

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB) BDR	WINNEBAGO	38	1
		ILLINOIS	CONTRACT NO. 64S25	

FOR INDEX OF SHEETS, SEE SHEET NO. 2
TOWNSHIP: ROCKFORD
SECTION: 29
CLASSIFICATION: MINOR ARTERIAL
ADT (2022): 3550

POSTED SPEED: 55 MPH

APPLY TO SHEETS:
1-14, 30-38



Patrick W. Keefe

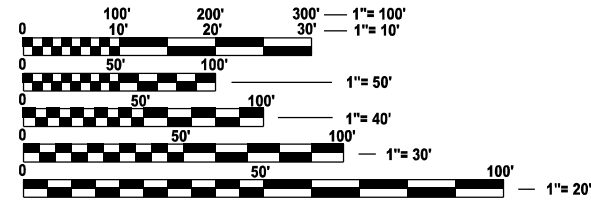
PATRICK W. KEEFE
NO.: 062-055351
DATE SIGNED: 4/29/2025
EXP. DATE: 11/30/2025

APPLY TO SHEETS:
16-29



Ralph J. Otrembiak

RALPH J. OTREMBIAK
NO.: 081-004812
DATE SIGNED: 4/29/2025
EXP. DATE: 11/30/2026



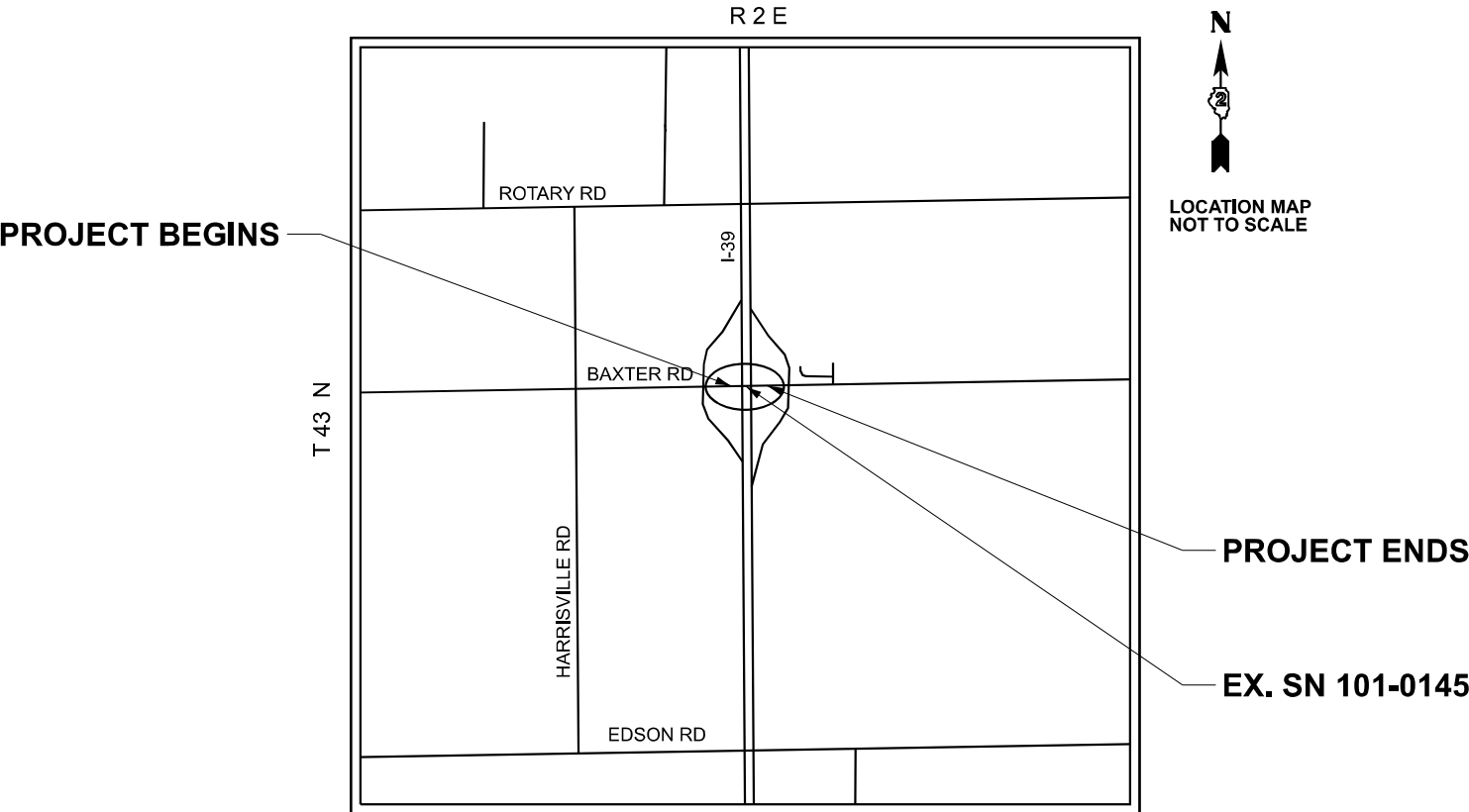
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DERRICK LOPEZ
PROJECT MANAGER

CONTRACT NO. 64S25

PROPOSED
HIGHWAY PLANS
BAXTER ROAD OVER FAI ROUTE 39 (I-39)
SECTION (201-1HB) BDR
PROJECT 1Y5B(220)
BRIDGE OVERLAY AND REHABILITATION
WINNEBAGO COUNTY
C-92-058-23



GROSS LENGTH = 262.5 FT. = 0.049 MILE
NET LENGTH = 262.5 FT. = 0.049 MILE



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED May 15 20 25

Julia Thompson
REGIONAL ENGINEER

Scott A. Etkin
ENGINEER OF DESIGN AND ENVIRONMENT

John P. [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

GENERAL NOTES

1. THE CONTRACTOR SHALL NOTIFY TRAFFIC OPERATIONS A MINIMUM OF 5 WORKING DAYS PRIOR TO PLACING PERMANENT PAVEMENT MARKING OR SIGNING.
2. QUALITY MANAGEMENT PROGRAM TO BE USED ROW WILL HAVE QC/QA (QUALITY CONTROL/QUALITY ASSURANCE), QCP (HMA QUALITY CONTROL FOR PERFORMANCE), OR PFP (HMA PAY FOR PERFORMANCE USING PERCENT WITHIN LIMITS JOBSITE SAMPLING), WHICH ONE TO USE WILL BE DETERMINED BY MATERIALS. THEY WILL NEED QUANTITIES FOR EACH TYPE OF MIX USED (I.E. SURFACE, BINDER, ETC.). SEE BDE MANUAL CH. 53-4.07 OR CONTACT THE DISTRICT MIXTURES CONTROL ENGINEER FOR MORE INFORMATION.
3. PAVEMENT MARKING SHALL BE DONE ACCORDING TO STANDARD 780001, EXCEPT AS FOLLOWS:
 1. ALL WORDS, SUCH AS ONLY, SHALL BE 8 FEET HIGH.
 2. ALL NON-FREEWAY ARROWS SHALL BE THE LARGE SIZE.
 3. THE DISTANCE BETWEEN YELLOW NO-PASSING LINES SHALL BE 8 INCHES, NOT 7 INCHES, AS SHOWN IN THE DETAIL OF TYPICAL LANE AND EDGE LINES.
 4. CENTERLINE SKIP DASH PAVEMENT MARKING ON MULTI-LANE DIVIDED, MULTI-LANE UNDIVIDED, AND ONE-WAY ROADWAY SHALL BE ACCORDING TO DISTRICT STANDARD 41.1.
4. IDOT IS NOT A MEMBER OF JULIE. IF YOU ARE NEAR ANY OVERHEAD LIGHTING, INTERSECTION LIGHTING OR TRAFFIC SIGNALS, CONTACT THE IDOT TRAFFIC OFFICE AT 815/284-5469 AT LEAST 48 HOURS PRIOR TO WORK
5. THE SUPPORTING DATA SHOWN IN THE PLANS WERE DEVELOPED THROUGH AERIAL PHOTOGRAPHY AND IS NOT THE RESULT OF A GROUND SURVEY. THEREFORE, THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS ARE FOR REFERENCE PURPOSES ONLY. THE RELATIVE ACCURACY OF THE INFORMATION IS UNKNOWN AND CANNOT BE GUARANTEED. THE CONTRACTOR MAY BE REQUIRED TO ADJUST LAYOUT TO MATCH ACTUAL FIELD CONDITIONS AND THE INTENT OF THE PLANS.

INDEX OF SHEETS

1	COVER SHEET
2	GENERAL NOTES, INDEX OF SHEETS AND STANDARD DRAWINGS
3 - 6	SUMMARY OF QUANTITIES
7	REMOVAL AND PROPOSED PLAN
8-9	PROPOSED PAVEMENT MARKINGS AND SIGNING
10	MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
11-14	MAINTENANCE OF TRAFFIC PLANS
15	TEMPORARY SIGNAL PLANS
16 - 29	STRUCTURAL PLANS
30 - 38	D2 HIGHWAY STANDARDS

IDOT STANDARD DRAWINGS

<u>STANDARD NUMBERS</u>	<u>DESCRIPTION</u>
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
606301-04	PC CONCRETE ISLANDS AND MEDIANS
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5m) AWAY
701201-05	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS > 45 MPH
701321-19	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER
701400-12	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-13	LANE CLOSURE, FREEWAY/EXPRESSWAY
701901-10	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
729001-01	APPLICATION OF TYPES A & B POSTS (FOR SIGNS & MARKERS)
780001-05	TYPICAL PAVEMENT MARKINGS
701406-13	LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION

DISTRICT 2 STANDARD DRAWINGS

STANDARD NUMBERS	DESCRIPTION
34.1	WORK ZONE SIGN DETAILS
41.1	TYPICAL PAVEMENT MARKINGS
55.1	GUARDRAIL REFLECTORS, TYPE C (SPECIAL)

HOT MIX ASPHALT MIXTURE REQUIREMENTS		
LOCATION AND MIXTURE USE(S):	TEMPORARY PAVEMENT	
PG:	SURFACE, 1.5"	BINDER, 6.5"
DESIGN AIR VOIDS	PG 64-22	PG 64-22
MIXTURE COMPOSITION	4.0 @ N50	4.0 @ N50
FRICTION AGGREGATE	IL 9.5	IL 19.0
MIX WEIGHT	C	N/A
QUALITY MANAGEMENT PROGRAM	112 LB/SY/IN	N/A
SUBLOT SIZE	QC/QA	QC/QA
MATERIAL TRANSFER DEVICE	N/A	N/A
	N/A	N/A

MODEL: Default FILE NAME: F:\2  GARZA KARHOFF ENGINEERING, LLC	USER NAME = Amalia_Baymundo	DESIGNED = AMB	REVISED =	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BAXTER RD OVER I-39 GENERAL NOTES, INDEX OF SHEETS AND STANDARD DRAWINGS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN = AMB	REVISED =			39	(201-1HB)BDR	WINNEBAGO	38	2
		CHECKED = PK	REVISED =			CONTRACT NO. 64S25				
	PLOT DATE = 4/29/2025	DATE = 4/29/2025	REVISED =			SCALE:	SHEET	OF	SHEETS	STA.

MODEL: Default
FILE NAME: F:\2087-TR01-2024-P1_PTB 213-030 Various Bridge Rehab\WO 02 - 64S25 Baxter Rd over I-39\05 CADD\Civil\Modals\D264S25-border.dgn

				CONSTRUCTION CODE	
				100% STATE	
CODE NO .	ITEM	UNIT	TOTAL QUANTITY	BRIDGE REHABILITATION	
				0059	
				RURAL	
				SN. 101-0145	
40600295	POL YMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	56	56	
40600982	HOT-MIX ASPHALT SUR FACE REMOVAL - BUTT JOINT	SQ YD	423	423	
44003100	MEDIAN REMOVAL	SQ FT	4521	4521	
40605030	POL YMERIZED HOT-MIX ASPHALT SUR FACE COURSE, STONE MATRIX ASPHALT, 12.5, MIX"C", N 50	TON	16	16	
50102400	CONCRETE REMOVAL	CU YD	15.8	15.8	
50157300	PROTECTIVE SHIELD	SQ YD	796	796	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	15.5	15.5	
50300260	BRIDGE DECK GROOVING	SQ YD	1120	1120	
50300300	PROTECTIVE COAT	SQ YD	1796	1796	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2340	2340	
50800515	BAR SPLICERS	EACH	16	16	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	125	125	
58700300	CONCRETE SEALER	SQ FT	165	165	
59000200	EPOXY CRACK INJECTION	FOOT	29	29	
60620800	CONCRETE MEDIAN, TYPE SB-9.12	SQ FT	4521	4521	

	USER NAME = Utkarsh.tappe	DESIGNED - AMB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BAXTER RD OVER I-39 SUMMARY OF QUANTITIES			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - AMB	REVISED -					88	(195-1HB-1)BDR	WHITESIDE	38	3
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	PLOT DATE = 3/24/2025	DATE - 3/19/2025	REVISED -		SCALE:	SHEET 1	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

MODEL: Default
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				CONSTRUCTION CODE
				100% STATE
CODE NO .	ITEM	UNIT	TOTAL QUANTITY	BRIDGE REHABILITATION
				0059
				RURAL
				SN. 101-0145
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3
67100100	MOBILIZATION	L SUM	1	1
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	2	2
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	1
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	2	2
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	56	56
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1779	1779
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	5084	5084
70307210	TEMPORARY PAVEMENT MARKING - LINE 24"- TYPE IV TAPE	FOOT	42	42
70400100	TEMPORARY CONCRETE BARRIER	FOOT	575	575
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	575	575
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2

	USER NAME = Utkarsh.tappe	DESIGNED - AMB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BAXTER RD OVER I-39 SUMMARY OF QUANTITIES				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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				CONSTRUCTION CODE	
				100% STATE	
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	BRIDGE REHABILITATION	
				0059	
				RURAL	
				SN. 101-0145	
70600350	IMPACT ATTENUATORS, RELOCATE (NON - REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
* 72000100	SIGN PANEL - TYPE 1	SQ FT	8	8	
* 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	2	2	
72900100	METAL POST - TYPE A	FOOT	22	22	
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	94	94	
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	4761	4761	
* 78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	490	490	
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	42	42	
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1654	1654	
X5110308	SLOPE WALL CRACK SEALING	FOOT	58	58	
X4400110	TEMPORARY PAVEMENT REMOVAL	SQ YD	502	502	
X5110310	SLOPE WALL REPAIR	SQ YD	2	2	
X5110312	SLOPE WALL SLURRY PUMPING	CU YD	1.3	1.3	
* X7820007	GUARDRAIL REFLECTORS, TYPE C (SPECIAL)	EACH	42	42	
Z0012142	BRIDGE DECK SCARIFICATION 2 1/4"	SQ YD	1211	1211	
Z0012162	BRIDGE DECK MICROSILICA CONCRETE OVERLAY 2 1/4"	SQ YD	1211	1211	

*= SPECIALTY ITEM

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	PLOT DATE = 3/24/2025	DATE - 3/19/2025	REVISED -				ILLINOIS FED. AID PROJECT				
SCALE:		SHEET 1 OF SHEETS		STA.		TO STA.					

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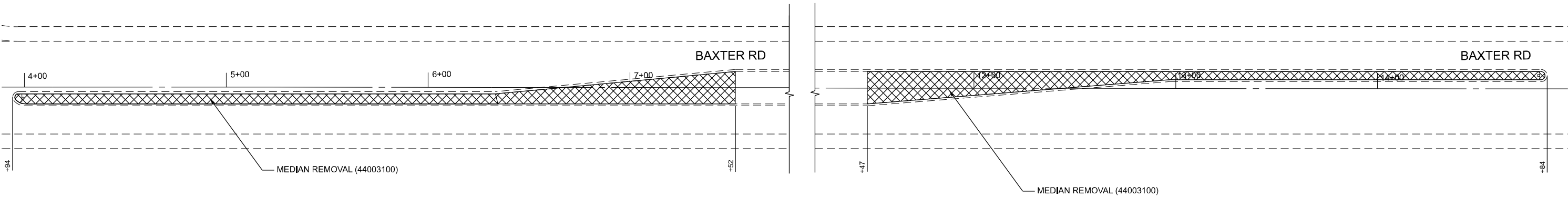
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	PLOT DATE = 3/24/2025	DATE - 3/19/2025	REVISED -		SCALE:	SHEET 1	OF	SHEETS	STA.	TO STA.			
					ILLINOIS FED. AID PROJECT								



MEDIAN WEST OF BRIDGE

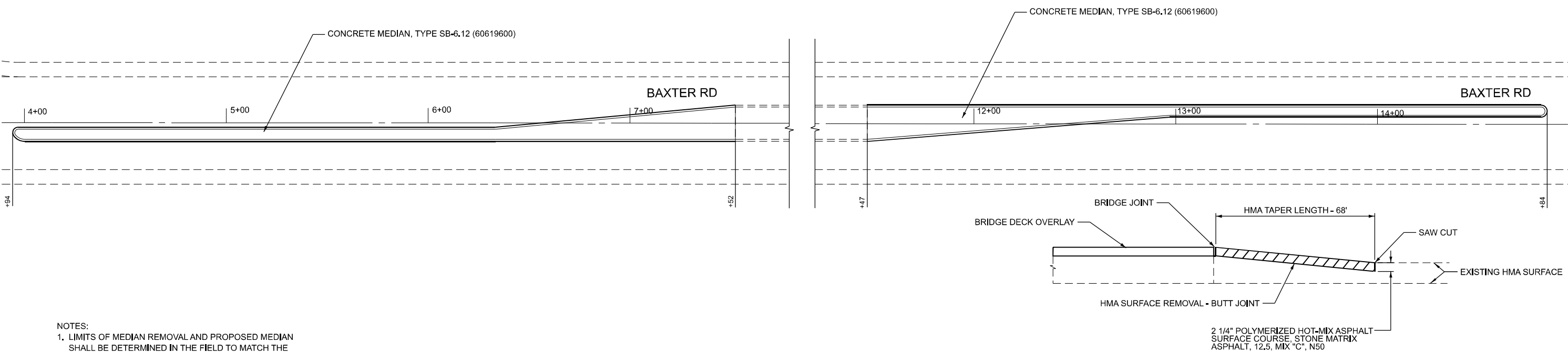
MEDIAN EAST OF BRIDGE



REMOVAL PLAN

MEDIAN WEST OF BRIDGE

MEDIAN EAST OF BRIDGE



NOTES:
1. LIMITS OF MEDIAN REMOVAL AND PROPOSED MEDIAN SHALL BE DETERMINED IN THE FIELD TO MATCH THE EXISTING CONDITIONS.

2. ALIGNMENT AND STATIONS ARE FOR INFORMATION ONLY.

PROPOSED PLAN

TYPICAL APPROACH DETAIL
WEST APPROACH AND EAST APPROACH

MODEL: Default
FILE NAME: F:\2097-TR01-2024-P1_PTB 213-030 Various Bridge Rehab\WO 02 - 64S25 Baxter Rd over I-39\05 CADD\Civil\Model\ID264S25-border.dgn



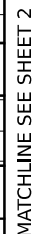
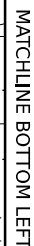
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		DRAWN	- AMB	REVISED	-
		CHECKED	- PK	REVISED	-
PLOT DATE	= 4/29/2025	DATE	- 4/29/2025	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAXTER RD OVER I-39
REMOVAL AND PROPOSED PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	7
CONTRACT NO. 64S25				
ILLINOIS FED. AID PROJECT				



BAXTER RD

0+00

1+00

EXISTING PAVEMENT MARKINGS TO REMAIN

EXISTING PAVEMENT
MARKINGS TO REMAIN
EXCEPT FOR THE
SOUTHEAST AND
NORTHEAST RADII



R4-7 (24" x 30")
SIGN PANEL - TYPE 1 = 5 SF
METAL POST - TYPE A = 11 FT

— MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (WHITE)

— MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (YELLOW)

— MODIFIED URETHANE PAVEMENT MARKING -
LINE 8" (6' SKIP - 2' DASH, WHITE)

BAXTER RD

SB 1-39

MATCHLINE TOP RIGHT

ETC 2+00

~~3+00~~

4+00 ↙

5+00 ↘

6+00

~~7.00~~
$$\overline{8+00}$$

9+00

MATCHLINE SEE SHEET 2

— MODIFIED URETHANE PAVEMENT
MARKING - LINE 8" (WHITE)

MODIFIED URETHANE PAVEMENT MARKING -
LETTERS & SYMBOLS (TYP.)

MODIFIED URETHANE PAVEMENT
MARKING - LINE 4" (YELLOW)

REMOVE EXISTING SIGN PANEL AND
POST. COST WILL BE PAID FOR AS
REMOVE SIGN PANEL ASSEMBLY - TYPE A

NOTE:
LIMITS OF PAVEMENT MARKINGS SHALL BE DETERMINED IN
THE FIELD TO MATCH THE EXISTING CONDITIONS.

BAXTER RD OVER I-39
PROPOSED PAVEMENT MARKINGS AND SIGNING

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	8
		CONTRACT NO. 64S25		
		ILLINOIS FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

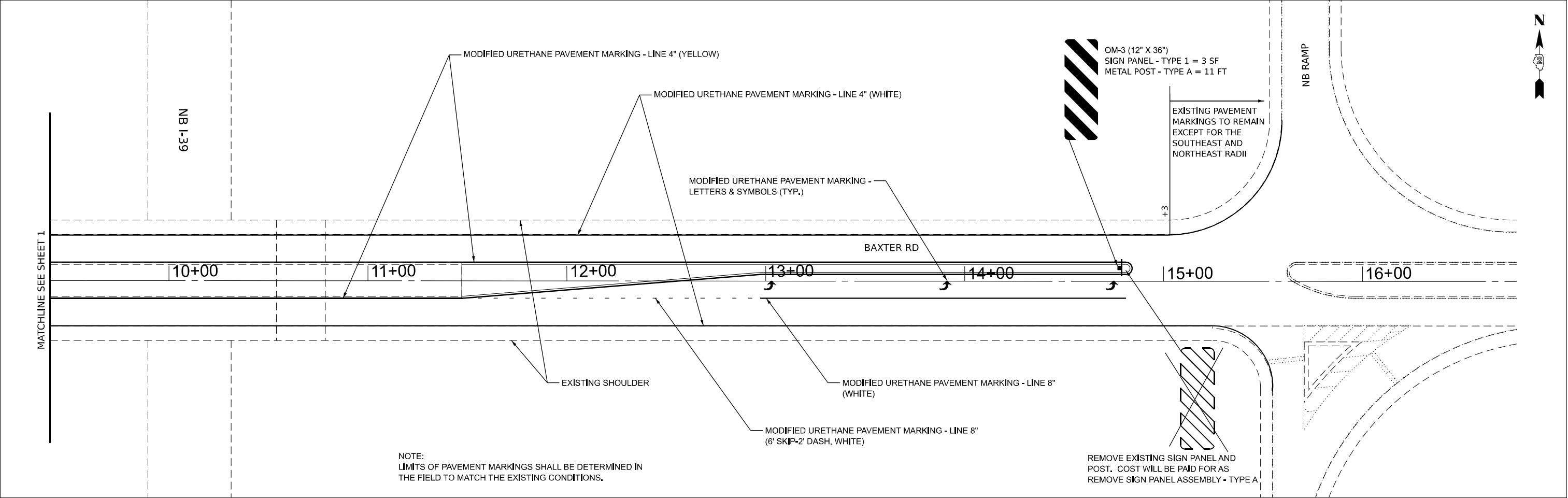
SCALE: 1" = 50'	SHEET 1	OF 2	SHEETS	STA.	TO STA.
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		CONT
ILLINOIS	FED. AID PROJECT	

MODEL: Default
FILE NAME: F:\2097.TRF01-2024-P1_PTB 213-030 Various Bridge Rehab\WO 02 - 64S25 Baxter Rd over I-39\05 CADD\Civil\Models\ID264S25-border.dgn



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GARZA KARHOFF
ENGINEERING, LLC

USER NAME	= Amalia.Baymundo	DESIGNED	-	AMB	REVISED	-
		DRAWN	-	AMB	REVISED	-
		CHECKED	-	PK	REVISED	-
PLOT DATE	= 4/29/2025	DATE	-	4/29/2025	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

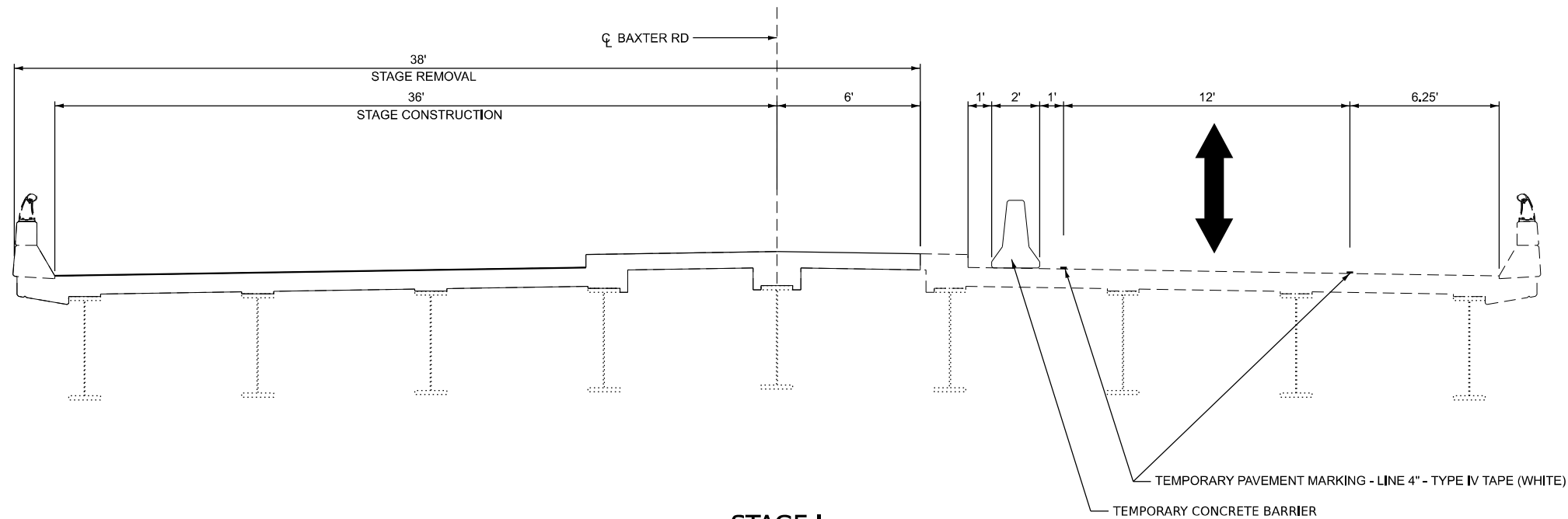
BAXTER RD OVER I-39
PROPOSED PAVEMENT MARKINGS AND SIGNING

SCALE: 1" = 50'

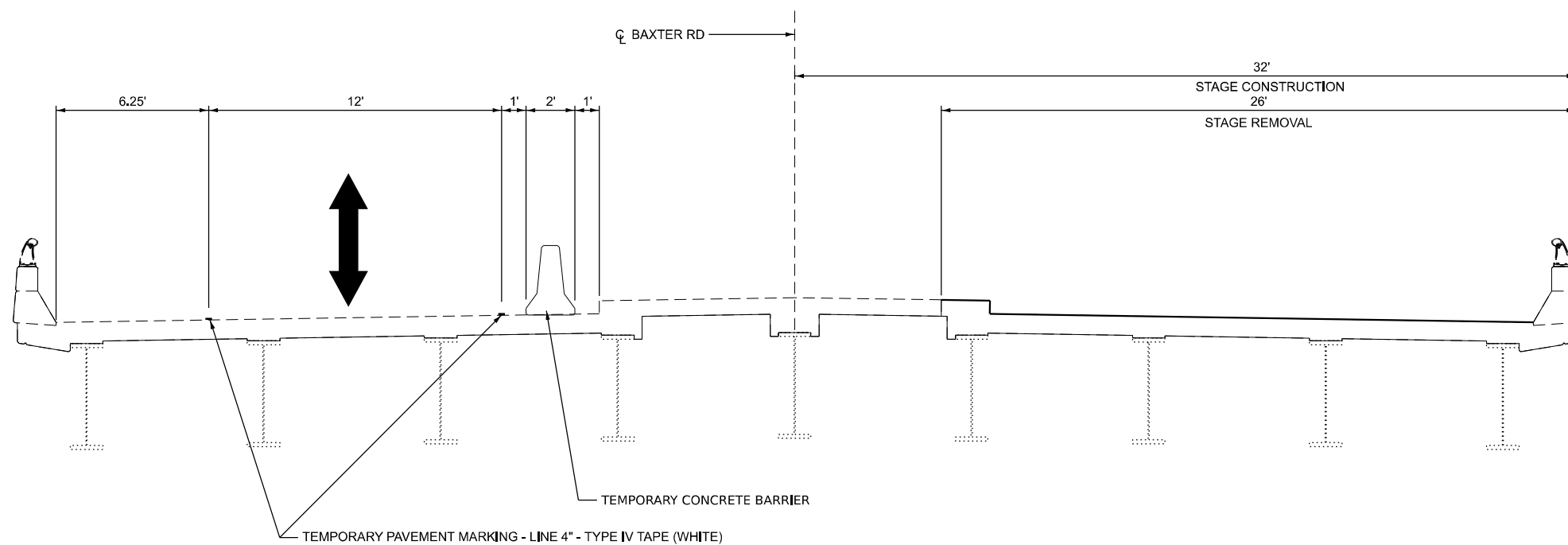
SHEET 2 OF 2 SHEETS

STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	9
CONTRACT NO. 64S25				
ILLINOIS FED. AID PROJECT				



STAGE I



STAGE II

MODEL: Default
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	DRAWN - AMB	REVISED -
	CHECKED - PK	REVISED -
PLOT DATE = 4/29/2025	DATE - 4/29/2025	REVISED -

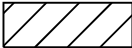
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAXTER RD OVER I-39
MAINTENANCE OF TRAFFIC TYPICAL SECTIONS

SCALE: 1" = 50' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	10
CONTRACT NO. 64S25				
ILLINOIS FED. AID PROJECT				

LEGEND



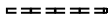
WORK ZONE



IMPACT ATTENUATOR



TRAFFIC SIGN



TEMPORARY CONCRETE BARRIER



DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
(@ 50' C-C TANGENT, 25' C-C TAPER, @ 10' C-C CURVE)



TYPE III BARRICADE



DIRECTION OF TRAFFIC



TEMPORARY PAVEMENT



CONTROLLER BOX

SUGGESTED MAINTENANCE OF TRAFFIC SEQUENCING

PRESTAGE

- REMOVE CONCRETE MEDIAN AND CURB AND GUTTER FROM APPROXIMATE STATIONS 4+00 TO 8+00 AND 12+00 TO 15+00 TO A DEPTH OF 8". ENGINEER SHALL VERIFY THE DEPTH REMOVAL.
- CONSTRUCT TEMPORARY PAVEMENT OVER REMOVED MEDIAN.
- REMOVE EXISTING SIGN PANELS AND SUPPORTS ON MEDIAN.
- INSTALL TEMPORARY TRAFFIC SIGNALS.

