

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

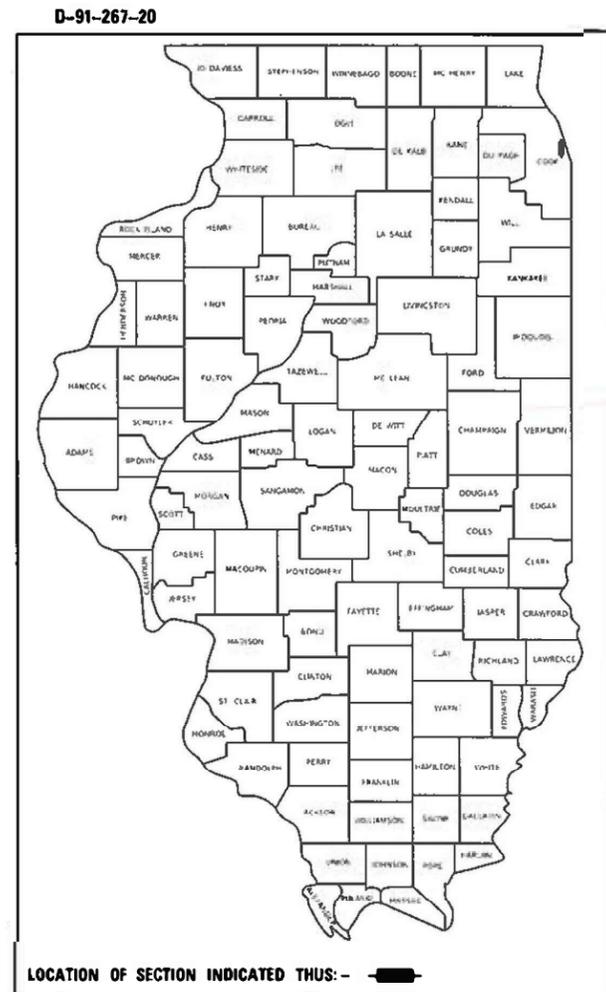
F.A.U. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-RS&SW	COOK	35	1
ILLINOIS			CONTRACT NO. 62J89	

\* 35+1+36 TOTAL SHEET

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE CITY OF CHICAGO

**PROPOSED  
HIGHWAY PLANS**  
ROUTE: F.A.U. 2911 (MICHIGAN AVE.)  
BALBO DR. TO GARFIELD BLVD.  
SECTION: 2019-097-RS&SW  
PROJECT: STP-AROX(962)  
STANDARD OVERLAY AND ADA IMPROVEMENTS  
COOK COUNTY  
C-91-048-20



**TRAFFIC DATA:**

SPEED LIMIT = 30 MPH

- E. BALBO DR. TO ROOSEVELT RD. = 21,100 ADT (2018)
- ROOSEVELT RD. TO E. CULLERTON = 16,000 ADT (2018)
- E. CULLERTON TO E. 31ST ST. = 10,000 ADT (2018)
- E. 31ST ST. TO GARFIELD BLVD. = 5,000 ADT (2018)

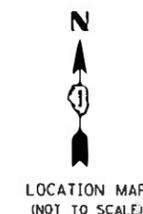
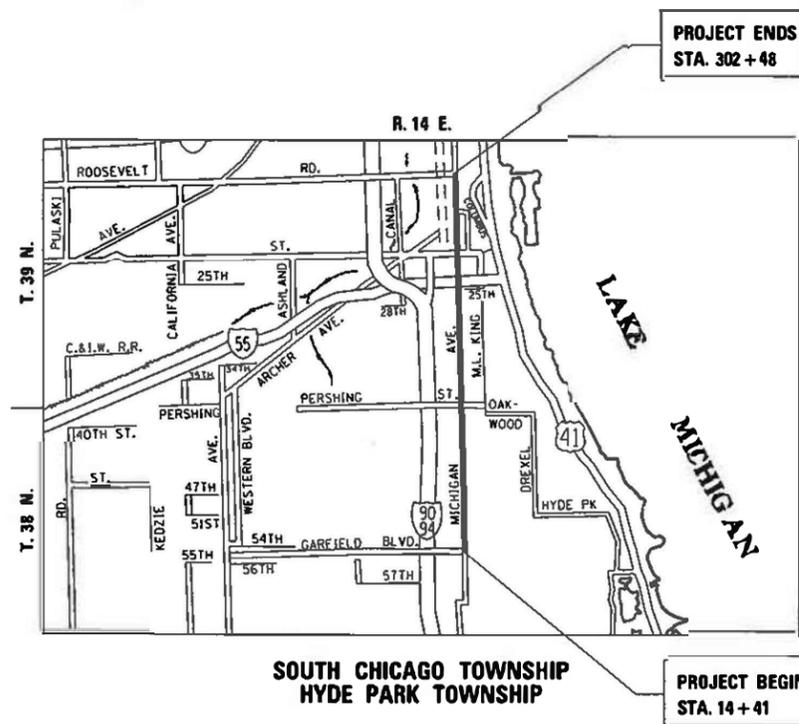


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: DANIEL WILGREEN (847) 705-4240  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62J89



**OMISSIONS:**

- STA 134+70 TO STA 135+59
- STA 208+79 TO STA 211+56
- STA 211+83 TO STA 227+66

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED: March 13 20 20

*[Signature]*  
REGIONAL ENGINEER

May 8, 2020

*[Signature]*  
ENGINEER OF DESIGN AND ENVIRONMENT

May 8, 2020

*[Signature]*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

GROSS LENGTH = 28,807 FT. = 5.46 MILES  
NET LENGTH = 26,858 FT. = 5.09 MILES

**INDEX OF SHEETS**

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1	COVER SHEET
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**STATE STANDARDS (CONT.)**

701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES

**GENERAL NOTES**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITY OF CHICAGO.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- SIDEWALK RAMPS MODIFICATIONS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO THE APPLICABLE HIGHWAY STANDARDS INCLUDED IN THE PLANS.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- SIDEWALK REMOVAL AND P.C.C. SIDEWALK 5' LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT [KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV](mailto:KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV) A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN AT [PATRICE.HARRIS@ILLINOIS.GOV](mailto:PATRICE.HARRIS@ILLINOIS.GOV) A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

**GENERAL NOTES (CONT.)**

- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
- THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.
- OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.

**CITY OF CHICAGO NOTES**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N." (CHICAGO UTILITY ALERT NETWORK) AT (312)744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.
- ALL CATCH BASINS IN THE CITY OF CHICAGO MUST MEET THE DEPARTMENT OF SEWERS' STANDARDS.
- PERFORATED LIDS SHALL BE PLACED ON ALL MANHOLES AND CATCH BASINS.
- BENCH MONUMENT LOCATIONS WITHIN THE LIMITS OF THE IMPROVEMENT CAN BE OBTAINED IN SUITE 410 - 333 SOUTH STATE STREET, CHICAGO, IL 60604. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF REPLACEMENT OF ANY BENCH MONUMENT IF DAMAGED OR DESTROYED DURING CONSTRUCTION.
- ALL BROKEN, CRACKED, WORN OR OTHERWISE DAMAGED OR BICYCLE UNSAFE FRAMES AND LIDS ON SEWER STRUCTURES, SHALL BE REPLACED WITH NEW DEPARTMENT OF SEWERS' STANDARD FRAMES AND LIDS.
- OPEN LID DRAINAGE STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION OF THIS ROADWAY WITHOUT THE WRITTEN PERMISSION FROM THE CITY OF CHICAGO.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY OF CHICAGO AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- PERMITS FROM THE DEPARTMENT OF SEWERS ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY OR COMBINED SEWER SYSTEM CONSTRUCTION, AND FOR RESURFACING WORK INVOLVING ADJUSTMENT OF SEWER STRUCTURES. THE DEPARTMENT OF SEWERS' PERMIT MUST BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF CONSTRUCTION.
- THE CONTRACTOR SHALL TAKE EXTRA CARE AND PRECAUTION WHEN REPAIRING THE CONCRETE CURB AND GUTTER AT LOCATIONS WHERE EXISTING TREES ARE PRESENT.
- CURB RAMPS AND LANDING (KEYSTONE) TO BE CONSTRUCTED WITH 8" THICK CONCRETE AT ALL TRAFFIC SIGNALIZED INTERSECTIONS AND INDUSTRIAL STREET INTERSECTIONS. AT ALL OTHER LOCATIONS, 5" THICK CONCRETE TO BE USED.

REV-SEP

STANDARD NO.	DESCRIPTION
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-04	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-04	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-05	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-05	DEPRESSED CORNER FOR SIDEWALKS
424026-03	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
424031-02	MEDIAN PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, SHORT TIME OPERATIONS
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS <= 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED

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PLOT DATE = 5/4/2020	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MICHIGAN AVE. (BALBO DR. TO GARFIELD BLVD.)  
INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	2
CONTRACT NO. 62J89				
ILLINOIS		FED. AID PROJECT		

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005					
20200100	EARTH EXCAVATION	CU YD	9	9					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	472	472					
25200110	SODDING, SALT TOLERANT	SO YD	472	472					
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SO YD	19	19					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	119037	119037					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	265	265					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	1853	1853					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	3153	3153					
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	7257	7257					
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	3	3					
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	14777	14777					
42001300	PROTECTIVE COAT	SO YD	2877	2877					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005					
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SO YD	105	105					
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	187	187					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	1573	1573					
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SO FT	467	467					
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	175906	175906					
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	311	311					
44000600	SIDEWALK REMOVAL	SO FT	2040	2040					
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SO YD	14074	14074					
44201777	CLASS D PATCHES, TYPE II, 11 INCH	SO YD	4352	4352					
44201781	CLASS D PATCHES, TYPE III, 11 INCH	SO YD	242	242					
44201783	CLASS D PATCHES, TYPE IV, 11 INCH	SO YD	9480	9480					
44209000	LONGITUDINAL PARTIAL DEPTH PATCHING	TON	75	75					

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>SUMMARY OF QUANTITIES</b>			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-RS&SW	COOK	35	3
			CONTRACT NO. 62J89	
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				

\* = SPECIALTY ITEMS  
REV-SEP

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005				
60250200	CATCH BASINS TO BE ADJUSTED	EACH	19	19				
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	4	4				
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	11	11				
60266600	VALVE BOXES TO BE ADJUSTED	EACH	43	43				
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	27	27				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	18	18				
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	15	15				
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	34	34				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	9	9				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2				
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1				
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1				
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	5	5				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12				
67100100	MOBILIZATION	L SUM	1	1				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1				
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	32708	32708				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	10903	10903				
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	1394	1394				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	42062	42062				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	4834	4834				
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	1191	1191				

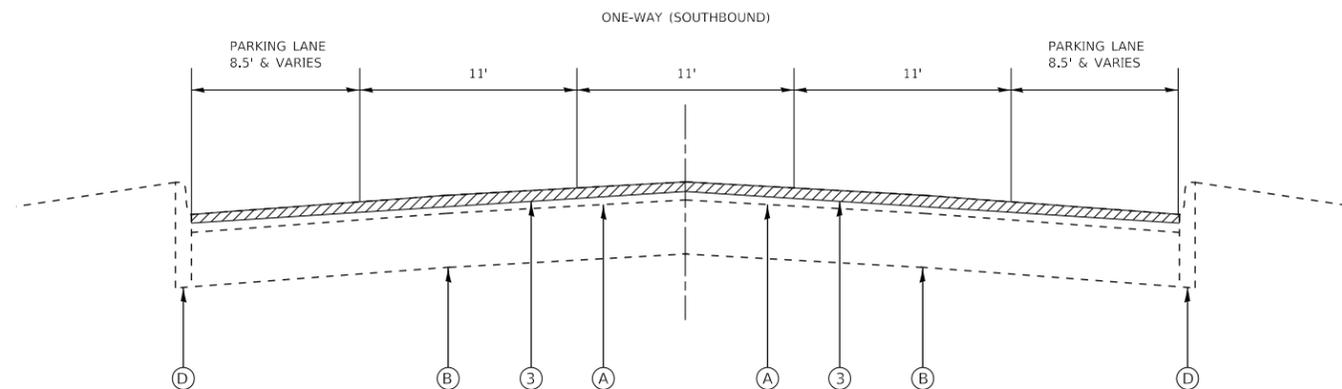
\* = SPECIALTY ITEMS

REV-SEP

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	80% FEDERAL 20% STATE 0005						CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	80% FEDERAL 20% STATE 0005					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	862	862						X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	2250	2250					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	9671	9671						X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	260	260					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	4807	4807						X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	35698	35698					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1394	1394						Z0001110	GAS VALVE TO BE ADJUSTED	EACH	1	1					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	42062	42062						Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	3087	3087					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	4834	4834						Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	225	225					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	1191	1191						Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	52	52					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	862	862						Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	60791	60791					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	9671	9671						Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1					
										Z0076600	TRAINEES	HOUR	500	500					
										Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	500	500					
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1															
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	7	7															
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	11	11															
X4240800	DETECTABLE WARNINGS (SPECIAL)	SO FT	244	244															
X4405020	LONGITUDINAL PARTIAL DEPTH REMOVAL 2"	FOOT	2000	2000															

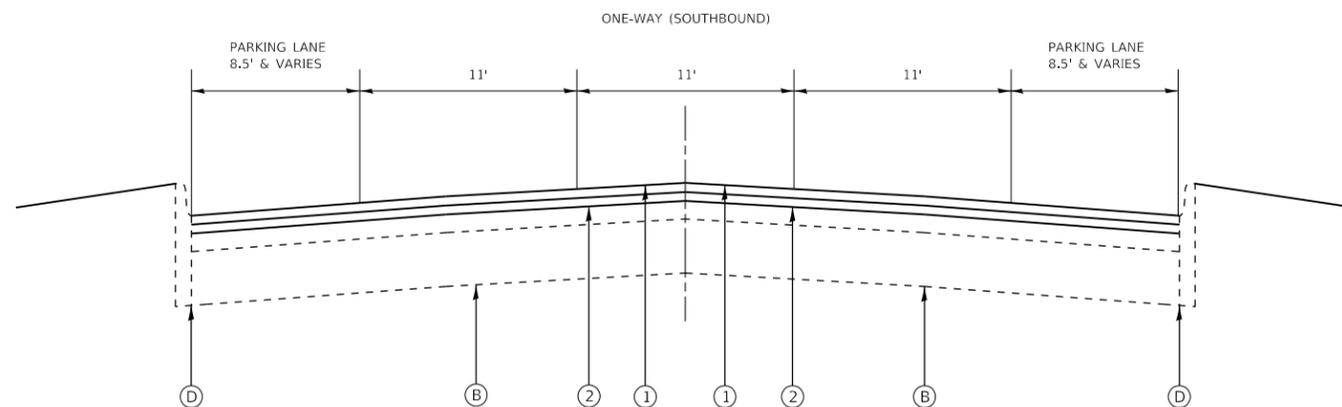
\* = SPECIALTY ITEMS  
 NON-PART 100% STATE REV-SEP

MICHIGAN AVE.



STA. 14+41 TO STA. 134+70  
 STA. 135+59 TO STA. 178+07

MICHIGAN AVE.



STA. 14+41 TO STA. 42+66  
 STA. 42+66 TO STA. 55+59 LT  
 STA. 55+59 TO STA. 134+70  
 STA. 135+59 TO STA. 178+07

**LEGEND - EXISTING:**

- (A) HOT-MIX ASPHALT PAVEMENT, 3.5" & VARIES
- (B) PORTLAND-CEMENT CONCRETE PAVEMENT, 10.75" & VARIES
- (C) COMBINATION CONCRETE CURB & GUTTER, 8-6.12
- (D) BARRIER CURB

**LEGEND - PROPOSED**

- (1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70; 1.50"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL 4.75 N50; 0.75"
- (3) HOT-MIX ASPHALT SURFACE REMOVAL, 2.25"

**NOTES:**

- 1.) THE CONTRACTOR SHALL DO PATCHES BEFORE MILLING.
- 2.) THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER POLYMERIZED HMA BINDER COURSE.
- 3.) ALL FINAL LOCATIONS FOR LONGITUDINAL PARTIAL DEPTH PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS @ Ndes	
<b>STANDARD OVERLAY</b>		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70; 1.50"	4.0% AT 70 GYR	PFP
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 0.75"	3.5% AT 50 GYR	QCP
<b>LONGITUDINAL PARTIAL DEPTH PATCHING</b>		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D" N70; 2"	4.0% AT 70 GYR	QC/QA
<b>DRIVEWAYS</b>		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D" N50; 2"	4.0% AT 50 GYR	QC/QA
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm); PE-6"	4.0% AT 50 GYR	QC/QA
<b>PATCHING</b>		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4.0% AT 70 GYR	QC/QA
HMA REPLACEMENT OVER PATCHES, 4" (HMA BINDER IL-19 mm)	4.0% AT 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

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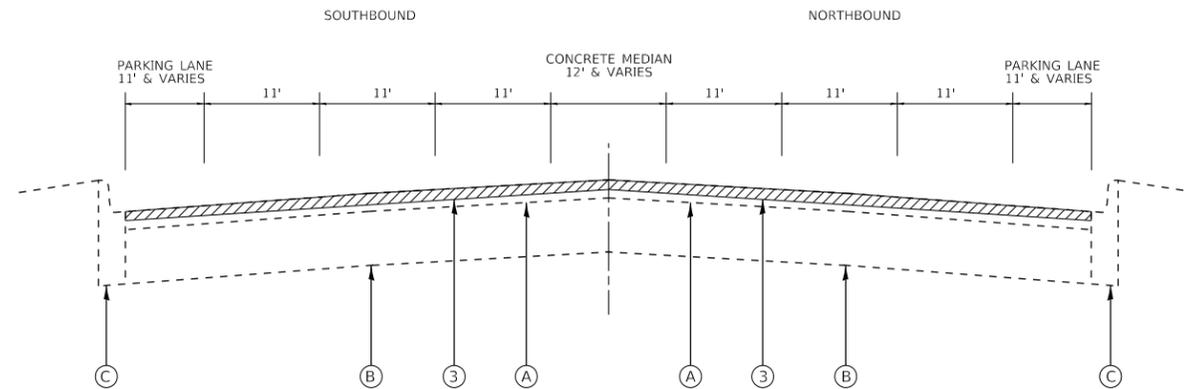
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
 MICHIGAN AVE. - BALBO DR. TO GARFIELD BLVD.**

SCALE: SHEET OF SHEETS STA. TO STA.

