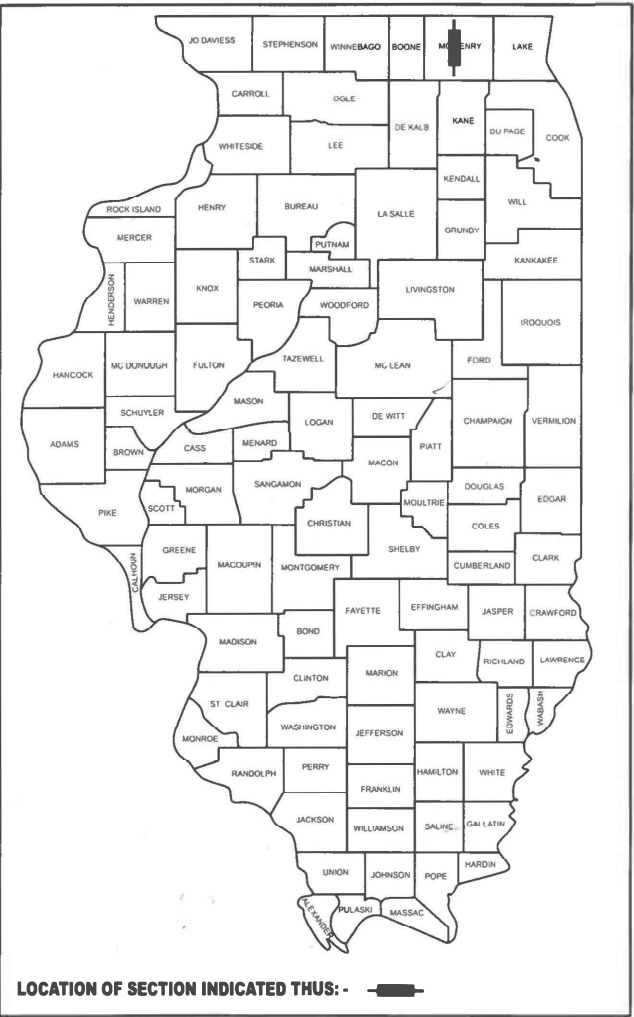


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	1
		ILLINOIS	CONTRACT NO. 62L61	

D-91-482-20



FOR INDEX OF SHEETS, SEE SHEET NO. 2

LOCATED IN THE CITY OF WOODSTOCK

TRAFFIC DATA:

IL 47 (EASTWOOD DR)

DESIGN CLASSIFICATION = OTHER PRINCIPAL ARTERIAL

POSTED SPEED = 40 - 55 MPH

2023 AADT = 14,800

FAP ROUTE 326: IL 47 (EASTWOOD DR)
NORTH OF IL 176 (NORTH JUNCTION) TO SOUTH OF US 14
SECTION: 2020-083-RS
STANDARD OVERLAY AND HMA SHOULDERS
MCHENRY COUNTY

C-91-280-20

R.7E

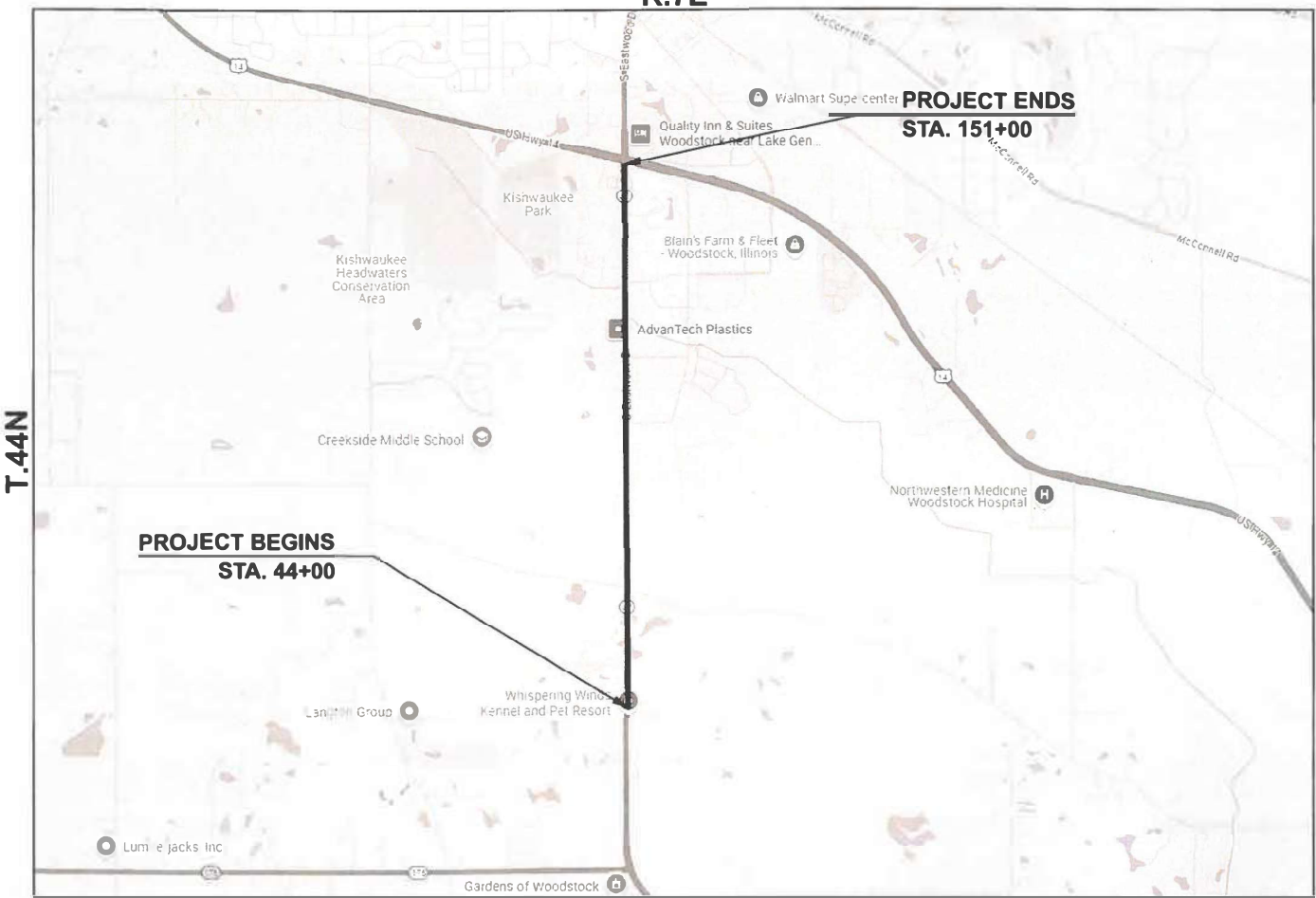


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: LUKASZ POCIECHA, PE
PROJECT MANAGER: VESELIN VELICHKOV, PE

CONTRACT NO. 62L61



WOODSTOCK TOWNSHIP

GROSS / NET LENGTH = 10,700 FT. = 2.02 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 25th 2025 IR
REGIONAL ENGINEER
May 9 2025
ENGINEER OF DESIGN AND ENVIRONMENT
May 9 2025
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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DISTRICT ONE STANDARDS

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HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
482011-03	HMA SHLD. STRIPS / SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
630001-13	STEEL PLATE BEAM GUARDRAIL
642006-01	SHOULDER RUMBLE STRIPS, 8 IN
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS 2L, 2W, DAY ONLY
701201-05	LANE CLOSURE, 2L 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS ≥ 45 MPH
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701336-07	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES FOR SPEEDS ≥ 45 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≥ 45 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-10	TRAFFIC CONTROL DEVICES

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ALL MILLED SURFACES SHALL BE A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
- ALL PAVEMENT PATCHING LOCATIONS SHALL BE DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION.
- PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT 1 TYPICAL PAVEMENT MARKINGS.
- PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKING ON ALL FINAL SURFACES.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- BUTT JOINTS SHALL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS".
- THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR SHALL DELIVER THE RECORD TO THE ENGINEER.
- THE RESIDENT ENGINEER SHALL CONTACT BRANDY KENNEDY, AREA TRAFFIC FIELD ENGINEER AT BRANDY.KENNEDY@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD, FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF THE PAVEMENT.
- THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- ALL GUARDRAIL REMOVAL AND REPLACEMENT LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.
- THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT, 12" LOWER LIFT SHALL BE CS 1 OR RR 1.
- ANY EXCAVATED SHOULDER WORK FOR SHOULDER WIDENING MUST HAVE 12" AGGREGATE SUBGRADE IMPROVEMENT INSTALLED BY THE END OF EACH WORK SHIFT.
- DROP-OFFS ADJACENT TO THE TRAVEL LANE SHALL BE KEPT TO A MINIMUM. PROTECTION OF THE DROP-OFF SHALL BE ACCORDING TO THE IDOT BUREAU OF SAFETY PROGRAMS AND ENGINEERING, SAFETY ENGINEERING POLICY MEMORANDUM 4-21. DROP-OFFS GREATER THAN OR EQUAL TO 12" AT LOCATIONS WHERE THE DROP-OFF IS LOCATED WITHIN 8 FT OF THE EDGE OF THE TRAVEL LANE SHALL BE BACKFILLED IN ACCORDANCE WITH TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE DROP-OFF AREAS MEET THE OFFSET, HEIGHT, AND DURATION REQUIREMENTS TO USE BARRICADES AT THE END OF EACH WORKDAY. THIS MAY REQUIRE THE CONTRACTOR TO REPLACE OR PLACE SUFFICIENT MATERIAL IN THE EXCAVATION TO REDUCE THE DROP-OFF TO BE COMPLIANT WITH THE REQUIREMENTS FOR USE OF BARRICADES. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT.
- ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENTS IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER AT THE CONTRACTORS EXPENSE.
- GEOTECHNICAL FABRIC FOR GROUND STABILIZATION AND/OR AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAVE BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ABOVE ITEM WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRRC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

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		DRAWN -	REVISED -						326	2020-083-RS	MCHENRY	24	2
		CHECKED -	REVISED -						CONTRACT NO. 62L61				
	PLOT DATE = 4/2/2025	DATE -	REVISED -		SCALE:	SHEET	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			

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				CONSTRUCTION CODE	
				100% STATE	100% STATE
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY
				0005	0005
				URBAN	URBAN
20200100	EARTH EXCAVATION	CU YD	5427	5427	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	888	888	
30300011	AGGREGATE SUBGRADE IMPROVEMENT	TON	592	592	
30300112	AGGREGATE SUBGRADE IMPROVEMENT, 12"	SQ YD	8881	8881	
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	153	153	
35600710	HOT-MIX ASPHALT BASE COURSE WIDENING, 8 1/2"	SQ YD	8881	8881	
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	19982	19982	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	28550	28550	
40600370	LONGITUDINAL JOINT SEALANT	FOOT	11965	11965	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	51	51	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	122	122	
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	731	731	
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	1379	1379	
44000100	PAVEMENT REMOVAL	SQ YD	1777	1777	
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	SQ YD	3553	3553	
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	33413	33413	

* SPECIALTY ITEM

	USER NAME = jammal.aramahi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.P. ROUTE 326 (IL 47) SUMMARY OF QUANTITIES			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					326	2020-083-RS	MCHENRY	24	3
		CHECKED -	REVISED -					CONTRACT NO. 62L61				
	PLOT DATE = 3/24/2025	DATE -	REVISED -		SCALE:	SHEET 1	OF 5 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

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				100% STATE	100% STATE
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY
				0005	0005
				URBAN	URBAN
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	287	287	
44002218	HOT-MIX ASPHALT REMOVAL OVER PATCHES 4 1/2"	SQ YD	3277	3277	
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	100	100	
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	1500	1500	
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	800	800	
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	500	500	
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	2985	2985	
63301210	REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A	FOOT	1671	1671	
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	15045	15045	
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	5430	5430	
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2	
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1	
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1	

* SPECIALTY ITEM

	USER NAME = jammal.aramahi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.P. ROUTE 326 (IL 47) SUMMARY OF QUANTITIES			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 3/24/2025	DATE -	REVISED -		SCALE:	SHEET 2	OF 5 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY
				0005	0005
				URBAN	URBAN
66901006	REGULATED SUBSTANCES MONITORING	CAL DA	25	25	
67100100	MOBILIZATION	L SUM	1	1	
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1	
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1	
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	39	39	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	96920	96920	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	30917	30917	
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	750	750	
70300221	TEMPORARY PAVEMENT MARKING - LINE 4" - PAINT	FOOT	148410	148410	

* SPECIALTY ITEM

	USER NAME = jammal.aramahi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.P. ROUTE 326 (IL 47) SUMMARY OF QUANTITIES			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					326	2020-083-RS	MCHENRY	24	5
		CHECKED -	REVISED -					CONTRACT NO. 62L61				
	PLOT DATE = 3/24/2025	DATE -	REVISED -		SCALE:	SHEET 3	OF 5 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