STAGE 1

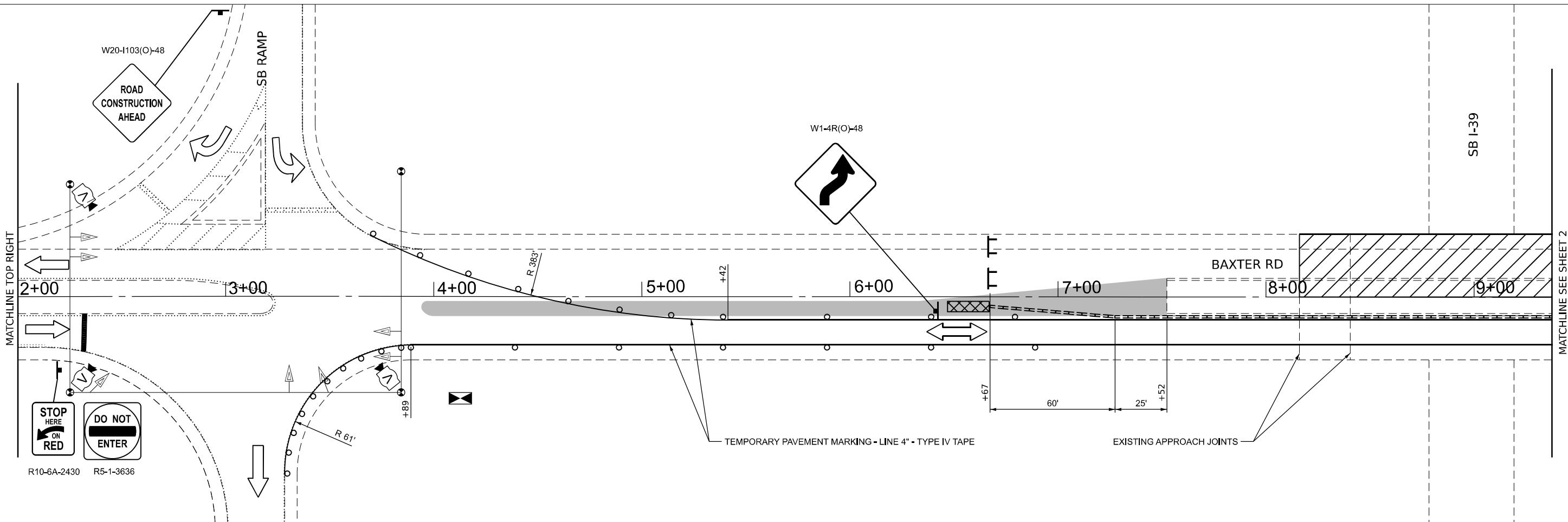
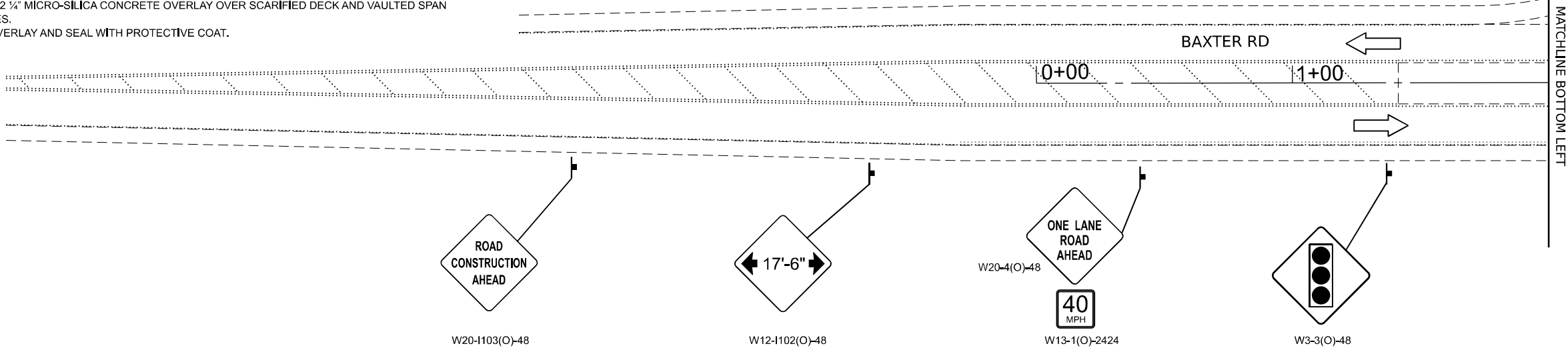
- REMOVE EXISTING PAVEMENT MARKINGS IN CONFLICT WITH STAGE 1 TRAFFIC AND INSTALL TEMPORARY PAVEMENT MARKINGS.
- INSTALL PROTECTIVE SHIELD.
- REMOVE 1 ½" EXISTING CONCRETE OVERLAY WITH 2 ¼" BRIDGE DECK SCARIFICATION ON THE EXISTING WB TRAFFIC LANES ON THE BRIDGE DECK AND VAULTED SPAN APPROACHES.
- COMPLETE DECK SLAB REPAIR (FULL AND PARTIAL).
- BEGIN EXPANSION JOINT REPLACEMENT.
- REMOVE CONCRETE FROM THE DECK, VAULTED SPAN AND RAISED MEDIAN AT THE EAST AND WEST EXPANSION JOINTS.
- INSTALL NEW PREFORMED JOINT STRIP SEAL AT THE ABUTMENTS AND POUR NEW CONCRETE IN THE DECK, VAULTED SPAN AND RAISED MEDIAN AREAS.
- POUR NEW 2 ¼" MICRO-SILICA CONCRETE OVERLAY OVER SCARIFIED DECK AND VAULTED SPAN APPROACHES.
- GROOVE OVERLAY AND SEAL WITH PROTECTIVE COAT.

STAGE 2

- REMOVE EXISTING PAVEMENT MARKINGS IN CONFLICT WITH STAGE 2 TRAFFIC AND INSTALL TEMPORARY PAVEMENT MARKINGS.
- INSTALL PROTECTIVE SHIELD, REMOVE 1½" EXISTING CONCRETE OVERLAY WITH 2½" BRIDGE DECK SCARIFICATION ON THE EXISTING EB TRAFFIC LANES ON THE BRIDGE DECK AND VAULTED SPAN APPROACHES.
- COMPLETE DECK SLAB REPAIR (FULL AND PARTIAL).
- COMPLETE EXPANSION JOINT REPLACEMENT.
- REMOVE CONCRETE FROM THE DECK AND VAULTED SPAN AT THE EAST AND WEST EXPANSION JOINTS.
- INSTALL NEW PREFORMED JOINT STRIP SEAL AT THE ABUTMENTS AND POUR NEW CONCRETE IN THE DECK, VAULTED SPAN AND RAISED MEDIAN AREAS.
- POUR NEW 2¼" MICRO-SILICA CONCRETE OVERLAY OVER SCARIFIED DECK AND VAULTED SPAN APPROACHES.
- GROOVE OVERLAY AND SEAL WITH PROTECTIVE COAT.
- PERFORM CONCRETE REPAIRS AT ABUTMENTS AND SLOPE WALLS.
- AFTER COMPLETION OF BRIDGE WORK, REMOVE TEMPORARY PAVEMENT AND CONSTRUCT PROPOSED CONCRETE MEDIAN FROM APPROXIMATE STATIONS 4+00 TO 8+00 AND 12+00 TO 15+00.
- INSTALL PROPOSED PAVEMENT MARKINGS AND SIGN PANELS AND SUPPORTS.

NOTES:

1. REMOVAL OF CONCRETE MEDIAN SHALL BE PAID FOR AS MEDIAN REMOVAL (44003100). ALL SAW CUTS NECESSARY TO COMPLETE THE WORK SHALL BE INCLUDED IN THE COST FOR THE VARIOUS PAY ITEMS INVOLVED.
2. LIMITS OF TEMPORARY PAVEMENT MARKINGS AND LOCATIONS OF TEMPORARY CONCRETE BARRIER, TEMPORARY IMPACT ATTENUATOR, DRUMS, AND SIGNAGE SHALL BE DETERMINED IN THE FIELD AND APPROVED BY ENGINEER.
3. CHANGEABLE MESSAGE SIGNS SHALL BE PLACED AT EACH DIRECTION OF I-39 AND BAXTER ROAD TWO WEEKS PRIOR TO START OF WORK.
4. MEDIAN REMOVAL AND REPLACEMENT SHALL BE COMPLETED UTILIZING FLAGGERS TO CLOSE A LANE DURING THIS OPERATION. TRAFFIC SHALL NOT BE ALLOWED TO TRAVEL ON BOTH SIDES OF WORKERS DURING THIS OPERATION.
5. FOR TRAFFIC CONTROL ON BRIDGE WORK AND ADVANCED SIGNING SPACINGS, SEE HIGHWAY STANDARD 701321. SIGNS USED FOR THE BRIDGE WORK SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, STANDARD 701321.



USER NAME = Amalia.Baymundo

DESIGNED - AMB

DRAWN - AMB

CHECKED - PK

REVISÉ -

REVISÉ -

REVISÉ -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**BAXTER RD OVER I-39
MAINTENANCE OF TRAFFIC - STAGE 1 PLAN**

SCALE: 1" = 50'	SHEET 1	OF 2	SHEETS	STA.	TO STA.
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F.A.I. RTE.
39

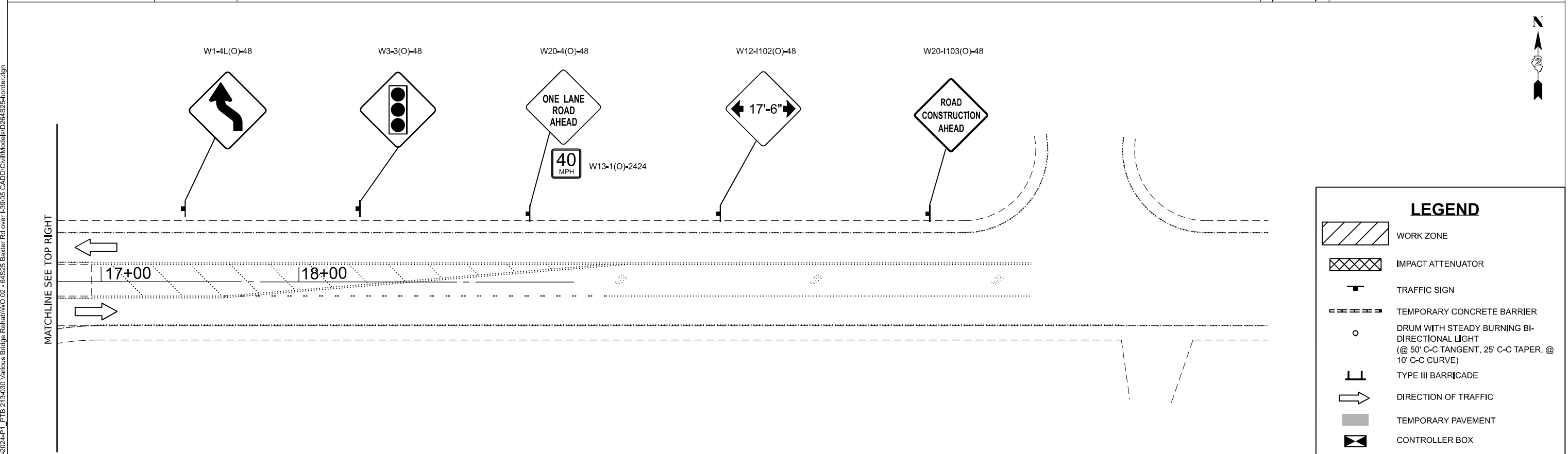
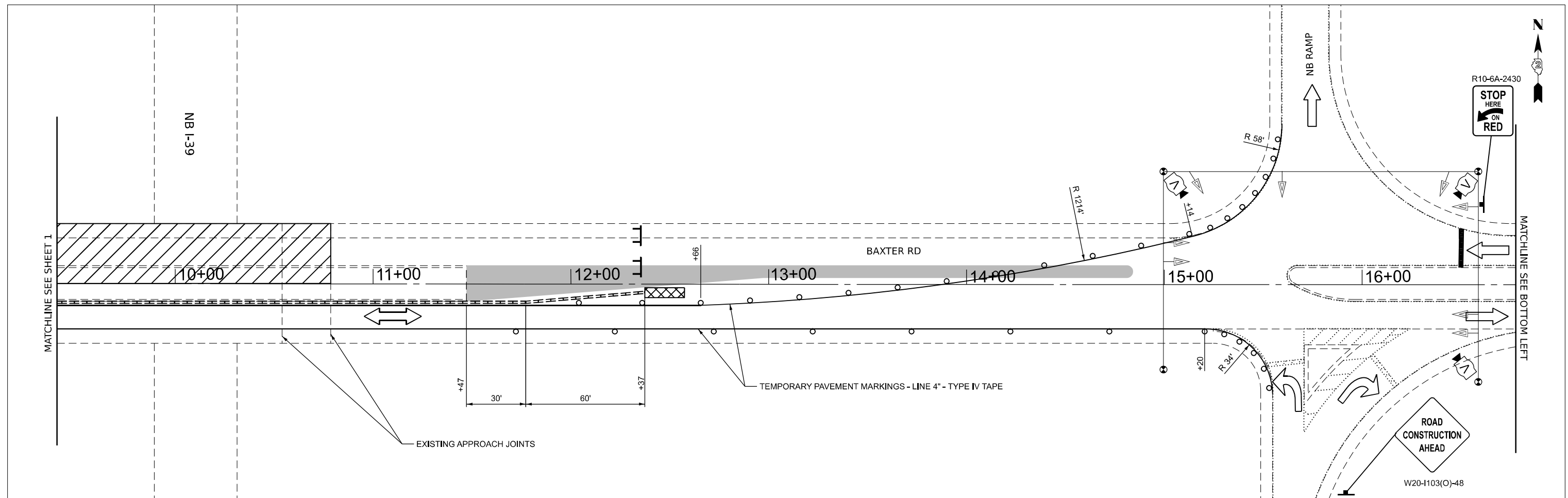
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(201-1HB)BD

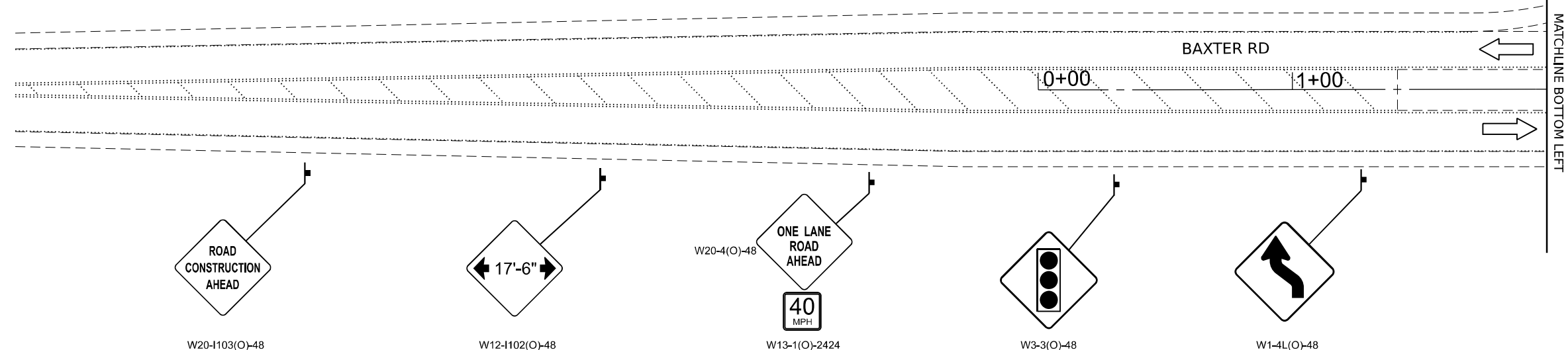
COURT
WINNER

TOTAL SHEETS	
38	

CONTRACT NO. 64S25

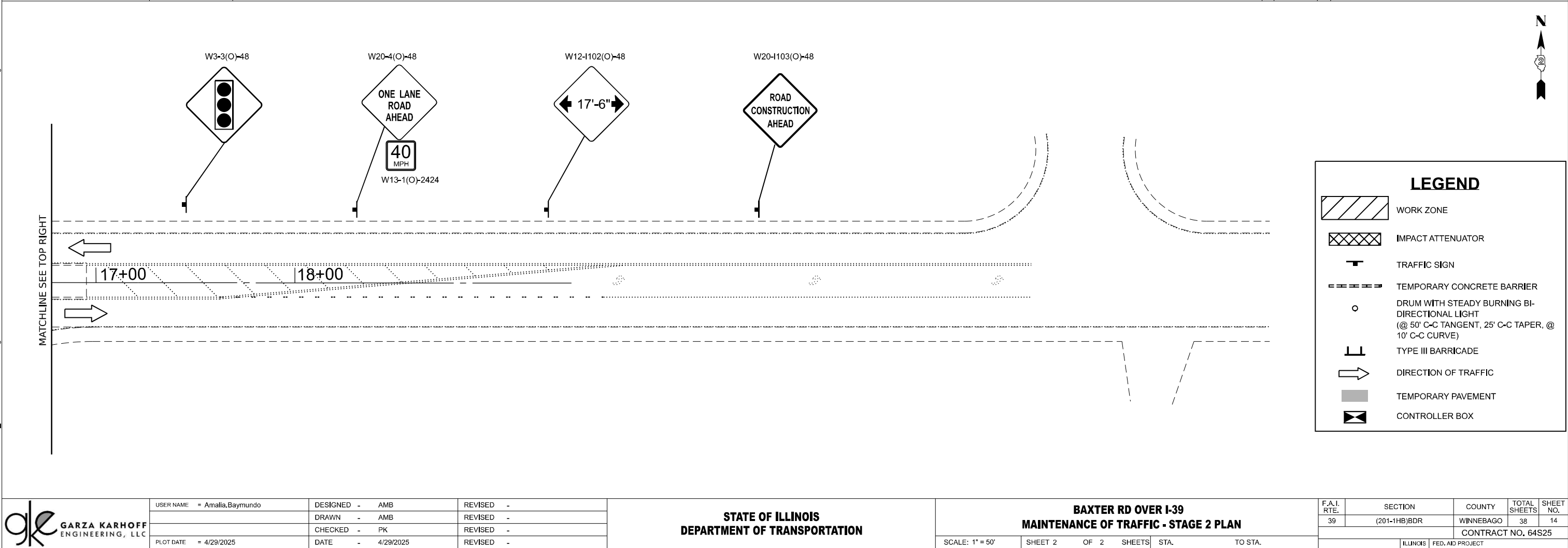
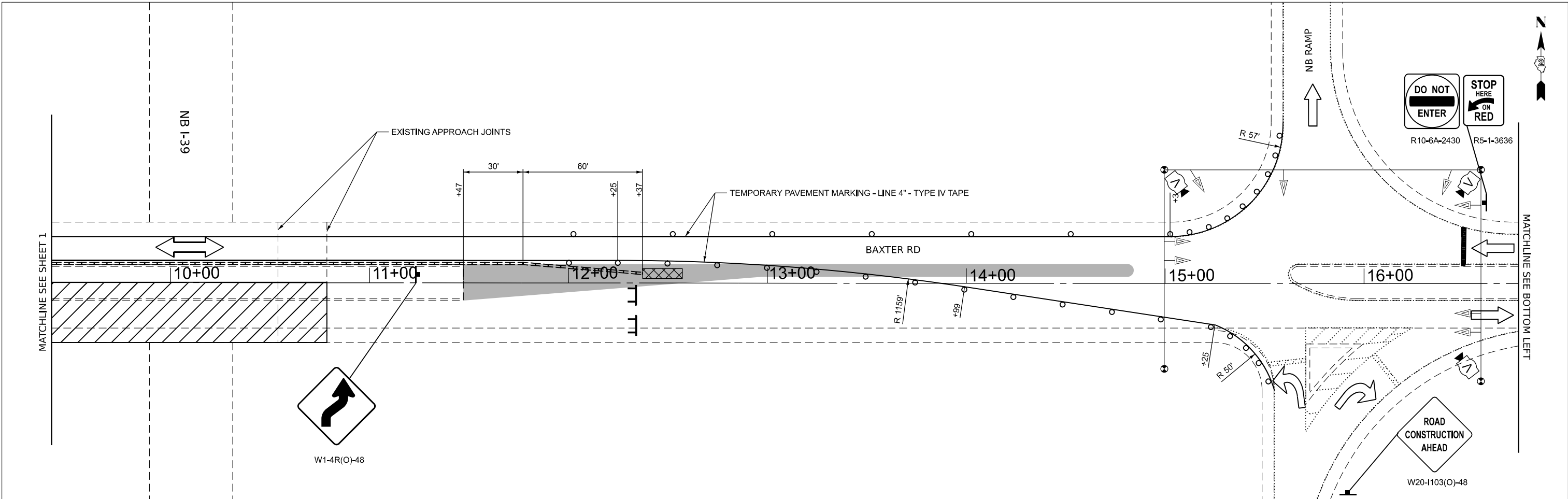
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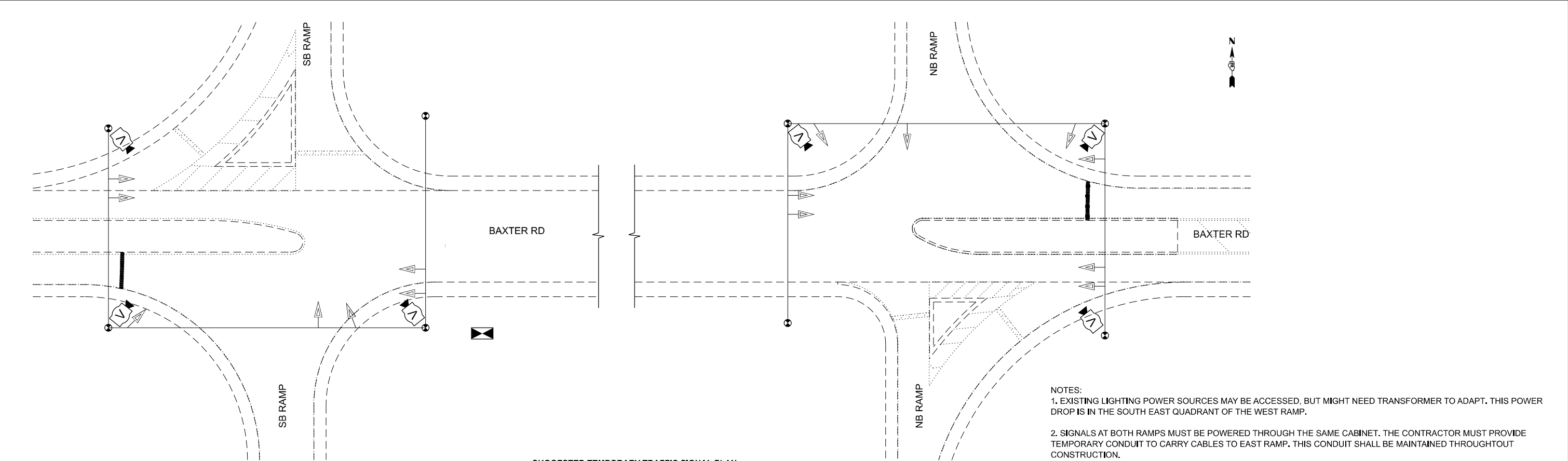


<div><div><div>GARZA KARHOFF</div><div>ENGINEERING, LLC</div></div></div>	USER NAME = Amalia.Baymundo	DESIGNED - AMB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BAXTER RD OVER I-39 MAINTENANCE OF TRAFFIC - STAGE 2 PLAN				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - AMB	REVISED -		39	(201-1HB)BDR	WINNEBAGO	38	13				
		CHECKED - PK	REVISED -		CONTRACT NO. 64S25								
	PLOT DATE = 4/29/2025	DATE - 4/29/2025	REVISED -		SCALE: 1" = 50'	SHEET 1	OF 2	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

