

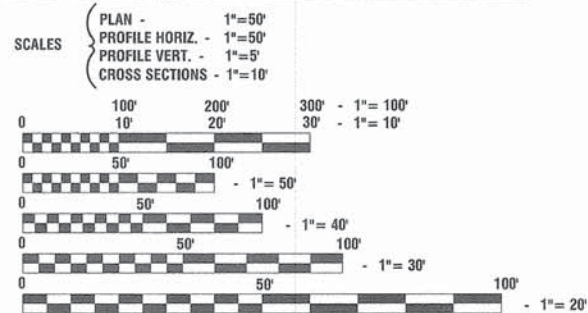
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- 3. TYPICAL SECTIONS
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- 10.-18. IDOT DISTRICT 1 STANDARD DETAILS

HIGHWAY STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 424001-07 PERPENDICULAR CURB RAMPS
- 424006-01 DIAGONAL CURB RAMPS FOR SIDEWALKS
- 424011-01 CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
- 424016-01 MID-BLOCK CURB RAMPS FOR SIDEWALKS
- 424021-02 DEPRESSED CORNER FOR SIDEWALKS
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- 442201-03 CLASS C AND D PATCHES
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- 606001-05 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 701011-04 OFF-ROAD MOVING OPERATIONS, 2L, 2W DAY ONLY
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- 720011-01 METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
- 780001-04 TYPICAL PAVEMENT MARKINGS
- 886001-01 DETECTOR LOOP INSTALLATIONS
- 886006-01 TYPICAL LAYOUT FOR DETECTOR LOOPS

RIDGE ROAD	
2012 ADT -	3,700
POSTED SPEED LIMIT -	25 mph
DESIGN PERIOD -	20 YEARS
DESIGN SPEED LIMIT -	30 mph
STREET CLASSIFICATION -	MAJOR COLLECTOR

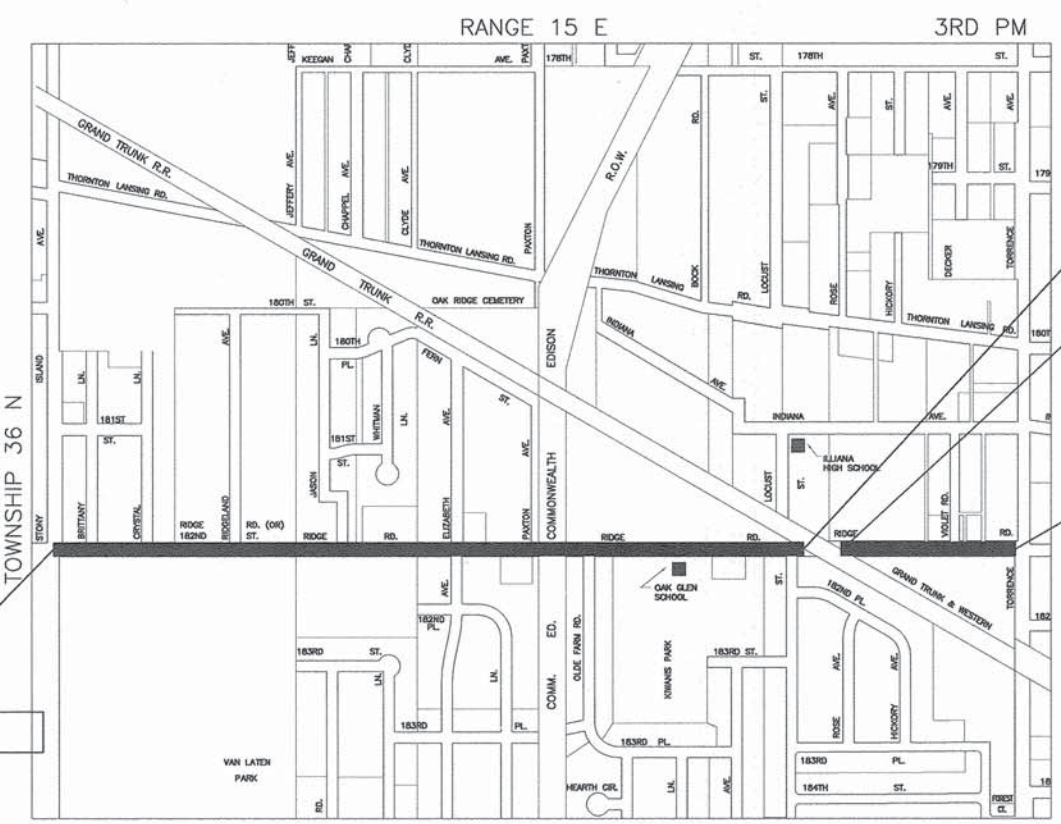


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 or 811

CONTRACT NO. 61A47

STATE OF ILLINOIS 06-13-14 LETTING ITEM 049
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**
FAU 1621 (RIDGE ROAD)
FAU 2933 (STONY ISLAND AVENUE) TO FAU 2937 (TORRENCE AVENUE)
ROADWAY RESURFACING
SECTION NO.: 13-00173-00-RS
PROJECT NO.: M-4003(314)
VILLAGE of LANSING
COOK COUNTY
JOB NO.: C-91-266-14

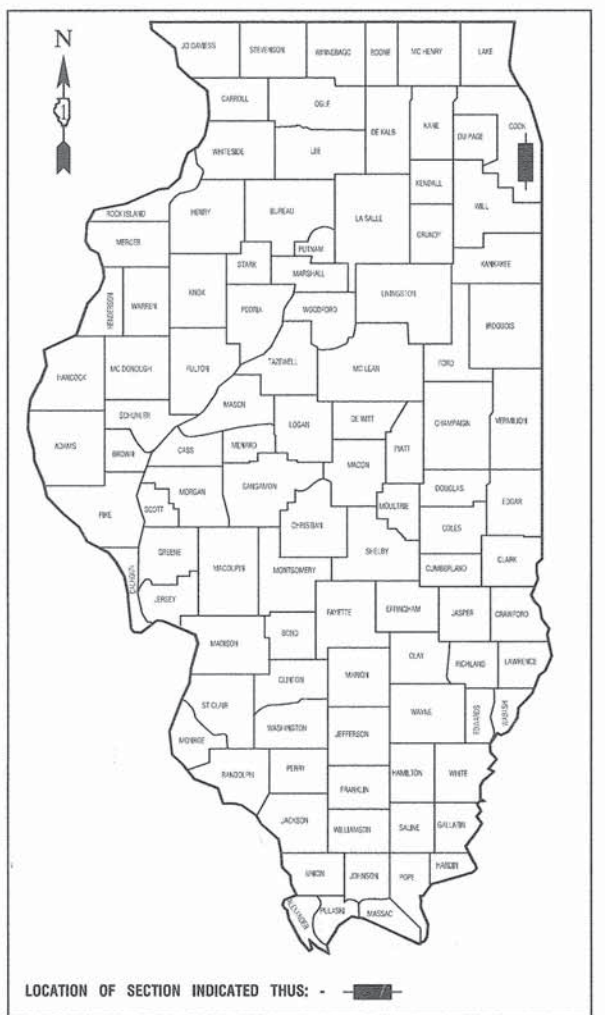


**PROJECT LOCATED IN THE
VILLAGE OF LANSING**

LOCATION MAP
GROSS LENGTH=5268 FEET=1.0 MILES
NET LENGTH=5213 FEET=0.99 MILES

F. A. L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	1
STA. TO STA.				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-4003(314)	

CONTRACT #61A47



STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved 3-20-2014
Norm Z. ...
President, Village of LANSING

Passed April 4, 2014
Christopher Holt
District 1 Engineer of Local Roads & Streets

Released for Bid Based on Limited Review
April 7, 2014
John Faustmann
Deputy Director of Highways, Region 1 Engineer

PRINTED BY THE AUTHORITY OF
THE STATE OF ILLINOIS

PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:
J. C. ...
3/20/14

JEFFREY C. P. ...
62-5946
REGISTERED PROFESSIONAL ENGINEER
STATE OF ILLINOIS
LICENSE EXPIRES: 11/30/15

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E. (847)705-4021, SCHAUMBURG, IL.
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE	
S.I.	CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	0005
	20200100	EARTH EXCAVATION	CU YD	5	5
	31101200	SUBBASE GRANULAR MATERIAL, TYPE B, 4"	SQ YD	154	154
	31101900	SUBBASE GRANULAR MATERIAL, TYPE C	TON	122	122
	35101600	AGGREGATE BASE COURSE, TYPE B, 4"	SQ YD	17	17
	35800100	PREPARATION OF BASE	SQ YD	198	198
	35800200	AGGREGATE BASE REPAIR	TON	65	65
	40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	65	65
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10	10
	40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	730	730
	40600895	CONSTRUCTING TEST STRIP	EACH	1	1
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	595	595
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1946	1946
	42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	60	60
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1537	1537
	42400800	DETECTABLE WARNINGS	SQ FT	184	184
	44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	17373	17373
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	327	327
	44000600	SIDEWALK REMOVAL	SQ FT	1385	1385
	44201725	CLASS D PATCHES, TYPE I, 7 INCH	SQ YD	9	9
	44201729	CLASS D PATCHES, TYPE II, 7 INCH	SQ YD	16	16
	44201733	CLASS D PATCHES, TYPE III, 7 INCH	SQ YD	38	38
	44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SQ YD	135	135
	48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	533	533
	60250200	CATCH BASINS TO BE ADJUSTED	EACH	18	18
	60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1

* - INDICATES SPECIALTY ITEMS

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE	
S.I.	CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	0005
	60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1
	60255500	MANHOLES TO BE ADJUSTED	EACH	12	12
	60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1
	60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1
	60260100	INLETS TO BE ADJUSTED	EACH	9	9
	60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1
	67100100	MOBILIZATION	L SUM	1	1
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
	70300100	SHORT-TERM PAVEMENT MARKING	FOOT	78	78
	72000100	SIGN PANEL - TYPE 1	SQ FT	329	329
	72900200	METAL POST - TYPE B	FOOT	410	410
	* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	158.8	158.8
	* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1920	1920
	* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	290	290
	* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	210	210
	* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	344	344
	* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	144	144
	X2520650	SODDING, SALT TOLERANT (SPECIAL)	SQ YD	204	204
	X40600110	BITUMINOUS MATERIALS (PRIME COAT)	POUND	15636	15636
	Z0004514	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 4"	SQ YD	267	267
	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1301	1301
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1

GENERAL NOTES

ROBINSON ENGINEERING SHALL BE NOTIFIED TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION. CALL (708) 331-6700

THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE 1 AND TYPE 2 BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE INSTALLED ON EACH BOTTOM RAIL.

THE REMOVAL OF ANY DRIVEWAYS, PAVEMENT, CURB, ETC. SHALL BE ACCOMPLISHED BY MEANS OF A SAW CUT JOINT, AT THE DIRECTION OF THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE VARIOUS REMOVAL ITEMS.

WHEN, IN THE CONSTRUCTION OPERATION, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH DAY BY THE CONTRACTOR AT HIS EXPENSE. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM ALL DIRT AND DEBRIS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL J.U.L.I.E. AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, COMMUNICATIONS, AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED.)

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

SCHEDULES INCLUDED IN THE PLANS HAVE BEEN PREPARED FROM FIELD NOTES. EXACT LOCATIONS FOR PATCHING SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.

AREAS DISTURBED BY CONSTRUCTION SHALL BE KEPT TO A MINIMUM. ALL AREAS DISTURBED UNNECESSARILY SHALL BE RESTORED AT THE CONTRACTORS EXPENSE.

UTILITIES INDICATED ON THE PLANS ARE PROVIDED FOR THE CONTRACTORS USE AND ARE BASED UPON INFORMATION AVAILABLE AT THE TIME OF THE ADVERTISEMENT FOR BIDS. THE OWNER AND ENGINEER DO NOT GUARANTEE THE ACCURACY OF THE UTILITY INFORMATION.