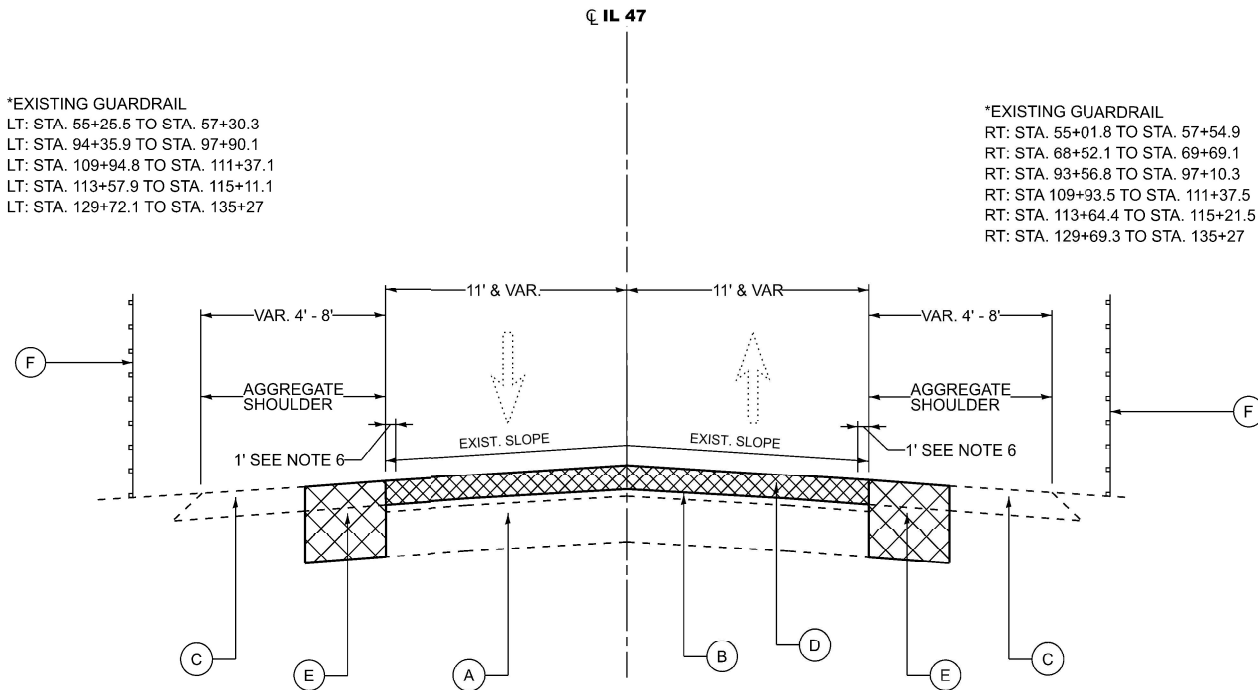
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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY
				0005	0005
				URBAN	URBAN
70300241	TEMPORARY PAVEMENT MARKING - LINE 6" - PAINT	FOOT	753	753	
70300261	TEMPORARY PAVEMENT MARKING - LINE 12" - PAINT	FOOT	117	117	
70300281	TEMPORARY PAVEMENT MARKING - LINE 24" - PAINT	FOOT	190	190	
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	42399	42399	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	250	250	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	49470	49470	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	251	251	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	39	39	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	63	63	
* 78100100	RAISED REFLECTIVE PAVEMENET MARKER	EACH	272	272	
78300200	RAISED REFLECTIVE PAVEMENET MARKER REMOVAL	EACH	272	272	
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	160	160	
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	6	6	
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	12	12	

* SPECIALTY ITEM

	USER NAME = jammal.aramahi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.P. ROUTE 326 (IL 47) SUMMARY OF QUANTITIES			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					326	2020-083-RS	MCHENRY	24	6
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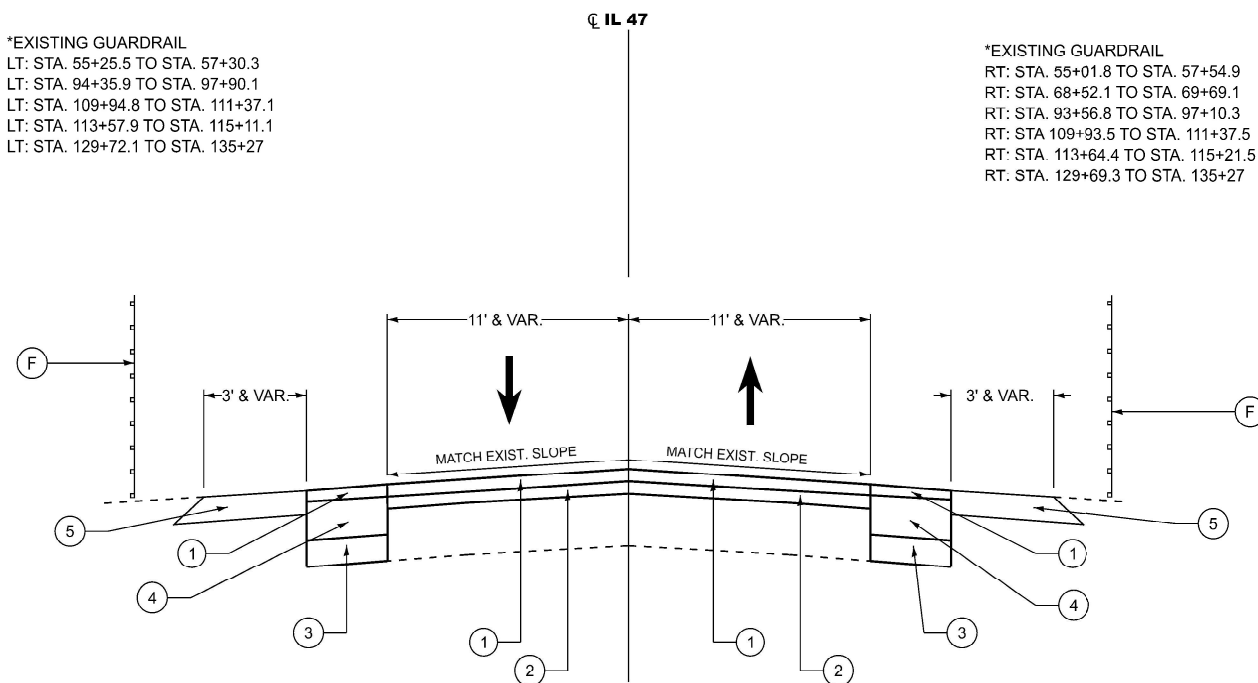
*EXISTING GUARDRAIL
RT: STA. 55+01.8 TO STA. 57+54.9
RT: STA. 68+52.1 TO STA. 69+69.1
RT: STA. 93+56.8 TO STA. 97+10.3
RT: STA 109+93.5 TO STA. 111+37.5
RT: STA. 113+64.4 TO STA. 115+21.5
RT: STA. 129+69.3 TO STA. 135+27



EXISTING TYPICAL SECTION

STA. 44+00 TO STA. 135+32

*EXISTING GUARDRAIL
RT: STA. 55+01.8 TO STA. 57+54.9
RT: STA. 68+52.1 TO STA. 69+69.1
RT: STA. 93+56.8 TO STA. 97+10.3
RT: STA 109+93.5 TO STA. 111+37.5
RT: STA 113+64.4 TO STA. 115+21.5
RT: STA. 129+69.3 TO STA. 135+27



PROPOSED TYPICAL SECTION

STA. 44+00 TO STA. 135+32

- (A) — PCC PAVEMENT, $\pm 9"$
- (B) — HMA SURFACE COURSE, $\pm 4 \frac{1}{2}"$
- (C) — AGGREGATE SHOULDER
- (D) — HMA SURFACE REMOVAL, $2 \frac{1}{4}"$
- (E) — EARTH EXCAVATION, 22" (AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER)
- (F) — EXISTING GUARDRAIL (THE ENGINEER SHALL DETERMINE THE LOCATIONS AND IF REMOVAL AND IF REERECTION IS REQUIRED.)

- ① HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 1/2"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
- ③ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ④ HOT-MIX ASPHALT BASE COURSE WIDENING, 8 1/2"
- ⑤ AGGREGATE WEDGE SHOULDER, TYPE B

NOTES:

6. SAWCUT THE EXISTING HMA SHOULDER 1' FROM THE OUTSIDE EDGE FOR A CLEAN LINE FOR THE HMA SHOULDER WIDENING. WHERE THE EXISTING HMA SHOULDER IS 1' OR LESS, SAWCUT ON THE 12' LANE LINE.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
	AIR VOIDS @ NDES	QUALITY MANAGEMENT PROGRAM (QMP)
STANDARD OVERLAY AND HMA SHOULDERS		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 1/2"	4% @ 70 GYR.	QCP
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	QC/QA
HOT-MIX ASPHALT BASE COURSE WIDENING (HMA BINDER IL-19.0), 8 1/2"	4% @ 70 GYR.	QC/QA
HOT-MIX ASPHALT PATCHING		
CLASS D PATCH (HOT-MIX ASPHALT BINDER IL-19.0)	4% @ 70 GYR.	QC/QA
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HOT-MIX ASPHALT BINDER IL-19.0)	4% @ 70 GYR.	QC/QA
DRIVEWAY		
HOT-MIX ASPHALT BASE COURSE, 6" (HMA BINDER IL-19.0)	4% @ 50 GYR.	QC/QA
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 2"	4% @ 50 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL / QUALITY ASSURANCE (QC/QA), QUALITY CONTROL FOR PERFORMANCE (QCP), PAY FOR PERFORMANCE (PFP)		

1. THE CONTRACTOR SHALL PATCH BEFORE MILLING.
2. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
3. THE "AC TYPE" FOR POLYMERIZED HOT-MIX ASPHALT MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HOT-MIX ASPHALT THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS
4. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50.

USER NAME = jammal.ahmadi	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 3/24/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE 326 (IL 47) TYPICAL SECTIONS

SCALE:	SHEET 1	OF 2	SHEETS	STA.	TO STA.
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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	8
		CONTRACT NO. 62L61		
		ILLINOIS	FED. AID PROJECT	

MODEL: IL-47 (TYP_1) [Sheet]
FILE NAME: c:\pw_work\pwidof\jammal.alramahi@illinois.gov\d10433041-sht-typical.dgn

- (A) — PCC PAVEMENT, $\pm 9"$
- (B) — HMA SURFACE COURSE, $\pm 4 \frac{1}{2}"$
- (C) — AGGREGATE SHOULDER
- (D) — HMA SURFACE REMOVAL, $2 \frac{1}{4}"$
- (E) — EARTH EXCAVATION, 22" (AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER)
- (F) — EXISTING GUARDRAIL (THE ENGINEER SHALL DETERMINE THE LOCATIONS AND IF REMOVAL AND REERECTION IS REQUIRED.)

- ① HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 1/2"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
- ③ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ④ HOT-MIX ASPHALT BASE COURSE WIDENING, 8 1/2"
- ⑤ AGGREGATE WEDGE SHOULDER, TYPE B

The diagram illustrates a typical cross-section of an existing road. Key features include:

- Centerline:** Labeled "CL 47" at the top.
- Turning Lanes:** Indicated by a dashed line and labeled "TURNING LANES VAR. 10 - 12'".
- Shoulders:** Labeled "AGGREGATE SHOULDER" on both sides.
- Dimensions:**
 - Left shoulder: "VAR. 4' - 8'" and "1' SEE NOTE 1".
 - Left turning lane: "11' & VAR.".
 - Right turning lane: "11' & VAR.".
 - Right shoulder: "VAR. 4' - 8'" and "1' SEE NOTE 1".
- Labels:** "EXIST. SLOPE" is labeled on both sides of the centerline.
- Reference Markers:** Circled letters A, B, C, D, E, and F are placed around the diagram to indicate specific points of interest.
- Section Title:** "EXISTING TYPICAL SECTION" is written in bold at the bottom.
- Stationing:** "STA. 135+32 TO STA. 151+00" is written at the bottom.

CL 47

*EXISTING GUARDRAIL
LT: STA. 135+27 TO STA. 137+08.1

*EXISTING GUARDRAIL
RT: STA. 135+27 TO STA. 137+08.5

11' & VAR.

TURNING LANES
VAR. 10 - 12'

11' & VAR.

3' & VAR.

MATCH EXIST. SLOPE

MATCH EXIST. SLOPE

1

2

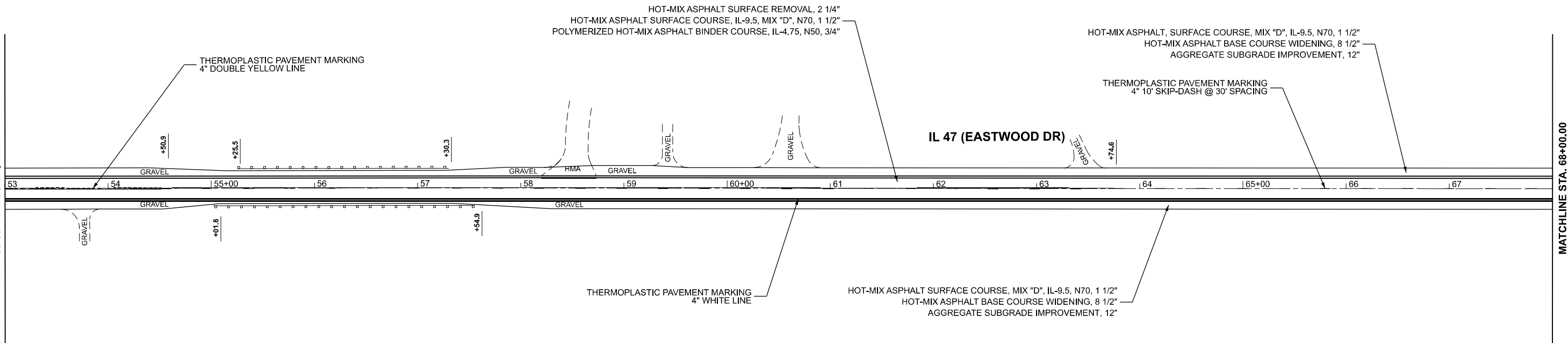
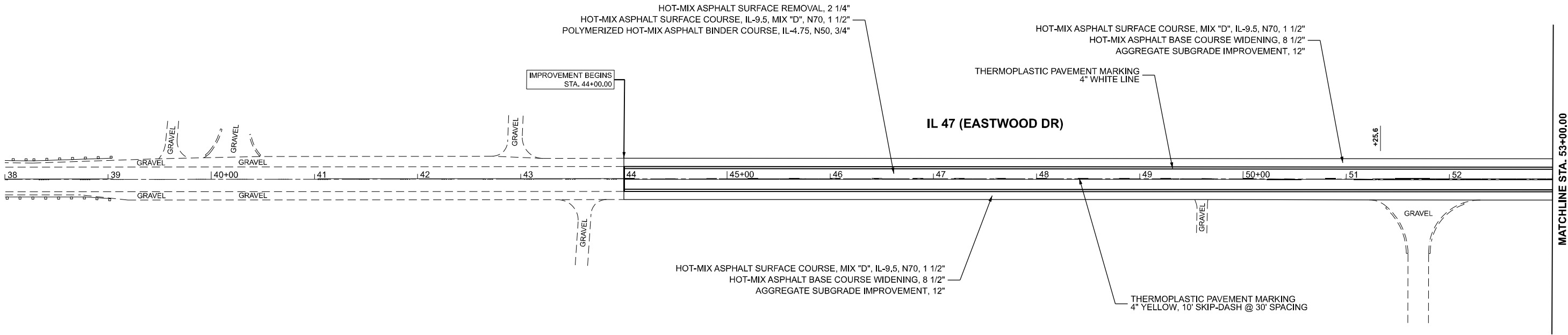
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4