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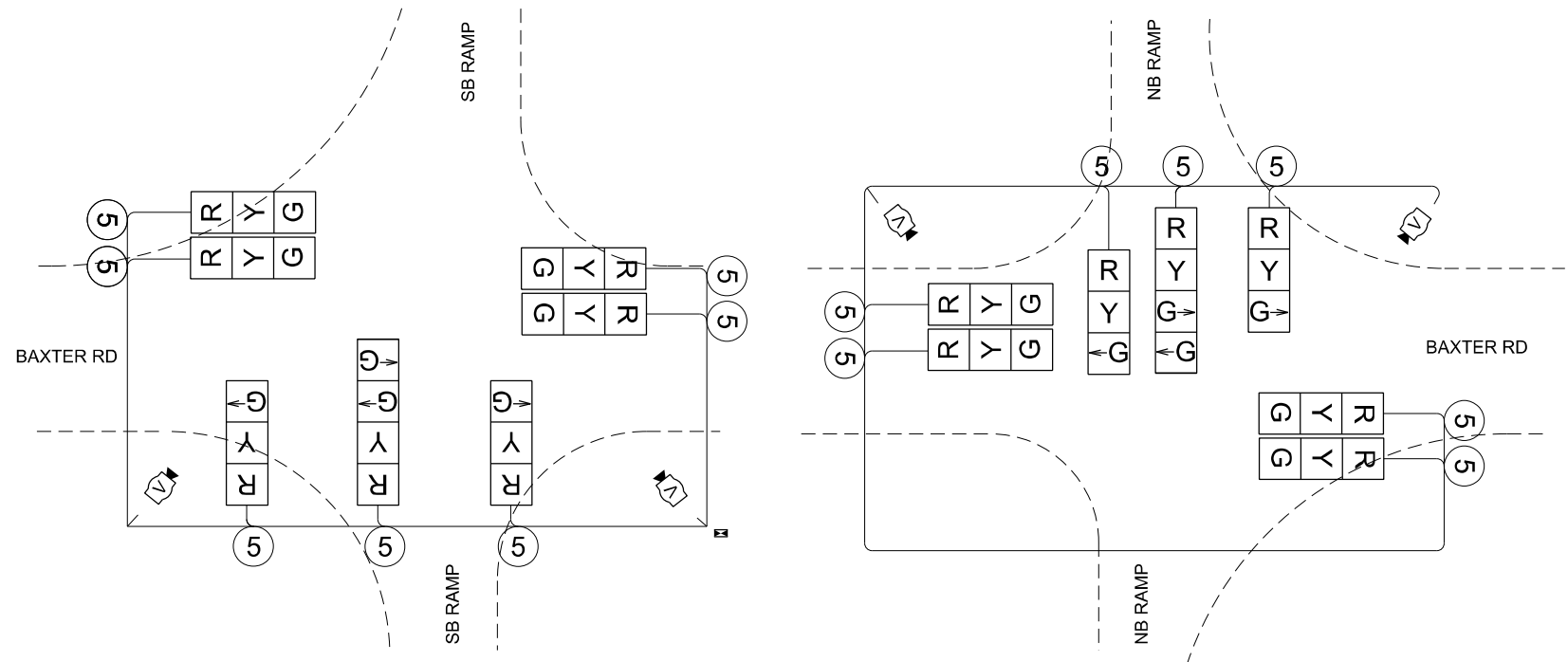


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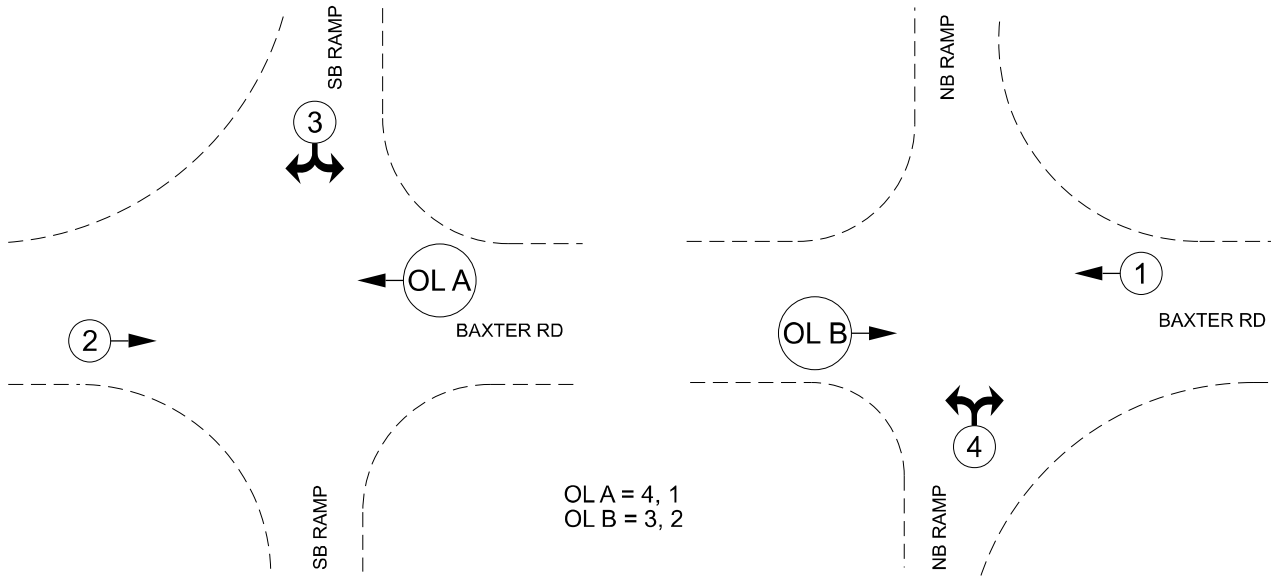


SUGGESTED TEMPORARY TRAFFIC SIGNAL PLAN

- NOTES:
- EXISTING LIGHTING POWER SOURCES MAY BE ACCESSED, BUT MIGHT NEED TRANSFORMER TO ADAPT. THIS POWER DROP IS IN THE SOUTH EAST QUADRANT OF THE WEST RAMP.
 - SIGNALS AT BOTH RAMPS MUST BE POWERED THROUGH THE SAME CABINET. THE CONTRACTOR MUST PROVIDE TEMPORARY CONDUIT TO CARRY CABLES TO EAST RAMP. THIS CONDUIT SHALL BE MAINTAINED THROUGHTOUT CONSTRUCTION.
 - IF A SECOND CABINET/POWER SOURCE IS NEEDED AT THE EAST RAMP TO POWER CAMERA DETECTION, THERE MAY BE AVAILABLE AT THE SHELL STATION.
 - THE MEDIAN NOSE, CURB AND CONCRETE SURFACES MUST BE REMOVED AND REPLACED WITH TEMPORARY PAVEMENT TO FACILITATE TRAFFIC WEARING INTO A SINGLE, OPEN LANE. THIS WILL BE SHOWN ON SEPARATE PAY ITEMS TO COMPENSATE FOR FINAL REPLACEMENT IN KIND.



TEMPORARY CABLE PLAN
(NOT TO SCALE)



PHASE DIAGRAM
(NOT TO SCALE)

Benchmark: #26 Brass tablet "B.M. 20'-D.M-6" in concrete post (flush) 32' Rt. of Baxter Rd. Sta. 317+73 Elev. 851.096.

Existing Structure:

The structure is a two-span continous, composite plate girder bridge with a 8-inch reinforced concrete deck with a 1½-inch microsilica concrete overlay. At the vaulted abutments are 24'-6" approach spans. Expansion joints are preformed joint seals. The original structure was built in 1980. Approach bents are supported by steel piles. Piers and abutments are supported by footings on rock.

Traffic will be maintained during construction using staged construction and temporary traffic signals.

No salvage.

DESIGN SPECIFICATIONS

2024 AASHTO LRFD Bridge Design
Specifications, 10th Edition

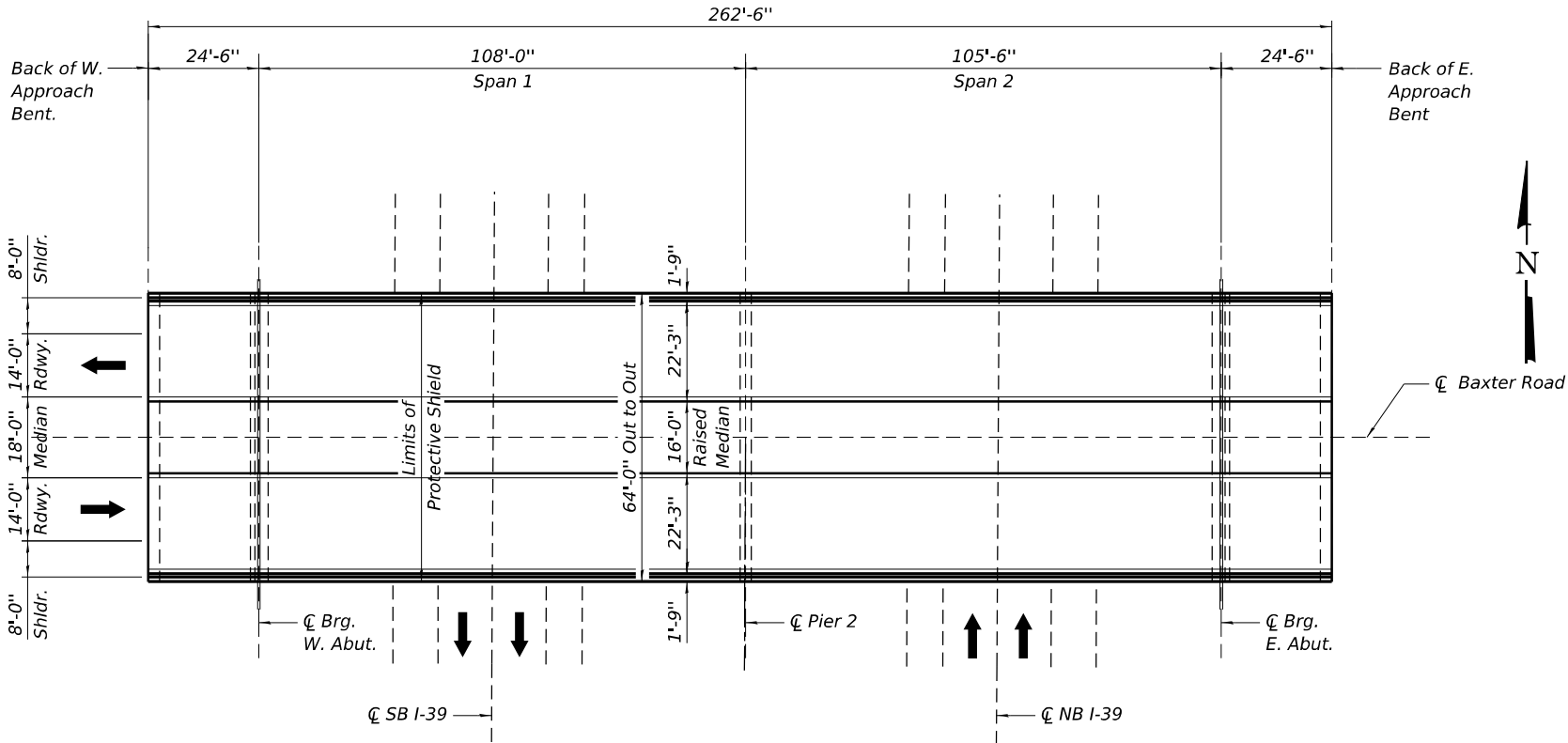
DESIGN STRESSES

FIELD UNITS

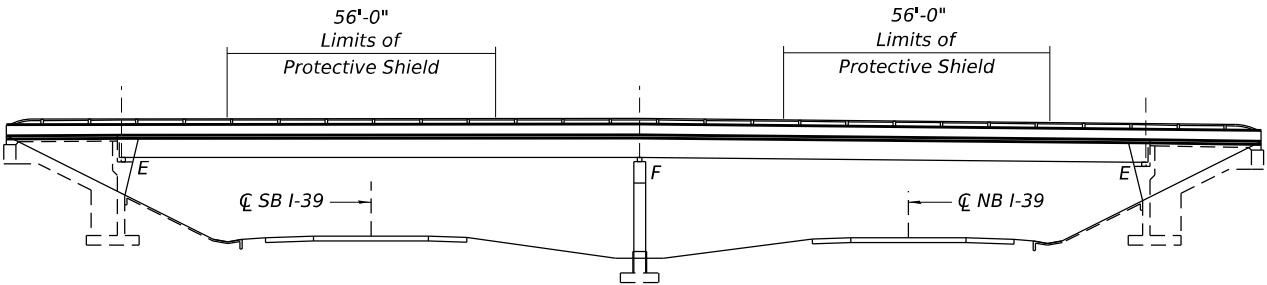
f'c = 4,000 psi (Superstructure)
f'c = 3,500 psi (Substructure)
fy = 60,000 psi (Reinforcement)

SCOPE OF WORK

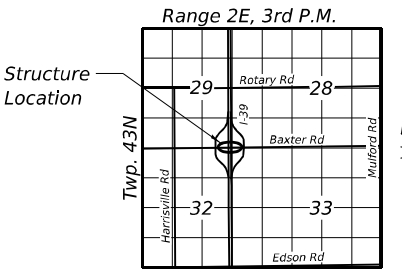
1. Remove and replace expansion joints at East and West Abutments.
2. Remove existing concrete overlay.
3. Repair areas of the bridge deck.
4. Repair approach slab.
5. Install new Microsilica overlay.
6. Repair abutments.
7. Repair piers.
8. Repair East and West Slope wall.



PLAN



ELEVATION



LOCATION SKETCH



Ralph Otrembiak
Signed: 4/29/2025
Expires: 11/30/2026

GENERAL PLAN AND ELEVATION
BAXTER ROAD OVER FAI-39 (I-39)

SECTION (201-1HB)BDR

WINNEBAGO COUNTY

STATION 2362+30.45

STRUCTURE NO. 101-0145



USER NAME	=	DESIGNED	-	RO	REVISED	-
		CHECKED	-	EG	REVISED	-
PLOT SCALE	=	DRAWN	-	RO	REVISED	-
PLOT DATE	=	CHECKED	-	EG	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 101-0145

SHEET 1 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	16
CONTRACT NO. 64S25				

ILLINOIS FED. AID PROJECT

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GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Prior to pouring the new concrete, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
3. As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.
Any cracks that cannot be removed by grinding 3/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Concrete Sealer shall be applied to the areas of Abutment Cap concrete repair. All surfaces to be sealed shall be cleaned thoroughly prior to sealer application. Cost included with Concrete Sealer.
5. Protective Coat shall be applied to the top and inside surface of new concrete at expansion joint replacement per Section 503.19 of the Standard Specifications.
6. The repairs in these plans were identified from a visual inspection and a chain drag deck delamination survey in February 2025. The Contractor shall verify the loactions and quantities in the field at the direction of the Engineer.

INDEX OF SHEETS

1. General Plan and Elevation
2. General Data
3. Stage Construction Details
4. Temporary Concrete Barrier
5. Deck Repair Plans and Details
6. Expansion Joint Repairs
7. Expansion Joint Repair Details I
8. Expansion Joint Repair Details II
9. Preformed Joint Strip Seal (1 of 3)
10. Preformed Joint Strip Seal (2 of 3)
11. Preformed Joint Strip Seal (3 of 3)
12. Abutment Repair Details
13. Slope Wall Repair Details
14. Bar Splicer Details

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	14.1	-	14.1
Protective Shield	Sq.Yd.	796	-	796
Concrete Superstructure	Cu. Yd.	14.1	-	14.1
Bridge Deck Grooving	Sq.Yd.	1,239	-	1,239
Protective Coat	Sq.Yd.	1,257	-	1,257
Reinforcement Bars, Epoxy Coated	Pound	2,210	-	2,210
Bar Splicers	Each	22	-	22
Preformed Joint Strip Seal	Foot	125	-	125
Concrete Sealer	Sq. Ft.	-	165	165
Epoxy Crack Injection	Foot	-	29	29
Slope Wall Crack Sealing	Foot	-	58	58
Slope Wall Repair	Sq.Yd.	-	2	2
Slope Wall Slurry Pumping	Cu. Yd.	-	1.3	1.3
Bridge Deck Microsilica Concrete Overlay 2 1/4"	Sq.Yd.	1,211	-	1,211
Bridge Deck Scarification 2 1/4"	Sq. Yd.	1,211	-	1,211
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq. Ft.	-	197	197
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	12	-	12
Deck Slab Repair (Partial)	Sq. Yd.	37	-	37



GARZA KARHOFF

ENGINEERING, LLC

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PLOT SCALE	=	DRAWN	-	RO	REVISED	-
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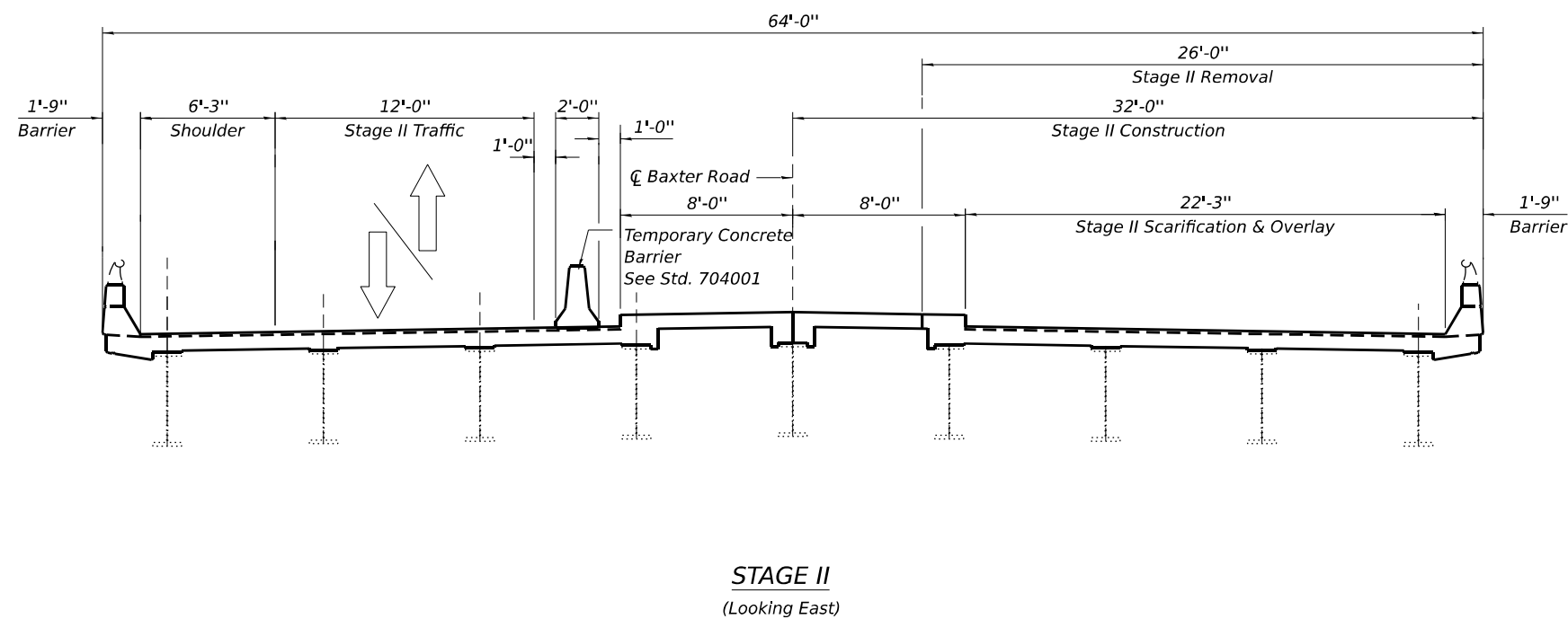
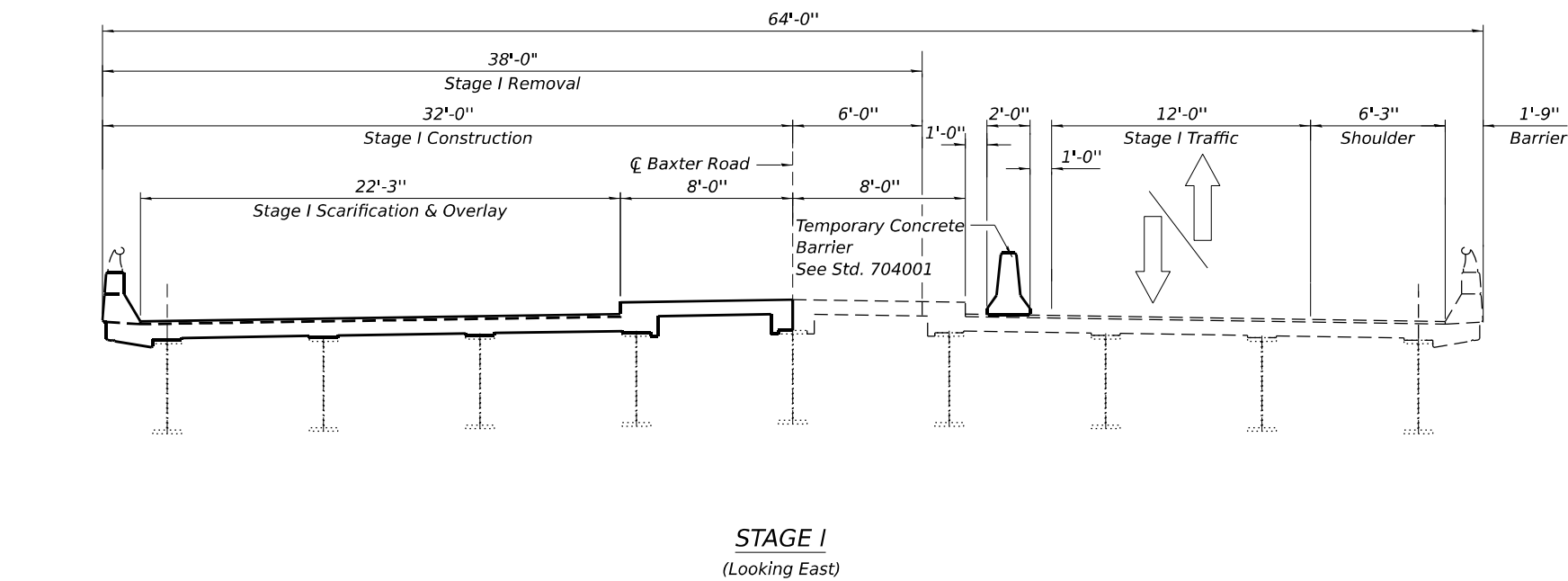
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL DATA
STRUCTURE NO. 101-0145

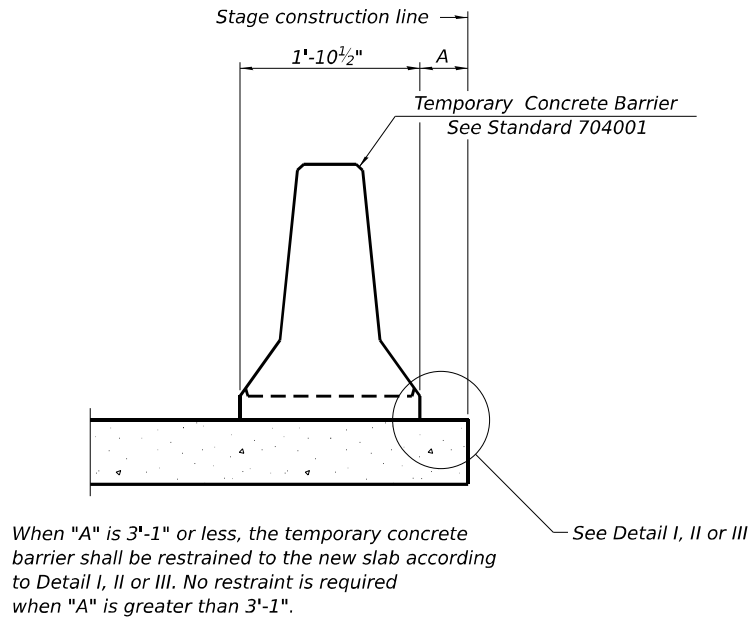
SHEET 2 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		CONTRACT NO. 64S25		
		ILLINOIS	FED. AID PROJECT	

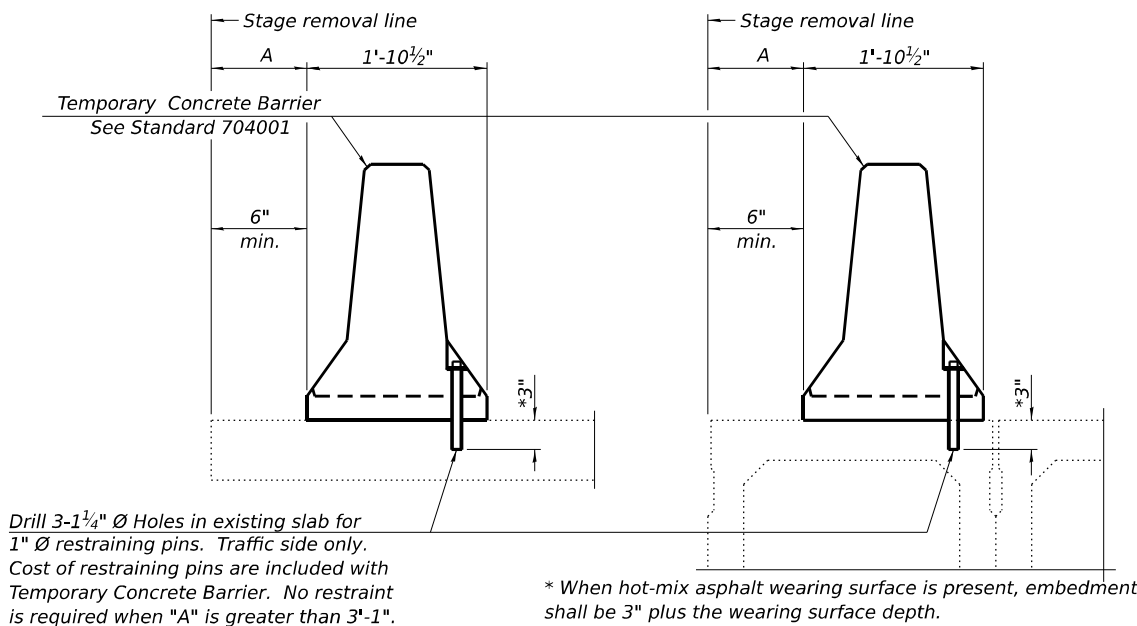
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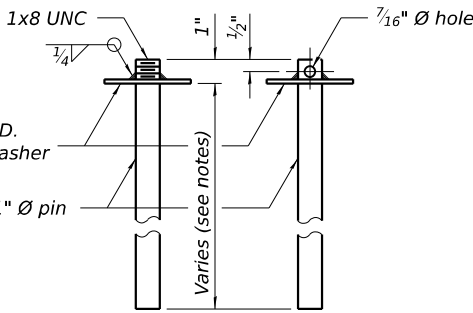
NEW SLAB OR NEW DECK BEAM



