

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
24	(44-3)BJR-1	JOHNSON	11	1
ILLINOIS CONTRACT NO. 78A96				

FOR INDEX OF SHEETS, SEE SHEET NO. 3  
FOR SUMMARY OF QUANTITIES, SEE SHEET NO. 4

**TRAFFIC DATA**  
EAGLE POINT BAY ROAD  
2021 ADT = 1150  
7% TRUCKS

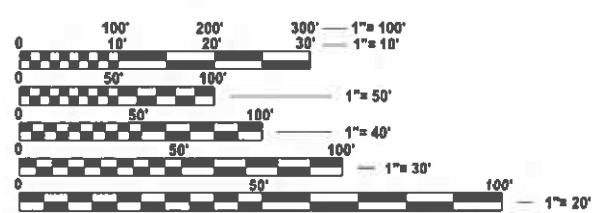
**TOWNSHIP**  
LAKE NO. 1 PRECINCT

**PROPOSED  
HIGHWAY PLANS**  
**FAI ROUTE 24 (I-24)  
SECTION (44-3)BJR-1  
BRIDGE JOINT RECONSTRUCTION  
JOHNSON COUNTY**

C-99-028-25

SN 044-0033  
EAGLE POINT BAY ROAD OVER I-24  
JOHNSON COUNTY

DESIGN DESIGNATION : N/A  
COORDINATE SYSTEM STATE PLAN ZONE: ILLINOIS EAST  
POSTED SPEED : 35 MPH

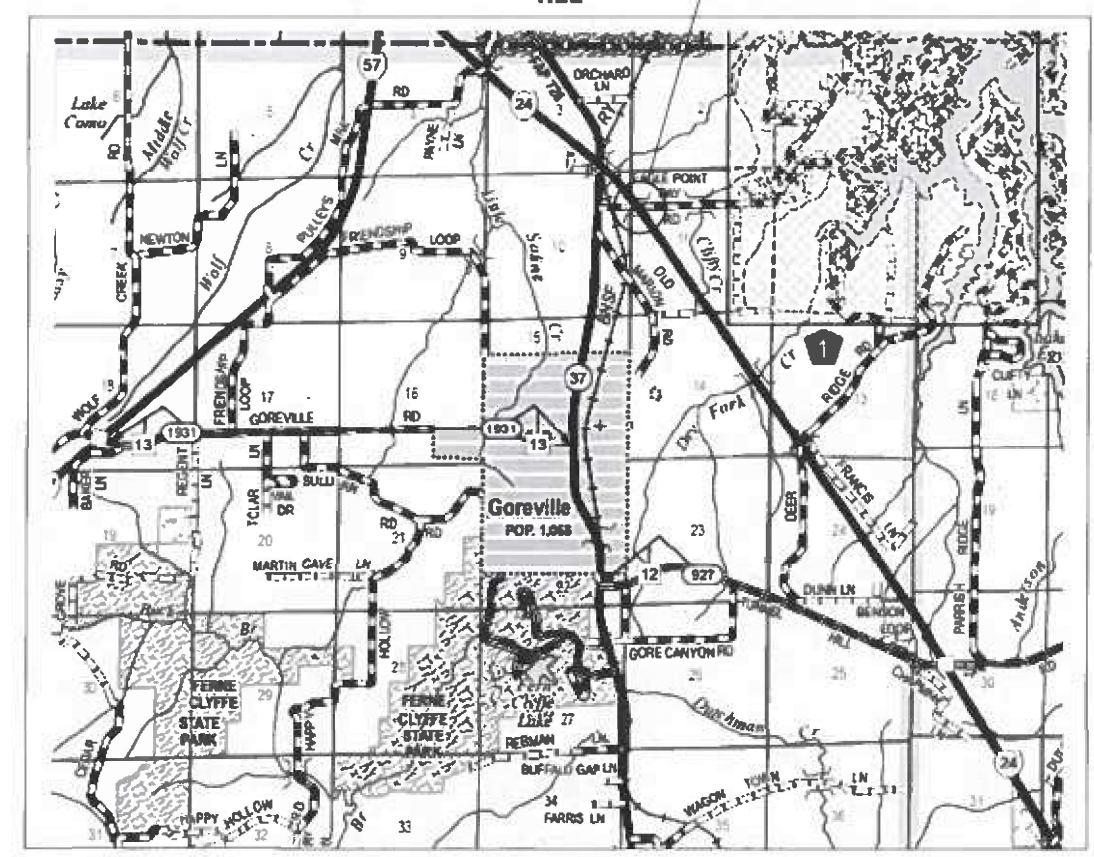


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER EHREN KIRBY  
PROJECT DESIGNER THOMAS JEFFERSON

CONTRACT NO. 78A96



GROSS LENGTH = 309.67 FT. = 0.059 MILE  
NET LENGTH = 309.67 FT. = 0.058 MILE



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 3 20 25  
Lisa S. Remington REGIONAL ENGINEER

May 9 20 25  
See A. E. K. ENGINEER OF DESIGN AND ENVIRONMENT

May 9 20 25  
Y. H. H. 5 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

MODEL: 02-Signature [Sheet]  
FILE NAME: c:\pwworking\illinois\gov\stephen.dillard\illinois\gov\1022228\09xxxx-SHA-Cover\_Sig.dgn

USER NAME	stephen.dillard	DESIGNED	-	REVISED	-
		DRAWN	-	REVISED	-
		CHECKED	-	REVISED	-
PLOT DATE	2/20/2025	DATE	-	REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SIGNATURES

SCALE: SHEET 2 OF 11 SHEETS STA. TO STA.

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
24	(44-3)BJR-1	JOHNSON	11	2
CONTRACT NO. 78A96				
ILLINOIS FED. AID PROJECT				

Prepared By: Susan R  
DISTRICT STUDIES & PLANS ENGINEER

Examined By: Nancy  
DISTRICT LAND ACQUISITION ENGINEER

Examined By: Carrie  
DISTRICT PROGRAM DEVELOPMENT ENGINEER

Examined By: R  
DISTRICT OPERATIONS ENGINEER

Examined By: \_\_\_\_\_  
DISTRICT PROJECT IMPLEMENTATION ENGINEER

Examined By: Brenda  
DISTRICT CONSTRUCTION ENGINEER

Examined By: Aam  
DISTRICT MATERIALS ENGINEER

MODEL: Untitled [Sheet]  
FILE NAME: c:\p\work\pwtol\jeffersons\1022228\09xxxx-SHB-Index\_GenNotes.dgn

GENERAL NOTES

NONE

COMMITMENTS

ALL EMERGENCY SERVICES INCLUDING POSTAL SERVICES IN  
JOHNSON AND WILLIAMSON COUNTY AND THE DISTRICT 9  
TRAFFIC CONTROL SUPERVISOR SHALL BE NOTIFIED THREE  
WEEKS PRIOR TO THE CLOSING OF EAGLE POINT BAY ROAD  
FOR THE BRIDGE REPAIRS OF SN 044-0033.  
THE ENGINEER SHALL NOTIFY BOTH THE JOHNSON AND  
WILLIAMSON COUNTY ENGINEERS THREE WEEKS PRIOR TO CLOSURE.  
BRANDON TANNER: 618-658-2741  
GEORGE SHEPARD: 618-998-2145

INDEX OF SHEETS

1	COVER SHEET
2	SIGNATURE SHEET
3	GENERAL NOTES, INDEX OF SHEETS AND STANDARDS
4	SUMMARY OF QUANTITIES
5	GENERAL PLAN AND ELEVATION SN 039-0074
6	JOINT RECONSTRUCTION DETAILS AT ABUTMENTS SN 039-0074
7	PREFORMED JOINT STRIP SEAL DETAILS
8	ROAD CLOSURE SIGN DETAIL
9-11	EXISTING PLANS

STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
701901-10	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
BLR 21-9	TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES FOR CONSTRUCTION ON RURAL LOCAL HIGHWAYS

	USER NAME = thomas.jefferson	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES, INDEX OF SHEETS AND STANDARDS			F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					24	(44-3)BJR-1	JOHNSON	11	3
		CHECKED -	REVISED -					CONTRACT NO. 78A96				
	PLOT DATE = 12/18/2024	DATE -	REVISED -		SCALE:	SHEET 3	OF 11 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	JOHNSON CO
			FAI 24
			100% STATE
			RURAL
			SN 044-0033
			0013
50102400	CONCRETE REMOVAL	CU YD	3.1
50300255	CONCRETE SUPERSTRUCTURE	CU YD	3.1
50300300	PROTECTIVE COAT	SQ YD	22
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1,260
52000110	PREFORMED JOINT STRIP SEAL	FOOT	74
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3
67100100	MOBILIZATION	L SUM	1
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	84
X7011800	TRAFFIC CONTROL AND PROTECTION, STANDARD BLR 21	L SUM	1
X7200203	DETOUR SIGNING	L SUM	1

MODEL: I24SQO [Sheet]  
FILE NAME: c:\p\work\pwrtd\illinois.gov\_stephen.dillard@illinois.gov\1022228D98xxx-ShtC-SqO.dgn



GENERAL NOTES

Reinforcement bars designated (E) shall be epoxy coated.