ITEMS OF WORK LISTED IN THE SUMMARY OF QUANTITIES NOT SPECIFICALLY CALLED OUT ON THE PLANS SHALL BE PERFORMED AS DIRECTED BY THE ENGINEER.

ANY REFERENCE TO THE STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

THE NOMINAL THICKNESS OF HOT-MIX ASPHALT MIXTURE STATED IN THE SPECIFICATIONS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT SURFACE IS PLACED.

CONTRACTOR SHALL MILL EXISTING PAVEMENT PRIOR TO PATCHING.

FILE NAME = 13636-QUAN-01 - IDOT.P01

USER NAME =	DESIGNED -- MN	REVISED --
	CHECKED -- JP	REVISED --
PLOT SCALE =	DRAWN -- ACAD	REVISED --
PLOT DATE = 01-13-14	CHECKED -- ACAD	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAU 1621 (RIDGE ROAD)
STONY ISLAND AVENUE TO TORRENCE AVENUE
SUMMARY OF QUANTITIES

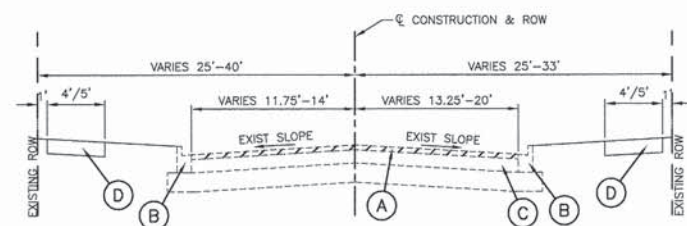
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SHEET NO. 2 OF 18 SHEETS

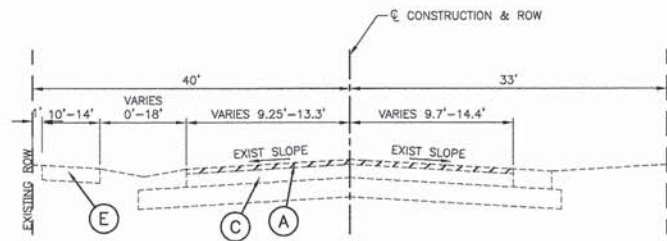
STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	2
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT M-4003(314)	

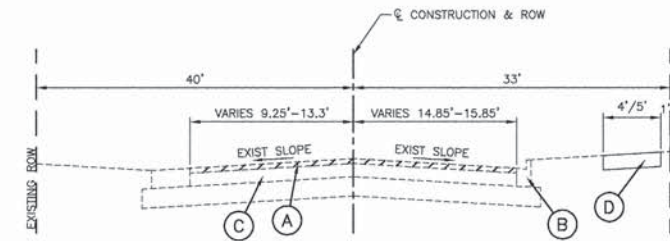
CONTRACT NO. 61A47



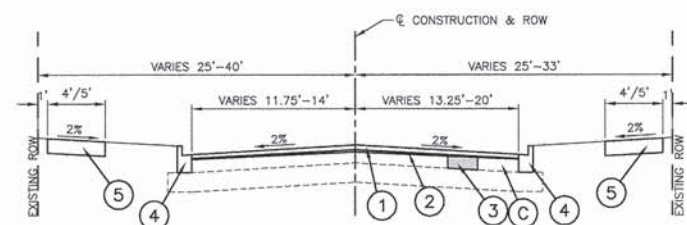
EXISTING TYPICAL SECTION
 RIDGE ROAD
 STA. 500+14 TO STA. 506+63
 STA. 519+95 TO STA. 552+81



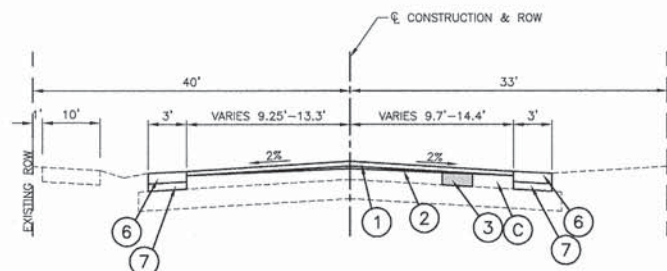
EXISTING TYPICAL SECTION
 RIDGE ROAD
 STA. 506+63 TO STA. 513+27



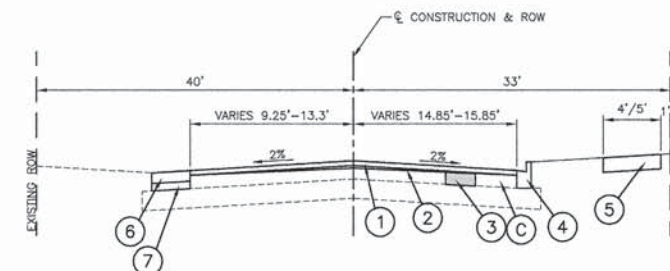
EXISTING TYPICAL SECTION
 RIDGE ROAD
 STA. 513+27 TO STA. 519+94



PROPOSED TYPICAL SECTION
 RIDGE ROAD
 STA. 500+14 TO STA. 506+63
 STA. 519+95 TO STA. 552+81
 NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING



PROPOSED TYPICAL SECTION
 RIDGE ROAD
 STA. 506+63 TO STA. 513+27
 NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING



PROPOSED TYPICAL SECTION
 RIDGE ROAD
 STA. 513+27 TO STA. 519+94
 NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS
 (CONTRACTOR SHALL MILL BEFORE PATCHING)

MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5 MM), 1 1/2"	4% @ 50 Gyr.	QCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" MIN. & VARIES	4% @ 50 Gyr.	QC/QA
PAVEMENT PATCHING		
HOT-MIX ASPHALT BINDER COURSE IL-19.0mm, N70, 7" (IN 2 LIFTS)	4% @ 70 Gyr.	QC/QA
HMA DRIVEWAYS		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 MM) (1 3/4")	4% @ 50 Gyr.	QC/QA
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 (2 1/4")	4% @ 50 Gyr.	QC/QA
HMA SHOULDER		
HOT-MIX ASPHALT SHOULDER, 8"	4% @ 50 Gyr.	QC/QA

QMP OPTIONS: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP), PAY FOR PERFORMANCE (PFP)

- NOTES:
1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

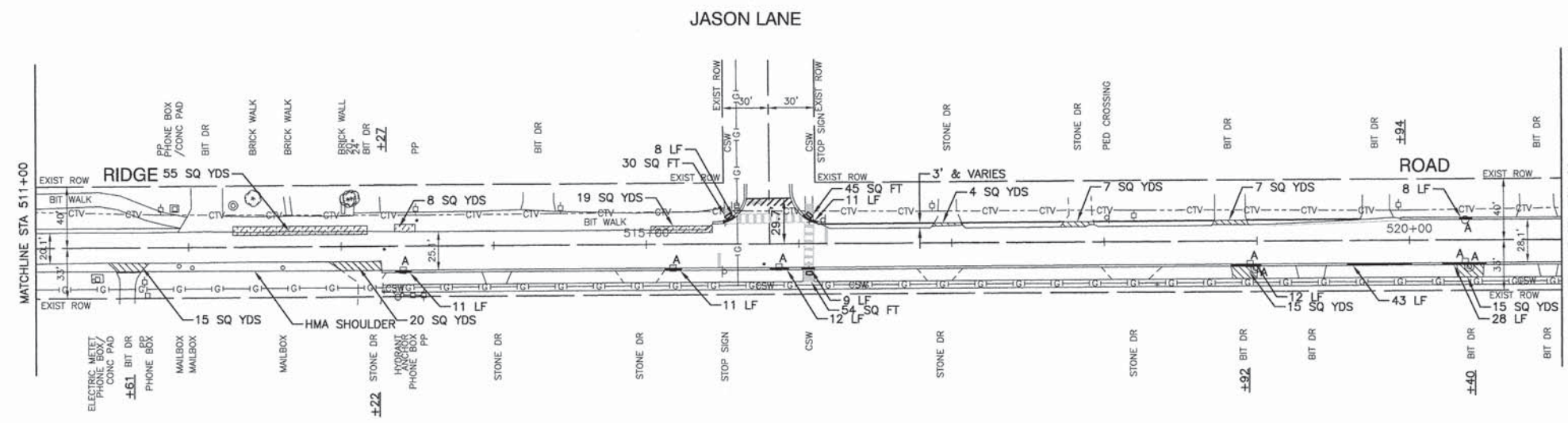
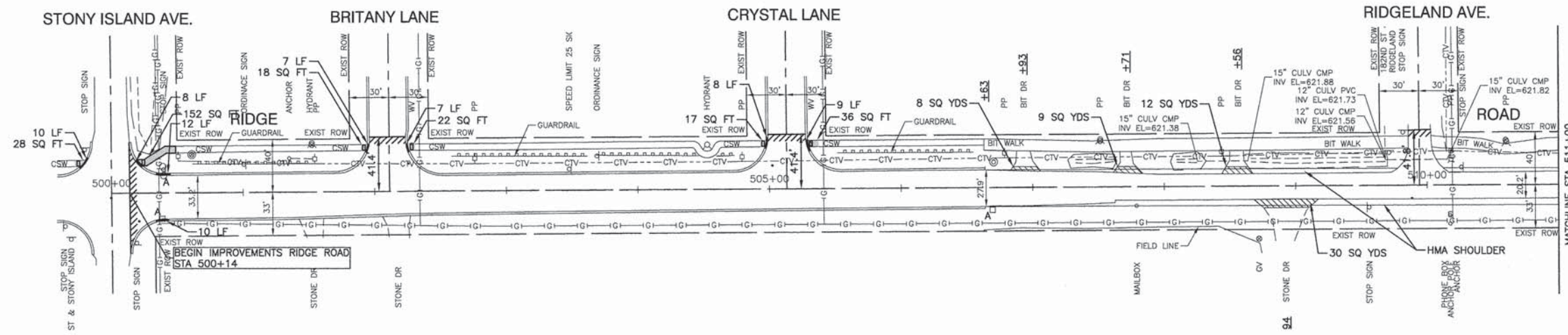
NOTE:
 CLASS D PATCHES, TYPE I, II, III & IV AT APPROXIMATE STATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

EXISTING LEGEND

- (A) HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (B) EXISTING CURB & GUTTER TO BE REMOVED AT LOCATIONS SHOWN ON PLANS OR DIRECTED BY ENGINEER
- (C) EXISTING HOT-MIX ASPHALT PAVEMENT
- (D) EXISTING PCC SIDEWALK TO BE REMOVED AT LOCATIONS SHOWN ON PLANS OR DIRECTED BY ENGINEER
- (E) EXISTING HMA SIDEWALK

PROPOSED LEGEND

- (1) HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (3) CLASS D PATCH, 7" AT LOCATIONS SHOWN ON PLANS OR DIRECTED BY ENGINEER
- (4) PROPOSED CURB AND GUTTER TO BE INSTALLED AT LOCATIONS SHOWN ON PLAN OR DIRECTED BY ENGINEER (IN KIND)
- (5) PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5" (REPLACEMENT AT LOCATIONS DIRECTED BY THE ENGINEER)
- (6) PROPOSED HMA SHOULDER, 8"
- (7) SUBBASE GRANULAR MATERIAL, TYPE C, 4"



