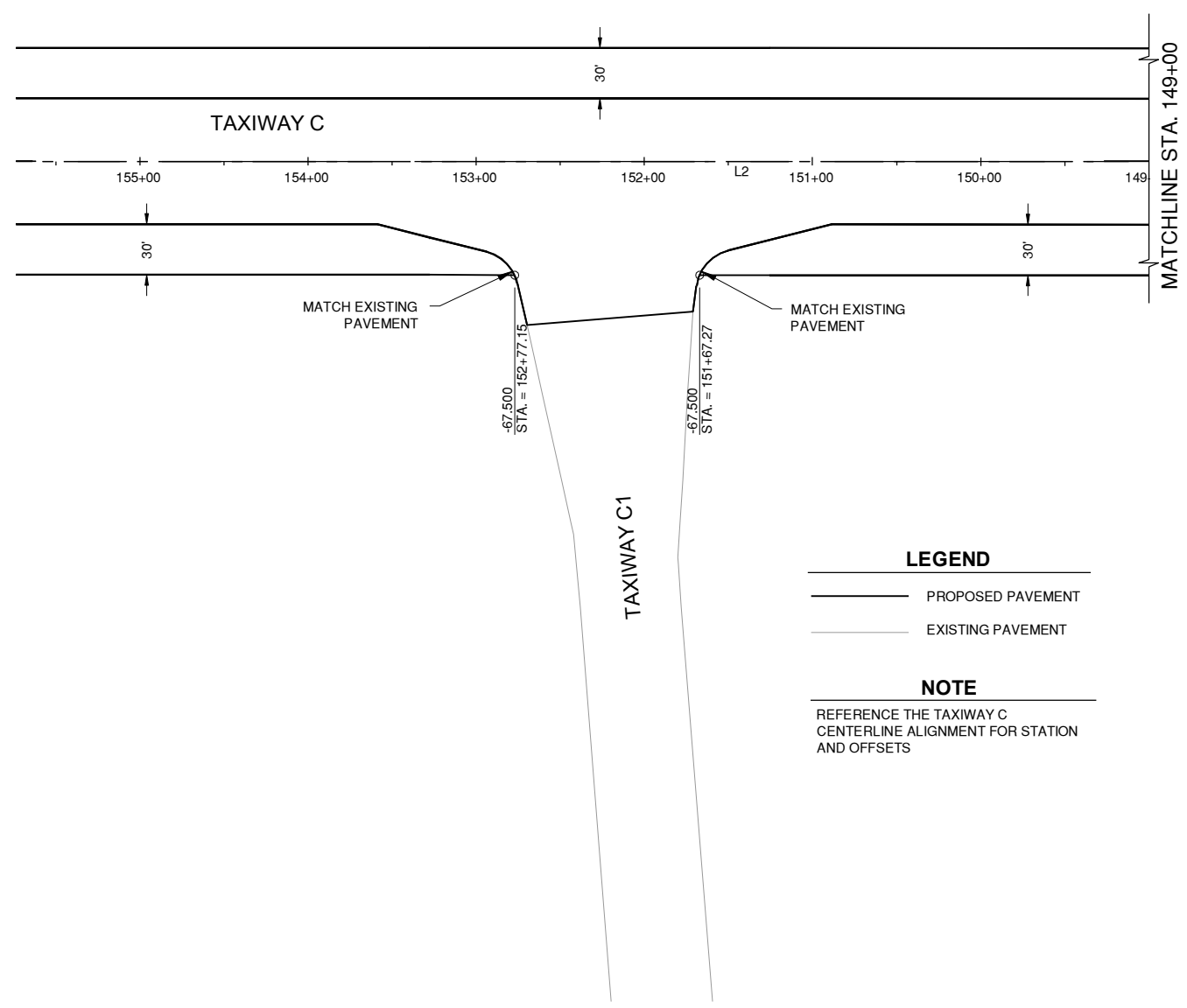
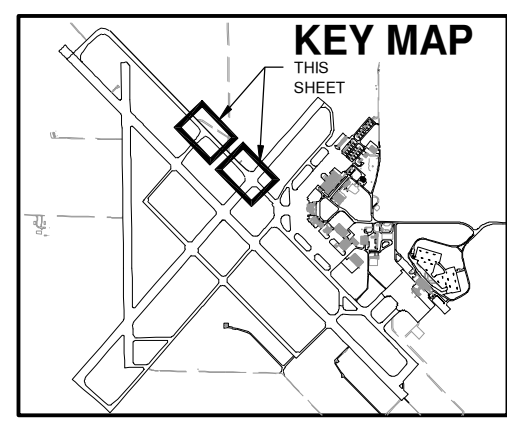


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**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PROPOSED GEOMETRY 2**



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APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO. CMI-4382 AIP PROJ. NO. 3-17-0016-XX	
SHEET 21 OF 52 SHEETS	

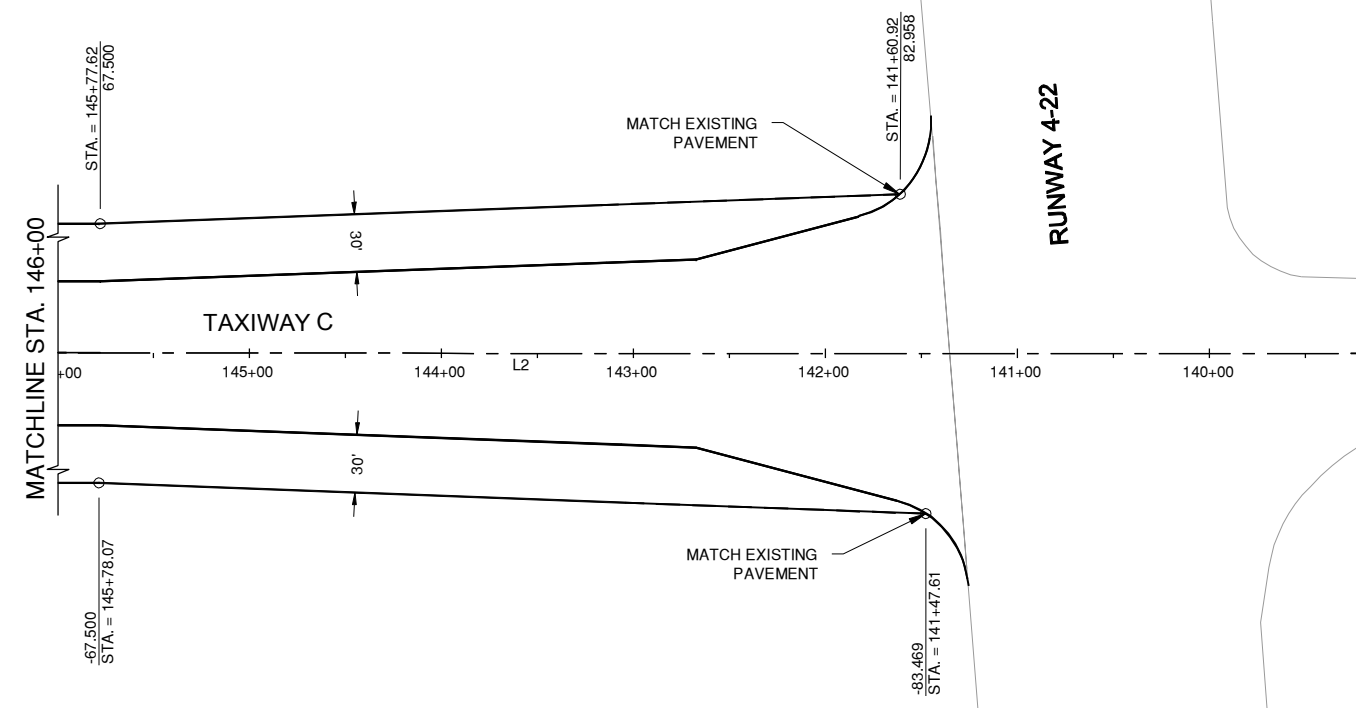
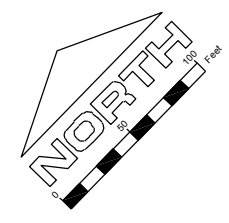


LEGEND

— PROPOSED PAVEMENT
 - - - EXISTING PAVEMENT

NOTE

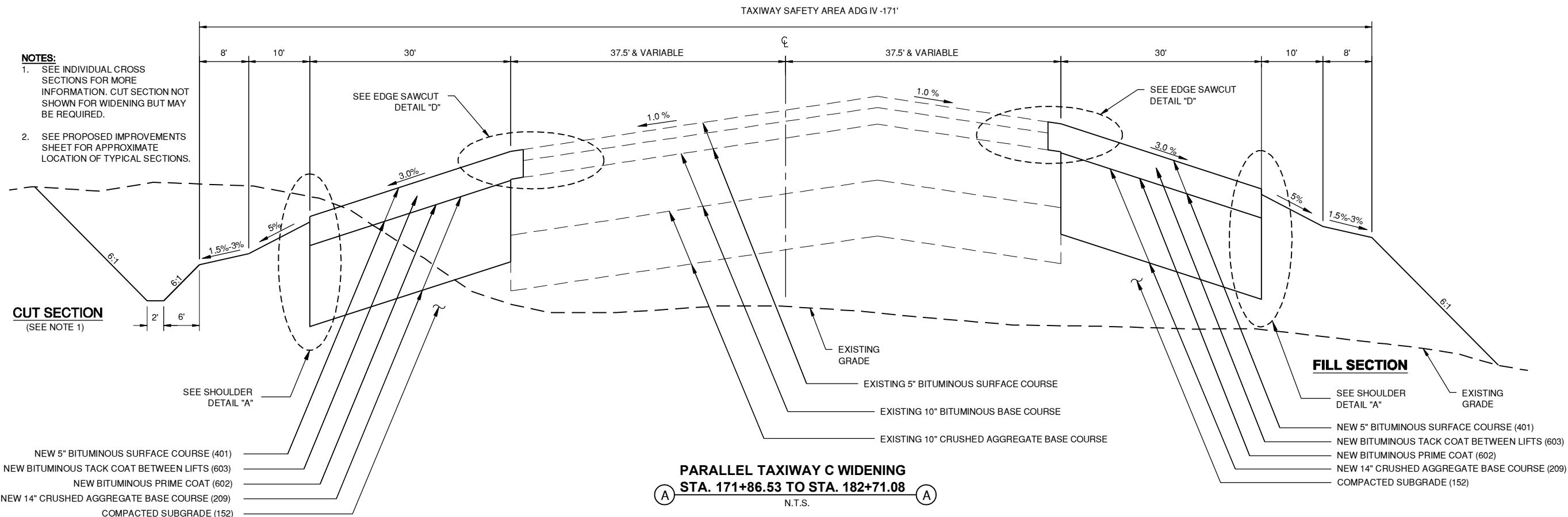
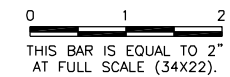
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 AND OFFSETS



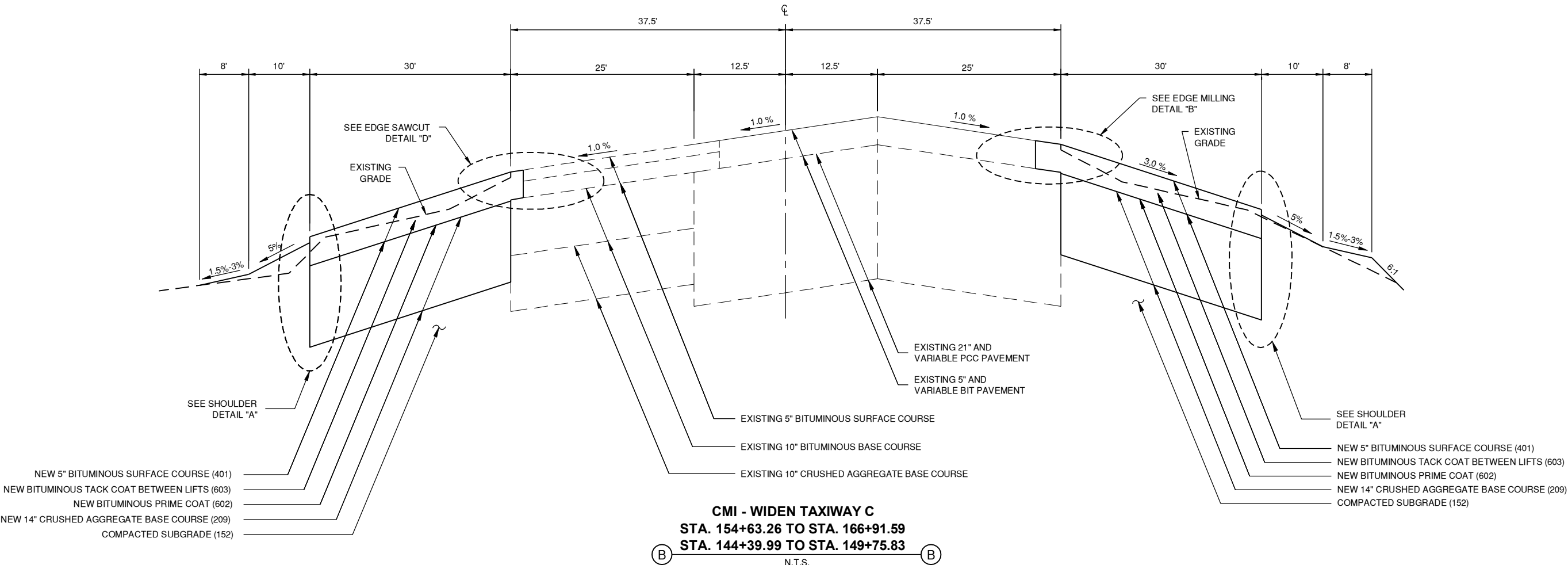
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ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L2	TAXIWAY C CENTERLINE	STA. 135+00.00	N:1228629.5661 E:1001130.5062	STA. 176+65.60	N: 1231604.3549 E: 998214.5386

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PARALLEL TAXIWAY C WIDENING
STA. 171+86.53 TO STA. 182+71.08
 N.T.S.



CMI - WIDEN TAXIWAY C
STA. 154+63.26 TO STA. 166+91.59
STA. 144+39.99 TO STA. 149+75.83
 N.T.S.

NOTES:
 1. SEE INDIVIDUAL CROSS SECTIONS FOR MORE INFORMATION. CUT SECTION NOT SHOWN FOR WIDENING BUT MAY BE REQUIRED.
 2. SEE PROPOSED IMPROVEMENTS SHEET FOR APPROXIMATE LOCATION OF TYPICAL SECTIONS.

CUT SECTION
 (SEE NOTE 1)

FILL SECTION

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CONSTRUCT TAXIWAY C PAVED SHOULDERS
TYPICAL SECTIONS 1



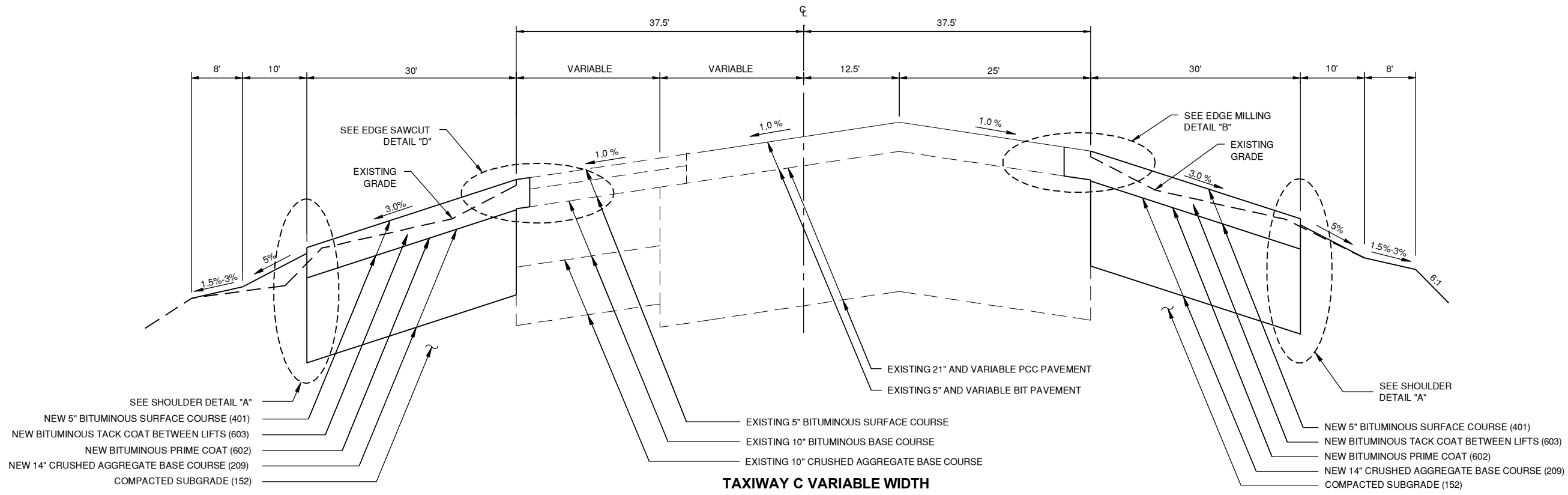
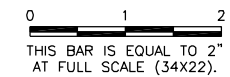
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DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO. CMI-4382	
AIP PROJ. NO. 3-17-0016-XX	
SHEET 22 OF 52 SHEETS	

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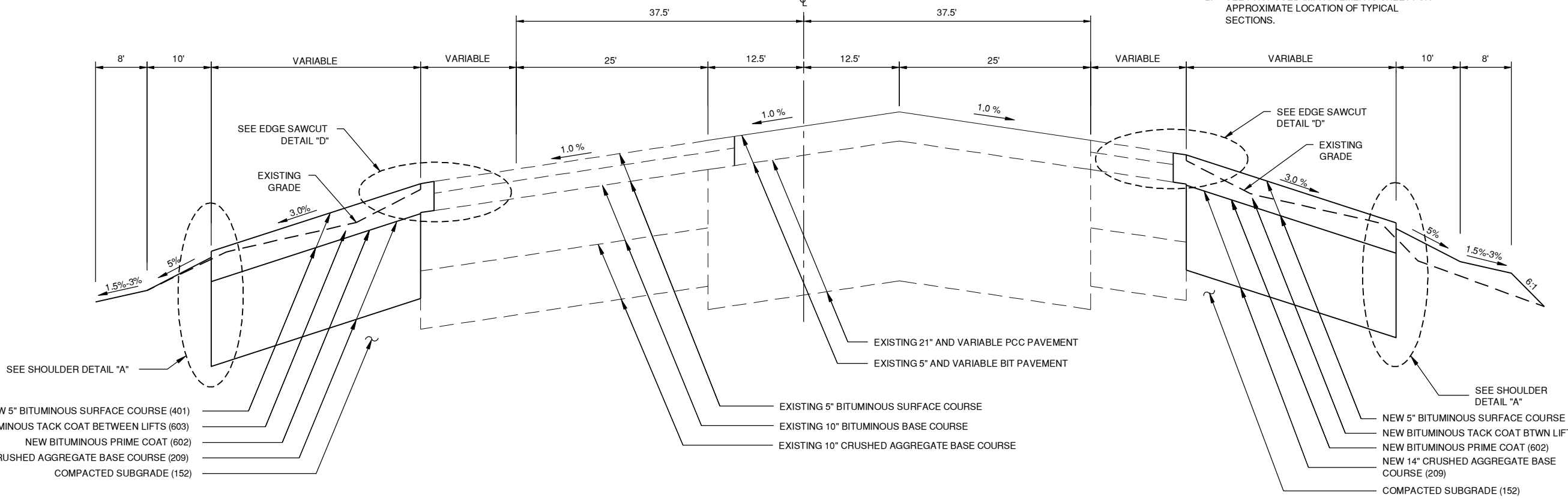


TAXIWAY C VARIABLE WIDTH
STA. 169+71.78 TO STA. 171+62.77
STA. 166+91.59 TO STA. 168+34.96
STA. 199+75.83 TO STA. 151+69.22
STA. 152+75.46 TO STA. 154+63.26
 (C) N.T.S. (C)

- SEE SHOULDER DETAIL "A"
- NEW 5" BITUMINOUS SURFACE COURSE (401)
- NEW BITUMINOUS TACK COAT BETWEEN LIFTS (603)
- NEW BITUMINOUS PRIME COAT (602)
- NEW 14" CRUSHED AGGREGATE BASE COURSE (209)
- COMPACTED SUBGRADE (152)

- SEE SHOULDER DETAIL "A"
- NEW 5" BITUMINOUS SURFACE COURSE (401)
- NEW BITUMINOUS TACK COAT BETWEEN LIFTS (603)
- NEW BITUMINOUS PRIME COAT (602)
- NEW 14" CRUSHED AGGREGATE BASE COURSE (209)
- COMPACTED SUBGRADE (152)

- NOTES**
- SEE INDIVIDUAL CROSS SECTIONS FOR MORE INFORMATION. CUT SECTION NOT SHOWN FOR WIDENING BUT MAY BE REQUIRED.
 - SEE PROPOSED IMPROVEMENTS SHEET FOR APPROXIMATE LOCATION OF TYPICAL SECTIONS.

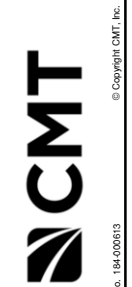


TAXIWAY C VARIABLE WIDTH BOTH SIDES
STA. 141+26.58 TO STA. 144+33.99
 (D) N.T.S. (D)

- NEW 5" BITUMINOUS SURFACE COURSE (401)
- NEW BITUMINOUS TACK COAT BETWEEN LIFTS (603)
- NEW BITUMINOUS PRIME COAT (602)
- NEW 14" CRUSHED AGGREGATE BASE COURSE (209)
- COMPACTED SUBGRADE (152)

- NEW 5" BITUMINOUS SURFACE COURSE (401)
- NEW BITUMINOUS TACK COAT BTWN LIFTS (603)
- NEW BITUMINOUS PRIME COAT (602)
- NEW 14" CRUSHED AGGREGATE BASE COURSE (209)
- COMPACTED SUBGRADE (152)

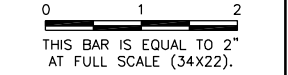
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 WILLARD AIRPORT
 SAVOY, ILLINOIS**
CONSTRUCT TAXIWAY C PAVED SHOULDERS
TYPICAL SECTIONS 2



DESIGN BY:	AJH
DRAWN BY:	ADD
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO. CMI-4382	
AIP PROJ. NO. 3-17-0016-XX	
SHEET	23 OF 52 SHEETS

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NUMBER	BY	DATE

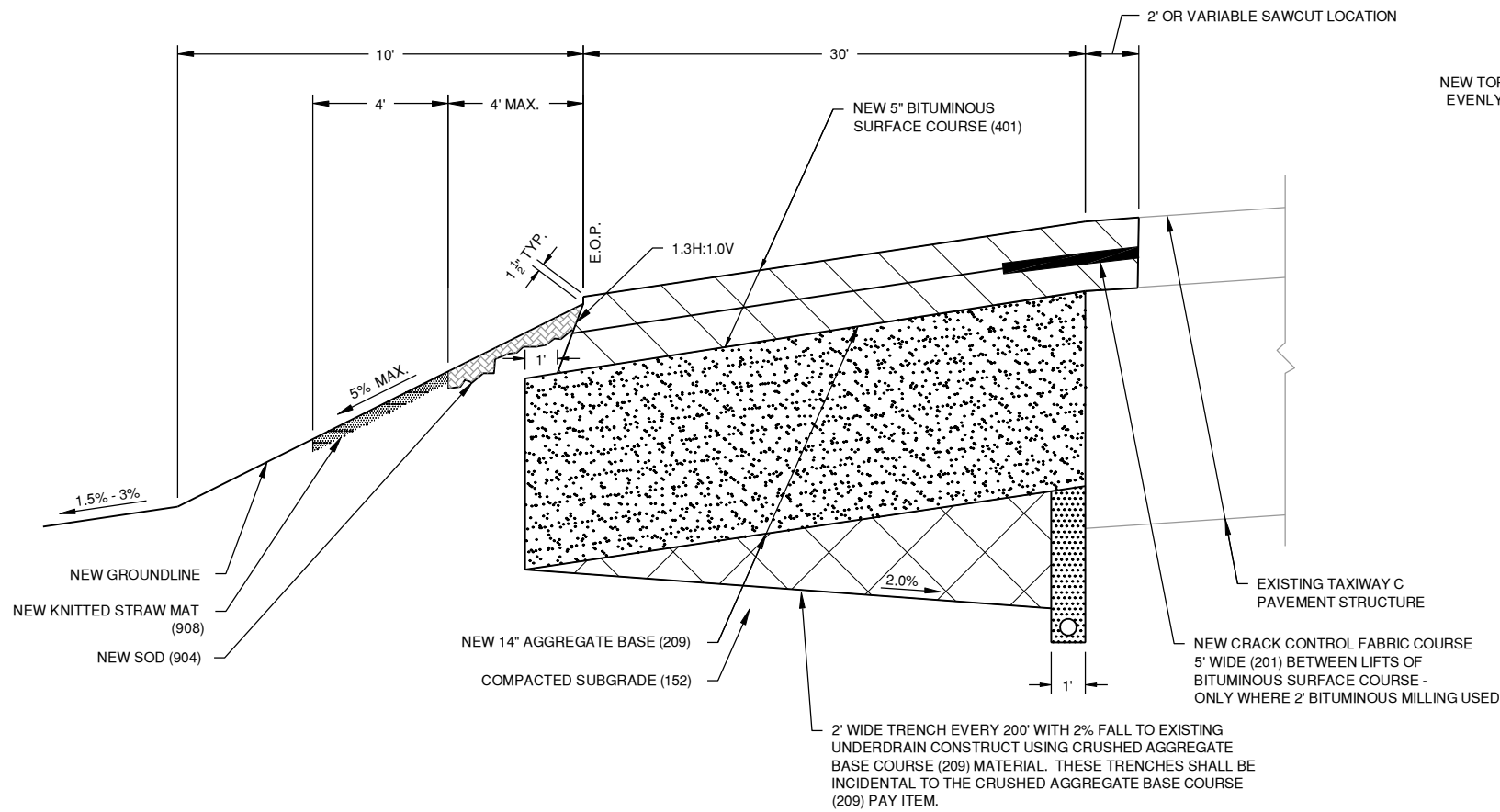


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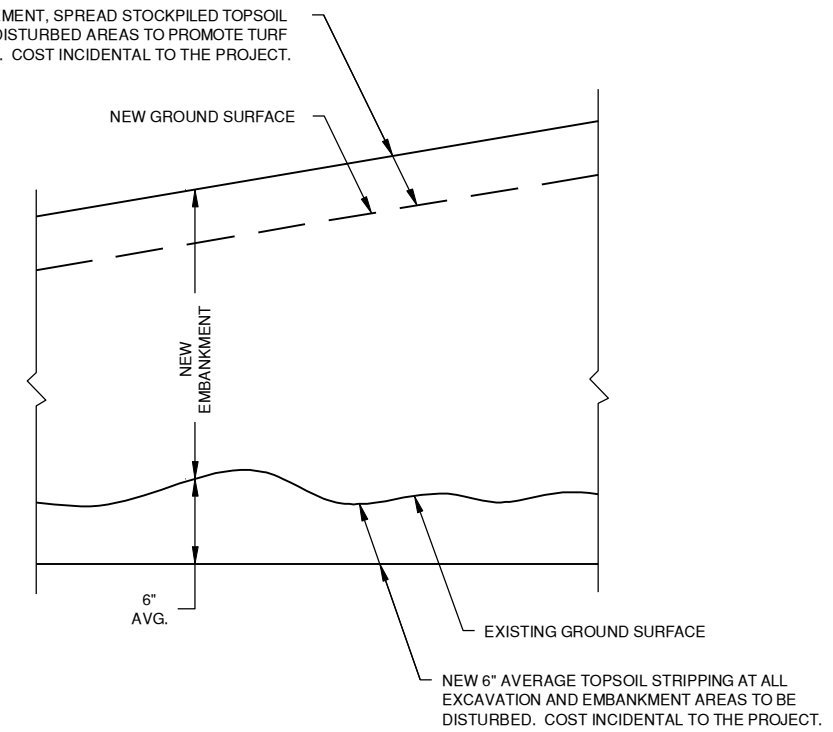
CONSTRUCT TAXIWAY C PAVED SHOULDERS
 TYPICAL SECTIONS 3



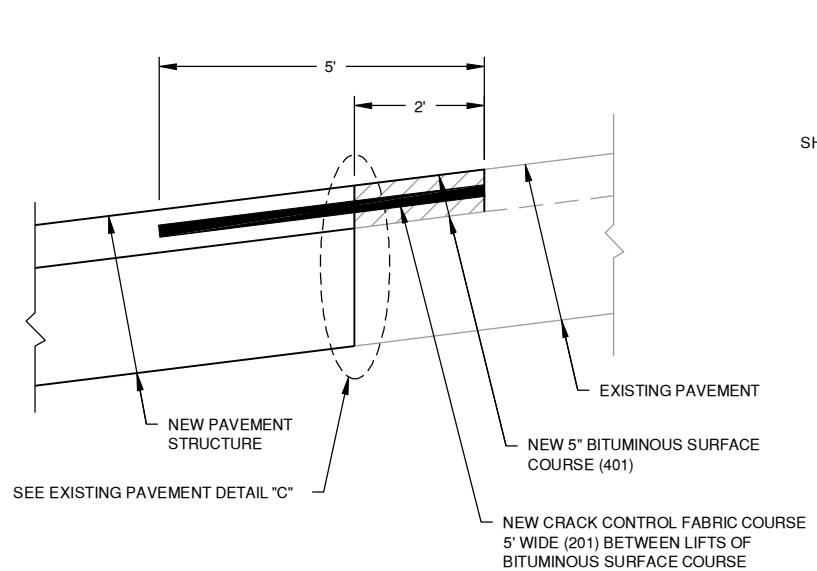
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DRAWN BY:	JLR
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APPROVED BY:	CET
DATE:	MAY 1, 2015
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AIP PROJ. NO. 3-17-0016-XX	
SHEET 24 OF 52 SHEETS	



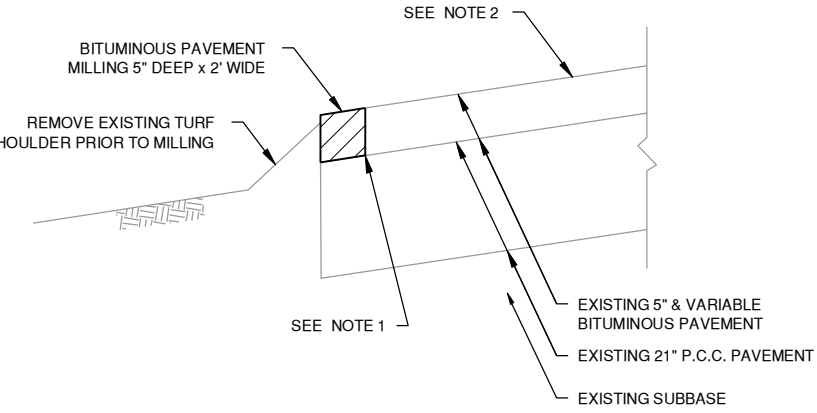
SHOULDER DETAIL "A"
 N.T.S.



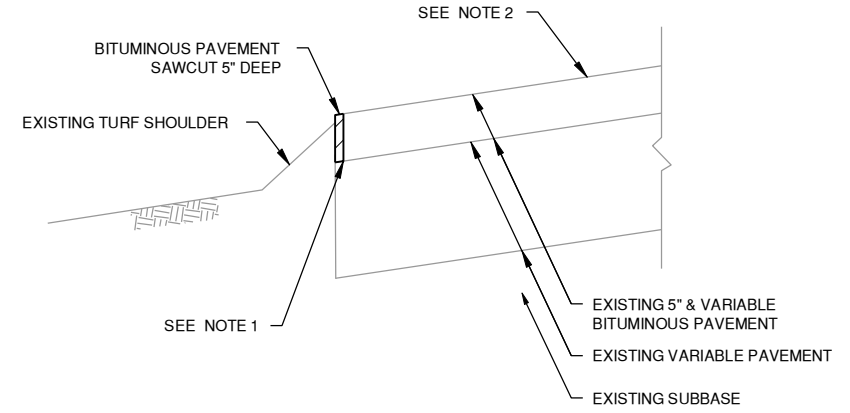
TOPSOIL STRIPPING DETAIL
 N.T.S.



EDGE MILLING DETAIL "B"
 N.T.S.



**EXISTING PAVEMENT DETAIL "C"
 EXCAVATION AND MILLING**
 N.T.S.



**EXISTING PAVEMENT DETAIL "D"
 EXCAVATION AND SAWCUT**
 N.T.S.

NOTES

- SAWCUTTING MAY BE REQUIRED AFTER MILLING TO PROVIDE CLEAN VERTICAL FACE TO ABUTMENT - SAWING IN THE MILLING AREAS SHALL BE INCIDENTAL TO ITEM AR401650 BITUMINOUS PAVEMENT MILLING. CONTRACTOR SHALL TAKE CARE NOT TO DAMAGE EDGE WITH OPERATIONS AFTER COMPLETING SAWCUT. ANY FURTHER OR ADDITIONAL DAMAGE DETERMINED BY THE R.E. SHALL BE SAWCUT AGAIN.
- CONTRACTOR SHALL TAKE PRECAUTIONS TO PRESERVE THE EXISTING LOWER P.C.C. EDGE FROM DAMAGE DURING EXCAVATION. CONTRACTOR SHALL NOT OVER EXCAVATE ADJACENT TO THE EXISTING PAVEMENT AND CAUSE UNDERMINING AT THE EXISTING PAVEMENT.
- EDGE MILLING WILL NOT BE REQUIRED WHERE NEW BITUMINOUS PAVED SHOULDERS WILL ABUT P.C.C. PAVEMENT AND AS DESIGNATED ON THE EXISTING CONDITIONS SHEETS. MILLING SHALL BE REQUIRED WHERE THE NEW PAVED SHOULDERS WILL ABUT PAVEMENT THAT IS OLDER THAN ONE (1) CALENDAR YEAR.

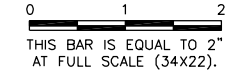
NOTES

- SAWCUT WILL BE REQUIRED TO PROVIDE CLEAN VERTICAL FACE TO ABUTMENT. THE DISTANCE IN FROM THE EXISTING EDGE SHALL BE OF SUFFICIENT WIDTH TO PROVIDE THIS VERTICAL FACE IN SOUND, FULL-DEPTH PAVEMENT. THIS DISTANCE AND SAWCUT PATH SHALL REMAIN RELATIVELY STRAIGHT AND TRUE WITHOUT JAGGED EDGES OR VARIATIONS. THIS SAWING SHALL BE PAID FOR UNDER ITEM AR401665 BITUMINOUS PAVEMENT SAWING. CONTRACTOR SHALL TAKE CARE NOT TO DAMAGE EDGE WITH OPERATIONS AFTER COMPLETING SAWCUT. ANY FURTHER OR ADDITIONAL DAMAGE DETERMINED BY THE R.E. SHALL BE SAWCUT AGAIN AT NO COST TO THE CONTRACT.
- CONTRACTOR SHALL TAKE PRECAUTIONS TO PRESERVE THE EXISTING LOWER P.C.C. EDGE FROM DAMAGE DURING EXCAVATION. CONTRACTOR SHALL NOT OVER EXCAVATE ADJACENT TO THE EXISTING PAVEMENT AND CAUSE UNDERMINING AT THE EXISTING PAVEMENT.
- SAWCUT WILL NOT BE REQUIRED WHERE NEW BITUMINOUS PAVED SHOULDERS WILL ABUT P.C.C. PAVEMENT UNLESS DETERMINED BY THE RESIDENT ENGINEER.