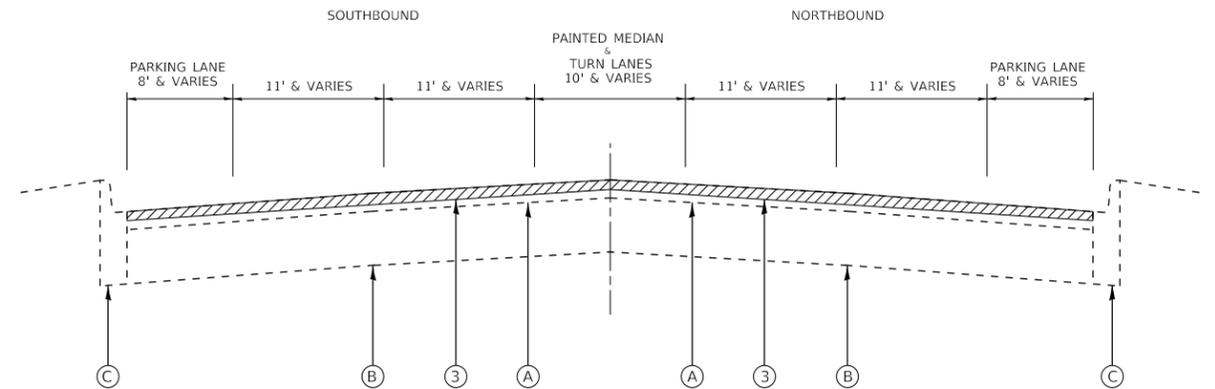
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	6
CONTRACT NO. 62J89				
ILLINOIS FED. AID PROJECT				

MICHIGAN AVE.  
⊕



STA. 178+07 TO STA. 201+44

MICHIGAN AVE.  
⊕



STA. 201+44 TO STA. 208+79  
STA. 211+56 TO STA. 211+83  
227+66 TO STA. 282+13

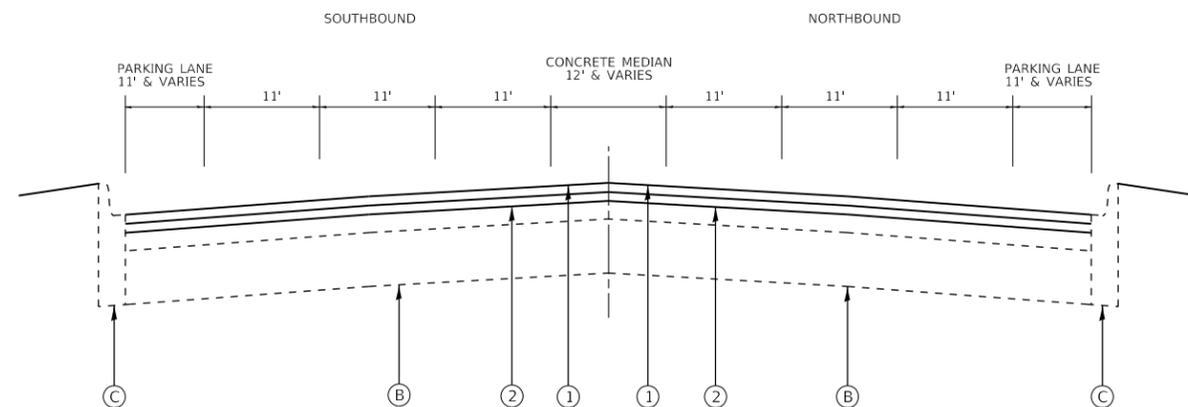
**LEGEND - EXISTING:**

- (A) HOT-MIX ASPHALT PAVEMENT, 3.5" & VARIES
- (B) PORTLAND-CEMENT CONCRETE PAVEMENT, 10.75" & VARIES
- (C) COMBINATION CONCRETE CURB & GUTTER, B-6.12
- (D) BARRIER CURB

**LEGEND - PROPOSED**

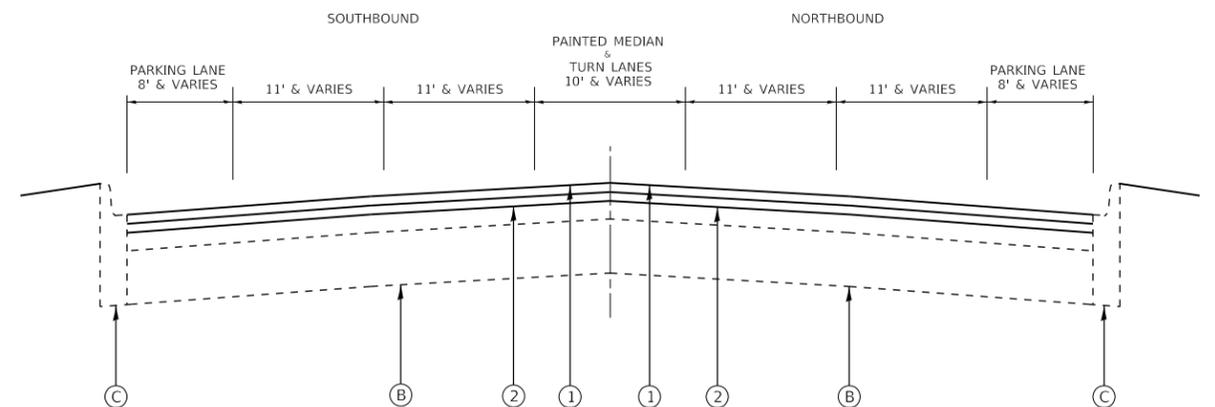
- (1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70; 1.50"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL 4.75 N50; 0.75"
- (3) HOT-MIX ASPHALT SURFACE REMOVAL, 2.25"

MICHIGAN AVE.  
⊕



STA. 178+07 TO STA. 201+44

MICHIGAN AVE.  
⊕



STA. 201+44 TO STA. 208+79  
STA. 211+56 TO STA. 211+83  
227+66 TO STA. 282+13

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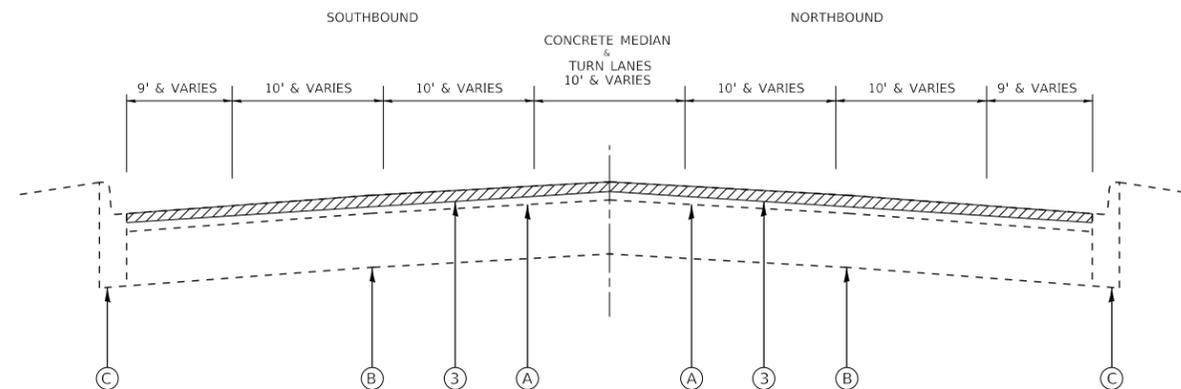
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
MICHIGAN AVE. - BALBO DR. TO GARFIELD BLVD.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62J89				
		ILLINOIS	FED. AID PROJECT	

MICHIGAN AVE.  

STA. 282+13 TO STA. 302+48

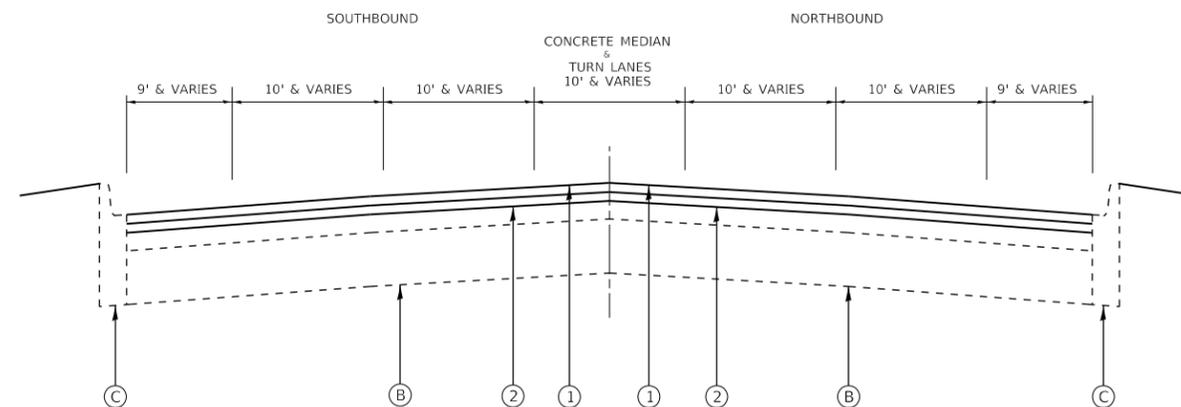
**LEGEND - EXISTING:**

- (A) HOT-MIX ASPHALT PAVEMENT, 3.5" & VARIES
- (B) PORTLAND-CEMENT CONCRETE PAVEMENT, 10.75" & VARIES
- (C) COMBINATION CONCRETE CURB & GUTTER, B-6.12
- (D) BARRIER CURB

**LEGEND - PROPOSED**

- ① HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70; 1.50"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL 4.75 N50; 0.75"
- ③ HOT-MIX ASPHALT SURFACE REMOVAL, 2.25"

MICHIGAN AVE.  

STA. 282+13 TO STA. 302+48

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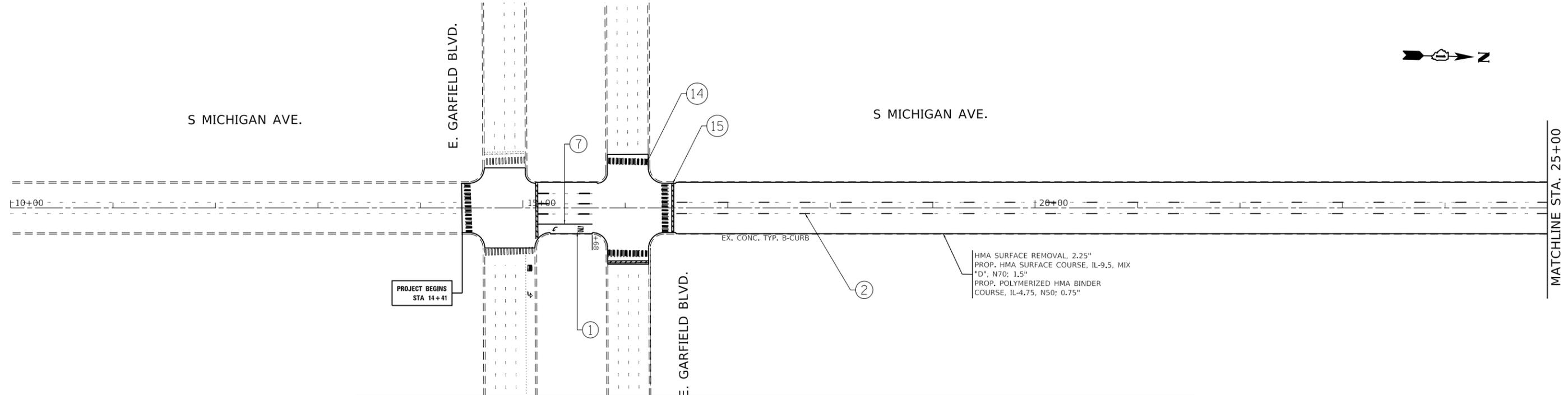
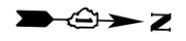
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
 MICHIGAN AVE. - BALBO DR. TO GARFIELD BLVD.**

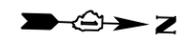
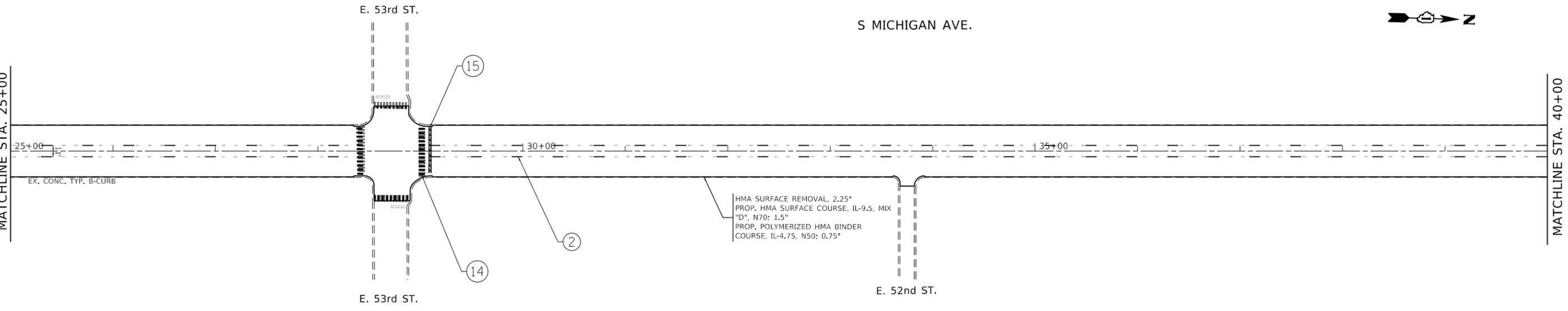
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62J89			ILLINOIS FED. AID PROJECT	



PAVEMENT MARKING LEGEND

① PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE	⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" DOTTED WHITE LINE (2' LINE WITH 6' SPACE)	⑪ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE ISLAND LINE (DIAGONAL AT 6' C-C)
② PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINES (6" LINE WITH 18" SPACE)	⑦ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE LINE	⑫ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE CHANNELIZING LINES (DIAGONAL AT 20' C-C)
③ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" SOLID DOUBLE YELLOW CENTERLINE, 8" C-C	⑧ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE TURN LANE LINE (2' LINE WITH 6" SPACE)	⑬ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW PAINTED MEDIAN LINE (DIAGONAL AT 20' C-C)
④ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW EDGE LINE	⑨ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE MARKING	⑭ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE LONGITUDINAL BARS (6' LENGTH, 2' SPACING)
⑤ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" WHITE PARKING LANE LINE	⑩ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE ISLAND	⑮ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP LINE



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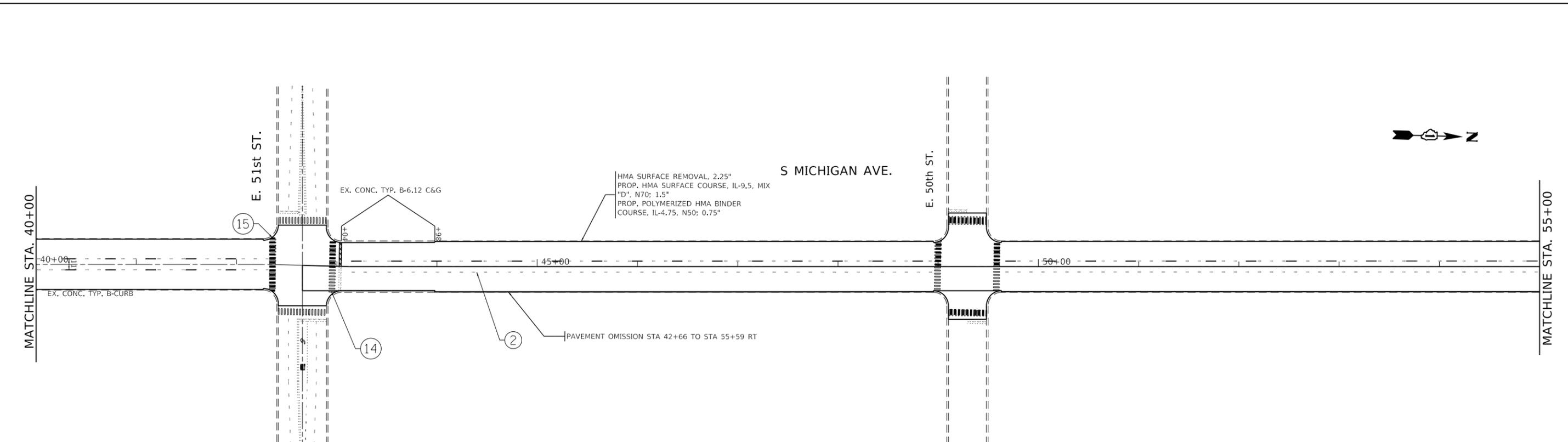
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN**  
**MICHIGAN AVE. (BALBO DR. - GARFIELD BLVD.)**