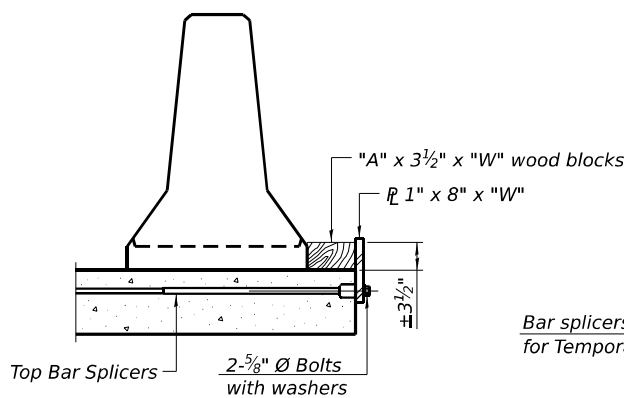
EXISTING SLAB

EXISTING DECK BEAM

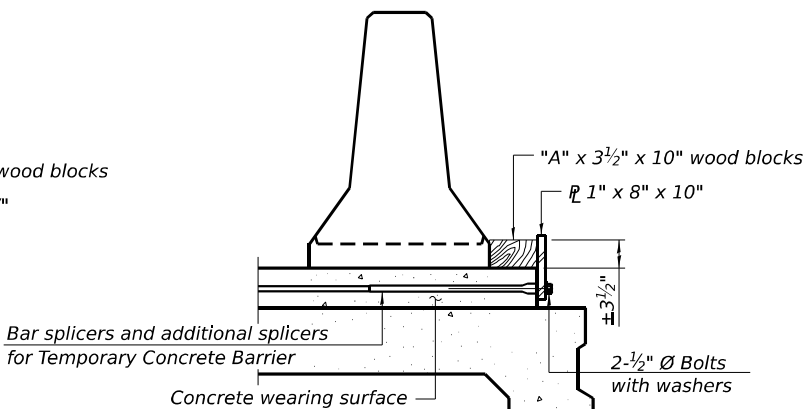
SECTIONS THRU SLAB OR DECK BEAM



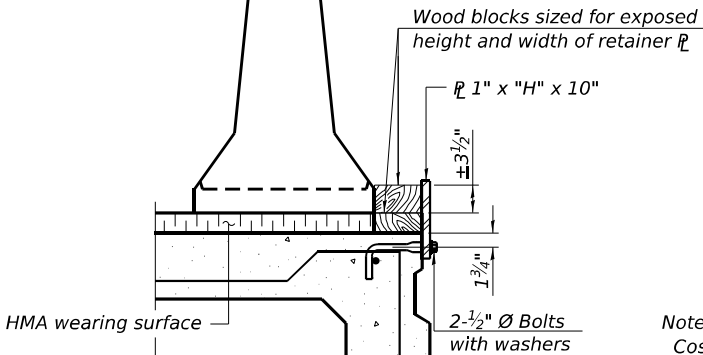
RESTRAINING PIN



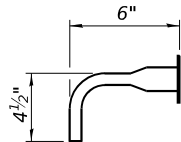
DETAIL I



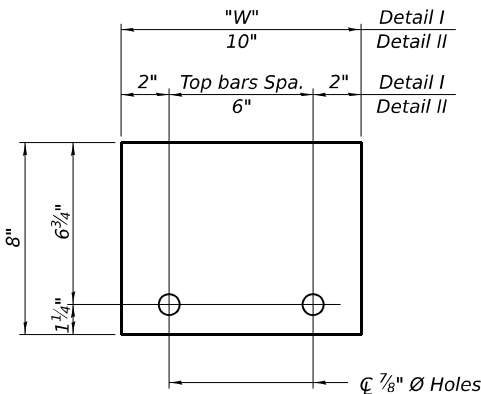
DETAIL II



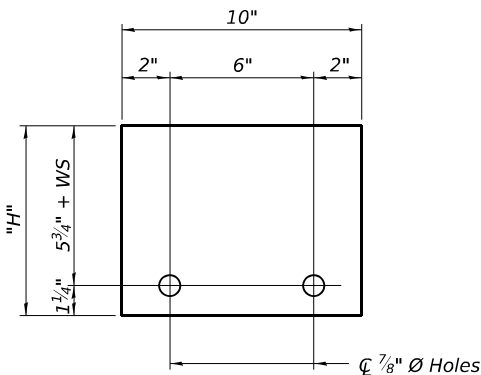
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER 1" x "H" x 10"
(Detail III)

Notes:
Cost of retainer assembly is included with Temporary Concrete Barrier.
A retainer assembly shall be located at the approximate C of each temporary concrete barrier.
The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
When the 'A' dimension is less than 1 1/2", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate.
For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.
Detail I - Installation for a new bridge deck or bridge slab.
Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27

5-15-2023



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PLOT DATE =	DRAWN - RO	REVISED -
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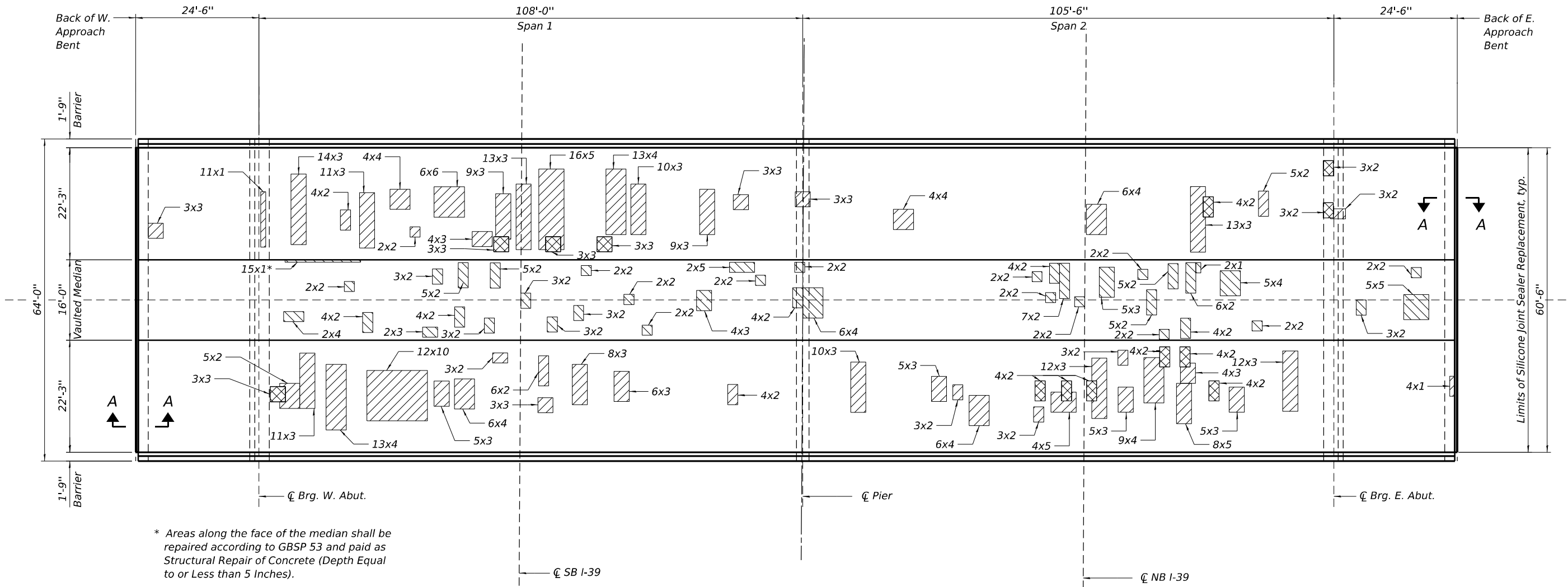
TEMPORARY CONCRETE BARRIER
STRUCTURE NO. 101-0145

SHEET 4 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	19
CONTRACT NO. 64S25				
ILLINOIS FED. AID PROJECT				

NOTES:

1. Partial depth deck repairs under the proposed overlay are shown for information only. Deck repairs under the proposed overlay shall be according to GBSP 29.
2. For Details of Expansion Joint Removal and Replacement, see sheets 6 thru 8 of 14.
3. Areas of delamination on the bottom of the deck shall be marked prior to installation of the protective shield.



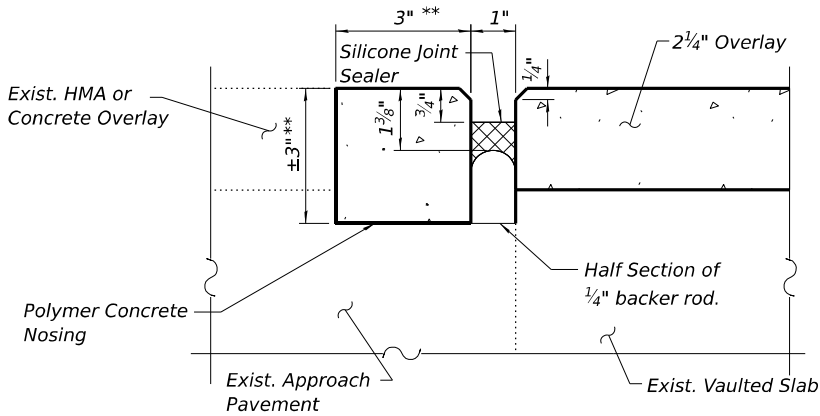
PLAN

BILL OF MATERIAL

Item	Unit	Total
Concrete Removal	Cu. Yd.	0.3
Bridge Deck Grooving	Sq. Yd.	1,097
Protective Coat	Sq. Yd.	1,248
Bridge Deck Scarification 2 1/4"	Sq. Yd.	1,211
Bridge Deck Microsilica Concrete Overlay 2 1/4"	Sq. Yd.	1,211
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq. Ft.	15
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	12
Deck Slab Repair (Partial)	Sq. Yd.	35
Silicone Joint Sealer, 1"	Foot	121
Polymer Concrete	Cu. Ft.	7.6

LEGEND

- Partial Depth Deck Patching
- Partial Depth Deck Patching (for Information Only)
- Full Depth Deck Patching



SECTION A-A

** Verify Concrete Removal and Polymer Concrete dimensions in the field.

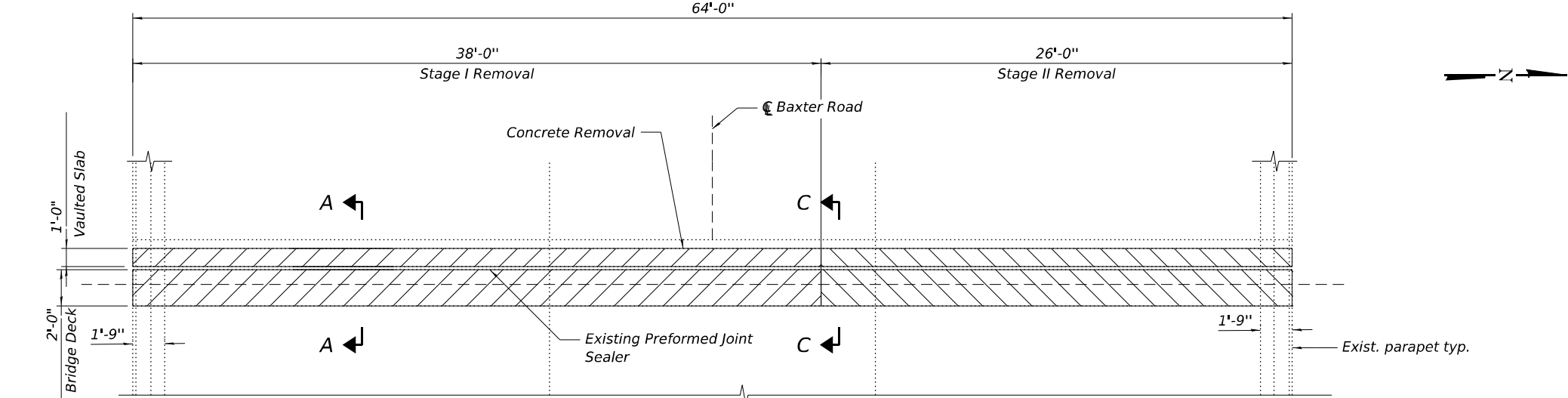
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REPAIR PLANS
STRUCTURE NO. 101-0145

SHEET 5 OF 14 SHEETS

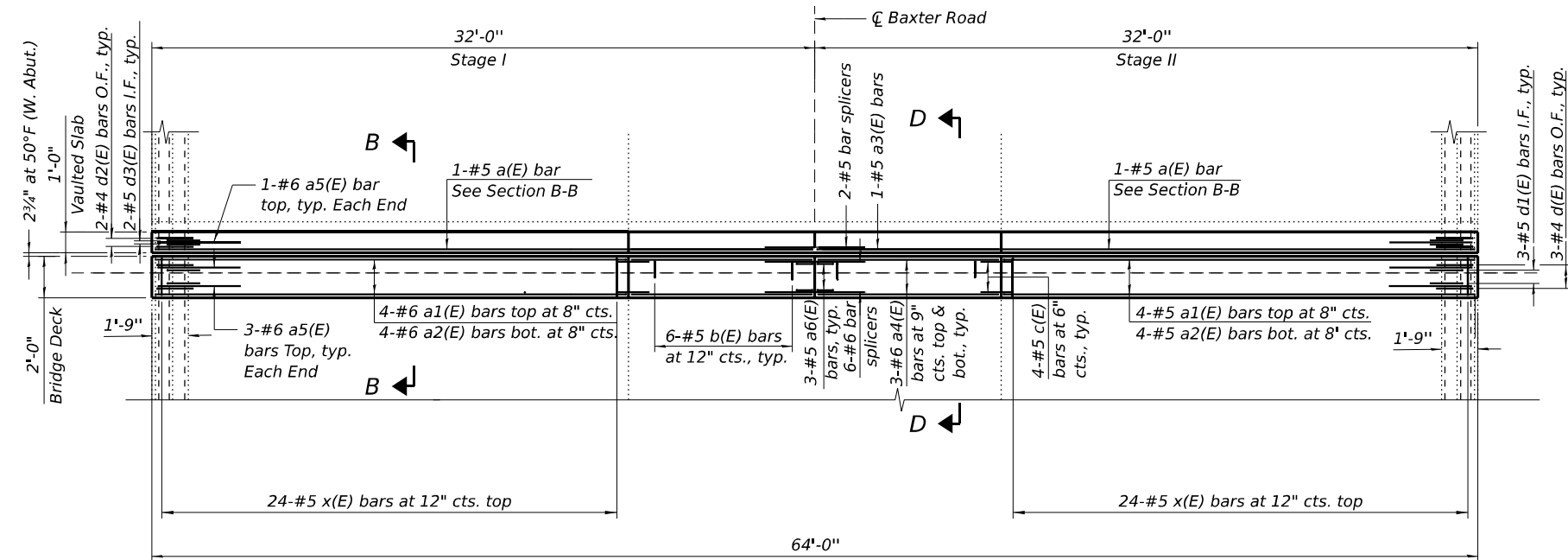
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CONTRACT NO. 64S25				
ILLINOIS FED. AID PROJECT				

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CONCRETE REMOVAL PLAN

(West Abutment shown,
East Abutment Similar)

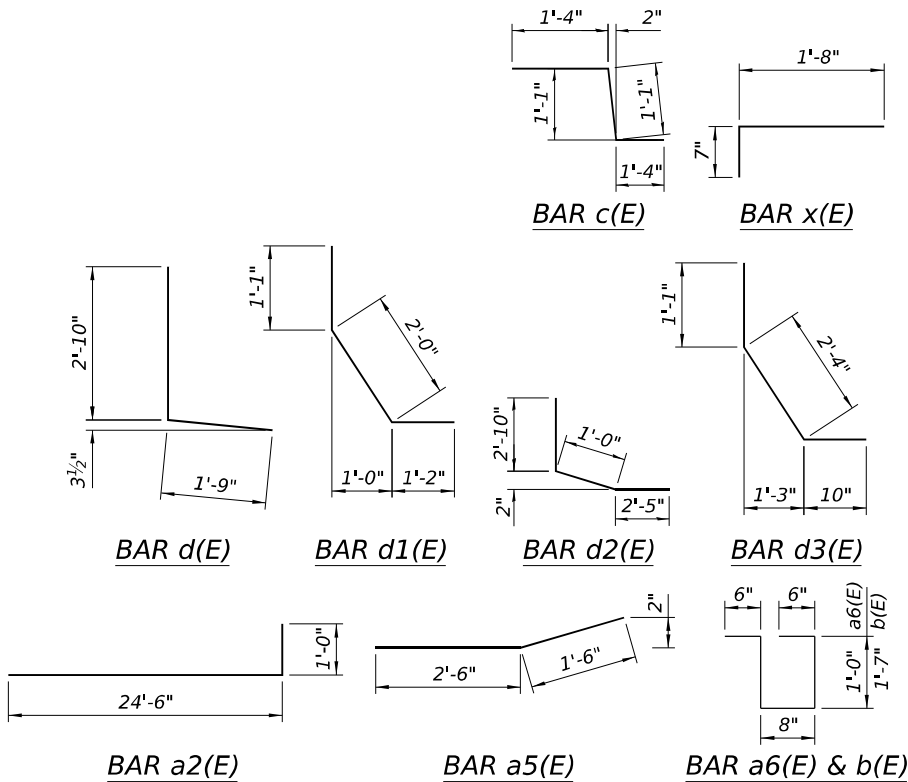


CONCRETE REPLACEMENT PLAN

(West Abutment shown,
East Abutment Similar)

**BILL OF MATERIAL
FOR TWO JOINTS**

Bar	No.	Size	Length	Shape
a(E)	4	#5	30'-9"	
a1(E)	16	#6	24'-6"	
a2(E)	16	#6	25'-6"	
a3(E)	3	#5	7'-6"	
a4(E)	24	#6	7'-6"	
a5(E)	16	#6	4'-0"	
a6(E)	12	#5	3'-8"	
b(E)	24	#5	4'-11"	
c(E)	16	#5	3'-9"	
d(E)	12	#4	4'-7"	
d1(E)	12	#5	4'-3"	
d2(E)	8	#4	6'-3"	
d3(E)	8	#5	4'-3"	
x(E)	96	#5	2'-3"	
Item			Unit	Total
Concrete Removal			Cu. Yd.	15.5
Concrete Superstructure			Cu. Yd.	15.5
Bridge Deck Grooving			Sq. Yd.	23
Protective Coat			Sq. Yd.	46
Reinforcement Bars, Epoxy Coated			Pound	2,340
Bar Splicers			Each	16



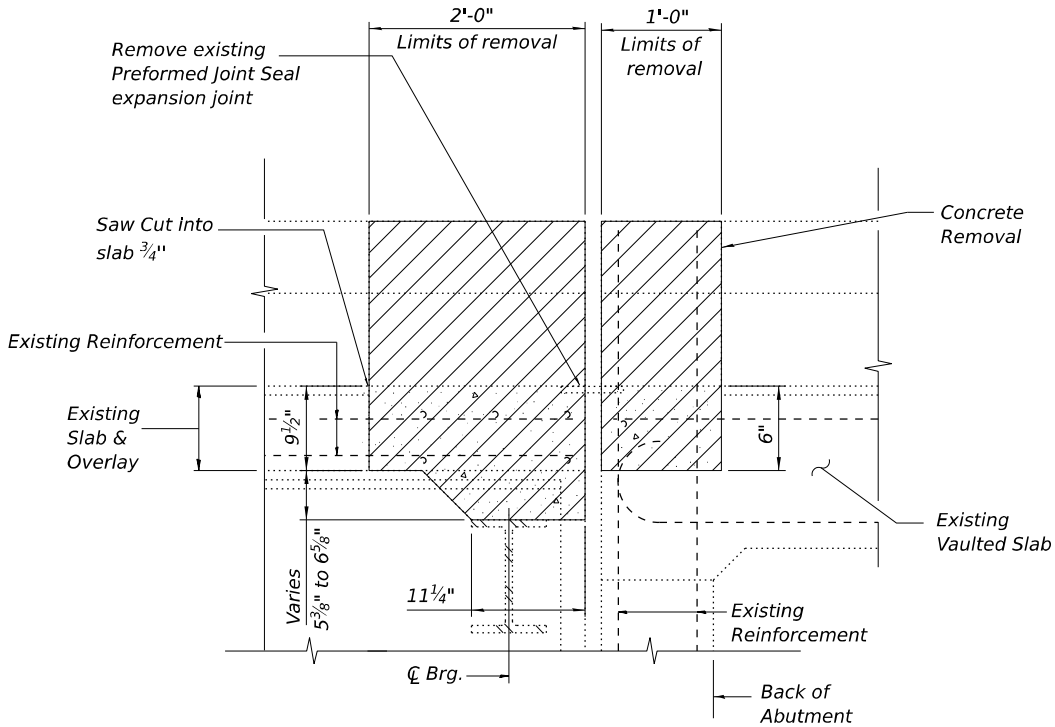
NOTES:

- I.F. denotes Inside Face. O.F. denotes Outside Face.
- See Sheet 7 of 14 for Sections A-A and B-B and Sections thru Parapet.
- See Sheet 8 of 14 for Sections C-C and D-D and Sections thru Median.

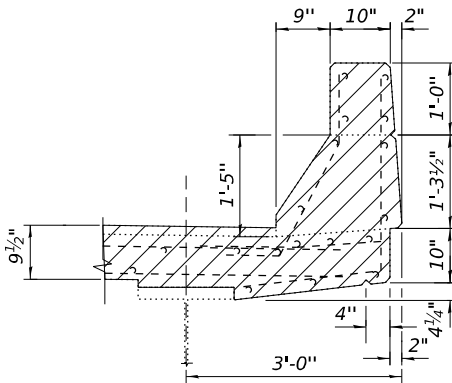
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		CHECKED - EG	REVISED -			39	(201-1HB)BDR	WINNEBAGO	38	21
	PLOT SCALE =	DRAWN - RO	REVISED -			CONTRACT NO. 64S25				
	PLOT DATE =	CHECKED - EG	REVISED -			ILLINOIS FED. AID PROJECT				
		SHEET 6 OF 14 SHEETS								

NOTES:

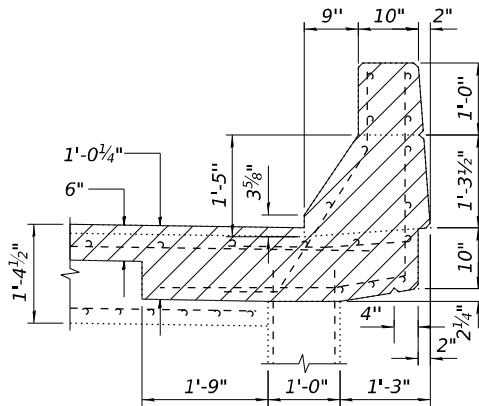
- Existing reinforcement bars extending into the concrete removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be repaired or replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Existing reinforcement bars in the concrete removal area parallel to the expansion joints shall be removed, unless noted otherwise.
- Removal and disposal of the existing expansion joints will not be paid for separately, but shall be included with the cost of Concrete Removal.
- If existing name plate falls within the limits of Concrete Removal, it shall be removed and reinstalled in its original location in accordance with IDOT Std. 515001. Cost included with Concrete Superstructure.
- If existing guardrail and/or end shoe fall within the limits of Concrete Removal, they shall be removed and reinstalled in their original location. Cost included with Concrete Superstructure.
- The Contractor shall exercise extreme care with the existing conduits in sections of the parapet to be removed and to protect and support the conduit. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer, at no additional cost to the Department. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable at no additional cost to the Department.
- See Sheet 6 of 14 for Bar Bending Details and Bill of Material.
- Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50 deg. F.



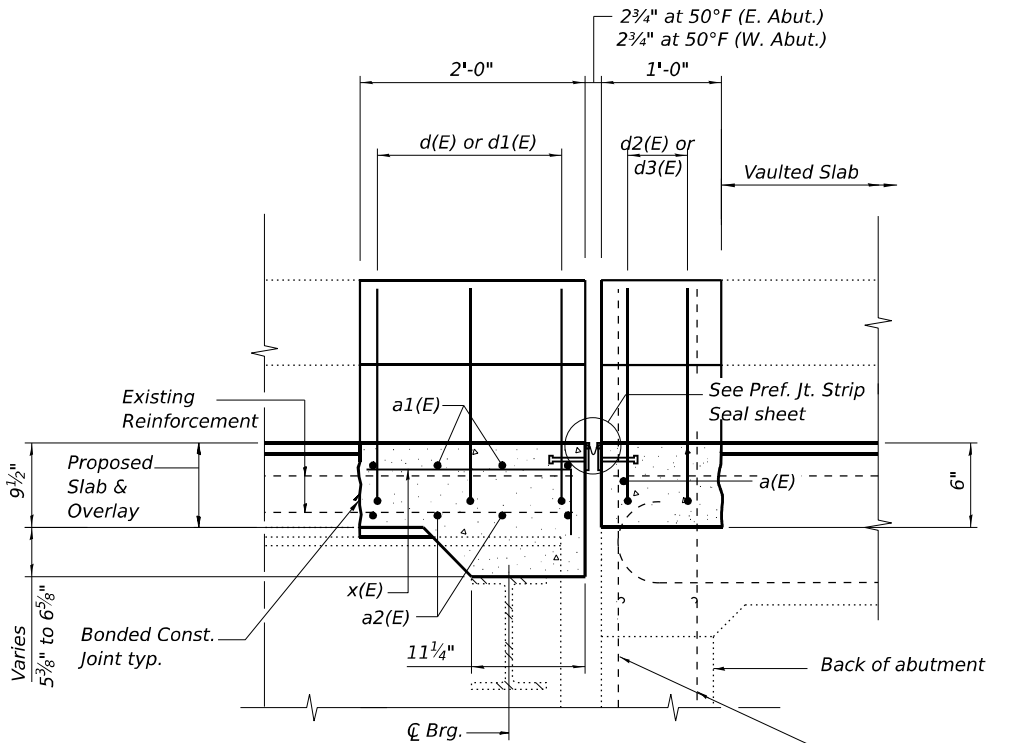
SECTION A-A



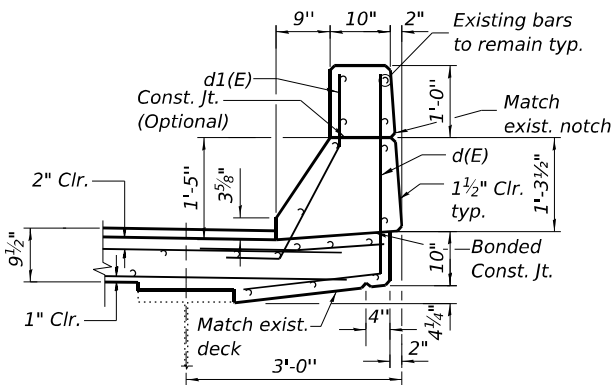
EXISTING DECK
PARAPET SECTION



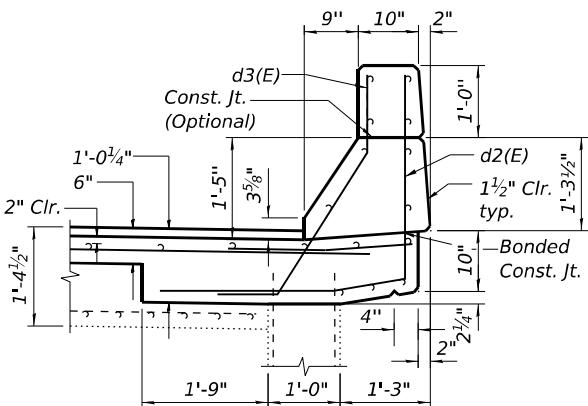
EXISTING VAULTED SLAB
PARAPET SECTION



SECTION B-B



PROPOSED DECK
PARAPET SECTION



PROPOSED VAULTED SLAB
PARAPET SECTION

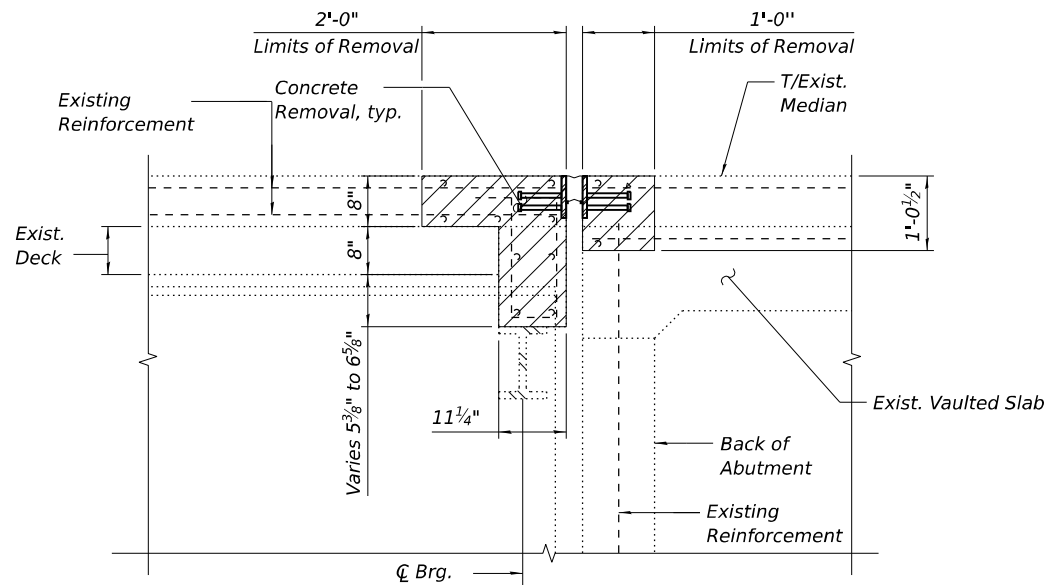
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT REPAIR DETAILS I
STRUCTURE NO. 101-0145

