

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	01
		ILLINOIS	CONTRACT NO. 61L65	

06-13-2025 LETTING ITEM 012

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
PLANS FOR PROPOSED FEDERAL AID HIGHWAY  
FAU 2770 (SHERMER ROAD)

FROM FAU ROUTE 2766 (HARLEM AVE)  
TO FAU ROUTE 1312 IL 58 (GOLF RD)

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF STATE STANDARDS, SEE SHEET NO. 2

RESURFACING

SECTION: 24-00114-00-RS

PROJECT: 38DJ (646)

VILLAGE OF MORTON GROVE  
COOK COUNTY

JOB: C-91-147-25



LOCATION OF SECTION INDICATED THIS -

AGENCY RESPONSIBLE FOR LETTING

APPROVED: *[Signature]* 03/04 2025  
VILLAGE OF MORTON GROVE, VILLAGE ENGINEER

PASSED: *[Signature]* APRIL 9 2025  
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASED FOR BID  
BASED ON LIMITED  
REVIEW: *[Signature]* APRIL 9 2025  
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

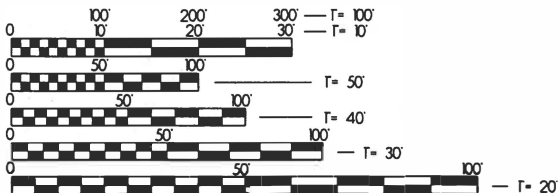
REGISTERED P.E., STATE OF ILLINOIS

EXPIRATION DATE

TRAFFIC DATA - SHERMER RD

ADT (2022) = 6,400  
POSTED SPEED 30 MPH  
DESIGN SPEED 35 MPH

FUNCTIONAL CLASSIFICATION:  
MAJOR COLLECTOR (URBAN)

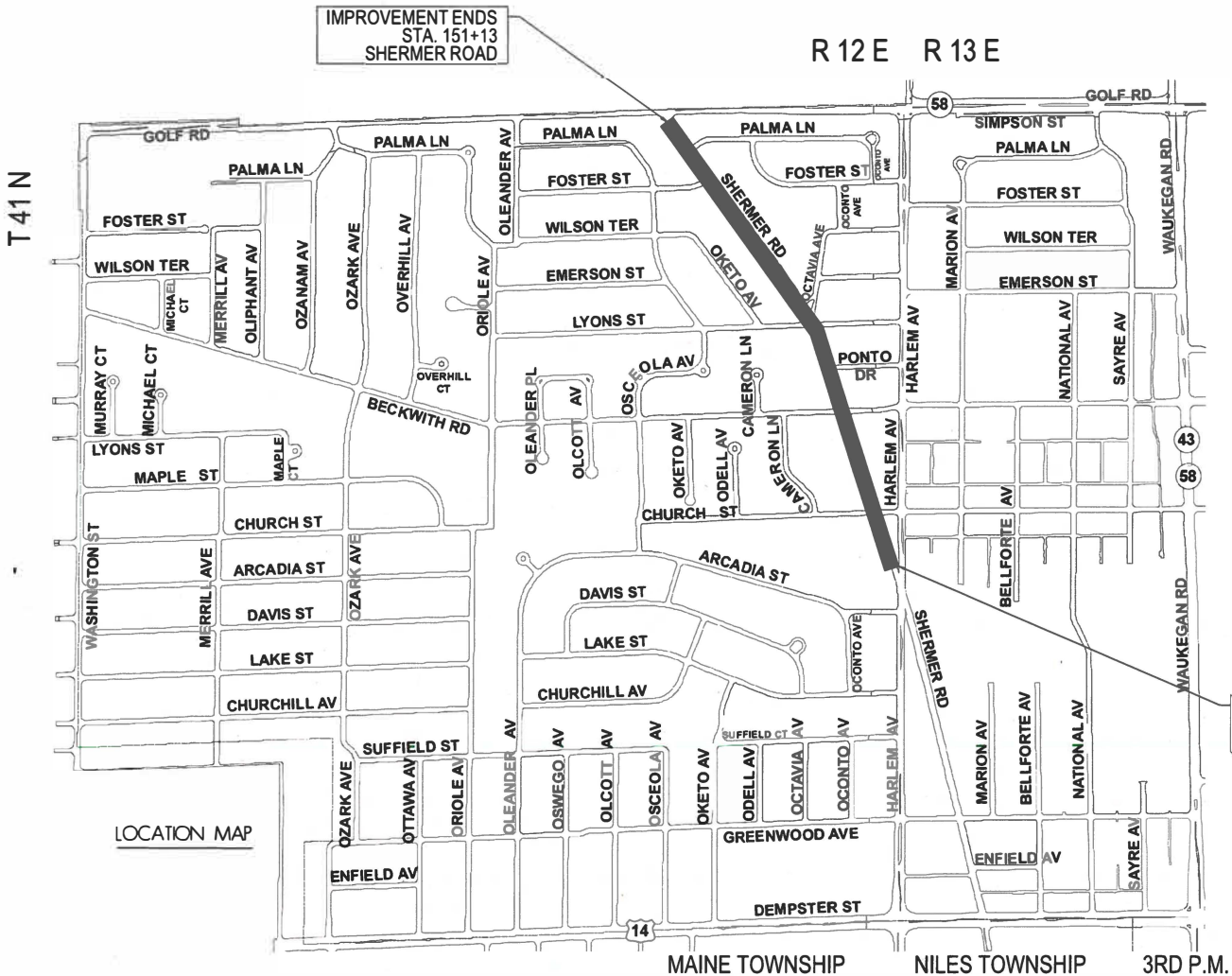


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: CHRIS COMIN  
PROJECT MANAGER:

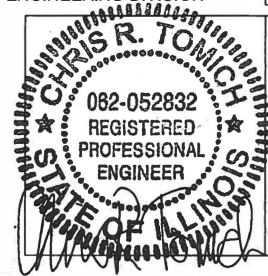
CONTRACT NO. 61L65



LOCATION MAP

GROSS LENGTH = 3,201 FT. = 0.61 MILE  
NET LENGTH = 3,201 FT. = 0.61 MILE

PLANS PREPARED BY THE  
VILLAGE OF MORTON GROVE  
ENGINEERING DIVISION



FEDERAL AID PROGRAM ENGINEER, CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

ILLINOIS	FED. AID PROJECT
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MODEL: S:\MODEL\MANES  
FILE NAME: ST11LS



USER NAME	= \$USERS
PLOT SCALE	= \$SCALES
PLOT DATE	= \$DATES

DESIGNED	-	JG/MG/CRT
DRAWN	-	JG
CHECKED	-	CRT
DATE	-	04-02-2025

REVISED	-	_____
REVISED	-	_____
REVISED	-	_____
REVISED	-	_____

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
SUMMARY OF QUANTITIES

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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F.A.U. RTE. 2770	SECTION 24-00114-00-RS	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 03
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				

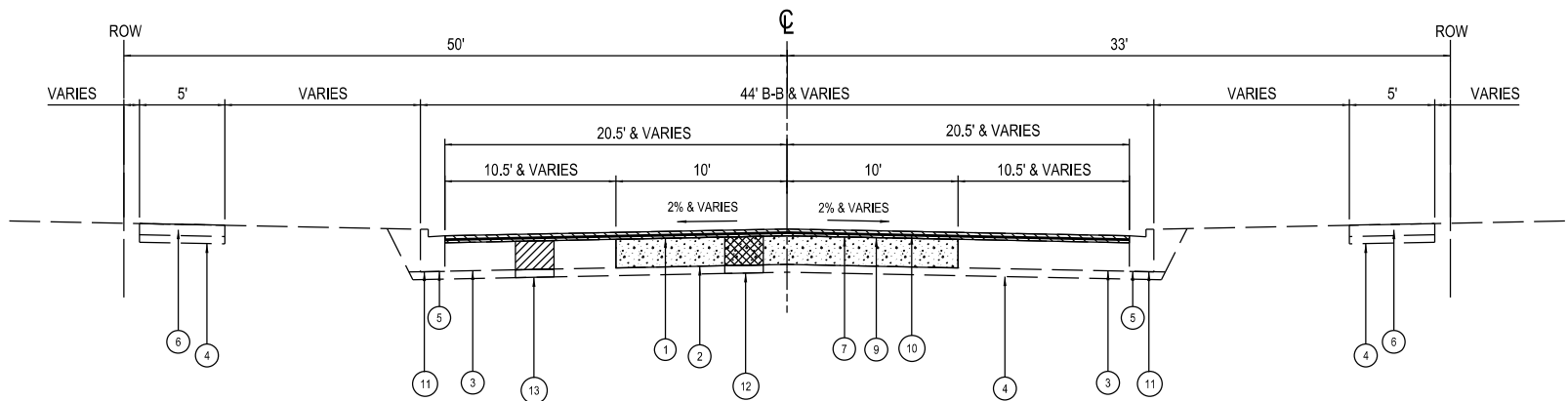
SPECIALTY ITEM	CODE NO	ITEM	UNIT	TOTAL	ROADWAY 80% FED / 20% LOCAL 0005	ROADWAY 100% LOCAL 0005
	20101100	TREE TRUNK PROTECTION	EACH	15	15	
	20200100	EARTH EXCAVATION	CU YD	10	10	
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	157	157	
	21101615	TOPSOIL, FURNISH AND PLACE, 4"	SQ YD	1186	1186	
	25200110	SODDING, SALT TOLERANT	SQ YD	1186	1186	
	25200200	SUPPLEMENTAL WATERING	UNIT	65	65	
	28000510	INLET FILTERS	EACH	24	24	
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	40	40	
	31101180	SUBBASE GRANULAR MATERIAL, TYPE B 2"	SQ YD	722	722	
	31101600	SUBBASE GRANULAR MATERIAL, TYPE B 8"	SQ YD	60	60	
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	7265	7265	
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	9603	9603	
	40600400	MIXTURE FOR CRACKS, JONTS, AND FLANGEWAYS	TON	30	30	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	217	217	
	40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	660	660	
	40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	1337	1337	
	42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	50	50	

\* SPECIALTY ITEM

SPECIALTY ITEM	CODE NO	ITEM	UNIT	TOTAL	ROADWAY 80% FED / 20% LOCAL 0005	ROADWAY 100% LOCAL 0005
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2092	2092	
	42400800	DETECTABLE WARNINGS	SQ FT	260	260	
	44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	15283	15283	
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	60	60	
	44000600	SIDEWALK REMOVAL	SQ FT	2278	2278	
	44200942	CLASS B PATCHES, TYPE III, 8 INCH	SQ YD	18	18	
	44200944	CLASS B PATCHES, TYPE IV, 8 INCH	SQ YD	484	484	
	44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	4	4	
	44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	40	40	
	44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	94	94	
	44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	772	772	
	60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	2	2	
	60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1	
	60265700	VALVE VAULTS TO BE ADJUSTED	EACH	4	4	
	60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	1	1	
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	32		32
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	3		3

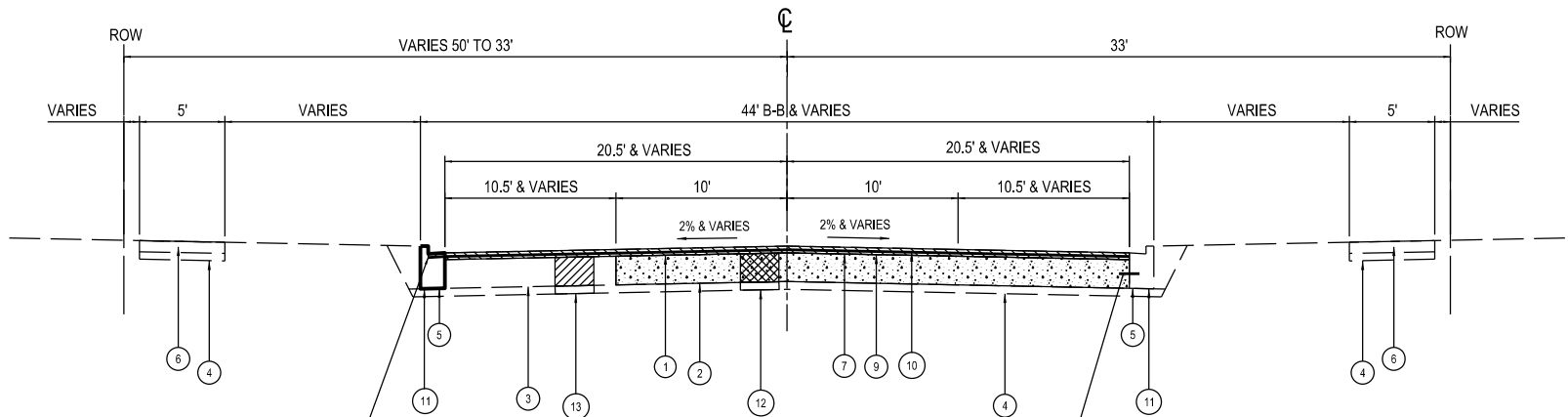
\* SPECIALTY ITEM





EXISTING AND PROPOSED  
FAU 2770 (SHERMER RD) - STA. 119+12 TO STA. 129+70  
FAU 2766 (HARLEM AVE) TO FAU 1313 (BECKWITH RD)

- LEGEND (EXISTING)
- ① HOT-MIX ASPHALT SURFACE, 4 1/4"-9".
  - ② PORTLAND CEMENT CONCRETE BASE COURSE, 6"-7 3/4".
  - ③ HOT-MIX ASPHALT MATERIAL BASE COURSE, 6"-7 3/4".
  - ④ SUBBASE GRANULAR MATERIAL, TYPE B, 4".
  - ⑤ COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 (TYP).
  - ⑥ PORTLAND CEMENT CONCRETE SIDEWALK, 5".
  - ⑦ HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/4".
  - ⑧ NOT USED



EXISTING AND PROPOSED  
FAU 2770 (SHERMER RD) - STA. 129+70 TO STA. 151+13  
FAU 1313 (BECKWITH RD) TO FAU 1312 (GOLF RD)

- LEGEND (PROPOSED)
- ⑨ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4".
  - ⑩ HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 1 1/2".
  - ⑪ INTERMITTENT COMBINATION CONCRETE CURB & GUTTER REMOVAL & REPLACEMENT (MATCH EXISTING).
  - ⑫ CLASS B PATCHES, 8", REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AND AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY THE ENGINEER).
  - ⑬ CLASS D PATCHES, 8", REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AND AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY THE ENGINEER).