5

PROPOSED TYPICAL SECTION

STA. 135+32 TO STA. 151+00



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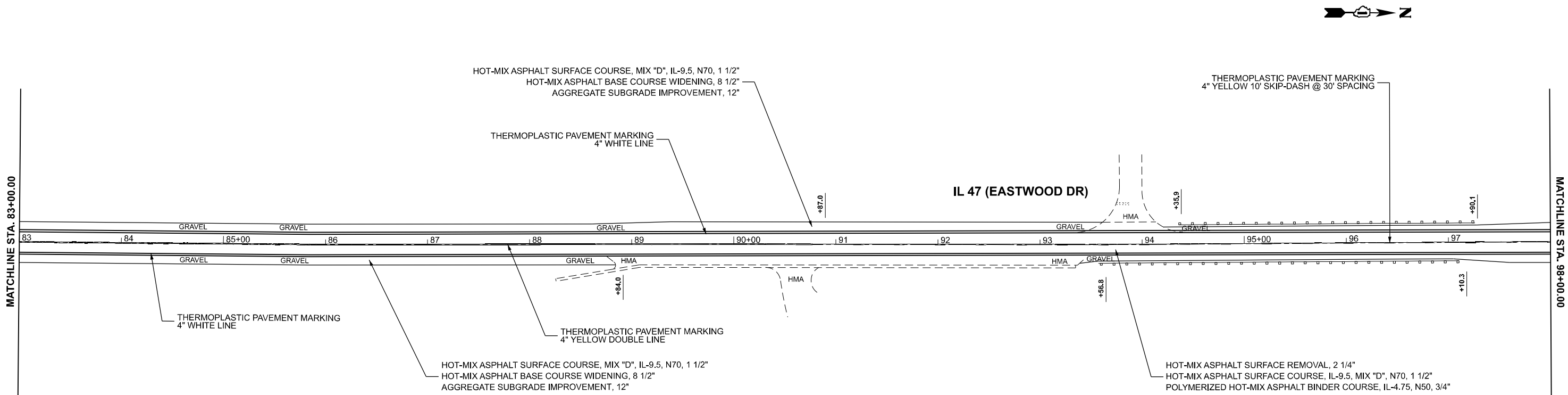
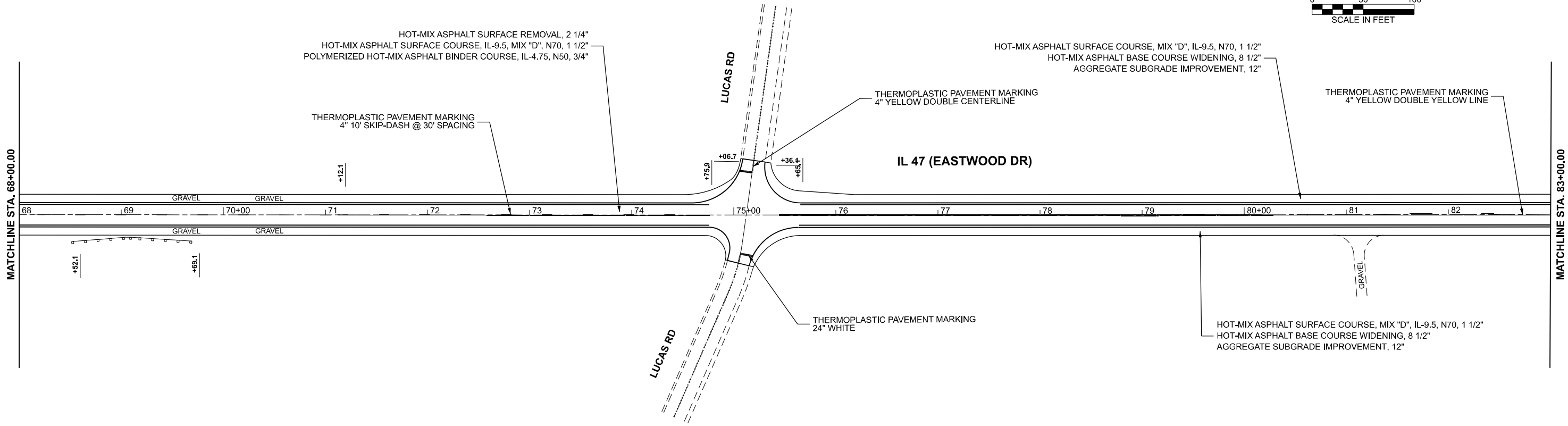
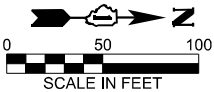
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE 326 (IL 47)
ROADWAY PLANS

SCALE: 1"=50' SHEET 2 OF 4 SHEETS STA. 38+00.00 TO STA. 68+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	10
CONTRACT NO. 62L61				
ILLINOIS FED. AID PROJECT				



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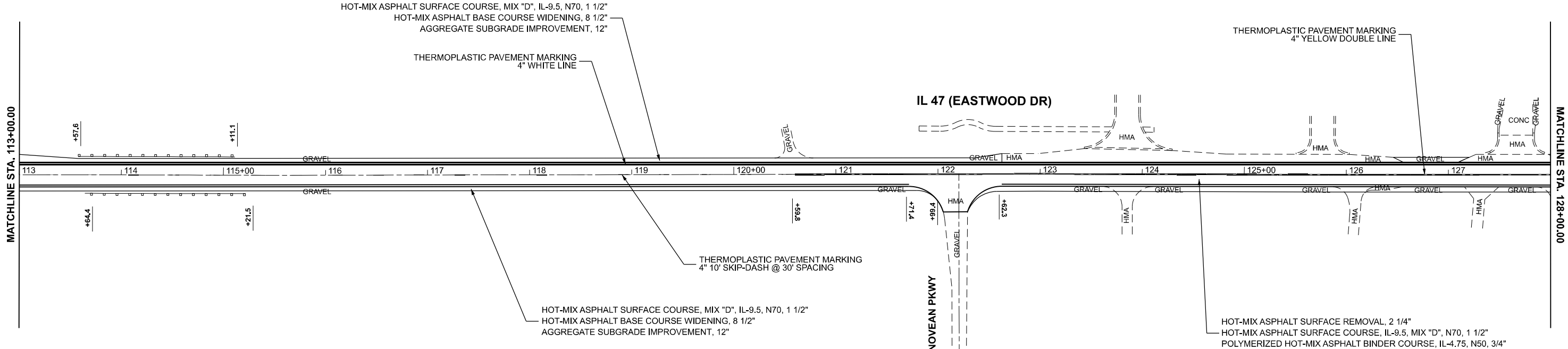
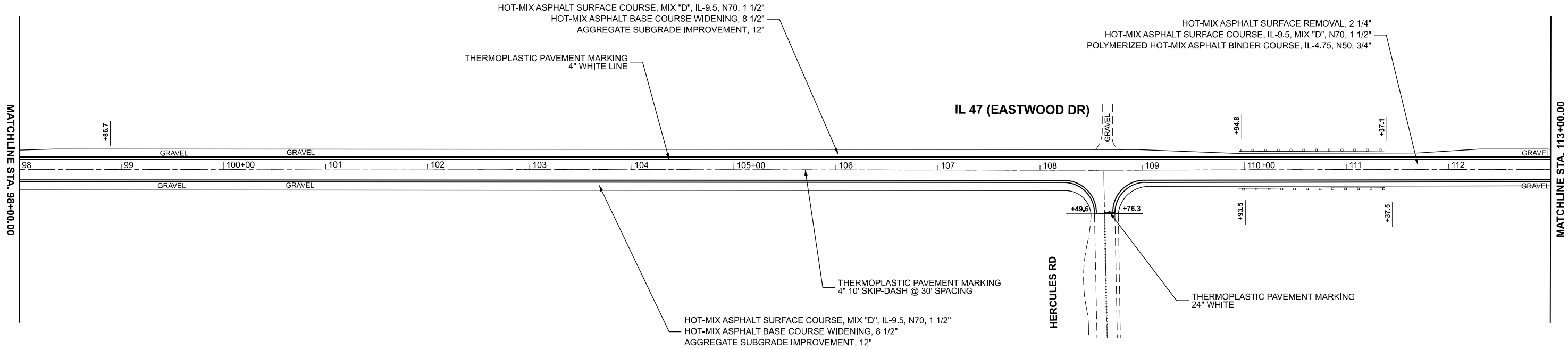
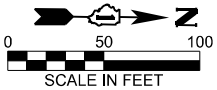
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	PLOT DATE	= 3/21/2025	DATE	-	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE 326 (IL 47)
ROADWAY PLANS

SCALE: 1"=50' SHEET 3 OF 4 SHEETS STA. 68+00.00 TO STA. 98+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	11
CONTRACT NO. 62L61				
ILLINOIS FED. AID PROJECT				



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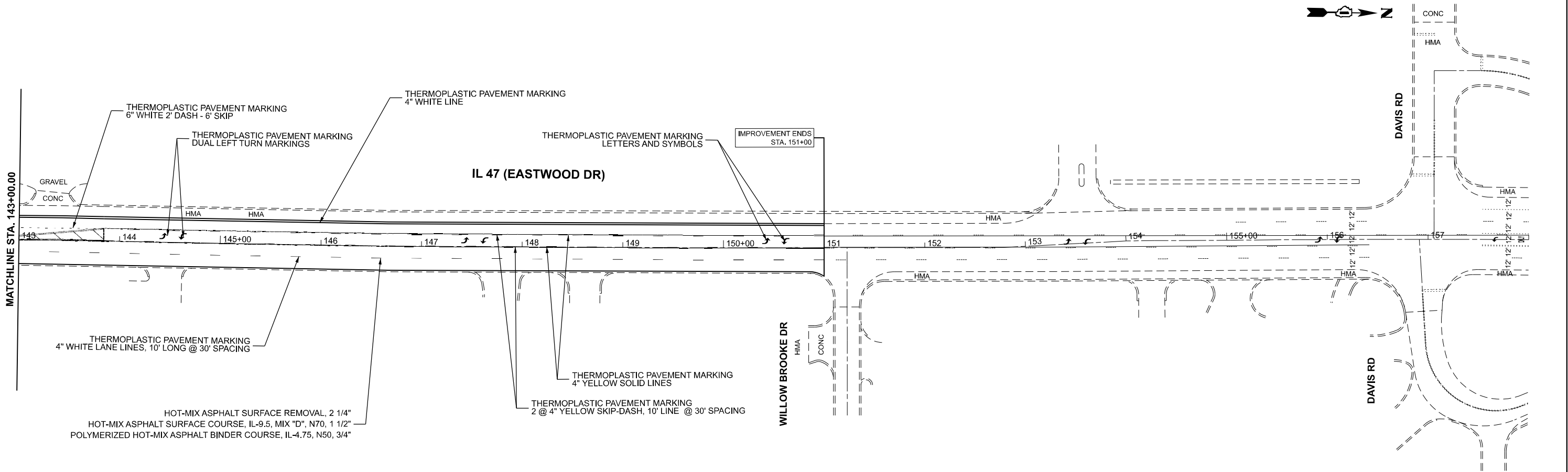
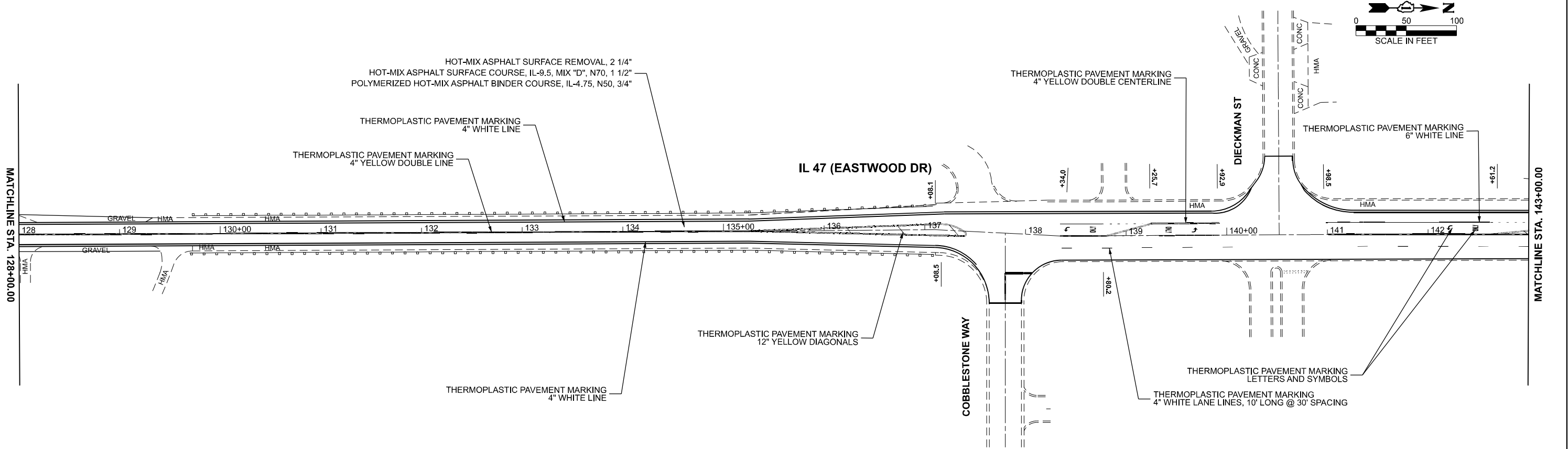
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE 326 (IL 47)
ROADWAY PLANS

