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701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
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701901-10	TRAFFIC CONTROL DEVICES
725001-01	OBJECT AND TERMINAL MARKERS


GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).
2. IDOT FACILITIES ARE NOT LOCATED BY JULIE OR DIGGER. IDOT ELECTRICAL FACILITIES INCLUDING ROADWAY LIGHTING, FIBER OPTIC, ITS EQUIPMENT, TRAFFIC SIGNAL AND PUMP STATION FACILITIES ARE LOCATED BY THE DEPARTMENT'S ELECTRICAL MAINTENANCE CONTRACTOR. AS OF THE LETTING DATE, CONTACT THE MEADE ELECTRIC COMPANY AT 773-287-7672.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, PACE BUSES AND THE VILLAGE OF THORNTON.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE DISTRICT ONE "BUTT JOINT AND HMA TAPER DETAILS" (BD-32).
6. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT OUTSIDE THE STAGING OR THE REMOVAL LINES SHOWN ON THE PLANS SHALL BE PLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
7. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
8. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
9. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
10. THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
11. THE RESIDENT ENGINEER SHALL CONTACT THE IDOT AREA TRAFFIC ENGINEER A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT.
12. THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
13. SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
14. FOR WORK OUTSIDE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT SHALL BE EPOXY COATED UNLESS NOTED ON THE PLANS.
15. MISCELLANEOUS PAY ITEMS HAVE BEEN INCLUDED FOR THE RESIDENT ENGINEER TO USE AT THEIR DISCRETION. ITEMS INCLUDE CLASS D PATCHES AND CONCRETE CURB AND GUTTER.
16. RAISED REFLECTIVE PAVEMENT MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS-RAISED REFLECTIVE PAVEMENT MAKERS (SNOW-PLOW RESISTANT)" SHOWN IN PLAN.

<div><div><div>GRAEF</div><div>8501 W. Higgins Road, Suite 280 Chicago, Illinois 60634 (773) 399-0122</div></div></div>	USER NAME = \$USERNAMES\$	DESIGNED -	REVISED -	<div><div>STATE OF ILLINOIS</div><div>DEPARTMENT OF TRANSPORTATION</div></div>	<div>GENERAL NOTES & INDEX OF SHEETS</div>					F.A.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -							2906	(T14)BR 24	COOK	34	2
		CHECKED -	REVISED -		2921	<div>CONTRACT NO. 62X29</div>								
	PLOT DATE = 4/17/2025	DATE -	REVISED -											
						SCALE:	SHEET 1	OF 1	SHEETS	STA.	TO STA.			

MODEL: SOQ 01
FILE NAME: X:\OH\2025\20250016\Design\CADD\Row\WO1\SheetsPlan\162X29-shr-SOQ.dgn

				CONSTRUCTION CODE 0047	
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	THORNTON ROAD (016-0622)	CHICAGO ROAD (016-0801)
				100% STATE	100% STATE
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	29	7	22
20200100	EARTH EXCAVATION	CU YD	25	8	17
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	738	738	
40600370	LONGITUDINAL JOINT SEALANT	FOOT	455	455	
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	445	445	
40604062	HOT-MIX ASPHALT, SURFACE COURSE, MIX "D", IL-9.5, N70	TON	138	138	
42001300	PROTECTIVE COAT	SQ YD	17	17	
44000153	HOT-MIX ASPHALT SURFACE REMOVAL, 1"	SQ YD	1021	1021	
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	296	296	
44000600	SIDEWALK REMOVAL	SQ FT	100		100
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	50	50	
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	50	50	
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	50	50	
50102400	CONCRETE REMOVAL	CU YD	12.0	5.4	6.6

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		DRAWN -	REVISED -
		CHECKED -	REVISED -
	PLOT DATE = 4/17/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES			
SCALE:	SHEET 1	OF 5 SHEETS	STA. TO STA.

F.A.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2906 2921	(T1-4)BR 24	COOK	34	3
CONTRACT NO. 62X29				
		ILLINOIS	FED. AID PROJECT	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 0047	
				THORNTON ROAD (016-0622)	CHICAGO ROAD (016-0801)
				100% STATE	100% STATE
50300225	CONCRETE STRUCTURES	CU YD	15.9	5.6	10.3
50300300	PROTECTIVE COAT	SQ YD	87.0	30.0	57.0
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2330	850	1480
52000005	PREFORMED JOINT SEAL 1"	FOOT	222	222	
52000025	PREFORMED JOINT SEAL 2"	FOOT	142		142
52000030	PREFORMED JOINT SEAL 2 1/2"	FOOT	148	148	
59300100	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	7.0	2.2	4.8
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	50	50	
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	2	2	
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	6	2	4
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2	2	
63200310	GUARDRAIL REMOVAL	FOOT	333	183	150
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	25	8	17
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	1	1

* SPECIALTY ITEM

MODEL: SQ003
FILE NAME: X:\OH\2025\20250016\Design\CADD\Row\WO1\SheetsPlan\162X29-shr-SQ00.dgn

				CONSTRUCTION CODE 0047	
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	THORNTON ROAD (016-0622)	CHICAGO ROAD (016-0801)
				100% STATE	100% STATE
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	0.5	0.5
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	0.5	0.5
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	40	20	20
67100100	MOBILIZATION	L SUM	1	0.5	0.5
70107004	PAVEMENT MARKING BLACKOUT TAPE, 4"	FOOT	9340	4840	4500
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	240	120	120
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	10540	4130	6410
70300221	TEMPORARY PAVEMENT MARKING - LINE 4" - PAINT	FOOT	2142	1991	151
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" -TYPE IV TAPE	FOOT	22270	7540	14730
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1512.5	787.5	725.0
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	687.5		687.5
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	4	2	2
70600330	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE), TEST LEVEL 3	EACH	2		2
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	2	2	

* SPECIALTY ITEM

MODEL: SQ0 04
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				CONSTRUCTION CODE 0047	
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	THORNTON ROAD (016-0622)	CHICAGO ROAD (016-0801)
				100% STATE	100% STATE
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1814	1814	
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	327	176	151
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	20	20	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	20	20	
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1440	1330	110
X5230154	CLEANING BRIDGE SCUPPERS AND DOWNSPOUTS	EACH	14	6	8
X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	242	124	118
X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	290	290	
X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	20	20	
X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	30	30	
X6060095	CLASS SI CONCRETE (MISCELLANEOUS)	CU YD	2.1	2.1	
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	6		
X7010216	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1	0.5	0.5
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	289	147	142

* SPECIALTY ITEM

REV-SEP

LEGEND

- (A) EXISTING HMA SURFACE COURSE, 1"
- (B) EXISTING P.C.C. BASE COURSE, 10"
- (C) EXISTING CONCRETE MEDIAN
- (D) EXISTING CURB AND GUTTER (NORTH OF BRIDGE)
- (E) EXISTING AGGREGATE SHOULDER (SOUTH OF BRIDGE)
- (F) AGGREGATE SUBGRADE
- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 1"
- (2) PROPOSED PORTLAND CEMENT SURFACE REMOVAL (VARIABLE DEPTH) - APPROXIMATELY 0" TO 1/2" TO REMOVE 1 1/2" TOTAL PAVEMENT AT EXISTING OUTSIDE CURB AND GUTTER OR SHOULDER
- (3) PROPOSED HMA SURFACE COURSE IL-9.5, MIX "D", N70, 1 1/2"

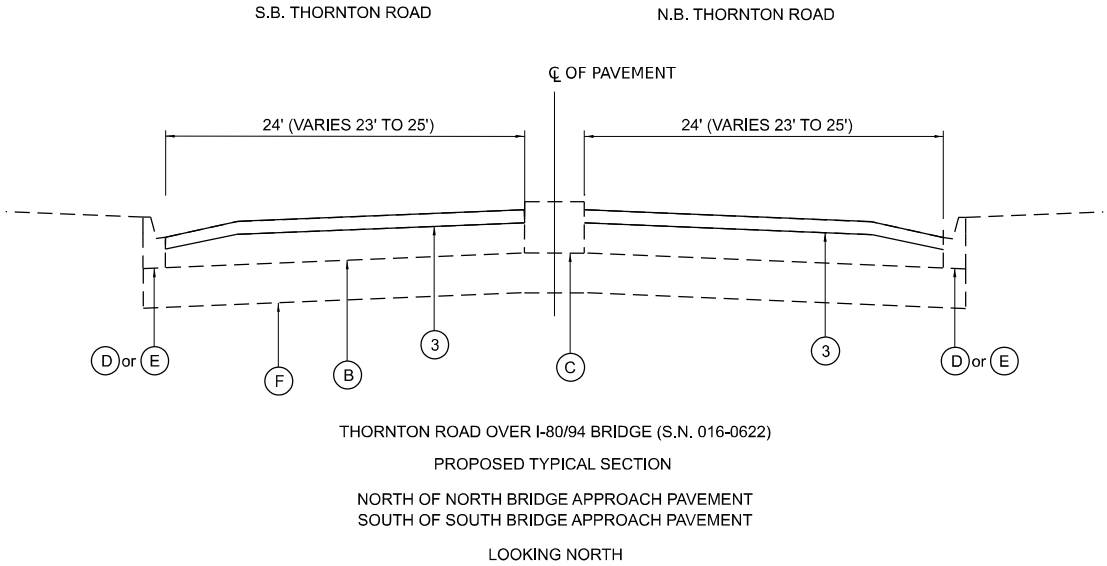
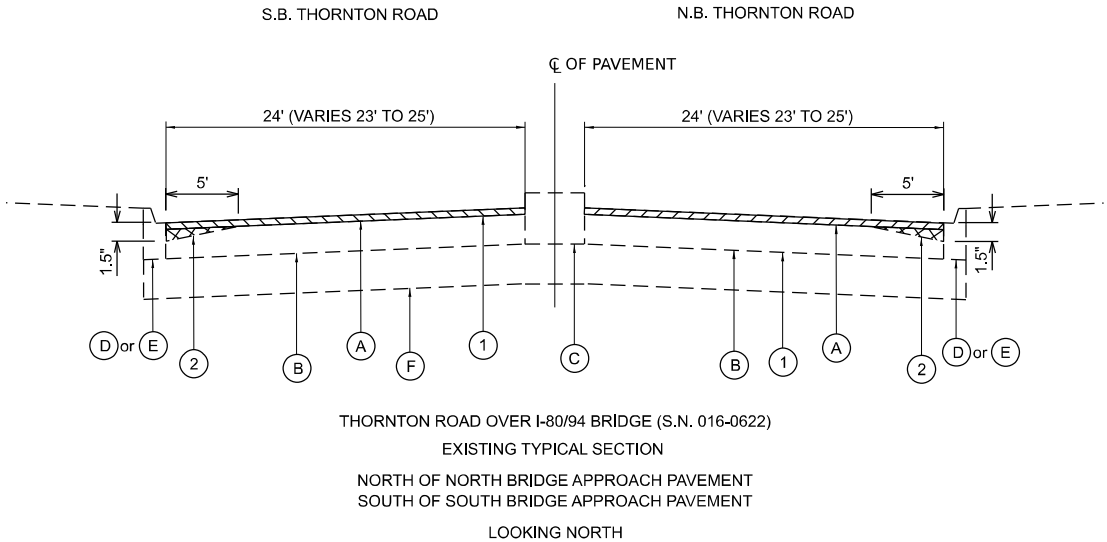
NOTES

- 1. CLASS D PATCHING WORK WILL BE DONE BEFORE RESURFACING IN ORDER TO PROVIDE A UNIFORM HMA SURFACE.
- 2. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE MILLED SURFACE.

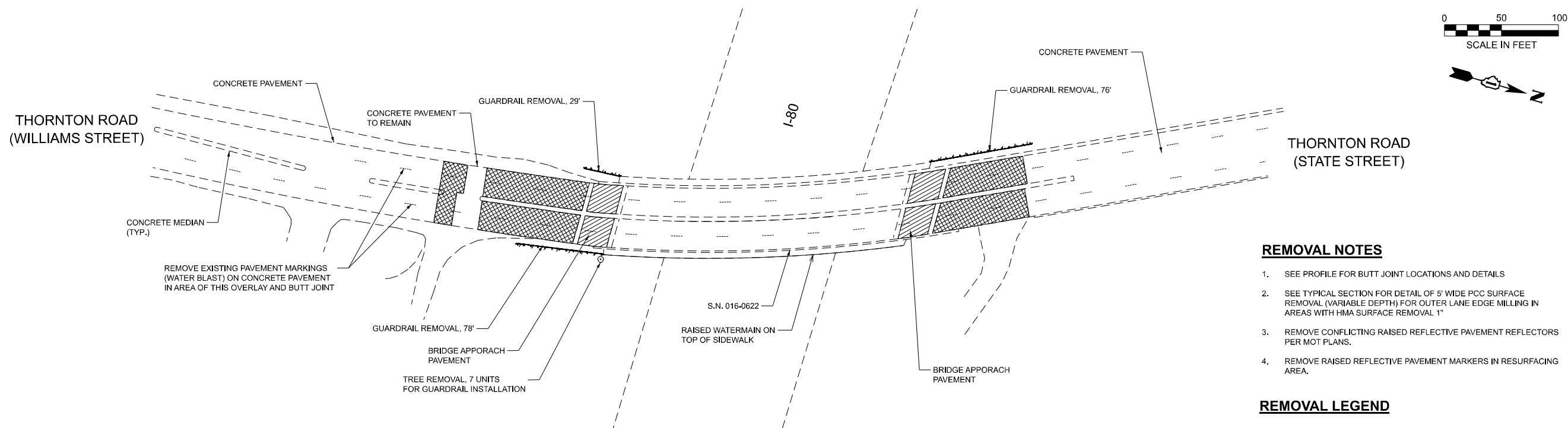
HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	4% @ 70 Gyr.	QC/QA
CLASS D PATCHING		
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	4% @ 70 Gyr.	QC/QA
QMP DESIGNATION: QUALITY CONTROL / QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

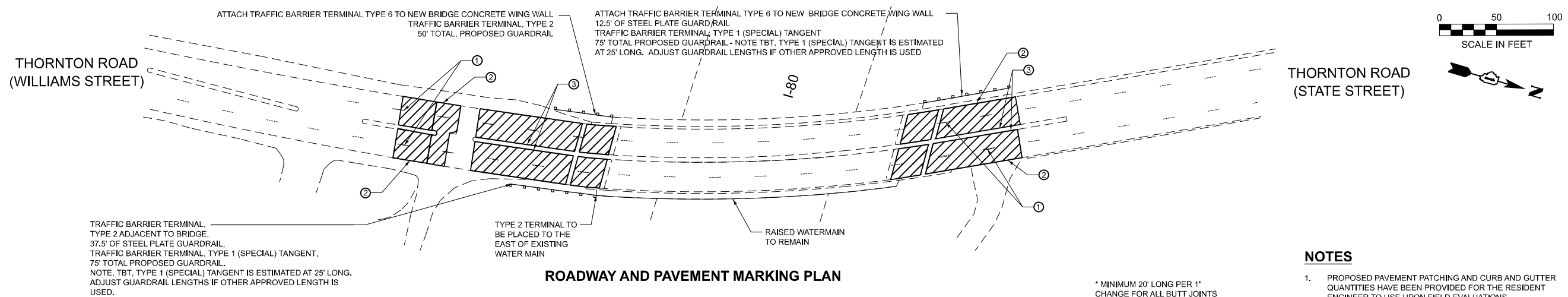


MODEL: Typical Sections
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REMOVAL NOTES

1. SEE PROFILE FOR BUTT JOINT LOCATIONS AND DETAILS
2. SEE TYPICAL SECTION FOR DETAIL OF 5' WIDE PCC SURFACE REMOVAL (VARIABLE DEPTH) FOR OUTER LANE EDGE MILLING IN AREAS WITH HMA SURFACE REMOVAL 1"
3. REMOVE CONFLICTING RAISED REFLECTIVE PAVEMENT REFLECTORS PER MOT PLANS.
4. REMOVE RAISED REFLECTIVE PAVEMENT MARKERS IN RESURFACING AREA.



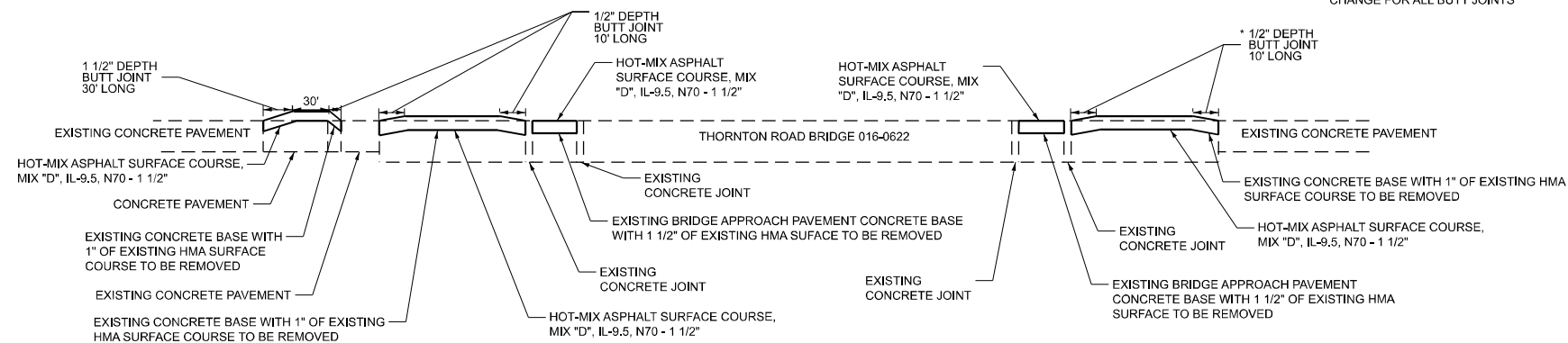
NOTES

1. PROPOSED PAVEMENT PATCHING AND CURB AND GUTTER QUANTITIES HAVE BEEN PROVIDED FOR THE RESIDENT ENGINEER TO USE UPON FIELD EVALUATIONS.
2. REPLACE IN LOCATION THE REMOVED RAISED REFLECTIVE PAVEMENT REFLECTORS WITH NEW RAISED REFLECTIVE PAVEMENT REFLECTORS. INSTALL NEW RAISED REFLECTIVE PAVEMENT MARKERS IN RESURFACED AREAS.
3. PROTECT RAISED WATERMAIN ON TOP OF EAST SIDE BRIDGE SIDEWALK.
4. EXCAVATION MATERIAL DISTURBED FOR WING WALL CONSTRUCTION TO BE REUSED AS NEEDED FOR COMPACT EMBANKMENT, AS PER SECTION 205 OF SSRBC.