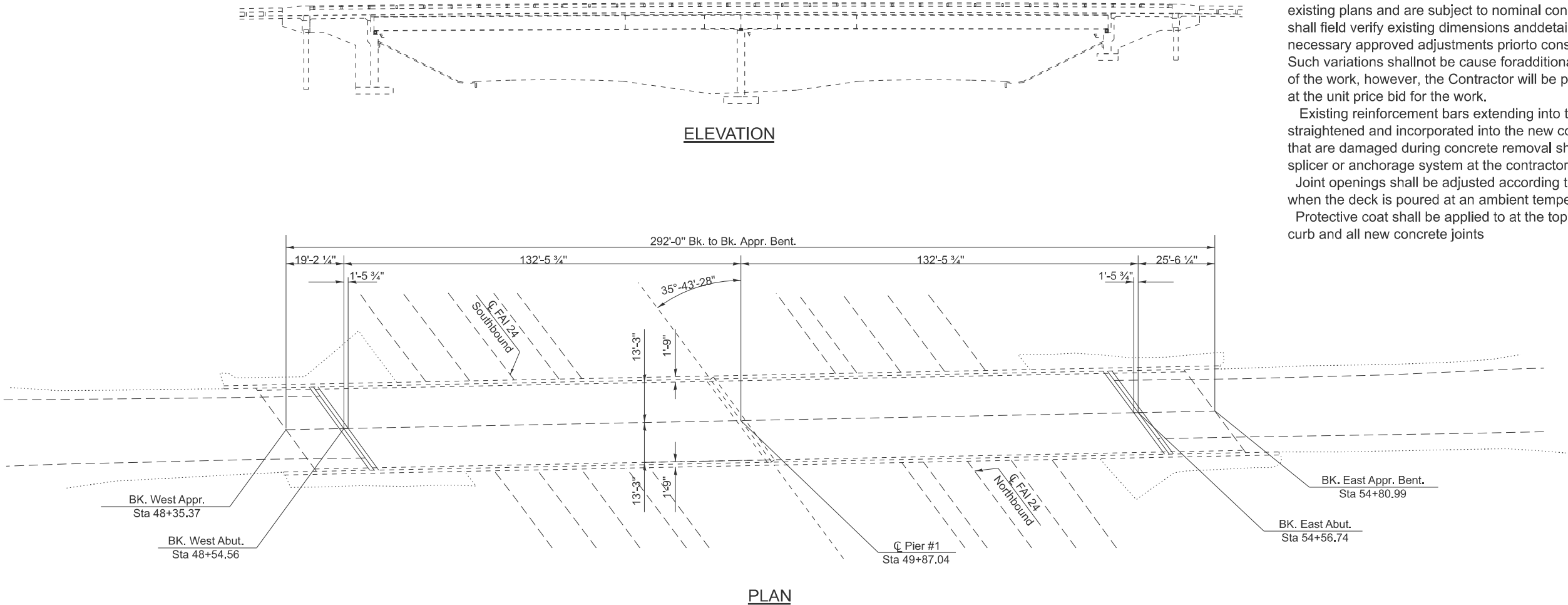
Prior to pouring the new concrete deck section, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

Plan dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system at the contractor's expense.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50°F.

Protective coat shall be applied to at the top and inside faces of all new concrete curb and all new concrete joints



TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	Cu. Yd.	7.3
Concrete Superstructure	Cu. Yd.	7.2
Reinforcement Bars, Epoxy Coated	Pound	1260
Preformed Joint Strip Seal	Foot	74

SCOPE OF WORK

Abutment Joint Reconstruction

DESIGN SPECIFICATIONS

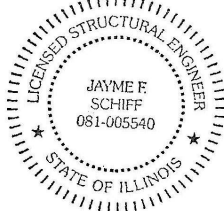
2002 AASHTO STANDARD  
SPECIFICATIONS FOR HIGHWAY BRIDGES

DESIGN STRESSES

FIELD UNITS (EXISTING CONSTRUCTION)  
DESIGN LOADING HS15-44  
f'c = 1,200 psi - Deck Slab  
fc = 1,400 psi - Curb, Parapet, Substructure  
f's = 20,000 psi (struct.)  
f's = 20,000 psi (reinf.)  
Vc = 75,000 psi (Ftgs)  
n = 10

