



CONSULTANTS

IL PROJECT: UGN-5190
IL LETTING ITEM: 09A
IL CONTRACT: WA082

REHABILITATE TAXIWAY D PAVEMENT

APRIL 18, 2025

OWNER



MARK DATE DESCRIPTION

CMT PROJECT NO: 24006512.00
CAD DWG FILE:
DESIGNED BY: CMJ
DRAWN BY: JRO
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APPROVED BY: CAL
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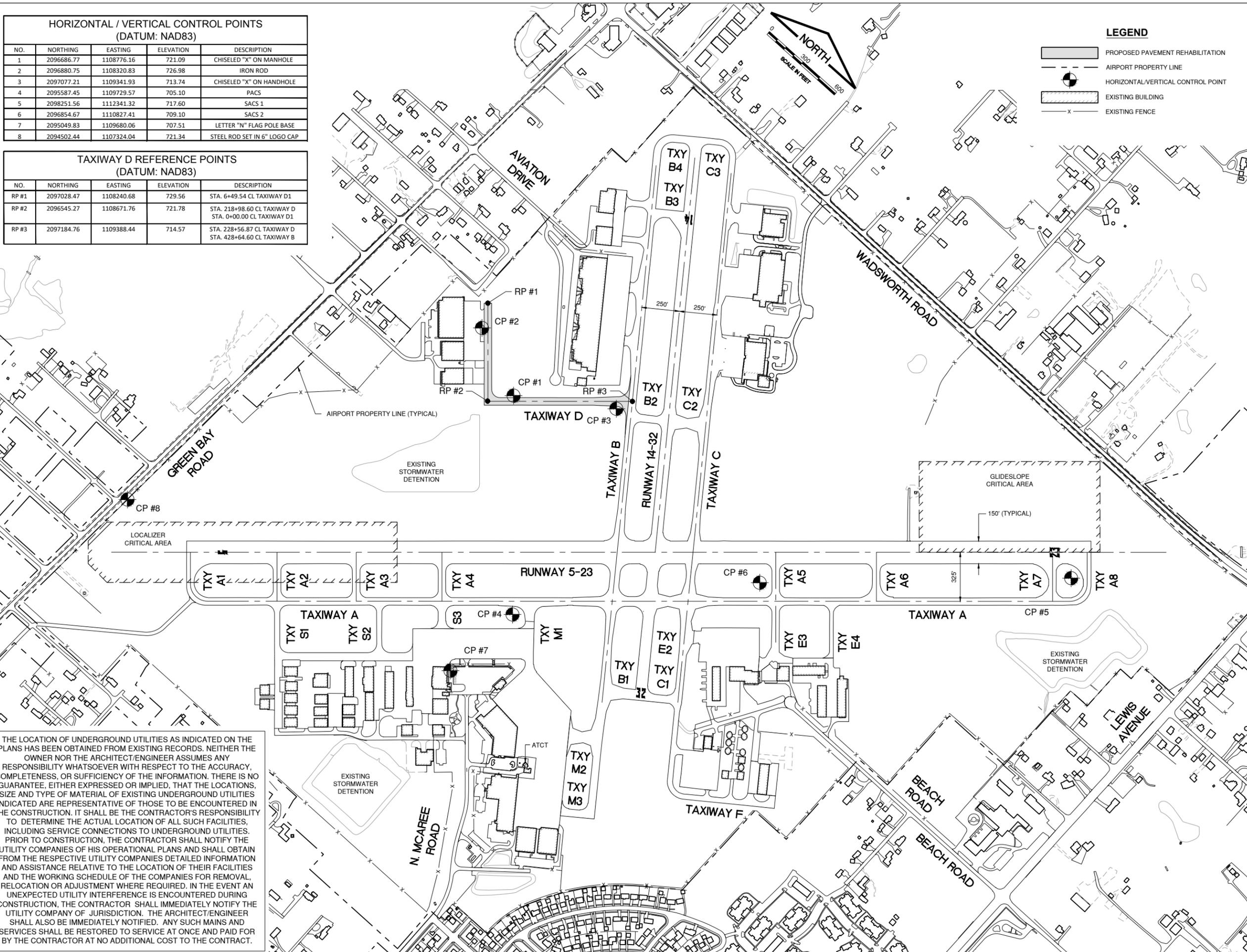
SHEET TITLE
SITE PLAN - PROJECT REFERENCE POINTS

HORIZONTAL / VERTICAL CONTROL POINTS (DATUM: NAD83)				
NO.	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	2096686.77	1108776.16	721.09	CHISELED "X" ON MANHOLE
2	2096880.75	1108320.83	726.98	IRON ROD
3	2097077.21	1109341.93	713.74	CHISELED "X" ON HANDHOLE
4	2095587.45	1109729.57	705.10	PACS
5	2098251.56	1112341.32	717.60	SACS 1
6	2096854.67	1110827.41	709.10	SACS 2
7	2095049.83	1109680.06	707.51	LETTER "N" FLAG POLE BASE
8	2094502.44	1107324.04	721.34	STEEL ROD SET IN 6" LOGO CAP

TAXIWAY D REFERENCE POINTS (DATUM: NAD83)				
NO.	NORTHING	EASTING	ELEVATION	DESCRIPTION
RP #1	2097028.47	1108240.68	729.56	STA. 6+49.54 CL TAXIWAY D1
RP #2	2096545.27	1108671.76	721.78	STA. 218+98.60 CL TAXIWAY D STA. 0+00.00 CL TAXIWAY D1
RP #3	2097184.76	1109388.44	714.57	STA. 228+56.87 CL TAXIWAY D STA. 428+64.60 CL TAXIWAY B

LEGEND

- PROPOSED PAVEMENT REHABILITATION
- AIRPORT PROPERTY LINE
- HORIZONTAL/VERTICAL CONTROL POINT
- EXISTING BUILDING
- EXISTING FENCE



THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ARCHITECT/ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER WITH RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL, RELOCATION OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ARCHITECT/ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

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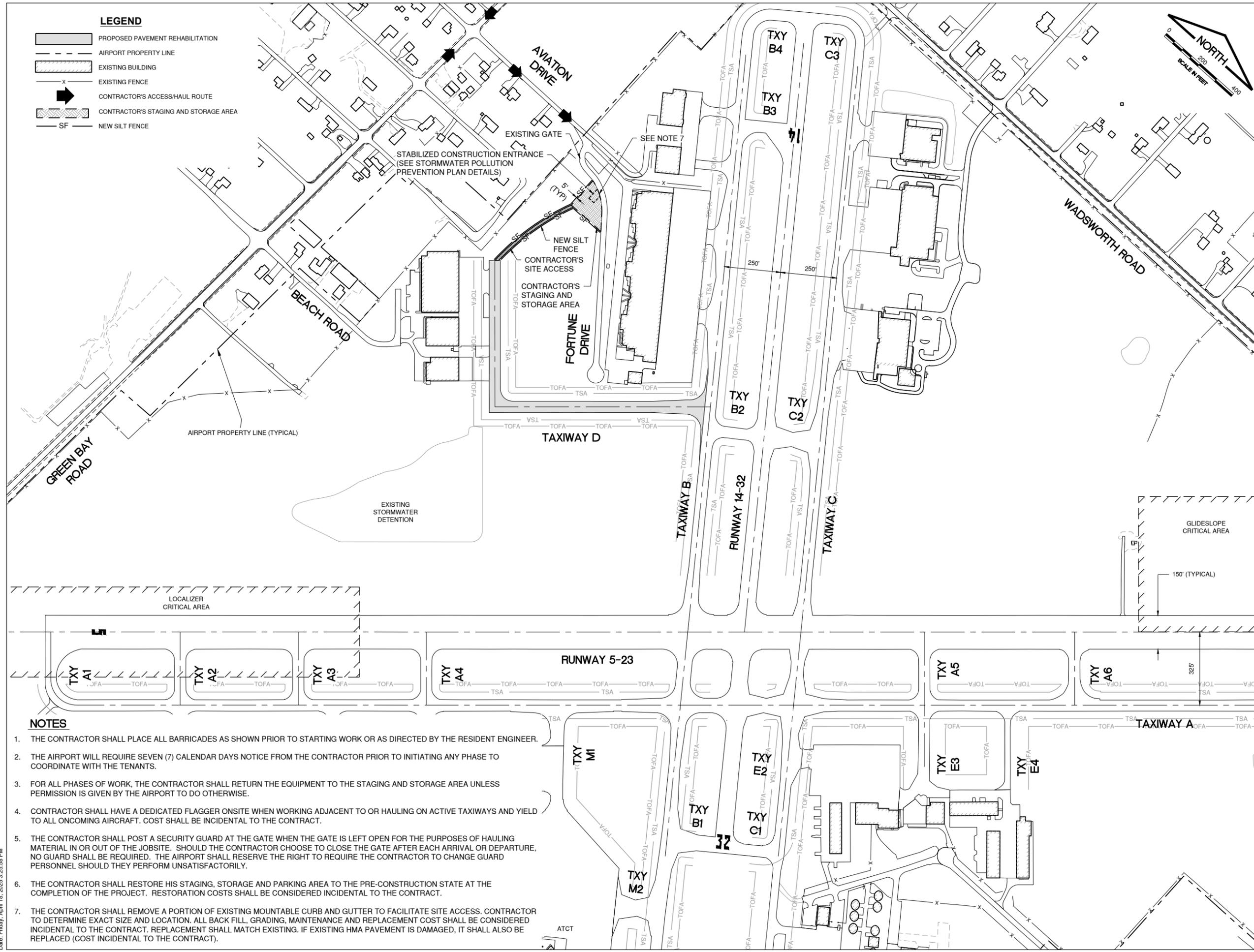
SHEET TITLE

CONTRACTOR'S ACCESS, HAULING AND STORAGE

SHEET 4 OF 47

LEGEND

- PROPOSED PAVEMENT REHABILITATION
- AIRPORT PROPERTY LINE
- EXISTING BUILDING
- EXISTING FENCE
- CONTRACTOR'S ACCESS/HAUL ROUTE
- CONTRACTOR'S STAGING AND STORAGE AREA
- NEW SILT FENCE