PROPOSED PAVEMENT MARKINGS

HMA PAVEMENT -

- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE 10' MARKING, 30' SKIP)
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID LINE)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID LINE)

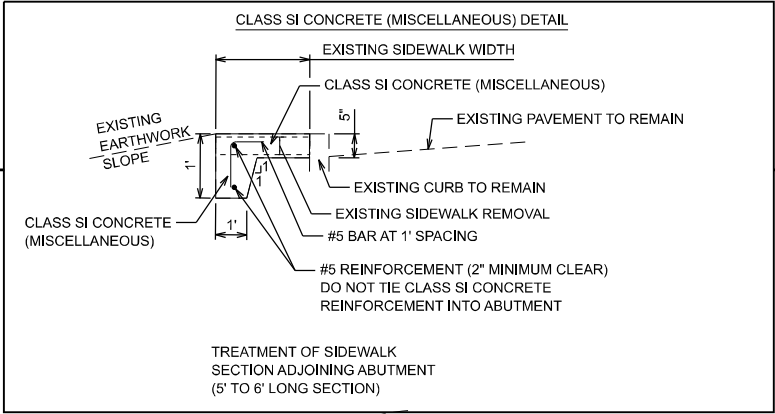
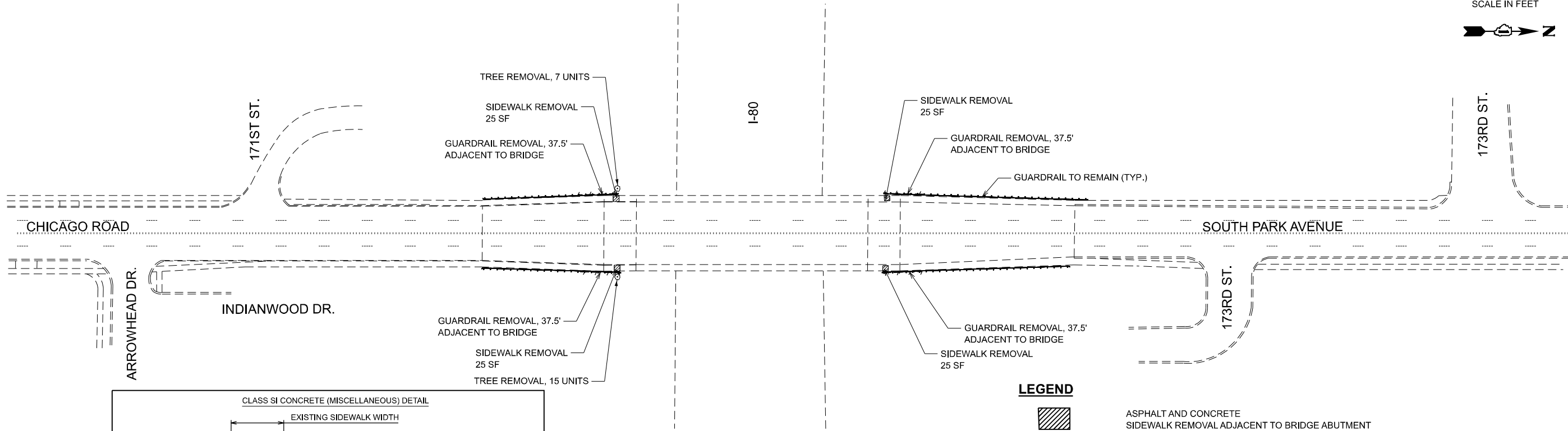
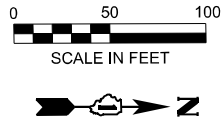


ROADWAY PROFILE

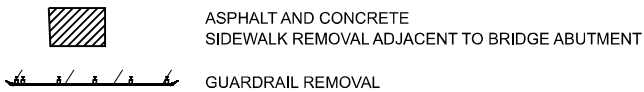
ROADWAY AND PAVEMENT MARKING PLAN THORNTON ROAD OVER I-80/94

F.A.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2906 2921	(T1-4)BR 24	COOK	34	9
		CONTRACT NO. 62X29		
		ILLINOIS	FED. AID PROJECT	

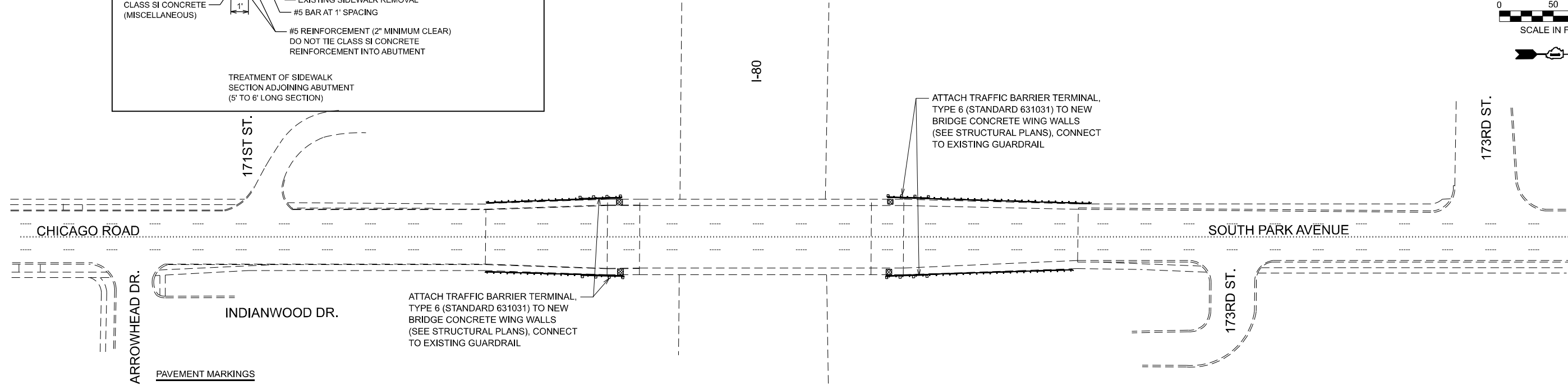
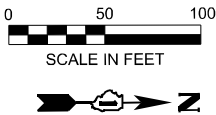
MODEL: 62X29 Chicago road plan existing (Sheet)
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LEGEND



REMOVAL PLAN

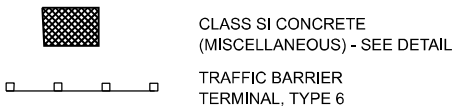


PAVEMENT MARKINGS

EXISTING PAVEMENT MARKINGS ARE COVERED BY PAVEMENT MARKING BLACKOUT TAPE 4" DURING CONSTRUCTION. IF DAMAGED DURING TAPE REMOVAL, REPLACE MARKINGS AS PER STANDARD TC-13 AS FOLLOWS:

- | | |
|---|--|
| ASPHALT PAVEMENT | - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (SOLID DOUBLE YELLOW, 11°C-C)
THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE 10' MARKING, 30' SKIP) |
| CONCRETE BRIDGE DECK
AND CONCRETE JOINTS | - MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (SOLID DOUBLE YELLOW, 11°C-C)
MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (WHITE 10' MARKING, 30' SKIP) |

LEGEND



NOTES


- EXCAVATION MATERIAL DISTURBED FOR WING WALL CONSTRUCTION TO BE REUSED AS NEEDED FOR COMPACTED EMBANKMENT, AS PER SECTION 205 OF SSRBC.

ROADWAY AND PAVEMENT MARKING PLAN

ROADWAY AND PAVEMENT MARKING PLAN
CHICAGO ROAD/SOUTH PARK AVENUE OVER I-80/94

SCALE: 1" = 50' SHEET 1 OF SHEETS STA. TO STA.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

 8501 W. Higgins Road Suite 280 Chicago, Illinois 60634 (773) 399-012	USER NAME = \$USERNAMES	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
	PLOT DATE = 4/17/2025	DATE -	REVISED -

F.A.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2906 2921	(T1-4)BR 24	COOK	34	10
CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				

MAINTENANCE OF TRAFFIC GENERAL NOTES

THE FOLLOWING NOTES ARE APPLICABLE TO BOTH THORNTON ROAD AND CHICAGO ROAD CONSTRUCTION, UNLESS OTHERWISE NOTED.

1.

THE TRAFFIC CONTROL DEPICTED HEREIN IS THE MINIMUM REQUIREMENT, ADDITIONAL TRAFFIC CONTROL DEVICES AS SPECIFIED IN THE HIGHWAY STANDARDS, SHOWN IN THE INDEX OF SHEETS, AND THE SPECIAL PROVISIONS SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL) UNLESS OTHERWISE INDICATED.
2.

DURING CONSTRUCTION STAGING OPERATIONS, THE ENGINEER AND THE VILLAGE OF THORNTON PUBLIC WORKS DIRECTION BRYAN ROBERTS (EMAIL ADDRESS: BROBERTS@THORNTONIL.US) SHALL BE NOTIFIED IN WRITING 48 HOURS PRIOR TO STREET CLOSURES ON THORNTON ROAD (ALSO KNOWN AS WILLIAMS STREET AND STATE STREET) AND CHICAGO ROAD (ALSO KNOWN AS SOUTH PARK AVENUE) AND ANY CHANGE IN CONSTRUCTION STAGING.
3.

THE FURNISHING, INSTALLATION, AND RELOCATION OF ALL TRAFFIC SIGNS SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL). ALL CONFLICTING TRAFFIC SIGNS SHALL BE COVERED AS DETERMINED BY THE ENGINEER. THIS SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).
4.

COORDINATE WITH PACE BUS FOR CHICAGO ROAD BUS ROUTE.
5.

WHEN WORK DOES NOT IMPACT SIDEWALKS AND WHEN PEDESTRIANS CAN SAFELY BE SHIFTED TO THE WORK ZONE SIDEWALK, SHIFT PEDESTRIANS TO WORK ZONE SIDEWALK AWAY FROM VEHICULAR TRAFFIC SO PEDESTRIANS ARE NOT IMMEDIATELY ADJACENT TO ACTIVE TRAFFIC LANES. UTILIZE STANDARD 701801 AND SEPARATE WORK ZONE FROM PEDESTRIANS BY TYPE II BARRICADES OR DRUMS.
6.

THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
7.

ALL EXISTING LANE RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS LOCATED WITHIN TEMPORARY LANE CLOSURE TAPERS, LANE SHIFT TAPERS, OR IN LOCATIONS THAT CONFLICT WITH THE TEMPORARY PAVEMENT MARKING TAPE USED FOR STAGING SHALL BE REMOVED IF THE STAGING WILL REMAIN IN PLACE FOR MORE THAN FOUR (4) DAYS. THE EXISTING RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS THAT WERE REMOVED SHALL BE RESTORED IN-KIND AFTER THE COMPLETION OF THE STAGING.
8.

CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO (2) WEEKS PRIOR TO ALL ROAD CLOSURE, TRAFFIC STAGE CHANGES, AND NEW TRAFFIC SIGNAL TURN-ON EVENTS ON EACH APPROACH OF THE AFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGNS SHALL BE REMOVED TWO (2) WEEKS THEREAFTER UNLESS THE SIGNS ARE NEEDED AGAIN FOR A SUBSEQUENT FUTURE EVENT THAT WILL OCCUR WITHIN TWO (2) WEEKS ON THE SAME APPROACH OF THE AFFECTED ROADWAY. THE SIGNS SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
9.

TEMPORARY INFORMATION SIGNS ON TEMPORARY SUPPORTS SHALL BE PROVIDED FOR ALL COMMERCIAL DRIVEWAYS THAT ARE LOCATED WITHIN A WORK AREA. THIS WORK SHALL BE PAID FOR PER DISTRICT 1 DETAIL TC-26. THESE SIGNS SHALL BE RELOCATED AS REQUIRED FOR EACH CONSTRUCTION STAGE AND SHALL BE PLACED AS DIRECTED BY THE ENGINEER. THIS SIGN RELOCATION WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR TEMPORARY INFORMATION SIGNING.

SEQUENCE OF CONSTRUCTION NOTES - THORNTON ROAD

STAGE 1

1.

SHIFT NORTHBOUND TRAFFIC TO THE OUTSIDE NORTHBOUND LANE.
2.

SHIFT SOUTHBOUND TRAFFIC TO THE INSIDE NORTHBOUND LANE.
3.

PERFORM BRIDGE DECK PATCHING, JOINT REPAIRS, AND CONCRETE END SECTION WORK AT WING WALL.
4.

REMOVE AND REPLACE WESTSIDE GUARDRAILS AS SHOWN ON PLANS.
5.

MILL EXISTING PAVEMENT AND CONSTRUCT NEW HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70 - 1 1/2".
6.

INSTALL PERMANENT PAVEMENT MARKINGS.

STAGE 2

1.

THE JOINT REPAIRS, GUARDRAIL REMOVAL AND REPLACEMENT, ASPHALT PAVEMENT PATCHING, MILLING AND RESURFACING ON THE NORTHBOUND LANES ARE TO BE SHORT-TERM (24 HOURS OR LESS) PROCESSES AND THEREFORE, LONGTERM STAGING AND TEMPORARY RAMPS ARE NOT PROVIDED.
2.

FOLLOW IDOT STANDARDS 701606 AND 701801 FOR SHORT TERM STAGING FOR THE NORTHBOUND LANES.

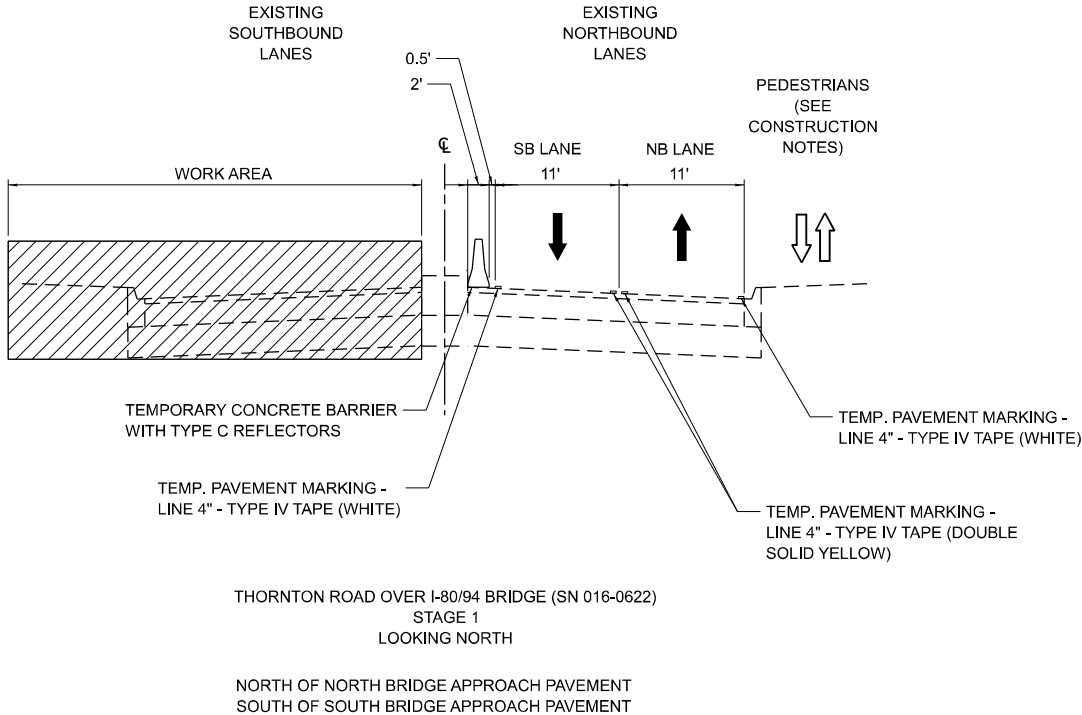
CONSTRUCTION NOTES - THORNTON ROAD

1.

EXISTING PAVEMENT MARKINGS ARE TO REMAIN EXCEPT IN AREAS OF MILLING AND RESURFACING. THEY WILL BE COVERED BY BLACKOUT TAPE DURING MOT.
2.

SHIFT EAST SIDEWALK PEDESTRIANS TO WEST SIDEWALK AS SOON AS OPERATIONS IMPACTING WEST SIDEWALK ARE COMPLETED, SO AS TO REDUCE TIME PEDESTRIANS ARE ADJACENT TO MOT OPERATIONS IN NORTHBOUND LANES. USE STANDARD 701801-06.
3.

EXISTING CENTER RAISED MEDIAN TO REMAIN. MOT LAYOUT HAS BEEN EXTENDED SO EXISTING CENTER RAISED MEDIAN REMAINS IN PLACE.



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED MOT GENERAL NOTES & TYPICAL SECTIONS
THORNTON ROAD (S.N. 016-0622)

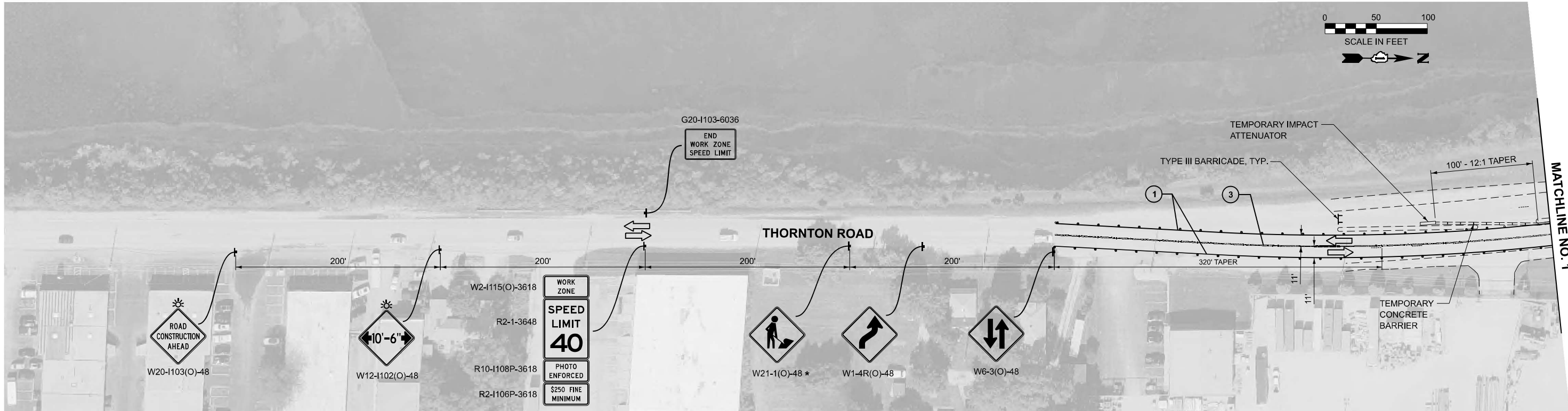
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F.A.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2906 2921	(T1-4)BR 24	COOK	34	11
CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				

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8501 W. Higgins Road Suite 280
Chicago, Illinois 60634 (773) 399-012

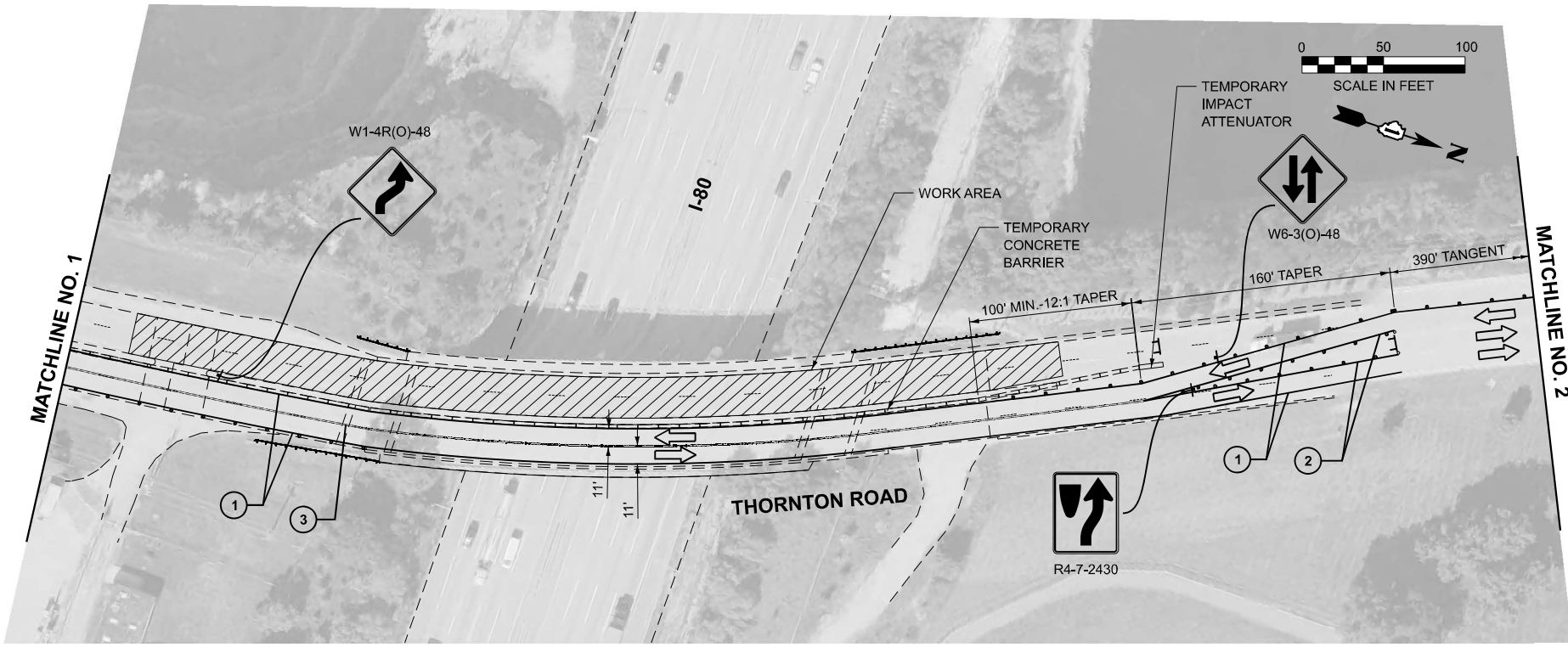
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	DRAWN -	REVISED -
	CHECKED -	REVISED -
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MOT LEGEND

- WORK AREA
- DRUMS OR TYPE II BARRICADES, 20' SPACING ON TAPERS, 25' SPACING ON TANGENTS
- TYPE III BARRICADE WITH FLASHING AMBER LIGHTS
- DIRECTION OF TRAFFIC FLOW
- TEMPORARY SIGN
- FLASHING AMBER LIGHT
- ARROW BOARD
- TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (WHITE)
- TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (YELLOW)
- TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (DOUBLE SOLID YELLOW)
- INDICATES SIGN TO BE REMOVED WHEN WORKERS ARE NOT PRESENT