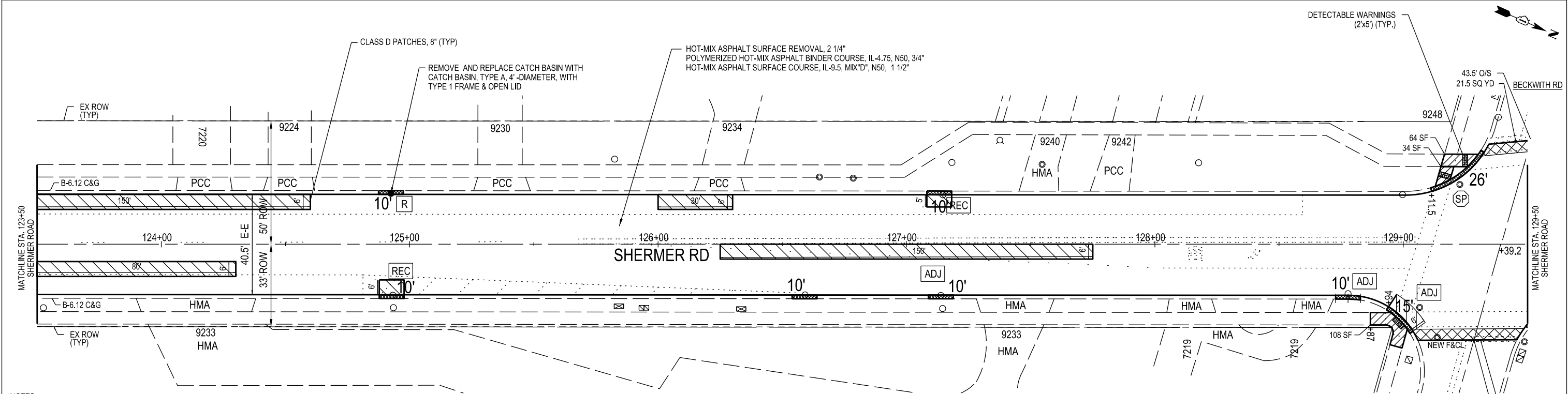
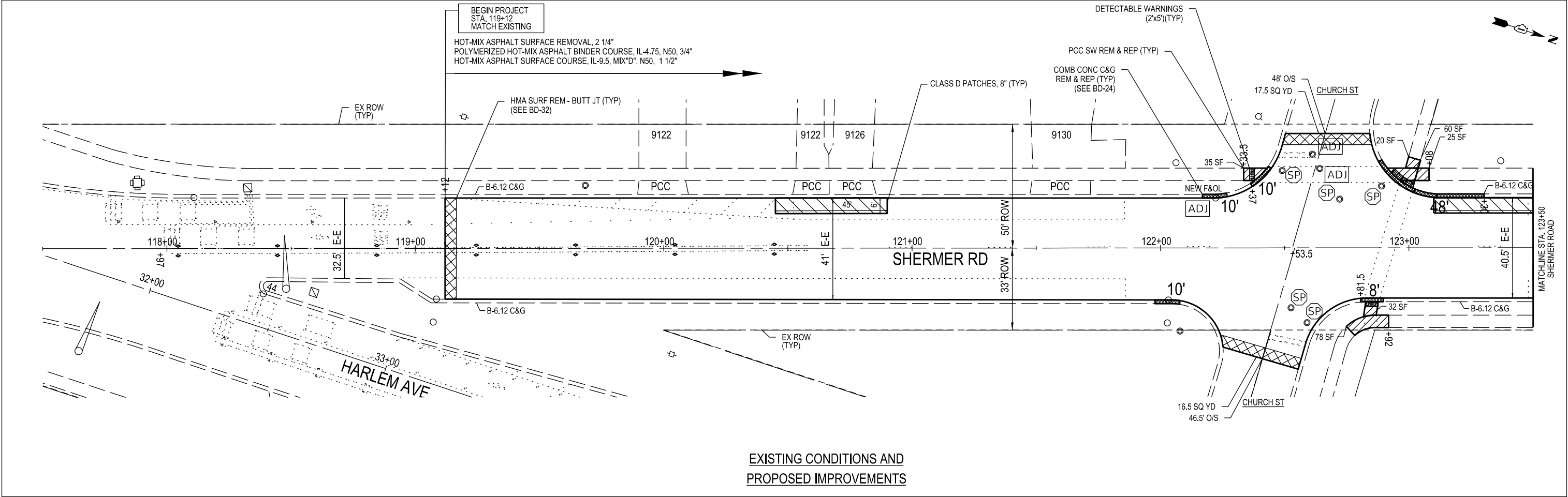
EXISTING OVERLAYED CURB & GUTTER ON THE LEFT SIDE BETWEEN STA 133+81 AND 150+62 IS TO BE REMOVED & REPLACED WITH STANDARD PROFILE CURB & GUTTER AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. ADJACENT ROAD CROSS SLOPE WILL BE ADJUSTED ACCORDINGLY.

EXISTING TIE BAR TO REMAIN IN PLACE  
(SEE CURB & GUTTER REMOVAL & REPLACEMENT DETAIL).

### MILL BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ Ndes	QMP
<b>PAVEMENT RESURFACING</b>		
HOT-MIX ASPHALT SURFACE COURSE, IL 9.5, MIX "D", N50; 1-1/2"	4% @ 50 GYR	LR 1030-2
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 3/4"	3.5% @ 50 GYR	LR 1030-2
<b>PATCHING</b>		
CLASS D PATCH (HMA BINDER IL-19 mm)	4% @ 70 GYR	LR 1030-2
<b>HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"</b>		
HOT-MIX ASPHALT SURFACE COURSE, IL 9.5, MIX "D", N50 3" (IN 2 LIFTS)	4% @ 50 GYR	LR 1030-2
QMP DESIGNATION: LOCAL QUALITY ASSURANCE / QUALITY MANAGEMENT QC/QA PER LR 1030-2		
1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN		
2. THE "AC" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIAL SPECIFICATIONS.		
3. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE P HMA BC IL-4.75 N50.		

MODEL: S:\MODEL\NAMES  
FILE: 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NOTES:

1. FOR ALL SIDE STREETS, THE RESURFACING LIMITS WILL BE MARKED BY THE ENGINEER IN THE FIELD.
2. CURB & GUTTER REMOVAL & REPLACEMENT WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
3. CLASS B AND CLASS D PATCHES WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
4. P.C.C. SIDEWALK REMOVAL & REPLACEMENT WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
5. STRUCTURES TO BE ADJUSTED OR RECONSTRUCTED, OR FRAMES AND LIDS TO BE REPLACED, WILL BE LOCATED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.

LEGEND	
	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	DRAINAGE STRUCTURE TO BE ADJUSTED
	DRAINAGE STRUCTURE TO BE RECONSTRUCTED

EXISTING CONDITIONS AND  
PROPOSED IMPROVEMENTS

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
ROADWAY PLANS

SCALE: 1"=20' SHEET 05 OF 09 SHEETS STA. 119+12 TO STA. 129+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	06
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				

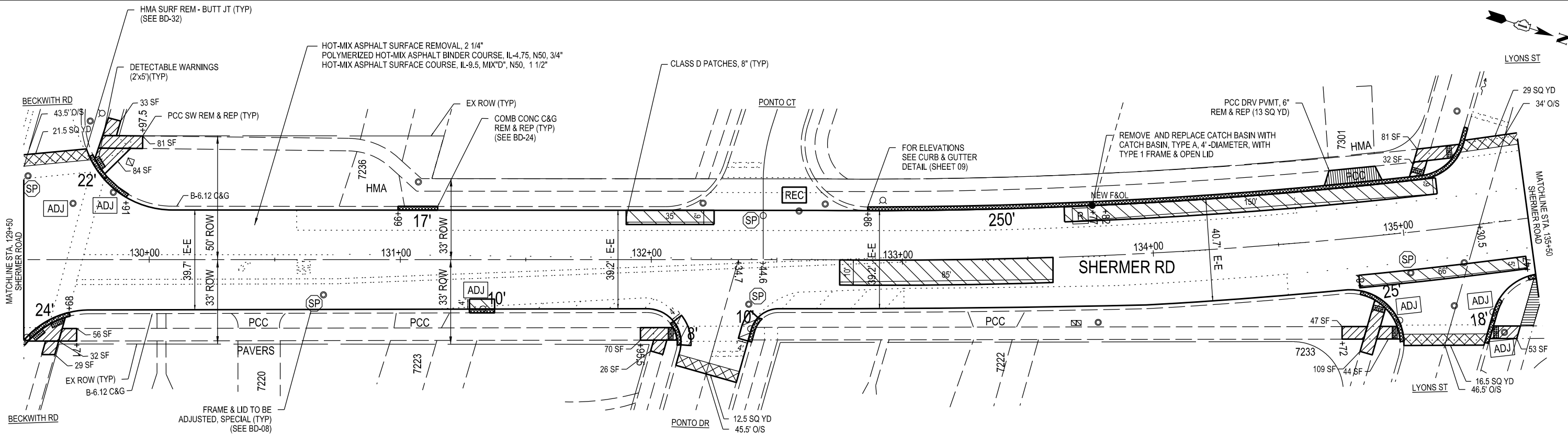
MODEL: SMOBELNAMES  
FILE: ROADWAY STYLES



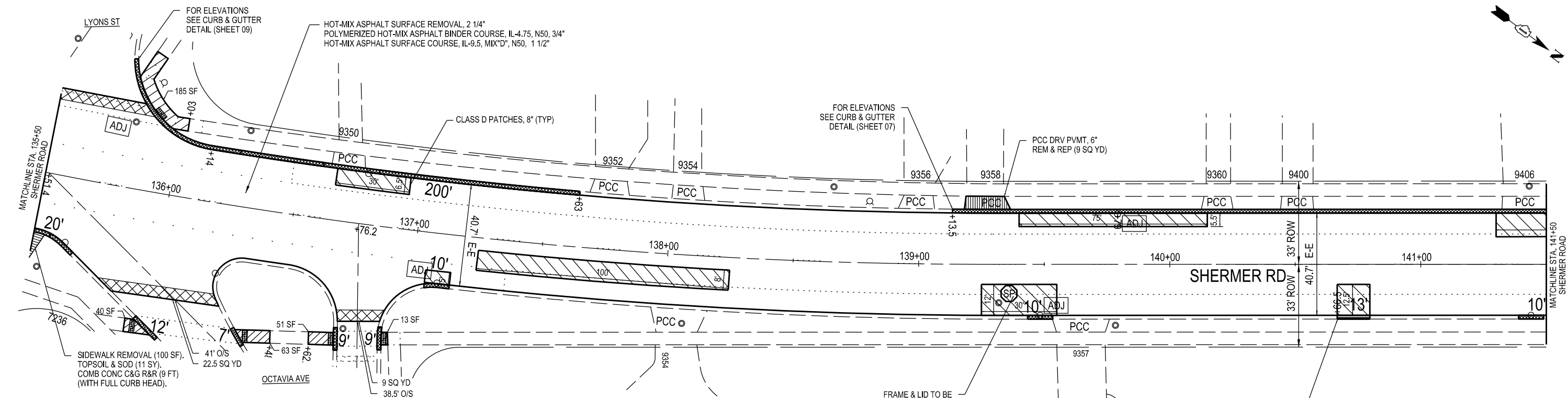
USER NAME = \$USERS	DESIGNED - MG / JG	REVISED - _____
PLOT SCALE = \$SCALE\$	DRAWN - JG	REVISED - _____
PLOT DATE = \$DATES	CHECKED - CRT	REVISED - _____
	DATE - 04-02-2025	REVISED - _____







EXISTING CONDITIONS AND  
PROPOSED IMPROVEMENTS



- NOTES:
1. FOR ALL SIDE STREETS, THE RESURFACING LIMITS WILL BE MARKED BY THE ENGINEER IN THE FIELD.
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  3. CLASS B AND CLASS D PATCHES WILL BE MARKED BY THE ENGINEER IN THE FIELD, NOT TO EXCEED PLAN QUANTITIES.
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LEGEND	
	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	DRAINAGE STRUCTURE TO BE ADJUSTED
	DRAINAGE STRUCTURE TO BE RECONSTRUCTED