NOTES

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK OR AS DIRECTED BY THE RESIDENT ENGINEER.
2. THE AIRPORT WILL REQUIRE SEVEN (7) CALENDAR DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING ANY PHASE TO COORDINATE WITH THE TENANTS.
3. FOR ALL PHASES OF WORK, THE CONTRACTOR SHALL RETURN THE EQUIPMENT TO THE STAGING AND STORAGE AREA UNLESS PERMISSION IS GIVEN BY THE AIRPORT TO DO OTHERWISE.
4. CONTRACTOR SHALL HAVE A DEDICATED FLAGGER ONSITE WHEN WORKING ADJACENT TO OR HAULING ON ACTIVE TAXIWAYS AND YIELD TO ALL ONCOMING AIRCRAFT. COST SHALL BE INCIDENTAL TO THE CONTRACT.
5. THE CONTRACTOR SHALL POST A SECURITY GUARD AT THE GATE WHEN THE GATE IS LEFT OPEN FOR THE PURPOSES OF HAULING MATERIAL IN OR OUT OF THE JOBSITE. SHOULD THE CONTRACTOR CHOOSE TO CLOSE THE GATE AFTER EACH ARRIVAL OR DEPARTURE, NO GUARD SHALL BE REQUIRED. THE AIRPORT SHALL RESERVE THE RIGHT TO REQUIRE THE CONTRACTOR TO CHANGE GUARD PERSONNEL SHOULD THEY PERFORM UNSATISFACTORILY.
6. THE CONTRACTOR SHALL RESTORE HIS STAGING, STORAGE AND PARKING AREA TO THE PRE-CONSTRUCTION STATE AT THE COMPLETION OF THE PROJECT. RESTORATION COSTS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
7. THE CONTRACTOR SHALL REMOVE A PORTION OF EXISTING MOUNTABLE CURB AND GUTTER TO FACILITATE SITE ACCESS. CONTRACTOR TO DETERMINE EXACT SIZE AND LOCATION. ALL BACK FILL, GRADING, MAINTENANCE AND REPLACEMENT COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. REPLACEMENT SHALL MATCH EXISTING. IF EXISTING HMA PAVEMENT IS DAMAGED, IT SHALL ALSO BE REPLACED (COST INCIDENTAL TO THE CONTRACT).

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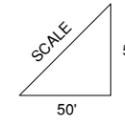
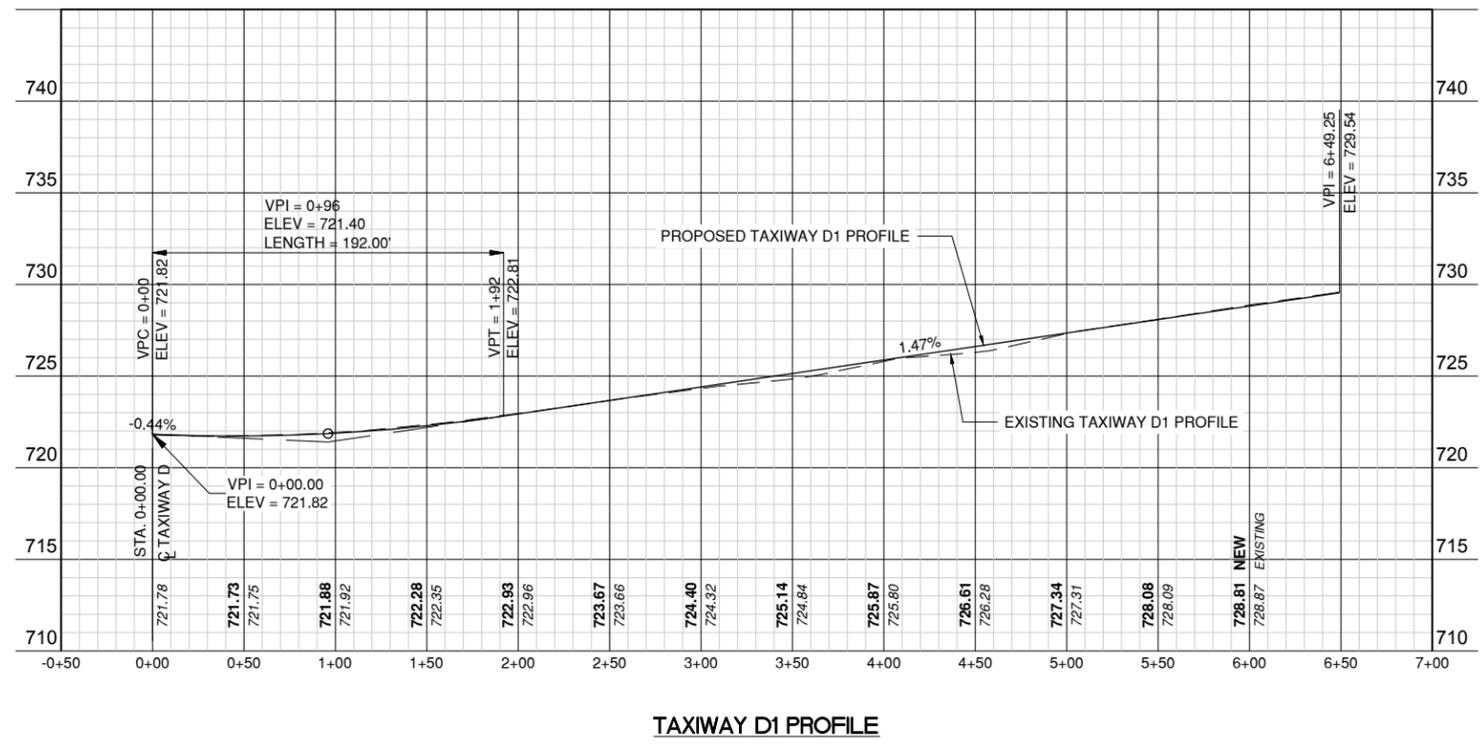
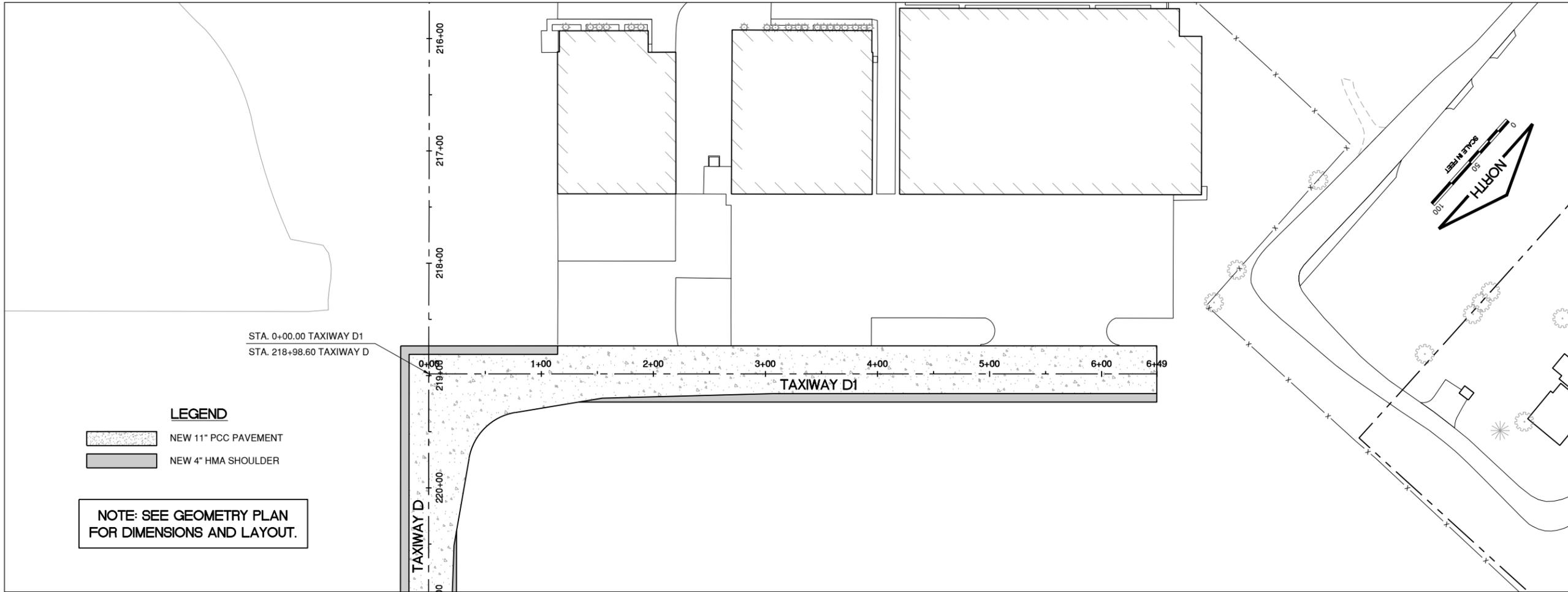


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SHEET TITLE

PLAN AND PROFILE - TAXIWAY D1



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REHABILITATE TAXIWAY D PAVEMENT

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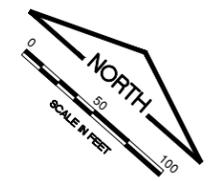


