

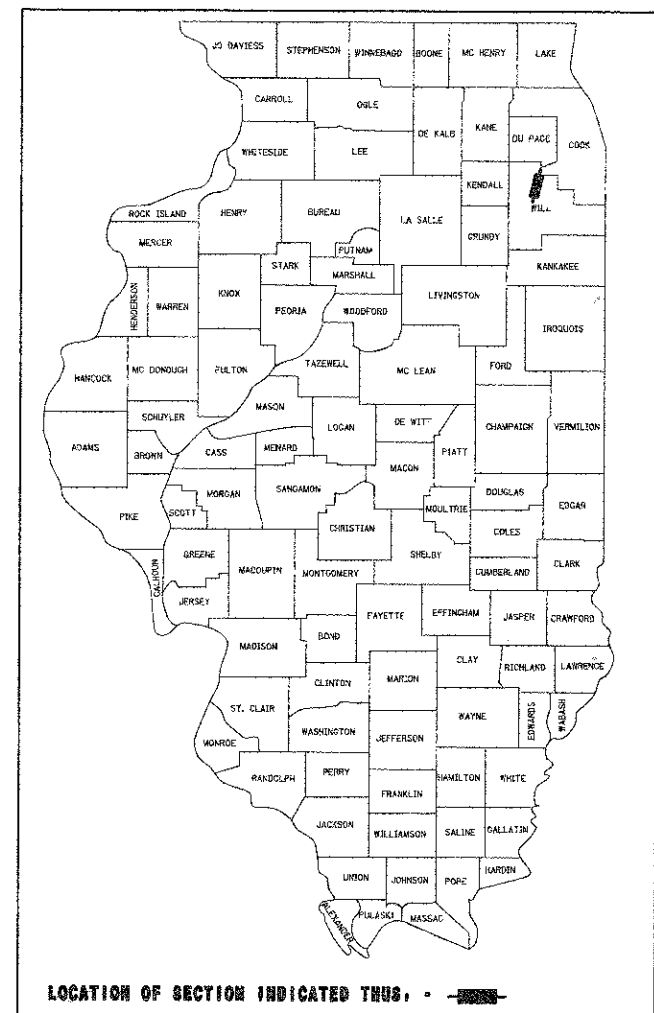
PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406, SCHAUWEURG L. CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC. CONTACT: JAMES R. LENZINI 630-466-6700

06-14-13 LETTING ITEM 038

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
FAP 577 IL 171 (STATE STREET)
AND 13TH STREET
INTERSECTION WIDENING/SIGNALIZATION
SECTION: 10-00068-00-TL
PROJECT NUMBER: M-9003(619)
CITY OF LOCKPORT
WILL COUNTY
JOB NUMBER: C-91-486-10**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	1
CONTRACT NO. 63788				

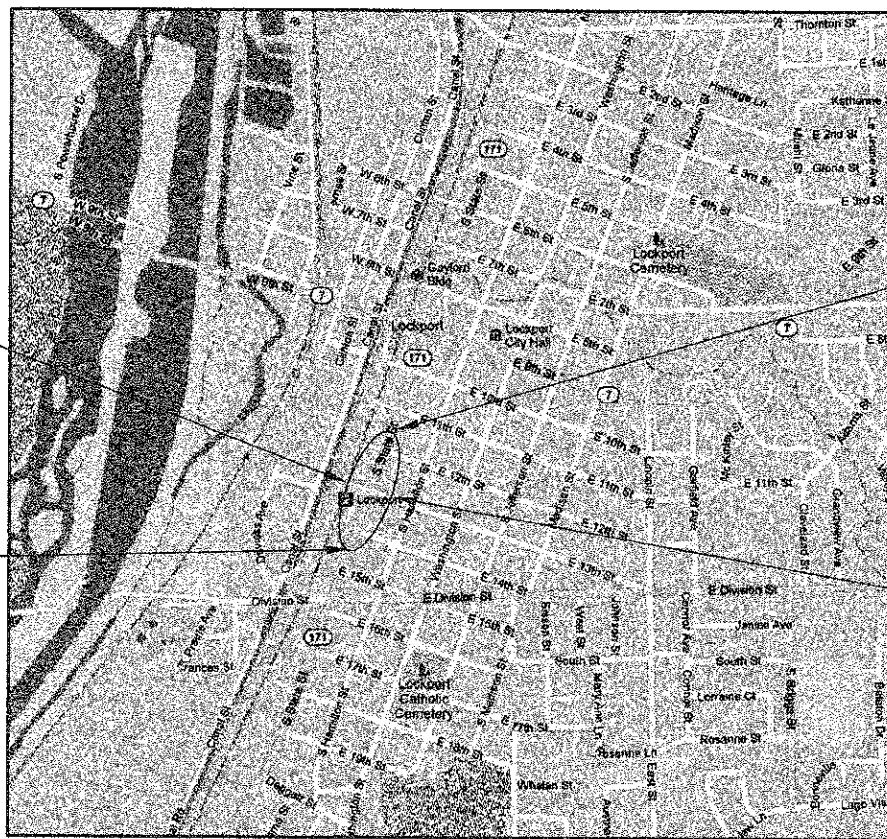


**FOR INDEX OF SHEETS
AND HIGHWAY STANDARDS
SEE SHEET NO. 2**

TRAFFIC DATA	2022 ADT	POSTED/DESIGN SPEED
IL 171	15,134	30/35
13TH STREET	891	NOT POSTED/25

DESIGN DESIGNATION
FAP 577 IL 171
1,347(22) OTHER PRINCIPAL ARTERIAL 3.56(FD-20)
13 STREET
363(22) LOCAL ROAD 0.50(FD-20)

**PROJECT LOCATED
IN THE CITY OF
LOCKPORT**

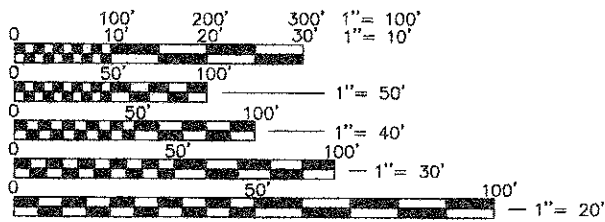


**PROJECT BEGINS
STA. 11+41.00
13TH STREET**

**PROJECT BEGINS
STA. 134+77.00
IL ROUTE 171**

**PROJECT ENDS
STA. 145+05.00
IL ROUTE 171**

**PROJECT ENDS
STA. 14+55.00
13TH STREET**



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

LOCATION MAP
SCALE: 1" = 1000'
IL 171 GROSS & NET LENGTH = 1,028 FEET (0.195 MILE)

**JULIE
JOINT
UTILITY
LOCATION
INFORMATION FOR
EXCAVATION
CALL 811**

Know what's below.
Call before you dig.

CONTRACT NO. 63788

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
Phone: (630) 466-6700

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

APPROVED: Jan 29 2013
CITY OF LOCKPORT, ASSISTANT CITY ENGINEER

PASSED: APRIL 5, 2013 2013
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR
BID BASED OR
LIMITED REVIEW: April 10 2013
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

DATE: 1/25/13

BY: James R. Lenzini
JAMES R. LENZINI

LICENSE EXPIRES: NOVEMBER 30, 2013

SEAL

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IDOT HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALK
424006-01	DIAGONAL CURB RAMPS FOR SIDEWALK
424026-01	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
542606-02	REINFORCED CONCRETE PIPE TEE
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602301-03	INLET - TYPE A
602401-03	MANHOLE TYPE A
602406-05	MANHOLE TYPE A 6' DIAMETER
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-03	FRAME AND LIDS TYPE 1
604056-03	FRAME AND GRATE TYPE 11V
604086-02	FRAME AND GRATE TYPE 23
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606001-05	CONCRETE CURB TYPE B AND COMBINATION CURB AND GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701427-01	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701606-08	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
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701901-02	TRAFFIC CONTROL DEVICES
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862001-01	UNINTERRUPTIBLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
877001-05	STEEL MAST ARM ASSEMBLY AND POLE 16'THROUGH 55'
878001-09	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS

SUPPLEMENTAL LEGEND

SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

	PAVEMENT REMOVAL
	PATCHING
	HOT-MIX ASPHALT SURFACE REMOVAL - VARIABLE DEPTH
	HOT-MIX ASPHALT SURFACE REMOVAL - 3/4"
	EXISTING CONCRETE CURB AND GUTTER, DRIVEWAY PAVEMENT AND SIDEWALK TO BE REMOVED
	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
	EXISTING CURB OR CURB & GUTTER
	PROPOSED CURB OR CURB & GUTTER

SEWER STRUCTURE AND PIPE NOTATION

STRUCTURE TYPE	CB TY C, 2' DIA.	DIAMETER
FRAME & GRATE/LID TYPE	W/ TY 11V FR & GR	
RIM OR TOP OF CURB (TC) ELEVATION	164+03, 14.5' LT	STATION & OFFSET
	TC 674.64	
	INV N,S 670.53, 12"	INVERT DIRECTION, ELEVATION & SIZE
PIPE DIAMETER	12" SS TY 1	SEWER TYPE
MATERIAL	RCCP CL IV	MATERIAL CLASS
LENGTH	7' @ 0.57%	SLOPE
TRENCH BACKFILL QUANTITY	TB 0.3 CY 2'	TRENCH BACKFILL LENGTH

STRUCTURE ADJUSTMENT / REMOVAL NOTATION

"ADJ" FOR ADJUST	ADJ TC	"C" FOR CLOSED
"REC" FOR RECONSTRUCT		"1" FRAME/LID TYPE
⊗ DENOTES STRUCTURE TO BE REMOVED		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, HIGHWAY
STANDARDS AND LEGEND**

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PLOT SCALE =	DRAWN - CLN	REVISED - CLN 03/19/13
PLOT DATE = March 18, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

SCALE: N.T.S.	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	2
CONTRACT NO. 63788			FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-9003(619)	

GENERAL NOTES

SPECIFICATIONS, STANDARDS, AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2012 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS), THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JANUARY 1, 2013, THE LATEST EDITION OF THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, THE STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS, SIXTH EDITION, THE CODES AND ORDINANCES OF THE CITY OF LOCKPORT, ILLINOIS, THE DETAILS IN THE PLANS AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

NO SUBSTITUTIONS OR VARIANCES WILL BE PERMITTED TO ANY STANDARD NOTES OR ORDINANCES UNLESS APPROVED OTHERWISE IN WRITING PRIOR TO COMMENCING CONSTRUCTION ACTIVITY.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS.

UTILITIES

THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, FIELD TILES AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND NOT NECESSARILY COMPLETE; THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THE OWNER'S AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT, UNLESS OTHERWISE NOTED. CURB AND GUTTER ELEVATIONS SHOWN AT POINTS OF CURVE, ETC., ARE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

STRUCTURE OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS ARE TO THE FOLLOWING POINTS:

- A) STRUCTURES FALLING IN THE CURB LINE ARE MEASURED TO THE EDGE OF PAVEMENT
- B) ALL OTHER STRUCTURES ARE MEASURED TO THE CENTER OF THE STRUCTURE

ALL ELEVATIONS ARE ON U.S.G.S. DATUM, NVD 29.

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC., ARE FROM THE CENTERLINE AS SHOWN ON THE PLANS.

SEWERS AND WATER MAINS

ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, IT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN IN AN OPERATING CONDITION TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS, AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES WHICH HAVE THE CAPACITY TO RECEIVE AND DISCHARGE THE STORM WATER FLOW RATES NORMALLY ACCEPTED AND RELEASED BY EXISTING DRAINAGE FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE COST OF INTERCONNECTIONS BETWEEN THE PROPOSED AND EXISTING SEWER SYSTEMS AND PROPOSED AND EXISTING WATER MAIN SYSTEMS SHALL BE INCLUDED IN THE VARIOUS UNIT PRICES ON THE ITEMS BEING CONNECTED. THE COSTS OF PLUGGING ANY EXISTING STORM SEWER CONNECTIONS AS INDICATED ON THE PLANS SHALL BE INCLUDED IN THE COST OF STORM SEWER REMOVAL.

ALL FRAMES, GRATES, OR LIDS SCHEDULED TO BE REMOVED FROM EXISTING STRUCTURES SHALL REMAIN THE PROPERTY OF THE CITY OR STATE, AS APPLICABLE. ANY ITEMS DAMAGED DURING REMOVAL SHALL BE REPLACED BY THE CONTRACTOR AT THEIR OWN EXPENSE. THE COST OF SALVAGING EXISTING FRAMES, GRATES, OR LIDS AND/OR STOCKPILING THEM ON THE JOB SITE FOR PICKUP BY THE CITY OR STATE OR DELIVERY TO THE CITY OR STATE MAINTENANCE YARD SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT FOR ANY SANITARY MANHOLE OR VALVE VAULT SHALL HAVE CAST INTO THE LID: "LOCKPORT" AND ONE OF THE FOLLOWING WORDS: "SANITARY", OR "WATER" AS APPLICABLE. ALL STORM MANHOLE, CATCH BASIN OR INLET FRAMES SHALL HAVE CAST INTO THE LID "STORM". ANY ADDITIONAL COST FOR THIS REQUIREMENT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE FRAME AND CLOSED LID PROVIDED.

FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION AND CROSS SLOPE OF THE AREA IN WHICH THEY ARE LOCATED. ALL FINAL ADJUSTMENTS OF FRAMES WILL BE ACCOMPLISHED BY THE USE OF CONCRETE ADJUSTING RINGS SET IN BUTYL ROPE JOINT SEALANT; MORTAR JOINTS WILL NOT BE ALLOWED. HEIGHT OF ADJUSTING RINGS SHALL NOT EXCEED EIGHT INCHES (8"). THE COST OF THE ADJUSTMENT TO FINAL ELEVATION IS INCLUDED IN THE COST OF THE ITEM CONSTRUCTED.

ALL CLASS A STORM SEWERS SHALL BE RCP CLASS IV, UNLESS NOTED OTHERWISE ON THE PLAN.

ALL CLASS B STORM SEWERS SHALL BE PVC C900 OR C905 AS APPLICABLE, DR 25, UNLESS NOTED OTHERWISE ON THE PLAN.

BACKFILL

ALL TRENCH BACKFILL QUANTITIES FOR STORM SEWER, SANITARY SEWER, AND WATER MAIN HAVE BEEN COMPUTED AND SHALL BE PAID FOR IN ACCORDANCE WITH THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, DIVISION OF HIGHWAYS, BUREAU OF CONSTRUCTION TRENCH BACKFILL TABLE.

STORM SEWER, SANITARY SEWER, AND WATER MAIN SHALL BE BACKFILLED IN ACCORDANCE WITH ARTICLE 550.07, METHOD 1 ONLY, OR AS DIRECTED BY THE ENGINEER, WITH THE FOLLOWING MODIFICATIONS.

INITIAL TRENCH BACKFILL SHALL BE CA-7. FINAL TRENCH BACKFILL SHALL BE GRADATION CA-6. THE FINAL TRENCH BACKFILL SHALL BE PLACED IN 6" LIFTS AND SHALL BE COMPACTED IN PLACE TO NINETY FIVE PERCENT (95%) OF MAXIMUM DENSITY AT OPTIMUM MOISTURE AS DETERMINED BY THE MODIFIED STANDARD PROCTOR TEST.

SIGNS

PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR AND ENGINEER SHALL INVENTORY THE LOCATION, SIZE, TYPE, AND CONDITION OF ALL EXISTING SIGNS. ANY SIGN DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

ALL REMOVED SIGNS WILL BE RETURNED TO THE CITY (17112 S. PRIME BLVD. LOCKPORT, IL) OR STATE, AS APPLICABLE.

MISCELLANEOUS

THE CONTRACTOR SHALL MAINTAIN EXISTING SIDE STREET, DRIVEWAY AND PEDESTRIAN ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING CONSTRUCTION OF THE PROJECT, UNLESS OTHERWISE NOTED IN THE PLANS OR DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE ITEM "AGGREGATE FOR TEMPORARY ACCESS".

SAWING OF REMOVAL ITEMS AS NOTED ON THE PLANS, SPECIFIED IN THE STANDARD SPECIFICATIONS, OR AS REQUIRED BY THE ENGINEER SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

AT ALL BUTT JOINT LOCATIONS, THE EXISTING SURFACE SHALL BE CUT TO A MINIMUM THICKNESS OF ONE AND A HALF (1.5) OR TWO (2) INCHES AS INDICATED ON THE PLANS.

PROTECTIVE COAT SHALL BE APPLIED TO ALL GUTTER FLAGS, FACE AND TOP OF CURB, PCC SIDEWALK, PCC DRIVEWAY PAVEMENT, AND AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE REQUIRED TO MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS, STRUCTURES, POLES, CABLES AND PIPE LINES, BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE ENGINEER AT THEIR OWN EXPENSE. ANY SHEETING AND/OR SHORING USED FOR THIS IMPROVEMENT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS CONTRACT.

EXISTING PAVEMENT THICKNESSES SHOWN ON THE PLANS ARE APPROXIMATE, BASED ON AVAILABLE INFORMATION AT THE TIME OF DESIGN. ANY ADDITIONAL COSTS REQUIRED BY THE CONTRACTOR DUE TO THICKNESSES OTHER THAN THOSE SHOWN ON THE PLANS WILL BE INCLUDED IN THE COST OF THE CONTRACT.

WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, THE CONTRACTOR SHALL FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION. IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES.

ALL DISTURBED AREAS WITHIN THE PROJECT THAT ARE NOT OTHERWISE SURFACED SHALL BE CLEANED, LAYERED WITH TOPSOIL, AND SODDED AS SHOWN IN THE PLANS. LIMITS SHOWN ON THE PLANS ARE THE MAXIMUM PAY WIDTHS FOR PAYMENT PURPOSES. ADDITIONAL AREAS DAMAGED BY MACHINERY, CONSTRUCTION EQUIPMENT, CONTRACTOR NEGLIGENCE OR OVER-EXCAVATION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED AT THE COST OF THE CONTRACTOR.

THE CONTRACTOR SHALL DISPOSE OF AND REMOVE FROM THE SITE EACH DAY ALL CURB AND GUTTER, PAVEMENT AND ALL OTHER EXCAVATED MATERIAL NOT FOR SALVAGE. THE COST FOR HAULING AND TRUCKING TO DISPOSAL LOCATIONS WILL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

THE NEED FOR REMOVAL OF UNSTABLE SOILS AND REPLACEMENT WITH AGGREGATE SUBGRADE IMPROVEMENT WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER (BY USE OF CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE STABILITY MANUAL). IF UNSTABLE SOILS ARE ENCOUNTERED, THE SOILS SHALL BE REMOVED AND REPLACED WITH AGGREGATE SUBGRADE IMPROVEMENT. IF UNSTABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY WILL BE DEDUCTED FROM THE CONTRACT WITH NO ADDITIONAL COMPENSATION PAID TO THE CONTRACTOR. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION SHALL BE PLACED AT ALL LOCATIONS REQUIRING REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AND AGGREGATE SUBGRADE IMPROVEMENT AS APPROVED BY THE ENGINEER.

THE ENGINEER IS NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF THEIR WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.

BITUMINOUS MATERIALS (PRIME COAT) SHALL BE APPLIED AT A RATE OF 0.1 GALLONS PER SQUARE YARD ON ASPHALT AND 0.5 GALLONS PER SQUARE YARD ON AGGREGATE. BITUMINOUS MATERIALS SHALL BE SS-1 ON ASPHALT AND MC-30 ON AGGREGATE.

AGGREGATE (PRIME COAT) SHALL BE MECHANICALLY SPREAD AT A UNIFORM RATE OF 4 POUNDS PER SQUARE YARD.

DRIVEWAY PAVEMENT REMOVAL SHALL INCLUDE REMOVAL OF ALL EXISTING MATERIAL (WHETHER ASPHALT, CONCRETE, STONE, OR EARTH) TO THE DEPTH REQUIRED FOR INSTALLATION OF THE NEW DRIVEWAY.

PAVEMENT WIDENING AREAS FOUR FOOT AND LESS IN WIDTH, ADJACENT TO PROPOSED CURB AND GUTTER, SHALL BE BACKFILLED WITH CLASS SI CONCRETE AND HAVE AN HMA SURFACE COURSE AS SHOWN IN THE SPECIAL DETAIL. THE CLASS SI CONCRETE WILL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE ENGINEER SHALL CONTACT CORY JUICIUS, ARTERIAL TRAFFIC OPERATIONS ENGINEER AT (847) 705-4411 A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

CONTRACTOR TO VERIFY ROCK DEPTH PRIOR TO INSTALLATION OF BELOW GRADE ITEMS AND NOTIFY ENGINEER IMMEDIATELY OF ANY CONFLICTS WITH PROPOSED INSTALLATIONS. GEOTECHNICAL REPORT IS AVAILABLE AT ENGINEER'S OFFICE.

CONTRACTOR TO CONTACT GROUNDWATER & ENVIRONMENTAL SERVICES, INC. (G.E.S.) (TIM MARKS 866-455-2419, EXT. 4037) PRIOR TO ANY DISTURBANCE NEAR EXISTING MONITORING WELLS. CONTRACTOR SHALL PROTECT EXISTING MONITORING WELLS TO REMAIN. THE PROTECTION OF THE EXISTING MONITORING WELLS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT. ALL WORK INVOLVING ADJUSTMENT OR ABANDONMENT OF EXISTING MONITORING WELLS WILL BE PERFORMED BY OTHERS.

THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ANY RESIDENT OR BUSINESS OF ANY REMOVAL AND REPLACEMENT ACTIVITIES THAT WILL INHIBIT OR PROHIBIT ACCESS TO THEIR DRIVEWAY, IN WRITING, A MINIMUM OF 48 HOURS BUT NOT MORE THAN 72 HOURS, PRIOR TO THE COMMENCEMENT OF THESE ACTIVITIES. THE MORNING OF THE WORK, THE CONTRACTOR SHALL AGAIN NOTIFY THE OWNER VERBALLY, TO ALLOW THE OWNER TIME TO MOVE THEIR VEHICLE SO AS NOT TO PROHIBIT THE VEHICLE FROM LEAVING THE DRIVEWAY UPON REMOVAL OF ANY MATERIAL. THE NOTICE GIVEN OUT BY THE CONTRACTOR SHALL PROVIDE INFORMATION REGARDING THE ANTICIPATED DATE THAT FULL ACCESS WILL BE RESTORED. COORDINATION BETWEEN ACTIVITIES SHOULD ALLOW ALL WORK TO BE DONE IN A TIMELY MANNER SO AS TO PERMIT ACCESS TO THE ROADWAY. ANY ADDITIONAL COST OF STAGING REQUIRED TO MAINTAIN ACCESS IS CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH THE ILLINOIS CENTRAL RAILROAD WHENEVER CONSTRUCTION ACTIVITIES IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE ILLINOIS CENTRAL RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT-OF-WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05

AT LOCATIONS SHOWN ON THE PLAN OR WHERE CONDITIONS WARRANT THE USE OF RADIAL DETECTABLE WARNING TILES, THE CONTRACTOR SHALL VERIFY WITH THE TILE MANUFACTURER THAT RADIAL TILES WITH RADII MATCHING THE CURB RADIUS ARE AVAILABLE.

Printed: March 19, 2013 @ 2:27 PM By: Larry Nolan - Tab: 03 Notes 22-34
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PLOT SCALE =	DRAWN - CLN	REVISED - CLN 03/19/13
PLOT DATE = March 19, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	3
CONTRACT NO. 63788			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-900316199	

SUMMARY OF QUANTITIES

SPECIAL PROVISIONS SPECIALTY ITEMS	CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	FEDERAL - 80%					
					STATE - 10%		CITY - 100%			
					ROADWAY	TRAFFIC SIGNAL	TRAINEES	EMERGENCY VEHICLE PREEMPTION	TREES	
					0004	0021	0042	0021	0031	
	20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	134	134					
	20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	88	88					
	20101200	TREE ROOT PRUNING	EACH	4	4					
A	20200100	EARTH EXCAVATION	CU YD	2,560	2,560					
	20200200	ROCK EXCAVATION	CU YD	100	100					
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	280	280					
	20800150	TRENCH BACKFILL	CU YD	120	120					
	21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	550	550					
*	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	2,155	2,155					
	21301072	EXPLORATION TRENCH 72" DEPTH	FOOT	250	250					
*	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	27	27					
*	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	27	27					
*	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	27	27					
*	25200110	SODDING, SALT TOLERANT	SQ YD	2,155	2,155					
*	25200200	SUPPLEMENTAL WATERING	UNIT	97	97					
	28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	45	45					
	28000400	PERIMETER EROSION BARRIER	FOOT	1,130	1,130					
	28000510	INLET FILTERS	EACH	24	24					
A	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	280	280					
A	30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	2,741	2,741					
	35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	572	572					
	35101700	AGGREGATE BASE COURSE, TYPE B 5"	SQ YD	268	268					
	35501305	HOT-MIX ASPHALT BASE COURSE, 5 1/4"	SQ YD	482	482					
	35501307	HOT-MIX ASPHALT BASE COURSE, 5 3/4"	SQ YD	114	114					
	35501318	HOT-MIX ASPHALT BASE COURSE, 6 1/2"	SQ YD	1,314	1,314					
A	40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	190	190					
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1,779	1,779					
	40800300	AGGREGATE (PRIME COAT)	TON	14	14					
	40800635	LEVELING BINDER (MACHINE METHOD), N70	TON	280	280					
	40800982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	115	115					
	40803340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	795	795					
	42001300	PROTECTIVE COAT	SQ YD	1,341	1,341					
	42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	257	257					
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2,810	2,810					
	42400400	PORTLAND CEMENT CONCRETE SIDEWALK 7 INCH	SQ FT	605	605					
	42400800	DETECTABLE WARNINGS	SQ FT	230	230					
	44000100	PAVEMENT REMOVAL	SQ YD	708	708					
	44000152	HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"	SQ YD	450	450					
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	848	848					
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	2,617	2,617					
	44000600	SIDEWALK REMOVAL	SQ FT	4,750	4,750					
	44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	23	23					
	44201773	CLASS D PATCHES, TYPE I, 11 INCH	SQ YD	10	10					
	44201777	CLASS D PATCHES, TYPE II, 11 INCH	SQ YD	12	12					

SPECIAL PROVISIONS SPECIALTY ITEMS	CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	FEDERAL - 80%					
					STATE - 10%		CITY - 100%			
					ROADWAY	TRAFFIC SIGNAL	TRAINEES	EMERGENCY VEHICLE PREEMPTION	TREES	
					0004	0021	0042	0021	0031	
	44201783	CLASS D PATCHES, TYPE IV, 11 INCH	SQ YD	188	188					
	44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	2,478	2,478					
A	50200400	ROCK EXCAVATION FOR STRUCTURES	CU YD	13	13					
	550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	106	106					
	550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	70	70					
	550A2320	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 12"	FOOT	182	182					
	550A2330	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 15"	FOOT	100	100					
	550A2360	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 24"	FOOT	35	35					
	550B0050	STORM SEWERS, CLASS B, TYPE 1 12"	FOOT	52	52					
	550B0120	STORM SEWERS, CLASS B, TYPE 1 24"	FOOT	295	295					
	85100400	STORM SEWER REMOVAL 10"	FOOT	10	10					
	85100500	STORM SEWER REMOVAL 12"	FOOT	119	119					
	85100700	STORM SEWER REMOVAL 15"	FOOT	71	71					
	85101200	STORM SEWER REMOVAL 24"	FOOT	10	10					
A *	56400300	FIRE HYDRANTS TO BE ADJUSTED	EACH	1	1					
	60107700	PIPE UNDERDRAINS 6"	FOOT	160	160					
	60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	12	12					
	60203040	CATCH BASINS, TYPE A, 6'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	1	1					
	60208230	CATCH BASINS, TYPE C, TYPE 23 FRAME AND GRATE	EACH	1	1					
	60208240	CATCH BASINS, TYPE C, TYPE 24 FRAME AND GRATE	EACH	1	1					
	60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1					
	60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3					
	60224039	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	3	3					
	60236625	INLETS, TYPE A, TYPE 11V FRAME AND GRATE	EACH	1	1					
	60237470	INLETS, TYPE A, TYPE 24 FRAME AND GRATE	EACH	4	4					
	60255500	MANHOLES TO BE ADJUSTED	EACH	3	3					
	60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	3	3					
	60258200	MANHOLES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	2	2					
	60260100	INLETS TO BE ADJUSTED	EACH	1	1					
	60261320	INLETS TO BE ADJUSTED WITH NEW TYPE 11V FRAME AND GRATE	EACH	1	1					
	60261540	INLETS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	2	2					
	60263900	INLETS TO BE RECONSTRUCTED WITH NEW TYPE 11 FRAME AND GRATE	EACH	2	2					
	60264140	INLETS TO BE RECONSTRUCTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	1	1					
	60265700	VALVE VAULTS TO BE ADJUSTED	EACH	1	1					
	60265900	VALVE VAULTS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	4	4					
	60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	2	2					
	60500040	REMOVING MANHOLES	EACH	2	2					
	60500050	REMOVING CATCH BASINS	EACH	1	1					
	60500060	REMOVING INLETS	EACH	12	12					
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	179	179					
	60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	125	125					
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	2,248	2,248					
A *	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	175	175					
A *	66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1					

Printed: May 7, 2013 @ 8:15 AM By: Larry Nolan - Tab: 04 Sum of Quant 22-34

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USER NAME = Larry Nolan	DESIGNED - JRL & SWM	REVISED - JPS 02/08/13
PLOT SCALE =	DRAWN - CLN	REVISED -
PLOT DATE = May 7, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 4
CONTRACT NO. 63788				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90036197				

SUMMARY OF QUANTITIES

SPECIAL PROVISIONS	SPECIALTY ITEMS	CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	FEDERAL - 80%				
						STATE - 10%				
						CITY - 10%				
						ROADWAY	TRAFFIC SIGNAL	TRAINEES	EMERGENCY VEHICLE PREEMPTION	TREES
0004	0021	0042	0021	0031						
	*	66900530	SOIL DISPOSAL ANALYSIS	EACH	5	5				
		67100100	MOBILIZATION	L SUM	1	1				
		70106800	CHANGEABLE MESSAGE SIGN	CAL MO	8	8				
		70900100	SHORT TERM PAVEMENT MARKING	FOOT	1,620	1,620				
		70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	36	36				
		70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	8,728	8,728				
		70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	115	115				
		70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	135	135				
		70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	160	160				
		70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	593	593				
		72000100	SIGN PANEL - TYPE 1	SQ FT	59	59				
		72000200	SIGN PANEL - TYPE 2	SQ FT	23	23				
		72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	4	4				
		72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	15	15				
		72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	105	105				
	*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	207	207				
	*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4,549	4,549				
	*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,569	1,569				
	*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	267	267				
	*	78000850	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	262	262				
	*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	92	92				
		78300100	PAVEMENT MARKING REMOVAL	SQ FT	3,273	3,273				
		78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	80	80				
	*	80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	1	1				
	*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	2,774	2,774				
	*	81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	31	31				
	*	81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	92	92				
	*	81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	380	380				
	*	81400100	HANDHOLE	EACH	8	8				
	*	81400200	HEAVY-DUTY HANDHOLE	EACH	4	4				
	*	81400300	DOUBLE HANDHOLE	EACH	3	3				
	*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2	2				
	*	86400100	TRANSCEIVER - FIBER OPTIC	EACH	2	2				
	*	87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2,585	2,585				
	*	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1,289	1,289				
	*	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,592	1,345		247		
	*	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1,130	1,130				
	*	87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1,467	1,467				
	*	87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2,562	2,562				
	*	87301750	ELECTRIC CABLE IN CONDUIT, RAILROAD, NO. 14 3C	FOOT	280	280				
	*	87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	82	82				
	*	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	694	694				
	*	87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2	2				
	*	87700180	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.	EACH	1	1				

SPECIAL PROVISIONS	SPECIALTY ITEMS	CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	FEDERAL - 80%				
						STATE - 10%				
						CITY - 10%				
						ROADWAY	TRAFFIC SIGNAL	TRAINEES	EMERGENCY VEHICLE PREEMPTION	TREES
0004	0021	0042	0021	0031						
	*	87700190	STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.	EACH	1					
	*	87700270	STEEL MAST ARM ASSEMBLY AND POLE, 46 FT.	EACH	2					
	*	87800100	CONCRETE FOUNDATION, TYPE A	FOOT	8					
	*	87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4					
	*	87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	10					
	*	87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	37					
	*	87900100	DRILL EXISTING FOUNDATION	EACH	1					
	*	87900200	DRILL EXISTING HANDHOLE	EACH	2					
	*	88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	6					
	*	88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4					
	*	88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4					
	*	88102710	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	2					
	*	88102740	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED	EACH	3					
	*	88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	10					
	*	88500100	INDUCTIVE LOOP DETECTOR	EACH	10					
	*	88600100	DETECTOR LOOP, TYPE I	FOOT	518					
	*	88700200	LIGHT DETECTOR	EACH	2				2	
	*	88700300	LIGHT DETECTOR AMPLIFIER	EACH	1					1
	*	88800100	PEDESTRIAN PUSH-BUTTON	EACH	8					
	*	A2000820	TREE, ACER PLATANOIDES CRIMSON KING, (CRIMSON KING NORWAY MAPLE), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	6	1				5
	*	A2006720	TREE, QUERCUS MACROCARPA (BUR OAK), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	6	2				4
	*	A2008120	TREE, TILIA CORDATA GREENSPIRE (GREENSPIRE LITTLE LEAF LINDEN), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	6	1				5
	Δ	X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	247					247
	Δ	X4240460	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH, SPECIAL	SQ FT	872	872				
	Δ	X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	5,224	5,224				
	Δ	X5422015	REINFORCED CONCRETE PIPE TEE, SPECIAL	EACH	1	1				
	Δ	X6024246	INLETS, SPECIAL, NO. 3	EACH	1	1				
	Δ	X6026058	SANITARY MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	8	8				
	Δ	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1				
	Δ	X8571215	RAILROAD, FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1				1	
	Δ	X8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1				1	
	Δ	X8710024	FIBER OPTIC CABLE IN CONDUIT, NO. 62 5/125, MM12F SM24F	FOOT	2,611				2,611	
	Δ	XX007180	SANITARY MANHOLES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID, (SPECIAL)	EACH	1	1				
	Δ	XX007940	INSULATION	SQ FT	575	575				
	Δ	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1				
	Δ	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	147	147				
	Δ	Z0033048	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	2				2	
	Δ	Z0048685	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1				
	Δ	Z0066500	STABILIZED DRIVEWAYS 8"	SQ YD	302	302				
	Δ	Z0066700	STABILIZED DRIVEWAYS 10"	SQ YD	124	124				
	Δ	Z0075600	TRAINEES	HOUR	500				500	
	Δ	Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500				500	

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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION CONTRACT NO. 63788

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 CONSULTING ENGINEERS
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 630.466.5700 / www.eesweb.com

USER NAME = Larry Nolon
 DESIGNED - JRL & SWM
 DRAWN - CLN
 CHECKED - JL
 DATE - 11/13/12

REVISED - JPS 02/08/13
 REVISED - CLN 03/19/13
 REVISED -
 REVISED -

PLOT SCALE =
 PLOT DATE = March 20, 2013

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES
 SCALE: N.T.S. SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.P. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)			CONTRACT NO. 63788	

- EXISTING LEGEND**
- (A) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
 - (B) EXISTING AGGREGATE BASE
 - (C) EXISTING CONCRETE PAVEMENT
 - (D) EXISTING HOT-MIX ASPHALT PAVEMENT
 - (E) EXISTING COMBINATION CONCRETE CURB & GUTTER, VARIOUS TYPES
 - (F) EXISTING COMBINATION CONCRETE CURB & GUTTER, (TO BE REMOVED)
 - (G) HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
 - (H) HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
 - (I) EARTH EXCAVATION

- PROPOSED LEGEND**
- (1) AGGREGATE SUBGRADE, 12"
 - (2) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - (3) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - (4) HOT-MIX ASPHALT BASE COURSE, 8 1/2"
 - (5) HOT-MIX ASPHALT BASE COURSE, 5 1/4"
 - (6) LEVELING BINDER
 - (7) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
 - (8) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
 - (9) BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
 - (10) PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" AT DRIVEWAYS)
 - (11) AGGREGATE BASE COURSE, TYPE B, 4"
 - (12) TOPSOIL, 4"; SODDING; FERTILIZER
 - (13) STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - (14) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
 - (15) HOT-MIX ASPHALT BASE COURSE, 5 3/4"

PAVEMENT DESIGN INFORMATION

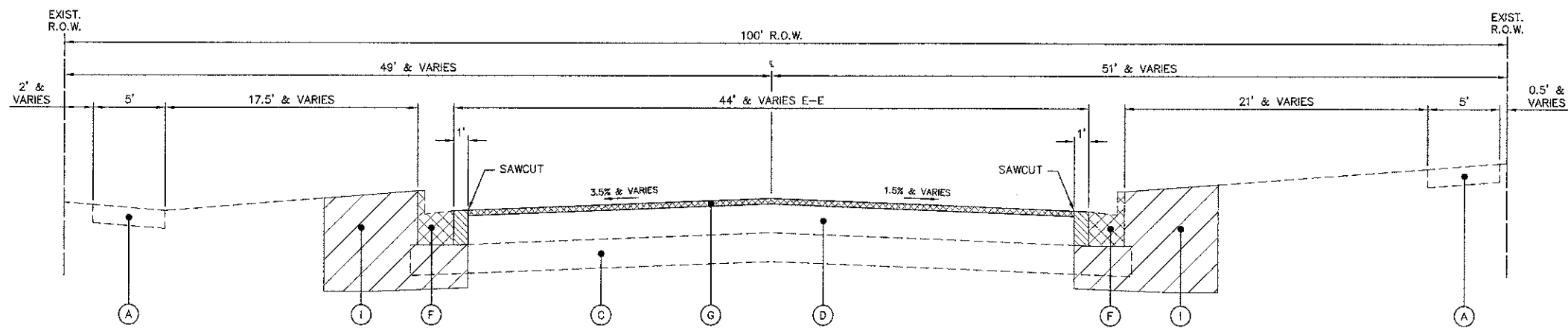
IL. ROUTE 171
HOT-MIX ASPHALT PAVEMENT
CLASS I
80,000 LB
FOUR LANE URBAN
2022 ADT 15,134
PV 14,937 (98.7%)
SU 91 (0.6%)
MU 106 (0.7%)
TF = 0.58 (ACTUAL)
TF = 3.56 (USED)
SSR POOR
AC MIX TEMP 75°
PG 64-22
MODULUS 690 KSI
MICROSTRAIN 84
THICKNESS REQUIRED = 10 1/2"
THICKNESS PROVIDED = 10 1/2"

PATCHING SHALL BE PERFORMED BEFORE MILLING

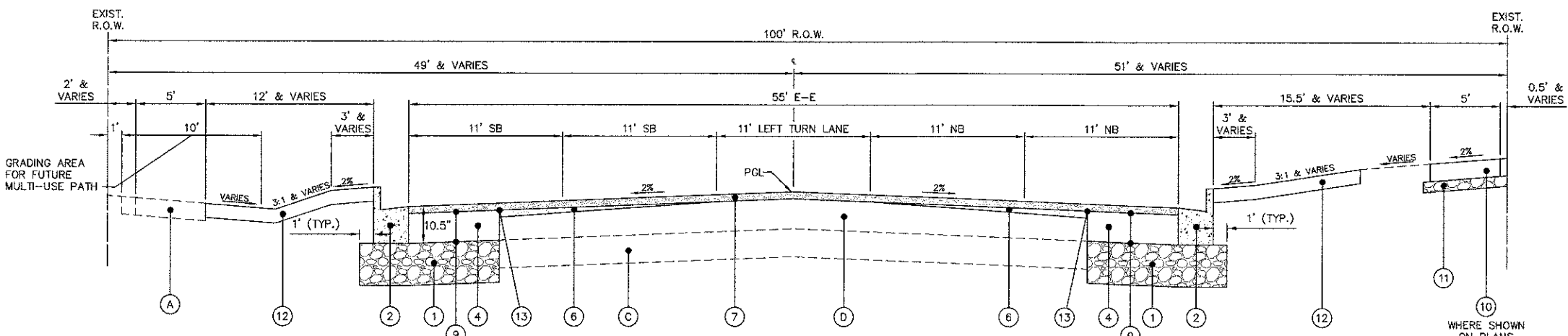
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

OPERATION	MIXTURE TYPE	AIR VOIDS @ N ₉₅
IL 171 WIDENING (STA 134+82 TO STA 145+00)	HOT-MIX ASPHALT BASE COURSE, 8 1/2" HMA BINDER COURSE, IL-19.0 mm, 8 1/2" (IN 3 LIFTS)	4% @ 70 Gyr.
IL 171 RESURFACING (STA 134+77 TO STA 145+05)	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" (IL 9.5 mm) LEVELING BINDER (MACHINE METHOD), N70, VARIES (IL 9.5 mm)	4% @ 70 Gyr. 4% @ 70 Gyr.
13TH STREET WIDENING (STA 11+41 TO STA 13+22.5)	HOT-MIX ASPHALT BASE COURSE, 5 1/4" HMA BINDER COURSE, IL-19.0 mm, 5 1/4" (IN 2 LIFTS)	4% @ 70 Gyr.
13TH STREET RESURFACING (STA 11+41 TO STA 13+22.5)	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2" (IL 9.5 mm) LEVELING BINDER (MACHINE METHOD), N70, VARIES (IL 9.5 mm)	4% @ 70 Gyr. 4% @ 70 Gyr.
13TH STREET RECONSTRUCTION (STA 13+77.5 TO STA 14+50)	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" (IL 9.5 mm) HMA BINDER COURSE, IL-19.0 mm, 5 1/4" (IN 2 LIFTS)	4% @ 70 Gyr. 4% @ 70 Gyr.
DRIVEWAY RECONSTRUCTION	STABILIZED DRIVEWAY HMA SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 3"	4% @ 60 Gyr. 4% @ 70 Gyr.
PATCHING	CLASS D PATCHES (HMA BINDER IL-19.0 mm), 10.5" (IN 3 LIFTS) CLASS D PATCHES (HMA BINDER IL-19.0 mm), 5" (IN 2 LIFTS)	4% @ 70 Gyr. 4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/INCH.
THE "AC TYPE" FOR HMA MIXES SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR "PERCENT OF RAP" SEE SPECIAL PROVISIONS



EXISTING SECTION
IL ROUTE 171 STA 134+82 TO STA 145+00
(N.T.S.)



PROPOSED SECTION NO. 1
IL ROUTE 171 STA 134+82 TO STA 145+00
(N.T.S.)

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USER NAME =	Larry Nolan
PLOT SCALE =	
PLOT DATE =	February 7, 2013

DESIGNED -	JRL & SWM
DRAWN -	CLN
CHECKED -	JL
DATE -	11/13/12

REVISED -	JPS 02/08/13
REVISED -	
REVISED -	
REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL ROUTE 171 TYPICAL SECTIONS

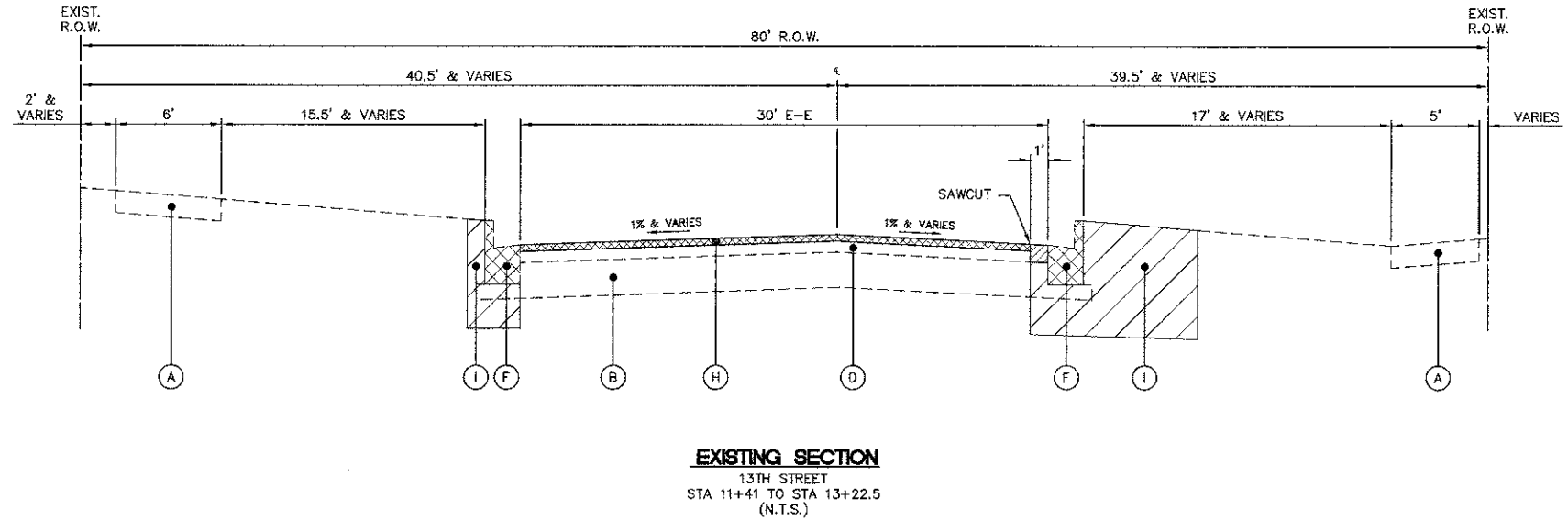
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	6
CONTRACT NO. 63788				
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT W-90036191				

Path: H:\ISD\PROJ\10068\10068-TYPICAL_FINAL_ENG\10068-TYPICAL_FINAL_ENG.dwg

PAVEMENT DESIGN INFORMATION

13TH STREET
 HOT-MIX ASPHALT PAVEMENT
 CLASS III
 80,000 LB
 TWO LANE URBAN
 2022 ADT 891
 PV 888 (99.7%)
 SU 3 (0.3%)
 MU 0 (0.0%)
 TF = 0.01 (ACTUAL)
 TF = 0.50 (USED)
 SSR POOR
 AC MIX TEMP 75'
 PG 64-22
 MODULUS 690 KSI
 MICROSTRAIN 145
 THICKNESS REQUIRED = 7 1/4"
 THICKNESS PROVIDED = 7 1/4"

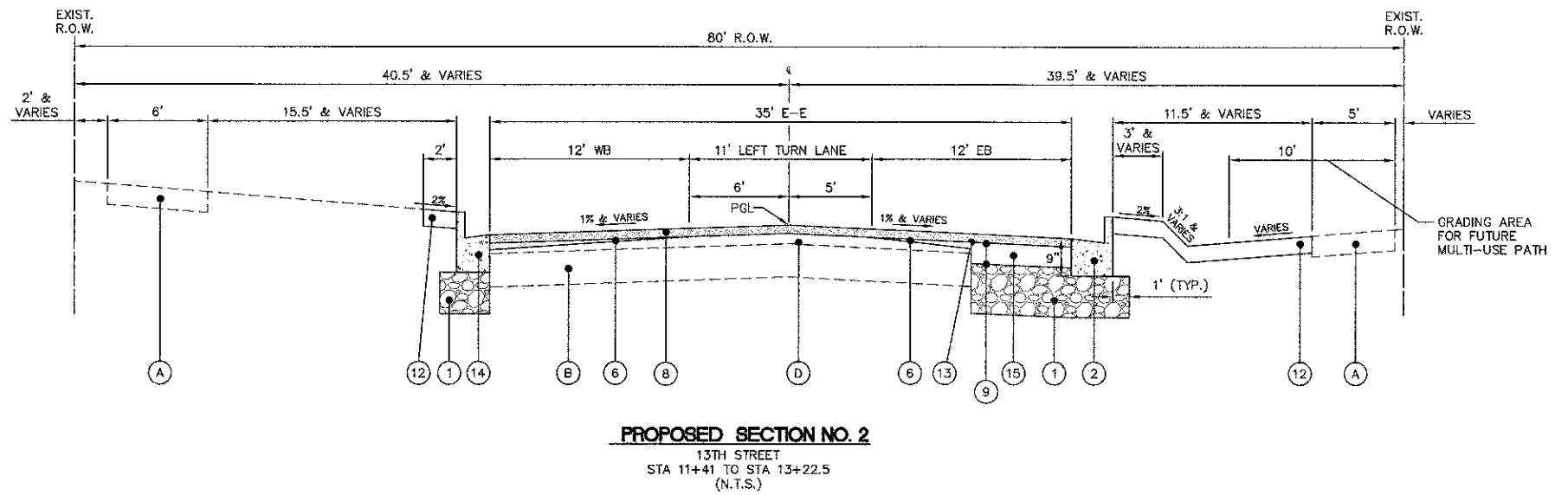


EXISTING LEGEND

- (A) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (B) EXISTING AGGREGATE BASE
- (C) EXISTING CONCRETE PAVEMENT
- (D) EXISTING HOT-MIX ASPHALT PAVEMENT
- (E) EXISTING COMBINATION CONCRETE CURB & GUTTER, VARIOUS TYPES
- (F) EXISTING COMBINATION CONCRETE CURB & GUTTER, (TO BE REMOVED)
- (G) HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- (H) HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- (I) EARTH EXCAVATION

PROPOSED LEGEND

- (1) AGGREGATE SUBGRADE, 12"
- (2) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (3) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (4) HOT-MIX ASPHALT BASE COURSE, 8 1/2"
- (5) HOT-MIX ASPHALT BASE COURSE, 5 1/4"
- (6) LEVELING BINDER
- (7) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (8) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- (9) BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
- (10) PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" AT DRIVEWAYS)
- (11) AGGREGATE BASE COURSE, TYPE B, 4"
- (12) TOPSOIL, 4"; SODDING; FERTILIZER
- (13) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (14) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (15) HOT-MIX ASPHALT BASE COURSE, 5 3/4"



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USER NAME = Larry Nolan	DESIGNED - JRL & SWM	REVISED - JPS 02/08/13
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PLOT DATE = February 7, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

13TH STREET - WEST LEG TYPICAL SECTIONS

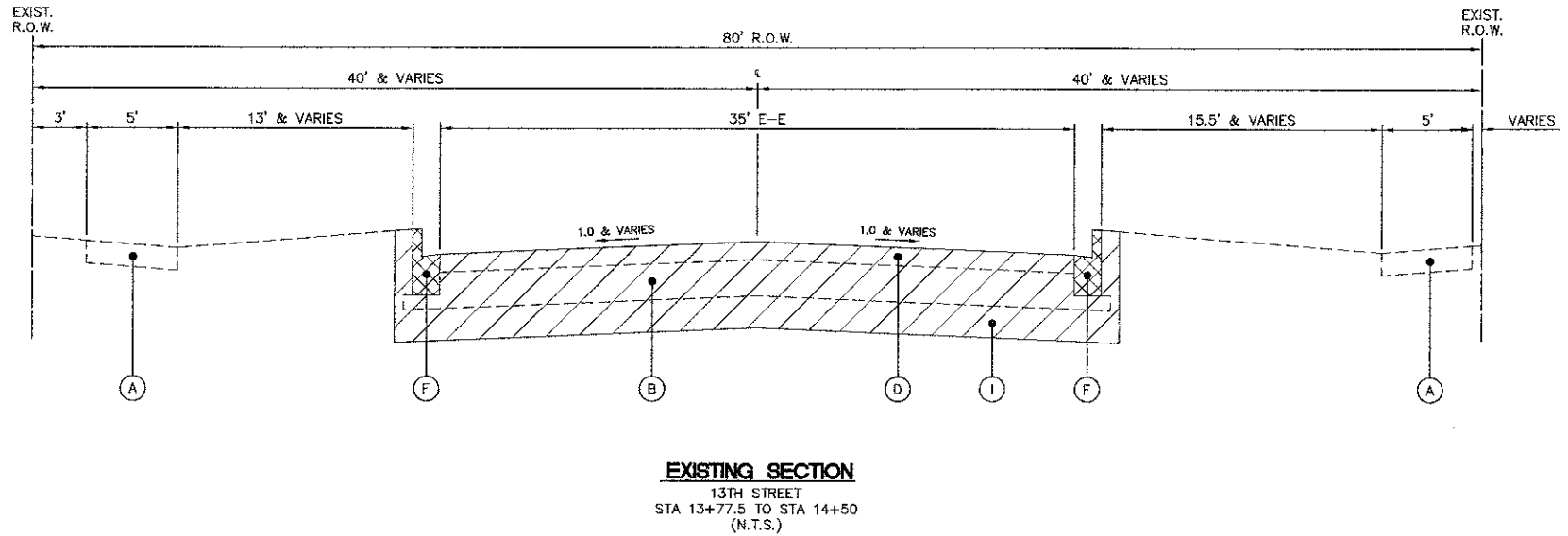
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F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 7
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63788	
FED. AID PROJECT M-9003619				

Path: H:\SDSR\PROJ\109602\DWG\DWG_FINAL_FNC\109602-CVR

PAVEMENT DESIGN INFORMATION

13TH STREET
 HOT-MIX ASPHALT PAVEMENT
 CLASS III
 80,000 LB
 TWO LANE URBAN
 2022 ADT 891
 PV 888 (99.7%)
 SU 3 (0.3%)
 MU 0 (0.0%)
 TF = 0.01 (ACTUAL)
 TF = 0.50 (USED)
 SSR POOR
 AC MIX TEMP 75°
 PG 64-22
 MODULUS 690 KSI
 MICROSTRAIN 145
 THICKNESS REQUIRED = 7 1/4"
 THICKNESS PROVIDED = 7 1/4"