LEGEND

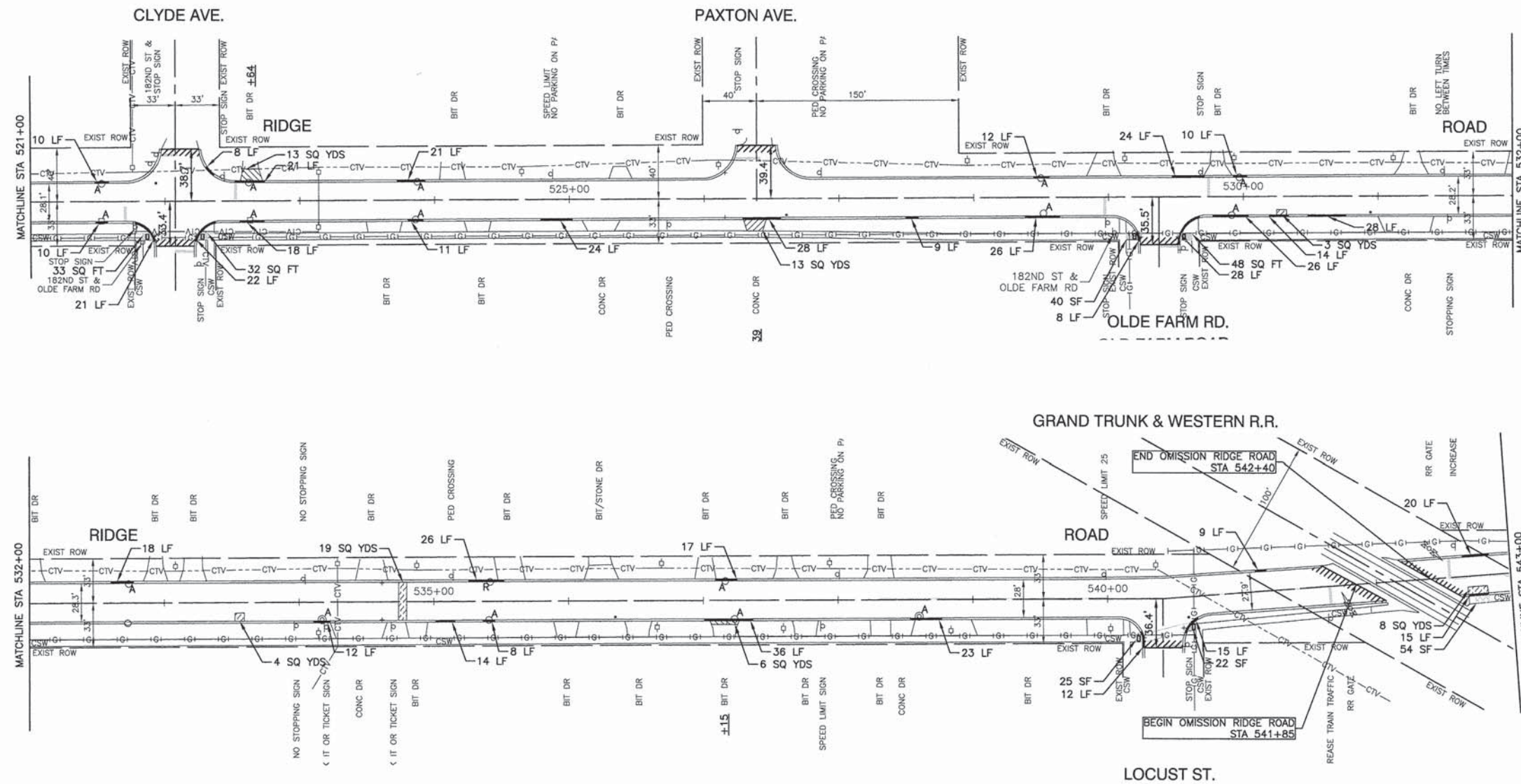
	CLASS D PATCHES
	HMA DRIVEWAY REMOVAL AND REPLACEMENT
	CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT
	CONCRETE SIDEWALK REMOVAL AND REPLACEMENT
	BUTT JOINTS
	CURB REMOVAL
	TREE REMOVAL
	"R" STRUCTURE TO BE REMOVED AND REPLACED
	"A" STRUCTURE TO BE ADJUSTED
	"A-FL" STRUCTURE TO BE ADJUSTED WITH NEW FRAME AND LID
	DETECTOR LOOP TO BE REPLACED
	DETECTABLE WARNINGS (8 S.F. UNLESS NOTED)

FILE NAME = 13636-PLAN-01 - IDOT P01	USER NAME =	DESIGNED - MN	REVISED -
		CHECKED - JP	REVISED -
	PLOT SCALE =	DRAWN - ACAD	REVISED -
	PLOT DATE = 01-13-14	CHECKED - ACAD	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAU 1621 (RIDGE ROAD) STONY ISLAND AVENUE TO TORRENCE AVENUE PAVEMENT PLAN			
SCALE: 1"=50'	SHEET NO. 4 OF 18 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	4
CONTRACT NO. 61A47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)				



- LEGEND**
- CLASS D PATCHES
 - HMA DRIVEWAY REMOVAL AND REPLACEMENT
 - CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT
 - CONCRETE SIDEWALK REMOVAL AND REPLACEMENT
 - BUTT JOINTS
 - CURB REMOVAL
 - TREE REMOVAL
 - "R" STRUCTURE TO BE REMOVED AND REPLACED
 - "A" STRUCTURE TO BE ADJUSTED
 - "A-FL" STRUCTURE TO BE ADJUSTED WITH NEW FRAME AND LID
 - DETECTOR LOOP TO BE REPLACED
 - DETECTABLE WARNINGS (8 S.F. UNLESS NOTED)

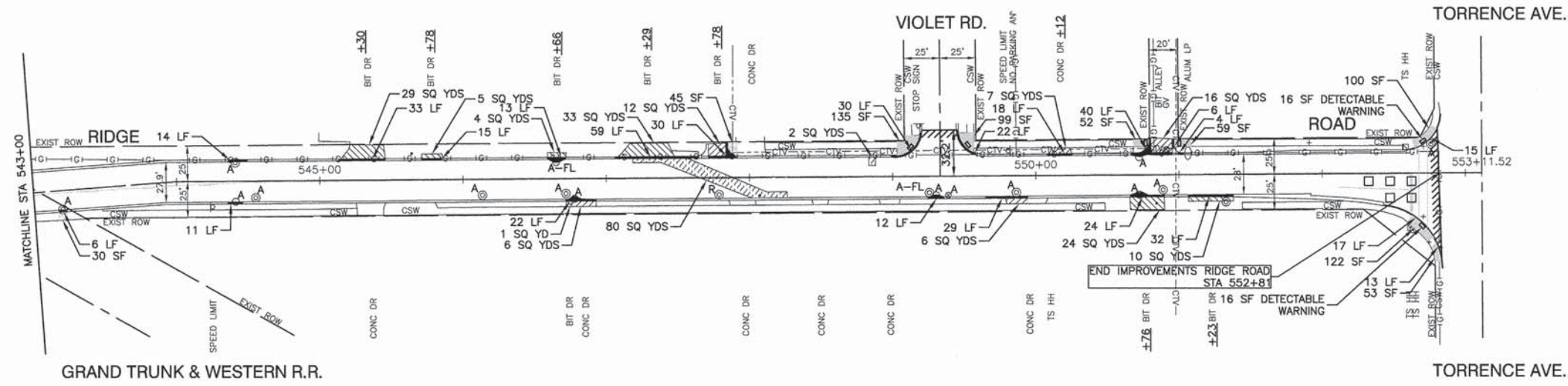
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 PLOT SCALE =
 PLOT DATE = 01-13-14

DESIGNED -- MN	REVISED --
CHECKED -- JP	REVISED --
DRAWN -- ACAD	REVISED --
CHECKED -- ACAD	REVISED --

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FAU 1621 (RIDGE)
 STONY ISLAND AVENUE TO TORRENCE AVENUE
 PAVEMENT PLAN
 SCALE: 1"=50'
 SHEET NO. 5 OF 18 SHEETS
 STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	5
CONTRACT NO. 61A47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)				



LEGEND

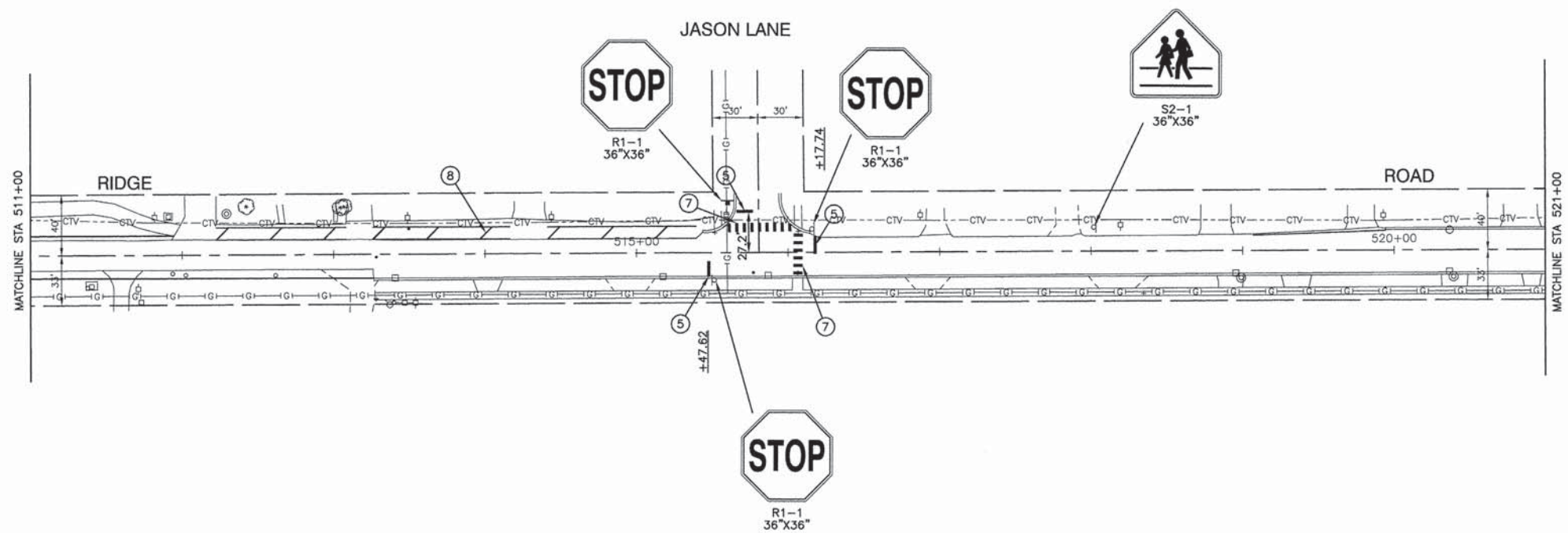
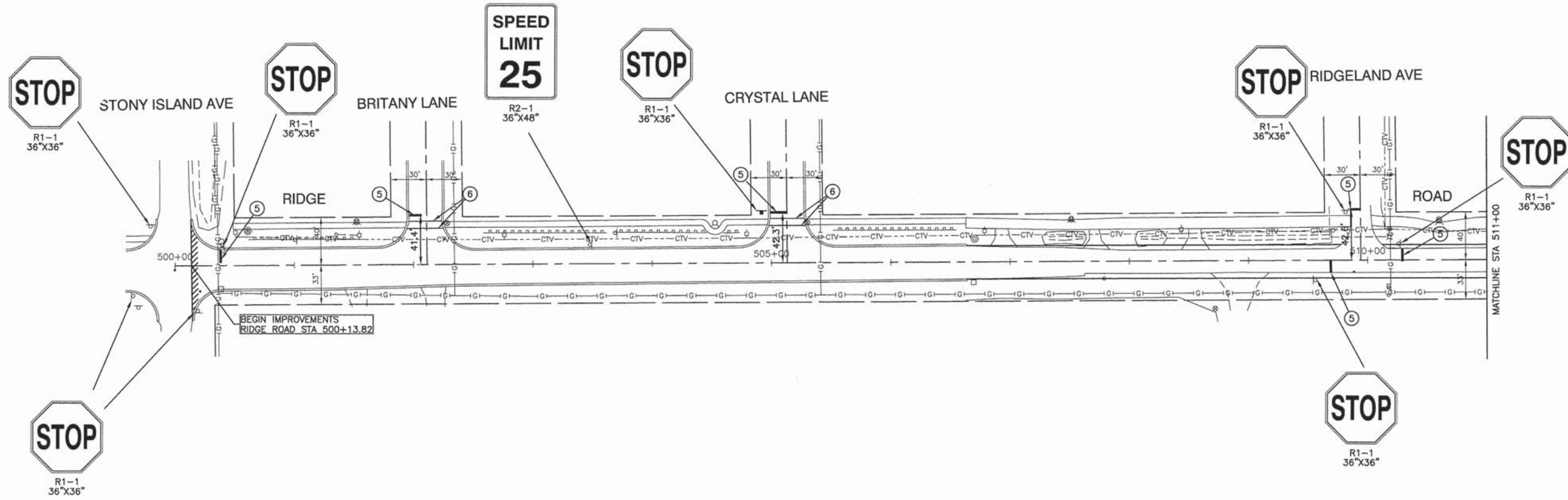
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	HMA DRIVEWAY REMOVAL AND REPLACEMENT
	CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT
	CONCRETE SIDEWALK REMOVAL AND REPLACEMENT
	BUTT JOINTS
	CURB REMOVAL
	TREE REMOVAL
"R"	STRUCTURE TO BE REMOVED AND REPLACED
"A"	STRUCTURE TO BE ADJUSTED
"A-FL"	STRUCTURE TO BE ADJUSTED WITH NEW FRAME AND LID
	DETECTOR LOOP TO BE REPLACED
	DETECTABLE WARNINGS (8 S.F. UNLESS NOTED)