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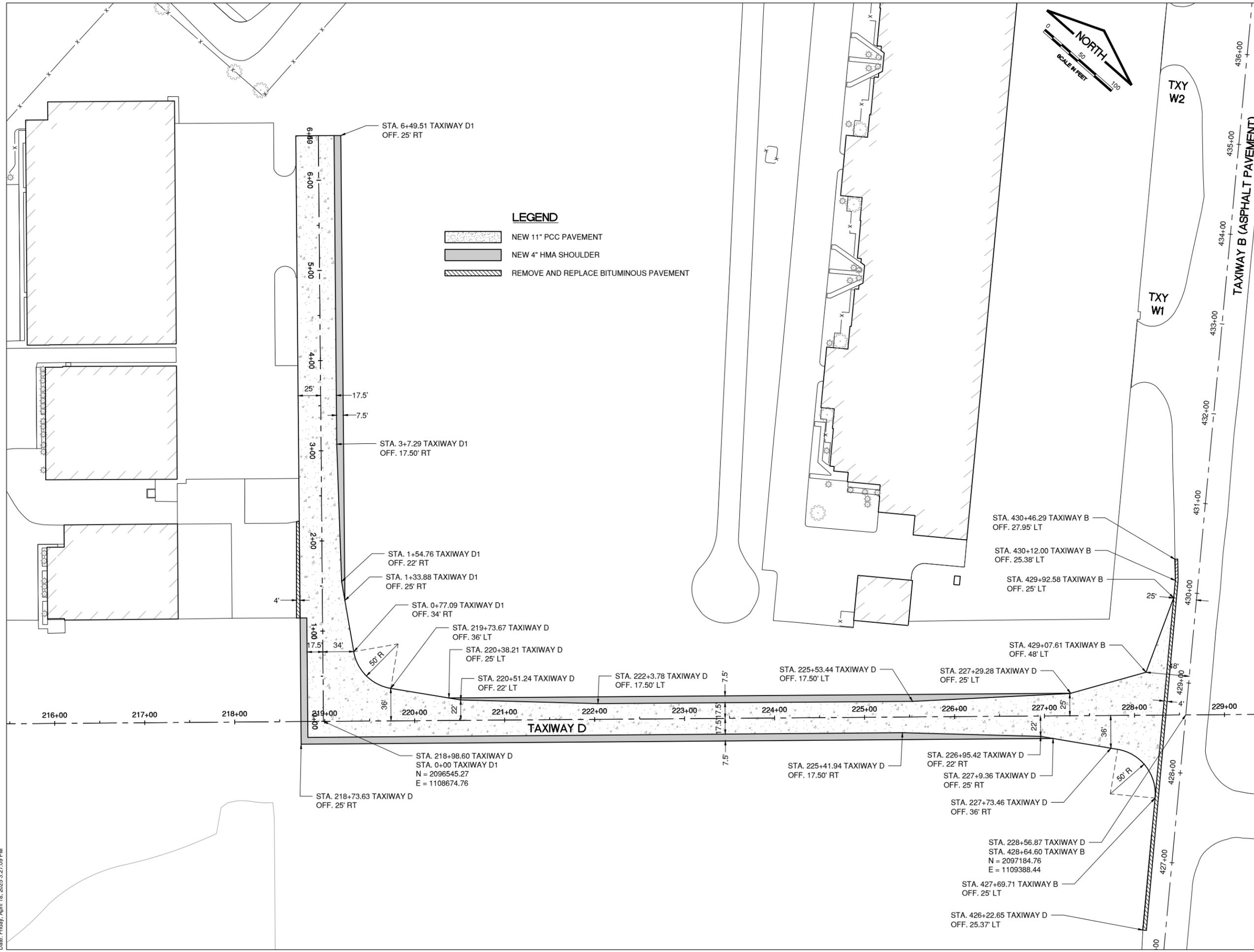
SHEET TITLE

GEOMETRY PLAN



LEGEND

- NEW 11" PCC PAVEMENT
- NEW 4" HMA SHOULDER
- REMOVE AND REPLACE BITUMINOUS PAVEMENT



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REHABILITATE TAXIWAY D PAVEMENT

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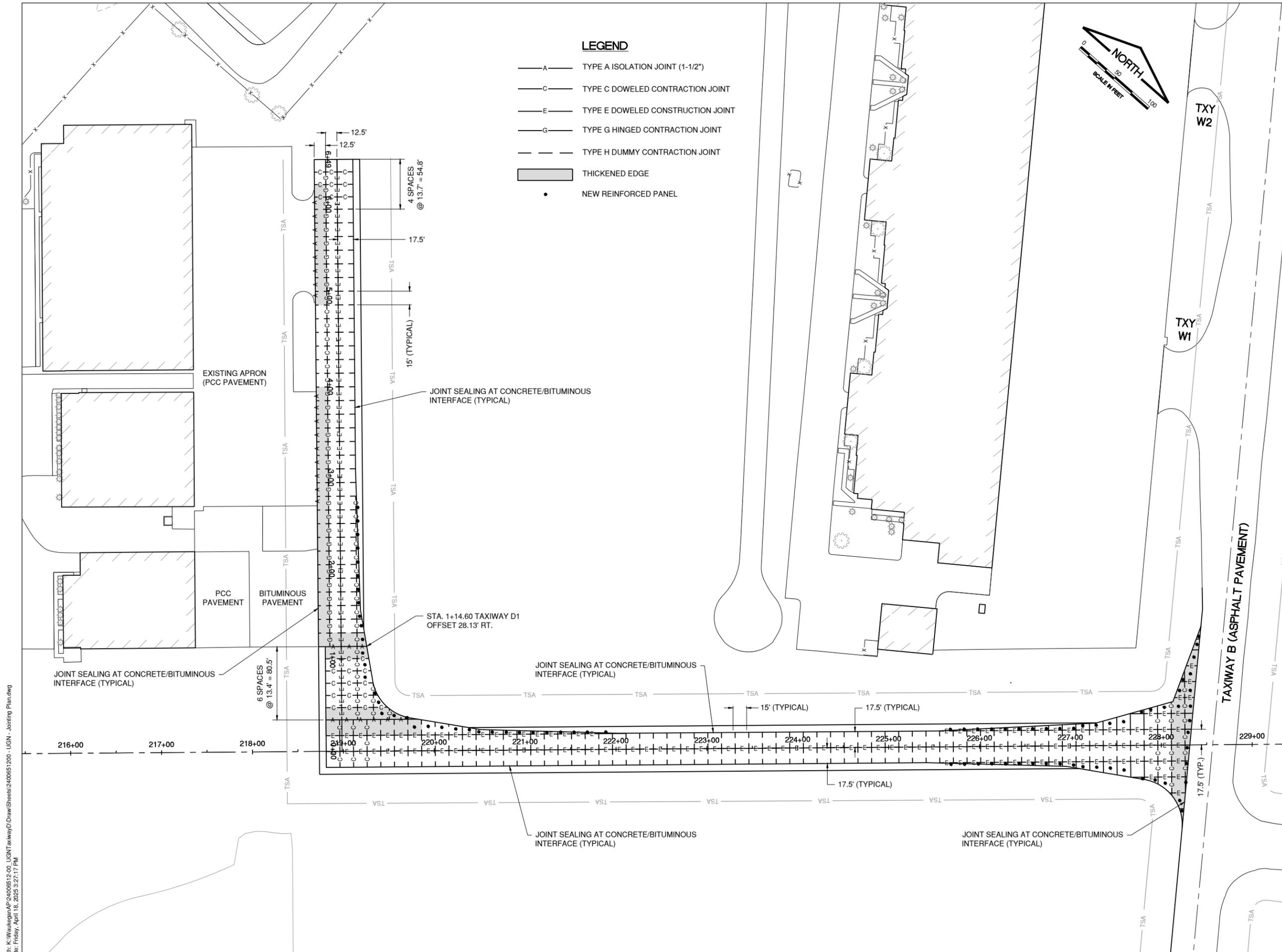
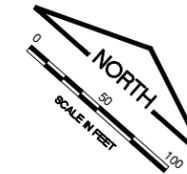


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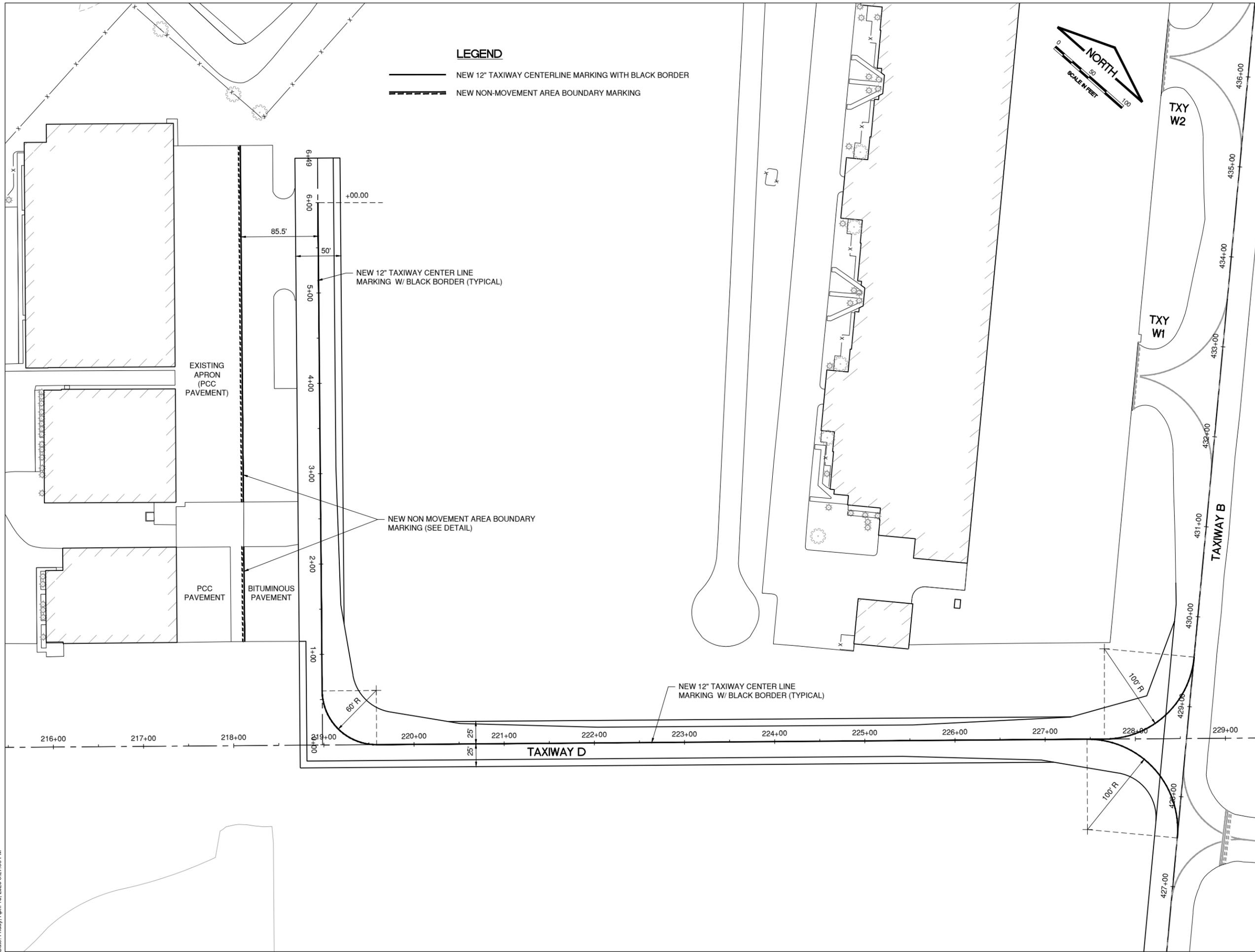
SHEET TITLE
JOINTING PLAN