NEW CONSTRUCTION

f'c = 4000 psi  
fy = 60000 psi



EXPIRES 11-30-2026

Jayme F. Schiff

BRIDGE JOINT RECONSTRUCTION  
EAGLE POINT BAY ROAD  
OVER I-24  
JOHNSON COUNTY  
SN 044-0033

MODEL: GENERAL PLAN AND ELEVATION-1 | Sheet1  
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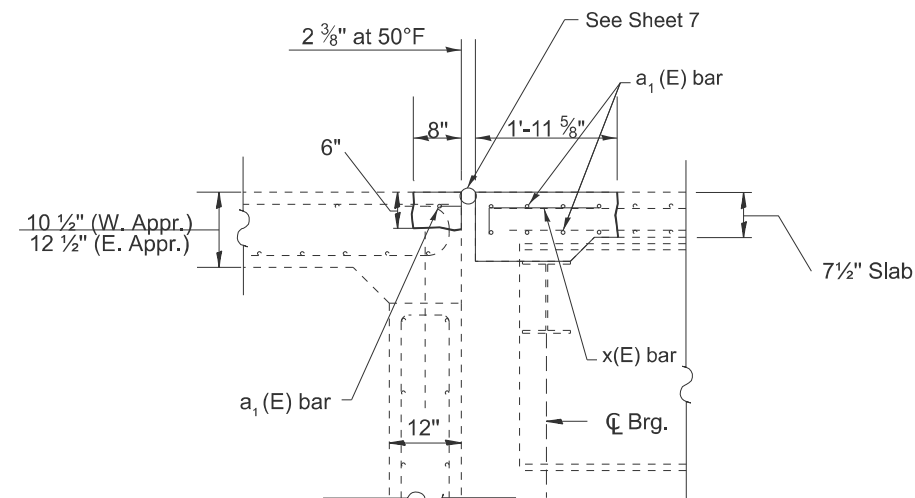
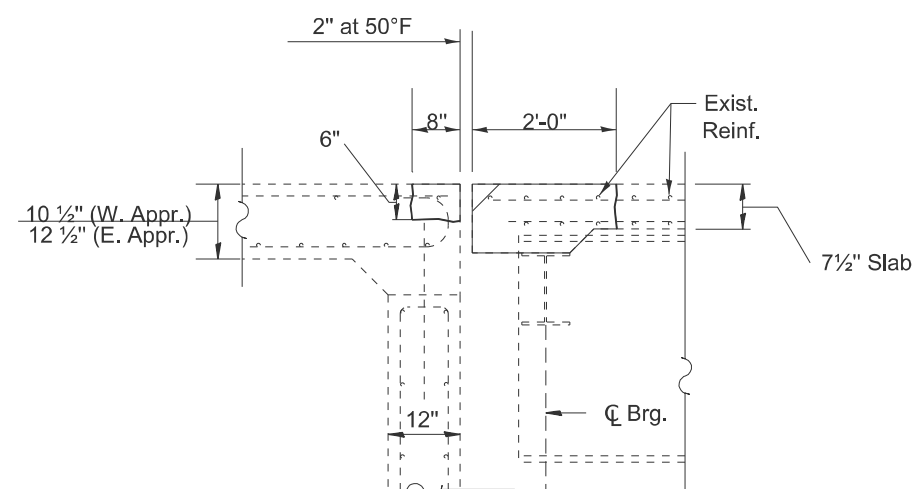
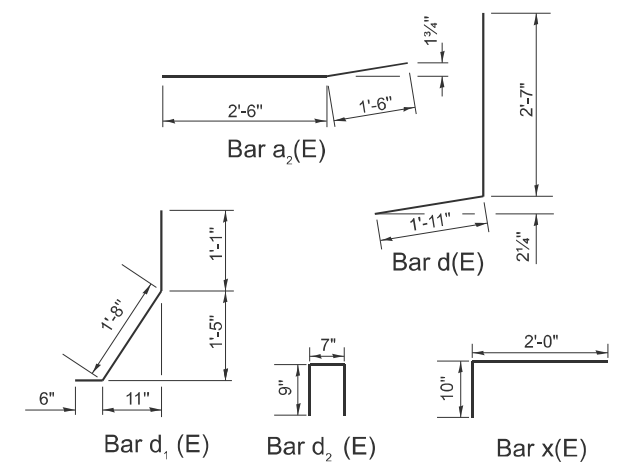
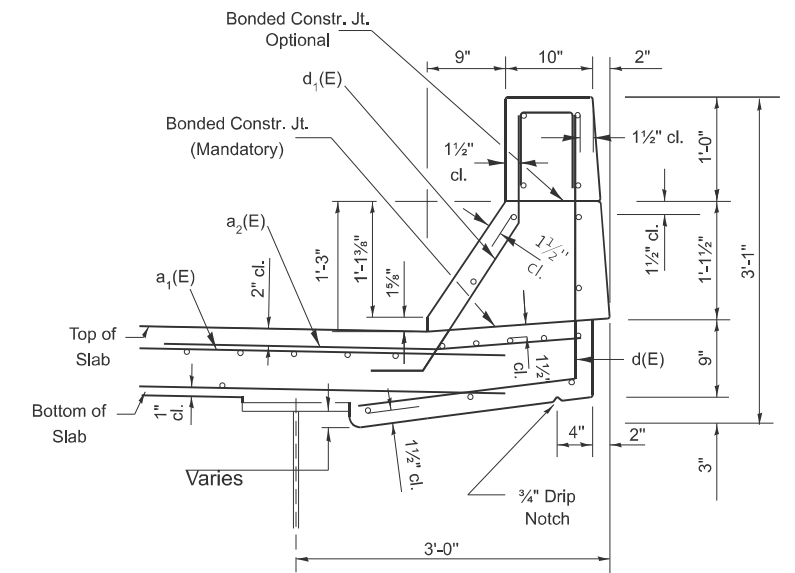
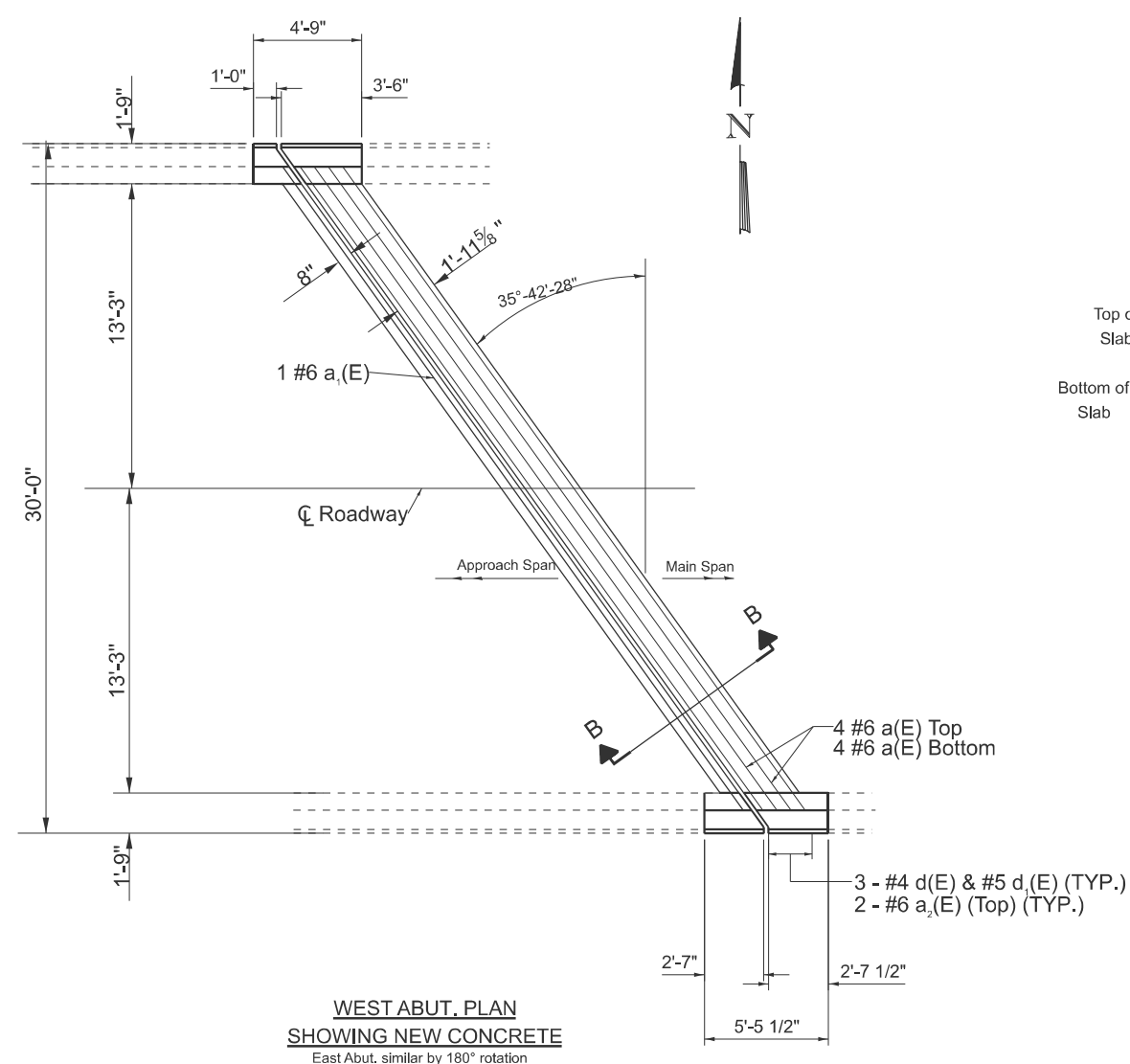
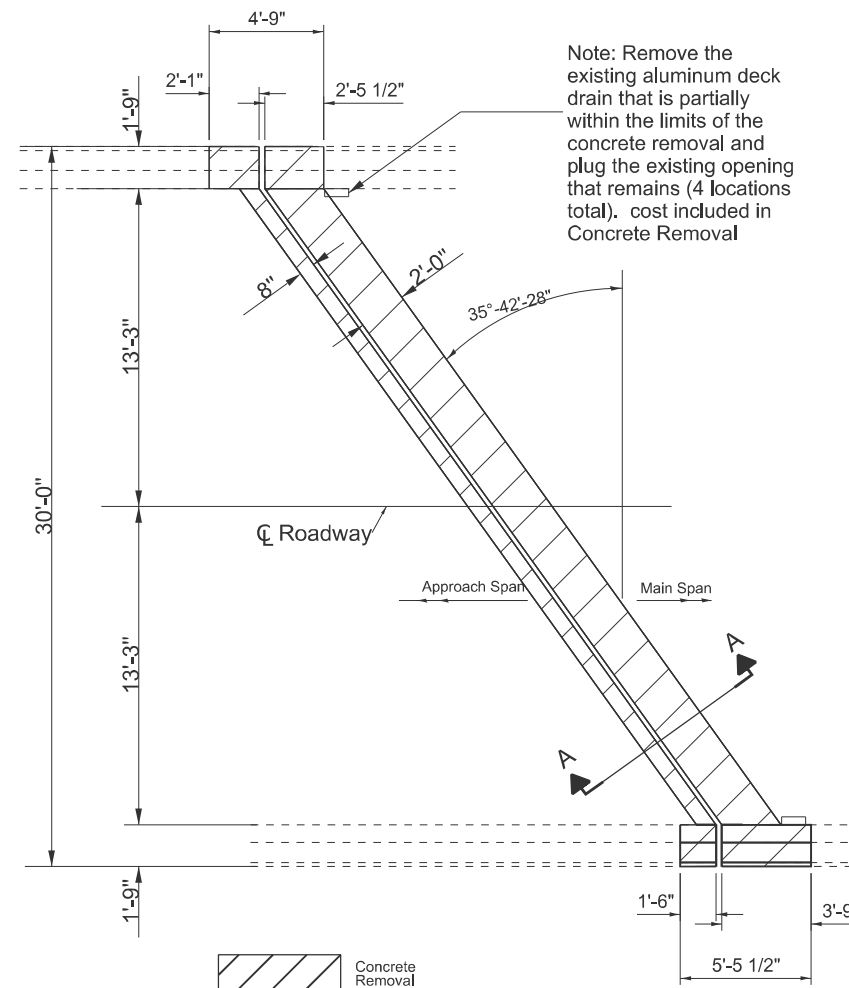
USER NAME = thomas.jefferson	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 4/8/2025	DATE -	REVISED -








STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION

SCALE: SHEET 5 OF 11 SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
24	(44-3)BJR-1	JOHNSON	11	5
CONTRACT NO. 78A96				
ILLINOIS FED. AID PROJECT				

BILL OF MATERIAL (2 ABUTMENTS)

Bar	No.	Size	Length	Shape
a(E)	16	#6	34'-6"	
a <sub>u</sub> (E)	2	#6	34'-6"	
a <sub>u</sub> (E)	8	#6	4'-0"	
d(E)	12	#4	4'-6"	
d <sub>u</sub> (E)	12	#5	3'-3"	
d <sub>u</sub> (E)	12	#4	2'-1"	
x(E)	60	#5	2'-10"	
Concrete Superstructure			Cu Yd	7.2
Concrete Removal			Cu Yd	7.3
Reinforcement Bars, Epoxy Coated			Pound	1260