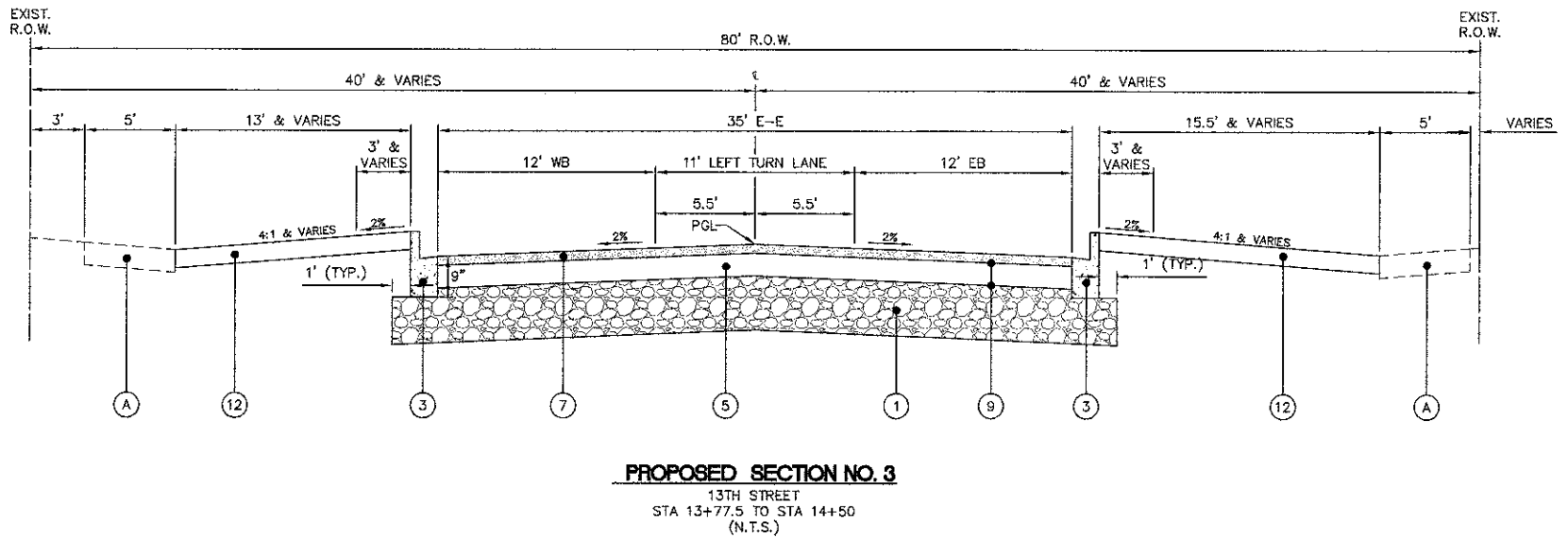


EXISTING LEGEND

- (A) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (B) EXISTING AGGREGATE BASE
- (C) EXISTING CONCRETE PAVEMENT
- (D) EXISTING HOT-MIX ASPHALT PAVEMENT
- (E) EXISTING COMBINATION CONCRETE CURB & GUTTER, VARIOUS TYPES
- (F) EXISTING COMBINATION CONCRETE CURB & GUTTER, (TO BE REMOVED)
- (G) HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- (H) HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- (I) EARTH EXCAVATION

PROPOSED LEGEND

- (1) AGGREGATE SUBGRADE, 12"
- (2) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (3) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (4) HOT-MIX ASPHALT BASE COURSE, 8 1/2"
- (5) HOT-MIX ASPHALT BASE COURSE, 5 1/4"
- (6) LEVELING BINDER
- (7) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (8) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- (9) BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
- (10) PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" AT DRIVEWAYS)
- (11) AGGREGATE BASE COURSE, TYPE B, 4"
- (12) TOPSOIL, 4"; SODDING; FERTILIZER
- (13) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (14) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (15) HOT-MIX ASPHALT BASE COURSE, 5 3/4"



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USER NAME =	Larry Nolan
DESIGNED -	JRL & SWM
DRAWN -	CLN
CHECKED -	JL
PLotted -	February 7, 2013

DESIGNED -	JRL & SWM	REVISED -	JPS 02/08/13
DRAWN -	CLN	REVISED -	
CHECKED -	JL	REVISED -	
DATE -	11/13/12	REVISED -	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

13TH STREET - EAST LEG TYPICAL SECTIONS

SCALE: N.T.S. SHEET NO. 3 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	8
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)			CONTRACT NO. 63788	

Plotted: February 7, 2013 @ 2:52 PM By: Larry Nolan - Tab: 08 Typ: Section 22x34

Path: S:\WORK\10-00068-TL\10-00068-TL\DWG\13TH ST EAST LEG TYPICAL SECTIONS - CR

STORM SEWER AND STORM SEWER STRUCTURES																									
STATION	OFFSET	LT./RT.	CB TC T23F&G EACH	CB TC T24F&G EACH	CB TA 4 DIA T24F&G EACH	CB TA 5 DIA T24F&G EACH	MAN TA 4 DIA T1F CL EACH	MAN TA 5 DIA T1F CL EACH	MAN TA 6 DIA T24F&G EACH	INLETS TA T1V F&G EACH	INLETS TA T24F&G EACH	R C PIPE TEE SPL EACH	INLETS SPL N3 EACH	PIPE DIA INCH	LENGTH FOOT	TRENCH BACKFILL CU YD	STORM SEW CL A 1 12 FOOT	STORM SEW CL A 1 15 FOOT	SS RG CL A 1 12 FOOT	SS RG CL A 1 15 FOOT	SS RG CL A 1 24 FOOT	STORM SEW CL B 1 12 FOOT	STORM SEW CL B 1 24 FOOT	PIPE UNDERDRAINS 6 FOOT	
12+24	16.00	RT					1																		
12+24	18.00	LT	1											12	34.0	5.9				34					
12+37	17.00	RT											1	12	10.0	2.4	10								
13+87.7	32.60	RT		1										12	12.0	2.6						12			
13+98	20.00	RT										1													
14+24.8	18.37	RT				1								24	36.0	6.1									
14+24.8	17.50	LT								1				12	37.0	9.5	37								
135+80.9	32.10	RT			1									12	10.0	0.4	10								
136+14.8	32.40	RT			1									12	34.0	5.9				34					
137+30	27.50	LT							1																
137+30	27.50	RT			1									12	57.0	7.5				57					
138+50	27.50	LT							1																
138+50	27.50	RT			1									12	57.0	7.5				57					
139+46.1	27.85	LT								1															
139+46.3	27.96	RT																							
140+30.1	39.77	RT			1									12	21.0	4.5	21								
140+30.1	22.00	LT								1				12	30.0	4.0						30			
140+58	27.50	RT			1									24	80.0	9.3							80		
140+58	27.50	LT			1									15	60.0	5.8				50					
140+58	27.50	RT			1									15	6.0	0.9			6						
140+58	27.50	RT			1									12	10.0	3.2	10								
140+58	27.50	LT			1									12	10.0	1.7	10								
141+66	21.50	LT							1					24	108.0	12.0								108	
141+66	27.50	LT			1									15	7.0	1.0			7						
141+66	27.50	RT			1									15	50.0	6.6			50						
142+73	21.50	LT								1				24	107.0	12.4								107	
142+73	27.50	RT			1									15	50.0	6.6				50					
142+73	27.50	LT			1									15	7.0	1.0			7						
143+69.2	25.50	LT			1									12	10.0	1.9						10			
143+71.4	30.50	RT			1									12	10.0	1.2	10								
TOTAL			1	1	15	1	1	3	3	1	1	1	1	6	0.0	0.0	120	108	70	182	100	35	52	295	160

FOR STORM SEWERS: STATION SHOWN IS FOR THE UPSTREAM END OF THE PIPE

PAVEMENT MARKING																			
		THERMOPLASTIC PAVEMENT MARKINGS						RAISED REFL PAVT MKR EACH	TEMP PVT MK LTR & SYM SQ FT	TEMP PVT MK LINE 4		TEMP PVT MK LINE 6 FOOT	TEMP PVT MK LINE 24 FOOT	PAVT MARKING REMOVAL SQ FT	RAISED REF PVT MK REM EACH	WORK ZONE PAVT MK REM SQ FT	PAVT MARK TAPE T3 4 FOOT	SHORT TERM PAVT MKING FOOT	
STATION	STATION	4" YELLOW FOOT	6" WHITE FOOT	12" WHITE FOOT	24" WHITE FOOT	LETTERS & SYMBOLS WHITE SQ FT	YELLOW FOOT			WHITE FOOT									
133+30	140+00	1,670	315	280	100	58	36.4	47	0.0	2,260	1,648	0	50	1,403	40	233	80	620	
140+00	146+60	1,570	480	379	135	80	36.4	45	0.0	2,240	1,600	0	50	1,380	40	227	80	600	
11+41	13+50	334	0	630	32	88	97.6	0	36.4	640	340	115	35	491	0	77	0	230	
13+50	14+55	200	0	300	0	36	38.4	0	0.0	0	0	0	0	0	0	57	0	170	
TOTALS		3,774	775	1,589	267	262	207	92	36	5,140	3,588	115	135	3,273	80	593	160	1,620	

LANDSCAPING								
STATION	STATION	NITROGEN FERT NUTR POUND	PHOSPHORUS FERT NUTR POUND	POTASSIUM FERT NUTR POUND	SODDING SALT TOLERANT SQ YD	TOPSOIL F & P 4 SQ YD	TEMP EROS CONTR SEED POUND	PERIMETER EROS BAR FOOT
IL ROUTE 171 (STATE STREET)								
134+80	140+00	17	17	17	1,360	1,360	28	535
140+00	145+05	8	8	8	610	610	13	470
13TH STREET								
11+41	13+50	1	1	1	85	85	2	125
13+50	14+55	1	1	1	100	100	2	0
TOTAL		27	27	27	2,155	2,155	45	1,130

DRIVEWAY SCHEDULE									
STATION	L/R	TYPE	PE/CE	DRIVE PAVEMENT REM SQ YD	AGG BASE CSE B 5 SQ YD	PCC DRIVEWAY PAVT 7 SQ YD	STAB DRIVEWAYS 8 SQ YD	STAB DRIVEWAYS 10 SQ YD	
IL ROUTE 171 (STATE STREET)									
136+12	L	HMA	PE	44.1	0.0	0.0	22.7	0.0	
137+51	L	HMA	PE	52.8	0.0	0.0	33.6	0.0	
137+69	R	PCC	PE	53.7	28.8	27.0	14.5	0.0	
138+18	R	HMA	PE	67.2	21.0	19.1	33.2	0.0	
138+19	L	HMA	PE	40.0	0.0	0.0	26.6	0.0	
138+75	R	HMA	PE	42.5	0.0	0.0	23.6	0.0	
138+78	L	HMA	PE	43.7	0.0	0.0	29.3	0.0	
138+95	R	PCC	PE	45.4	56.9	55.0	0.0	0.0	
140+50	R	HMA	CE	28.0	0.0	0.0	0.0	28.0	
140+88	R	PCC	CE	0.0	35.2	33.3	0.0	0.0	
141+52	L	HMA	PE	44.8	0.0	0.0	30.2	0.0	
141+66	R	PCC	CE	105.0	76.2	74.3	0.0	0.0	
141+75	L	HMA	PE	20.9	0.0	0.0	15.8	0.0	
142+10	L	HMA	PE	30.0	0.0	0.0	23.1	0.0	
142+17	R	HMA	PE	41.1	0.0	0.0	30.6	0.0	
142+38	L	HMA	PE	28.2	0.0	0.0	18.3	0.0	
143+16	R	HMA	CE	76.1	0.0	0.0	0.0	65.0	
13TH STREET									
11+71	L	HMA	CE	4.3	0.0	0.0	0.0	4.3	
11+98	R	HMA	CE	32.0	0.0	0.0	0.0	26.8	
14+47	L	PCC	CE	48.3	49.9	48.3	0.0	0.0	
TOTAL				848	268	257	302	124	

SIDEWALK SCHEDULE							
STATION	STATION	SIDEWALK REM SQ FT	AGG BASE CSE B 4 SQ YD	PC CONC SIDEWALK 5 SQ FT	PC CONC SIDEWALK 7 SQ FT	PC CONC SIDEWALK 8 SP SQ FT	DETECTABLE WARNINGS SQ FT
IL ROUTE 171 (STATE STREET)							
134+77	139+00	1,580.0	193.7	820.0	385.0	247.5	16.0
139+00	141+00	1,605.0	182.5	800.0	115.0	463.9	90.0
141+00	145+05	825.0	94.0	670.0	95.0	0.0	64.0
13TH STREET							
11+41	13+00	295.0	42.0	245.0	70.0	0.0	60.0
14+00	14+55	435.0	59.4	275.0	0.0	170.5	0.0
TOTAL		4,750	572	2,810	605	872	230

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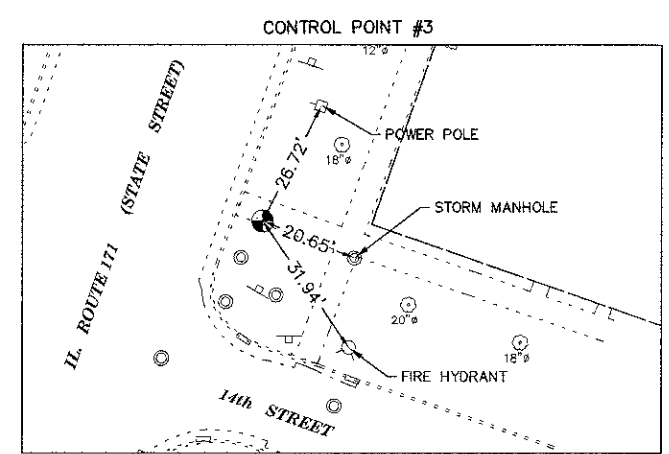
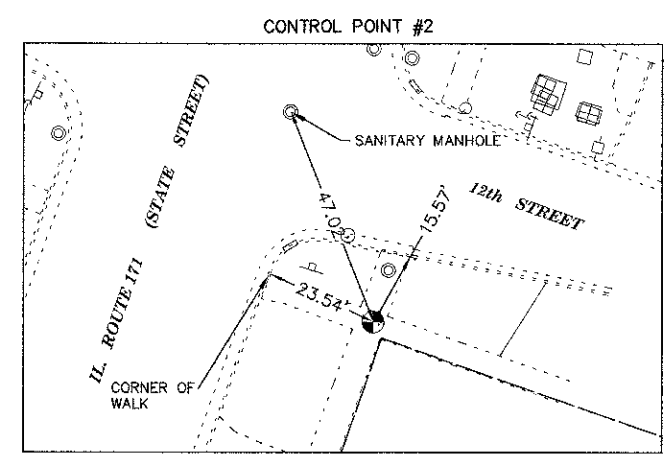
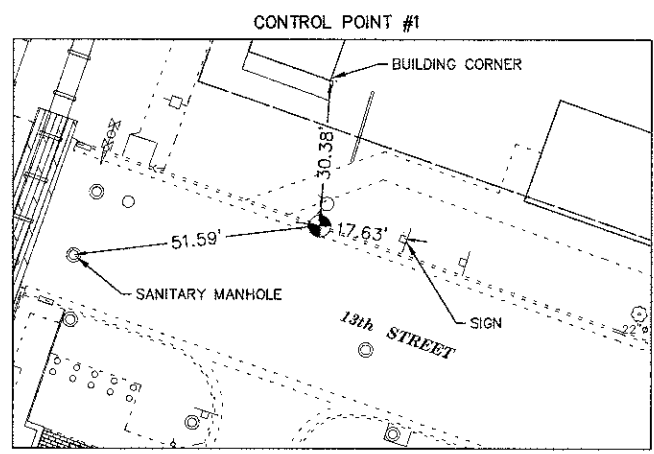
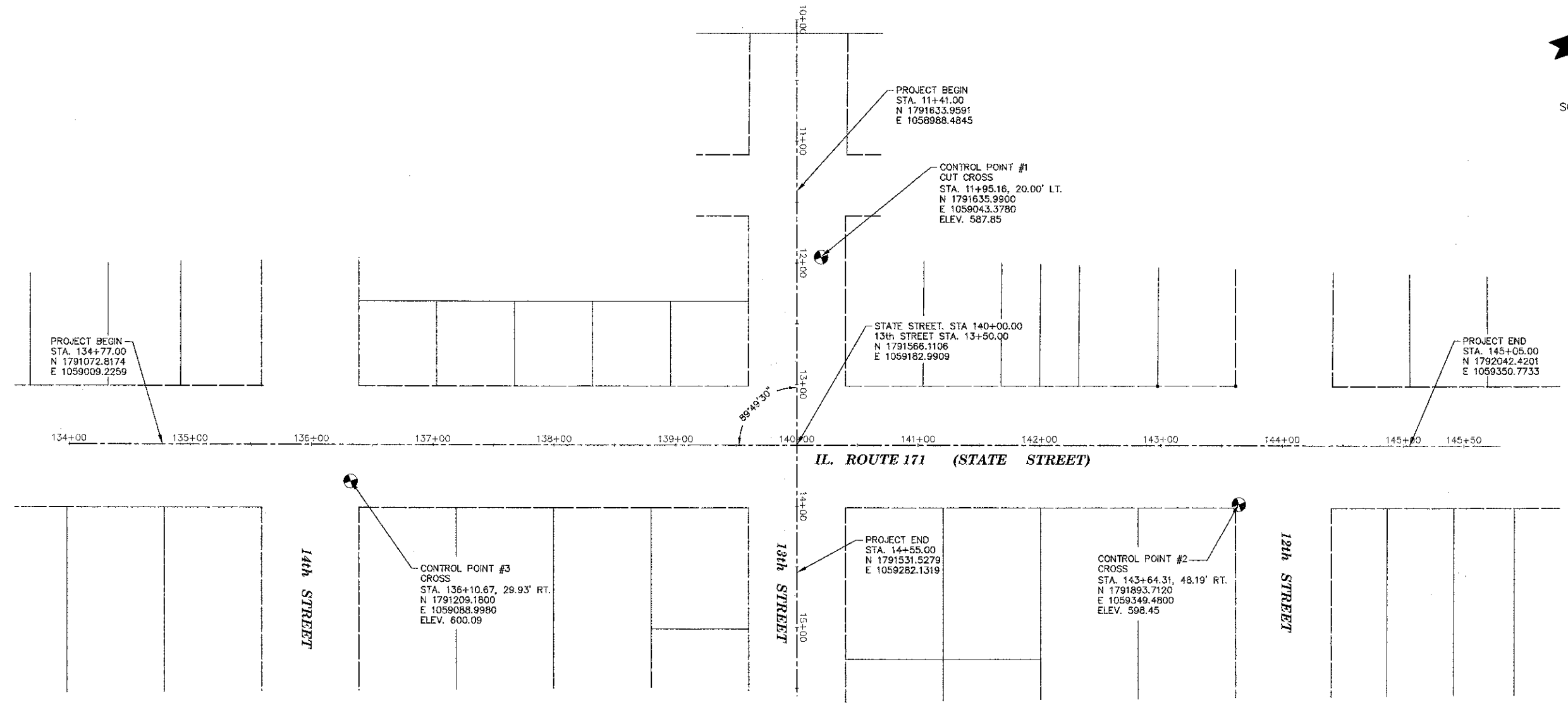
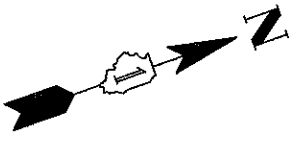
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USER NAME = Jim Schmidt	DESIGNED - JRL & SWM	REVISED - JPS 02/08/13
PLOT SCALE =	DRAWN - CLN	REVISED -
PLOT DATE = February 11, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES			
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS NO.
577	10-00068-00-TL	WILL	65 10
SCALE: N.T.S.			CONTRACT NO. 63788
SHEET NO. 2 OF 2 SHEETS		STA.	TO STA.

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)



BENCHMARKS

- BM#1 BONNET BOLT WITH ARROW POINTING TOWARDS ON THE FIRE HYDRANT LOCATED AT THE NORTHEAST CORNER OF 12TH STREET AND STATE STREET. ELEVATION = 600.46
- BM#2 NORTHWEST BONNET BOLT ON THE FIRE HYDRANT LOCATED AT THE NORTHEAST CORNER OF 13TH STREET AND STATE STREET. ELEVATION = 601.79
- BM#3 BONNET BOLT WITH ARROW POINTING TOWARDS ON THE FIRE HYDRANT LOCATED AT THE NORTHEAST CORNER OF 14TH STREET AND STATE STREET. ELEVATION = 603.54

Plotted: February 8, 2013 @ 9:14 AM By: Jim Schmidt - Trae 11 Alignment 22x34
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 Sugar Grove, Illinois 80554
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USER NAME = Jim Schmidt	DESIGNED - JRL & SWM	REVISED - JPS 02/08/13
PLOT SCALE =	DRAWN - CLN	REVISED -
PLOT DATE = February 8, 2013	CHECKED - JIL	REVISED -
	DATE - 11/13/12	REVISED -

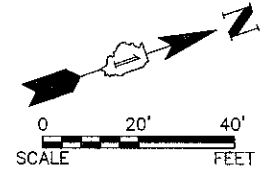
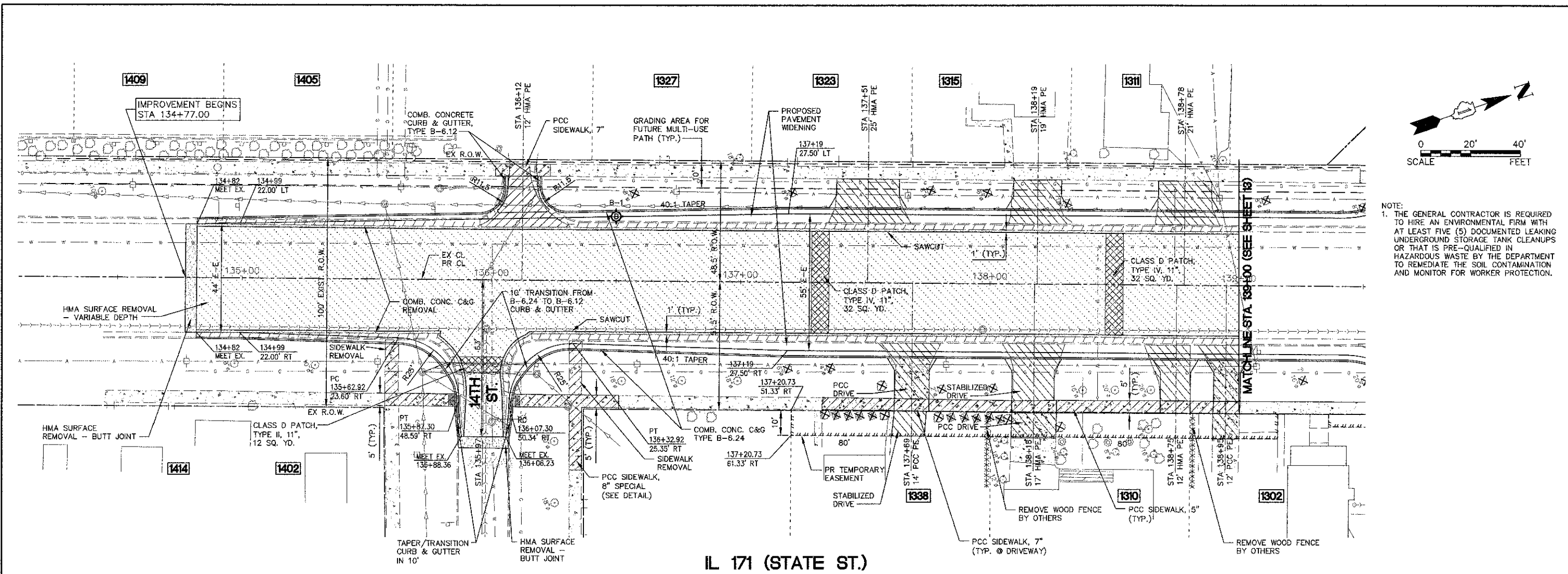
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

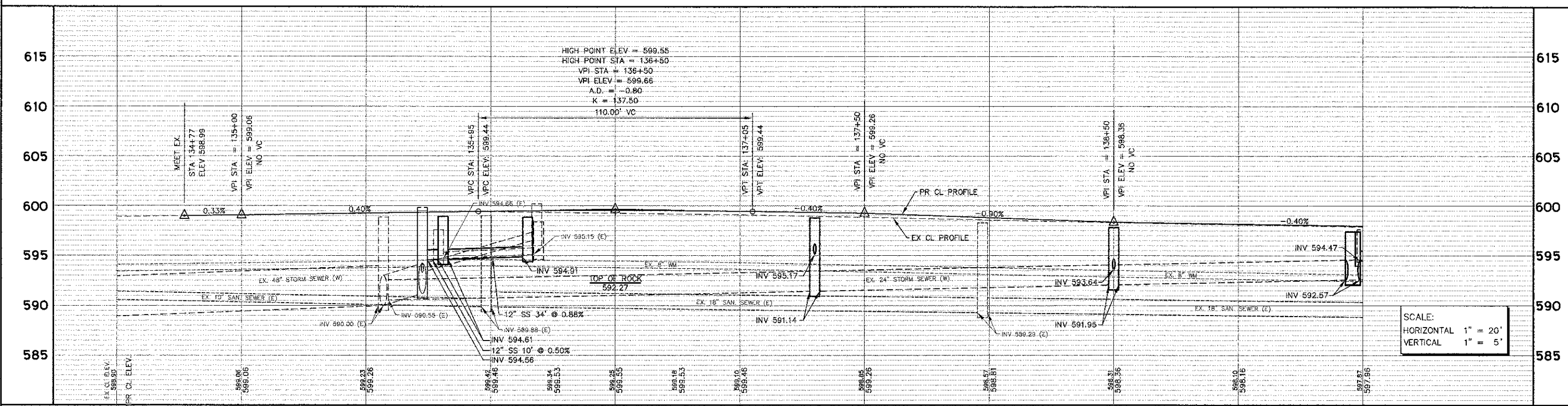
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	11
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT N-9003(619)			CONTRACT NO. 63788	

Path: S:\SHPROJ\10068\DWG\ENG\FINAL\ENG\10068-ALIGNMENT



NOTE:
 1. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

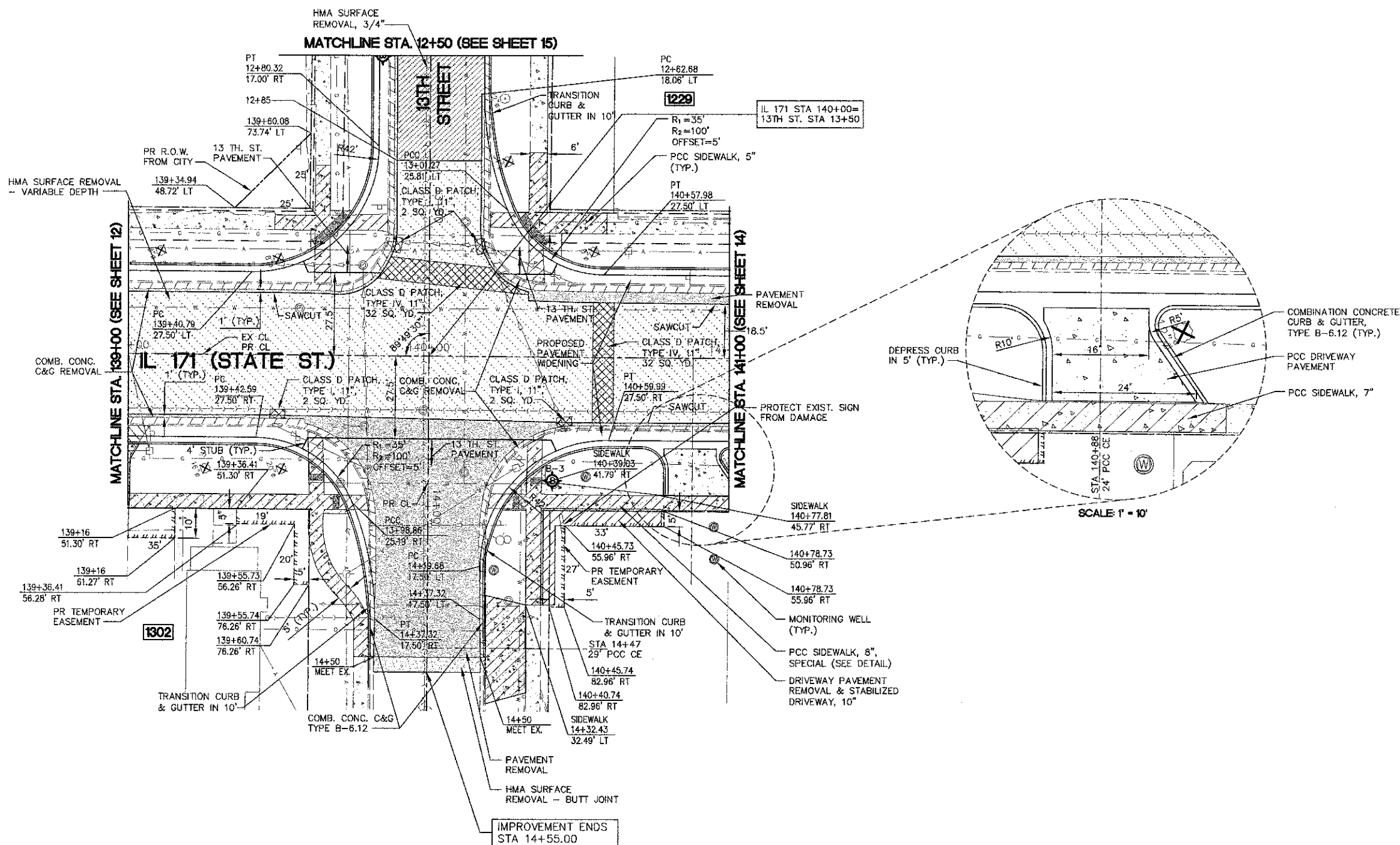
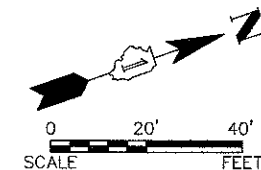
IL 171 (STATE ST.)



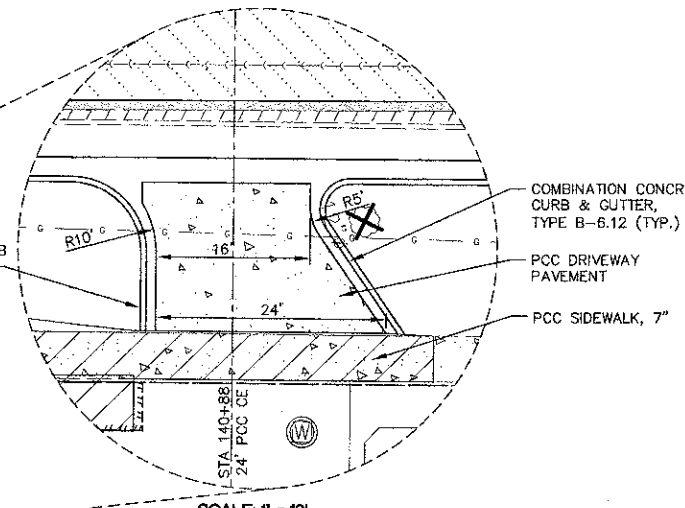
SCALE:
 HORIZONTAL 1" = 20'
 VERTICAL 1" = 5'

Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eeiweb.com		USER NAME = Jim Schmidt DESIGNED - JRL & SWM DRAWN - CLN CHECKED - JL DATE - 11/13/12	REVISED - JPS 02/08/13 REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		IL 171 (STATE STREET) PLAN AND PROFILE		F.A.P. RTE. = 577 SECTION = 10-00068-00-TL COUNTY = WILL TOTAL SHEETS = 65 SHEET NO. = 12 CONTRACT NO. = 63788 FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(619)
COPYRIGHT © 2013 ENGINEERING ENTERPRISES, INC.				SCALE: HORIZ=20' VERT=5' SHEET NO. 1 OF 4 SHEETS STA. 134+50 TO STA. 139+00				

Plot Date: February 8, 2013 @ 9:22 AM By: Jim Schmidt - Tab: 12 Route: 171_220-34



NOTE:
 1. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.



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PLOT SCALE =	DRAWN - CLN	REVISED -
PLOT DATE = February 8, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

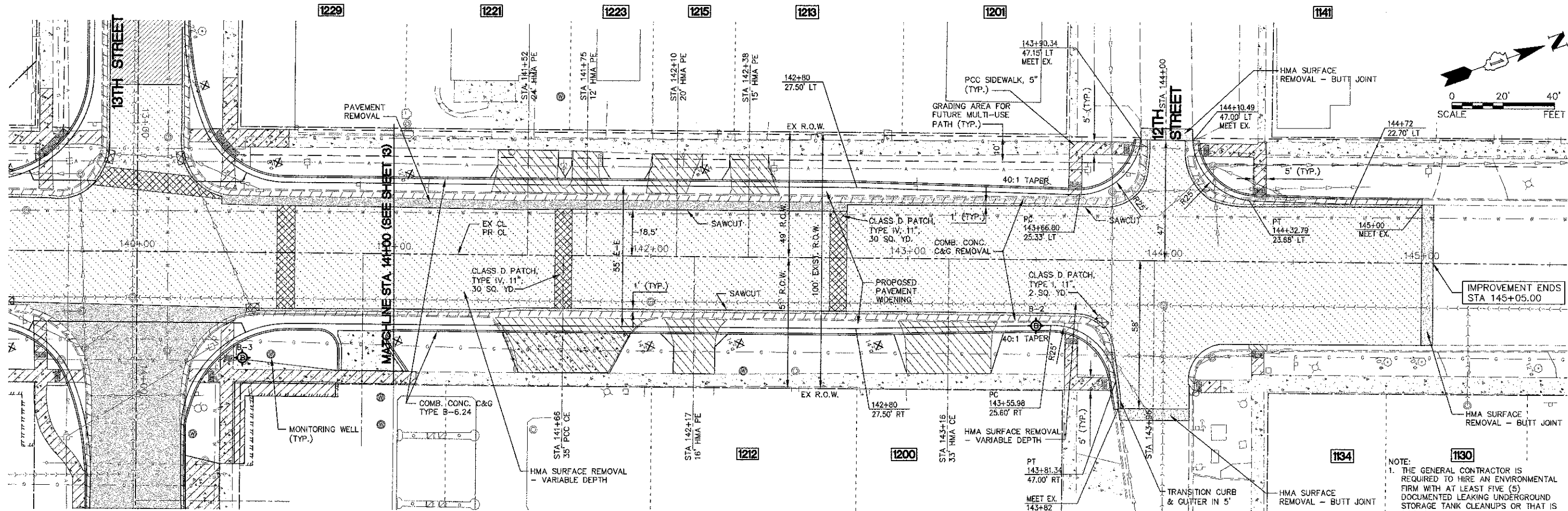
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 171 (STATE STREET) PLAN

SCALE: H=1"=20' V=1"=5' SHEET NO. 2 OF 4 SHEETS STA. 139+00 TO STA. 141+00

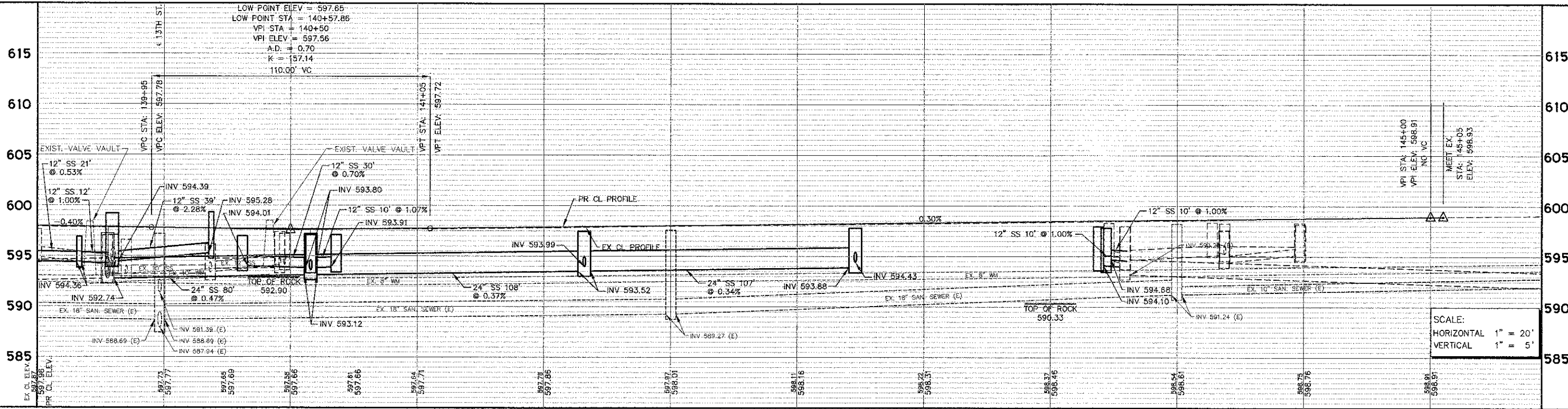
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00065-00-TL	WILL	66	13
CONTRACT NO. 63788			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT W-90031619	

Path: H:\S05KPROJ\109602\DWG\FINAL_ENG\109602-PP-ROADWAY



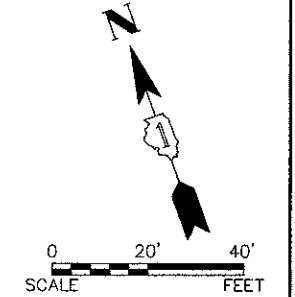
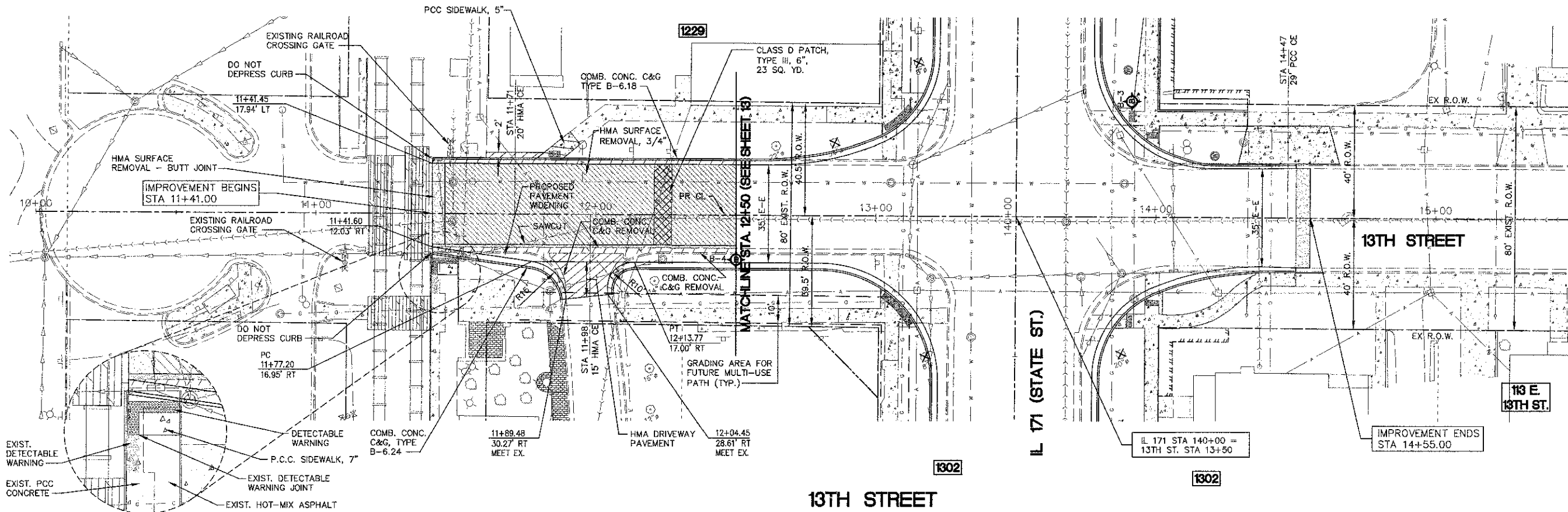
IL 171 (STATE ST.)

NOTE:
 1. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

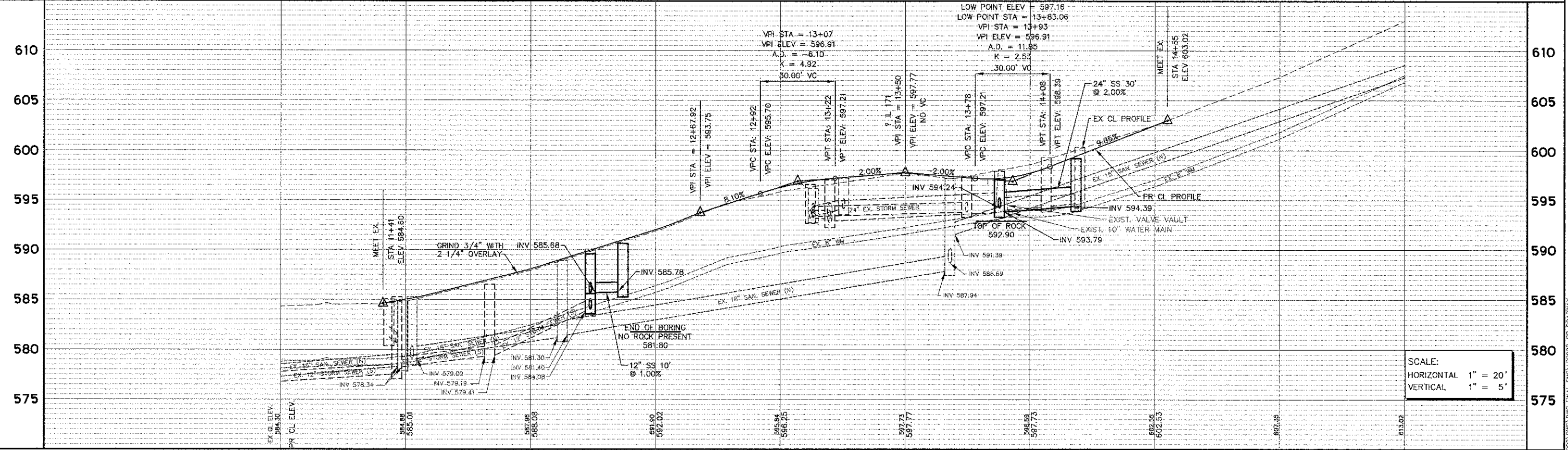


Plotted: February 8, 2013 @ 9:17 AM By: Jim Schmidt - Feb. 14 Route 171 22.34
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NOTE:
 1. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.



SCALE:
 HORIZONTAL 1" = 20'
 VERTICAL 1" = 5'

Plotter: February 8, 2013 @ 9:15 AM By: Jim Schmidt - Tab: 15 13th 22d34

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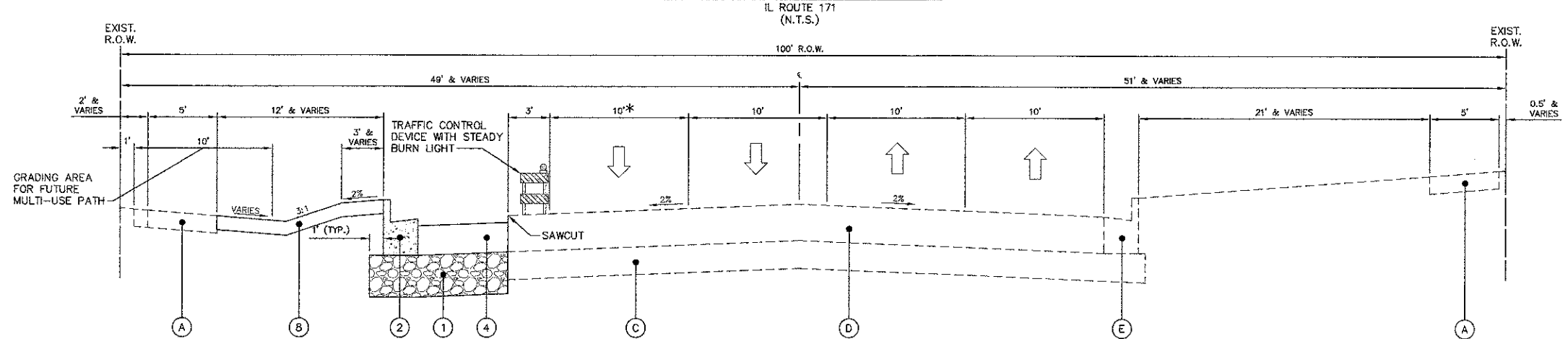
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PLOT DATE = February 8, 2013	CHECKED - JL	REVISED -
	DATE = 11/13/12	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

13th STREET
 PLAN AND PROFILE
 SCALE: HORIZ. 1"=20' VERT. 1"=5' SHEET NO. 4 OF 4 SHEETS STA. 11+00 TO STA. 15+50

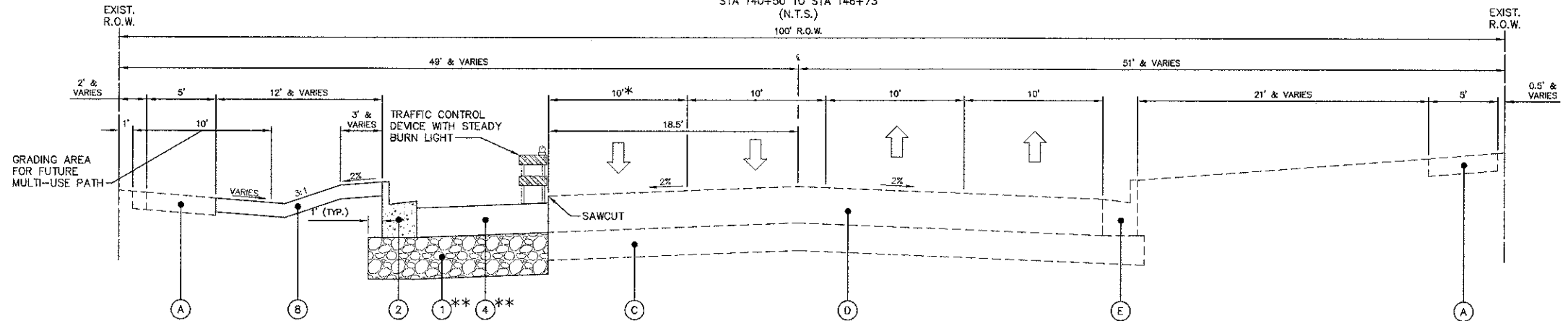
F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 15
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-30031619			CONTRACT NO. 63788	

CONSTRUCTION SECTION - STAGE 1



CONSTRUCTION SECTION - STAGE 1

IL ROUTE 171 - STORM SEWER TRUNK LINE
STA 140+50 TO STA 146+73
(N.T.S.)



* NOTE:
OUTSIDE LANES MAY BE CLOSED DURING
WORK HOURS (9:00 AM - 3:00 PM ONLY)
TO PROVIDE A SAFETY WORK ZONE FOR
CONSTRUCTION OPERATIONS.

** NOTE:
CONTRACTOR TO INSTALL AGGREGATE FOR
TEMPORARY ACCESS IMMEDIATELY AFTER
STORM SEWER IS INSTALLED UP TO A LEVEL
SO THAT THE MAXIMUM DROPOFF FROM
EXISTING PAVEMENT IS NOT MORE THAN 2"
DURING THE INTERIM PERIOD PRIOR TO
PLACEMENT OF BASE COURSE. AGGREGATE
FOR TEMPORARY ACCESS TO BE REMOVED
IMMEDIATELY PRIOR TO PLACEMENT OF BASE
COURSE.

EXISTING LEGEND

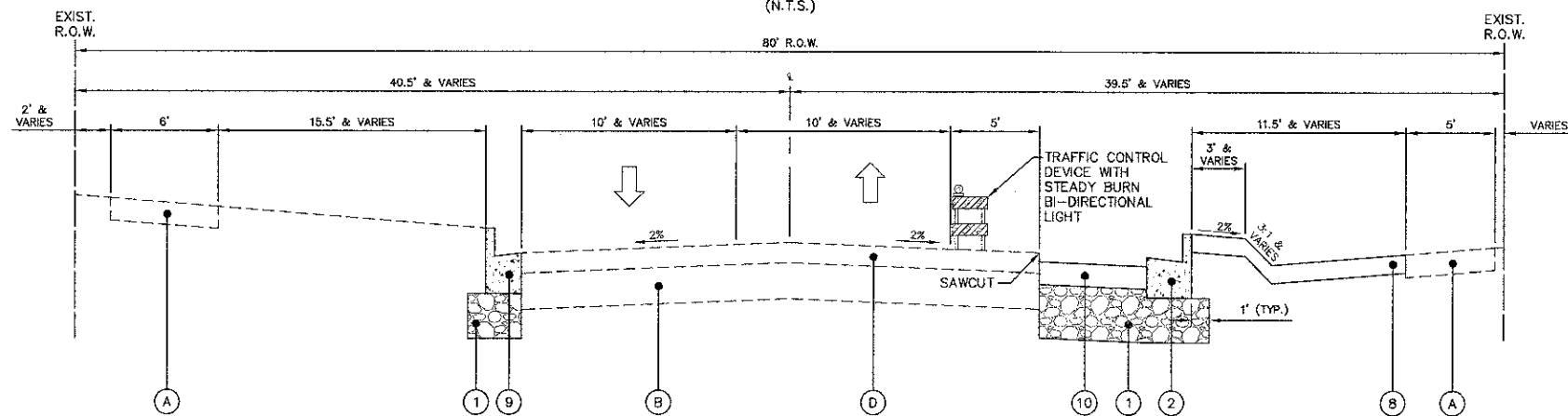
- (A) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (B) EXISTING AGGREGATE BASE
- (C) EXISTING CONCRETE PAVEMENT
- (D) EXISTING HOT-MIX ASPHALT PAVEMENT
- (E) EXISTING COMBINATION CONCRETE CURB & GUTTER, VARIOUS TYPES

PROPOSED LEGEND

- (1) AGGREGATE SUBGRADE, 12"
- (2) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (3) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (4) HOT-MIX ASPHALT BASE COURSE, 8 1/2"
- (5) HOT-MIX ASPHALT BASE COURSE, 5 1/4"
- (6) PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- (7) AGGREGATE BASE COURSE, TYPE B, 4"
- (8) TOPSOIL, 4"; SODDING; FERTILIZER
- (9) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (10) HOT-MIX ASPHALT BASE COURSE, 5 3/4"

CONSTRUCTION SECTION - STAGE 1

13TH STREET - WEST LEG
(N.T.S.)



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED CONSTRUCTION STAGING PLAN
TYPICAL SECTION

SCALE: N.T.S. SHEET NO. 1 OF 4 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	16
CONTRACT NO. 63788			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)	

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DRAWN - CLN
CHECKED - JL
DATE - 11/13/12

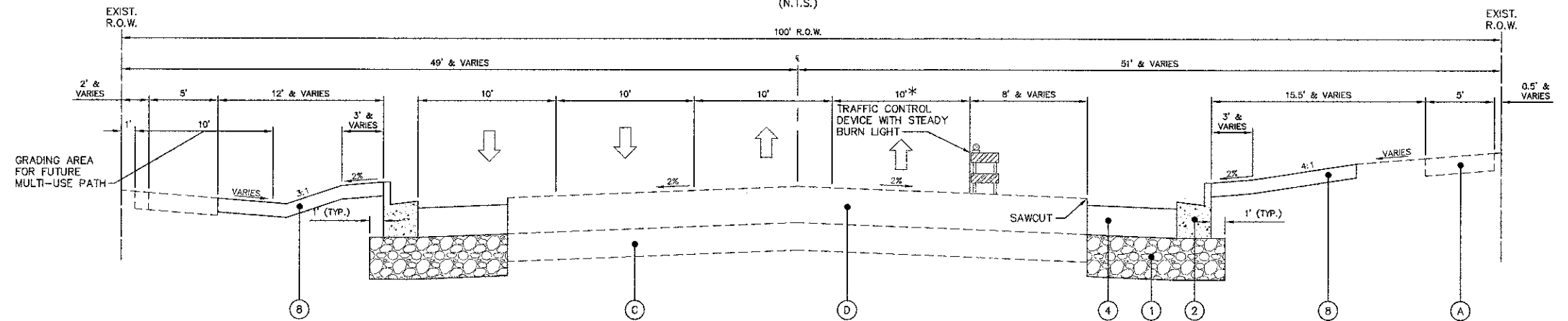
REVISED - JPS 02/08/13
REVISED -
REVISED -
REVISED -

PLDT SCALE =
PLDT DATE = February 11, 2013

PLDT DATE = February 11, 2013
DATE - 11/13/12

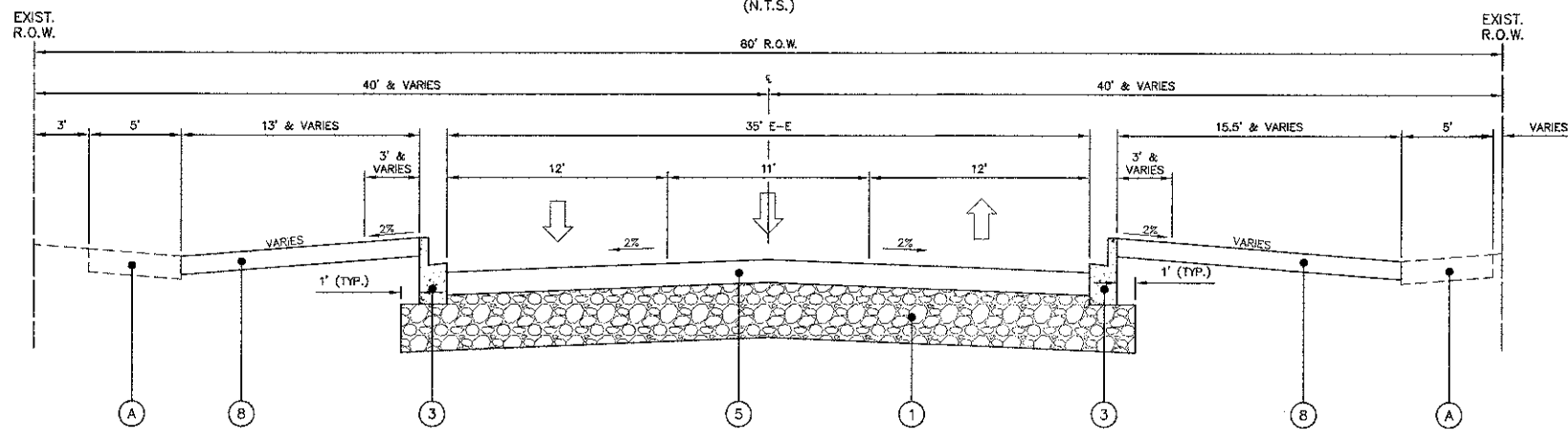
CONSTRUCTION SECTION - STAGE 2

IL ROUTE 171
(N.T.S.)



CONSTRUCTION SECTION - STAGE 2

13TH STREET - EAST LEG
(N.T.S.)