- LEGEND**
- A — TYPE A ISOLATION JOINT (1-1/2")
 - C — TYPE C DOWELED CONTRACTION JOINT
 - E — TYPE E DOWELED CONSTRUCTION JOINT
 - G — TYPE G HINGED CONTRACTION JOINT
 - H — TYPE H DUMMY CONTRACTION JOINT
 - █ THICKENED EDGE
 - NEW REINFORCED PANEL

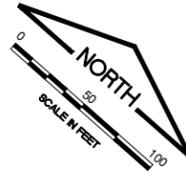


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LEGEND
 ——— NEW 12" TAXIWAY CENTERLINE MARKING WITH BLACK BORDER
 - - - - - NEW NON-MOVEMENT AREA BOUNDARY MARKING



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SHEET TITLE
PAVEMENT MARKING PLAN



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REHABILITATE TAXIWAY D PAVEMENT

APRIL 18, 2025

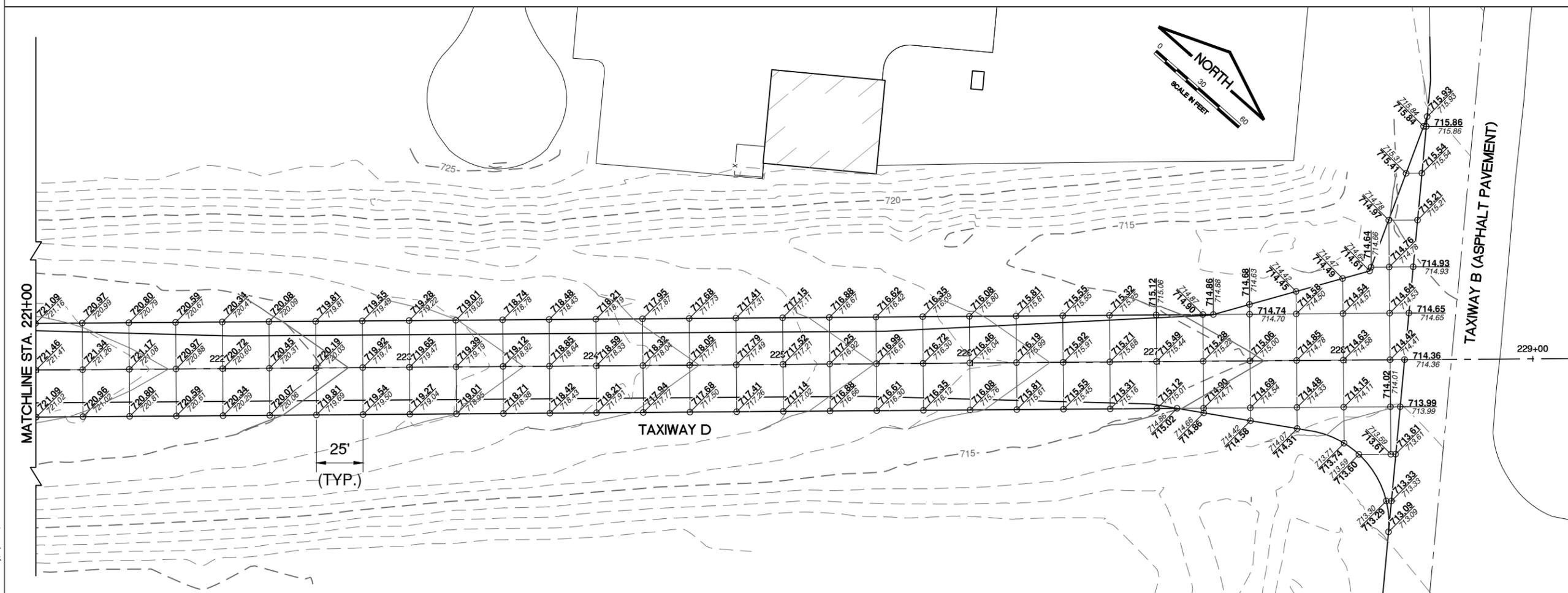
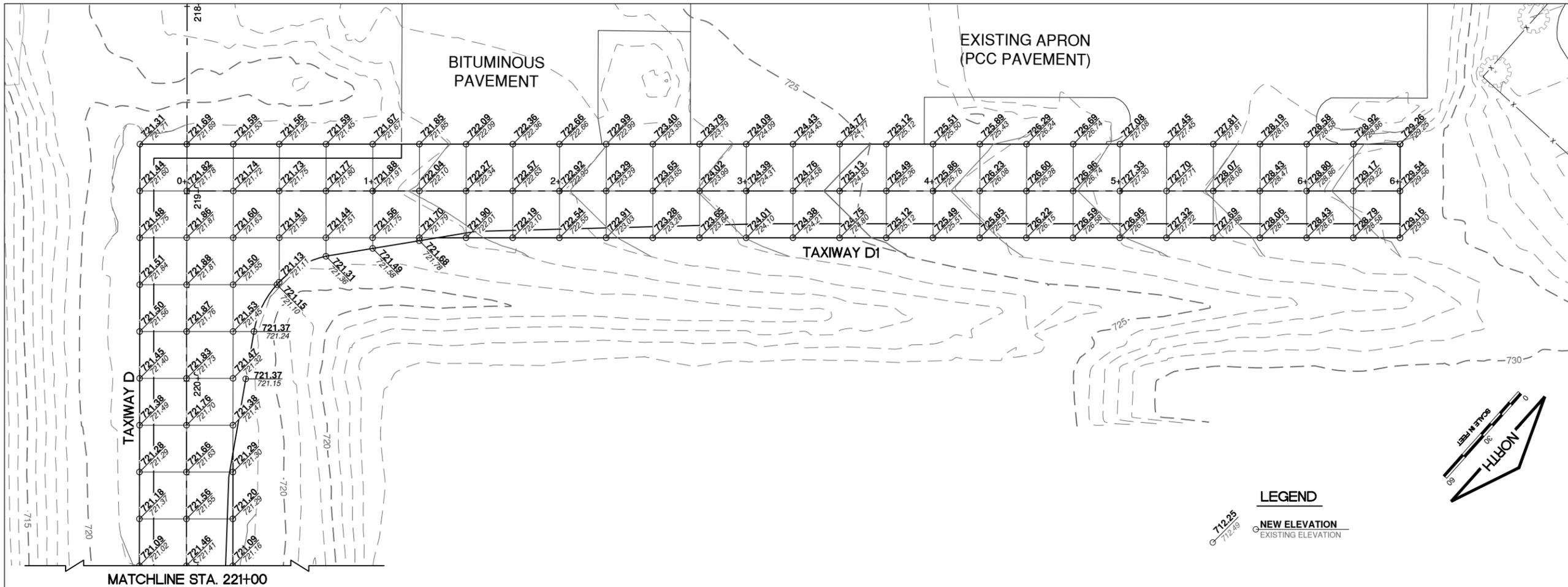
OWNER



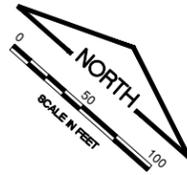
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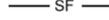
SHEET TITLE
STAKING PLAN - TAXIWAY D



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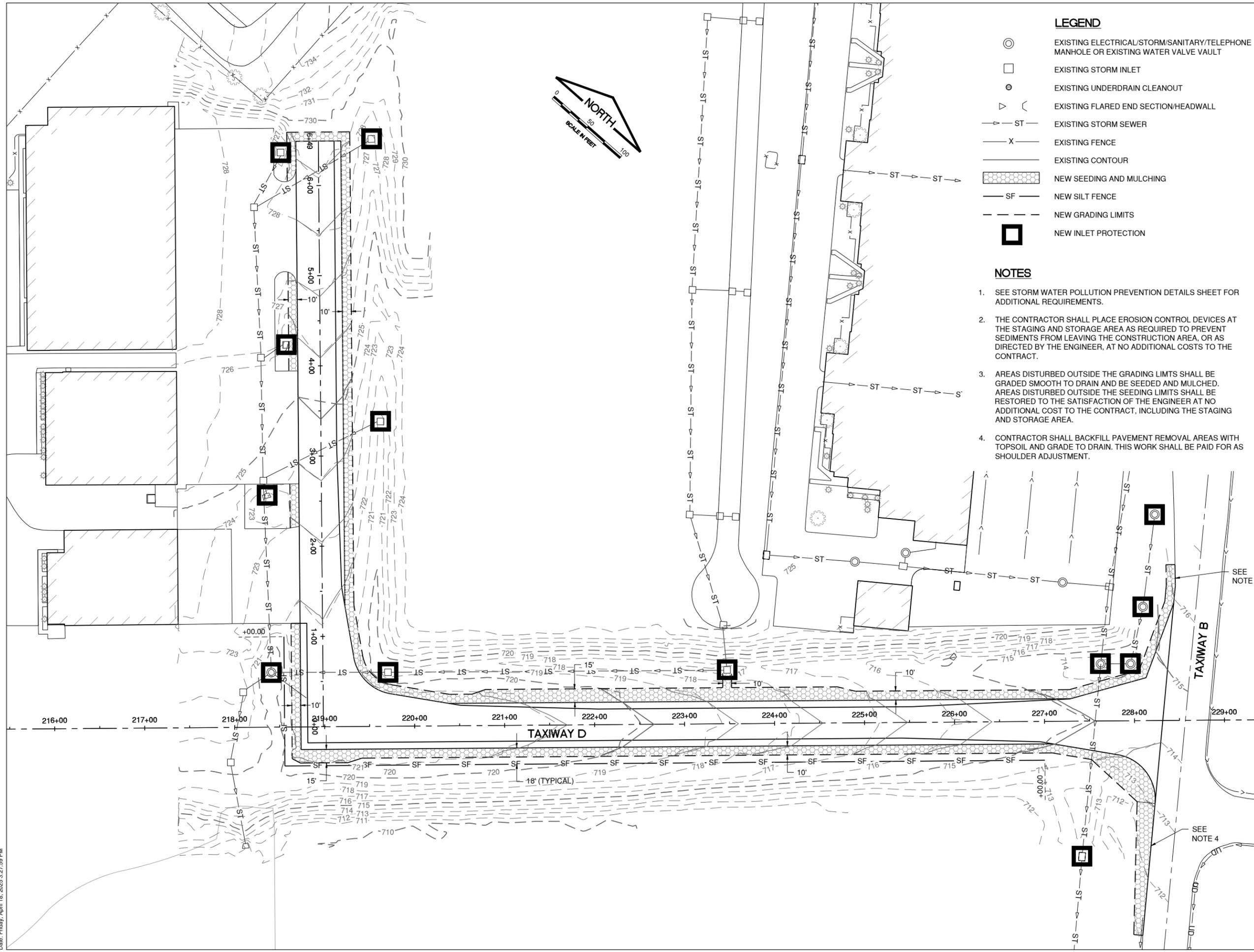