EXISTING CONDITIONS AND  
PROPOSED IMPROVEMENTS



MODEL: SMODELNAMES  
FILE: ROADWAY STYLES

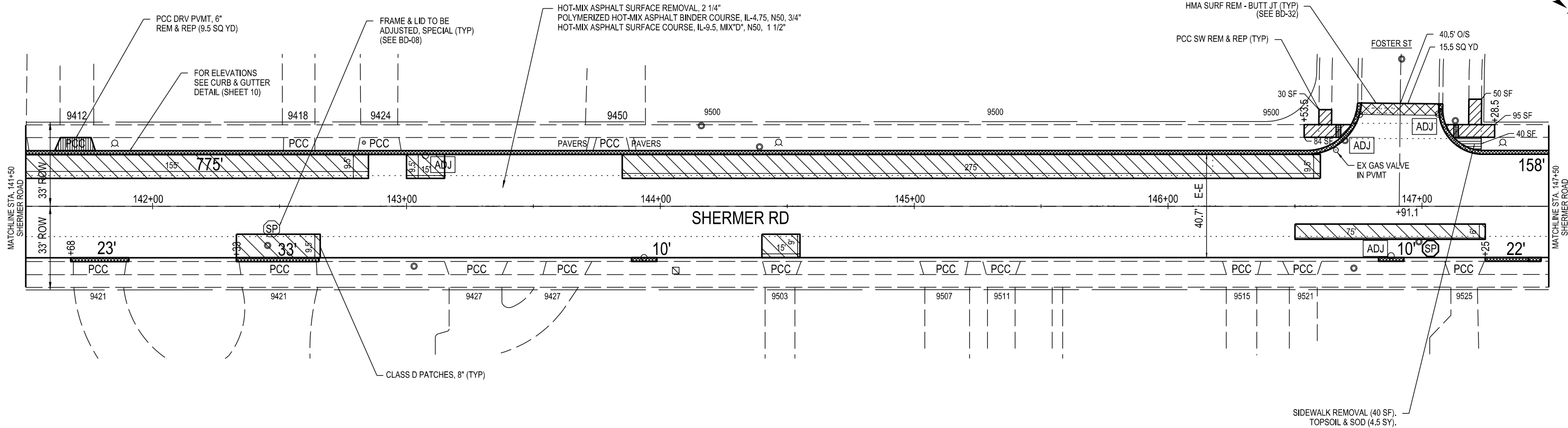


USER NAME = \$USERS	DESIGNED - MG / JG	REVISED - _____
	DRAWN - JG	REVISED - _____
PLOT SCALE = \$SCALE\$	CHECKED - CRT	REVISED - _____
PLOT DATE = \$DATES	DATE - 04-02-2025	REVISED - _____

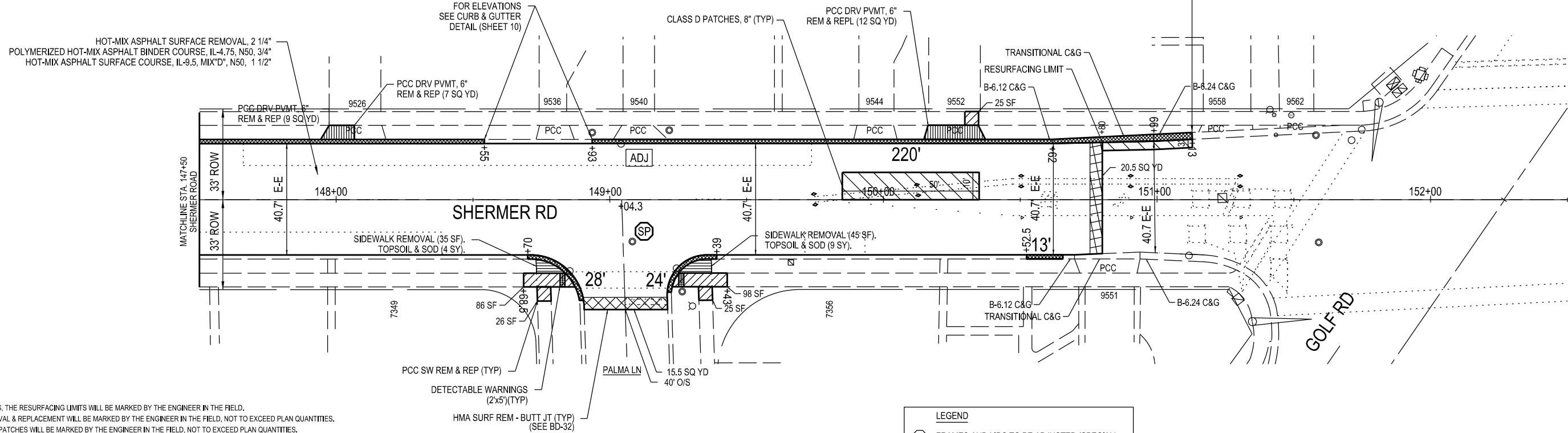
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY			
ROADWAY PLANS			
SCALE: 1"=20'	SHEET X	OF SHEETS	STA. 129+50 TO STA. 141+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	07
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				



EXISTING CONDITIONS AND  
PROPOSED IMPROVEMENTS



- NOTES:
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LEGEND	
	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
	DRAINAGE STRUCTURE TO BE ADJUSTED
	DRAINAGE STRUCTURE TO BE RECONSTRUCTED

EXISTING CONDITIONS AND  
PROPOSED IMPROVEMENTS

MODEL: SMODELNAMES  
FILE: RWDG: STYLES



USER NAME = \$USERS	DESIGNED - MG / JG	REVISED - _____
	DRAWN - JG	REVISED - _____
PLOT SCALE = \$SCALE\$	CHECKED - CRT	REVISED - _____
PLOT DATE = \$DATES	DATE - 04-02-2025	REVISED - _____

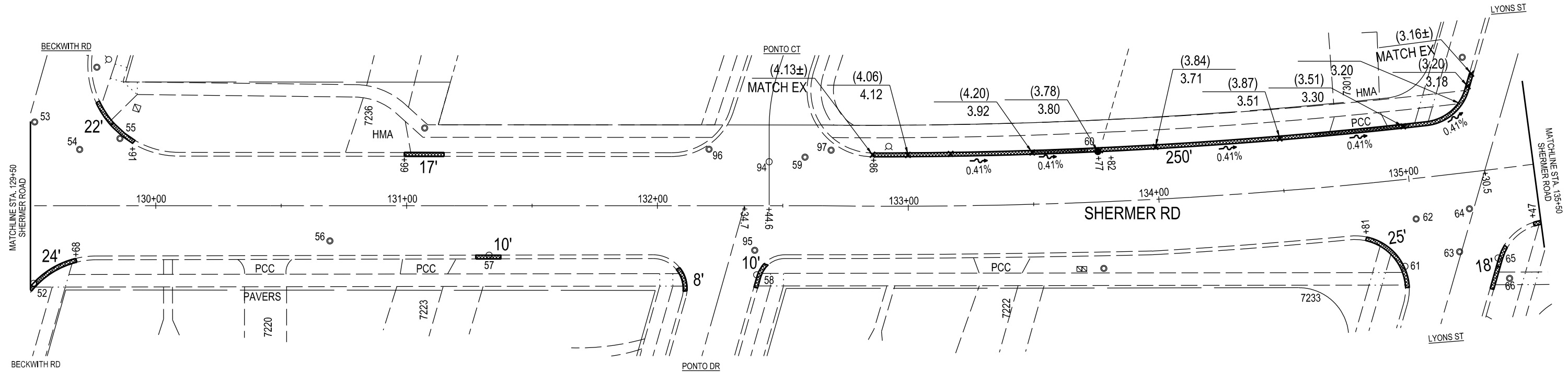
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY			
ROADWAY PLANS			
SCALE: 1"=20'	SHEET X	OF SHEETS	STA. 141+50 TO STA. 151+77

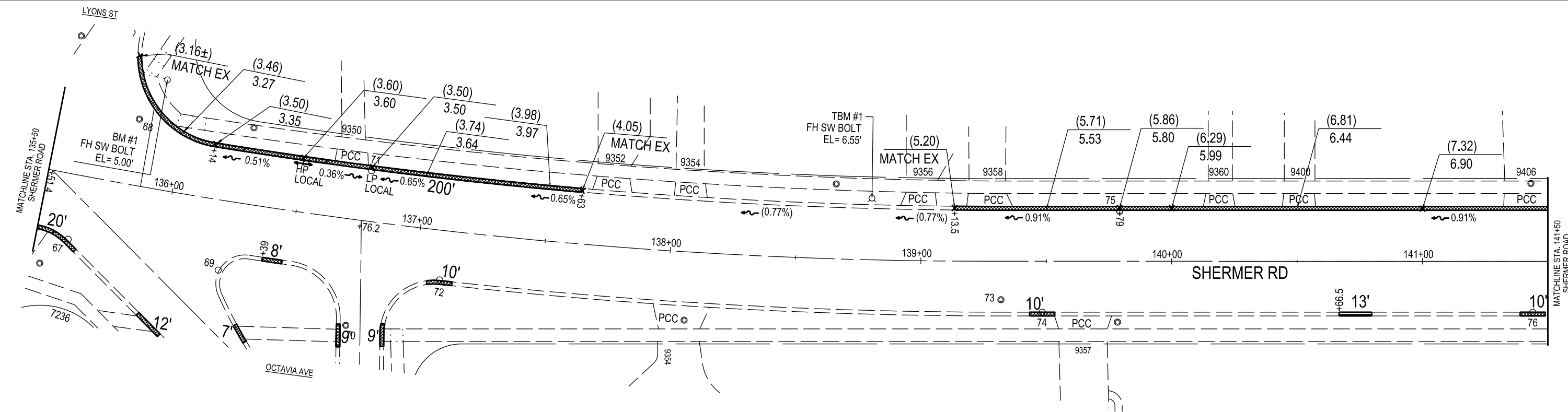
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2770	24-00114-00-RS	COOK	24	08
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				







EXISTING CONDITIONS AND  
PROPOSED IMPROVEMENTS



EXISTING CONDITIONS AND  
PROPOSED IMPROVEMENTS



- NOTES:
- ELEVATIONS SHOWN FOR CURB & GUTTER ARE FLOW LINE UNLESS OTHERWISE NOTED.
  - EXISTING ELEVATIONS AND SLOPES ARE IN PARENTHESIS. PROPOSED ELEVATIONS AND SLOPES ARE WITHOUT PARENTHESIS.

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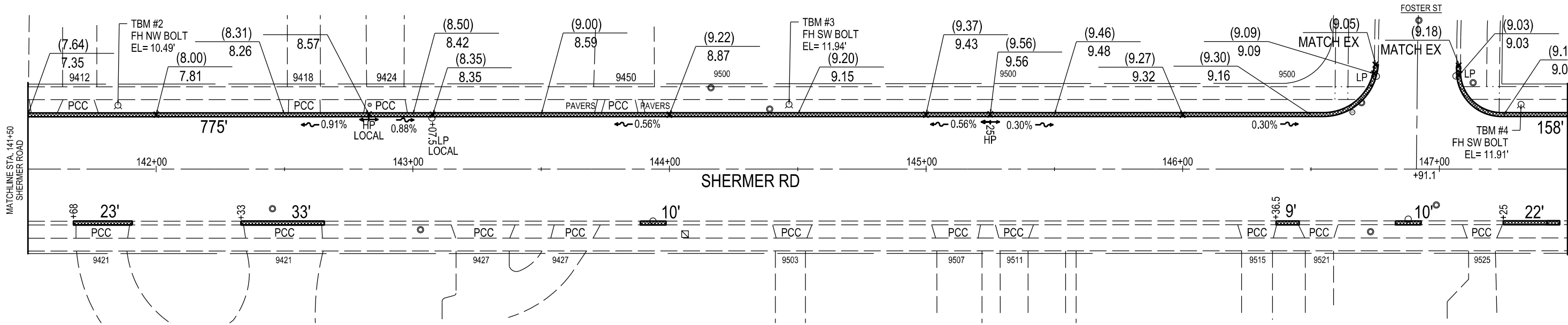


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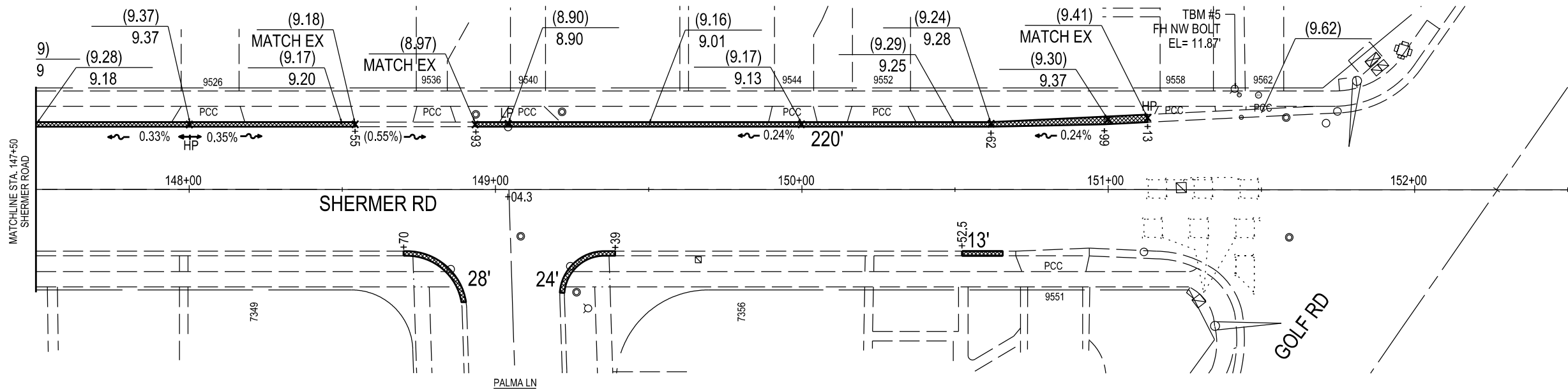
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY			
CURB & GUTTER DETAIL			
SCALE: 1"=20'	SHEET X OF	SHEETS	STA. 129+50 TO STA. 141+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-R5	COOK	24	09
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				



EXISTING CONDITIONS AND  
PROPOSED IMPROVEMENTS



- NOTES:
- ELEVATIONS SHOWN FOR CURB & GUTTER ARE FLOW LINE UNLESS OTHERWISE NOTED.
  - EXISTING ELEVATIONS AND SLOPES ARE IN PARENTHESIS. PROPOSED ELEVATIONS AND SLOPES ARE WITHOUT PARENTHESIS.