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		CHECKED -- JP	REVISED --
	PLOT SCALE =	DRAWN -- ACAD	REVISED --
	PLOT DATE = 01-13-14	CHECKED -- ACAD	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAU 1621 (RIDGE ROAD) STONY ISLAND AVENUE TO TORRENCE AVENUE PAVEMENT PLAN			
SCALE: 1"=50'	SHEET NO. 6	OF 18 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	6
CONTRACT NO. 61A47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)				



PAVEMENT MARKING LEGEND

- ① WHITE LETTERS & SYMBOLS
- ② DOUBLE 4" YELLOW CENTERLINE (11" C/C)
- ③ 4" YELLOW LINE
- ④ 4" YELLOW SKIP - DASH LINE (10' LINE - 30' SPACE)
- ⑤ 24" WHITE STOP BAR
- ⑥ 6" WHITE CROSSWALK (6' C/C)
- ⑦ 12" WHITE CROSSWALK WITH 2' SPACE (6' WIDE)
- ⑧ 4" WHITE LINE

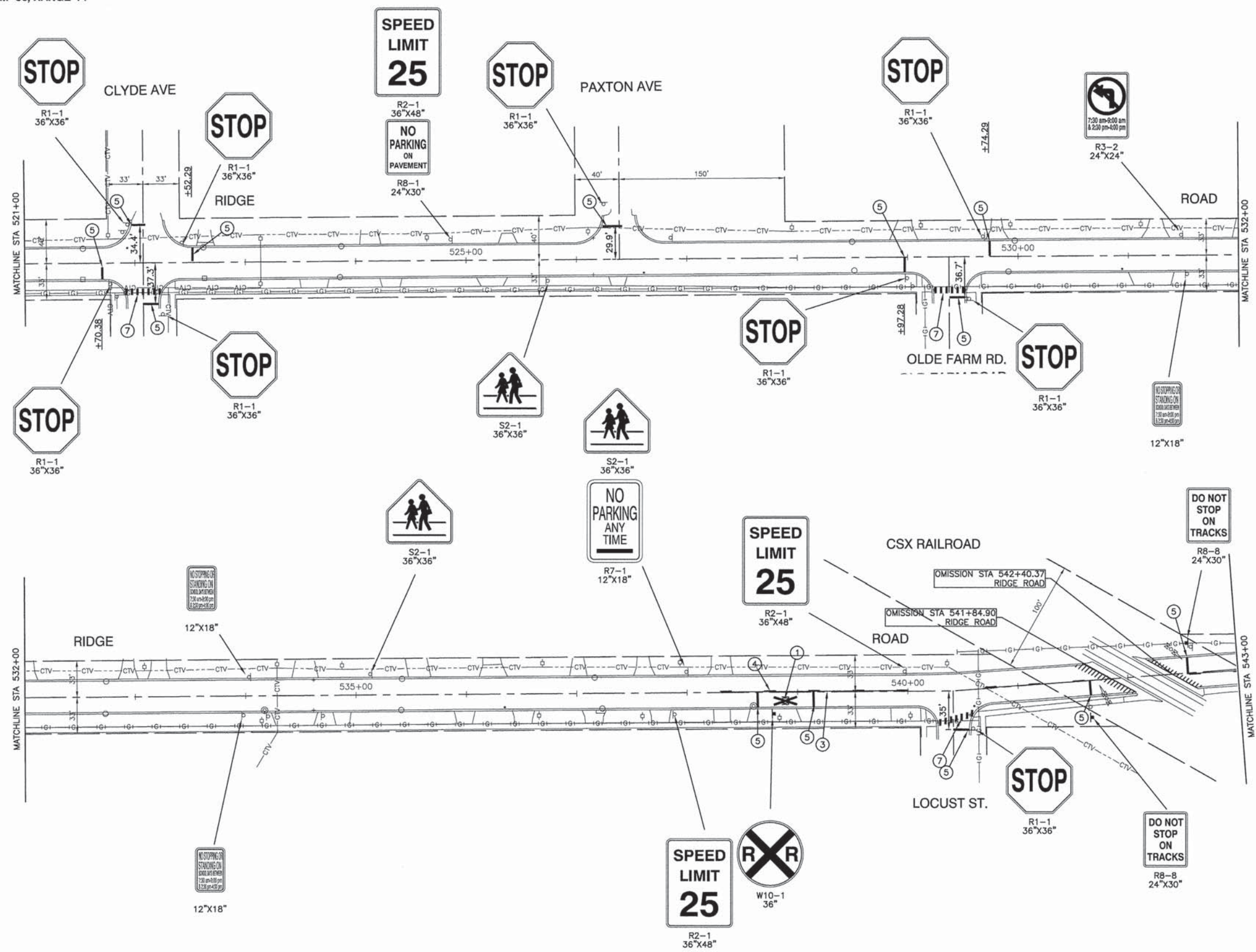
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 PLOT SCALE =
 PLOT DATE = 01-13-14

DESIGNED -- MN	REVISD --
CHECKED -- JP	REVISD --
DRAWN -- ACAD	REVISD --
CHECKED -- ACAD	REVISD --

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FAU 1621 (RIDGE ROAD)
 STONY ISLAND AVENUE TO TORRENCE AVENUE
 PAVEMENT MARKING
 SCALE: 1"=50'
 SHEET NO. 7 OF 18 SHEETS
 STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	7
CONTRACT NO. 61A47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)				



- PAVEMENT MARKING LEGEND**
- ① WHITE LETTERS & SYMBOLS
 - ② DOUBLE 4" YELLOW CENTERLINE (11" C/C)
 - ③ 4" YELLOW LINE
 - ④ 4" YELLOW SKIP - DASH LINE (10' LINE - 30' SPACE)
 - ⑤ 24" WHITE STOP BAR
 - ⑥ 6" WHITE CROSSWALK (6' C/C)
 - ⑦ 12" WHITE CROSSWALK WITH 2' SPACE (6' WIDE)
 - ⑧ 4" WHITE LINE

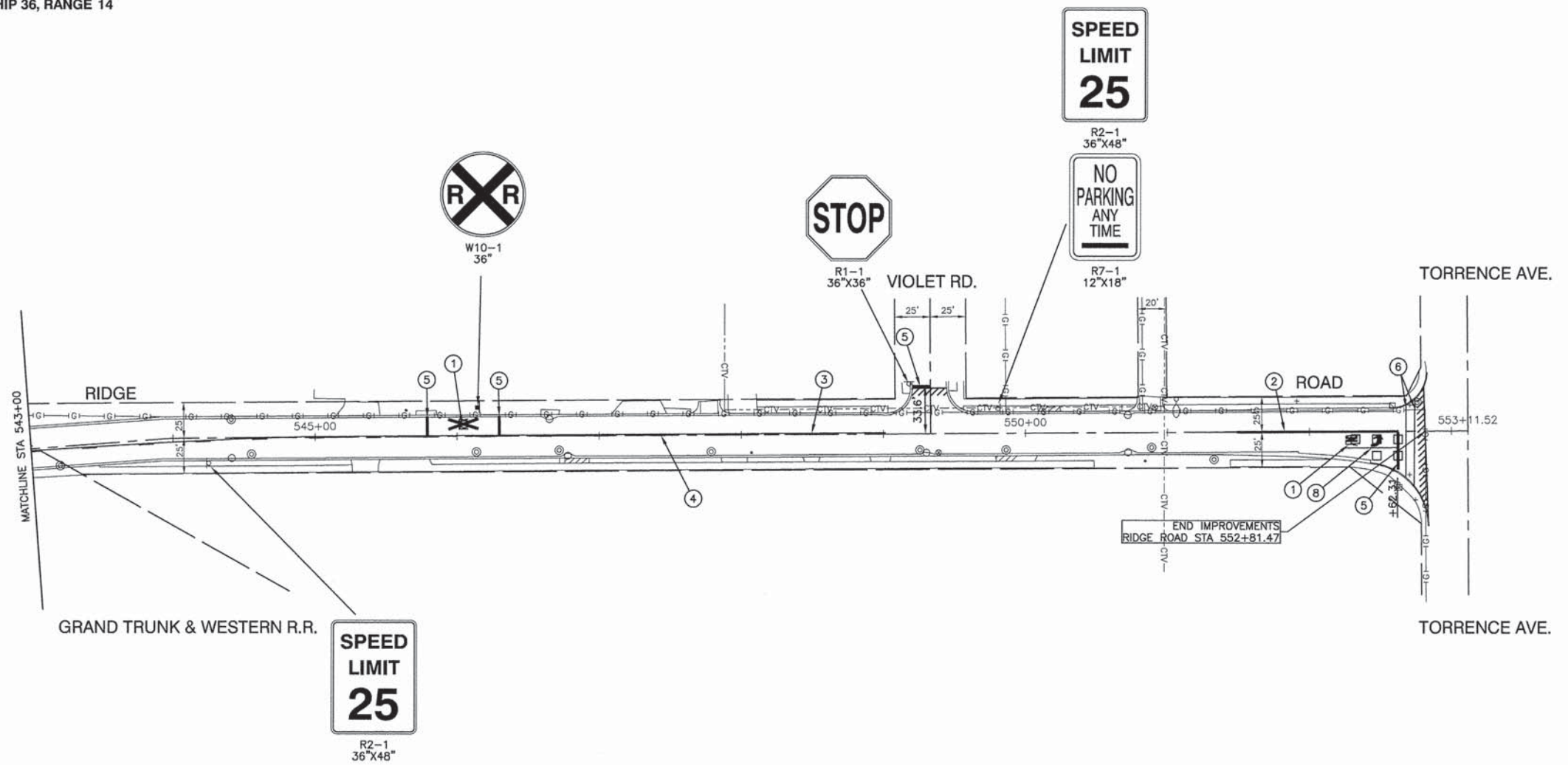
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 PLOT SCALE =
 PLOT DATE = 01-13-14

USER NAME =	DESIGNED - MN	REVISED -
CHECKED - JP	REVISOR -	
DRAWN - ACAD	REVISOR -	
CHECKED - ACAD	REVISOR -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FAU 1621 (RIDGE ROAD)
 STONY ISLAND AVENUE TO TORRENCE AVENUE
 PAVEMENT MARKING
 SCALE: 1"=50'
 SHEET NO. 8 OF 18 SHEETS
 STA. TO STA.