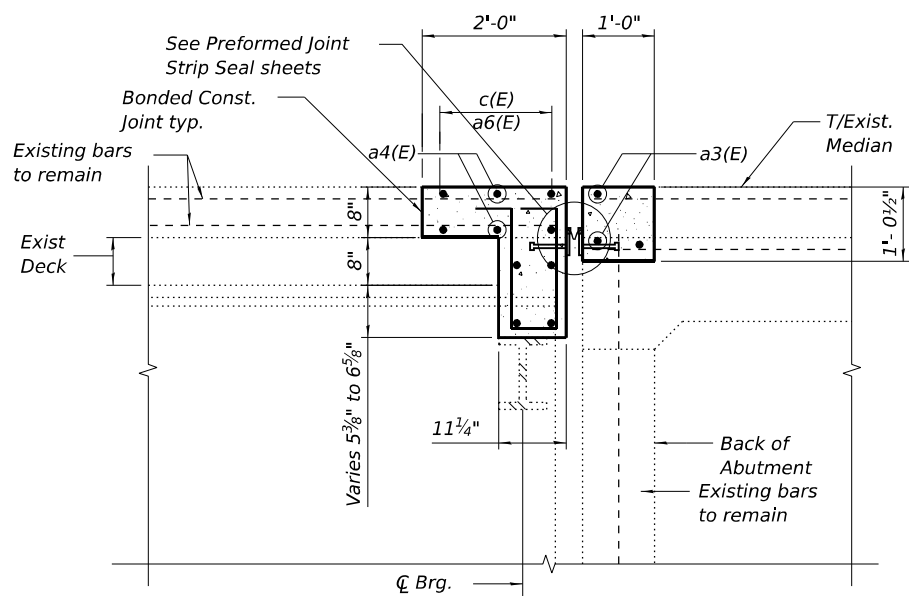
SHEET 7 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	22
CONTRACT NO. 64S25				
ILLINOIS FED. AID PROJECT				

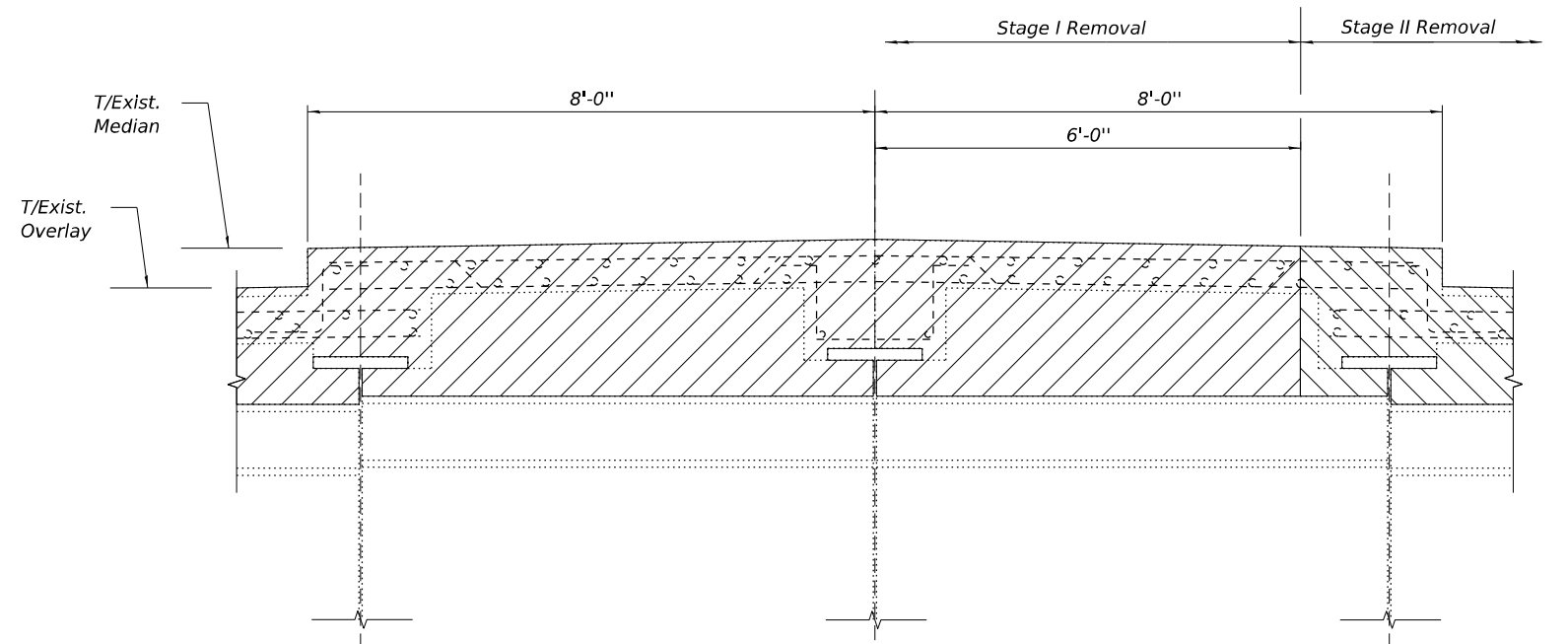
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FILE NAME: F:\2097-TR01-2024-P1_PTB 213-030 Various Bridge Rehab\WO 02 - 64525 Baxier Rd over I-39\05 CADD\Structure\Sheets\264525-sh1-101-0145-08-Exp\Details3.dgn.dgn
4/30/2025 3:55:51 PM



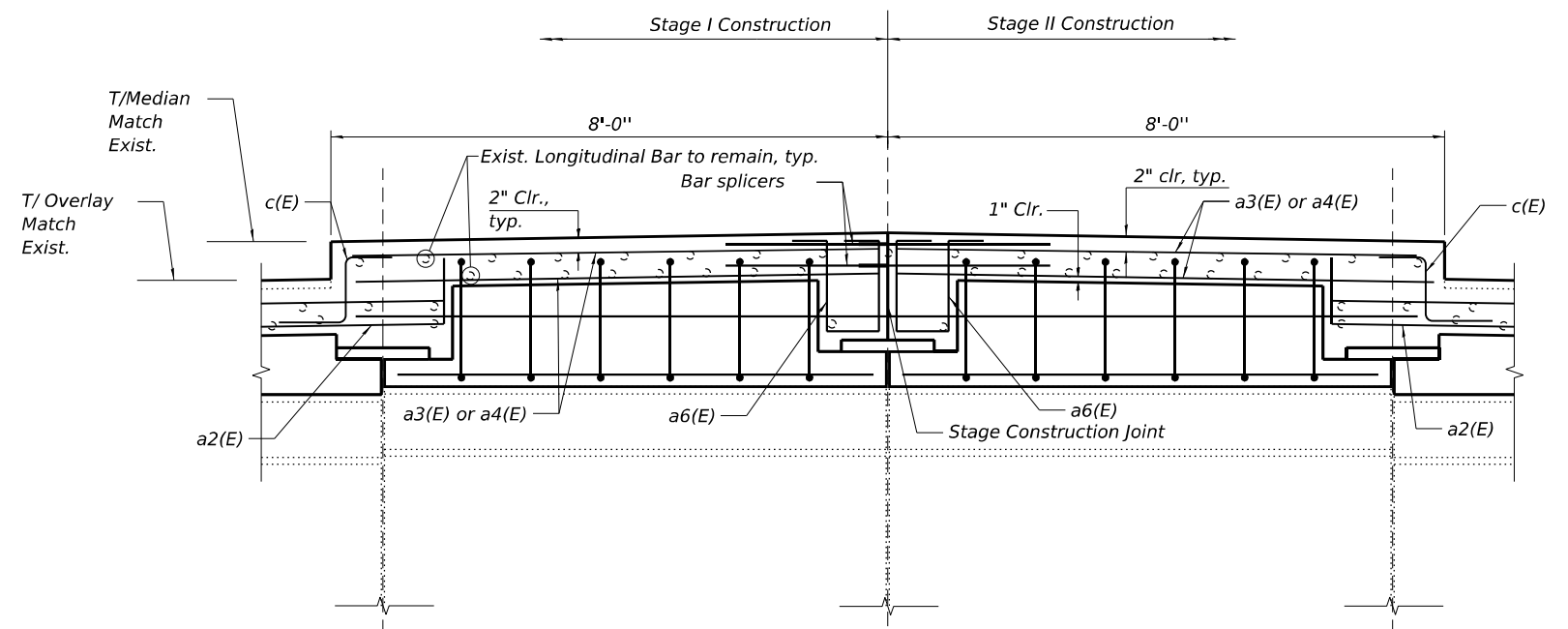
SECTION C-C



SECTION D-D



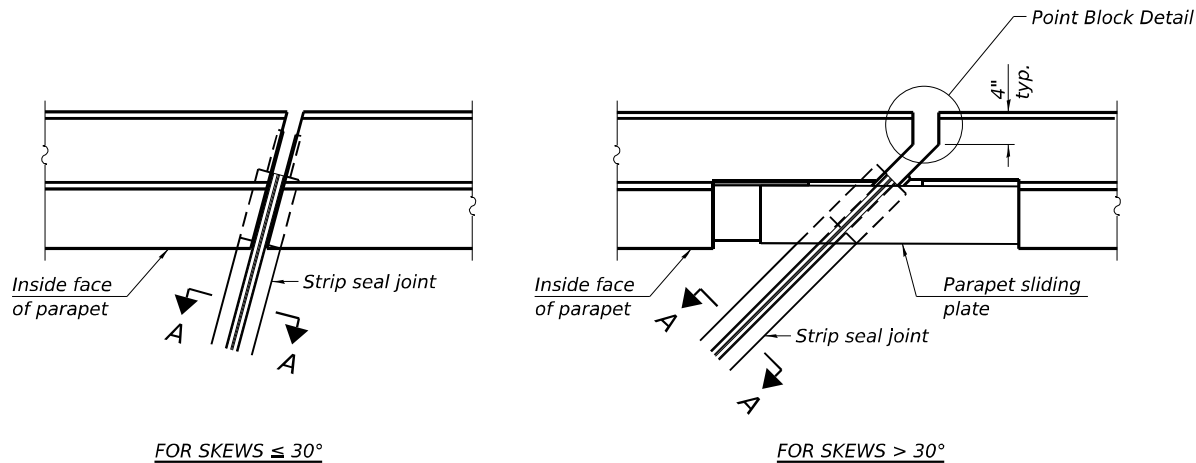
EXISTING SECTION THRU MEDIAN



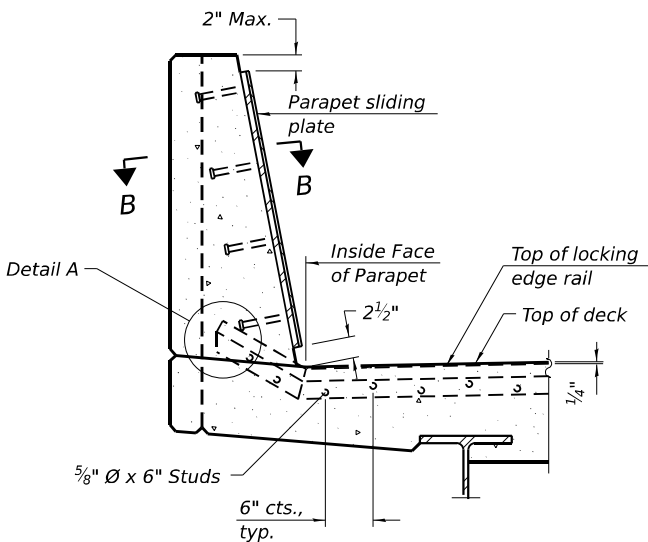
PROPOSED SECTION THRU MEDIAN

NOTE:
See Sheet 6 of 14 for Bar Bending Details and Bill of Material.

MODEL: Default
FILE NAME: F:\2097-TR01-2024-P1_PTB 213-0130 Various Bridge Rehab\WO 02 - 64525 Baxter Rd over I-39\05 CADD\Structure\Sheets\264525-sh1-101-0145-09-Pre\J15StripSeal.dgn
4/30/2025 3:56:01 PM

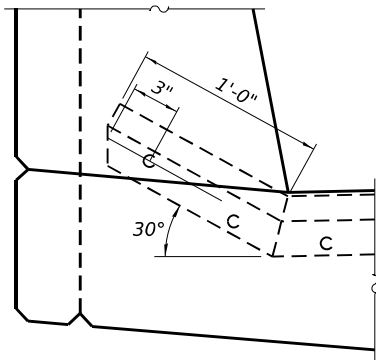


PLAN AT PARAPET

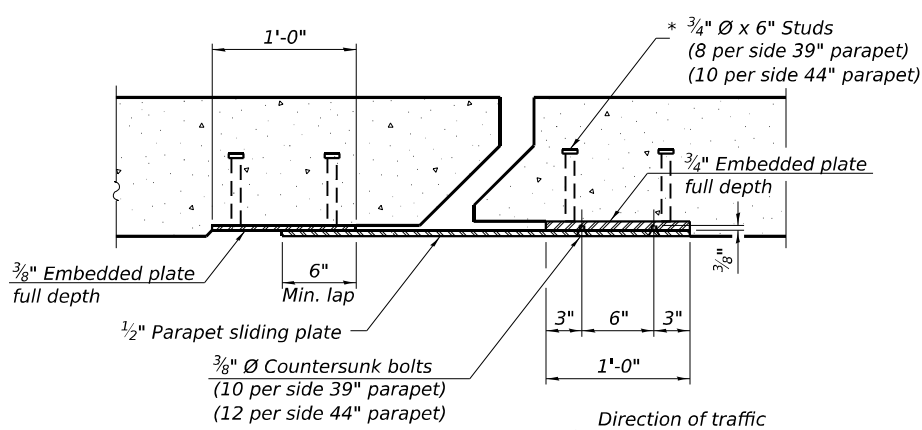


SECTION AT PARAPET

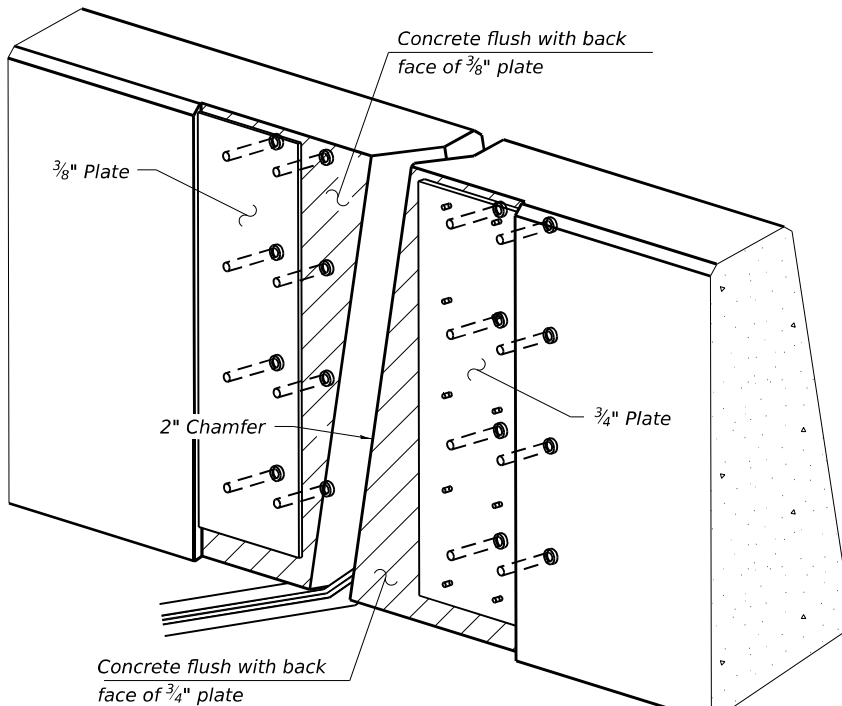
(Skews $> 30^\circ$ shown. Skews $\leq 30^\circ$ similar except as shown in plan view.)



DETAIL A

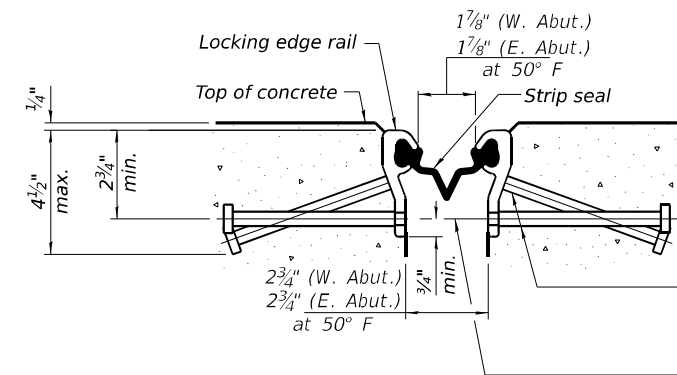


SECTION B-B



TRIMETRIC VIEW

(Showing embedded plates only)



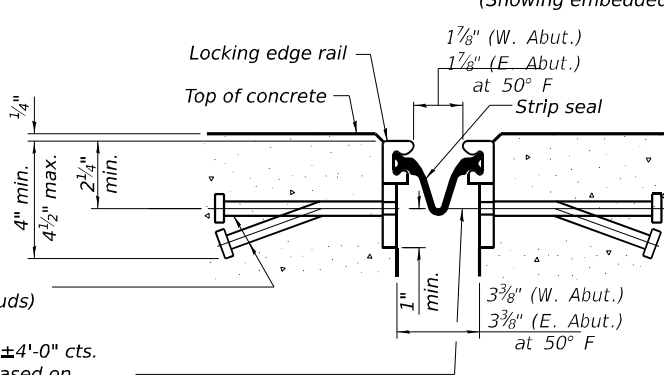
SHOWING ROLLED RAIL JOINT

* 5/8" \varnothing x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

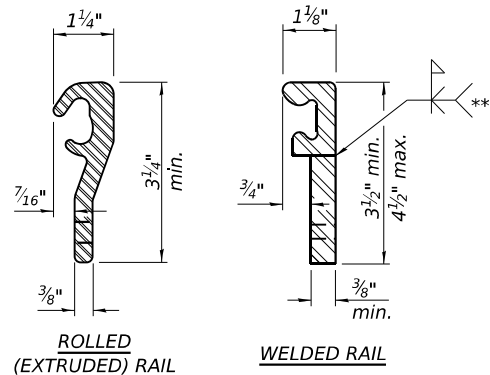
3/8" \varnothing threaded rods in 7/16" \varnothing holes at $\pm 4'-0"$ cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

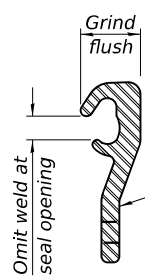


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	125

EJ-SS-S

5-15-2023

(Sheet 1 of 3)



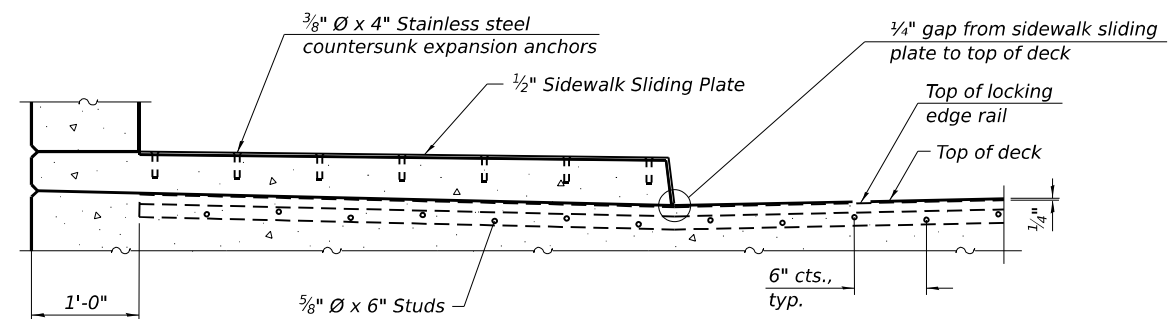
USER NAME =	DESIGNED - RO	REVISED -
PLOT SCALE =	CHECKED - EG	REVISED -
PLOT DATE =	DRAWN - RO	REVISED -
	CHECKED - EG	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

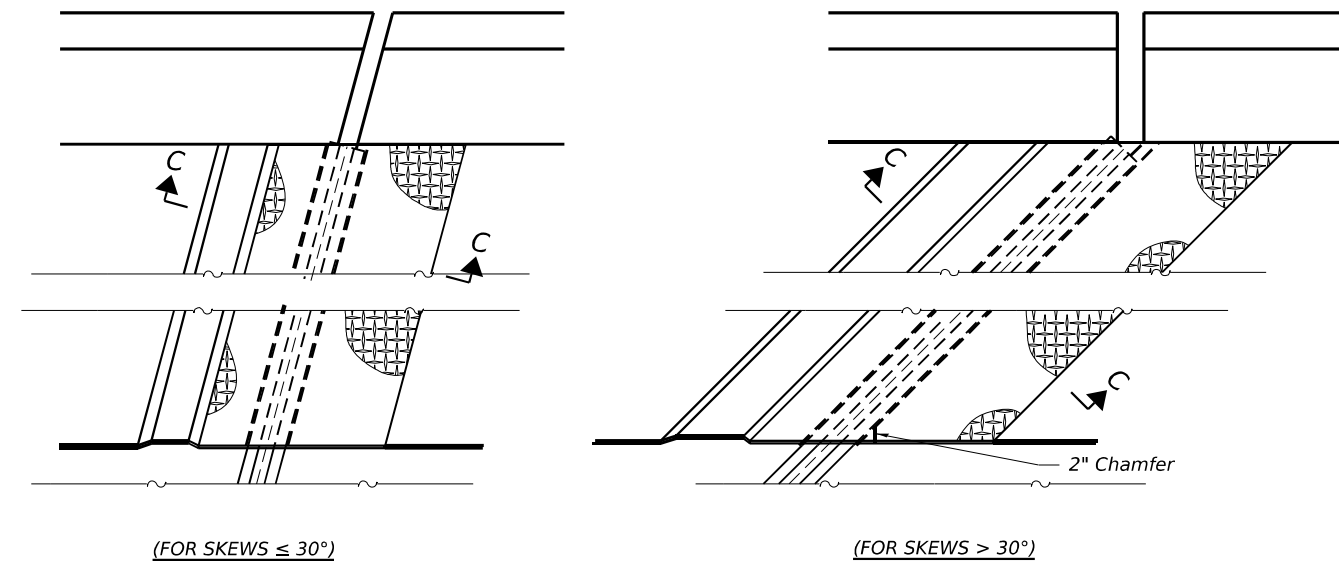
PREFORMED JOINT STRIP SEAL - SIDEWALK
STRUCTURE NO. 101-0145

SHEET 9 OF 14 SHEETS

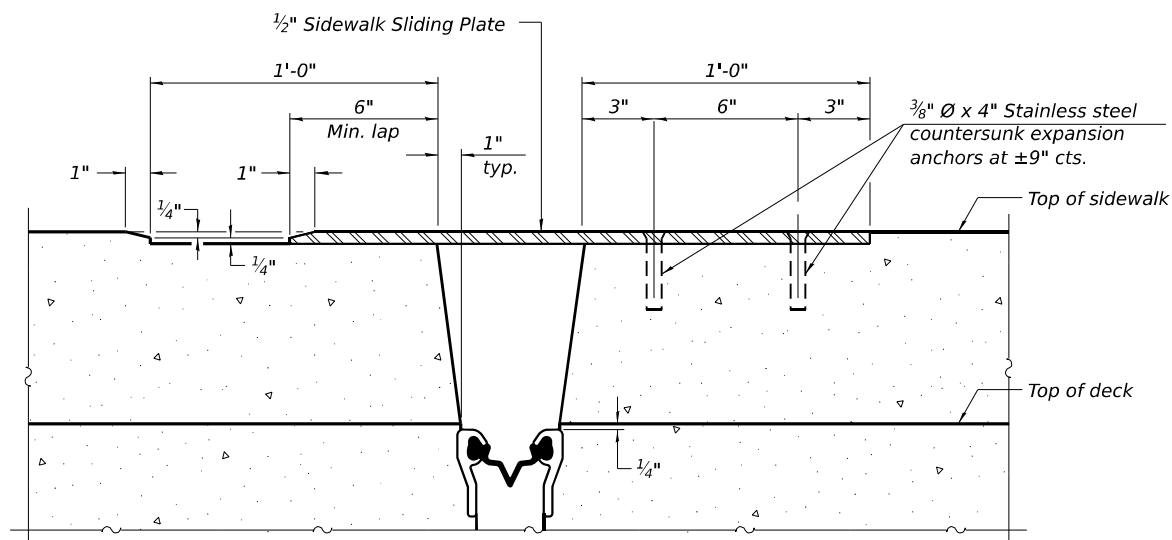
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	24
CONTRACT NO. 64S25				
ILLINOIS FED. AID PROJECT				



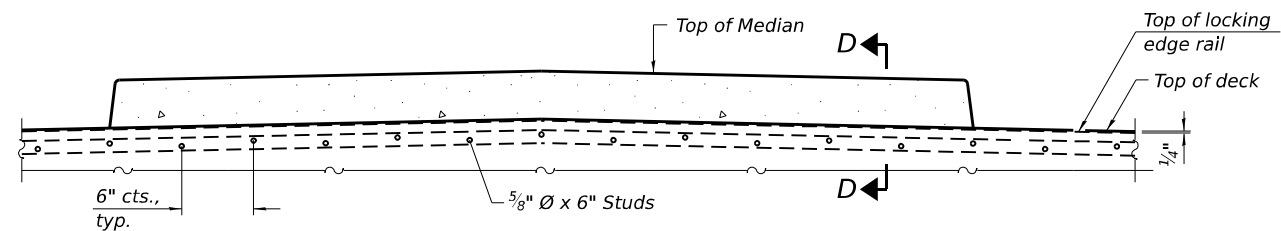
SECTION AT RAISED SIDEWALK



PLAN AT RAISED SIDEWALK

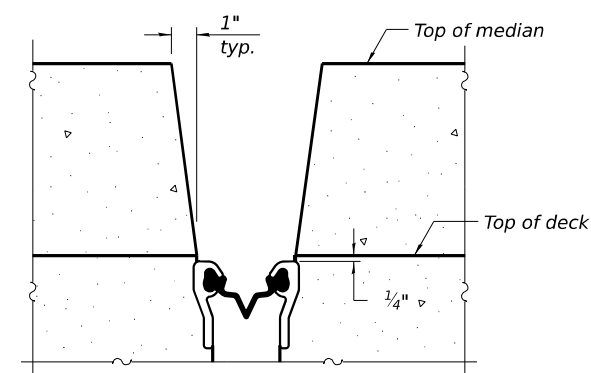


SECTION C-C

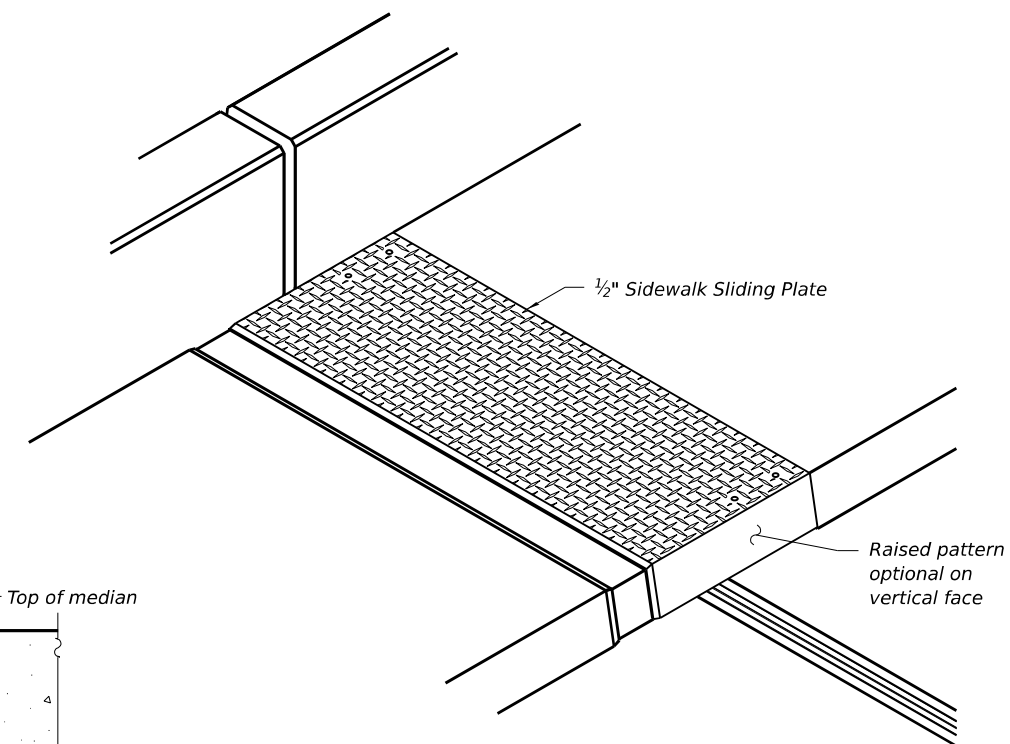


SECTION AT MEDIAN

For skews $> 30^\circ$, chamfer acute corners 2" similar to sidewalk.



