

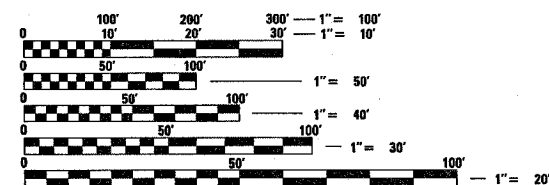
INDEX OF SHEETS

- 1 COVER SHEET
- 2 GENERAL NOTES AND LOCATION MAP
- 3 SUMMARY OF QUANTITIES
- 4 TYPICAL SECTIONS
- 5 SCHEDULE OF QUANTITIES
- 6 DETAILS

STANDARDS

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001006 DECIMAL OF AN INCH AND OF A FOOT
- 406201-01 MAILBOX TURNOUT
- 630301-04 SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
- 701001-01 OFF-ROAD OPERATIONS 2L, 2W, MORE THAN 4.5 m (15') AWAY
- 701006-02 OFF-ROAD OPERATIONS 2L, 2W, 4.5 m (15') TO 600 mm (24") FROM PAVEMENT EDGE
- 701011-01 OFF-ROAD MOVING OPERATIONS 2L, 2W, DAY ONLY
- 70101-01 OFF-ROAD OPERATIONS MULTILANE, 4.5 m (15') TO 600 mm (24") FROM PAVEMENT EDGE
- 701201-02 LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS P 45 MPH
- 701301-02 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701306-01 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS P 45 MPH
- 701901 TRAFFIC CONTROL DEVICES
- 780001-01 TYPICAL PAVEMENT MARKINGS
- 781001-02 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

MICROFILMED _____
 REEL NUMBER _____
 AWARDED _____
 RESIDENT ENGINEER _____
 AS BUILT CHANGES WERE MADE
 ON THE FOLLOWING SHEETS _____



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

PROJECT ENGINEER : JOE KANNEL
UNIT CHIEF : PAT BRABOY
 DISTRICT 3 NO. (815) 434-6131
CONTRACT NO. 66803

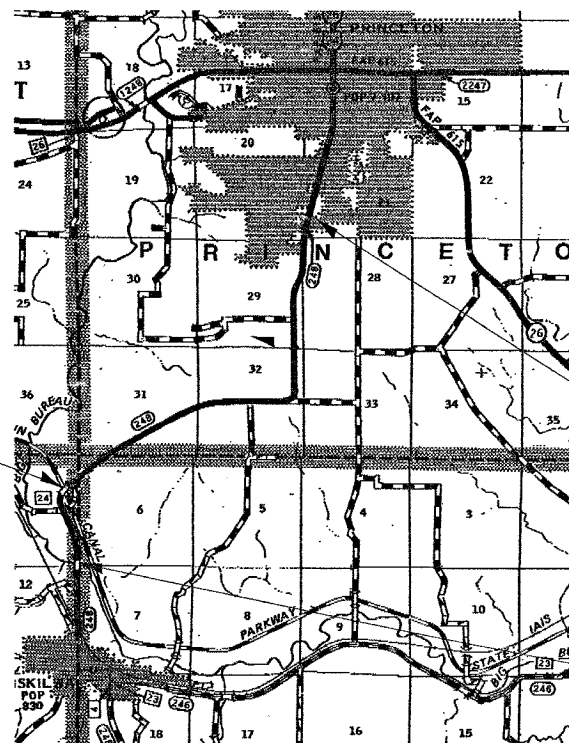
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED
HIGHWAY PLANS