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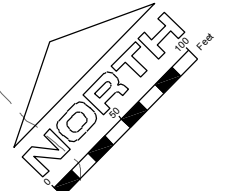
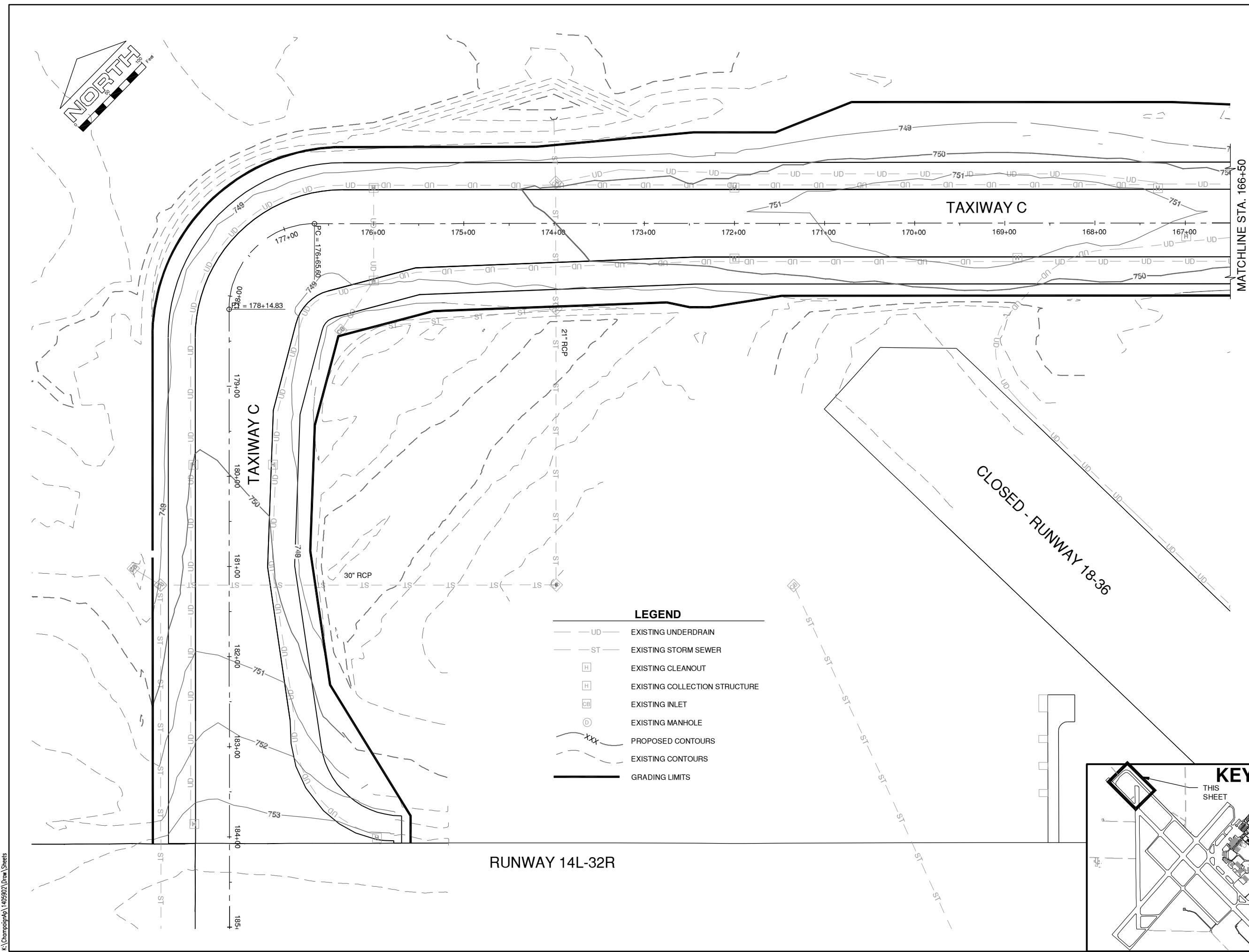
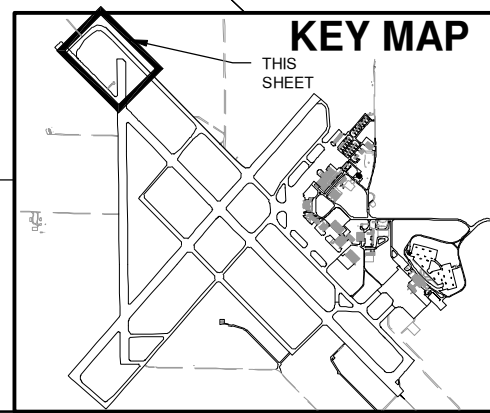


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 SAVOY, ILLINOIS

**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PROPOSED DRAINAGE AND GRADING PLAN 1**



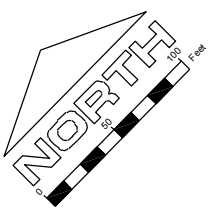
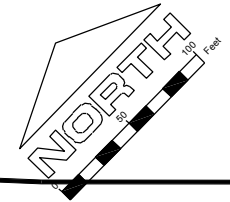
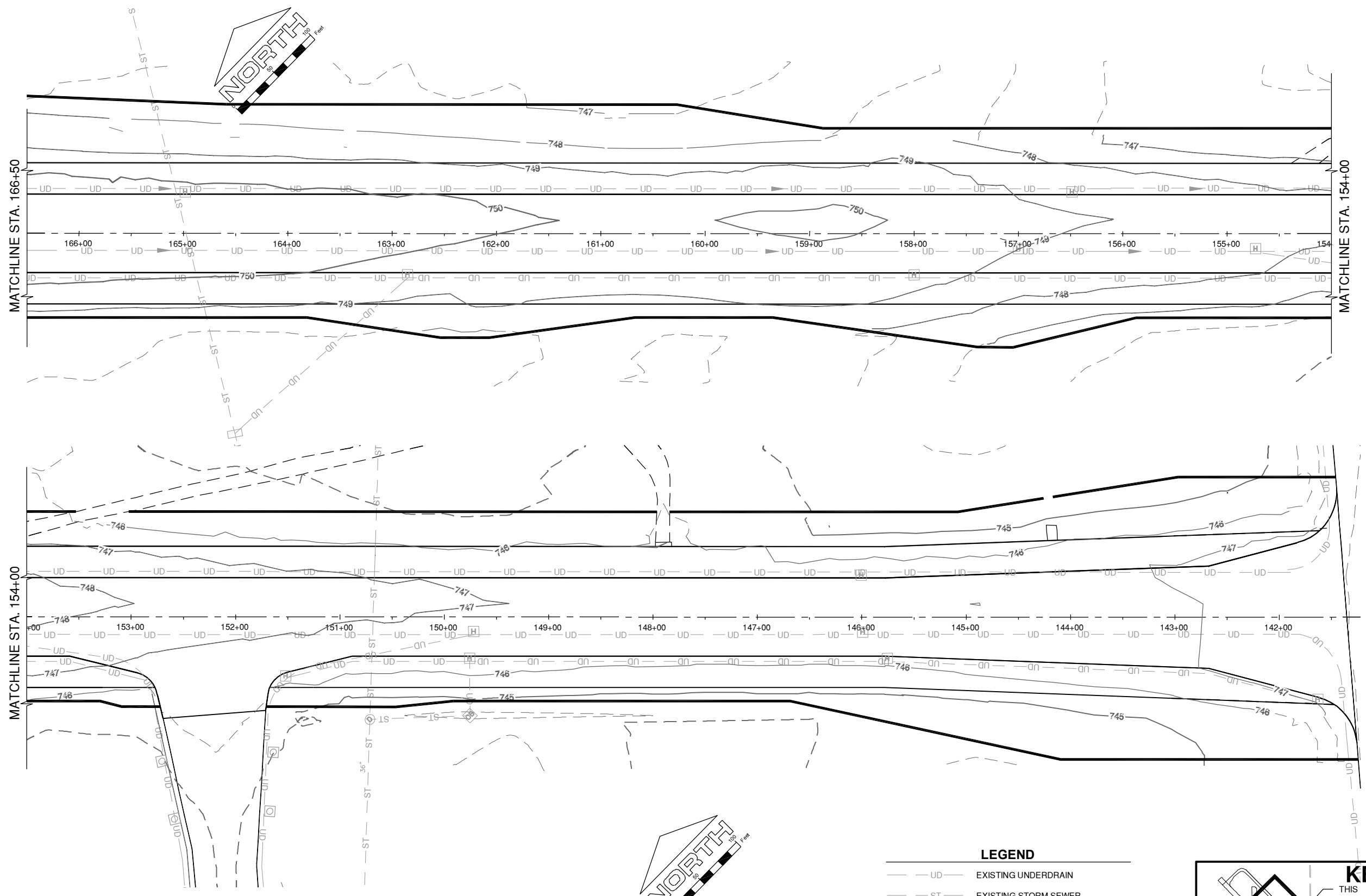
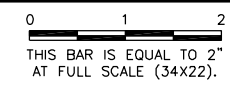
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APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL. PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET	25 OF 52 SHEETS



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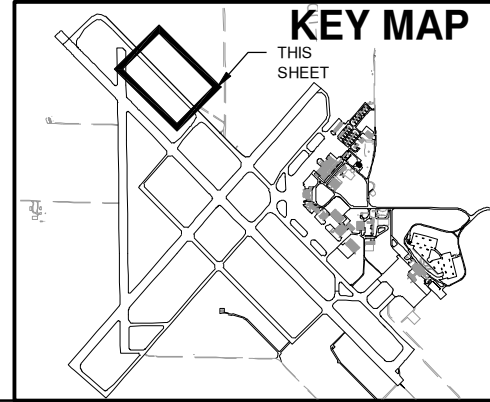
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LEGEND

— UD —	EXISTING UNDERDRAIN
— ST —	EXISTING STORM SEWER
[H]	EXISTING CLEANOUT
[H]	EXISTING COLLECTION STRUCTURE
[CB]	EXISTING INLET
[O]	EXISTING MANHOLE
XXX	PROPOSED CONTOURS
- - -	EXISTING CONTOURS
—	GRADING LIMITS



UNIVERSITY OF ILLINOIS
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 SAVOY, ILLINOIS

CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PROPOSED DRAINAGE AND GRADING PLAN 2



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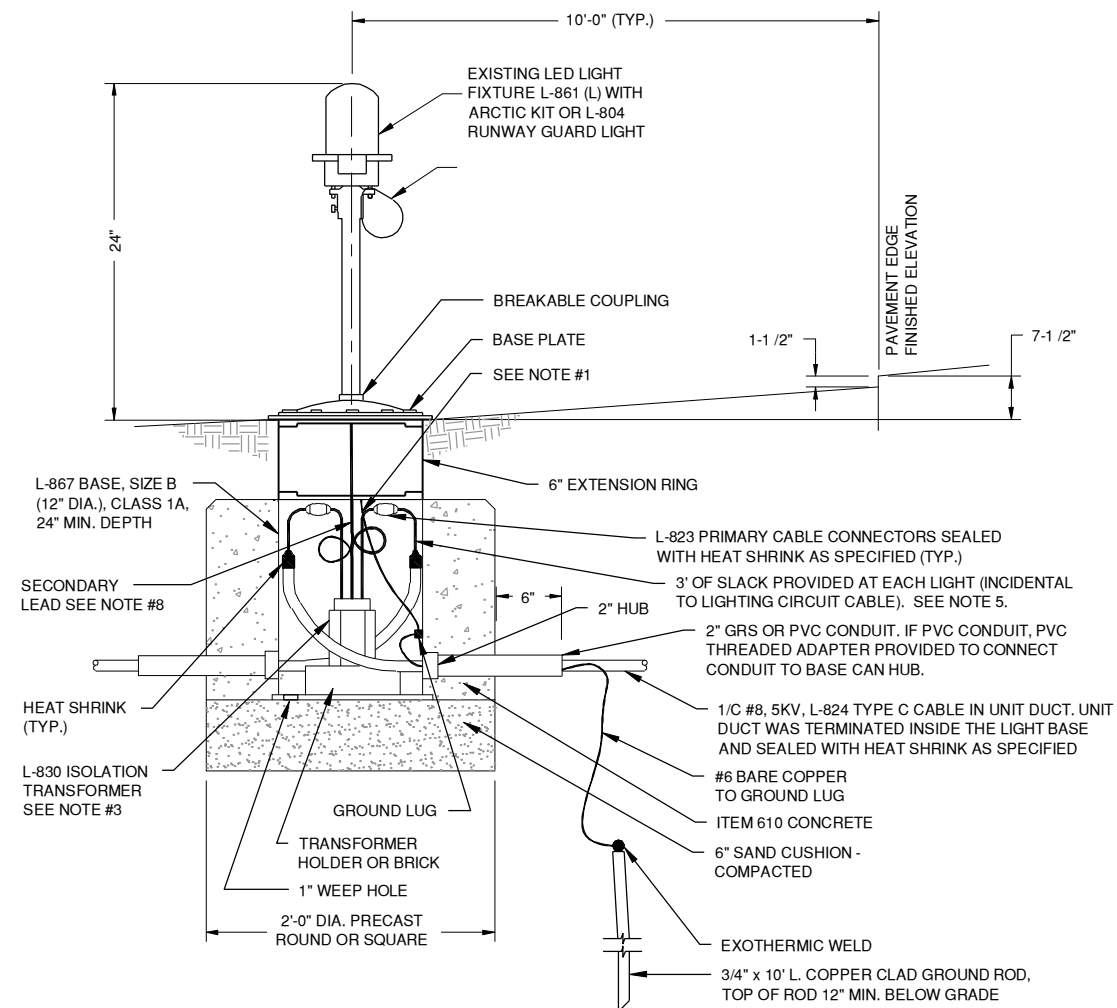
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CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL. PROJ. NO. CMI-4382	
AIP PROJ. NO. 3-17-0016-XX	
SHEET 26 OF 52 SHEETS	

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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

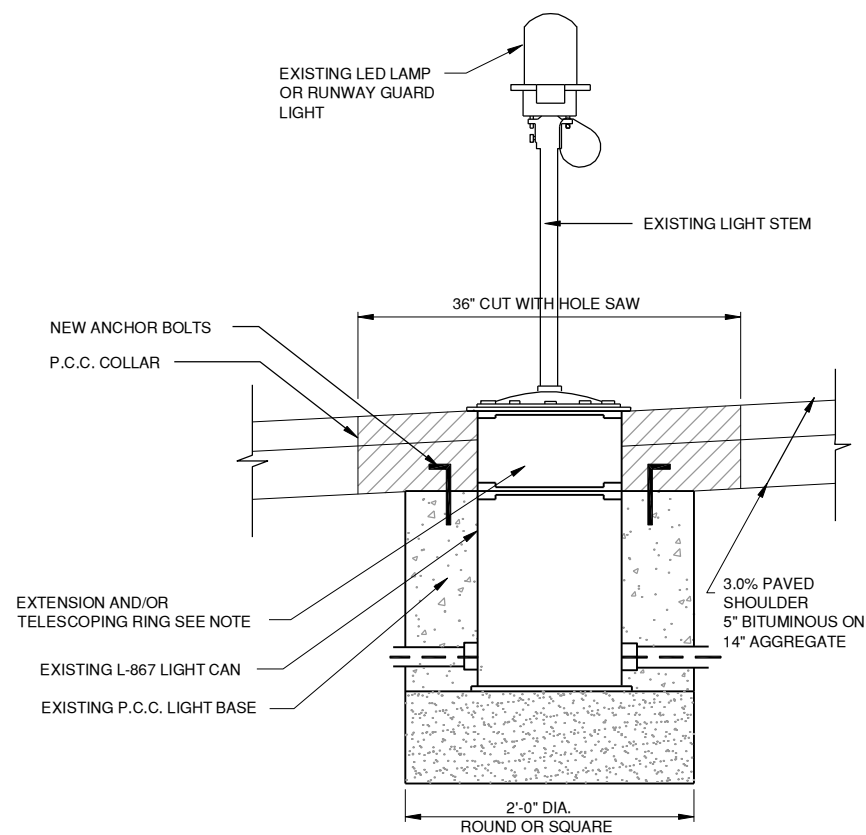


EXISTING BASE MOUNTED EDGE LIGHT INSTALLATION ALONG TAXIWAY C

N.T.S.

NOTES

1. THE LIGHT FIXTURE IS BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH IS SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR THE ATTACHMENT METHOD OF THIS BONDING WIRE.
2. LIGHT FIXTURES ARE LED, TYPE L-861T (L), WITH HEATER.
3. LED EDGE LIGHTS WITH HEATERS ARE L-830-17, 20/25 WATT OR L-830-1, 30/45 WATT, AS RECOMMENDED BY LIGHT FIXTURE MANUFACTURER.
4. ENTRANCES INTO L-867 BASES ARE PLUGGED FROM THE INSIDE WITH DUCT SEAL.
5. THE TOLERANCE FOR THE HEIGHT OF RUNWAY/TAXIWAY EDGE LIGHTS IS ± 1 INCH. THE TOLERANCE FOR THE LATERAL SPACING (LIGHT LANE TO RUNWAY/TAXIWAY CENTERLINE) OF RUNWAY/TAXIWAY EDGE LIGHTS IS ± 1 INCH.
6. DIRECTION OF PRIMARY CABLES IS IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING LIGHT WITH BACK FACING PAVEMENT, CABLE TO THE LEFT IS CODED RED AND CABLE TO THE RIGHT IS CODED BLUE.
7. A CORROSION INHIBITING, ANTI-SEIZE COMPOUND WAS USED ON ALL SCREWS, NUTS AND FRANGIBLE COUPLING THREADS. COATED BOLTS PER ENGINEERING BRIEF #83, DO NOT HAVE ANTI-SEIZE COMPOUND.
8. ELECTRICAL INSULATING GREASE WAS APPLIED WITHIN THE L-830 ISOLATION TRANSFORMER SECONDARY TWO CONDUCTOR CONNECTORS TO PREVENT WATER ENTRANCE. THE CONNECTORS WERE NOT BE TAPED.



BASE MOUNTED LIGHT INSTALLATION AFTER COMPLETION OF PAVED SHOULDER

N.T.S.

NOTES

1. PRIOR TO PAVING REMOVE LIGHT STEM AND STORE FOR FUTURE USE. REMOVE 6" EXTENSION RING AND REPLACE WITH MUD PLATE AND RECORD THE LOCATION OF THE LIGHT BASE.
2. AFTER PAVING, LOCATE BASES AND CUT 4" CORE THROUGH BITUMINOUS PAVEMENT AND LOCATE CENTER OF LIGHT CAN.
3. USE EXTREME CAUTION TO PROTECT THE NEWLY CONSTRUCTED BITUMINOUS SHOULDER SURFACE FROM ANY DAMAGE FROM EQUIPMENT WHILE CORING AND CONSTRUCTING NEW COLLAR.
4. CUT 36" WITH HOLE SAW, CENTERED AT CENTER OF LIGHT BASE.
5. INSTALL MINIMUM 4 ANCHOR BOLTS IN EXISTING LIGHT BASE.
6. VERIFY ELEVATION FROM LIGHT BASE TO TOP AT PAVED SHOULDER AND INSTALL EXTENSION RING(S) OR TELESCOPING ADJUSTMENTS PIECE TO MATCH PAVEMENT ELEVATION.
7. POUR P.C.C. COLLAR AROUND LIGHT BASE AND MATCH ELEVATION.
8. REINSTALL LIGHT STEMS.

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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 ELECTRICAL DETAILS 1

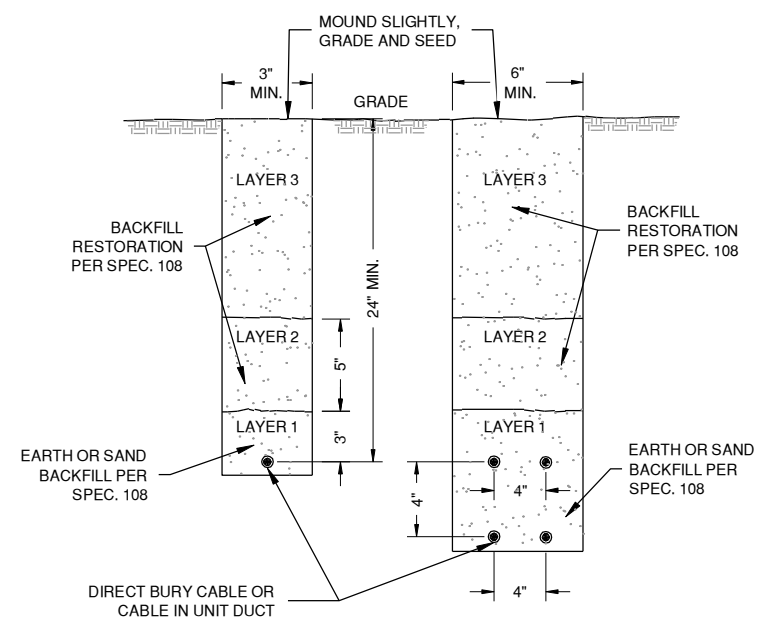
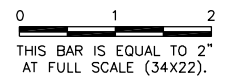


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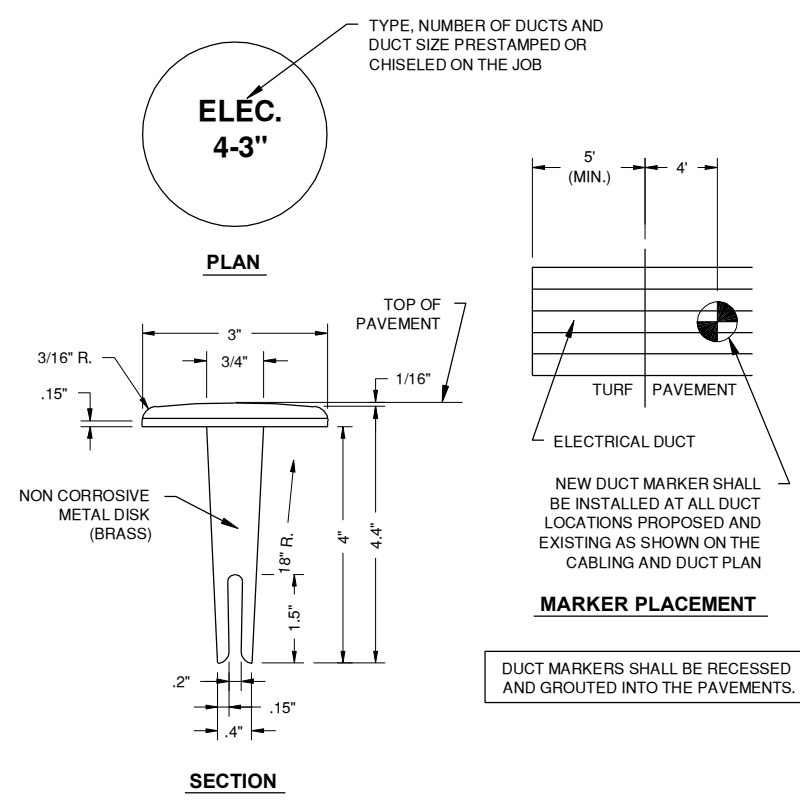


CABLE TRENCH DETAIL
N.T.S.

NOTES

1. CABLES SHALL NOT BE PLACED LESS THAN 24" DEEP IN ANY ONE TRENCH UNLESS PERMITTED BY ENGINEER.
2. WHERE PERMITTED, CONTRACTOR MAY INSTALL CABLE IN UNIT DUCT BY PLOWING METHOD.

NEW DUCT MARKERS SHALL BE INSTALLED AT BOTH EDGES OF PAVEMENT AT ALL NEW DIRECTIONAL BORE LOCATIONS. THE INSTALLATION OF THE DUCT MARKERS SHALL BE INCIDENTAL TO THE DUCT BEING INSTALLED.



IN PAVEMENT DUCT MARKER DETAIL
N.T.S.

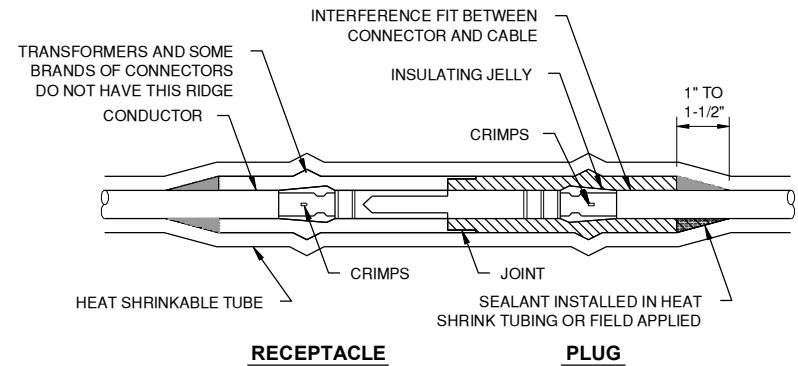
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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 ELECTRICAL DETAILS 2



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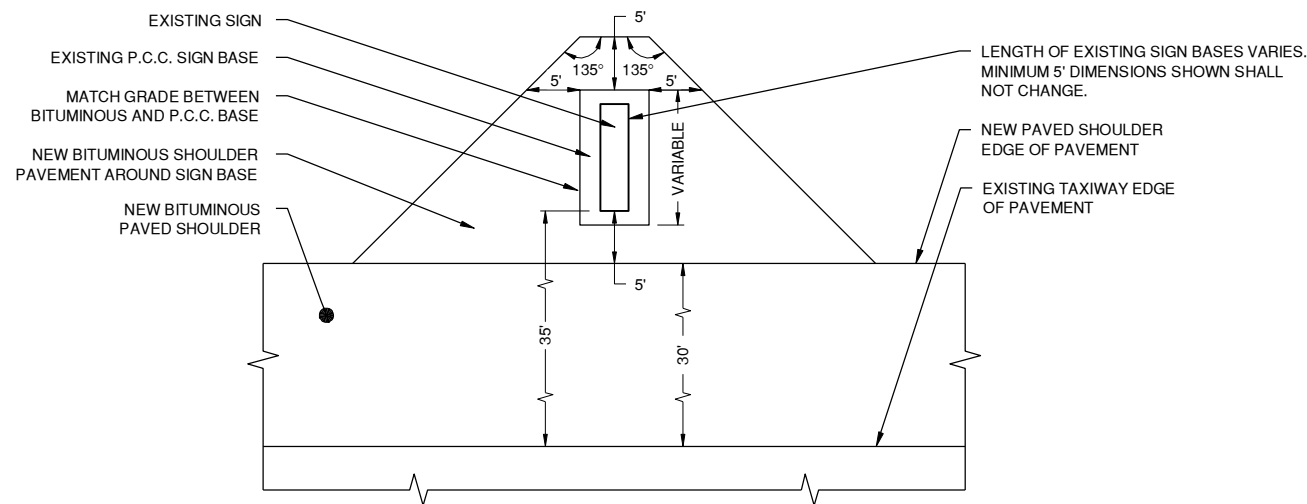
RECEPTACLE PLUG

CABLE CONNECTION DETAIL

N.T.S.

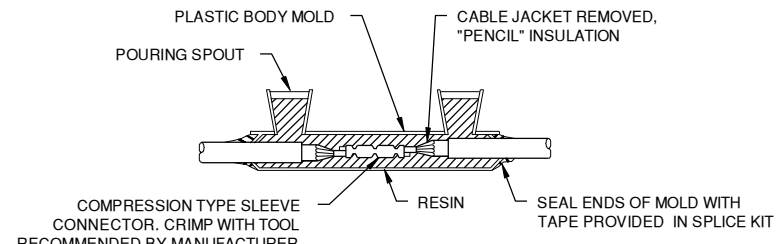
INSTALLATION INSTRUCTIONS TO SUPPLEMENT THE MANUFACTURER'S INSTRUCTIONS

- CLEAN THE CABLE THOROUGHLY 9" MIN. FROM THE END.
- REMOVE INSULATION PER MANUFACTURER'S INSTRUCTIONS. DO NOT NICK THE CONDUCTOR. DO NOT PENCIL INSULATION ON L-824 TYPE C CABLE.
- INSTALL PIN AND/OR RECEPTICAL WITH CRIMPING TOOL WHICH MUST BE COMPLETELY CLOSED BEFORE THE TOOL MAY BE REMOVED.
- BE SURE CABLE AND CONDUCTOR FITTINGS ARE CLEAN. COAT THE CABLE INSULATION WITH INSULATION JELLY FROM THE CONNECTOR.
- CAREFULLY INSERT CABLE INTO CONNECTOR TO THE PROPER DEPTH.
- SLIP 14 INCH LENGTH OF HEAT SHRINK TUBING ON TRANSFORMER LEAD RAYCHEM TCS-13-14-U OR APPROVED EQUAL.
- COMPLETE CONNECTION BY MATING THE PLUG AND RECEPTICAL. **CAUTION** BE SURE THE CABLE DOES NOT SLIP WHEN THE CONNECTION IS MADE.
- APPLY RUBBER TAPE AND PLASTIC TAPE, ONE HALF LAPPED 1-1/2" ON EACH SIDE OF JOINT.
- ANY CONNECTOR WHICH IS CONTAMINATED BY DIRT OR OTHER DELETERIOUS MATERIAL SHALL BE REMOVED NOT REINSTALLED.
- CLEAN CONNECTOR AND CABLE INSULATION WITH WAX OR GREASE SOLVENT TO REMOVE SURFACE SILICONE JELLY.
- WRAP SEALANT SECURELY AROUND THE CABLE. INSULATION TO EXTEND 1-1/2" BEYOND BOTH ENDS OF CONNECTORS. SEALANT SHALL BE RAYCHEM S-1052 (STRIPS) OR APPROVED EQUAL.
- CENTER HEAT SHRINK OVER THE CONNECTOR. APPLY HEAT EVENLY BEGINNING AT THE CENTER AND WORKING AROUND CABLE TO ENDS. THERMOCHROMIC PAINT SHALL SHOW PROPER HEAT HAS BEEN USED. *** DO NOT OVER HEAT ***.
- THE HEAT SOURCE SHALL BE AN ELECTRIC HEAT GUN OR A PROPANE WITH FLAME SPREADER.



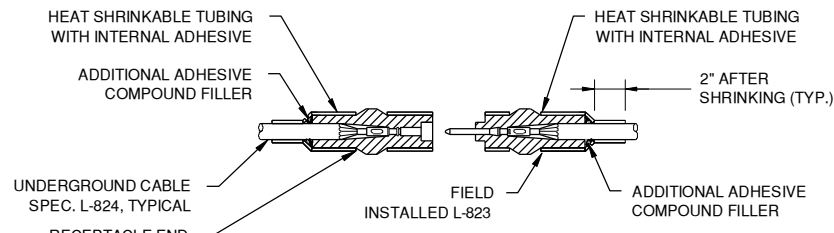
EXISTING SIGN IN NEW PAVED SHOULDER

N.T.S.



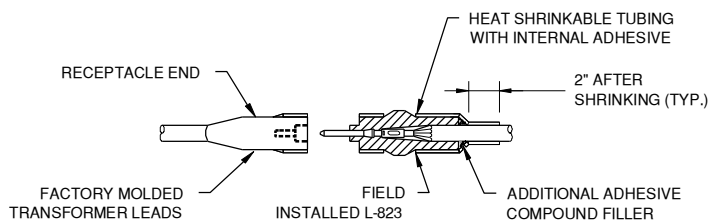
TYPE A

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLE ONLY



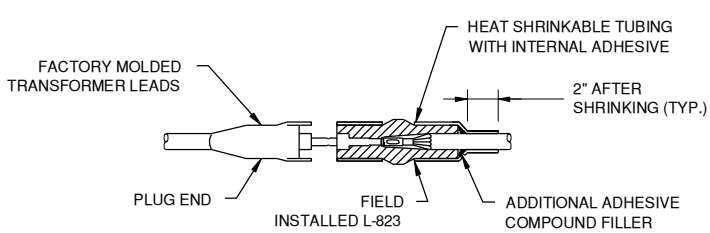
TYPE B

NOT TO BE USED UNLESS DIRECTED BY THE ENGINEER



TYPE C

FOR SPLICES AT RUNWAY LIGHTS, TAXIWAY LIGHT AND SIGNS



TYPE D

FOR SPLICES AT RUNWAY LIGHTS, TAXIWAY LIGHT AND SIGNS

CABLE CONNECTOR TYPES

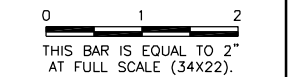
N.T.S.

NOTES

- MATCH THE OUTSIDE DIAMETER OF CABLE INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.