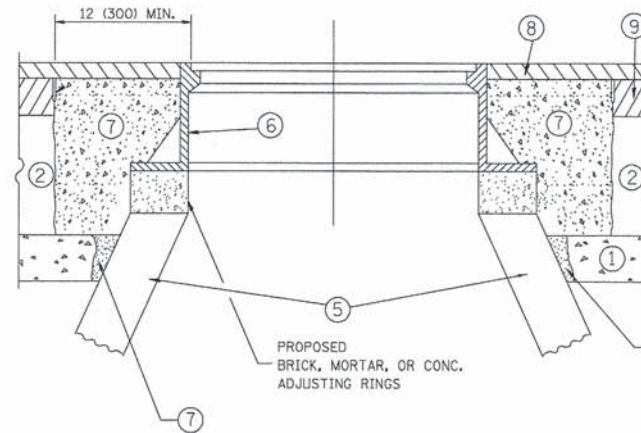
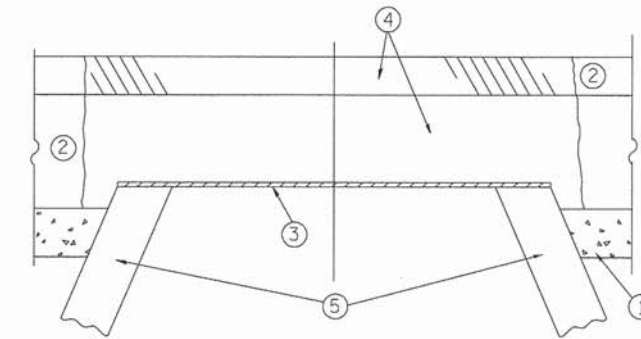
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	8
CONTRACT NO. 61A47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)				



PAVEMENT MARKING LEGEND

- ① WHITE LETTERS & SYMBOLS
- ② DOUBLE 4" YELLOW CENTERLINE (11" C/C)
- ③ 4" YELLOW LINE
- ④ 4" YELLOW SKIP - DASH LINE (10' LINE - 30' SPACE)
- ⑤ 24" WHITE STOP BAR
- ⑥ 6" WHITE CROSSWALK (6' C/C)
- ⑦ 12" WHITE CROSSWALK WITH 2' SPACE (6' WIDE)
- ⑧ 4" WHITE LINE

FILE NAME = 13636-PMKG-01 - PMKG P01 (8) PLOT SCALE = PLOT DATE = 01-13-14	USER NAME = DESIGNED -- MN CHECKED -- JP DRAWN -- ACAD CHECKED -- ACAD	REVISED -- REVISED -- REVISED -- REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 1621 (RIDGE ROAD) STONY ISLAND AVENUE TO TORRENCE AVENUE PAVEMENT MARKING	SCALE: 1"=50' SHEET NO. 9 OF 18 SHEETS STA. TO STA.	<table border="1" style="font-size: 8px;"> <tr> <th>F.A.U. RTE.</th> <th>SECTION</th> <th>COUNTY</th> <th>TOTAL SHEETS</th> <th>SHEET NO.</th> </tr> <tr> <td>1621</td> <td>13-00173-00-RS</td> <td>COOK</td> <td>18</td> <td>9</td> </tr> </table> CONTRACT NO. 61A47 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	1621	13-00173-00-RS	COOK	18	9
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.												
1621	13-00173-00-RS	COOK	18	9												



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07
R. BORO	03/09/11
R. BORO	12/06/11

ILLINOIS DEPARTMENT OF TRANSPORTATION
**DETAILS FOR
 FRAMES AND LIDS ADJUSTMENT
 WITH MILLING**

SCALE: VERT. NONE
 HORIZ.

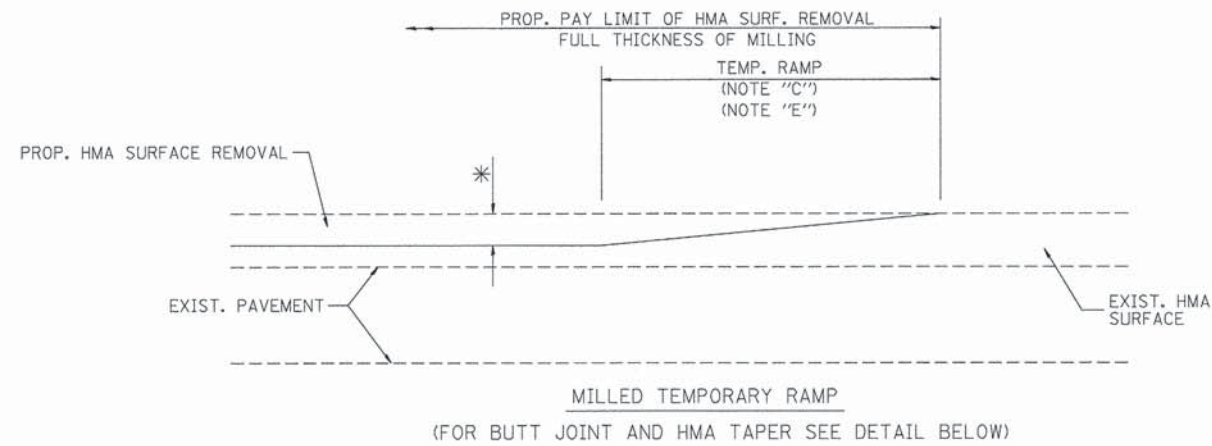
DRAWN BY
 CHECKED BY

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	10
CONTRACT NO. 61A47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)				

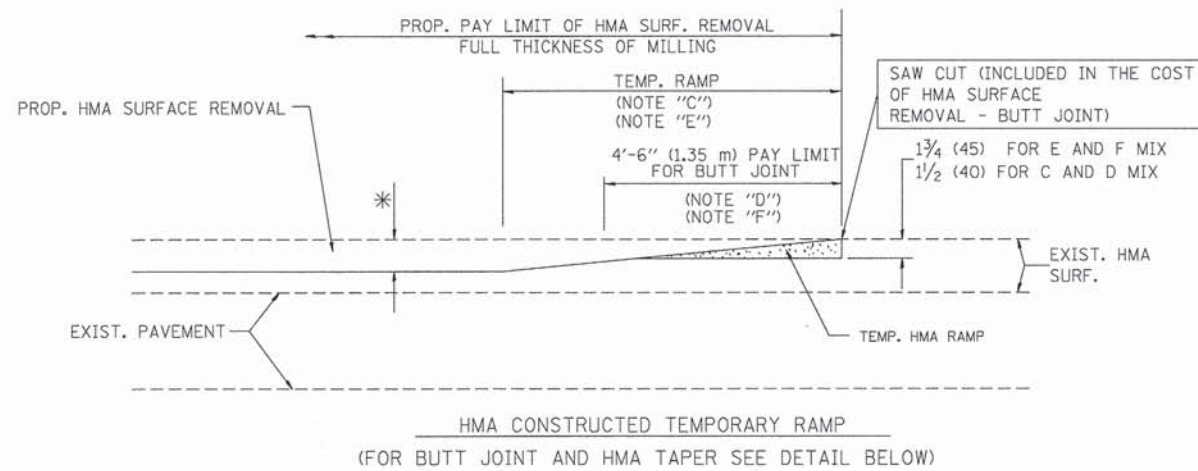
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
 FRAMES AND LIDS ADJUSTMENT WITH MILLING

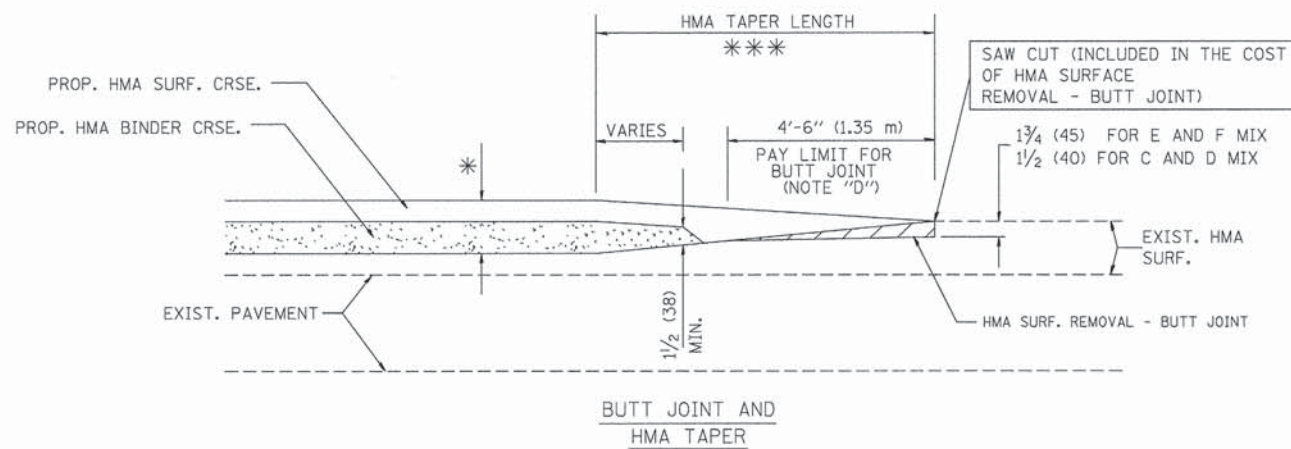
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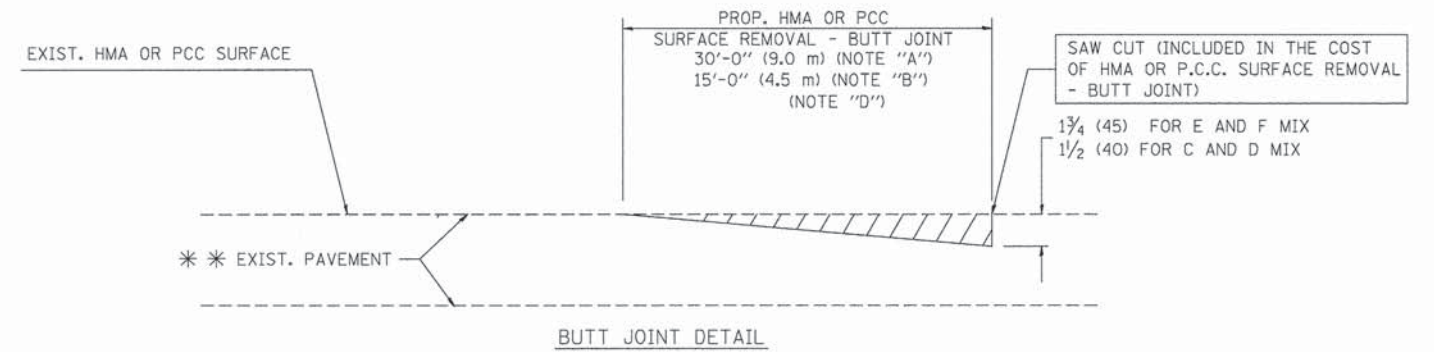
OPTION 1