FAS ROUTE 248 (TISKILWA SPUR)
SECTION 117RS-3
PROJECT RS-0248(104)
BUREAU COUNTY

C - 93 - 080 - 08

PROJECT DESCRIPTION

3P RESURFACING OF TISKILWA SPUR



SN 006-2552

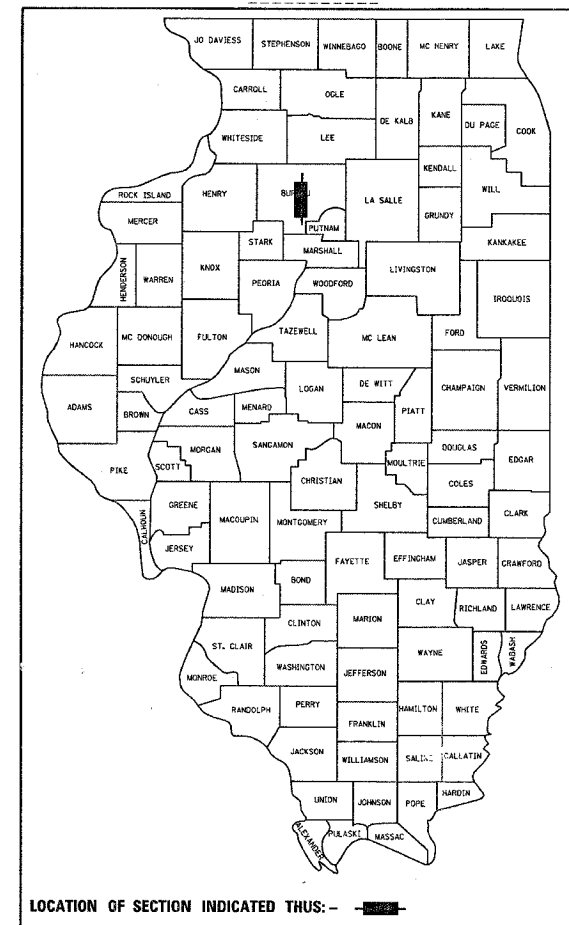
BEGIN PROJECT LM 0.00
 PRINCETON CITY LIMITS

END PROJECT LM 4.50
 N. ABUTMENT SN 006-0114

GROSS LENGTH = NET LENGTH = 23,760FT. = 4.5 MI.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
248	117RS-3	BUREAU	6	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 66803		

D-93-040-08



LOCATION OF SECTION INDICATED THUS: - [Symbol] -

FUNCTION CLASSIFICATION

MAJOR COLLECTOR

2006 ADT = 2150

P.V. = 92% S.U. = 6.2% M.U. = 1.8%

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED _____ 20____

George F. Ryan
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 9, 2008
Eric E. Harms
 INTERIM ENGINEER OF DESIGN AND ENVIRONMENT

May 9, 2008
Christine M. Reed
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS**

PLOT DATE = Mar 17, 2008 - 07:36:22 AM
 FILE NAME = c:\projects\366803\366803-shr-cover.dgn
 PLOT SCALE = 50.0000 / IN.
 USER NAME = braboyjc

GENERAL NOTES

(Revised October 5, 2006)

THE THICKNESS OF HMA SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA IS PLACED.

THE HMA SURFACE OF ALL MAILBOX TURNOUTS, PRIVATE ENTRANCES, COMMERCIAL ENTRANCES, AND SIDE ROADS SHALL BE MADE NEATLY, IN A WORKMANLIKE MANNER, AND SHALL ACCURATELY CONFORM TO THE SHAPES AND DIMENSIONS SHOWN ON THE PLAN DETAILS. IF REQUIRED BY THE ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SAW CUT THE HMA SURFACE TO CONFORM TO THE SHAPES AND DIMENSIONS SHOWN ON THE PLAN DETAILS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE HMA SURFACE

BEFORE ORDERING PIPE CULVERTS OR PIPE DRAINS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR EXACT LENGTHS.

THE ENGINEER WILL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS HMA LIFTS.

FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.

ON EXISTING PAVEMENT WHICH MAY BE SUPERELEVATED, THE NEW HMA PAVEMENT SHALL BE BUILT WITH THE SAME SUPERELEVATION UNLESS NEW SUPERELEVATION RATES ARE GIVEN ON THE PLANS.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

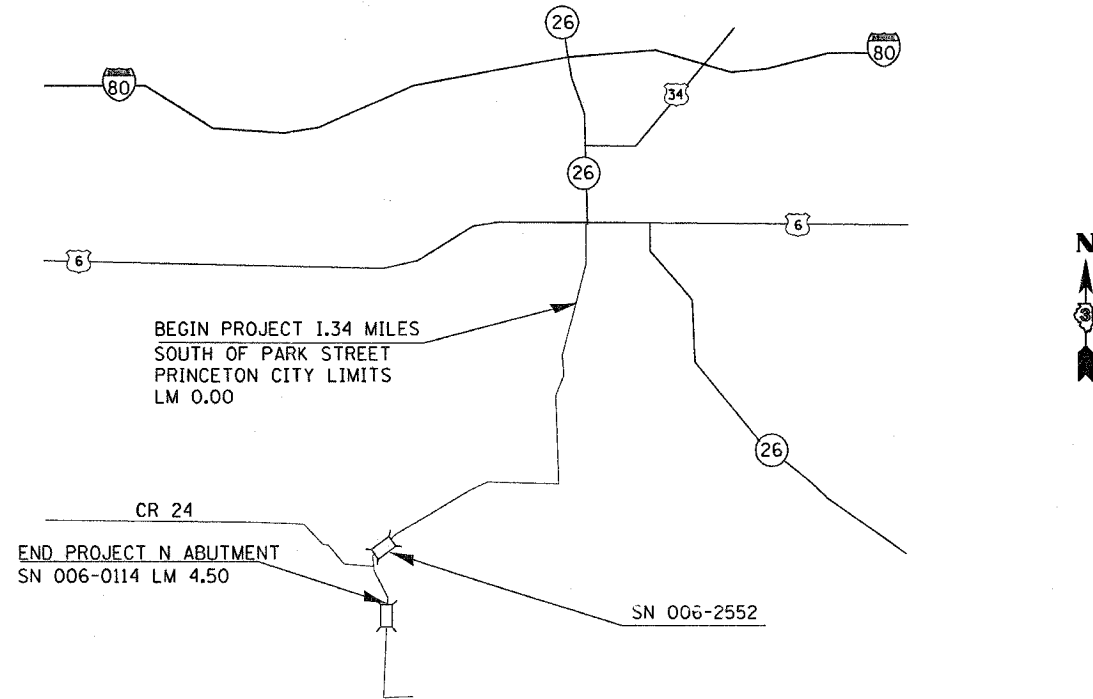
THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