NOTE: ALL EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH MOT PLANS SHALL BE COVERED WITH PAVEMENT MARKING BLACKOUT TAPE, 4"



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DATE	-

DESIGNED	-
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DESIGNED	-
REVIS	-
REVIS	-
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REVIS	-

F.A.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2906 2921	(T1-4)BR 24	COOK	34	14
CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				

SEQUENCE OF CONSTRUCTION NOTES - CHICAGO ROAD

STAGE 1

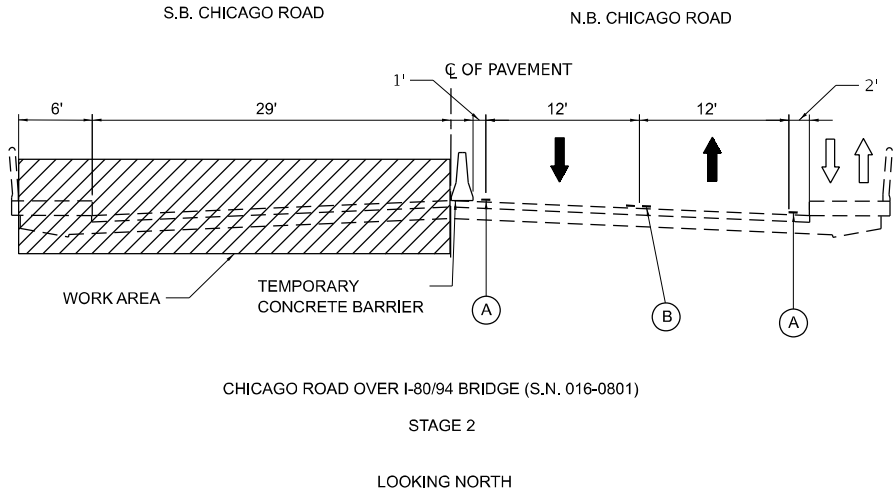
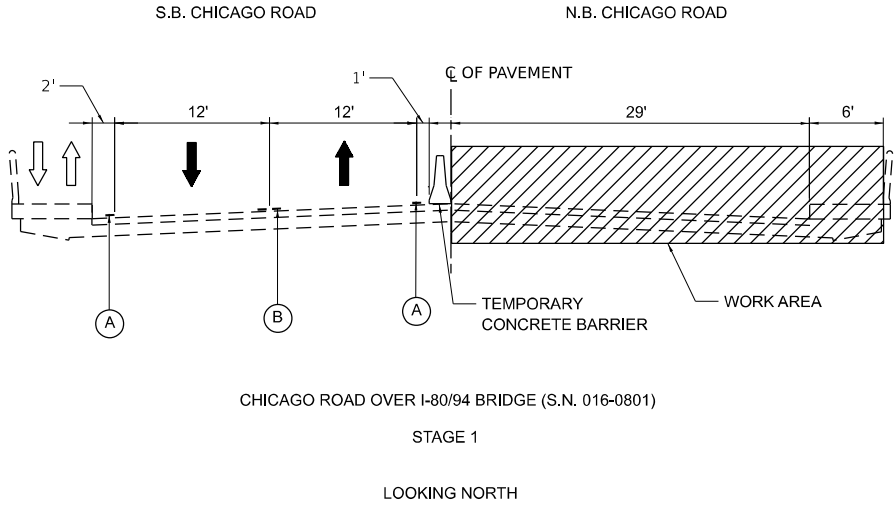
- SHIFT SOUTHBOUND TRAFFIC TO THE FAR WEST LANE OF CHICAGO ROAD, SHIFT NORTHBOUND TRAFFIC TO THE INNER WEST LANE OF CHICAGO ROAD.
- PERFORM BRIDGE DECK PATCHING, JOINT REPAIRS, AND CONCRETE END SECTION AT WINGWALL WORK.
- REMOVE AND REPLACE EASTSIDE GUARDRAILS AS SHOWN ON THE PLANS.
- REMOVE SIDEWALK AND PLACE CLASS SI CONCRETE AS PER DETAIL.

STAGE 2

- SHIFT NORTHBOUND TRAFFIC TO THE FAR EAST LANE OF CHICAGO ROAD, SHIFT SOUTHBOUND TRAFFIC TO THE INNER EAST LANE OF CHICAGO ROAD.
- PERFORM BRIDGE DECK PATCHING, AND JOINT REPAIRS.
- REMOVE AND REPLACE WESTSIDE GUARDRAILS AS SHOWN ON THE PLANS.
- REMOVE SIDEWALK AND PLACE CLASS SI CONCRETE AS PER DETAIL.

CONSTRUCTION NOTES - CHICAGO ROAD












- EXISTING PAVEMENT MARKINGS ARE TO REMAIN EXCEPT IN AREAS OF MILLING AND RESURFACING. THEY WILL BE COVERED BY BLACKOUT TAPE DURING MOT.



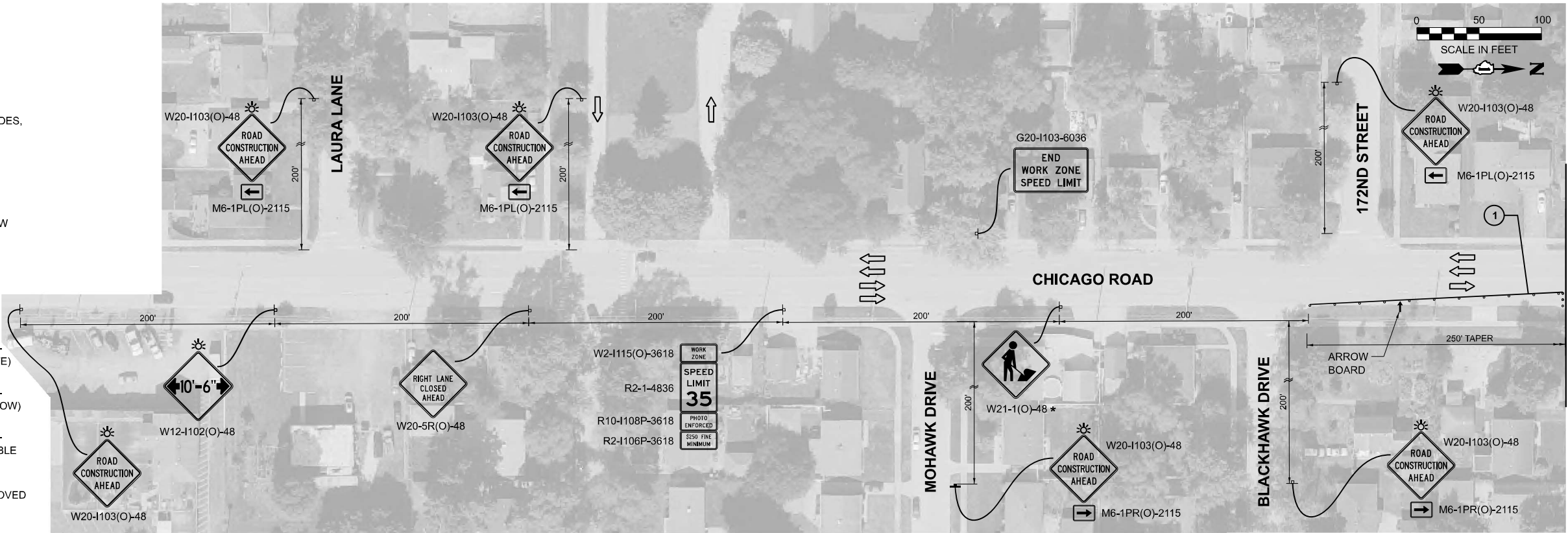
LEGEND

- DIRECTION OF TRAFFIC
- DIRECTION OF TRAFFIC - PEDESTRIANS
- WORK AREA
- TEMPORARY CONCRETE BARRIER
- PAVEMENT MARKING TAPE, TYPE IV - LINE 4" (WHITE)
- PAVEMENT MARKING TAPE, TYPE IV - LINE 4" (DOUBLE SOLID YELLOW)

MOT LEGEND

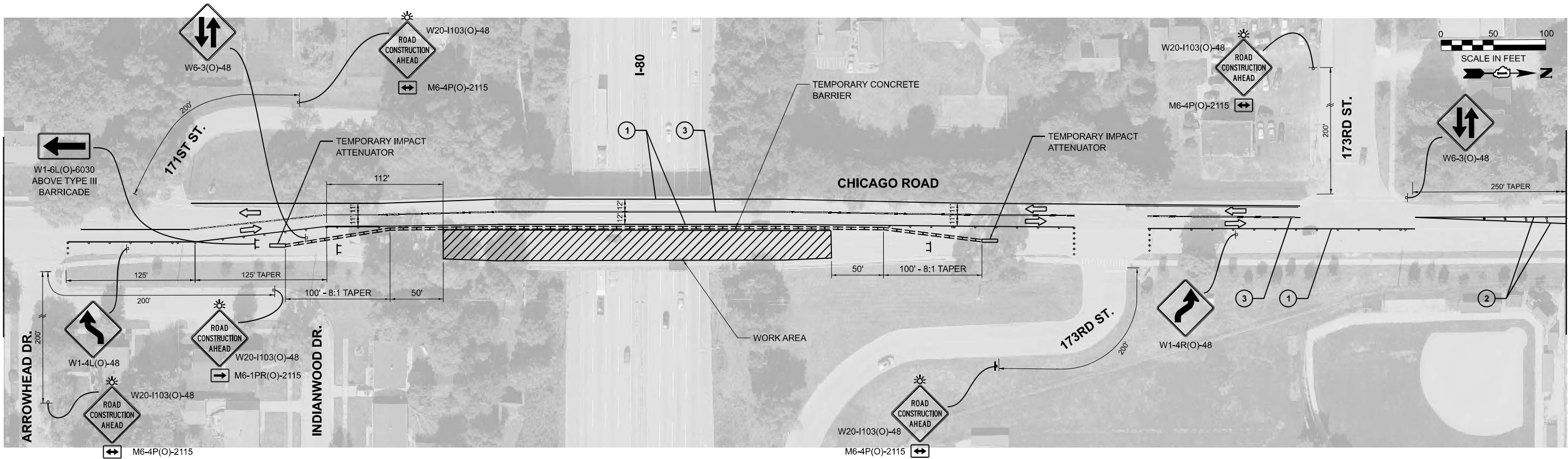
-  — WORK AREA
-  — DRUMS OR TYPE II BARRICADES, 20' SPACING ON TAPERS, 25' SPACING ON TANGENTS
-  — TYPE III BARRICADE WITH FLASHING AMBER LIGHTS
-  — DIRECTION OF TRAFFIC FLOW
-  — TEMPORARY SIGN
-  — FLASHING AMBER LIGHT
-  — ARROW BOARD
-  — TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (WHITE)
-  — TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (YELLOW)
-  — TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (DOUBLE SOLID YELLOW)
-  — INDICATES SIGN TO BE REMOVED WHEN WORKERS ARE NOT PRESENT

NOTE: ALL EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH MOT PLANS SHALL BE COVERED WITH PAVEMENT MARKING BLACKOUT TAPE, 4"



MATCHLINE NO. 1

MATCHLINE NO. 1

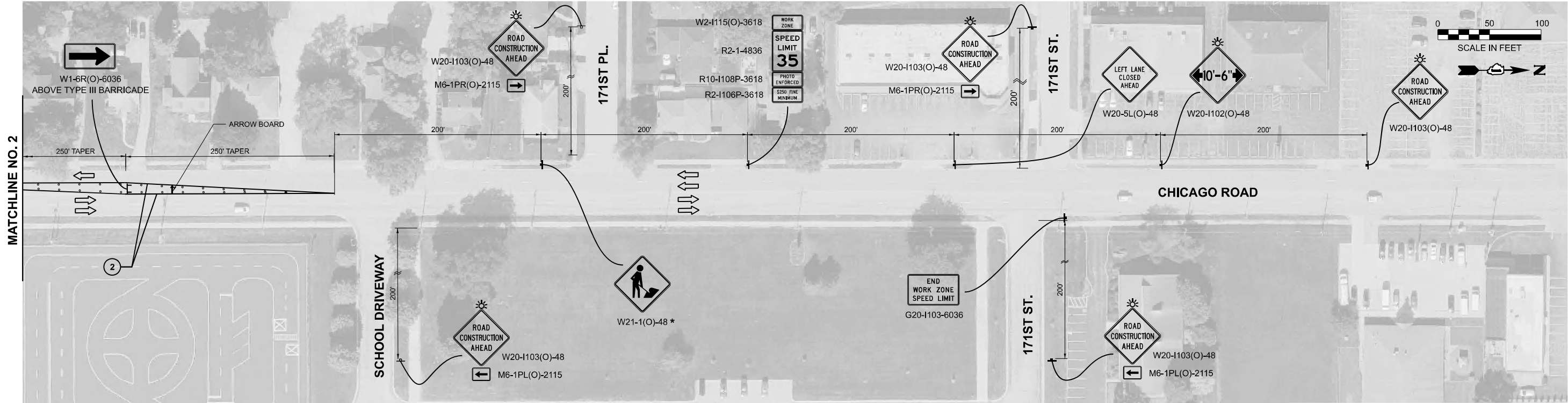


MATCHLINE NO. 2

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F.A.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				



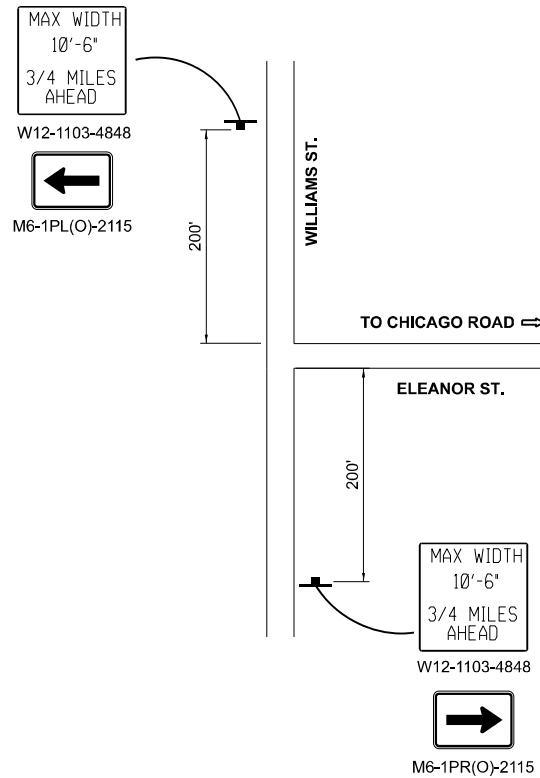
MOT LEGEND

- WORK AREA
- DRUMS OR TYPE II BARRICADES, 20' SPACING ON TAPERS, 25' SPACING ON TANGENTS
- TYPE III BARRICADE WITH FLASHING AMBER LIGHTS
- DIRECTION OF TRAFFIC FLOW
- TEMPORARY SIGN
- FLASHING AMBER LIGHT
- ARROW BOARD
- TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (WHITE)
- TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (YELLOW)
- TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (DOUBLE SOLID YELLOW)
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NOTE: ALL EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH MOT PLANS SHALL BE COVERED WITH PAVEMENT MARKING BLACKOUT TAPE, 4"

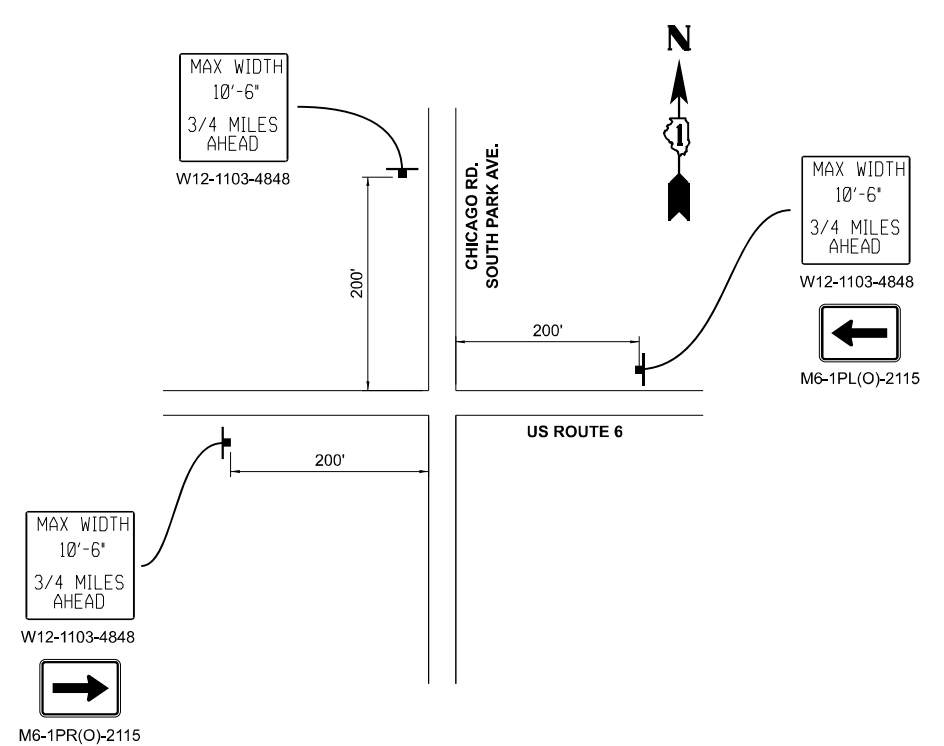
ADVANCE WARNING PLAN - SOUTH

TO REMAIN IN-PLACE FOR STAGES 1 AND 2



ADVANCE WARNING PLAN - NORTH

TO REMAIN IN-PLACE FOR STAGES 1 AND 2



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8501 W. Higgins Road Suite 280
Chicago, Illinois 60634 (773) 399-012

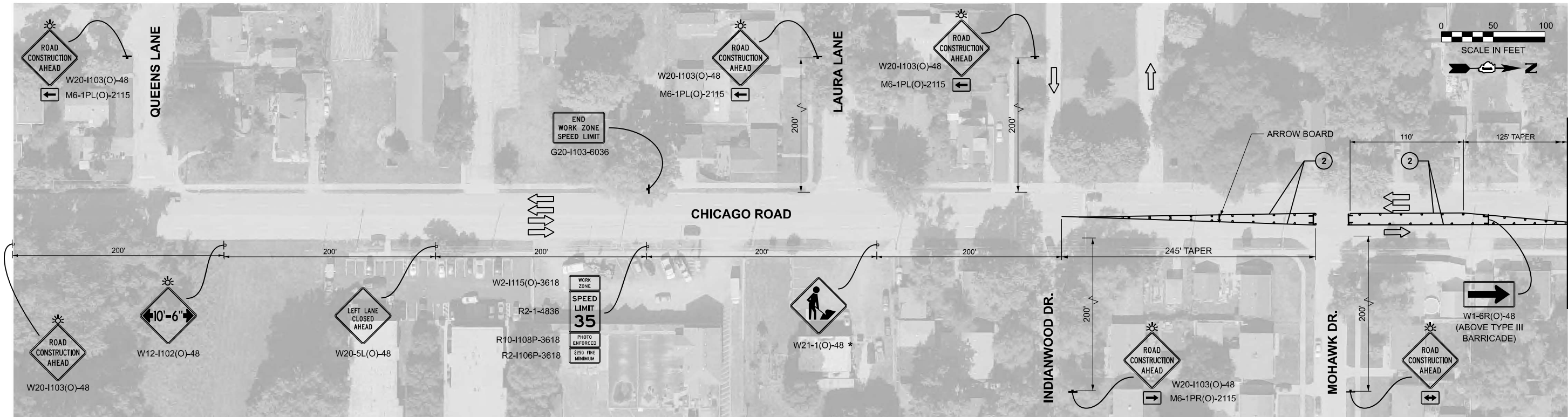
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PLOT DATE = 4/17/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED MAINTENANCE OF TRAFFIC PLAN - STAGE 1
CHICAGO ROAD/SOUTH PARK AVENUE (S.N. 016-0801)

SCALE: 1" = 50' SHEET 2 OF 2 SHEETS STA. TO STA.