**BRIDGE JOINT RECONSTRUCTION**  
**EAGLE POINT BAY ROAD**  
**OVER I-24**  
**JOHNSON COUNTY**  
**SN 044-0033**

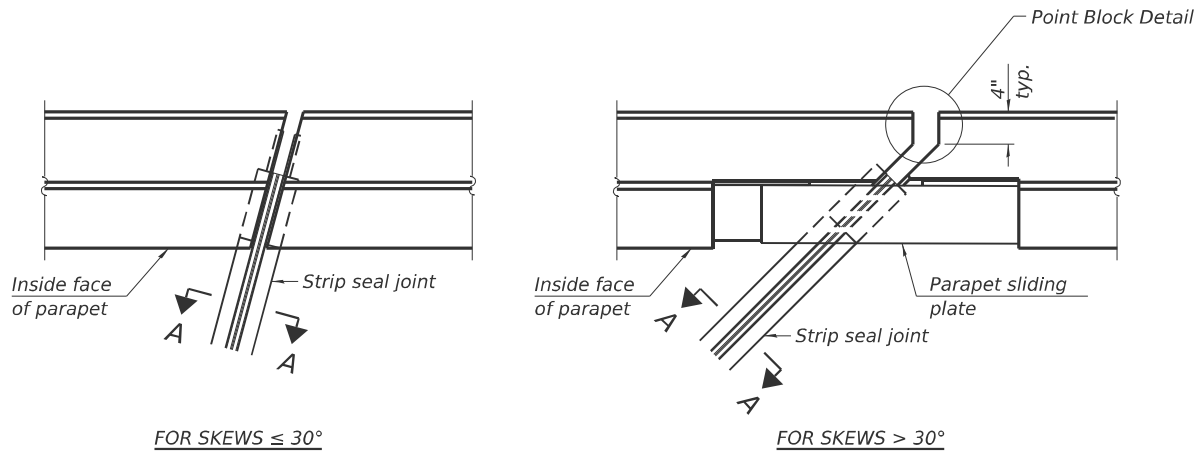
USER NAME = thomas.jefferson	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 4/8/2025	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

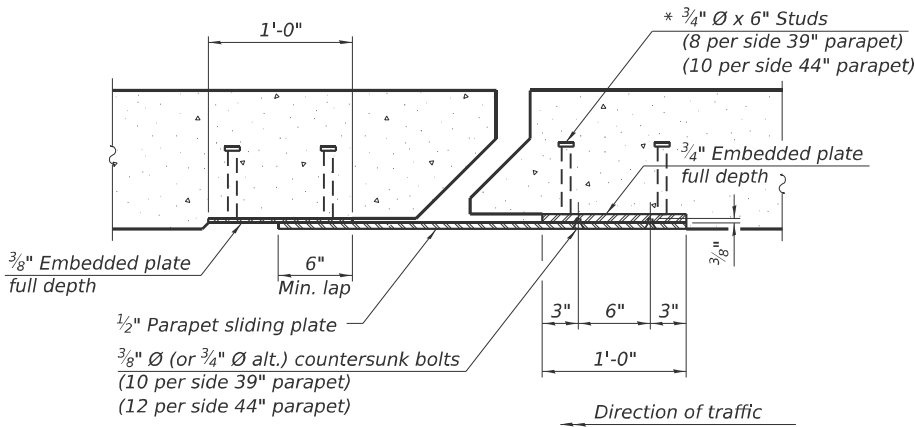
### JOINT RECONSTRUCTION DETAIL

SCALE:	SHEET 6	OF 11	SHEETS	STA.	TO STA.
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F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
24	(44-3)BJR-1	JOHNSON	11	6
CONTRACT NO. 78A96				
ILLINOIS		FED. AID PROJECT		

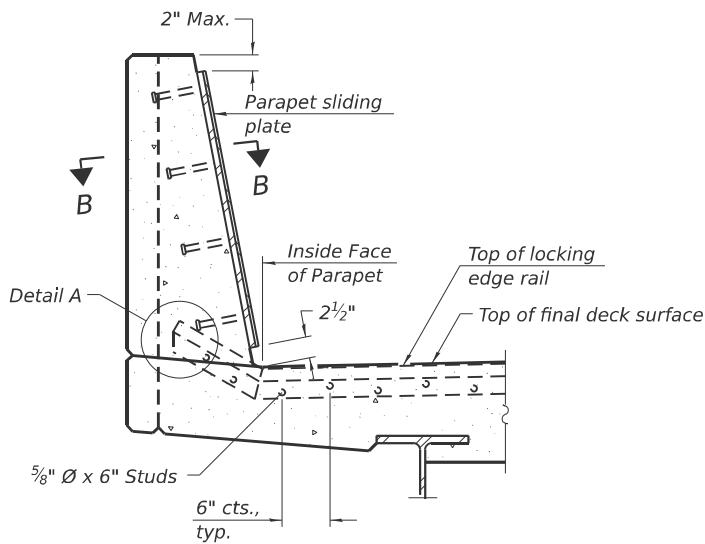


PLAN AT PARAPET



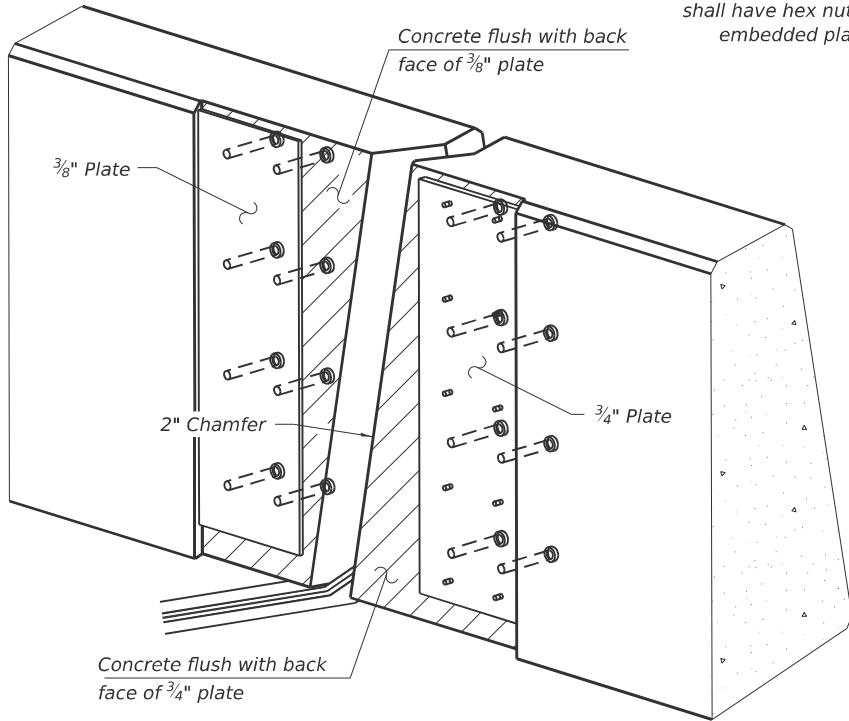
SECTION B-B

( $\frac{3}{4}$ "  $\varnothing$  countersunk bolts extending into concrete shall have hex nuts tack welded to the back of the embedded plates with end caps provided.)



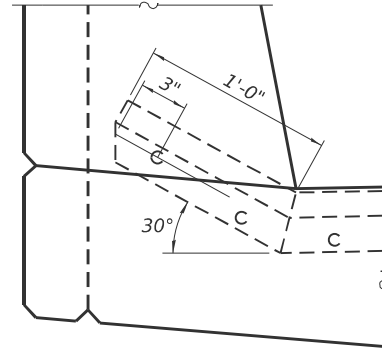
SECTION AT PARAPET

(Skews  $> 30^\circ$  shown. Skews  $\leq 30^\circ$  similar except as shown in plan view.)



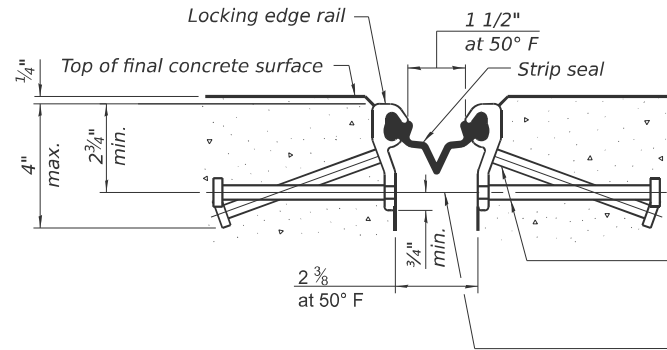
TRIMETRIC VIEW

(Showing embedded plates only)



DETAIL A

(Kick-up at parapet locations shown. See sheet of for kick-up at curb locations.)