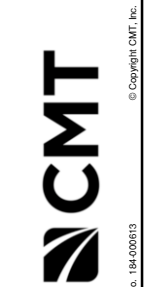
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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 ELECTRICAL DETAILS 3

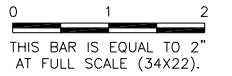


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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 LOCALIZER CABLING PLAN



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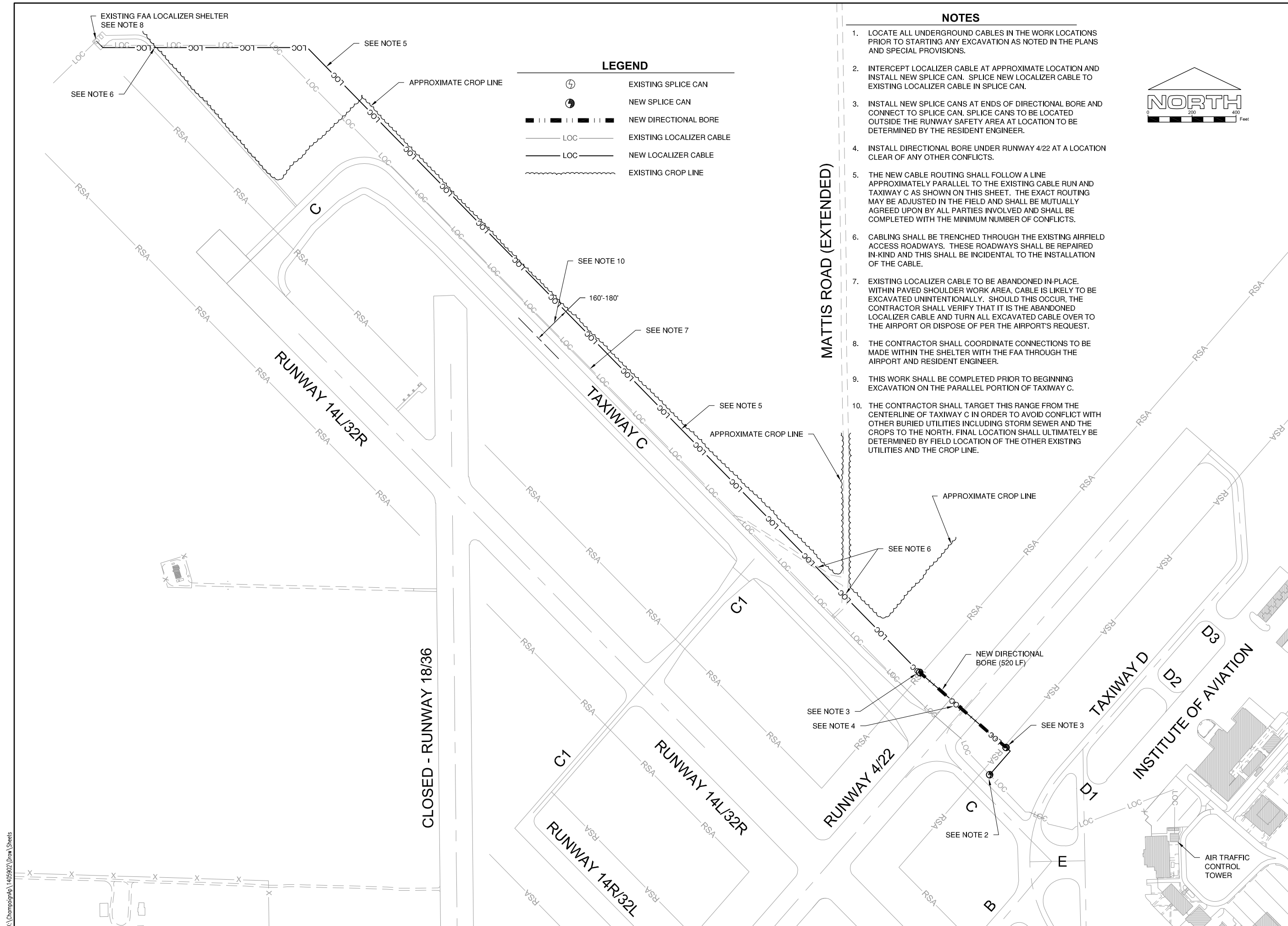
NOTES

- LOCATE ALL UNDERGROUND CABLES IN THE WORK LOCATIONS PRIOR TO STARTING ANY EXCAVATION AS NOTED IN THE PLANS AND SPECIAL PROVISIONS.
- INTERCEPT LOCALIZER CABLE AT APPROXIMATE LOCATION AND INSTALL NEW SPLICE CAN. SPLICE NEW LOCALIZER CABLE TO EXISTING LOCALIZER CABLE IN SPLICE CAN.
- INSTALL NEW SPLICE CANS AT ENDS OF DIRECTIONAL BORE AND CONNECT TO SPLICE CAN. SPLICE CANS TO BE LOCATED OUTSIDE THE RUNWAY SAFETY AREA AT LOCATION TO BE DETERMINED BY THE RESIDENT ENGINEER.
- INSTALL DIRECTIONAL BORE UNDER RUNWAY 4/22 AT A LOCATION CLEAR OF ANY OTHER CONFLICTS.
- THE NEW CABLE ROUTING SHALL FOLLOW A LINE APPROXIMATELY PARALLEL TO THE EXISTING CABLE RUN AND TAXIWAY C AS SHOWN ON THIS SHEET. THE EXACT ROUTING MAY BE ADJUSTED IN THE FIELD AND SHALL BE MUTUALLY AGREED UPON BY ALL PARTIES INVOLVED AND SHALL BE COMPLETED WITH THE MINIMUM NUMBER OF CONFLICTS.
- CABLING SHALL BE TRENCHED THROUGH THE EXISTING AIRFIELD ACCESS ROADWAYS. THESE ROADWAYS SHALL BE REPAIRED IN-KIND AND THIS SHALL BE INCIDENTAL TO THE INSTALLATION OF THE CABLE.
- EXISTING LOCALIZER CABLE TO BE ABANDONED IN-PLACE. WITHIN PAVED SHOULDER WORK AREA, CABLE IS LIKELY TO BE EXCAVATED UNINTENTIONALLY. SHOULD THIS OCCUR, THE CONTRACTOR SHALL VERIFY THAT IT IS THE ABANDONED LOCALIZER CABLE AND TURN ALL EXCAVATED CABLE OVER TO THE AIRPORT OR DISPOSE OF PER THE AIRPORT'S REQUEST.
- THE CONTRACTOR SHALL COORDINATE CONNECTIONS TO BE MADE WITHIN THE SHELTER WITH THE FAA THROUGH THE AIRPORT AND RESIDENT ENGINEER.
- THIS WORK SHALL BE COMPLETED PRIOR TO BEGINNING EXCAVATION ON THE PARALLEL PORTION OF TAXIWAY C.
- THE CONTRACTOR SHALL TARGET THIS RANGE FROM THE CENTERLINE OF TAXIWAY C IN ORDER TO AVOID CONFLICT WITH OTHER BURIED UTILITIES INCLUDING STORM SEWER AND THE CROPS TO THE NORTH. FINAL LOCATION SHALL ULTIMATELY BE DETERMINED BY FIELD LOCATION OF THE OTHER EXISTING UTILITIES AND THE CROP LINE.

LEGEND

- EXISTING SPLICE CAN
- NEW SPLICE CAN
- NEW DIRECTIONAL BORE
- EXISTING LOCALIZER CABLE
- NEW LOCALIZER CABLE
- EXISTING CROP LINE

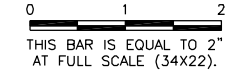
MATTIS ROAD (EXTENDED)



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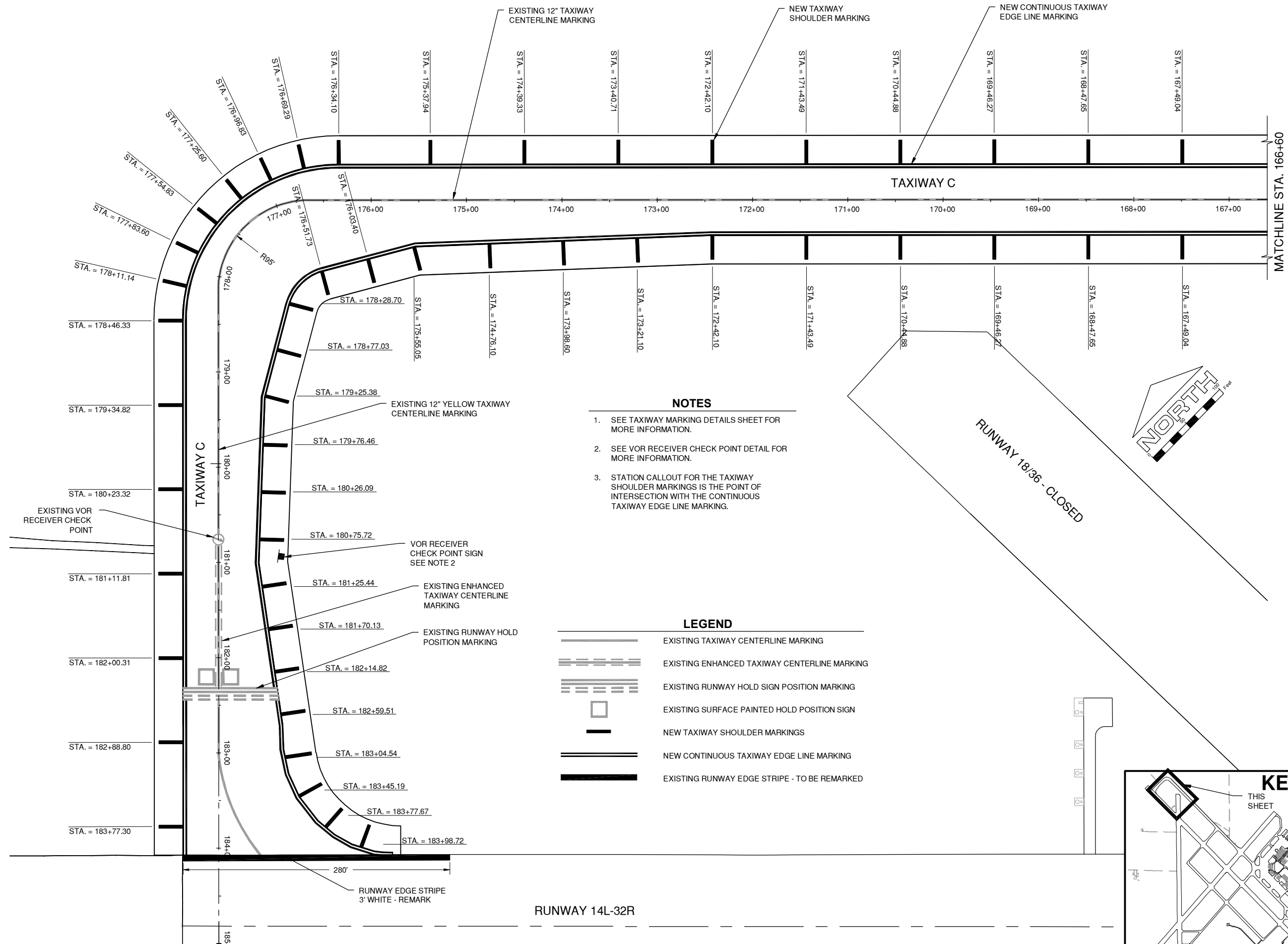
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**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PROPOSED MARKING IMPROVEMENTS 1**



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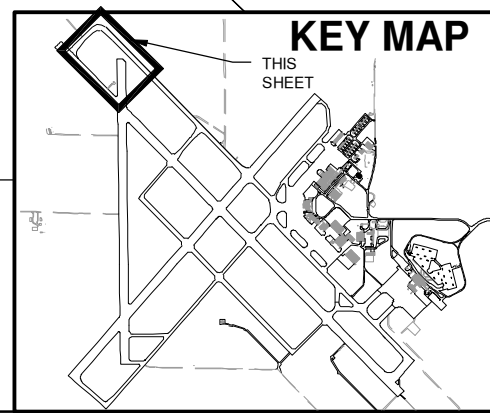


NOTES

- SEE TAXIWAY MARKING DETAILS SHEET FOR MORE INFORMATION.
- SEE VOR RECEIVER CHECK POINT DETAIL FOR MORE INFORMATION.
- STATION CALLOUT FOR THE TAXIWAY SHOULDER MARKINGS IS THE POINT OF INTERSECTION WITH THE CONTINUOUS TAXIWAY EDGE LINE MARKING.

LEGEND

- EXISTING TAXIWAY CENTERLINE MARKING
- EXISTING ENHANCED TAXIWAY CENTERLINE MARKING
- EXISTING RUNWAY HOLD SIGN POSITION MARKING
- EXISTING SURFACE PAINTED HOLD POSITION SIGN
- NEW TAXIWAY SHOULDER MARKINGS
- NEW CONTINUOUS TAXIWAY EDGE LINE MARKING
- EXISTING RUNWAY EDGE STRIPE - TO BE REMARKED

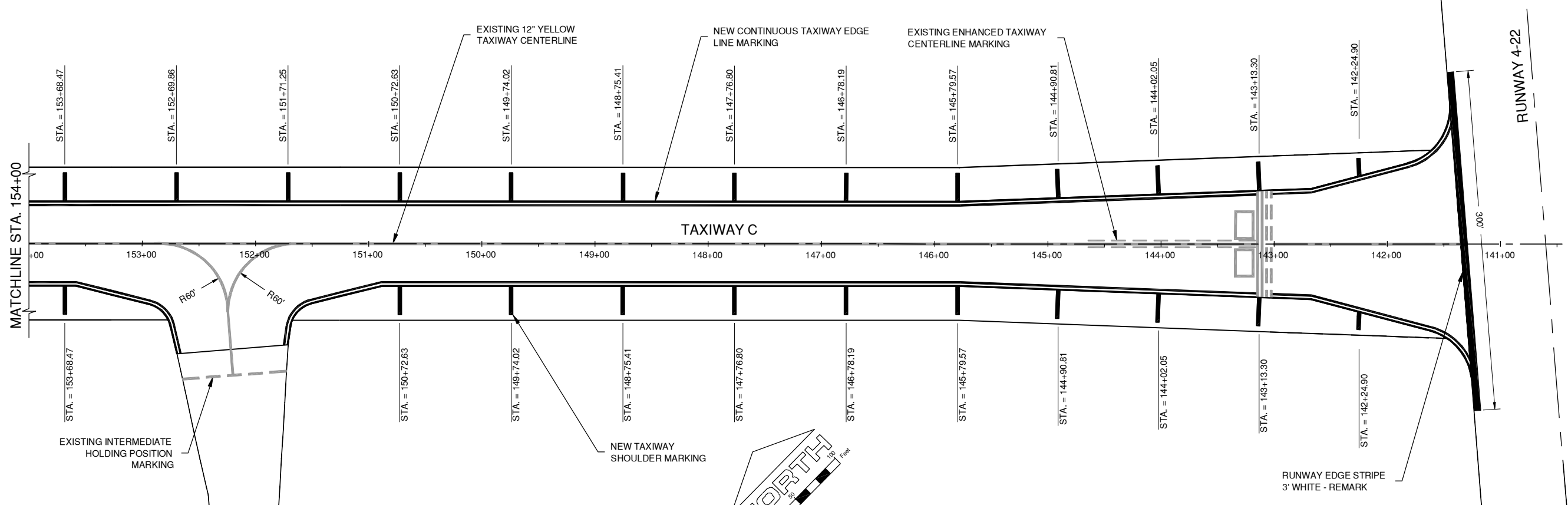
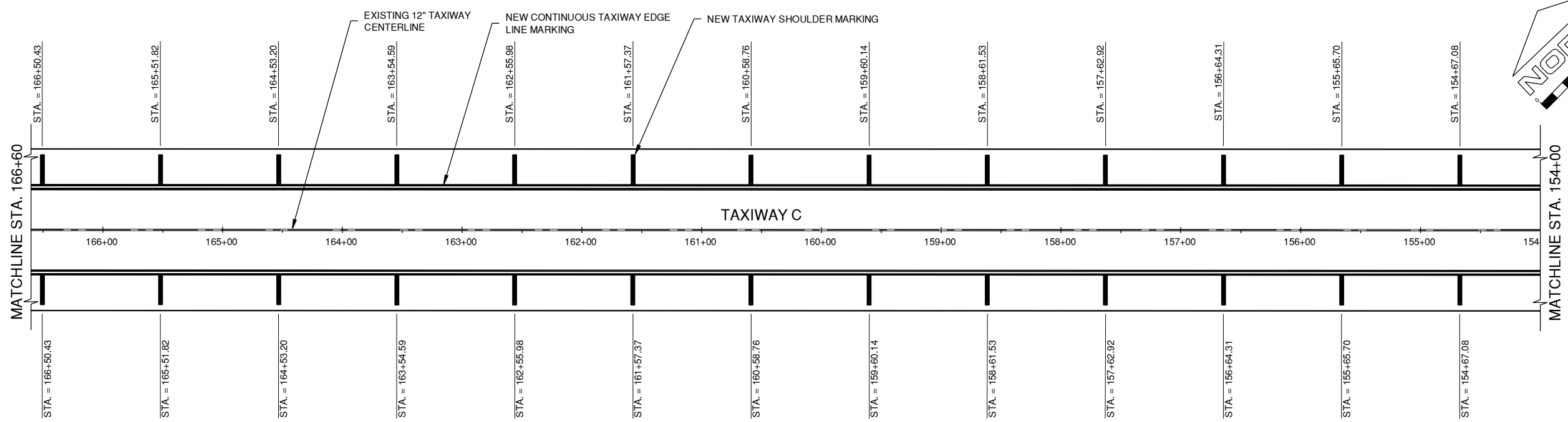
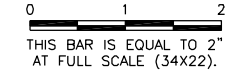


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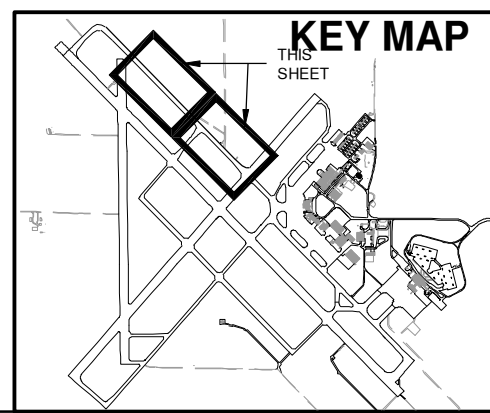
EXISTING INTERMEDIATE HOLDING POSITION MARKING

LEGEND

	EXISTING TAXIWAY CENTERLINE MARKING
	EXISTING ENHANCED TAXIWAY CENTERLINE MARKING
	EXISTING RUNWAY HOLD SIGN POSITION MARKING
	EXISTING INTERMEDIATE HOLDING POSITION SIGN
	EXISTING SURFACE PAINTED HOLD POSITION SIGN
	NEW TAXIWAY SHOULDER MARKINGS
	NEW CONTINUOUS TAXIWAY EDGE LINE MARKING
	EXISTING RUNWAY EDGE STRIPE - TO BE REMARKED

NOTES

- SEE TAXIWAY MARKINGS DETAILS SHEET FOR MORE INFORMATION.
- STATION LABELS FOR THE TAXIWAY SHOULDER MARKINGS IS THE POINT OF INTERSECTION WITH THE CONTINUOUS TAXIWAY EDGE LINE MARKING.



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**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PROPOSED MARKING IMPROVEMENTS 2**



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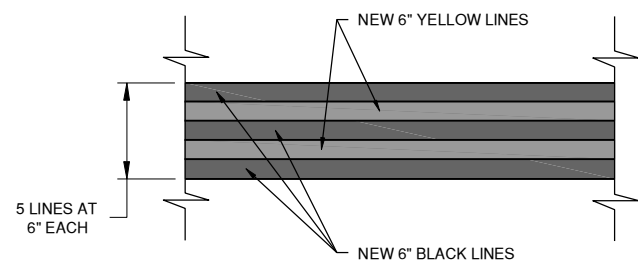
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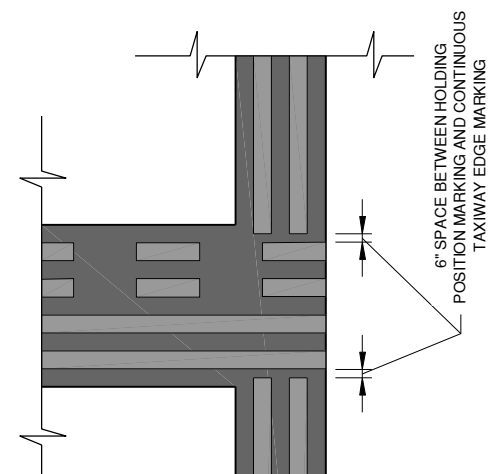
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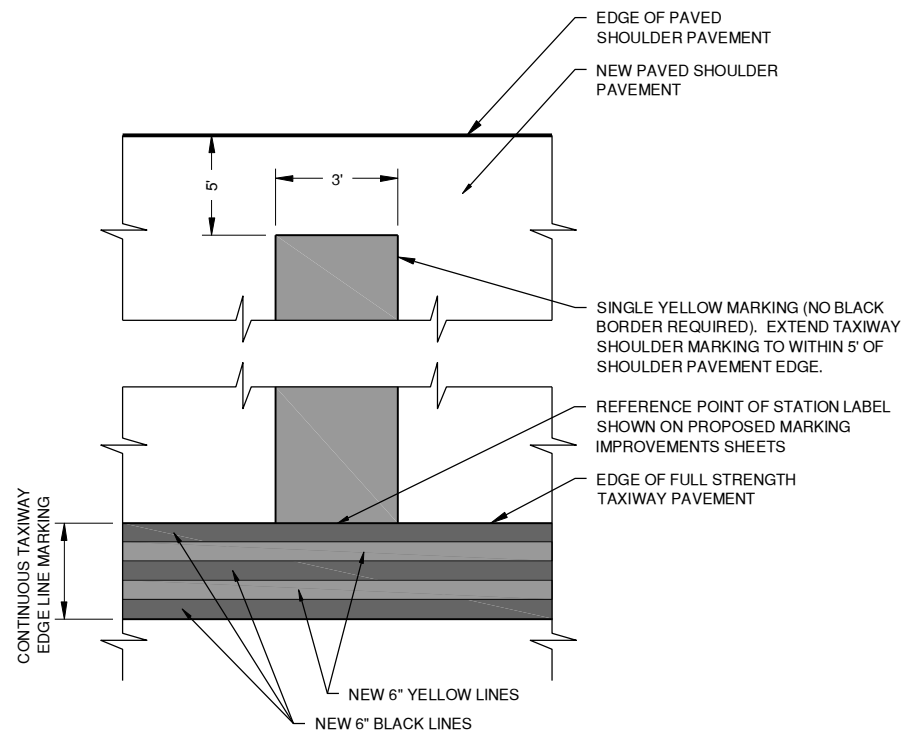
CONTINUOUS TAXIWAY EDGE LINE MARKING DETAIL

N.T.S.



**EDGE LINE MARKING DETAIL AT EXISTING
 RUNWAY HOLD POSITION MARKING**

N.T.S.



TAXIWAY SHOULDER MARKING DETAIL

N.T.S.

MARKING NOTE

ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS AND 6" BLACK BORDER UNLESS OTHERWISE NOTED. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.

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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 TAXIWAY MARKING DETAILS

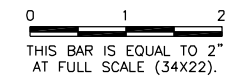


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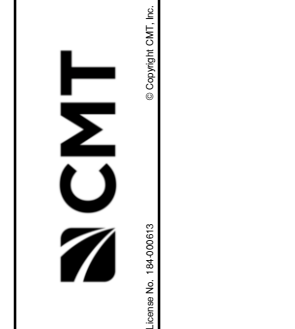
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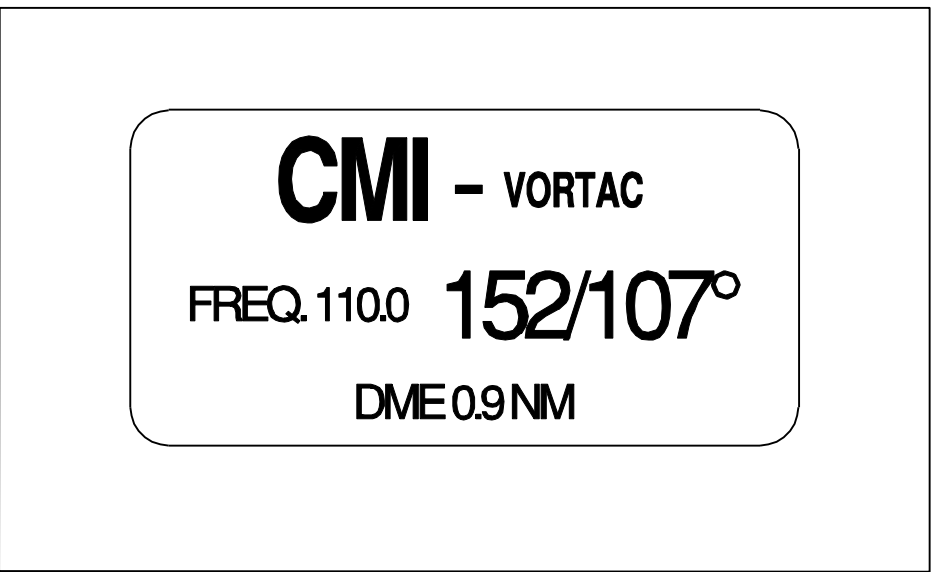
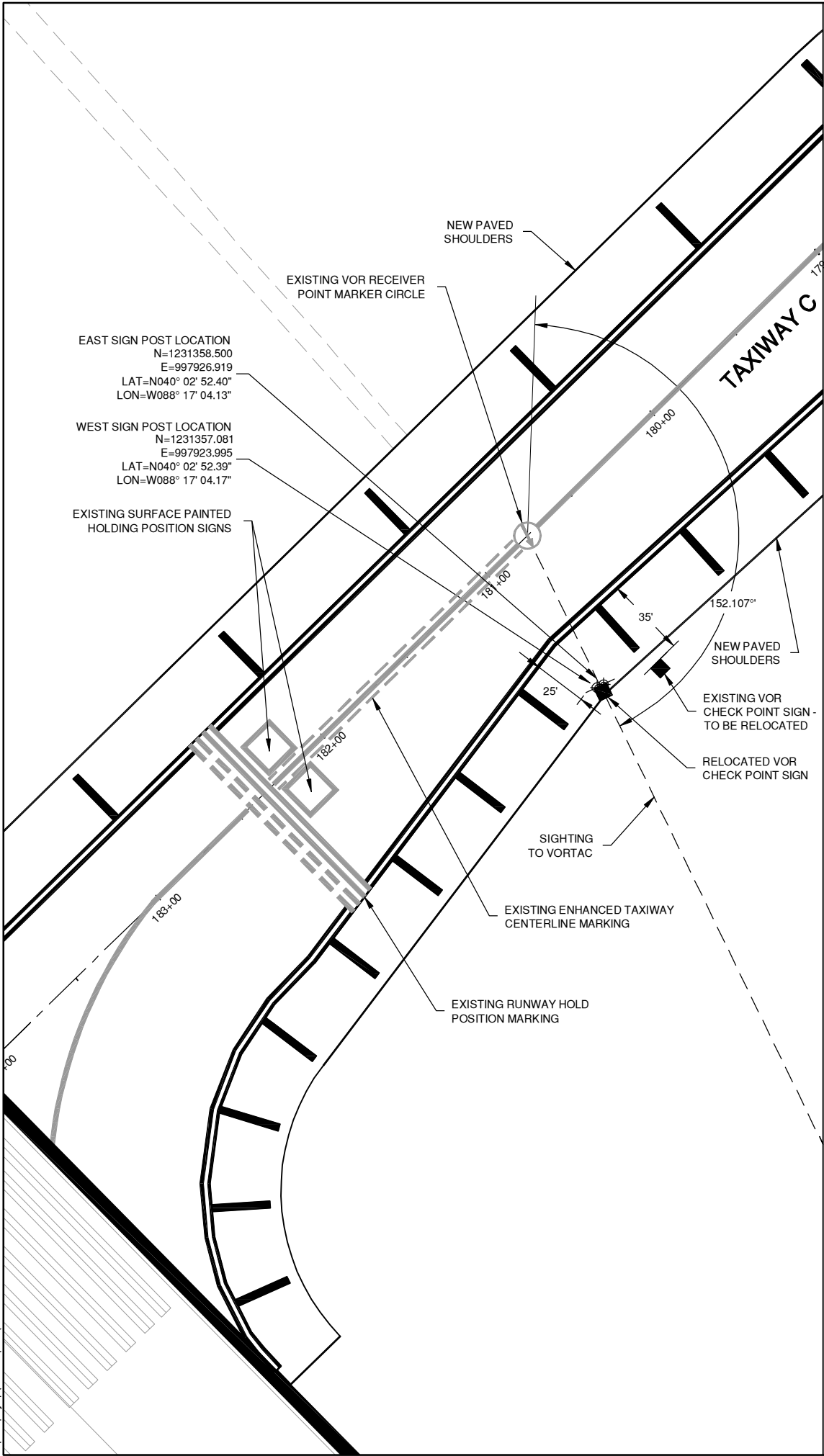


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 CONSTRUCT TAXIWAY C PAVED SHOULDERS
 VOR CHECKPOINT DETAILS



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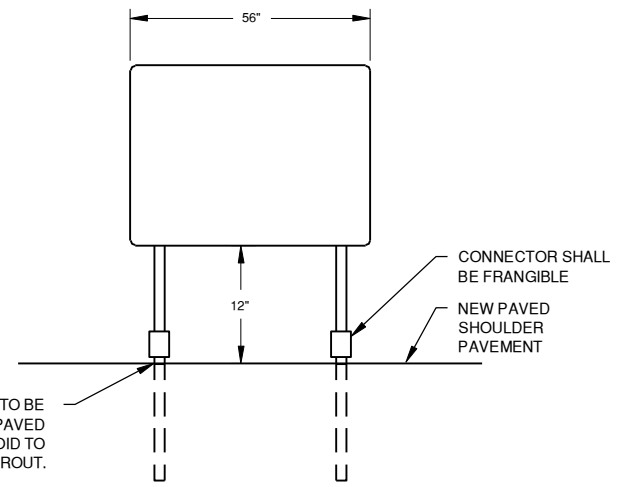


EXISTING VOR RECEIVER CHECK POINT SIGN

N.T.S.

NOTES

1. CONTRACTOR SHALL MOVE AND STORE IN A SAFE AND APPROVED LOCATION THE EXISTING SIGN BEFORE BEGINNING CONSTRUCTION OPERATIONS IN THE VICINITY. CARE SHALL BE GIVEN TO ALL COMPONENTS OF THE EXISTING SIGN AS ALL PARTS SHALL BE CLEANED AND REUSED IN THE RELOCATED POSITION.
2. ANY DAMAGE TO THE SIGN AND/OR COMPONENTS IN THIS PROCESS OR DURING CONSTRUCTION WILL BE REPAIRED AND/OR REPLACED BY CONTRACTOR AT HIS EXPENSE.
3. SIGN TO BE RELOCATED WITHIN THE PAVED SHOULDER PAVEMENT AND ON THE SIGHTING LINE TO THE VORTAC.
4. SIGN FACE TO BE FACED PERPENDICULARLY TO THE LINE-OF-SIGHT OF THE VIEWER IN THE VOR CHECKPOINT MARKING CIRCLE.



INFORMATION SIGN LEGEND

N.T.S.

NEW SIGN POST LOCATIONS TO BE CORE-DRILLED INTO NEW PAVED SHOULDERS. REMAINING VOID TO BE BACKFILLED WITH GROUT.

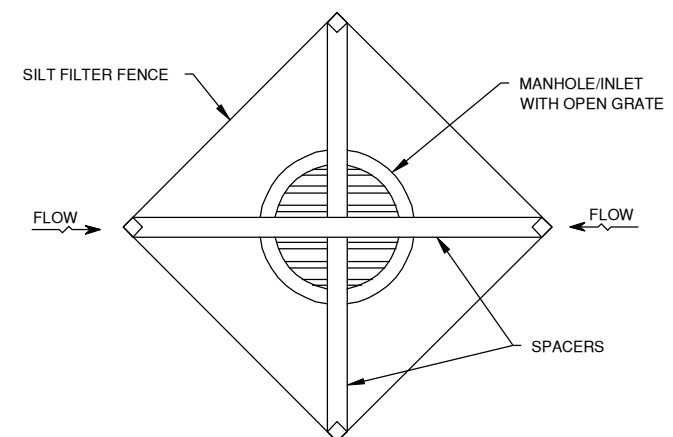
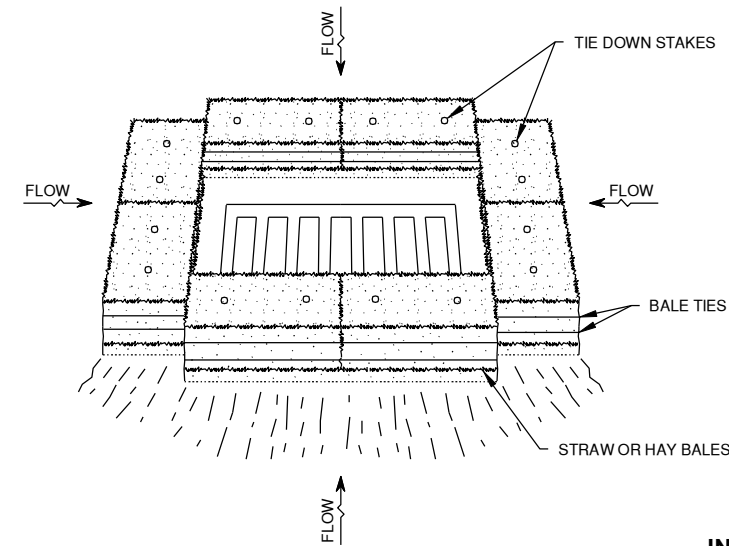
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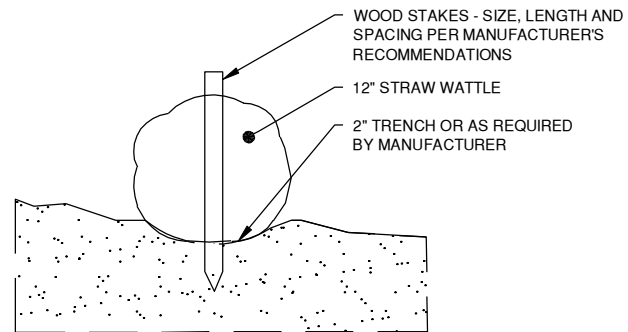
REVISIONS

NUMBER	BY	DATE

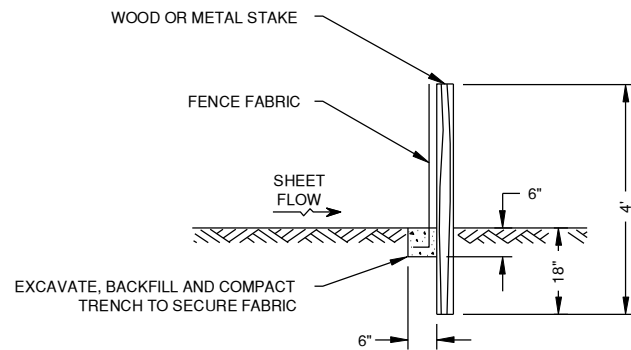
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34x22).



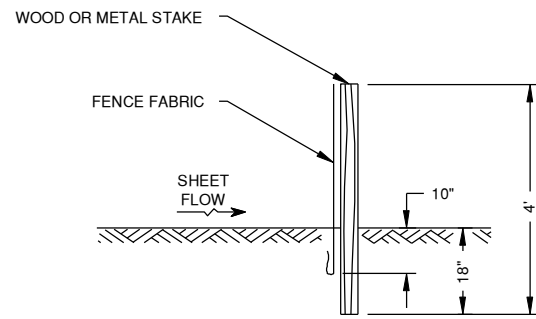
INLET PROTECTION
 N.T.S.



SECTION



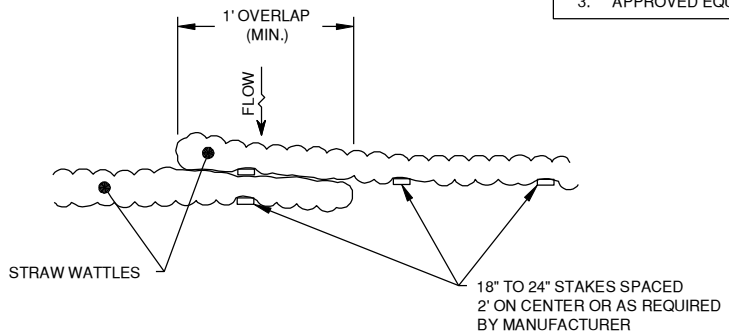
TRENCH METHOD SECTION A-A



SLICE METHOD

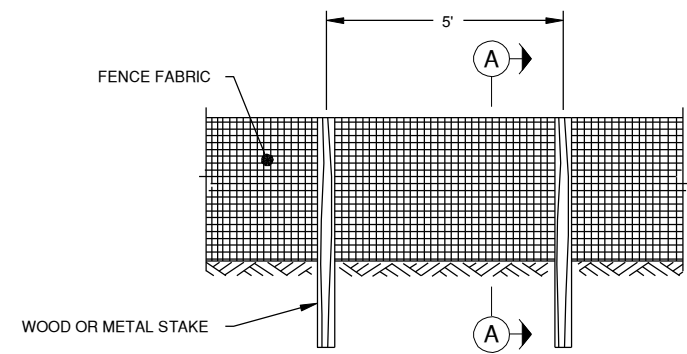
MANUFACTURERS

1. NORTH AMERICAN GREEN
2. WESTERN EXCELSIOR
3. APPROVED EQUAL



PLAN

STRAW WATTLES
 N.T.S.