EXISTING CONDITIONS AND  
PROPOSED IMPROVEMENTS



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FILE: ROAD: STILES



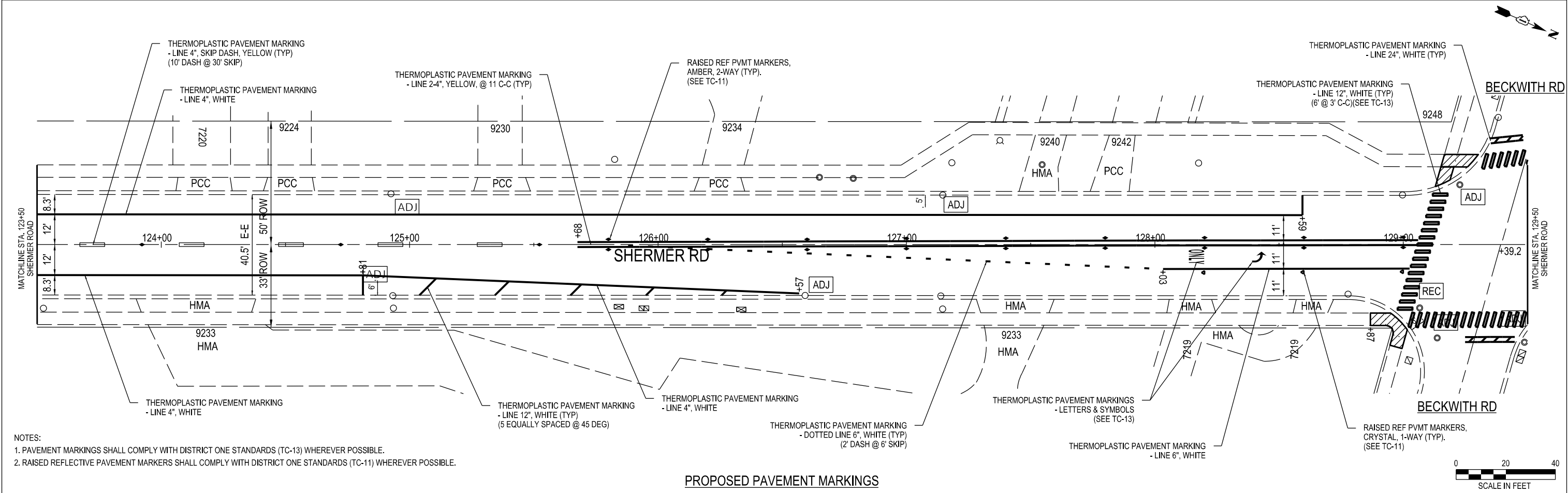
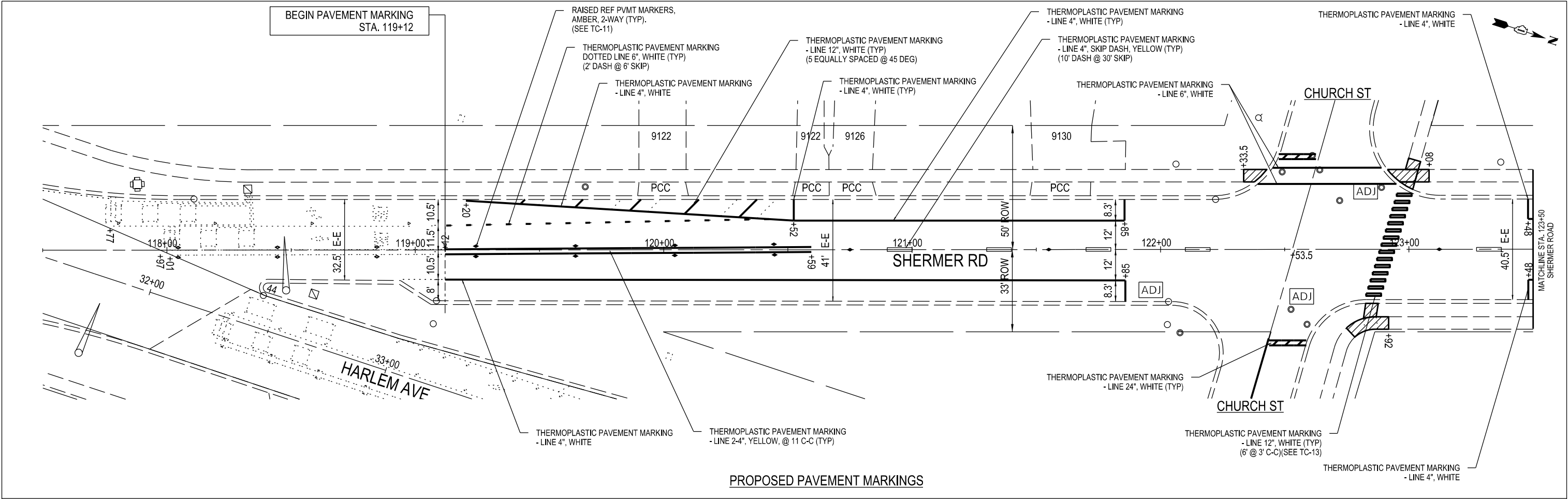
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	DRAWN - JG	REVISED - _____
PLOT SCALE = \$SCALES	CHECKED - CRT	REVISED - _____
PLOT DATE = \$DATES	DATE - 04-02-2025	REVISED - _____

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
CURB & GUTTER DETAIL

SCALE: 1"=20' SHEET X OF SHEETS STA. 141+50 TO STA. 151+13

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	10
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				



- NOTES:
- 1. PAVEMENT MARKINGS SHALL COMPLY WITH DISTRICT ONE STANDARDS (TC-13) WHEREVER POSSIBLE.
  - 2. RAISED REFLECTIVE PAVEMENT MARKERS SHALL COMPLY WITH DISTRICT ONE STANDARDS (TC-11) WHEREVER POSSIBLE.

MODEL: SHOMELNAMES  
FILE: ROAD11.DWG



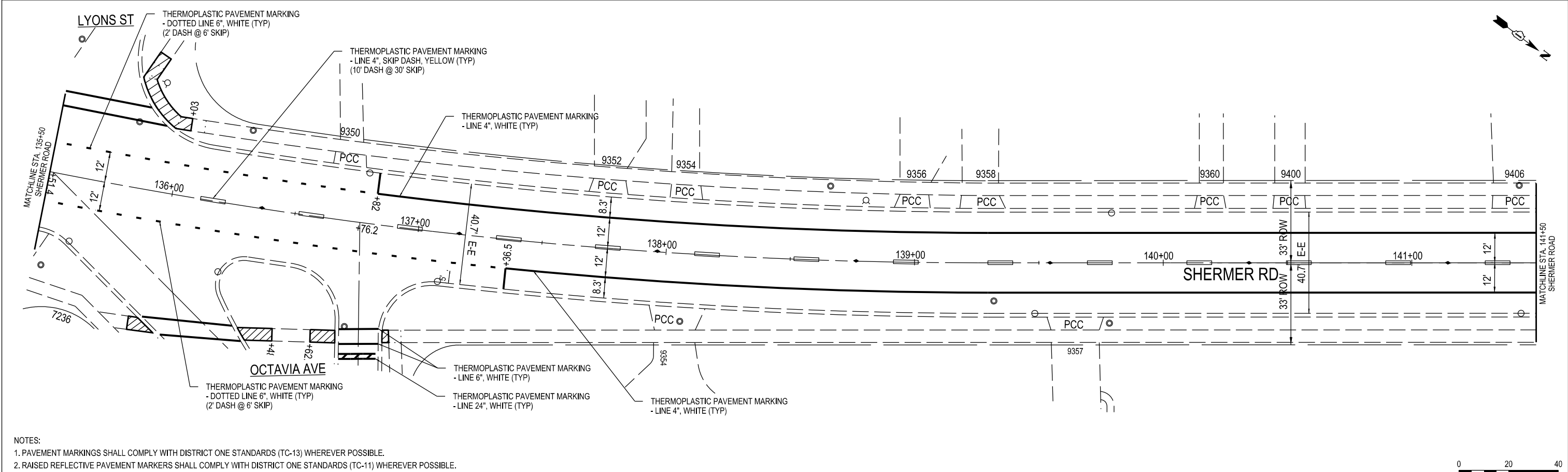
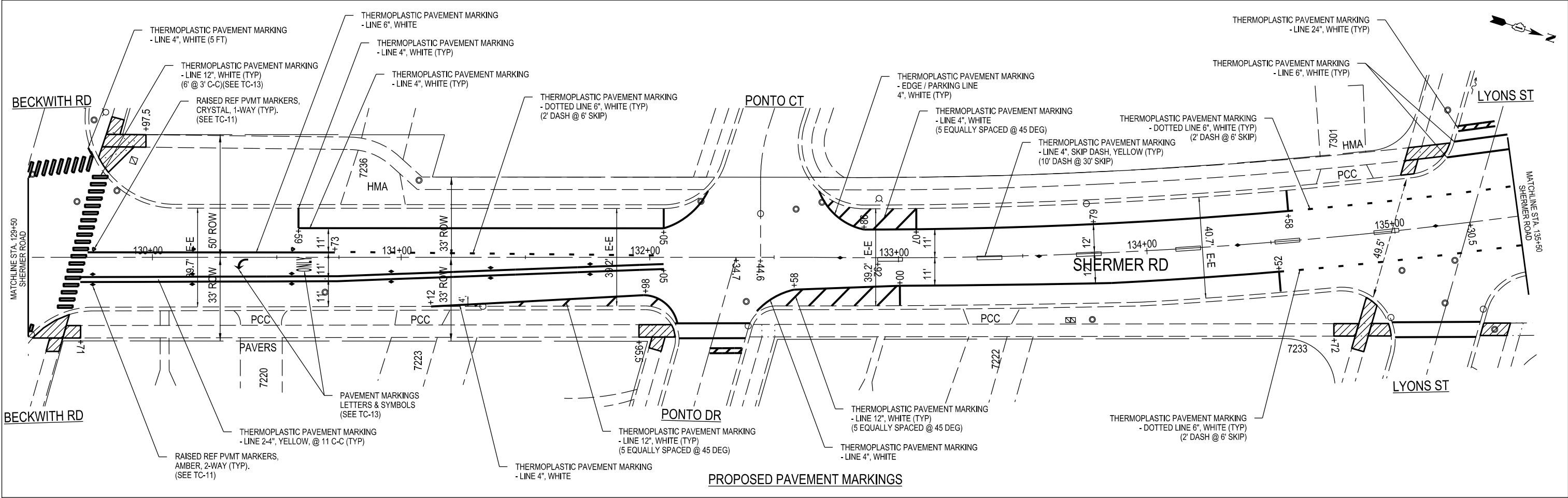
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PLOT SCALE = \$SCALE\$	CHECKED - CRT	REVISED - _____
PLOT DATE = \$DATES	DATE - 04-02-2025	REVISED - _____

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY  
PAVEMENT MARKING PLANS

SCALE: 1"=20' SHEET 10 OF 12 SHEETS STA. 119+12 TO STA. 129+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-R5	COOK	24	11
CONTRACT NO. 61L65				
ILLINOIS FED. AID PROJECT				



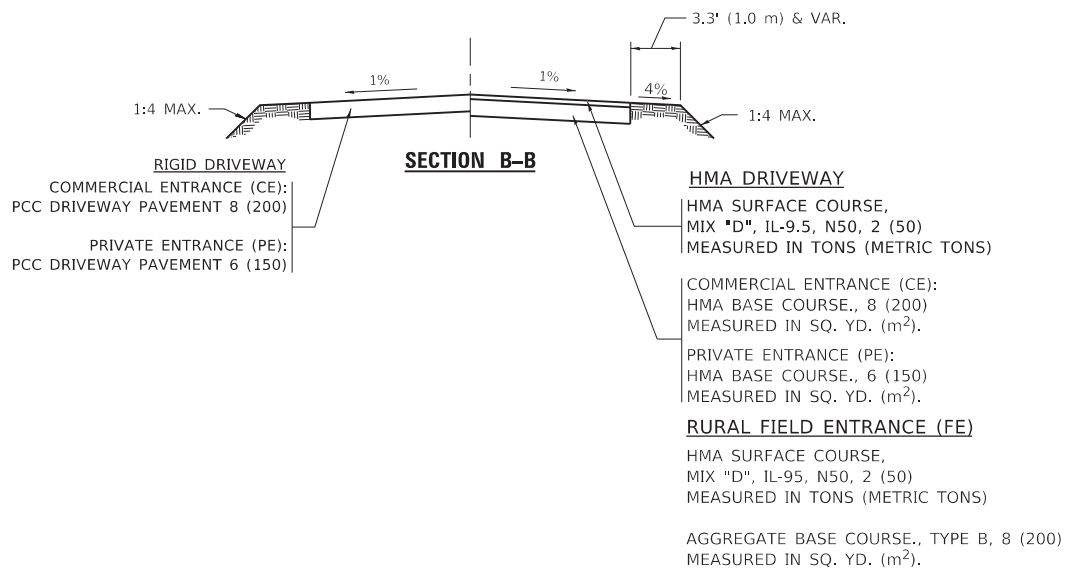
- NOTES:
- 1. PAVEMENT MARKINGS SHALL COMPLY WITH DISTRICT ONE STANDARDS (TC-13) WHEREVER POSSIBLE.
  - 2. RAISED REFLECTIVE PAVEMENT MARKERS SHALL COMPLY WITH DISTRICT ONE STANDARDS (TC-11) WHEREVER POSSIBLE.