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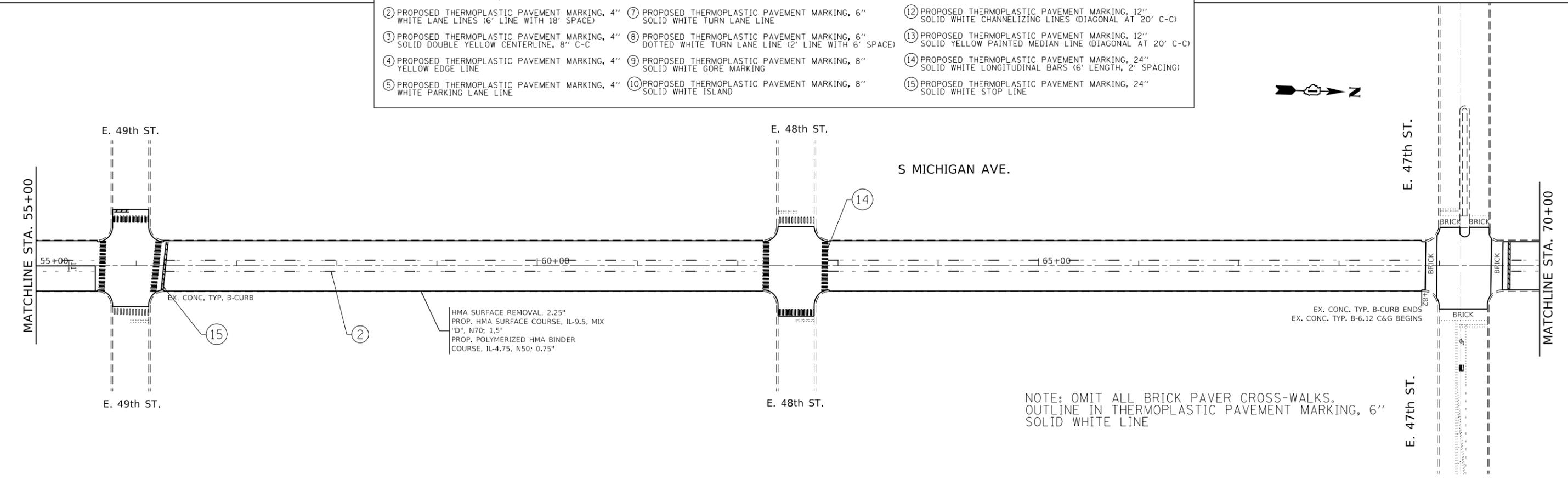
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CONTRACT NO. 62J89				
ILLINOIS FED. AID PROJECT				

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**PAVEMENT MARKING LEGEND**

① PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE	⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" DOTTED WHITE LINE (2' LINE WITH 6' SPACE)	⑪ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE ISLAND LINE (DIAGONAL AT 6' C-C)
② PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINES (6' LINE WITH 18' SPACE)	⑦ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE LINE	⑫ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE CHANNELIZING LINES (DIAGONAL AT 20' C-C)
③ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" SOLID DOUBLE YELLOW CENTERLINE, 8" C-C	⑧ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE TURN LANE LINE (2' LINE WITH 6' SPACE)	⑬ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW PAINTED MEDIAN LINE (DIAGONAL AT 20' C-C)
④ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW EDGE LINE	⑨ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE MARKING	⑭ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE LONGITUDINAL BARS (6' LENGTH, 2' SPACING)
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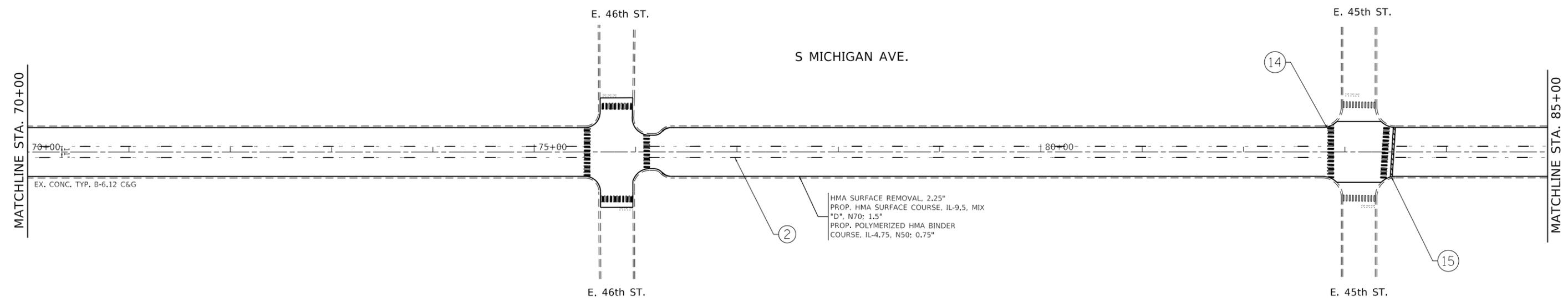
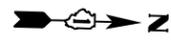
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN  
 MICHIGAN AVE. (BALBO DR. - GARFIELD BLVD.)**

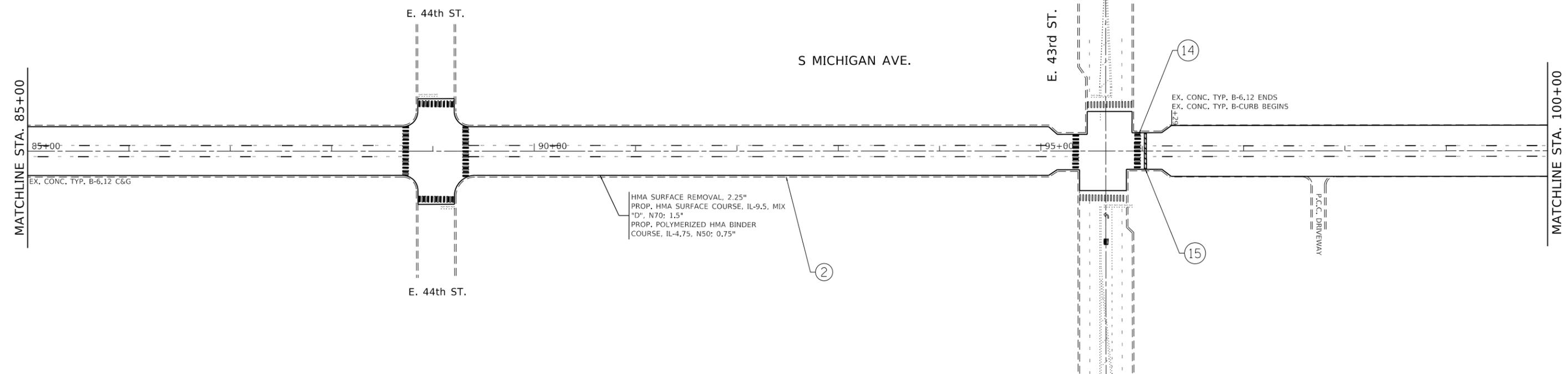
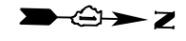
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CONTRACT NO. 62J89				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

① PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE	⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" DOTTED WHITE LINE (2' LINE WITH 6' SPACE)	⑪ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE ISLAND LINE (DIAGONAL AT 6' C-C)
② PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINES (6' LINE WITH 18' SPACE)	⑦ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE LINE	⑫ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE CHANNELIZING LINES (DIAGONAL AT 20' C-C)
③ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" SOLID DOUBLE YELLOW CENTERLINE, 8" C-C	⑧ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE TURN LANE LINE (2' LINE WITH 6' SPACE)	⑬ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW PAINTED MEDIAN LINE (DIAGONAL AT 20' C-C)
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⑤ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" WHITE PARKING LANE LINE	⑩ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE ISLAND	⑮ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP LINE



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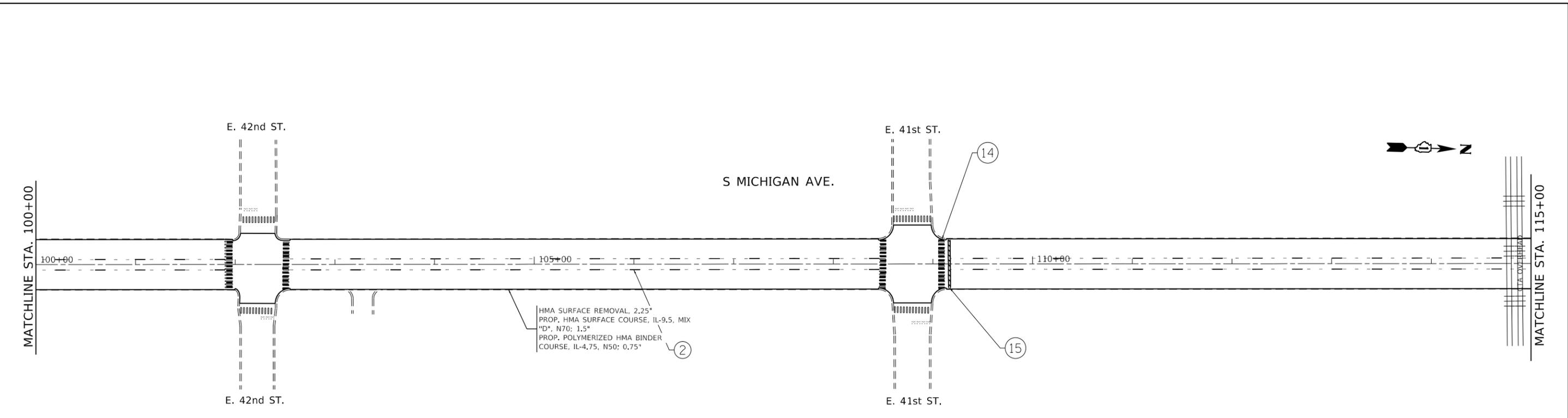
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN  
MICHIGAN AVE. (BALBO DR. - GARFIELD BLVD.)**

SCALE: 1"=50'      SHEET      OF      SHEETS      STA.      TO      STA.

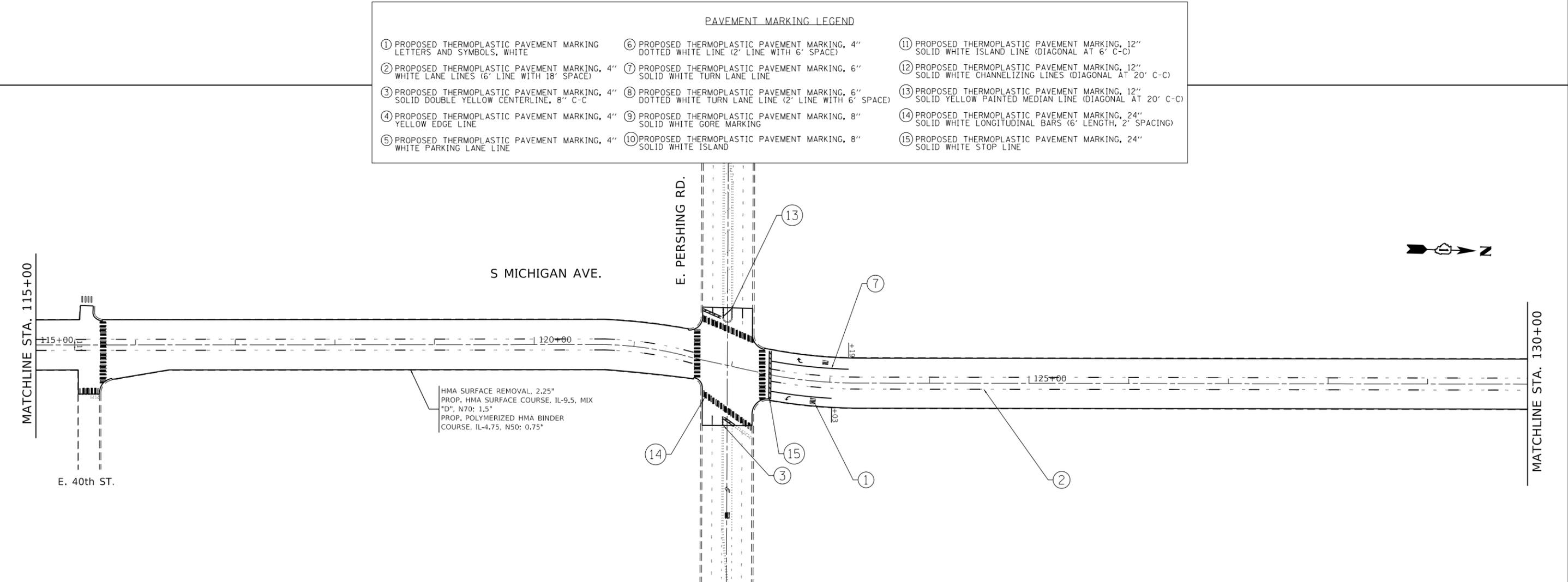
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CONTRACT NO. 62J89				
ILLINOIS FED. AID PROJECT				

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**PAVEMENT MARKING LEGEND**

① PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE	⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" DOTTED WHITE LINE (2' LINE WITH 6' SPACE)	⑪ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE ISLAND LINE (DIAGONAL AT 6' C-C)
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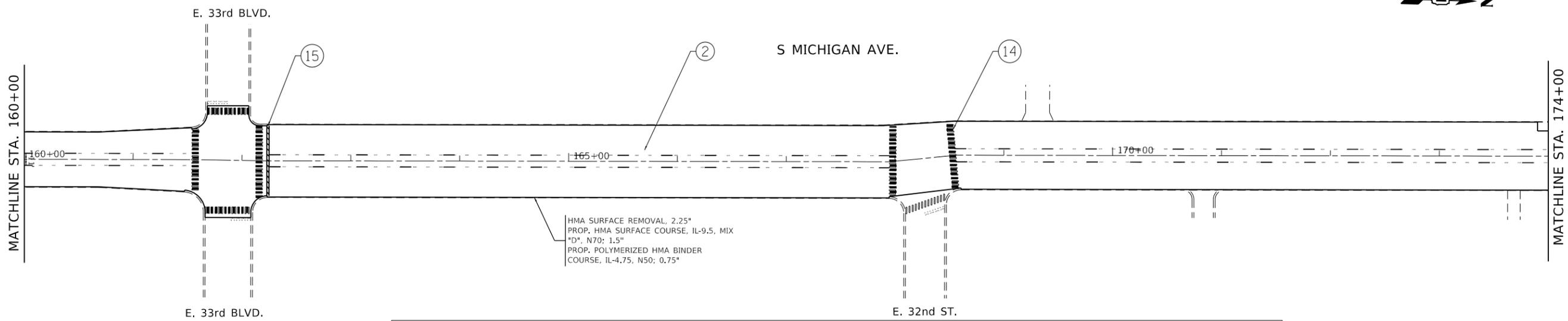
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 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN  
 MICHIGAN AVE. (BALBO DR. - GARFIELD BLVD.)**

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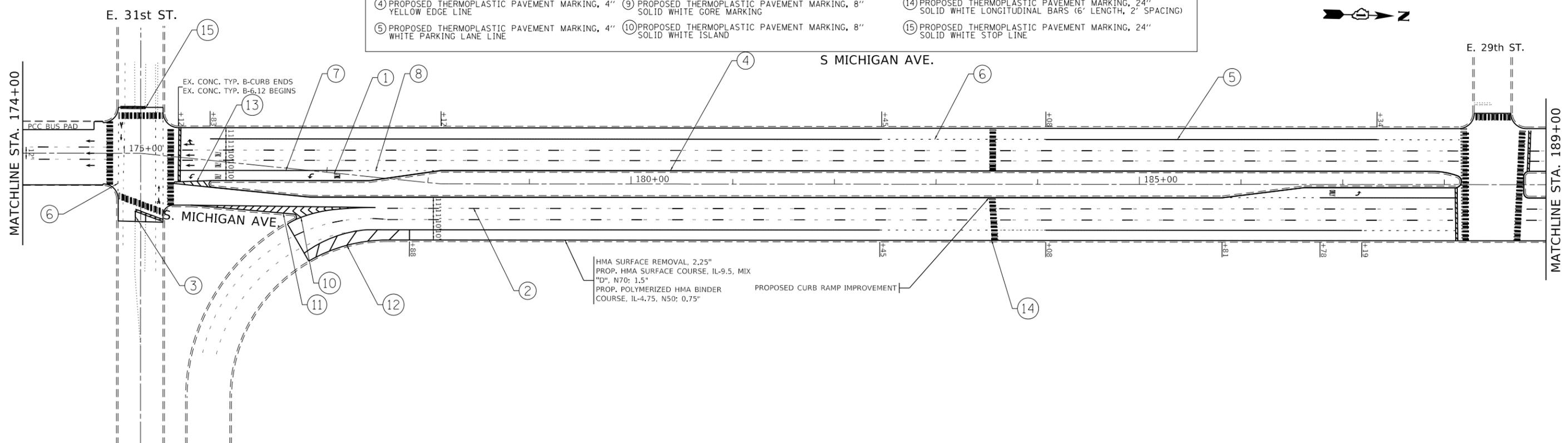
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2911	2019-097-R5&SW	COOK	35	11
CONTRACT NO. 62J89				
ILLINOIS FED. AID PROJECT				





HMA SURFACE REMOVAL, 2.25"  
 PROP. HMA SURFACE COURSE, IL-9.5, MIX  
 "D", N70: 1.5"  
 PROP. POLYMERIZED HMA BINDER  
 COURSE, IL-4.75, N50: 0.75"

PAVEMENT MARKING LEGEND		
① PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE	⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" DOTTED WHITE LINE (2' LINE WITH 6' SPACE)	⑪ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE ISLAND LINE (DIAGONAL AT 6' C-C)
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EX. CONC. TYP. B-CURB ENDS  
 EX. CONC. TYP. B-6.12 BEGINS

HMA SURFACE REMOVAL, 2.25"  
 PROP. HMA SURFACE COURSE, IL-9.5, MIX  
 "D", N70: 1.5"  
 PROP. POLYMERIZED HMA BINDER  
 COURSE, IL-4.75, N50: 0.75"