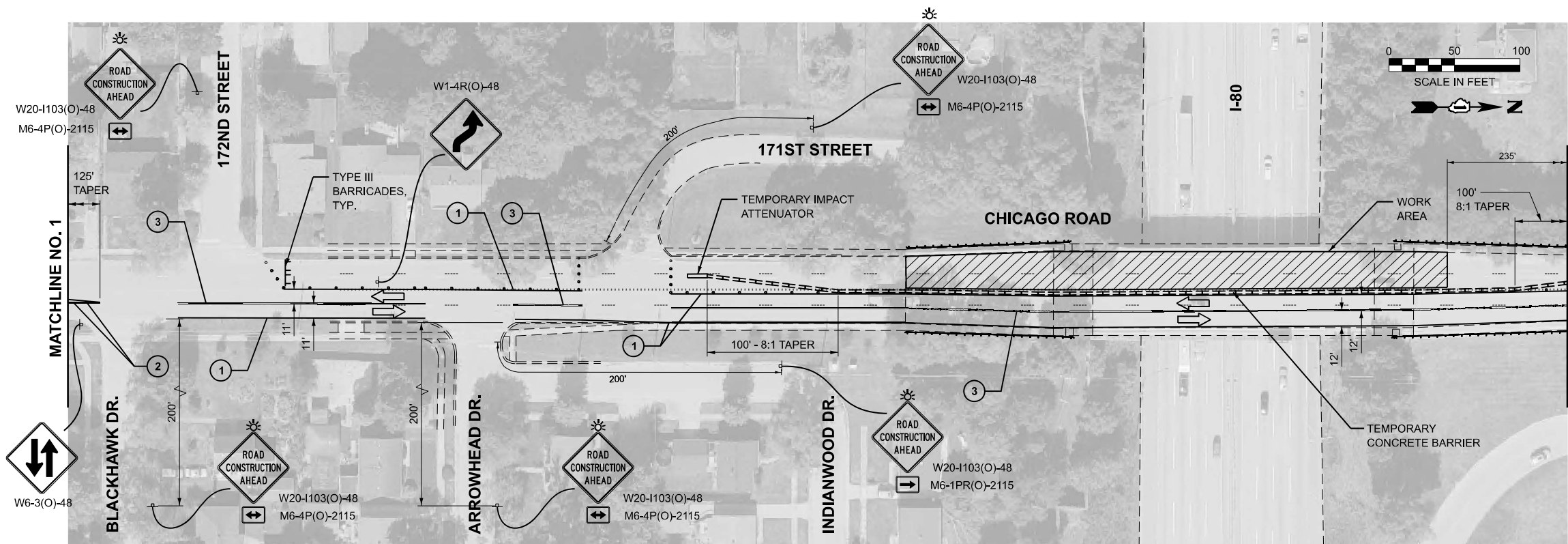
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2906 2921	(T1-4)BR 24	COOK	34	16
CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				



MOT LEGEND

- WORK AREA
- DRUMS OR TYPE II BARRICADES, 20' SPACING ON TAPERS, 25' SPACING ON TANGENTS
- TYPE III BARRICADE WITH FLASHING AMBER LIGHTS
- DIRECTION OF TRAFFIC FLOW
- TEMPORARY SIGN
- FLASHING AMBER LIGHT
- ARROW BOARD
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NOTE: ALL EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH MOT PLANS SHALL BE COVERED WITH PAVEMENT MARKING BLACKOUT TAPE, 4"



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Chicago, Illinois 60634 (773) 399-062

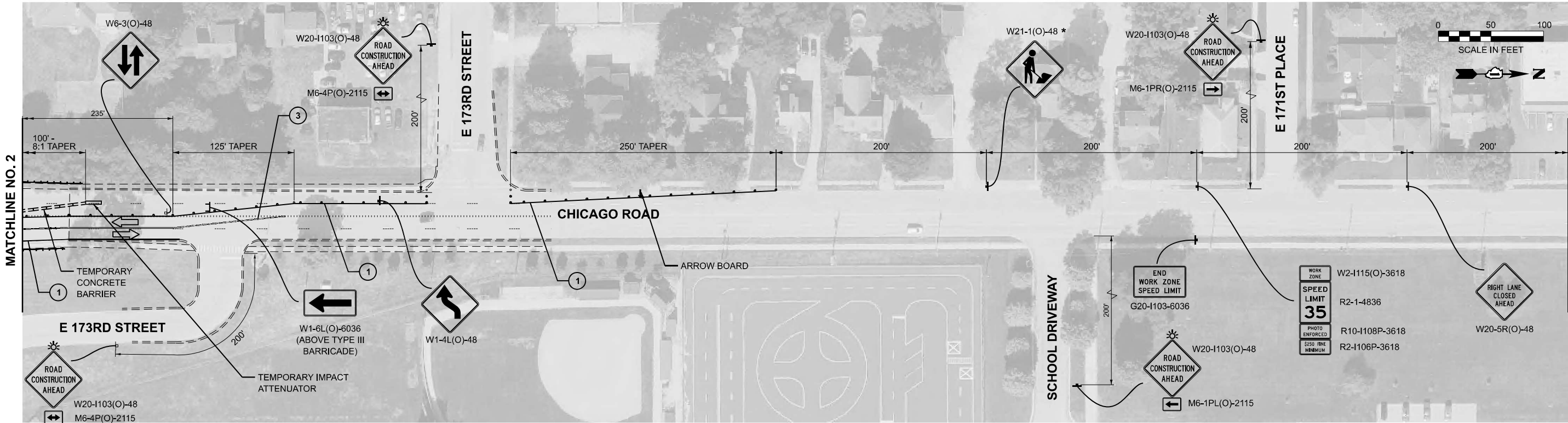
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED MAINTENANCE OF TRAFFIC PLAN - STAGE 2
CHICAGO ROAD/SOUTH PARK AVENUE (S.N. 016-0801)

SCALE: 1" = 50' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2906 2921	(T1-4)BR 24	COOK	34	17
CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				



MOT LEGEND

- WORK AREA
- DRUMS OR TYPE II BARRICADES, 20' SPACING ON TAPERS, 25' SPACING ON TANGENTS
- TYPE III BARRICADE WITH FLASHING AMBER LIGHTS
- DIRECTION OF TRAFFIC FLOW
- TEMPORARY SIGN
- FLASHING AMBER LIGHT
- ARROW BOARD
- TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (WHITE)
- TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (YELLOW)
- TEMP. PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (DOUBLE SOLID YELLOW)
- INDICATES SIGN TO BE REMOVED WHEN WORKERS ARE NOT PRESENT

NOTE: ALL EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH MOT PLANS SHALL BE COVERED WITH PAVEMENT MARKING BLACKOUT TAPE, 4"



MODEL: SN016-0622-MOT-stage1-01
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8501 W. Higgins Road, Suite 280
Chicago, Illinois 60634 (773) 399-012

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	DRAWN -	REVISED -
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PLOT DATE = 4/17/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED MAINTENANCE OF TRAFFIC PLAN - STAGE 2
CHICAGO ROAD/SOUTH PARK AVENUE (S.N. 016-0801)

SCALE: 1" = 50' SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2906 2921	(T1-4)BR 24	COOK	34	18
CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				

Traffic will be maintained utilizing stage construction, see Roadway plans.

No Salvage:

Allow 25#/sq. ft for future wearing surface

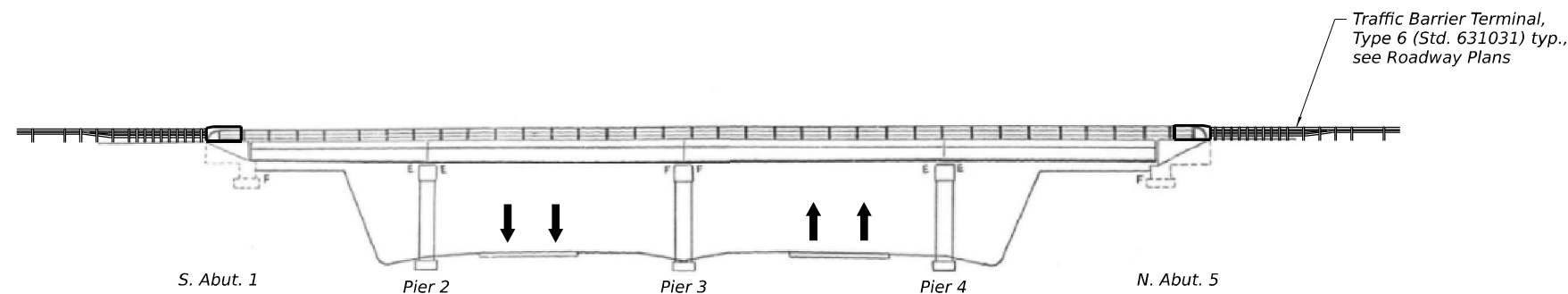
1992 AASHTO Standard Specifications
with 1993 and 1994 Interims

FIELD UNITS (EXIST. CONST.)

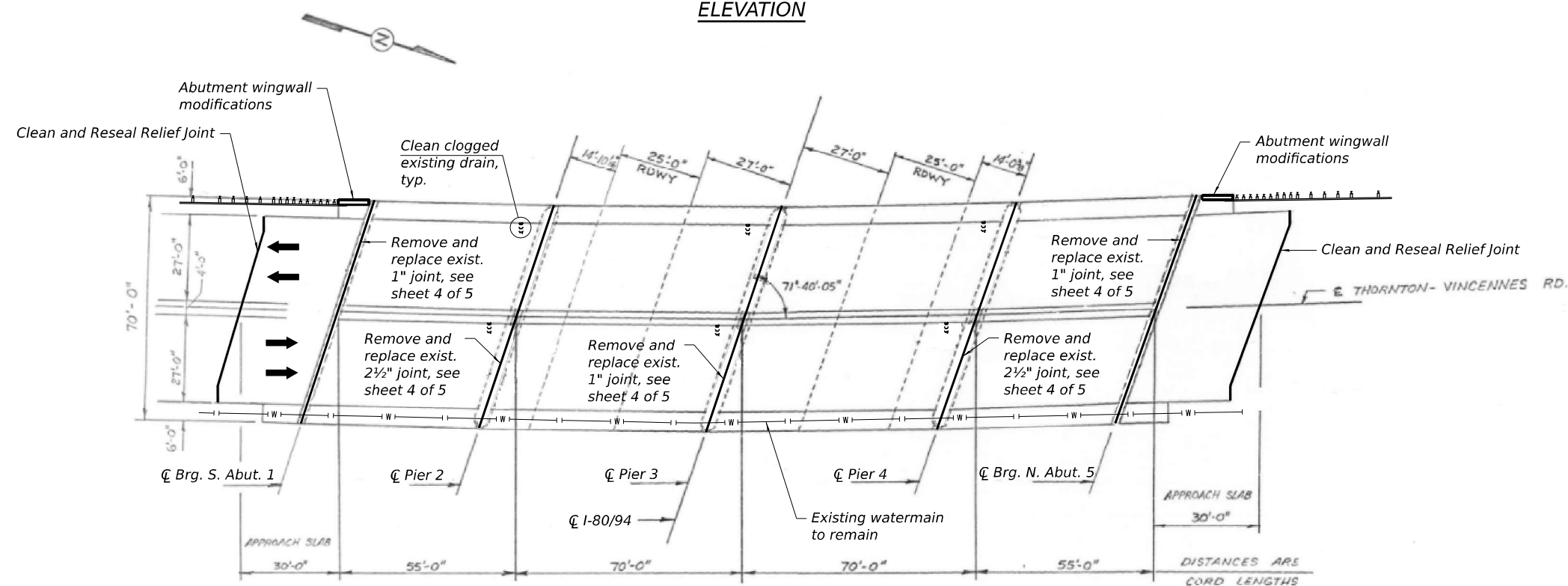
$f'_c = 3,500 \text{ psi}$ (Superstructure)
 $f'_c = 800 \text{ psi}$ (Substructure)
 $f_y = 60,000 \text{ psi}$ (Superstructure Reinforcement)
 $f_y = 20,000 \text{ psi}$ (Substructure Reinforcement)
 $f_y = 18,000 \text{ psi}$ (1954 Struct. Steel)
 $f_y = 20,000 \text{ psi}$ (2001 Struct. Steel)

FIELD UNITS (NEW CONST.)

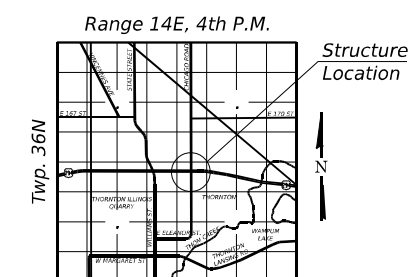
$$f_c = 4,000 \text{ psi}$$

$$f_y = 60,000 \text{ psi (Reinforcement)}$$


ELEVATION



PLAN



LOCATION SKETCH

GENERAL PLAN & ELEVATION
THORNTON - VINCENNES RD.
OVER I-80/94
F.A.U. RTE. 2906
COOK COUNTY
STRUCTURE NO. 016-0622

MODEL: Default
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SCOTT H. HRNICH
081-005750
LICENSED
STRUCTURAL
ENGINEER
OF
ILLINOIS

ENGINEER FULL NAME: SCOTT HINRICHS DATE: 04-11-2025
ILLINOIS REGISTERED ENGINEER NO. 081-005750
REGISTRATION EXPIRES 11. 30. 2026

USER NAME =	DESIGNED - KM	REVISED -	<div>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</div>						F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	CHECKED - CG	REVISED -		2006, 2021	(T1-4)BR 24					COOK	34	19	
PLOT SCALE =	DRAWN - DCP	REVISED -							CONTRACT NO. 62X29				
PLOT DATE =	CHECKED - VG	REVISED -		SHEET 1 OF 5 SHEETS					ILLINOIS	FED. AID PROJECT			

GENERAL NOTES:

1.

Reinforcement bars designated (E) shall be epoxy coated.
2.

Plan dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
3.

The Contractor shall use extreme care during concrete removal so as not to damage the PPC I-Beam.
4.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50°F.
5.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system at the Contractor's expense.
6.

Epoxy grout d(E) and d1(E) bars in 9" minimum holes according to Article 584 of Standard Specifications.
7.

The Contractor shall use extreme care when reconstructing the proposed joint through the east sidewalk so as not to damage the existing watermain.
8.

The Contractor shall coordinate this work with the Village of Thornton.

INDEX OF SHEETS

1.

General Plan and Elevation
2.

General Data
3.

Top of Deck Repairs.
4.

Joint Repair Details
5.

Wingwall Modification Details

SCOPE OF WORK

1.

Replace all joints with preformed joint seal and repair headers with concrete polymer.
2.

Repair of Curb Spalls.
3.

Clean and Reseal Relief Joints.
4.

Modify the existing wingwall at SW and NW approach corners to accommodate the upgraded Type 6 guardrail terminal connection.
5.

Clean Bridge Scuppers and Downspouts.
6.

Apply protective coating to the front face and top surfaces of new concrete parapets, the exposed faces of new concrete wingwalls, and location of Deck Slab Repair & Approach Slab Repair.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd	5.4	-	5.4
Concrete Structure	Cu Yd	5.6	-	5.6
Protective Coat	Sq Yd	30	-	30
Reinforcement Bars, Epoxy Coated	Pound	850	-	850
Preformed Joint Seal 1"	Foot	222	-	222
Preformed Joint Seal 2 1/2"	Foot	148	-	148
Controlled Low-Strength Material	Cu Yd	-	2.2	2.2
Cleaning Bridge Scuppers and Downspouts	Each	6	-	6
Clean & Reseal Relief Joint	Foot	124	-	124
Approach Slab Repair (Partial Depth)	Sq Yd	0.6	-	0.6
Structural Repair of Concrete (Depth Equal to or Less than 5 inches	Sq Ft	39	-	39
Deck Slab Repair (Partial Depth)	Sq Yd	1.4	-	1.4
Polymer Concrete	Cu Ft	46.1	-	46.1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

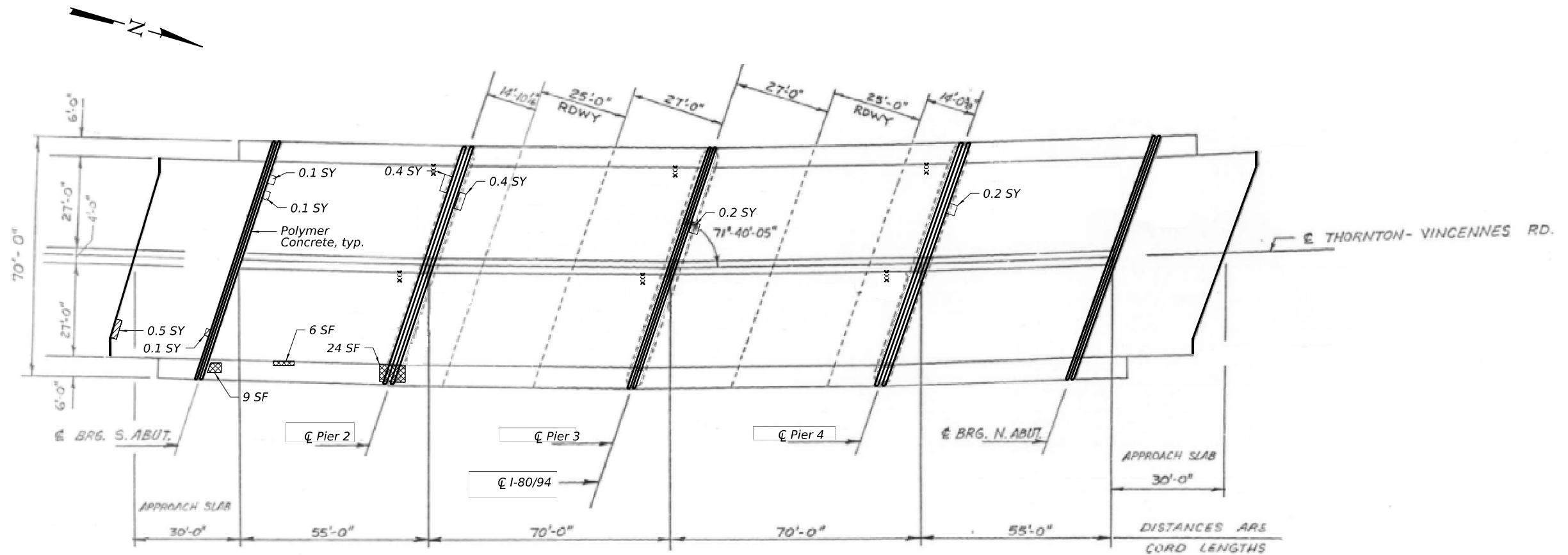
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SHEET 2 OF 5 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62X29				
		ILLINOIS	FED. AID PROJECT	

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

PLAN

- Notes:
- See Sheet 4 of 5 for Joint repair details.