ELEVATION

SILT FILTER FENCE AS A PERIMETER EROSION BARRIER
 N.T.S.

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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 EROSION CONTROL DETAILS



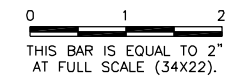
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DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO. CMI-4382	
AIP PROJ. NO. 3-17-0016-XX	
SHEET 35 OF 52 SHEETS	

CMI-V-AERL
 KEYMAP
 CMI 1405902-C-SPAL
 CMI 1405902-C-UPDR
 UTILITY_CMI
 CMI-1405902-C-LP

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NUMBER	BY	DATE

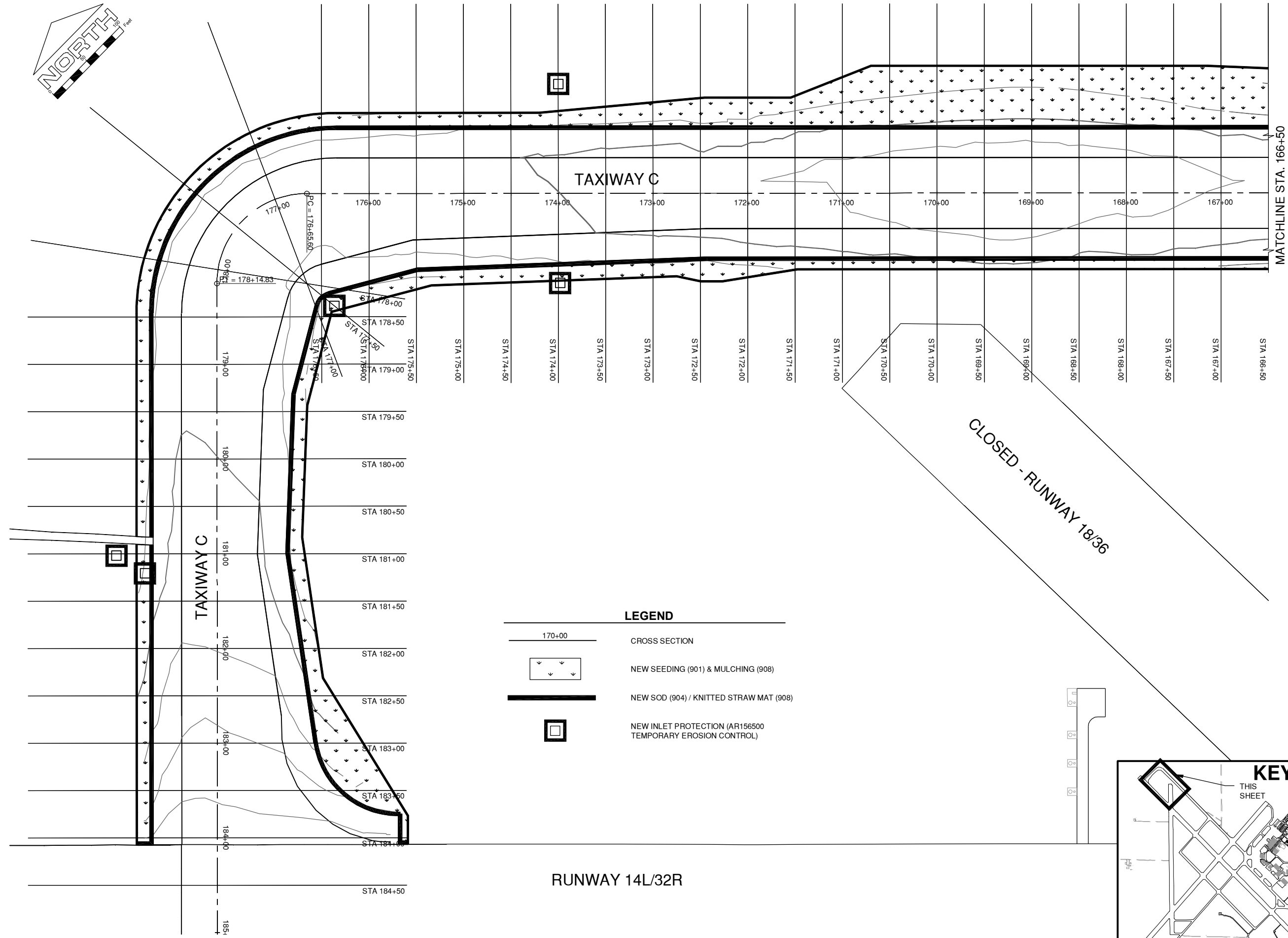
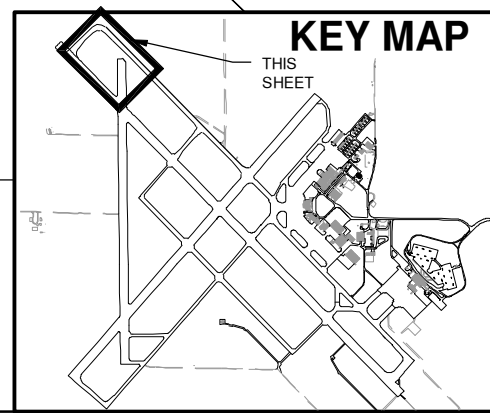


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**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 TURFING PLAN AND INDEX TO CROSS
 SECTIONS SHEET 1**



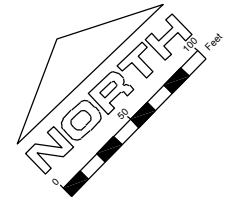
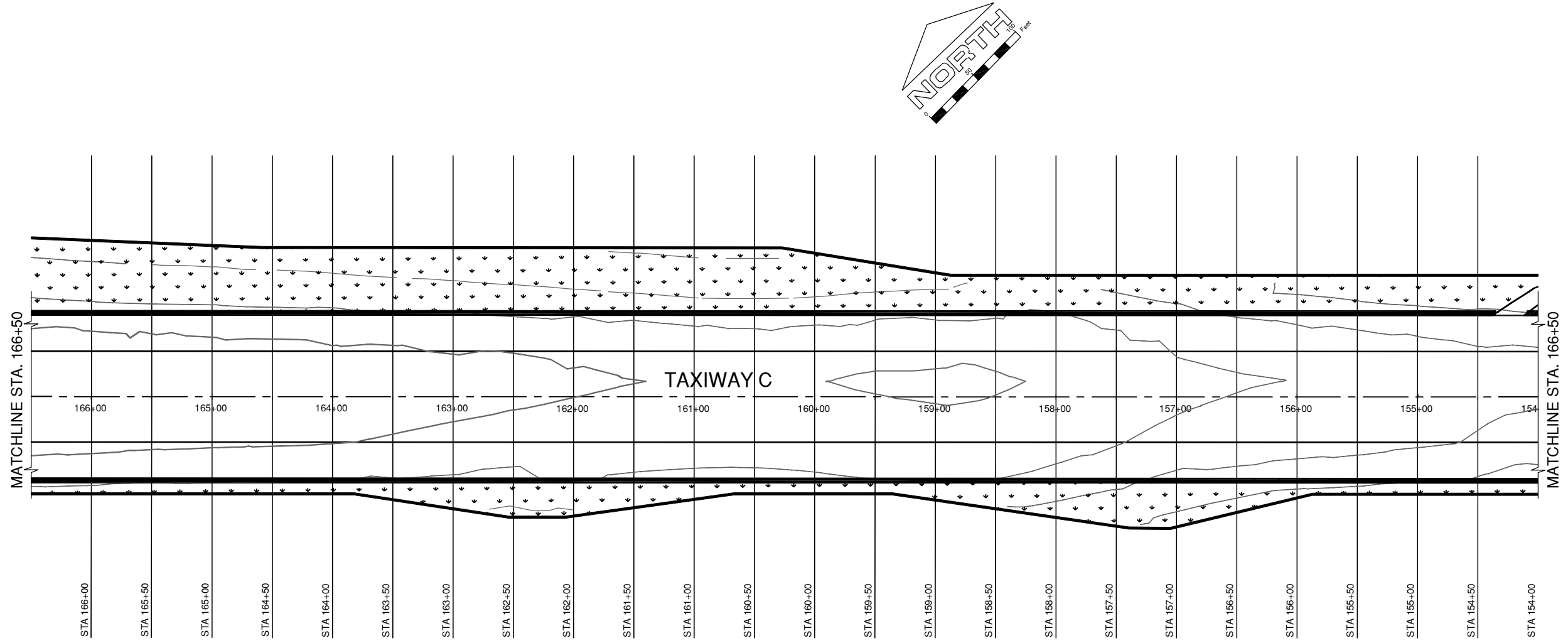
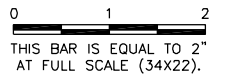
DESIGN BY:	AJH
DRAWN BY:	ADD
CHECKED BY:	CBG
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JOB No:	14059-02-00
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AIP PROJ. NO.	3-17-0016-XX
SHEET	36 OF 52 SHEETS




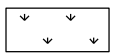


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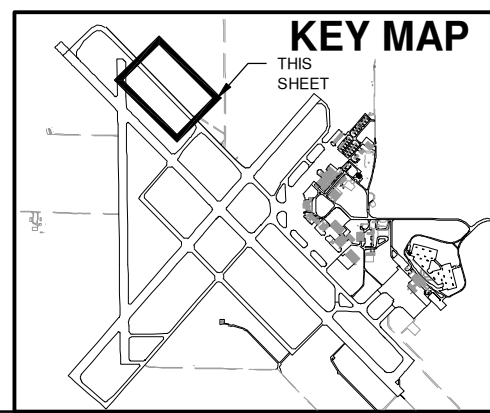
UN055

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NUMBER	BY	DATE



LEGEND

-  CROSS SECTION
-  NEW SEEDING (901) & MULCHING (908)
-  NEW SOD (904) / KNITTED STRAW MAT (908)
-  NEW INLET PROTECTION (AR156500 TEMPORARY EROSION CONTROL)



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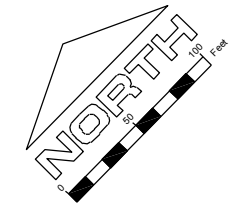
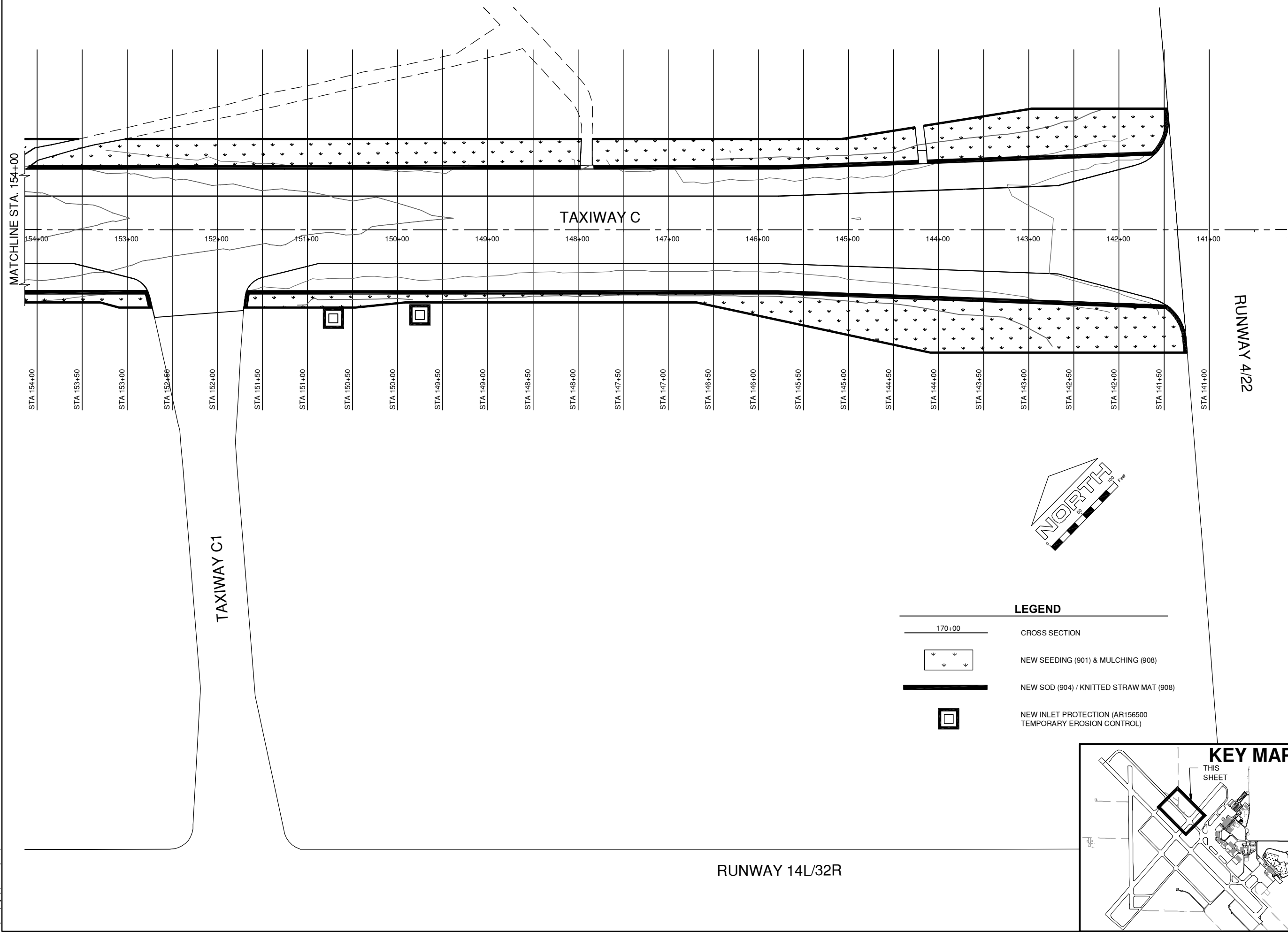
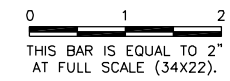
**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 TURFING PLAN AND INDEX TO CROSS
 SECTIONS SHEET 2**



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AIP PROJ. NO.	3-17-0016-XX
SHEET	37 OF 52 SHEETS

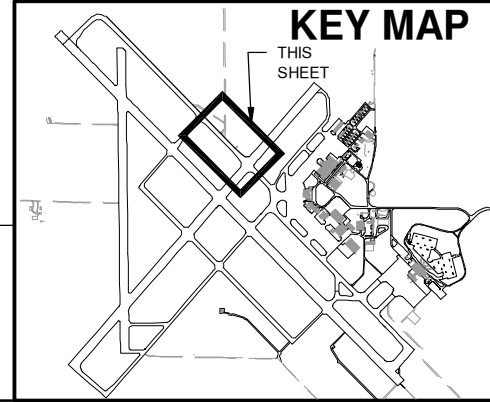
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REVISIONS		
NUMBER	BY	DATE



LEGEND

	170+00	CROSS SECTION
		NEW SEEDING (901) & MULCHING (908)
		NEW SOD (904) / KNITTED STRAW MAT (908)
		NEW INLET PROTECTION (AR156500) TEMPORARY EROSION CONTROL



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**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 TURFING PLAN AND INDEX TO CROSS
 SECTIONS SHEET 3**

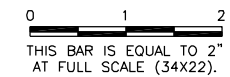


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IL. PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET	38 OF 52 SHEETS

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NUMBER	BY	DATE



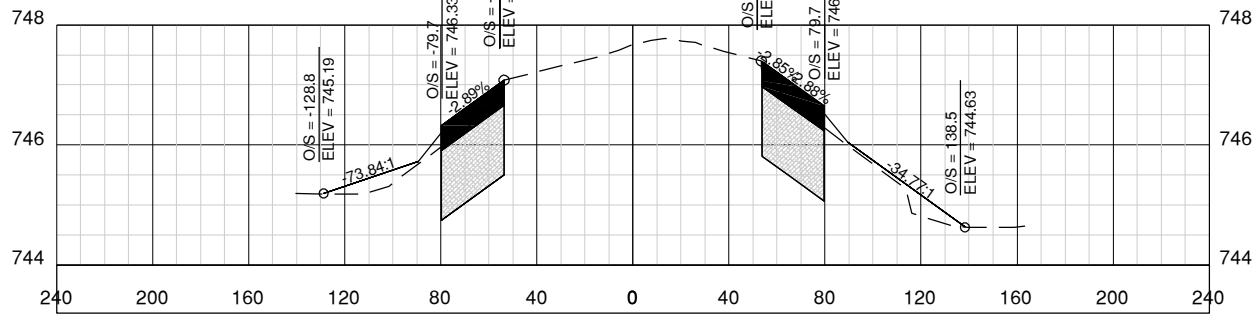
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**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 1**

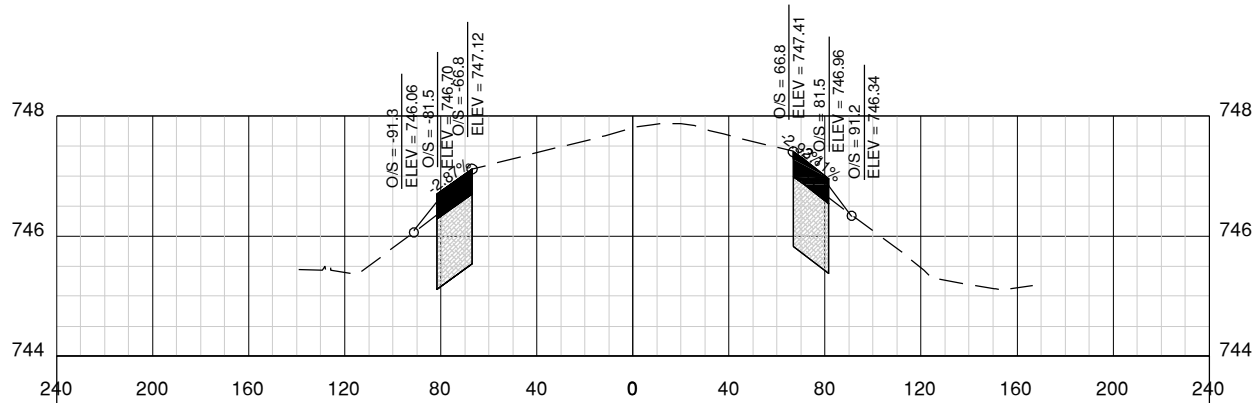


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JOB No:	14059-02-00
IL PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET	39 OF 52 SHEETS

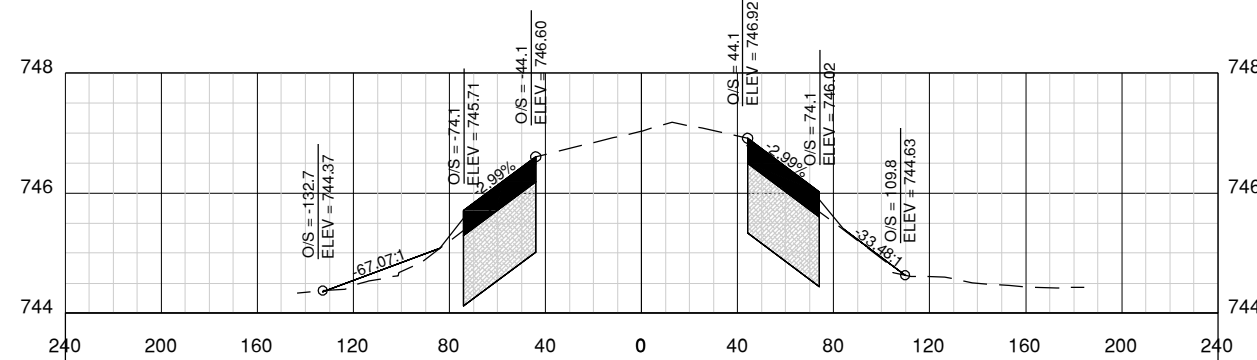
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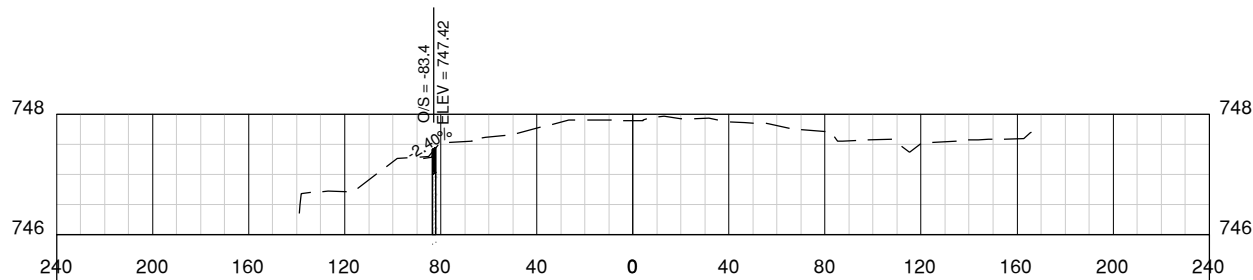
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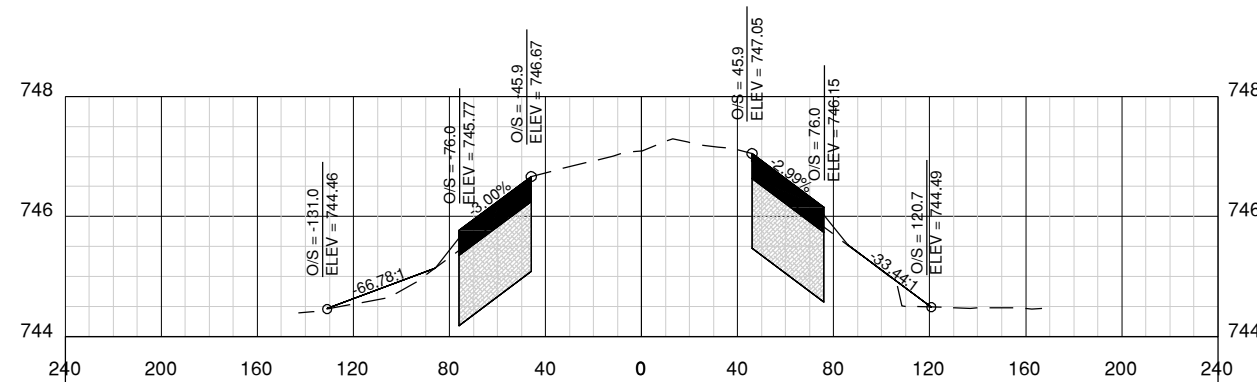
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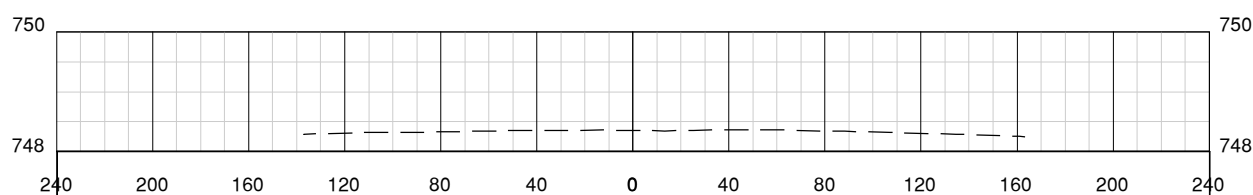
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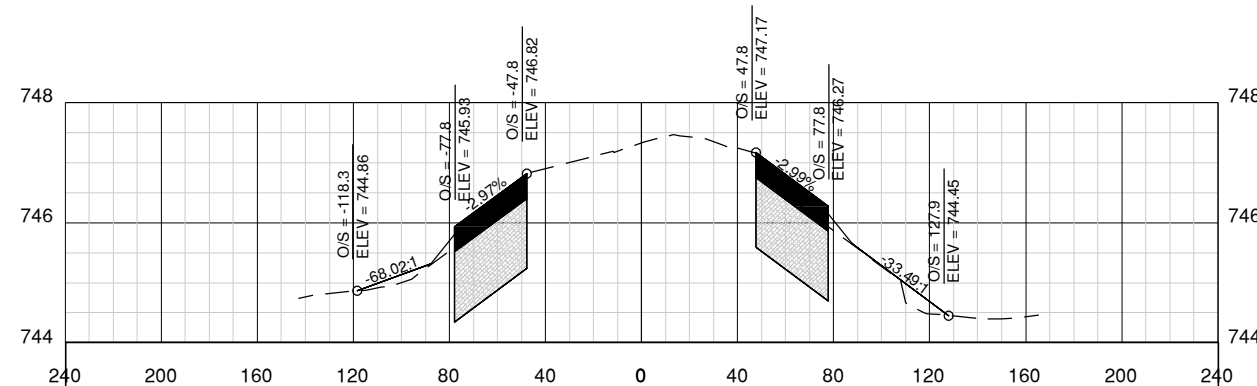
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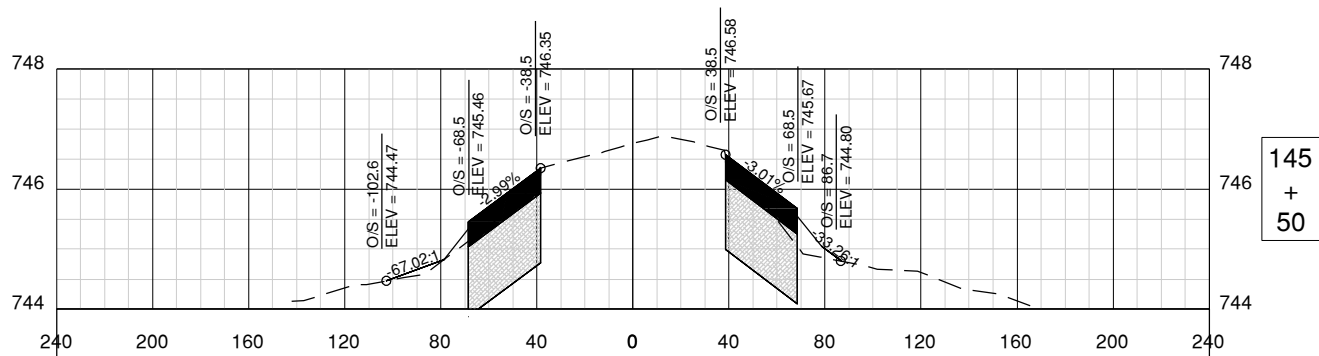
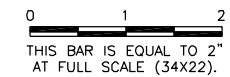
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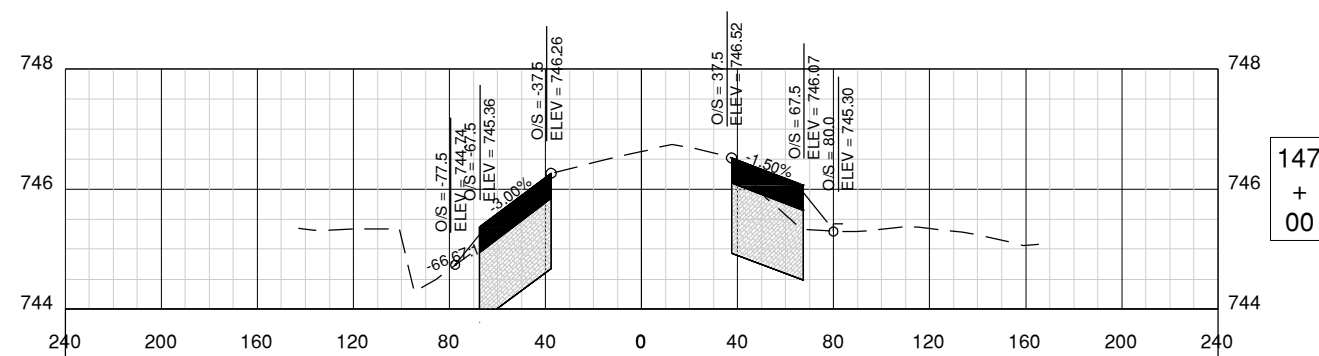
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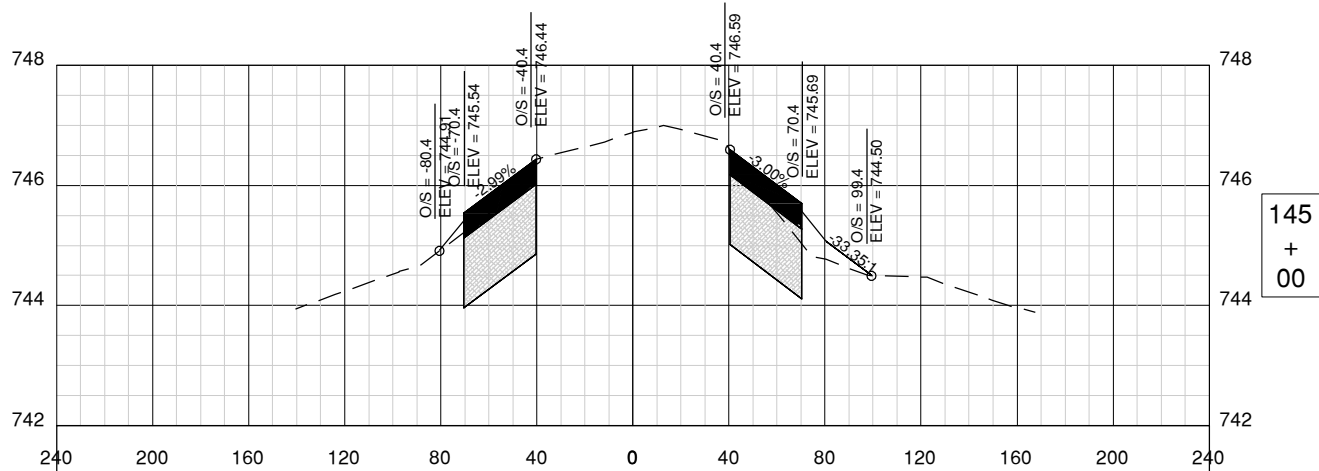
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NUMBER	BY	DATE



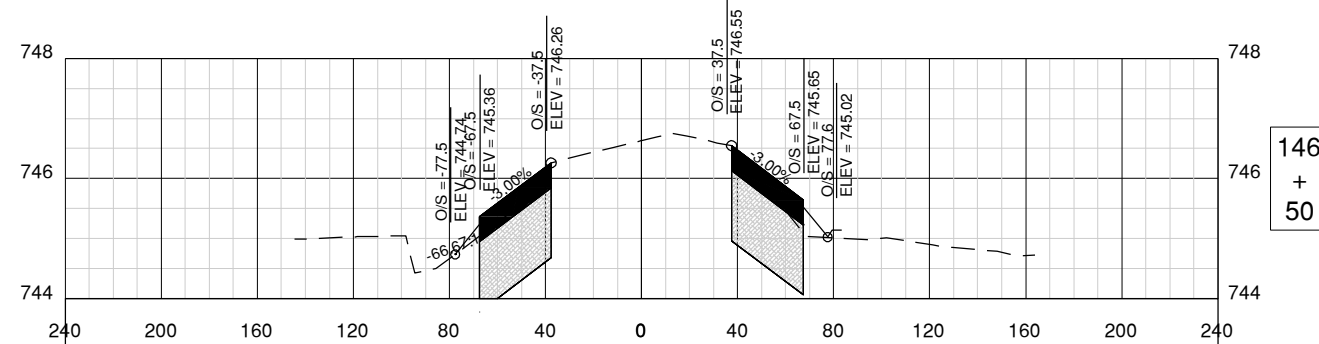
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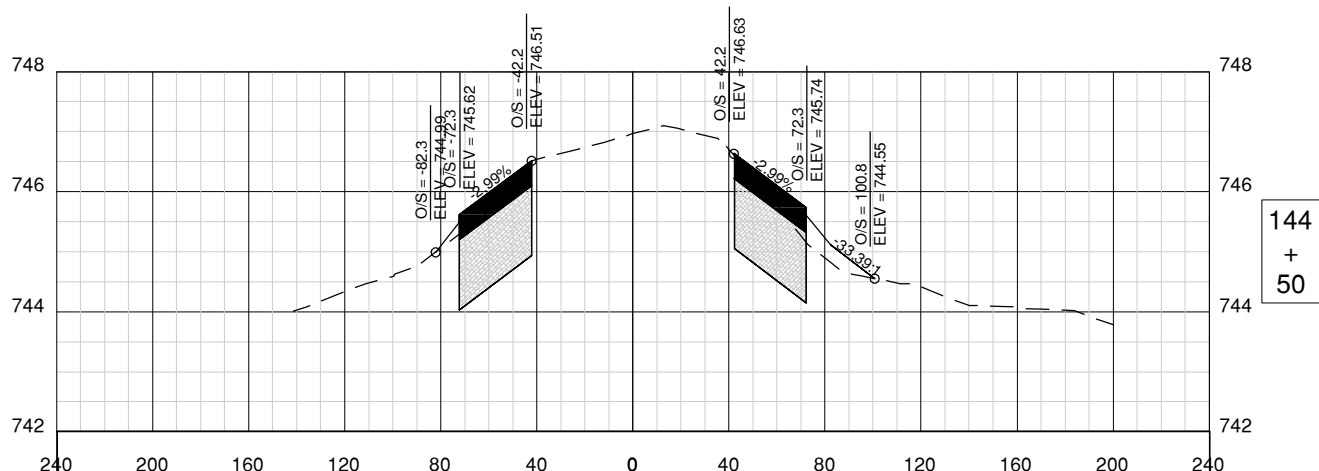
147
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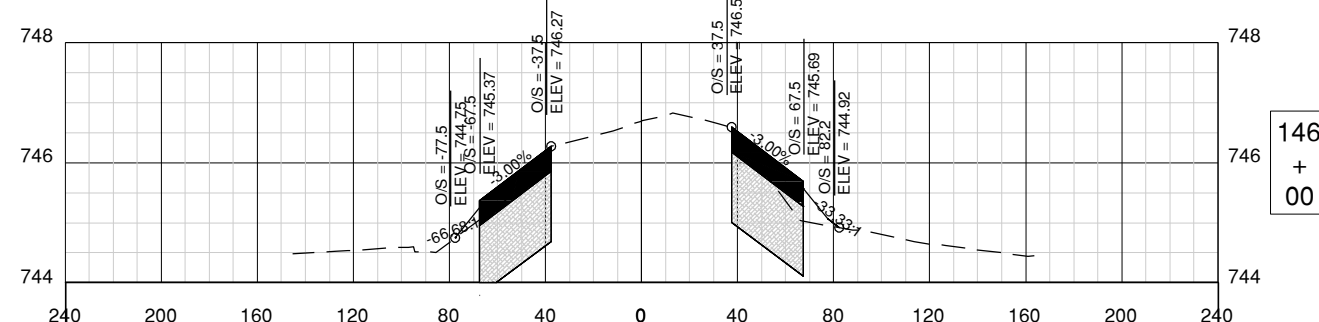
145
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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 2

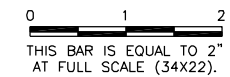


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JOB No:	14059-02-00
IL PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET 40 OF 52 SHEETS	