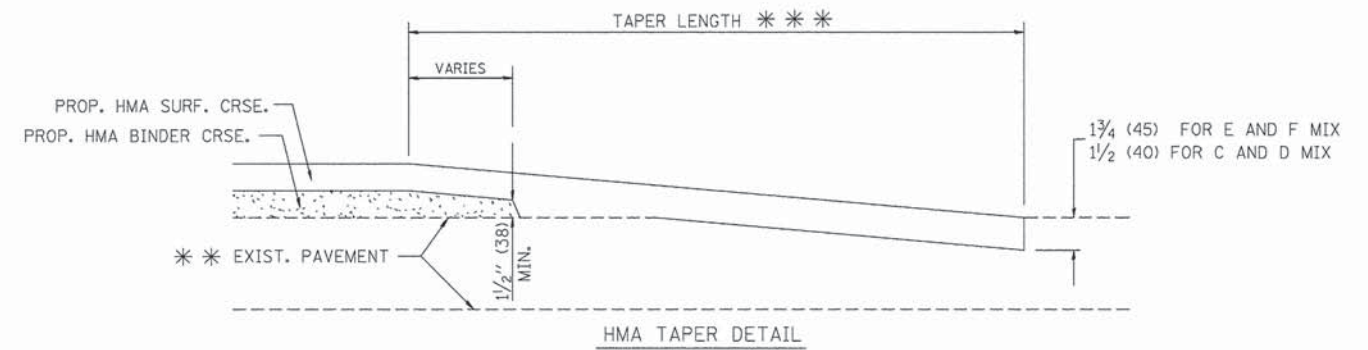
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =
W:\diststd\22x34\bd32.dgn

USER NAME = gaglianobt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90

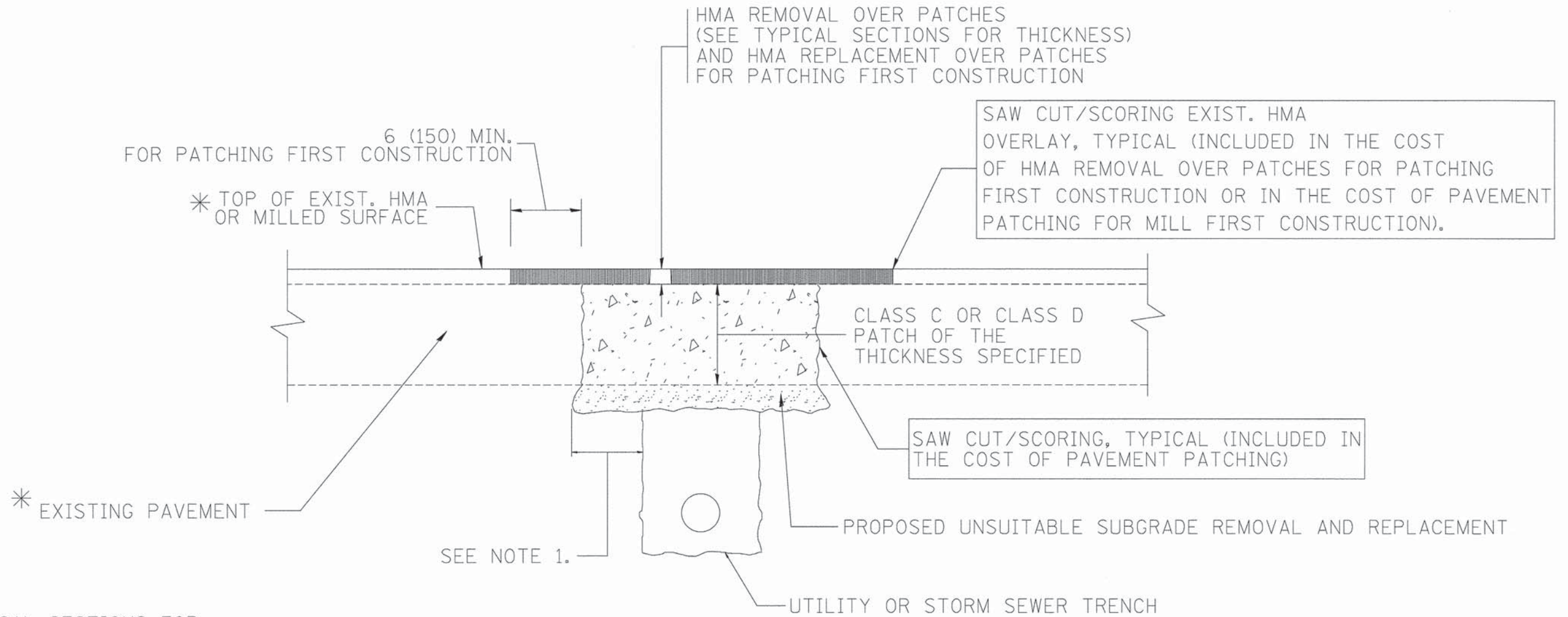
REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
BUTT JOINT AND HMA TAPER
DETAILS

SCALE: NONE SHEET NO. 11 OF 18 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	11
BD400-05 BD32			CONTRACT NO. 61A47	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\distatd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.000' / 1".	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT		F.A.U. RTE. 1621	SECTION 13-00173-00-RS	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 12
SCALE: NONE		SHEET NO. 12 OF 18 SHEETS		STA.	TO STA.	
		BD400-04 (BD-22)		CONTRACT NO. 61A47		
		FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-4003(314)		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

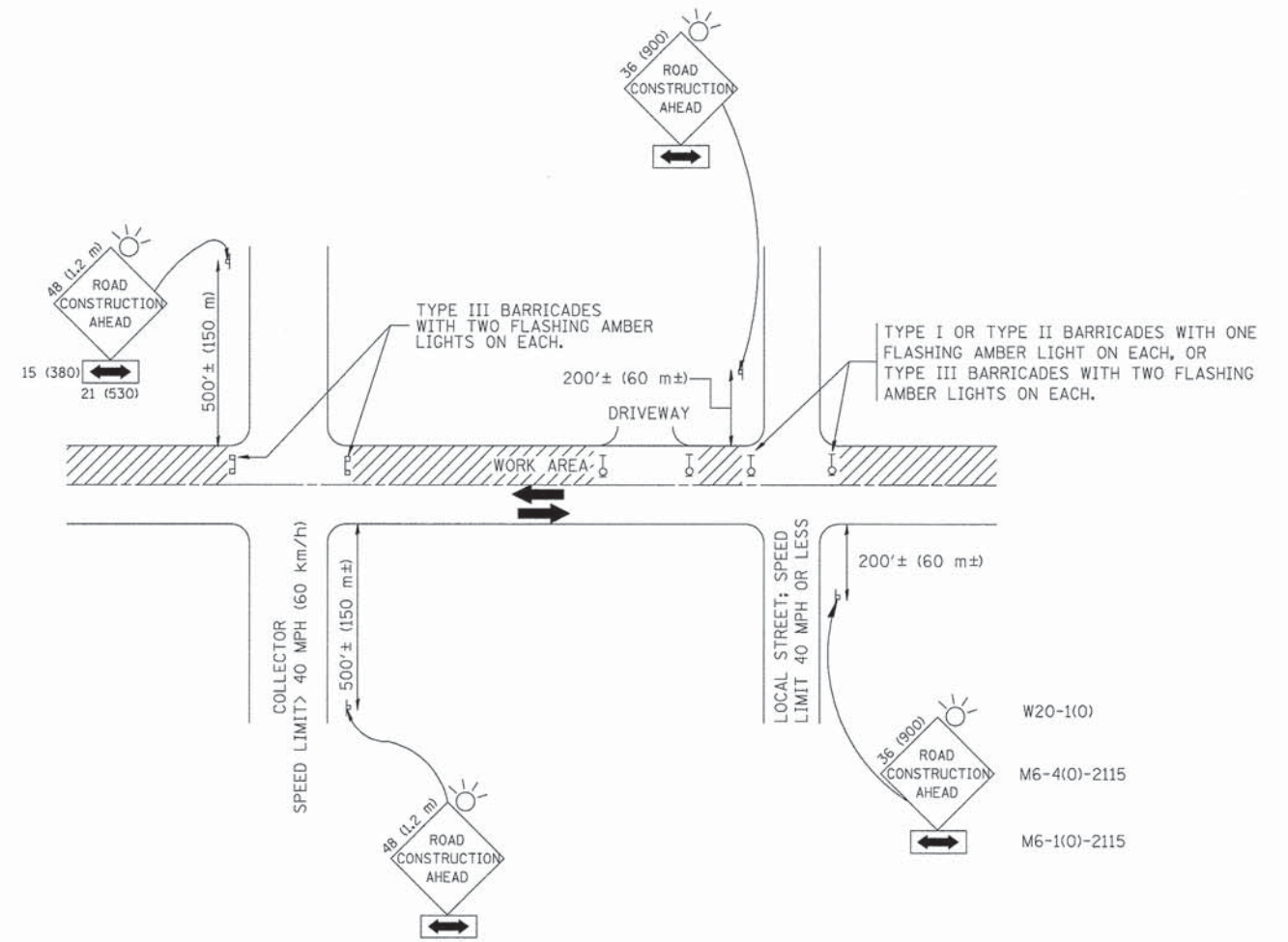
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96
os\pw\work\pwsdot\drivakosgn\d0128315\bd24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 58.800' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	
SCALE: NONE	SHEET NO. 13 OF 18 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	13
BD600-06 (BD-24)		CONTRACT NO. 61A47		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-4003(314)		



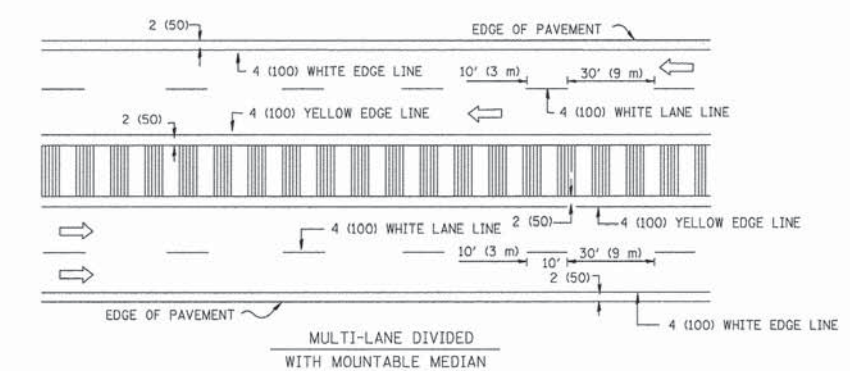
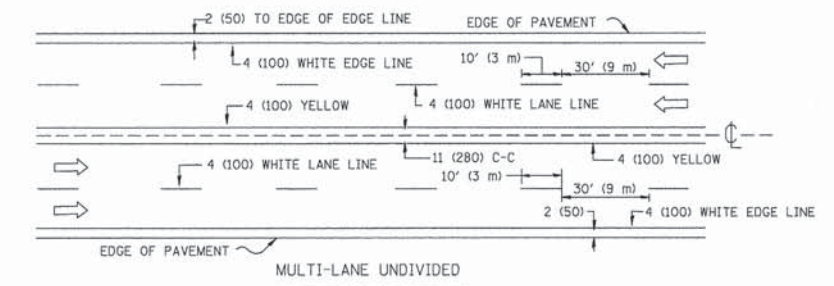
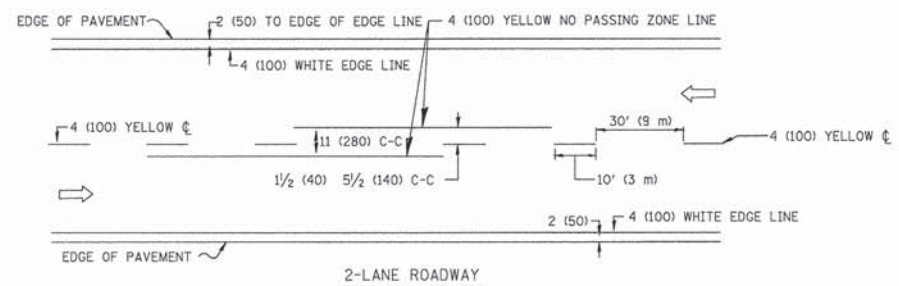
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

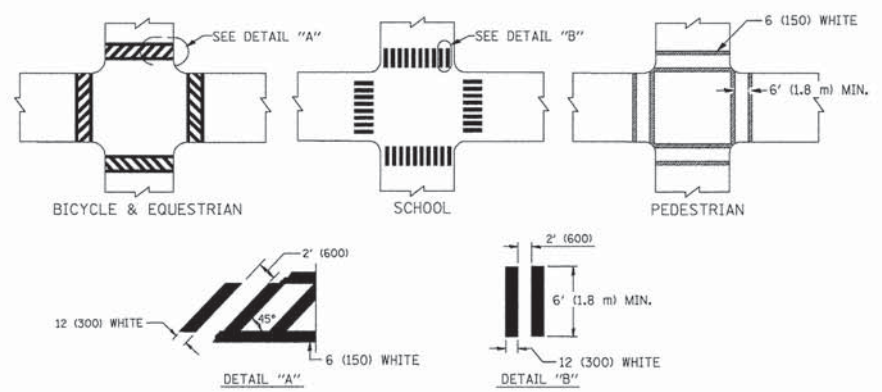
All dimensions are in millimeters (Inches) unless otherwise shown.