PROPOSED CURB RAMP IMPROVEMENT

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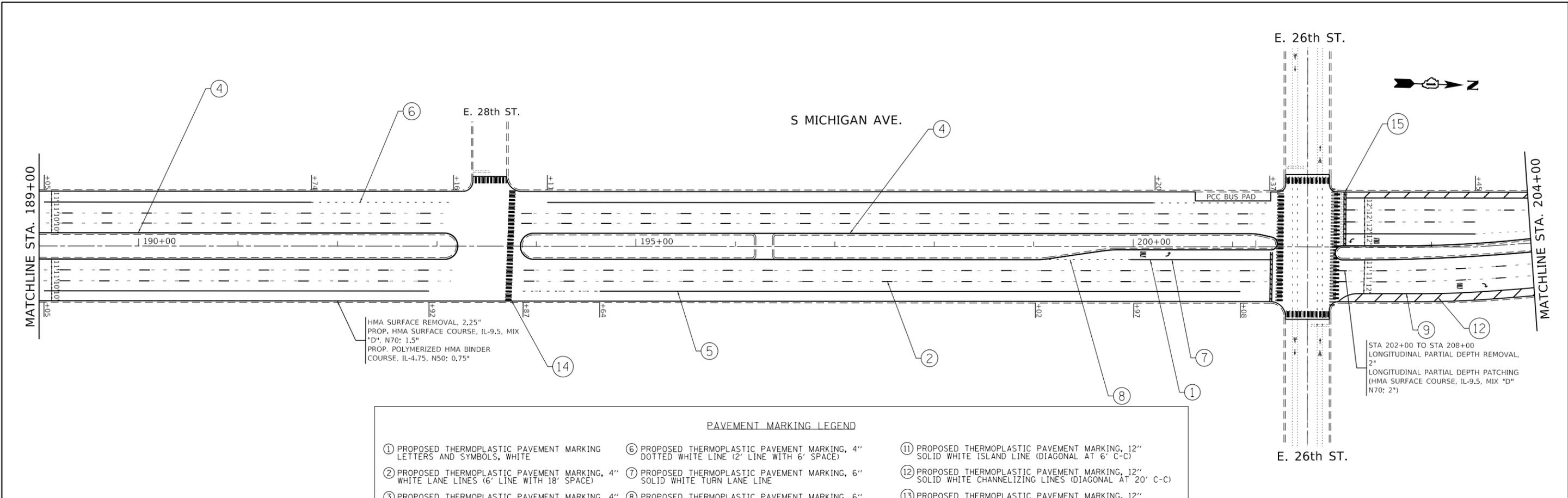
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN  
 MICHIGAN AVE. (BALBO DR. - GARFIELD BLVD.)**

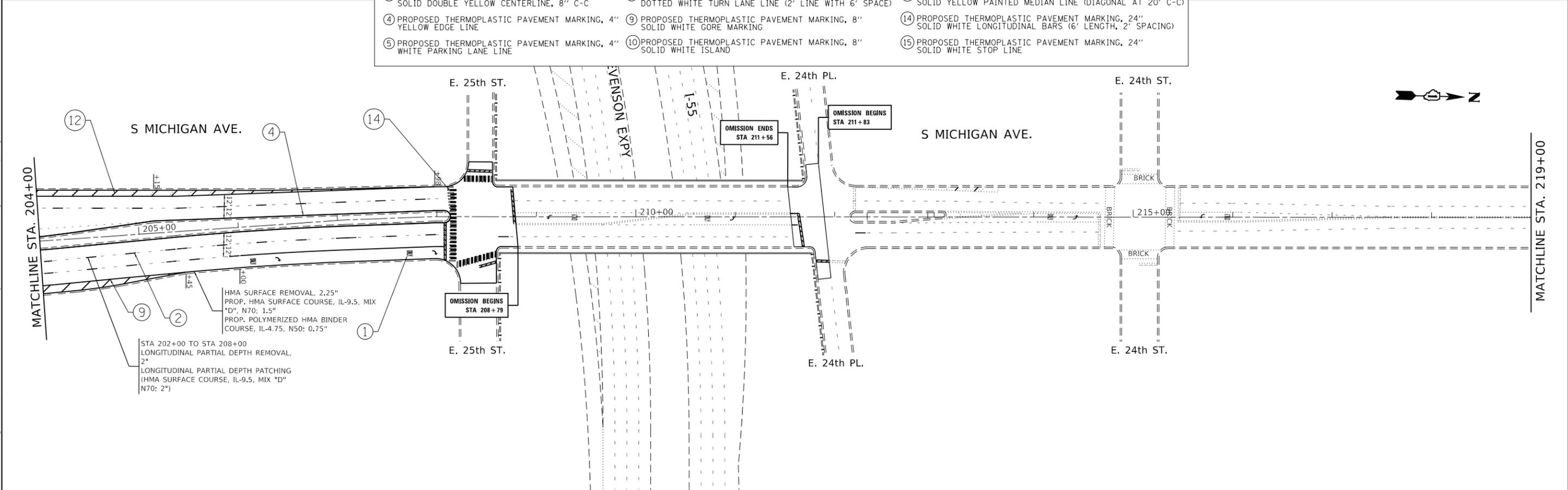
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CONTRACT NO. 62J89				
ILLINOIS FED. AID PROJECT				



**PAVEMENT MARKING LEGEND**

① PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE	⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" DOTTED WHITE LINE (2' LINE WITH 6' SPACE)	⑪ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE ISLAND LINE (DIAGONAL AT 6' C-C)
② PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINES (6' LINE WITH 18' SPACE)	⑦ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE LINE	⑫ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE CHANNELIZING LINES (DIAGONAL AT 20' C-C)
③ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" SOLID DOUBLE YELLOW CENTERLINE, 8" C-C	⑧ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE TURN LANE LINE (2' LINE WITH 6' SPACE)	⑬ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW PAINTED MEDIAN LINE (DIAGONAL AT 20' C-C)
④ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW EDGE LINE	⑨ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE MARKING	⑭ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE LONGITUDINAL BARS (6' LENGTH, 2" SPACING)
⑤ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" WHITE PARKING LANE LINE	⑩ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE ISLAND	⑮ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP LINE



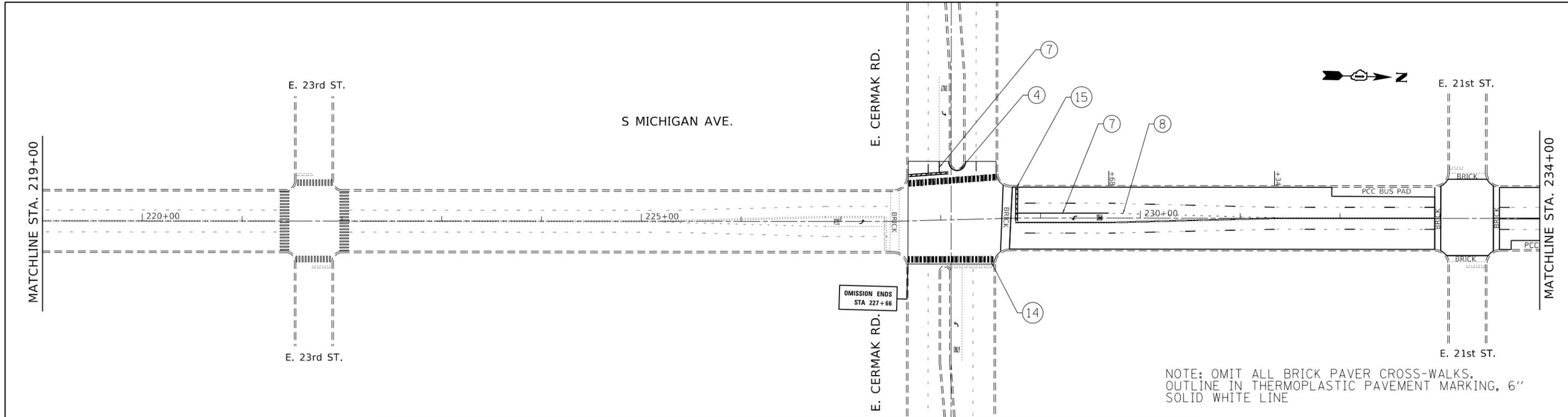
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

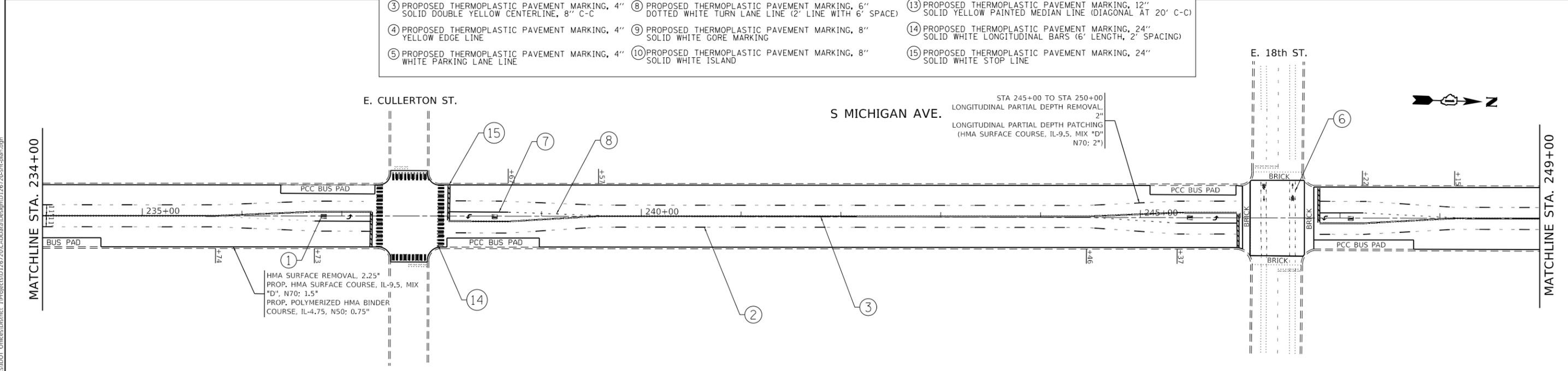
<b>ROADWAY AND PAVEMENT MARKING PLAN</b>	
<b>MICHIGAN AVE. (BALBO DR. - GARFIELD BLVD.)</b>	
SCALE: 1"=50'	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	14
CONTRACT NO. 62J89				
ILLINOIS FED. AID PROJECT				



**PAVEMENT MARKING LEGEND**

① PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE	⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" DOTTED WHITE LINE (2' LINE WITH 6' SPACE)	⑪ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE ISLAND LINE (DIAGONAL AT 6' C-C)
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NOTE: OMIT ALL BRICK PAVER CROSS-WALKS. OUTLINE IN THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE LINE

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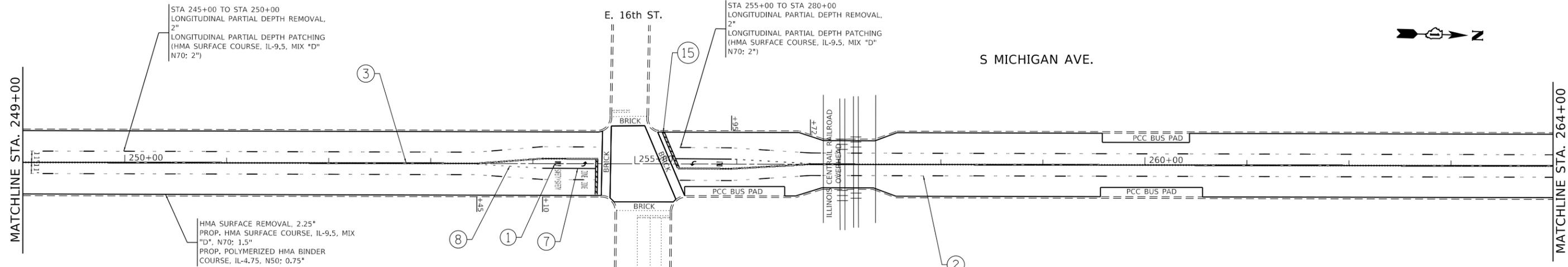
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	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN  
MICHIGAN AVE. (BALBO DR. - GARFIELD BLVD.)**

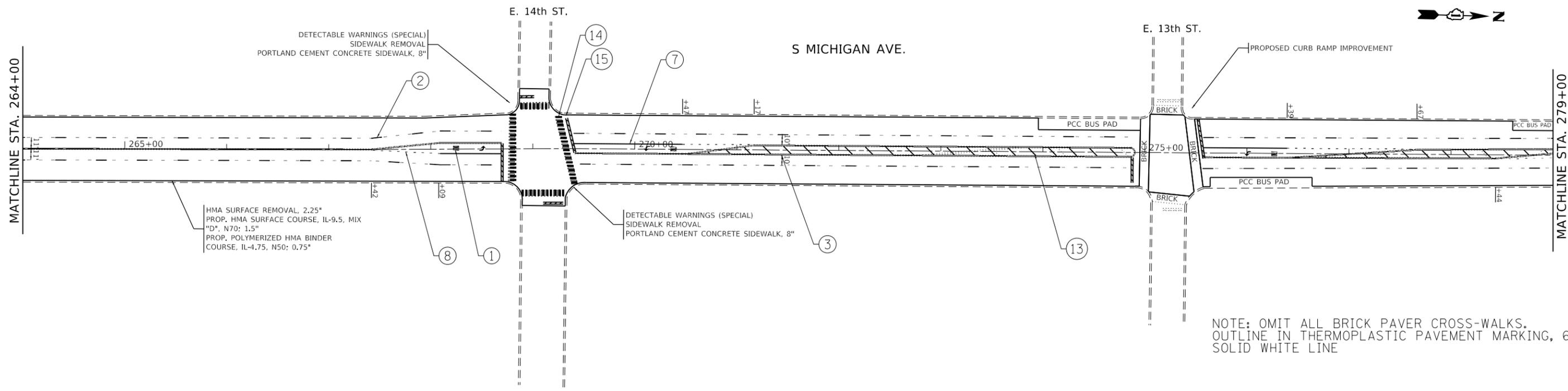
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62J89				
ILLINOIS FED. AID PROJECT				



NOTE: OMIT ALL BRICK PAVER CROSS-WALKS.  
 OUTLINE IN THERMOPLASTIC PAVEMENT MARKING, 6"  
 SOLID WHITE LINE

PAVEMENT MARKING LEGEND		
① PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE	⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" DOTTED WHITE LINE (2' LINE WITH 6' SPACE)	⑪ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE ISLAND LINE (DIAGONAL AT 6' C-C)
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NOTE: OMIT ALL BRICK PAVER CROSS-WALKS.  
 OUTLINE IN THERMOPLASTIC PAVEMENT MARKING, 6"  
 SOLID WHITE LINE

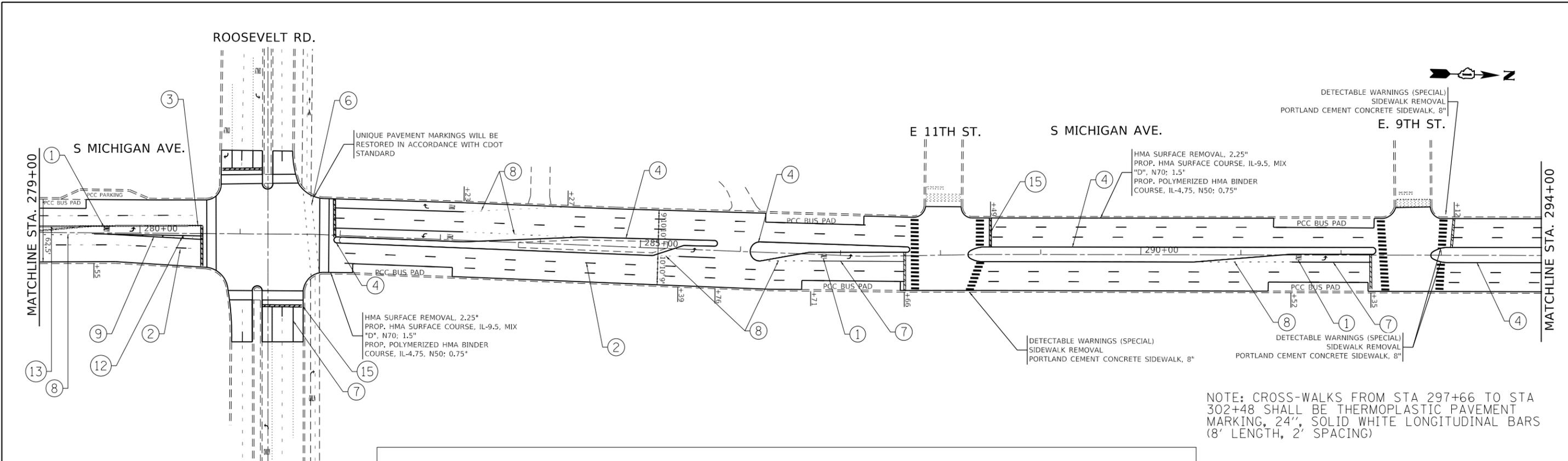
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	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

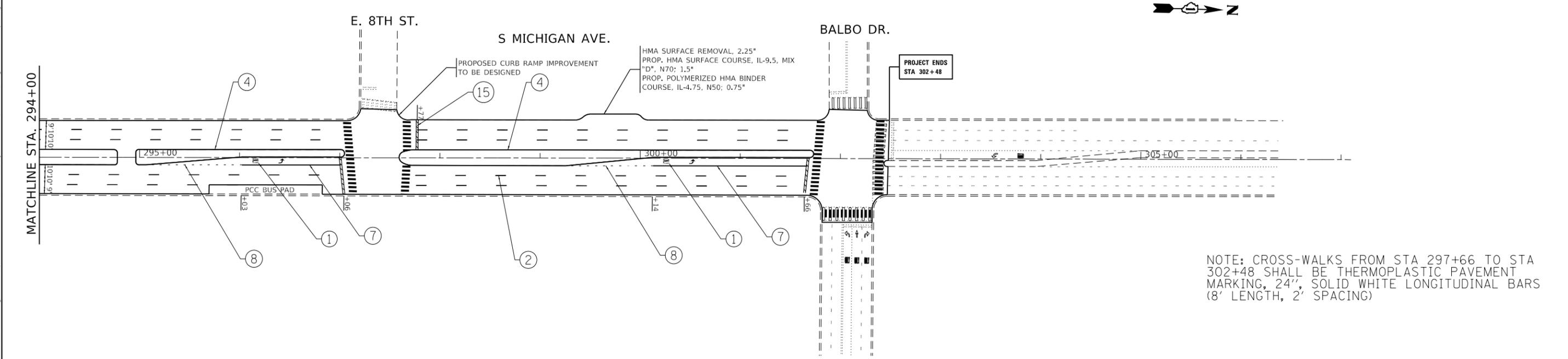
<b>ROADWAY AND PAVEMENT MARKING PLAN</b>			
<b>MICHIGAN AVE. (BALBO DR. - GARFIELD BLVD.)</b>			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	16
CONTRACT NO. 62J89				
ILLINOIS FED. AID PROJECT				