* NOTE:
OUTSIDE LANES MAY BE CLOSED DURING
WORK HOURS (9:00 AM - 3:00 PM ONLY)
TO PROVIDE A SAFETY WORK ZONE FOR
CONSTRUCTION OPERATIONS.

EXISTING LEGEND

- (A) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (B) EXISTING AGGREGATE BASE
- (C) EXISTING CONCRETE PAVEMENT
- (D) EXISTING HOT-MIX ASPHALT PAVEMENT
- (E) EXISTING COMBINATION CONCRETE CURB & GUTTER, VARIOUS TYPES

PROPOSED LEGEND

- (1) AGGREGATE SUBGRADE, 12"
- (2) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (3) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (4) HOT-MIX ASPHALT BASE COURSE, 8 1/2"
- (5) HOT-MIX ASPHALT BASE COURSE, 5 1/4"
- (6) PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- (7) AGGREGATE BASE COURSE, TYPE B, 4"
- (8) TOPSOIL, 4"; SODDING; FERTILIZER
- (9) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (10) HOT-MIX ASPHALT BASE COURSE, 5 3/4"

Plotted: February 8, 2013 @ 7:53 AM By: Larry Nolan - Tab: 17 Const Staging 22-34
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PLOT DATE = February 8, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**SUGGESTED CONSTRUCTION STAGING PLAN
TYPICAL SECTION**

SCALE: N.T.S. SHEET NO. 2 OF 4 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	17
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT N-90036191			CONTRACT NO. 63788	

For: H:\GIS\PROJ\10068\DWG\STAGING_FINAL_ENG\10068-STAGE

STAGE 1 SUGGESTED CONSTRUCTION TRAFFIC CONTROL AND PROTECTION GENERAL NOTES

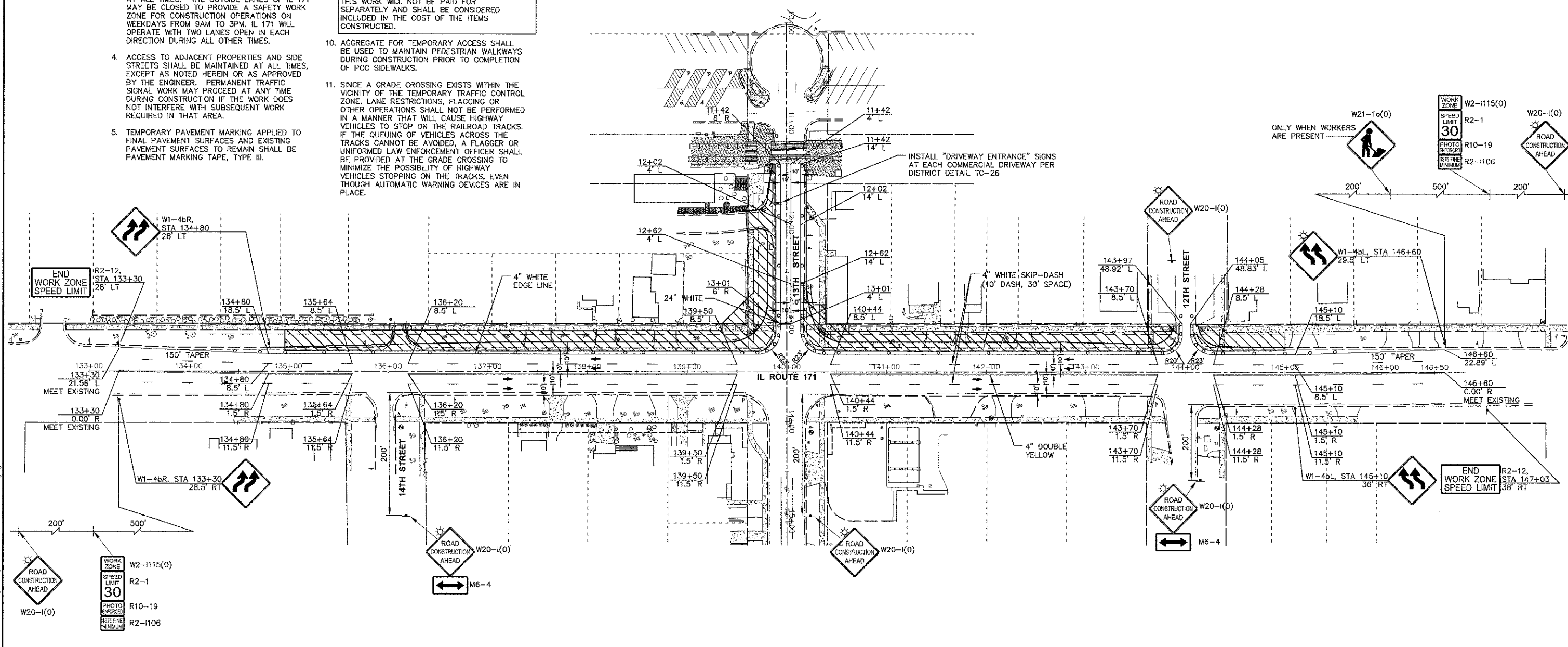
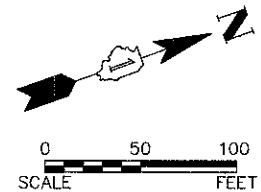
- TRAFFIC CONTROL AND PROTECTION SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, CONTRACT SPECIAL PROVISIONS, CONSTRUCTION STAGING PLANS, THE LATEST EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS," AND AS DIRECTED BY THE ENGINEER. TRAFFIC CONTROL SHOWN IN THE CONSTRUCTION STAGING PLAN REPRESENTS A GUIDE FOR THE SAFE MANAGEMENT OF TRAFFIC DURING THE EXECUTION OF THE WORK. MODIFICATIONS MAY BE NECESSARY DUE TO LOCAL CONDITIONS AT THE TIME OF CONSTRUCTION. ANY PROPOSED CHANGES BY THE CONTRACTOR TO THESE TRAFFIC CONTROL PLANS SHALL BE APPROVED BY THE ENGINEER PRIOR TO BEING IMPLEMENTED. ANY MODIFICATIONS OR ADDITIONS REQUIRED BY THE ENGINEER WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL) UNLESS A SEPARATE PAY ITEM HAS BEEN ESTABLISHED FOR THE WORK.
- ANY EXISTING OR TEMPORARY MARKINGS WHICH CONFLICT WITH MARKINGS REQUIRED FOR CONSTRUCTION STAGING SHALL BE REMOVED ACCORDING TO SECTION 703 OR 783 OF THE STANDARD SPECIFICATIONS, AS APPLICABLE.
- THERE SHALL BE AT LEAST ONE LANE OF TRAFFIC OPEN ON IL 171 IN BOTH DIRECTIONS AT ALL TIMES. THE OUTSIDE LANES OF IL 171 MAY BE CLOSED TO PROVIDE A SAFETY WORK ZONE FOR CONSTRUCTION OPERATIONS ON WEEKDAYS FROM 9AM TO 3PM. IL 171 WILL OPERATE WITH TWO LANES OPEN IN EACH DIRECTION DURING ALL OTHER TIMES.
- ACCESS TO ADJACENT PROPERTIES AND SIDE STREETS SHALL BE MAINTAINED AT ALL TIMES, EXCEPT AS NOTED HEREIN OR AS APPROVED BY THE ENGINEER. PERMANENT TRAFFIC SIGNAL WORK MAY PROCEED AT ANY TIME DURING CONSTRUCTION IF THE WORK DOES NOT INTERFERE WITH SUBSEQUENT WORK REQUIRED IN THAT AREA.
- TEMPORARY PAVEMENT MARKING APPLIED TO FINAL PAVEMENT SURFACES AND EXISTING PAVEMENT SURFACES TO REMAIN SHALL BE PAVEMENT MARKING TAPE, TYPE III.
- PLATING AND/OR TEMPORARY DRAINAGE STRUCTURE ADJUSTMENTS MAY BE REQUIRED DUE TO THE STAGING OF CONSTRUCTION. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
- THE PROPOSED DRIVEWAY FOR THE METRA STATION ON 13TH STREET (STATION 11+98) SHALL ONLY BE INSTALLED ON A WEEKEND IN ORDER TO MINIMIZE THE AFFECTS OF CONSTRUCTION ON THE MOTORING PUBLIC. THIS WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS CONSTRUCTED.
- DUE TO LARGE TRAFFIC VOLUMES THE CONTRACTOR WILL NOT BE ALLOWED TO WORK ON THE WEST LEG OF 13TH STREET OR AT THE METRA STATION DRIVEWAY ON 13TH STREET BETWEEN THE HOURS OF 6:00 A.M. AND 7:30 A.M. AND BETWEEN 5:00 P.M. AND 7:00 P.M. THE CONTRACTOR SHALL MAKE EFFORTS TO WORK WITH CITY POLICE TO PROVIDE SAFE PASSAGE FOR PEDESTRIANS AND THE MOTORING PUBLIC DURING HIGH VOLUME HOURS.
- ALL STORM SEWER LATERALS SHALL BE CONSTRUCTED USING TRAFFIC CONTROL STANDARD 701606 AS APPLICABLE. NO OVERNIGHT LANE CLOSURE WILL BE PERMITTED. THIS WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS CONSTRUCTED.
- AGGREGATE FOR TEMPORARY ACCESS SHALL BE USED TO MAINTAIN PEDESTRIAN WALKWAYS DURING CONSTRUCTION PRIOR TO COMPLETION OF PCC SIDEWALKS.
- SINCE A GRADE CROSSING EXISTS WITHIN THE VICINITY OF THE TEMPORARY TRAFFIC CONTROL ZONE, LANE RESTRICTIONS, FLAGGING OR OTHER OPERATIONS SHALL NOT BE PERFORMED IN A MANNER THAT WILL CAUSE HIGHWAY VEHICLES TO STOP ON THE RAILROAD TRACKS. IF THE QUEUING OF VEHICLES ACROSS THE TRACKS CANNOT BE AVOIDED, A FLAGGER OR UNIFORMED LAW ENFORCEMENT OFFICER SHALL BE PROVIDED AT THE GRADE CROSSING TO MINIMIZE THE POSSIBILITY OF HIGHWAY VEHICLES STOPPING ON THE TRACKS, EVEN THOUGH AUTOMATIC WARNING DEVICES ARE IN PLACE.

CONSTRUCTION STAGING LEGEND

- TRAFFIC CONTROL DEVICE (BARRICADE, DRUM, VERTICAL PANEL) WITH STEADY BURN LIGHT AT 50' C/C (TYP.); 25' C/C ON TAPERS; 10' C/C AT INTERSECTION RETURNS AND DRIVEWAYS
- ▨ WORK ZONE
- DIRECTION OF TRAFFIC MOVEMENT
- ⊥ TYPE III BARRICADE WITH 2 2-WAY FLASHING LIGHTS WITH ROAD CLOSED SIGN
- ⊥ TEMPORARY INFORMATION SIGN/TRAFFIC CONTROL SIGN

STAGE 1 SUGGESTED CONSTRUCTION SEQUENCE

- INSTALL CHANGEABLE MESSAGE SIGNS (4 TOTAL) ON NORTHBOUND AND SOUTHBOUND IL 171 AND EASTBOUND AND WESTBOUND 13TH STREET TWO WEEKS PRIOR TO COMMENCEMENT OF WORK TO NOTIFY THE MOTORING PUBLIC CONCERNING THE UPCOMING ROADWAY CONSTRUCTION.
- INFORMATIONAL WARNING SIGNS FOR NARROW TRAVEL LANES (10'-0") SHALL BE INSTALLED IN ADVANCE OF THE PROJECT LIMITS ON IL 171. THE SIGNS SHALL BE POSITIONED PER THE ENGINEER.
- ESTABLISH TRAFFIC CONTROL AND PAVEMENT MARKING PER PLAN AND APPLICABLE IDOT STANDARDS.
- INSTALL TEMPORARY EROSION CONTROL DEVICES PER PLAN AND APPLICABLE DETAILS.
- CONSTRUCT STORM SEWER.
- CONSTRUCT CURB AND GUTTER.
- COMMENCE CONSTRUCTION OF PROPOSED TRAFFIC SIGNALS.
- CONSTRUCT AGGREGATE SUBGRADE AND HMA BASE COURSE.
- CONSTRUCT SIDEWALK.
- INSTALL TEMPORARY SEEDING AS REQUIRED.



PLOTTED: March 20, 2013 @ 2:13 pm By: Larry Nolan - Top: 18 Const Staging 22x34
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	SCALE: N.T.S. SHEET NO. 3 OF 4 SHEETS STA. N/A TO STA. N/A					

FILE: H:\2013\000\UP0602\DWG\DWG_FINAL_ENG\UP0602-STAGE1

STAGE 2 SUGGESTED CONSTRUCTION TRAFFIC CONTROL AND PROTECTION GENERAL NOTES

- TRAFFIC CONTROL AND PROTECTION SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, CONTRACT SPECIAL PROVISIONS, CONSTRUCTION STAGING PLANS, THE LATEST EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS," AND AS DIRECTED BY THE ENGINEER. TRAFFIC CONTROL SHOWN IN THE CONSTRUCTION STAGING PLAN REPRESENTS A GUIDE FOR THE SAFE MANAGEMENT OF TRAFFIC DURING THE EXECUTION OF THE WORK. MODIFICATIONS MAY BE NECESSARY DUE TO LOCAL CONDITIONS AT THE TIME OF CONSTRUCTION. ANY PROPOSED CHANGES BY THE CONTRACTOR TO THESE TRAFFIC CONTROL PLANS SHALL BE APPROVED BY THE ENGINEER PRIOR TO BEING IMPLEMENTED. ANY MODIFICATIONS OR ADDITIONS REQUIRED BY THE ENGINEER WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL) UNLESS A SEPARATE PAY ITEM HAS BEEN ESTABLISHED FOR THE WORK.
- ANY EXISTING OR TEMPORARY MARKINGS WHICH CONFLICT WITH MARKINGS REQUIRED FOR CONSTRUCTION STAGING SHALL BE REMOVED ACCORDING TO SECTION 703 OR 783 OF THE STANDARD SPECIFICATIONS, AS APPLICABLE.
- THERE SHALL BE AT LEAST ONE LANE OF TRAFFIC OPEN ON IL 171 IN BOTH DIRECTIONS AT ALL TIMES. THE OUTSIDE LANES OF IL 171 MAY BE CLOSED TO PROVIDE A SAFETY WORK ZONE FOR CONSTRUCTION OPERATIONS ON WEEKDAYS FROM 9AM TO 3PM. IL 171 WILL OPERATE WITH TWO LANES OPEN IN EACH DIRECTION DURING ALL OTHER TIMES.
- ACCESS TO ADJACENT PROPERTIES AND SIDE STREETS SHALL BE MAINTAINED AT ALL TIMES, EXCEPT AS NOTED HEREIN OR AS APPROVED BY THE ENGINEER. PERMANENT TRAFFIC SIGNAL WORK MAY PROCEED AT ANY TIME DURING CONSTRUCTION IF THE WORK DOES NOT INTERFERE WITH SUBSEQUENT WORK REQUIRED IN THAT AREA.
- 13TH STREET BETWEEN APPROXIMATELY STA 13+50 AND STA 14+50 WILL BE SIGNED AS A ROAD CLOSED IN ORDER TO FACILITATE THE RECONSTRUCTION OF 13TH STREET. THE CONTRACTOR SHALL INSTALL THE SIGNAGE AND APPROPRIATE TRAFFIC CONTROL MEASURES THE DAY BEFORE WORK IN THE AREA IS TO COMMENCE. THE SIGNAGE SHALL BE REMOVED ONCE THE PERTINENT CONSTRUCTION ITEMS ARE COMPLETED AND TWO LANES OF TRAFFIC CAN RESUME AS DIRECTED BY THE ENGINEER. THE MAXIMUM LENGTH OF THE ROAD CLOSED SHALL BE ONE (1) WEEK. SEE 13TH STREET DETOUR PLAN.
- ACCESS TO ADJACENT PROPERTIES AND SIDE STREETS SHALL BE MAINTAINED AT ALL TIMES, EXCEPT AS NOTED HEREIN OR AS APPROVED BY THE ENGINEER. PERMANENT TRAFFIC SIGNAL WORK MAY PROCEED AT ANY TIME DURING CONSTRUCTION AFTER THE TEMPORARY SIGNALS ARE INSTALLED AS LONG AS THAT WORK DOES NOT INTERFERE WITH SUBSEQUENT WORK REQUIRED IN THAT AREA.
- TEMPORARY PAVEMENT MARKING APPLIED TO FINAL PAVEMENT SURFACES AND EXISTING PAVEMENT SURFACES TO REMAIN SHALL BE PAVEMENT MARKING TAPE, TYPE III.
- PLATING AND/OR TEMPORARY DRAINAGE STRUCTURE ADJUSTMENTS MAY BE REQUIRED DUE TO THE STAGING OF CONSTRUCTION. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
- AGGREGATE FOR TEMPORARY ACCESS SHALL BE USED TO MAINTAIN PEDESTRIAN WALKWAYS DURING CONSTRUCTION PRIOR TO COMPLETION OF PCC SIDEWALKS.
- SINCE A GRADE CROSSING EXISTS WITHIN THE VICINITY OF THE TEMPORARY TRAFFIC CONTROL ZONE, LANE RESTRICTIONS, FLAGGING OR OTHER OPERATIONS SHALL NOT BE PERFORMED IN A MANNER THAT WILL CAUSE HIGHWAY VEHICLES TO STOP ON THE RAILROAD TRACKS. IF THE QUEUING OF VEHICLES ACROSS THE TRACKS CANNOT BE AVOIDED, A FLAGGER OR UNIFORMED LAW ENFORCEMENT OFFICER SHALL BE PROVIDED AT THE GRADE CROSSING TO MINIMIZE THE POSSIBILITY OF HIGHWAY VEHICLES STOPPING ON THE TRACKS, EVEN THOUGH AUTOMATIC WARNING DEVICES ARE IN PLACE.

CONSTRUCTION STAGING LEGEND

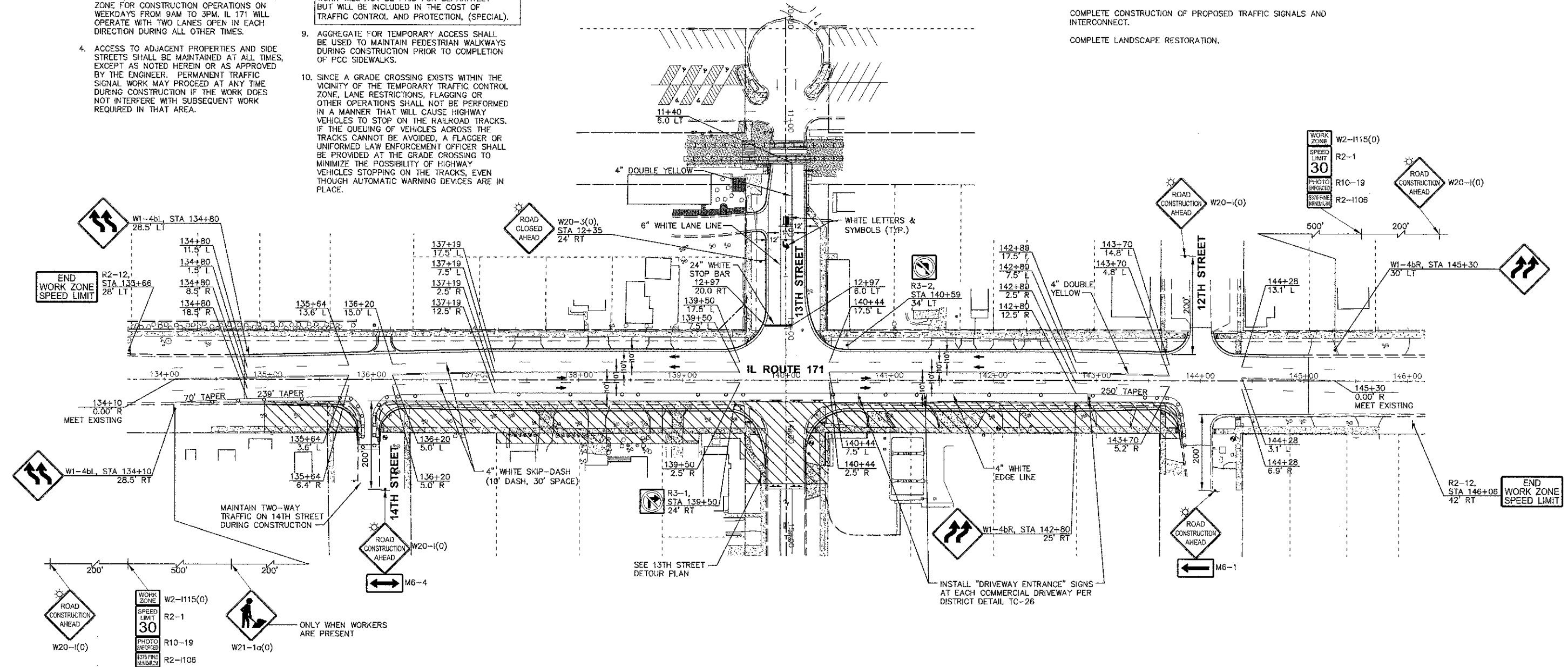
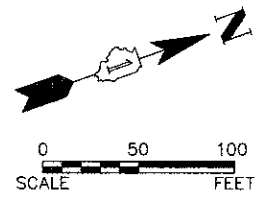
- TRAFFIC CONTROL DEVICE (BARRICADE, DRUM, VERTICAL PANEL) WITH STEADY BURN LIGHT AT 50' C/C (TYP.); 25' C/C ON TAPERS; 10' C/C AT INTERSECTION RETURNS AND DRIVEWAYS
- ▨ WORK ZONE
- DIRECTION OF TRAFFIC MOVEMENT
- ⊥ TYPE III BARRICADE WITH 2 2-WAY FLASHING LIGHTS WITH ROAD CLOSED SIGN
- ⊥ TEMPORARY INFORMATION SIGN/TRAFFIC CONTROL SIGN

STAGE 2 SUGGESTED CONSTRUCTION SEQUENCE

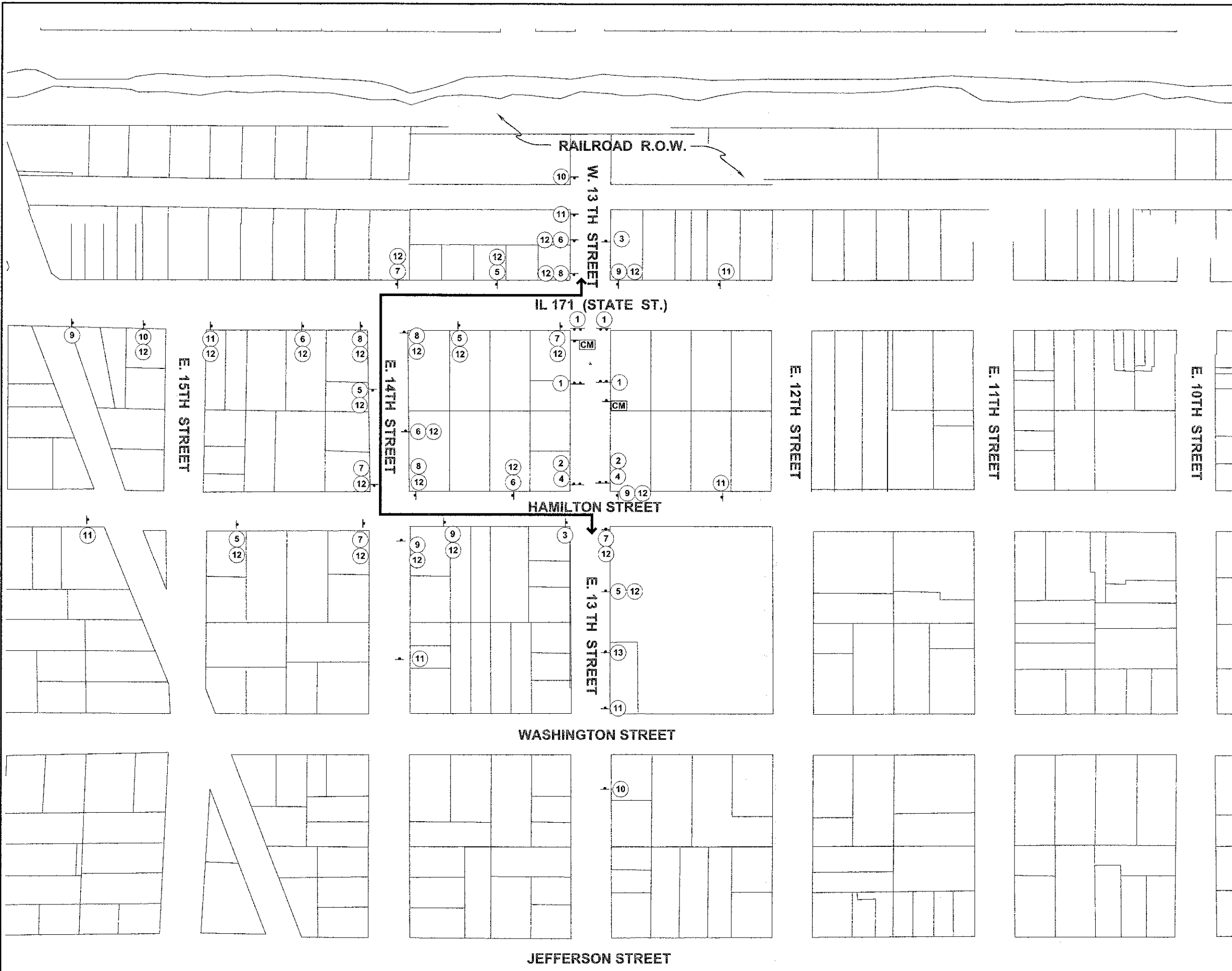
- UPDATE CHANGEABLE MESSAGE SIGNS (4 TOTAL) ON NORTHBOUND AND SOUTHBOUND IL 171 AND EASTBOUND AND WESTBOUND 13TH STREET FIVE DAYS PRIOR TO COMMENCEMENT OF WORK TO NOTIFY THE MOTORING PUBLIC CONCERNING THE UPCOMING ROADWAY RECONSTRUCTION AND CONSTRUCTION.
- ESTABLISH TRAFFIC CONTROL AND PAVEMENT MARKING PER PLAN AND APPLICABLE IDOT STANDARDS.
- INSTALL TEMPORARY EROSION CONTROL DEVICES PER PLAN AND APPLICABLE DETAILS.
- CONSTRUCT CURB AND GUTTER.
- COMMENCE CONSTRUCTION OF PROPOSED TRAFFIC SIGNALS.
- CONSTRUCT AGGREGATE SUBGRADE AND HMA BASE COURSE.
- CONSTRUCT SIDEWALK.
- INSTALL TEMPORARY SEEDING AS REQUIRED.

STAGE 3 SUGGESTED CONSTRUCTION SEQUENCE

- REMOVE EXISTING SURFACE AT INDICATED AREAS.
- CONSTRUCT LEVELING BINDER.
- COMPLETE FINAL PAVEMENT SURFACE COURSE, PAVEMENT MARKING AND SIGNING.
- COMPLETE CONSTRUCTION OF PROPOSED TRAFFIC SIGNALS AND INTERCONNECT.
- COMPLETE LANDSCAPE RESTORATION.

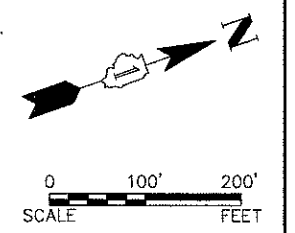


<p>Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sigler Grove, Illinois 60554 630.466.6700 / www.eeiweb.com</p>	USER NAME = Larry Nolan DESIGNED - JRL & SWM DRAWN - CLN CHECKED - JL DATE - 11/13/12	REVISED - JPS 02/08/13 REVISED - CLN 03/19/13 REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED CONSTRUCTION STAGING PLAN STAGE 2 AND STAGE 3	F.A.P. RTE. = 577 SECTION = 10-00068-00-TL COUNTY = WILL SHEETS = 65 TOTAL SHEETS = 19 CONTRACT NO. = 63788 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)
	PLOT SCALE = PLOT DATE = March 20, 2013	SCALE: N.T.S. SHEET NO. 4 OF 4 SHEETS STA. N/A TO STA. N/A			
	COPYRIGHT © 2013 ENGINEERING ENTERPRISES, INC.	ONLY WHEN WORKERS ARE PRESENT			



NOTES:

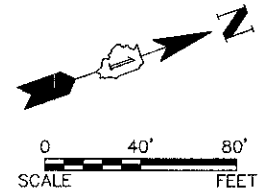
1. LOCATIONS OF ALL SIGNS ARE APPROXIMATE. FINAL LOCATION SHALL BE BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER.
2. SIGN SPACING = 200 FEET (TYP.), UNLESS NOTED OTHERWISE OR AS DIRECTED BY THE ENGINEER.
3. STREET NAME SIGN (12) TO BE PLACED ABOVE DETOUR SIGN.
4. CHANGEABLE MESSAGE SIGNS (CM) TO BE INSTALLED A MINIMUM OF TWO WEEKS IN ADVANCE OF ANY CONSTRUCTION WORK NOTIFYING PUBLIC OF PENDING ROAD WORK AND CLOSURE.



- ← - DETOUR ROUTE
- ① **ROAD CLOSED** R11-2 48X30
ON TYPE III BARRICADE W/ FLASHING LIGHT (3 ACROSS ROAD)
 - ② **ROAD CLOSED TO THRU TRAFFIC** R11-4 60X30
ON TYPE III BARRICADE W/ FLASHING LIGHT (2 STAGGERED ACROSS ROAD)
 - ③ **END DETOUR** M4-8A 24X18
 - ④ **DETOUR** M4-10L 48X18
 - ⑤ **DETOUR** M4-9L 30X30
 - ⑥ **DETOUR** M4-9R 30X30
 - ⑦ **DETOUR** M4-9L 30X24
 - ⑧ **DETOUR** M4-9R 30X24
 - ⑨ **DETOUR** M4-9 30X30
 - ⑩ **ROAD CLOSED AHEAD** W20-3 36X36
 - ⑪ **DETOUR AHEAD** W20-2 36X36
 - ⑫ **E. 13TH ST** 30X18
 - ⑬ **ROAD CLOSED 500 FT** W20-3 36X36
- CM** CHANGEABLE MESSAGE SIGN

Plotted: February 8, 2013 @ 8:54 AM By: Jim Schmidt - Tab: 20 Detour Plan 22-34
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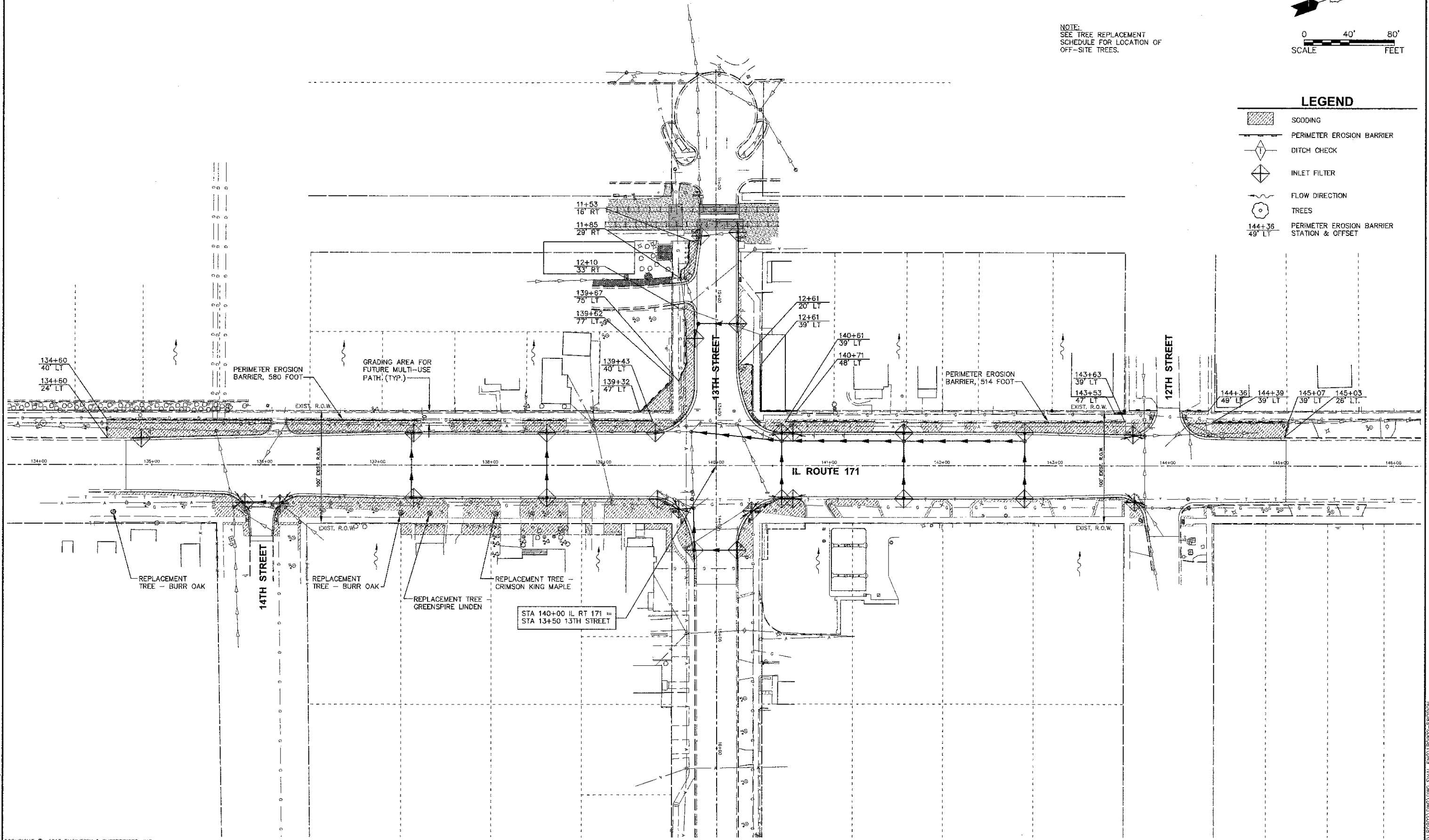
Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.ee1web.com	USER NAME = Jim Schmidt PLOT SCALE = PLOT DATE = February 8, 2013	DESIGNED - JRL & SWM DRAWN - CLN CHECKED - JL DATE - 11/13/12	REVISED - JPS 02/08/13 REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	13TH STREET DETOUR PLAN		F.A.P. RTE. 577 SECTION 10-00068-00-TL COUNTY WILL TOTAL SHEETS 65 SHEET NO. 20 CONTRACT NO. 63788
	SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. N/A TO STA. N/A				FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-9003619		



NOTE:
SEE TREE REPLACEMENT
SCHEDULE FOR LOCATION OF
OFF-SITE TREES.

LEGEND

	SODDING
	PERIMETER EROSION BARRIER
	DITCH CHECK
	INLET FILTER
	FLOW DIRECTION
	TREES
	PERIMETER EROSION BARRIER STATION & OFFSET



Plotted February 8, 2013 @ 9:24 AM Dwg. Jlm.Schmidt - Feb. 21 Erosion 22x34
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 CONSULTING ENGINEERS
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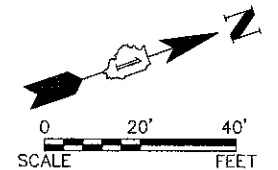
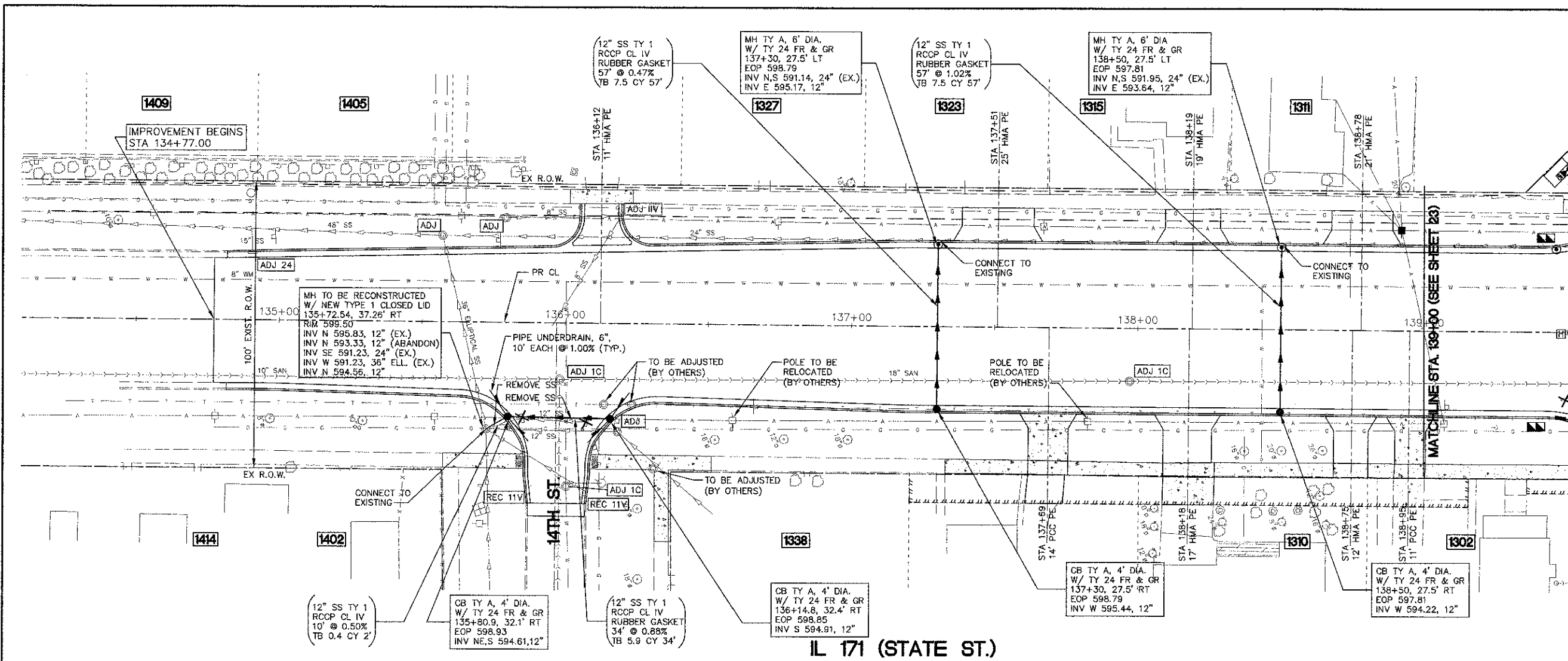
USER NAME = Jlm.Schmidt	DESIGNED - JRL & SWM	REVISED - JPS 02/08/13
PLOT SCALE =	DRAWN - CLN	REVISED -
PLOT DATE = February 8, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

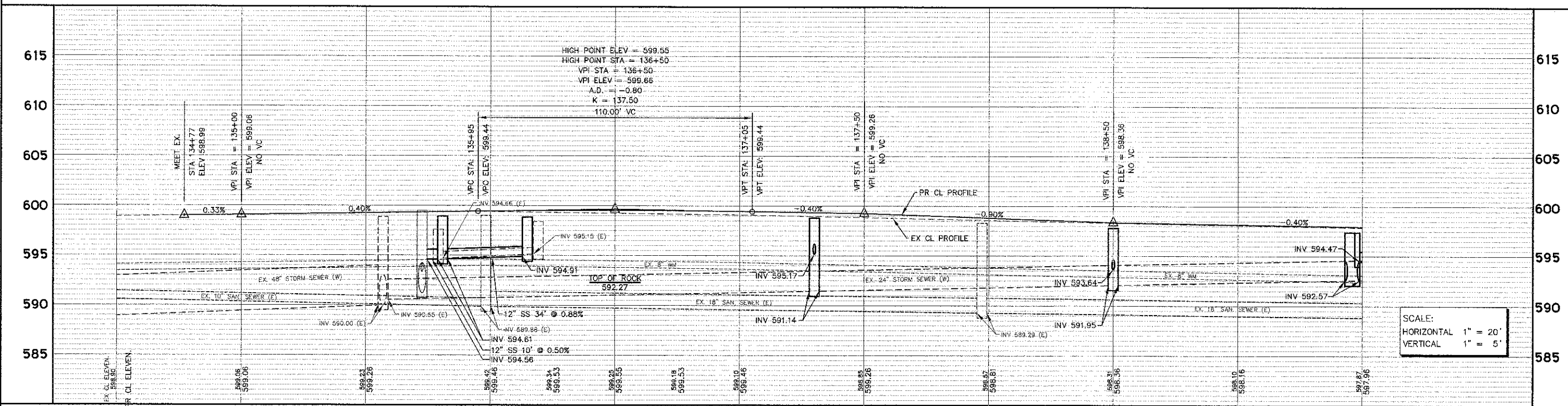
EROSION CONTROL AND LANDSCAPING PLAN
 SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 21
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT N-9003619			CONTRACT NO. 63788	

FOR: \GIS\PROJ\16802\DWG\FINAL_EWS\16802-EROSION



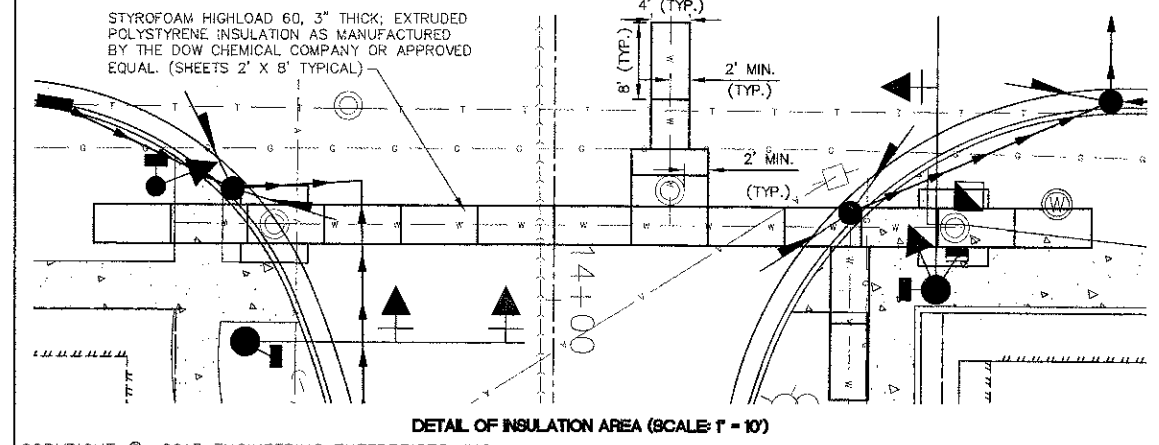
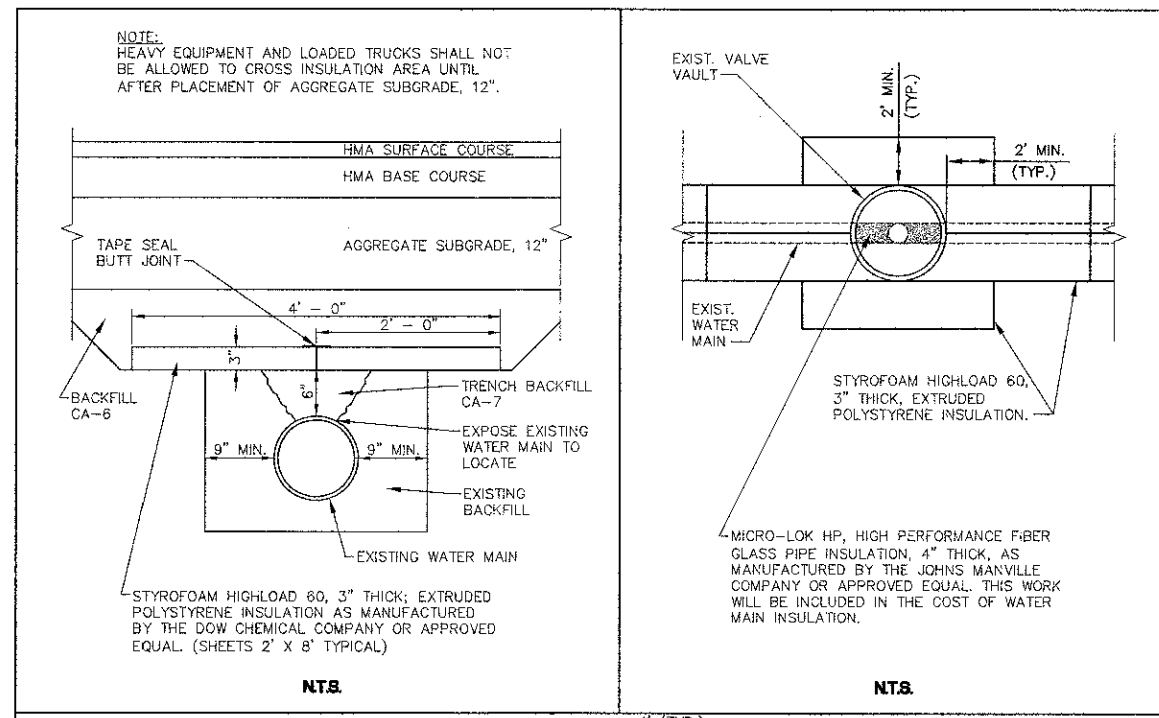
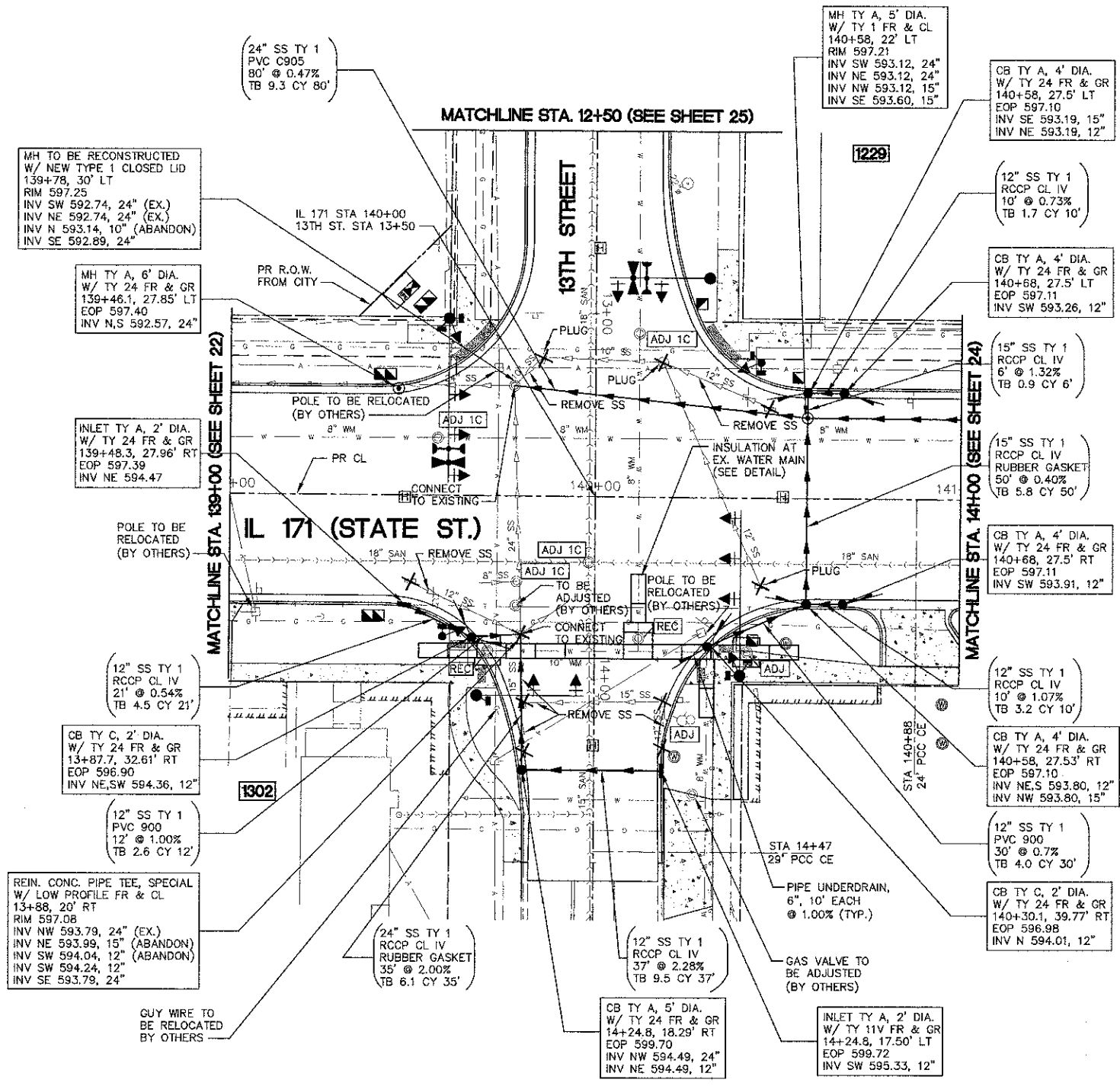
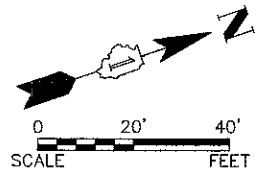
- NOTES:
1. ALL PROPOSED CATCH BASINS SHALL HAVE A 15" SUMP.
 2. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.



SCALE:
 HORIZONTAL 1" = 20'
 VERTICAL 1" = 5'

Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eetweb.com		USER NAME = Jim Schmidt PLOT SCALE = PLOT DATE = February 8, 2013	DESIGNED - JRL & SWM DRAWN - CLN CHECKED - JL DATE - 11/13/12	REVISED - JPS 02/08/13 REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		IL 171 (STATE STREET) DRAINAGE AND UTILITY PLAN AND PROFILE		F.A.P. RTE. = 577 SECTION = 10-00068-00-TL COUNTY = WILL TOTAL SHEETS = 65 SHEET NO. = 22 CONTRACT NO. = 63788 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-903(619)
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PLOTTED: February 8, 2013 @ 9:26 AM By: jim.schmidt Tab: 22 Route 171-22-34



- NOTES:
1. ALL PROPOSED CATCH BASINS SHALL HAVE A 15" SUMP.
 2. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

INSULATION AT EXISTING WATER MAIN DETAIL

Engineering Enterprises, Inc.
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 Phone: (630) 466-9350

SCALE: AS NOTED
 DATE: 5/21/12

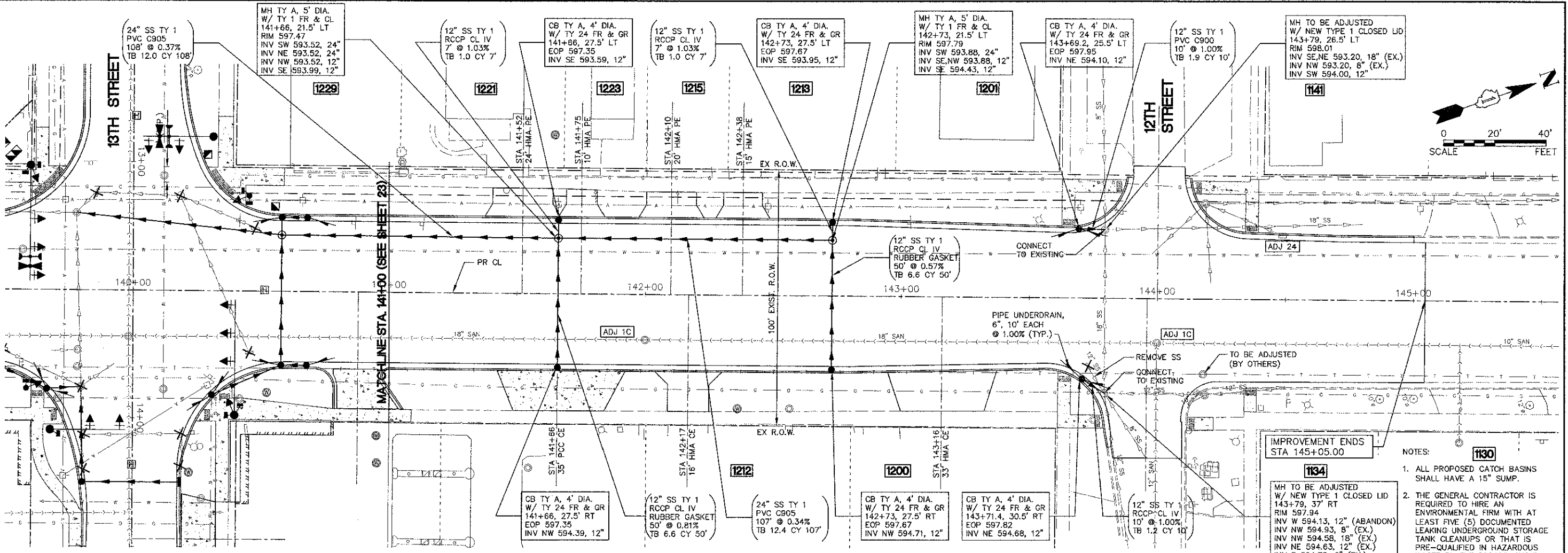
FOLDER: DRAWING NUMBER: DRAWN BY: CLN
 REVISED:

Plotter: March 19, 2013 @ 4:00 PM By: Larry Nalan - Tab: 23 Route 171 22x34
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Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.9700 / www.eefeb.com	USER NAME = Larry Nalan DESIGNED - JRL & SWM DRAWN - CLN CHECKED - JL PLOT DATE = March 19, 2013	DESIGNED - JRL & SWM DRAWN - CLN CHECKED - JL DATE = 11/13/12	REVISED - JPS 02/08/13 REVISED - CLN 03/19/13 REVISED - REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

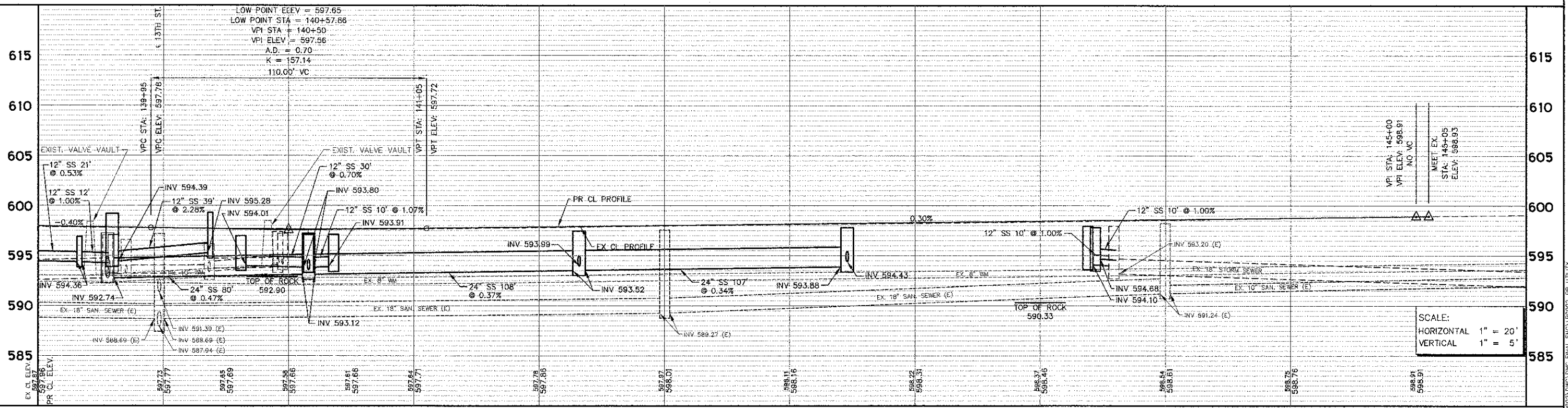
IL 171 (STATE STREET) DRAINAGE AND UTILITY PLAN		F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 23
SCALE: H=1"=20' V=1"=5'		SHEET NO. 2 OF 4 SHEETS		STA. 139+00 TO STA. 141+00		CONTRACT NO. 63788
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90036191						



IL 171 (STATE ST.)