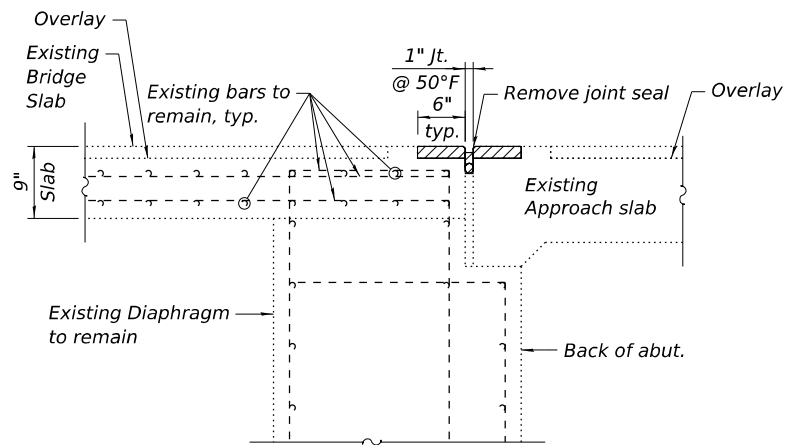
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu Yd	1.7
Protective Coat	Sq Yd	6
Preformed Joint Seal 1"	Foot	222
Preformed Joint Seal 2 1/2"	Foot	148
Cleaning Bridge Scuppers and Downspouts	Each	6
Clean & Reseal Relief Joint	Foot	124
Approach Slab Repair (Partial Depth)	Sq Yd	0.6
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq Ft	39
Deck Slab Repair (Partial Depth)	Sq Yd	1.4
Polymer Concrete	Cu Ft	46.1

LEGEND

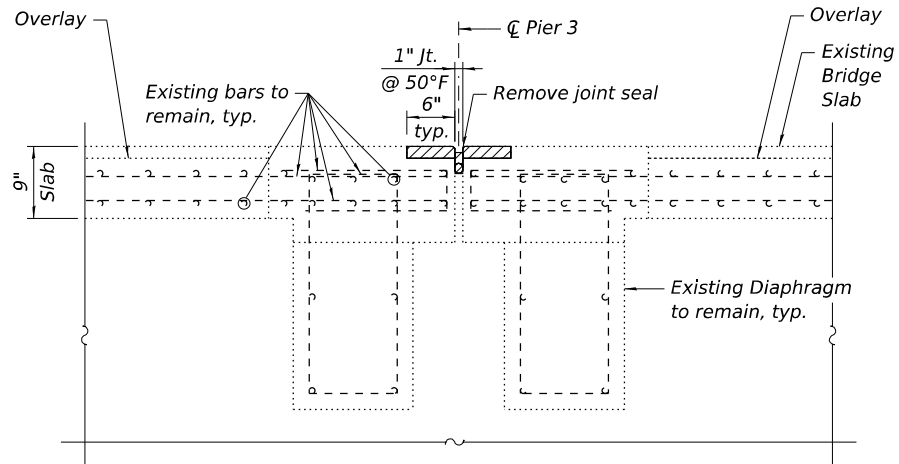
-  Structural Repair of Concrete (Depth Equal to or Less than 5 inches)
-  Approach Slab Repair (Partial Depth)
-  Deck Slab Repair (Partial Depth)

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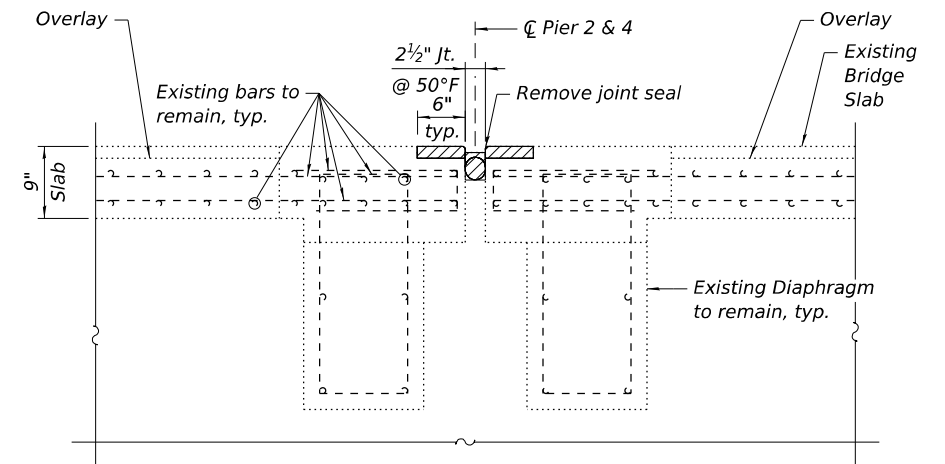


EXISTING FIXED JOINT REMOVAL

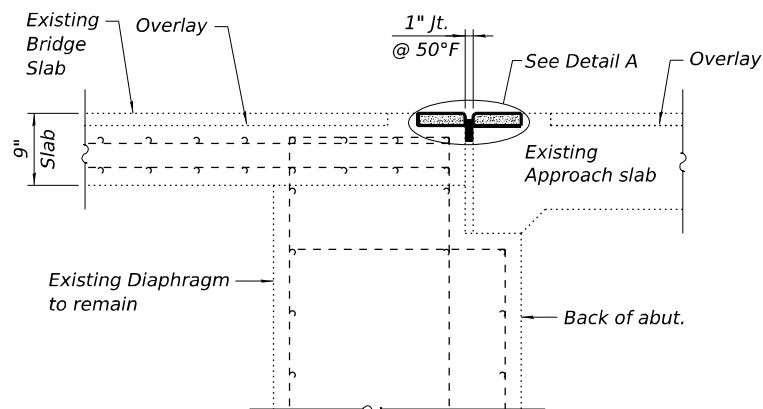
(North abut. shown South abut. similar)



EXISTING FIXED JOINT REMOVAL - PIER 3

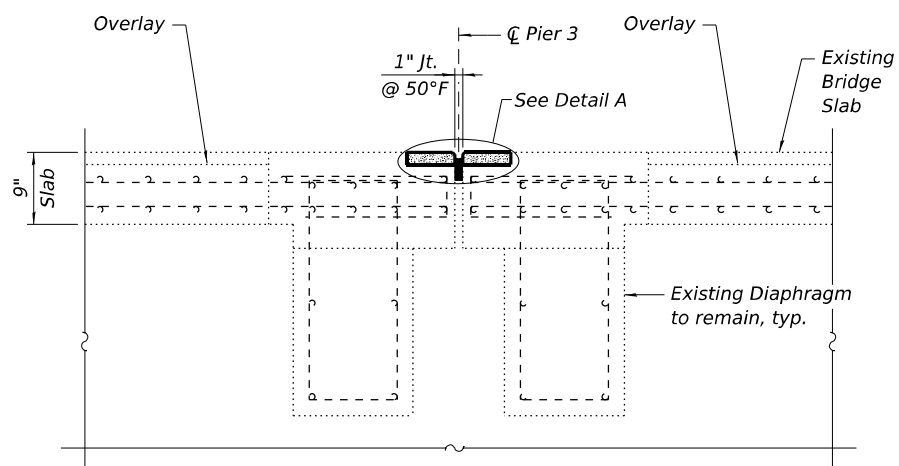


EXISTING EXPANSION JOINT REMOVAL - PIERS 2 & 4

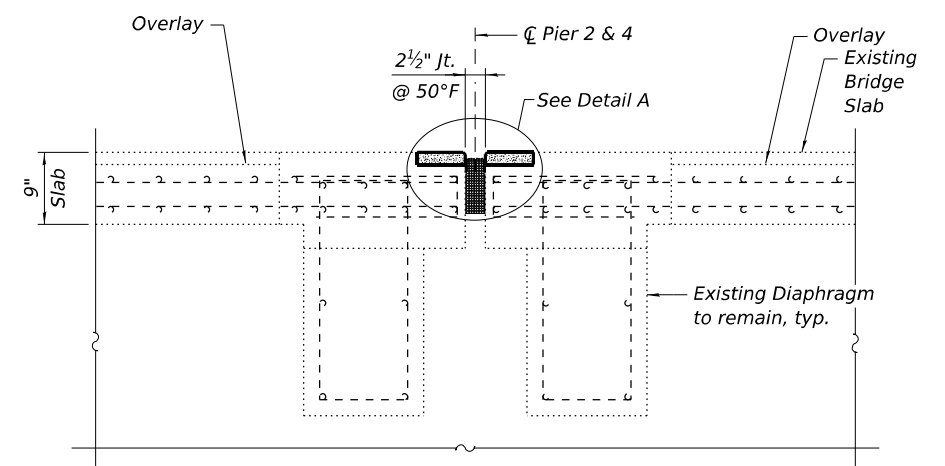


PROPOSED FIXED JOINT

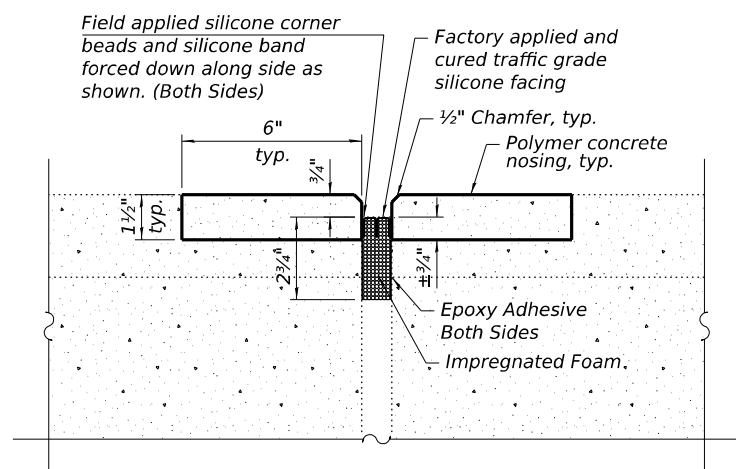
(North abut. shown South abut. similar)



PROPOSED FIXED JOINT - PIER 3



PROPOSED EXPANSION JOINT - PIERS 2 & 4



DETAIL A

LEGEND

	Existing Concrete/ Joint Removal
	Polymer concrete nosing

MODEL: Default
FILE NAME: X:\012025\20250016\Design\CADD\Structures\Sheets\Plan\SN-016-0622\0160622-62X29-004-EXP.dgn

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Chicago, Illinois 60634 (773) 399-002

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PLOT SCALE =	DRAWN - DCP	REVISED -
PLOT DATE =	CHECKED - VG	REVISED -

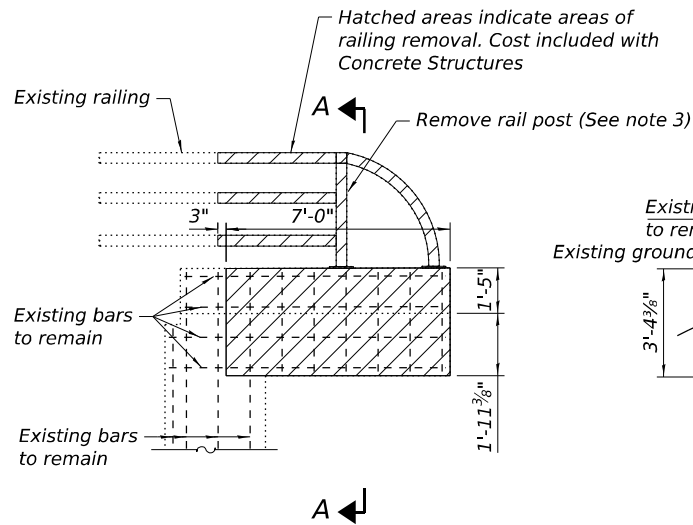
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JOINT REPAIR DETAILS
STRUCTURE NO. 016-0622

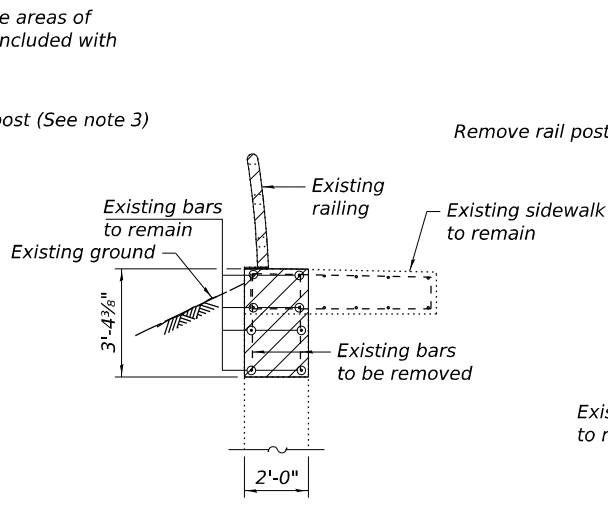
SHEET 4 OF 5 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2908, 2921	(T1-4)BR 24	COOK	34	22
CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				

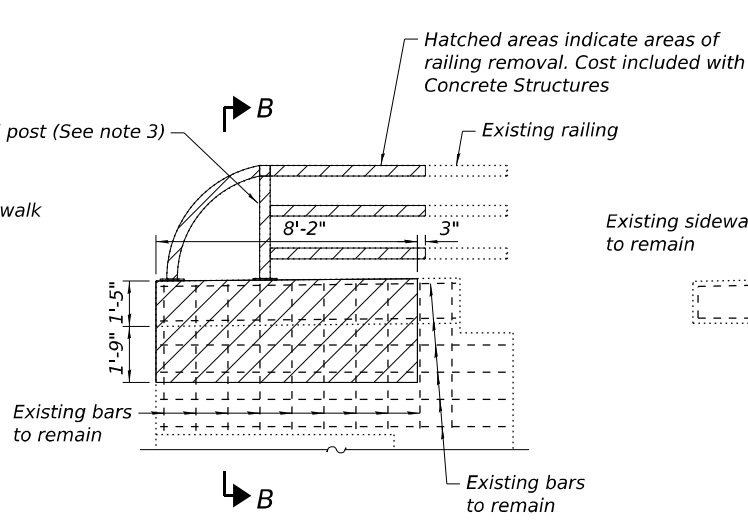
5/6/2025 4:42:56 PM



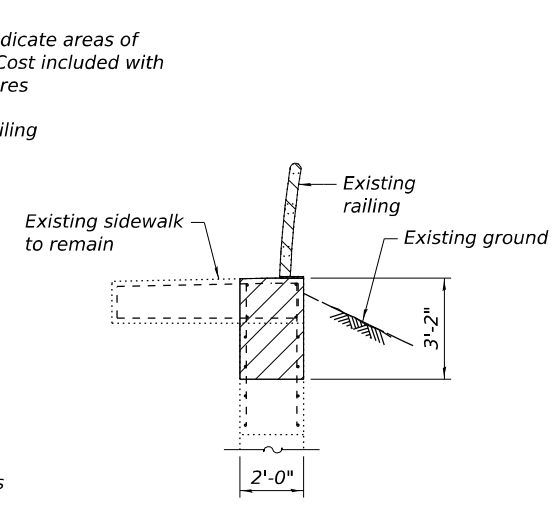
SW WINGWALL REMOVAL ELEVATION



SECTION A-A



NW WINGWALL REMOVAL ELEVATION



SECTION B-B

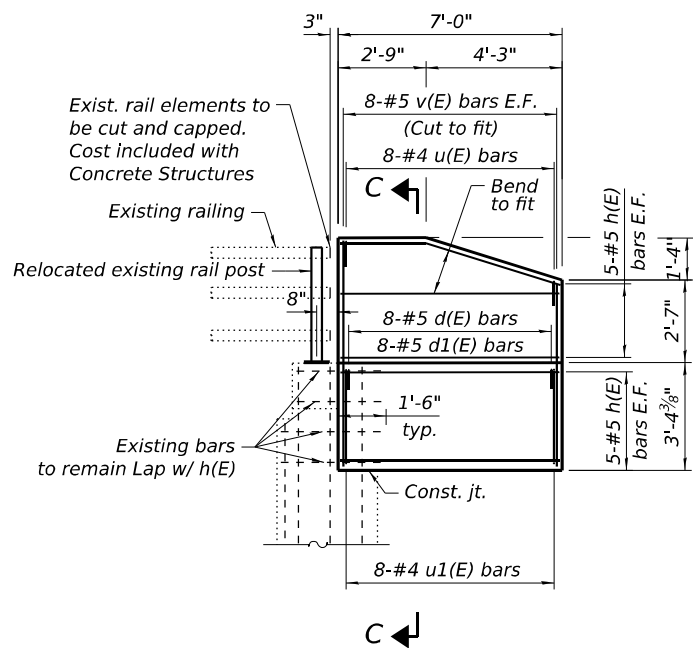
BILL OF MATERIAL
NW & SW WINGWALLS

Bar	No.	Size	Length	Shape
d(E)	35	#5	2'-7"	
d1(E)	17	#5	3'-4"	
h(E)	20	#5	6'-9"	
h1(E)	18	#5	7'-11"	
v(E)	16	#5	7'-0"	
v1(E)	18	#5	6'-10"	
u(E)	17	#4	3'-9"	
u1(E)	8	#4	10'-7"	
u2(E)	9	#4	9'-3"	
Concrete Removal			Cu Yd	3.7
Concrete structures			Cu Yd	5.6
Protective Coat			Sq Yd	24
Reinforcement Bars, Epoxy Coated			Pound	850
Controlled Low-Strength Material			Cu Yd	2.2

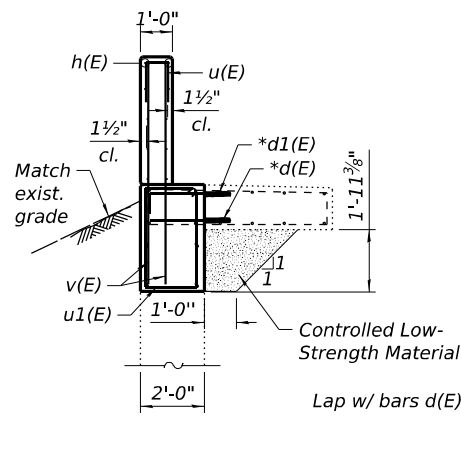
Notes:

- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost shall be included with Concrete Removal.
- Reset last vertical post as shown with similar anchor bolt size, location and clear distance. Work to be incidental to concrete structures.
- Existing rail posts to be removed as part of partial railing removal shall be carefully extracted to prevent damage and shall be preserved to be reused at the existing rail end run using the existing rail post base plate. Contractor shall provide new anchor bolts to secure the reused post and base plate to the existing structure at the locations shown. Any damage to the existing rail post or base plate during removal shall be the contractor's responsibility, and replacement with a new post and/or base plate shall be at no additional cost to the Department.

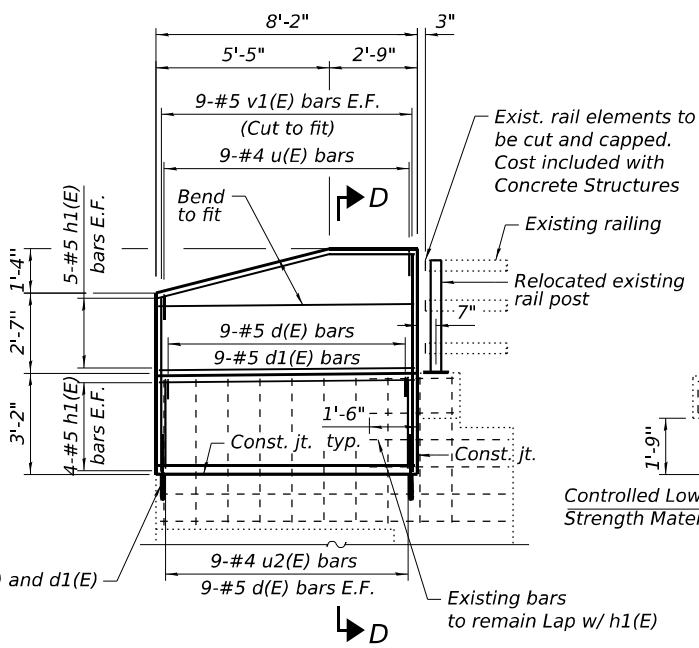
All costs for removal, salvage, and reinstallation of posts, including furnishing and installing new anchor bolts, shall be included in the cost of Concrete Structures.



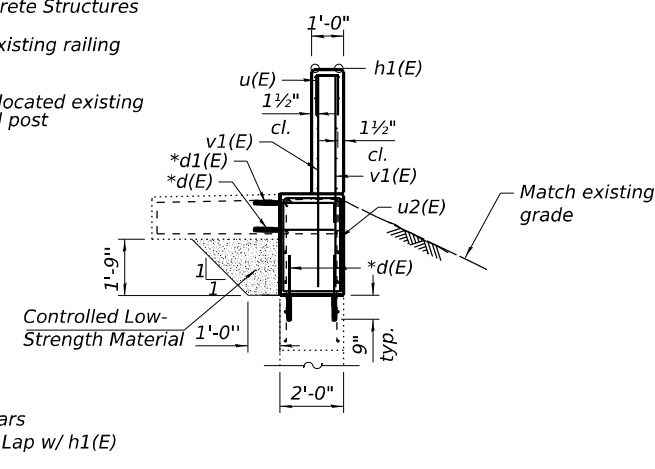
SW WINGWALL RECONSTRUCTION ELEVATION



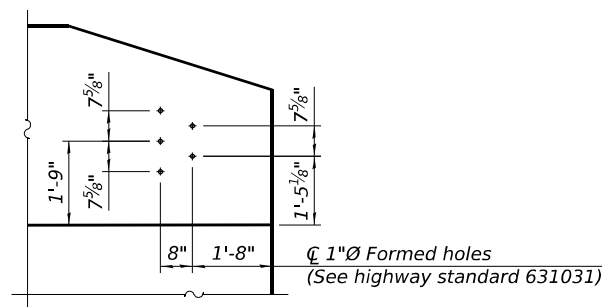
SECTION C-C



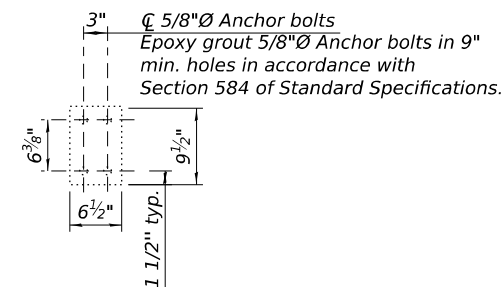
NW WINGWALL RECONSTRUCTION ELEVATION



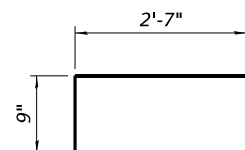
SECTION D-D



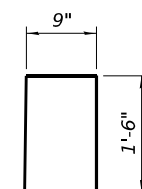
ANCHOR BOLT SPACING TYPICAL
(For Terminal, Type 6)



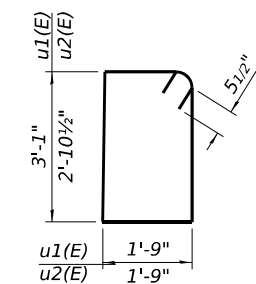
RELOCATED EXISTING RAIL POST BASE TYPICAL
(All existing rail post base dimensions shall be verified in the field)



BAR d1(E)



BAR u(E)



BARS u1(E) & u2(E)

LEGEND

- Concrete and Railing Removal
- E.E. Each End
- E.F. Each Face

* Epoxy grout #5 d(E) or d1(E) bars in 9" min. holes in accordance with Section 584 of Standard Specifications.