NOTE: CROSS-WALKS FROM STA 297+66 TO STA 302+48 SHALL BE THERMOPLASTIC PAVEMENT MARKING, 24", SOLID WHITE LONGITUDINAL BARS (8' LENGTH, 2' SPACING)

PAVEMENT MARKING LEGEND		
① PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" LETTERS AND SYMBOLS, WHITE	⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" DOTTED WHITE LINE (2' LINE WITH 6' SPACE)	⑪ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE ISLAND LINE (DIAGONAL AT 6" C-C)
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NOTE: CROSS-WALKS FROM STA 297+66 TO STA 302+48 SHALL BE THERMOPLASTIC PAVEMENT MARKING, 24", SOLID WHITE LONGITUDINAL BARS (8' LENGTH, 2' SPACING)

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	DRAWN -	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

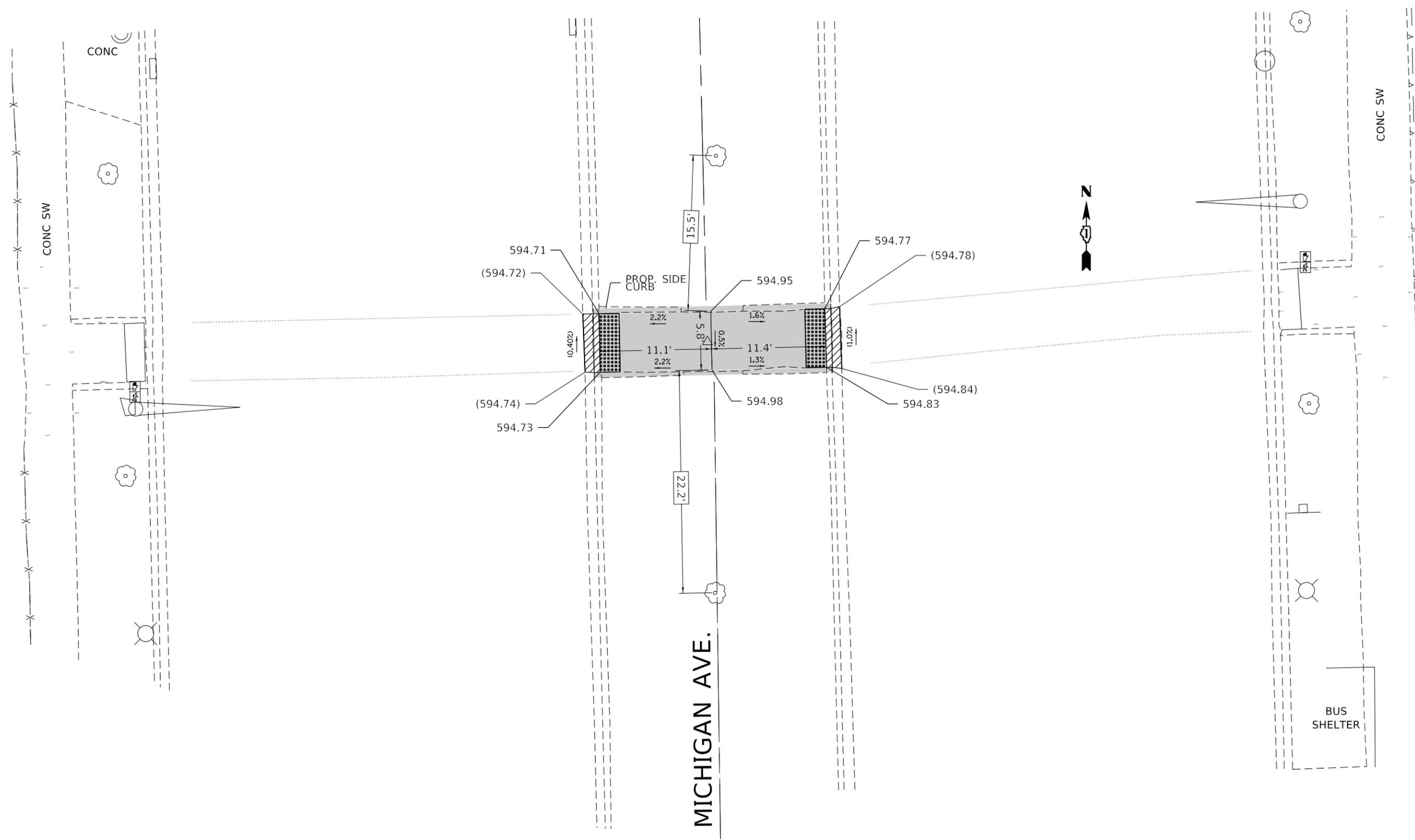
**ROADWAY AND PAVEMENT MARKING PLAN  
MICHIGAN AVE. (BALBO DR. - GARFIELD BLVD.)**

SCALE: 1"=50'      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	17
CONTRACT NO. 62J89				
ILLINOIS   FED. AID PROJECT				



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**LEGEND**

- xx.xx' EXISTING LENGTH
- ==== PROPOSED SIDE CURB
- ( ) EXISTING ELEVATION/SLOPE
- PROPOSED DEPRESSED CURB & GUTTER  
(PAID FOR AS "COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT")
- PROPOSED SIDEWALK  
(PAID FOR AS "PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH")
- DETECTABLE WARNINGS

REFERENCE BENCHMARK ELEV 596.49  
 BENCHMARK DESCRIPTION: X-CUT ON NE BOLT OF FIRE HYDRANT  
 BENCHMARK LOCATION: EAST SIDE OF MICHIGAN AVE. APPROX. 90 FT S/O CROSSING

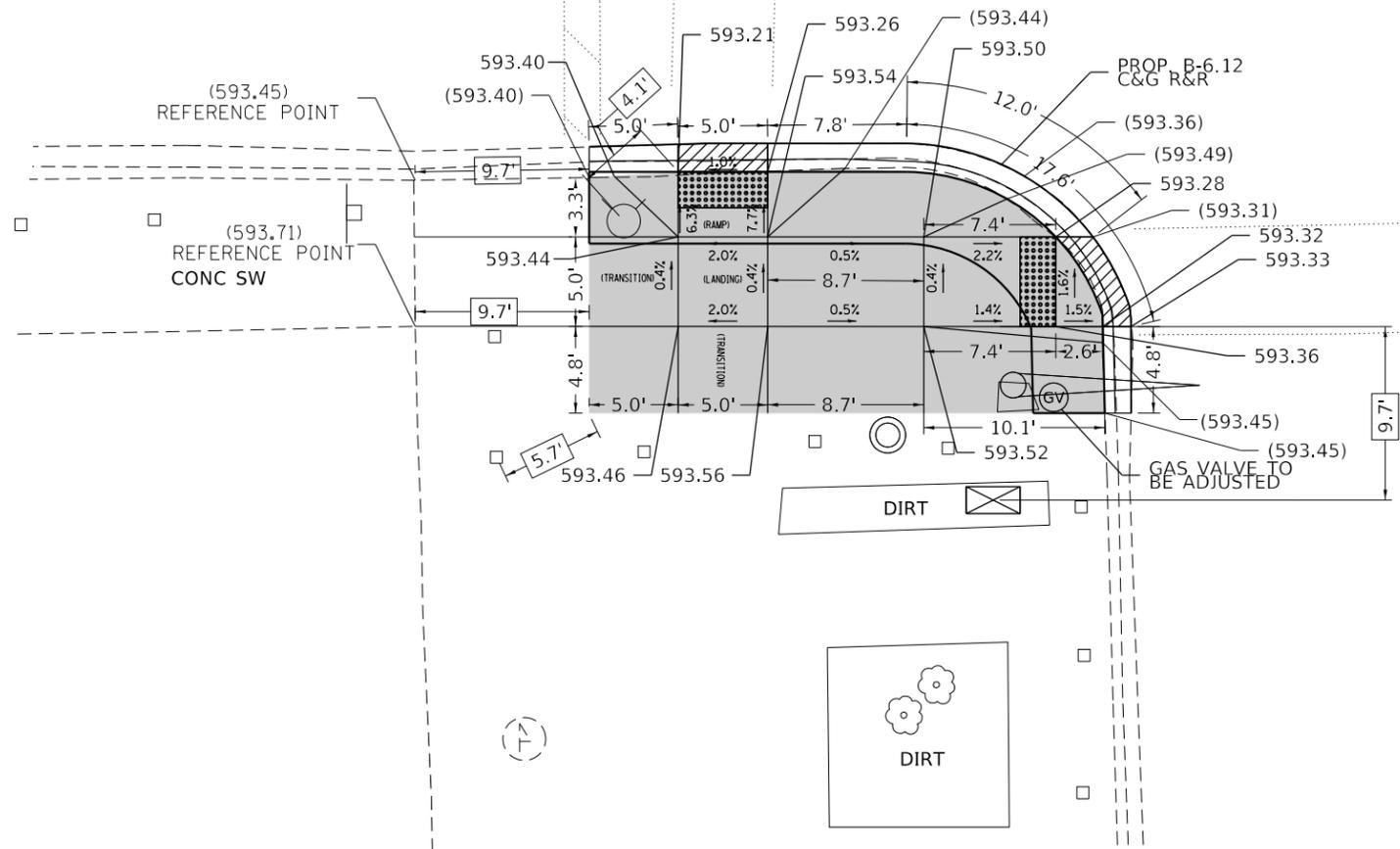
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	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

SIDEWALK DETAIL PLAN			
PED. CROSSING APPROX. 500' S/O 29TH STREET AND MICHIGAN AVE.			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	19
CONTRACT NO. 62J89				
ILLINOIS FED. AID PROJECT				

# 35th STREET



MICHIGAN AVE.

## LEGEND

xx.xx'

EXISTING LENGTH

=====

PROPOSED SIDE CURB

( )

EXISTING ELEVATION/SLOPE



PROPOSED DEPRESSED CURB & GUTTER  
(PAID FOR AS "COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT")



PROPOSED SIDEWALK  
(PAID FOR AS "PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH")



DETECTABLE WARNINGS

REFERENCE BENCHMARK ELEV 595.08

BENCHMARK DESCRIPTION: X-CUT ON NE BOLT OF FIRE HYDRANT

BENCHMARK LOCATION: IN THE NE CORNER OF 35TH STREET AND MICHIGAN AVE.

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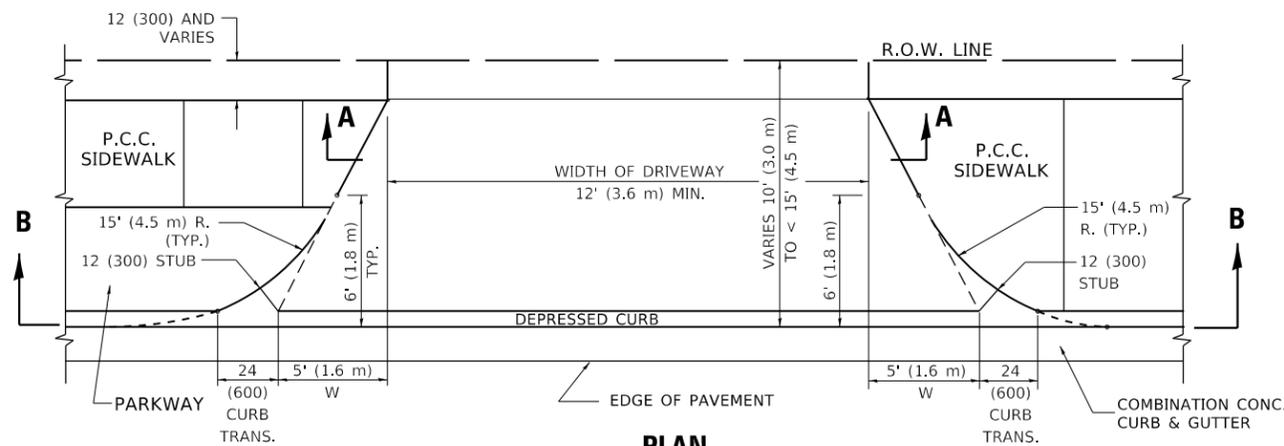
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN  
INTERSECTION OF 35TH STREET AND MICHIGAN AVE.

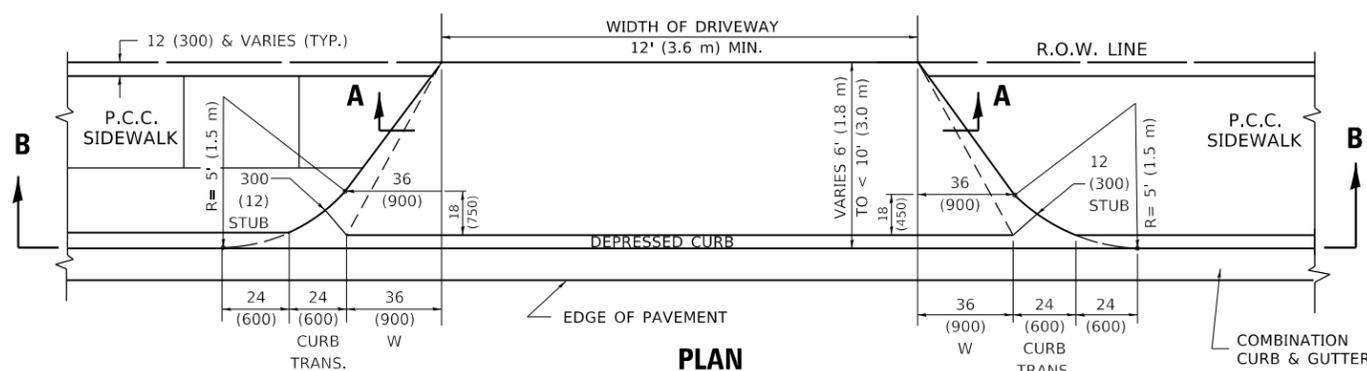
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CONTRACT NO. 62J89				
ILLINOIS FED. AID PROJECT				

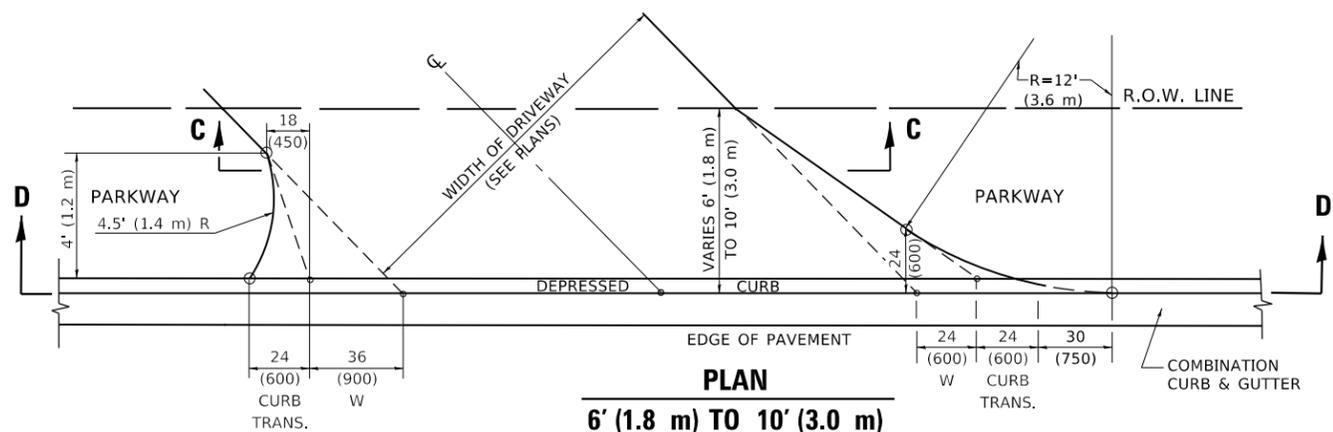




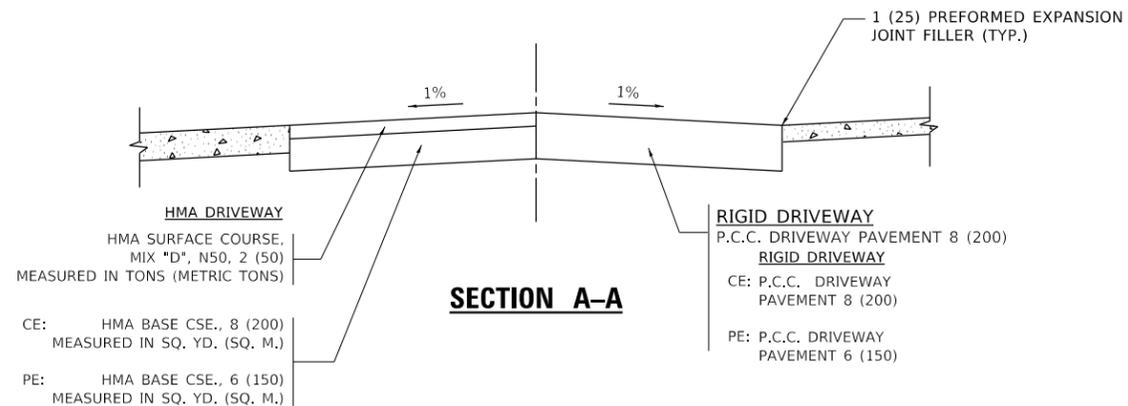
**PLAN**  
10' (3.0 m) TO < 15' (4.5 m)