PROPOSED PAVEMENT MARKINGS

<div><div></div><div>MORTON GROVE</div><div>Incredibly Close &amp; Amazingly Open</div></div>	USER NAME = \$USERS		DESIGNED - MG / JG		REVISED - _____		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				SHERMER ROAD LOCAL AGENCY FUNCTIONAL OVERLAY PAVEMENT MARKING PLANS						F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - JG		CHECKED - CRT		REVISED - _____												2770	24-00114-00-RS	COOK	24	12
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	PLOT DATE = \$DATES										ILLINOIS   FED. AID PROJECT										
								SCALE: 1"=20'		SHEET X		OF SHEETS		STA. 129+50		TO STA. 141+50					

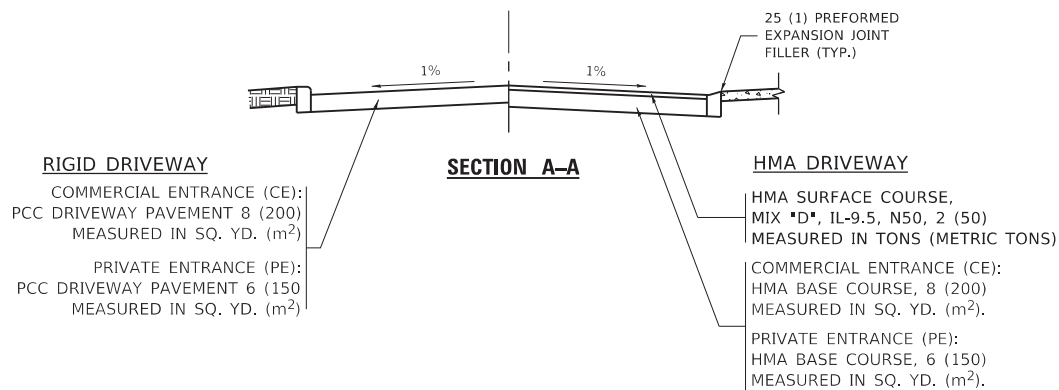




## GENERAL NOTES

1. DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.
2. COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

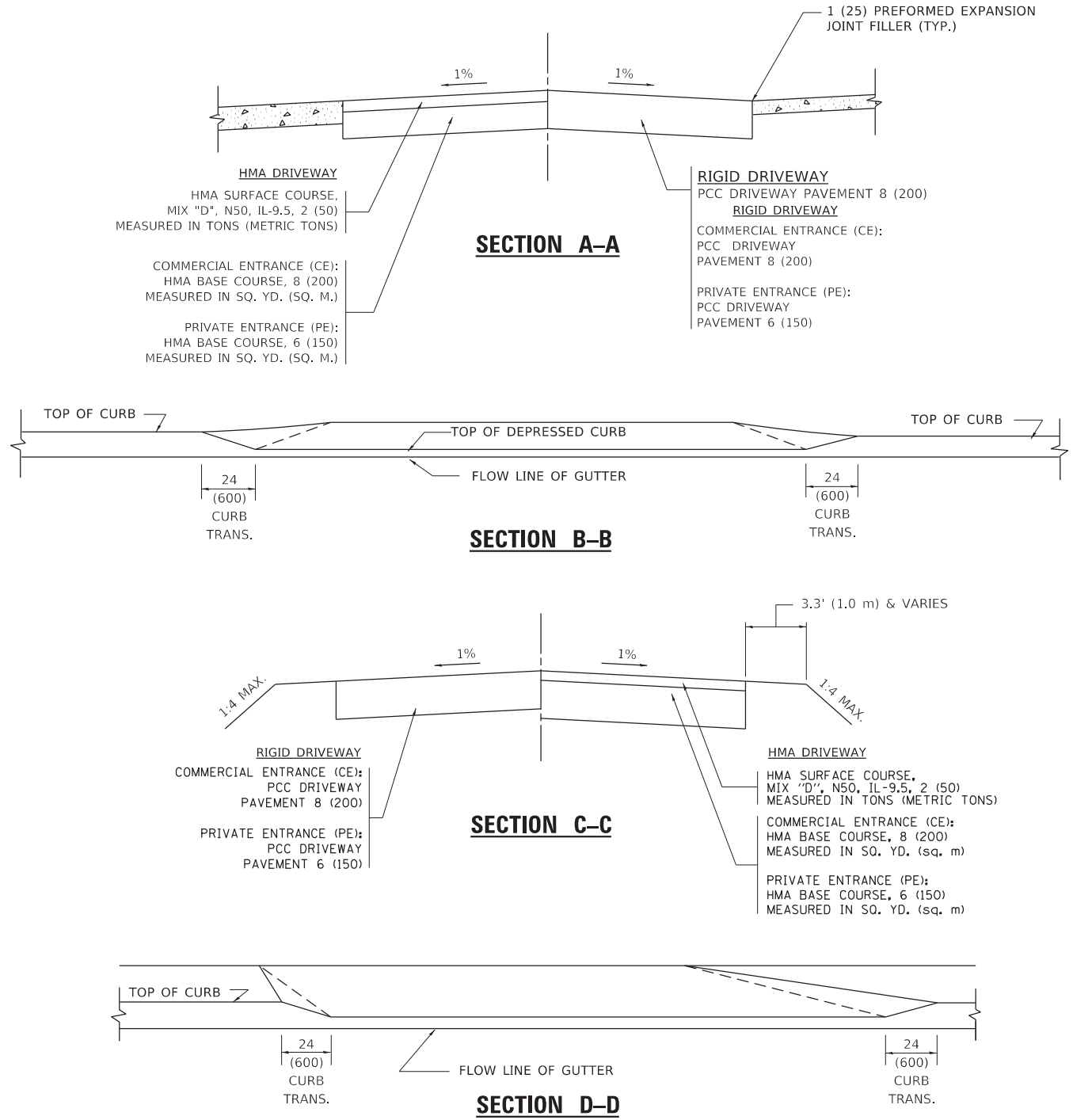
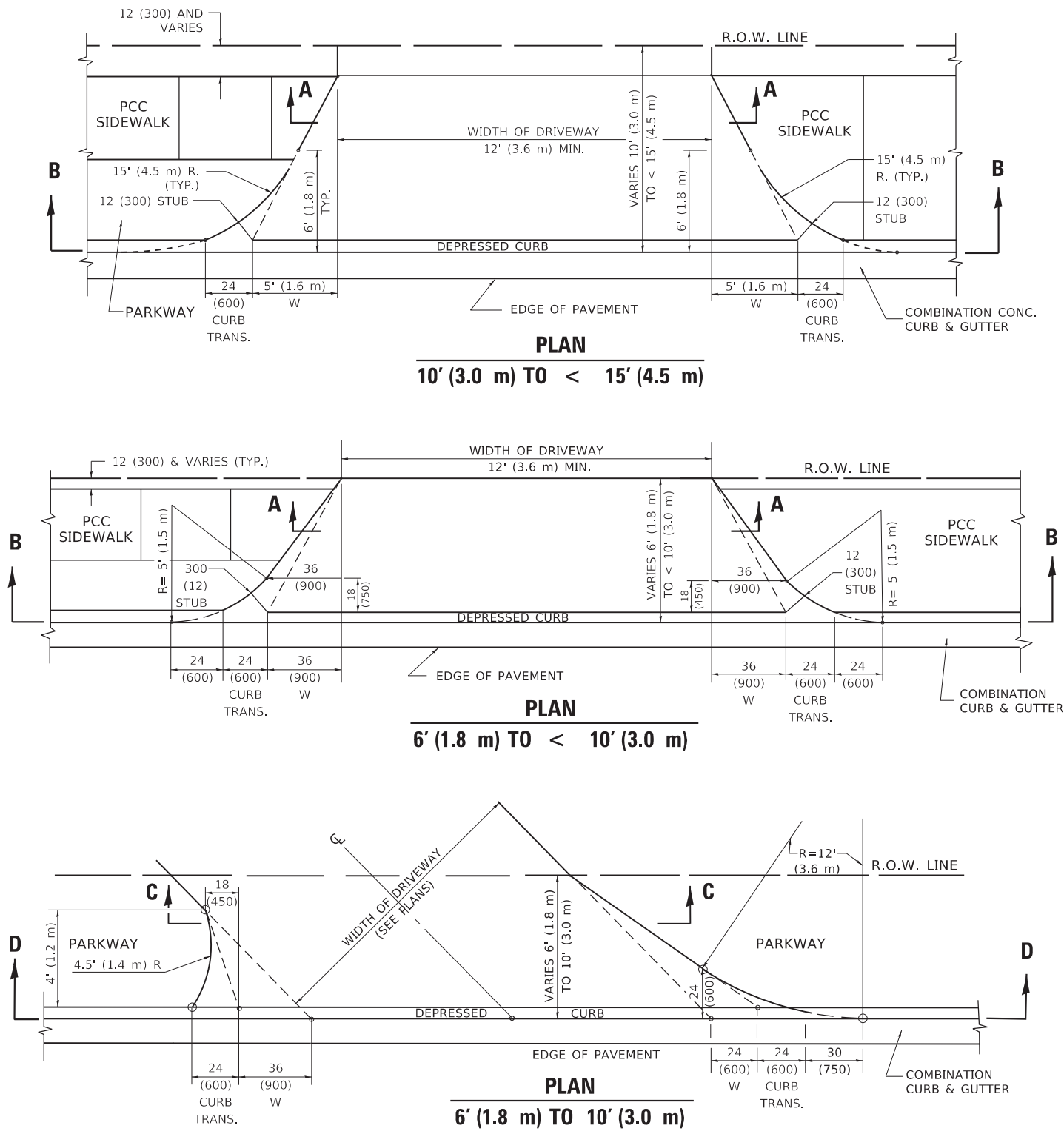
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE NOTED.



	USER NAME = Lawrence,DeManche	DESIGNED - R. SHAH	REVISED - R. BORO 06-11-08	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER ≥15'(4.5m)				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100,000' / in.	DRAWN -	REVISED - R. BORO 09-06-11						2770	24-00114-00-RS	COOK	24	14
	PLOT DATE = 11/18/2022	CHECKED -	REVISED - K. SMITH 08-28-19		BD400-01 (BD-01)				CONTRACT NO. 61L65				
		DATE - 11-04-95	REVISED - K. SMITH 11-18-22										
					SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.					



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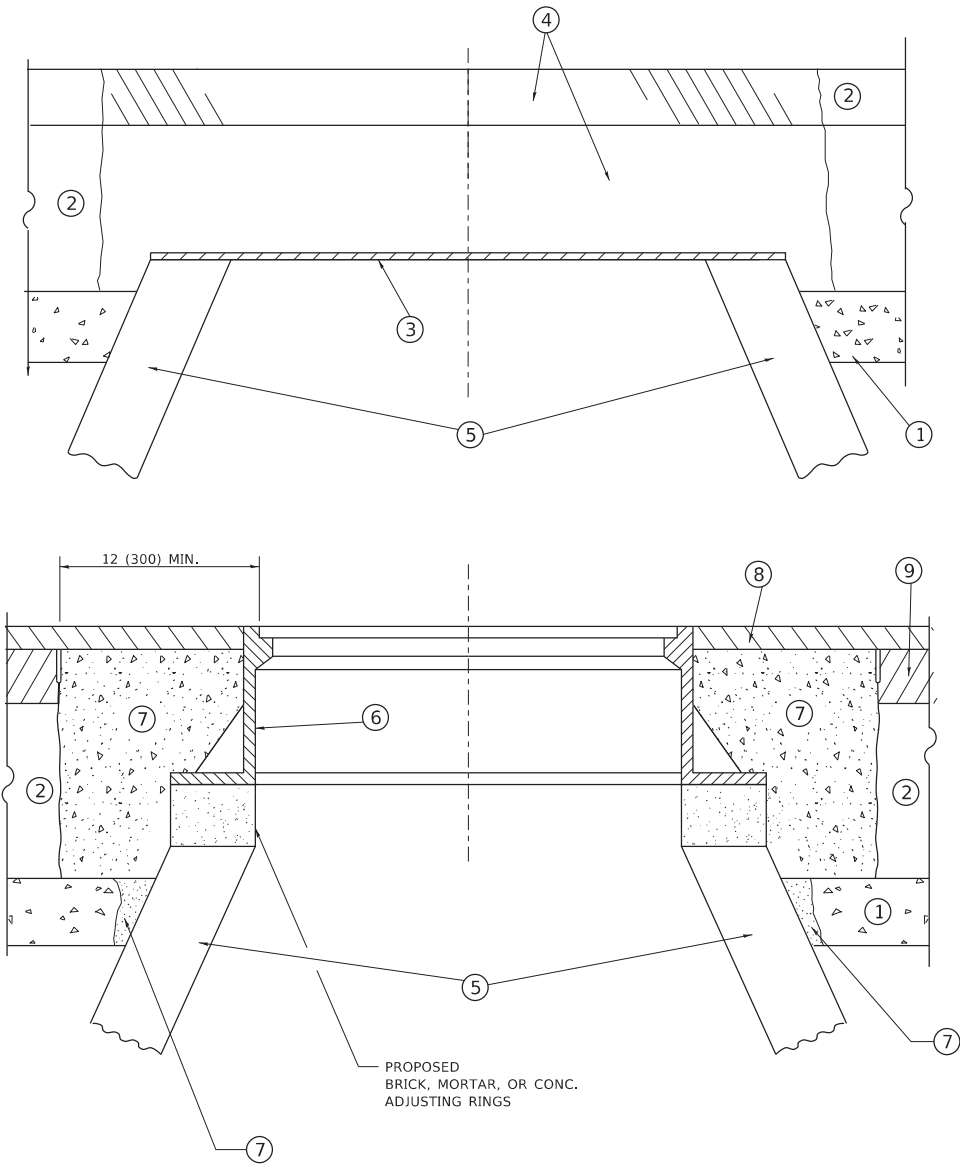
**GENERAL NOTES**

- DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.
- WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE PCC SIDEWALK SHALL EXTEND TO THE BACK OF CURB.
- "W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE NOTED.