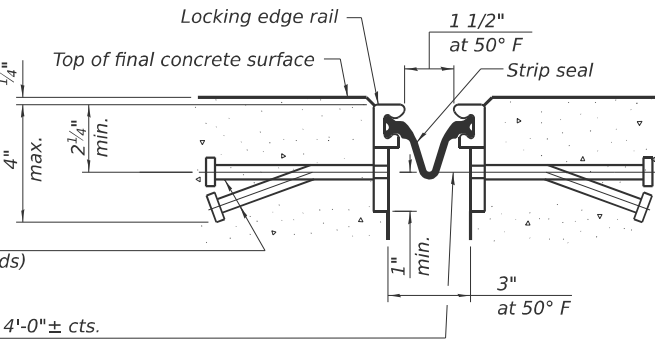


SHOWING ROLLED RAIL JOINT

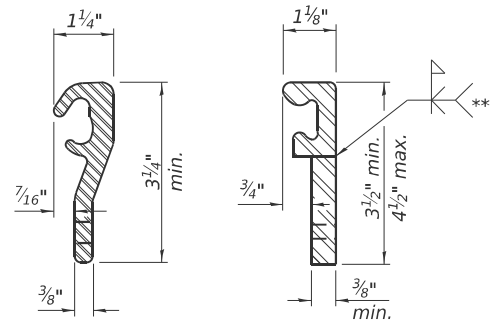
$\frac{3}{8}$ "  $\varnothing$  threaded rods in  $\frac{7}{16}$ "  $\varnothing$  holes at 4'-0"  $\pm$  cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

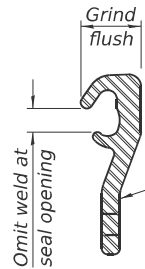


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

\*\* Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	74

EJ-SS

05-15-2023

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PREFORMED STRIP SEAL DETAIL

SCALE: SHEET 7 OF 11 SHEETS STA. TO STA.


F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
24	(44-3)BJR-1	JOHNSON	11	7
CONTRACT NO. 78A96				
ILLINOIS FED. AID PROJECT				

MODEL: EaglePointStripSeal [Sheet]  
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MODEL: IL37DETOUR-2 (Sheet)  
FILE NAME: c:\p\work\pilot\jeffersons\1022228\ZAZA1D9xxxx-SHE-SignDetails.dgn

SIGN DETAILS


1



W20-3(O)-36

2 REQUIRED



2



W20-3(O)-36

2 REQUIRED

3


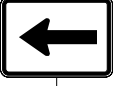
  


W20-3(O)-36

M6-1 R-2115

1 REQUIRED

4



  


W20-3(O)-36

M6-1 L-2115

1 REQUIRED

5



  


W20-3(O)-36

M5-1 R-2115

1 REQUIRED

6




  


W20-3(O)-36

M5-1 L-2115

1 REQUIRED

7




  
  


M4-8(O)-2412

M1-I100-3024

M06-3-2115

8




  
  


M4-8(O)-2412

M1-I100-3024

M01R-3-2115

9

M4-8(O)-2412

M1-I100-3024

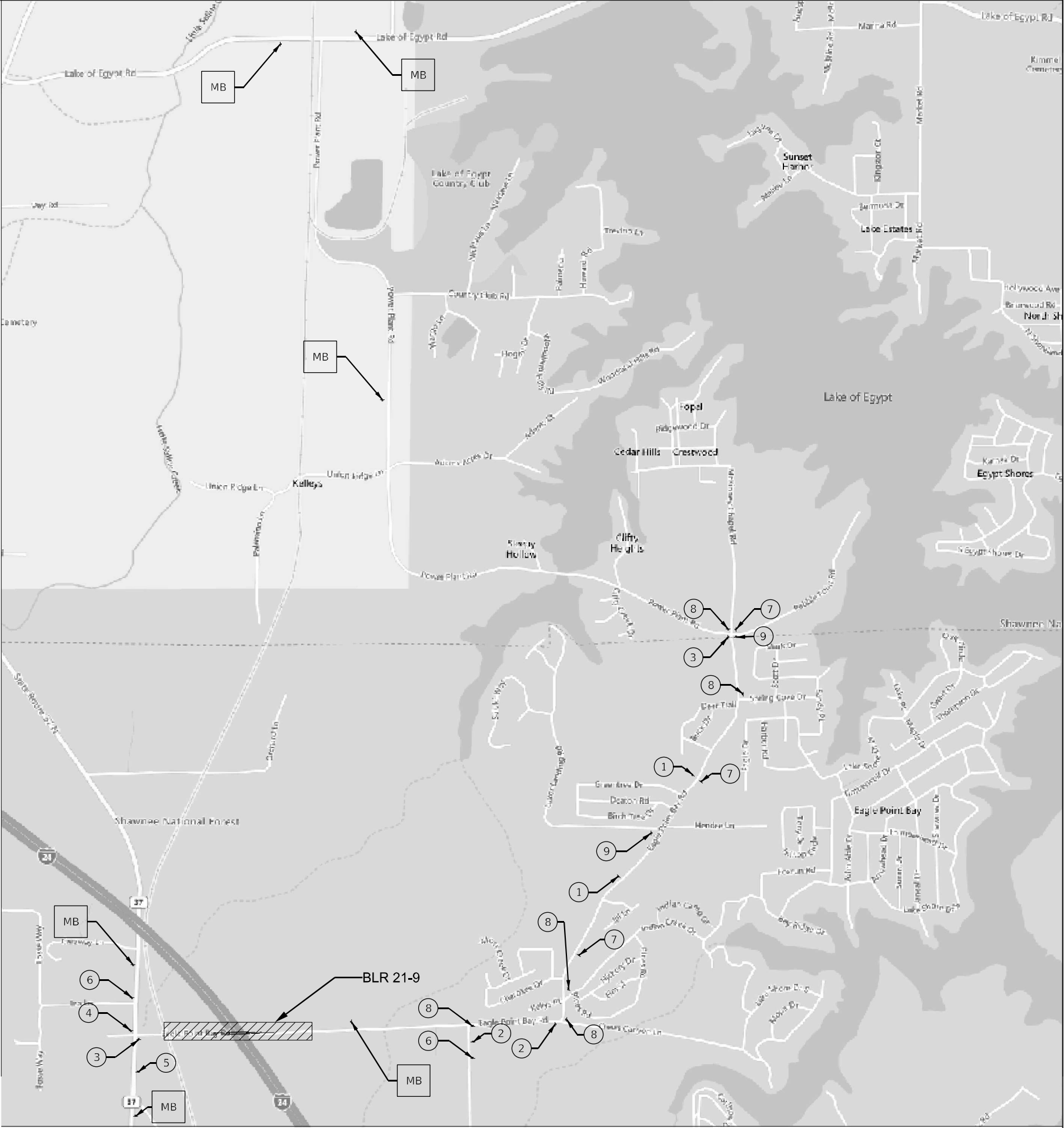
M01L-3-2115

MESSAGE BOARDS

MB

6 REQUIRED  
(2 WEEKS BEFORE CLOSURE)

- NOTES:
- THE CONTRACTOR HAS THE OPTION OF USING METAL POSTS INSTEAD OF WOOD.
  - ALL SIGNS SHALL BE BLACK ON ORANGE. THE POSTS SHALL BE PLACED 12 FEET FROM THE EDGE OF PAVEMENT AS DIRECTED BY THE ENGINEER.
  - ALL SIGNS AND HARDWARE REQUIRED TO INSTALL THE DETOUR ROUTE SIGNING SHALL BE INCLUDED IN COST OF DETOUR SIGNING.
  - SIGNS SHALL BE REMOVED ON COMPLETION OF THE CONTRACT AT THE DIRECTION OF THE ENGINEER. THIS WORK SHALL BE COMPLETED ACCORDING TO SECTION 724. THIS WORK WILL NOT BE PAID FOR. THE SIGNS AND POSTS SHALL BECOME THE PROPERTY OF THE CONTRACTOR.
  - THE RELOCATING, MAINTENANCE OR REPLACEMENT OF SIGNS SHALL BE PAID FOR IN ACCORDANCE WITH DETOUR SIGNING.
  - EXACT SIGN PLACEMENT WILL BE DETERMINED BY THE ENGINEER.
  - ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE ROAD CLOSURE IS IN EFFECT SHALL BE COMPLETELY COVERED BY THE CONTRACTOR.
  - THE DISTRICT 9 OPERATIONS ENGINEER SHALL BE CONTACTED 10 DAYS PRIOR TO THE ERECTION OF SIGNS.



	USER NAME	= thomas.jefferson	DESIGNED	-	REVISED	-
			DRAWN	-	REVISED	-
			CHECKED	-	REVISED	-
	PLOT DATE	= 1/22/2025	DATE	-	REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SIGN DETAIL

SCALE: SHEET 8 OF 11 SHEETS STA. TO STA.