- NOTES:
1. ALL PROPOSED CATCH BASINS SHALL HAVE A 15" SUMP.
 2. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

1134
 MH TO BE ADJUSTED
 W/ NEW TYPE 1 CLOSED LID
 143+79, 26.5' LT
 RIM 598.01
 INV SE, NE 593.20, 18" (EX.)
 INV NW 593.20, 8" (EX.)
 INV SW 594.00, 12"



SCALE:
 HORIZONTAL 1" = 20'
 VERTICAL 1" = 5'

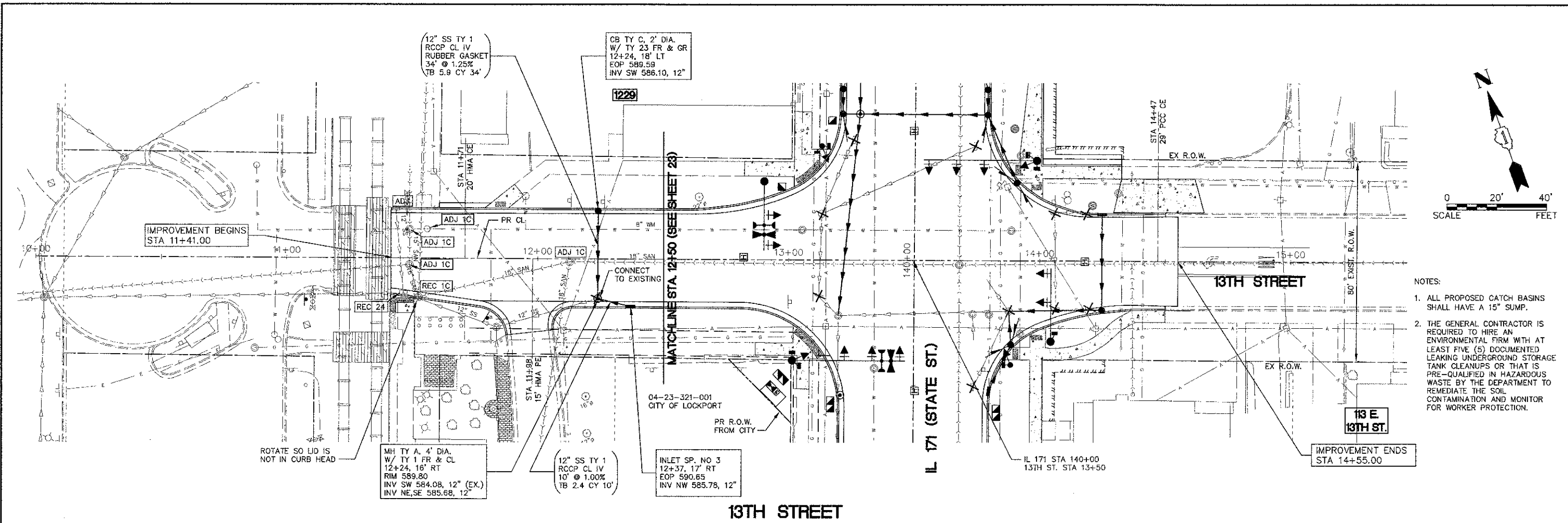
Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60054
 630.466.6700 / www.eeiweb.com

USER NAME = Jim Schmitt	DESIGNED - JRL & SWM	REVISED - JPS 02/08/13
PLOT SCALE =	DRAWN - CLN	REVISED -
PLOT DATE = February 8, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

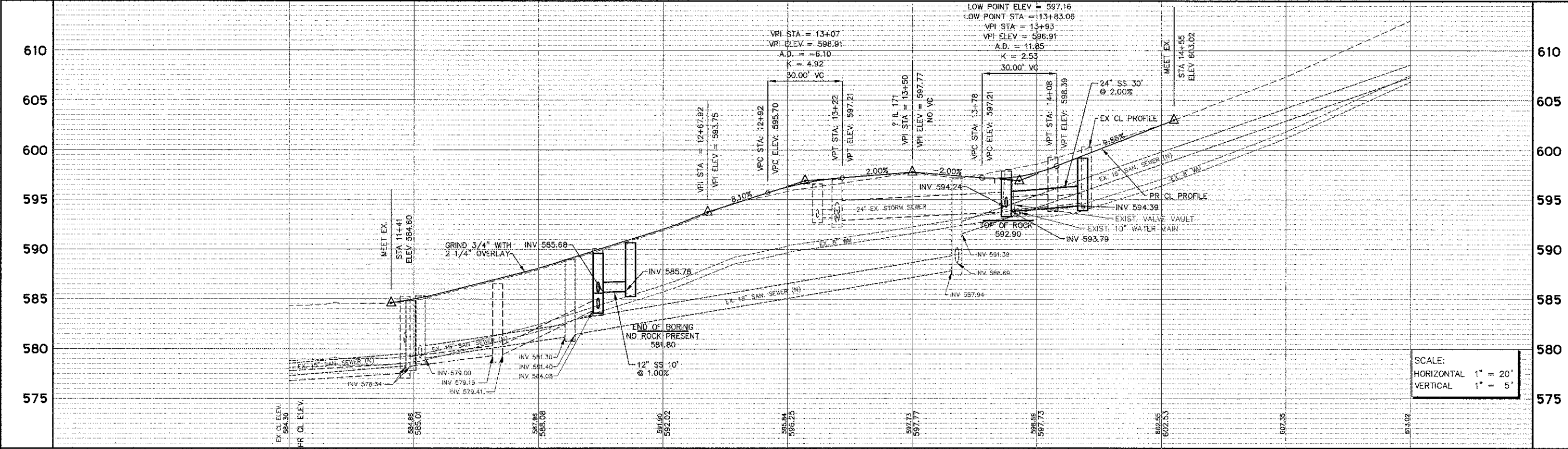
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IL 171 (STATE STREET)
 DRAINAGE AND UTILITY PLAN AND PROFILE
 SCALE: 1/4" = 20' VERT. SHEET NO. 3 OF 4 SHEETS
 STA. 141+00 TO STA. 145+00

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 24
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003619			CONTRACT NO. 63788	



- NOTES:
1. ALL PROPOSED CATCH BASINS SHALL HAVE A 15" SUMP.
 2. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

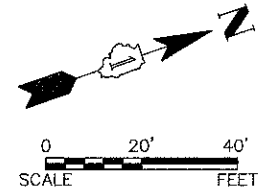


SCALE:
 HORIZONTAL 1" = 20'
 VERTICAL 1" = 5'

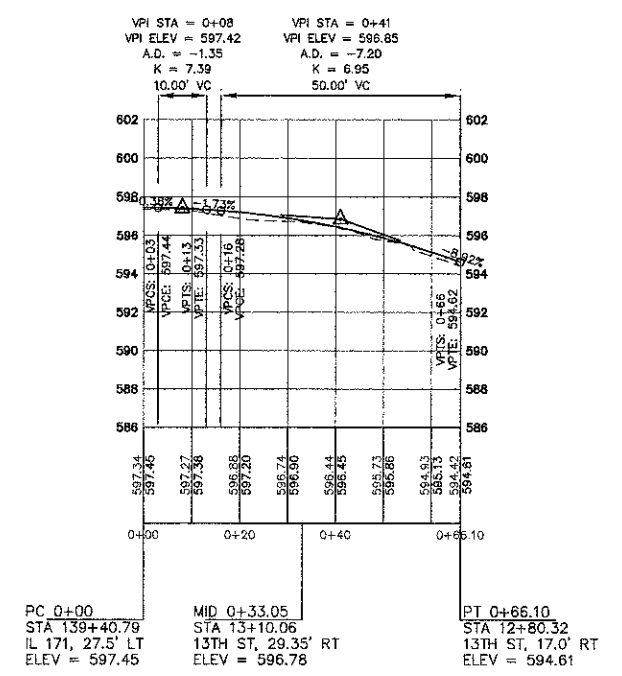
Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eelweb.com		USER NAME = Jim Schmidt PLOT SCALE = PLOT DATE = February 8, 2013	DESIGNED - JRL & SWM DRAWN - CLN CHECKED - JL DATE - 11/13/12	REVISED - JPS 02/08/13 REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		13th STREET DRAINAGE AND UTILITY PLAN AND PROFILE		F.A.P. RTE. 577 SECTION 10-00066-00-TL COUNTY WILL TOTAL SHEETS 65 SHEET NO. 25 CONTRACT NO. 63788	SCALE: HORIZ=20' VERT=5' SHEET NO. 4 OF 4 SHEETS STA. 11+00 TO STA. 15+50 FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-90036191
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Plotted: February 8, 2013 @ 9:31 AM By: Jim Schmidt - Job: 25 13th 22x34

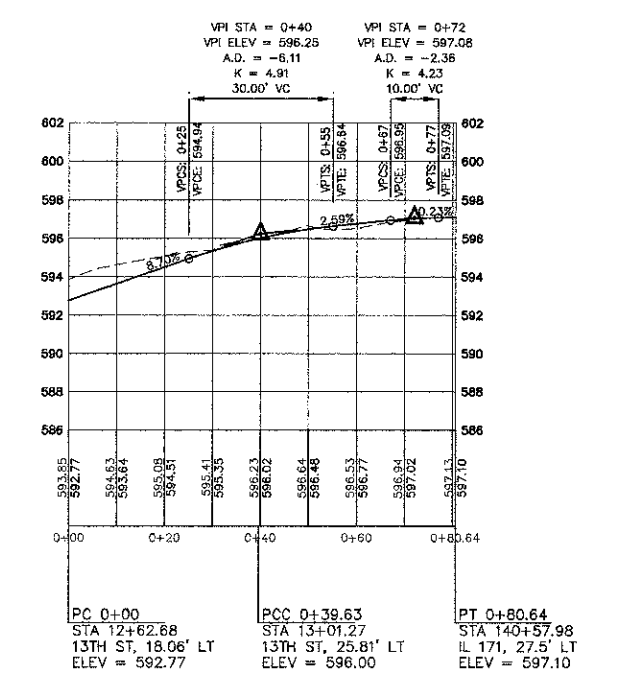
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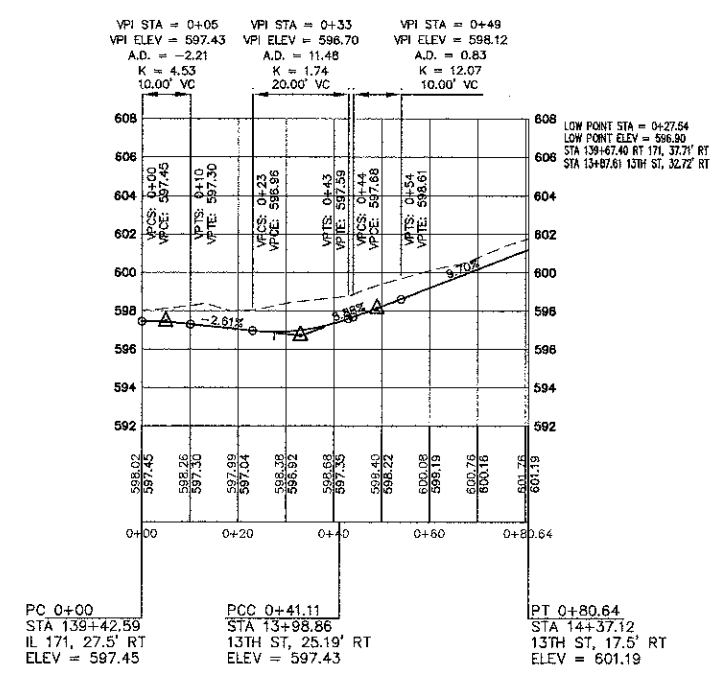
SW PROFILE



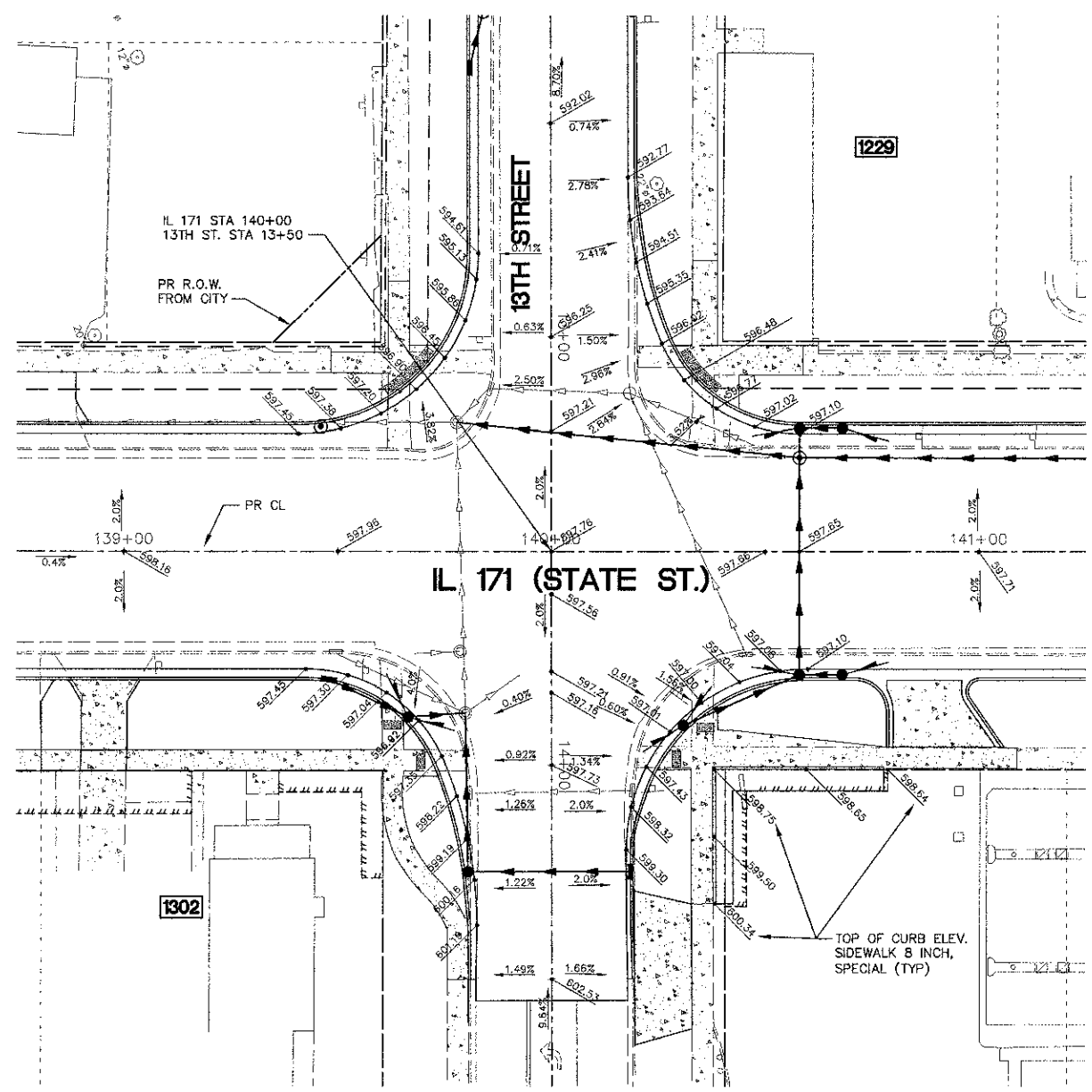
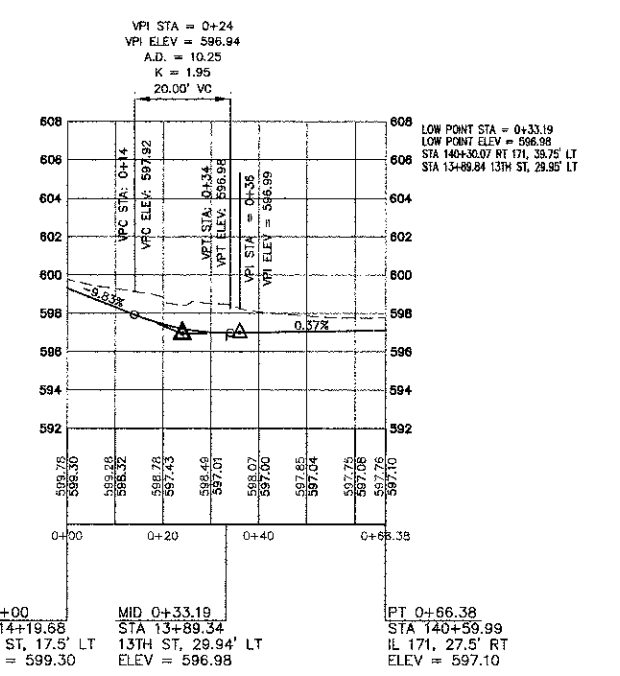
NW PROFILE



SE PROFILE



NE PROFILE

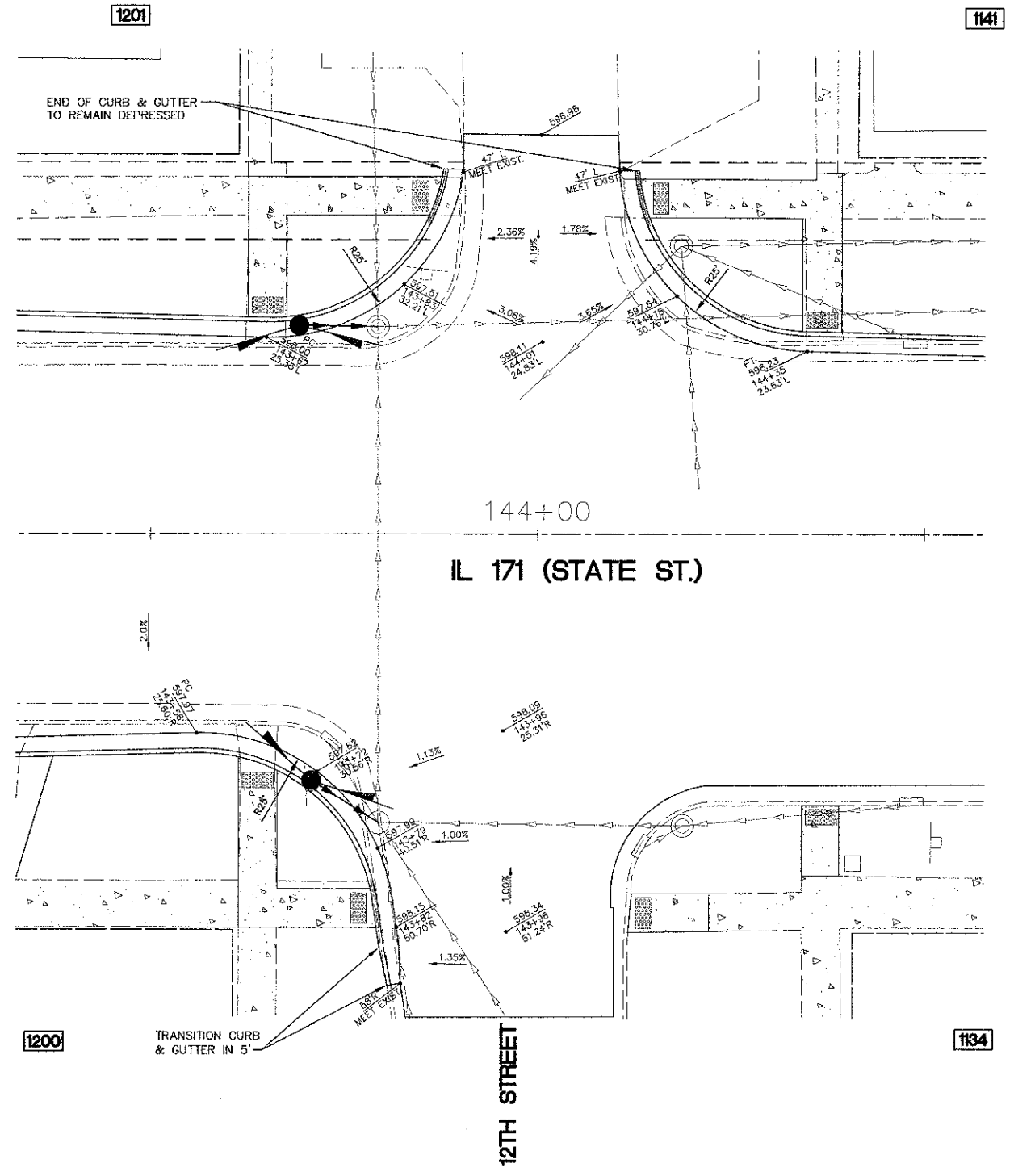
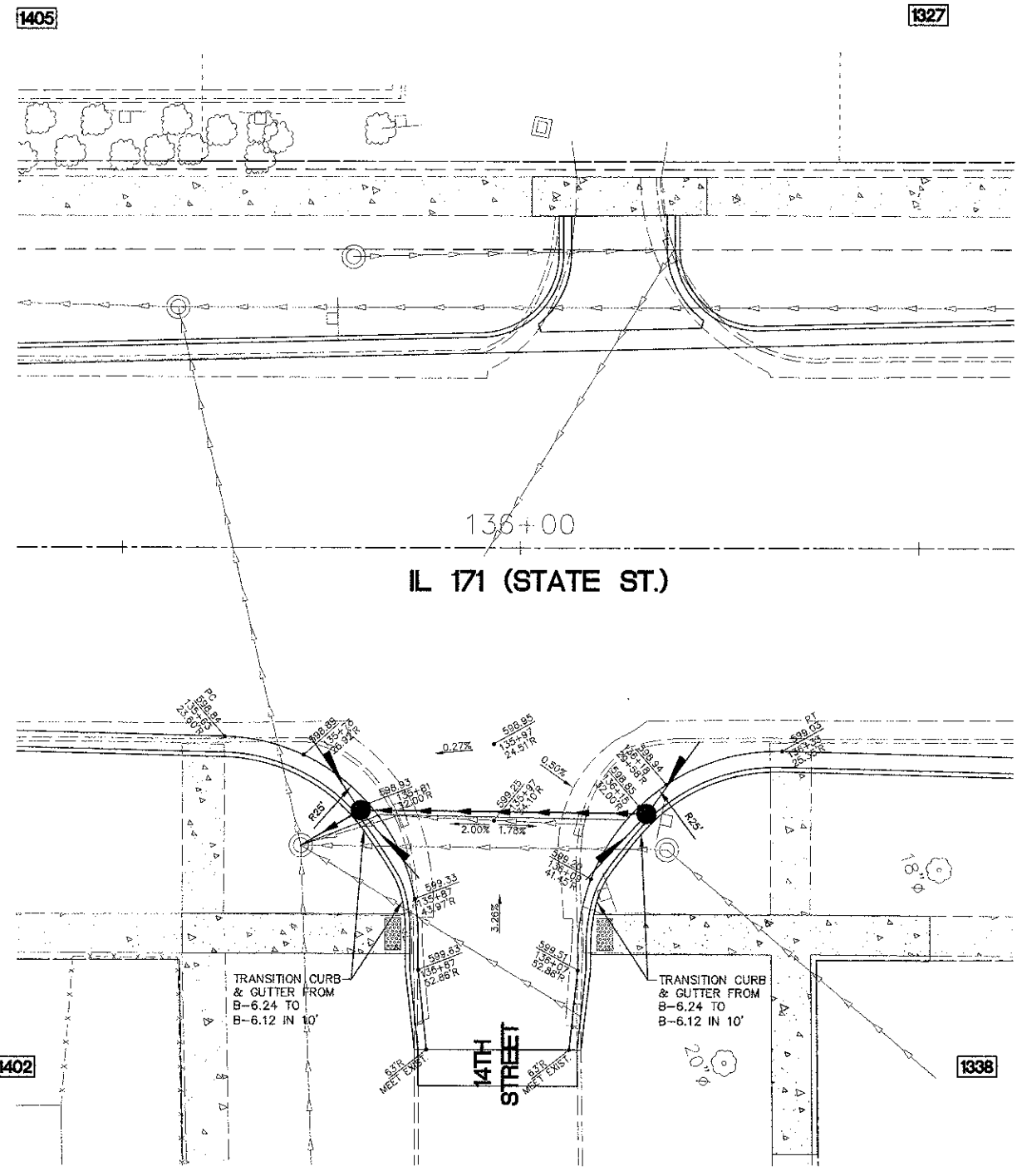
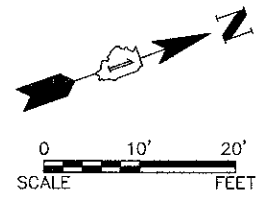


SCALE:
HORIZONTAL 1" = 20'
VERTICAL 1" = 5'

Plotted: February 8, 2013 @ 9:33 AM By: Jim Schmidt - Title: 26 Intersection 22x34
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Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eeiweb.com	USER NAME = Jim Schmidt PLOT SCALE = PLOT DATE = February 8, 2013	DESIGNED - JRL & SWM DRAWN - CLN CHECKED - JL DATE - 11/13/12	REVISED - JPS 02/08/13 REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		IL 171 AND 13TH STREET INTERSECTION DETAIL		F.A.P. RTE. = 577 SECTION = 10-00068-00-TL COUNTY = WILL TOTAL SHEETS = 65 SHEET NO. = 26 CONTRACT NO. = 63788 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)
	SCALE: H=1"=20' V=1"=5' SHEET NO. 1 OF 2 SHEETS STA. N/A TO STA. N/A							

For: C:\Users\jpschmi\Documents\DWG\DWG FINAL ENGL\9002-INTERSECTION



Plotted: February 8, 2013 @ 9:32 AM By: Jim Schmidt - Tab: 27 Intersection 27x34
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USER NAME =	Jim Schmidt
PLOT SCALE =	
PLOT DATE =	February 8, 2013

DESIGNED -	JRL & SWM
DRAWN -	CLN
CHECKED -	JL
DATE -	11/13/12

REVISED -	JPS 02/08/13
REVISED -	
REVISED -	
REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

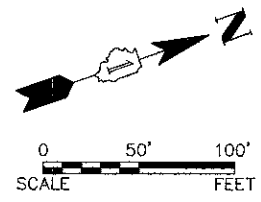
IL 171 AND 14TH STREET AND IL 171 AND 12TH STREET INTERSECTION DETAILS
 SCALE: HORIZ=20' VERT=5' SHEET NO. 2 OF 2 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	27
				CONTRACT NO. 63788
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)				

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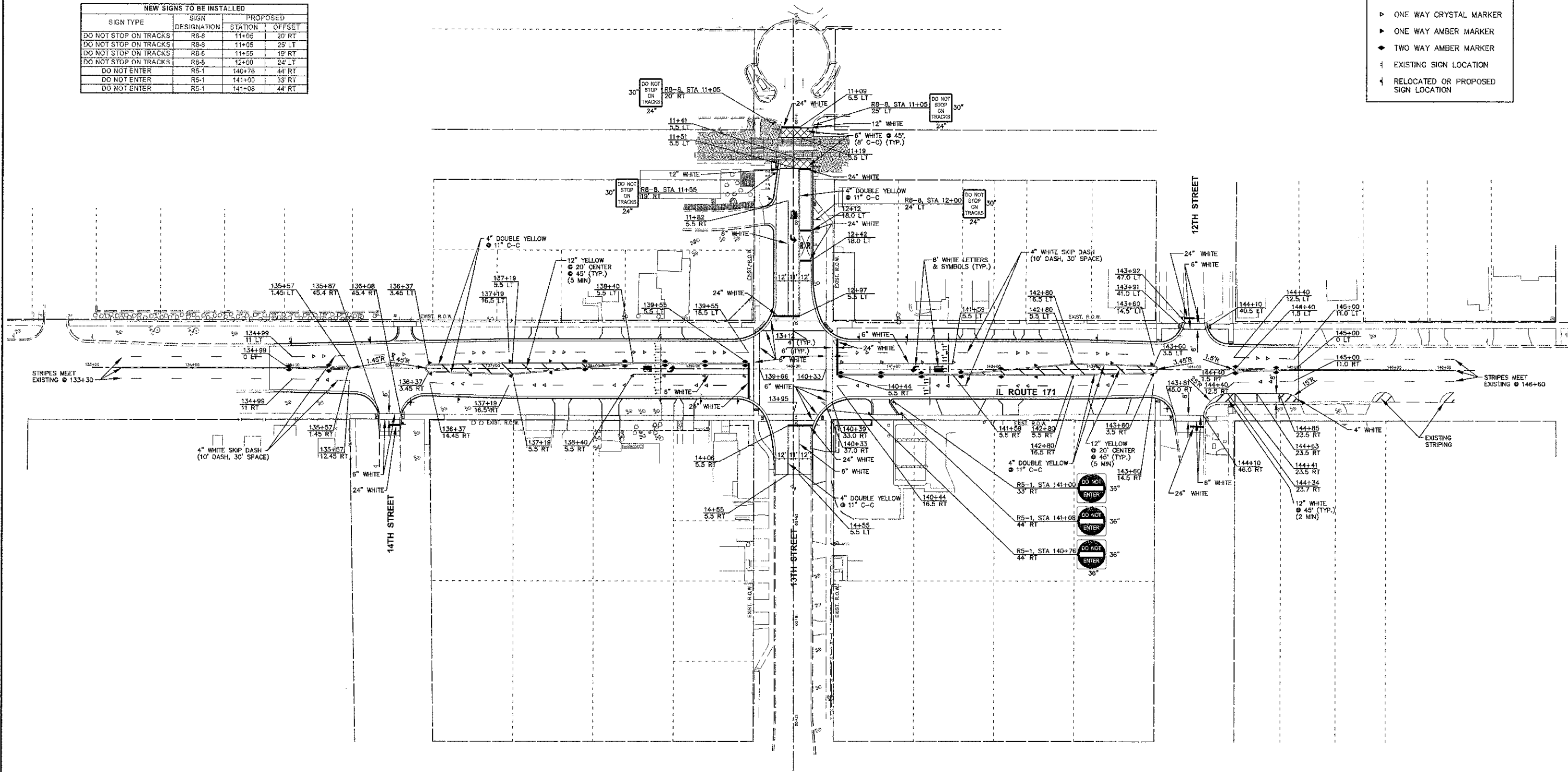
SIGNS TO BE RELOCATED					
SIGN TYPE	SIGN DESIGNATION	STATION	EXISTING OFFSET	PROPOSED STATION	PROPOSED OFFSET
ONE WAY	R6-2	11+75	32' RT	11+80	24' RT
SPEED LIMIT	R2-1	134+98	28' LT	134+98	26' LT
SCHOOL	S1-1	136+77	28' LT	135+77	28' LT
STOP	R1-1	136+10	43' RT	136+10	55' RT
STREET NAME	D3-2	136+18	35' RT	136+18	37' RT
SPEED LIMIT	R2-1	136+66	29' RT	136+66	31' RT
PARKING RESTRICTIONS	R7-1	136+66	29' RT	136+68	31' RT
METRA		139+08	27' RT	139+08	32' RT
PACE		139+67	28' RT	139+45	32' RT
PACE		140+41	28' LT	140+65	32' LT
PARKING RESTRICTIONS	R7-1	140+86	27' LT	143+03	32' LT
METRA		141+13	27' LT	141+13	32' LT
SPEED LIMIT	R2-1	143+03	29' LT	143+03	32' LT
STREET NAME	D3-2	143+71	33' RT	143+73	40' RT
STOP	R1-1	143+86	34' LT	143+38	52' LT

NEW SIGNS TO BE INSTALLED			
SIGN TYPE	SIGN DESIGNATION	STATION	PROPOSED OFFSET
DO NOT STOP ON TRACKS	R8-8	11+05	20' RT
DO NOT STOP ON TRACKS	R8-8	11+05	25' LT
DO NOT STOP ON TRACKS	R8-8	11+55	19' RT
DO NOT STOP ON TRACKS	R8-8	12+00	24' LT
DO NOT ENTER	RS-1	140+78	44' RT
DO NOT ENTER	RS-1	141+00	33' RT
DO NOT ENTER	RS-1	141+08	44' RT



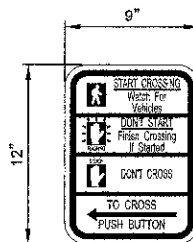
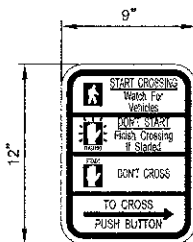
LEGEND

- ▶ ONE WAY CRYSTAL MARKER
- ▶ ONE WAY AMBER MARKER
- ◀ TWO WAY AMBER MARKER
- ⊥ EXISTING SIGN LOCATION
- ⊥ RELOCATED OR PROPOSED SIGN LOCATION



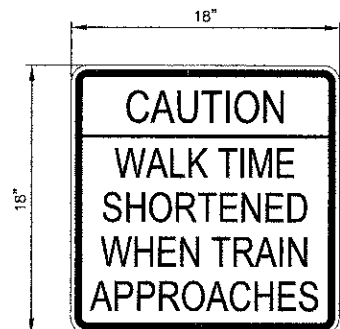
2/14/13 March 21, 2013 @ 1:49 PM By: Larry Nolan - Tab. 28 Stripes & Sign 2/2x24
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Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60654 630.466.6700 / www.eelweb.com	USER NAME = Larry Nolan PLOT SCALE = PLOT DATE = March 21, 2013	DESIGNED - JRL & SWM DRAWN - CLN CHECKED - JL DATE - 11/13/12	REVISED - JPS 02/08/13 REVISED - CLN 03/19/13 REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		PAVEMENT MARKING AND SIGNAGE PLAN				F.A.P. RTE. = 577 SECTION = 10-00068-00-TL COUNTY = WILL TOTAL SHEETS = 65 SHEET NO. = 28 CONTRACT NO. = 63768
	SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. N/A TO STA. N/A				FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-9003(619)					
	STRIPES MEET EXISTING @ 133+30 STRIPES MEET EXISTING @ 146+60									
	EXISTING STRIPING									



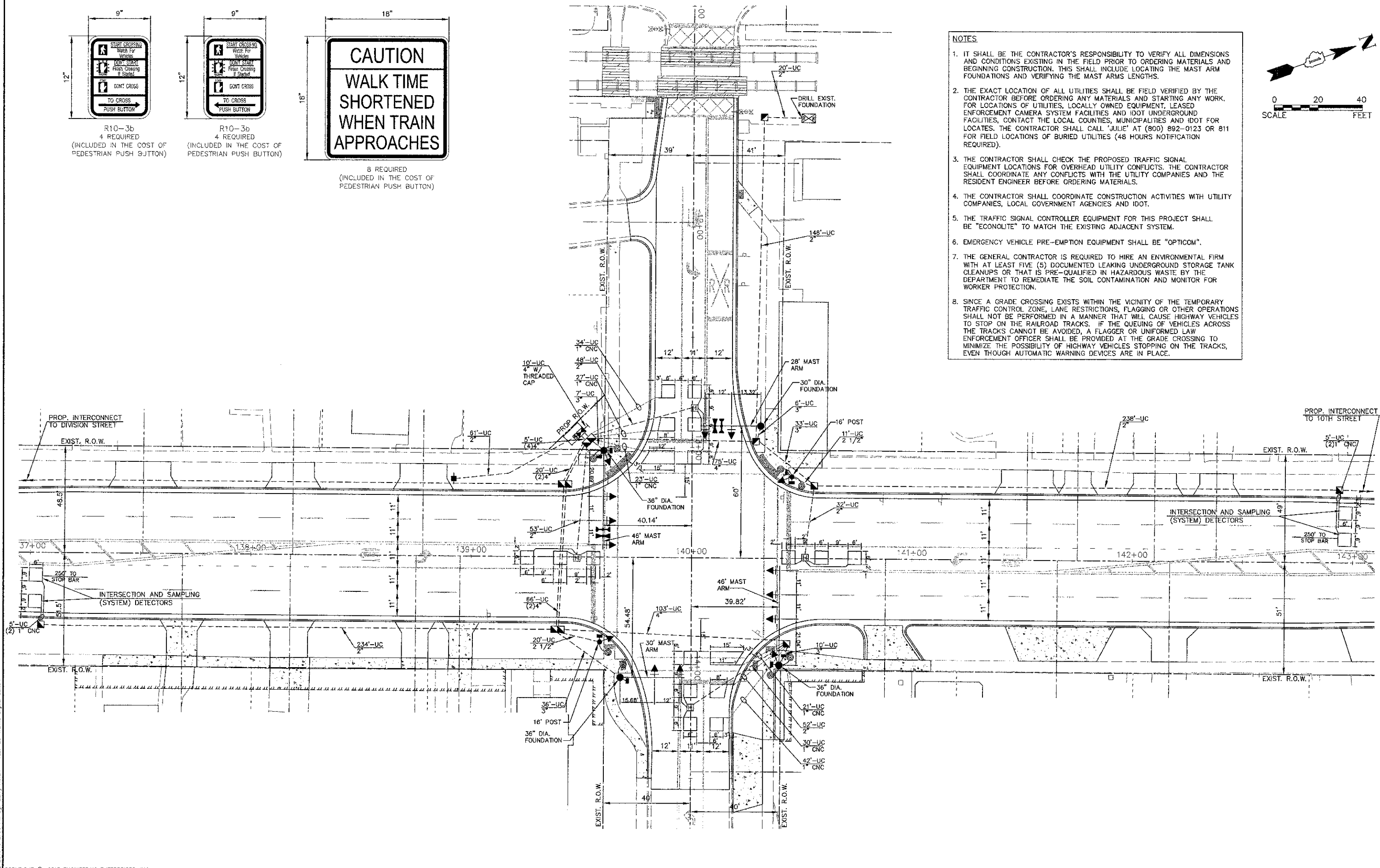
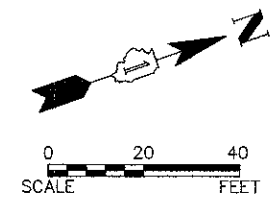
R10-3b
4 REQUIRED
(INCLUDED IN THE COST OF PEDESTRIAN PUSH BUTTON)

R10-3b
4 REQUIRED
(INCLUDED IN THE COST OF PEDESTRIAN PUSH BUTTON)



8 REQUIRED
(INCLUDED IN THE COST OF PEDESTRIAN PUSH BUTTON)

- NOTES**
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARMS LENGTHS.
 - THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL 'JULIE' AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
 - THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
 - THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
 - THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.
 - EMERGENCY VEHICLE PRE-EMPTION EQUIPMENT SHALL BE "OPTICOM".
 - THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.
 - SINCE A GRADE CROSSING EXISTS WITHIN THE VICINITY OF THE TEMPORARY TRAFFIC CONTROL ZONE, LANE RESTRICTIONS, FLAGGING OR OTHER OPERATIONS SHALL NOT BE PERFORMED IN A MANNER THAT WILL CAUSE HIGHWAY VEHICLES TO STOP ON THE RAILROAD TRACKS. IF THE QUEUING OF VEHICLES ACROSS THE TRACKS CANNOT BE AVOIDED, A FLAGGER OR UNIFORMED LAW ENFORCEMENT OFFICER SHALL BE PROVIDED AT THE GRADE CROSSING TO MINIMIZE THE POSSIBILITY OF HIGHWAY VEHICLES STOPPING ON THE TRACKS, EVEN THOUGH AUTOMATIC WARNING DEVICES ARE IN PLACE.



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USER NAME = Larry Nolan
 PLOT SCALE =
 PLOT DATE = March 20, 2013

DESIGNED - JRL & SWM
 DRAWN - CLN
 CHECKED - JL
 DATE - 11/13/12

REVISED - JPS 02/08/13
 REVISED - CLN 03/19/13
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

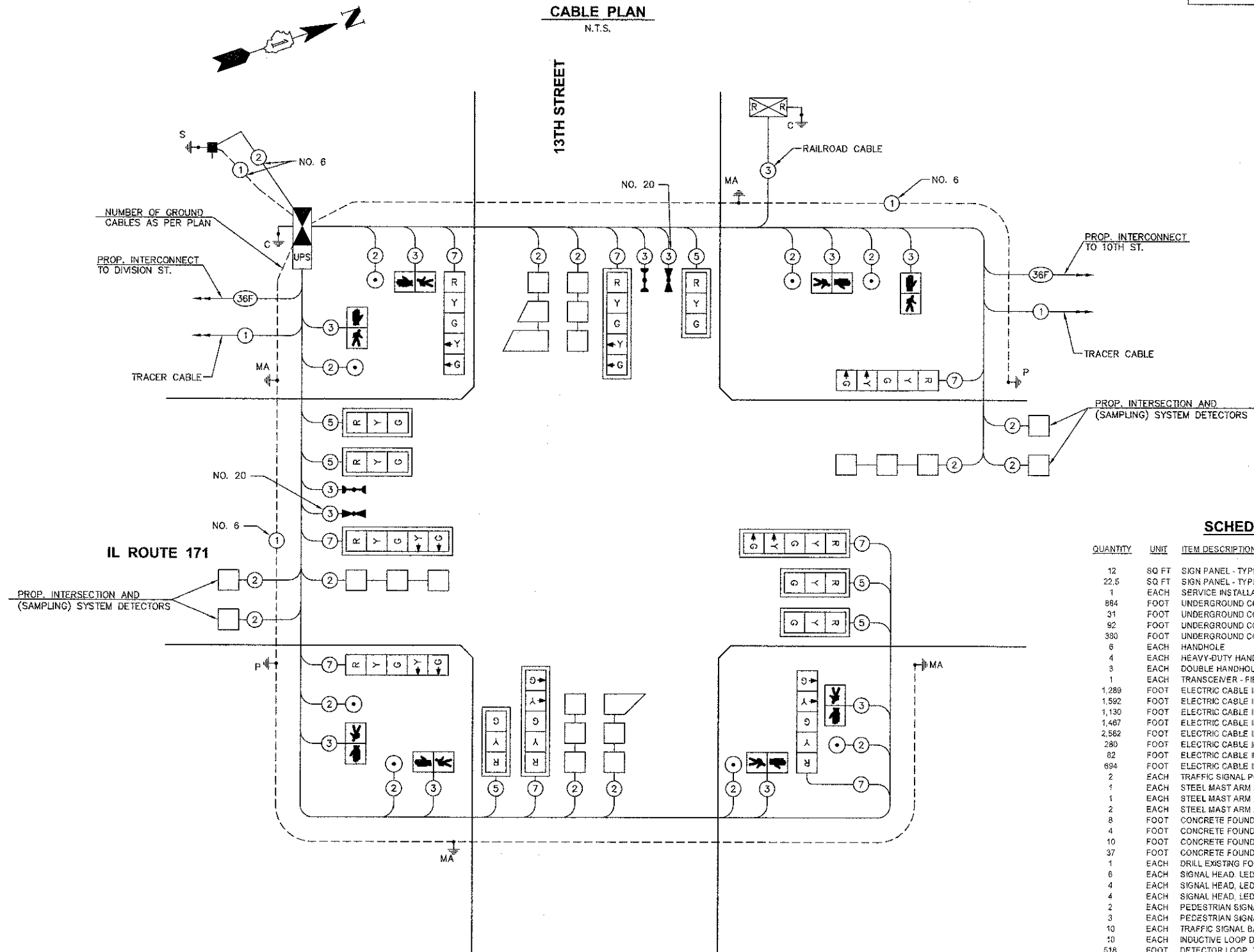
**TRAFFIC SIGNAL PLAN
 IL RTE 171 AND 13TH STREET**

SCALE: 1"=20' SHEET NO. 1 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	29
CONTRACT NO. 63788				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)				

- NOTES:**
1. THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.
 2. EMERGENCY VEHICLE PRE-EMPTION EQUIPMENT SHALL BE "OPTICOM".
 3. THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

CABLE PLAN
N.T.S.



SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM DESCRIPTION
12	SQ FT	SIGN PANEL - TYPE 1
22.5	SQ FT	SIGN PANEL - TYPE 2
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
884	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
31	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
82	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
380	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
6	EACH	HANDHOLE
4	EACH	HEAVY-DUTY HANDHOLE
3	EACH	DOUBLE HANDHOLE
1	EACH	TRANSCEIVER - FIBER OPTIC
1,289	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
1,592	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1,130	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1,487	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
2,562	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14-1 PAIR
280	FOOT	ELECTRIC CABLE IN CONDUIT, RAILROAD, NO. 14 3C
82	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE NO. 6 2 C
694	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1 C
2	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.
2	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 46 FT.
8	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
10	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
37	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
1	EACH	DRILL EXISTING FOUNDATION
6	EACH	SIGNAL HEAD LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
4	EACH	SIGNAL HEAD LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
4	EACH	SIGNAL HEAD LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED
3	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED
10	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
20	EACH	INDUCTIVE LOOP DETECTOR
518	FOOT	DETECTOR LOOP, TYPE I
2	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
6	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	RAILROAD, FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	EACH	UNINTERRUPTIBLE POWER SUPPLY, SPECIAL
247	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE	% OPERATIONS		
SIGNAL (RED)	14	17	0.50		119.00
(YELLOW)	14	25	0.25		87.50
(GREEN)	14	15	0.25		52.50
ARROW	16	12	0.10		19.20
PED. SIGNAL	8	25	1.00		200.00
CONTROLLER	1	100	1.00		100.00
ILLUM. SIGN	-	25	0.05		-
FLASHER					0.50
TOTAL					578.20

ENERGY COST - BILLED TO: CITY OF LOCKPORT (ADDRESS) 222 EAST NINTH STREET LOCKPORT, IL 60441

ENERGY SUPPLY - CONTACT: JAMES GLOVER (PHONE) 815-724-5054 (COMPANY) COMED

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REVISD - CLN 03/19/13
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REVISD -

PLDT SCALE =
PLDT DATE = March 20, 2013

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES, CABLE PLAN,
IL RTE 171 AND 13TH STREET**

SCALE: N.T.S. SHEET NO. 2 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	30

CONTRACT NO. 63788
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(6)9

Plotted: March 20, 2013 @ 2:36 PM By: Larry Nolan - Tab: 30 Cable Plan 22x34

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PHASE 2+6 SHALL BE PLACED ON RECALL

SEQUENCE OF OPERATION

MOVEMENT	5-1		6-1		5-2		2-6		7-3		3-8		4-7		4-8																			
PHASE	1+5		1+6		2+5		2+6		3+7		3+8		4+7		4+8		F																	
INTERVAL	1	2	3	4	5	6	7	8	9	10	11	12	13A	13B	14	15	16	17	18	19	20A	20B	21	22	23	24A	24B	25	26	27	28A	28B	L	
CHANGE TO		1+6	2+5	2+6	θ	θ	θ	θ					3+7 3+8	4+7 4+8		1+5 1+6 2+5 2+6 4+8	3+8	4+7	θ	θ	1+5 1+6	2+5 2+6	4+8	θ	θ	1+5 1+6	2+5 2+6	4+8			1+5 1+6	2+5 2+6	A	
IL 171 MID AND FAR RIGHT MAST ARM SIGNALS	N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
IL 171 FAR LEFT AND END MAST ARM SIGNALS	N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
IL 171 MID AND FAR RIGHT MAST ARM SIGNALS	S/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
IL 171 FAR LEFT AND END MAST ARM SIGNALS	S/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
13TH STREET FAR RIGHT MAST ARM SIGNAL	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
13TH STREET FAR LEFT AND END MAST ARM SIGNALS	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
13TH STREET FAR RIGHT MAST ARM SIGNAL	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
13TH STREET FAR LEFT AND END MAST ARM SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
PEDESTRIAN SIGNALS CROSSING IL 171 ON NORTH SIDE OF 13TH STREET		H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H
PEDESTRIAN SIGNALS CROSSING IL 171 ON SOUTH SIDE OF 13TH STREET		H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H
PEDESTRIAN SIGNALS CROSSING 13TH STREET ON EAST SIDE OF IL 171		H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H
PEDESTRIAN SIGNALS CROSSING 13TH STREET ON WEST SIDE OF IL 171		H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H

- * TO APPEAR ONLY UPON PUSHBUTTON ACTUATION.
- ** FLASHING "H" IS TO TERMINATE AT THE COMPLETION OF THE PEDESTRIAN INTERVAL CLEARANCE.
- θ THIS "H" OR FLASHING "H" INTERVAL MAY FINISH TIMING IN THE BIDIRECTIONAL STRAIGHT THROUGH MOVEMENT IF THE LEFT ARROW TIME IS NOT SUFFICIENT TO COMPLETE "H" OR FLASHING "H" INTERVALS.
- P = ILLUMINATED PERSON = WALK
- FH = ILLUMINATE FLASHING HAND = FLASHING DON'T WALK
- H = ILLUMINATED SOLID HAND = DON'T WALK

RAILROAD PREEMPTION SEQUENCE OF OPERATION

CHANGES FROM NORMAL SEQUENCE OF OPERATIONS INTERVAL NUMBER	PREEMPTOR NUMBER 3		PREEMPTOR NUMBER 4		PREEMPTOR NUMBER 2												CLEAR TO NORMAL SEQUENCE																						
	1	5	8	11	14	18	22	26	2	3	4	5	6	7	8	9		10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	
CHANGES FROM NORMAL SEQUENCE OF OPERATIONS INTERVAL NUMBER																																							
CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER																																							
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	2	3	4	5																		
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	2	1C	2	1E	2	1G	2	2	1K	2	2	1N	2	1Q	2	1S	2	3	4	5																			
IL 171 MID AND FAR RIGHT MAST ARM SIGNALS	N/B	R	R	R	Y	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R		
IL 171 FAR LEFT AND END MAST ARM SIGNALS	N/B	R	R	R	Y	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R		
IL 171 MID AND FAR RIGHT MAST ARM SIGNALS	S/B	R	Y	R	R	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R		
IL 171 FAR LEFT AND END MAST ARM SIGNALS	S/B	R	Y	R	R	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R		
13TH STREET FAR RIGHT MAST ARM SIGNAL	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R		
13TH STREET FAR LEFT AND END MAST ARM SIGNALS	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R		
13TH STREET FAR RIGHT MAST ARM SIGNAL	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R		
13TH STREET FAR LEFT AND END MAST ARM SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R		
PEDESTRIAN SIGNALS CROSSING IL 171 ON NORTH SIDE OF 13TH STREET		H	H	H	H	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H			
PEDESTRIAN SIGNALS CROSSING IL 171 ON SOUTH SIDE OF 13TH STREET		H	H	H	H	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H			
PEDESTRIAN SIGNALS CROSSING 13TH STREET ON EAST SIDE OF IL 171		H	H	H	FH	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H			
PEDESTRIAN SIGNALS CROSSING 13TH STREET ON WEST SIDE OF IL 171		H	FH	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H			

Δ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

6/21
 3/19/2013 9:41 PM By: Larry Nolan - Tab: 31 Sequence 22-34

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PLOT SCALE =	DRAWN - CLN	REVISED - CLN 03/19/13
PLOT DATE = March 19, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SEQUENCE OF OPERATION AND RAILROAD
PREEMPTION SEQUENCE OF OPERATION
IL RTE 171 AND 13TH STREET

SCALE: N.T.S. SHEET NO. 3 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	31
CONTRACT NO. 63788				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)				

Path: H:\S05\PROJ\106062\DWG\DWG_FINAL_ENG\106062-303A1

EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION

CHANGES FROM NORMAL SEQUENCE OF OPERATIONS INTERVAL NUMBER	1	5		5		8		8		11		11		14		18		18		22		22		26		26		PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	CLEAR TO NORMAL SEQUENCE					
		1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	1V	1W	1X	1Y	1Z	1AA	1BB	1CC	1DD		1EE	1FF	2	3	
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	2 OR 3	1C	2	1E	1F	3	1H	2	1K	1L	3	2	1P	1Q	3	2	1T	1U	2	1W	3	1Y	1Z	2	1BB	3	1DD	1EE	2	3			◇		
IL 171 MID AND FAR RIGHT MAST ARM SIGNALS	N/B	R	R	R	R	R	R	G	G	G	Y	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	◇	
IL 171 FAR LEFT AND END MAST ARM SIGNALS	N/B	←Y	R	R	R	R	R	←G	←G	←G	Y	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	◇	
IL 171 MID AND FAR RIGHT MAST ARM SIGNALS	S/B	R	G	G	G	Y	R	R	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	◇	
IL 171 FAR LEFT AND END MAST ARM SIGNALS	S/B	←Y	←G	←G	←G	Y	R	R	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	◇	
13TH STREET FAR RIGHT MAST ARM SIGNAL	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	◇	
13TH STREET FAR LEFT AND END MAST ARM SIGNALS	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	←Y	←G	Y	R	←G	←Y	R	R	R	R	R	R	R	R	R	R	R	◇	
13TH STREET FAR RIGHT MAST ARM SIGNAL	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	◇	
13TH STREET FAR LEFT AND END MAST ARM SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	←Y	R	R	R	R	R	←G	Y	R	←G	←Y	G	Y	R	G	R	G	◇	
PEDESTRIAN SIGNALS CROSSING IL 171 ON NORTH SIDE OF 13TH STREET		H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	◇	
PEDESTRIAN SIGNALS CROSSING IL 171 ON SOUTH SIDE OF 13TH STREET		H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◇
PEDESTRIAN SIGNALS CROSSING 13TH STREET ON EAST SIDE OF IL 171		H	H	H	H	H	H	FH	H	FH	H	H	FH	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◇	
PEDESTRIAN SIGNALS CROSSING 13TH STREET ON WEST SIDE OF IL 171		H	FH	H	FH	H	H	H	H	H	H	FH	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◇	