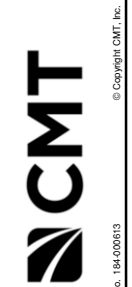
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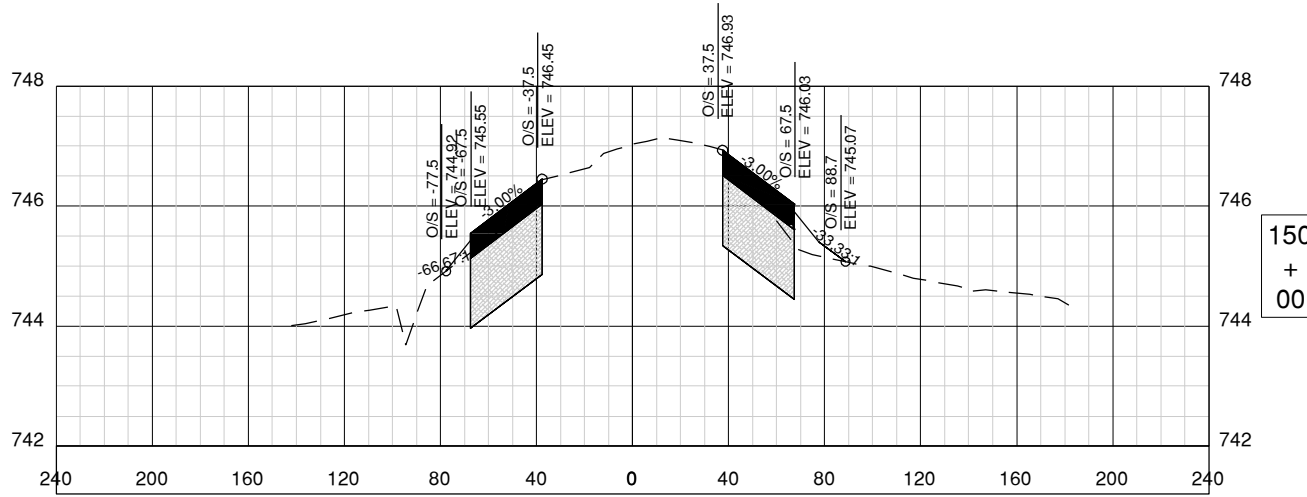


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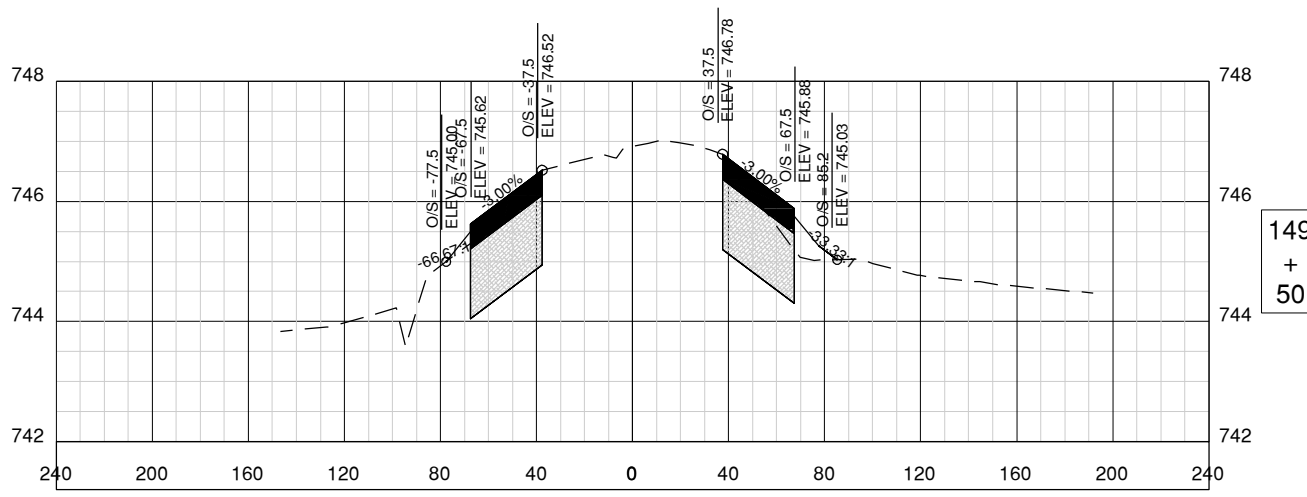
**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 3**



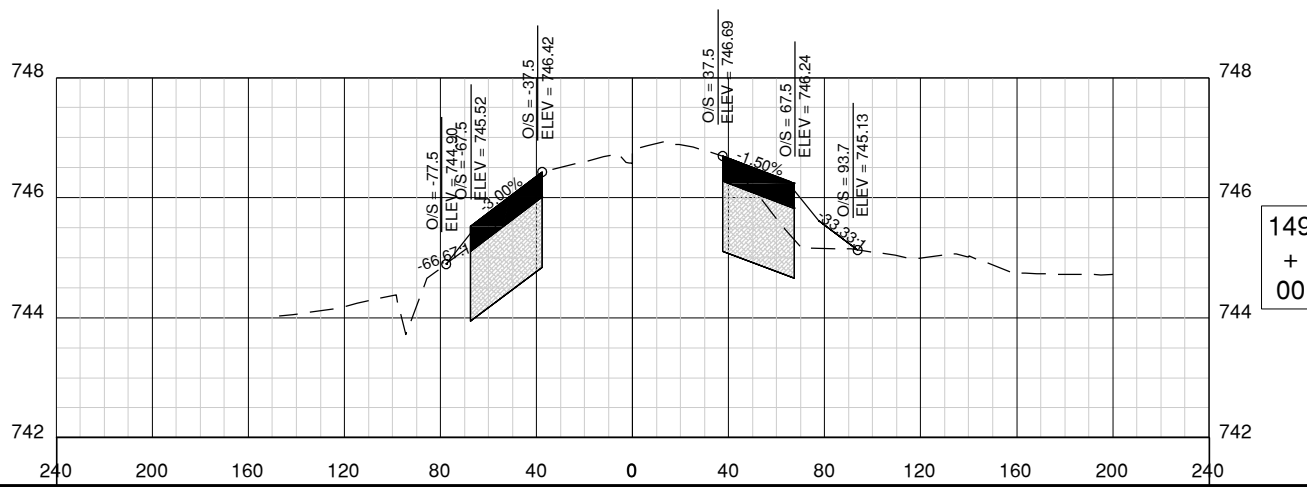
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SHEET 41 OF 52 SHEETS	



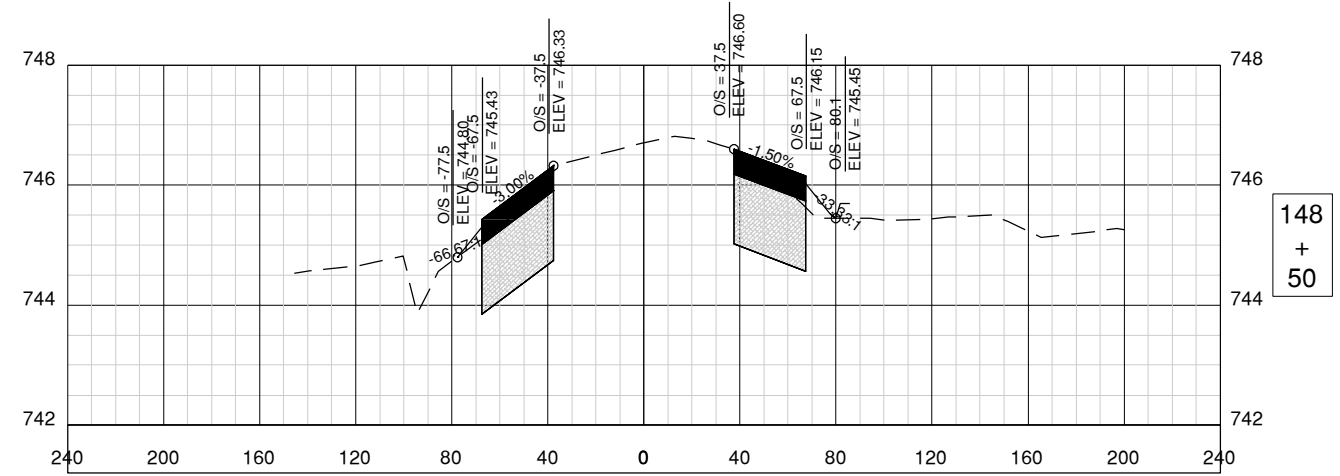
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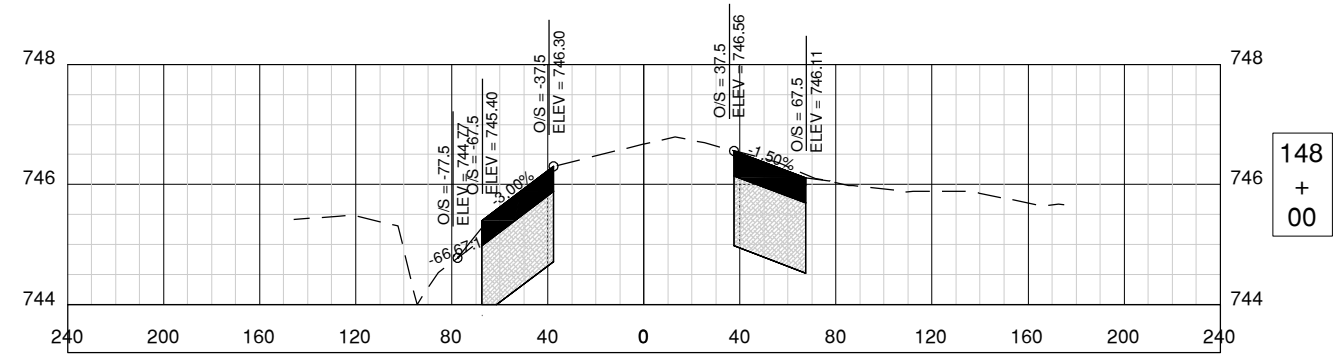
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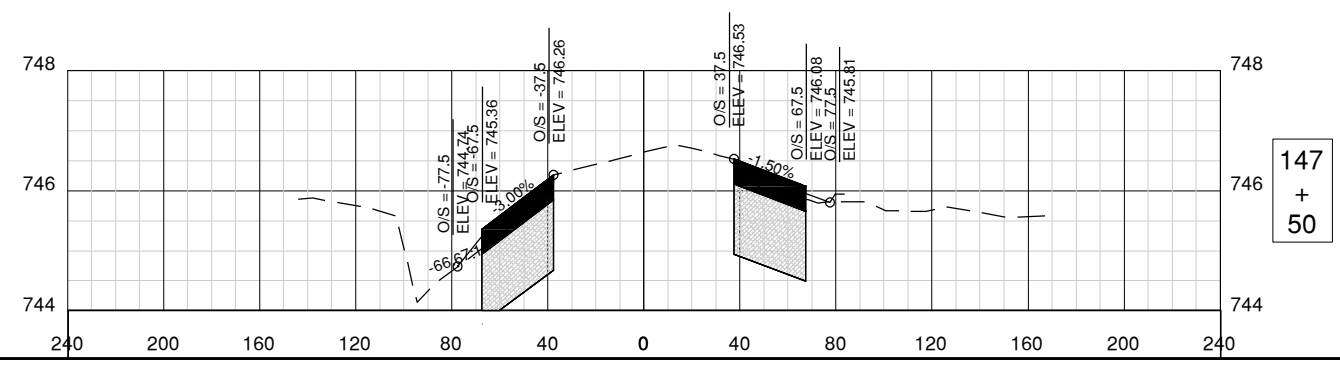
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0 1 2
 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34x22).

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**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 4**

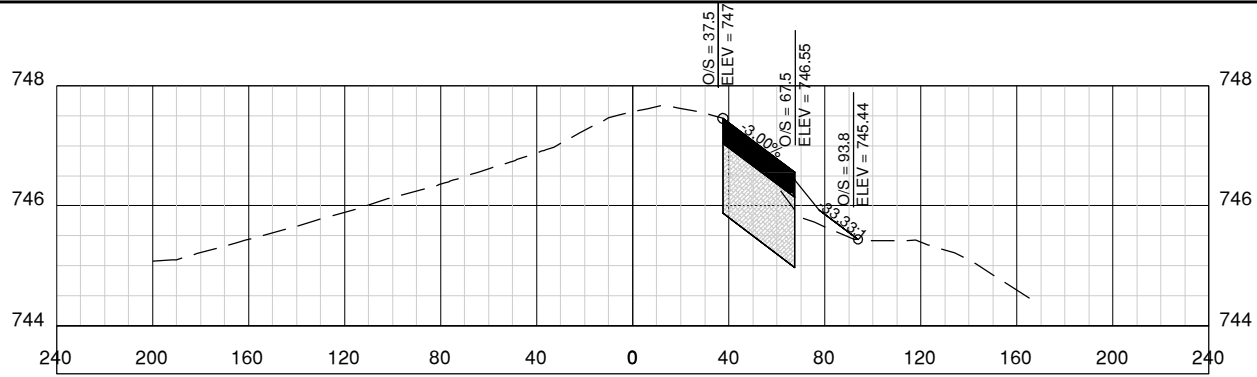


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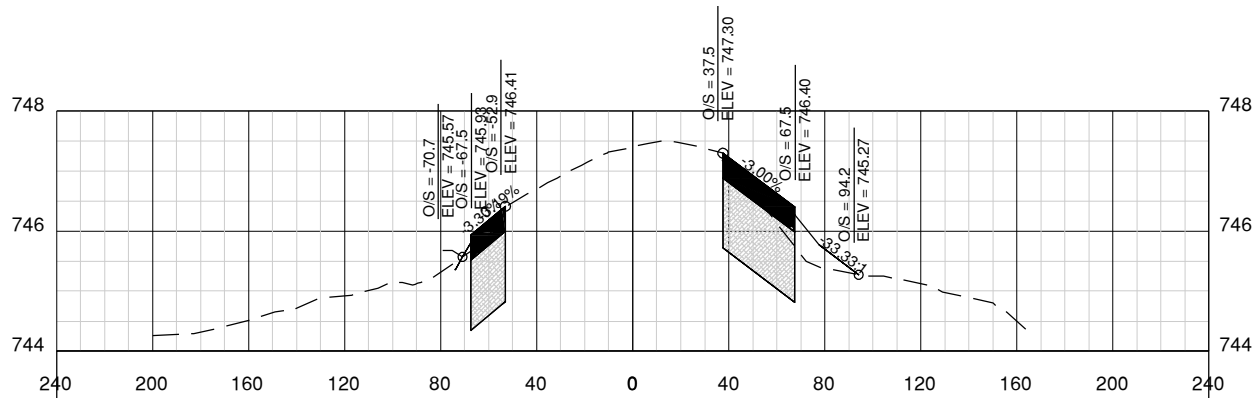
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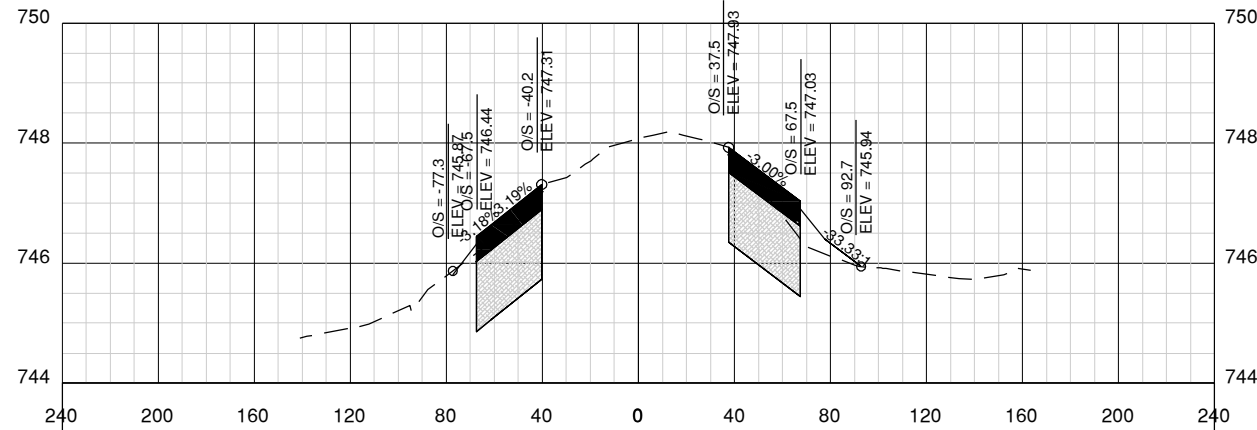
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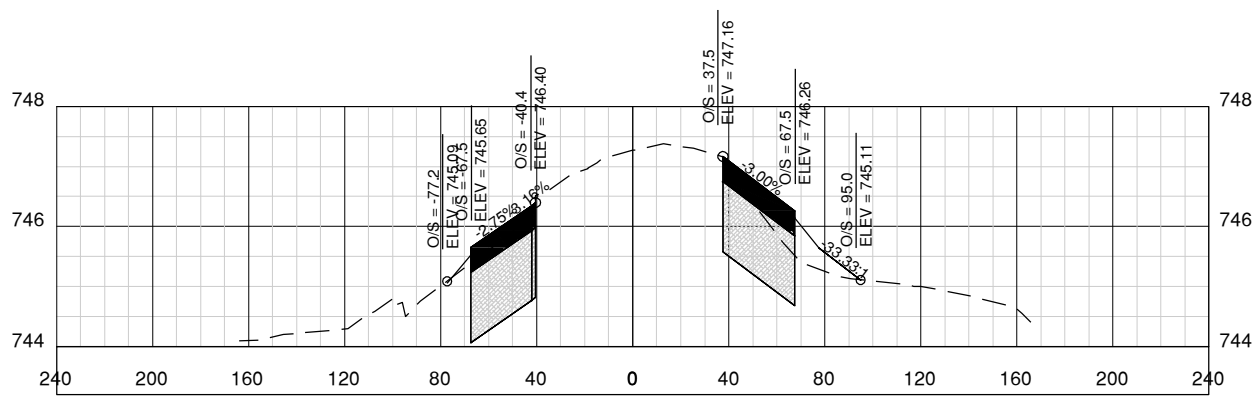
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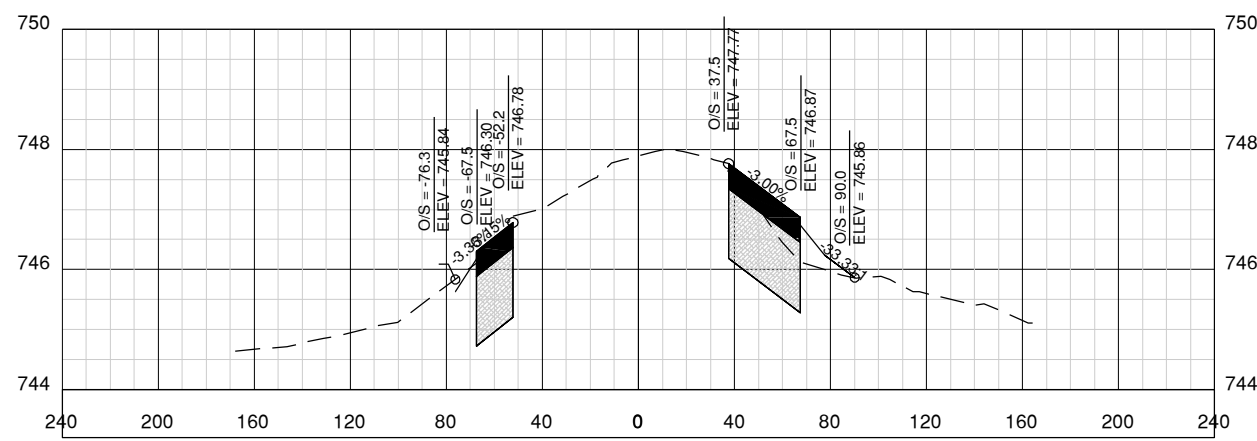
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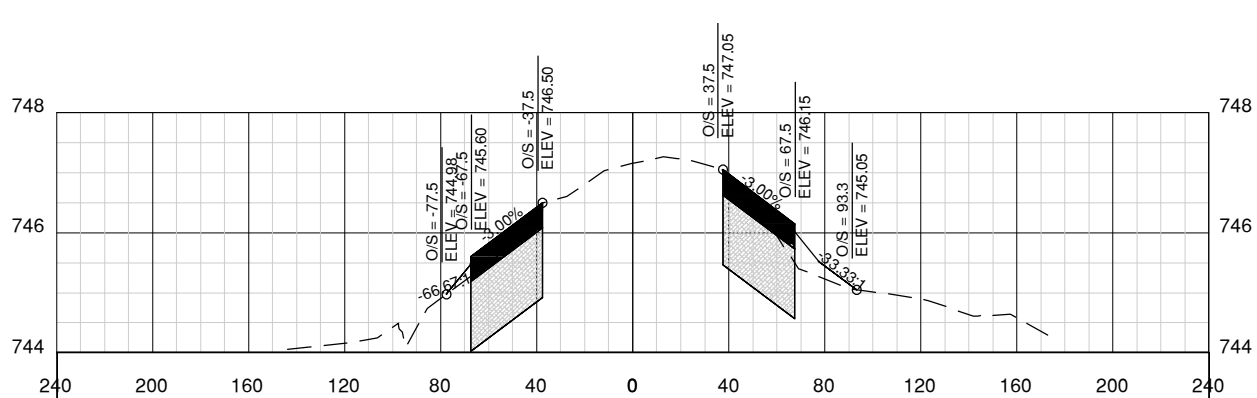
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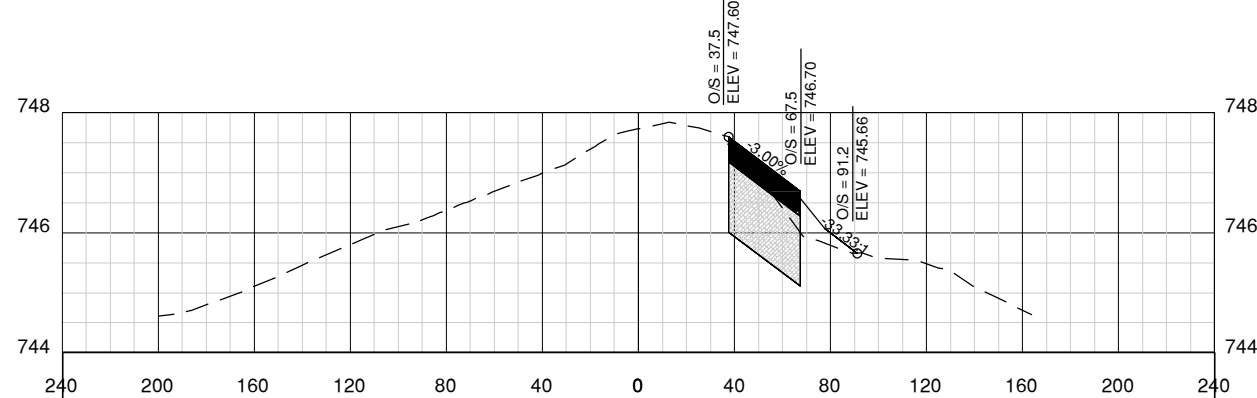
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150
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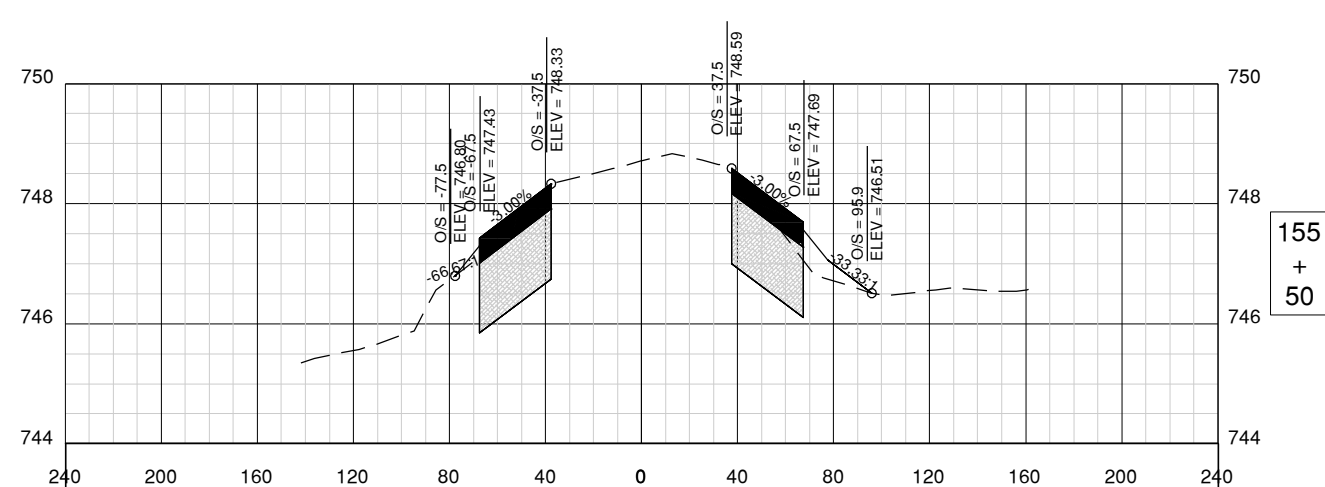
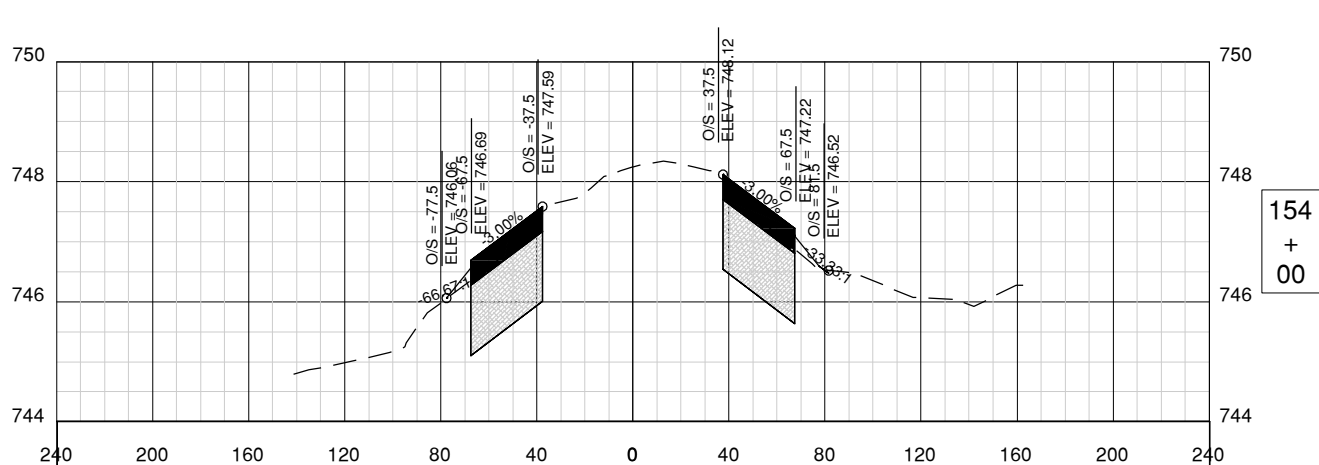
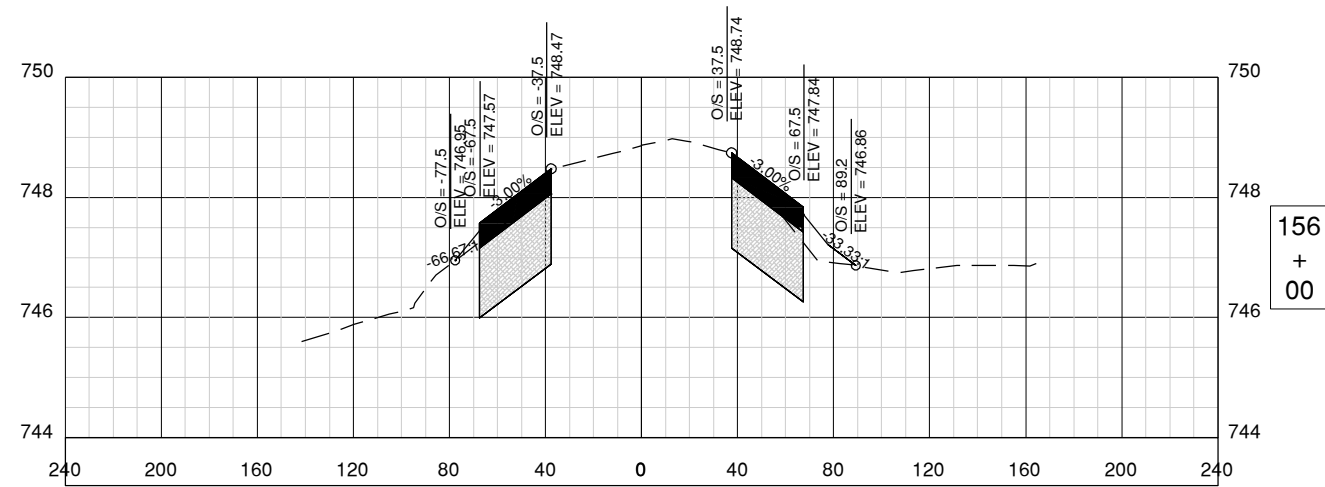
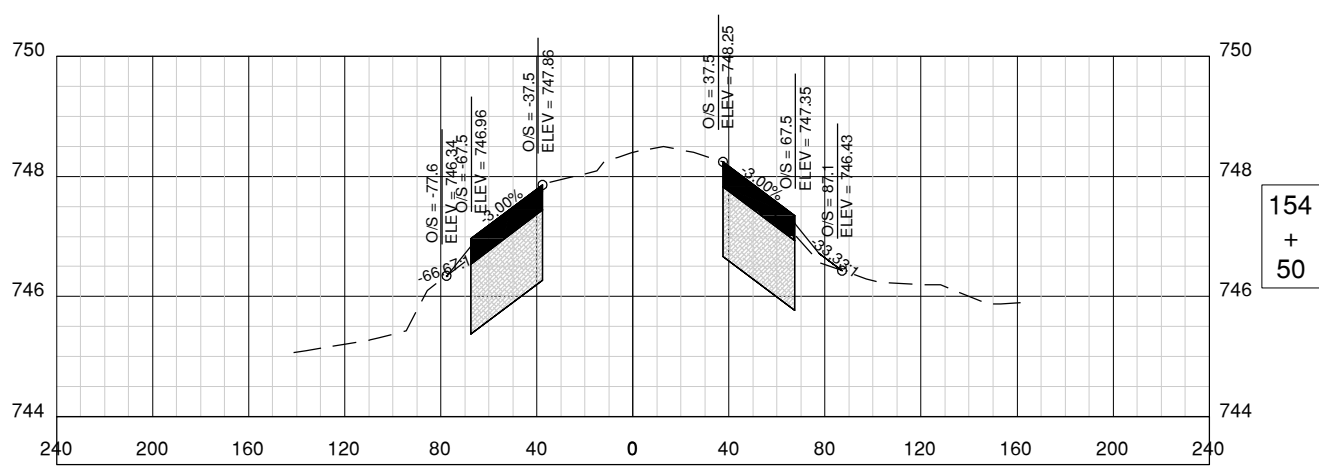
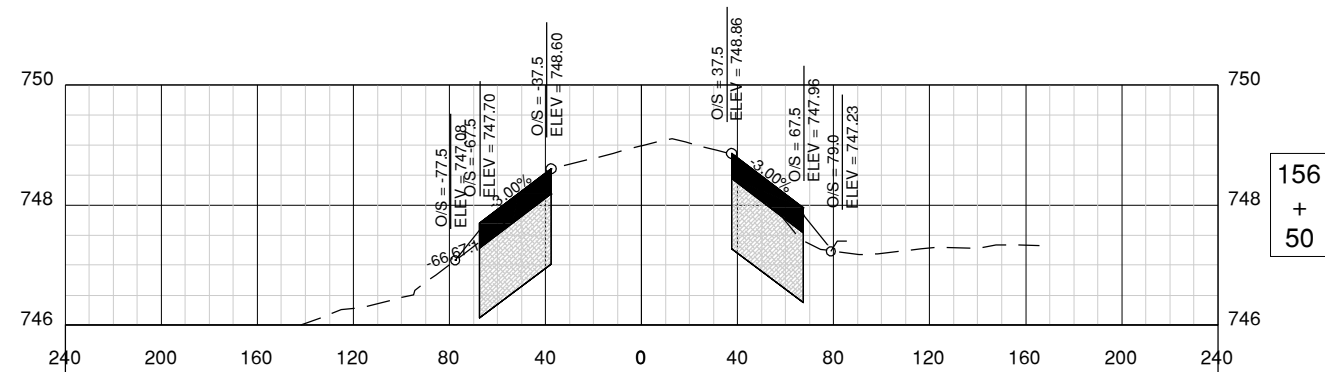
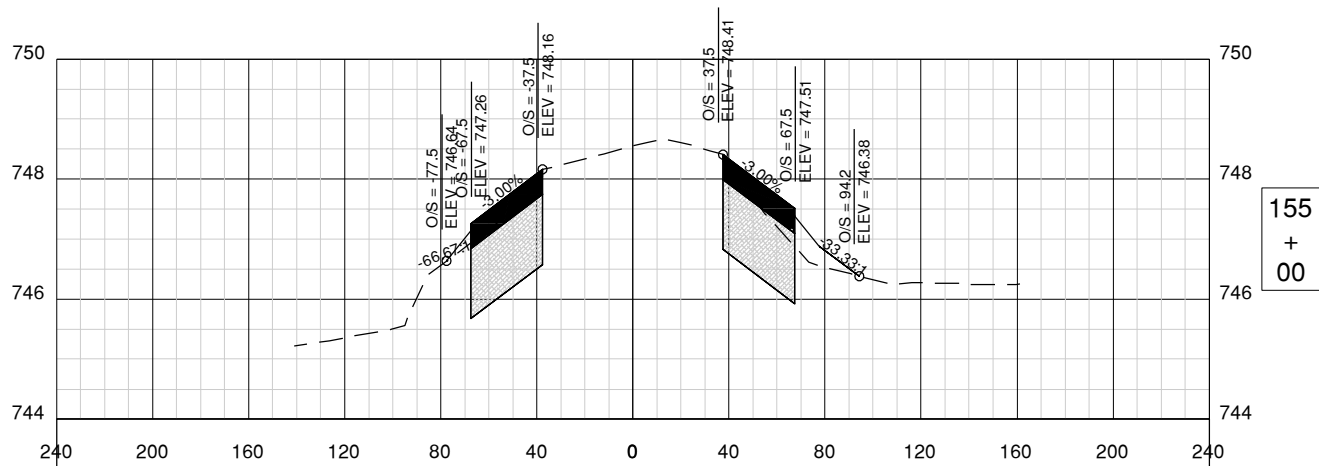
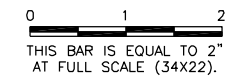


152
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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 5