LEGEND

-  EXISTING ELECTRICAL/STORM/SANITARY/TELEPHONE MANHOLE OR EXISTING WATER VALVE VAULT
-  EXISTING STORM INLET
-  EXISTING UNDERDRAIN CLEANOUT
-  EXISTING FLARED END SECTION/HEADWALL
-  ST EXISTING STORM SEWER
-  EXISTING FENCE
-  EXISTING CONTOUR
-  NEW SEEDING AND MULCHING
-  SF NEW SILT FENCE
-  NEW GRADING LIMITS
-  NEW INLET PROTECTION

NOTES

1. SEE STORM WATER POLLUTION PREVENTION DETAILS SHEET FOR ADDITIONAL REQUIREMENTS.
2. THE CONTRACTOR SHALL PLACE EROSION CONTROL DEVICES AT THE STAGING AND STORAGE AREA AS REQUIRED TO PREVENT SEDIMENTS FROM LEAVING THE CONSTRUCTION AREA, OR AS DIRECTED BY THE ENGINEER, AT NO ADDITIONAL COSTS TO THE CONTRACT.
3. AREAS DISTURBED OUTSIDE THE GRADING LIMITS SHALL BE GRADED SMOOTH TO DRAIN AND BE SEEDED AND MULCHED. AREAS DISTURBED OUTSIDE THE SEEDING LIMITS SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE CONTRACT, INCLUDING THE STAGING AND STORAGE AREA.
4. CONTRACTOR SHALL BACKFILL PAVEMENT REMOVAL AREAS WITH TOPSOIL AND GRADE TO DRAIN. THIS WORK SHALL BE PAID FOR AS SHOULDER ADJUSTMENT.



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SHEET TITLE
STORMWATER POLLUTION PREVENTION PLAN

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 Date: Friday, April 18, 2025 3:27:59 PM

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATING PAVEMENT AND DRAINAGE IMPROVEMENTS AT WAUKEGAN NATIONAL AIRPORT.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. INSTALL AND MAINTAIN TEMPORARY EROSION CONTROL MEASURES.
2. REMOVE EXISTING PAVEMENT.
3. EXCAVATION FOR NEW PAVEMENT.
4. CONSTRUCT TEMPORARY CONCRETE WASHOUT.
5. INSTALL NEW TAXIWAY PAVEMENT.
6. INSTALL NEW UNDERDRAIN AND ELECTRICAL ITEMS.
7. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
8. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.

AREA OF CONSTRUCTION SITE AND RUNOFF COEFFICIENTS:

THE TOTAL AREA OF THE SITE ESTIMATED TO BE DISTURBED BY EXCAVATION, GRADING OR OTHER ACTIVITIES IS 2.0 ACRES. COMPOSITE RUNOFF COEFFICIENT PRIOR TO CONSTRUCTION = 0.67. COMPOSITE RUNOFF COEFFICIENT AFTER CONSTRUCTION = 0.67. THE TOTAL AREA OF CONSTRUCTION SITE IS ESTIMATED TO BE 5.2 ACRES.

EXISTING SOILS WITHIN PROJECT BOUNDARIES:

- 232 A - ASHKUM SILTY CLAY LOAM, 0 - 2% SLOPES, K = 0.20
- 298 A - BEECHER SILT LOAM, 0 - 2% SLOPES, K = 0.37
- 298 B - BEECHER SILT LOAM, 2 - 4% SLOPES, K = 0.37
- 530 B - OZAUKEE SILT LOAM, 2 - 4% SLOPES, K = 0.43
- 530 C2 - OZAUKEE SILT LOAM, 4 - 6% SLOPES, ERODED, K = 0.43
- 531 B - MARKHAM SILT LOAM, 2 - 4% SLOPES, K = 0.37

MS4 PERMITTEES IN THE AREA OF THIS PROJECT:

CITY OF WAUKEGAN
WAUKEGAN TOWNSHIP
LAKE COUNTY

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION LOCATED IN DES PLAINES RIVER WATERSHED. THE SITE DRAINS THROUGH A SERIES OF OVERLAND FLOW ROUTES/STORM SEWER INTO THE SUBURBAN COUNTRY CLUB TRIBUTARY THAT OUTLET INTO ULTIMATELY THE DES PLAINES RIVER.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, WITHIN A DAY AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTIONS SHALL BE INSTALLED AS CALLED OUT IN THE PLAN AND DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN (14) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES.

SITE INSPECTIONS MUST INCLUDE DISTURBED AREAS OF THE CONSTRUCTION SITE AND LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE. AREAS INACCESSIBLE DURING INSPECTIONS DUE TO FLOODING OR OTHER UNSAFE CONDITIONS MUST BE INSPECTED WITHIN 72 HOURS OF BECOMING ACCESSIBLE.

THE RESIDENT ENGINEER SHALL INSPECT DISTURBED AREAS OF THE CONSTRUCTION SITE THAT HAVE NOT BEEN FINALLY STABILIZED, STRUCTURAL CONTROL MEASURES, AND LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE AT LEAST ONCE EVERY SEVEN CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM OR BY THE END OF THE FOLLOWING BUSINESS OR WORK DAY THAT IS 0.50 INCHES OR GREATER. INSPECTIONS MAY BE REDUCED TO ONCE PER MONTH WHEN CONSTRUCTION ACTIVITIES HAVE CEASED DUE TO FROZEN CONDITIONS. WEEKLY INSPECTIONS MUST RECOMMENCE WHEN CONSTRUCTION ACTIVITIES ARE CONDUCTED, IF THERE IS A 0.50 INCHES OR GREATER RAIN EVENT, OR A DISCHARGE DUE TO SNOWMELT OCCURS.

DISTURBED AREAS, AREAS USED FOR THE STORAGE OF MATERIALS THAT ARE EXPOSED TO PRECIPITATION AND ALL AREAS WHERE STORMWATER TYPICALLY FLOWS WITHIN THE SITE MUST BE INSPECTED FOR EVIDENCE OF, OR THE POTENTIAL FOR, POLLUTANTS ENTERING THE DRAINAGE SYSTEM. ALL LOCATIONS WHERE STABILIZATION MEASURES HAVE BEEN IMPLEMENTED MUST BE OBSERVED TO ENSURE THAT THEY ARE STILL STABILIZED. WHERE DISCHARGE LOCATION OR POINTS ARE ACCESSIBLE, THEY MUST BE INSPECTED TO ASCERTAIN WHETHER EROSION CONTROL MEASURES ARE EFFECTIVE IN PREVENTING SIGNIFICANT IMPACTS TO RECEIVING WATERS. LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE MUST BE NSPECTED FOR EVIDENCE OF OFFSITE SEDIMENT TRACKING.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

THE CONTRACTOR SHALL PROVIDE THE RESIDENT ENGINEER WITH A SPILL RESPONSE PLAN, WHICH SHALL INCLUDE CLEANUP PROCEDURES, CONTAINMENT EFFORTS, AND PROPER EVACUATION PROCEDURES IF APPLICABLE. THE CONTRACTOR SHALL ALSO PROVIDE PROVISIONS FOR REPORTING WHEN THEY RELEASE SPILL EQUAL TO OR EXCEEDING REPORTABLE QUANTITIES.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCLUDED IN THE UNIT BID PRICE FOR THE VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

DOCUMENTATION:

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF INTENT (NOI)" PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF THIS IS NOT POSSIBLE, THEN IT MAY BE PERMITTED TO POST THIS NOTICE IN A LOCAL PUBLIC BUILDING. THE SIGN OR NOTICE MUST CONTAIN THE FOLLOWING:

1. A COPY OF THE COMPLETED NOTICE OF INTENT (NOI) AS SUBMITTED TO THE IEPA
2. THE LOCATION OF THE SWPPP AND NAME AND 24/7 TELEPHONE NUMBER OF THE CONTACT PERSON.

THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AND UPDATE AN "AS-BUILT" SET OF STORM WATER POLLUTION PREVENTION PLANS IN THE PROJECT FILES. THE SWPPP SHALL BE UPDATED WITHIN 7-DAYS OF ANY MODIFICATIONS TO THE PLANS. THE SWPPP AND ALL REVISIONS SHALL BE RETAINED FOR THREE YEARS AFTER FINAL STABILIZATION OF THE SITE, WHICH SHALL BE DEFINED AS VEGETATION COVER OF AT LEAST 70% OF HISTORIC CONDITIONS.

A STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (FORM BC 2259) SHALL BE COMPLETED WITH INSPECTION FREQUENCIES AS OUTLINED HEREIN. SWPPP REPORTS SHALL BE RETAINED FOR THREE YEARS AFTER THE DATE OF FINAL STABILIZATION AS DEFINED HEREIN. ALL INSPECTION REPORTS MUST BE RETAINED AT THE CONSTRUCTION SITE.

IF ANY VIOLATION OF THE PROVISIONS OF THE PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION COVERED IN THIS PLAN, THE ENGINEER AND/OR CONTRACTOR SHALL COMPLETE AND FILE AN "INCIDENT OF NONCOMPLIANCE (ION)" REPORT FOR THE IDENTIFIED VIOLATION. THE FORMS SHALL BE AS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND SHALL INCLUDE SPECIFIC INFORMATION ON THE INCIDENT THAT CAUSED NONCOMPLIANCE, ACTIONS THAT WERE TAKEN TO CORRECT THE NONCOMPLIANCE AND TO PREVENT ITS REOCCURRENCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G. OF THE GENERAL PERMIT.