◇ EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

Plotted: March 19, 2013 @ 4:45 PM By: Larry Nolan - Tab. 32 Emergency 22c34

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CHECKED - JL
DATE - 11/13/12

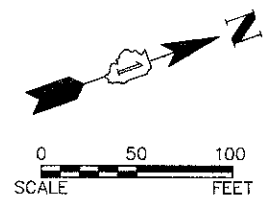
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REVISED - CLN 03/19/13
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

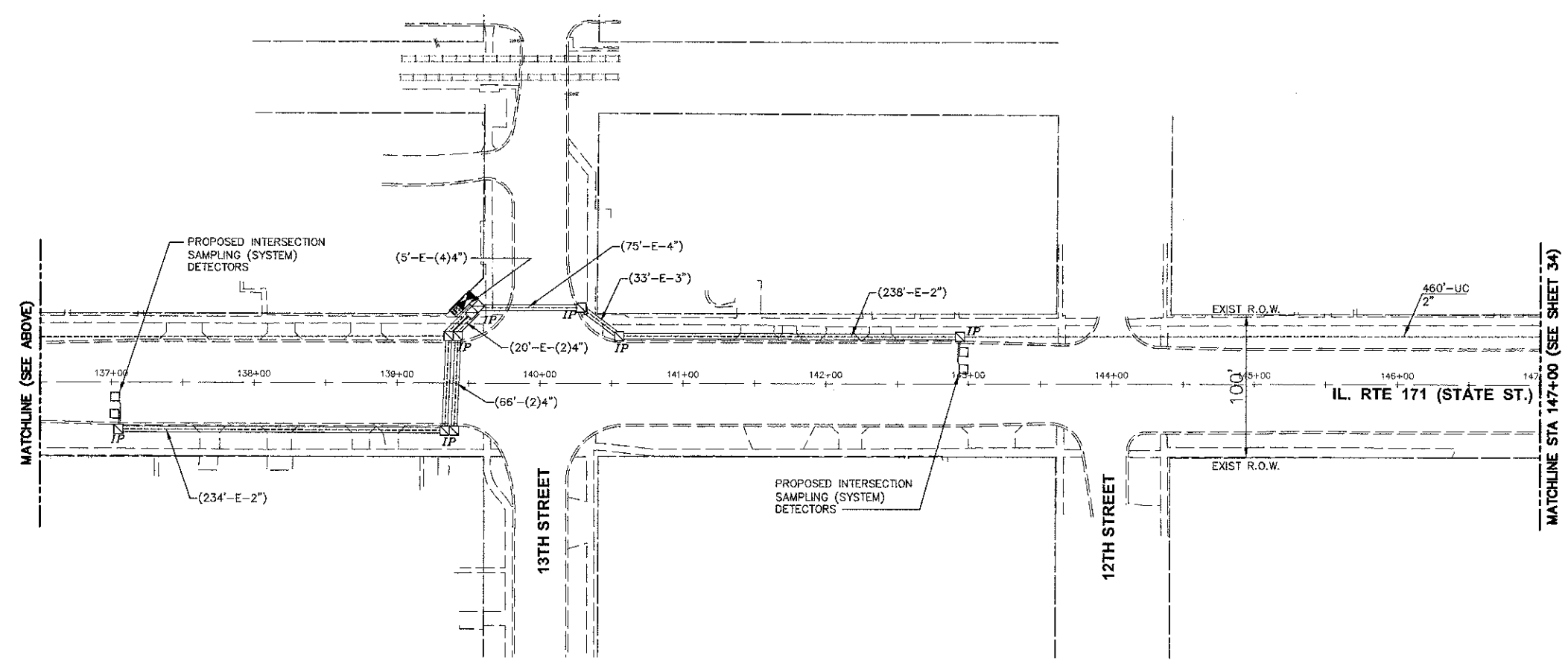
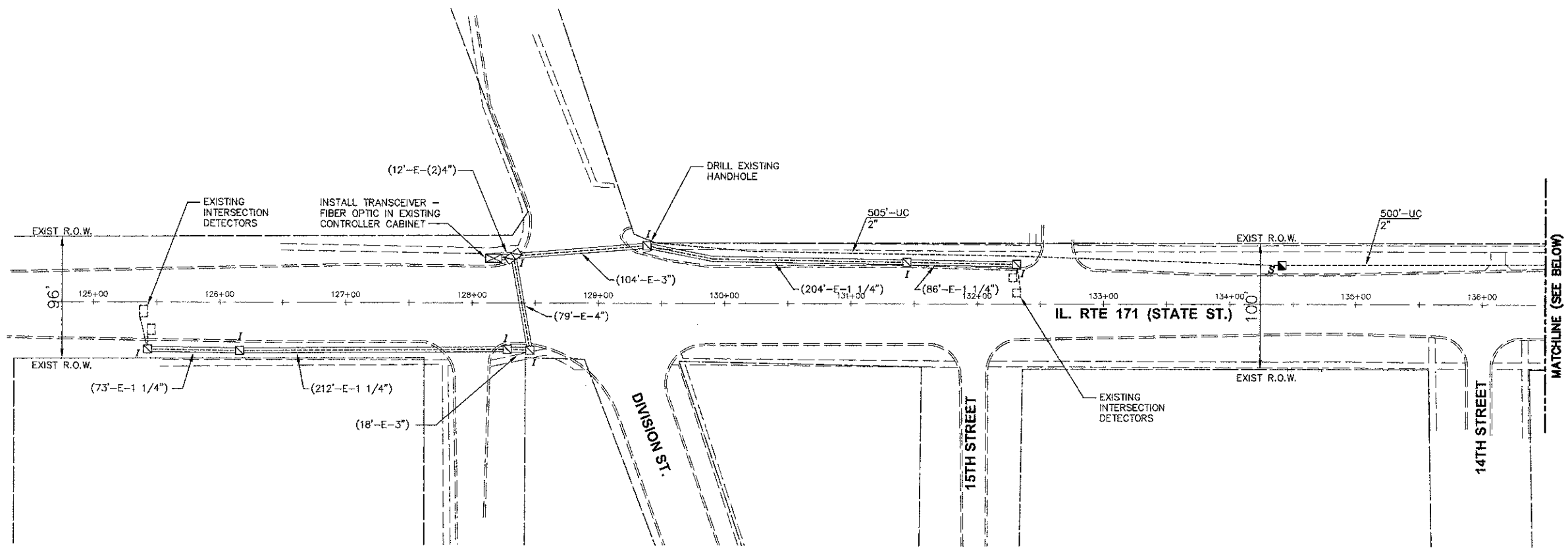
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION
IL RTE 171 AND 13TH STREET
SCALE: N.T.S. SHEET NO. 4 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	32
CONTRACT NO. 63788			FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-9003(619)	

Plot: H:\50583800\110902\DWG\DWG_FINAL_ENG\110902-SIGNAL



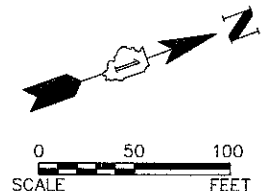
NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



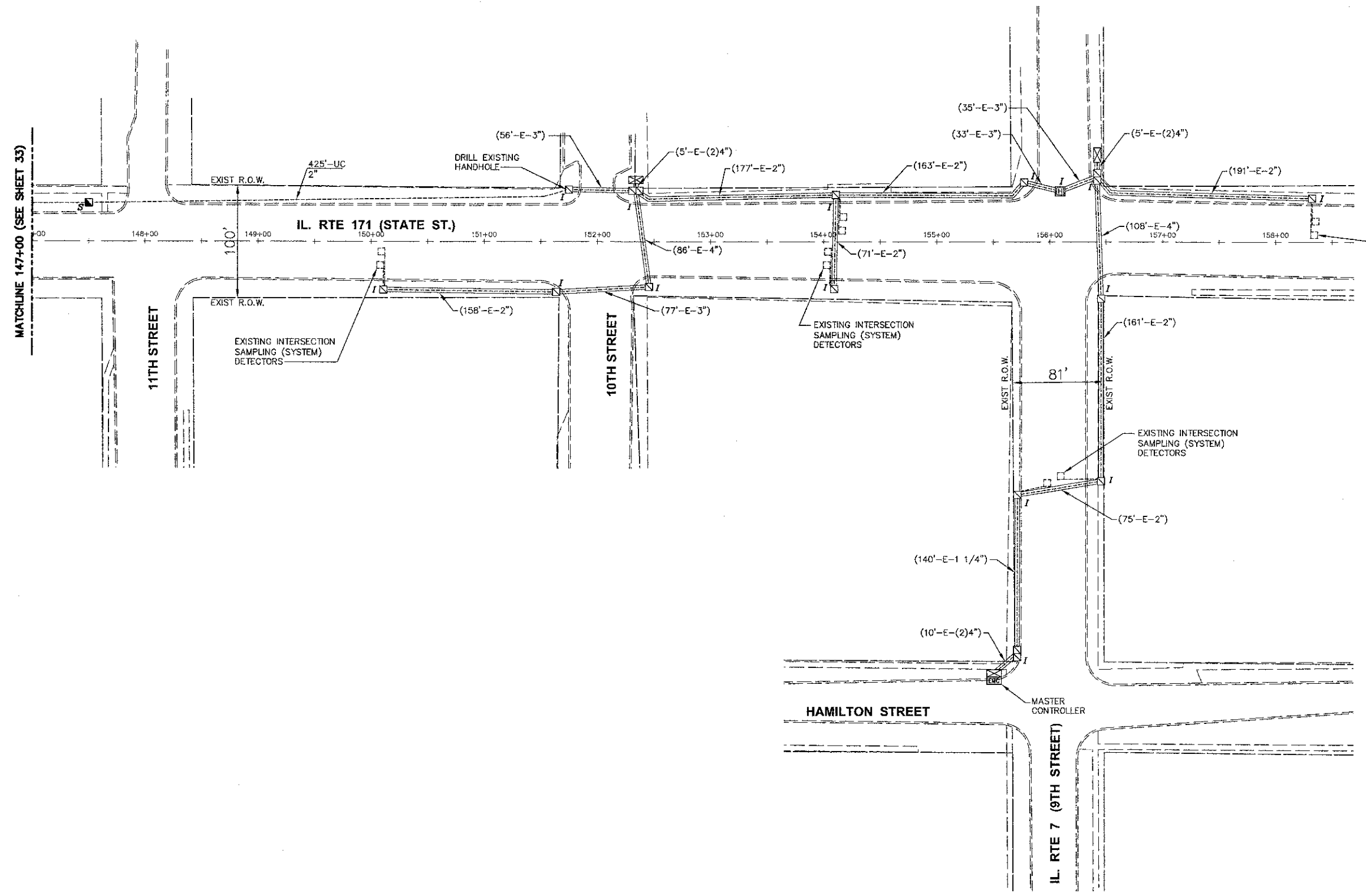
NOTE:
SINCE A GRADE CROSSING EXISTS WITHIN THE VICINITY OF THE TEMPORARY TRAFFIC CONTROL ZONE, LANE RESTRICTIONS, FLAGGING OR OTHER OPERATIONS SHALL NOT BE PERFORMED IN A MANNER THAT WILL CAUSE HIGHWAY VEHICLES TO STOP ON THE RAILROAD TRACKS. IF THE QUEUING OF VEHICLES ACROSS THE TRACKS CANNOT BE AVOIDED, A FLAGGER OR UNIFORMED LAW ENFORCEMENT OFFICER SHALL BE PROVIDED AT THE GRADE CROSSING TO MINIMIZE THE POSSIBILITY OF HIGHWAY VEHICLES STOPPING ON THE TRACKS, EVEN THOUGH AUTOMATIC WARNING DEVICES ARE IN PLACE.

Plot: March 20, 2013 @ 2:38 PM By: Larry Nolan - Job: 33 Interconnect 22334
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Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60654 630.466.6700 / www.eeiweb.com	USER NAME = Larry Nolan PLOT SCALE = PLOT DATE = March 20, 2013	DESIGNED - JRL & SWM DRAWN - CLN CHECKED - JL DATE - 11/13/12	REVISED - JPS 02/08/13 REVISED - CLN 03/19/13 REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		INTERCONNECT PLAN IL RTE 171 AND 13TH STREET		F.A.P. RTE. 577 SECTION 10-00068-00-TL COUNTY WILL TOTAL SHEETS 65 SHEET NO. 33	CONTRACT NO. 63788 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)
	SCALE: 1"=50' SHEET NO. 5 OF 7 SHEETS STA. TO STA.								



NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



NOTE:
SINCE A GRADE CROSSING EXISTS WITHIN THE VICINITY OF THE TEMPORARY TRAFFIC CONTROL ZONE, LANE RESTRICTIONS, FLAGGING OR OTHER OPERATIONS SHALL NOT BE PERFORMED IN A MANNER THAT WILL CAUSE HIGHWAY VEHICLES TO STOP ON THE RAILROAD TRACKS. IF THE QUEUING OF VEHICLES ACROSS THE TRACKS CANNOT BE AVOIDED, A FLAGGER OR UNIFORMED LAW ENFORCEMENT OFFICER SHALL BE PROVIDED AT THE GRADE CROSSING TO MINIMIZE THE POSSIBILITY OF HIGHWAY VEHICLES STOPPING ON THE TRACKS, EVEN THOUGH AUTOMATIC WARNING DEVICES ARE IN PLACE.

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PLOT DATE = March 20, 2013

DESIGNED - JRL & SWM
DRAWN - CLN
CHECKED - JL
DATE - 11/13/12

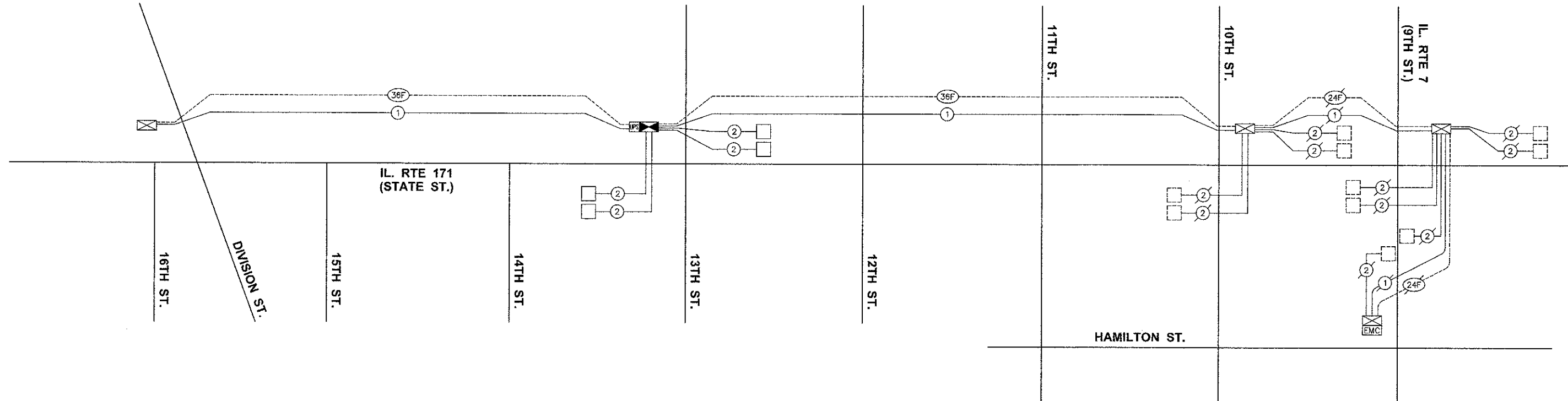
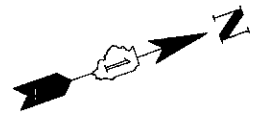
REVISED - JPS 02/08/13
REVISED - CLN 03/19/13
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INTERCONNECT PLAN
IL. RTE 171 AND 13TH STREET**

SCALE: 1"=50' SHEET NO. 6 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	34
			CONTRACT NO. 63788	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)				



NOTE:
 THE TRAFFIC SIGNAL CONTROL EQUIPMENT
 FOR THIS PROJECT SHALL BE "ECONOLITE"
 TO MATCH THE EXISTING ADJACENT SYSTEM.

INTERCONNECT SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM DESCRIPTION
1,890	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
2	EACH	HANDHOLE
2	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
1	EACH	TRANSCIVER - FIBER OPTIC
2,611	FOOT	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F
2,565	FOOT	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C
2	EACH	DRILL EXISTING HANDHOLE
2	EACH	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2

Plot Date: March 20, 2013 @ 7:36 AM By: Larry Nolan - Tab: 05 Inter Schematic 22x34

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PLOT SCALE =	DRAWN - CLN	REVISED - CLN 03/19/13
PLOT DATE = March 20, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

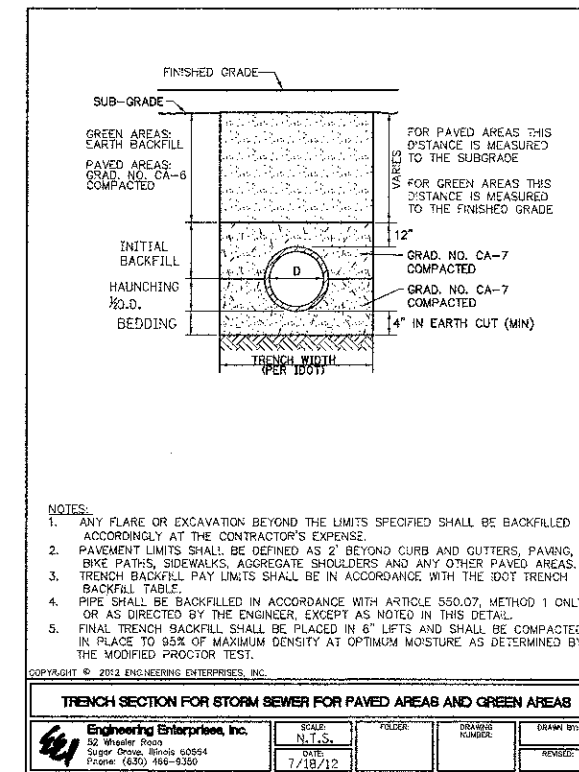
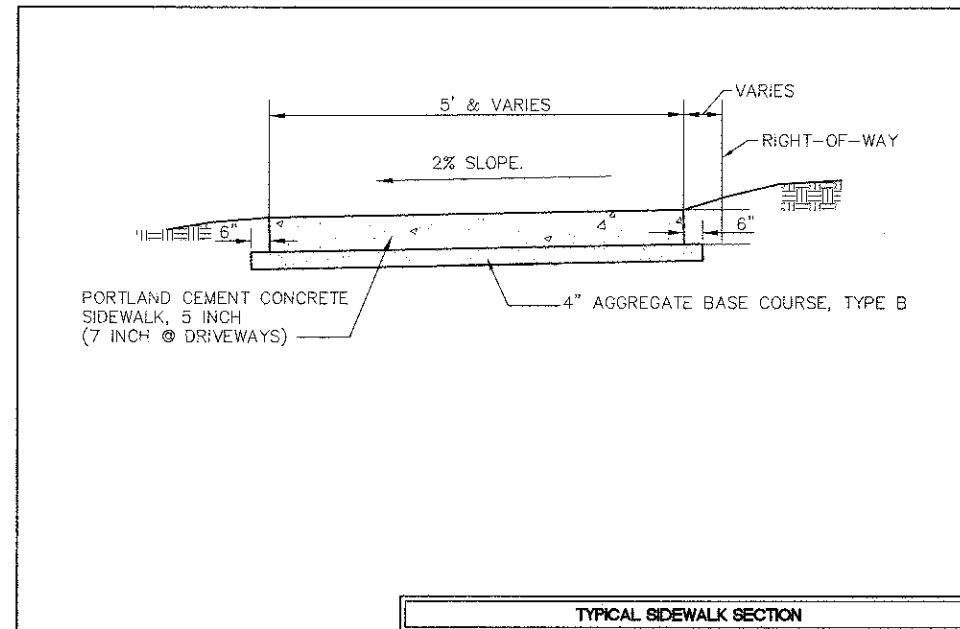
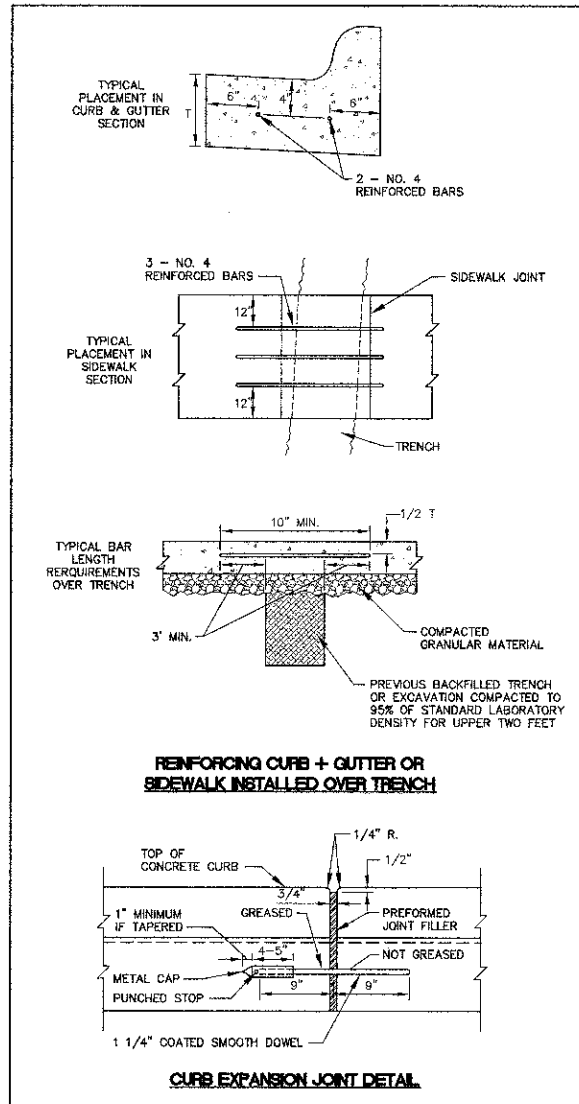
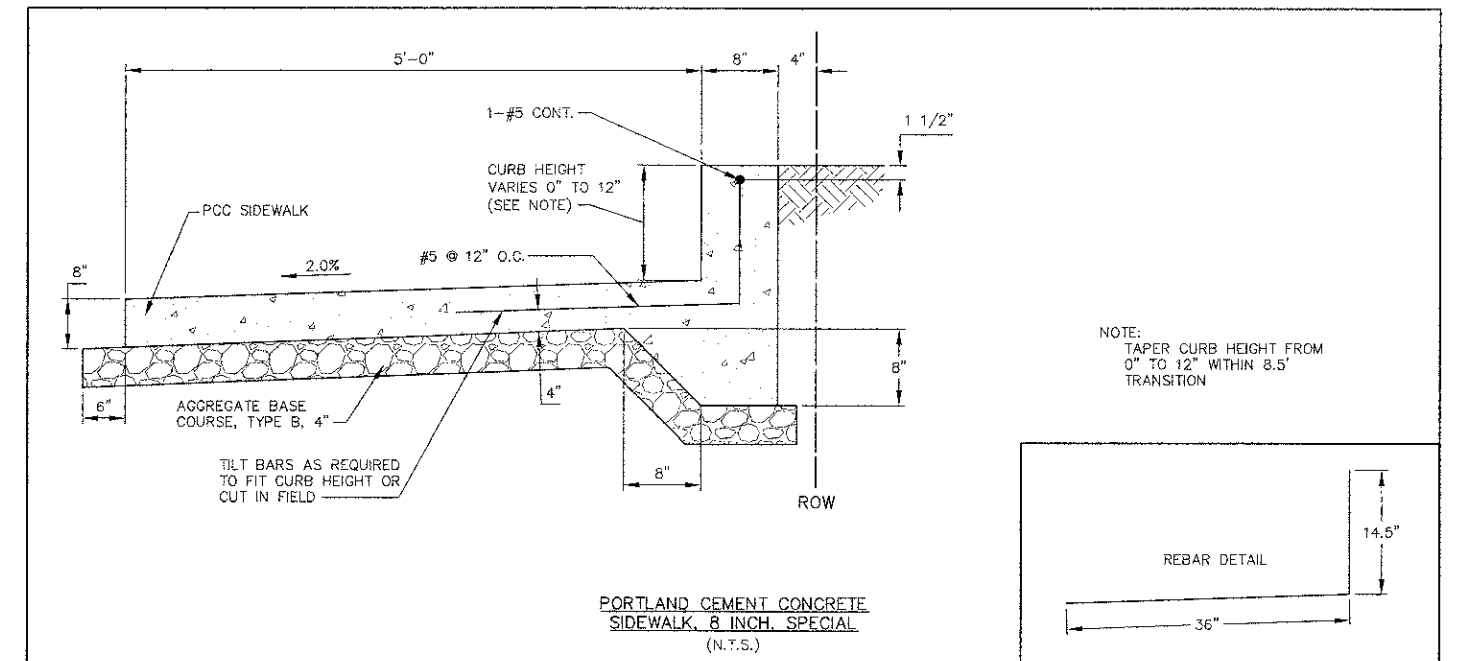
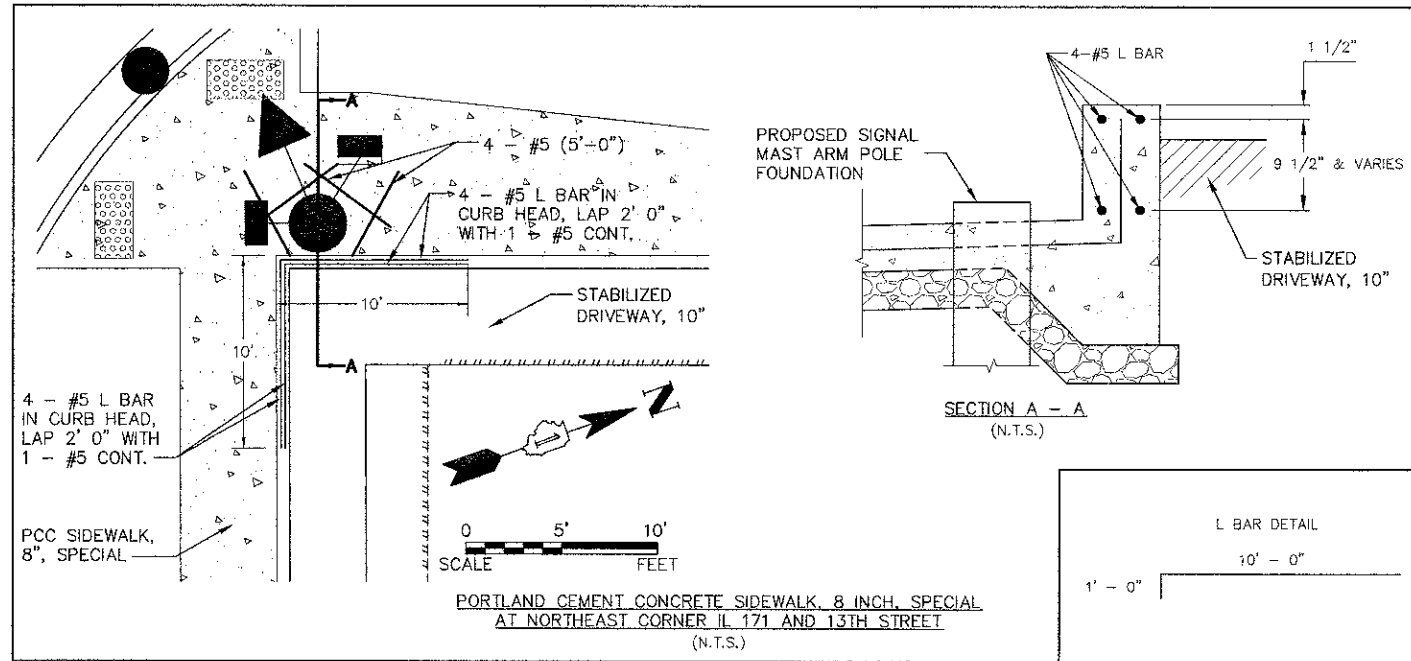
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INTERCONNECT SCHEMATIC AND INTERCONNECT QUANTITIES
IL. RTE 171 AND 13TH STREET

SCALE: N.T.S. SHEET NO. 7 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	35
CONTRACT NO. 63788				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90031619				

Path: H:\S05\PROJ\100002\DWG\DWG_FINAL_ENG\100002-INTERCONNECT



Plotted: February 7, 2013 @ 2:04 PM By: Larry Nolan - Tab: 36 Special Details 22x34

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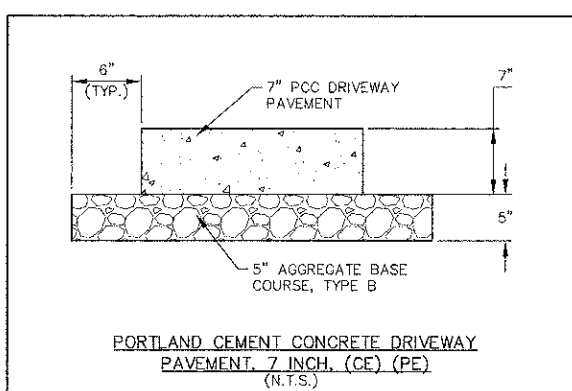
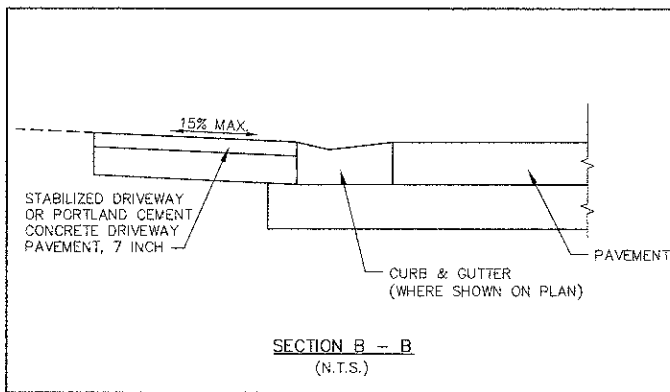
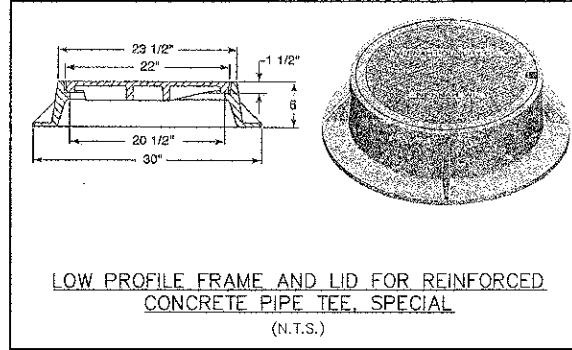
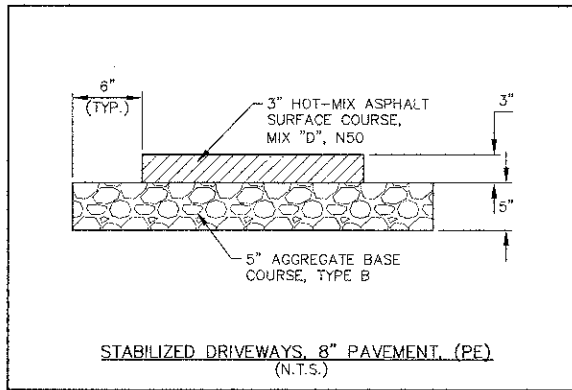
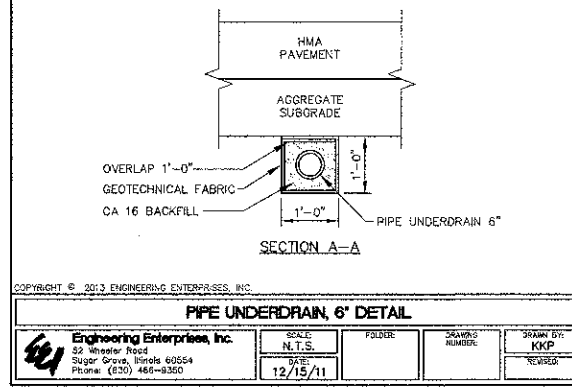
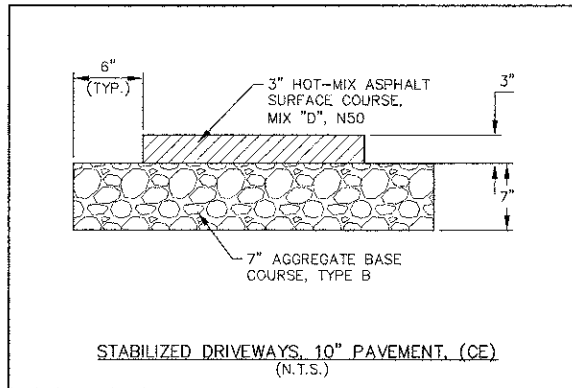
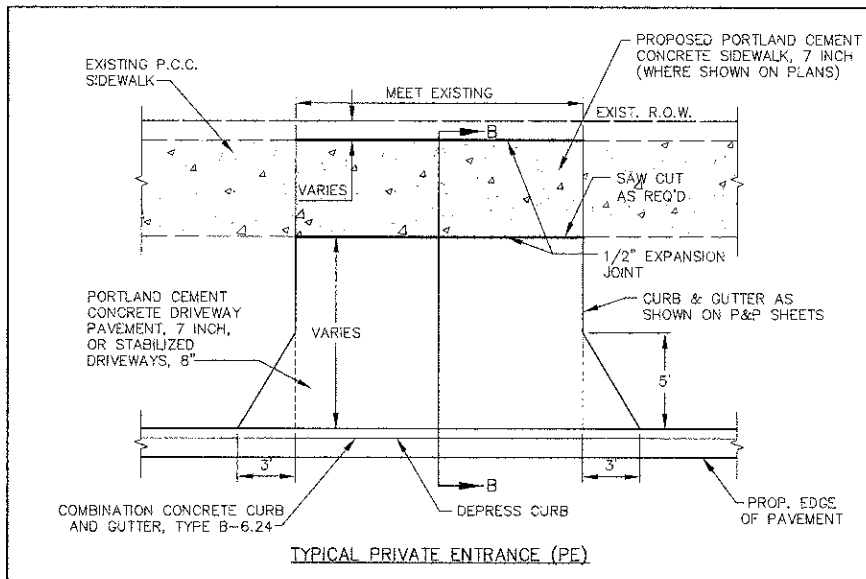
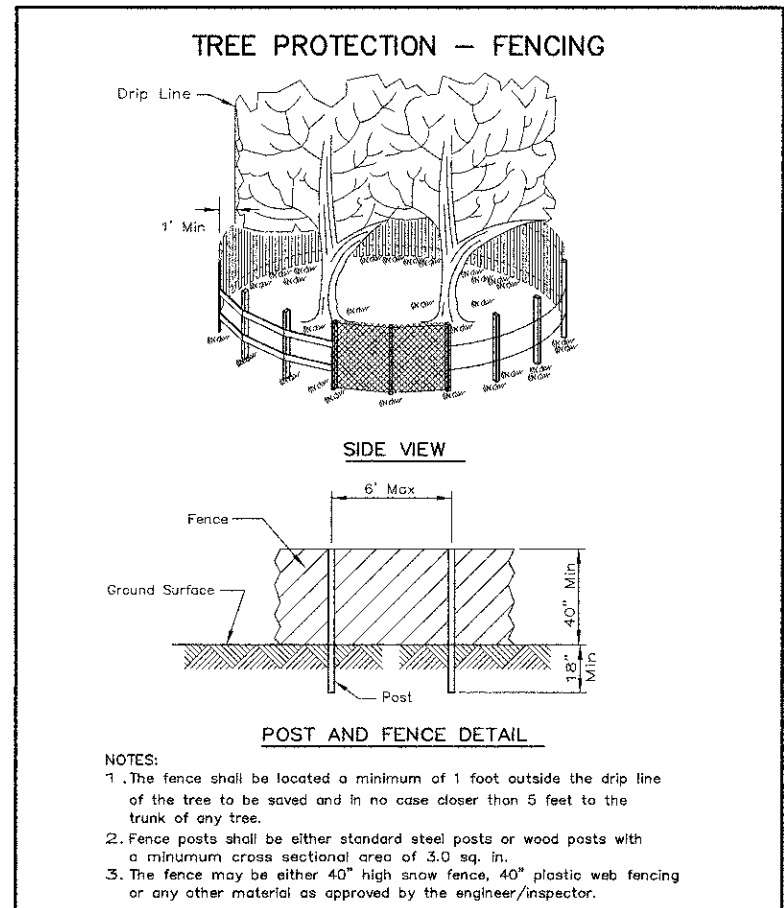
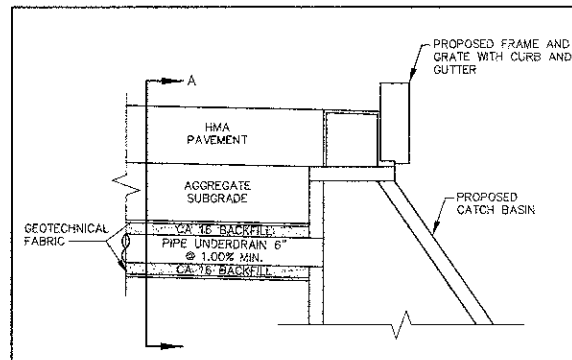
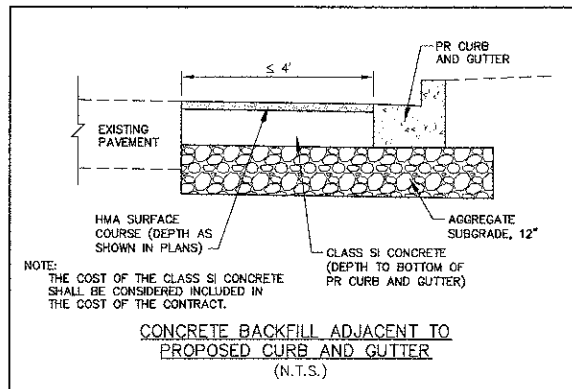
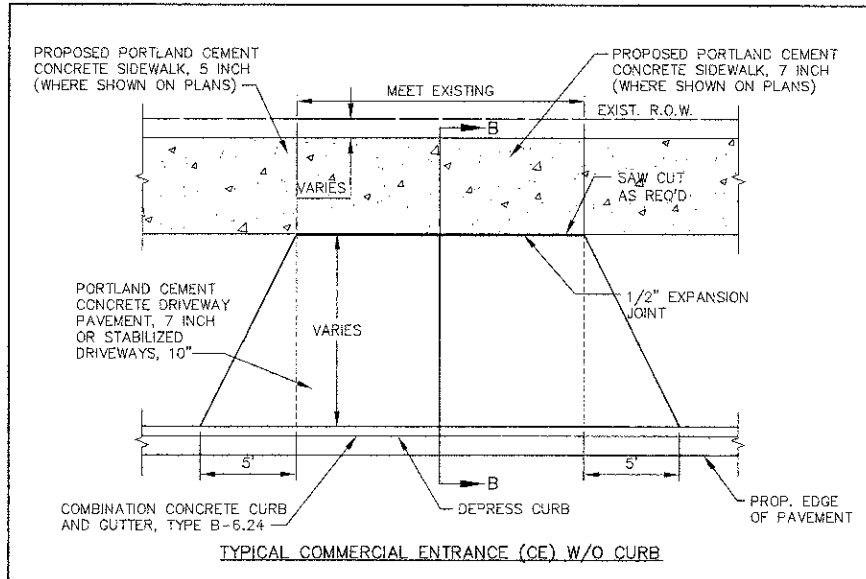
Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eeiweb.com	USER NAME = Larry Nolan	DESIGNED - JRL & SWM	REVISED - JPS 02/08/13
	PLOT SCALE =	DRAWN - CLN	REVISED -
	PLOT DATE = February 7, 2013	CHECKED - JL	REVISED -
		DATE - 11/13/12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SPECIAL DETAILS	
SCALE: N.T.S.	SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 36
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)			CONTRACT NO. 63788	

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Plates: February 7, 2013 @ 2:55 PM By: Larry Nolan - Tab: 37 Special Details 22x34
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PLOT SCALE =	DRAWN - CLN	REVISED -
PLOT DATE = February 7, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

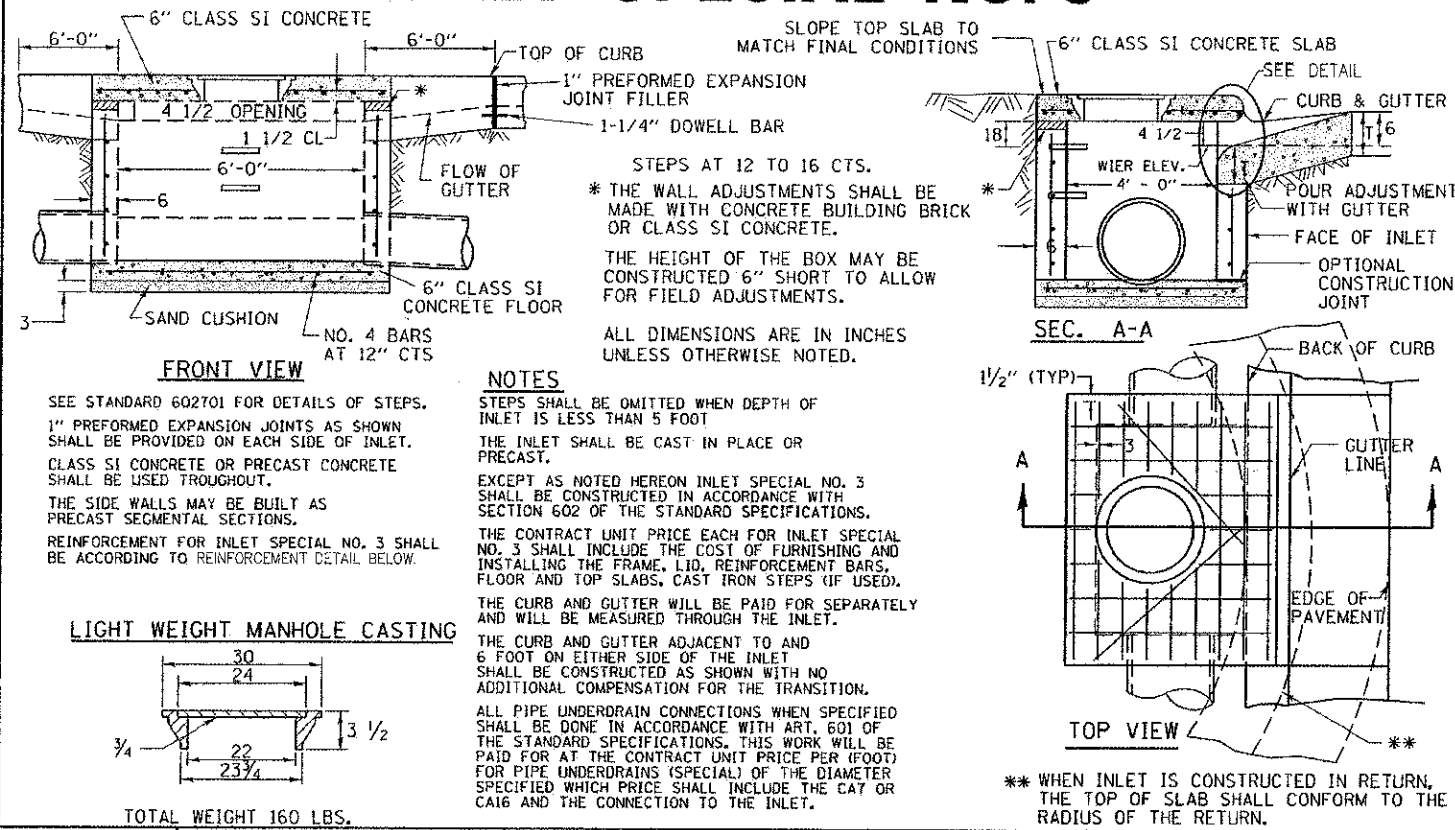
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SPECIAL DETAILS			
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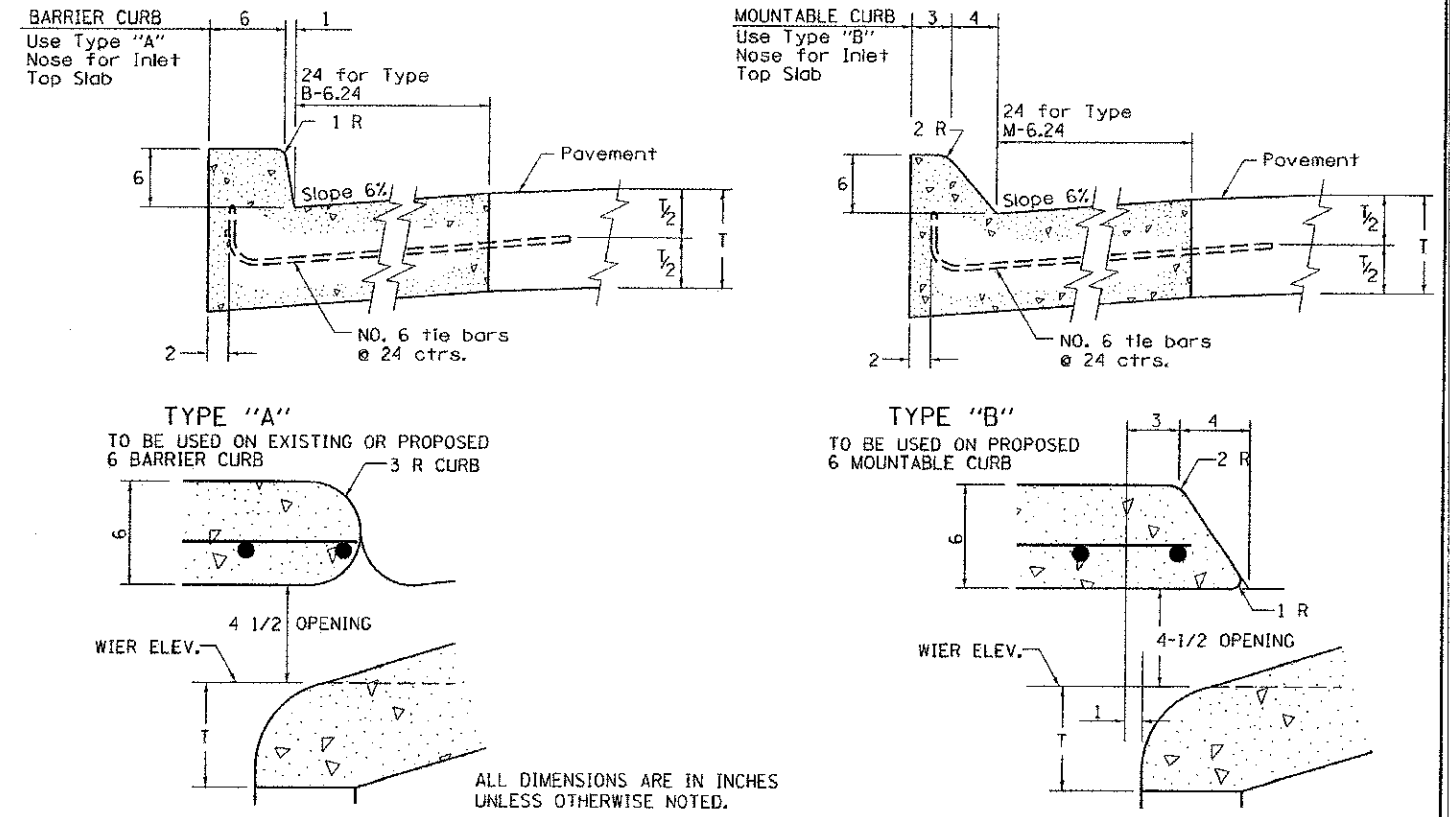
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577	10-00068-00-TL	WILL	65	37
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63788	
FED. AID PROJECT M-9003(619)				

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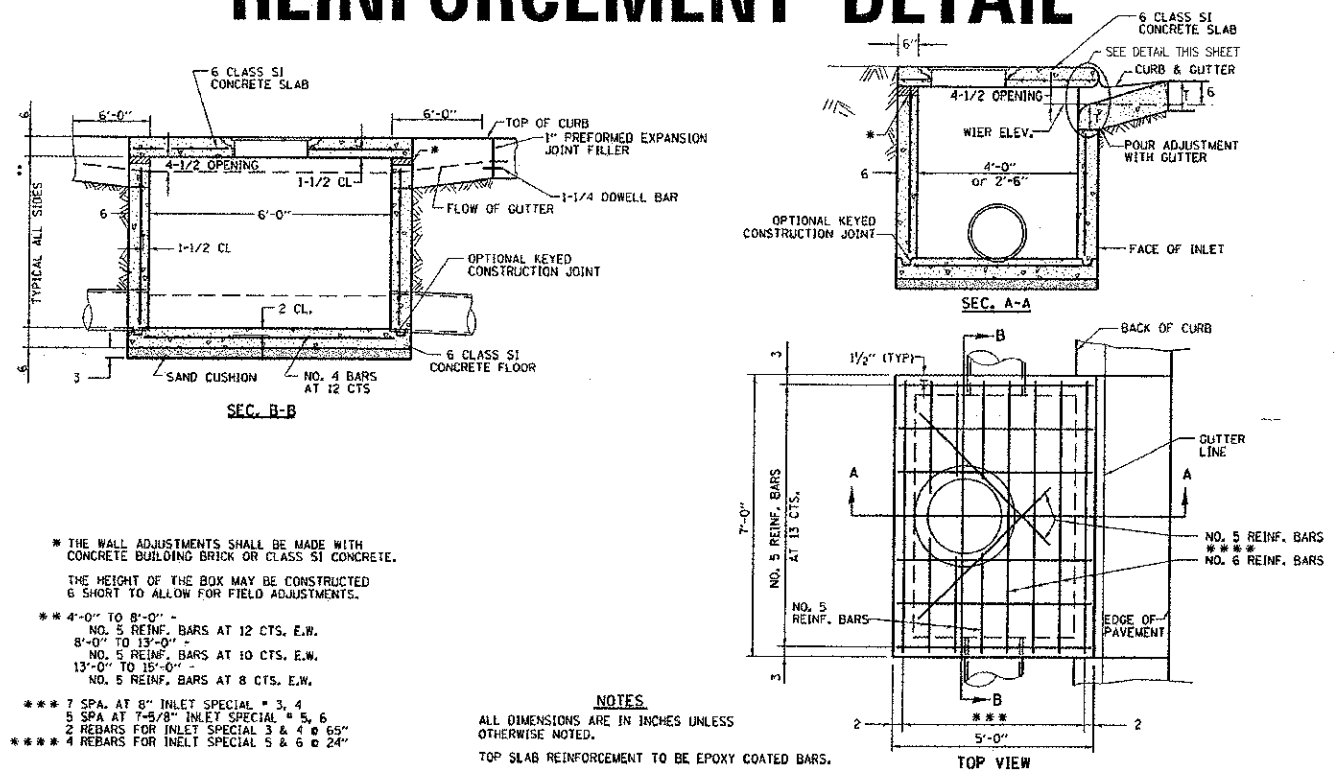
INLET SPECIAL NO. 3



NOSE TYPE FOR INLET TOP SLAB



INLET SPECIAL NO. 3, 4, 5, 6 REINFORCEMENT DETAIL



STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SPECIAL DETAILS

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 DATE - 11/13/12

REVISOR - JPS 02/08/13
 REVISOR -
 REVISOR -
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SCALE: N.T.S. SHEET NO. 3 OF 3 SHEETS STA. TO STA.