FAJ RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
24	(44-3)BJR-1	JOHNSON	11	8
CONTRACT NO. 78A96				

ILLINOIS FED. AID PROJECT

3. M: P.S. spike in PR 77' at Sta. 225+36  
Elevation FAI 24 EL 677.29

## FOR INFORMATIONAL USE ONLY

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
44-3HB	-3	JOHNSON	76	16
SHEET NO. 1				
12 SHEETS				

### GENERAL NOTES

All reinforcement bars shall be lapped 24 diameters unless otherwise shown.

Field connections shall be bolted using high strength bolts. Bolts  $\frac{3}{4}$ " open holes  $\frac{1}{4}$ ", unless otherwise noted.

Calculated weight of Structural Steel = 264,500 lbs.

Cast steel shall be Class 70 Structural steel weldments of equal sections and meeting A.S.T.M. A-36, may be substituted for castings at the option of the Contractor, subject to approval by the Engineer prior to fabrication. No additional compensation will be allowed the Contractor for this substitution.

The Basic Lead Silico Chromate paint system shall be used for shop and field painting of structural steel.

Field welding of construction accessories will not be permitted to the bottom flange of beams or girders nor to the top flange for a distance equal to one-fourth the span length each way from the pier supports. Field welding in other areas will be permitted only when approved by the Engineer.

Anchor bolts shall be set before bolting cross frames over supports.

Slope wall shall be reinforced with welded wire fabric 6"x6" mesh, weighing 58# per 100 sq. ft.

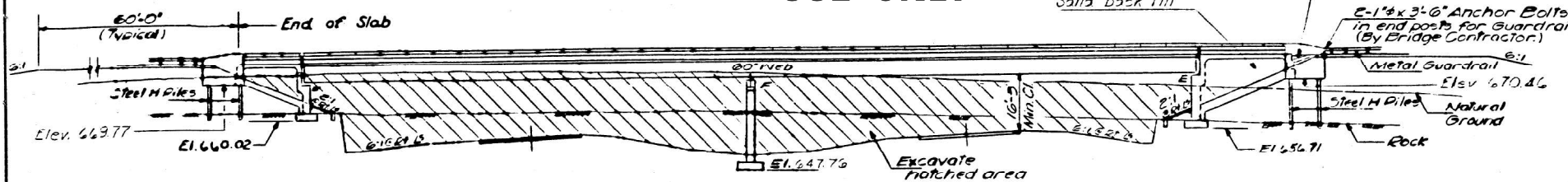
The embankment configuration shown shall be the minimum embankment that must be constructed prior to construction of the abutments.

The concrete rail section above the mandatory construction joint at the top of the slab shall be constructed of Class X Concrete, except the aggregates shall conform to the requirements of Handrail Concrete.

The Contractor shall drive one Steel Test Pile in a permanent location at East Appr. Bent as directed by the Engineer before ordering the remainder of piles.

### TOTAL BILL OF MATERIAL

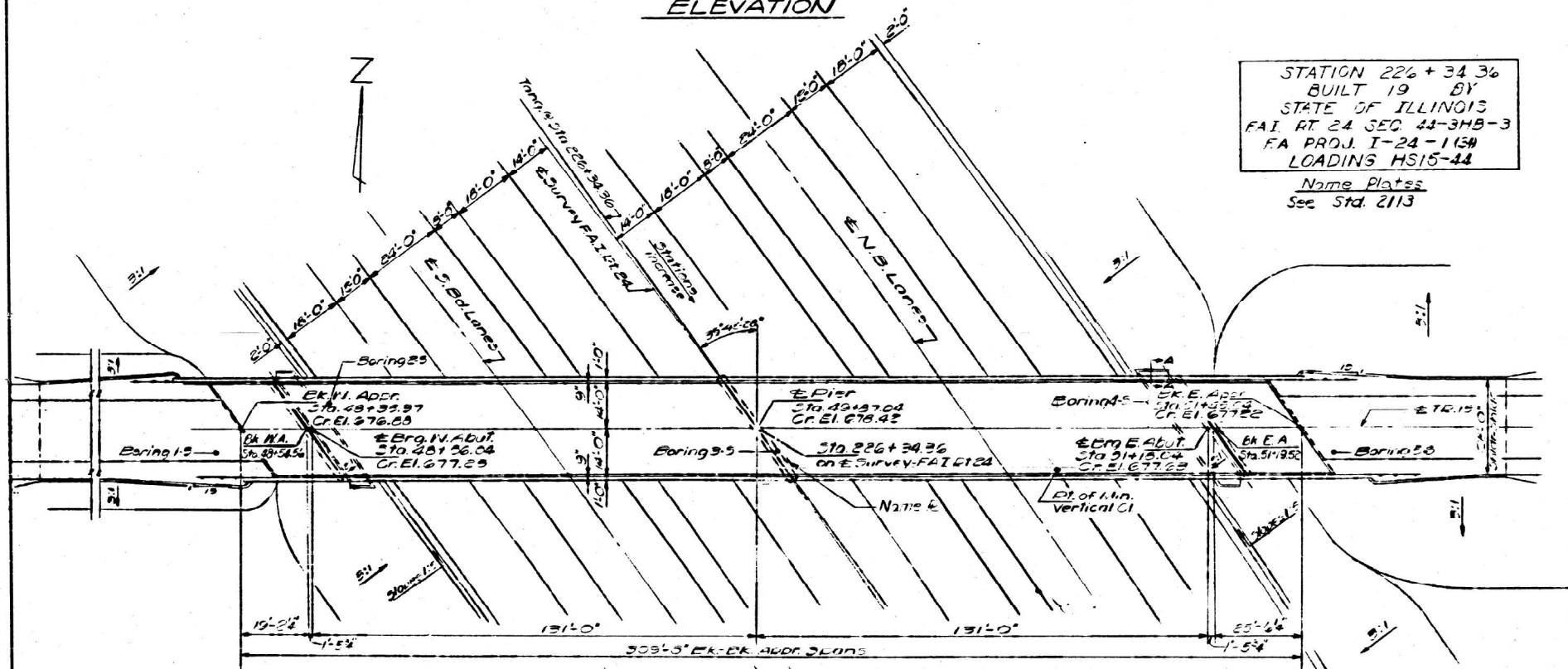
Item	Unit	Super	Sub	Total
Rock Excavation for Structures	Cu. Yds		150	150
STRUCTURE EXCAVATION	Cu. Yds		160	160
Class X Concrete	Cu. Yds	2993	2593	5586
Protective Coat	Sq. Yds	1150		1150
Structural Steel	L.S.	L.S.		L.S.
Stud Shear Connectors	Each	1260		1260
Reinforcement Bars	Lbs.	77,170	29,920	107,090
Steel Piles (HPB30)	Lin Ft		150	150
Test Piles Steel (HPB30)	Each		1	1
Name Plates	Each		1	1
Slope Wall (4')	Sq. Yds		95	95
Aluminum Siding	Lin Ft	607		607
Neoprene Expansion Jt. (2')	Lin Ft	71		71
Sand Back Fill	Cu. Yds		250	250



### ELEVATION

STATION 226 + 34.36  
BUILT 19 BY  
STATE OF ILLINOIS  
FAI RT 24 SEC. 44-3HB-3  
FA PROJ. I-24-115H  
LOADING HS15-44

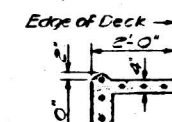
Name Plates  
See Std. 2113



### FAI RTE 24 CURVE DATA

$\Delta = 16^\circ 12' 02''$   
 $D = 20^\circ 30' 00''$   
 $R = 11,459.156'$   
 $T = 1335.52'$   
 $L = 3940.11'$   
 $E = 146.07'$   
 $PI = 226+34.36$   
 $PC = 226+34.36$   
 $PT = 226+34.36$   
 $Sta. 226+34.36$   
 $Sta. 226+34.36$   
 $Sta. 226+34.36$

### PLAN



### SEC. A-A

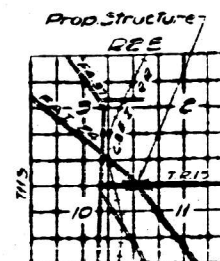
### PROFILE-TR 15

### DESIGN STRESSES

$F_c = 1200 \text{ psi. Deck Slab}$   
 $F_c = 1400 \text{ psi. Curb, Parapet, Sub}$   
 $F_s = 20,000 \text{ psi. Reinf.}$   
 $F_s = 20,000 \text{ psi. Struct.}$   
 $F_s = 15 \text{ psi. Figs.}$   
 $n = 12$

Allowable Fut. W.S. 25' 0"  
Allowable 4.0 4/100 Comp.

LOADING HS15-44



PROJ. I-24-115H  
T. 213 OVER FAI RT. 24  
FAI ROUTE 24  
SECTION 44-3HB-3  
JOHNSON COUNTY  
STATION 226+34.36

DESIGNED	Hand Singh
CHECKED	Hand Singh
DRAWN	G.E. Wilkins
CHECKED	A.K.