THE RESIDENT ENGINEER AND/OR CONTRACTOR MUST NOTIFY THE APPROPRIATE AGENCY FIELD OPERATIONS SECTION OFFICE BY EMAIL AT: EPA.SWNONCOMP@ILLINOIS.GOV, TELEPHONE, OR FAX (SEE ATTACHMENT A OF THE GENERAL NPDES STORMWATER PERMIT FOR CONSTRUCTION ACTIVITIES) WITHIN 24 HOURS OF ANY ION FOR ANY VIOLATION OF THE SWPPP OBSERVED DURING ANY INSPECTION CONDUCTED, OR FOR VIOLATIONS OF ANY CONDITION OF THIS PERMIT. THE RESIDENT ENGINEER AND/OR CONTRACTOR MUST COMPLETE AND SUBMIT WITHIN 5 DAYS AN ION REPORT. CORRECTIVE ACTIONS MUST BE UNDERTAKEN IMMEDIATELY TO ADDRESS THE IDENTIFIED NON-COMPLIANCE ISSUE(S).

AFTER THE INITIAL CONTACT HAS BEEN MADE WITH THE APPROPRIATE AGENCY FIELD OPERATIONS SECTION OFFICE, ALL REPORTS OF NONCOMPLIANCE MUST BE MAILED TO THE AGENCY AT THE NEW ADDRESS, NOT THE ONE IN THE PERMIT. THIS ADDRESS IS AS FOLLOWS: ILLINOIS EPA/2520 W. ILES AVE./P.O. BOX 19276/SPRINGFIELD, IL 62794-9276. PLEASE NOTE THAT IF THESE ARE DELIVERED VIA FEDEX OR UPS, THESE CARRIERS CANNOT DELIVER TO OUR P.O. BOX AND THIS NUMBER MUST BE EXCLUDED FROM THE MAILING ADDRESS.

AFTER PROJECT FINAL ACCEPTANCE, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF TERMINATION (NOT)" FORM PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY. FORMS FOR THE IEPA SHALL BE MAILED TO THE FOLLOWING ADDRESS"

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL, MAIL CODE #15
ATTN: PERMIT SECTION
1021 NORTH GRAND AVENUE EAST
P.O. BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276

NPDES PERMIT #	_____
DATE ISSUED	_____
DATE EXPIRED	_____

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, LAKE COUNTY, CITY OF WAUKEGAN, WAUKEGAN NATIONAL AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR. THE CONTRACTOR SHALL ALSO ENSURE AND DEMONSTRATE COMPLIANCE WITH LAKE COUNTY, IDOT, CITY OF WAUKEGAN'S LOCAL WASTE DISPOSAL, SANITARY SEWER OR SEPTIC SYSTEM REGULATIONS.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY AT A MINIMUM OF 50 FEET FROM RECEIVING WATERS, CONSTRUCTED OR NATURAL SITE DRAINAGE FEATURES, AND STORM DRAIN INLETS.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. INLET PROTECTION MEASURES MUST BE INSTALLED PRIOR TO STORM WATER RUNOFF FROM THE SITE ENTERING ANY STORM DRAIN INLET THAT CARRIES STORMWATER FLOW FROM THE SITE TO A WATER OF THE U.S., PROVIDED THERE IS THE AUTHORITY TO DO SO. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. CLEAN, OR REMOVE AND REPLACE, THE PROTECTION MEASURES AS SEDIMENT ACCUMULATES, THE FILTER BECOMES CLOGGED, AND/OR PERFORMANCE IS COMPROMISED. WHERE THERE IS EVIDENCE OF SEDIMENT ACCUMULATION ADJACENT TO THE INLET PROTECTION MEASURE, THE DEPOSITED SEDIMENT MUST BE REMOVED BY THE FOLLOWING BUSINESS DAY.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. STABILIZATION EFFORTS MUST BE INITIATED WITHIN 1 WORKING DAY OF THE CESSATION OF CONSTRUCTION ACTIVITIES IF THE PORTION WILL NOT BE DISTURBED FOR AT LEAST 14 CALENDAR DAYS. STABILIZATION EFFORTS MUST BE COMPLETED WITHIN 14 CALENDAR DAYS. EXCEPTIONS TO THESE REQUIREMENTS ARE PROVIDED IN THE PERMIT. A RECORD OF THE DATES WHEN MAJOR GRADING ACTIVITIES OCCUR, WHEN CONSTRUCTION ACTIVITIES TEMPORARILY OR PERMANENTLY CEASE, AND WHEN STABILIZATION MEASURES ARE INITIATED MUST BE INCLUDED IN THE SWPPP.
17. THE CONTRACTOR SHALL MINIMIZE THE DISCHARGE OF POLLUTANTS FROM EQUIPMENT AND VEHICLE WASHING, WHEEL WASH WATER, AND OTHER WASH WATERS. WASH WATERS MUST BE TREATED IN A SEDIMENT BASIN OR ALTERNATIVE CONTROL THAT PROVIDES EQUIVALENT OR BETTER TREATMENT PRIOR TO DISCHARGE.
18. THE CONTRACTOR SHALL MINIMIZE THE EXPOSURE OF FUEL, OIL, HYDRAULIC FLUIDS, OTHER PETROLEUM PRODUCTS, AND OTHER CHEMICALS BY STORING IN COVERED AREAS OR CONTAINMENT AREAS. ANY CHEMICAL CONTAINERS WITH A STORAGE OF 55 GALLONS OR MORE MUST BE STORED A MINIMUM OF 50 FEET FROM RECEIVING WATERS, CONSTRUCTED OR NATURAL SITE DRAINAGE FEATURES, AND STORM DRAIN INLETS. IF INFEASIBLE DUE TO SITE CONSTRAINTS, STORE CONTAINERS AS FAR AWAY AS THE SITE PERMITS AND DOCUMENT IN THE SWPPP THE SPECIFIC REASONS WHY THE 50-FOOT SETBACK IS INFEASIBLE AND HOW THE CONTAINERS WILL BE STORED.
19. NO SOLID MATERIALS, INCLUDING BUILDING MATERIALS, WILL BE DISCHARGED TO WATERS OF THE UNITED STATES, EXCEPT AS AUTHORIZED BY A SECTION 404 PERMIT.
20. CORRECTIVE ACTIONS MUST BE TAKEN TO ADDRESS ANY OF THE FOLLOWING CONDITIONS IDENTIFIED AT THE SITE: A STORMWATER CONTROL NEEDS REPAIR OR REPLACEMENT; A STORMWATER CONTROL NECESSARY TO COMPLY WITH THE REQUIREMENTS OF THIS PERMIT WAS NEVER INSTALLED OR WAS INSTALLED INCORRECTLY; DISCHARGES ARE CAUSING AN EXCEEDANCE OF APPLICABLE WATER QUALITY STANDARDS; OR A PROHIBITED DISCHARGE HAS OCCURRED. CORRECTIVE ACTIONS MUST BE COMPLETED AS SOON AS POSSIBLE AND DOCUMENTED WITHIN 7 DAYS IN AN INSPECTION REPORT OR REPORT OF NONCOMPLIANCE. IF IT IS INFEASIBLE TO COMPLETE THE INSTALLATION OR REPAIR WITHIN 7 CALENDAR DAYS, IT MUST BE DOCUMENTED IN THE RECORDS WHY IT IS INFEASIBLE TO COMPLETE THE INSTALLATION OR REPAIR WITHIN THE 7-DAY TIMEFRAME AND DOCUMENT THE SCHEDULE FOR INSTALLING THE STORMWATER CONTROL(S) AND MAKING IT OPERATIONAL AS SOON AS FEASIBLE AFTER THE 7-DAY TIMEFRAME. IF MAINTENANCE IS REQUIRED FOR THE SAME STORMWATER CONTROL AT THE SAME LOCATION THREE OR MORE TIMES, THE CONTROL MUST BE REPAIRED IN A MANNER THAT PREVENTS CONTINUED FAILURE TO THE EXTENT FEASIBLE, AND THE CONDITION AND HOW IT WAS REPAIRED MUST BE RECORDED. ALTERNATIVELY, IT MUST BE DOCUMENTED IN THE RECORDS WHY THE SPECIFIC REOCCURRENCE OF THIS SAME ISSUE SHOULD CONTINUE TO BE ADDRESSED AS A ROUTINE MAINTENANCE FIX.

CONTRACTOR CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PROJECT INFORMATION:

ROUTE: WAUKEGAN NATIONAL AIRPORT MARKED: REHABILITATE TAXIWAY D
SECTION: 32 PROJECT NUMBER: UGN-5190
COUNTY: LAKE CONTRACT NUMBER: 3-17-SBGP-TBD

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: _____ DATE: _____
PRINTED NAME: _____ TITLE: _____
NAME OF FIRM: _____
STREET ADDRESS: _____
CITY, STATE, ZIP: _____
PHONE NUMBER: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.



CONSULTANTS

IL PROJECT: UGN-5190
IL LETTING ITEM: 09A
IL CONTRACT: WA082

REHABILITATE TAXIWAY D PAVEMENT

APRIL 18, 2025

OWNER



MARK	DATE	DESCRIPTION

CMT PROJECT NO:	24006512.00
CAD DWG FILE:	
DESIGNED BY:	CMJ
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	CAL
COPYRIGHT:	

SHEET TITLE
STORMWATER POLLUTION PREVENTION PLAN NOTES

CONSULTANTS

IL PROJECT: UGN-5190
 IL LETTING ITEM: 09A
 IL CONTRACT: WA082

REHABILITATE TAXIWAY D PAVEMENT

APRIL 18, 2025

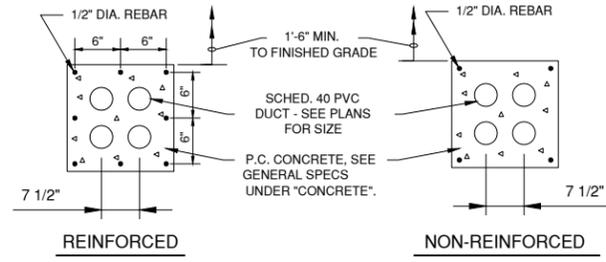
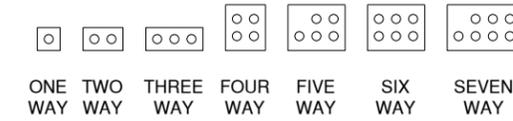
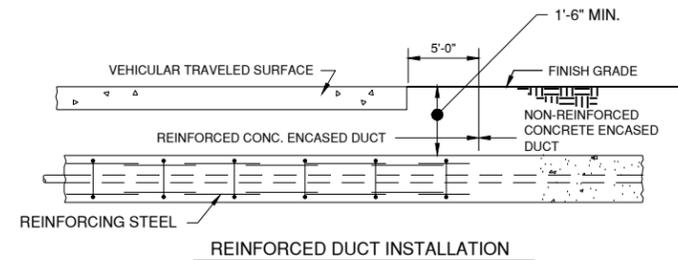
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DESIGNED BY:	CMJ
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SHEET TITLE
**ELECTRICAL
 DETAILS - 1**