MODEL: DEFENDANT  
FILE NAME: DND

MODEL: DEFENDANT FILE NAME: DND	USER NAME = Lawrence,DeManche	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY DETAILS				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
		DRAWN -	REVISED - R. BORO 09-06-11		DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5m)				2770	24-00114-00-RS	COOK	24	15		
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	PLOT DATE = 11/18/2022	DATE - 11-06-95	REVISED - K. SMITH 11-18-22						ILLINOIS		FED. AID PROJECT				
					SCALE: NONE				SHEET 1	OF 1 SHEETS	STA.	TO STA.			



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT  
WITH MILLING**

**NOTES**

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).
- STAGE 2 (AFTER PAVEMENT MILLING)**
- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- \* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- |  |                               |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL                 | ⑥ FRAME AND LID (SEE NOTES)   |
| ② EXISTING PAVEMENT                          | ⑦ CLASS PP-2* CONCRETE        |
| ③ 36 (900) DIAMETER METAL PLATE              | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE  |
| ⑤ EXISTING STRUCTURE                         |                               |

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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PLOT DATE = 9/15/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

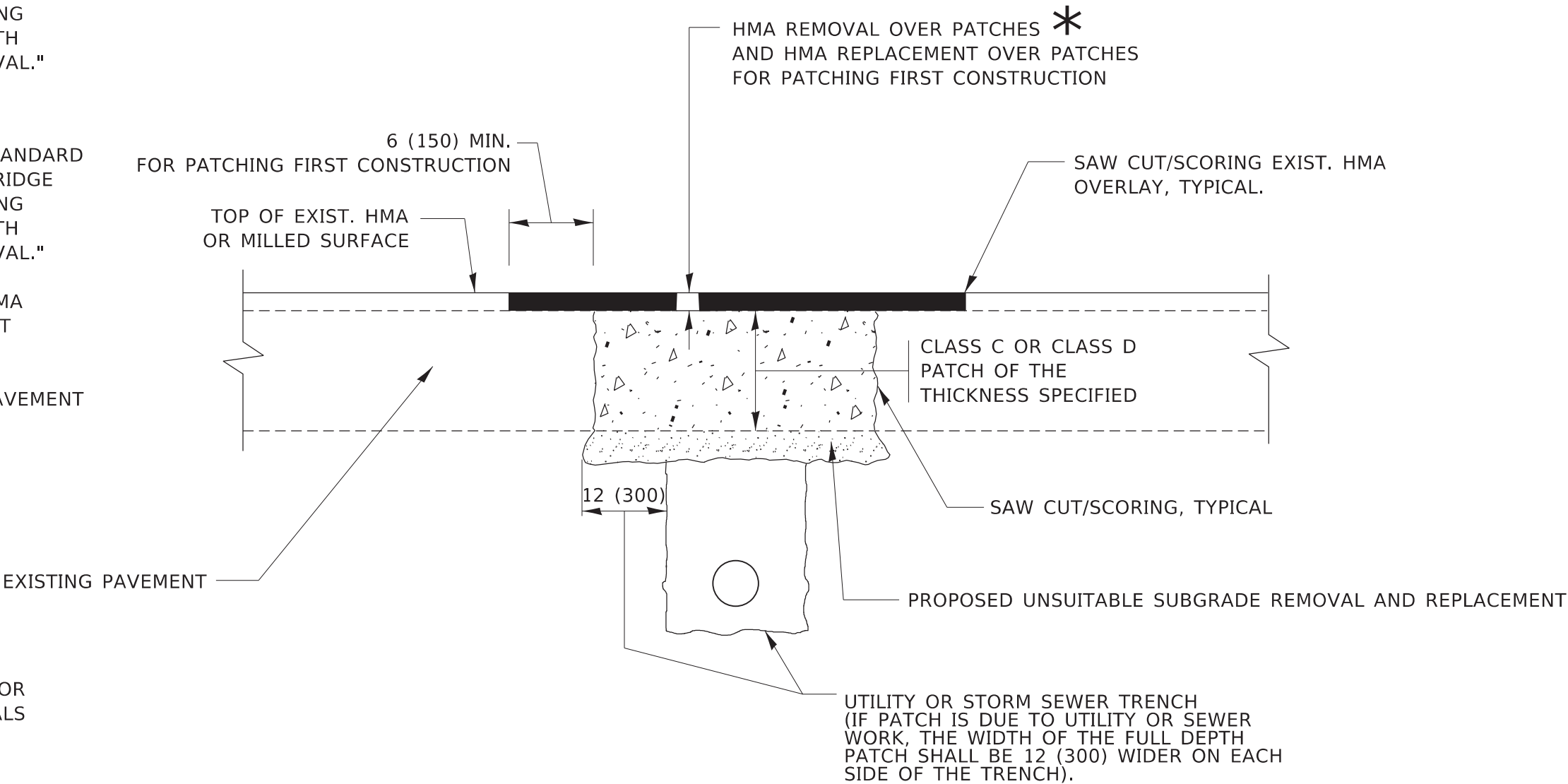
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2770	24-00114-00-RS	COOK	24	16
BD600-03 (BD-08)		CONTRACT NO. 61L65		
		ILLINOIS	FED. AID PROJECT	

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

- 1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
- 2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
- 3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

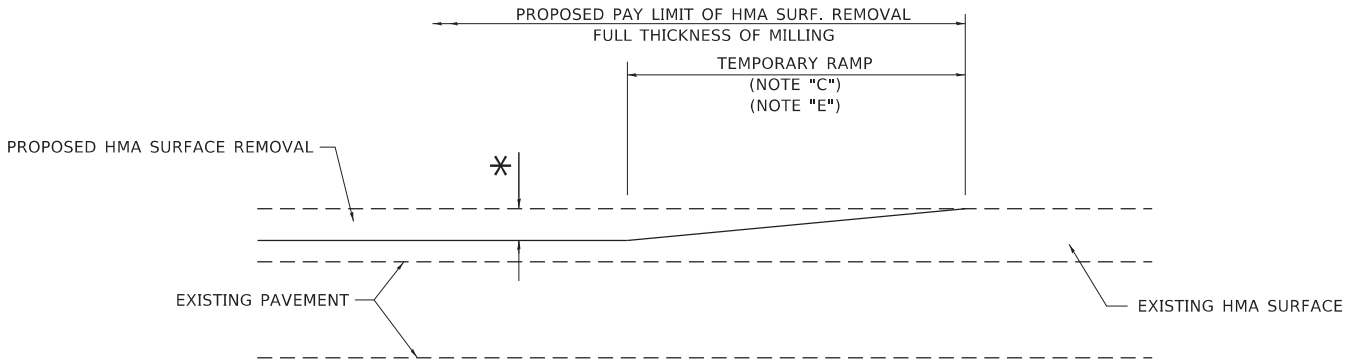
- 1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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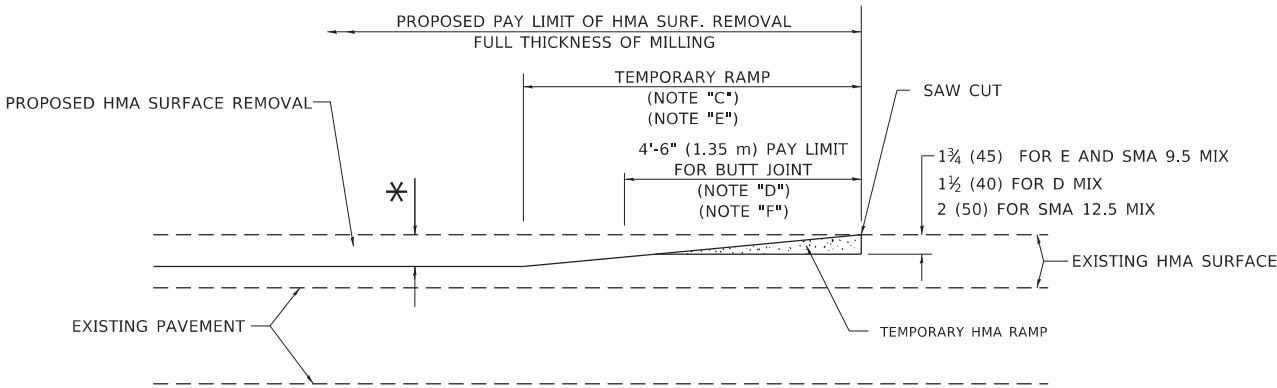
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	PLOT DATE = 11/18/2022	DATE - 10-25-94	REVISED - K. SMITH 11-18-22		ILLINOIS FED. AID PROJECT								
SCALE: NONE		SHEET 1	OF 1	SHEETS	STA.	TO STA.							





**MILLED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

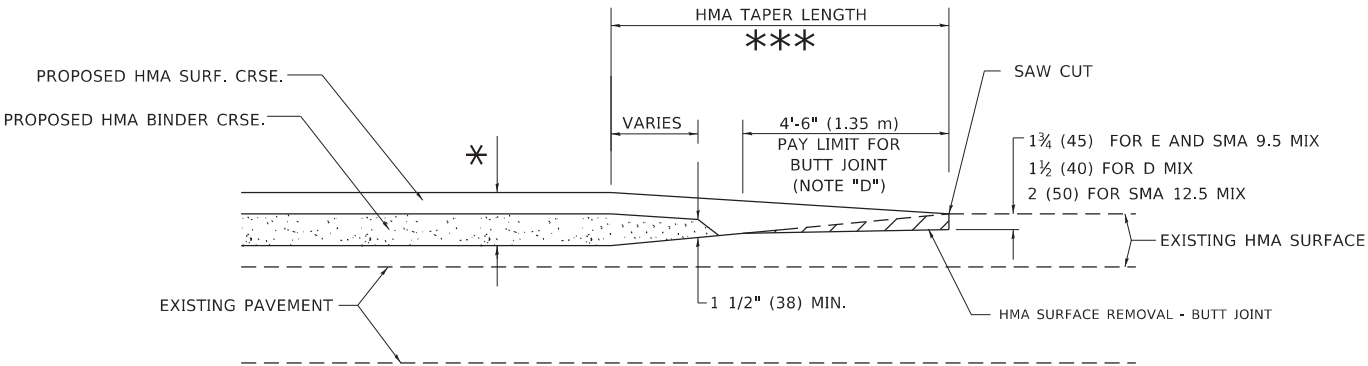
**OPTION 1**



**HMA CONSTRUCTED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

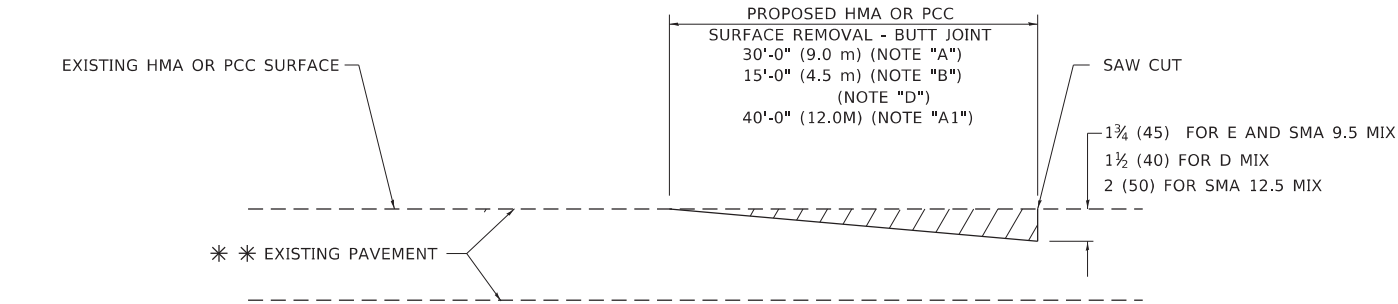
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