GRANULAR MATERIALS	2.05	TONS / CU YD
BITUMINOUS MAT PRIME COAT	0.08	GAL / SQ YD OR
	0.375	GAL / SQ YD
AGGREGATE PRIME COAT	0.002	TONS / SQ YD
HMA RESURFACING	112	LBS / SQ YD / IN
SHORT TERM PAVEMENT MARKING	10	FT / 100 FT OF APPLICATION
MIX FOR CRACKS, JTS & FLGWYS	0.0003	TONS / SQ YD
LEVEL BINDER (HAND METHOD)	0.0005	TONS / SQ YD

ALL DAMAGE TO DEPARTMENT OWNED UNDERGROUND FACILITIES, CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTOR'S EXPENSE. THIS SHALL INCLUDE ALL TEMPORARY REPAIRS REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS. SPLICING OF ELECTRIC CABLE SHALL NOT BE ALLOWED. ELECTRIC CABLE SHALL BE REPLACED FROM POLE TO POLE OR CONTROLLER.

THE FINISHED EARTHWORK SHALL HAVE A VEGETATION SUSTAINING SOIL COVERING THE TOP FOUR INCHES IN AREAS TO BE SEEDDED OR SODDED. THE VEGETATION SUSTAINING SOIL REQUIRED WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF FURNISHED EXCAVATION.

AREAS THAT WILL BE WIDENING WITH FURNISHED EXCAVATION BY THE USE OF STANDARD 630301 SHALL BE SEEDDED, MULCHED AND FERTILIZED. THESE ITEMS SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE INCLUDED IN THE COST OF FURNISHED EXCAVATION.



LOCATION MAP

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DISTRICT THREE

PREPARED BY: *Rob Powell*
DISTRICT STUDIES & PLANS ENGINEER

DATE: 3-17-08

EXAMINED BY: *Hubert*
DISTRICT CONSTRUCTION ENGINEER

Jim
DISTRICT MATERIALS ENGINEER

James A. ...
DISTRICT OPERATIONS ENGINEER

FILE NAME *	USER NAME = braboyan	DESIGNED -	REVISD -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISD -			248	11783-3	BUREAU	6	2	
		CHECKED -	REVISD -			CONTRACT NO. 66803					
		DATE	REVISD -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					
PLOT SCALE = 50:000 / IN				SCALE: _____ SHEET NO. ___ OF ___ SHEETS STA. _____ TO STA. _____							
PLOT DATE = Mar 15, 2008 - 01:56:38 PM											