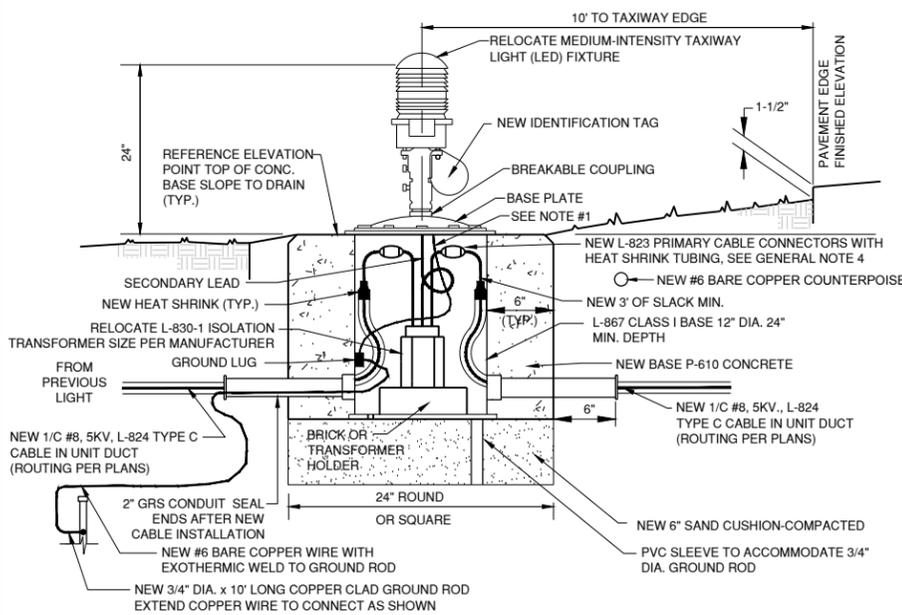
CONCRETE ENCASED DUCT DETAILS (4-WAY SHOWN)
 NOT TO SCALE

NOTES

- 1.) REINFORCED STEEL TO BE MIN. #4 REBAR, MIN. 18" LAP.

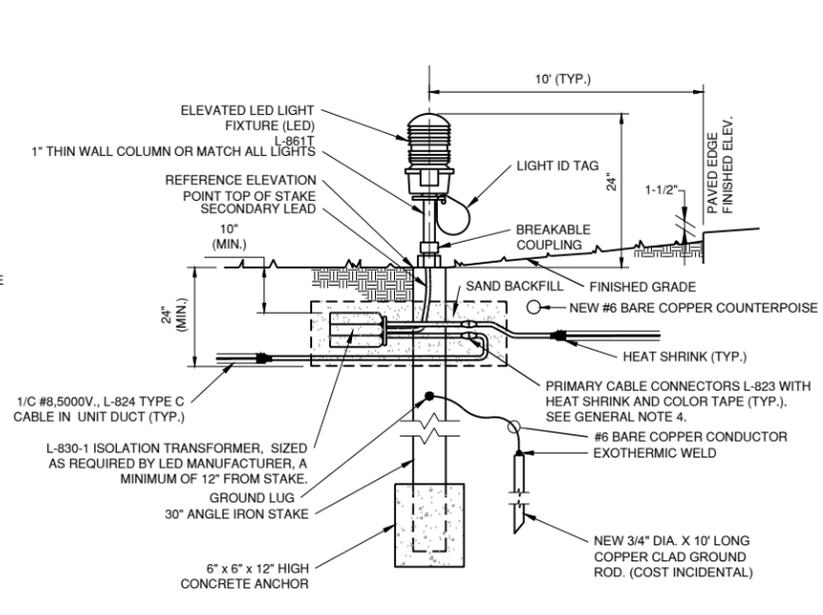
DUCT BANK DETAIL NOTES

1. DIMENSIONS SHOWN ARE MINIMUM.
2. TOP OF CONCRETE ENCASEMENT TO BE NOT LESS THAN 18" BELOW FINISHED SUBGRADE.
3. DUCT CONCRETE SHALL BE ITEM 610 STRUCTURAL P.C.C. CONCRETE.
4. LOCATIONS SHOWN ARE APPROXIMATE. DUCT BANKS SHALL BE INSTALLED AT LOCATIONS DESIGNATED BY THE RESIDENT ENGINEER.
5. DUCT BANKS SHALL EXTEND A MINIMUM OF 5' BEYOND THE NEW EDGE OF PAVEMENT.
6. INSTALL SPLIT DUCT AROUND ALL EXISTING CABLES TO REMAIN. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
7. THE DUCT BANK TRENCH BELOW PAVEMENT SHALL BE BACKFILLED WITH CA6 AGGREGATE (SEE SPECS FOR COMPACTION REQUIREMENTS)(COST INCIDENTAL TO THE DUCT PAY ITEM).



RELOCATED BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT - LED
 NOT TO SCALE

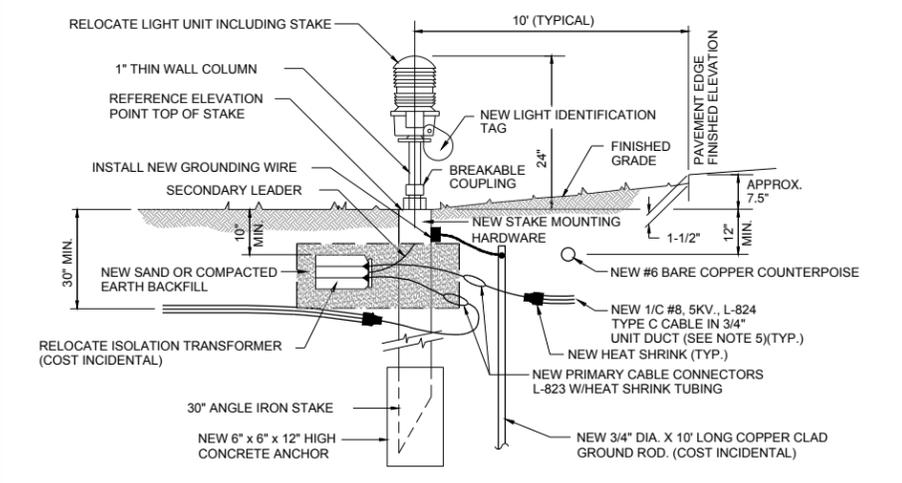
1. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.
2. LED LIGHT FIXTURES SHALL BE L-861T AS INDICATED ON THE PLANS AND SPECIFICATIONS.
3. AFTER INSTALLATION OF CABLE IN UNIT DUCT, SEAL END OF CONDUIT TO MAKE WATERTIGHT.
4. PRECAST BASE MAY BE USED.
5. COUNTERPOISE SHALL BE INSTALLED 5' FROM PAVEMENT EDGE AND MINIMUM 6" BELOW REFERENCE ELEVATION.



NEW STAKE MOUNTED TAXIWAY EDGE LIGHTS (LED)
 N.T.S.

NOTES

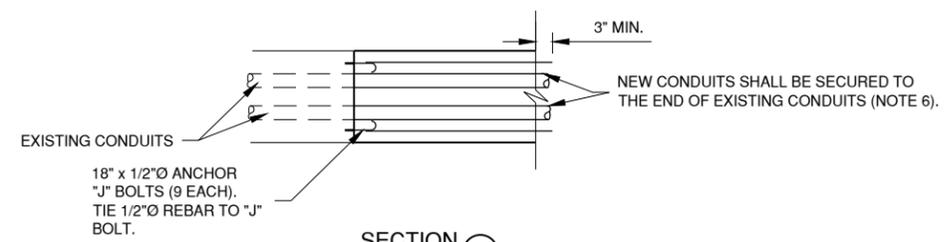
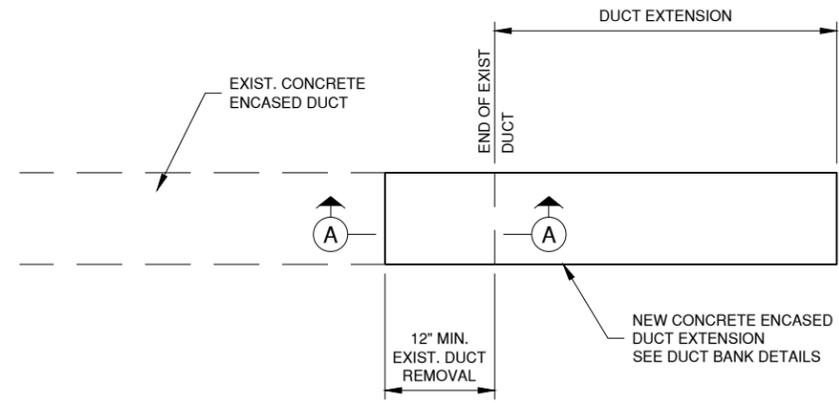
1. CABLES SHALL NOT BE LESS THAN 24" DEEP. ENCASE ISOLATION TRANSFORMER, L-823 CONNECTORS, AND 2 FOOT MINIMUM OF L-824 CABLE SLACK IN SAND BACKFILL.
2. UNIT DUCT, WHERE INSTALLED, SHALL BE TERMINATED AT L-823 CONNECTORS AND SEALED TO MAKE WATERTIGHT.
3. DO NOT INSTALL ANGLE IRON STAKE BY DRIVING. MAKE ELECTRICAL CONNECTIONS AND BACKFILL AROUND THE STAKE WITH EARTH PASSING THE 1-INCH SIEVE. COMPACT AS REQUIRED TO PROVIDE FIRM SUPPORT FOR STAKE, AND TO THE SATISFACTION OF THE RESIDENT ENGINEER.
4. COUNTERPOISE SHALL BE INSTALLED 5' FROM PAVEMENT EDGE AND MINIMUM 6" BELOW REFERENCE ELEVATION.