SECTION D-D
(at Rt. \angle 's)

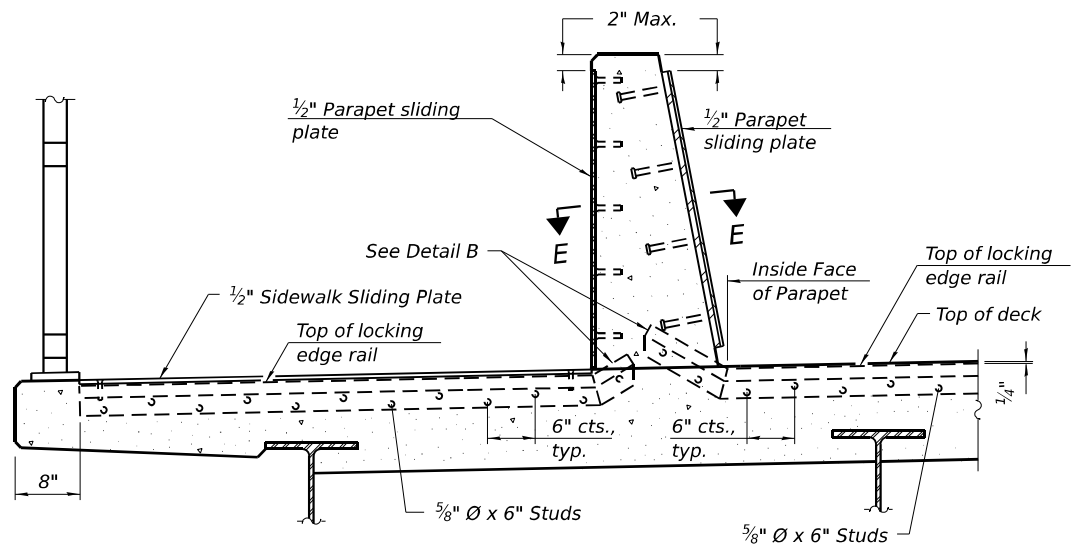


TRIMETRIC VIEW

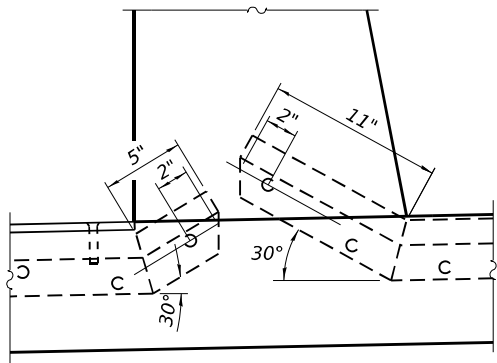
EJ-SS-S

5-15-2023

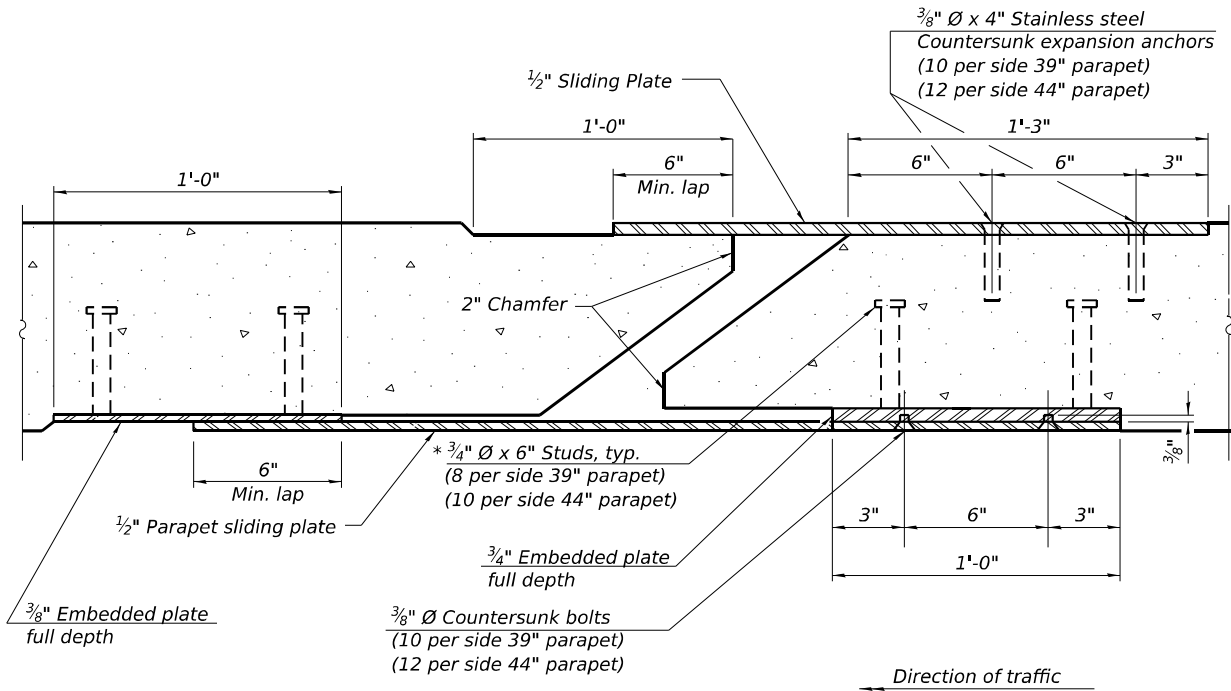
(Sheet 2 of 3)



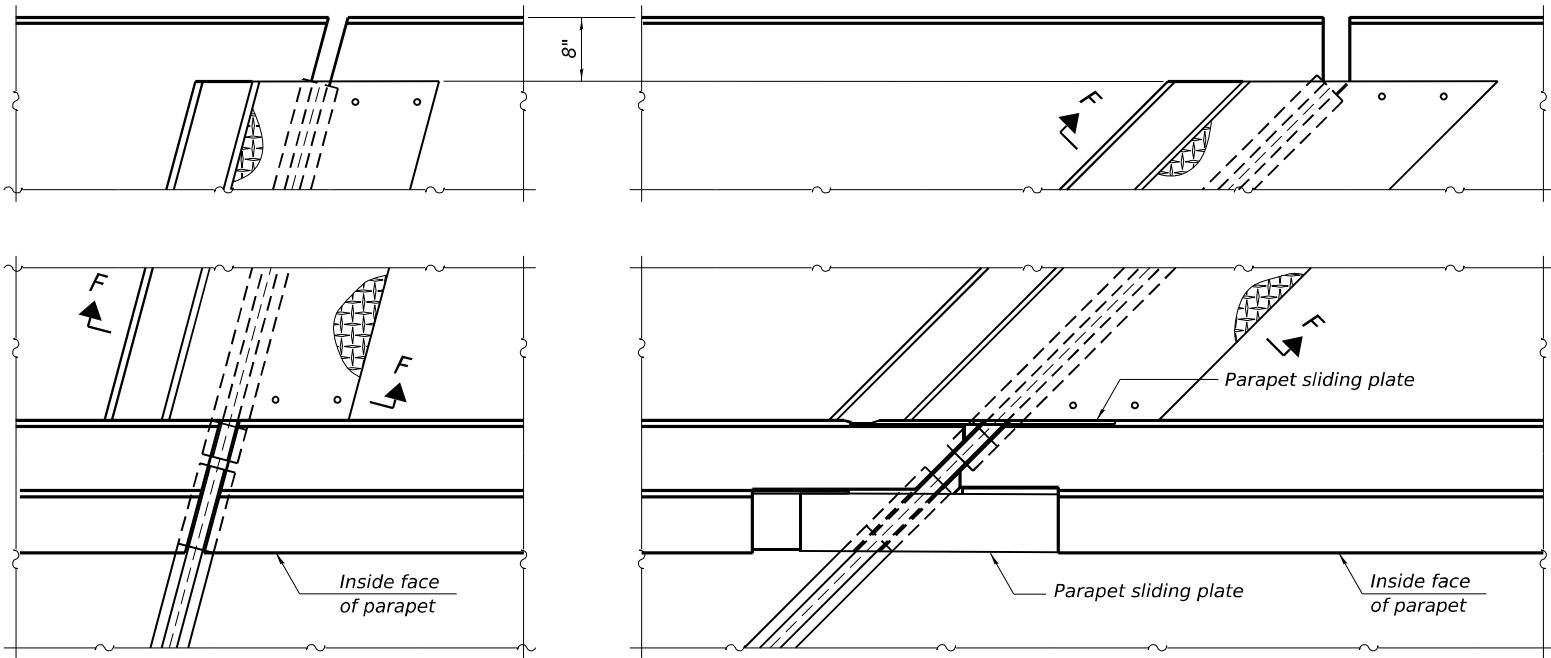
SECTION AT DECK LEVEL SIDEWALK
(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)



DETAIL B



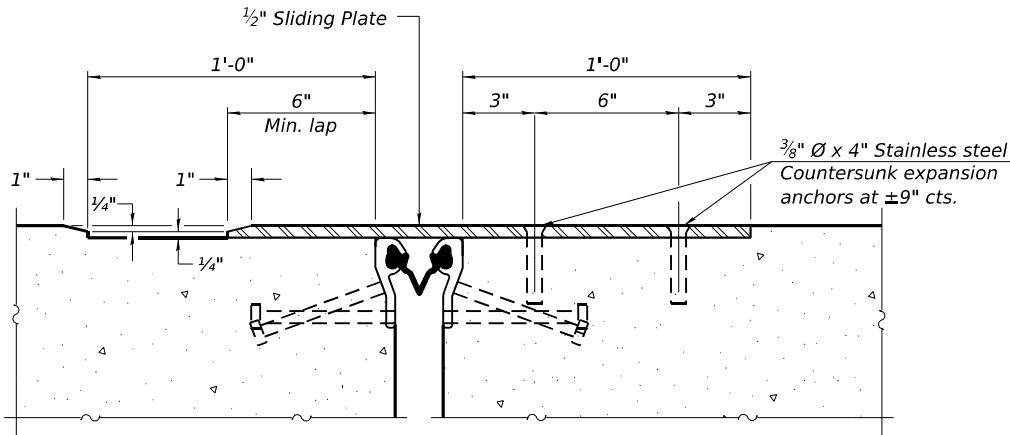
SECTION E-E



(FOR SKEWS ≤ 30°)

(FOR SKEWS > 30°)

PLAN AT DECK LEVEL SIDEWALK



SECTION F-F

(Sheet 3 of 3)

MODEL: Default
FILE NAME: F:\2097-TR01-2024-P1_PTB 213-030 Various Bridge Rehab\WO 02 - 64525 Baxter Rd over I-39\05 CADD\Structure\Sheets\264525-shi-101-0145-11-Pre\11StripSeal.dgn.dgn

EJ-SS-S

5-15-2023



USER NAME =	DESIGNED - RO	REVISED -
CHECKED - EG	REVISED -	
PLOT SCALE =	DRAWN - RO	REVISED -
PLOT DATE =	CHECKED - EG	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

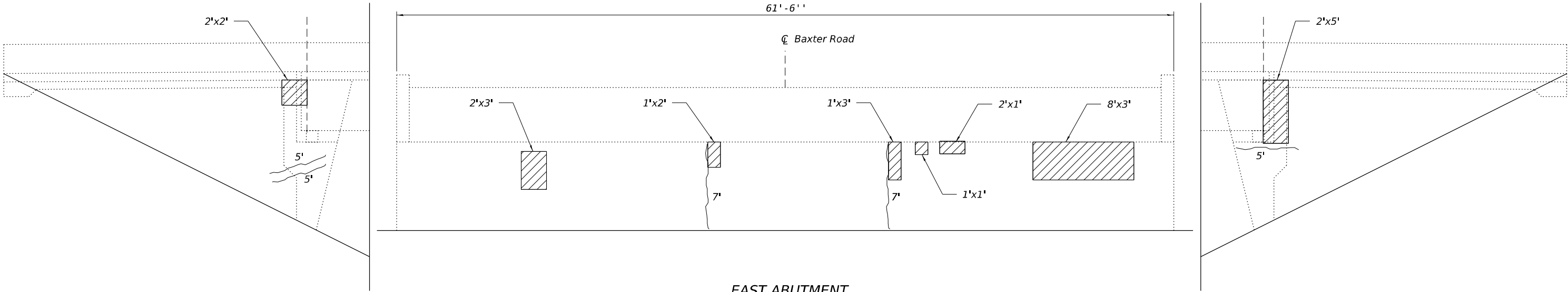
**PREFORMED JOINT STRIP SEAL - SIDEWALK
STRUCTURE NO. 101-0145**

SHEET 11 OF 14 SHEETS

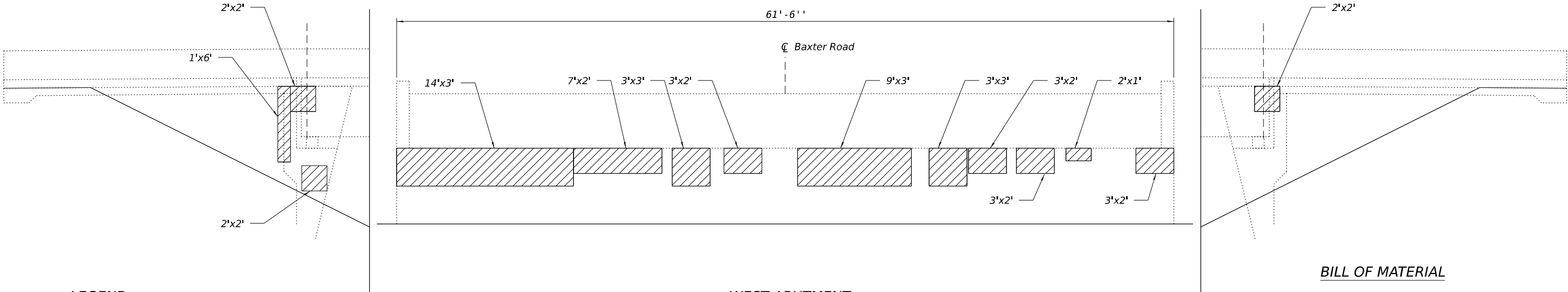
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	26
CONTRACT NO. 64S25				
ILLINOIS FED. AID PROJECT				

4/30/2025 3:56:04 PM

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EAST ABUTMENT



WEST ABUTMENT

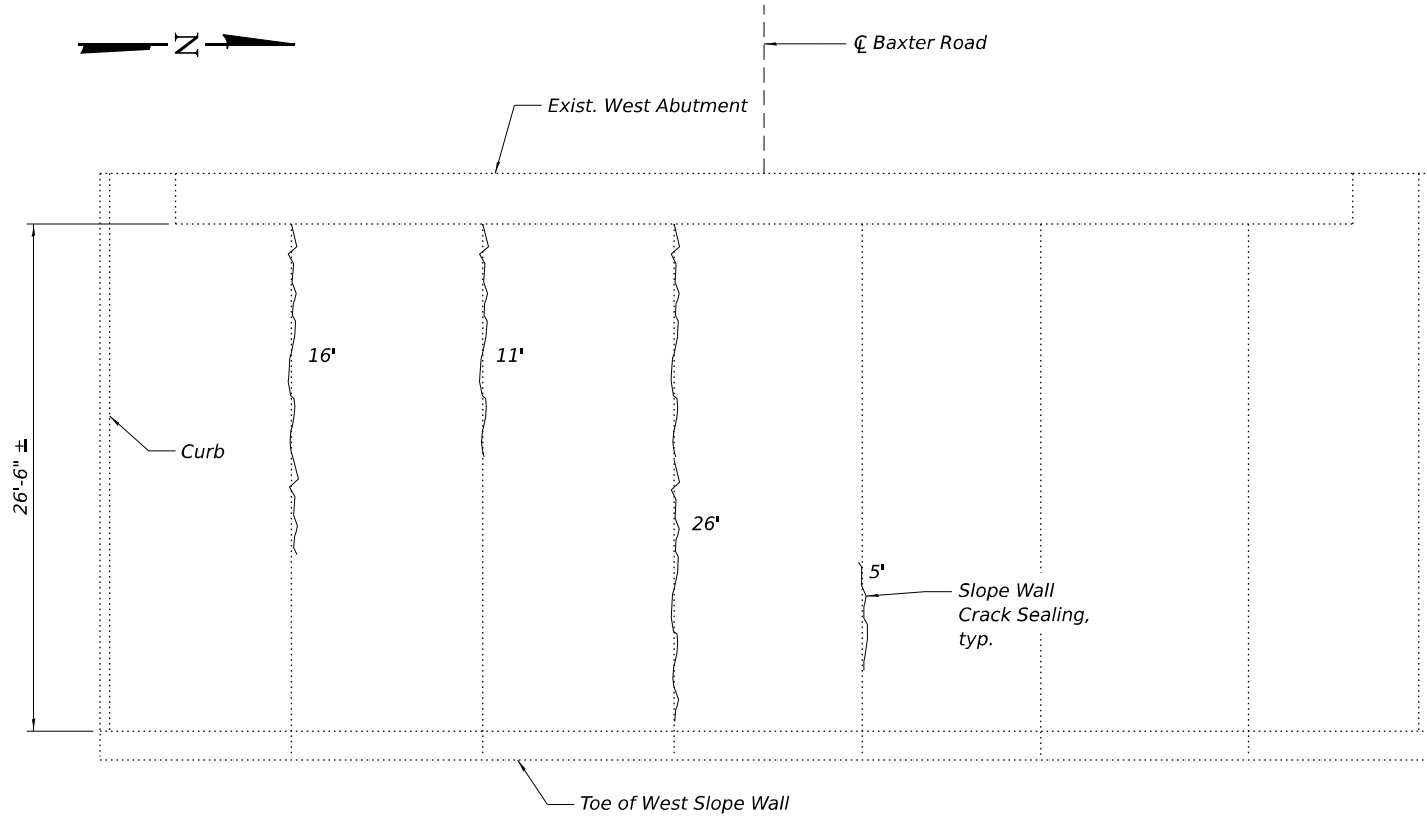
LEGEND

- Structural Repair of Concrete (Depth \leq 5")
- Epoxy Crack Injection

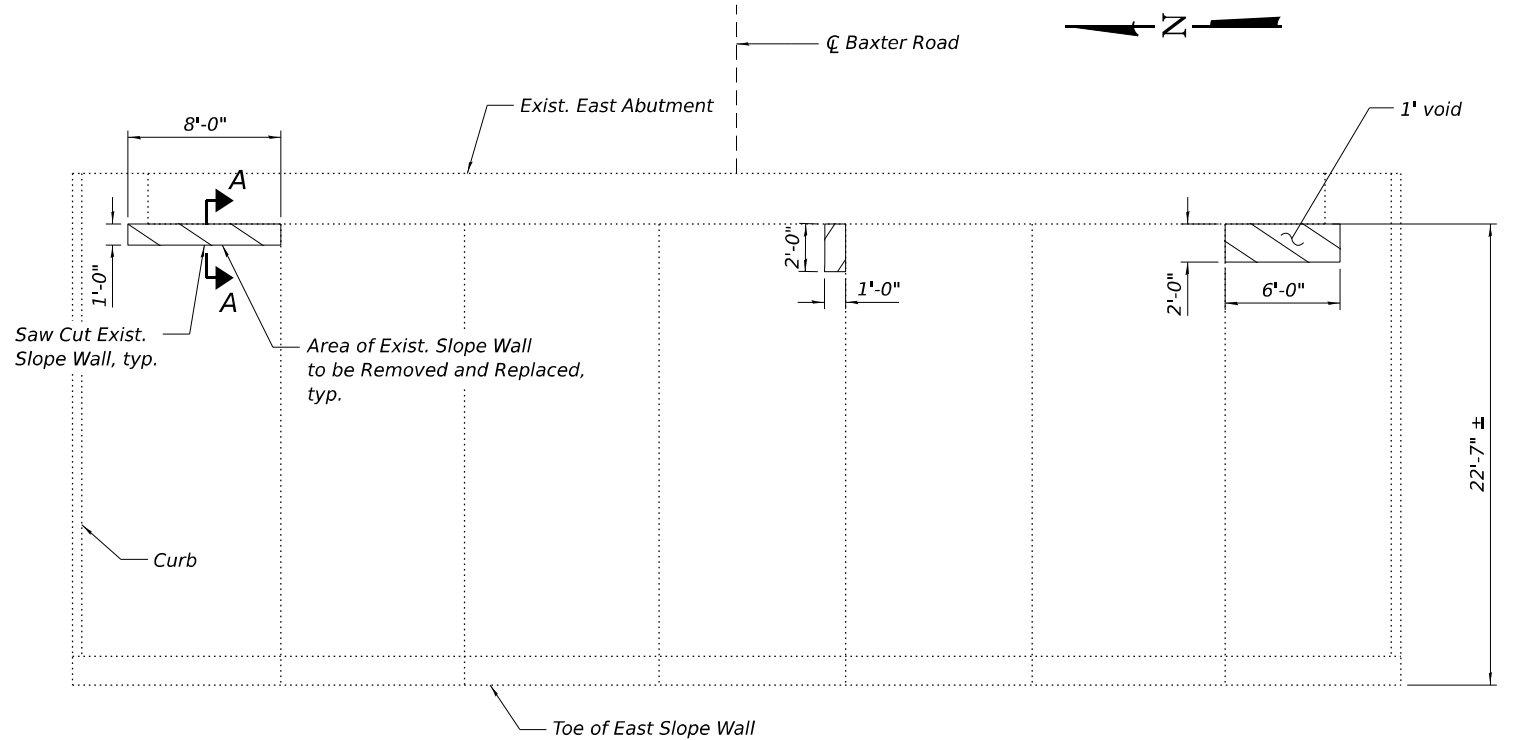
BILL OF MATERIAL

Item	Unit	Total
Concrete Sealer	Sq. Ft.	165
Epoxy Crack Injection	Foot	29
Structural Repair of Concrete (Depth equal to or less 5 inches)	Sq. Ft.	197

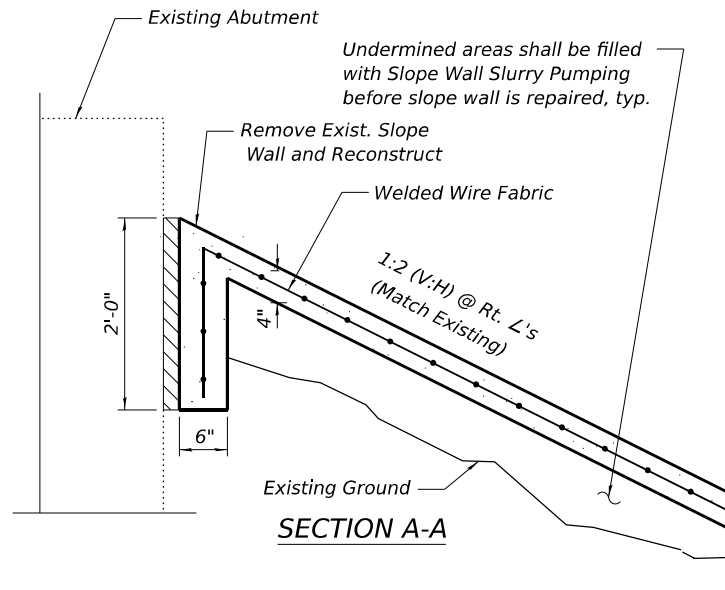
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FILE NAME: F:\2097-TR01-2024-PTB 213-030 Various Bridge Rehab\WO 02 - 64525 Baxter Rd over I-39\05 CADD\Structure\Sheets\264525-shl-101-0145-13-SlopeWallRepairDet.dgn



WEST SLOPE WALL REPAIR PLAN



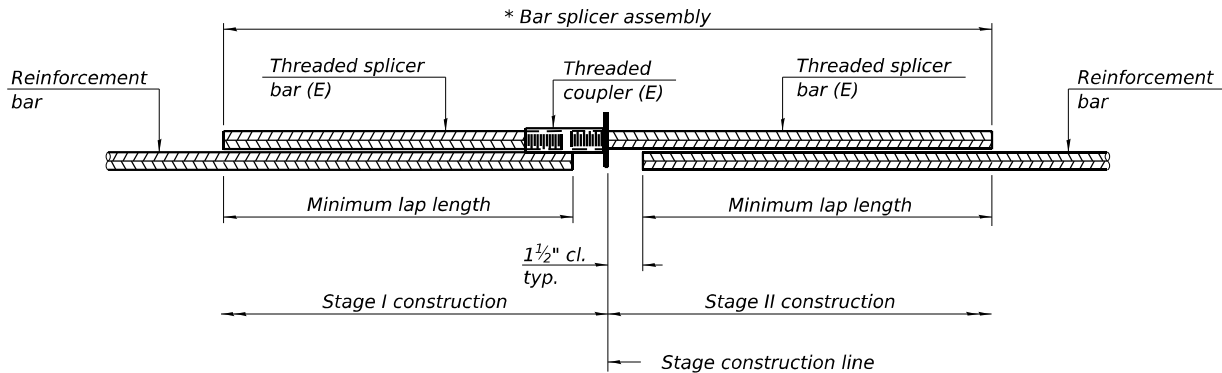
EAST SLOPE WALL REPAIR PLAN



BILL OF MATERIAL

ITEM	UNIT	TOTAL
Slope Wall Crack Sealing	Foot	58
Slope Wall Repair	Sq. Yd.	2
Slope Wall Slurry Pumping	Cu. Yd.	1.3

MODEL: Default
FILE NAME: F:\2097-TR01-2024-P1_PTB 213-030 Various Bridge Rehab\WO 02 - 64525 Baxter Rd over I-39\05 CADD\Structure\Sheets\264525-sh1-101-0145-14-Bar SplicerDetails.dgn-dgn



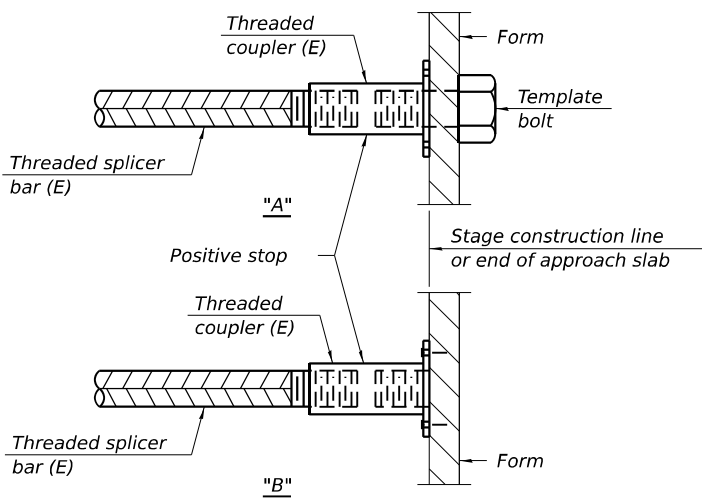
STANDARD BAR SPLICER ASSEMBLY PLAN

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + 1½" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
W. Abut. Exp. Jt.	#5	2	3'-0"
W. Abut. Exp. Jt.	#6	6	3'-7"
E. Abut. Exp. Jt.	#5	2	3'-0"
E. Abut. Exp. Jt.	#6	6	3'-7"