FILE NAME = W:\diststd\22x34\to10.dgn	USER NAME = geglennob	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			F.A.U. RTE. 1621	SECTION 13-00173-00-RS	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 14
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - A. HOUSEH 03-06-96		SCALE: NONE	SHEET NO. 14 OF 18 SHEETS	STA.	TO STA.	TC-10		CONTRACT NO. 61A47	
	PLOT DATE = 1/4/2006	DATE - 06-89	REVISED - A. HOUSEH 10-15-96									
					REVISED - T. RAMMACHER 01-06-00	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)						

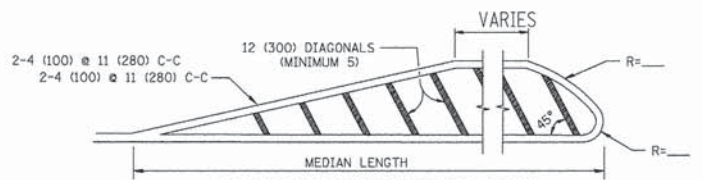
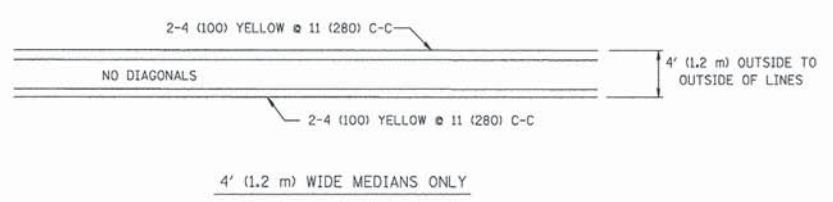


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

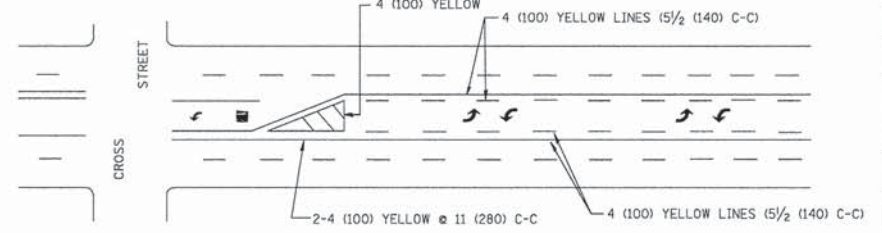


TYPICAL CROSSWALK MARKING

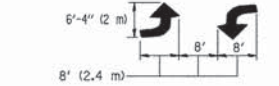


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

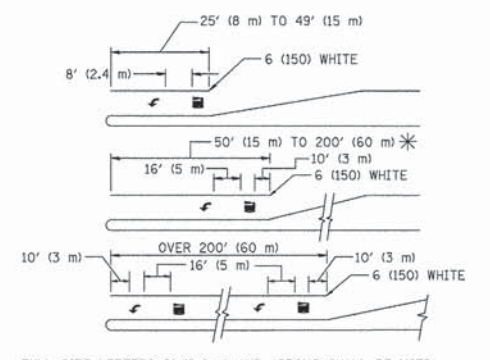


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

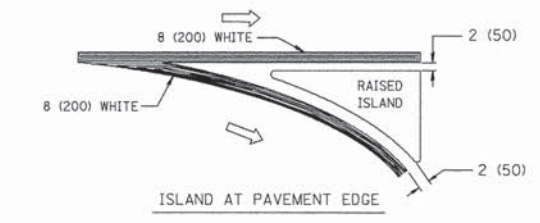
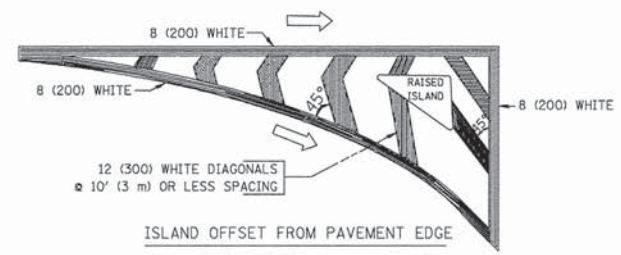
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakoagn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
al\pw_work\p\wdot\drivakoagn\d0188315\td\3.dgn		DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

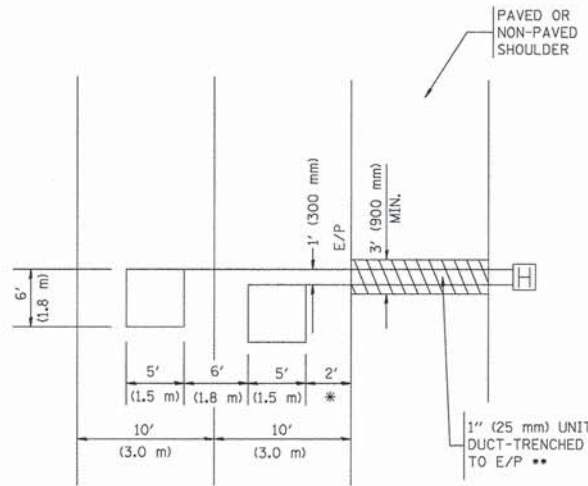
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 15 OF 18 SHEETS STA. TO STA.

FAU RTR.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	15
TC-13		CONTRACT NO. 61A47		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-4003(314)		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



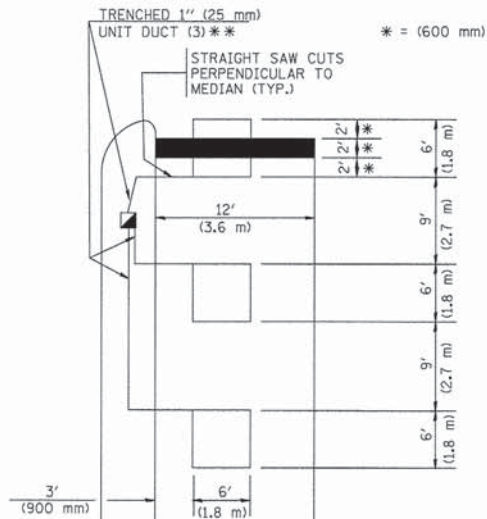
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



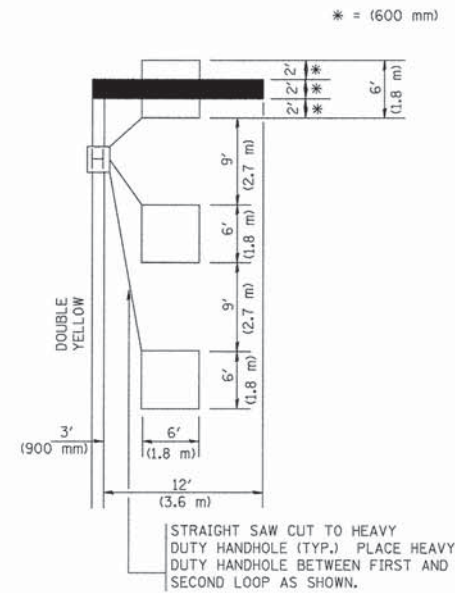
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

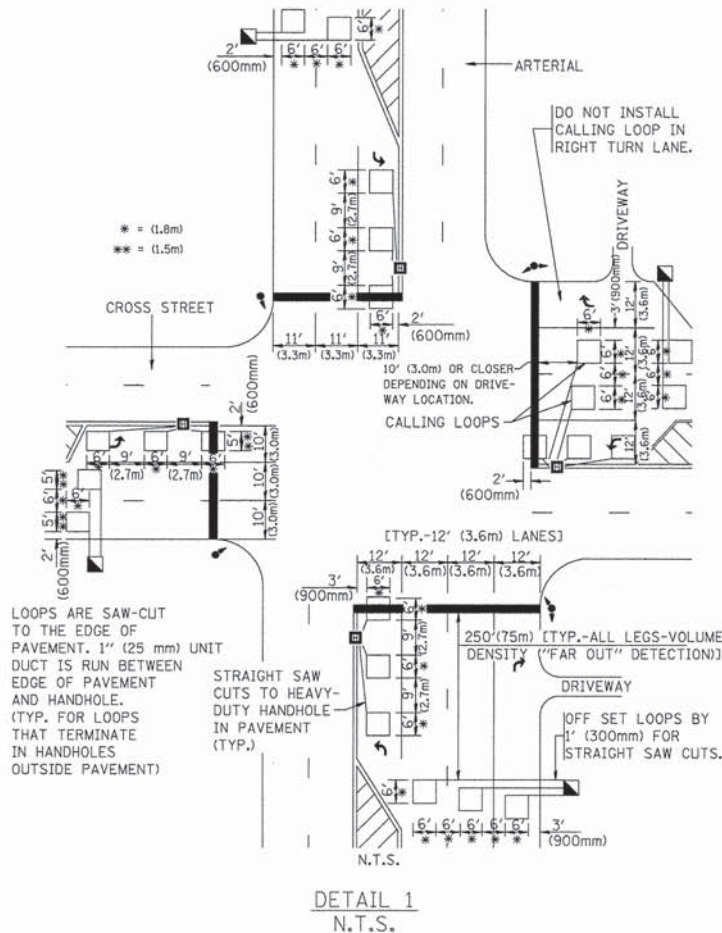
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



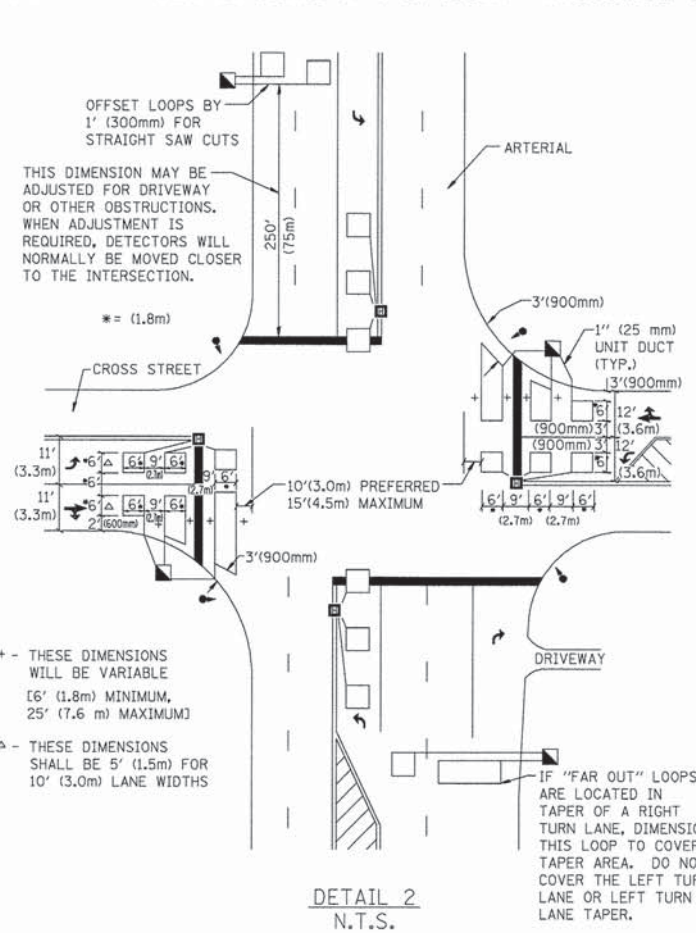
* = (1.8m)
** = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