RELOCATED STAKE MOUNTED MEDIUM INTENSITY EDGE LIGHT DETAIL
 NOT TO SCALE

GENERAL NOTES

1. BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
2. ISOLATION TRANSFORMERS HAVE A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
3. TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
4. AT THE CONTRACTOR'S OPTION, IN LIEU OF TAPE AND HEAT SHRINKABLE TUBING, A SELF-SEALING STYLE CONNECTOR L-823 "COMPLETE KITS" OR FAA APPROVED EQUAL MAY BE USED.
5. CONTRACTOR SHALL LOCATE AND EXCAVATE THE EXISTING POWER CABLES TO CREATE SUFFICIENT SLACK TO CONNECT THE EXISTING CABLES TO THE RELOCATED STAKE MOUNTED LIGHT. COST INCIDENTAL TO RELOCATED STAKE PAY ITEM.
6. ALL GROUND AND COUNTERPOISE WIRES SHALL BE EXOTHERMICALLY WELDED TO GROUND ROD (COST INCIDENTAL).



DUCT EXTENSION DETAIL
 NOT TO SCALE

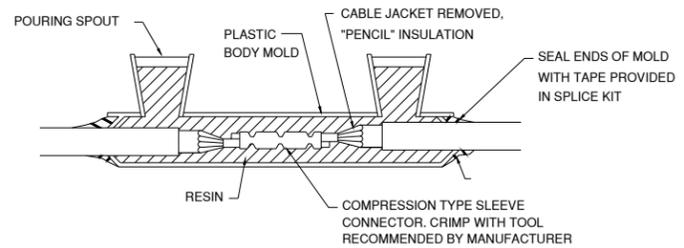
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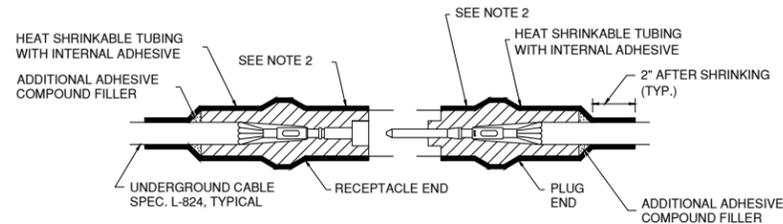
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DRAWN BY:	JRO
CHECKED BY:	CAL
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SHEET TITLE
**ELECTRICAL
 DETAILS - 2**



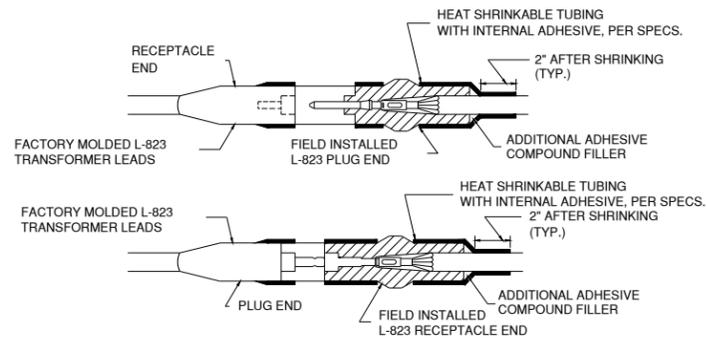
TYPE A - CABLE SPlice

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS
 TO EXISTING CABLES ONLY



TYPE B - CABLE SPlice

NOT TO SCALE
 FOR SPLICES FOR USE AT JUNCTION OF
 HOMERUN WITH LOOP CIRCUIT



TYPE C AND D - CABLE SPlice

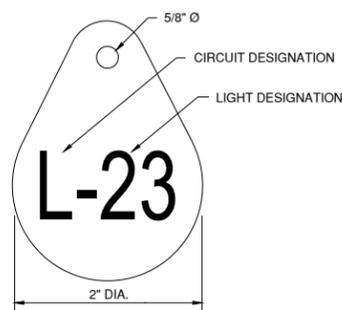
NOT TO SCALE
 FOR SPLICES AT
 RUNWAY/TAXIWAY LIGHTS AND SIGNS

SPLICE NOTES

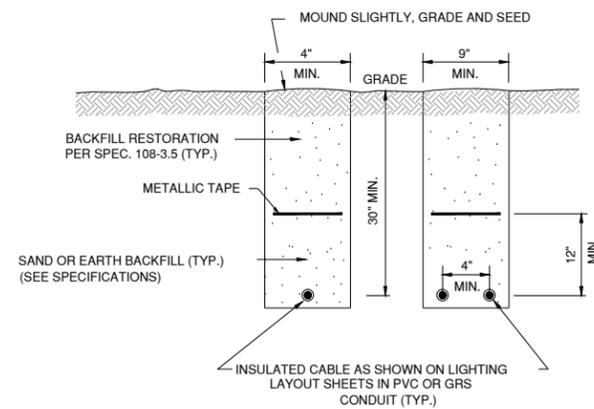
- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF FIVE (5) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
- THE CONTRACTOR MAY ELECT TO USE AN FAA APPROVED PRIMARY CONNECTOR KIT IN LIEU OF HEAT SHRINK KIT AT NO ADDITIONAL COST.

NOTES

- INSTALL A NON-CORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
- NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS AND SIGNS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (PROPOSED OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



LIGHT IDENTIFICATION DETAIL
 NOT TO SCALE

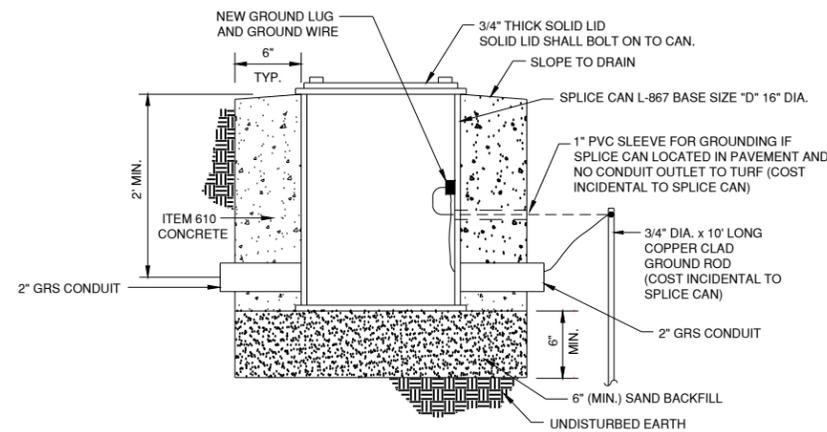


TURF AREA CABLE TRENCH DETAIL

NOT TO SCALE

NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL.

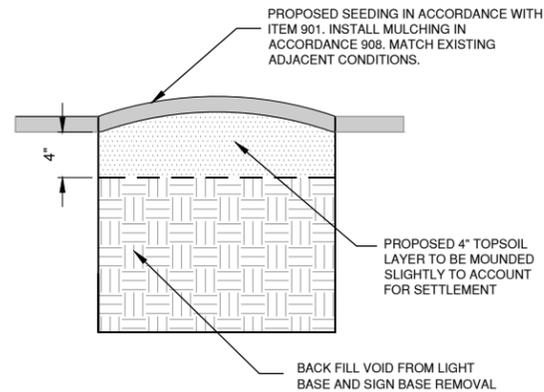


SPLICE CAN DETAIL

NOT TO SCALE

NOTES

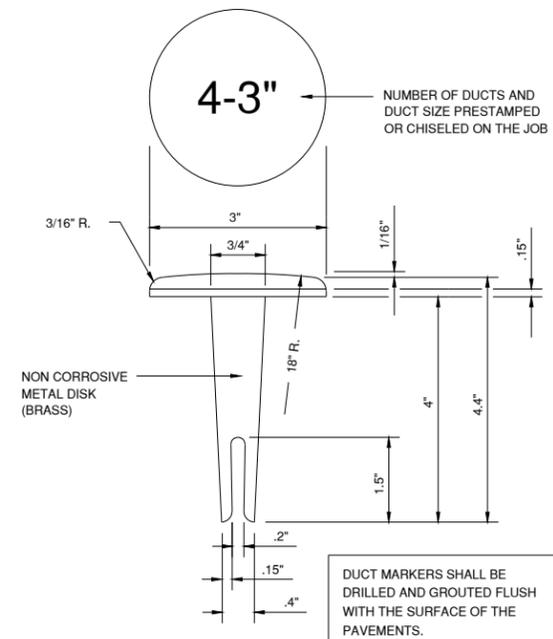
- INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELDED TO GROUND ROD (COST INCIDENTAL).



**COMPLETED BASE MOUNTED
 LIGHT AND SIGN REMOVAL**

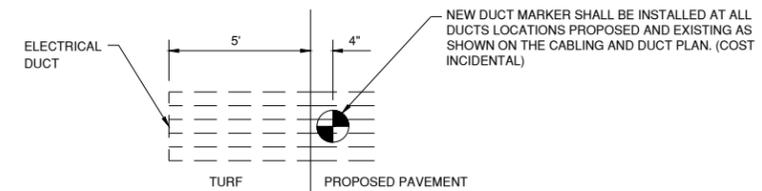
NOT TO SCALE

NOTE: COST OF BACKFILL AND RESTORATION SHALL
 BE INCIDENTAL TO THE CONTRACT.



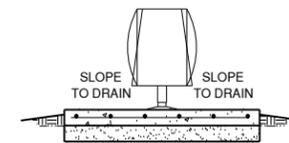
DUCT MARKER DETAIL

NOT TO SCALE

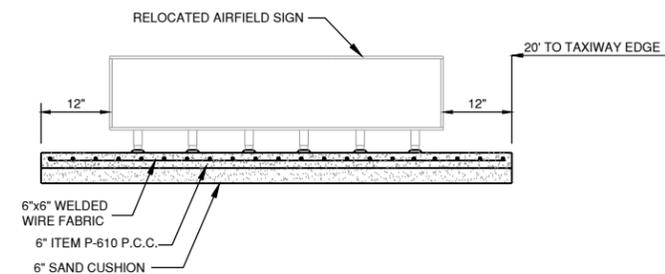


DUCT MARKER DETAIL

NOT TO SCALE



ELEVATION VIEW



NEW SIGN BASE DETAIL

NOT TO SCALE

NOTE: ALL SHOWN ITEMS ARE NEW EXCEPT RELOCATED SIGN.