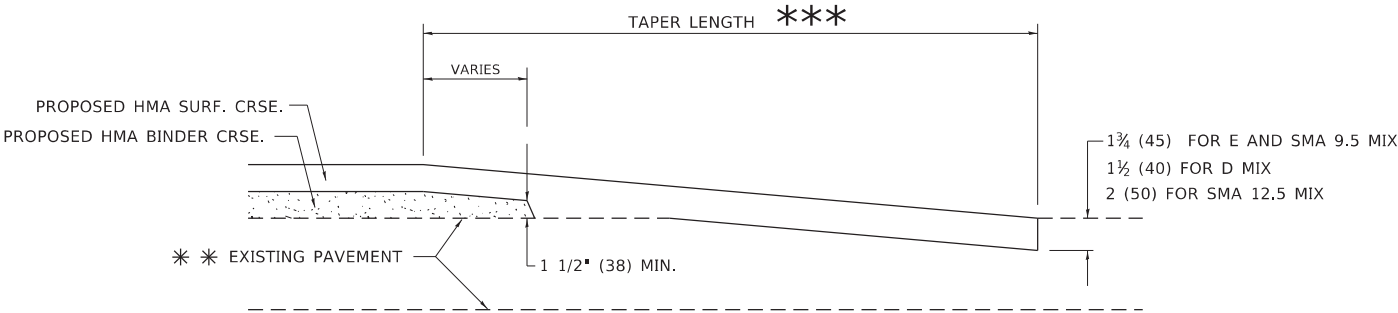


**BUTT JOINT AND  
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

**\*\*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**GENERAL NOTES**

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
**\*** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
**\*\*\*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = Lawrence,DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

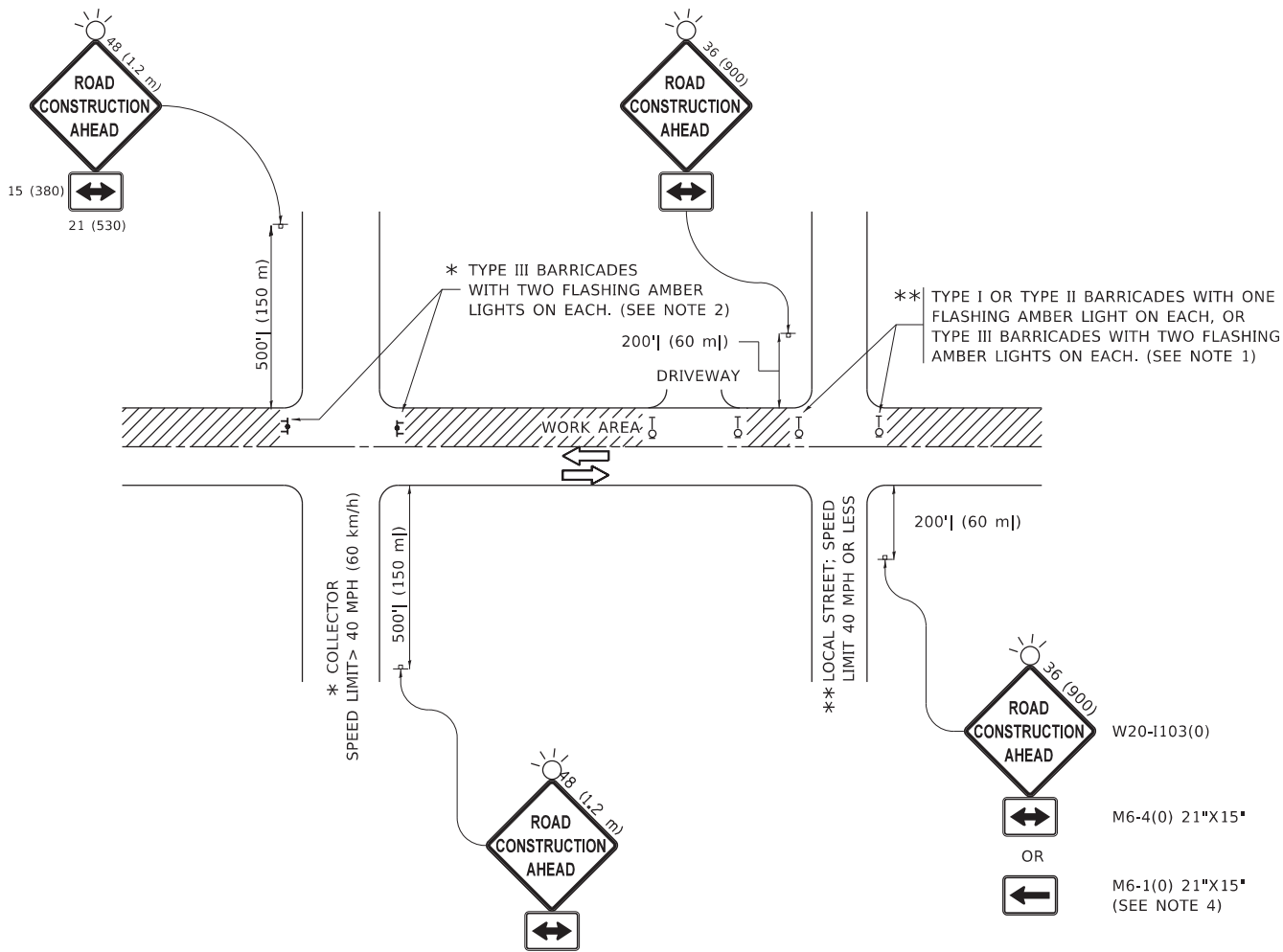
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	19
BD400-05 BD-32		CONTRACT NO. 61L65		
ILLINOIS		FED. AID PROJECT		

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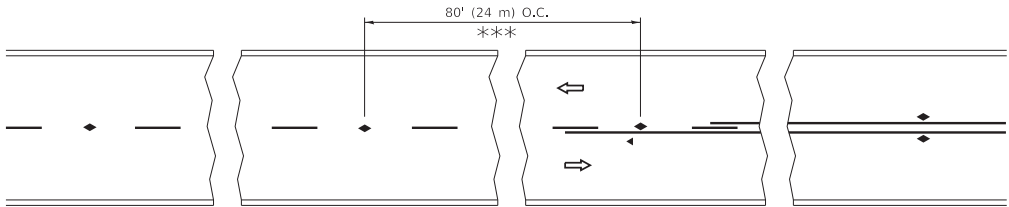
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters)  
unless otherwise shown.

	USER NAME	= Lawrence,DeManche	DESIGNED	-	L,H,A,	REVISED	-	T. RAMMACHER 01-06-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE	= 5/3/2024	DATE	-	06-89	REVISED	-	D. SENDERAK 05-03-24									ILLINOIS		FED. AID PROJECT		

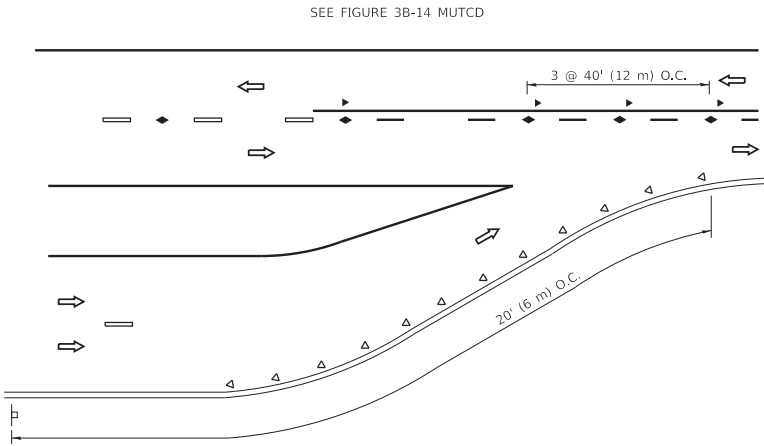


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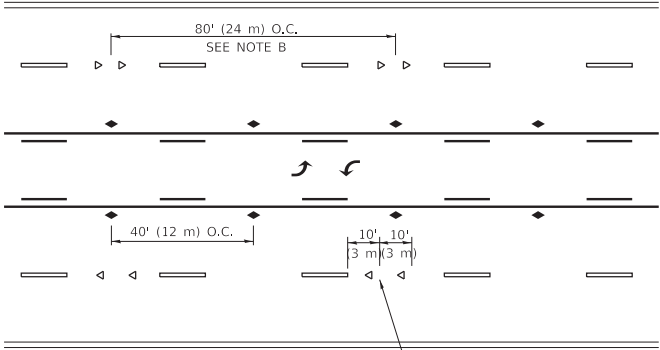


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

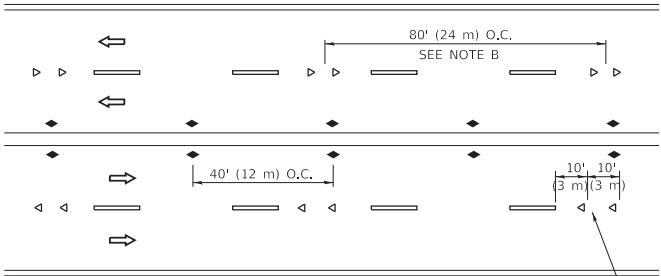
### TWO-LANE/TWO-WAY



### LANE REDUCTION TRANSITION

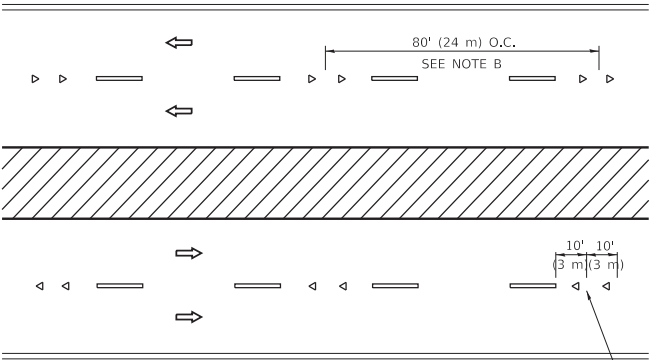


### TWO-WAY LEFT TURN



SEE NOTE A

### MULTI-LANE/UNDIVIDED



SEE NOTE A

### MULTI-LANE/DIVIDED

### GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

### LANE MARKER NOTES

- USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

### SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

### DESIGN NOTES

- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footem]	DESIGNED -	REVISED - T. RAMMACHER 03-12-99
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13

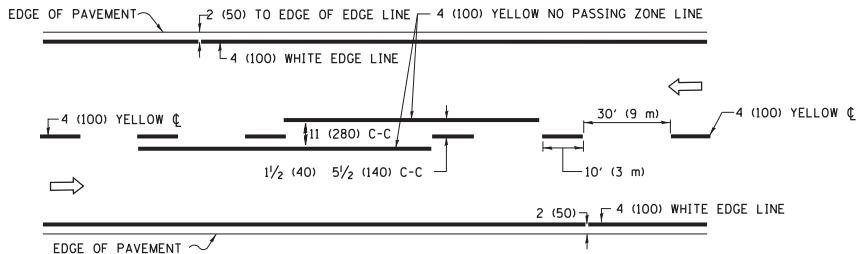
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS  
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

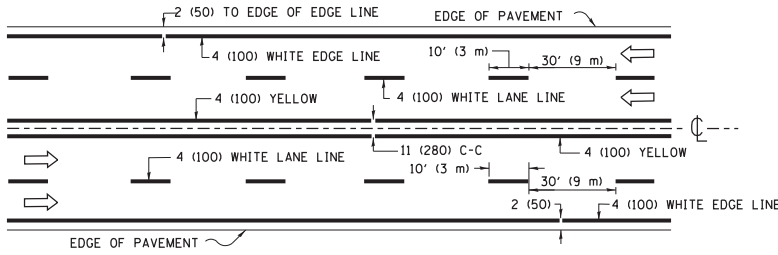
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		ILLINOIS	FED. AID PROJECT	

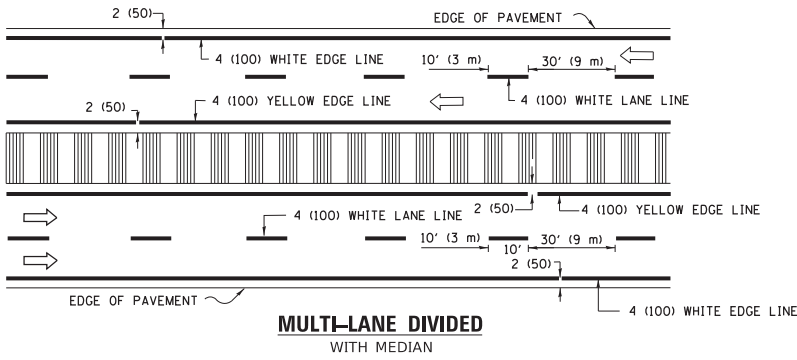
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2-LANE ROADWAY

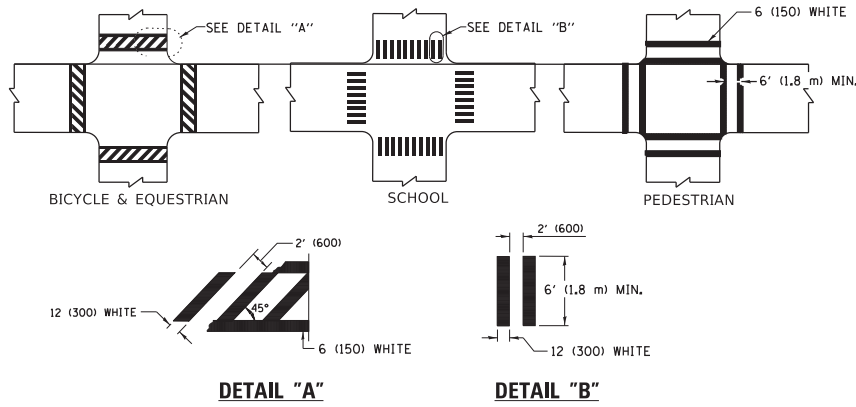


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED  
WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