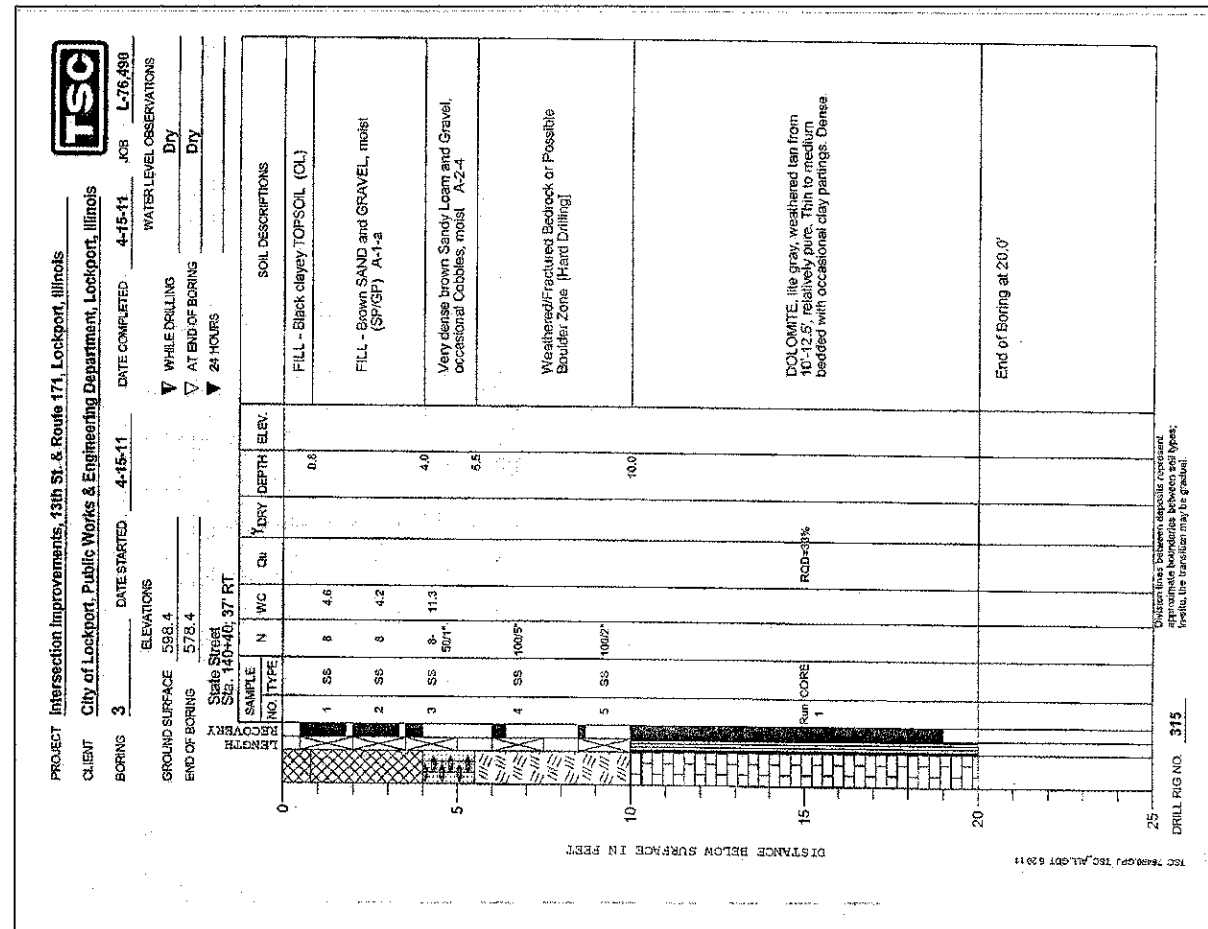
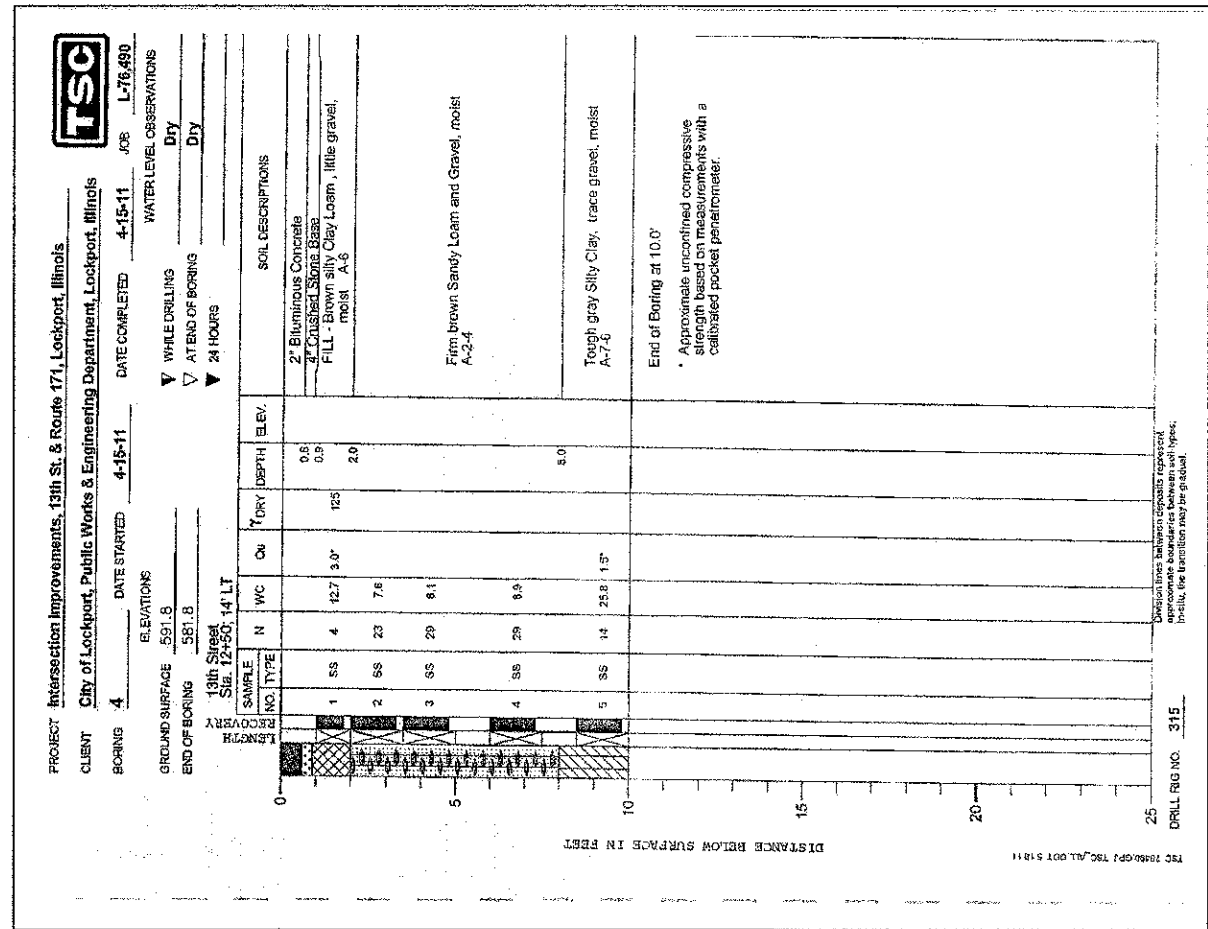
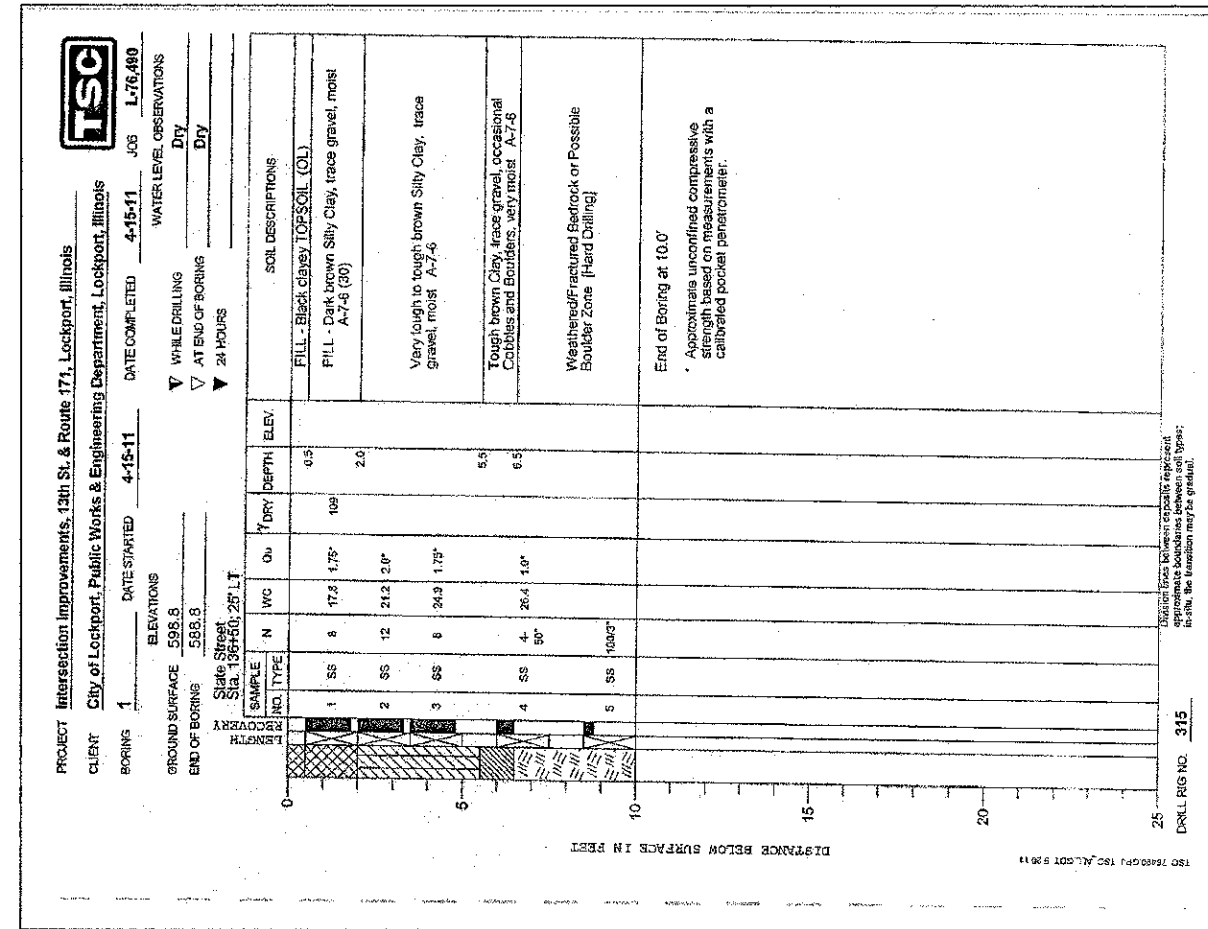
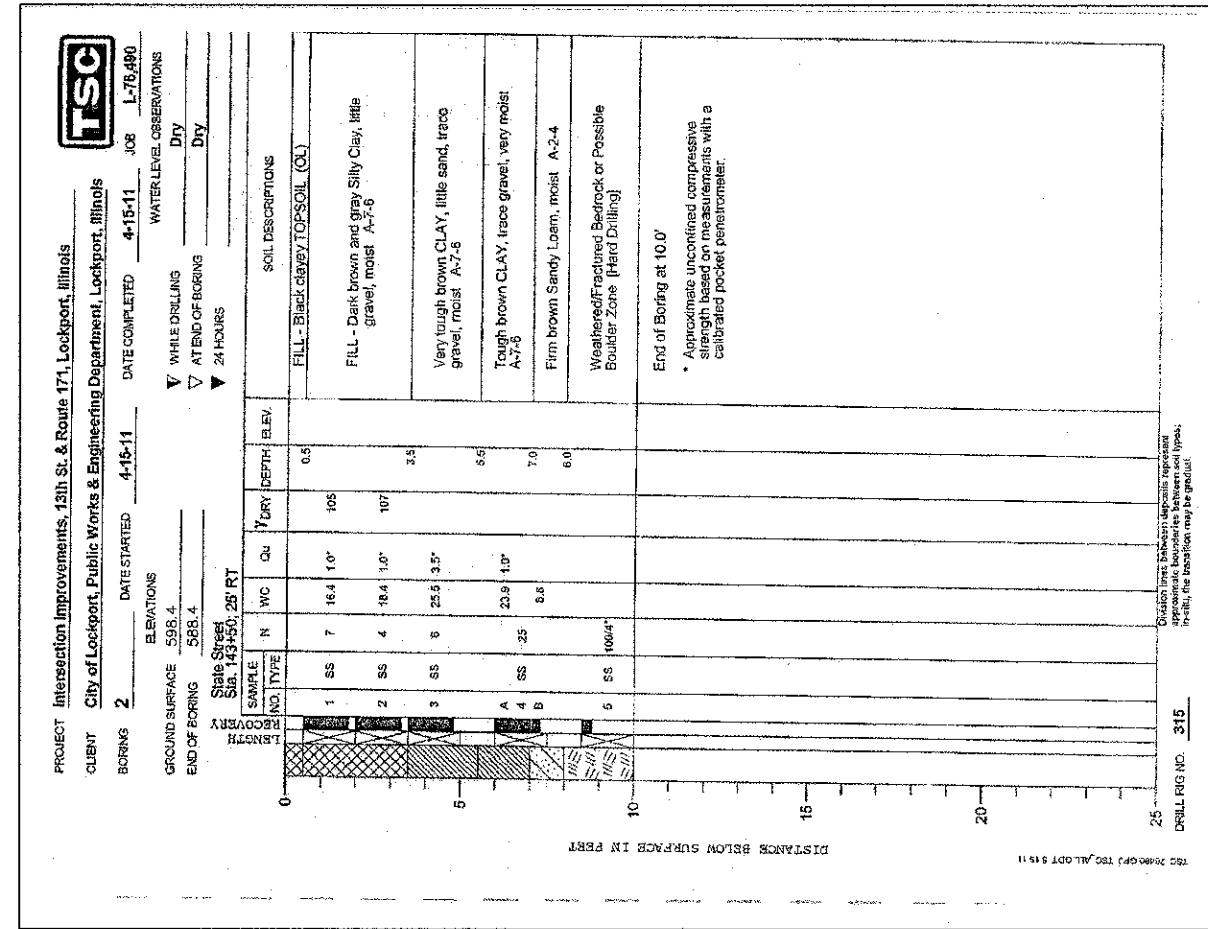
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	38
				CONTRACT NO. 63788
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)				

Plotted: February 7, 2013 @ 2:55 PM By: Larry Nolan - Tab: 38 Special Details 22x34

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Plotted: February 7, 2013 @ 2:57 PM By: Larry Nolan - Tab: 39 Soil Boring Logs 22x34

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PLOT DATE = February 7, 2013	CHECKED - JL	REVISION -
	DATE - 11/13/12	REVISION -

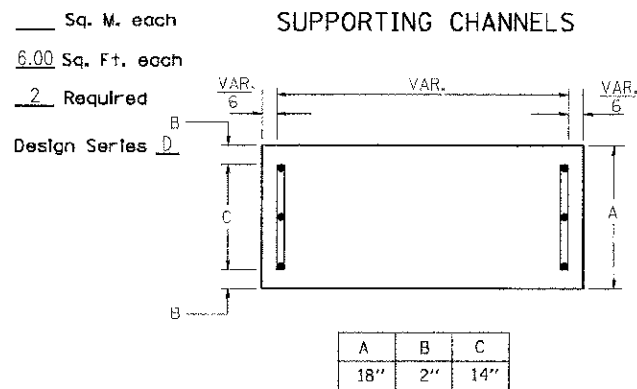
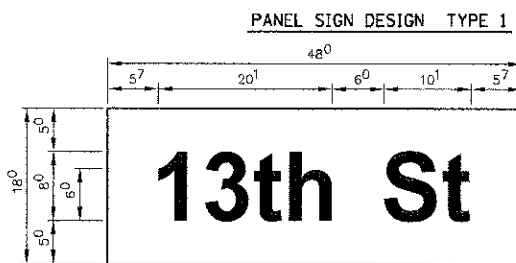
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS

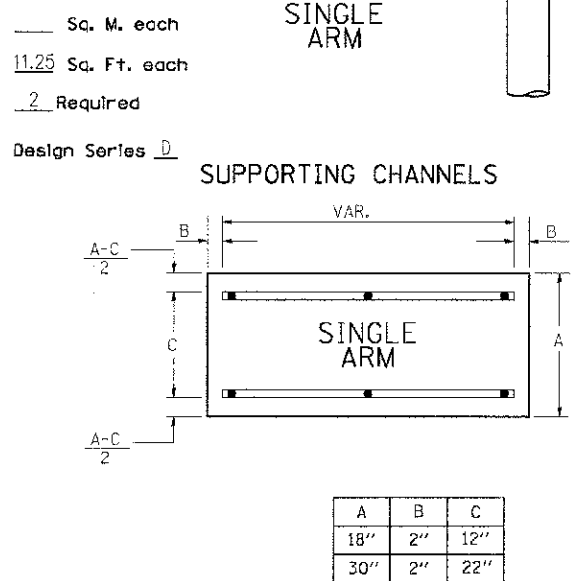
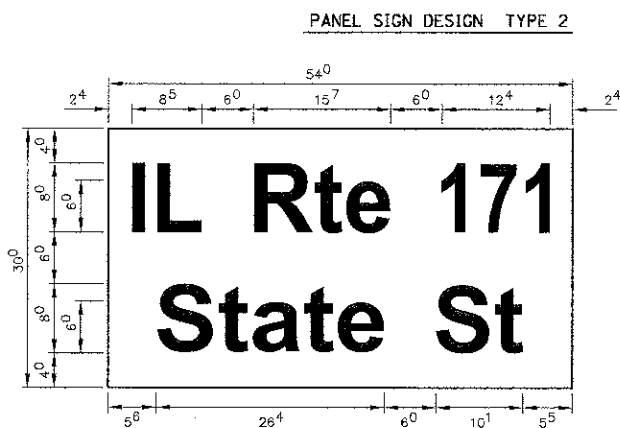
SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 39
FED. ROAD DIST. NO. 1 ILLINOIS		CONTRACT NO. 63788		
FED. AID PROJECT M-90331619				

Path: X:\SOS\PROJECTS\63788\DWG\DWG-FINAL-ENG\1702-CVR



NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS



GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 8'-0".
- ALL BORDERS SHALL BE 3/8" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

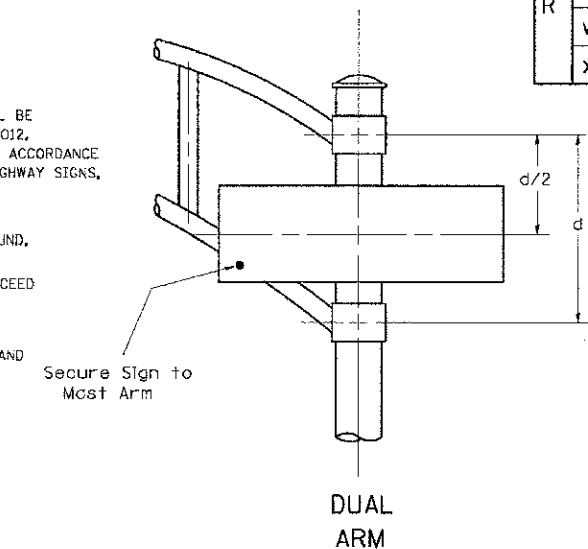
* J.O. HERBERT CO.
MIDLOTHIAN, VA.

* WESTERN REMAC INC.
WOODRIDGE, IL.

PARTS LISTING:

- SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
- SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
- SELF TAPPING WITH NEOPRENE WASHER
- BRACKETS PART #HPN034 (UNIVERSAL)
- CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.



SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.

Upper Case To Lower Case
Spacing Chart 8-6 Inch Series "C & D"

SERIES	SECOND LETTER															
	acde		bhikl		f w		j		s t		v y		x		z	
	g	o	q	m	n	p	r	u								
A W X	12	14	14	15	12	14	06	10	11	14	06	10	11	12	12	14
B	14	15	20	21	14	15	11	12	14	15	12	14	12	14	16	17
C E G	14	15	20	21	12	14	06	10	12	14	12	14	14	15	14	15
D O Q R	14	15	20	21	14	15	06	10	12	14	12	14	14	15	14	15
F	05	06	14	15	06	10	05	06	06	10	06	10	06	10	11	12
H I M N	20	21	22	24	20	21	14	15	16	17	16	17	20	21	20	21
J U	20	21	20	21	16	17	14	15	16	17	16	17	16	17	20	21
K L	11	12	16	17	11	12	05	06	11	12	11	12	11	12	12	14
P	12	14	14	15	12	14	05	06	11	12	11	12	12	14	12	14
S	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
T	11	12	16	17	06	10	06	10	11	12	11	12	11	12	12	14
V	06	10	14	15	11	12	06	10	12	14	12	14	12	14	12	14
Y	05	06	14	15	06	10	05	06	05	07	05	06	06	10	11	12
Z	16	17	22	24	16	17	12	14	16	17	16	17	16	17	20	21

Lower Case To Lower Case
Spacing Chart 6 Inch Series "C & D"

SERIES	SECOND LETTER															
	acde		bhikl		f w		j		s t		v y		x		z	
	g	o	q	m	n	p	r	u								
ad h g l j	16	17	22	24	16	17	12	14	14	15	14	15	16	17	16	17
l m n q u																
b f k o p s	12	14	16	17	11	12	05	06	11	12	11	12	12	14	12	14
c e	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
r	06	10	12	14	06	10	03	03	05	06	05	06	06	10	06	10
t z	12	14	16	17	12	14	06	10	11	12	11	12	12	14	12	14
v y	11	12	14	15	11	12	05	06	06	10	06	10	11	12	11	12
w	11	12	14	15	11	12	05	06	11	12	11	12	11	12	12	14
x	12	14	16	17	11	12	05	06	11	12	11	12	11	12	12	14

Number To Number
Spacing Chart 8 Inch Series "C & D"

SERIES	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
0 9	16	17	16	17	14	15	12	14	14	15	14	15	16	17	12	14	16	17	16	17
1	20	21	20	21	20	21	16	17	14	15	20	21	20	21	14	15	20	21	20	21
2 3 4	14	15	14	15	14	15	12	14	12	14	14	15	14	15	11	12	16	17	14	15
5	14	15	14	15	14	15	11	12	11	12	14	15	14	15	11	12	14	15	14	15
6	16	17	14	15	14	15	12	15	12	14	14	15	14	15	11	12	14	15	14	15
7	12	14	12	14	14	15	12	15	05	06	12	14	14	15	11	12	14	15	12	14
8	16	17	16	17	14	15	12	15	12	14	14	15	16	17	12	14	16	17	14	15

EXAMPLE, 2³ DENOTES 3" / 8

UPPER AND LOWER CASE
LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	3 ⁶	5 ⁰	5 ⁰	6 ⁵	a	3 ⁵	4 ²
B	3 ²	4 ⁰	4 ³	5 ³	b	3 ⁵	4 ²
C	3 ²	4 ⁰	4 ³	5 ³	c	3 ⁵	4 ¹
D	3 ²	4 ⁰	4 ³	5 ³	d	3 ⁵	4 ²
E	3 ⁰	3 ⁵	4 ⁰	4 ⁷	e	3 ⁵	4 ²
F	3 ⁰	3 ⁵	4 ⁰	4 ⁷	f	2 ³	2 ⁶
G	3 ²	4 ⁰	4 ³	5 ³	g	3 ⁵	4 ²
H	3 ²	4 ⁰	4 ³	5 ³	h	3 ⁵	4 ²
I	0 ⁷	0 ⁷	1 ¹	1 ²	i	1 ¹	1 ¹
J	3 ⁰	3 ⁶	4 ⁰	5 ⁰	j	2 ⁰	2 ²
K	3 ²	4 ¹	4 ³	5 ⁴	k	3 ⁵	4 ²
L	3 ⁰	3 ⁵	4 ⁰	4 ⁷	l	1 ¹	1 ¹
M	3 ⁷	4 ⁵	5 ¹	6 ¹	m	6 ⁰	7 ⁰
N	3 ²	4 ⁰	4 ³	5 ³	n	3 ⁵	4 ²
O	3 ⁴	4 ²	4 ⁵	5 ⁵	o	3 ⁶	4 ³
P	3 ²	4 ⁰	4 ³	5 ³	p	3 ⁵	4 ²
Q	3 ⁴	4 ²	4 ⁵	5 ⁵	q	3 ⁵	4 ²
R	3 ²	4 ⁰	4 ³	5 ³	r	2 ⁶	3 ²
S	3 ²	4 ⁰	4 ³	5 ³	s	3 ⁶	4 ²
T	3 ⁰	3 ⁵	4 ⁰	4 ⁷	t	2 ⁷	3 ²
U	3 ²	4 ⁰	4 ³	5 ³	u	3 ⁵	4 ²
V	3 ⁵	4 ⁴	4 ⁷	6 ⁰	v	4 ²	4 ⁷
W	4 ⁴	5 ²	6 ⁰	7 ⁰	w	5 ⁵	6 ⁴
X	3 ⁴	4 ⁰	4 ⁵	5 ³	x	4 ⁴	5 ¹
Y	3 ⁶	5 ⁰	5 ⁰	6 ⁶	y	4 ⁶	5 ³
Z	3 ²	4 ⁰	4 ³	5 ³	z	3 ⁶	4 ³

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	1 ²	1 ⁴	1 ⁵	2 ⁰
2	3 ²	4 ⁰	4 ³	5 ³
3	3 ²	4 ⁰	4 ³	5 ³
4	3 ⁵	4 ³	4 ⁷	5 ⁷
5	3 ²	4 ⁰	4 ³	5 ³
6	3 ²	4 ⁰	4 ³	5 ³
7	3 ²	4 ⁰	4 ³	5 ³
8	3 ²	4 ⁰	4 ³	5 ³
9	3 ²	4 ⁰	4 ³	5 ³
0	3 ⁴	4 ²	4 ⁵	5 ⁵

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Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60954 630.466.6700 / www.eelweb.com	USER NAME = bowenrd dgn PLOT SCALE = 49.9999" / IN. PLOT DATE = 11/4/2009	DESIGNED - DAG/BCK DRAWN - BCK CHECKED - DAG/DAD DATE - 03-15-09	REVISED - DAG 10/26/09 REVISED - REVISED - REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE MAST ARM MOUNTED STREET NAME SIGNS		F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 40
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10	TS-02		CONTRACT NO. 63788		
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. N/A	TO STA. N/A			

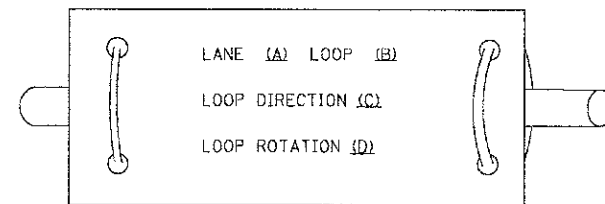
DISTRICT ONE MAST ARM MOUNTED STREET NAME SIGNS		F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 40
PROJECT NO. M-9003(619)		JOB NO. C-91-486-10		CONTRACT NO. 63788		
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. N/A	TO STA. N/A			

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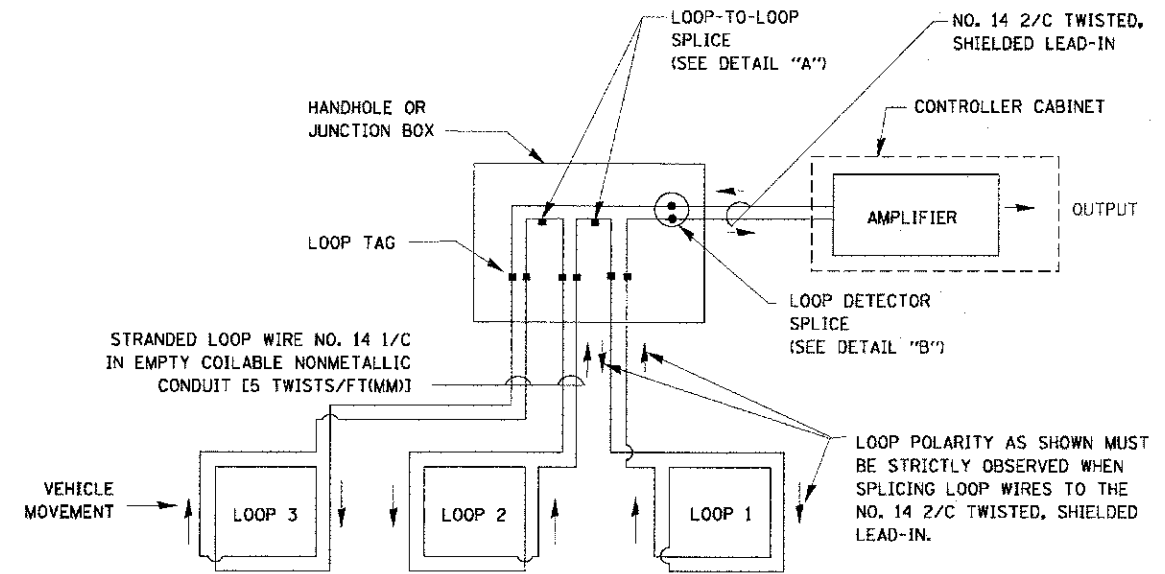
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

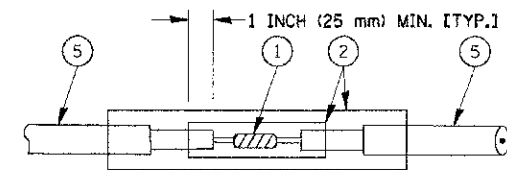


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

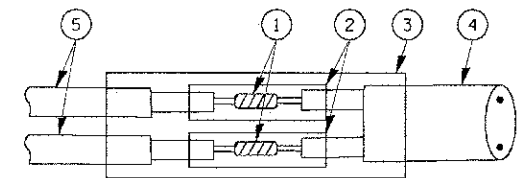


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

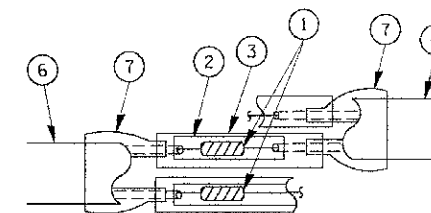


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

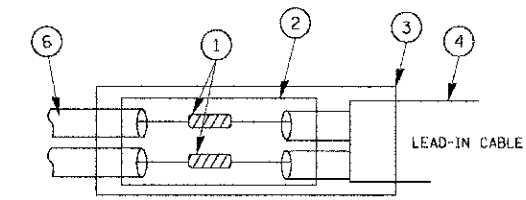


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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sign	DRAWN - BCK	REVISED -
PLOT SCALE = 50.0000' / IN.	CHECKED - DAD	REVISED -
PLOT DATE = 2/4/2009	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

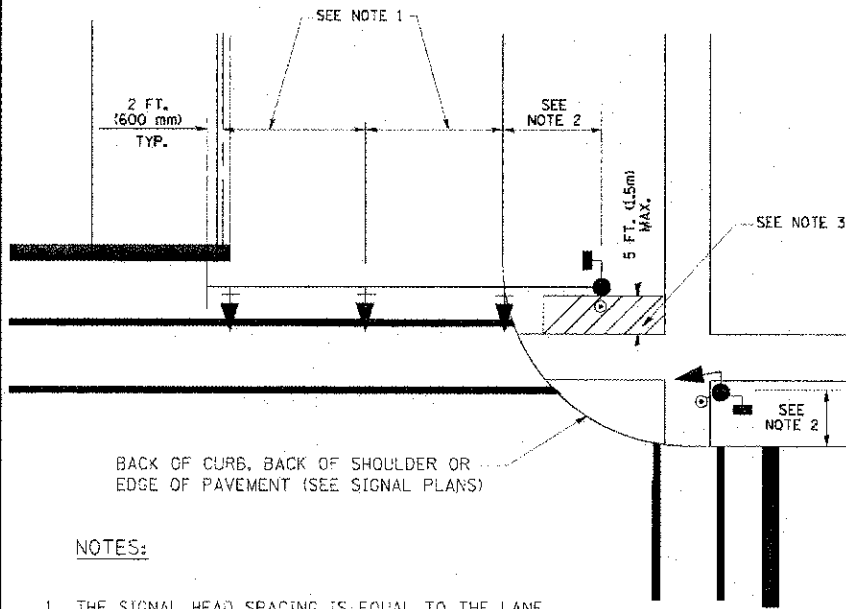
DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10
SCALE: NONE	SHEET NO. 1 OF 6 SHEETS
STA. N/A	TO STA. N/A

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 41
TS-05			CONTRACT NO. 63788	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.

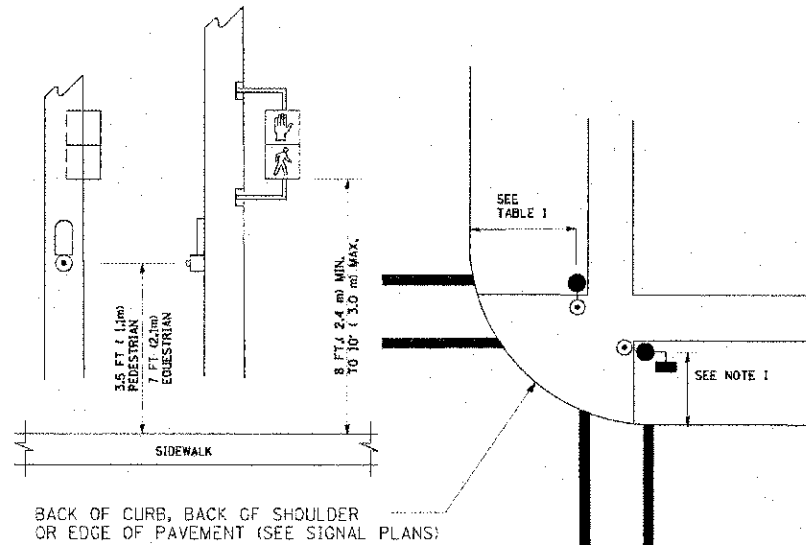


BACK OF CURB, BACK OF SHOULDER OR EDGE OF PAVEMENT (SEE SIGNAL PLANS)

NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST

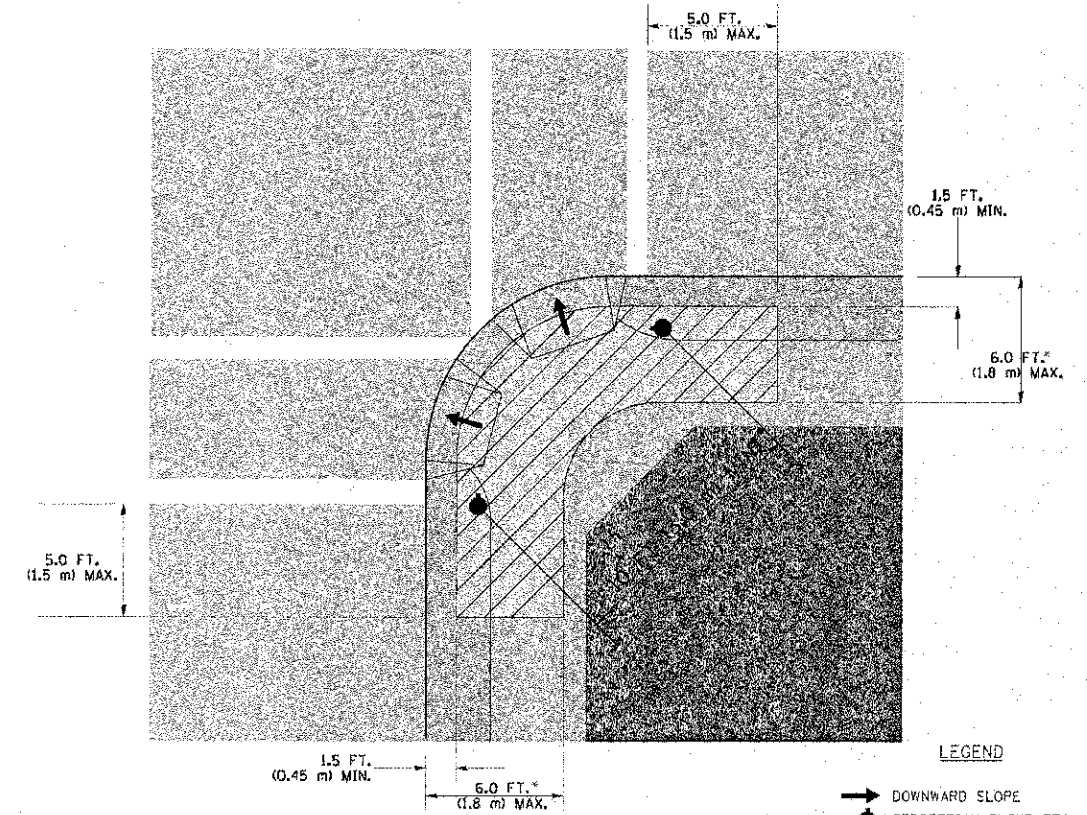


BACK OF CURB, BACK OF SHOULDER OR EDGE OF PAVEMENT (SEE SIGNAL PLANS)

NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPARATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 19 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD AFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

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CHECKED - DAD	REVISED -
DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

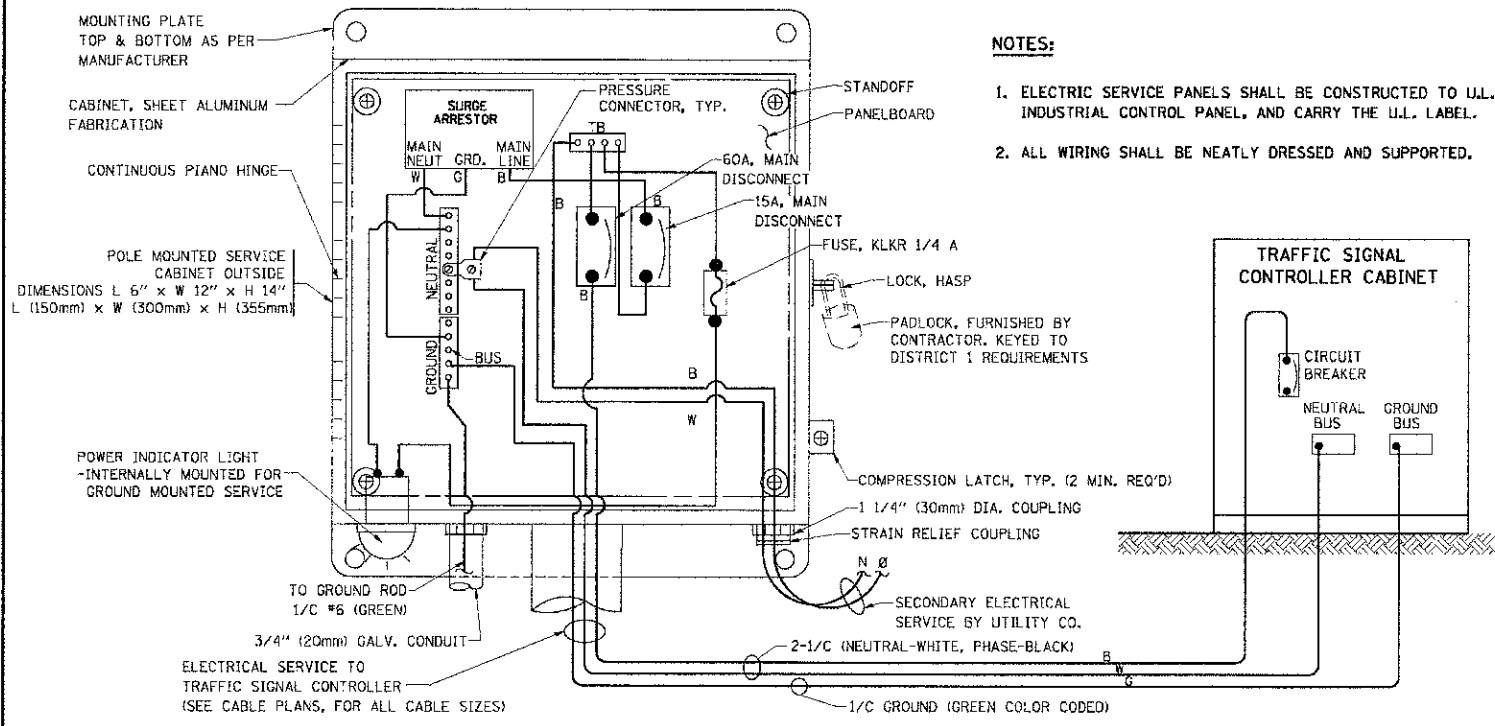
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

PROJECT NO. M-9003(619)	JOB NO. C-91-486-10
SHEET NO. 2 OF 6 SHEETS	STA. N/A TO STA. N/A

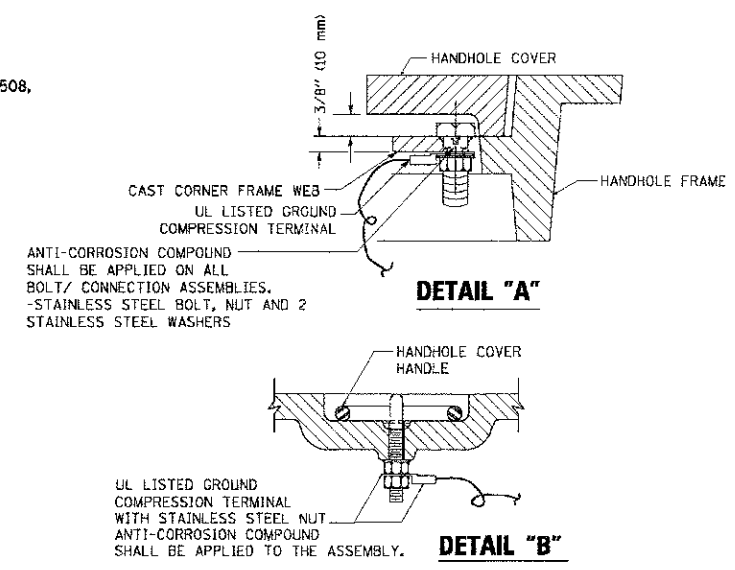
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TS-05		CONTRACT NO. 63788		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Plot Date: February 8, 2013 @ 9:45 AM By: Jim Schmitt Tab: 42 (TS-05B) 2/23/13

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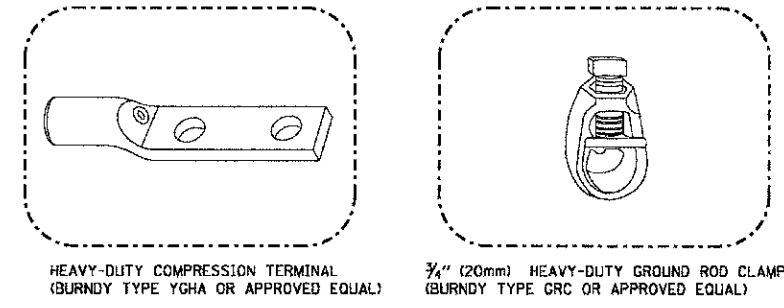


ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)

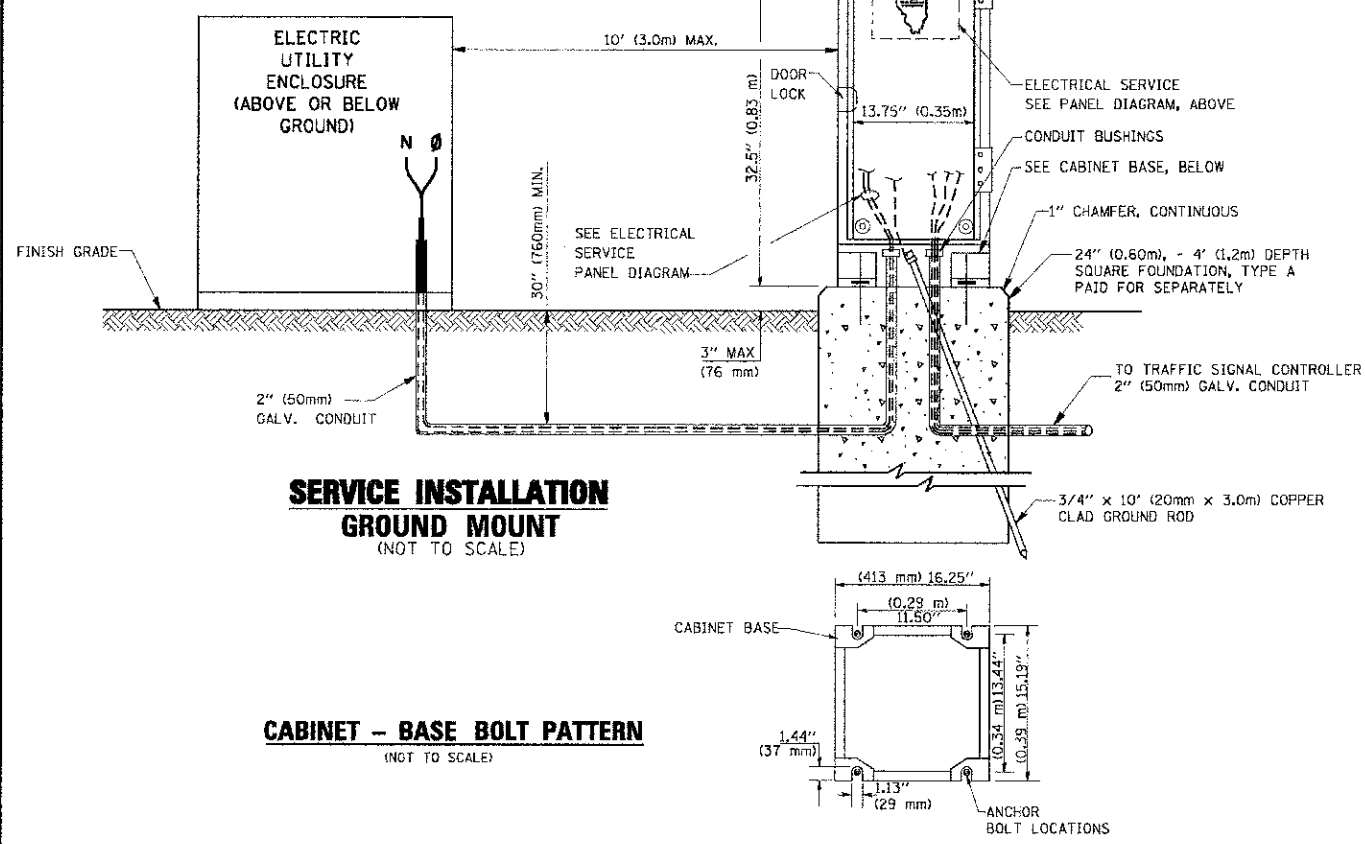
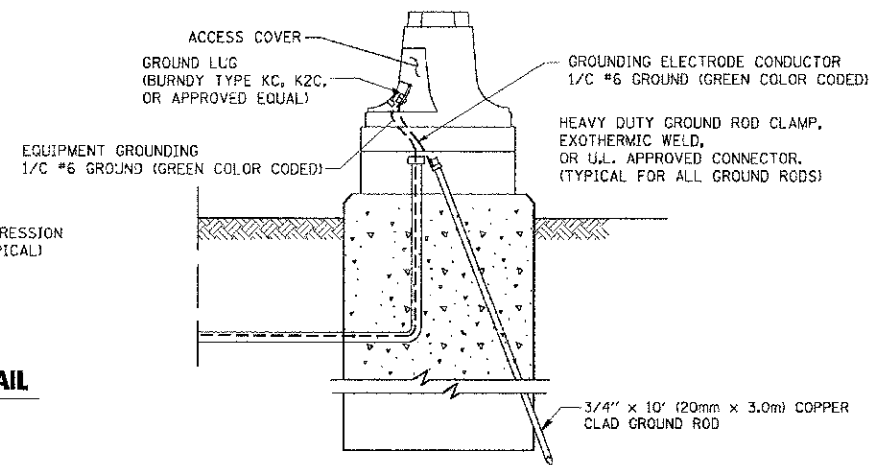
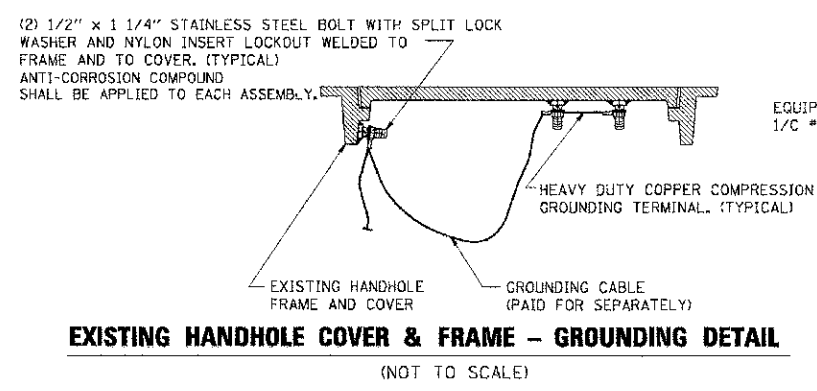
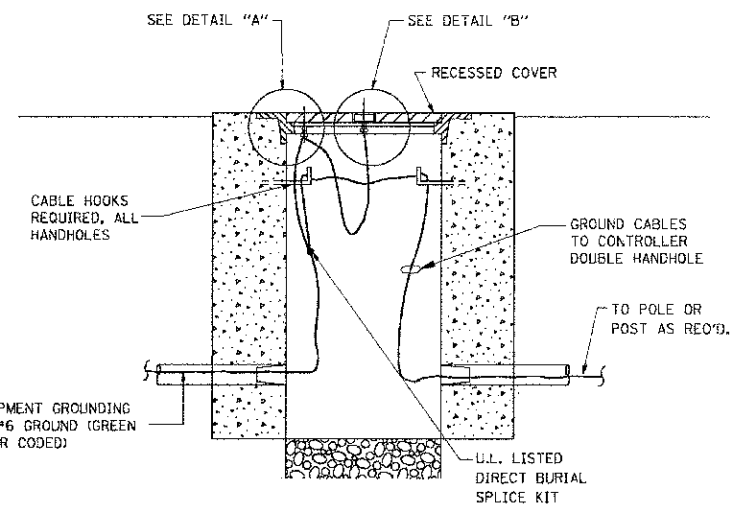


NOTES:
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
 - 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
 - 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
 - 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



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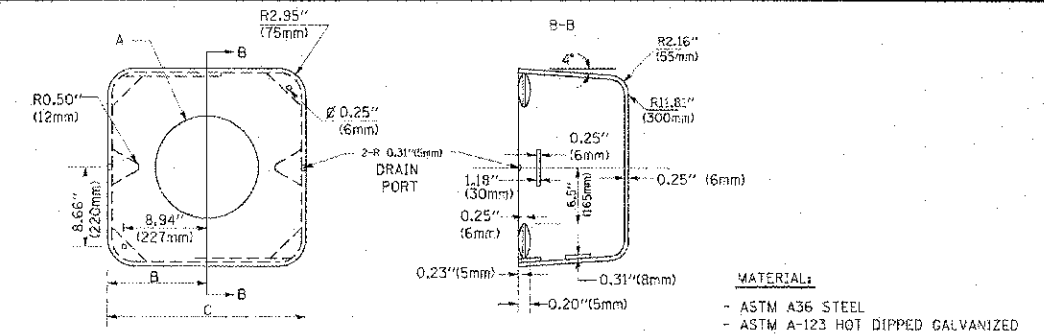
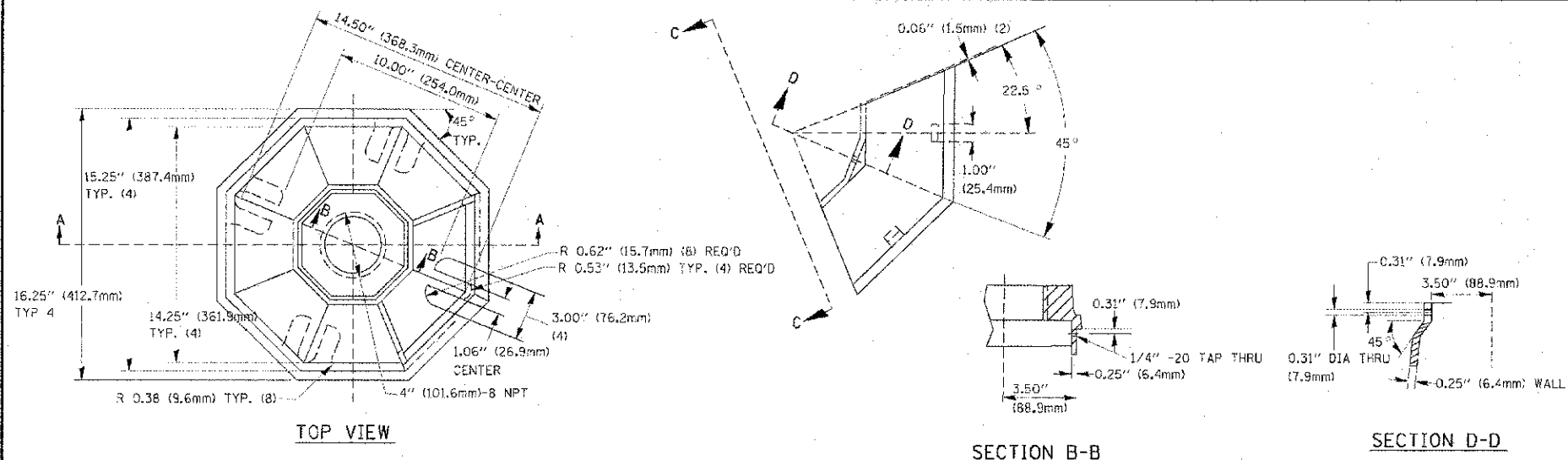
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DATE - 10-28-09	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STANDARD TRAFFIC SIGNAL DESIGN DETAILS		577	10-00068-00-TL	WILL	65	43
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10	TS-05		CONTRACT NO. 63788		
SCALE: NONE	SHEET NO. 3 OF 6 SHEETS	STA. N/A	TO STA. N/A	FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT		

Printed: February 8, 2013 @ 9:47 AM By: Jim Schmidt - Job: 43 (TS-05C) 22x34
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 10/28/09

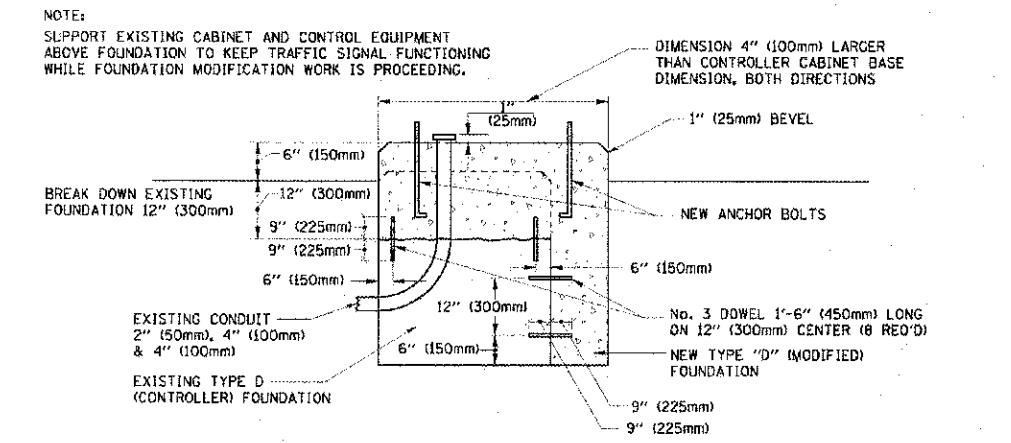
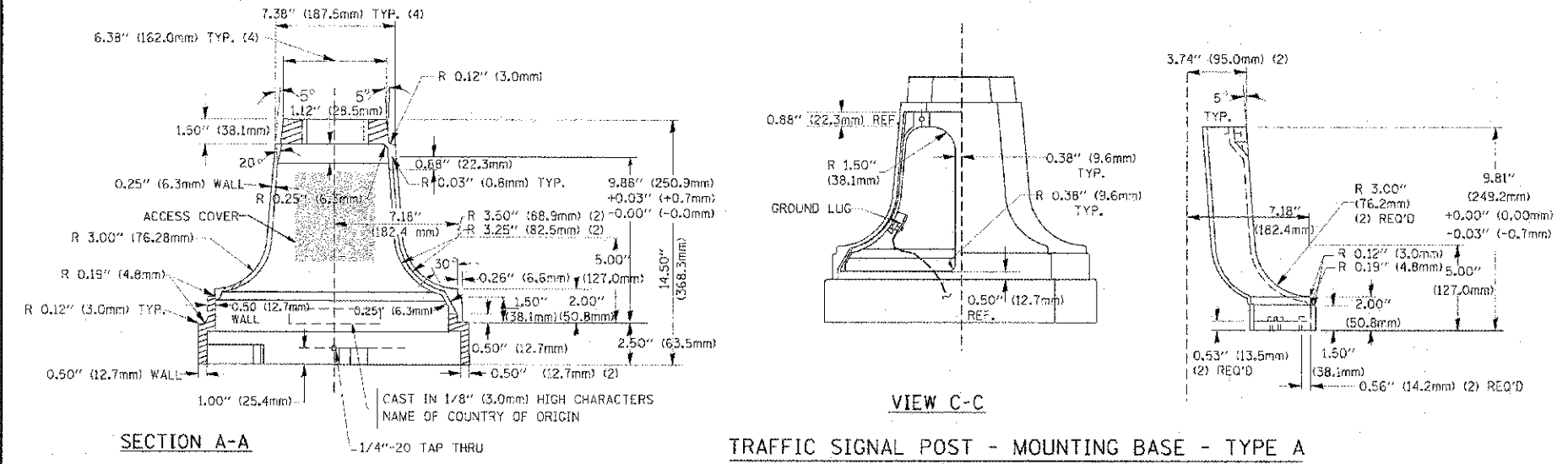
Path: C:\p0002\p0002.dwg FINAL ENG LAYOUT-DETAILS



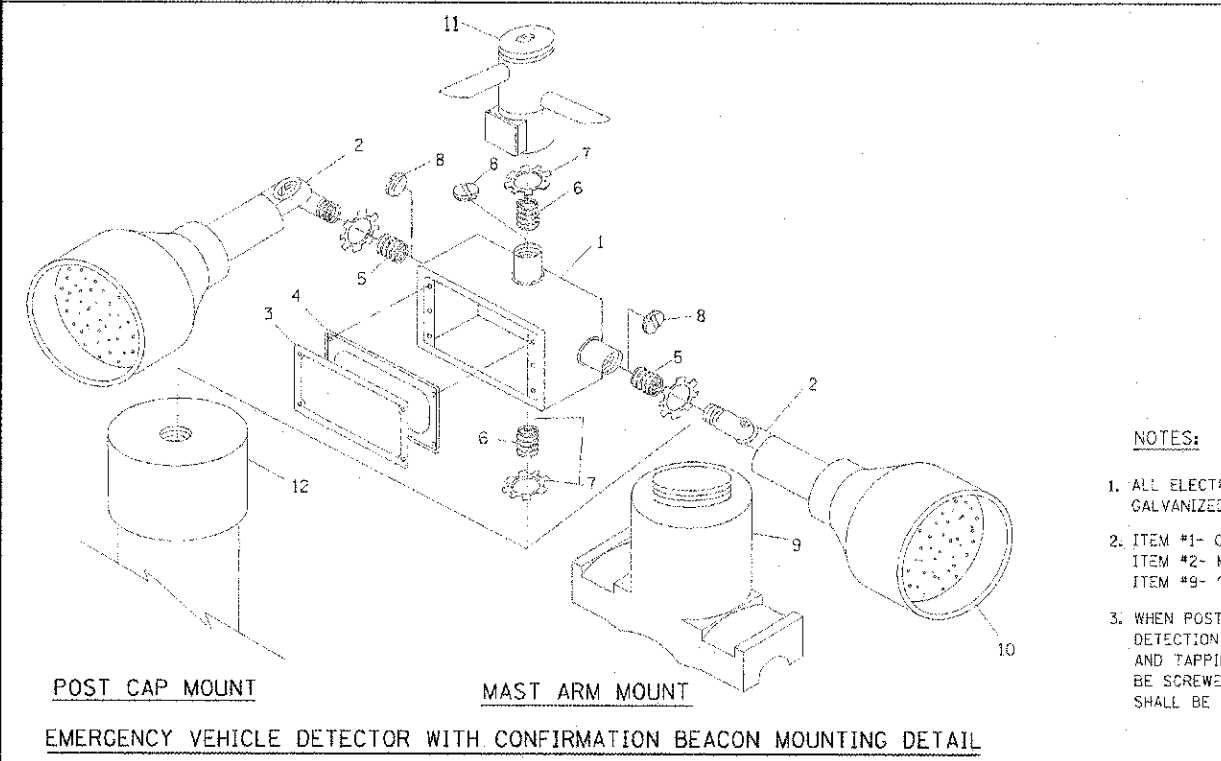
	A	B	C	HEIGHT	WEIGHT
	VARIES	9.5\" (241mm)	19\" (483mm)	7\" (178mm) - 12\" (300mm)	53 lbs (24kg)
	VARIES	10.75\" (273mm)	21.5\" (546mm)	7\" (178mm) - 12\" (300mm)	68 lbs (31 kg)
	VARIES	13.0\" (330mm)	26\" (660mm)	7\" (178mm) - 12\" (300mm)	81 lbs (37 kg)
	VARIES	16.5\" (470mm)	37\" (940mm)	7\" (178mm) - 12\" (300mm)	126 lbs (57 kg)

NOTES:

1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



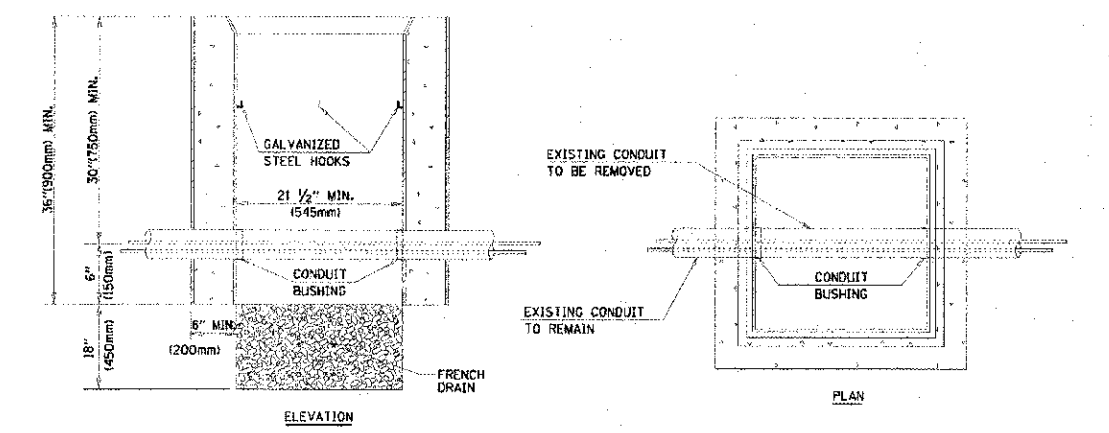
MODIFY EXISTING TYPE "D" FOUNDATION



ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\" (19 mm) CLOSE NIPPLE
7	3/4\" (19 mm) LOCKNUT
8	3/4\" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP (18\" (4.6 m) POST MIN.)