SCALE: 1"=50' SHEET 4 OF 4 SHEETS STA. 98+00.00 TO STA. 128+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	12
CONTRACT NO. 62L61				
ILLINOIS FED. AID PROJECT				

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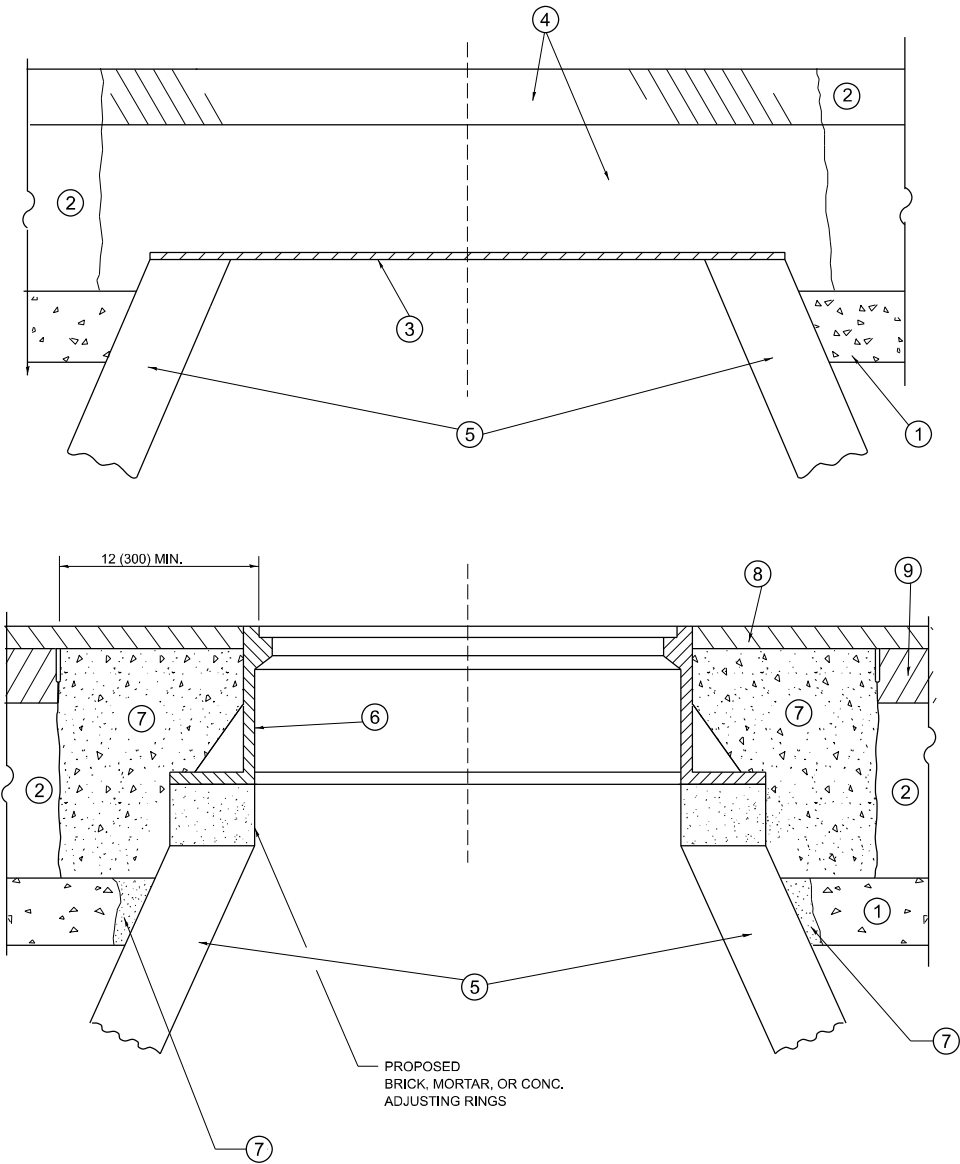
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE 326 (IL 47) ROADWAY PLANS			
SCALE: 1"=50'	SHEET 5	OF 4 SHEETS	STA. 128+00.00 TO STA. 158+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	13
CONTRACT NO. 62L61				
ILLINOIS FED. AID PROJECT				

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DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

- STAGE 1** (BEFORE PAVEMENT MILLING)
- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

- STAGE 2** (AFTER PAVEMENT MILLING)
- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

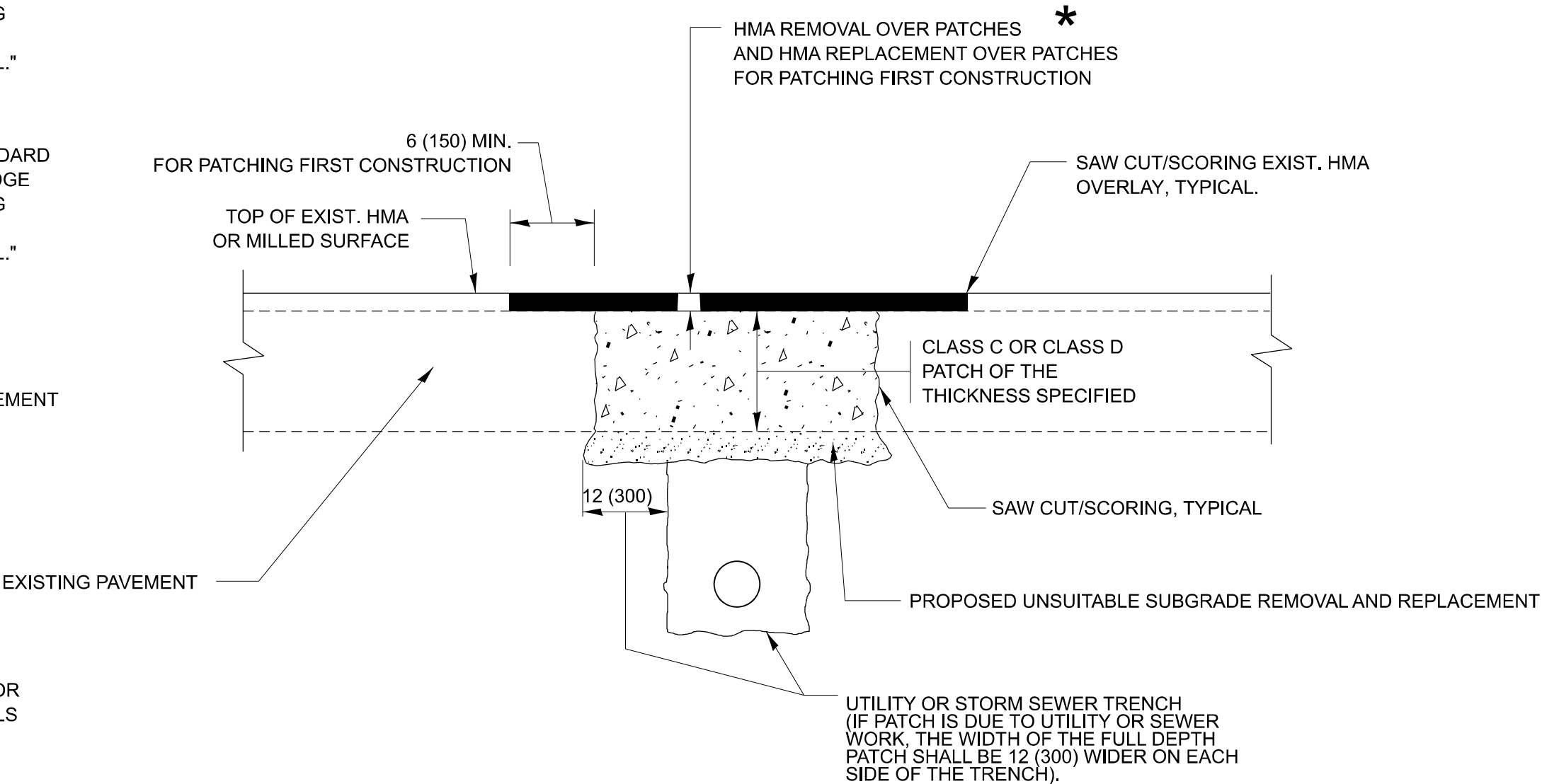
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		DRAWN -	REVISED - R. BORO 12-06-11			326	2020-083-RS	MCHENRY	24	14
		CHECKED -	REVISED - K. SMITH 11-18-22			BD600-03 (BD-08)		CONTRACT NO. 62L61		
	PLOT DATE = 3/21/2025	DATE - 10-25-94	REVISED - K. SMITH 09-15-23			ILLINOIS FED. AID PROJECT				
	SCALE: NONE		SHEET 11 OF 11 SHEETS		STA.		TO STA.			

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

- 1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
- 2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
- 3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

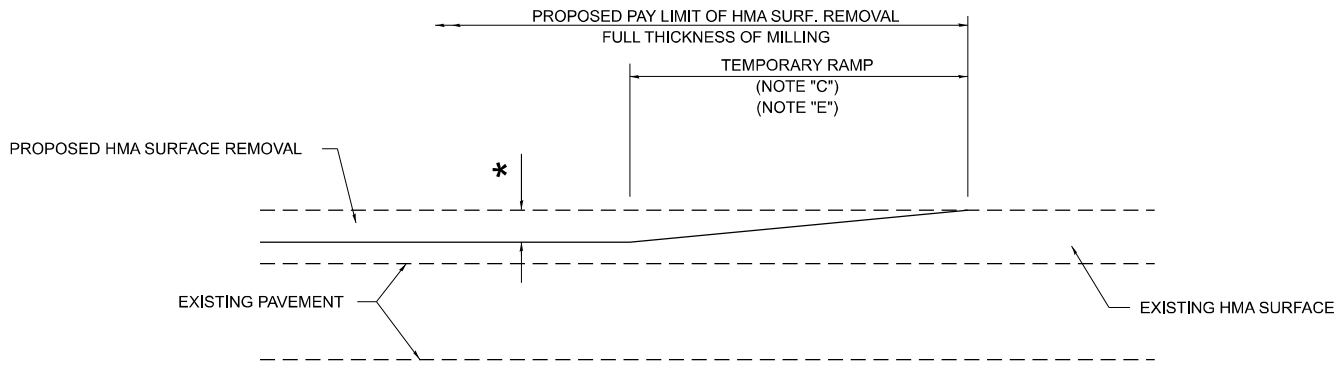
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

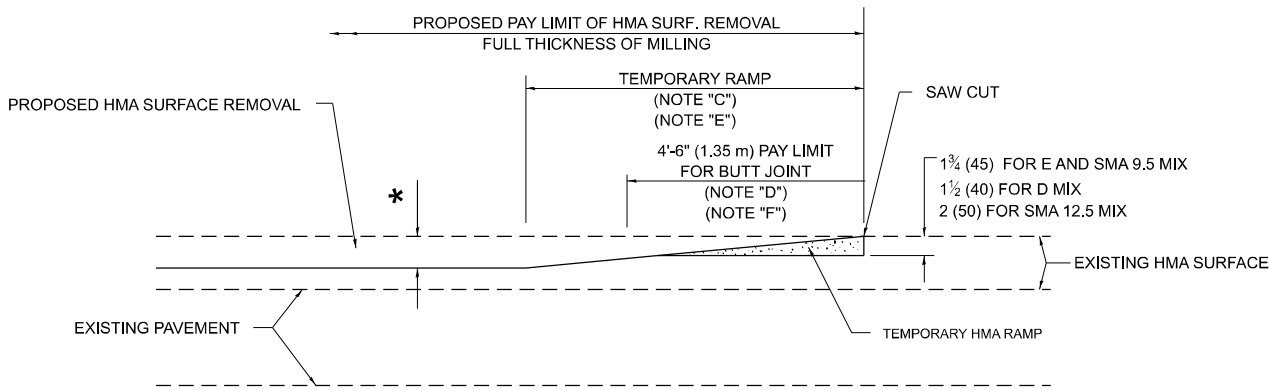
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							ILLINOIS FED. AID PROJECT							



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

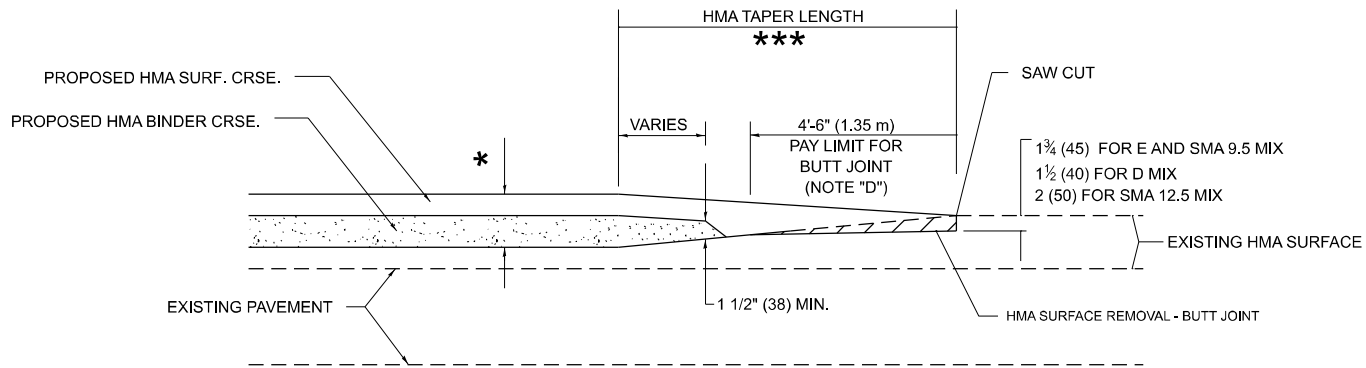
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