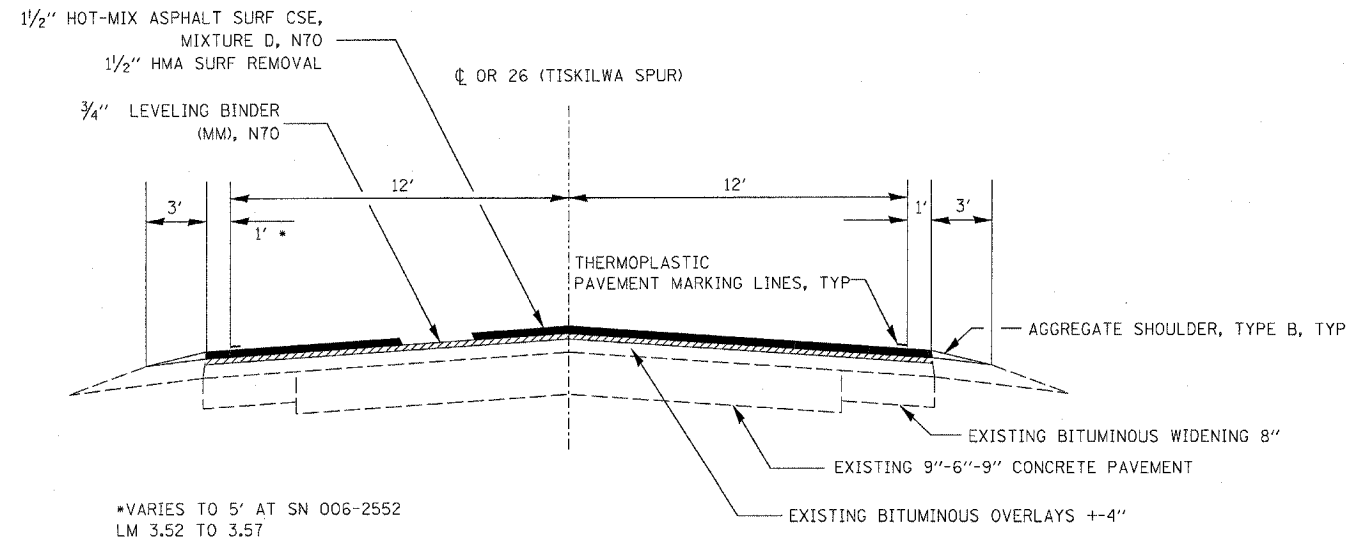
SUMMARY OF QUANTITIES				80% FED. 20% STATE	
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE	
				ROADWAY I000	
20400800	FURNISHED EXCAVATION	CU YD	122		122
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	5792		5792
40600300	AGGREGATE (PRIME COAT)	TON	160		160
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	22		22
40600535	LEVELING BINDER (HAND METHOD), N70	TON	36		36
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	2980		2980
40600990	TEMPORARY RAMP	SQ YD	99		99
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	5990		5990
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	159		159
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	69227		69227
44201007	CLASS B PATCHES, TYPE II, 13 INCH	SQ YD	264		264
44213200	SAW CUTS	FOOT	1584		1584
48101200	AGGREGATE SHOULDERS, TYPE B	TON	1838		1838
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1(SPECIAL) TANGENT	EACH	16		16
63200310	GUARDRAIL REMOVAL	FOOT	800		800
67100100	MOBILIZATION	L SUM	1		1
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1		1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	9504		9504
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	47520		47520
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	5940		5940
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	785		785
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	47520		47520
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	5940		5940
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	297		297
* 78201000	TERMINAL MARKERS-DIRECT APPLIED	EACH	16		16
Z0017100	DOWEL BARS	EACH	330		330

* SPECIALTY ITEMS

	HMA SURFACE	HMA LEVELING BINDER
PG GRADE	PG 64-22	PG 64-22
MAX % RAP ALLOWABLE**	10%***	15%***
DESIGN AIR VOIDS	4% @ N70	4% @ N70
MIXTURE COMPOSITION	IL 12.5 OR IL 9.5	IL 9.5
FRICTION AGGREGATE	MIXTURE D	-
DENSITY CONTROL LIMITS	CORRELATION	SATISFACTION OF ENGINEER

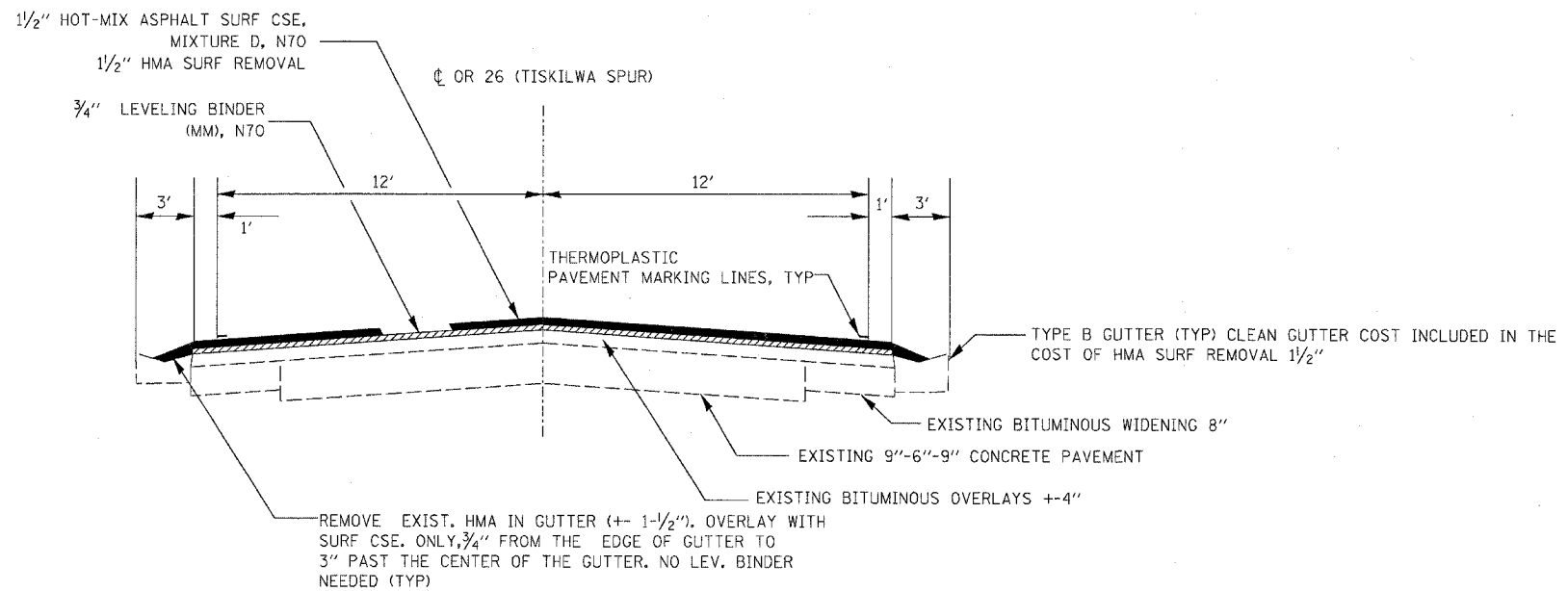
** IF RAP OPTION IS SELECTED, THE ASPHALT CEMENT GRADE MAY NEED TO BE ADJUSTED. THIS WILL BE DETERMINED BY THE ENGINEER.