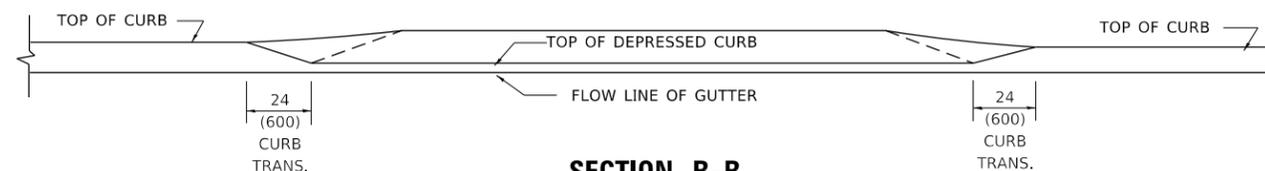
**PLAN**  
6' (1.8 m) TO < 10' (3.0 m)



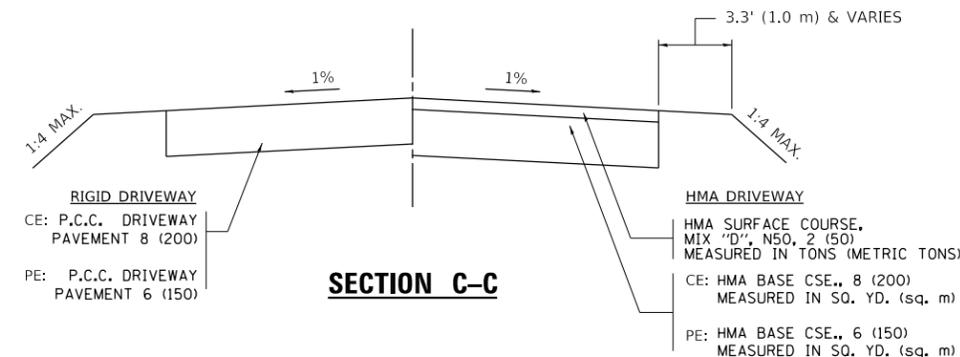
**PLAN**  
6' (1.8 m) TO 10' (3.0 m)



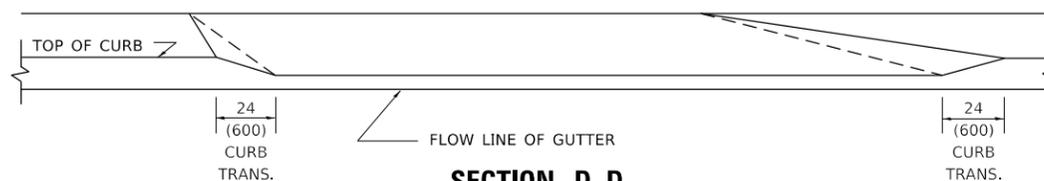
**SECTION A-A**



**SECTION B-B**



**SECTION C-C**



**SECTION D-D**

**GENERAL NOTES**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY DETAILS**  
**DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5m)**

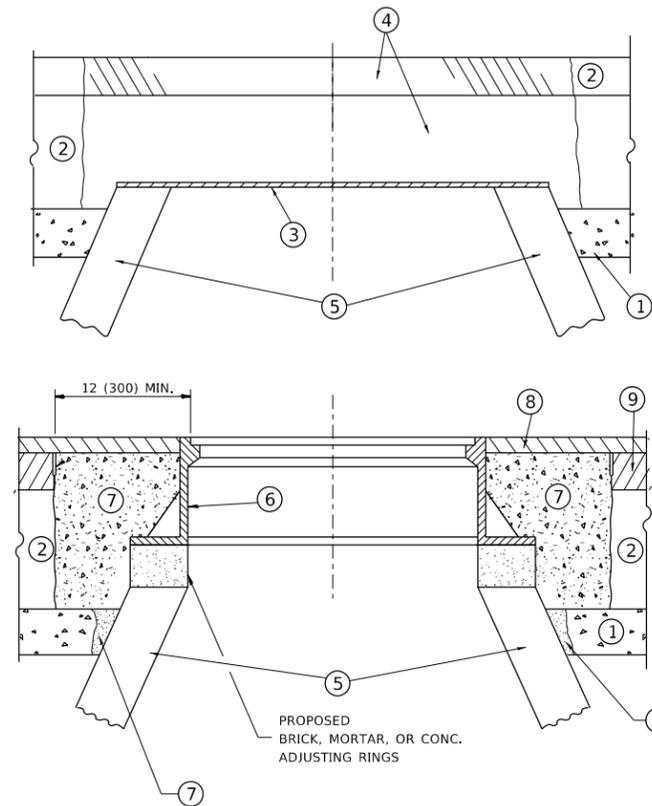
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	22
<b>BD400-02 (BD-02)</b>			CONTRACT NO. 62J89	

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PLOT DATE = 3/31/2020	CHECKED -	REVISED - R. BORO 01-01-07
	DATE - 11-06-95	REVISED - R. BORO 09-06-11



**NOTES**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 \* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT \*THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 \*CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR \*FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).\*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING

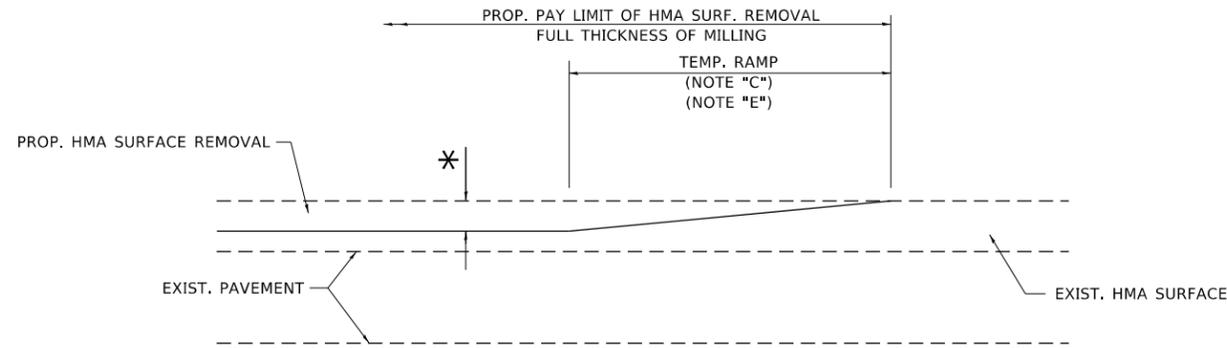
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	23
BD600-03 (BD-8)		CONTRACT NO. 62J89		
ILLINOIS FED. AID PROJECT				

USER NAME = skinneral	DESIGNED - R. SHAH	REVISED - R. WEDEMAN 05-14-04
	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
PLOT DATE = 3/31/2020	DATE - 10-25-94	REVISED - R. BORO 12-06-11



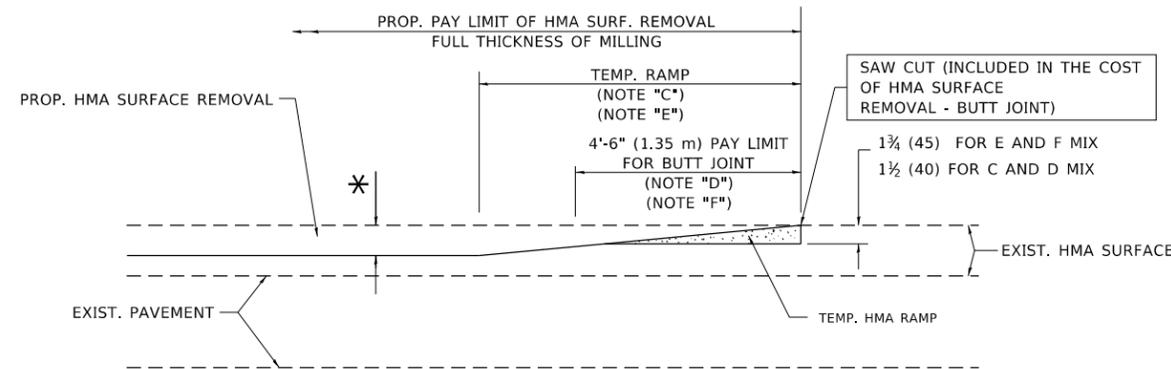




**MILLED TEMPORARY RAMP**

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

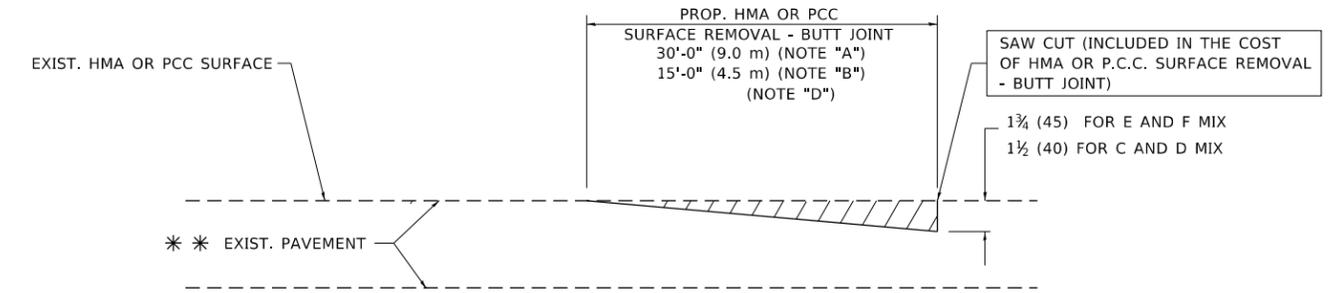


**HMA CONSTRUCTED TEMPORARY RAMP**

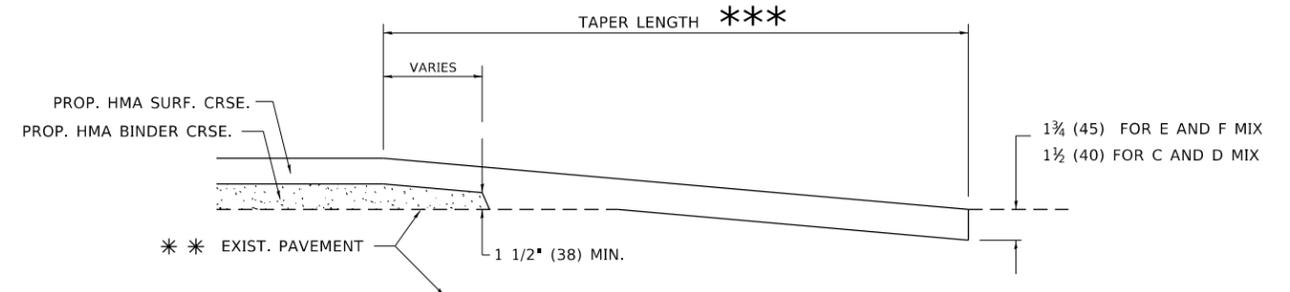
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

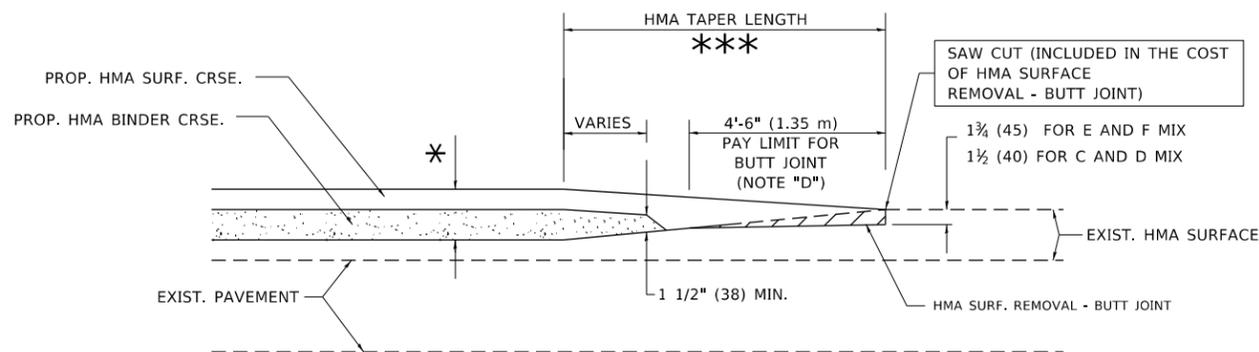
**NOTES**

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

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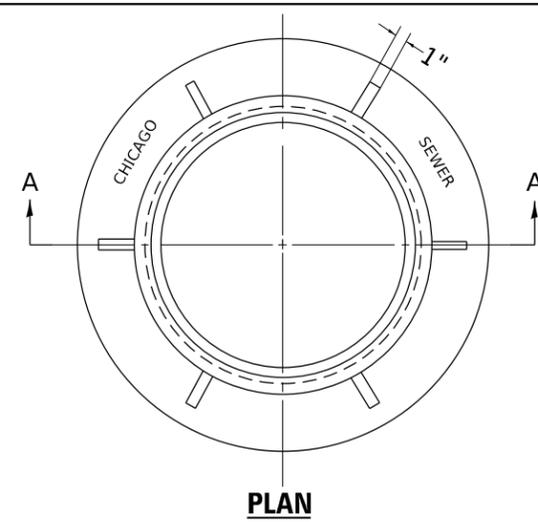
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	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 3/31/2020	DATE - 06-13-90	REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

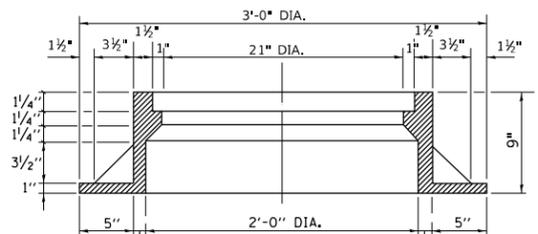
**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	26
<b>BD400-05 BD32</b>		CONTRACT NO. 62J89		
ILLINOIS FED. AID PROJECT				



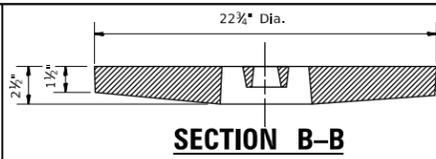
**PLAN**



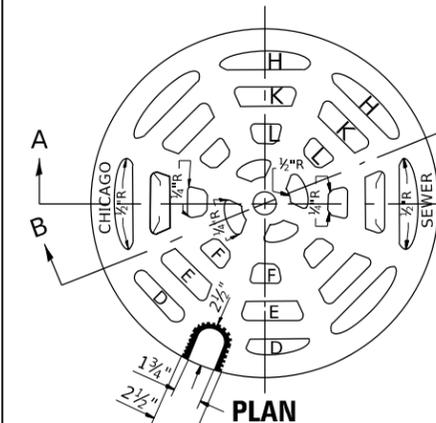
**SECTION A-A**

NOTE:  
METAL PLATES MUST BE FURNISHED FOR PERFORATED LIDS ON MANHOLES

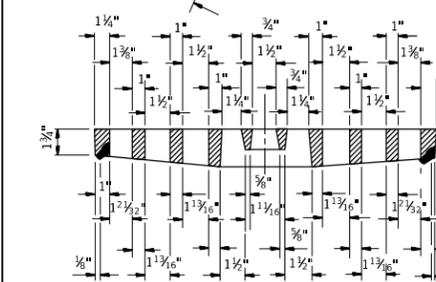
**CHICAGO STANDARD MANHOLE FRAME**  
SCALE: 1 1/2"=1'-0"  
MATERIAL: CAST IRON



**SECTION B-B**

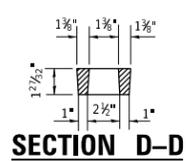


**PLAN**

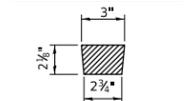


**SECTION A-A**

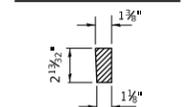
**PERFORATED LID FOR CATCH BASINS & MANHOLES**  
SCALE: 2"=1'-0"  
MATERIAL: CAST IRON



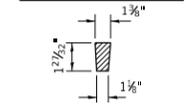
**SECTION D-D**



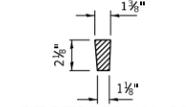
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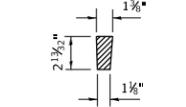
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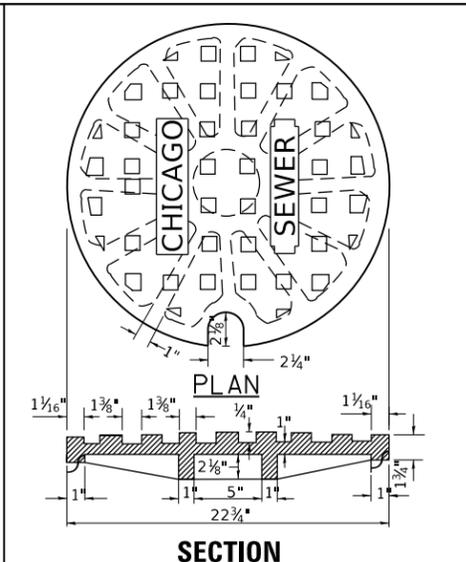
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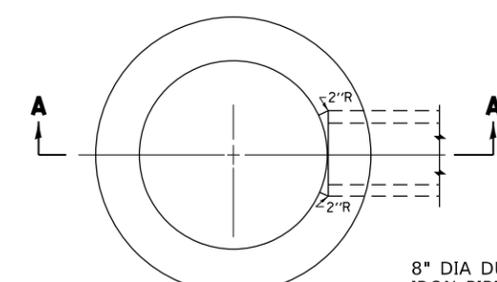
**SECTION K-K**