USER NAME =	DESIGNED - KM	REVISED -
PLOT SCALE =	CHECKED - CG	REVISED -
PLOT DATE =	DRAWN - DCP	REVISED -
	CHECKED - VG	REVISED -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2908, 2921	(T1-4)BR 24	COOK	34	23
CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				

Existing Description: SN 016-0801 was originally built in 1954. The bridge was redecked and the east abutment and wingwalls were modified in 1998. The structure has a C brg. to C brg. length of 215'-0" and an out-to-out width of 70'-0". The superstructure consists of a 7½" thick reinforced concrete slab supported on four-span continuous concrete beams of span lengths 42'-0", 65'-6", 65'-6" and 42'-0". The substructure consists of reinforced concrete abutments and solid wall piers supported on precast concrete piles.

Traffic will be maintained utilizing stage construction, see Roadway plans.

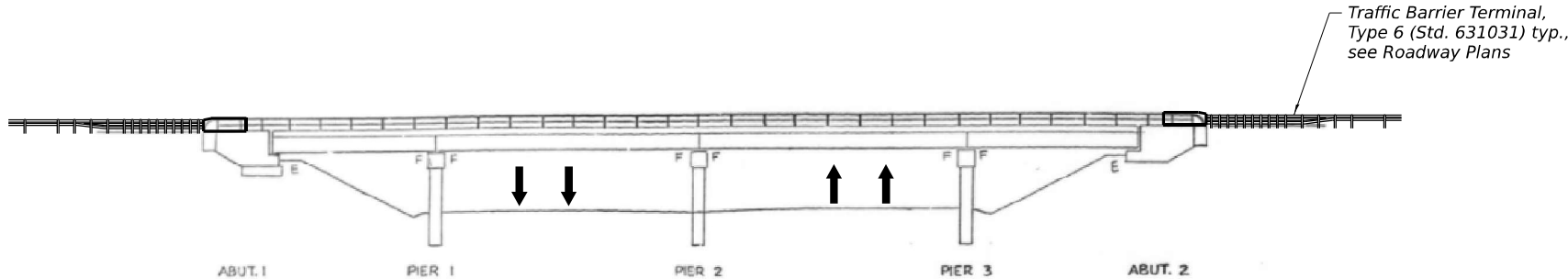
No Salvage:

LOADING HS20-44
Allow 25#/sq. ft for future wearing surface

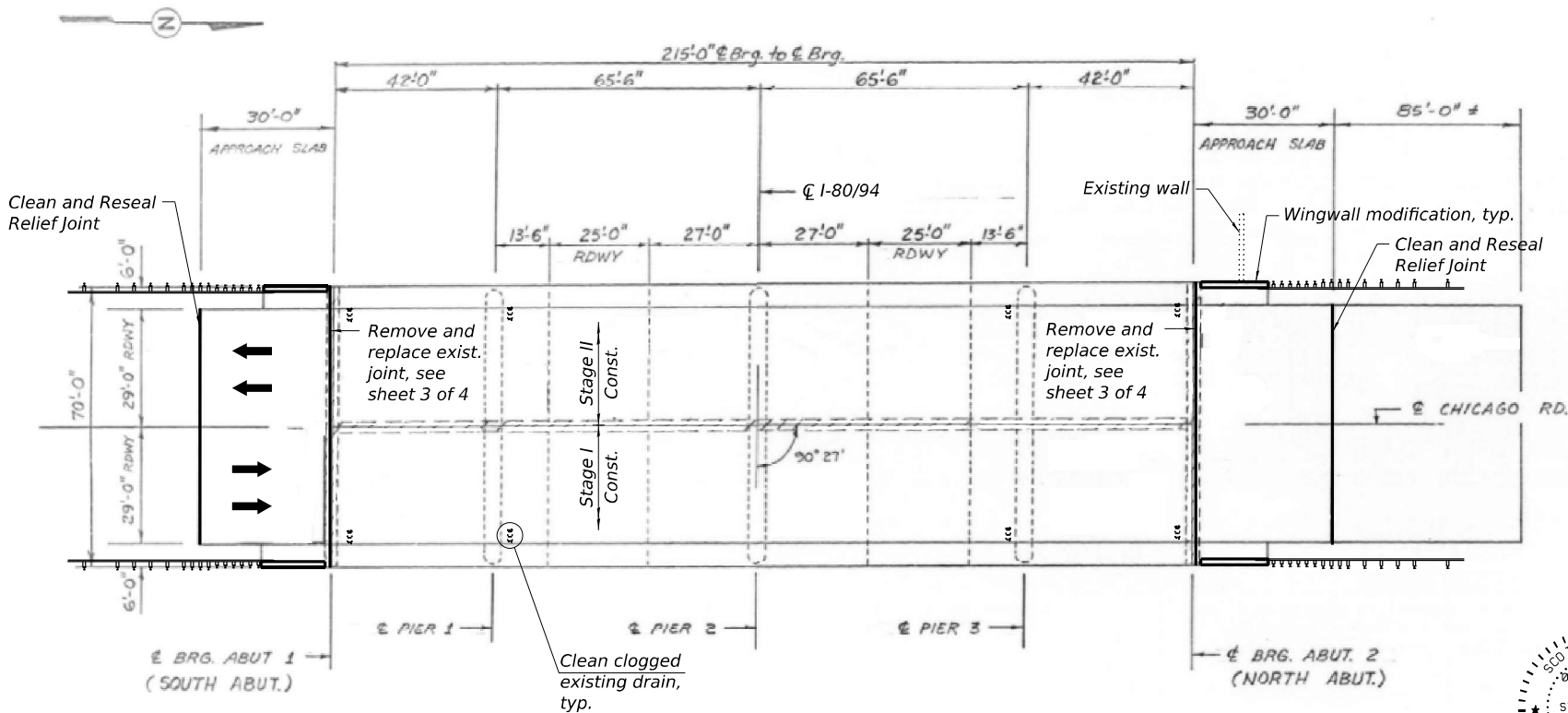
DESIGN SPECIFICATIONS
1992 AASHTO Standard Specifications
with 1993 and 1994 Interims

DESIGN STRESSES
FIELD UNITS (EXIST. CONST.)
 $f_c = 3,500$ psi (Superstructure)
 $f_c = 800$ psi (Substructure)
 $f_y = 60,000$ psi (Superstructure Reinforcement)
 $f_y = 20,000$ psi (Substructure Reinforcement)
 $f_y = 18,000$ psi (1954 Struct. Steel)
 $f_y = 20,000$ psi (2001 Struct. Steel)

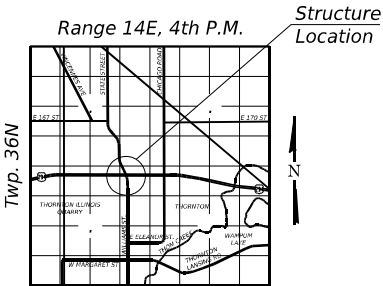
FIELD UNITS (NEW CONST.)
 $f_c = 4,000$ psi
 $f_y = 60,000$ psi (Reinforcement)



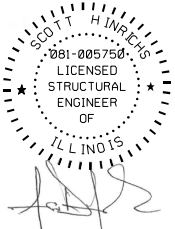
ELEVATION



PLAN



LOCATION SKETCH



ENGINEER FULL NAME: SCOTT HINRICHS DATE: 04-11-2025
ILLINOIS REGISTERED ENGINEER NO. 081-005750
REGISTRATION EXPIRES 11. 30, 2026

GENERAL PLAN & ELEVATION
CHICAGO RD./SOUTH PARK AVE.
OVER I-80/94
F.A.U. RTE. 2921
COOK COUNTY
STRUCTURE NO. 016-0801

MODEL: Default
FILE NAME: X:\OH\2025\20250016\Design\CADD\Structures\Sheets\Plan\SN-016-0801-62X29-01-GPE.dgn

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PLOT DATE =	CHECKED - VG	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET 1 OF 4 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2908, 2921	(T1-4)BR 24	COOK	34	24
CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES:

1.

Reinforcement bars designated (E) shall be epoxy coated.
2.

Plan dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
3.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened, and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system at the contractor's expense.
4.

The Contractor shall use extreme care during concrete removal so as not to damage the PPC I-Beam.
5.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.
6.

Epoxy grout v(E), v1(E) and h1(E) bars in 9" minimum holes according to Article 584 of Standard Specifications.

INDEX OF SHEETS

1.

General Plan and Elevation
2.

General Data
3.

Top of Deck Repairs
4.

Wingwall Modification Details

SCOPE OF WORK

1.

Replace abutments Joints with preformed joint seal and header repair with concrete polymer.
2.

Clean and Reseal Relief Joint.
3.

Resurface HMA on Bridge Approaches and Pavement near bridge. Refer to roadway plans for quantities.
4.

Modify the existing wingwall at all approach corners to accommodate the upgraded Type 6 guardrail terminal connection.
5.

Clean Bridge Scuppers and Downspouts.
6.

Apply protective coating to the front face and top surfaces of new concrete parapets, and location of Deck Slab Repair & Approach Slab Repair.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd	6.6	-	6.6
Concrete Structure	Cu Yd	10.3	-	10.3
Protective Coat	Sq Yd	57	-	57
Reinforcement Bars, Epoxy Coated	Pound	1480	-	1480
Preformed Joint Seal 2"	Foot	142	-	142
Controlled Low-Strength Material	Cu Yd	-	4.8	4.8
Cleaning Bridge Scuppers and Downspouts	Each	8	-	8
Clean & Reseal Relief Joints	Cu Yd	118	-	118
Approach Slab Repair (Partial Depth)	Sq Yd	14.7	-	14.7
Polymer Concrete	Cu Ft	17.5	-	17.5

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

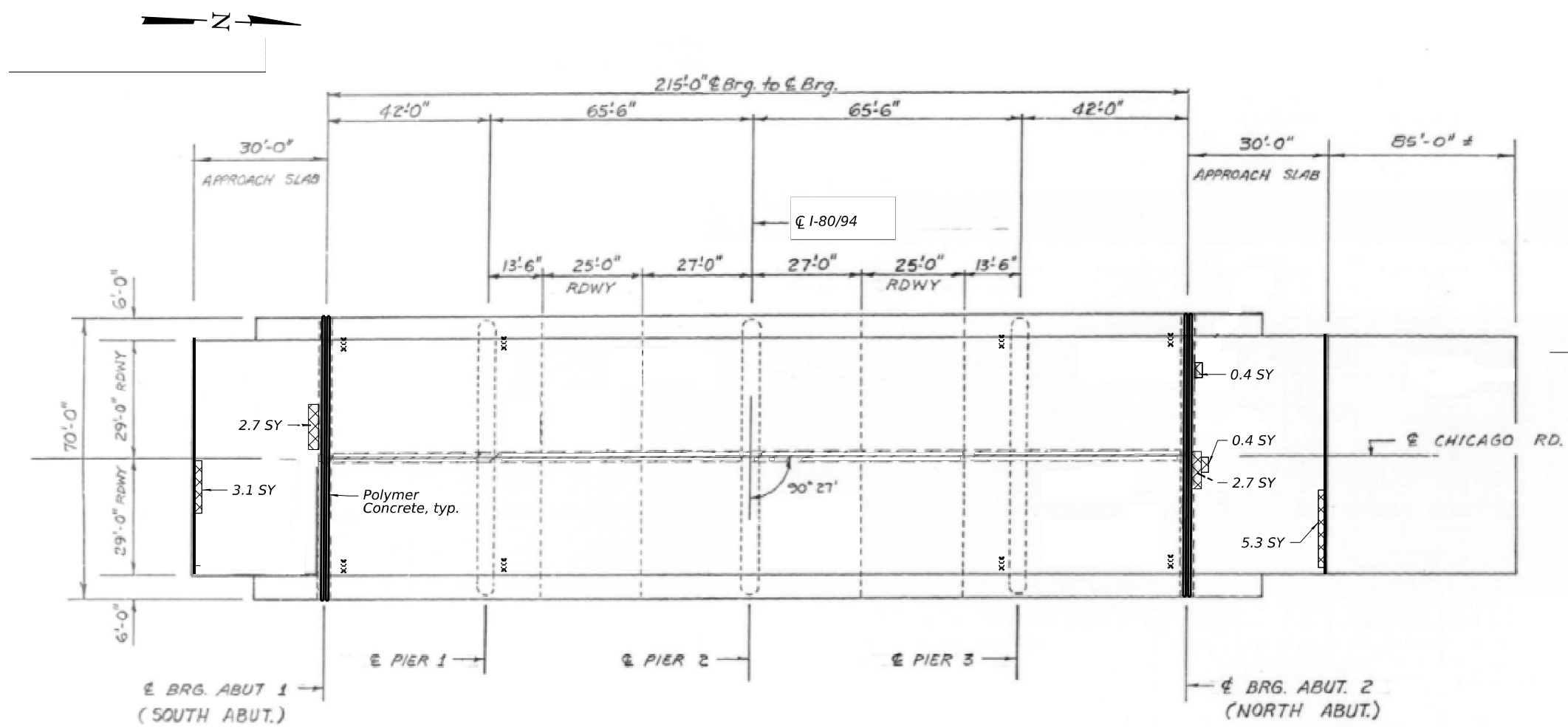
GENERAL DATA
STRUCTURE NO. 016-0801

SHEET 2 OF 4 SHEETS

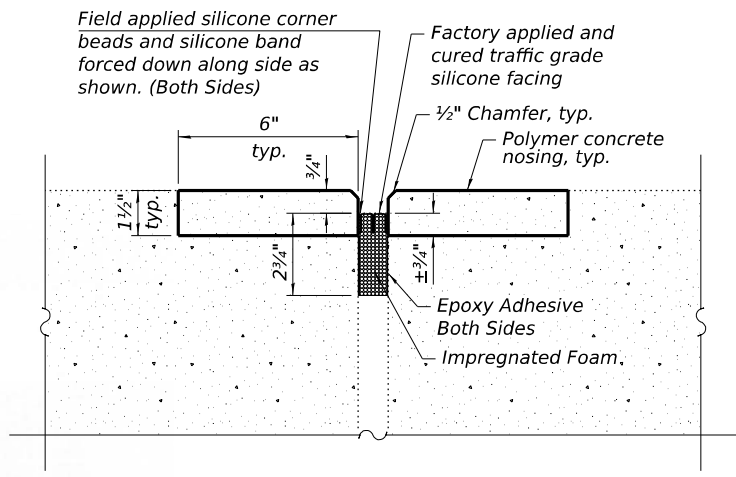
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2908, 2921	(T1-4)BR 24	COOK	34	25
CONTRACT NO. 62X29				
		ILLINOIS	FED. AID PROJECT	

GRAEF

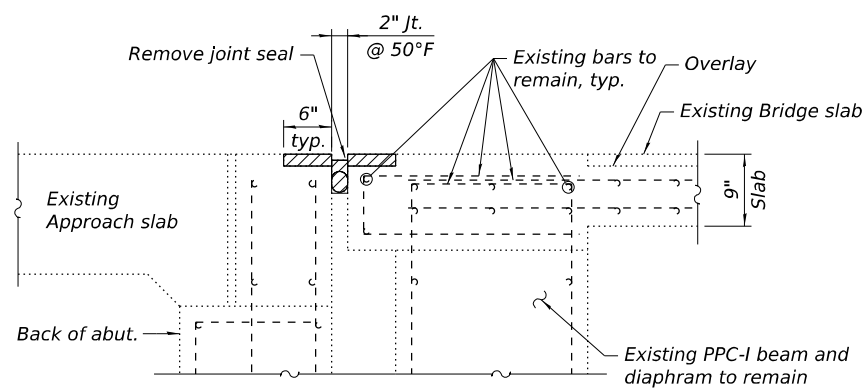
8501 W. Higgins Road, Suite 280
Chicago, Illinois 60634 (773) 399-012



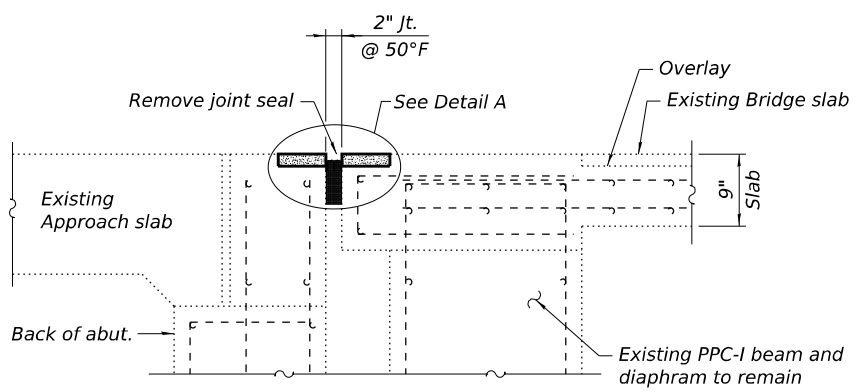
PLAN



DETAIL A



EXISTING FIXED JOINT REMOVAL
(South abut. shown North abut. similar)



PROPOSED FIXED JOINT
(South abut. shown North abut. similar)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu Yd	0.6
Protective Coat	Sq Yd	15
Preformed Joint Seal 2"	Foot	142
Cleaning Bridge Scuppers and Downspouts	Each	8
Clean & Reseal Relief Joint	Foot	118
Approach Slab Repair (Partial Depth)	Sq Yd	14.7
Polymer Concrete	Cu Ft	17.5

LEGEND

- Approach Slab Repair (Partial Depth)
- Existing Concrete/ Joint Removal

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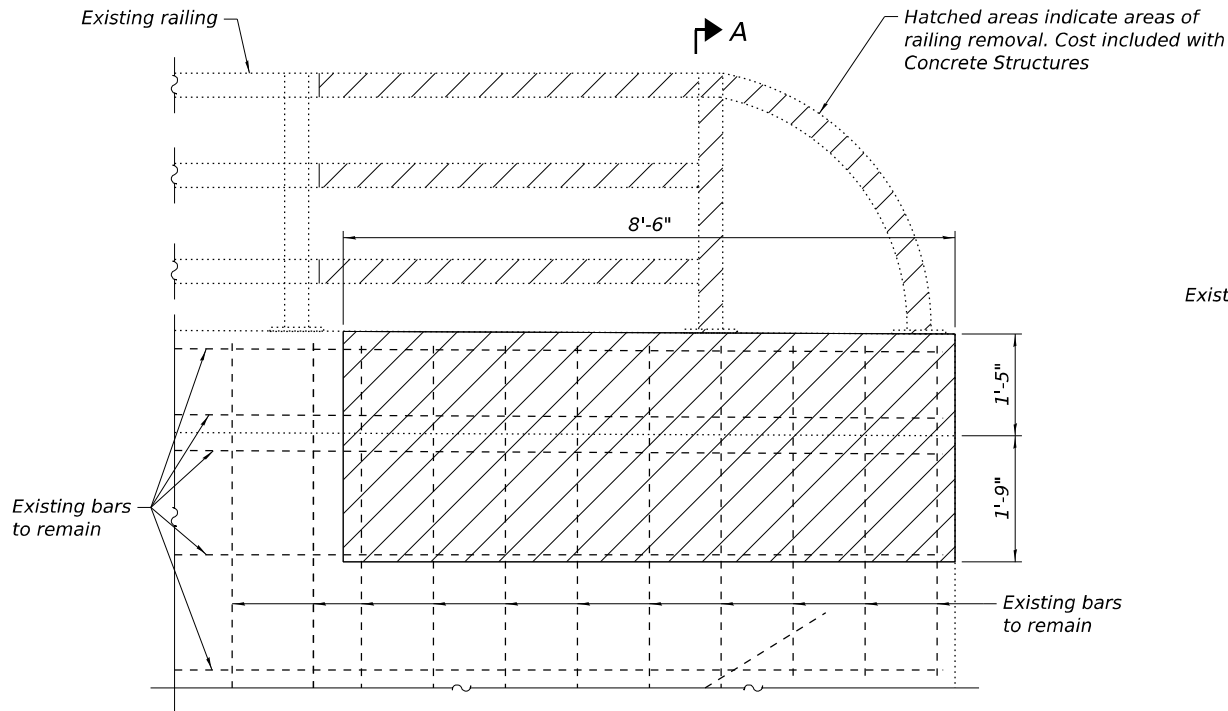
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	CHECKED - CG	REVISED -
PLOT SCALE =	DRAWN - DCP	REVISED -
PLOT DATE =	CHECKED - VG	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