EXAMINED	Hand Singh
PASSED	Hand Singh
APPROVED	Hand Singh

### SECTION AT SLOPE WALL

### PROFILE-FAI RT. 24

Rev 7-8-71 WH

USER NAME = stephen.dillard	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 2/20/2025	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

### EXISTING PLANS

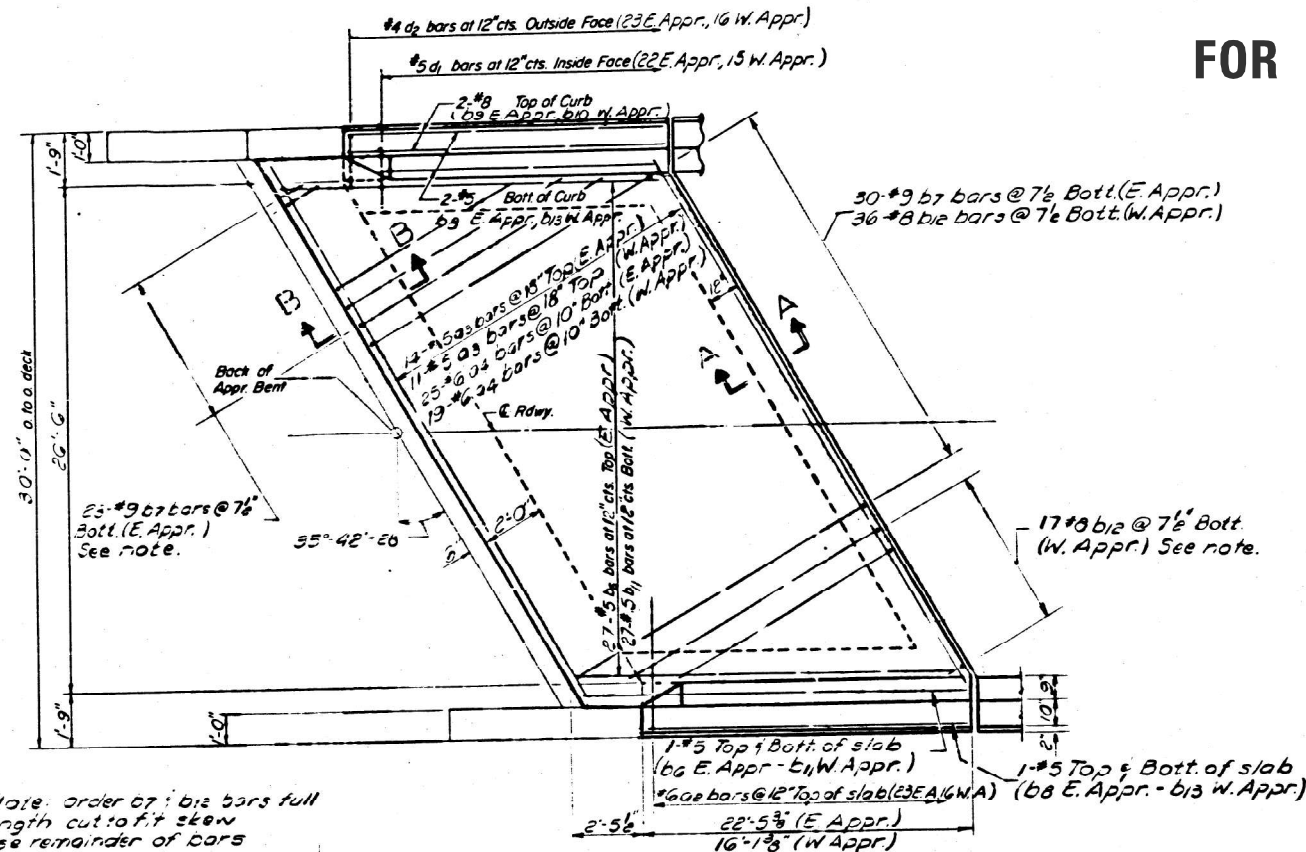
SCALE: SHEET 9 OF 11 SHEETS STA. TO STA.

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
24	(44-3)BJR-1	JOHNSON	11	9
CONTRACT NO. 78A96				
ILLINOIS FED. AID PROJECT				



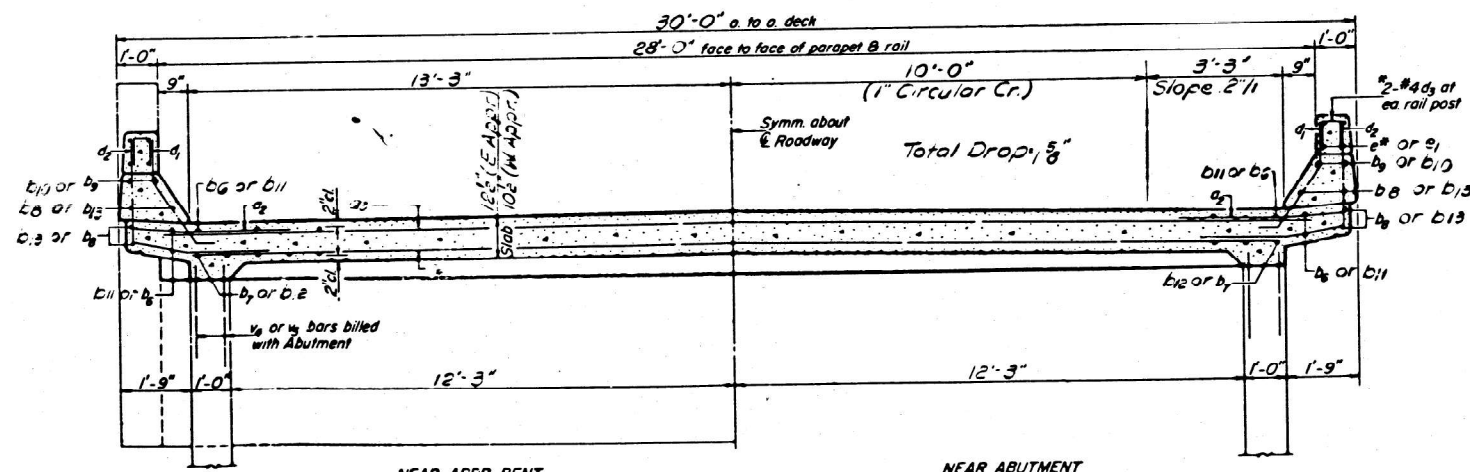
**SECTION B-B**

**SECTION A-A**



Note: order 67; big bars full length cut off skew use remainder of bars in opposite end.

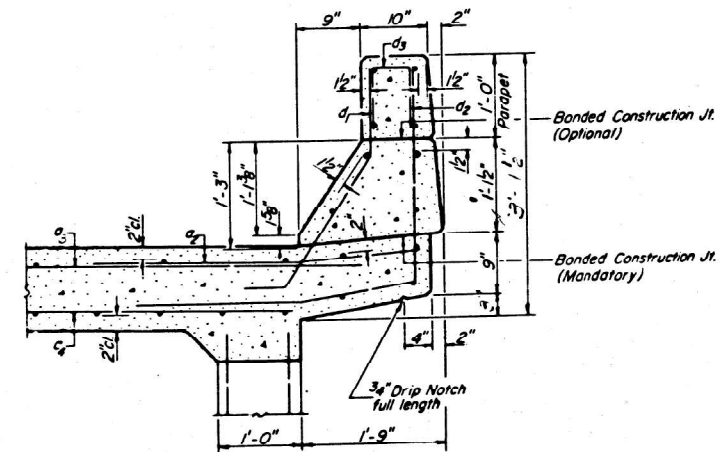
PLAN



NEAR APPR. BENT

**CROSS SECTION**

NEAR ABUTMENT



**CURB SECTION**

TWO APPR. SLABS  
BILL OF MATERIAL

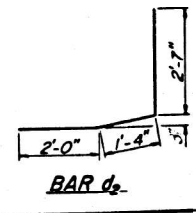
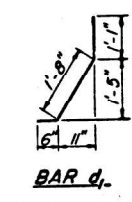
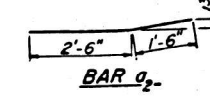
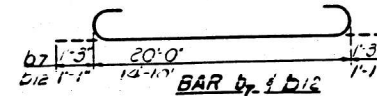
Bar	No	Size	Length	Shape
a2	79	#6	4'-0"	—
a9	25	#5	34'-3"	—
a6	44	#6	32'-3"	—
b6	31	#5	34'-6"	—
b7	53	#9	22'-6"	U
b8	8	#5	22'-0"	—
b9	6	#8	22'-0"	—
d10	4	#9	15'-9"	—
d11	27	#5	18'-3"	—
d12	53	#8	17'-0"	—
d13	8	#5	15'-3"	—
d1	74	#5	3'-3"	J
d2	75	#4	5'-1"	J
Reinforcement Bars			Lbs	125.40
Class X Concrete			Cu Yds.	53.3

\*Parapet Reinforcement and Class X Concrete  
are billed on sheet # 3

DESIGNED	Harold Single
CHECKED	A. Karamat
DRAWN	F. Mercado
CHECKED	A. K.