**SECTION L-L**

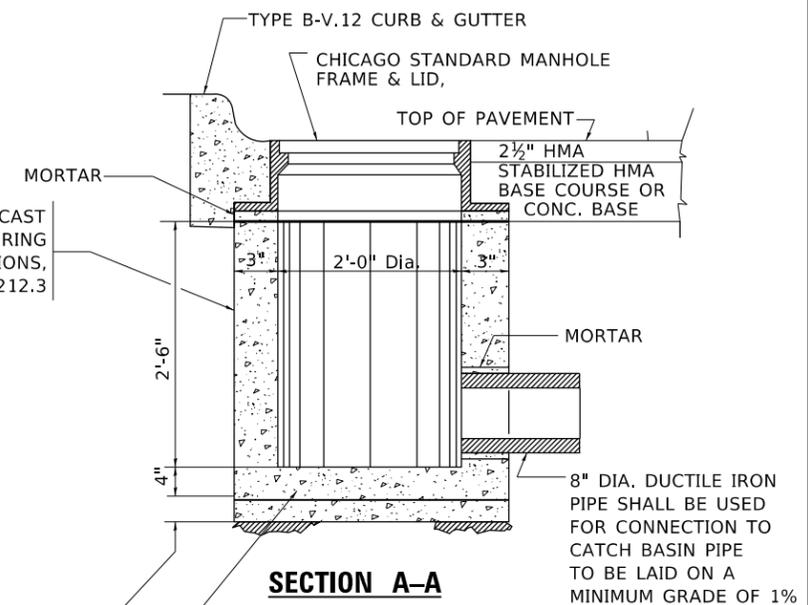


**SOLID LID FOR MANHOLES**  
SCALE: NONE  
MATERIAL: CAST IRON



**PLAN**  
(FRAME & LID NOT SHOWN)

8" DIA DUCTILE IRON PIPE, PART OF ITEM 7G



**SECTION A-A**

24" DIA. PRECAST CONC. RING SEE SPECIFICATIONS, PART 2, ART 212.3

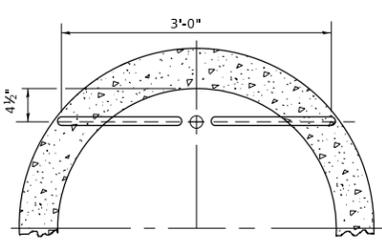
2 1/2" HMA STABILIZED HMA BASE COURSE OR CONC. BASE

8" DIA. DUCTILE IRON PIPE SHALL BE USED FOR CONNECTION TO CATCH BASIN PIPE TO BE LAID ON A MINIMUM GRADE OF 1%

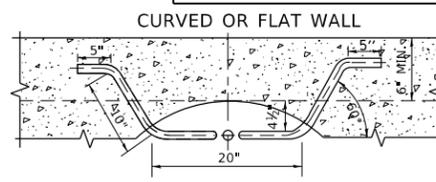
**STANDARD INLETS**  
SCALE 1"= 1'-0"  
ITEM 12

THIS INLET DETAIL IS SOMETIMES REFERRED TO AS "CHICAGO STANDARD INLET, TYPE A"

NOTE:  
INLETS SHALL NOT BE CONSTRUCTED UNLESS IT IS IMPOSSIBLE TO CONSTRUCT A CATCH BASIN. THE CONTRACTOR SHALL HAVE THE DEPARTMENT OF SEWERS APPROVAL BEFORE CONSTRUCTING INLETS.

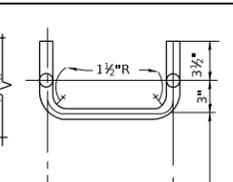


**PLAN**



**ELEVATION TYPE Y**

SCALE: 1"=1'-0"

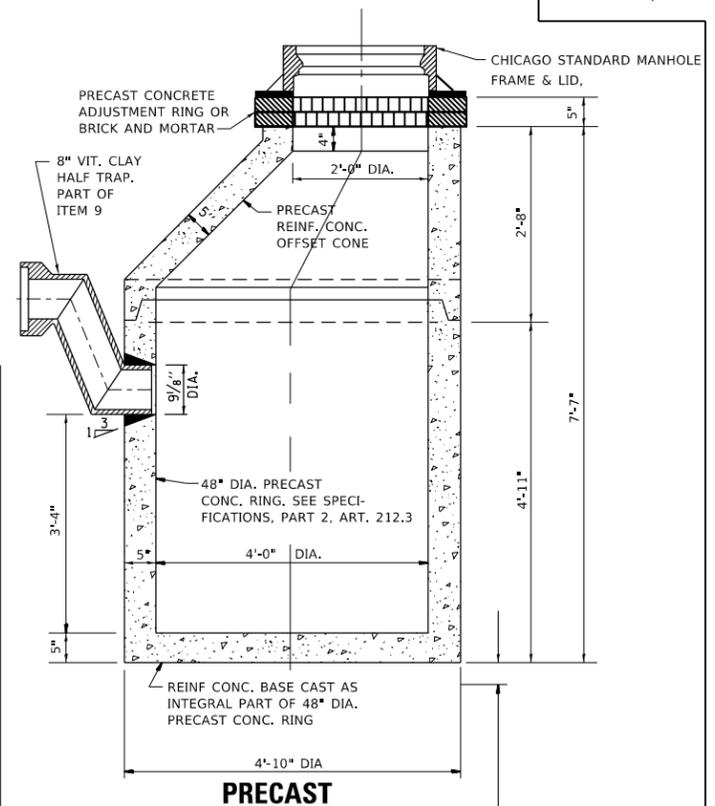


**ELEVATION TYPE Z**

SCALE: 1 1/2"=1'-0"

**STANDARD LADDER RUNGS**

ALL LADDER RUNGS SHALL BE ALUMINUM OR GALVANIZED WROUGHT IRON AS SPECIFIED IN THE SPECIFICATIONS, PART 2, ARTICLE 214.2. RUNGS SHALL BE 1" DIAMETER OR OF A SHAPE HAVING AN EQUIVALENT CROSS-SECTIONAL AREA.



**PRECAST**

**STANDARD CATCH BASINS**  
SCALE: 3/4"= 1'-0"  
ITEM 9

NOTE:  
6" MINIMUM GRANULAR EMBEDMENT UNDER ALL CATCH BASINS

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO  
CATCH BASIN, INLET AND MANHOLE DETAILS

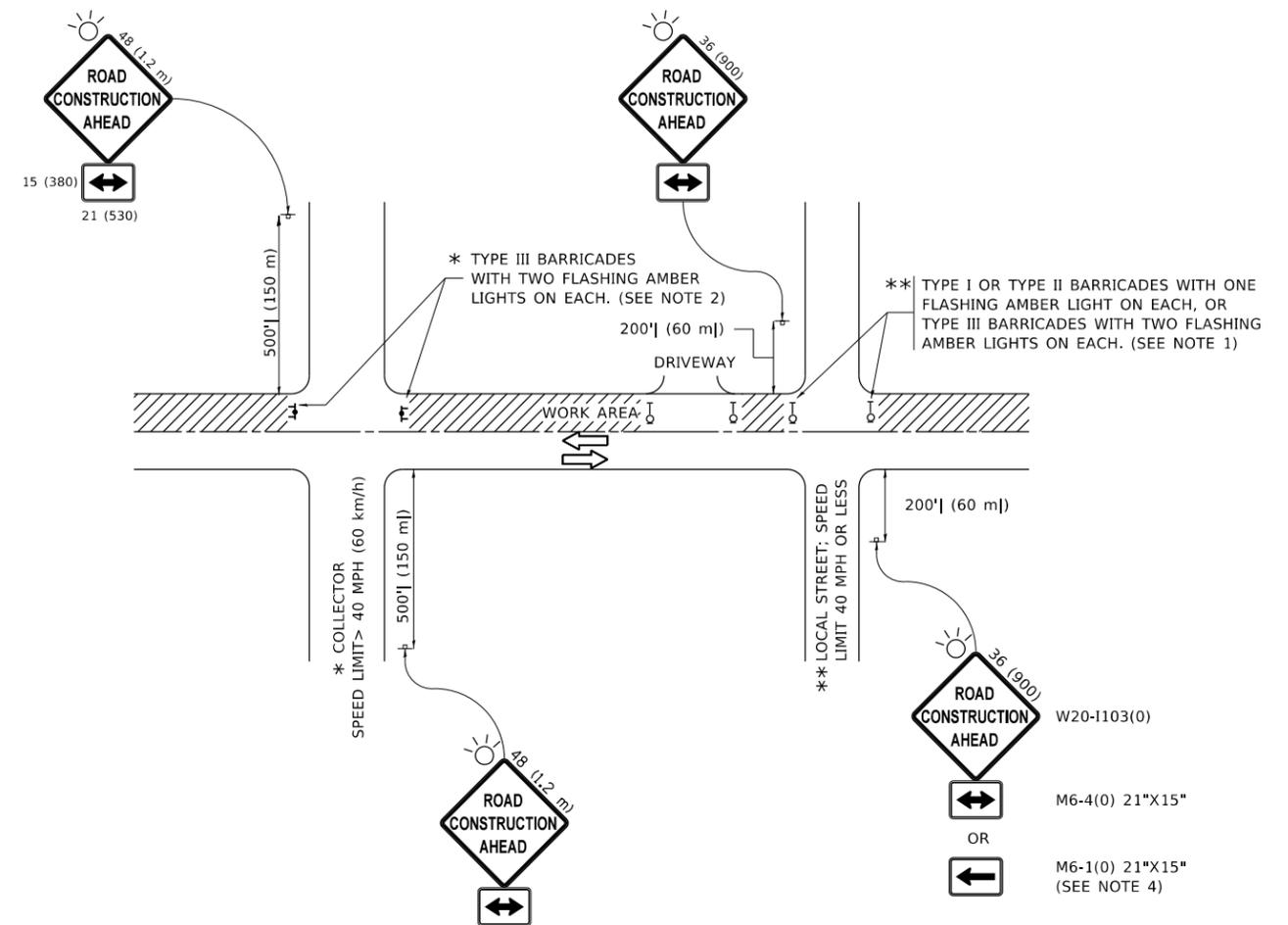
CITY OF CHICAGO  
DEPARTMENT OF SEWERS  
ENGINEERING DIVISION

USER NAME = skinneral	DESIGNED - M. GOMEZ	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 3/31/2020	CHECKED -	REVISED -
	DATE - 01-25-01	REVISED -

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	F.A.U. RTE. 2911	SECTION 2019-097-R5&SW	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 27
				BD600-13 (BD47)		CONTRACT NO. 62J89	
				ILLINOIS FED. AID PROJECT			

MODEL: Default  
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**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = skinnerl	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 3/31/2020	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

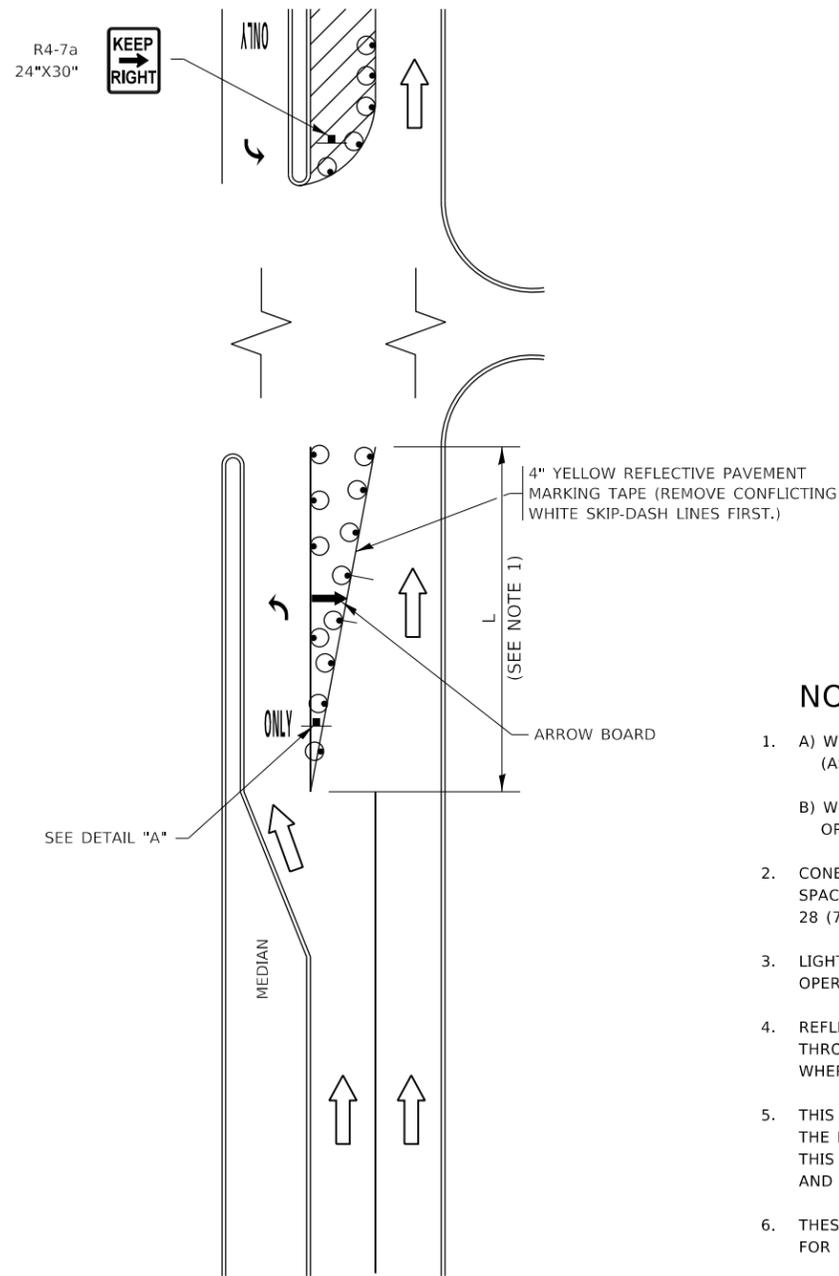
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

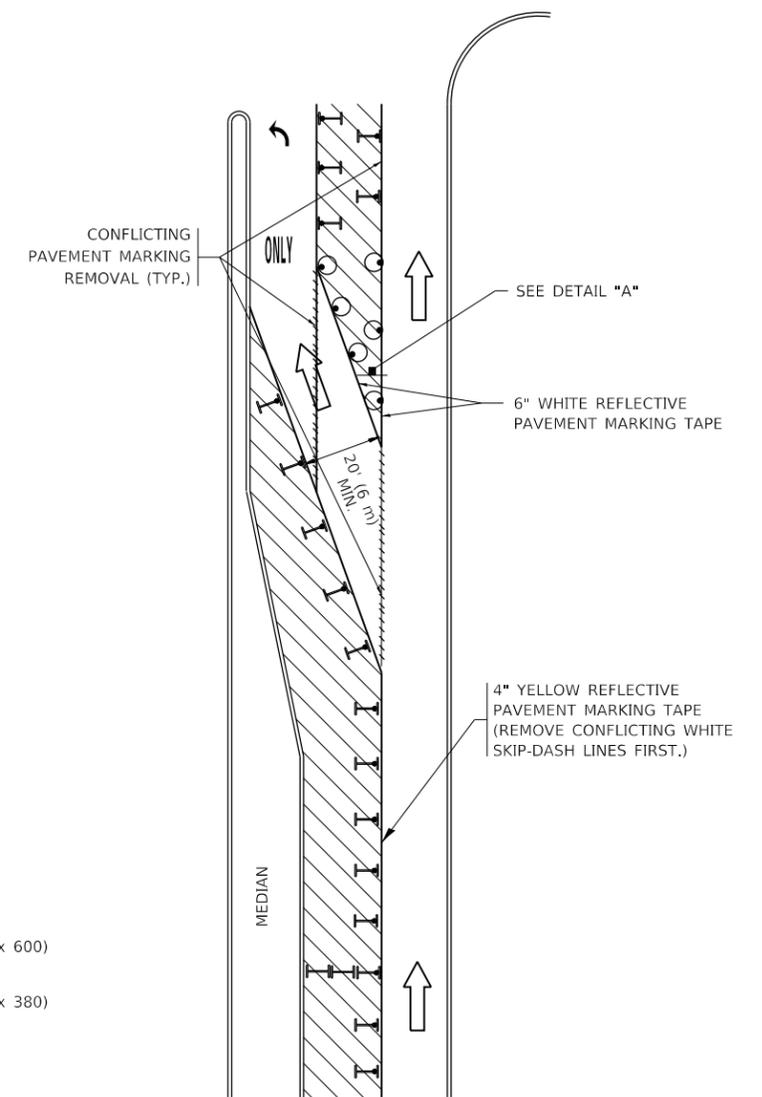
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	29
<b>TC-10</b>			CONTRACT NO. 62J89	
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



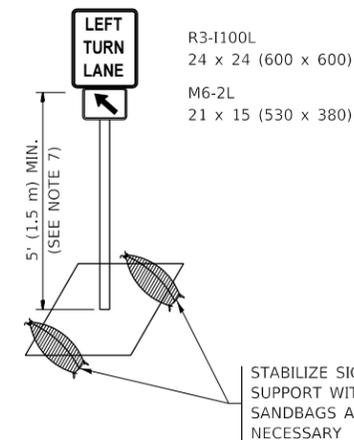
**FIGURE 2**

## LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

## NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

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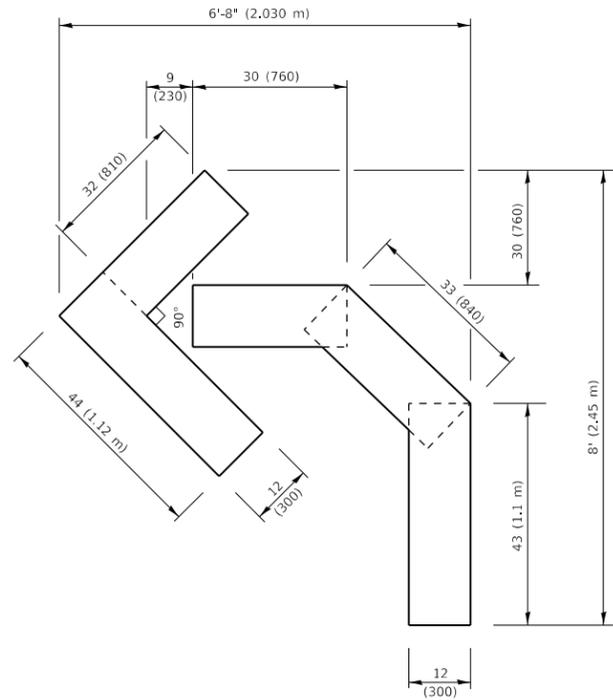
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	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100,0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 3/31/2020	DATE - T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)