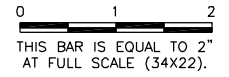


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SHEET 43 OF 52 SHEETS	

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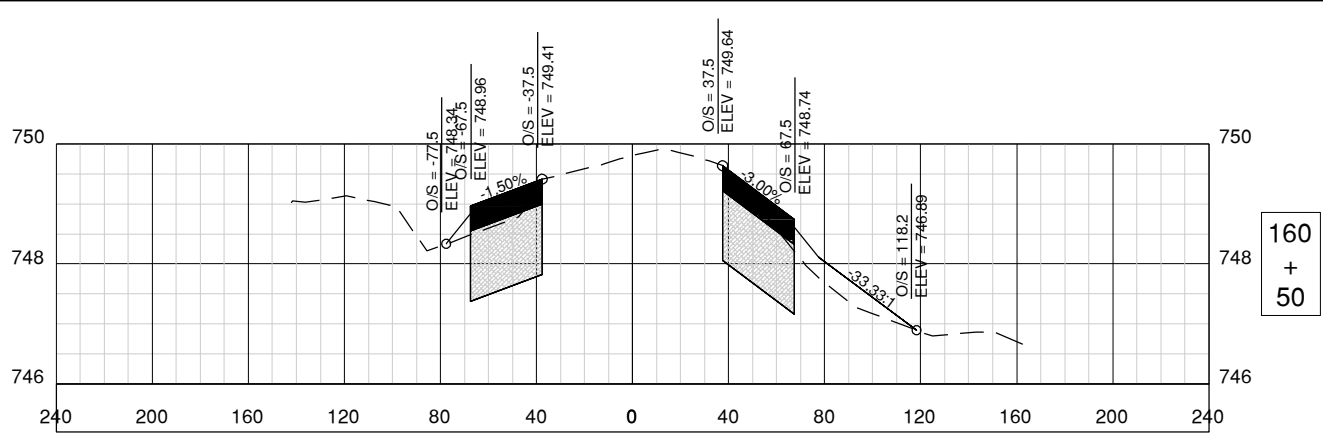


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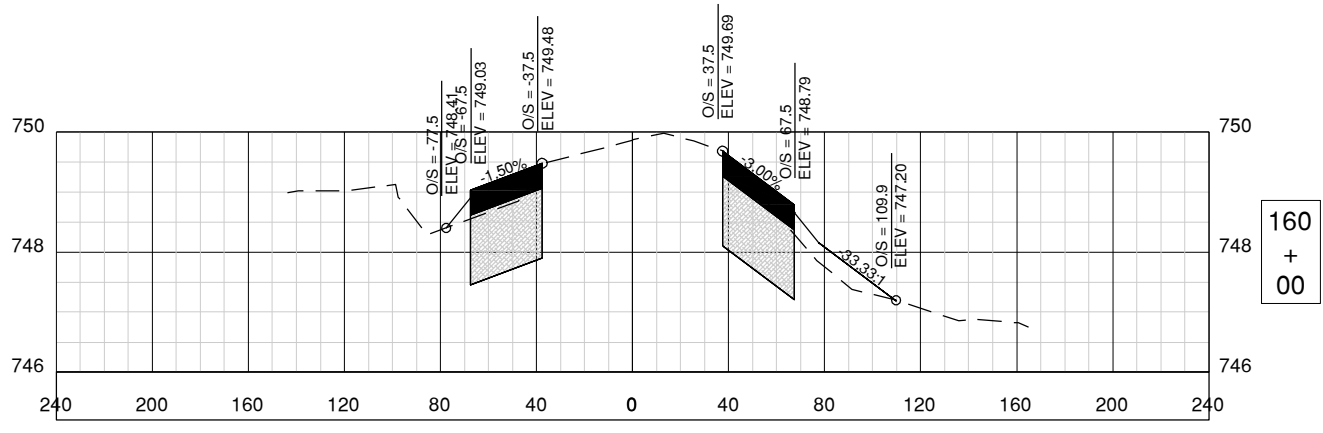
**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 6**



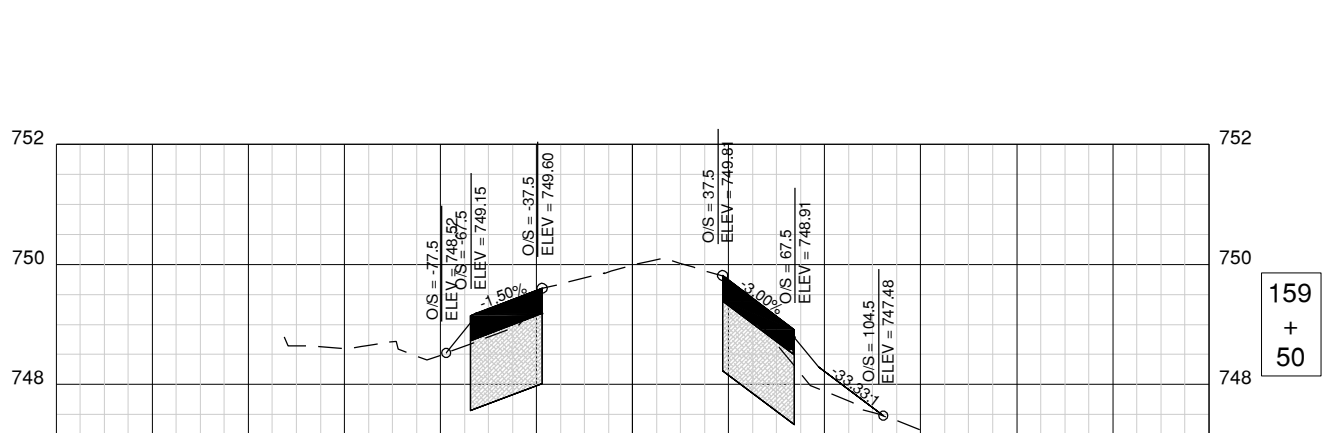
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DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL. PROJ. NO. CMI-4382 AIP PROJ. NO. 3-17-0016-XX	
SHEET 44 OF 52 SHEETS	



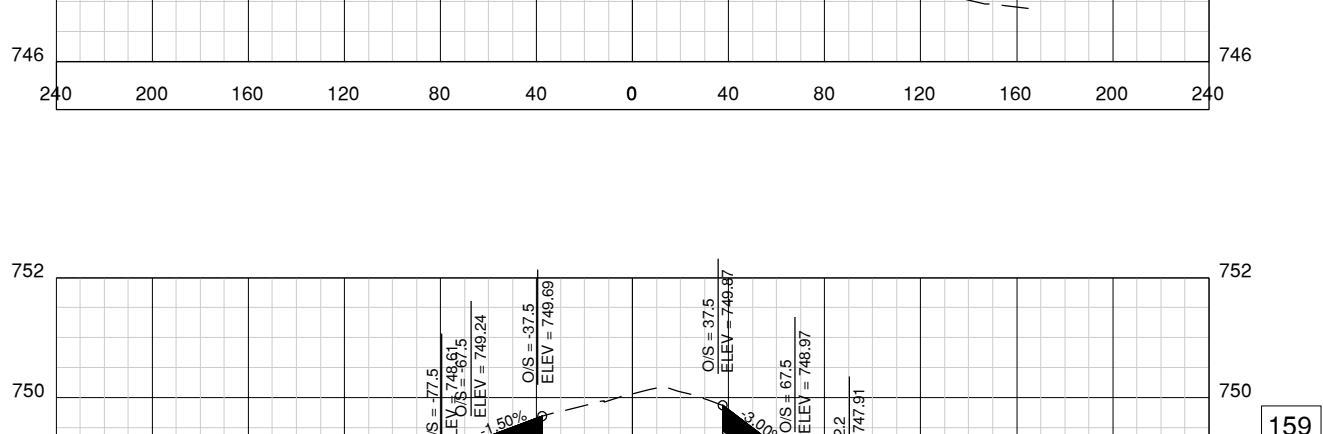
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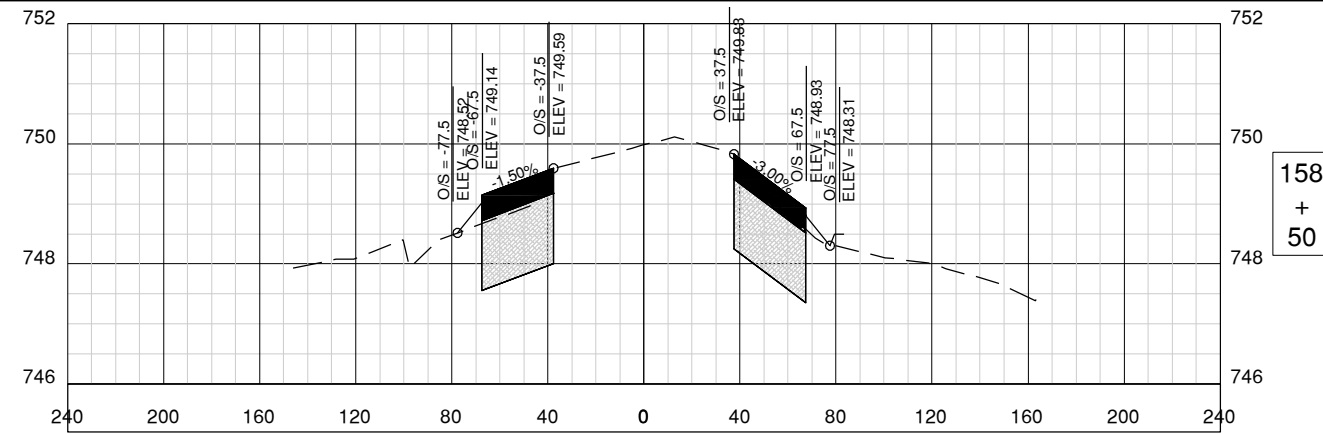
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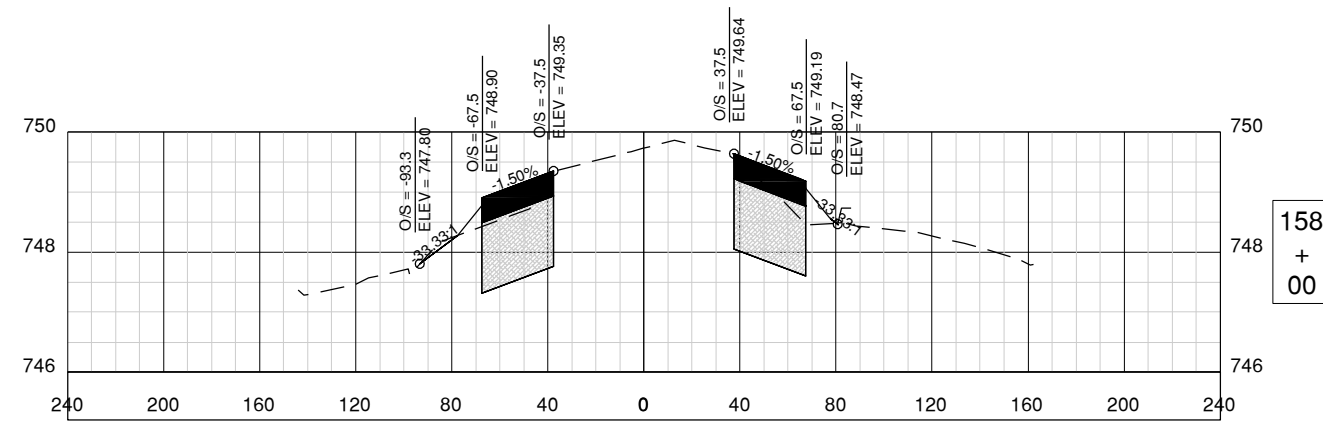
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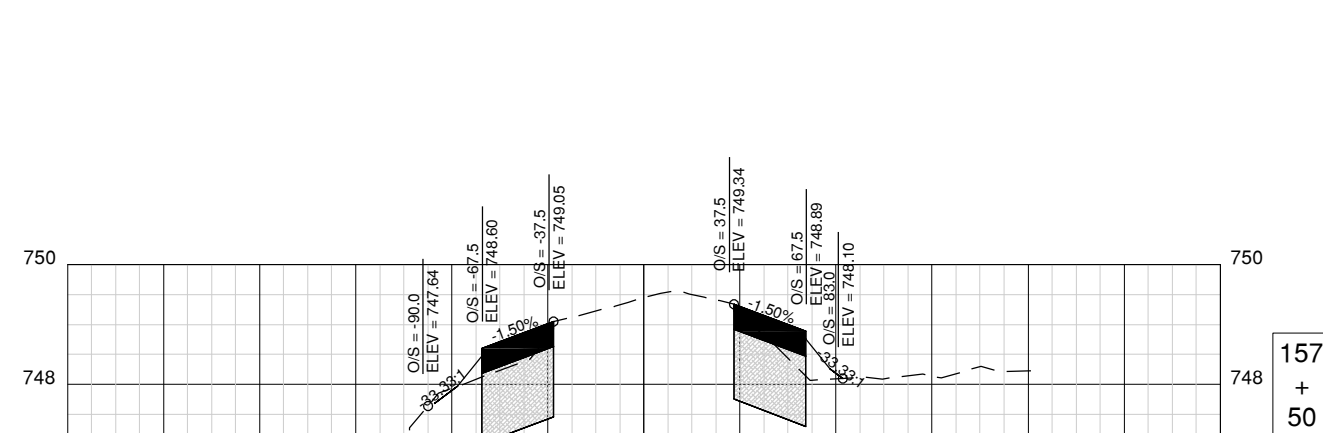
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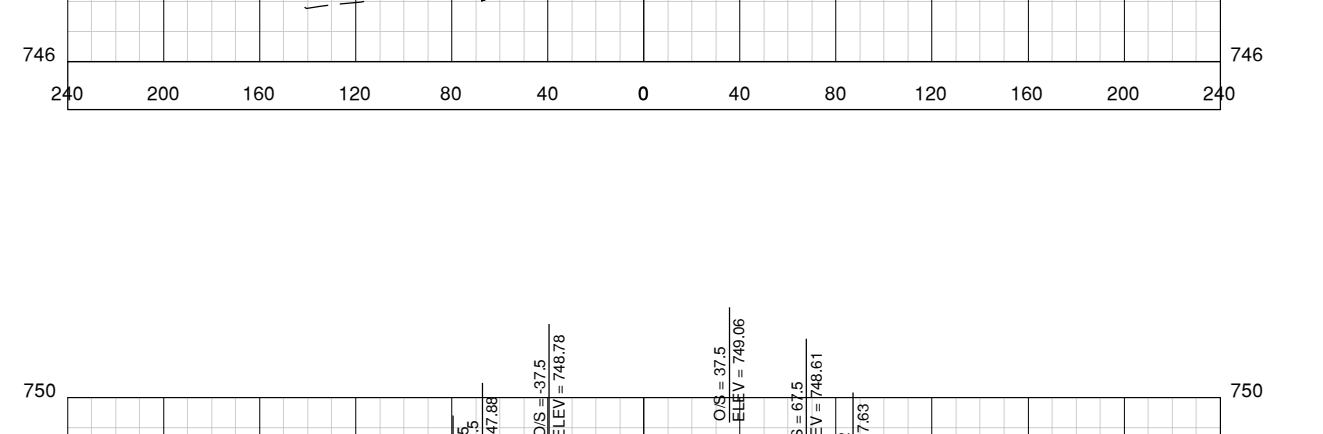
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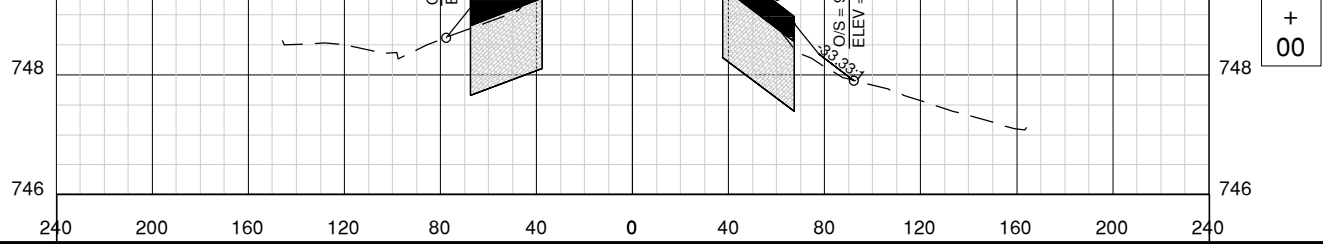
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157
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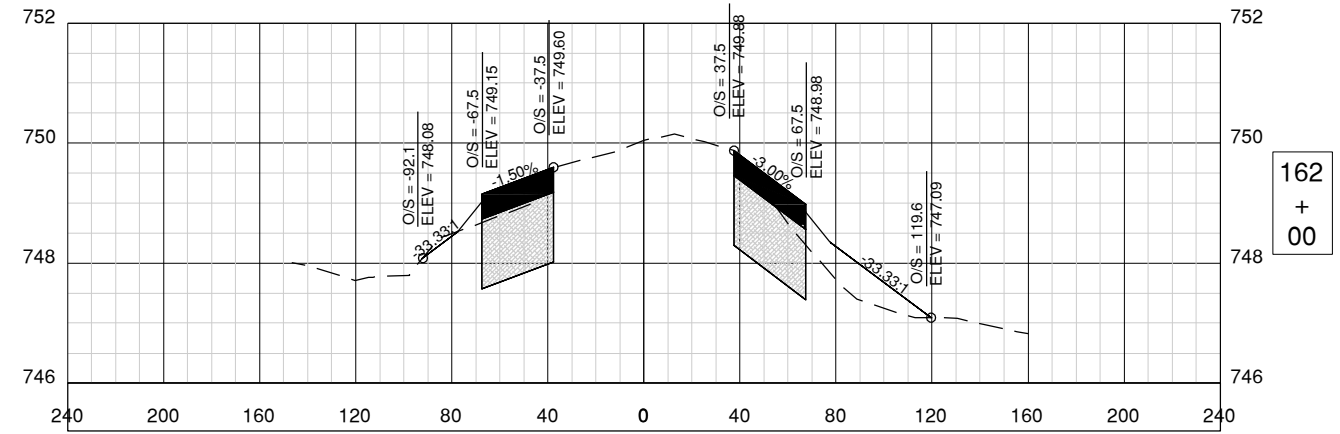
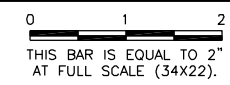


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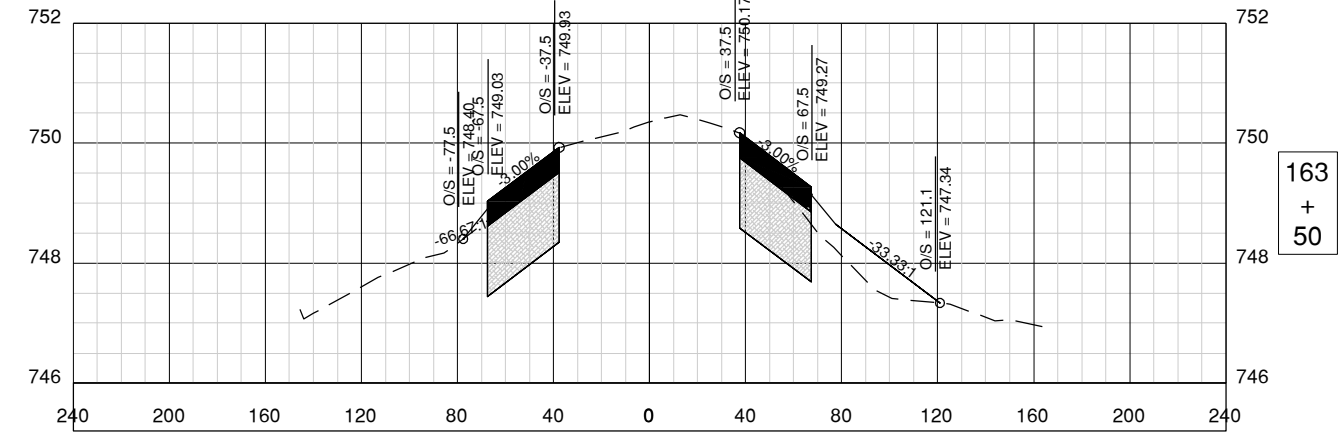
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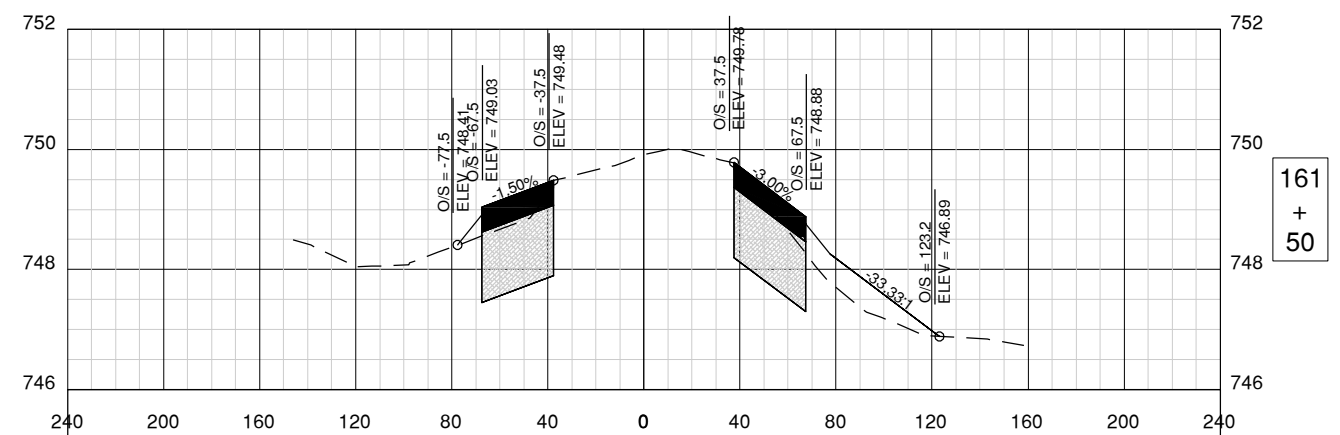
NUMBER	BY	DATE



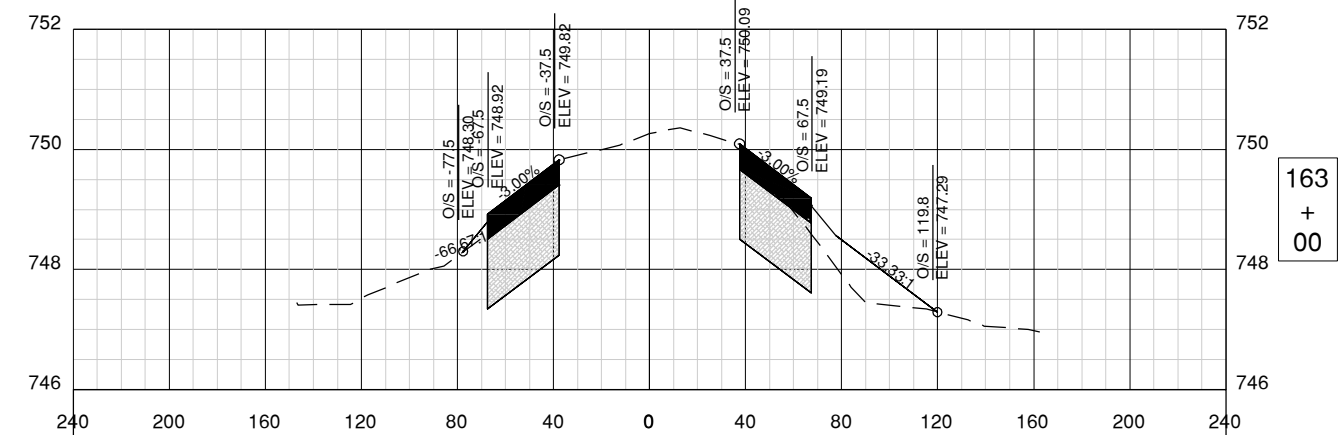
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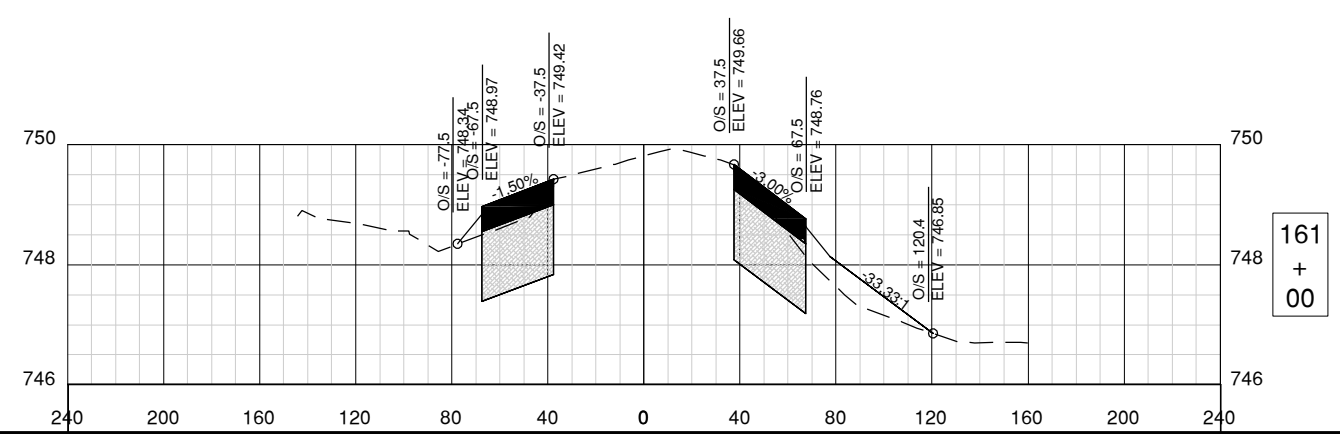
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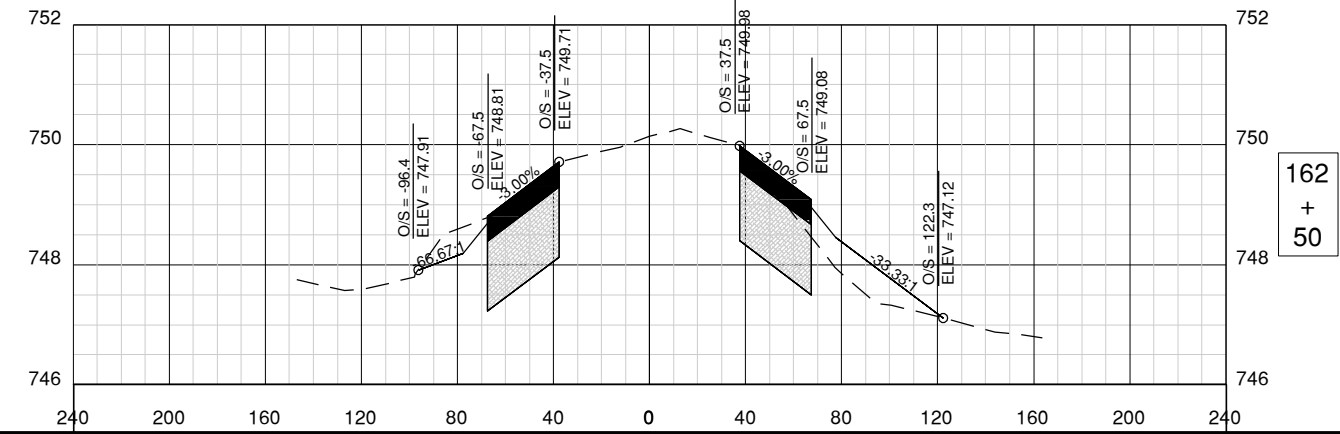
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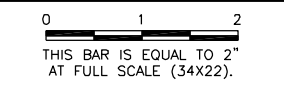
CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 7



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CHECKED BY:	CBG
APPROVED BY:	CET
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IL PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET	45 OF 52 SHEETS

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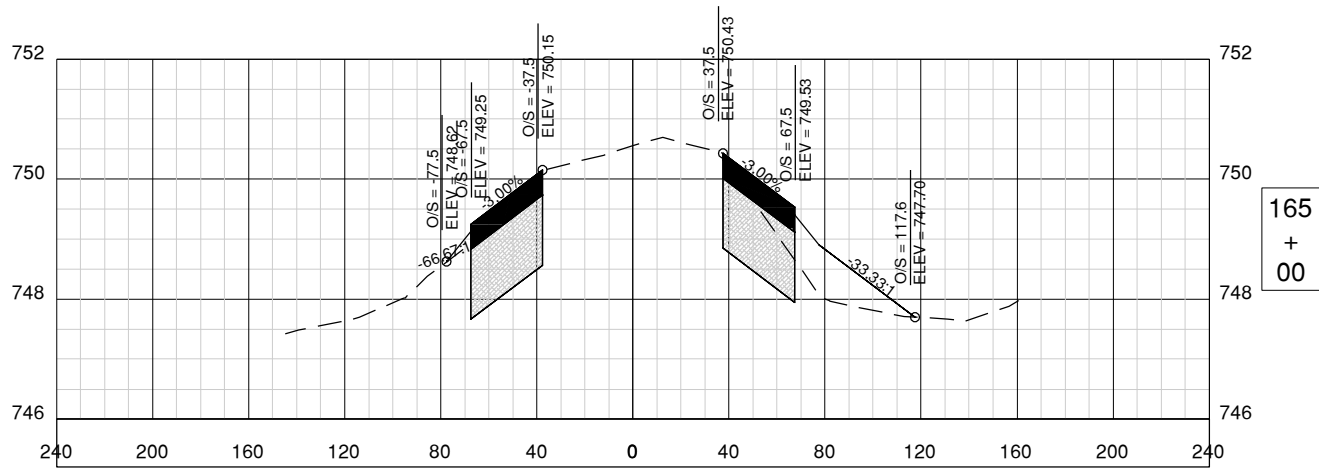


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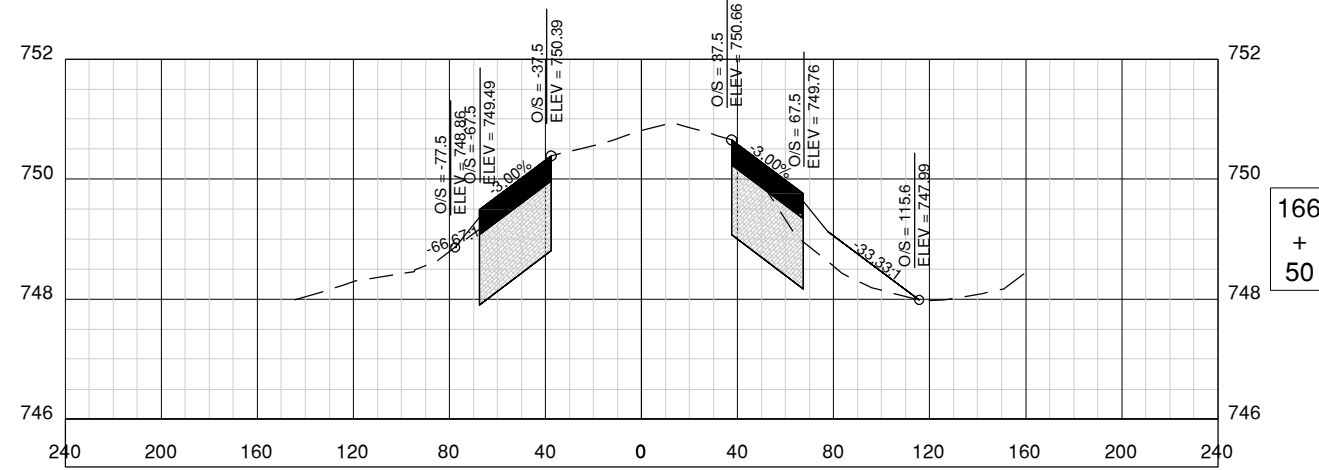
**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 8**



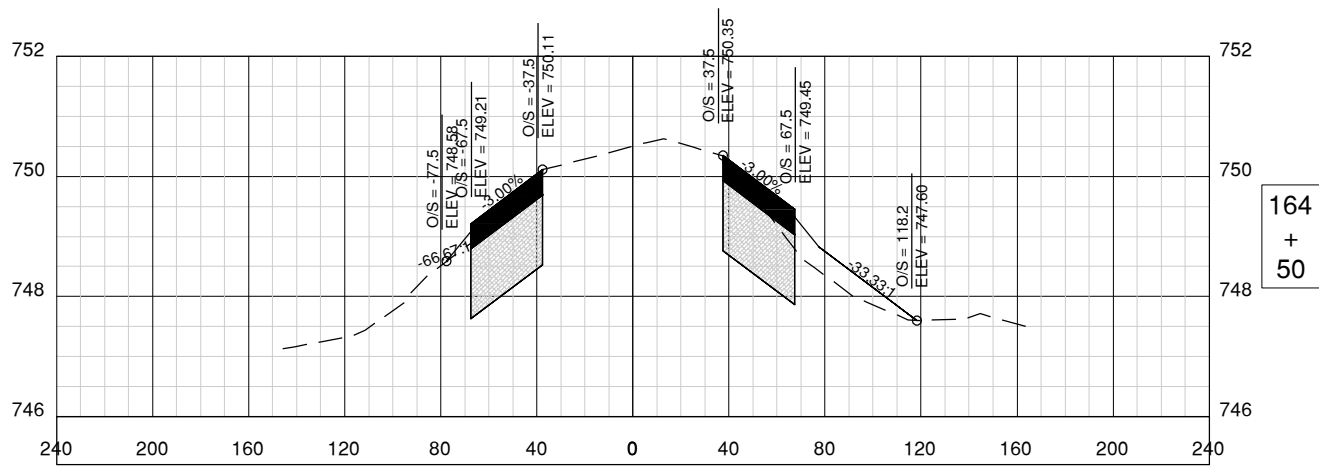
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DATE:	MAY 1, 2015
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IL. PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET 46 OF 52 SHEETS	