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD B14001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

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DESIGNED - DAD	REVISED -
DRAWN - BCK	REVISED -
CHECKED - DAG	REVISED -
DATE - 10-26-09	REVISED -

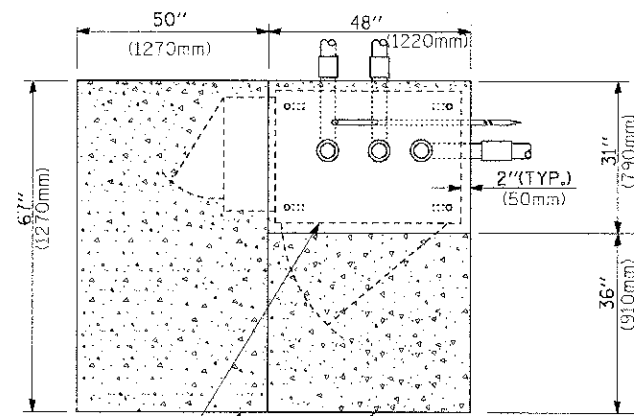
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10
SHEET NO. 4 OF 6 SHEETS	STA. N/A TO STA. N/A

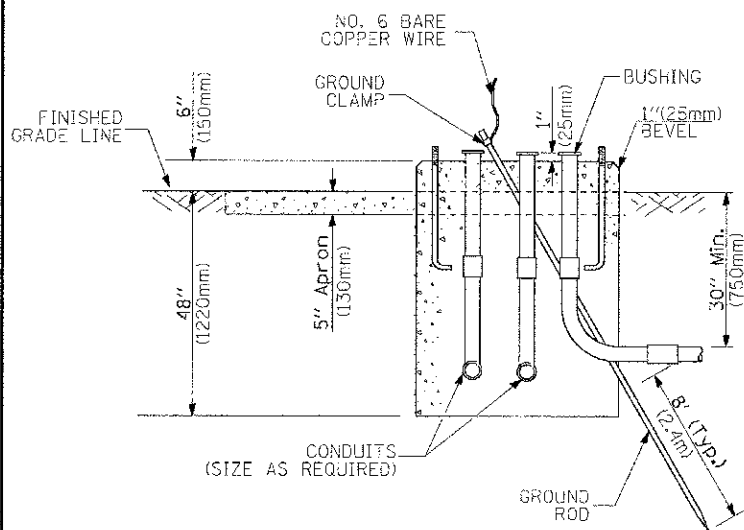
F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	44
TS-05		CONTRACT NO. 63788		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

Printed: February 8, 2013 @ 9:43 AM By: Jim Schmidt - Tab: 44 (TS-05) 22x34
 PLOT DATE: 12/4/2009

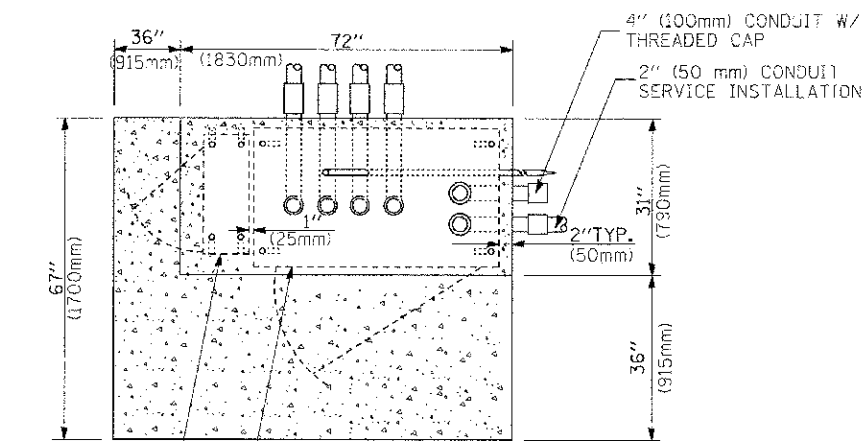
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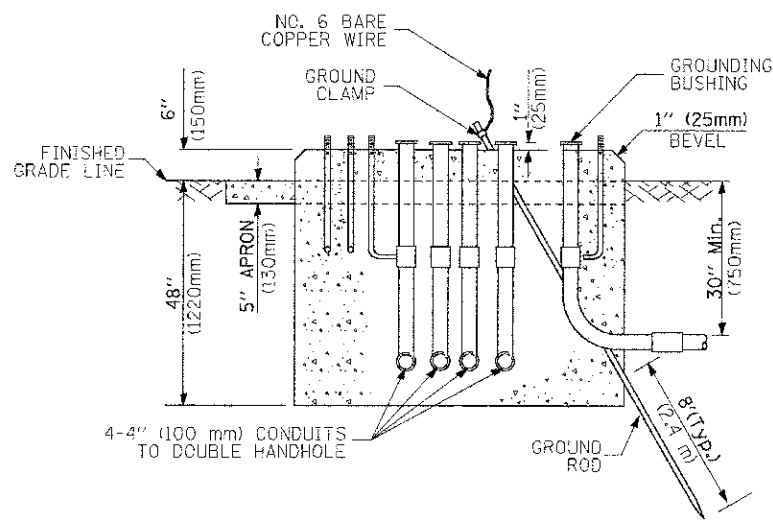
TOP VIEW



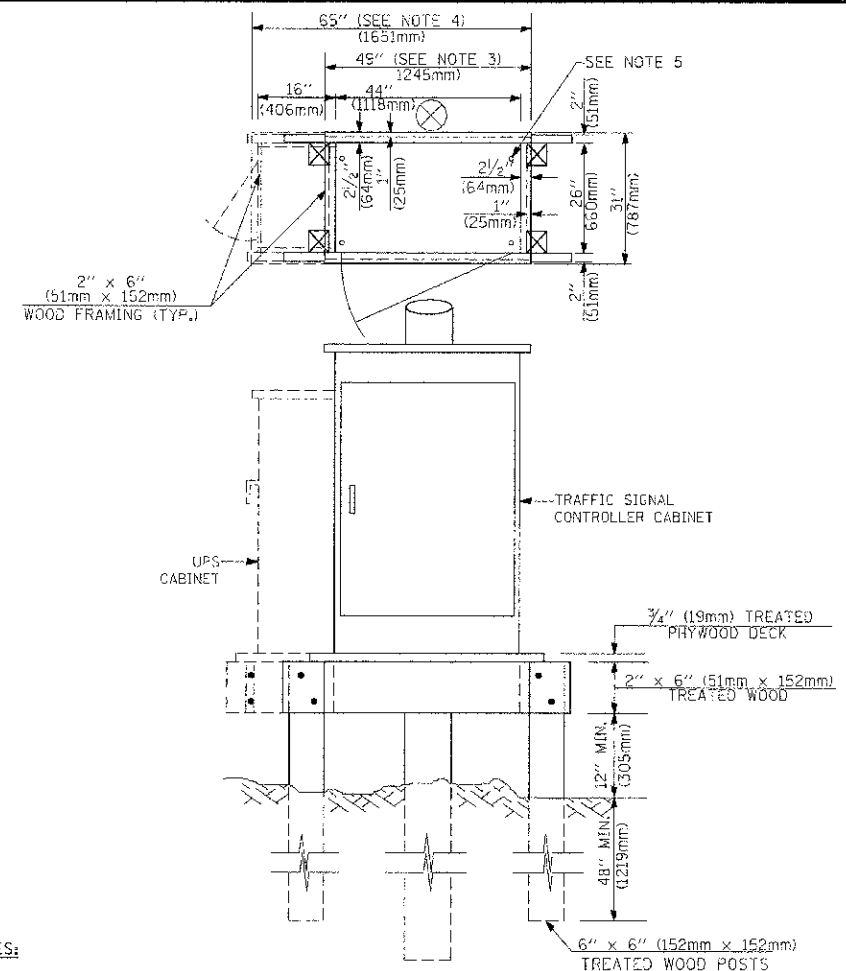
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



TOP VIEW



TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



NOTES:

- BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
- BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
- PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
- PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
- DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
- FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

MAST ARM LENGTH	FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

- These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
- Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
- Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
- For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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USER NAME = bauer-d	DESIGNED - DAG	REVISED -
Drawn	DRAWN - BCK	REVISED -
PLOT SCALE = 50.0000' / 1"	CHECKED - DAD	REVISED -
PLOT DATE = 11/4/2009	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10	577	10-00068-00-TL	WILL	65	45
SCALE: NONE	SHEET NO. 5 OF 6 SHEETS	STA. N/A TO STA. N/A	TS-05	CONTRACT NO. 63788		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S	S	CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I	IP	STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM	R			ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM	RL			STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
GUY WIRE				ABANDON ITEM	A			SIGNAL POST AND FOUNDATION TO BE REMOVED			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				EXISTING INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD				EXISTING PREFORMED INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				PREFORMED SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				RAILROAD SYMBOLS			
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER				EXISTING		PROPOSED	
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT				RAILROAD CONTROL CABINET			
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER				RAILROAD CANTILEVER MAST ARM			
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED				FLASHING SIGNAL			
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)				CROSSING GATE			
MICROWAVE VEHICLE SENSOR								CROSSBUCK			
VIDEO DETECTION CAMERA								RAILROAD SYMBOLS			
VIDEO DETECTION ZONE								RAILROAD CONTROL CABINET			
PAN, TILT, ZOOM CAMERA								RAILROAD CANTILEVER MAST ARM			
WIRELESS DETECTOR SENSOR								FLASHING SIGNAL			
WIRELESS ACCESS POINT								CROSSING GATE			
								CROSSBUCK			

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PLN	DRAWN: BCK	REVISED: -
PLT SCALE: 50.0000' / 1" IN.	CHECKED: DAD	REVISED: -
PLT DATE: 11/4/2009	DATE: 10-28-09	REVISED: -

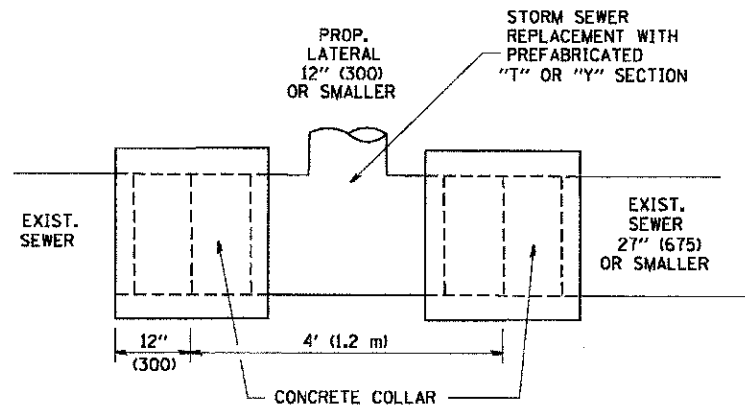
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
F.A.P. RTE. 577	SECTION 10-00068-00-TL
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10
SHEET NO. 6 OF 6 SHEETS	STA. N/A TO STA. N/A

CONTRACT NO. 63788	TOTAL SHEETS 65	SHEET NO. 46
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

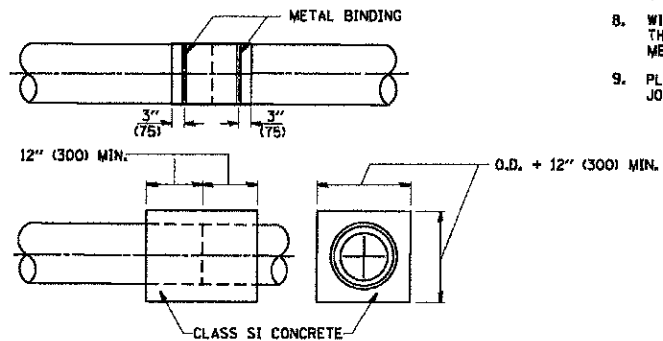
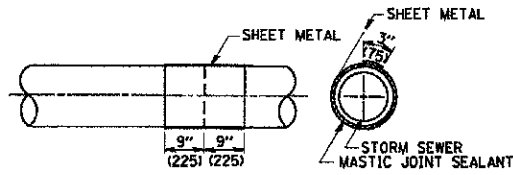
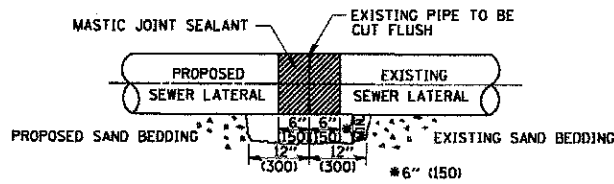
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DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

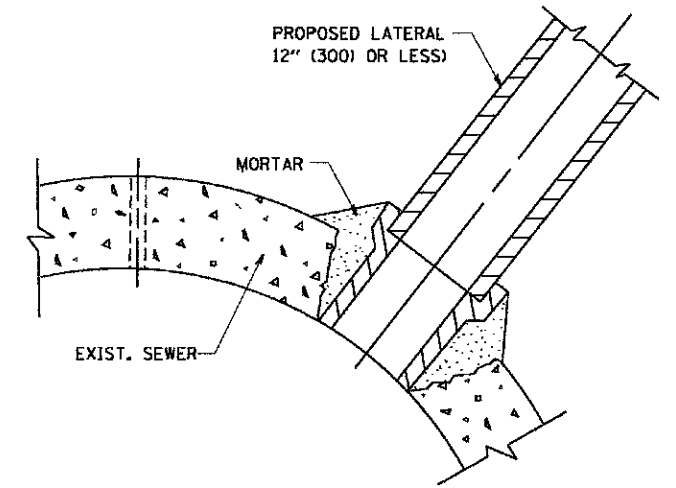


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" X 6" (300 X 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.

II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:

- PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
- PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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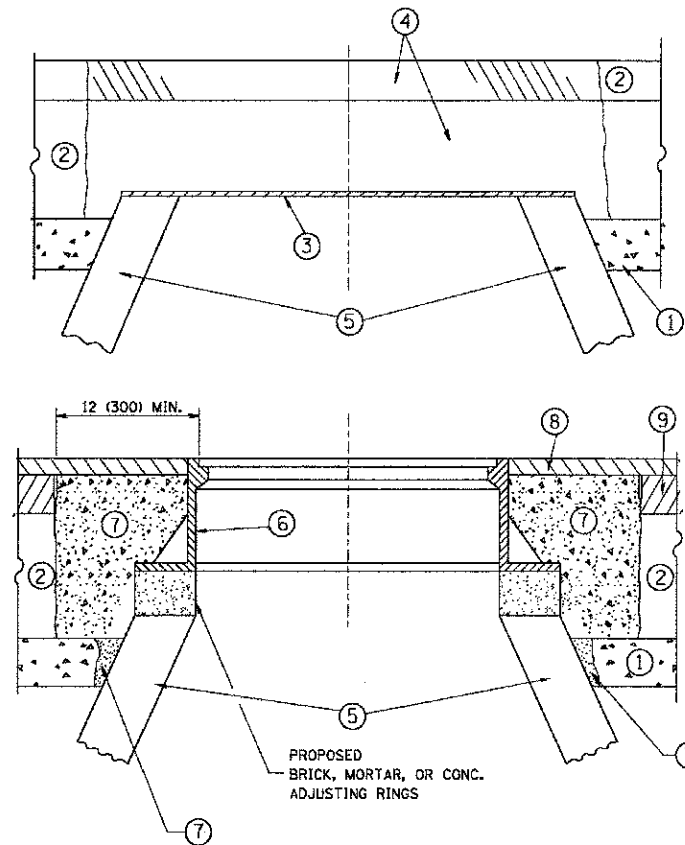
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USER NAME = goglanabt	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92
PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED - R. SHAH 09-09-94
PLOT DATE = 1/4/2008	DATE - 07-25-90	REVISED - R. SHAH 10-25-94
		REVISED - R. SHAH 06-12-96

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER			
PROJECT NO. M-9003(619)	JOB NO. G-91-486-10	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA. N/A	TO STA. N/A		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	47
BD500-01 (BD-7)			CONTRACT NO. 63788	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

Plotted: February 8, 2013 @ 9:55 AM By: Jim Schmidt - Tab: 48 (80-8) 22x34

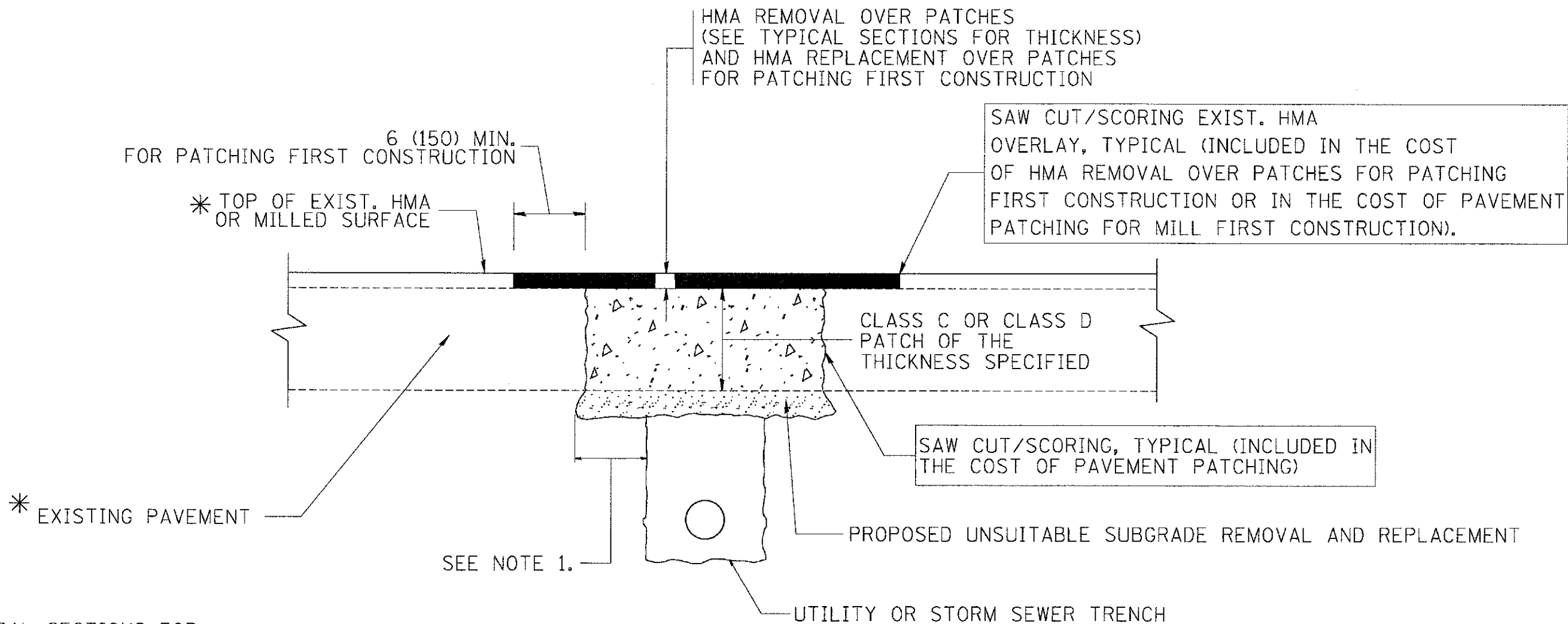
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Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eeinc.com	USER NAME = board1	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
	PLT SCALE = 1/8"=1'-0"	CHECKED -	REVISED - R. BORO 01-01-07
	PLT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 03-09-11
			REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10
SHEET NO. 1 OF 1 SHEETS	STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	48
BD600-03 (BD-8)		CONTRACT NO. 63788		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

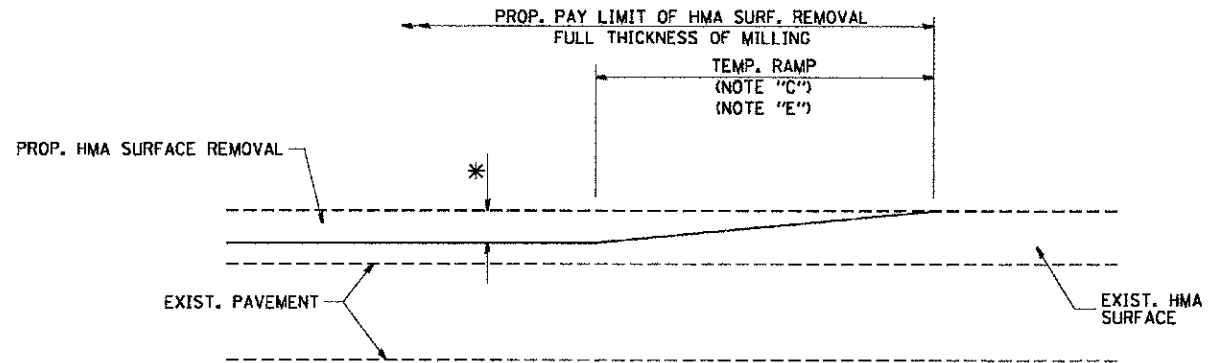
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

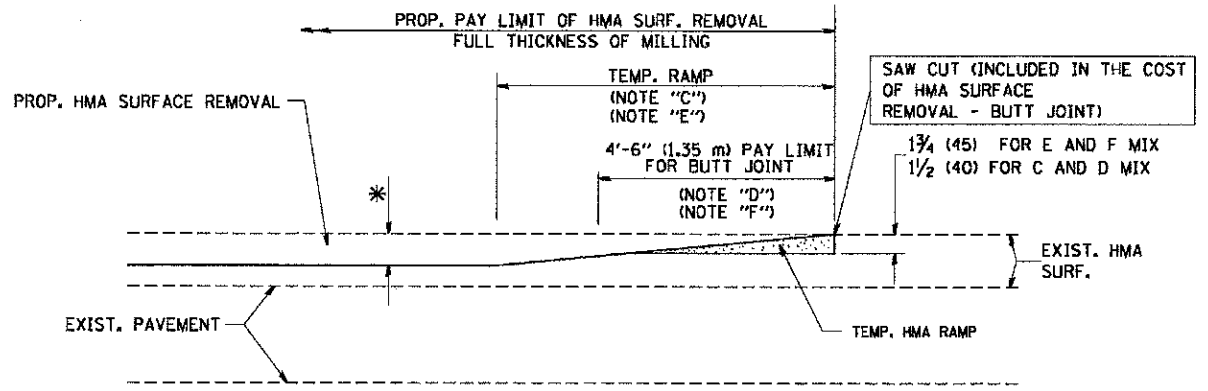
Plotted: February 8, 2013 @ 9:55 AM By: Jim Schmidt - Tab: 49 (BD-22) 22x34

COPYRIGHT © 2013 ENGINEERING ENTERPRISES, INC.				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT		F.A.P. RTE. 577		SECTION 10-00068-00-TL		COUNTY WILL		TOTAL SHEETS 65		SHEET NO. 49		
Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eeiweb.com				USER NAME = baumrd1	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98			PROJECT NO. M-9003(619)		JOB NO. C-91-486-10		BD400-04 (BD-22)		CONTRACT NO. 63788			
				PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. BORO 01-01-07			SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. N/A TO STA. N/A		FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT	
				PLOT DATE = 12/27/2009	CHECKED -	REVISED - R. BORO 09-04-07												
				DATE = 10-25-94		REVISED - K. ENG 10-27-08												



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

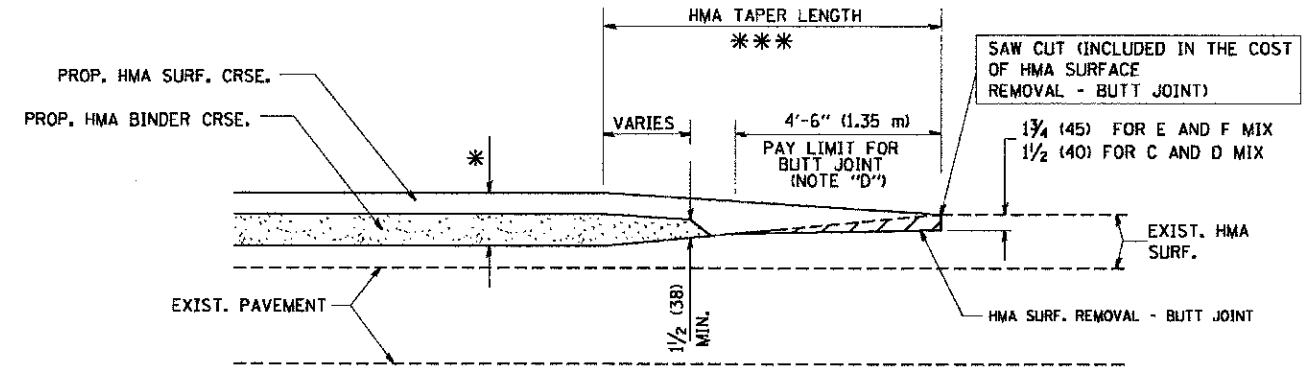
OPTION 1



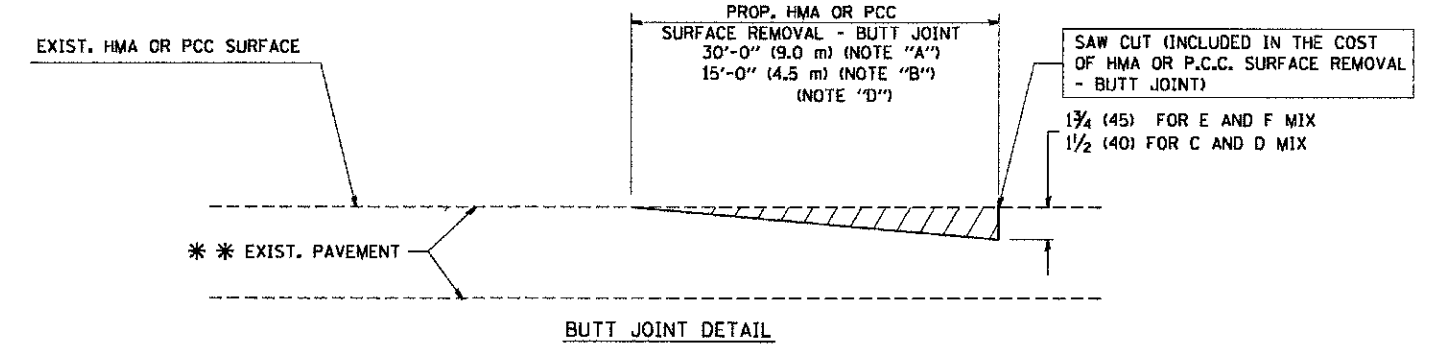
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

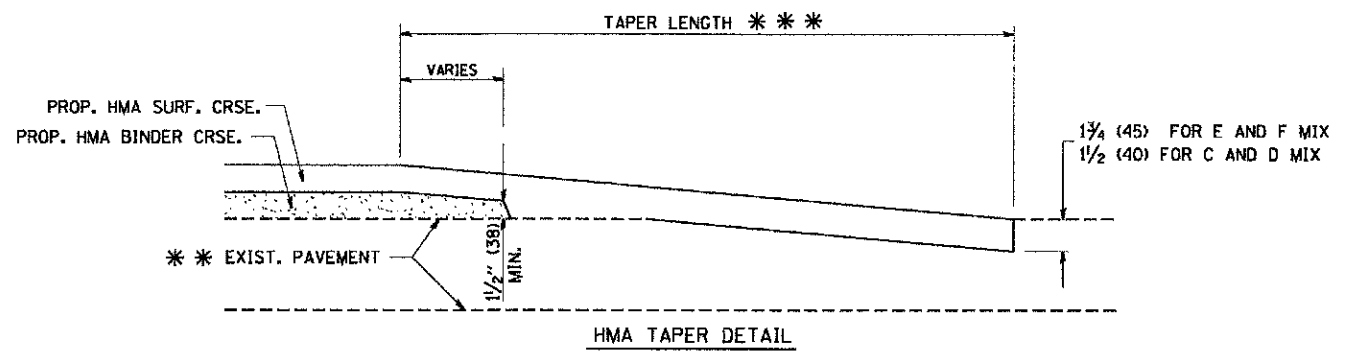
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

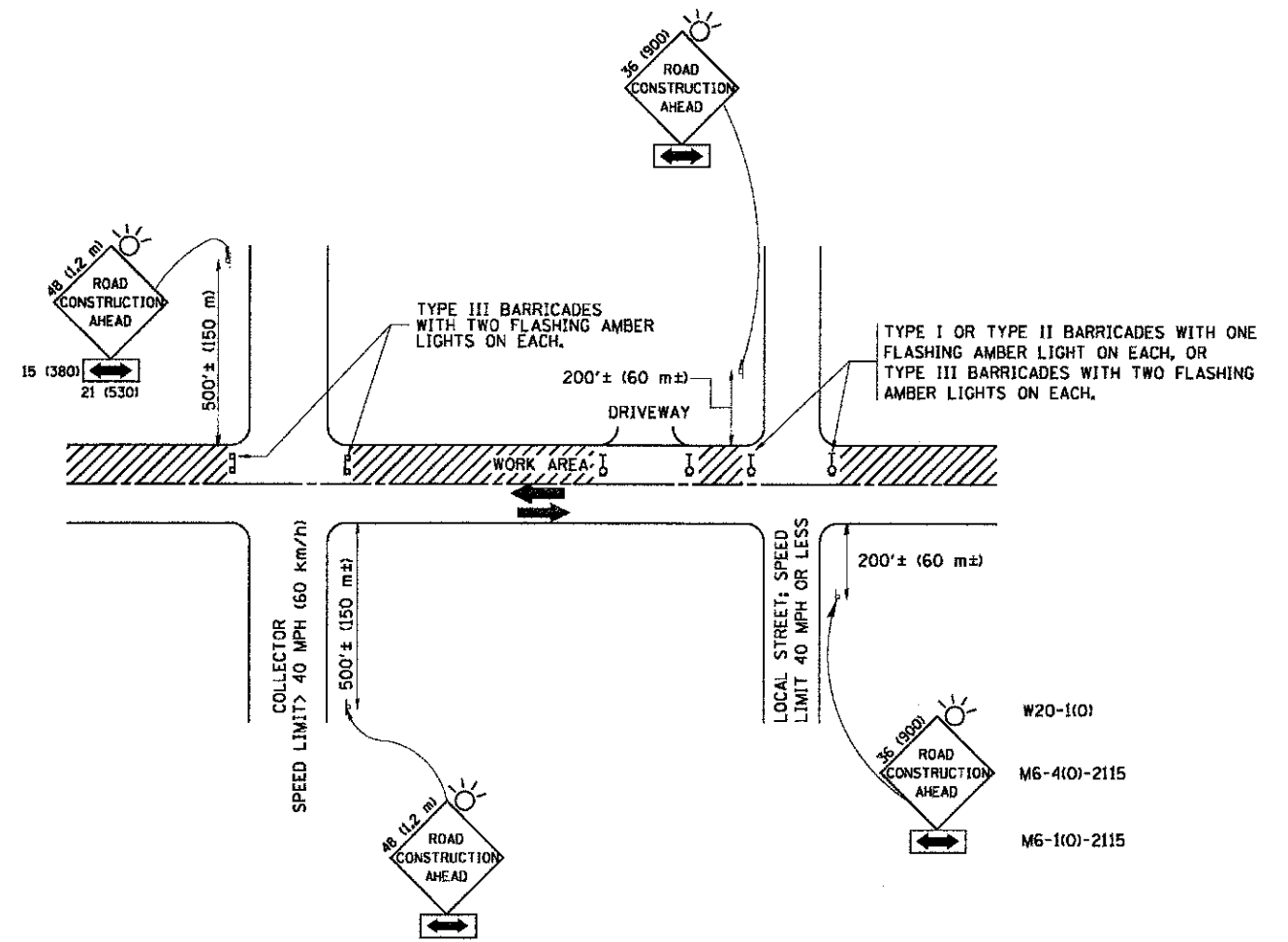
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	PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED - A. ABBAS 03-21-97
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
			REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS	
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10
SHEET NO. 1 OF 1 SHEETS	STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	50
BD400-05 BD32		CONTRACT NO. 63788		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

Plotted: February 8, 2013 @ 10:02 AM By: jsm_schmidt - Tab: 51 (10-10) 22x34

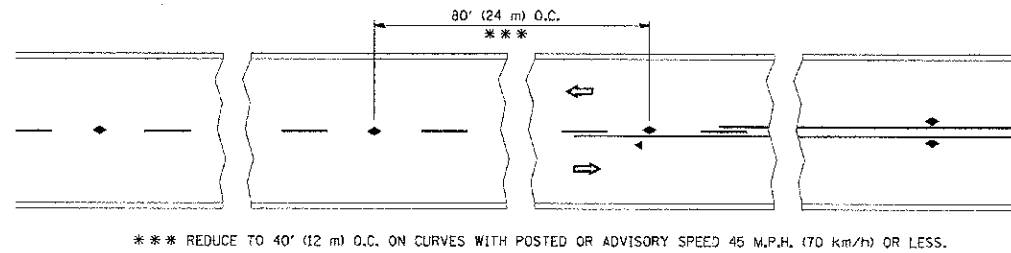
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 630.466.6700 / www.eefweb.com

USER NAME = gaglionob	DESIGNED - LHA	REVISOR - J. OBERLE 10-18-95
PLOT SCALE = 50.000' / IN.	DRAWN -	REVISOR - A. HOUSEH 03-06-96
PLOT DATE = 1/4/2008	CHECKED -	REVISOR - A. HOUSEH 10-15-96
	DATE - 06-09	REVISOR - T. RAMMACHER 01-06-00

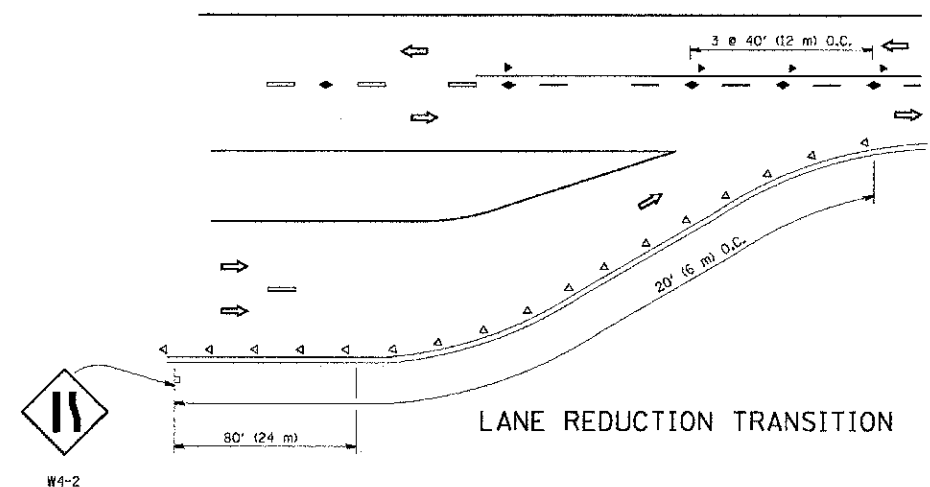
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA. N/A	TO STA. N/A

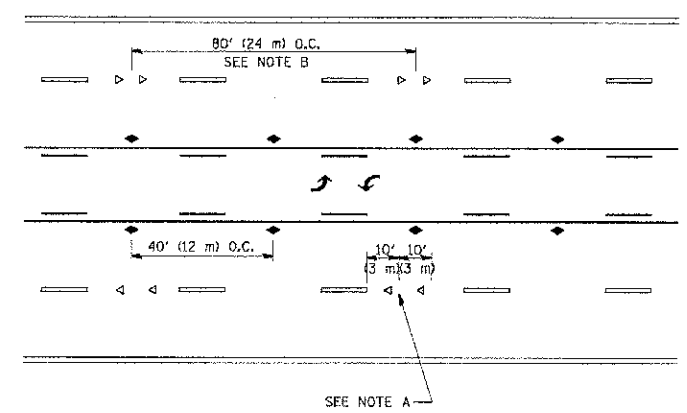
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	51
TC-10		CONTRACT NO. 63788		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



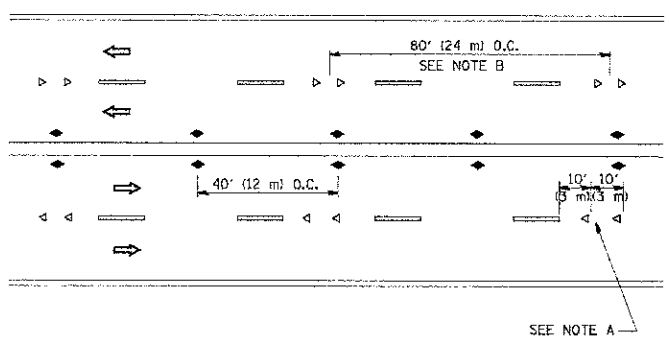
TWO-LANE/TWO-WAY



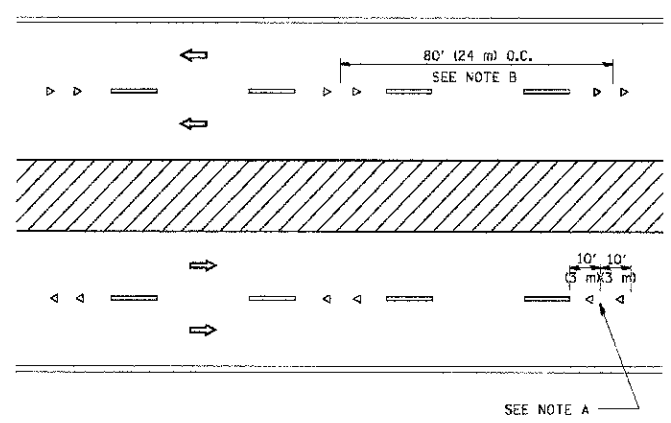
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

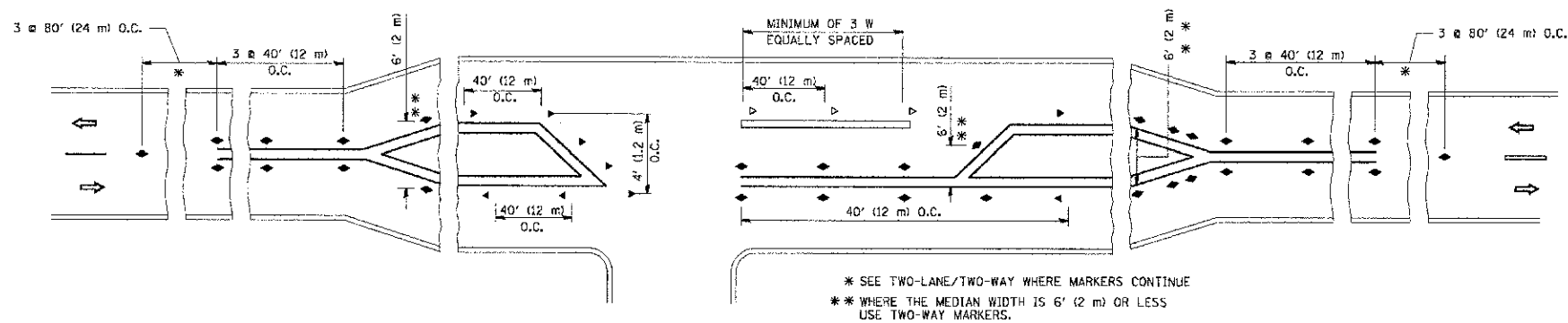
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

Plotted: February 8, 2013 @ 10:03 AM By: Jim Schmidt - Job: 52 (TC-11) 22x34
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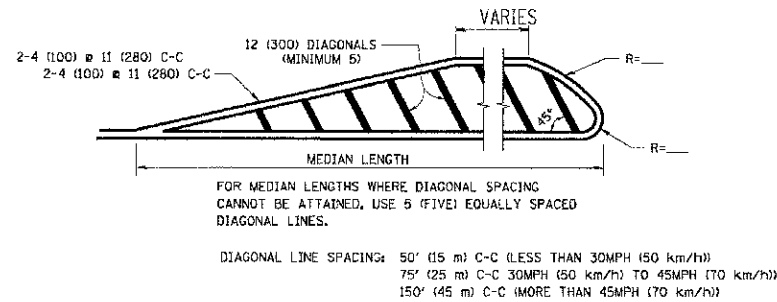
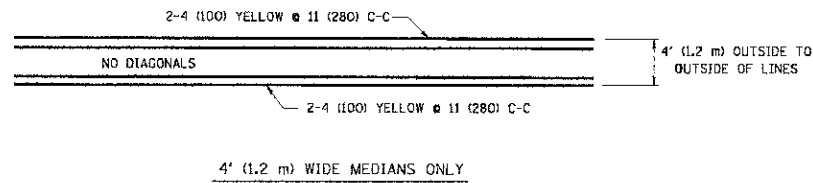
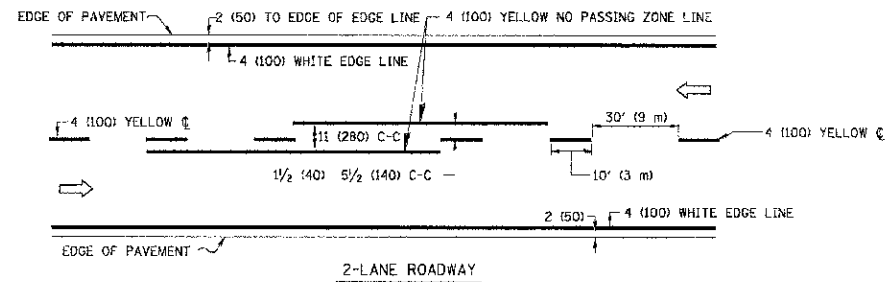
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PLOT SCALE = 50.000' / 1" IN.	DRAWN: -	REVISED: - T. RAMMACHER 03-12-99
PLOT DATE = 3/2/2011	CHECKED: -	REVISED: - T. RAMMACHER 01-06-00
	DATE: -	REVISED: - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

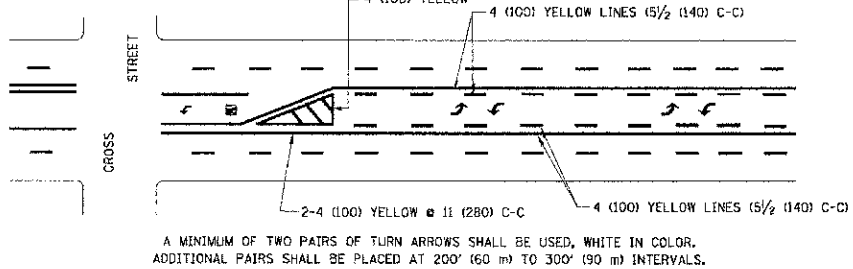
TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	PROJECT NO. M-9003(619)	JOB NO. C-91-486-10	STA. N/A TO STA. N/A
SHEET NO. 1 OF 1 SHEETS			

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	52
TC-11			CONTRACT NO. 63788	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Path: H:\SDSKPROJ\UP0902\DWG\FINAL_ENG\UP0902-DETAILS

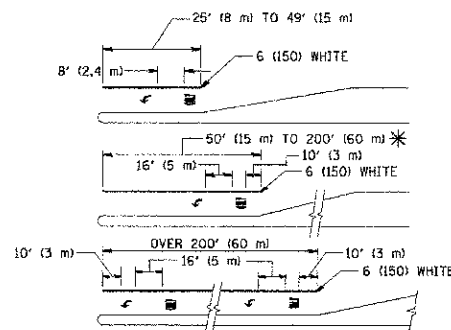


MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

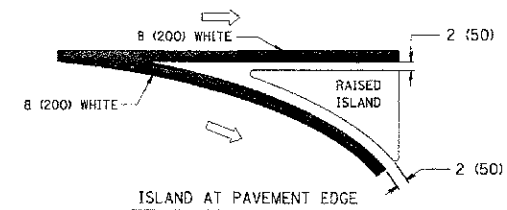
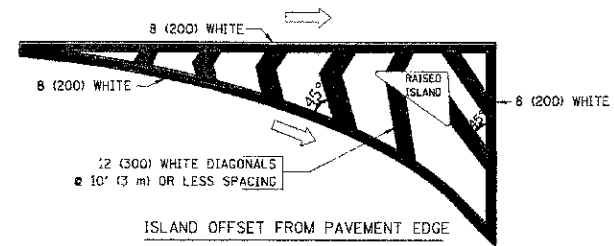


FULL SIZE LETTERS 6" (1.5 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

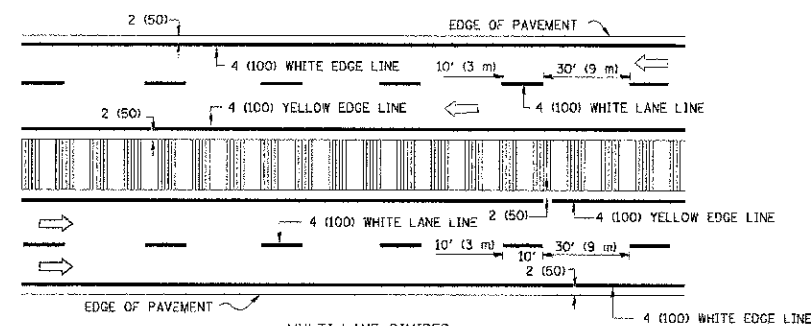
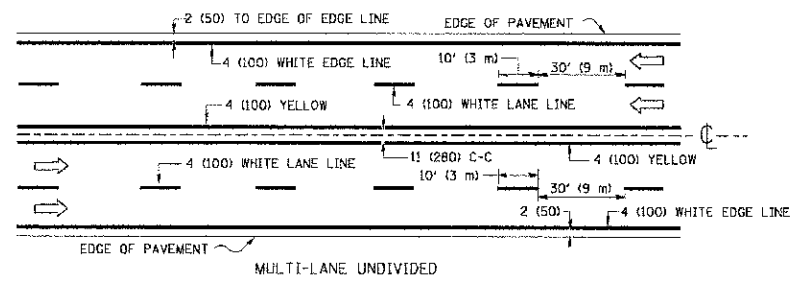


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 & 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 & 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 & 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 & 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 & 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

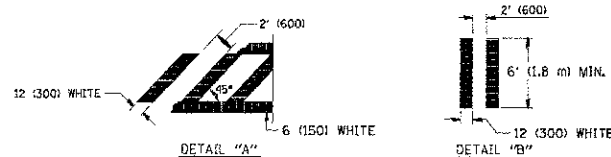
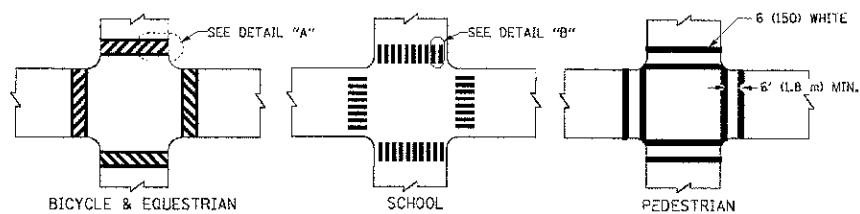
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