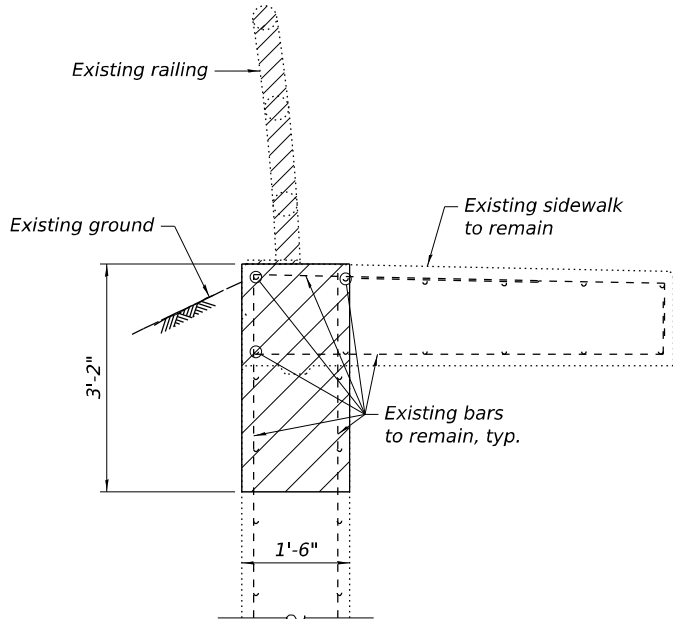
TOP OF DECK REPAIRS
STRUCTURE NO. 016-0801

SHEET 3 OF 4 SHEETS

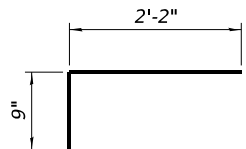
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2908, 2921	(T1-4)BR 24	COOK	34	26
CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				



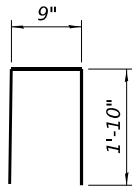
NW WINGWALL REMOVAL ELEVATION
(Northwest wingwall shown NE, SW and SE similar)



SECTION A-A



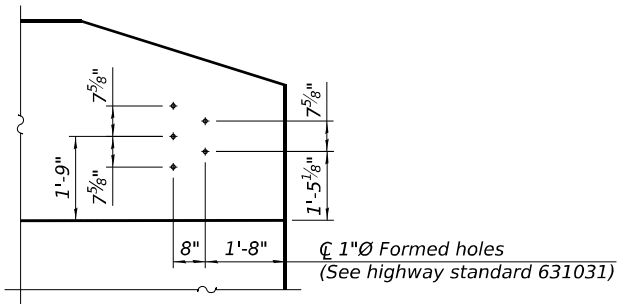
BAR d1(E)



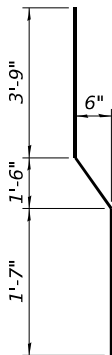
BAR u(E)

BILL OF MATERIAL
NE, NW, SE & SW WINGWALLS

Bar	No.	Size	Length	Shape
d(E)	108	#5	2'-3"	
d1(E)	36	#5	2'-11"	
h(E)	72	#5	8'-3"	
v(E)	36	#5	6'-10"	
v1(E)	36	#5	7'-0"	
u(E)	36	#4	4'-5"	
Concrete Removal			Cu Yd	6.0
Concrete structures			Cu Yd	10.3
Protective Coat			Cu Yd	42
Reinforcement Bars, Epoxy Coated			Pound	1480
Controlled Low-Strength Material			Cu Yd	4.8



ANCHOR BOLT SPACING TYPICAL
(For Terminal, Type 6)



BAR v1(E)

Notes:

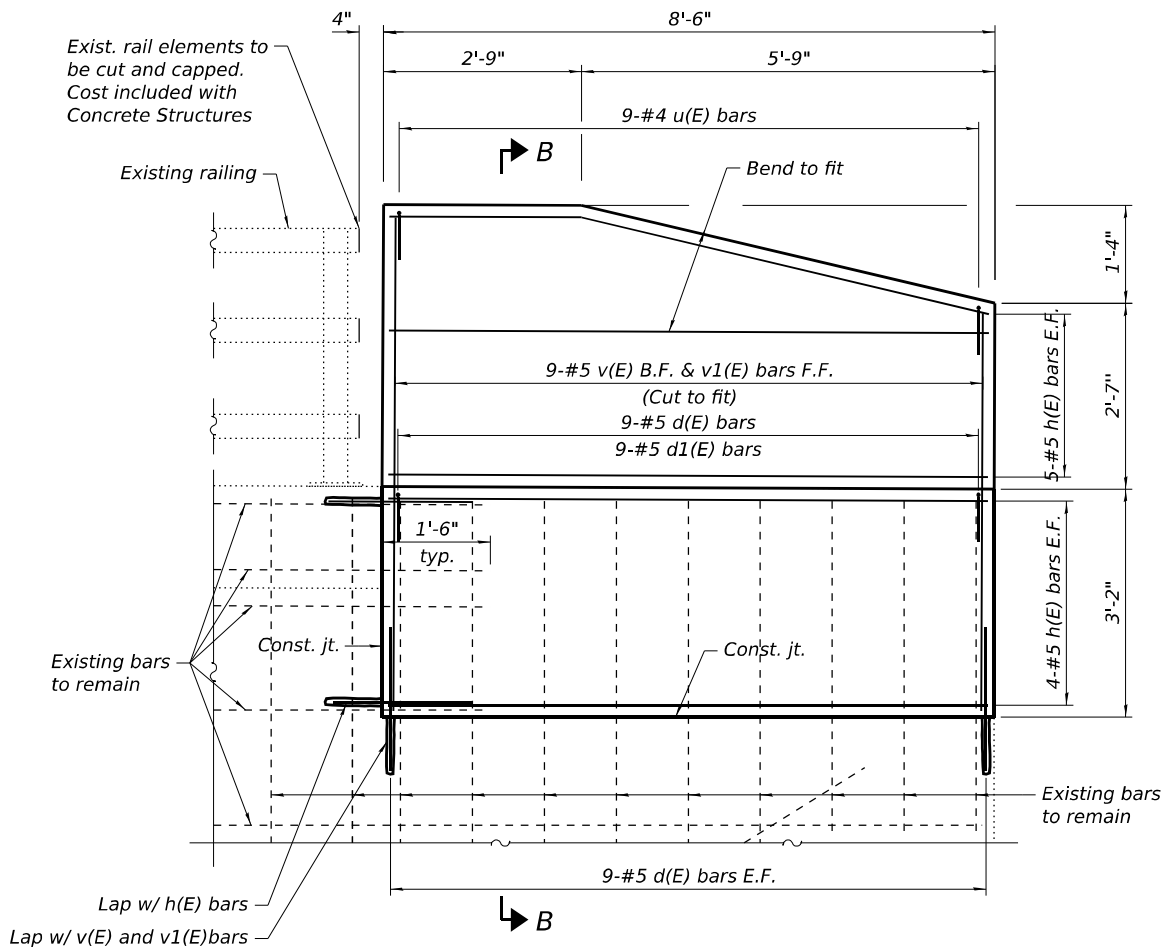
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost shall be included with Concrete Removal.

LEGEND

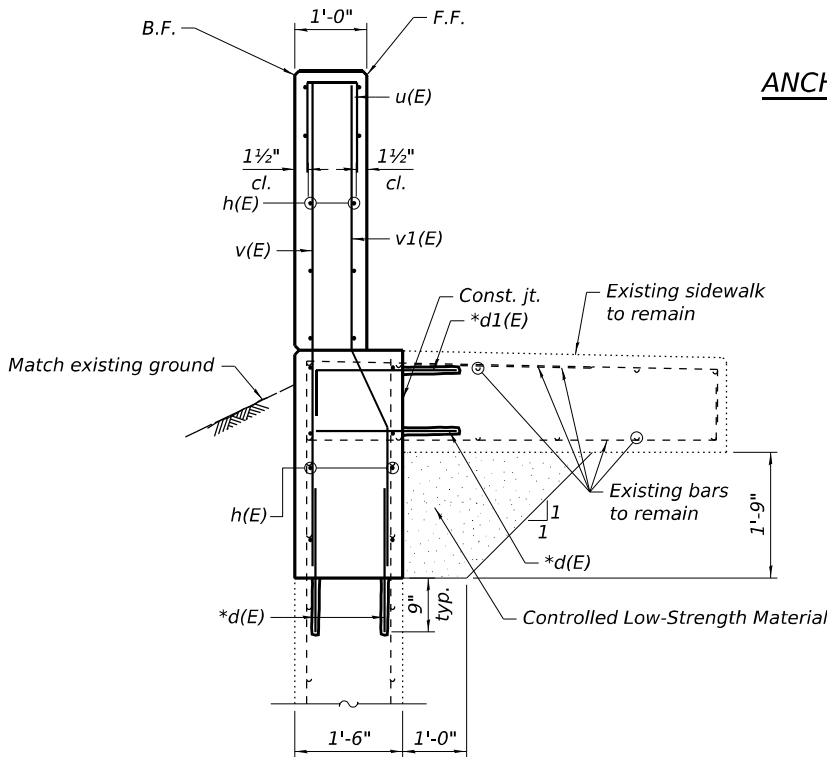
Concrete and Railing Removal

B.F. Back Face
E.F. Each Face

* Epoxy grout #5 d(E) or d1(E) bars in 9" min. holes in accordance with Section 584 of Standard Specifications.



NW WINGWALL RECONSTRUCTION ELEVATION
(Northwest wingwall shown NE, SW and SE similar)



SECTION B-B

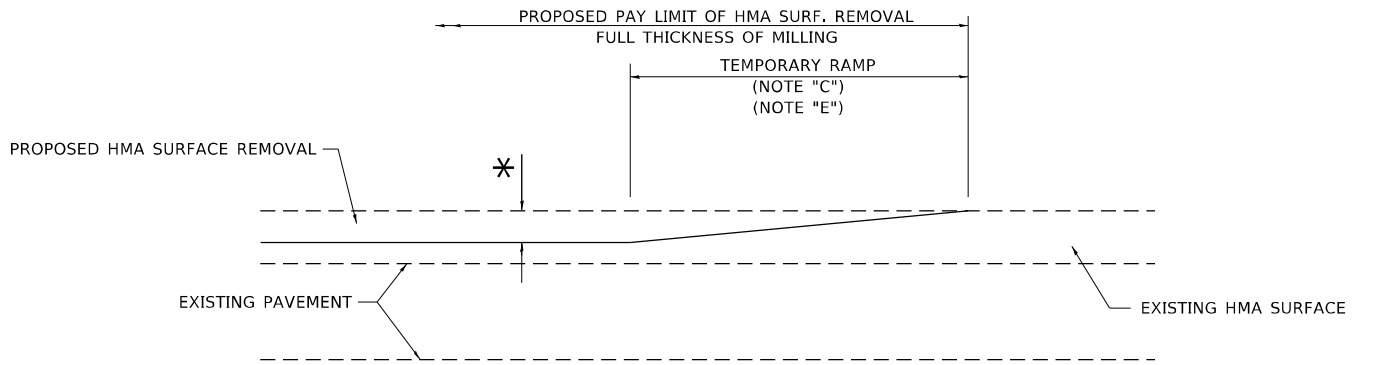
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WINGWALL MODIFICATION DETAILS
STRUCTURE NO. 016-0801

SHEET 4 OF 4 SHEETS

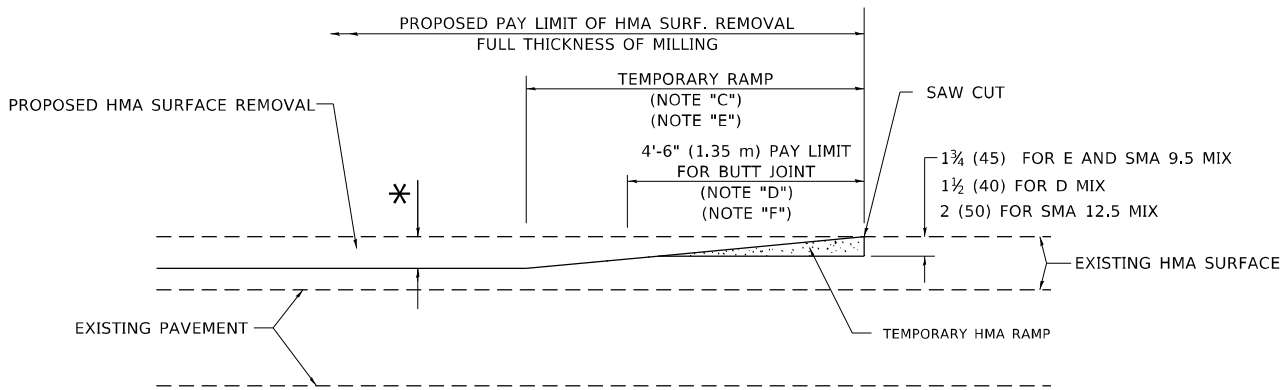
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2908, 2921	(T1-4)BR 24	COOK	34	27
CONTRACT NO. 62X29				
ILLINOIS FED. AID PROJECT				

USER NAME =	DESIGNED - KM	REVISED -
PLOT SCALE =	CHECKED - CG	REVISED -
PLOT DATE =	DRAWN - DCP	REVISED -
	CHECKED - VG	REVISED -



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

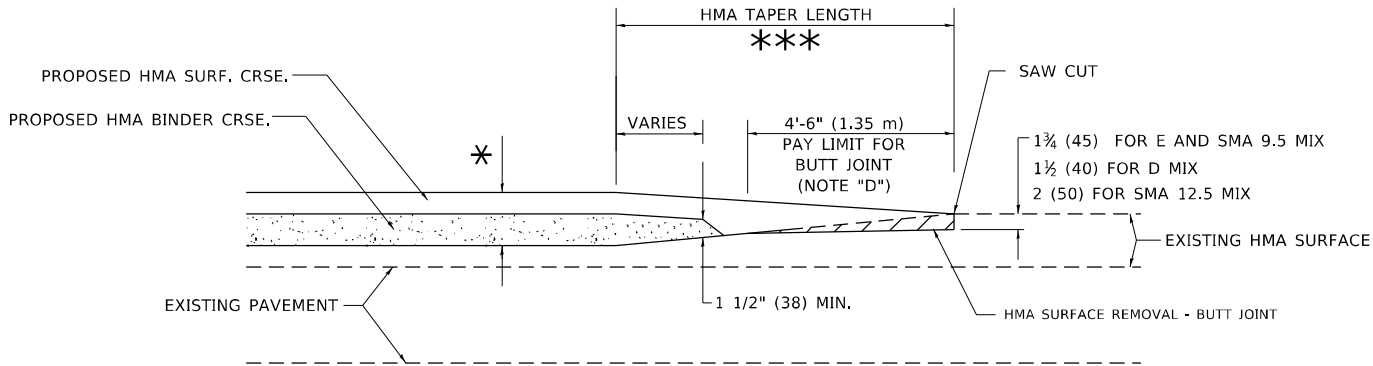
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

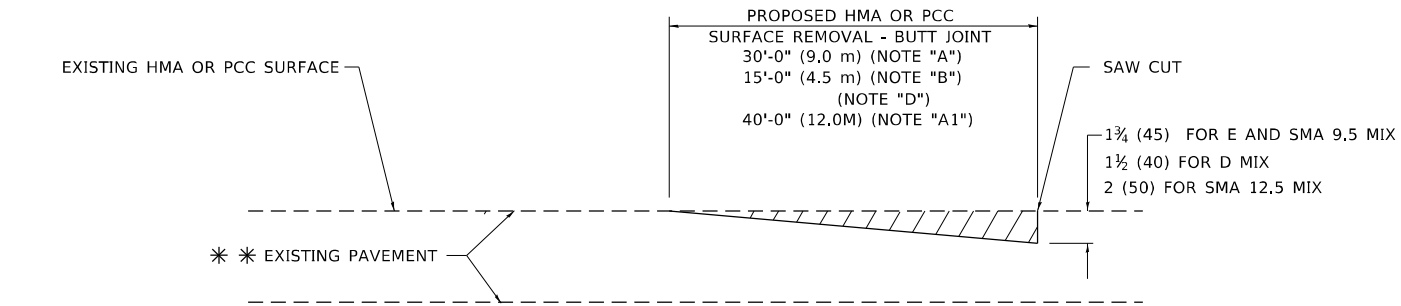
OPTION 2

TYPICAL TEMPORARY RAMP

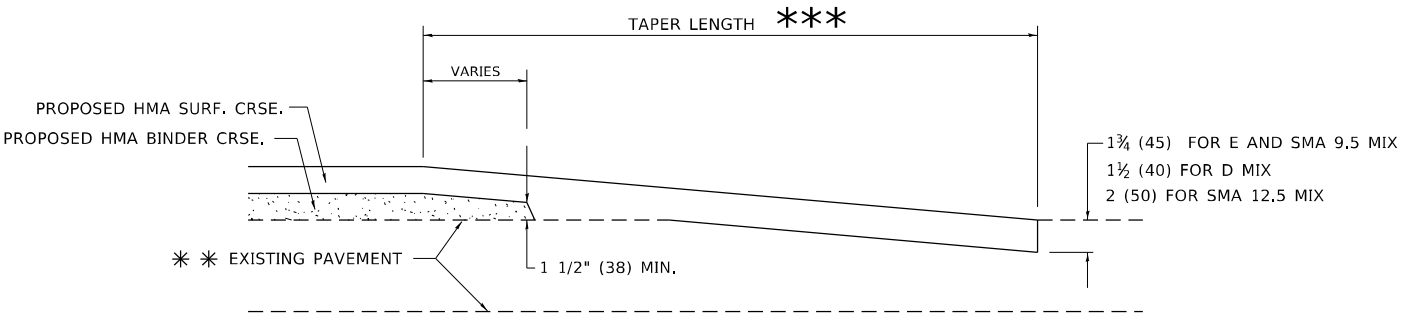


**BUTT JOINT AND
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

****** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
***** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
******* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
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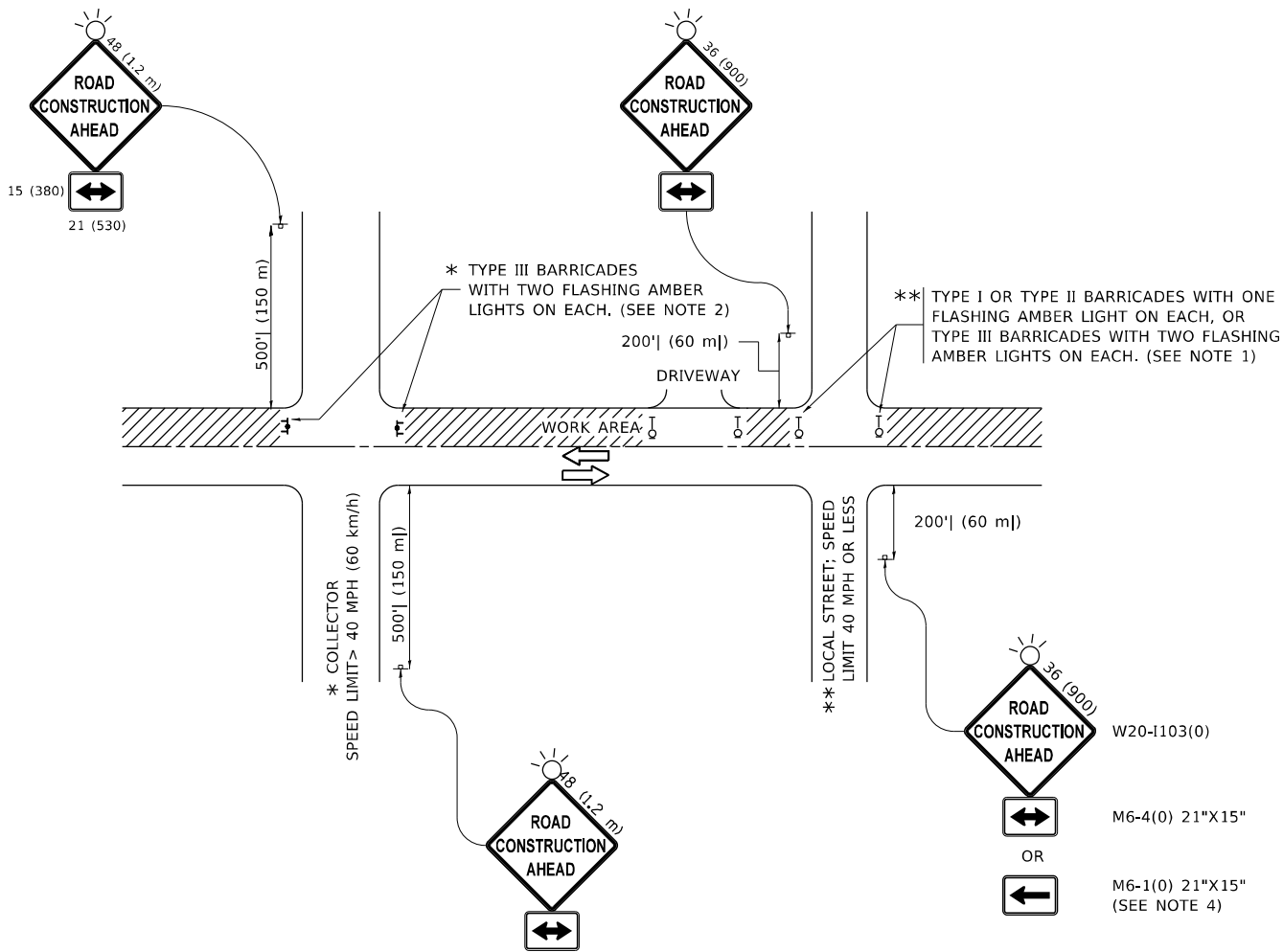
USER NAME = Lawrence,DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			34	29
BD400-05 BD-32		CONTRACT NO.		
		ILLINOIS	FED. AID PROJECT	