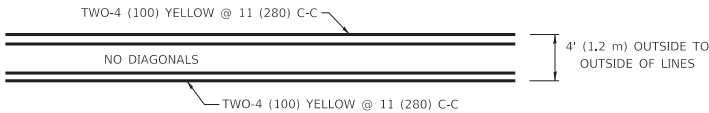


DETAIL "A"

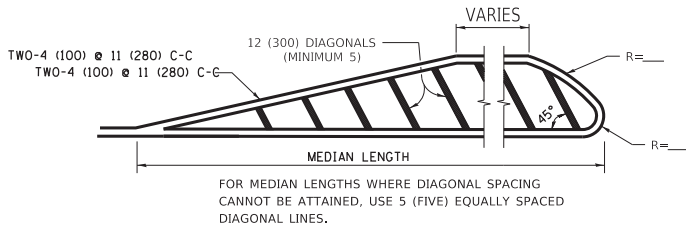
DETAIL "B"

TYPICAL CROSSWALK MARKING

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

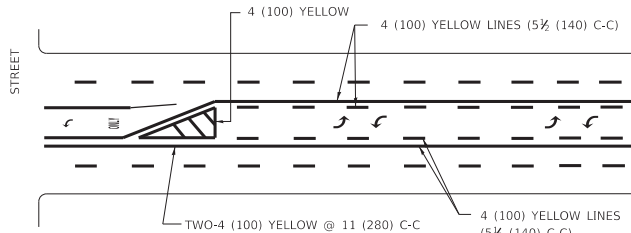


4' (1.2 m) WIDE MEDIANS ONLY

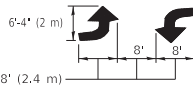


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

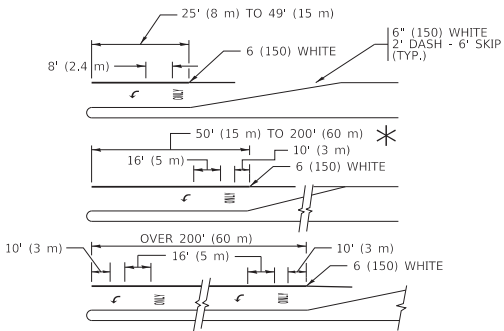


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



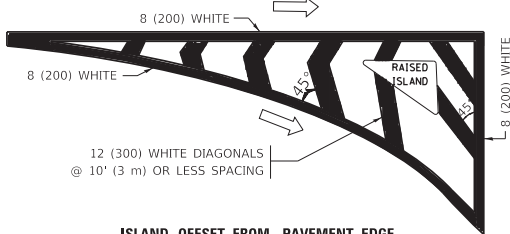
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.

\* AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

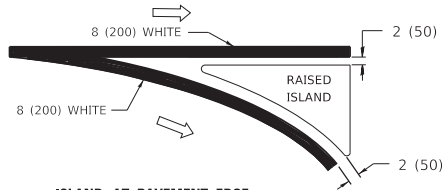
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

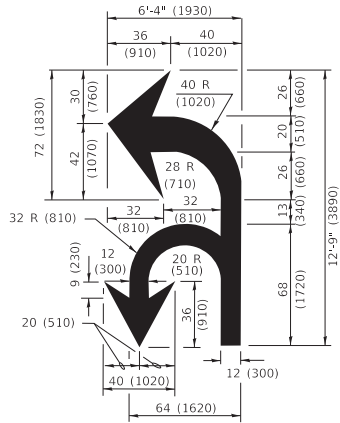


ISLAND OFFSET FROM PAVEMENT EDGE

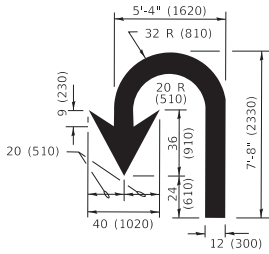


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION  
LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGTUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH *X*=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8' )	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO  
STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE  
CONSTRUCTION AND STATE STANDARD 780001.

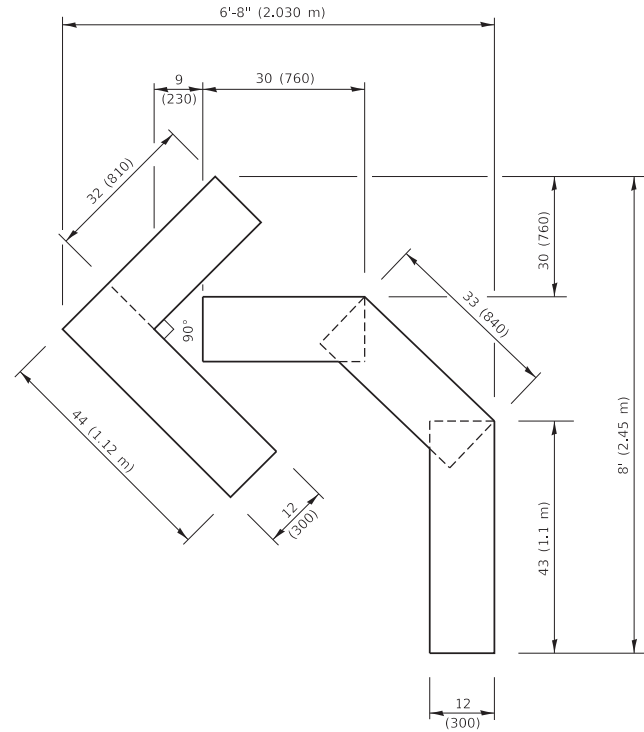
All dimensions are in inches (millimeters)  
unless otherwise shown.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

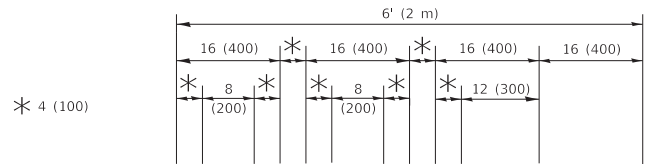
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-13		CONTRACT NO. 61L65		
		ILLINOIS FED. AID PROJECT		



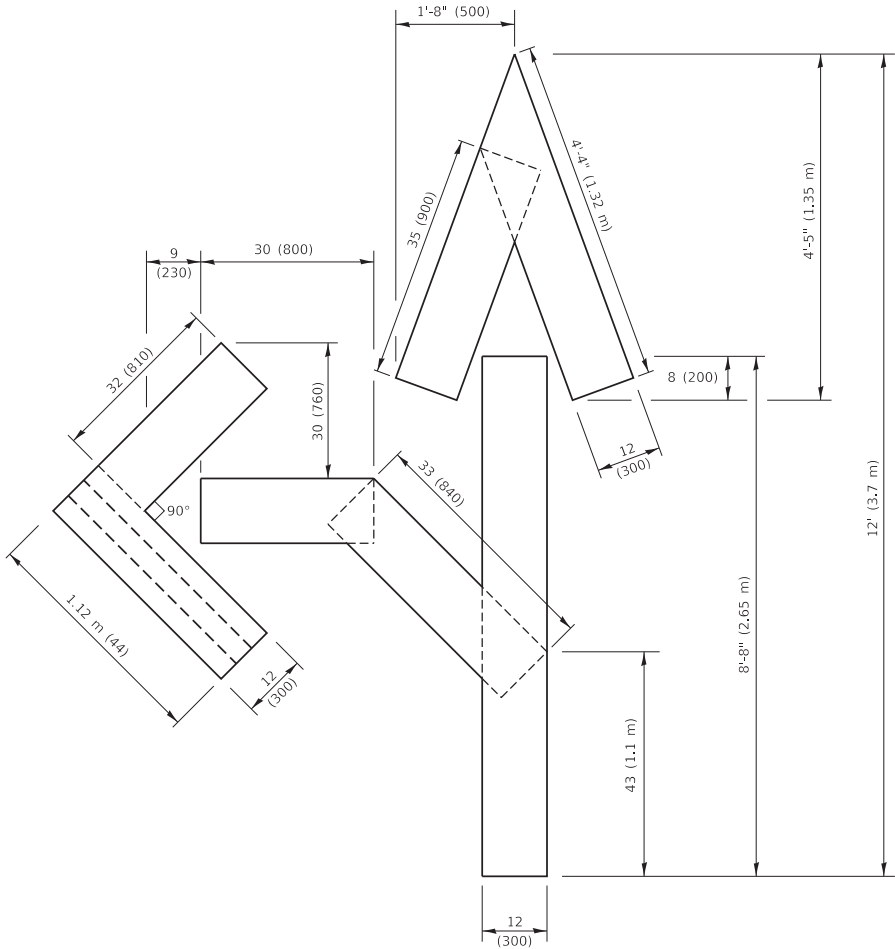
**QUANTITY**

4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.41 sq. m)



**QUANTITY**

4 (100) LINE = 64.1 ft. (19.5 m)  
21.4 sq. ft. (1.99 sq. m)

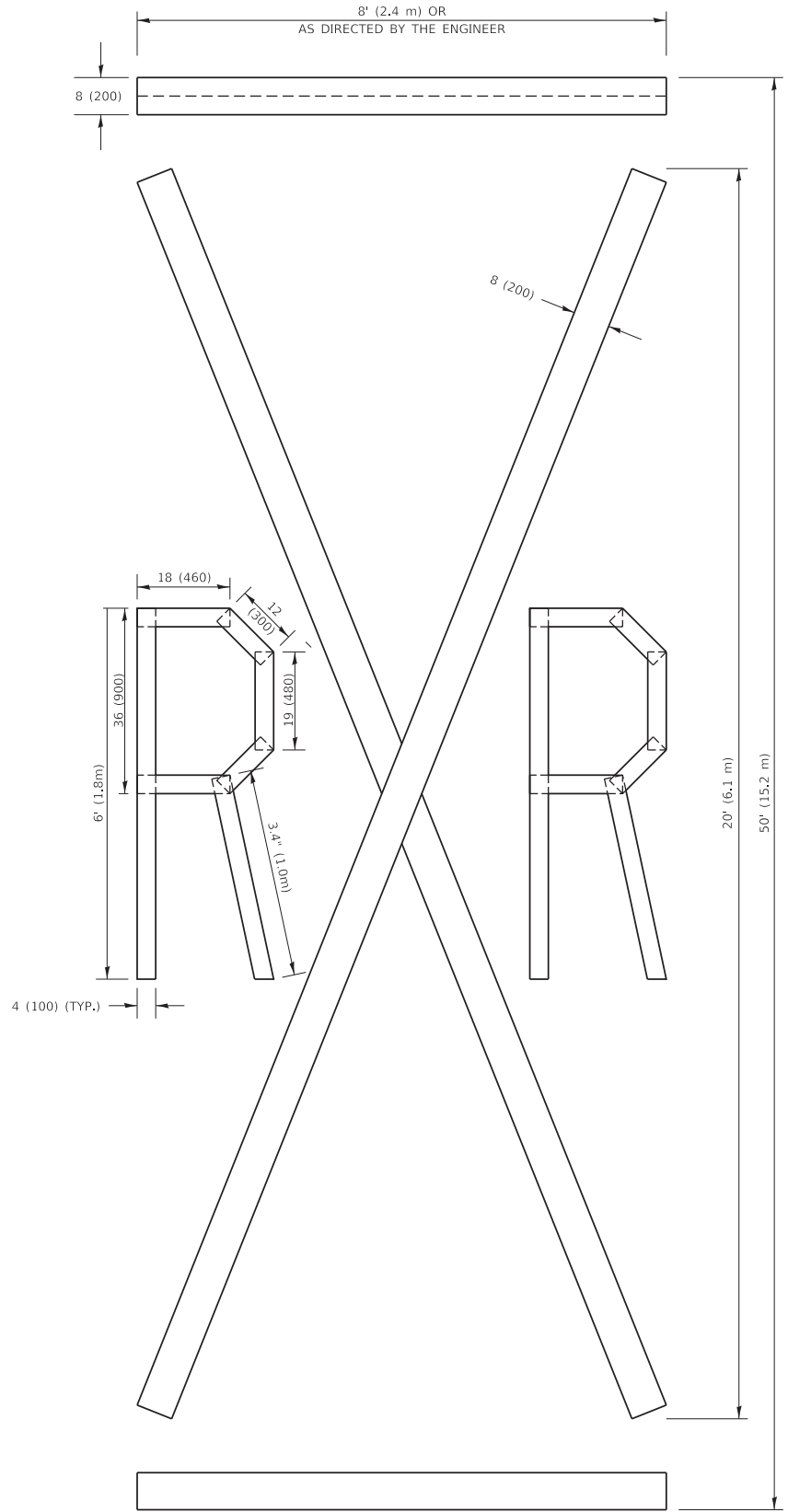


**QUANTITY**

4 (100) LINE = 82.5 ft. (25.1 m)  
27.5 sq. ft. (2.53 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED  
IN LINEAR FEET OF 4" LINES TO MATCH THE  
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS  
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**

4 (100) LINE = 225.9 ft. (68.9 m)  
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)  
unless otherwise shown.

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	24-00114-00-RS	COOK	24	23
TC-16		CONTRACT NO. 61L65		
		ILLINOIS	FED. AID PROJECT	