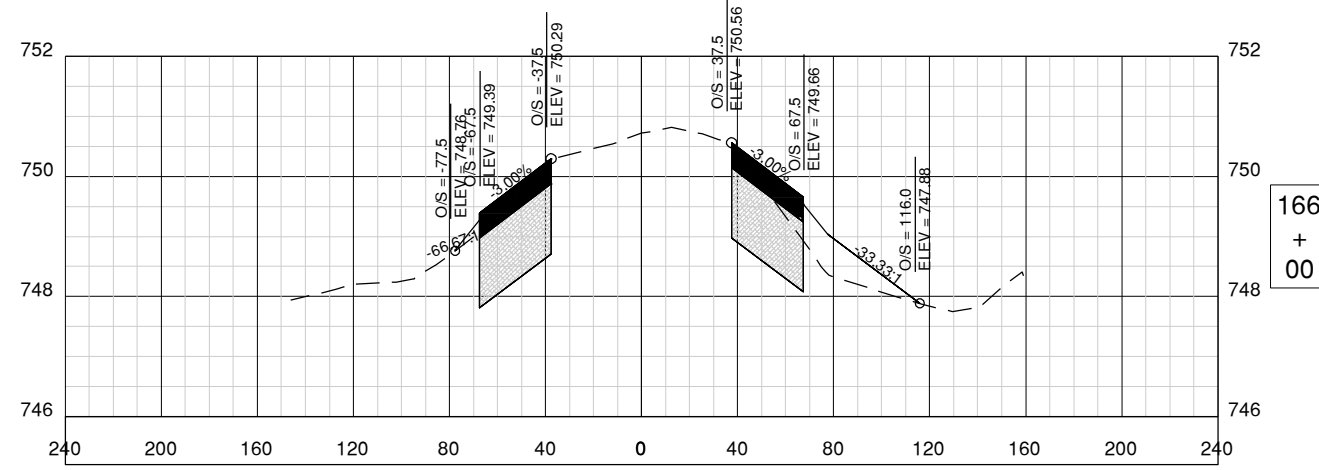
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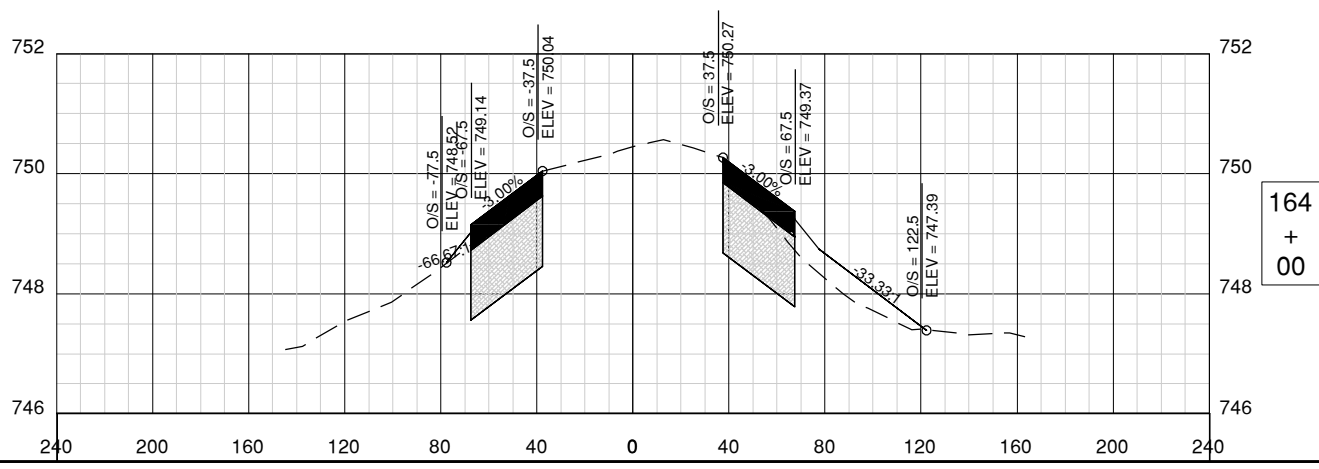
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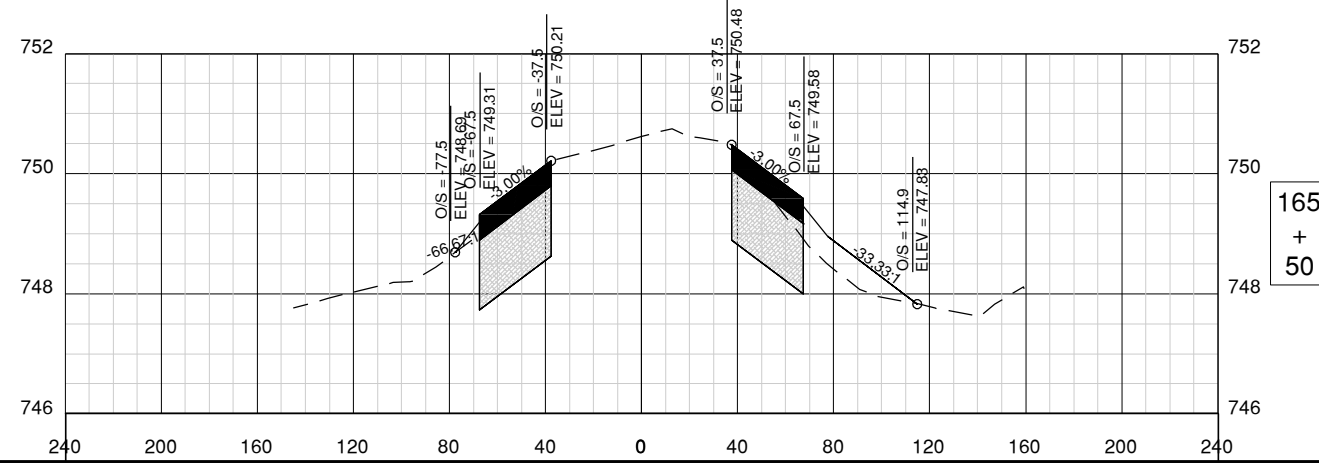
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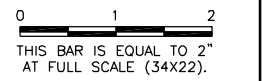
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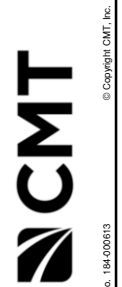
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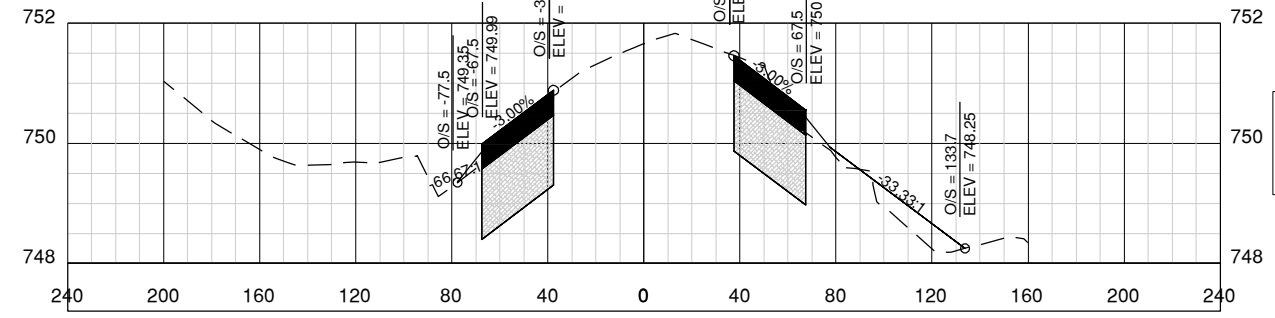
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**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 9**

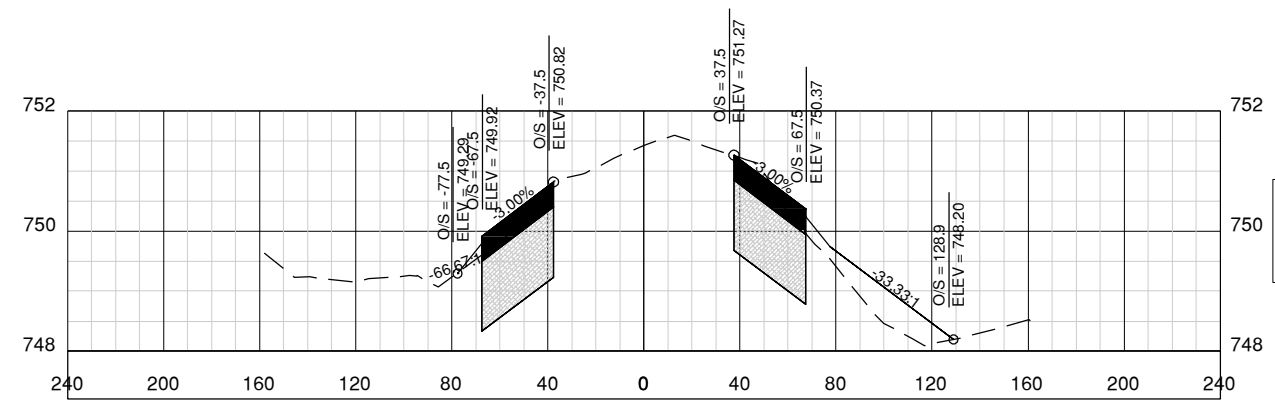


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AIP PROJ. NO:	3-17-0016-XX
SHEET 47 OF 52 SHEETS	

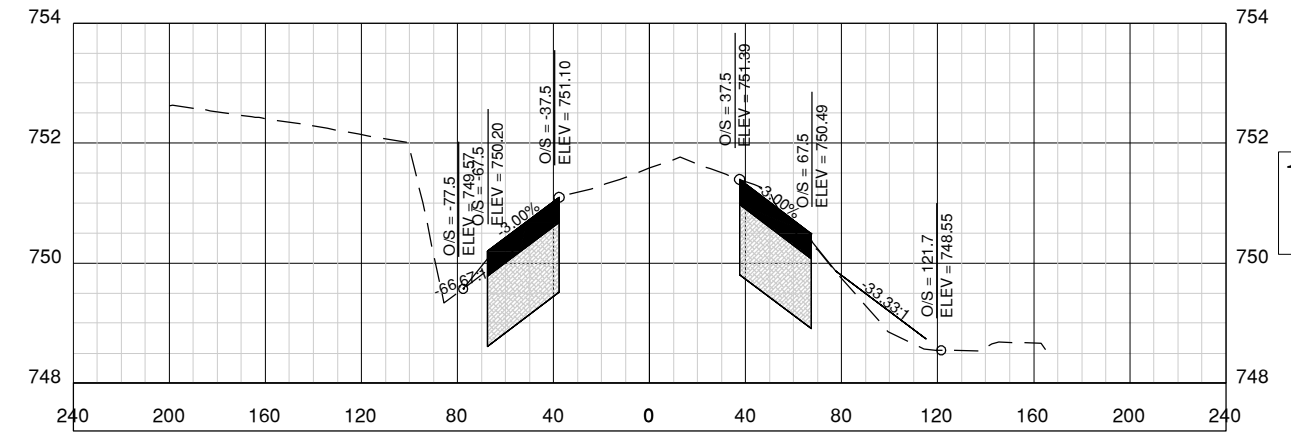
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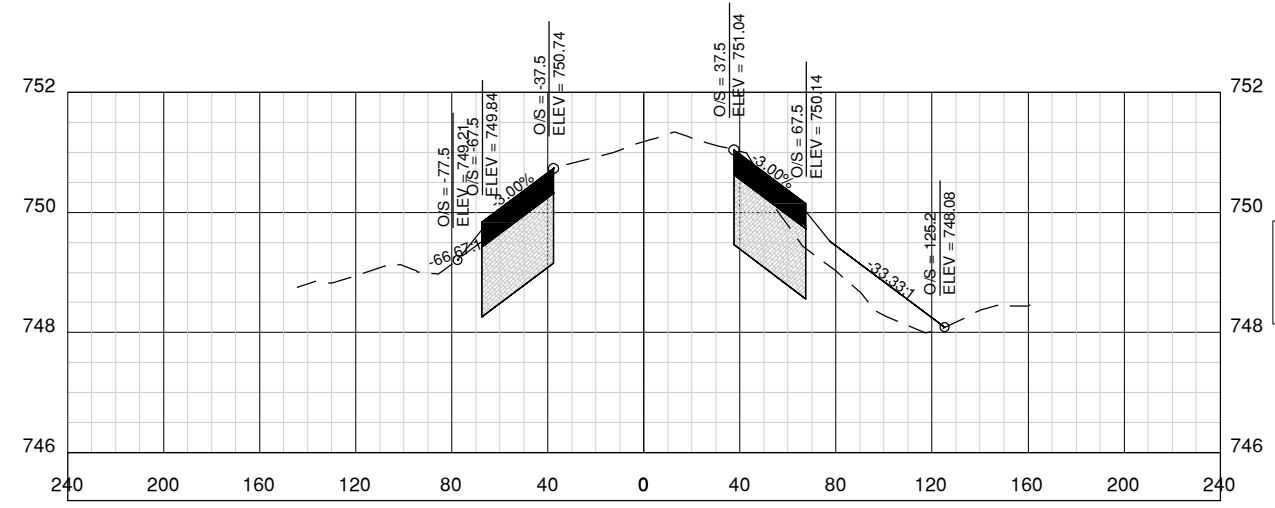
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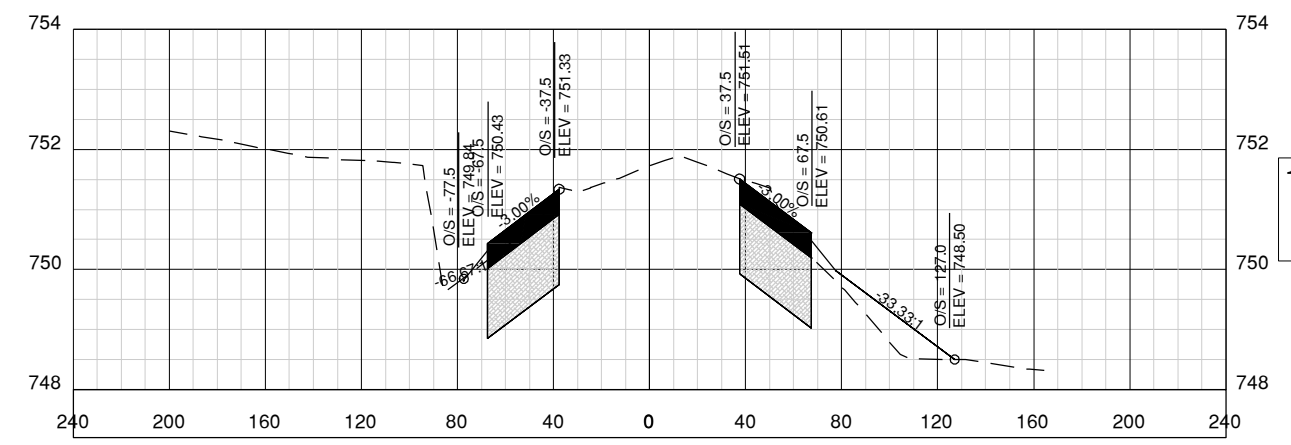
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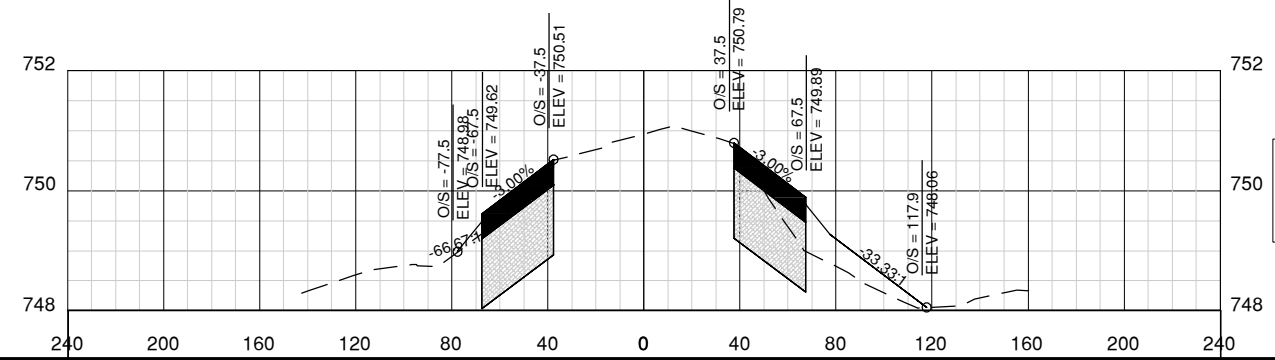
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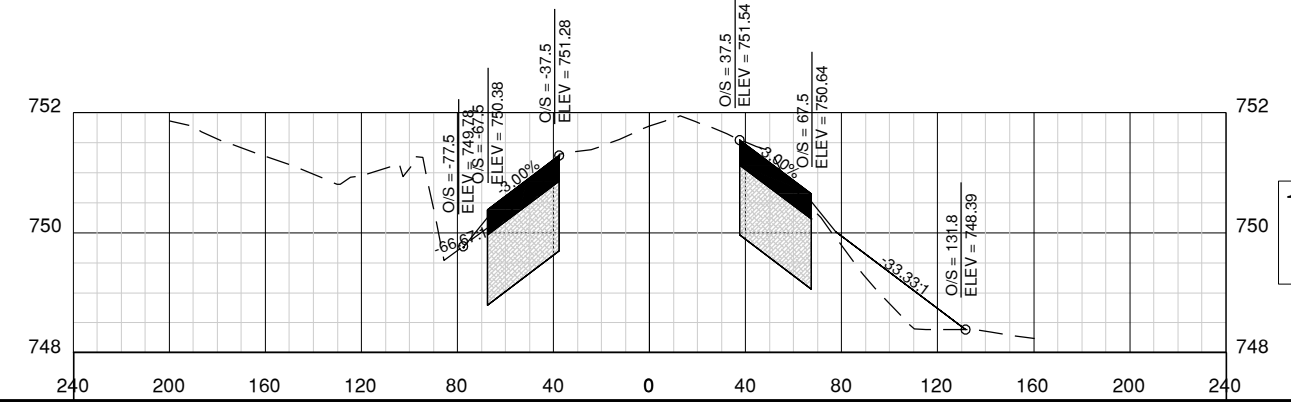
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 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34x22).

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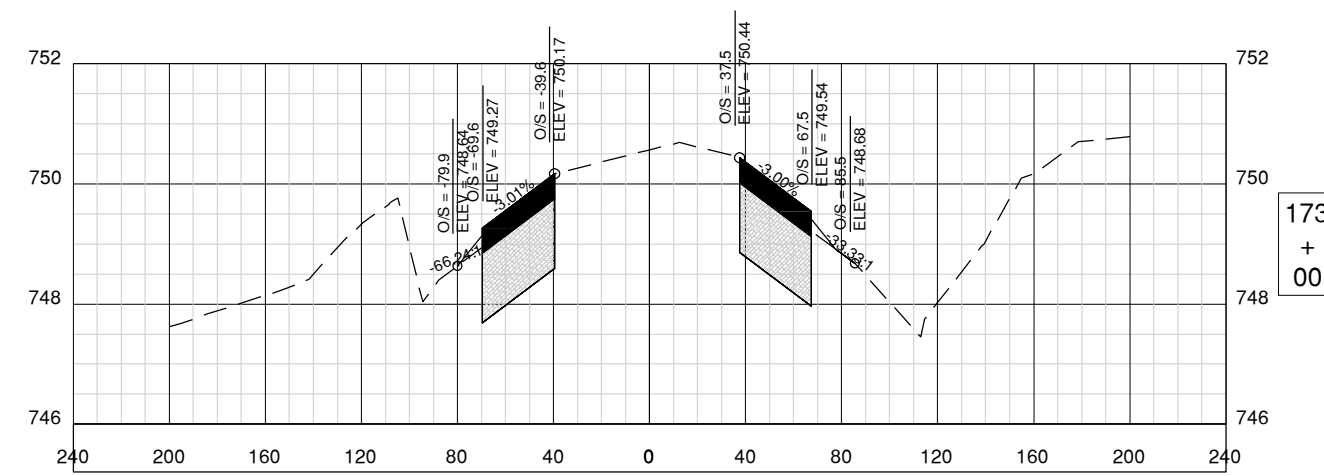
CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 10



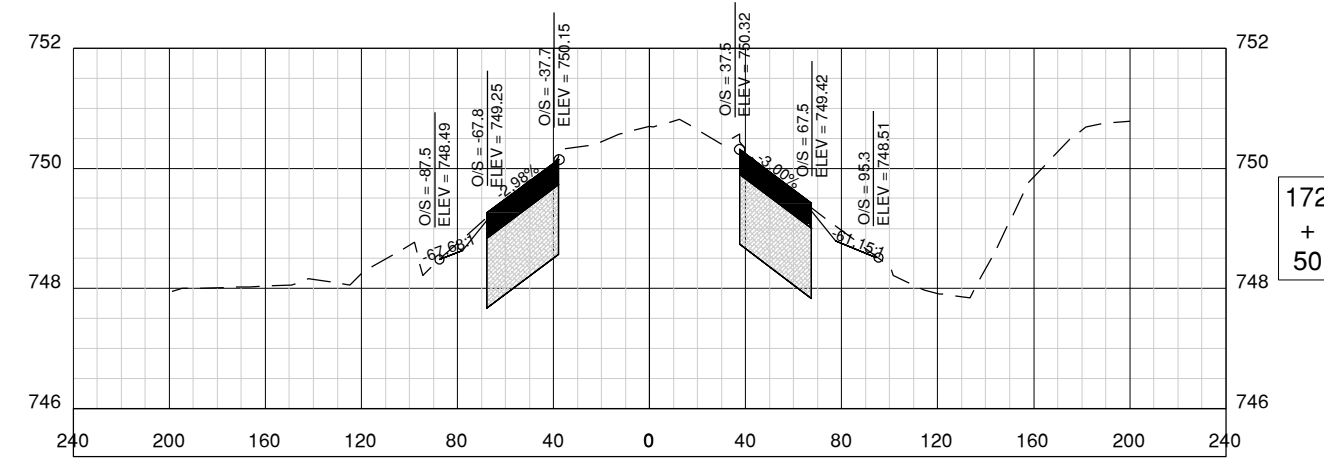
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JOB No:	14059-02-00

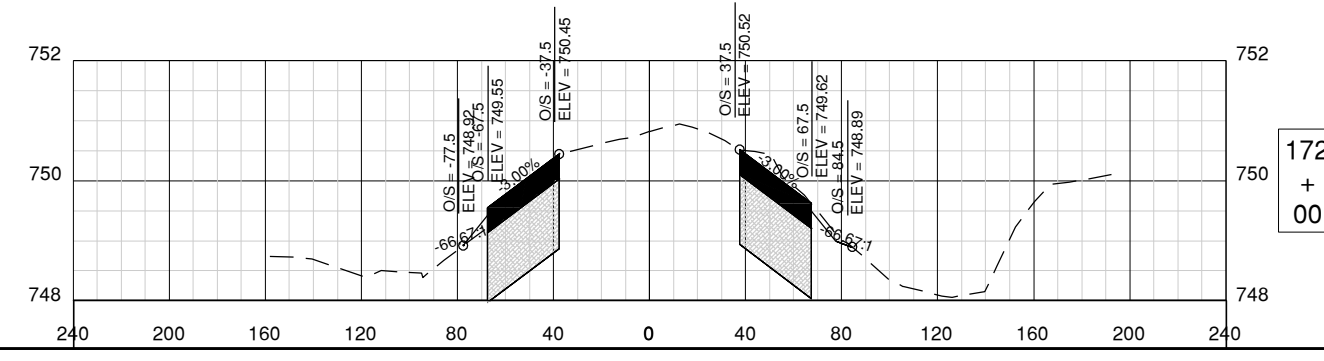
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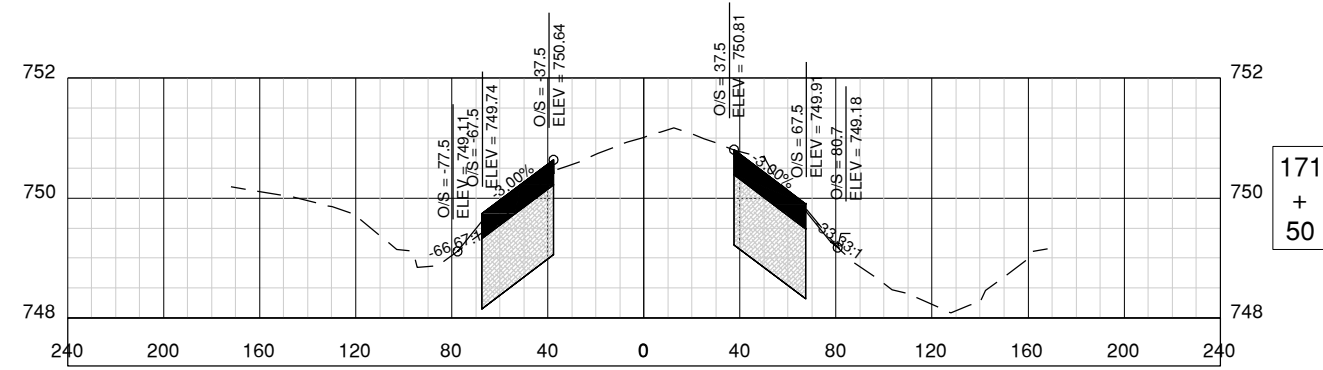
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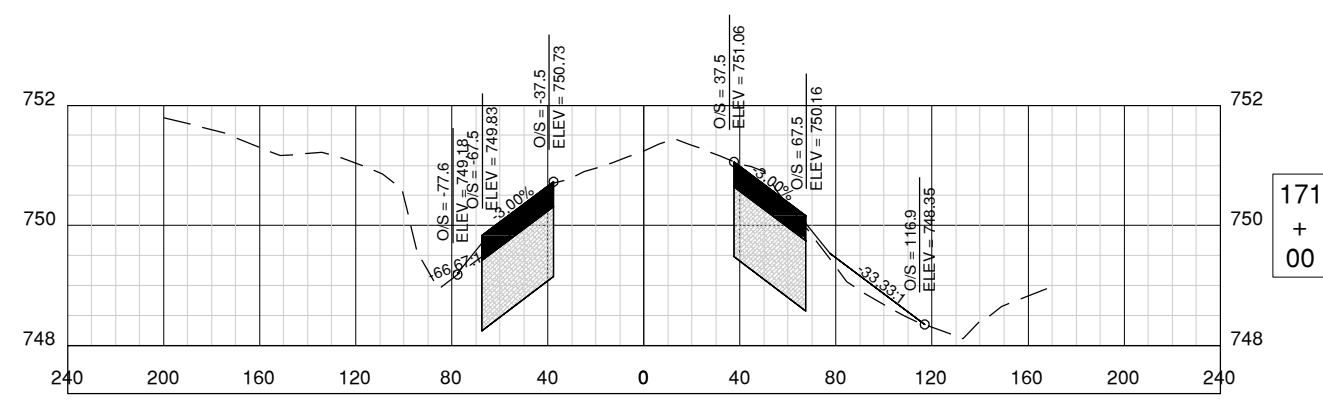
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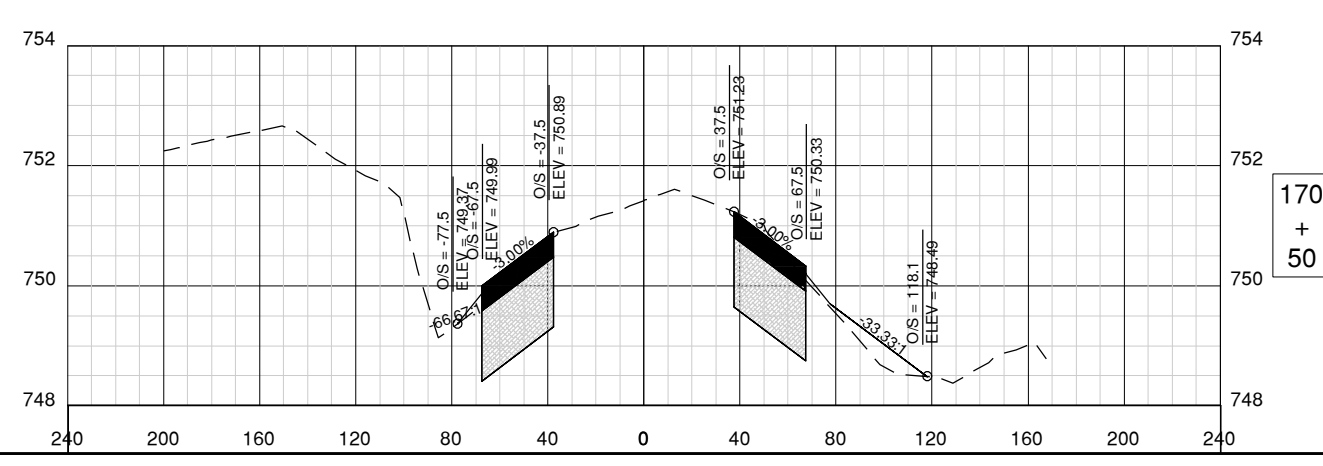
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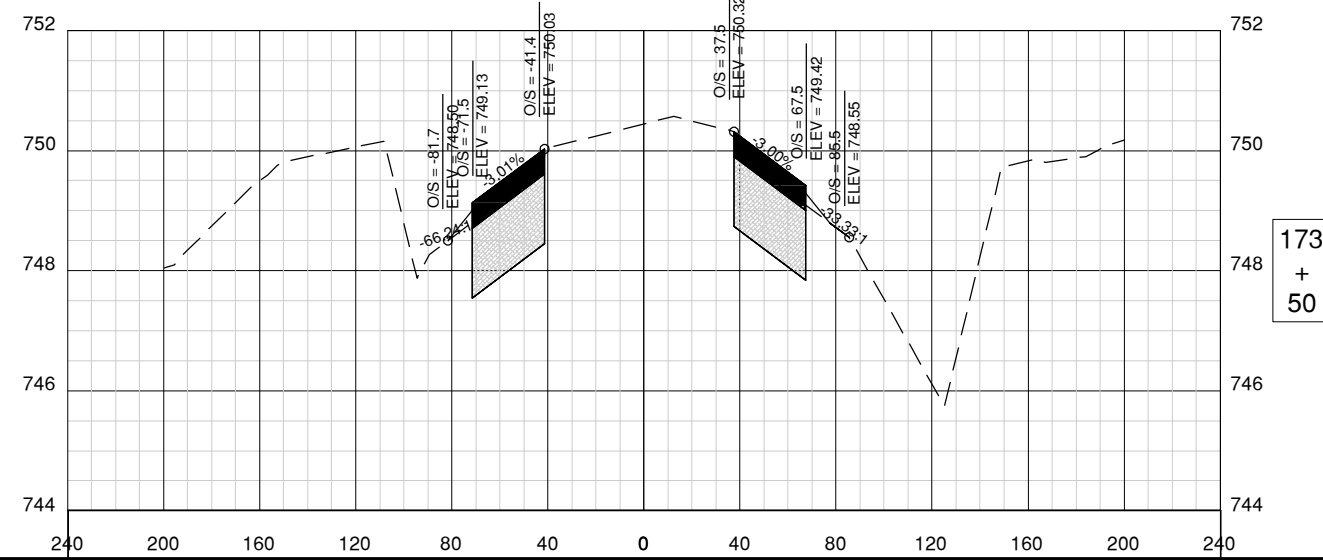
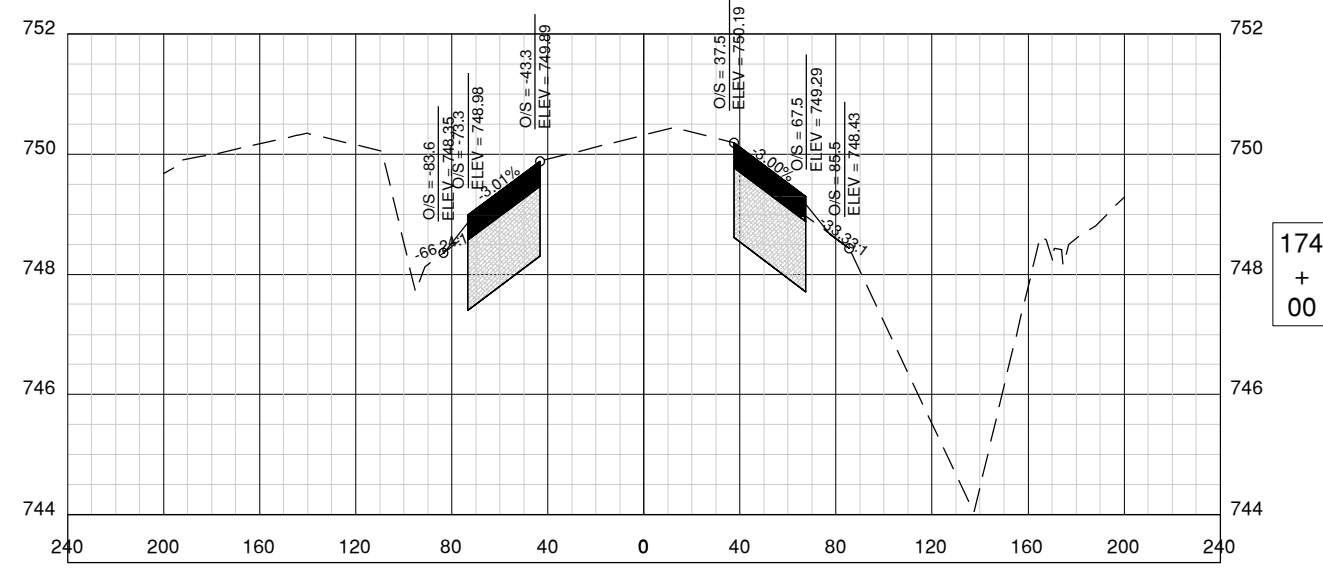
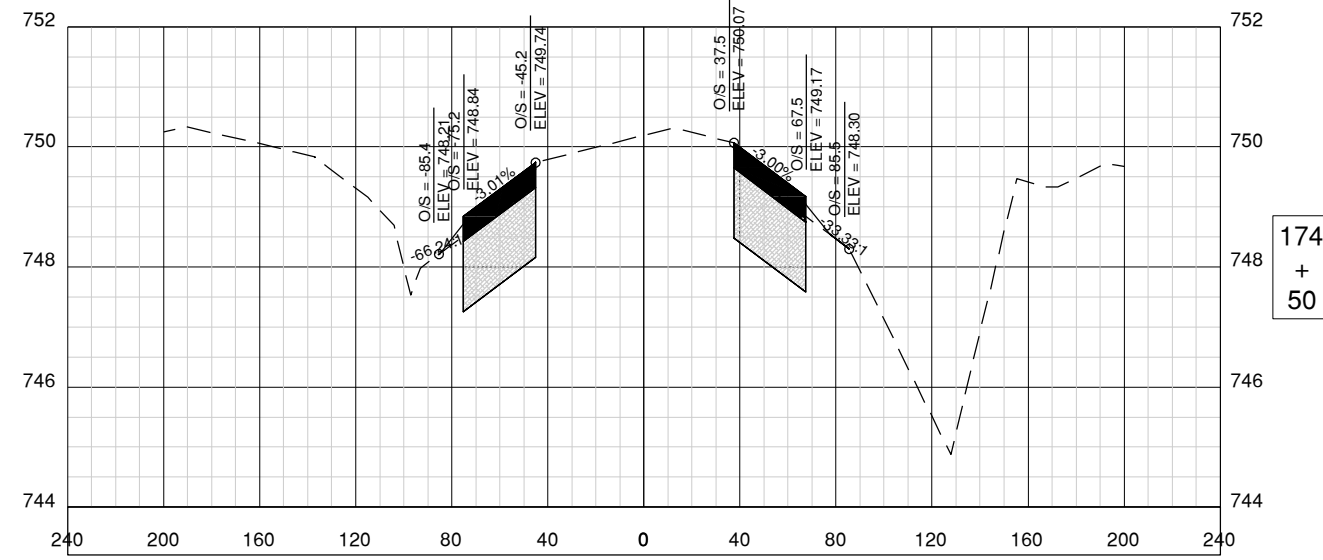
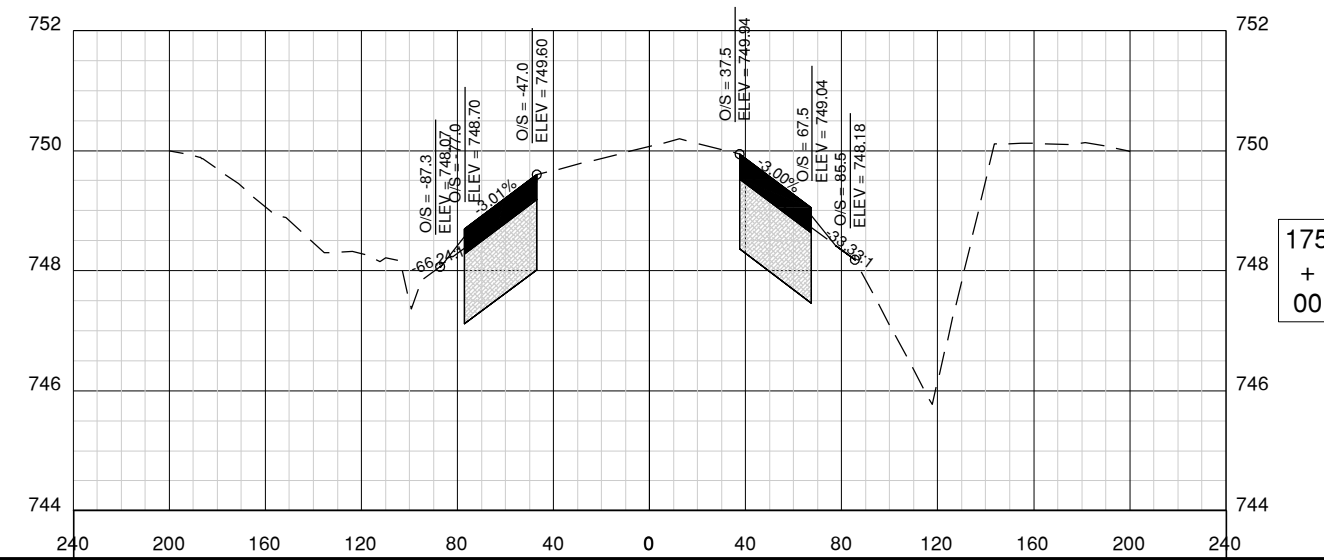
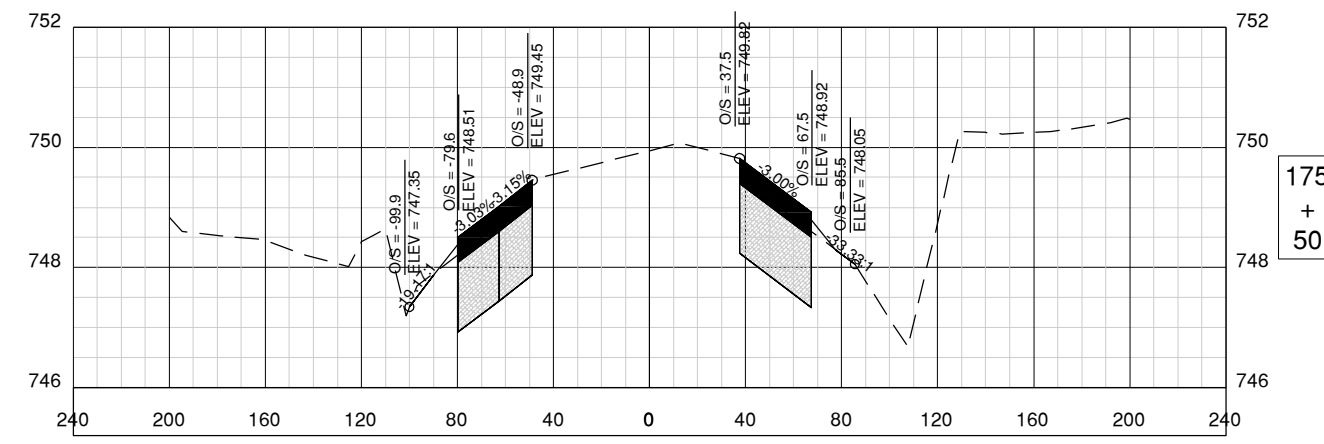
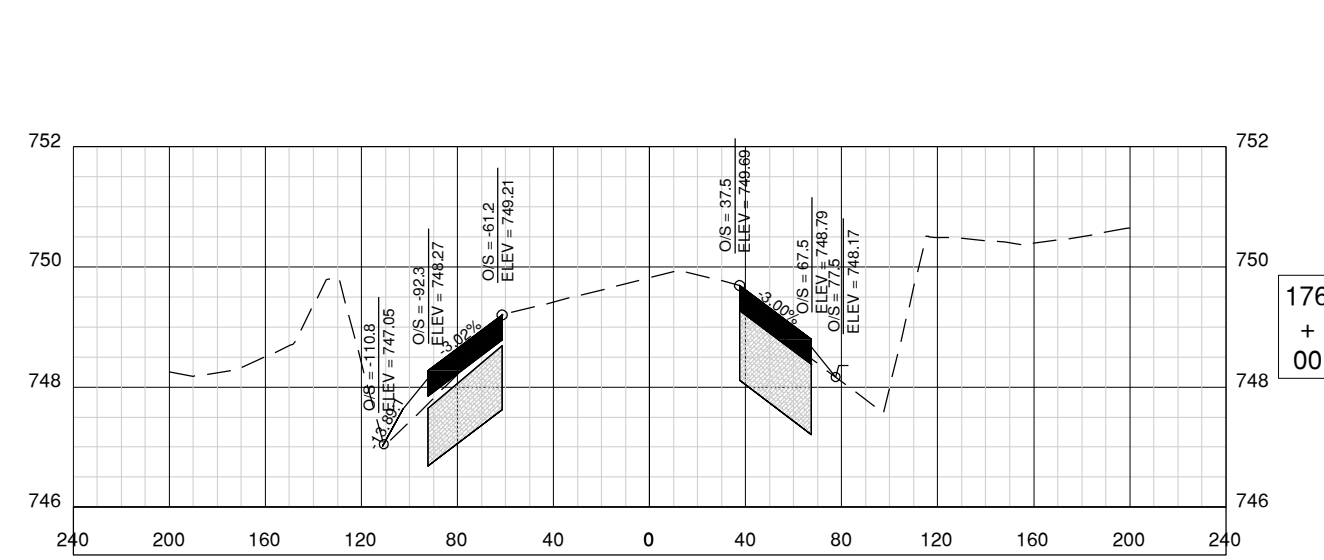
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CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 11