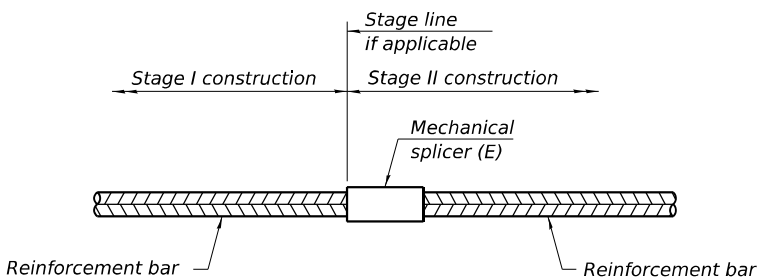


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

5-15-2023



USER NAME =	DESIGNED - RO	REVISED -
	CHECKED - EG	REVISED -
PLOT SCALE =	DRAWN - RO	REVISED -
PLOT DATE =	CHECKED - EG	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

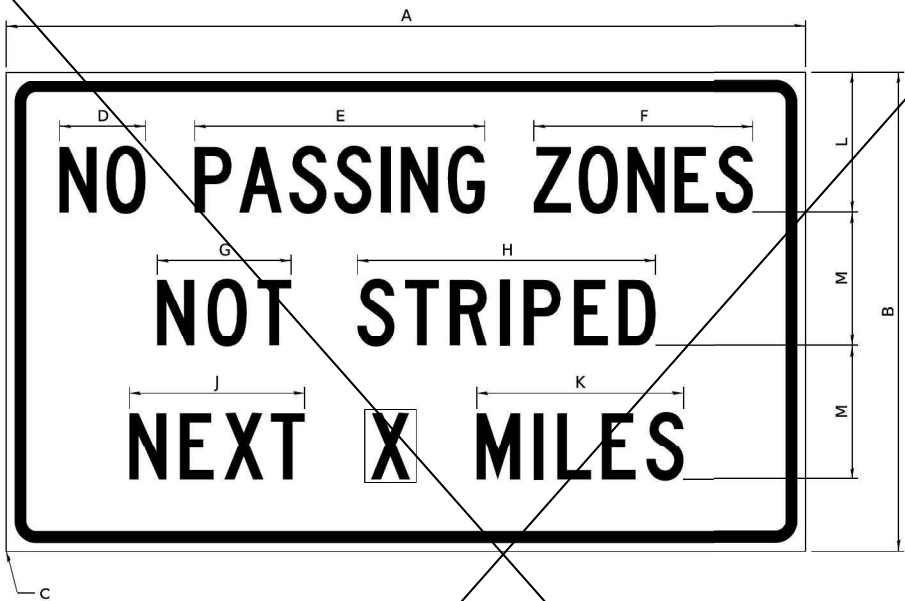
BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 101-0145

SHEET 14 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	29
CONTRACT NO. 64S25				
ILLINOIS FED. AID PROJECT				

WORK ZONE SIGN DETAILS

ILLINOIS STANDARD G20-I100



SIGN SIZE	DIMENSIONS											
	A	B	C	D	E	F	G	H	J	K	L	M
60 x 36	60.00	36.00	2.25	6.4	21.80	16.40	10.00	22.40	13.20	15.50	10.50	10.00

SIGN SIZE	SERIES BY LINE			MARGIN	BORDER
	1	2	3		
60 x 36	5C	5C	5C	0.625	0.875

Sign not to scale

GENERAL NOTES

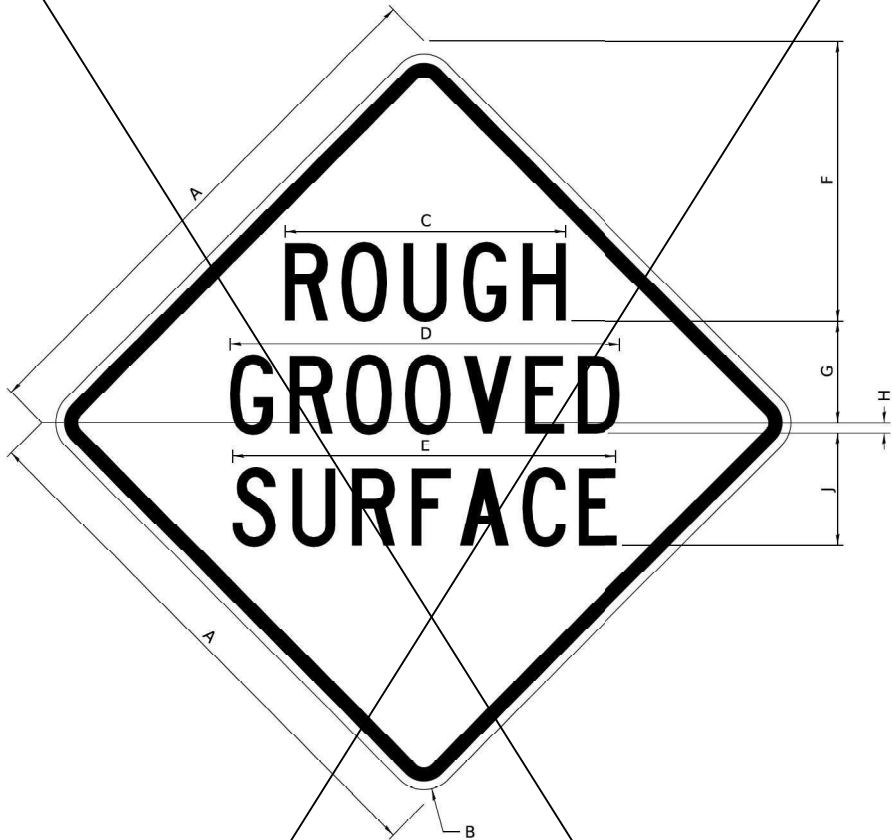
All work to furnish and install these signs shall be included in the cost of the specified traffic control standards and shall not be paid separately.

All Illinois Standard signs shall conform to the latest edition of the "Illinois Standard Highway Signs Book" in effect on the date of invitation for bids.

Signs shall meet the applicable portions of Sections 701 and 720 of the Standard Specifications.

All dimensions are in inches unless otherwise noted.

ILLINOIS STANDARD W8-I107



COLOR	LEGEND AND BORDER BACKGROUND				BLACK ORANGE	NON-REFLECTORIZED REFLECTORIZED			
SIGN SIZE	DIMENSIONS								
	A	B	C	D	E	F	G	H	J
48 x 48	48.00	3.00	25.00	34.80	34.20	24.94	9.00	1.00	10.00

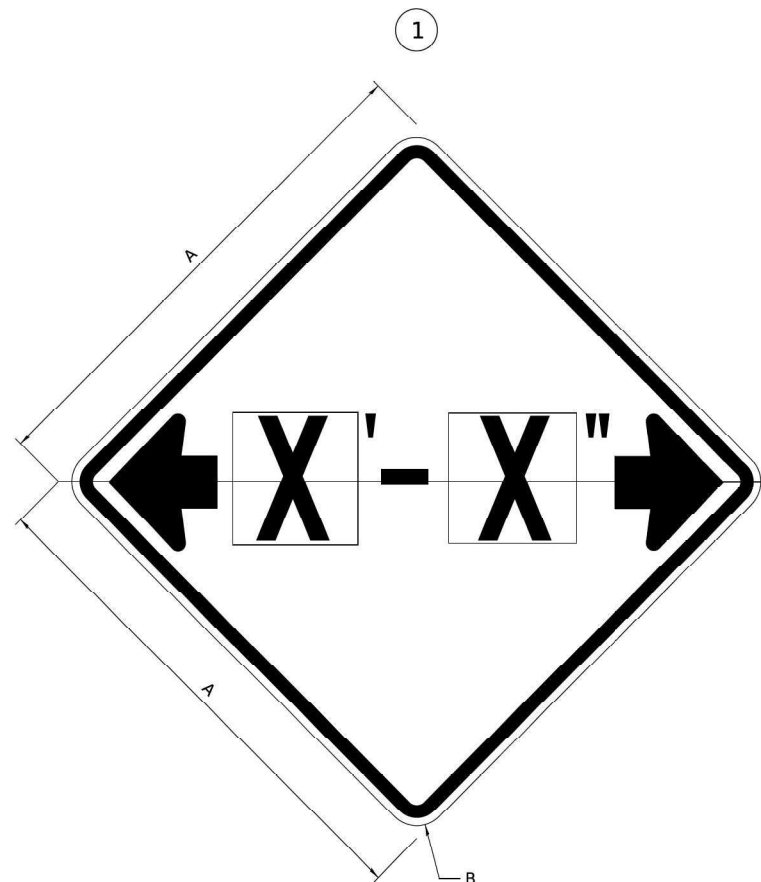
SIGN SIZE	SERIES BY LINE			MARGIN	BORDER
	1	2	3		
48 x 48	7C	7C	7C	1.250	0.750

Sign not to scale

MODEL: Default
FILE NAME: F:\2007-TR01-2024-P1_PTB 213-030 Various Bridge Rehab\W0 02 - 64S25 Baxter Rd over I-39\05 CADD\Civil\Model\ID264S25-border.dgn

WORK ZONE SIGN DETAILS

ILLINOIS STANDARD W12-I102



GENERAL NOTES

All work to furnish and install these signs shall be included in the cost of the specified traffic control standards and shall not be paid separately.

All Illinois Standard signs shall conform to the latest edition of the "Illinois Standard Highway Signs Book" in effect on the date of invitation for bids.

Signs shall meet the applicable portions of Sections 701 and 720 of the Standard Specifications.

All dimensions are in inches unless otherwise noted.

COLOR LEGEND AND BORDER BACKGROUND BLACK FL ORANGE NON-REFLECTORIZED REFLECTORIZED

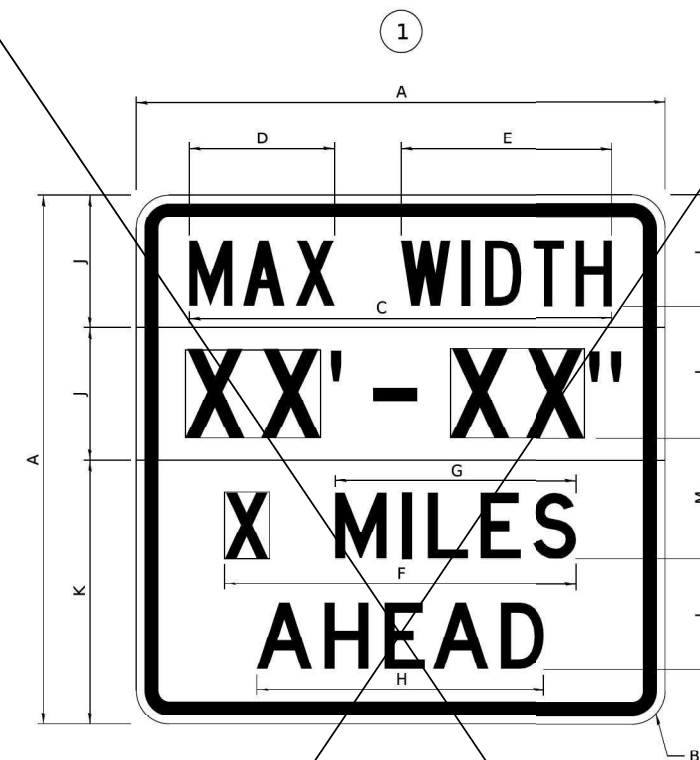
SIGN SIZE	DIMENSIONS	
	A	B
48 x 48	48.00	3.00

① Illinois Standard signs W12-I102 and W12-I103 shall be used as described in the special provisions.

SIGN SIZE	SERIES BY LINE	MARGIN	BORDER
	1		
48 x 48	12C	0.750	1.250

Sign not to scale

ILLINOIS STANDARD W12-I103



COLOR LEGEND AND BORDER BACKGROUND (WIDTH) BLACK WHITE FL ORANGE NON-REFLECTORIZED REFLECTORIZED REFLECTORIZED

SIGN SIZE	DIMENSIONS											
	A	B	C	D	E	F	G	H	J	K	L	M
48 x 48	48.00	3.00	38.40	13.20	19.20	32.00	22.00	26.20	12.00	24.00	10.00	11.00

SIGN SIZE	SERIES BY LINE				MARGIN	BORDER
	1	2	3	4		
48 x 48	6C	8D	6D	6D	0.750	1.250

Sign not to scale

XX'-XX" WIDTH AND X MILES ARE VARIABLE TOP AND BOTTOM OF BACKGROUND WHITE

SHEET 2 OF 4 34.1

MODEL: Default
FILE NAME: F:\2007-TR01-2024-P1_PTB 213-030 Various Bridge Rehab\WO 02 - 64S25 Baxter Rd over I-39\05 CADD\Civil\Model\ID264S25-border.dgn



USER NAME = Amalia.Baymundo	DESIGNED - AMB	REVISED -
	DRAWN - AMB	REVISED -
	CHECKED - PK	REVISED -
PLOT DATE = 4/29/2025	DATE - 4/29/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

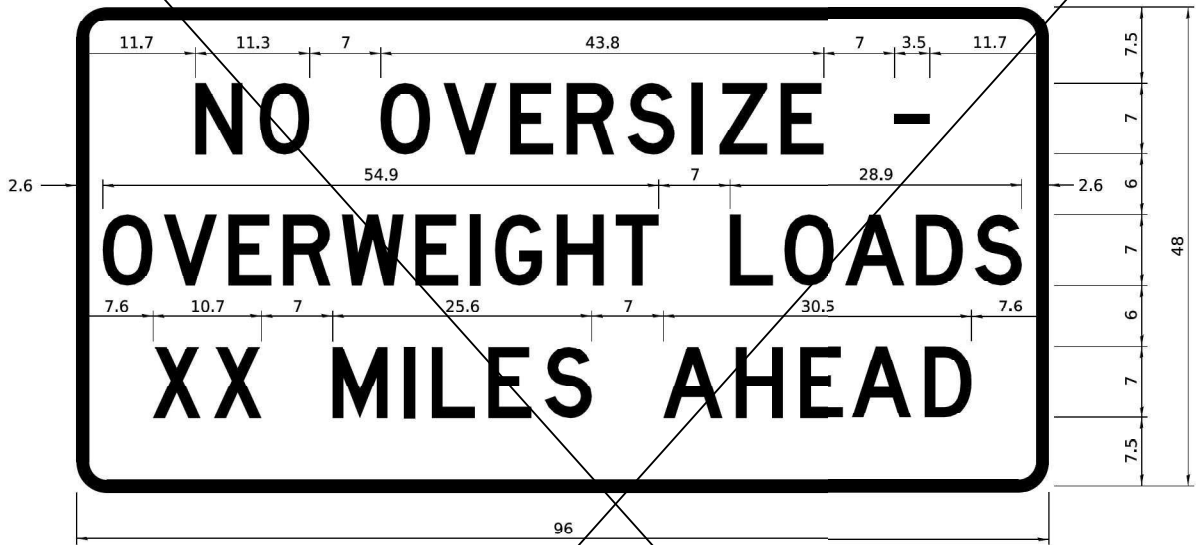
BAXTER RD OVER I-39
REGION 2/DISTRICT 2 STANDARDS

SCALE: SHEET 2 OF 9 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	31
CONTRACT NO. 64S25				
ILLINOIS FED. AID PROJECT				

WORK ZONE SIGN DETAILS

ROAD CLOSED TO OVERSIZED LOADS



COLOR	LEGEND AND BORDER BACKGROUND	BLACK ORANGE	NON-REFLECTORIZED REFLECTORIZED
-------	------------------------------	--------------	---------------------------------

Permit Loads - Loads Over 13 Feet; 3,0" Radius, 1,3" Border;
[NO OVERSIZE -] D; [OVERWEIGHT LOADS] D 85% spacing; [XX MILES AHEAD] D;
Table of letter and object lefts.

N	O	O	V	E	R	S	I	Z	E	-
11.7	18.1	30.0	36.2	42.8	48.4	54.4	60.7	63.5	69.5	80.8

O	V	E	R	W	E	I	G	H	T	L	O	A	D	S
2.6	8.6	15.0	20.4	26.2	33.4	38.8	41.3	47.4	53.2	64.5	69.9	75.9	82.9	88.7

X	X	M	I	L	E	S	A	H	E	A	D
7.6	13.6	25.3	32.3	35.1	40.6	46.2	57.9	65.1	71.4	76.6	83.7

Sign not to scale

STOP LINE SIGN FOR TEMPORARY SIGNALS



COLOR	LEGEND AND BORDER BACKGROUND	BLACK WHITE	NON-REFLECTORIZED REFLECTORIZED
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SIGN SIZE	SERIES BY LINE		
	1	2	3
24 x 24	4C	4C	4C

Sign not to scale

GENERAL NOTES

All work to furnish and install these signs shall be included in the cost of the specified traffic control standards and shall not be paid separately.

All Illinois Standard signs shall conform to the latest edition of the "Illinois Standard Highway Signs Book" in effect on the date of invitation for bids.

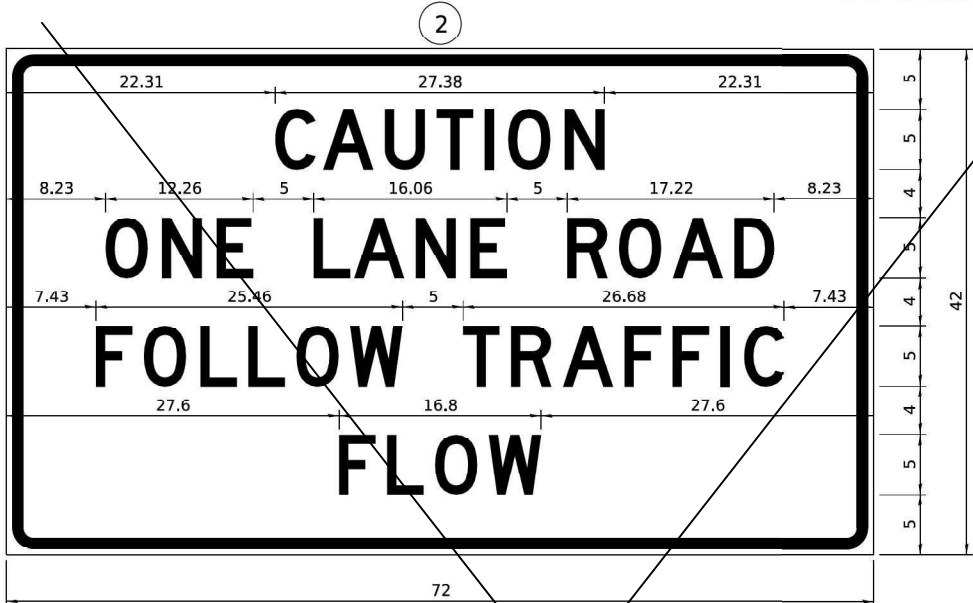
Signs shall meet the applicable portions of Sections 701 and 720 of the Standard Specifications.

All dimensions are in inches unless otherwise noted.

MODEL: Default
FILE NAME: F:\2007-TR01-2024-P1_PTB 213-030 Various Bridge Rehab\WO 02 - 64S25 Baxter Rd over I-39\05 CADD\Civil\Modals\ID264S25-border.dgn

ENTRANCE SIGN FOR USE
WITH TEMPORARY SIGNALS

WORK ZONE SIGN DETAILS



COLOR LEGEND AND BORDER
BACKGROUND BLACK ORANGE NON-REFLECTORIZED
REFLECTORIZED

2.25" Radius, 0.88" Border, 0.50" Indent;
[CAUTION] D; [ONE LANE ROAD] D;
[FOLLOW TRAFFIC] D; [FLOW] D

2 This sign shall be installed at entrances located between
the temporary signals as shown in the staging plans.

Table Of Widths And Spaces

22.31	C	3.36	0.62	A	4.18	0.94	U	3.36	0.94	T	3.04	0.94	I	0.78	1.17	O	3.52	1.17	N	3.36	22.31
8.23	O	3.51	1.17	N	3.36	1.18	E	3.04													
5.00	L	3.05	0.31	A	4.18	0.94	N	3.36	1.17	E	3.05										
5.00	R	3.36	0.93	O	3.52	0.94	A	4.18	0.93	D	3.36	8.23									
7.43	F	3.04	0.94	O	3.52	1.17	L	3.04	0.94	L	3.05	0.94	O	3.51	0.94	W	4.37				
5.00	T	3.05	0.94	R	3.36	0.94	A	4.18	0.93	F	3.05	0.94	F	3.04	0.94	I	0.78	1.18	C	3.35	7.43
27.60	F	3.05	0.94	L	3.04	0.94	O	3.52	0.93	W	4.38	27.60									

Sign not to scale

GENERAL NOTES

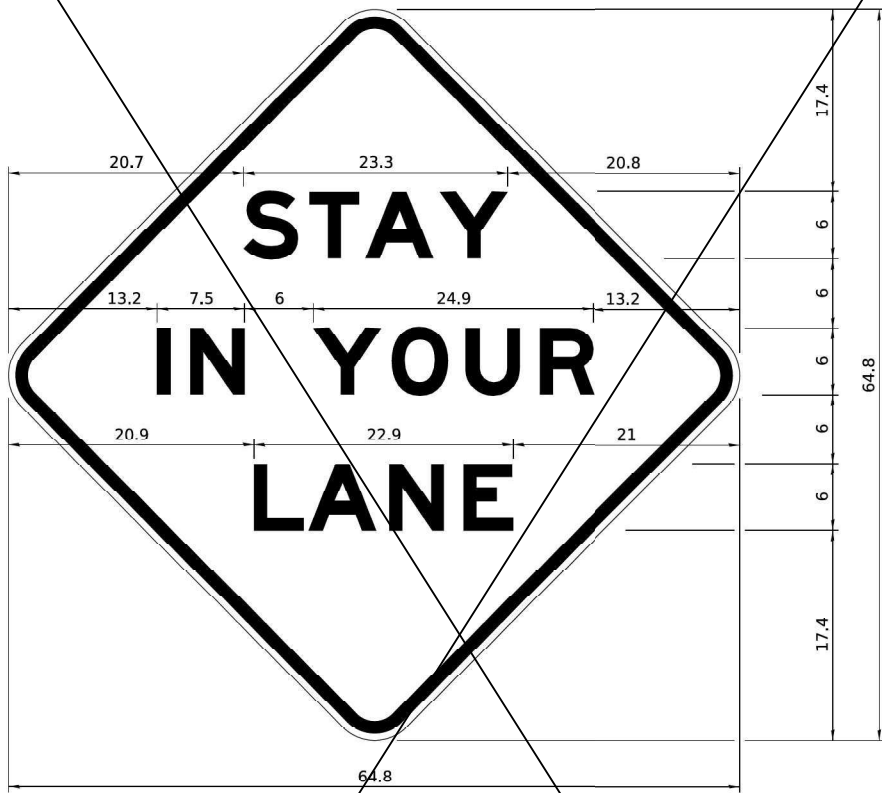
All work to furnish and install these signs shall be included in the cost of the specified traffic control standards and shall not be paid separately.

All Illinois Standard signs shall conform to the latest edition of the "Illinois Standard Highway Signs Book" in effect on the date of invitation for bids.

Signs shall meet the applicable portions of Sections 701 and 720 of the Standard Specifications.

All dimensions are in inches unless otherwise noted.

STAY IN YOUR LANE



COLOR LEGEND AND BORDER
BACKGROUND BLACK ORANGE NON-REFLECTORIZED
REFLECTORIZED

48.0" across sides 3.8" Radius, 1.0" Border, 0.6" Indent;
"STAY" E Mod; "IN YOUR" E Mod; "LANE" E Mod;

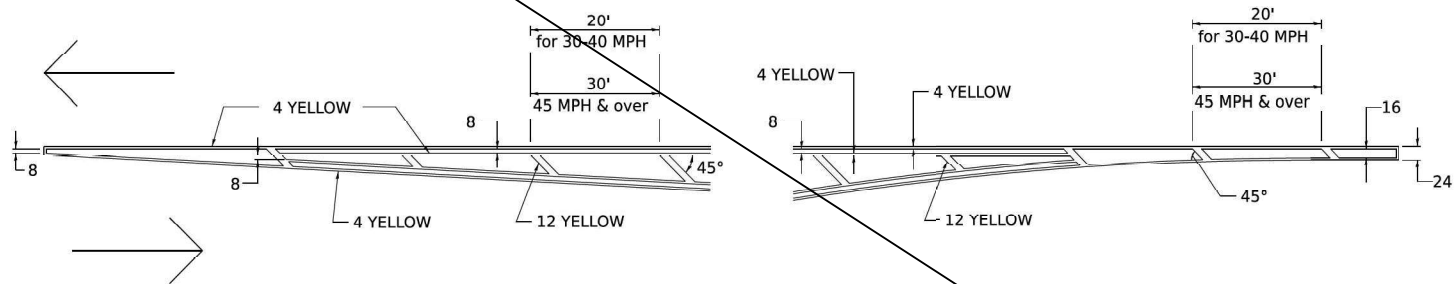
Table of Letter and Object Lefts

S	T	A	Y		
20.7	26.8	31.6	38.0		
I	N	Y	O	U	R
13.2	15.9	26.7	33.9	40.5	46.8
L	A	N	E		
20.9	25.8	33.1	39.4		

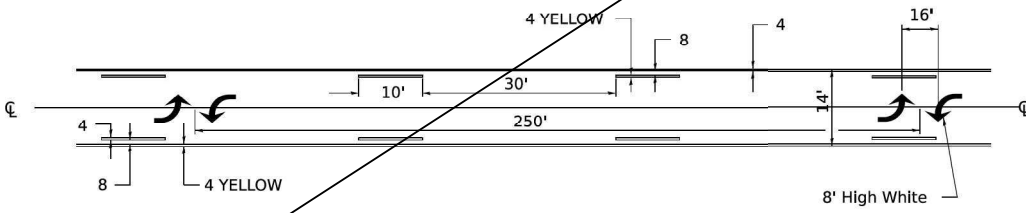
Sign not to scale

TYPICAL PAVEMENT MARKINGS

TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN AT LEFT TURN LANE

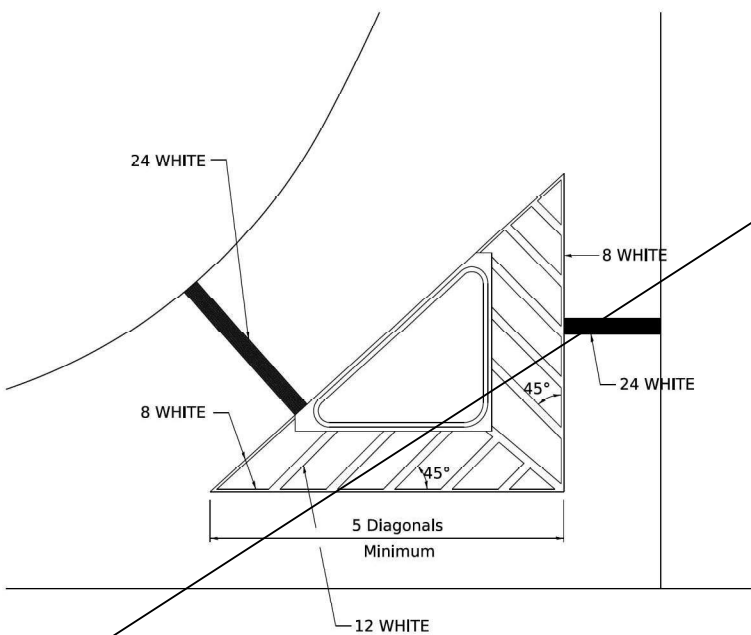


MEDIAN PAVEMENT MARKING



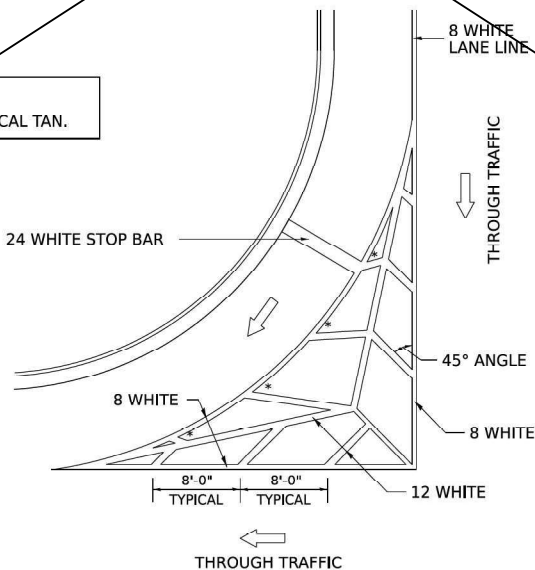
** ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

TYPICAL ISLAND OFFSET SHOULDER WIDTH



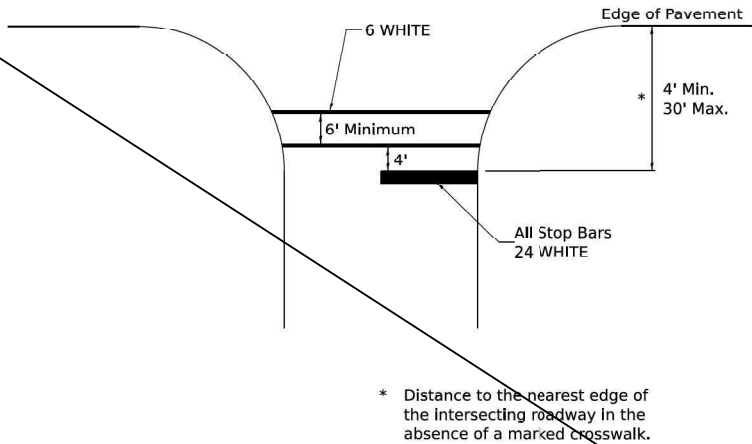
TYPICAL MARKING FOR PAINTED ISLANDS

NOTE:
* 45° TO LOCAL TAN.