***IF 3/8" RAP IS UTILIZED, MAXIMUM PERCENT MAY BE INCREASED TO 25% FOR BINDER AND 15% FOR SURFACE. SEE RAP SPECIAL PROVISIONS



TYPICAL SECTION

LM 0.00 TO LM 2.70
LM 2.90 TO LM 4.50



TYPICAL SECTION

LM 2.7 TO LM 2.9

FILE NAME =	USER NAME = braboygo	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PROJECT NO. = 366803	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -			248	11785-3	BURBANK	6	4	
PLOT DATE = Mar 17, 2008 - 04:45:56 PM	DATE -	CHECKED -	REVISED -			CONTRACT NO. 66803					
						FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

MAINLINE													
LOCATION	WIDTH	LENGTH	AREA	HMA SURF REM 1 1/2"	HMA LEVEL BIND (HM)	LEVEL BINDER (MM) N70	HMA SC MIX "D" N70	MIX CR JTS FLANGEWYS	BIT MATLS PR CT	AGG PR CT	HMA SURF REM BUTT JT.	TEMP RAMP	AGG SHLD TY B
STATION	FT	FT	SQ YD	SQ YD	TON	TON	TON	TON	GAL	TON	SQ YD	SQ YD	TON
0.00 TO 2.7	26'	14256.0	41184.0	41184.0	20.6	1729.7	3459.5	12.4	3294.7	82.4	116.0	8.0	1082.4
2.7 TO 2.9	29'	1056.0	3402.7	3402.7	1.7	128.1	285.8	1.0	272.2	6.8	116.0	8.0	80.2
2.9 TO 3.52	26'	3274.0	9458.2	9458.2	4.7	397.2	794.5	2.8	756.7	18.9	116.0	8.0	248.6
3.52 TO 3.57	34'	264.0	997.3	997.3	0.5	41.9	83.8	0.3	79.8	2.0	116.0	8.0	8.0
3.57 TO 4.50	26'	4910.0	14184.4	14184.4	7.1	595.7	1191.5	4.3	1134.8	28.4	116.0	8.0	372.8
TOTALS		23760.0	69226.7	69226.7	35.7	2979.5	5989.5	21.4	5704.3	142.6	464.0	41.2	1837.5

PAVEMENT MARKING							
LOCATION	THPL PVT	THPL MK LN	TEMP PVT	TEMP MK LN	RAISED REF	SHORT TERM	WORK ZONE
	4"	6"	4"	6"	MKR	MKR	MKR
	WHITE	YEL	WHITE	YEL			REM
	FT	FT	FT	FT	EACH	FT	SQ FT
	0.00 TO 4.5	47520.0	5940.0	47520.0	5940.0	297	9504
TOTALS	47520.0	5940.0	47520.0	5940.0	297.0	9504.0	785.0