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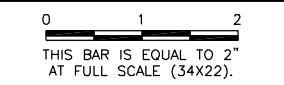
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DATE:	MAY 1, 2015
JOB No:	14059-02-00
IL PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET	49 OF 52 SHEETS



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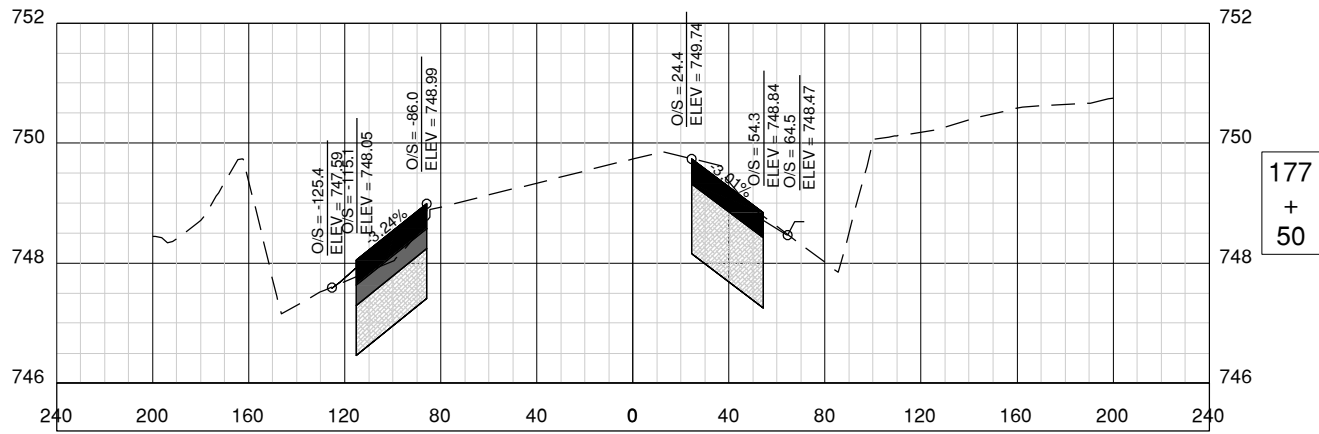


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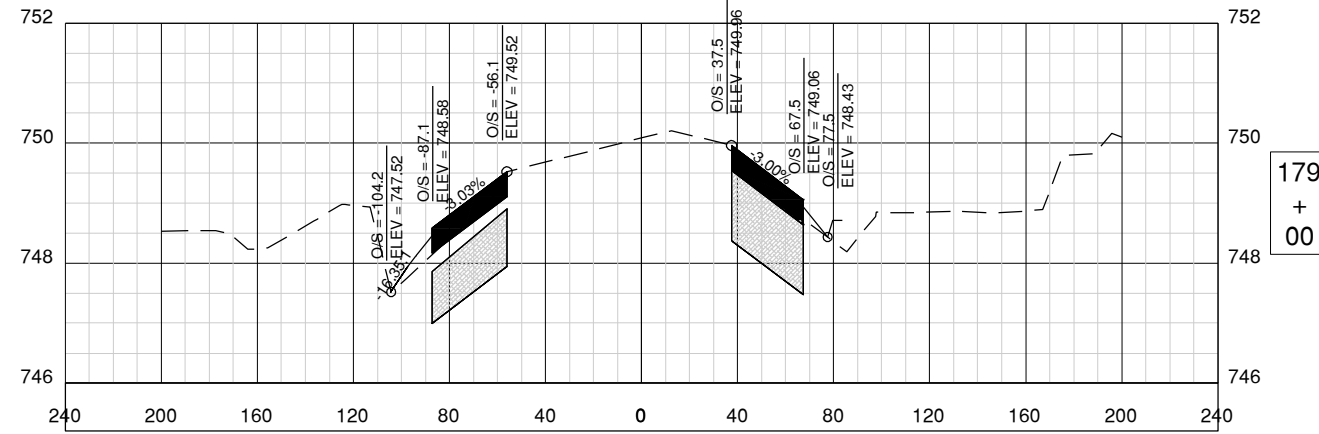
**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 12**



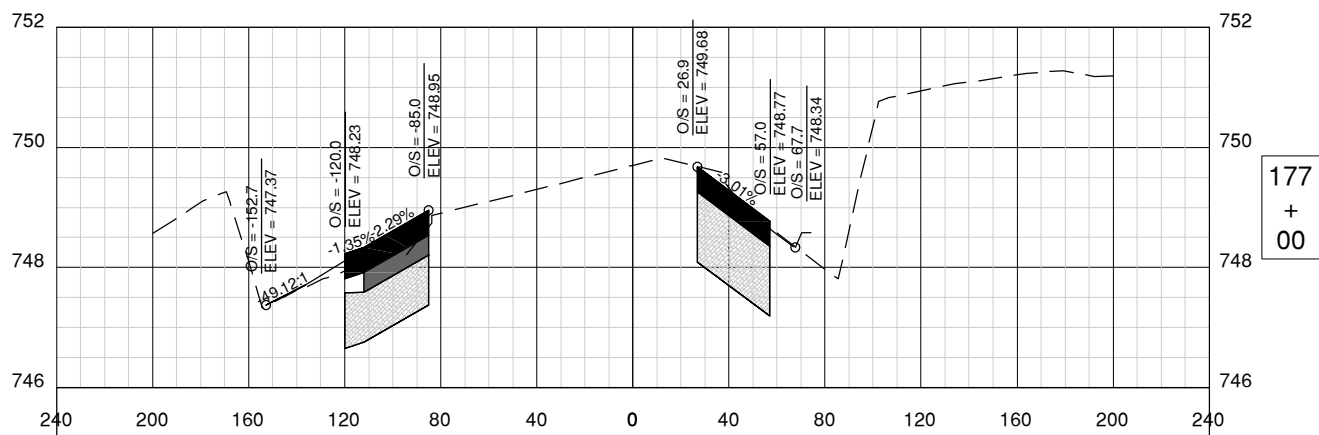
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AIP PROJ. NO.	3-17-0016-XX
SHEET 50 OF 52 SHEETS	



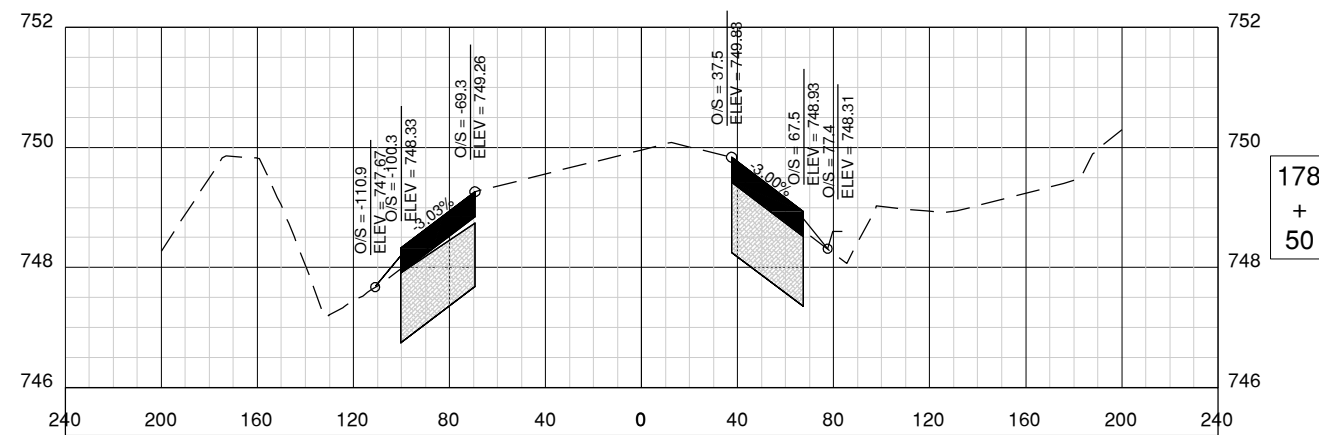
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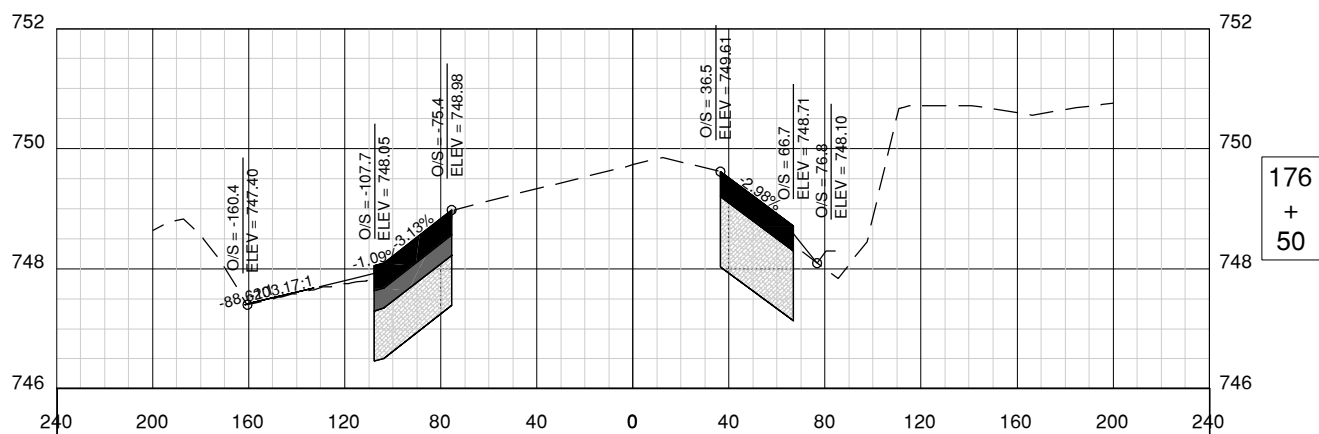
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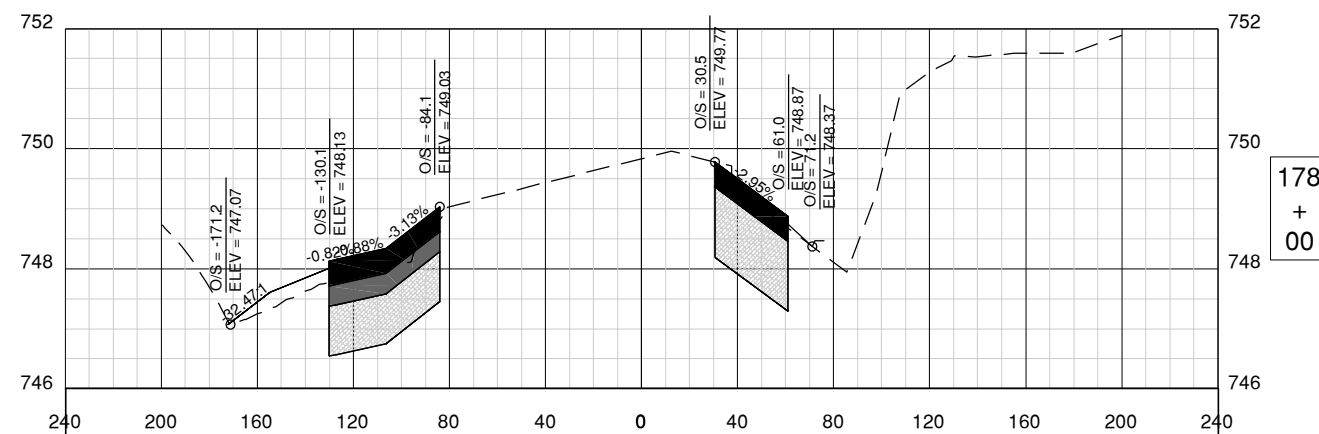
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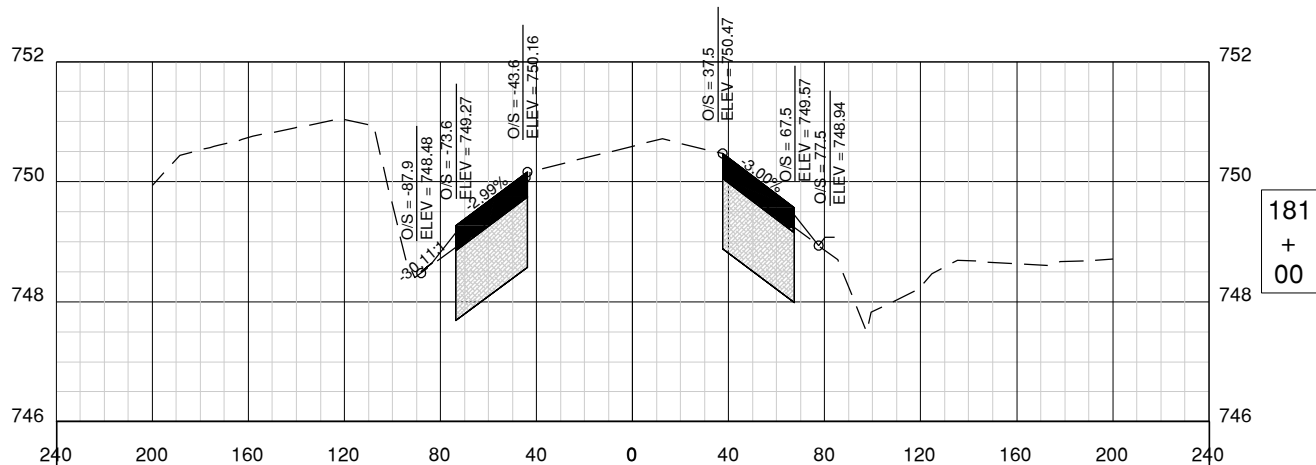
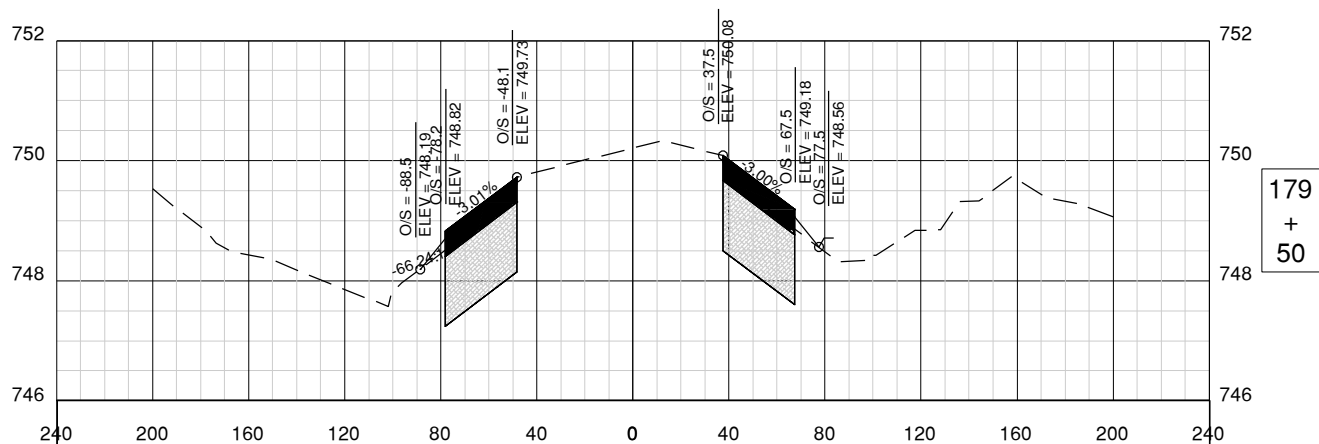
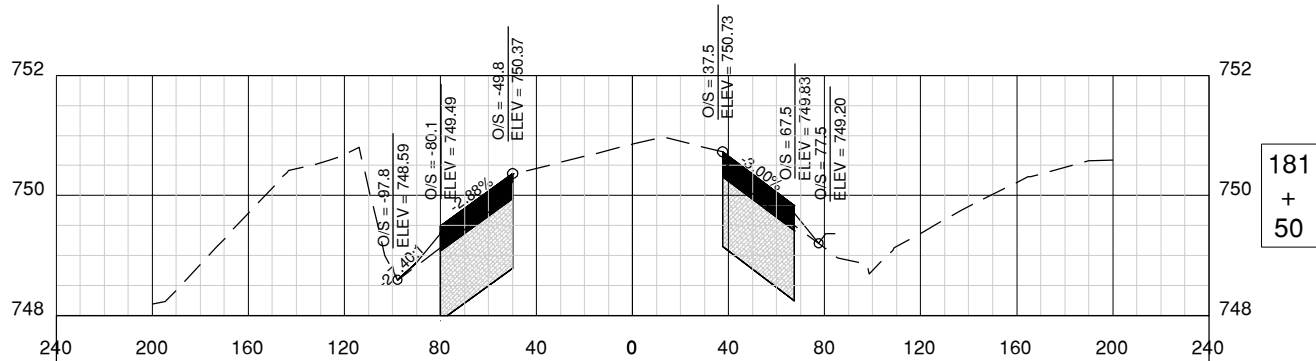
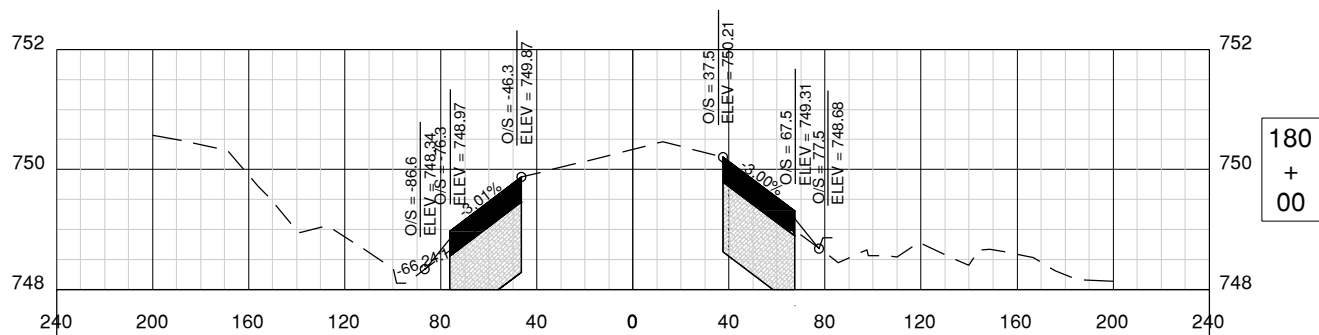
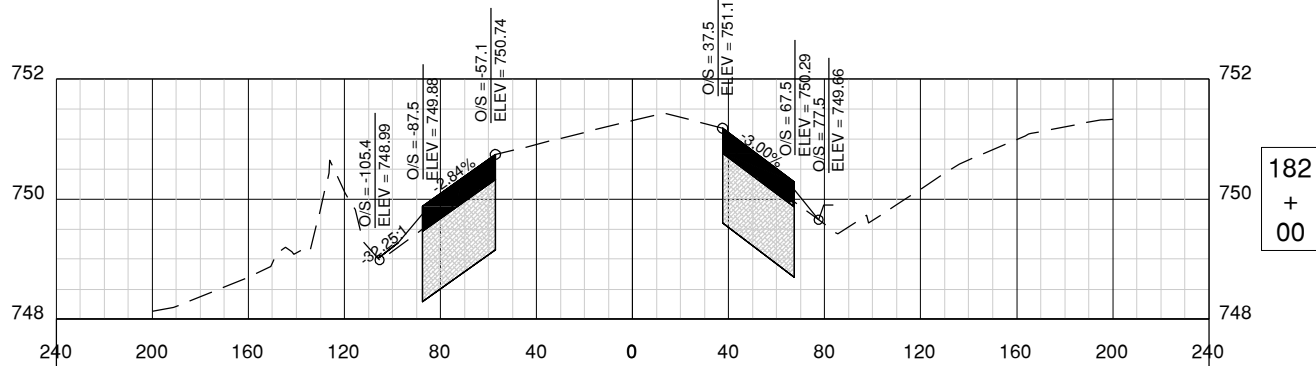
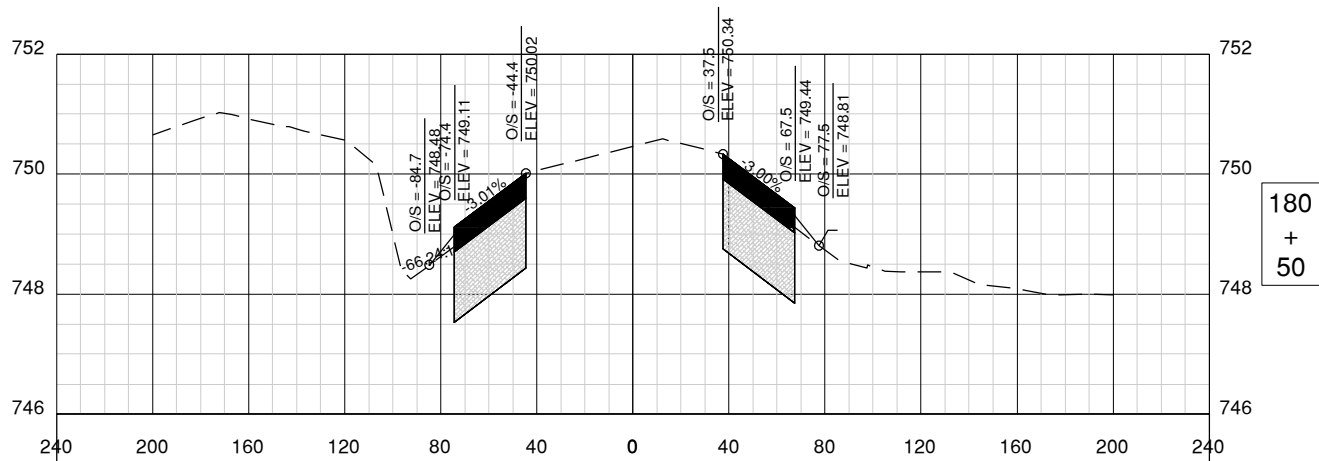
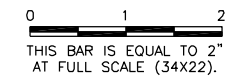
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**CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 13**



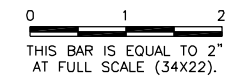
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IL PROJ. NO.	CMI-4382
AIP PROJ. NO.	3-17-0016-XX
SHEET	51 OF 52 SHEETS

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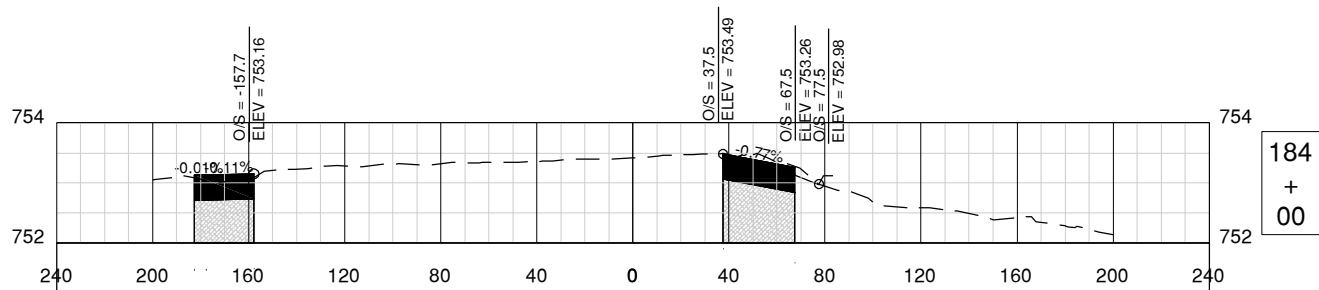


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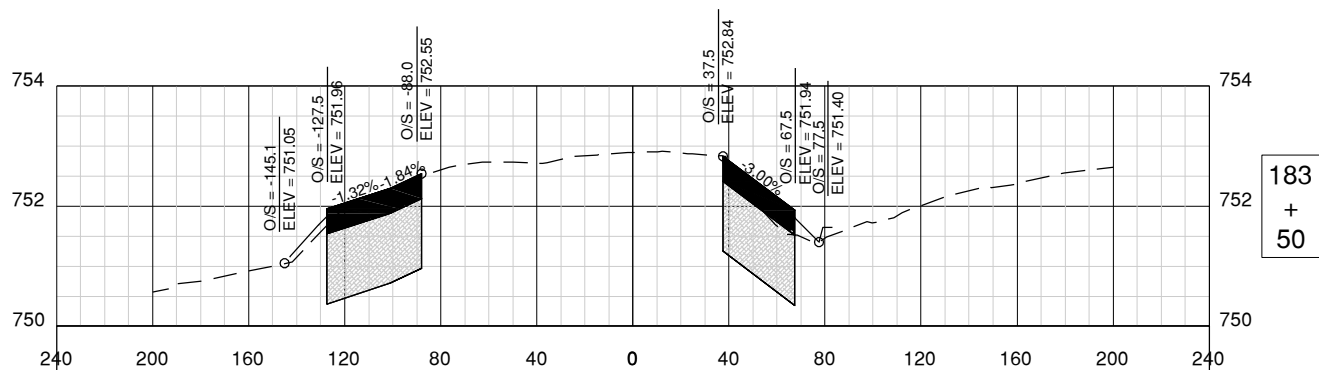
CONSTRUCT TAXIWAY C PAVED SHOULDERS
 PAVED SHOULDERS CROSS SECTIONS 14



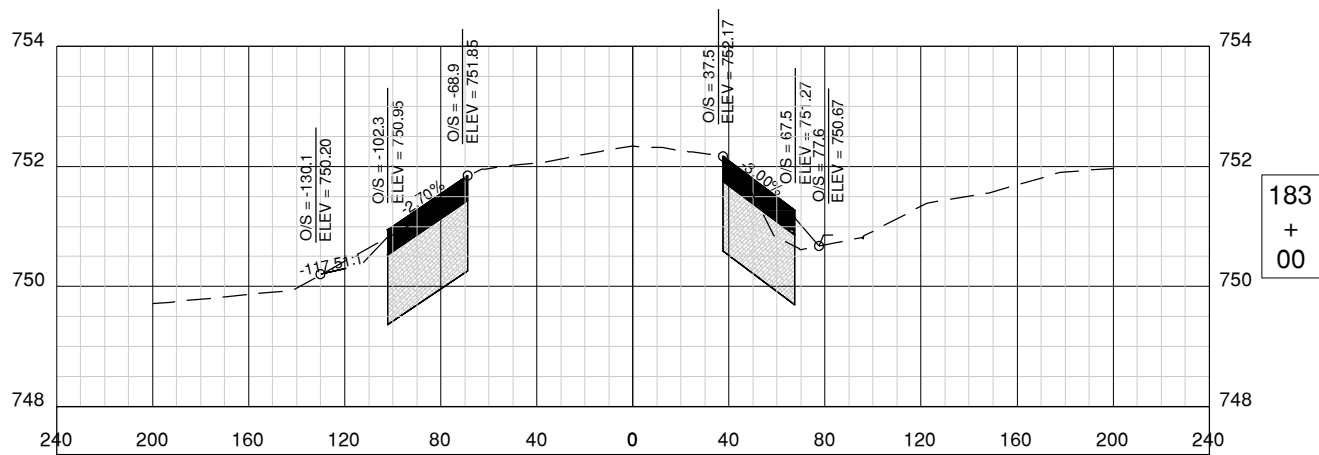
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AIP PROJ. NO.	3-17-0016-XX
SHEET	52 OF 52 SHEETS



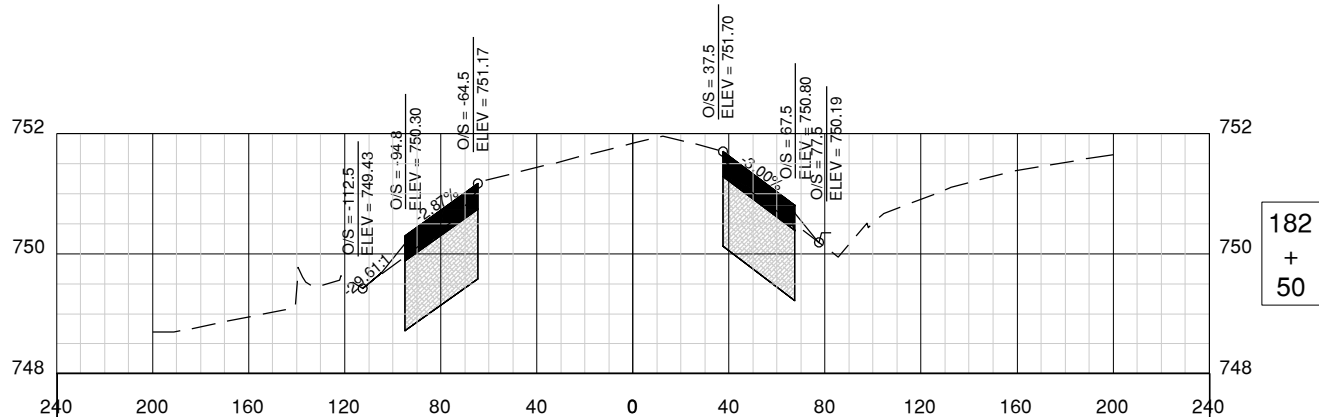
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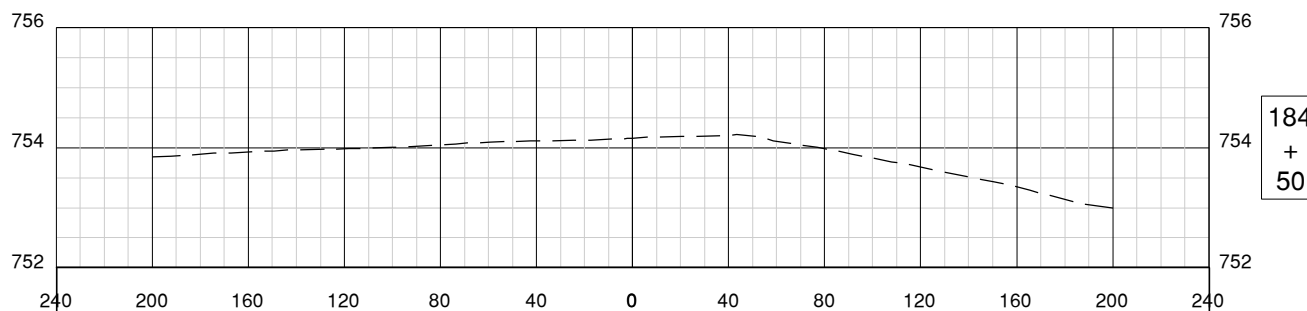
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