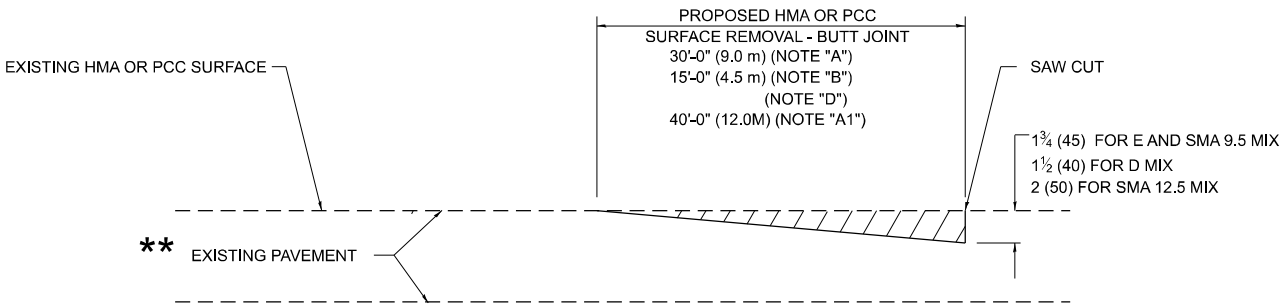
OPTION 2

TYPICAL TEMPORARY RAMP

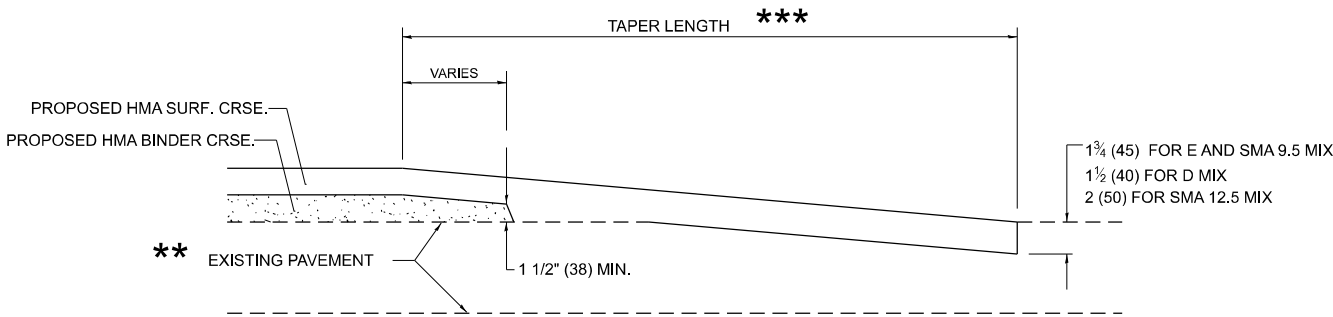


**BUTT JOINT AND
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

****** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4' (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
***** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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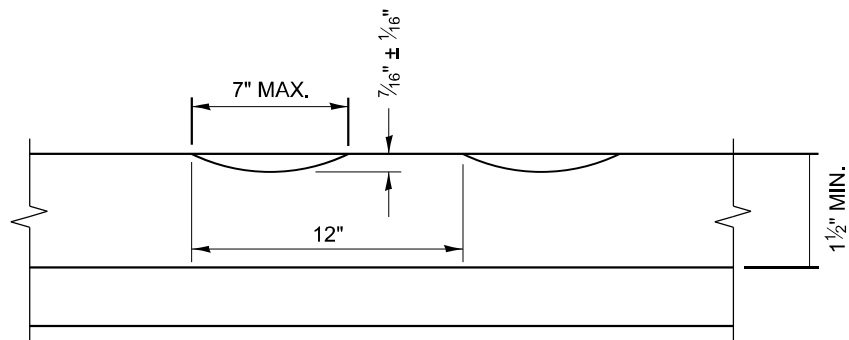
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

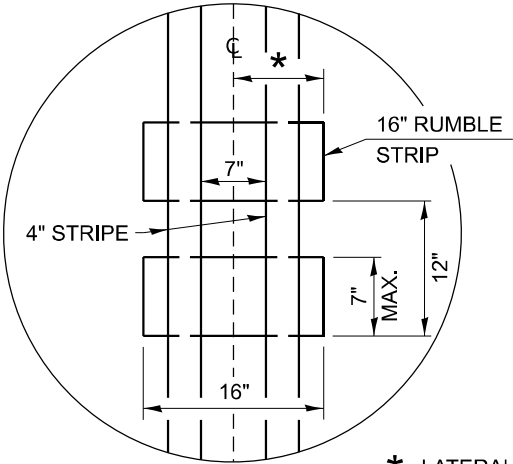
**BUTT JOINT AND
HMA TAPER DETAILS**

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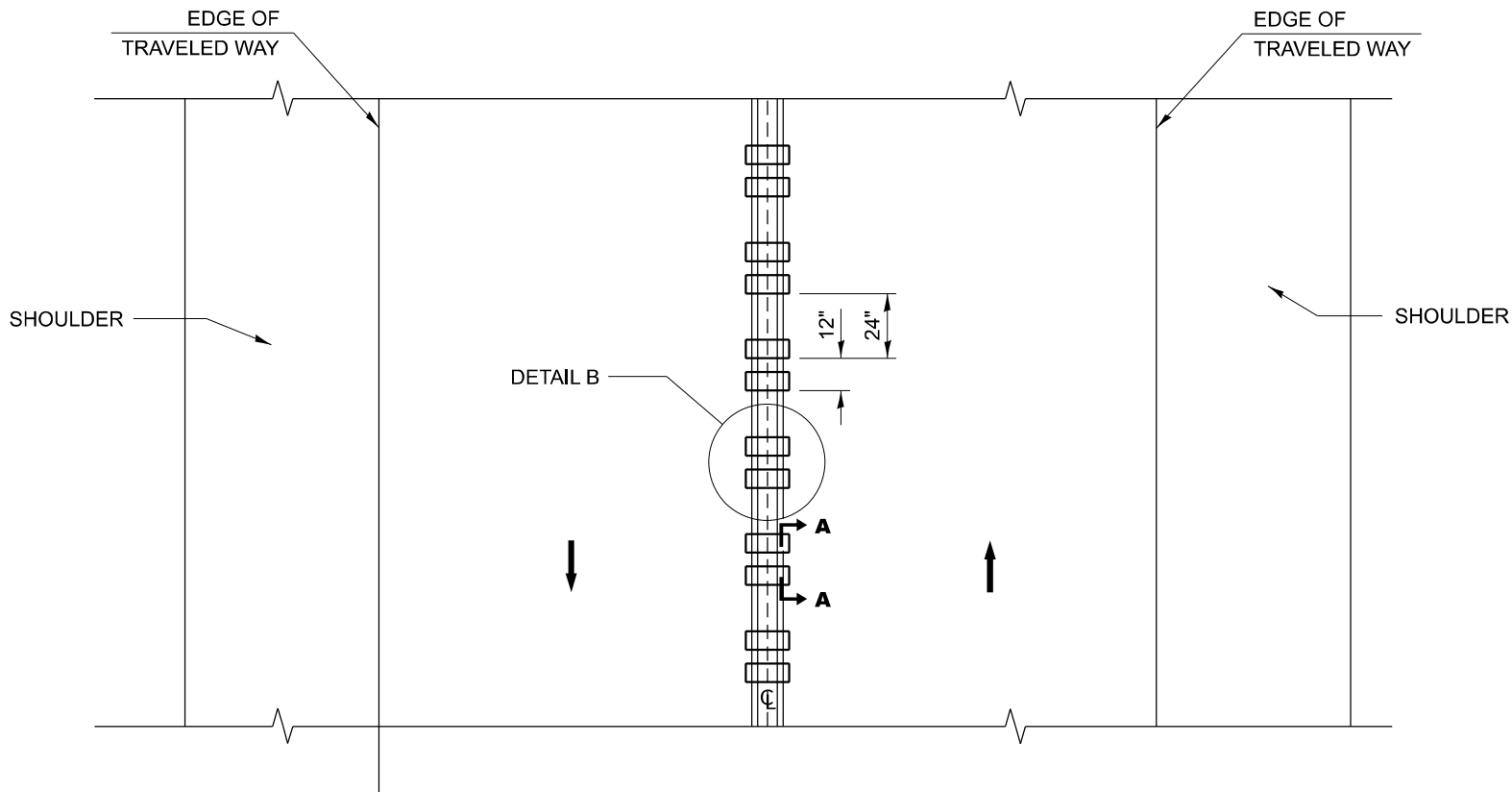
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326	2020-083-RS	MCHENRY	24	16
BD400-05 BD-32		CONTRACT NO. 62L61		
ILLINOIS		FED. AID PROJECT		



SECTION A-A



DETAIL B



TWO-WAY ROAD

GENERAL NOTES

1. CENTERLINE RUMBLE STRIPS SHALL BE CONSTRUCTED ACCORDING TO SECTION 642 ALONG THE CENTERLINE OF PAVEMENT.
2. SEE STANDARD 780001 FOR OTHER STRIPING LAYOUTS.
3. RUMBLE STRIPS SHALL NOT BE PLACED ON BRIDGES.
4. ALL RUMBLE STRIPS SHALL BE MILLED.
5. CENTERLINE RUMBLE STRIPS SHALL BE CONTINUOUS THROUGH CONNECTIONS OF SIDEROADS WITH NO LEFT TURN LANES.
6. DISCONTINUE CENTERLINE RUMBLE STRIPS THROUGH THE LIMITS OF ALL LEFT TURN LANES, INCLUDING ANY LANE TAPER SECTIONS.
7. AFTER RUMBLE STRIPS ARE INSTALLED, THE PAVEMENT SURFACE SHALL BE SWEEPED CLEAN PRIOR TO THE PLACEMENT OF THE NEW PAVEMENT MARKINGS.
8. WHERE USED, ADJUST SPACING OF RAISED REFLECTIVE PAVEMENT MARKERS TO FALL IN WIDER GAP BETWEEN RUMBLE STRIPS.

BASIS OF PAYMENT

1. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR CENTERLINE-RUMBLE STRIP OF THE WIDTH SPECIFIED.
2. HOT-SPRAY THERMOPLASTIC PAVEMENT MARKING WILL BE USED OVER THE RUMBLE STRIPS, AND WILL BE PAID FOR SEPARATELY.

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	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 3/21/2025	DATE - 08-06-2012	REVISED -

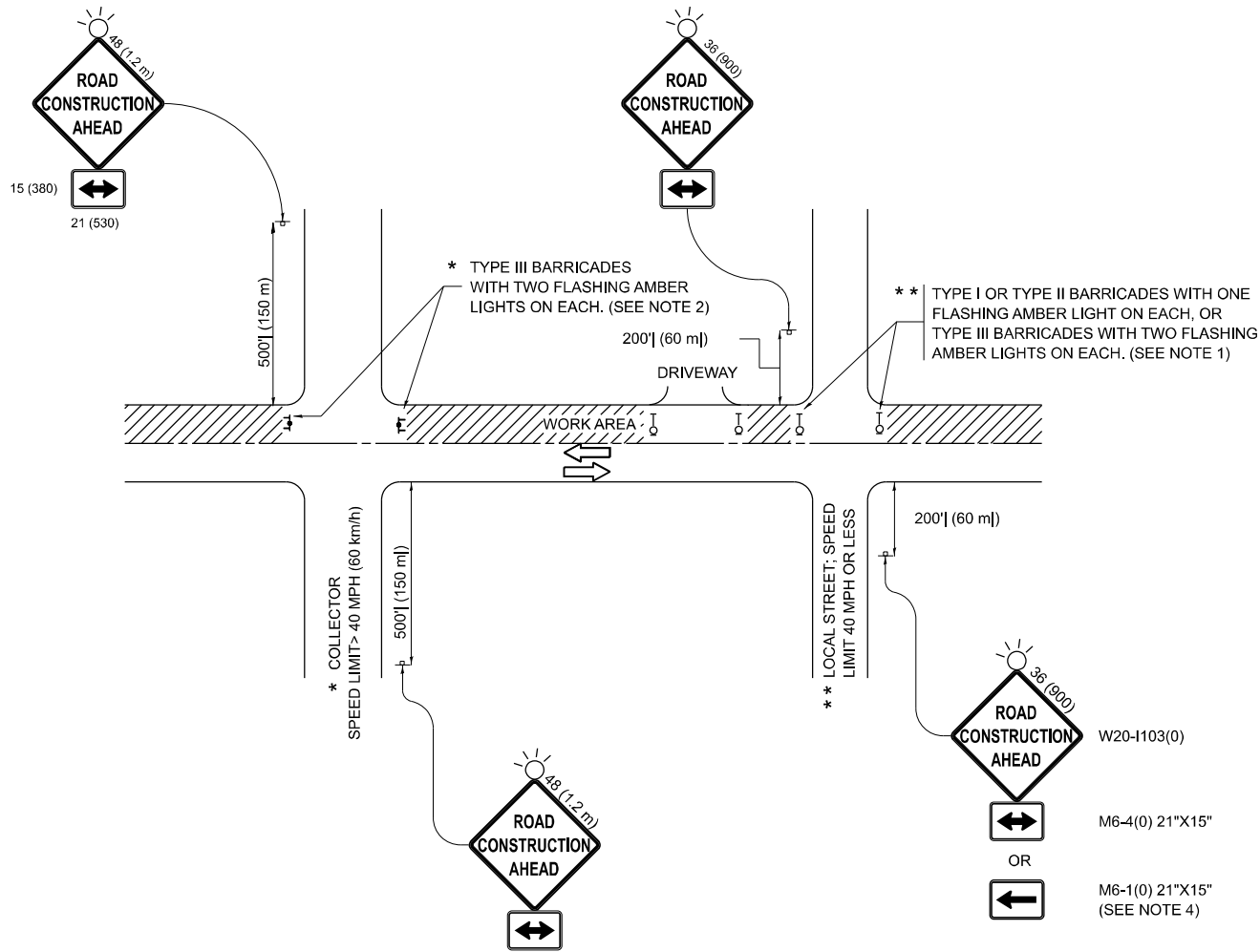
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RUMBLE STRIPS FOR CENTERLINE, NON-FREEWAY