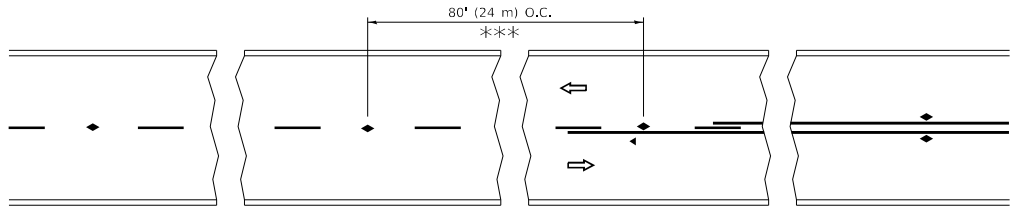


NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE,
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE,
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

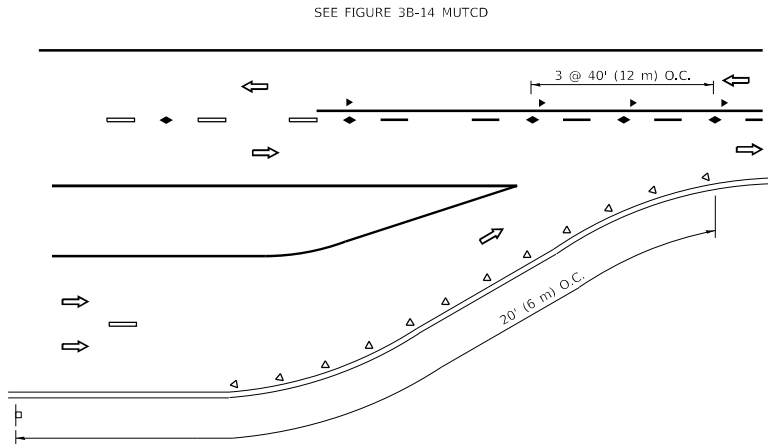
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME: D:\PROJECTS\2024\11-06-00\11-06-00.DWG	USER NAME = Lawrence,DeManche	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-00	<div>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</div>	<div>TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS</div>				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. SCHUETZE 07-01-13									34	30
	PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 09-15-16		<div>TC-10</div>								
	PLOT DATE = 5/3/2024	DATE = 06-89	REVISED - D. SENDERAK 05-03-24										
						SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	<div>CONTRACT NO.</div>	

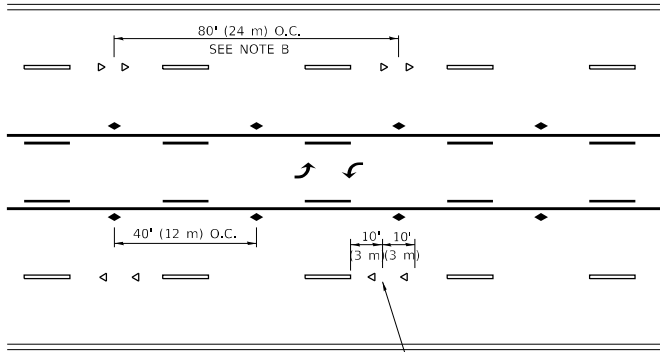


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

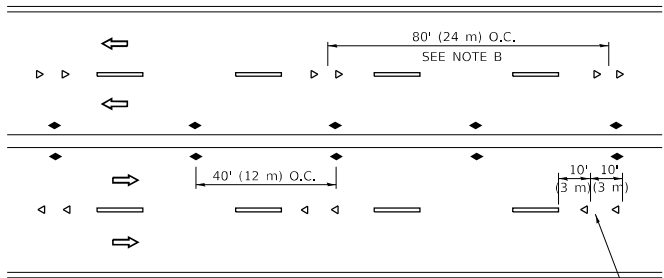


LANE REDUCTION TRANSITION



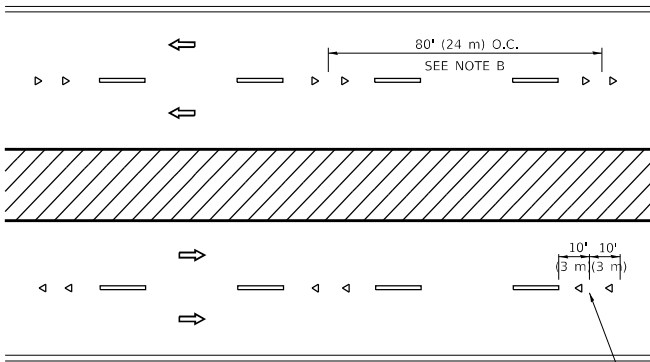
SEE NOTE A

TWO-WAY LEFT TURN



SEE NOTE A

MULTI-LANE/UNDIVIDED



SEE NOTE A

MULTI-LANE/DIVIDED

GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

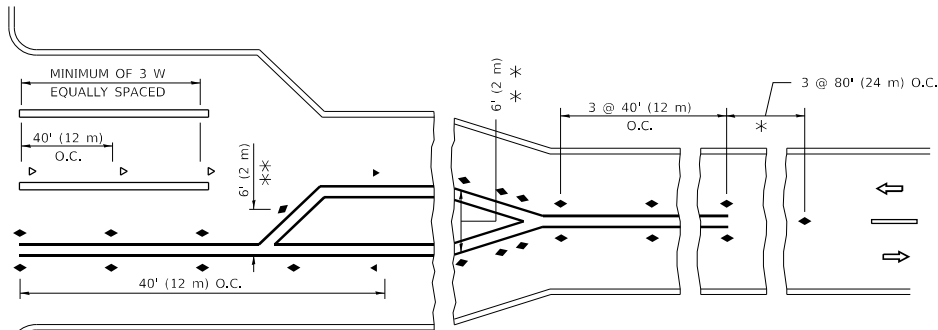
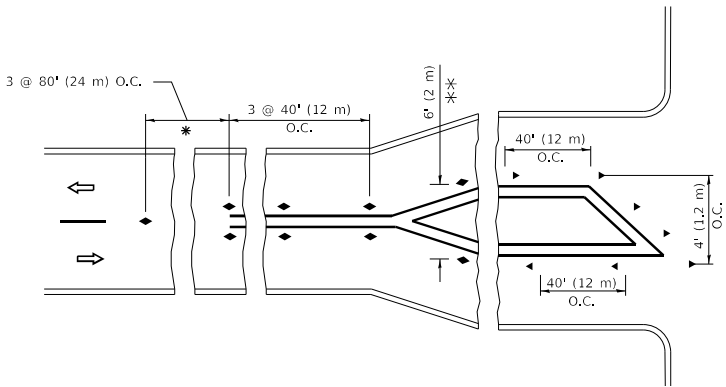
- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

DESIGN NOTES

- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

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* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

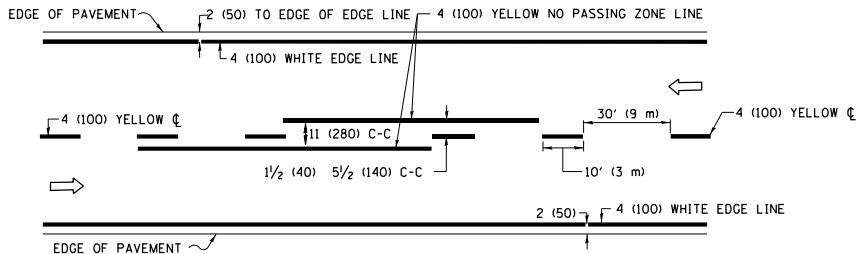
TURN LANES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

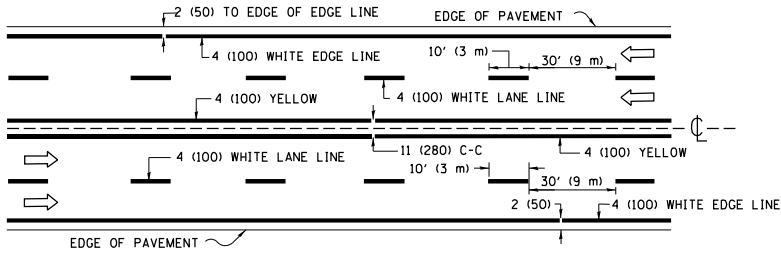
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

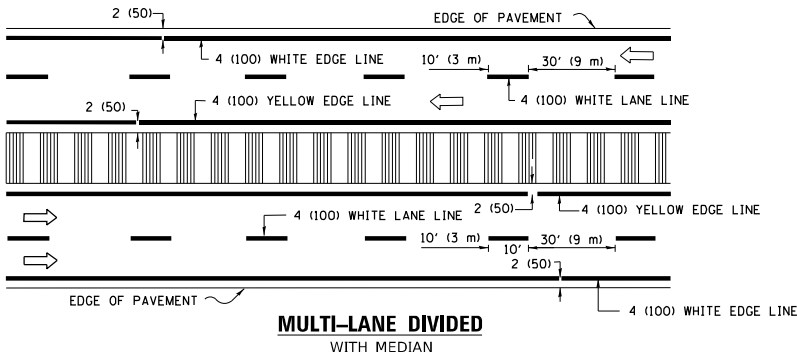
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			34	31
TC-11		CONTRACT NO.		
		ILLINOIS FED. AID PROJECT		



2-LANE ROADWAY

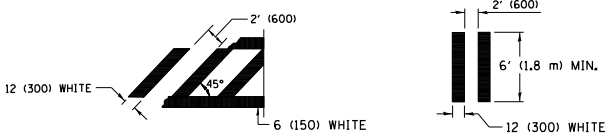
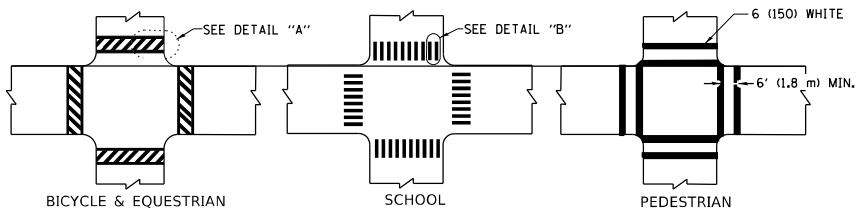


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

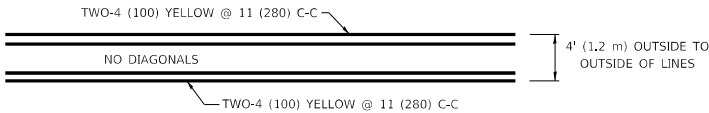


DETAIL "A"

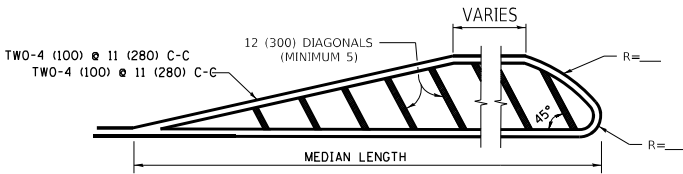
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

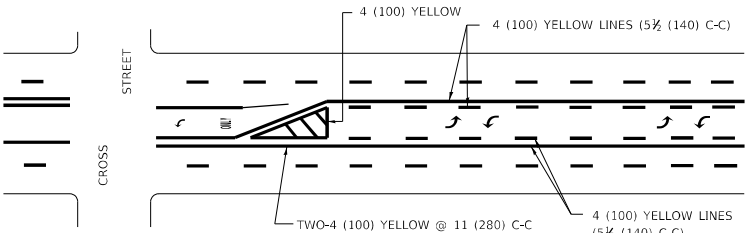


4' (1.2 m) WIDE MEDIANS ONLY

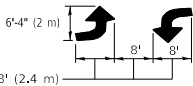


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

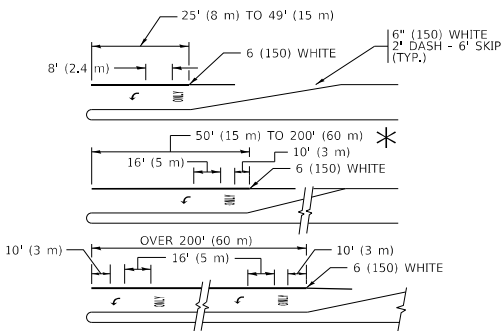


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



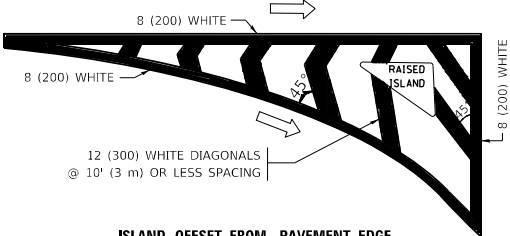
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.

AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

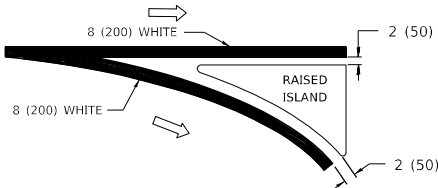
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

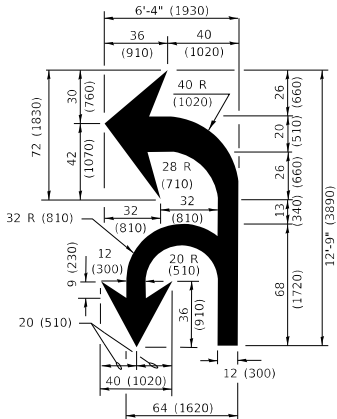


ISLAND OFFSET FROM PAVEMENT EDGE

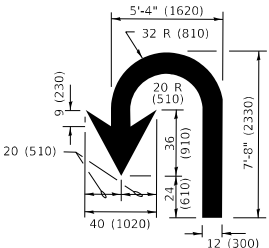


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGTUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ²) EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footeam]	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
	DRAWN -	REVISED - C. JUCIUS 07-01-13
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

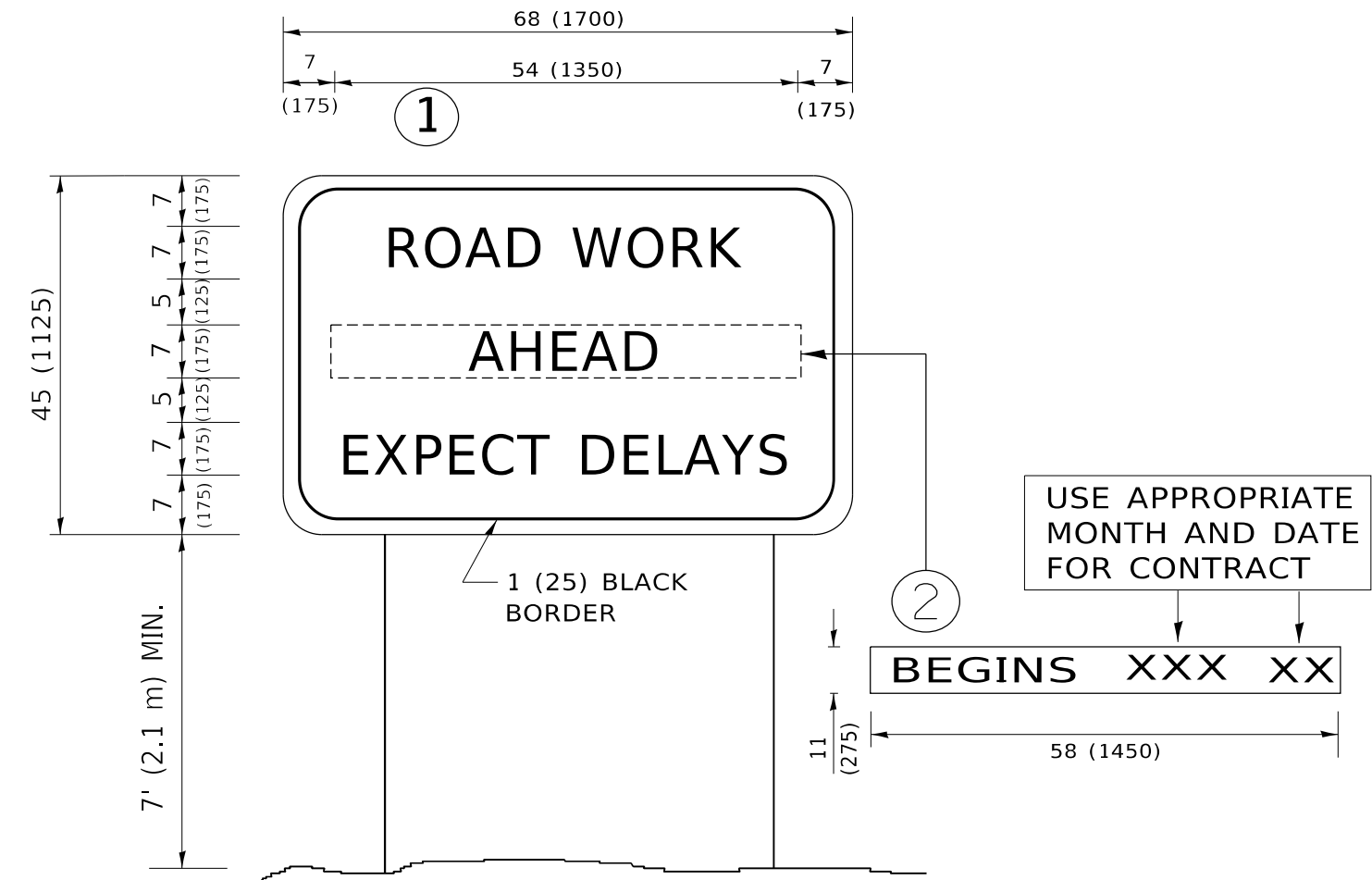
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			34	32
TC-13		CONTRACT NO.		
		ILLINOIS	FED. AID PROJECT	

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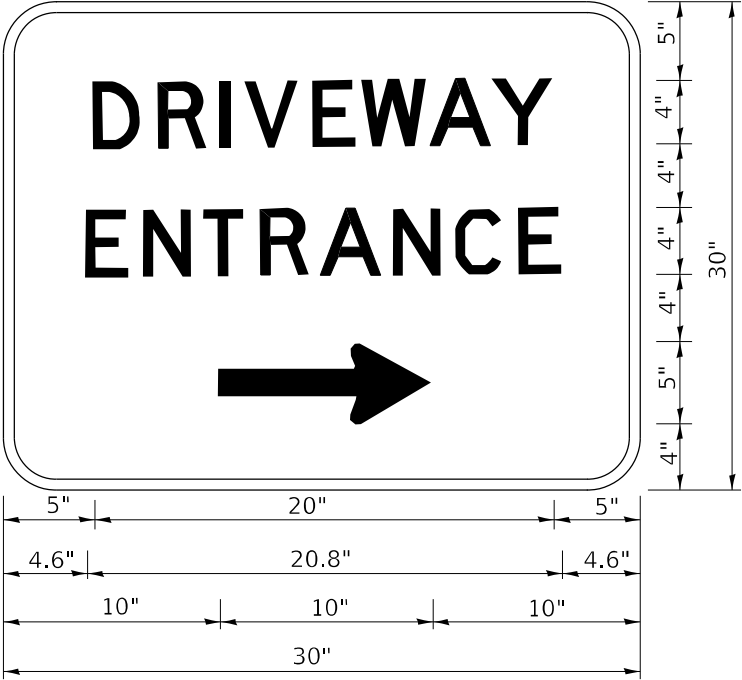


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN①WITH INSTALLED PANEL②ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL②SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

	USER NAME = footemj	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. MIRS 12-11-97								34	33
	PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99					TC-22		CONTRACT NO.		
	PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07					ILLINOIS FED. AID PROJECT				
					SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.			



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

- NOTES:
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
 3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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	USER NAME = Ieysa	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING					F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 8/6/2021	DATE -	REVISED -		SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.									
										ILLINOIS FED. AID PROJECT				