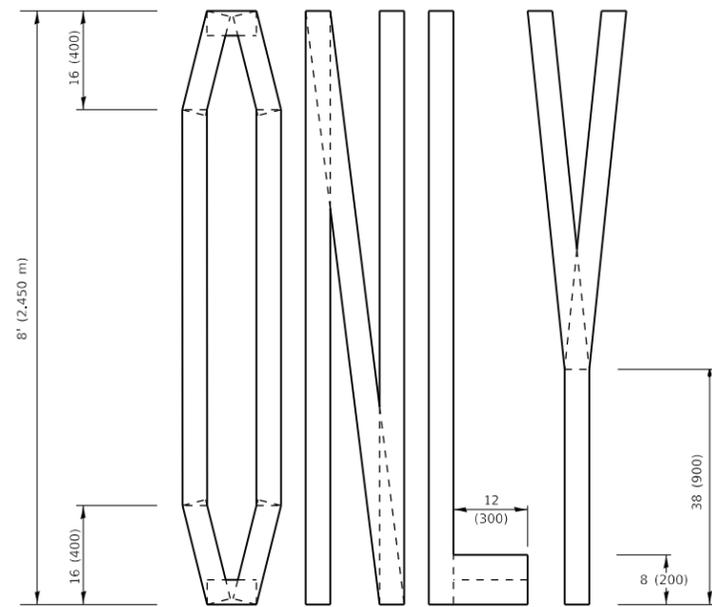
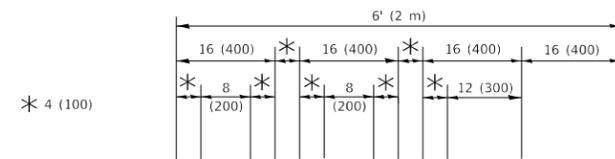
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-14		CONTRACT NO. 62J89		
ILLINOIS FED. AID PROJECT				



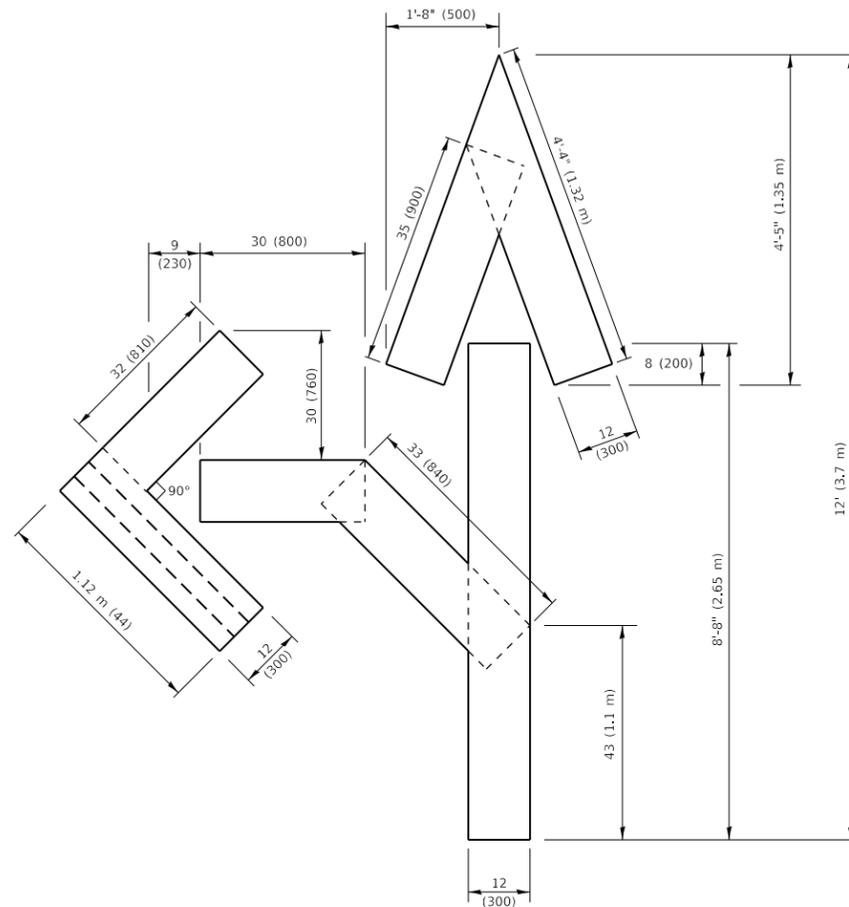
**QUANTITY**

4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.41 sq. m)



**QUANTITY**

4 (100) LINE = 64.1 ft. (19.5 m)  
21.4 sq. ft. (1.99 sq. m)

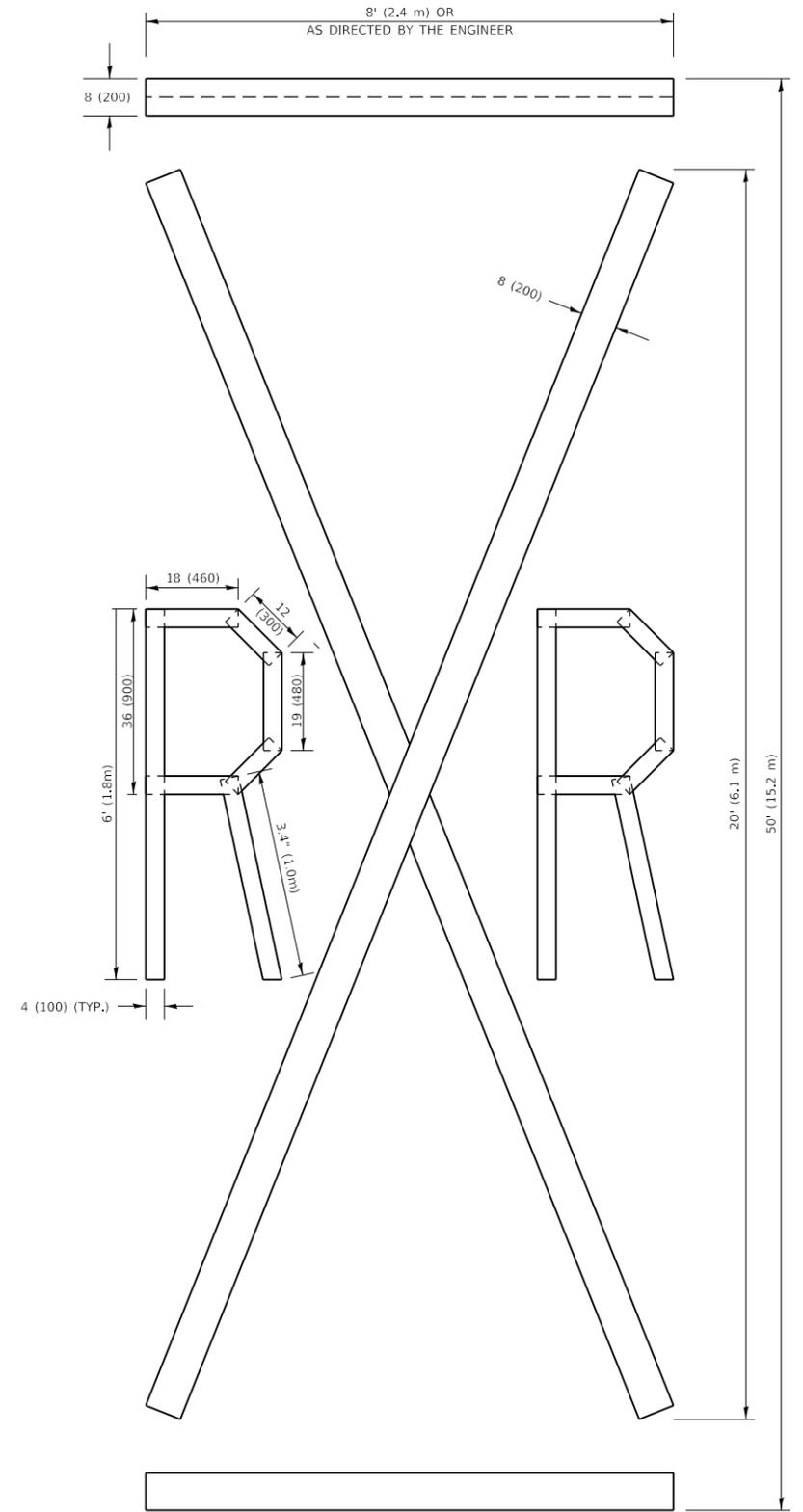


**QUANTITY**

4 (100) LINE = 82.5 ft. (25.1 m)  
27.5 sq. ft. (2.53 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**

4 (100) LINE = 225.9 ft. (68.9 m)  
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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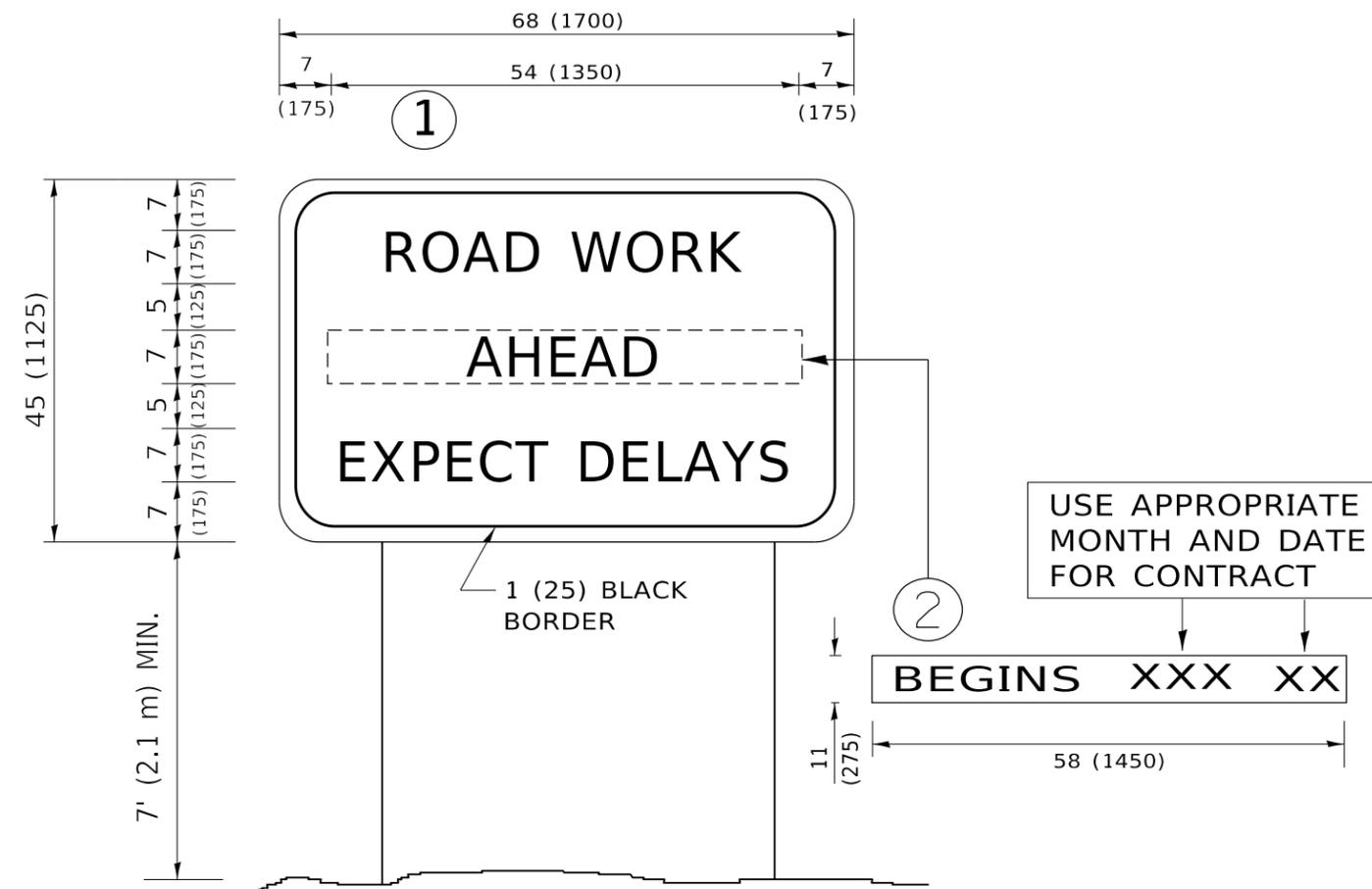
USER NAME = sklnneral	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 100,0010 ' / In.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/31/2020	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	31
<b>TC-16</b>			CONTRACT NO. 62J89	
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = skinnerl	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/31/2020	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

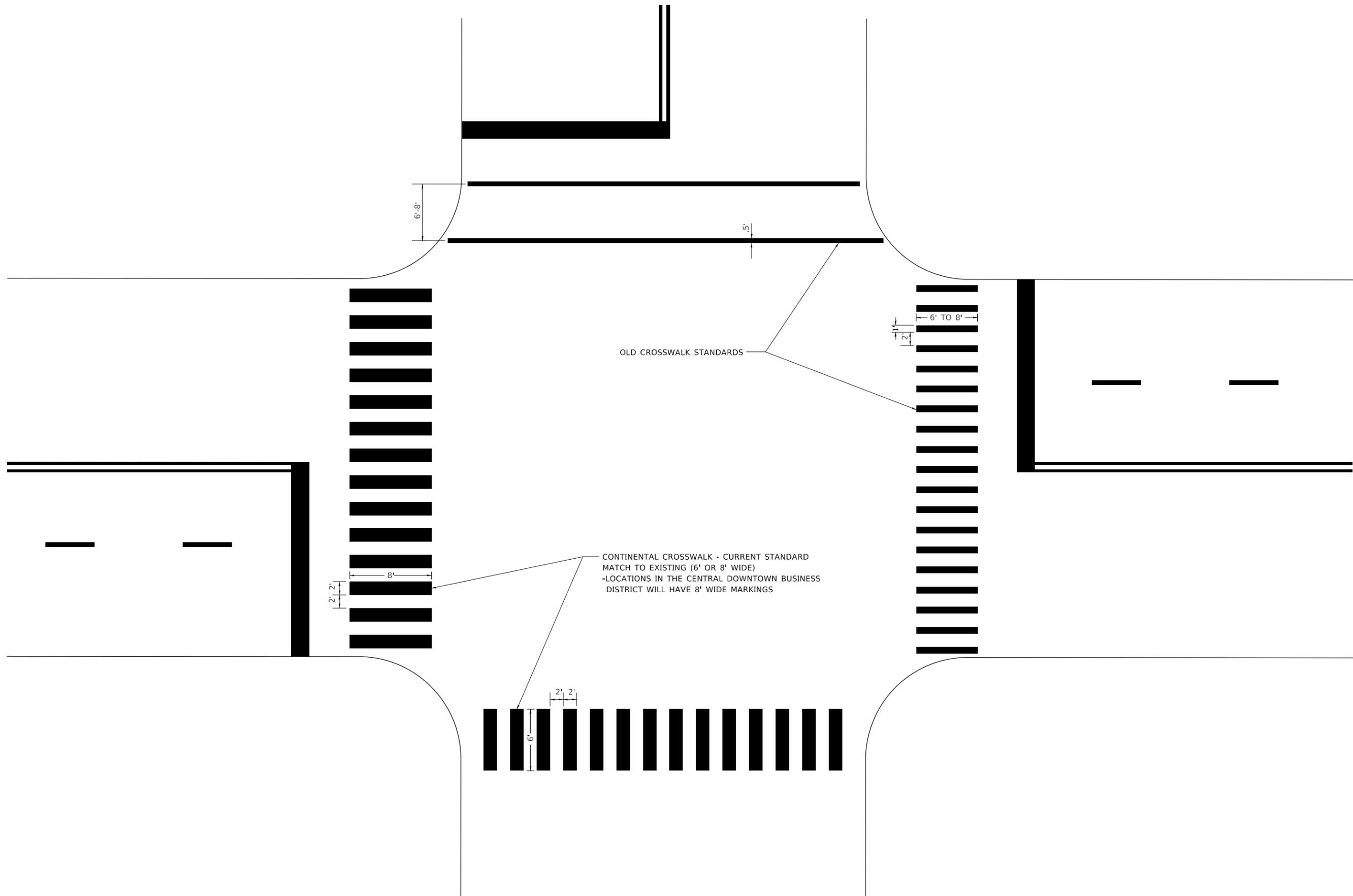
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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<b>TC-22</b>			CONTRACT NO. 62J89	
ILLINOIS FED. AID PROJECT				





MODEL: Default  
 FILE: Model: 2019-097-R5&SW\DOT Documents\DOT Offices\District 1\Projects\126720\CADD\Drawings\Design\DES.dgn



USER NAME = skinnerl	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
	DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 3/31/2020	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO  
 TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2911	2019-097-R5&SW	COOK	35	35
<b>TC-24</b>			CONTRACT NO. 62J89	
ILLINOIS FED. AID PROJECT				