SCALE: NONE SHEET 41 OF 11 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	17
BD 55		CONTRACT NO. 62L61		
ILLINOIS		FED. AID PROJECT		

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NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

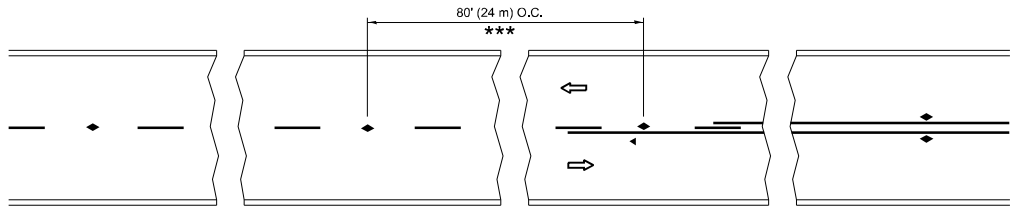
All dimensions are in inches (millimeters)
unless otherwise shown.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

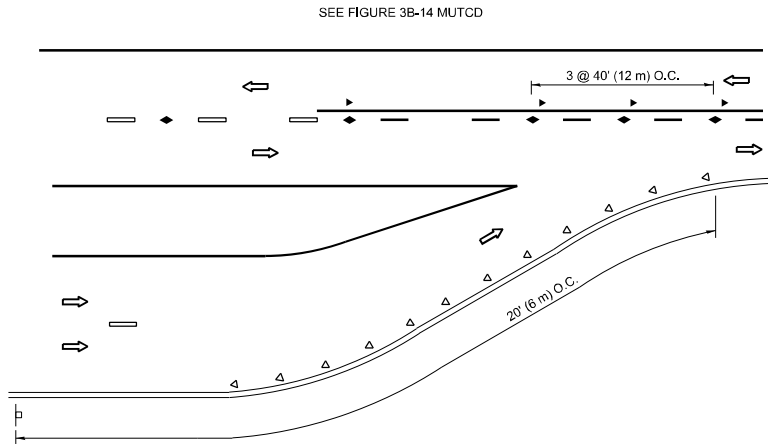
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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	18
TC-10		CONTRACT NO. 62L61		
ILLINOIS		FED. AID PROJECT		

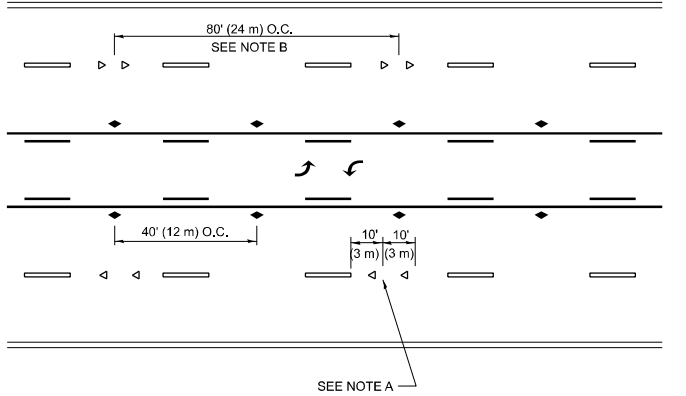


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

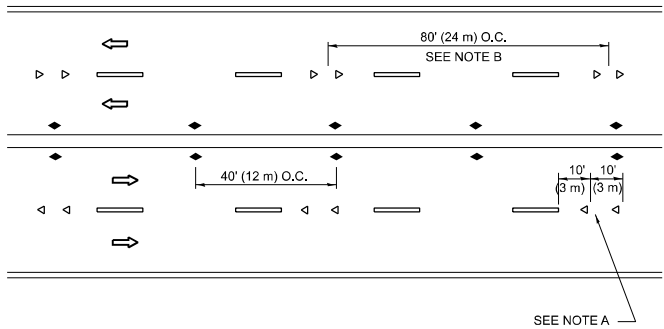
TWO-LANE/TWO-WAY



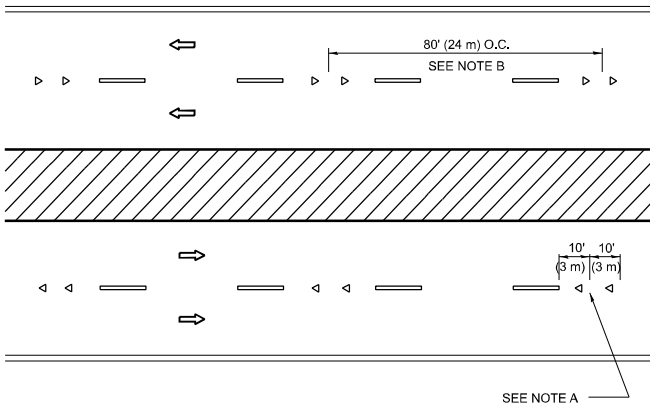
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

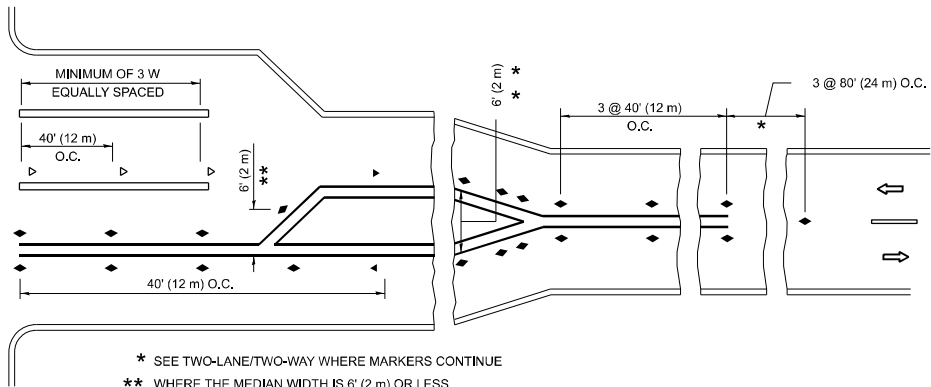
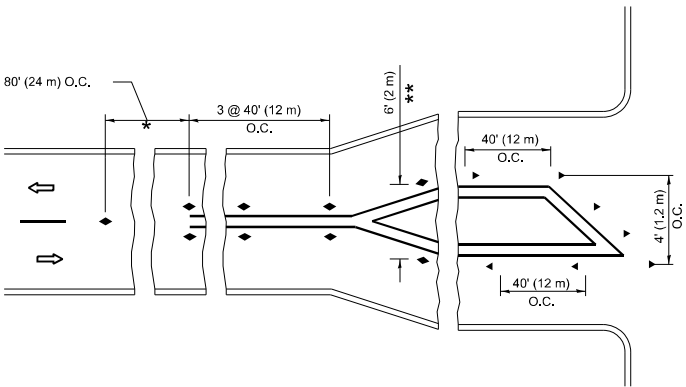
- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



- * SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
- ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

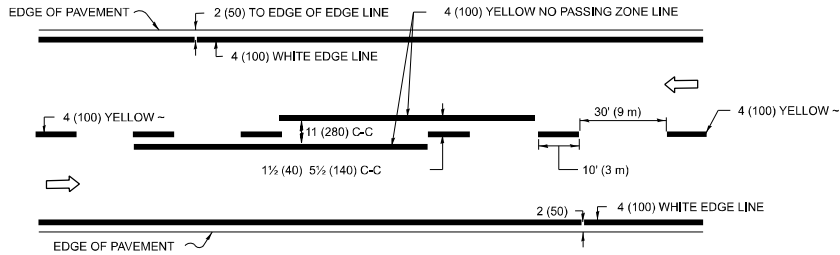
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

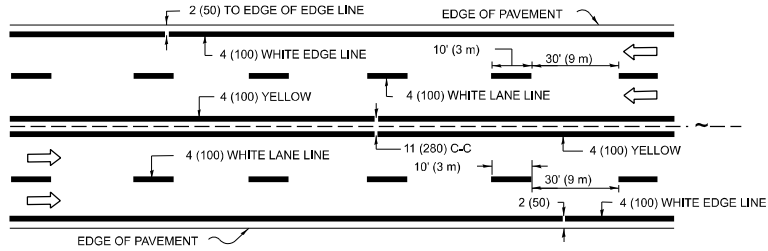
SCALE: NONE SHEET 01 OF 11 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	19
TC-11 CONTRACT NO. 62L61				
ILLINOIS FED. AID PROJECT				

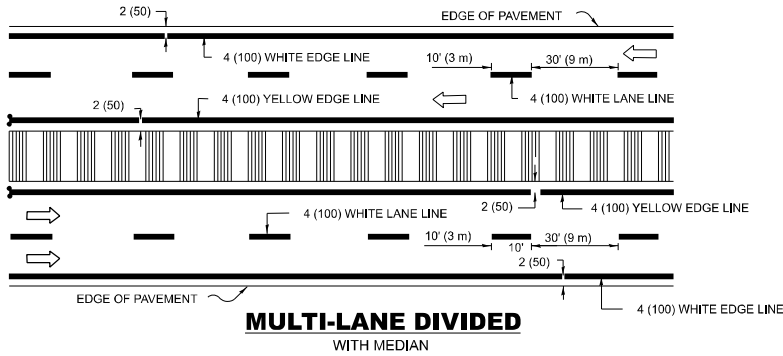
MODEL: TC-13
FILE NAME: c:\pwworking\jammal.aramah\1043304-shd-DistStd.dgn



2-LANE ROADWAY

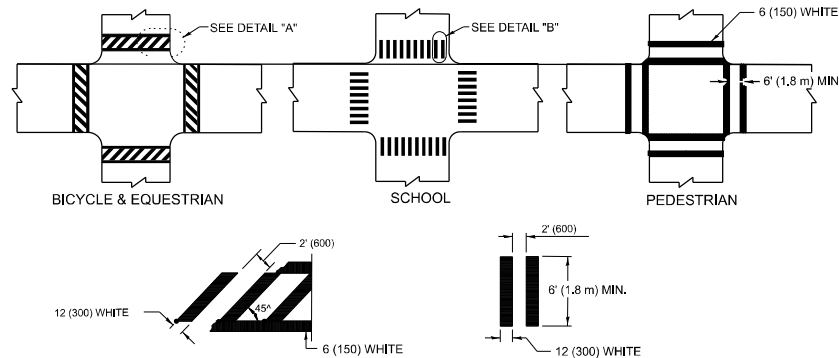


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED
WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

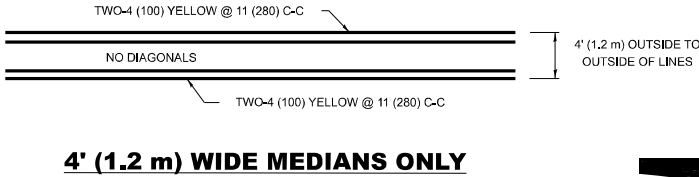


DETAIL "A"

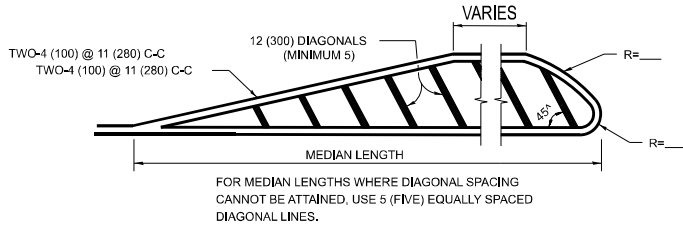
DETAIL "B"

TYPICAL CROSSWALK MARKING

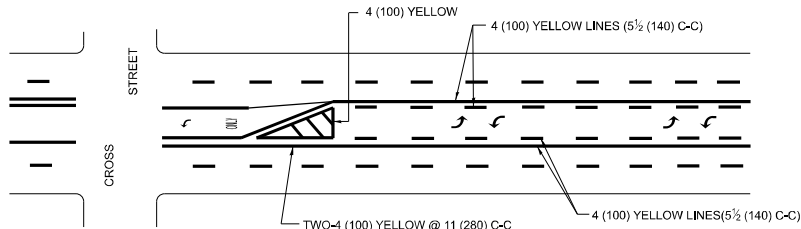
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



4' (1.2 m) WIDE MEDIANS ONLY

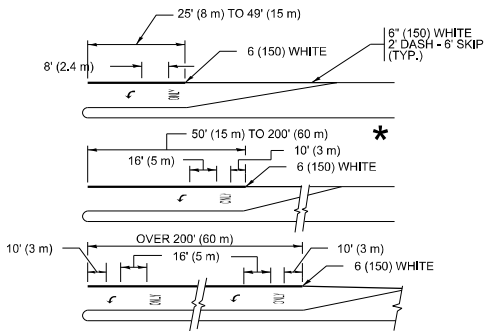


MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

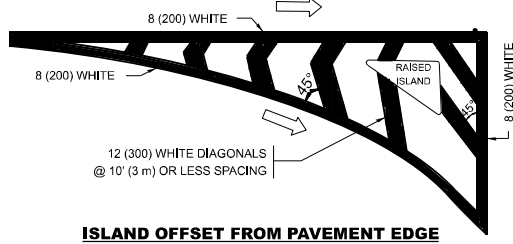


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

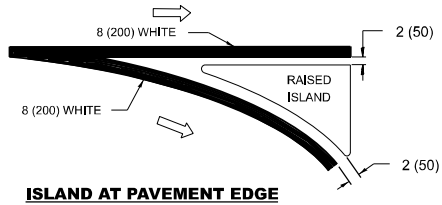
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

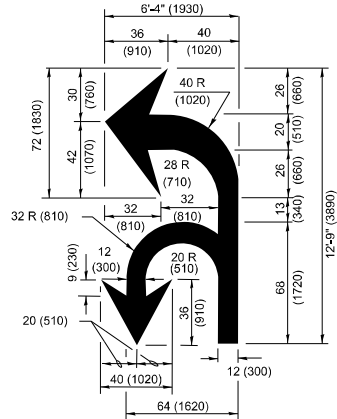


ISLAND OFFSET FROM PAVEMENT EDGE

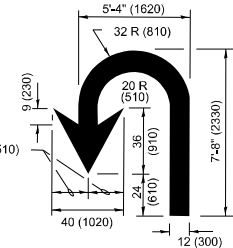


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION
LEFT AND U-TURN



U-TURN

LANE REDUCTION
TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE- FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO
STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE
CONSTRUCTION AND STATE STANDARD 7800001.