STANDARD CROSSWALK MARKING

See Schedules for Locations

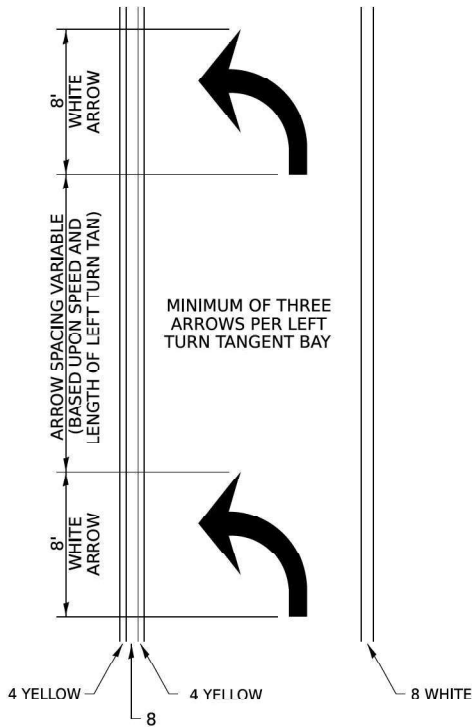


* Distance to the nearest edge of the intersecting roadway in the absence of a marked crosswalk.

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TYPICAL PAVEMENT MARKINGS

ARROW LAYOUT

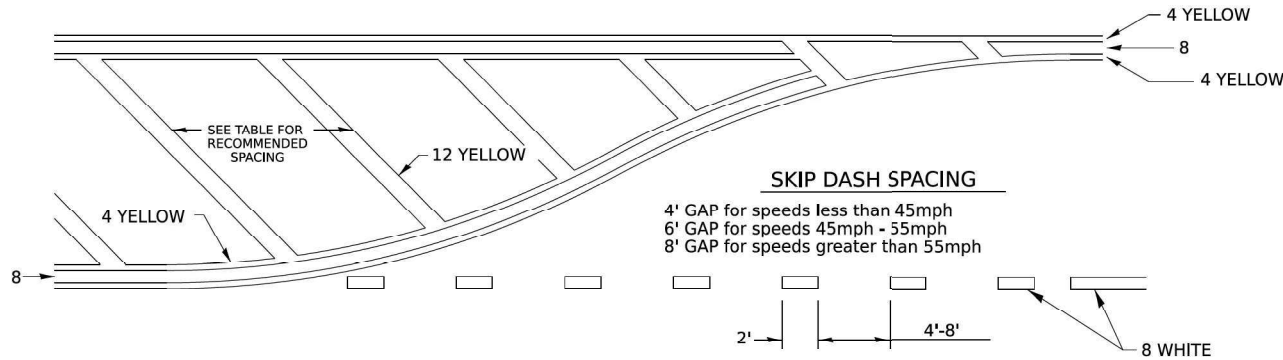


SYMBOLS

- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER
- TWO-WAY AMBER MARKER

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

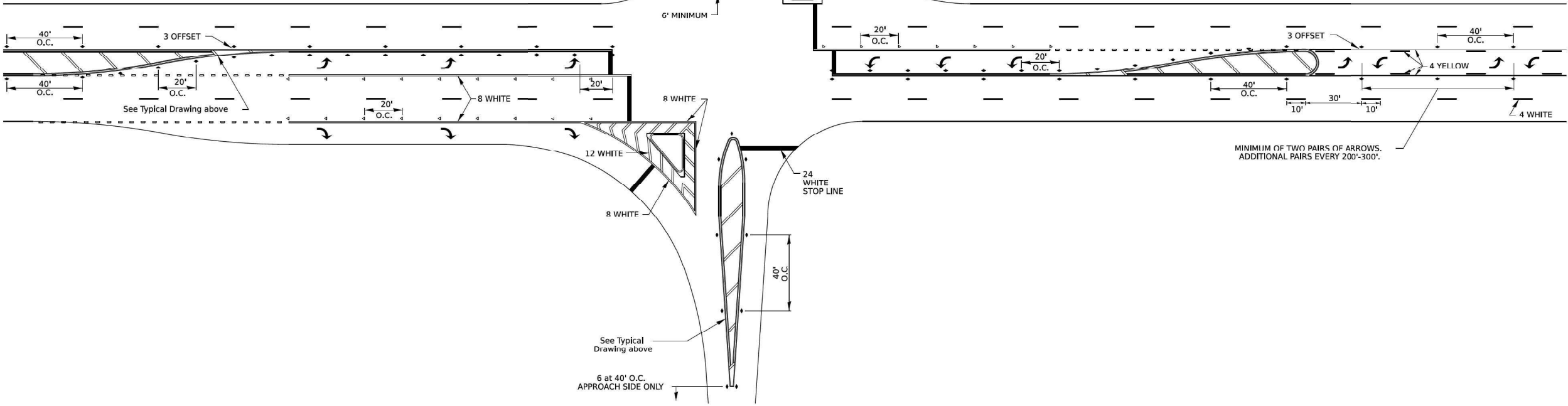
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN



RECOMMENDED SPACING BETWEEN DIAGONALS (IN FEET)

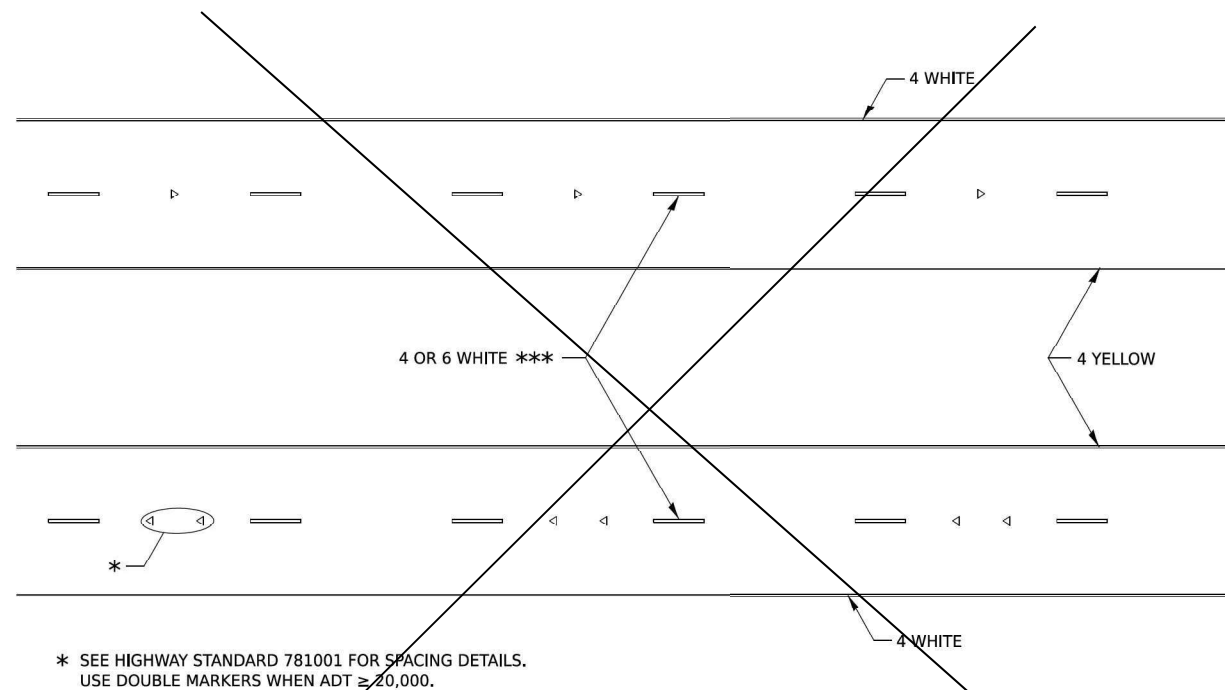
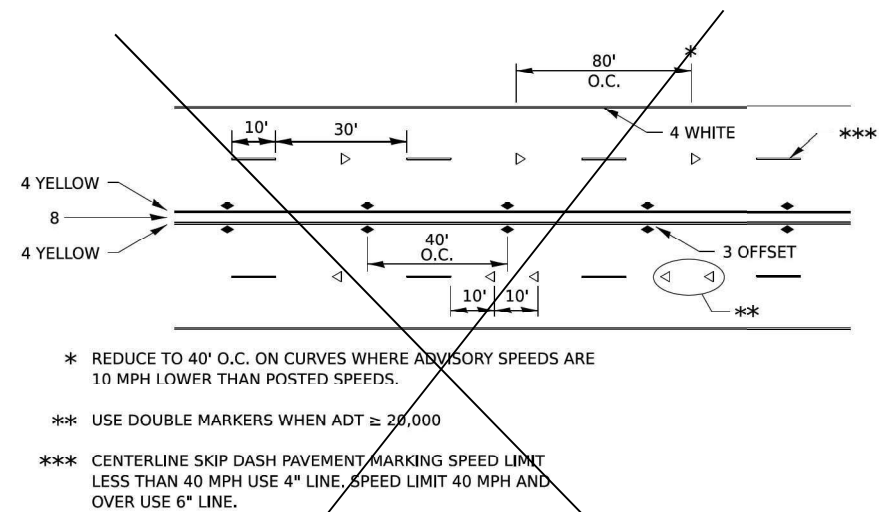
Speed Limit Range	Continuous Median Area	Intersection Channelization	Objects (Islands)
less than 30MPH	50'	15'	10'
30-40MPH	75'	20'	15'
45MPH & over	75'	30'	20'

NOTE: If the spacing recommended in the Table does not permit at least five diagonal lines in the area being marked, the spacing from the next lowest speed range should be used. The recommended spacing is measured parallel to the pavement center line.



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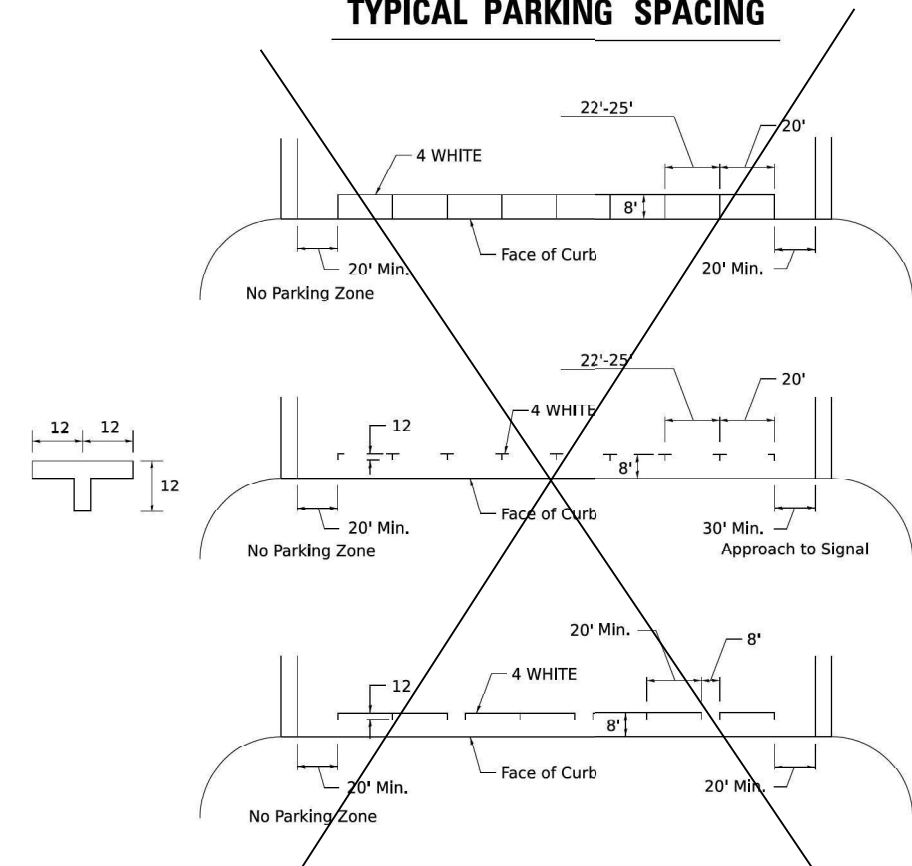
TYPICAL PAVEMENT MARKINGS

**MULTI-LANE / DIVIDED**

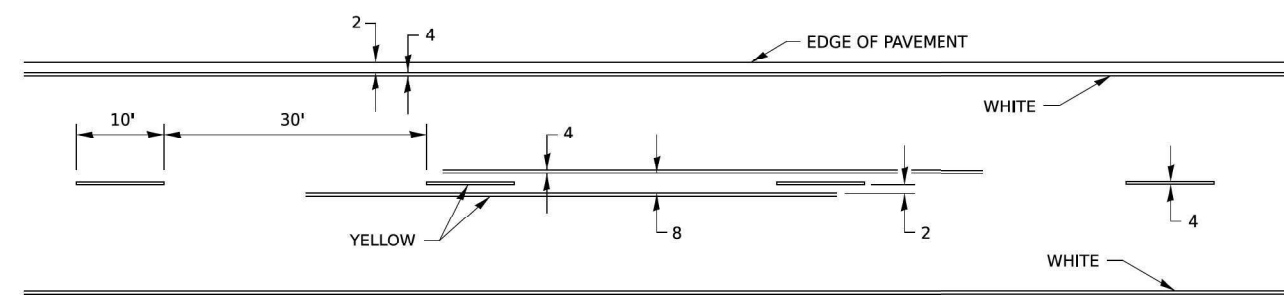
MULTI-LANE / UNDIVIDED & ONE WAY

(FOR MULTI-LANE UNDIVIDED HIGHWAYS USE THIS
DETAIL NOT HIGHWAY STANDARD 781001)

TYPICAL PARKING SPACING



TYPICAL PAVEMENT MARKING FOR TWO LANE SECTION – NO PASSING ZONES

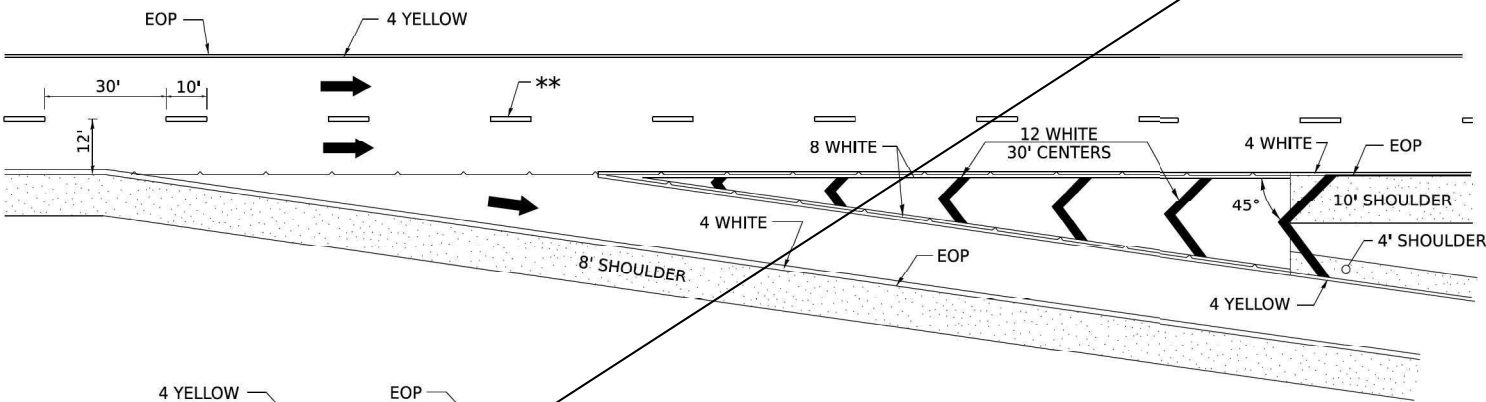


TYPICAL PAVEMENT MARKINGS

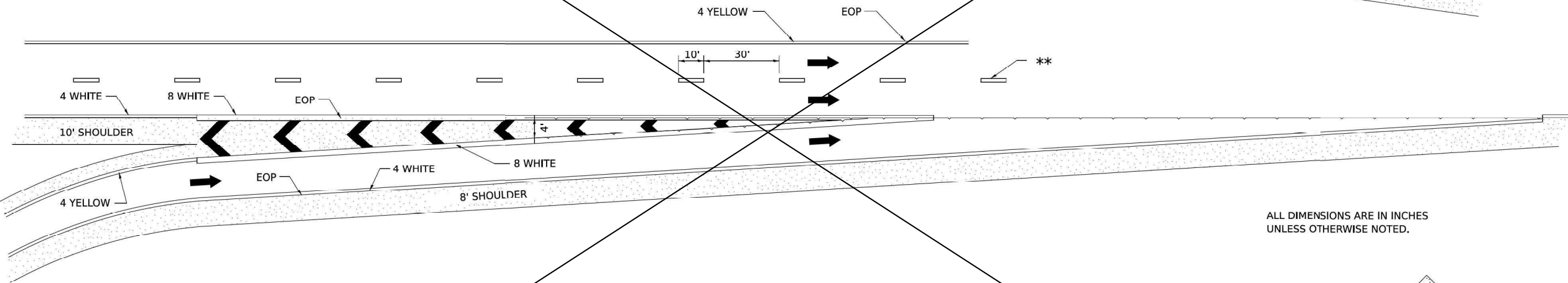
*** 6" WHITE ON INTERSTATES, WHERE THE SPEED LIMIT IS 65 MPH, OR WHEN DIRECTED BY THE ENGINEER.
4" WIDE AT ALL OTHER LOCATIONS.

NOTE: GORE HATCHING PLACED ONLY WHEN SCHEDULED IN THE PLANS

EXIT RAMP

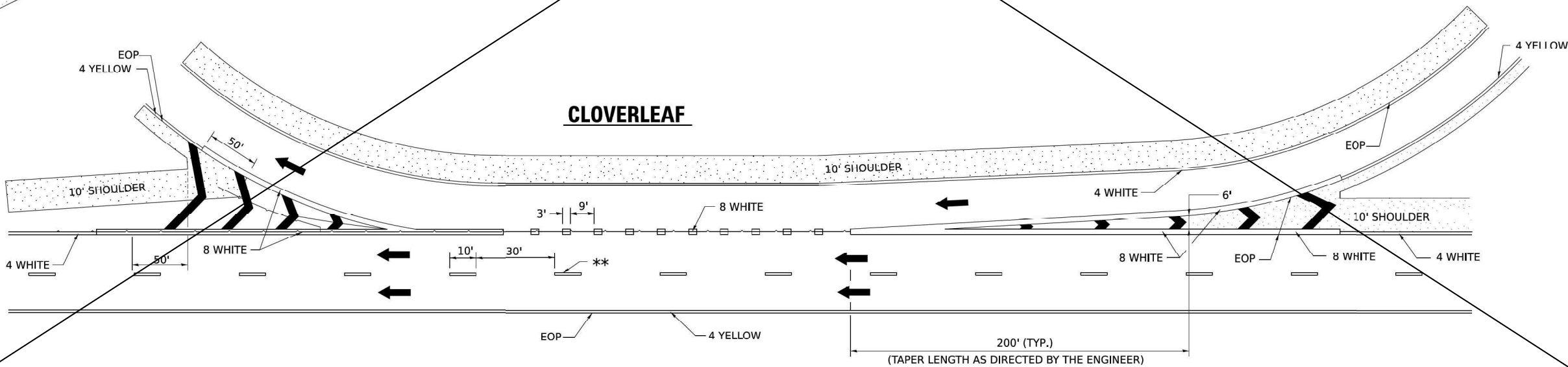


ENTRANCE RAMP



ALL DIMENSIONS ARE IN INCHES
UNLESS OTHERWISE NOTED.

CLOVERLEAF



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FILE NAME: F:\2007-TR01-2024-P1_PTB 213-030 Various Bridge Rehab\WO 02-64S25 Baxter Rd over I-39\05 CADD\Civil\Modals\ID264S25-border.dgn



USER NAME = Amalia.Baymundo	DESIGNED - AMB	REVISED -
	DRAWN - AMB	REVISED -
	CHECKED - PK	REVISED -
PLOT DATE = 4/29/2025	DATE - 4/29/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

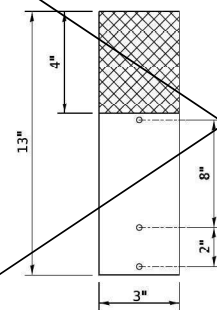
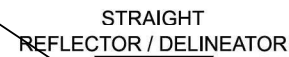
BAXTER RD OVER I-39
REGION 2/ DISTRICT 2 STANDARDS

SCALE: SHEET 8 OF 9 SHEETS STA. TO STA.

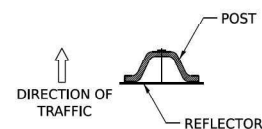
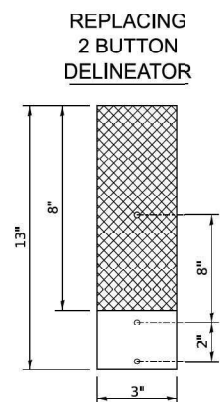
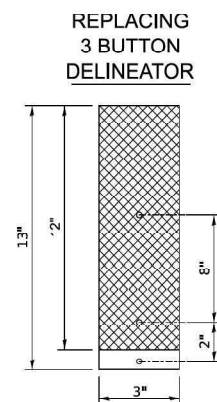
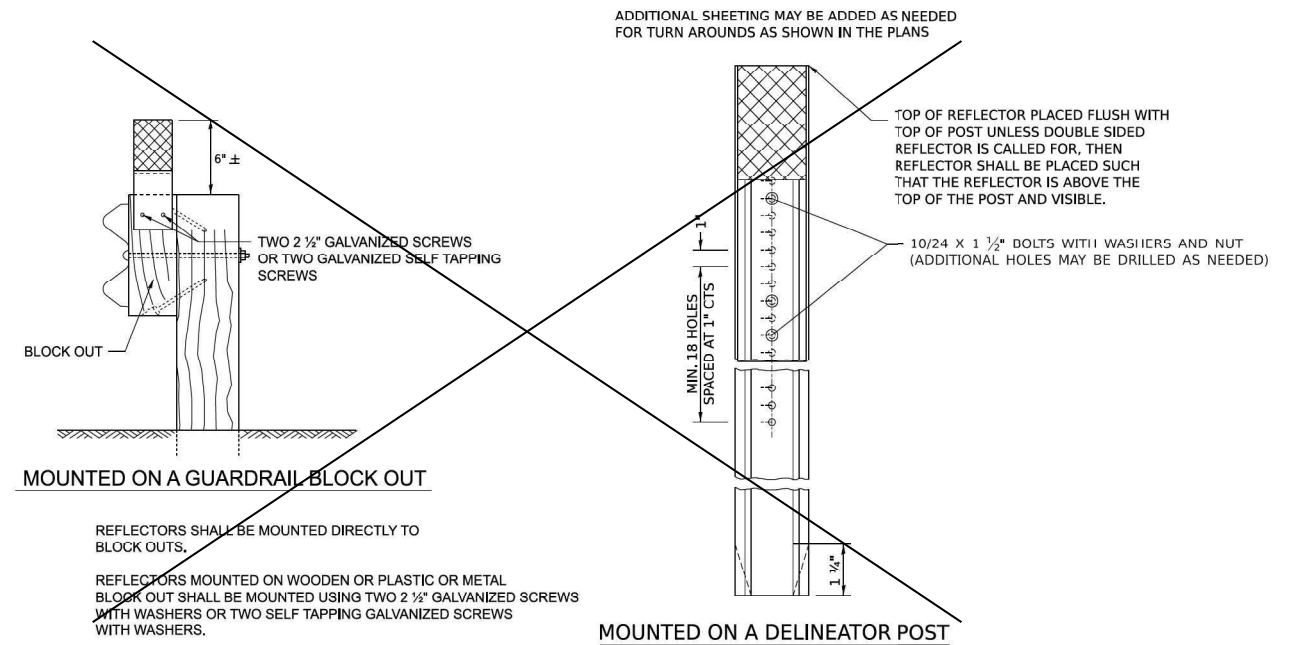
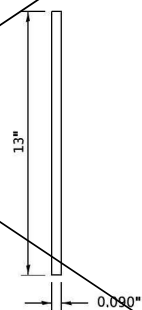
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1HB)BDR	WINNEBAGO	38	37
CONTRACT NO. 64S25				
ILLINOIS FED. AID PROJECT				

GUARDRAIL REFLECTORS, TYPE C (SPECIAL)

~~REFLECTORS FOR GUARDRAIL BLOCK OUT OR DELINEATOR POST~~



SIDE VIEW



ADDITIONAL HOLES SHALL BE DRILLED IN THE REFLECTORS AS SHOWN ABOVE.

DELINEATORS SHALL BE INSTALLED ACCORDING TO STANDARD 635001 EXCEPT THAT THE POST SHALL BE ROTATED 180" . THE POST WILL HAVE THE WIDE SIDE FACING TRAFFIC AND THE REFLECTOR ATTACHECD AS SHOWN ABOVE.

NOTE:

REFLECTORS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR GUARDRAIL REFLECTORS, TYPE C (SPECIAL), WHICH PRICE SHALL ALSO INCLUDE SCREWS, WASHERS OR AN APPROVED BONDING AGENT.

REFLECTORS INSTALLED ON TWO LANE ROADS SHALL BE DOUBLE SIDED
AND BOTH SIDES SHALL BE CRYSTAL.

REFLECTORS INSTALLED ON CENTER BARRIER OR IN THE MEDIAN SHALL BE DOUBLE SIDED AND BOTH SIDES SHALL BE AMBER.

REFLECTORS INSTALLED ON DIVIDED HIGHWAYS ON THE OUTSIDE OF THE ROADWAY SHALL BE DOUBLE SIDED CRYSTAL.

SPACING FOR REFLECTORS SHALL BE ACCORDING TO STANDARD 782006
UNLESS OTHERWISE NOTED IN THE PLANS.

NOTE:

REFLECTORS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR GUARDRAIL REFLECTORS, TYPE C (SPECIAL), WHICH PRICE SHALL ALSO INCLUDE SCREWS, WASHERS OR AN APPROVED BONDING AGENT.

REFLECTORS INSTALLED ON TWO LANE ROADS SHALL BE DOUBLE SIDED
AND BOTH SIDES SHALL BE CRYSTAL.

REFLECTORS INSTALLED ON CENTER BARRIER SHALL BE DOUBLE SIDE AND BOTH SIDES SHALL BE AMBER.

REFLECTORS INSTALLED ON DIVIDED HIGHWAYS ALONG THE OUSTIDE OF
THE HIGHWAY SHALL BE DOUBLE SIDED CRYSTAL.

SPACING FOR REFLECTORS SHALL BE ACCORDING TO STANDARD 782006
UNLESS OTHERWISE NOTED IN THE PLANS.

REFLECTORS MOUNTED ON BARRIER WALL