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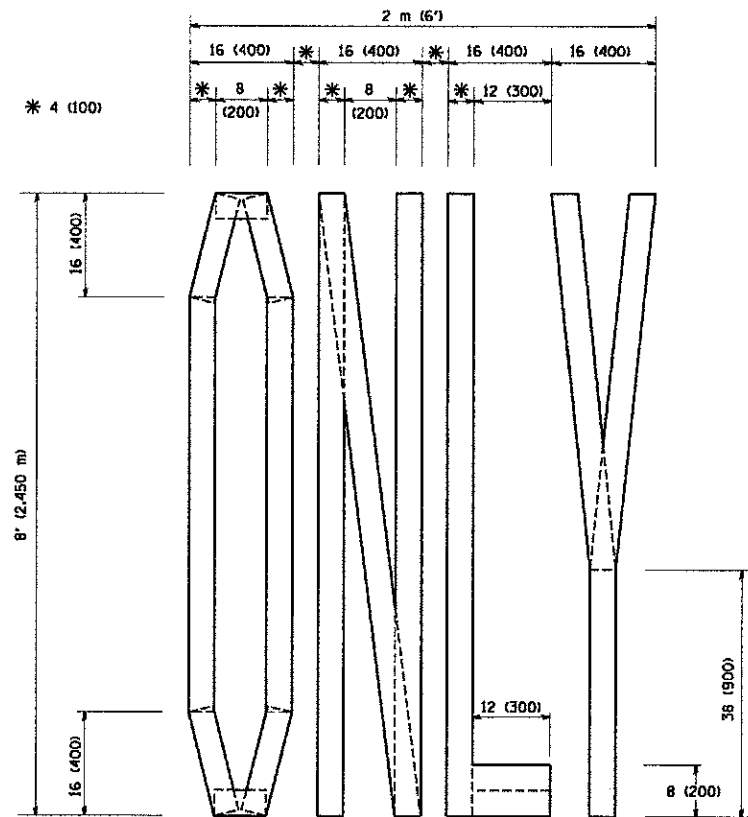
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DESIGNER = EVERS
DRAWN =
CHECKED =
DATE = 03-19-90

DESIGNED - EVERS
REVISED - T. RAMMACHER 10-27-94
DRAWN =
CHECKED =
DATE = 03-19-90

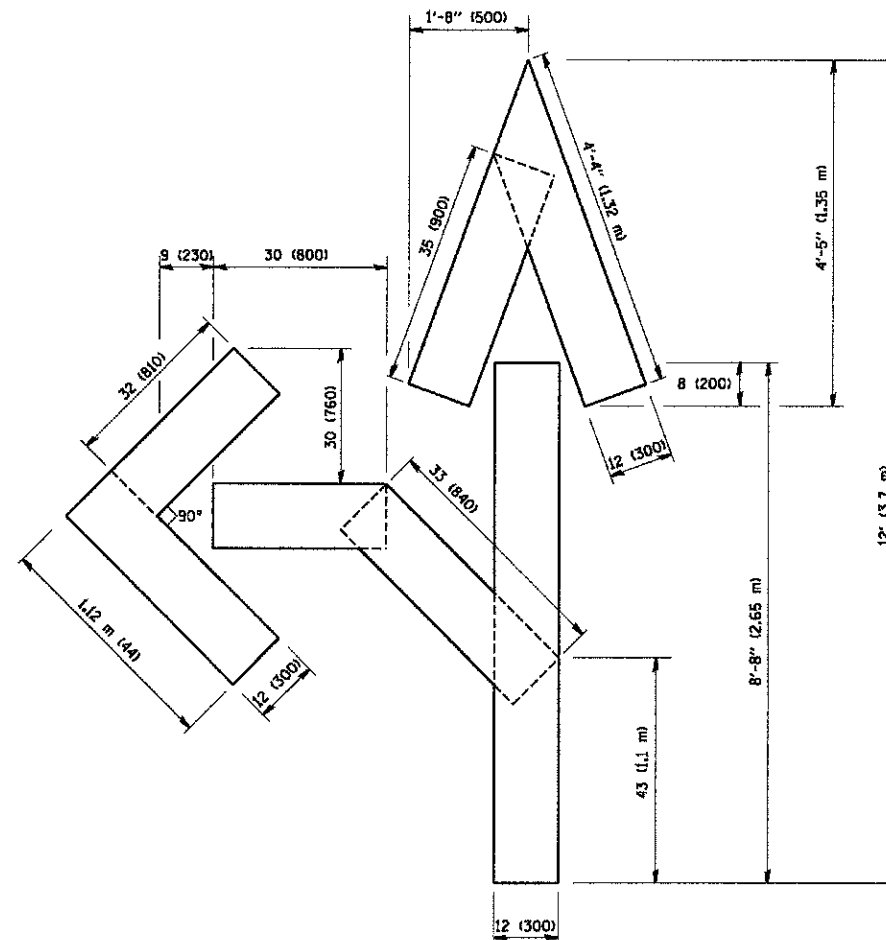
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS
PROJECT NO. M-9003(619) JOB NO. C-91-486-10
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. N/A TO STA. N/A

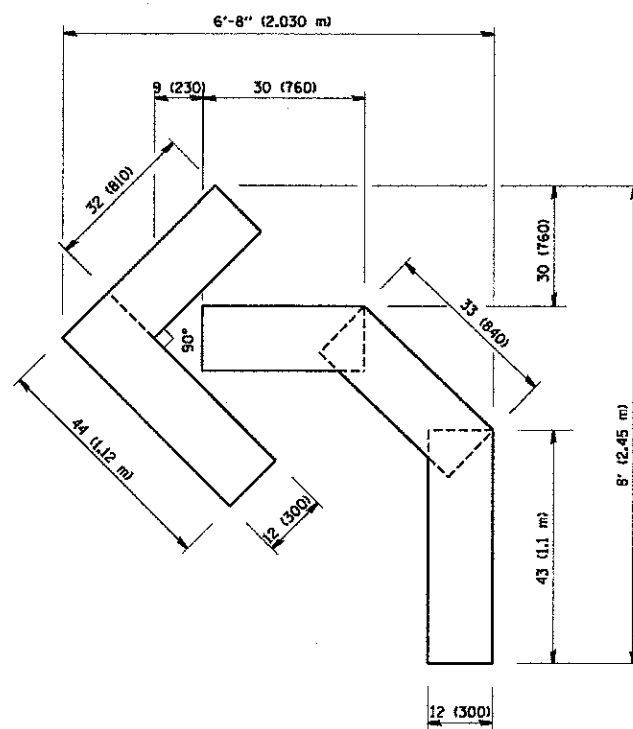
F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
577 10-00068-00-TL WILL 65 53
TC-13 CONTRACT NO. 63788
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = gaglionabt	DESIGNED -	REVISED - T. RAMMACHER 06-05-96
PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - T. RAMMACHER 11-04-97
PLOT DATE = 1/4/2008	CHECKED -	REVISED - T. RAMMACHER 03-02-98
	DATE = 09-18-94	REVISED - E. GOMEZ 08-28-00

DESIGNED -	REVISED - T. RAMMACHER 06-05-96
DRAWN -	REVISED - T. RAMMACHER 11-04-97
CHECKED -	REVISED - T. RAMMACHER 03-02-98
DATE = 09-18-94	REVISED - E. GOMEZ 08-28-00

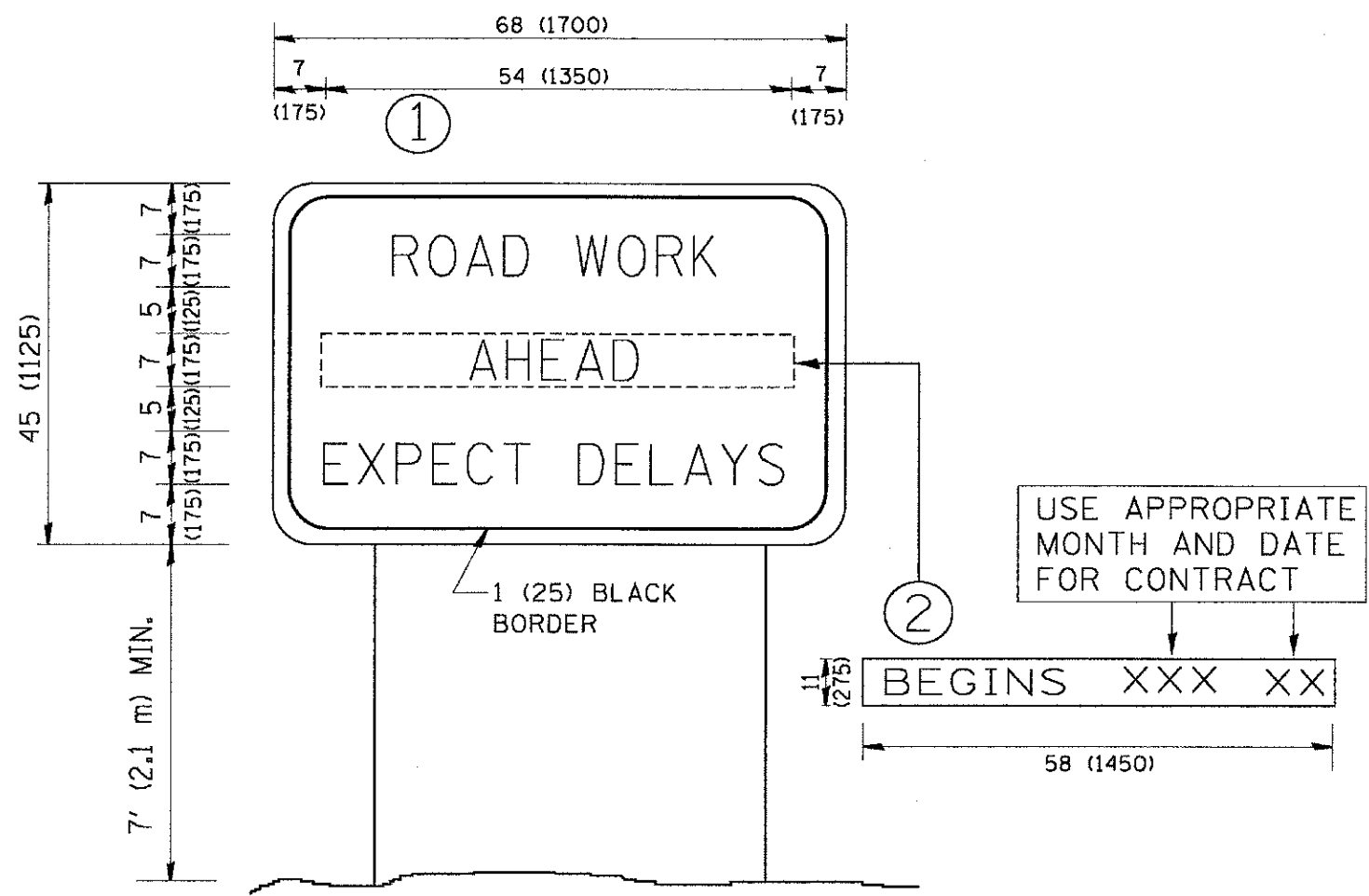
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
FOR TRAFFIC STAGING

PROJECT NO. M-9003(619)	JOB NO. C-91-486-10
SHEET NO. 1 OF 1 SHEETS	STA. N/A TO STA. N/A

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 54
TC-16			CONTRACT NO. 63788	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Path: Y:\SIS\PROJ\10902\QMS\WG\Final_ENG\10902-DETAILS



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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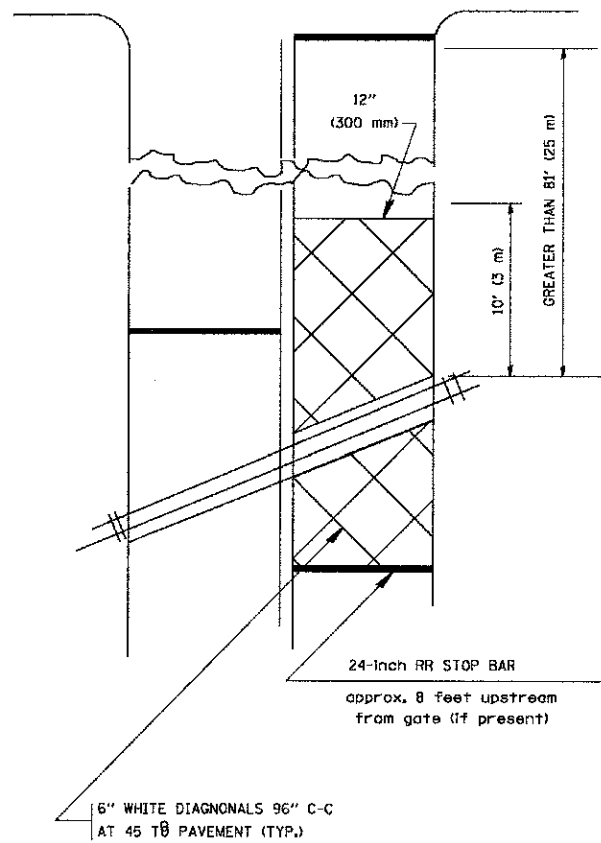
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PLOT DATE = 1/4/2008	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN			
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10		
SHEET NO. 1 OF 1 SHEETS	STA. N/A	TO STA. N/A	

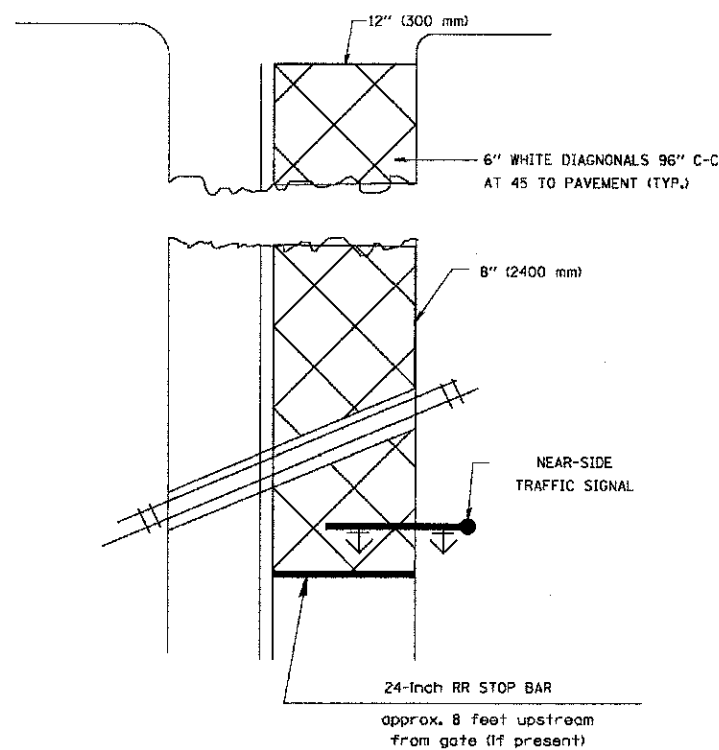
F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 55
TC-22		CONTRACT NO. 63788		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

WITH INTERSECTION TRAFFIC SIGNALS
(SEE NOTE 1)



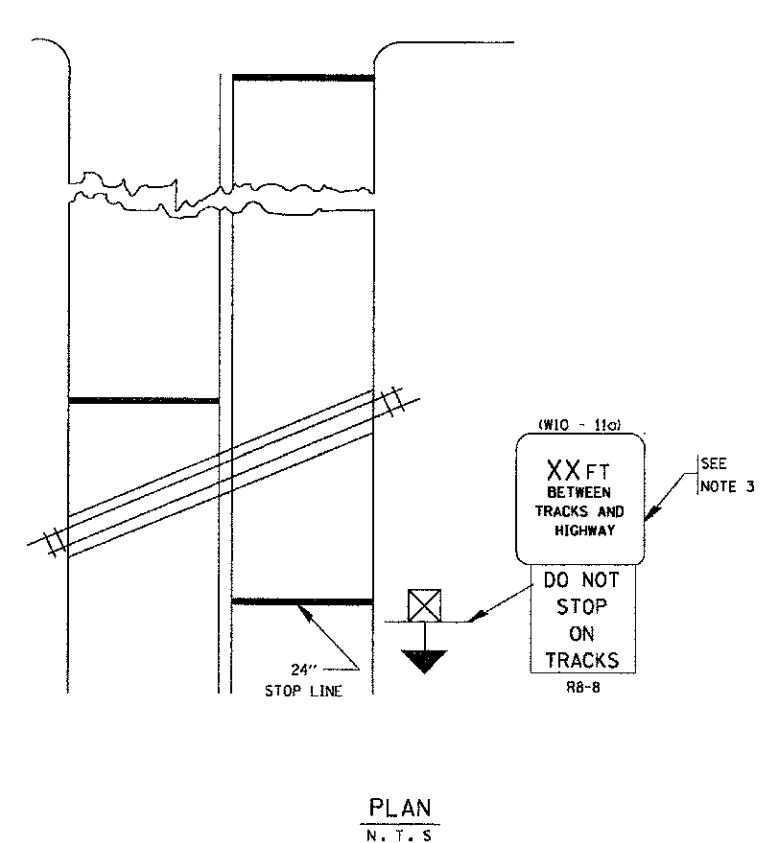
PLAN
N. T. S.

WITH NEAR-SIDE TRAFFIC SIGNALS
(SEE NOTE 1 & 2)



PLAN
N. T. S.

WITH NONSIGNALIZED INTERSECTION
81' (25 m) OR LESS TO CLOSEST RAIL



PLAN
N. T. S.

NOTES:

- PAVEMENT MARKINGS TO BE INSTALLED ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED, THE PAVEMENT MARKINGS EXTENDS TO THE INTERSECTION.
- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET (1.8 m) FROM THE RAIL CLOSEST TO THE INTERSECTION TO THE STOP LINE OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET (1.5 m). WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE THE DRIVER HAS A VIEW OF APPROACHING TRAFFIC. THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6-FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION.

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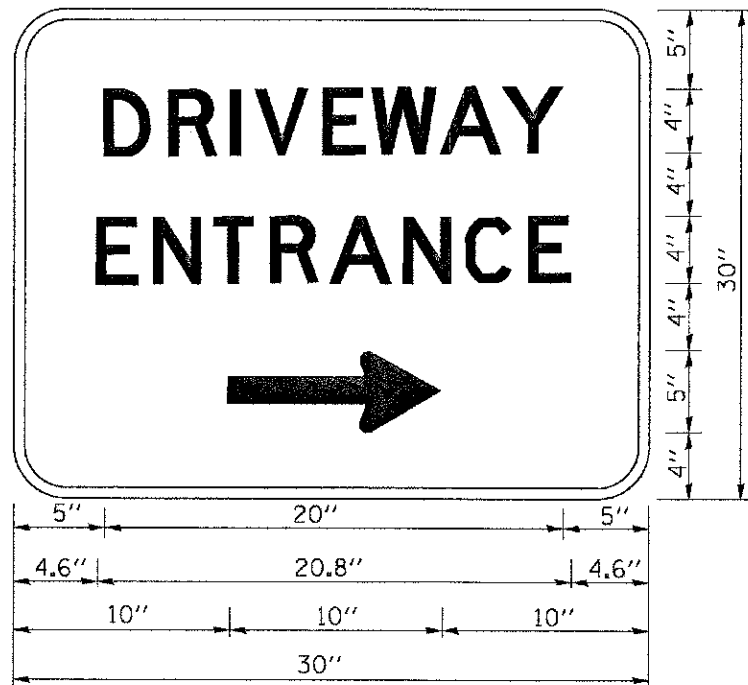
USER NAME = dir cvakosgn	DESIGNED -	REVISED - 02-25-11
	DRAWN -	REVISED - 04-26-12
PLOT SCALE = 50.000' / 1"	CHECKED -	REVISED -
PLOT DATE = 5/7/2012	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS			
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10	F.A.P. RTE. 577	SECTION 10-00068-00-TL
SHEET NO. 1 OF 1 SHEETS	STA. N/A TO STA. N/A	TC-23	

COUNTY	TOTAL SHEETS	SHEET NO.
WILL	65	56
CONTRACT NO. 63788		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

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3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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USER NAME = gagliardi	DESIGNED -	REVISED - C. JUCIUS 02-15-07
EB:ogn	DRAWN -	REVISED -
PLOT SCALE = 50.000' / 1"	CHECKED -	REVISED -
PLOT DATE = 12/13/2012	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING			
PROJECT NO. M-9003(619)	JOB NO. C-91-486-10	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA. N/A	TO STA. N/A		

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 57
TC-26			CONTRACT NO. 63788	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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USER NAME = Jim Schmidt	DESIGNED - JRL & SWM	REVISED - JPS 02/08/13
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	DATE - 11/13/12	REVISED -

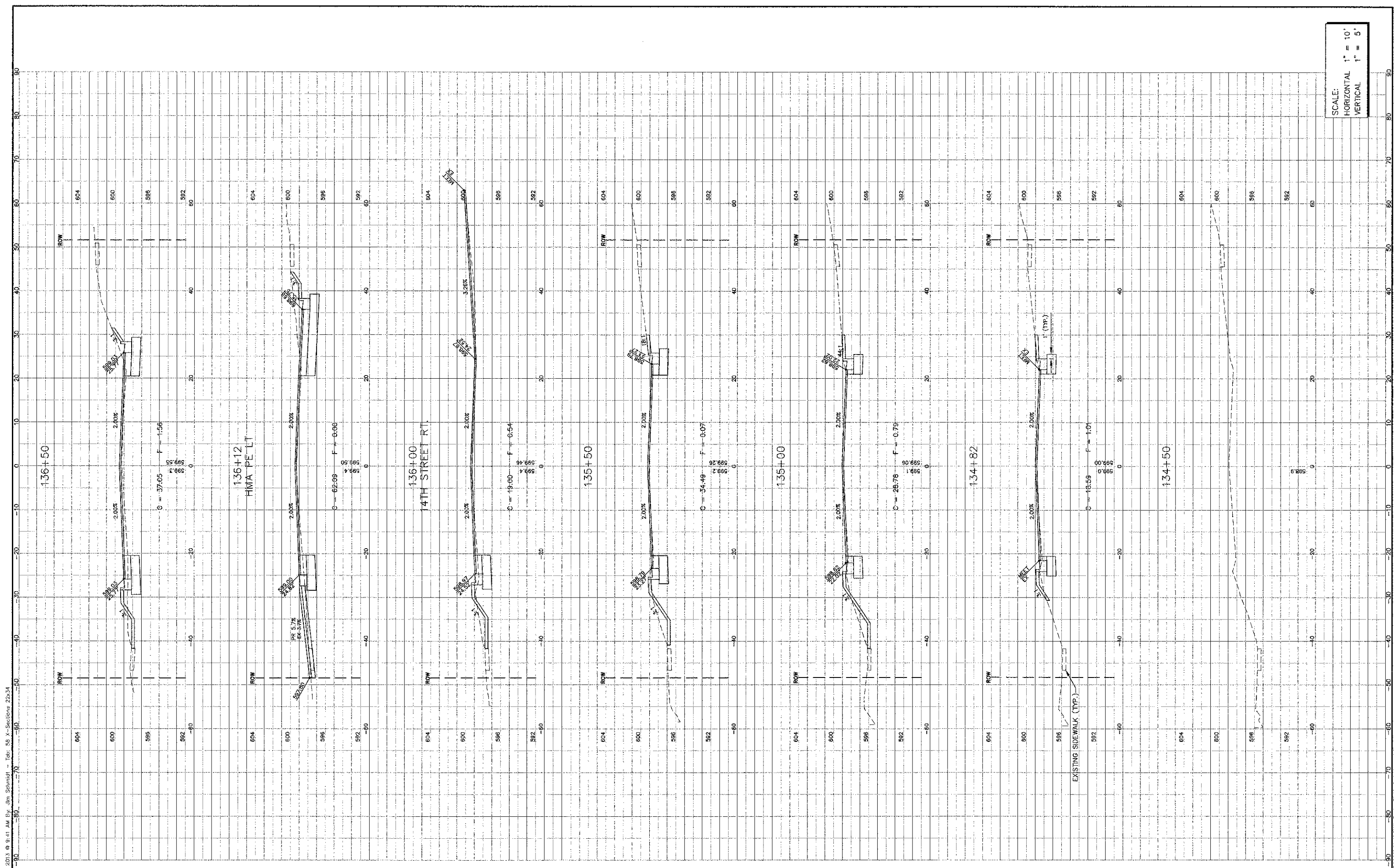
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS - IL 171

SCALE: H¹"=10' V¹"=5' SHEET NO. 1 OF 8 SHEETS STA. 134+50 TO STA. 136+50

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 58
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-800316199			CONTRACT NO. 63788	

SCALE:
 HORIZONTAL 1" = 10'
 VERTICAL 1" = 5'



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USER NAME =	Jim Schmidt
PLOT SCALE =	
PLOT DATE =	February 11, 2013

DESIGNED -	JRL & SWM
DRAWN -	CLN
CHECKED -	JL
DATE -	11/13/12

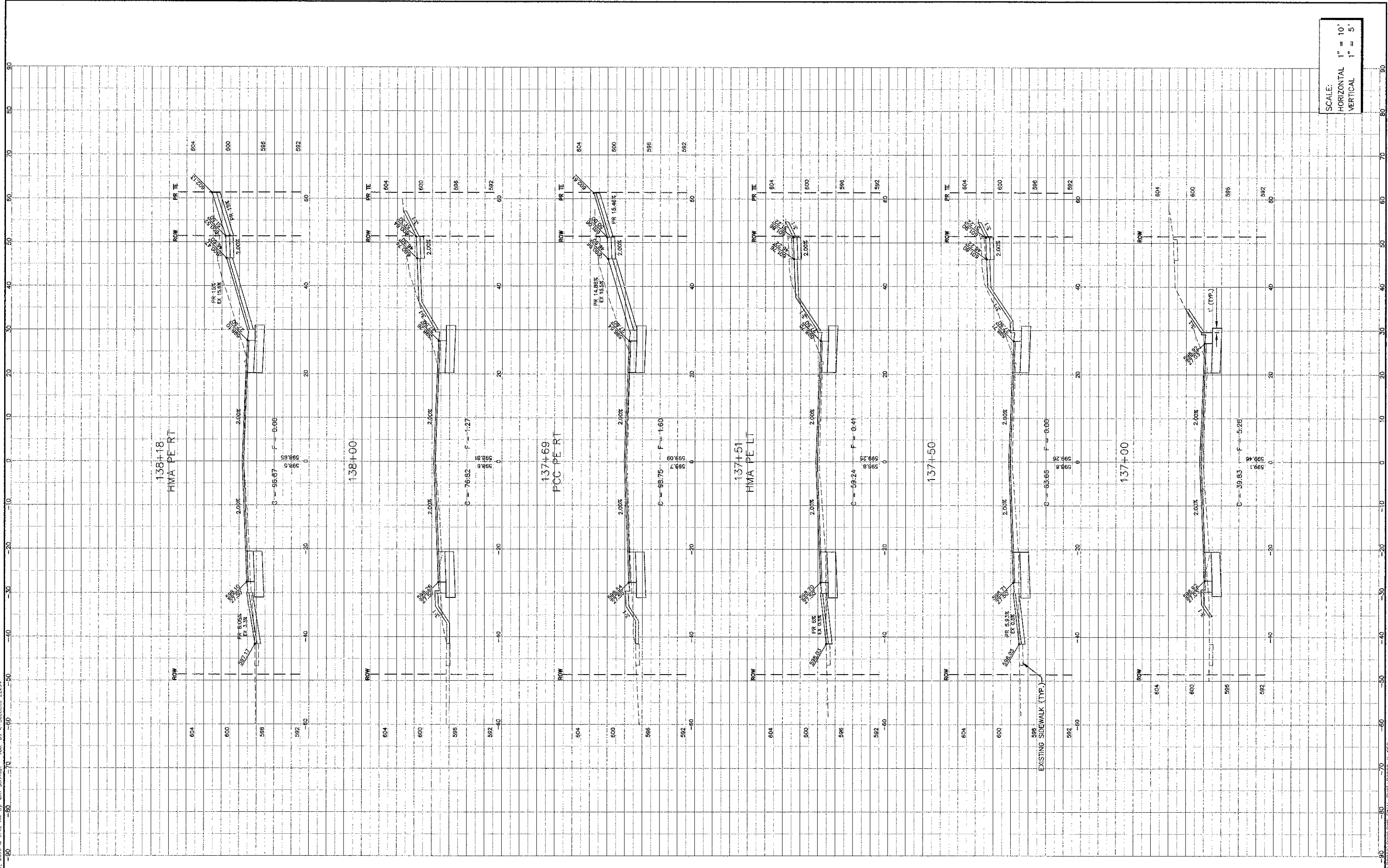
REVISED -	JPS 02/08/13
REVISED -	
REVISED -	
REVISED -	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS - IL 171

SCALE: HORIZ. 1" = 10' VERT. 1" = 5' SHEET NO. 2 OF 8 SHEETS STA. 137+00 TO STA. 138+18

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	59
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(619)			CONTRACT NO. 63788	



SCALE:
 HORIZONTAL 1" = 10'
 VERTICAL 1" = 5'

Plotted: February 11, 2013 @ 9:43 AM By: Jim Schmidt -- Tab: 60 X-Sections 2x23.1

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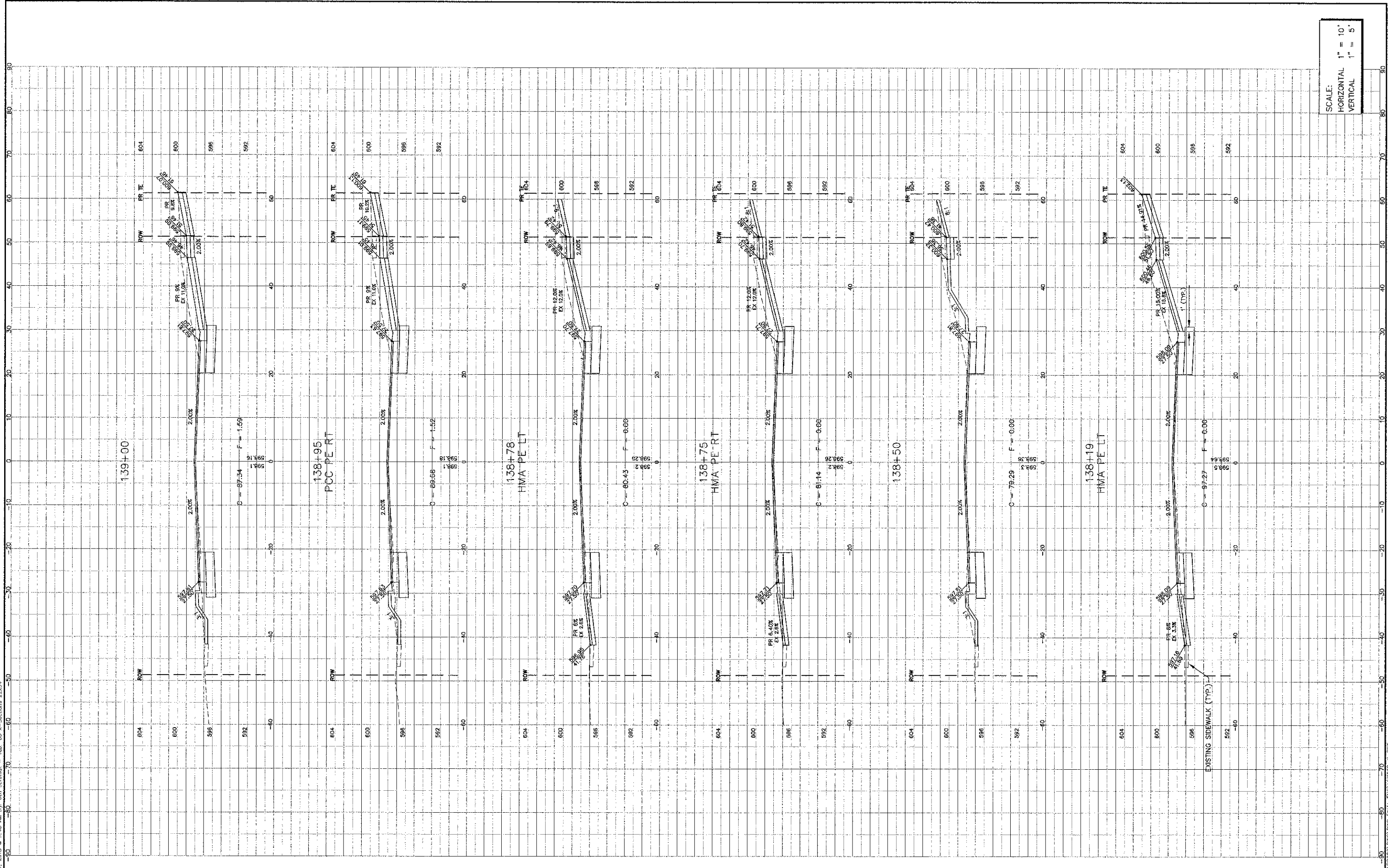
USER NAME = jim schmidt	DESIGNED - JRL & SWM	REVISED - JPS 02/08/13
PLOT SCALE =	DRAWN - CLN	REVISED -
PLOT DATE = February 11, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS - IL 171

SCALE: HORIZ. 1" = 10' VERT. 1" = 5'
 SHEET NO. 3 OF 8 SHEETS STA. 138+19 TO STA. 139+00

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 60
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT M-30031619		
		CONTRACT NO. 63788		



SCALE:
 HORIZONTAL 1" = 10'
 VERTICAL 1" = 5'

Path: Y:\SRP\10082\10082\DWG\FINAL ENG\10082-K-SEC

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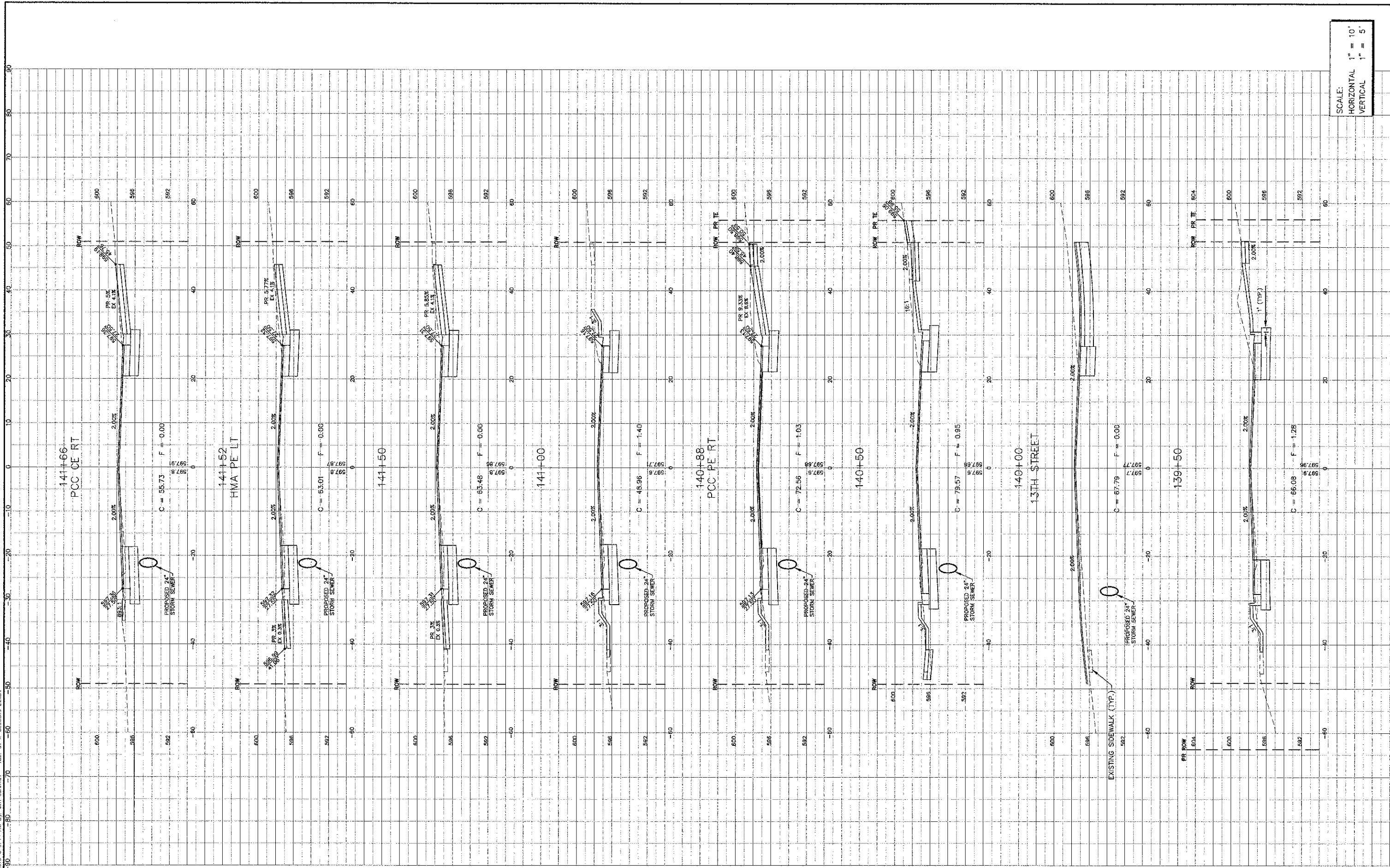
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PLOT DATE = February 11, 2013	CHECKED - JL	REVISED -
	DATE - 11/13/12	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS - IL 171

SCALE: H¹/₄"=10' V¹/₈"=5' SHEET NO. 4 OF 8 SHEETS STA. 139+50 TO STA. 141+66

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 61
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-900316139			CONTRACT NO. 63788	



SCALE:
 HORIZONTAL 1" = 10'
 VERTICAL 1" = 5'

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USER NAME = Jim Schmidt
PLOT SCALE =
PLOT DATE = February 11, 2013

DESIGNED - JRL & SWM
DRAWN - CLN
CHECKED - JL
DATE - 11/13/12

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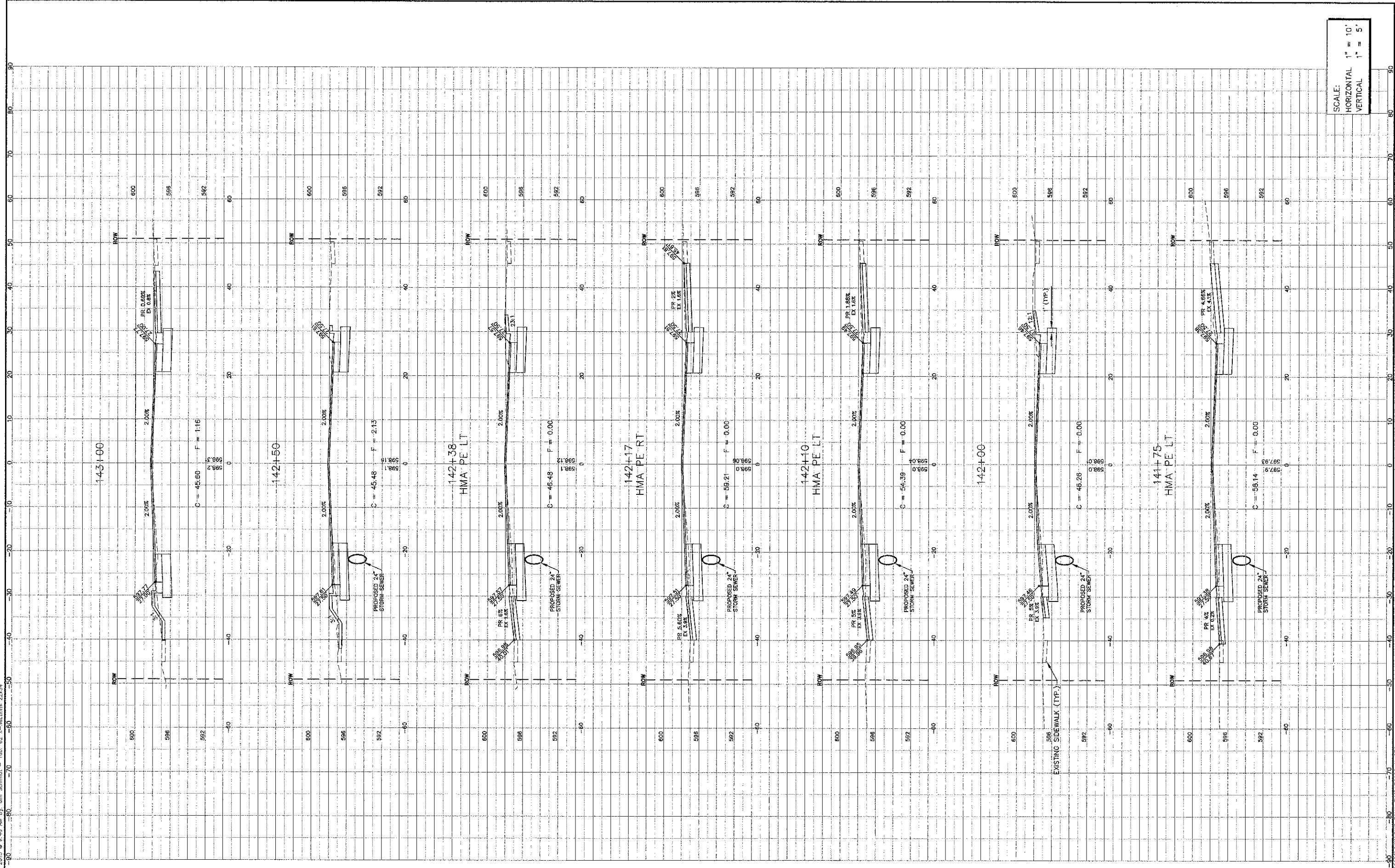
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS - IL 171

SCALE: H¹/₄"=10' V¹/₄"=5' SHEET NO. 5 OF 8 SHEETS STA. 141+75 TO STA. 143+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	62
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90031619			CONTRACT NO. 63788	

SCALE:
HORIZONTAL 1" = 10'
VERTICAL 1" = 5'



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USER NAME = jim_schmidt
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 PLOT DATE = February 11, 2013

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 DRAWN - CLN
 CHECKED - JL
 DATE - 11/13/12

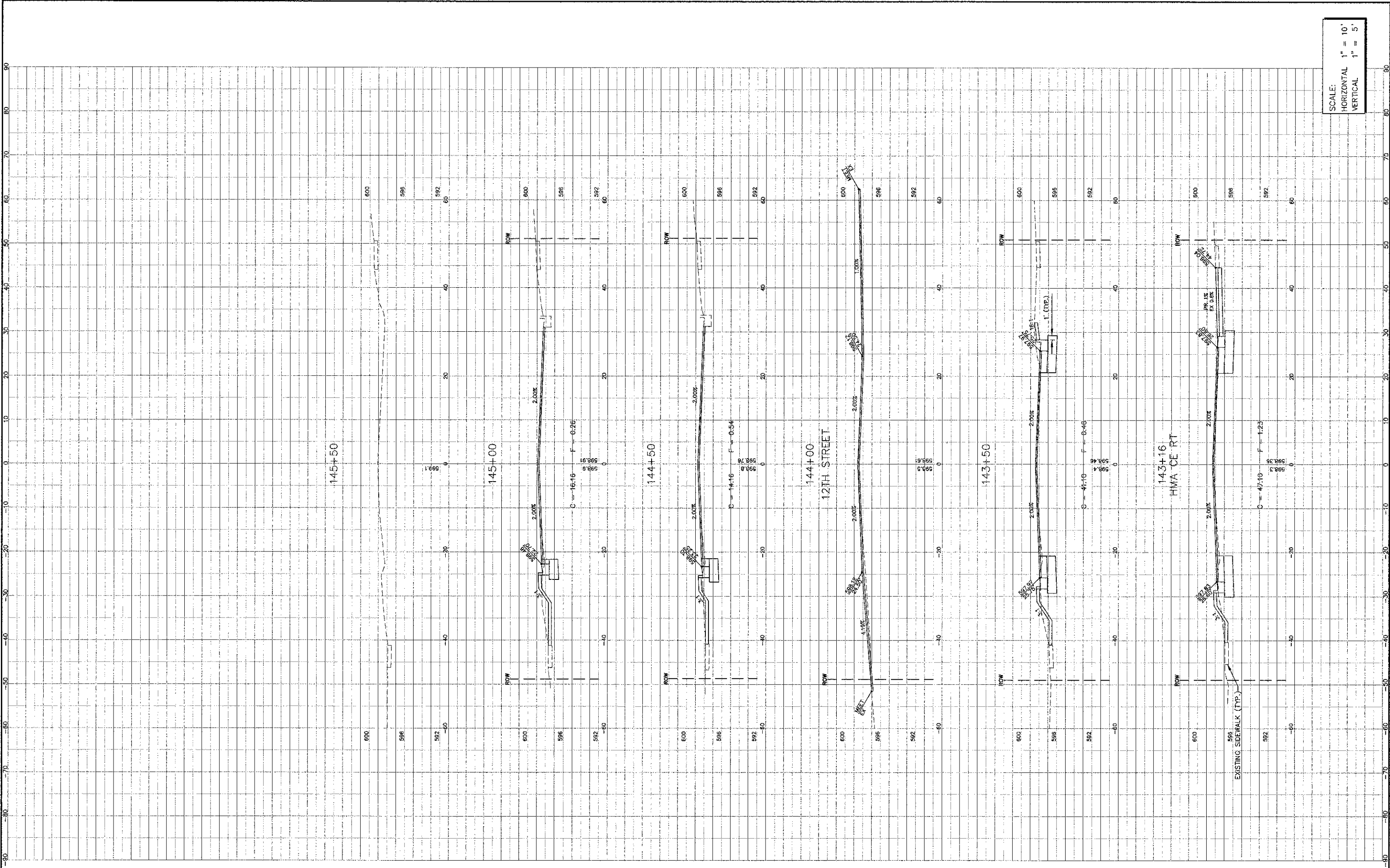
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 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
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CROSS SECTIONS - IL 171

SCALE: HORIZ. 1" = 10' VERT. 1" = 5'
 SHEET NO. 6 OF 8 SHEETS STA. 143+16 TO STA. 145+00

F.A.P. RTE. 577	SECTION 10-00068-00-TL	COUNTY WILL	TOTAL SHEETS 65	SHEET NO. 63
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(6159)			CONTRACT NO. 63788	



SCALE:
 HORIZONTAL 1" = 10'
 VERTICAL 1" = 5'

Plotfile: February 11, 2013 @ 10:23 AM By: Jim Schmidt - Tab: 64 X-Sections 27x34

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USER NAME = Jim Schmidt
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 PLOT DATE = February 11, 2013

DESIGNED - JRL & SWM
 DRAWN - CLN
 CHECKED - JL
 DATE - 11/13/12

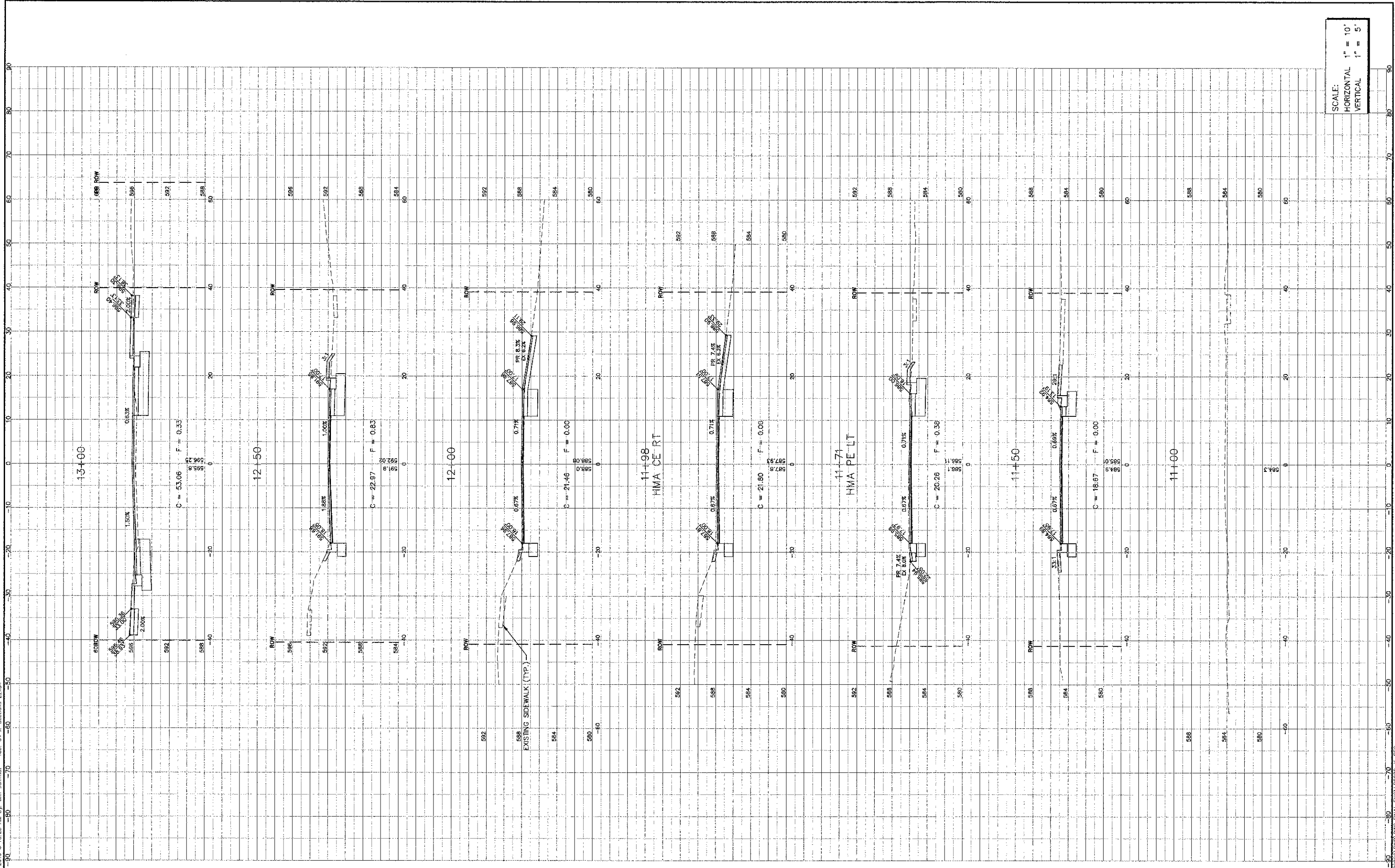
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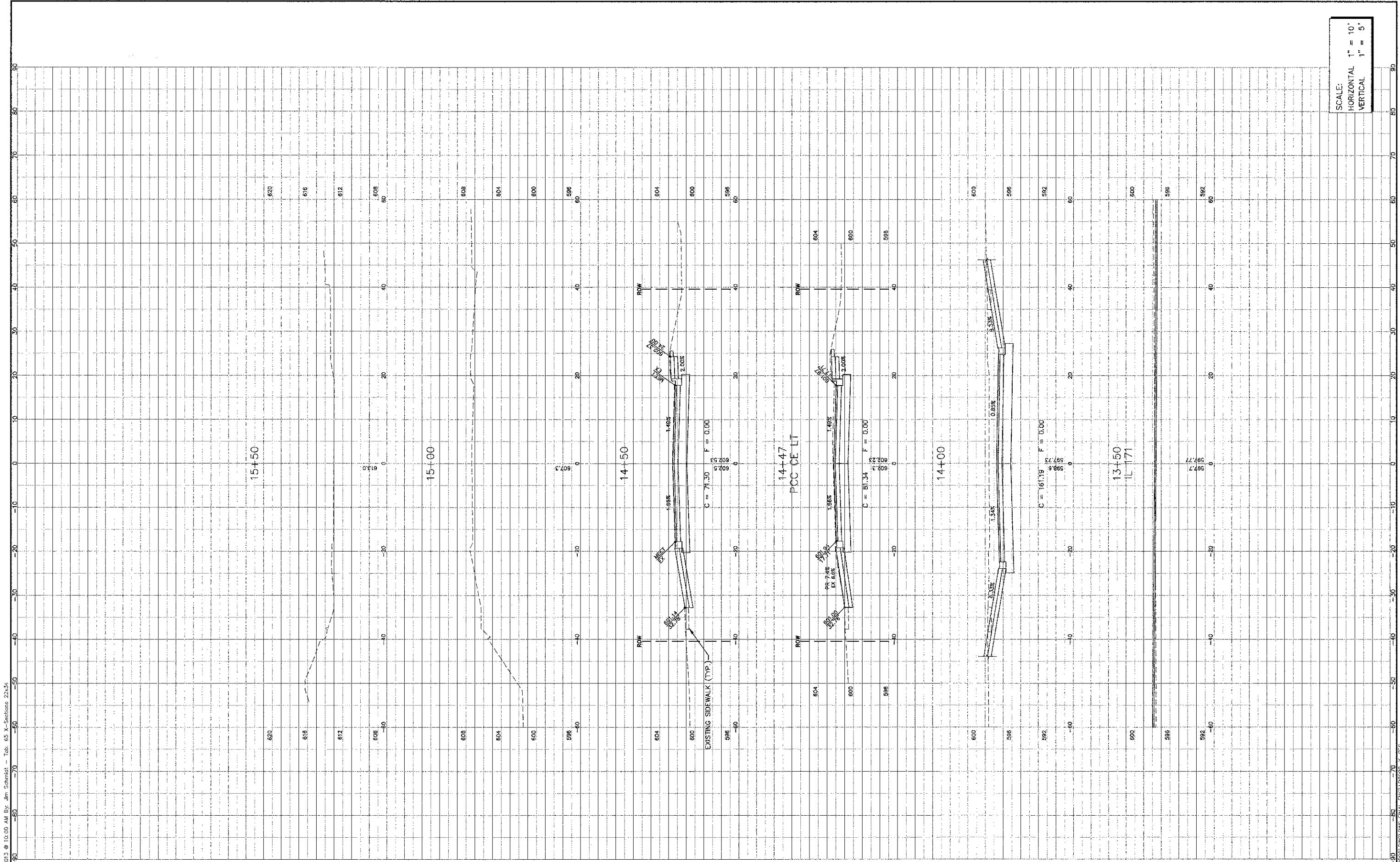
CROSS SECTIONS - 13TH STREET
 SCALE: H=1"=10' V=1"=5' SHEET NO. 7 OF 8 SHEETS STA. 11+00 TO STA. 13+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	64
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-3003(G19)			CONTRACT NO. 63788	

SCALE:
 HORIZONTAL 1" = 10'
 VERTICAL 1" = 5'



SCALE:
HORIZONTAL 1" = 10'
VERTICAL 1" = 5'



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USER NAME =	Jim Schmidt
PLOT SCALE =	
PLOT DATE =	February 11, 2013

DESIGNED -	JRL & SWM
DRAWN -	CLN
CHECKED -	JL
DATE -	11/13/12

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS - 13TH STREET

SCALE: HORIZONTAL 1" = 10' VERTICAL 1" = 5' SHEET NO. 8 OF 6 SHEETS STA. 13+50 TO STA. 15+50

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
577	10-00068-00-TL	WILL	65	65
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-9003(619)			CONTRACT NO. 63788	

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