SIDE ROAD & ENTRANCES								
LOCATION	MILE	RT/LT	INCIDENTAL HMA SURF	BIT MATLS PR CT	HMA SURF REM. BUTT JT.	TEMP. RAMP	FURN. EX*	AGG PR CT
			TON	GAL	SQ YD	SQ YD	CU YD	TON
PROJECT BEGINS @ CITY LIMITS								
	0.00							
P.E.	0.05	RT	2.00	1.00	13.00			0.30
P.E./MAILBOX TURNOUT	0.10	RT	3.00	2.00				0.40
P.E./MAILBOX TURNOUT	0.12	RT	3.00	2.00				0.40
P.E./MAILBOX TURNOUT	0.40	RT	3.00	2.00				0.40
P.E./MAILBOX TURNOUT	0.50	RT	3.00	2.00				0.40
P.E.	0.55	LT	2.00	1.00				0.30
P.E./MAILBOX TURNOUT	0.60	LT	3.00	2.00	13.00			0.30
LOVER'S LANE	0.61	RT	10.00	5.00		8.00	2.00	1.20
P.E.	0.70	RT	2.00	1.00				0.30
P.E./MAILBOX TURNOUT	0.73	RT	3.00	2.00				0.40
P.E.	0.80	LT	2.00	1.00				0.20
P.E./MAILBOX TURNOUT	1.00	RT	3.00	2.00				0.30
P.E./MAILBOX TURNOUT	1.20	RT	3.00	2.00				0.30
P.E./MAILBOX TURNOUT	1.30	LT	3.00	2.00	13.00			0.30
SIDEROAD	1.40	LT	10.00	5.00	80.00	8.00		1.20
P.E.	1.50	LT	2.00	1.00				0.20
P.E.	1.60	LT	2.00	1.00				0.20
1950 EAST RD	1.70	LT	10.00	5.00	80.00	8.00	2.00	1.20
P.E.	1.71	RT	2.00	1.00				0.20
P.E.	2.00	RT	2.00	1.00				0.20
P.E./MAILBOX TURNOUT	2.01	LT	3.00	2.00				0.30
P.E.	2.10	LT/RT	4.00	2.00				0.30
P.E./MAILBOX TURNOUT	2.20	LT	3.00	2.00				0.30
P.E.	2.30	RT	2.00	1.00				0.20
P.E.	2.30	RT	2.00	1.00				0.20
MAILBOX TURNOUT	2.31	RT	1.00	1.00				0.20
P.E./MAILBOX TURNOUT	2.38	LT/RT	3.00	2.00				0.30
P.E./MAILBOX TURNOUT	2.40	LT/RT	3.00	2.00				0.30
P.E./MAILBOX TURNOUT	2.41	LT	3.00	2.00	13.00			0.30
P.E./MAILBOX TURNOUT	2.50	LT	3.00	2.00	13.00			0.30
P.E./MAILBOX TURNOUT	2.70	LT	3.00	2.00	13.00			0.30
P.E.	3.10	RT	2.00	1.00				0.20
P.E.	3.10	LT	2.00	1.00				0.20
P.E./MAILBOX TURNOUT	3.20	LT	2.00	1.00				0.20
MAILBOX TURNOUT	3.36	LT	1.00	1.00				0.20
P.E.	3.40	RT	2.00	1.00				0.20
P.E./MAILBOX TURNOUT	3.45	LT	3.00	2.00				0.30
SIDEROAD	3.60	LT	10.00	5.00		8.00	2.00	1.00
P.E./MAILBOX TURNOUT	3.61	RT	3.00	2.00				0.30
P.E./MAILBOX TURNOUT	3.70	RT	3.00	2.00				0.30
SIDEROAD	3.85	LT/RT	20.00	10.00	160.00	16.00	4.00	2.00
P.E./MAILBOX TURNOUT	4.30	LT	3.00	2.00				0.30
TOTALS			158.62	88.00	398.00	48.00	12.00	17.41

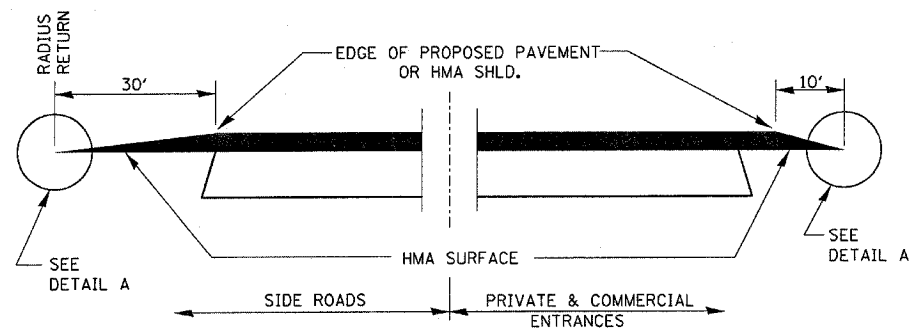
GUARDRAIL SCHEDULE					
LOCATION	SIDE	GUARDRAIL REMOVAL	TERMINAL MARKER, DIRECT APPLIED	TBT TYPE 1 SPECIAL TANGENT	FURNISHED EXCAVATION*
MILE MARKER	LT/RT	FOOT	FOOT	FOOT	CU YD
2.81	LT/RT	100	2	2	10
2.91	LT/RT	100	2	2	10
3	LT/RT	200	4	4	20
3.45	LT/RT	200	4	4	50
4.35	LT/RT	100	2	2	10
4.6	LT/RT	100	2	2	10
TOTAL		800	16	16	110

BUILD EMBANKMENT BY TBT, T1 END SECTIONS ACCORDING TO STANDARD 630301. THIS AREA TO BE SEEDED FERTILIZED, AND MULCHED-THESE ITEMS NOT TO BE PAID FOR SEPERATELY BUT INCLUDED IN THE COST OF FURNISHED EXCAVATION.