OFFSET LOOPS BY 1' (300mm) FOR STRAIGHT SAW CUTS
THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
N.T.S.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\dist\dtd\22x34\ts07.dgn

USER NAME = gaglianob
PLOT SCALE = 58.0000" / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

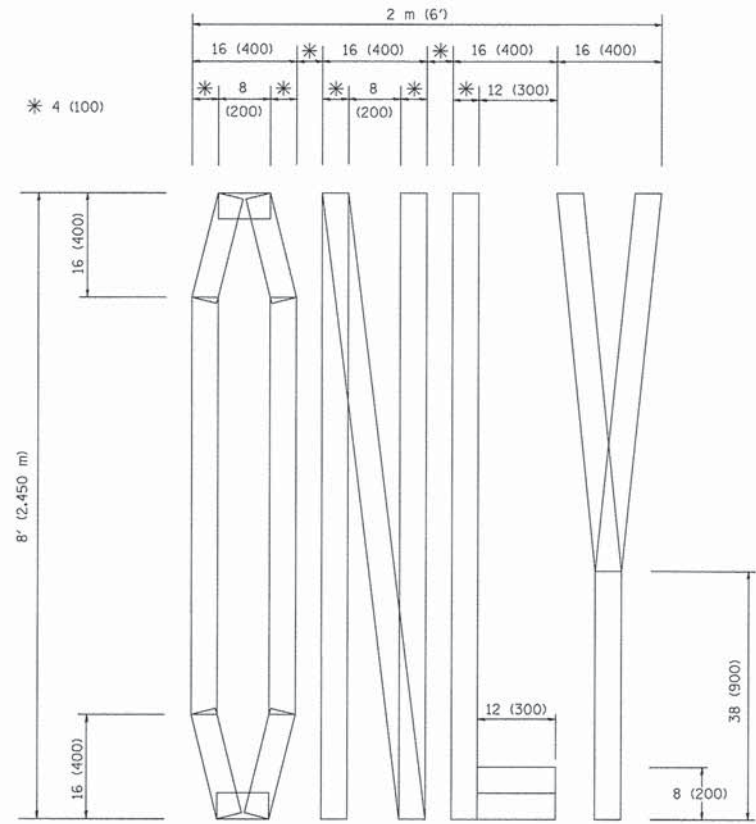
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

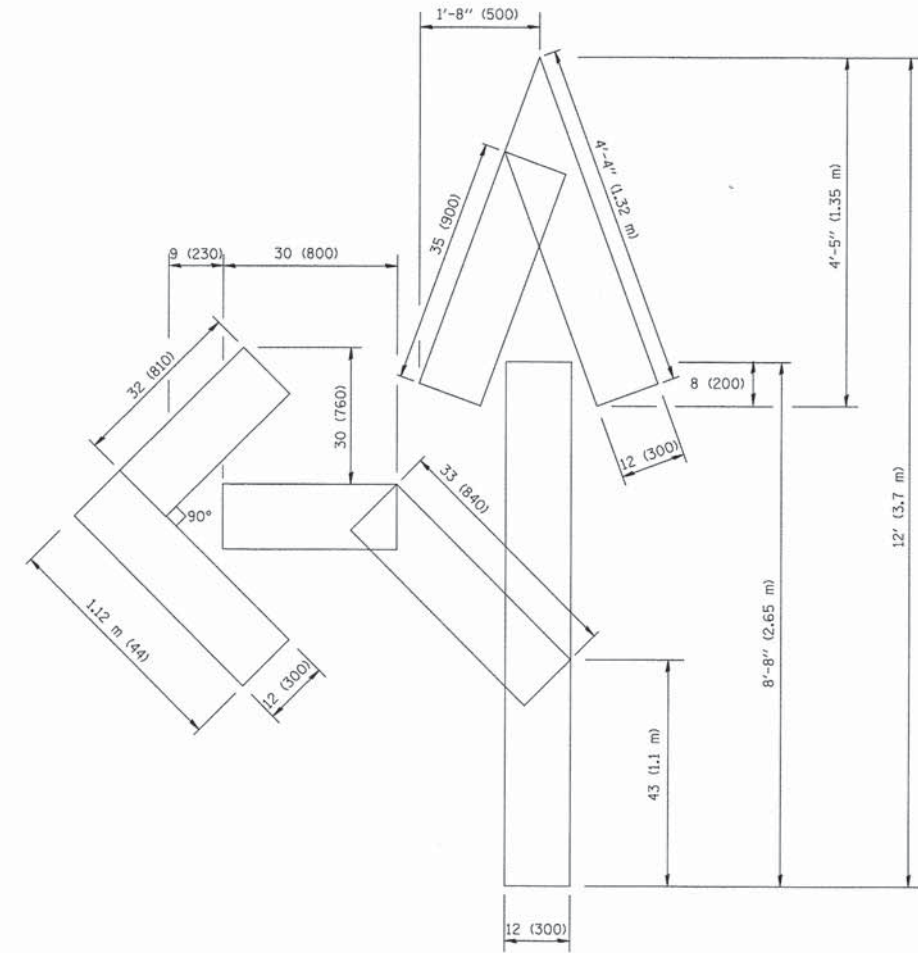
DISTRICT ONE - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 16 OF 18 SHEETS STA. TO STA.

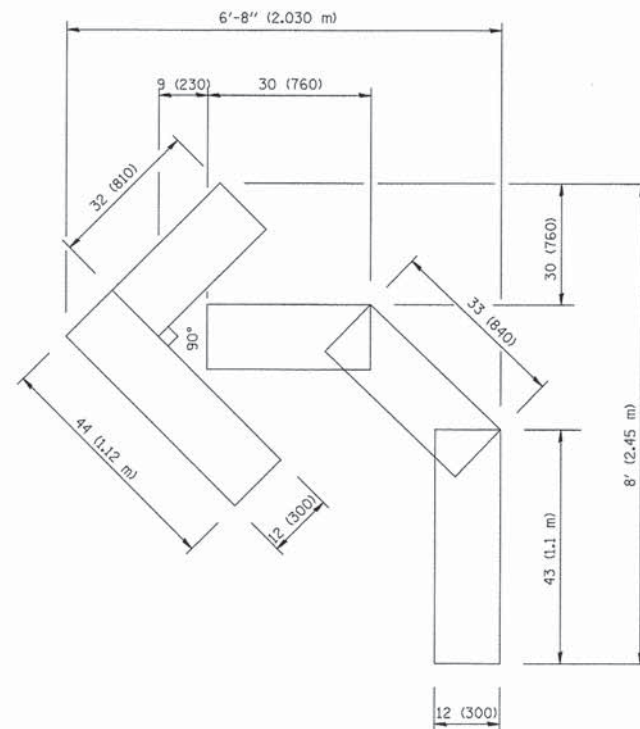
FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	16
TS-07			CONTRACT NO. 61A47	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - PAVEMENT MARKING LETTERS AND
 SYMBOLS FOR TRAFFIC STAGING

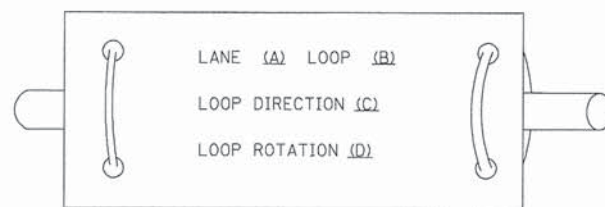
SCALE: NONE SHEET NO. 17 OF 18 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	17
TC-16			CONTRACT NO. 61A47	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)				

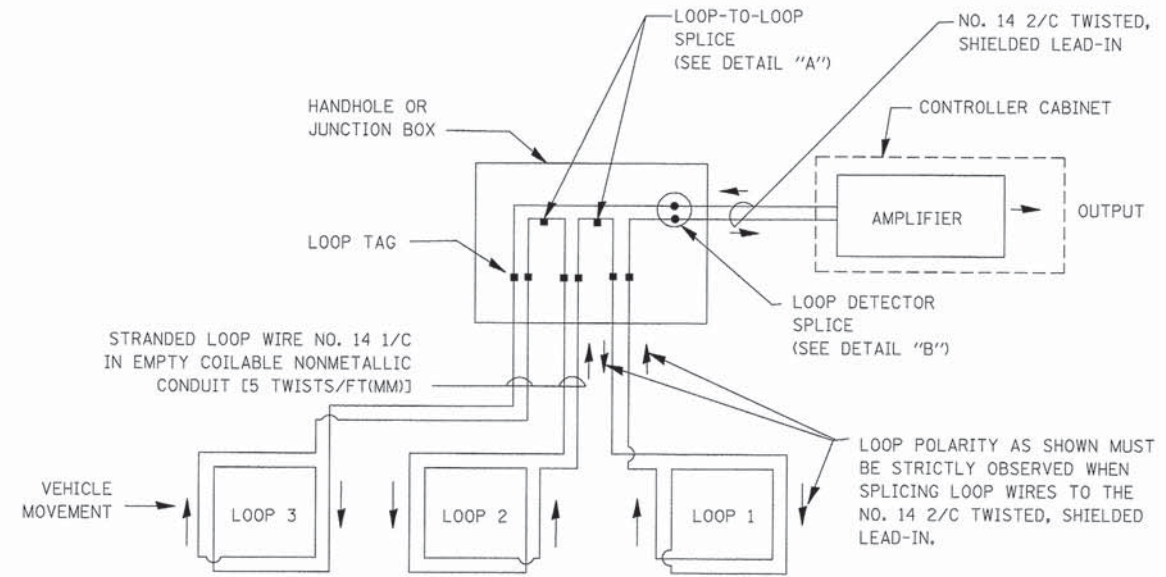
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

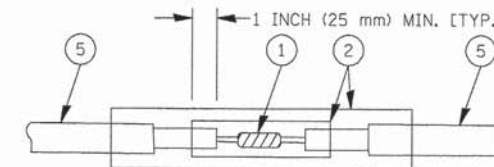


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

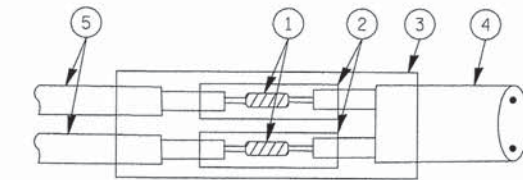


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

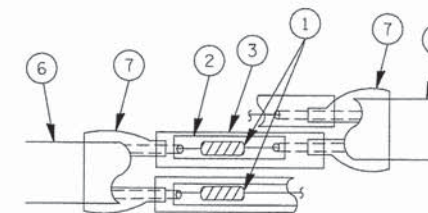


DETAIL "A" LOOP-TO-LOOP SPLICE

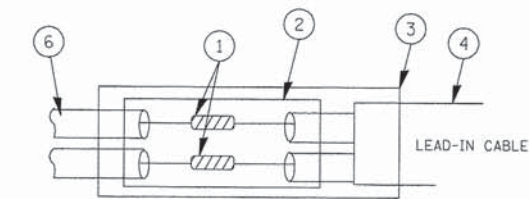


DETAIL "B" LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A" LOOP-TO-LOOP SPLICE



DETAIL "B" LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PRE-FORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1621	13-00173-00-RS	COOK	18	18
TS-05			CONTRACT NO. 61A47	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(314)				

FILE NAME =	USER NAME = bouard1	DESIGNED - DAD	REVISED -
et\pw_wor\k\PWIDOT\BAUERDL\d0108315\ts05.dgn		DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE - 10-28-09	REVISED -

SCALE: NONE SHEET NO. 18 OF 18 SHEETS STA. TO STA.