All dimensions are in inches (millimeters)
unless otherwise shown.

USER NAME = jammal.aramah	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
	DRAWN -	REVISED - C. JUCIUS 07-01-13
	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 3/21/2025	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 71 OF 11 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	20
TC-13		CONTRACT NO. 62L61		
ILLINOIS		FED. AID PROJECT		

TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER

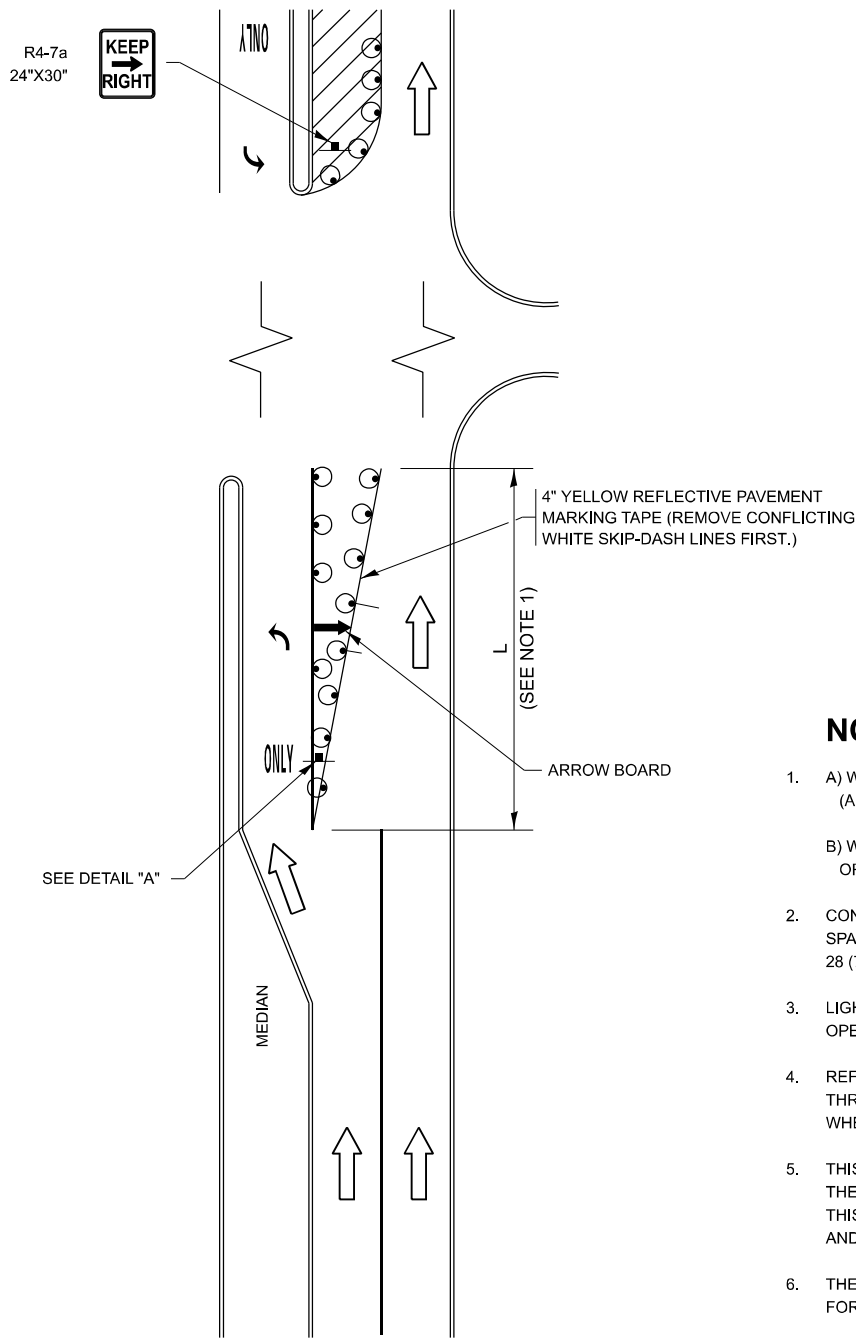
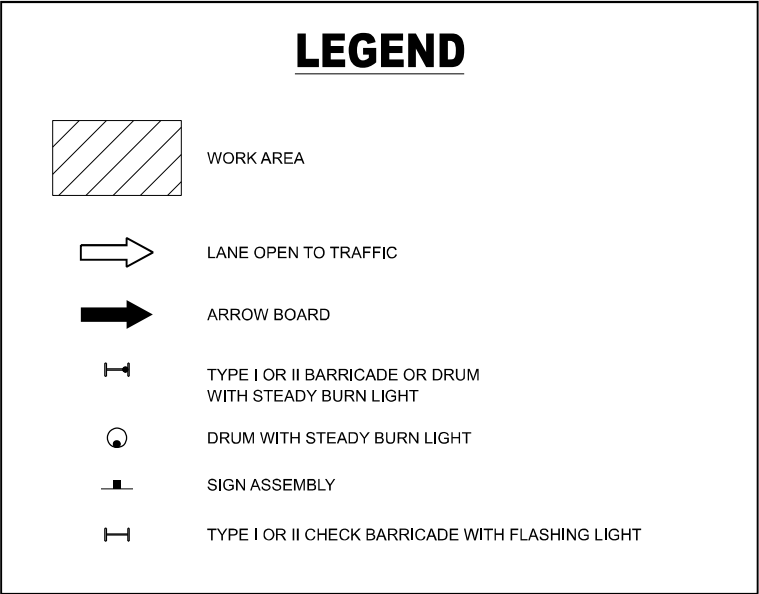


FIGURE 1

LEGEND



NOTES:

- A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE
WITHIN A LANE CLOSURE

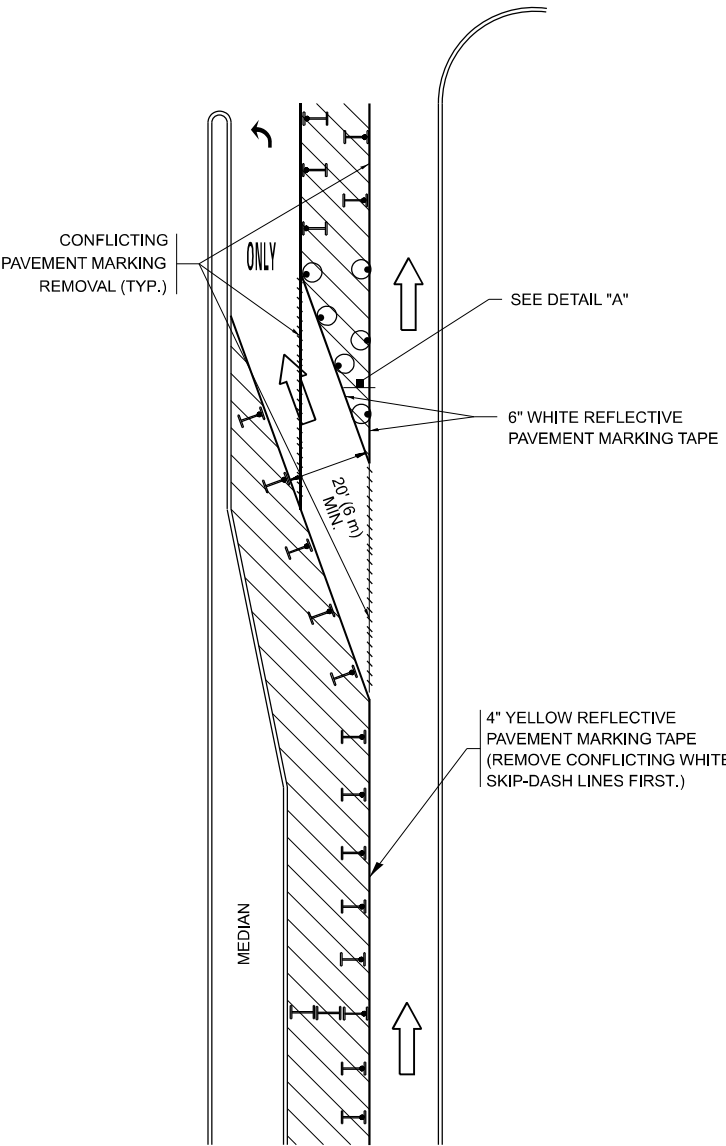
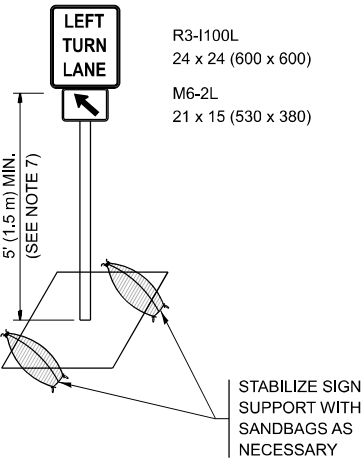


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: TC-14
FILE NAME: c:\pw_work\jwktol\jammal.aramahi\illinois.gov\1043304\sh-DistStd.dgn

USER NAME	= jammal.aramahi
DESIGNED	- T. RAMMACHER 09-08-94
DRAWN	- A. HOUSEH 11-07-95
CHECKED	- A. HOUSEH 10-12-96
DATE	- T. RAMMACHER 01-06-00
REVIS	-

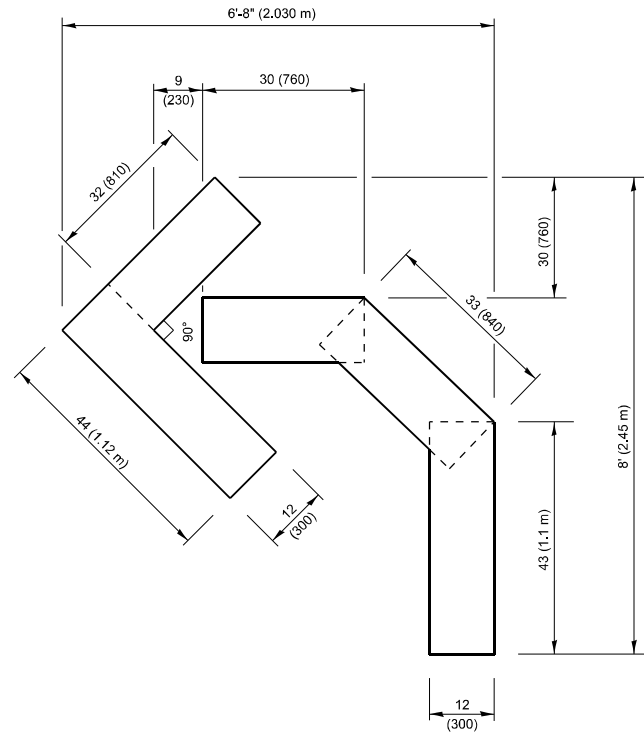
REVISED	- R. BORO 09-14-09
REVISED	- A. SCHUETZE 07-01-13
REVISED	- A. SCHUETZE 09-15-16
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

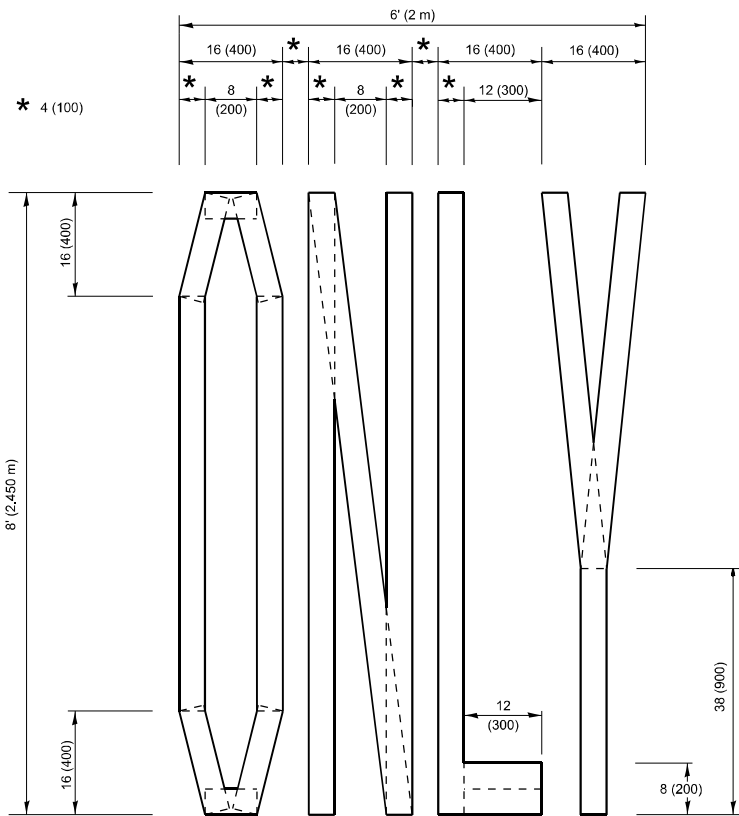
SCALE: NONE SHEET 8 OF 11 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	21
TC-14		CONTRACT NO. 62L61		
ILLINOIS		FED. AID PROJECT		