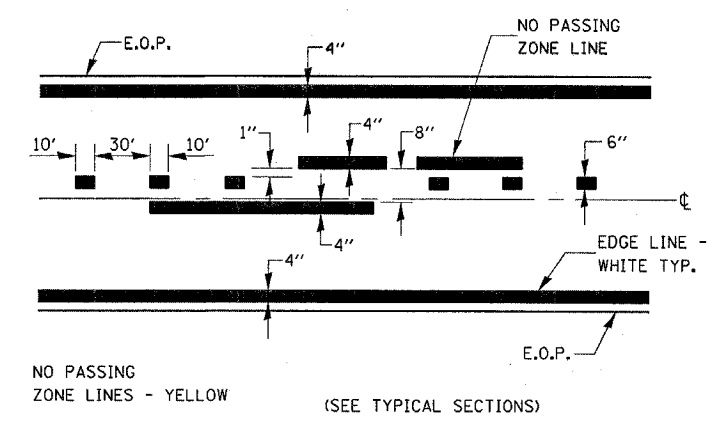
CLASS B PATCHING 13"						
STA (LOG MILES)	CL B PATCH TY II		DOWEL BARS		SAW CUTS	
	SQ YD		EACH		FOOT	
	NBL	SBL	NBL	SBL	NBL	SBL
LM 0.00 IS 1.34 MILES SOUTH OF PARK STREET						
0.1		8		10		48
0.3	8		10		48	
0.7	8	8	10	10	48	48
1		8		10		48
1.1		8		10		48
1.5	8		10		48	
2	8	8	10	10	48	48
2.3	8	8	10	10	48	48
3	8	8	10	10	48	48
3.2	8	8	10	10	48	48
3.4	8	8	10	10	48	48
3.9	8	8	10	10	48	48
4	8	8	10	10	48	48
4.1	8	8	10	10	48	48
4.15	8	8	10	10	48	48
4.15	8	8	10	10	48	48
4.25	8	8	10	10	48	48
4.3	8	8	10	10	48	48
4.35	8	8	10	10	48	48
TOTAL	128	136	160	170	768	816
GRAND TOTAL		264		330		1584

*PLACE FURNISHED EX AROUND SIDERAOD RADII. THIS AREA TO BE SEEDED, FERTILIZED AND MULCHED- NOT PAID FOR SEPERATELY BUT INCLUDED IN THE COST OF FURNISHED EXCAVATION

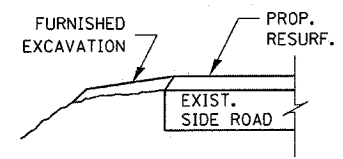
FILE NAME =	USER NAME = brcaypgo	DESIGNED - ---	REVISED - ---	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES	F.A.S. RTE. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =
ca\proj\octs\366883\366883-sht-details.dgn		DRAWN - ---	REVISED - ---			248	1178S-3	BUREAU =	6	5
PLOT SCALE = 50.0000' / IN.		CHECKED - ---	REVISED - ---			CONTRACT NO. 66803				
PLOT DATE = Mar 15, 2008 - 01:51:36 PM		DATE - ---	REVISED - ---			FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				



SECTION A-A
DETAILS AT ENTRANCES & SIDE ROADS

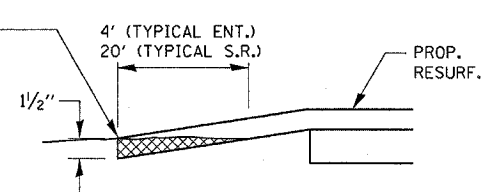


PAVEMENT MARKING 780-8

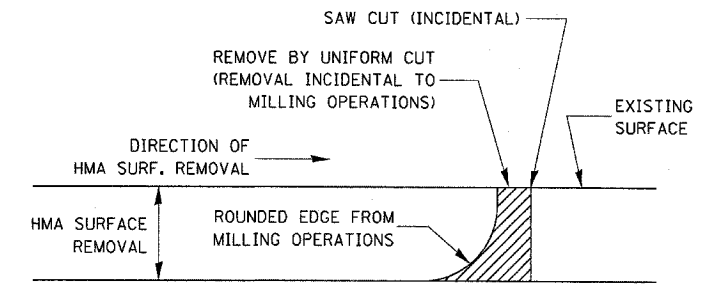


SECTION B-B

THE COST OF REMOVAL AT EXISTING HMA OR P.C.C. LOCATIONS SHALL BE PAID FOR PER SQ. YD. BY THE APPROPRIATE PAY ITEM. REMOVAL AT THE EXISTING AGG. LOCATIONS SHALL BE INCIDENTAL TO THE HMA. A-3 LOCATIONS SHALL BE FEATHER TAPERED.