SAS-R ( $\approx 30^\circ$ ) 3.1-69

June 26 1969  
 EXAMINED Carol E. Thurner  
CHIEF, LABORATORY OF FORENSIC CHEMISTRY  
 PASSED H.E. Benner  
LABORATORY OF TOXICOLOGY  
 APPROVED Richard H. Holtzman

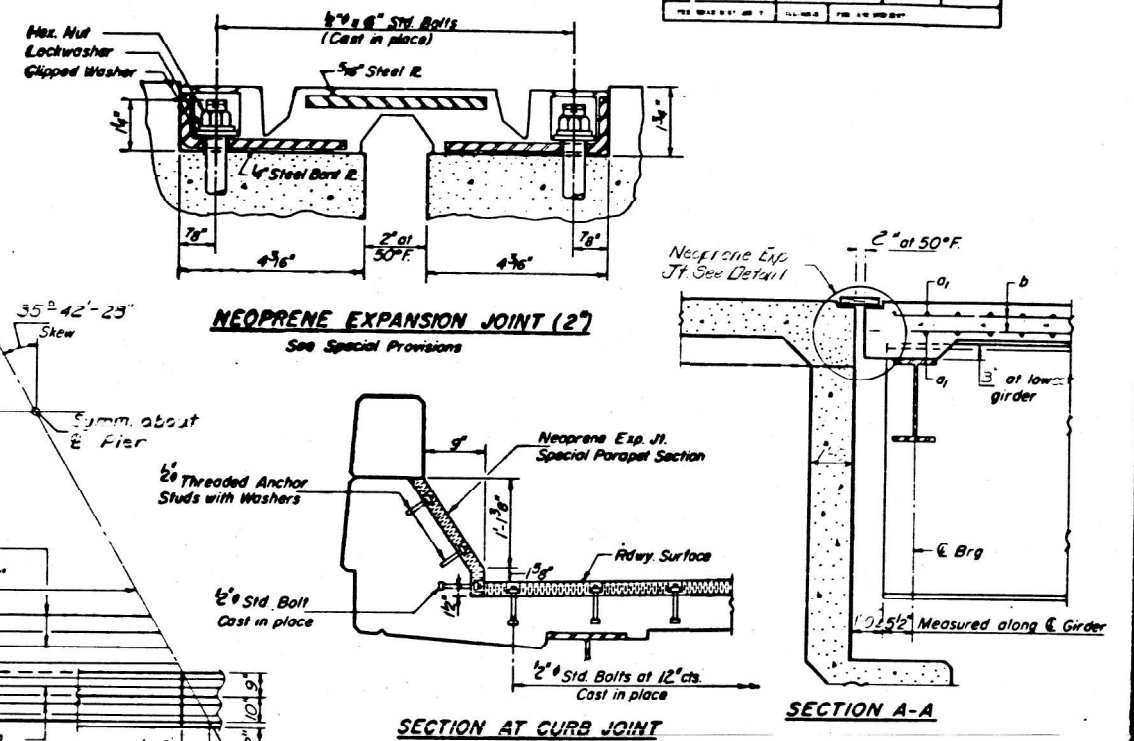


SUPERSTRUCTURE  
F.A.I.R.T.E. 24 SEC 443HB-3  
JOHNSON COUNTY  
STA 226+34.56



QUART NO	SECTION	LOCALITY	TOTAL SHEETS	SHEET NO
801	44-3RB	JOHNSON	76	19
VAL 24	-3			
75° 34' 30" N 101° 10' 00" W		75° 34' 30" N 101° 10' 00" W		

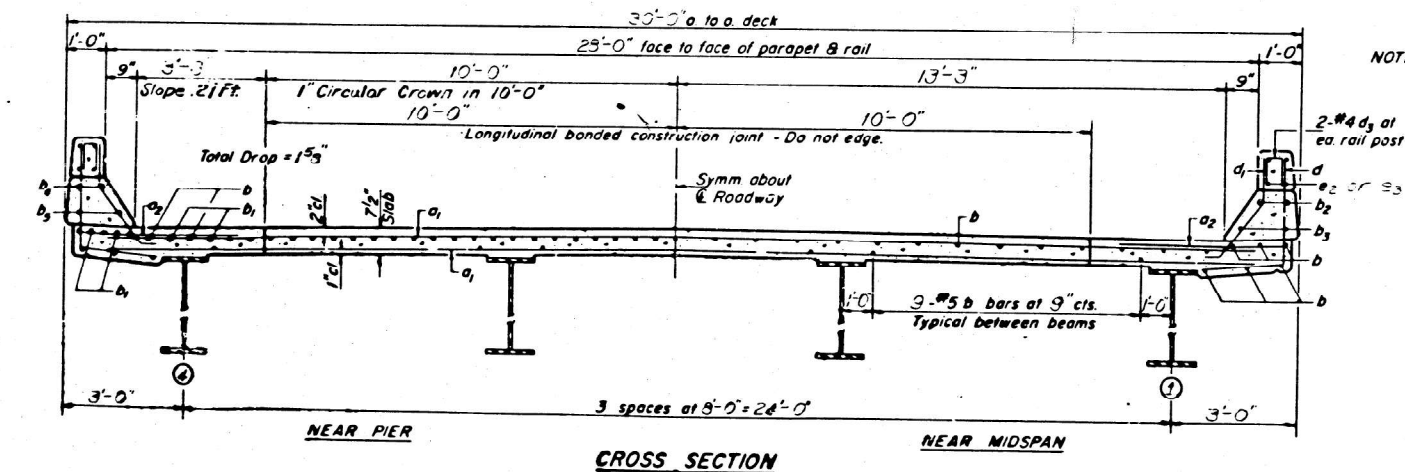
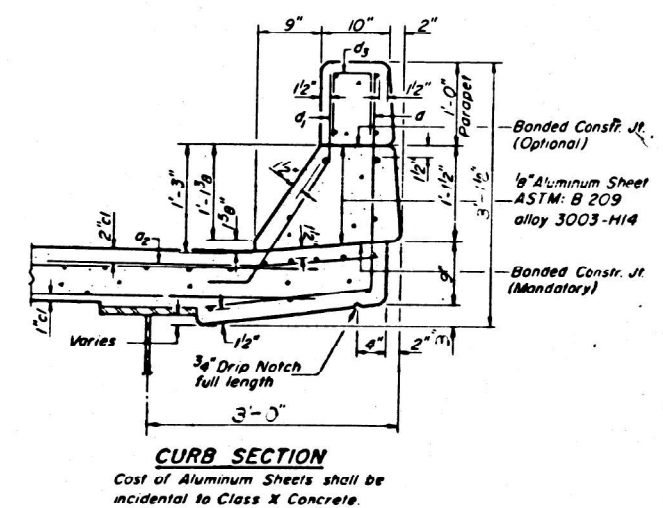
SHEET NO. 4  
12 SHEETS



<b>BILL OF MATERIAL</b>				
<b>B3r</b>	<b>No</b>	<b>Size</b>	<b>Length</b>	<b>Shape</b>
<b>a<sub>1</sub></b>	743	#6	28'-0"	————
<b>a<sub>2</sub></b>	374	#6	4'-0"	————
<b>b</b>	462	#5	39'-0"	————
<b>b<sub>1</sub></b>	90	#6	21'-6"	————
<b>b<sub>2</sub></b>	24	#8	37'-6"	————
<b>b<sub>3</sub></b>	24	#5	37'-0"	————
<b>b<sub>4</sub></b>	8	#8	24'-0"	————
<b>b<sub>5</sub></b>	3	#5	24'-0"	————
<b>d</b>	530	#4	2'-0"	J
<b>d<sub>1</sub></b>	530	#5	3'-3"	J
<b>Reinforcement Bars</b>			<b>Lbs</b>	<b>62840</b>
<b>Class X Concrete</b>			<b>Cu Yds</b>	<b>2258</b>
<b>Structural Steel</b>			<b>Lbs</b>	<b>264,300</b>

\* Weight of bearing assemblies with lead plates and anchor bolts are included as Structural Steel. WT = 5062 LBS

Parapet Reinforcement and Class X  
Concrete are billed on sheet #5



NOTE: For placement of bars  $d_3$  and  $e_2$  thru  $e_3$  see sheet # 8

DESIGNED	Harold Singh
CHECKED	A. K. Kaur
DRAWN	C. E. Williams
CHECKED	A. K.

June 26 1969

REASSEMBLED

PAID

APPROVED

5-4-R (230) - 22-68

**Aluminum Sheets Welded**  
ASTM: B209 alloy 6061-T3  
or Aluminum Extrusions  
ASTM: B221 alloy 6061-T6

**FLOOR DRAIN**  
Cons. incidental to Class X Concrete

1/2" x 1-1/2" Aluminum bar  
ASTM: 2024 alloy 6061-T6

3/16" Typ.

12"

30°

Fill slot with weld