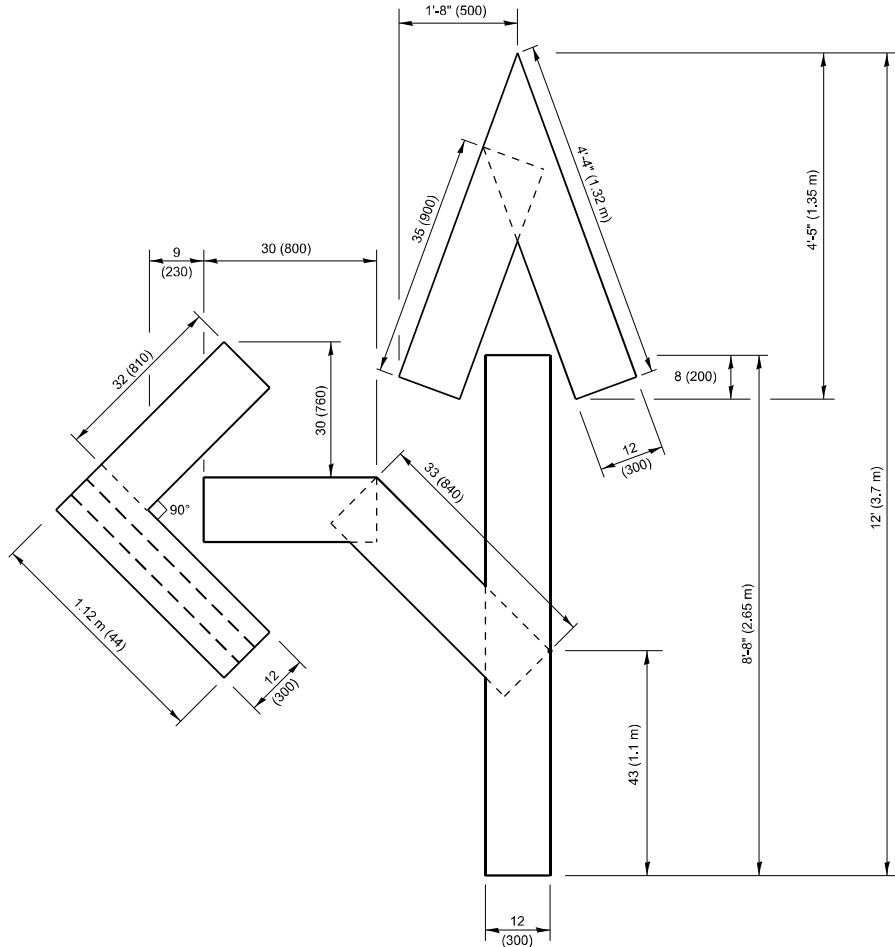
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

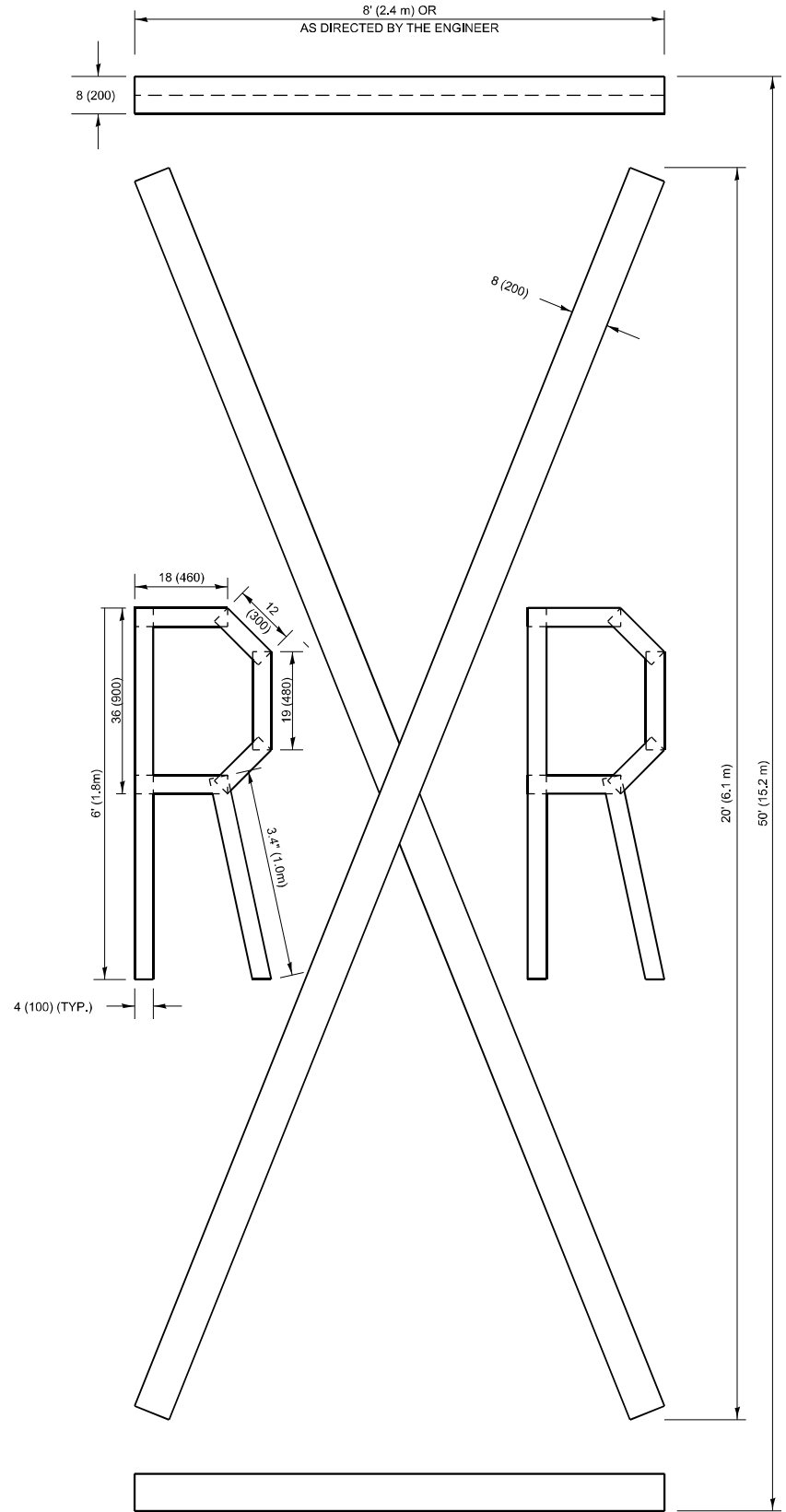


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
IN LINEAR FEET OF 4" LINES TO MATCH THE
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)
unless otherwise shown.

MODEL: TC-16
FILE NAME: c:\p\work\jammal.aramahi\illinois.gov\1043304-shl-DistStd.dgn

USER NAME	= jammal.aramahi	DESIGNED	-	REVISED	- T. RAMMACHER 03-02-98
		DRAWN	-	REVISED	- E. GOMEZ 08-28-00
		CHECKED	-	REVISED	- E. GOMEZ 08-28-00
PLOT DATE	= 3/21/2025	DATE	- 09-18-94	REVISED	- A. SCHUETZE 09-15-16

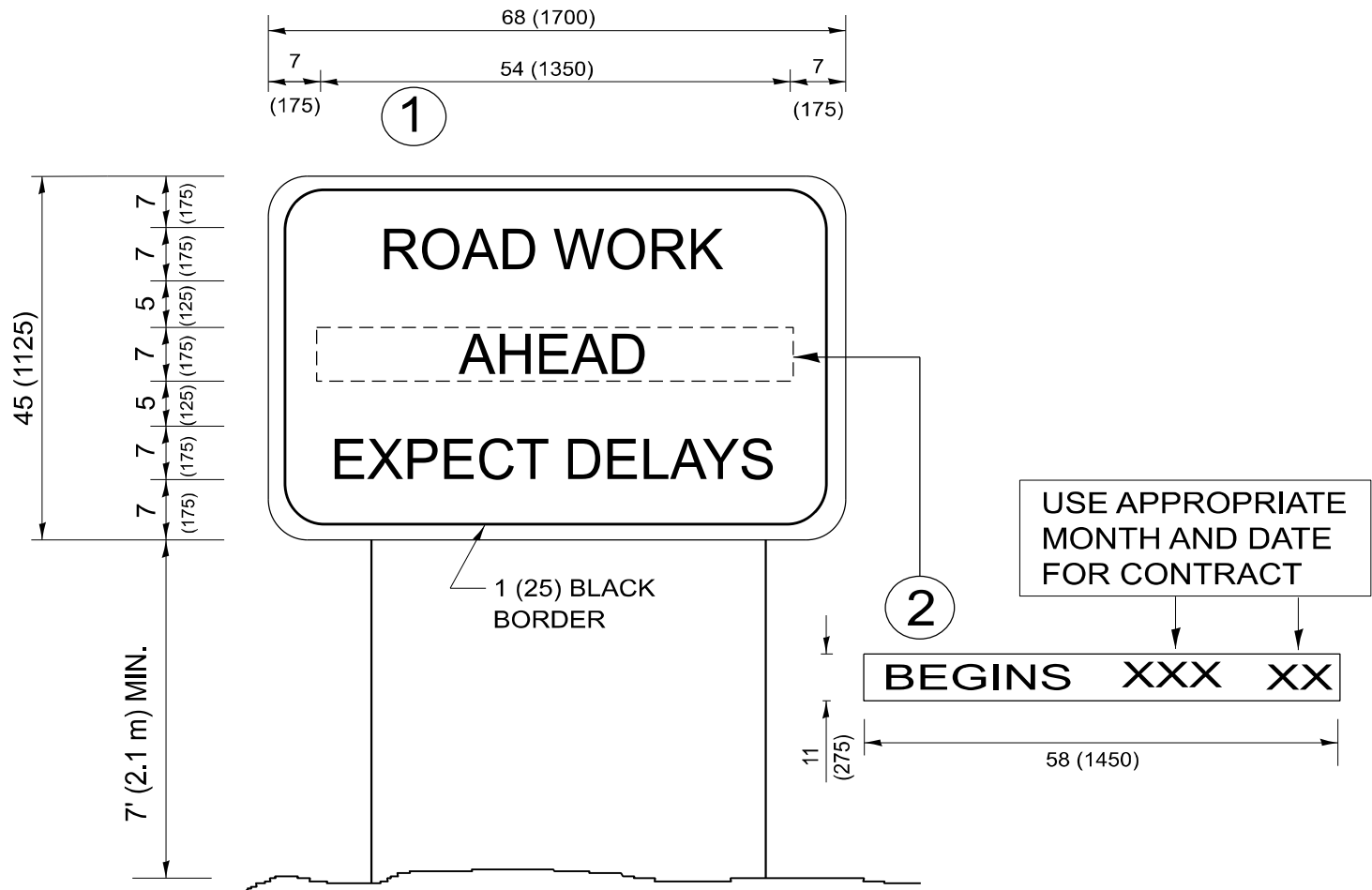
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 91 OF 11 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2020-083-RS	MCHENRY	24	22
TC-16		CONTRACT NO. 62L61		
		ILLINOIS	FED. AID PROJECT	

MODEL: TC-22
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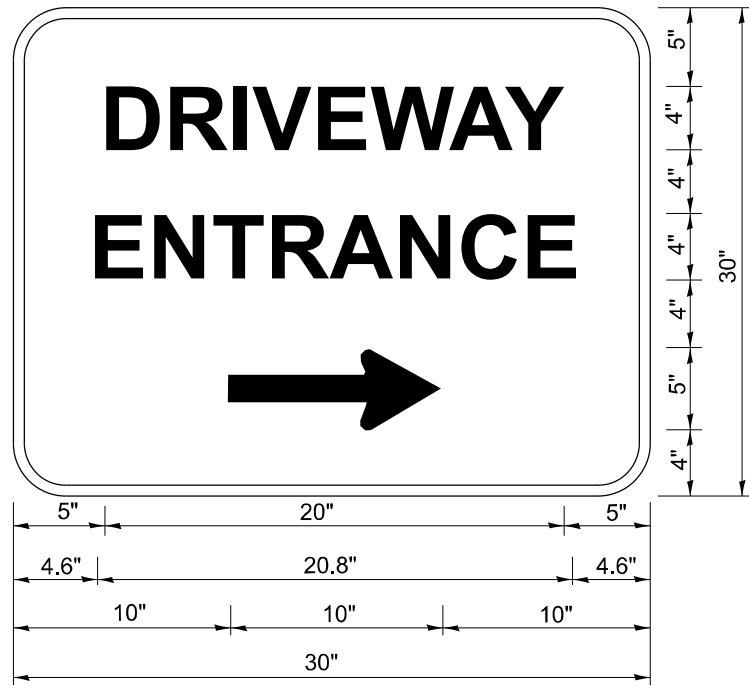


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

	USER NAME = jammal.aramahi	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. MIRS 12-11-97						326	2020-083-RS	MCHENRY	24	23
		CHECKED -	REVISED - T. RAMMACHER 02-02-99						TC-22		CONTRACT NO. 62L61		
	PLOT DATE = 3/21/2025	DATE -	REVISED - C. JUCIUS 01-31-07		ILLINOIS FED. AID PROJECT								
					SCALE: NONE	SHEET 10	OF 11	SHEETS	STA.	TO STA.			



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

- NOTES:**
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
 3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: TC-26
FILE NAME: c:\pwwork\jammal.aramahi\illinois.gov\1043304-shd-DistStd.dgn

	USER NAME = jammal.aramahi	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					326	2020-083-RS	MCHENRY	24	24
		CHECKED -	REVISED -		TC-26			CONTRACT NO. 62L61				
	PLOT DATE = 3/21/2025	DATE -	REVISED -		SCALE: NONE	SHEET 11	OF 11 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		