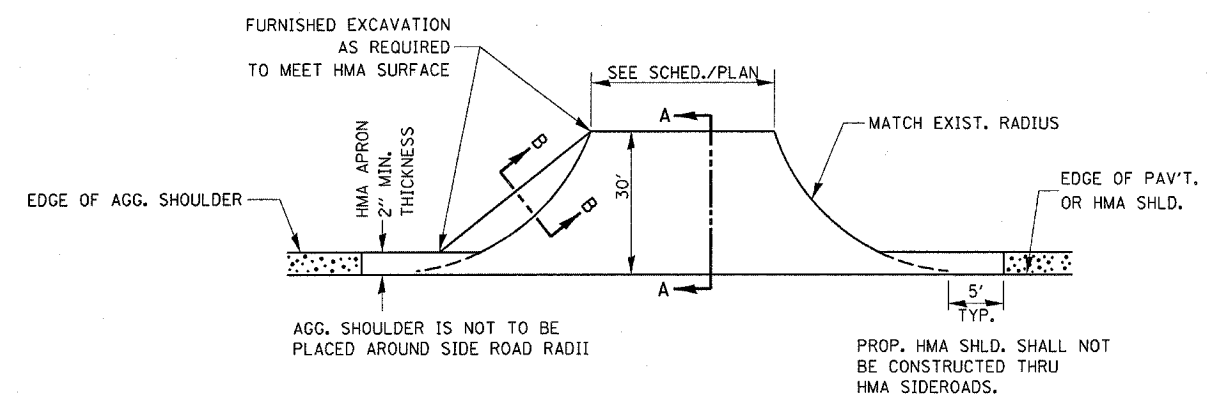


DETAIL A

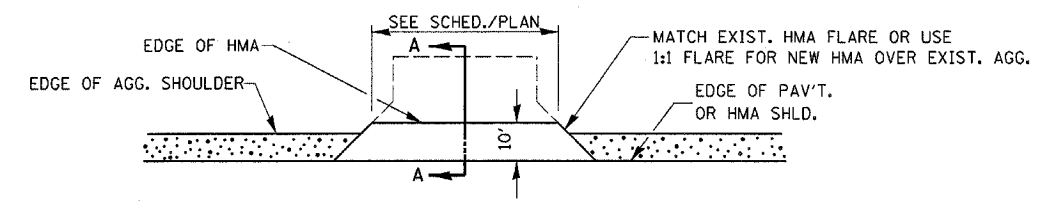


NOTE:
WHEN MILLING OPERATIONS PRODUCE A ROUNDED EDGE, THEN A SAW CUT SHALL BE USED TO MANUFACTURE A PERPENDICULAR EDGE AS SHOWN IN THE DETAIL. THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE USE OF THIS DETAIL.

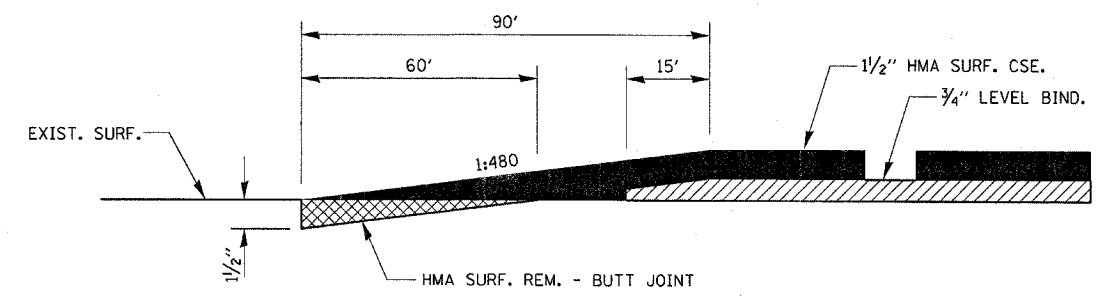
HMA DETAIL AT BUTT JOINTS



PLAN AT SIDE ROADS



PLAN AT PRIVATE & COMMERCIAL ENTRANCES
(DO NOT RESURFACE FIELD ENTRANCES)



FILE NAME =	USER NAME = braboyco	DESIGNED -	REVISD -
cr\projects\366803\366803-ent-detail.sgn		DRAWN -	REVISD -
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISD -
	PLOT DATE = Mar 15, 2008 - 01:51:44 PM	DATE -	REVISD -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS	
SCALE: _____	SHEET NO. ___ OF ___ SHEETS
STA. _____ TO STA. _____	

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
248	11ZBS-3	BUBEAU	6	6
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 66803	