

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY**

**FAU 1033 (HIRSCH STREET)  
CUL-DE-SAC TO 43RD STREET  
ROADWAY RESURFACING  
Section No.: 12-00079-00-RS  
Project No. M-4003(125)  
CITY OF NORTHLAKE  
COOK COUNTY  
JOB NO.: C-91-128-13**

F.A.U. RTE. 1033	SECTION 12-00079-00-RS	COUNTY COOK	TOTAL SHEET SHEETS: NO. 17 1
ILLINOIS FED. AID PROJECT			

CONTRACT NO. 63831

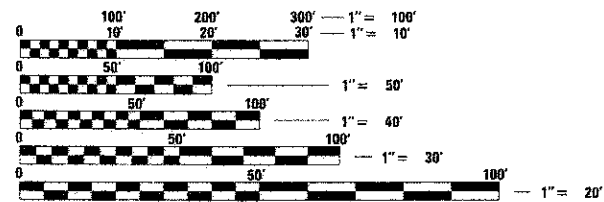
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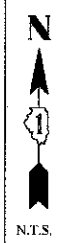
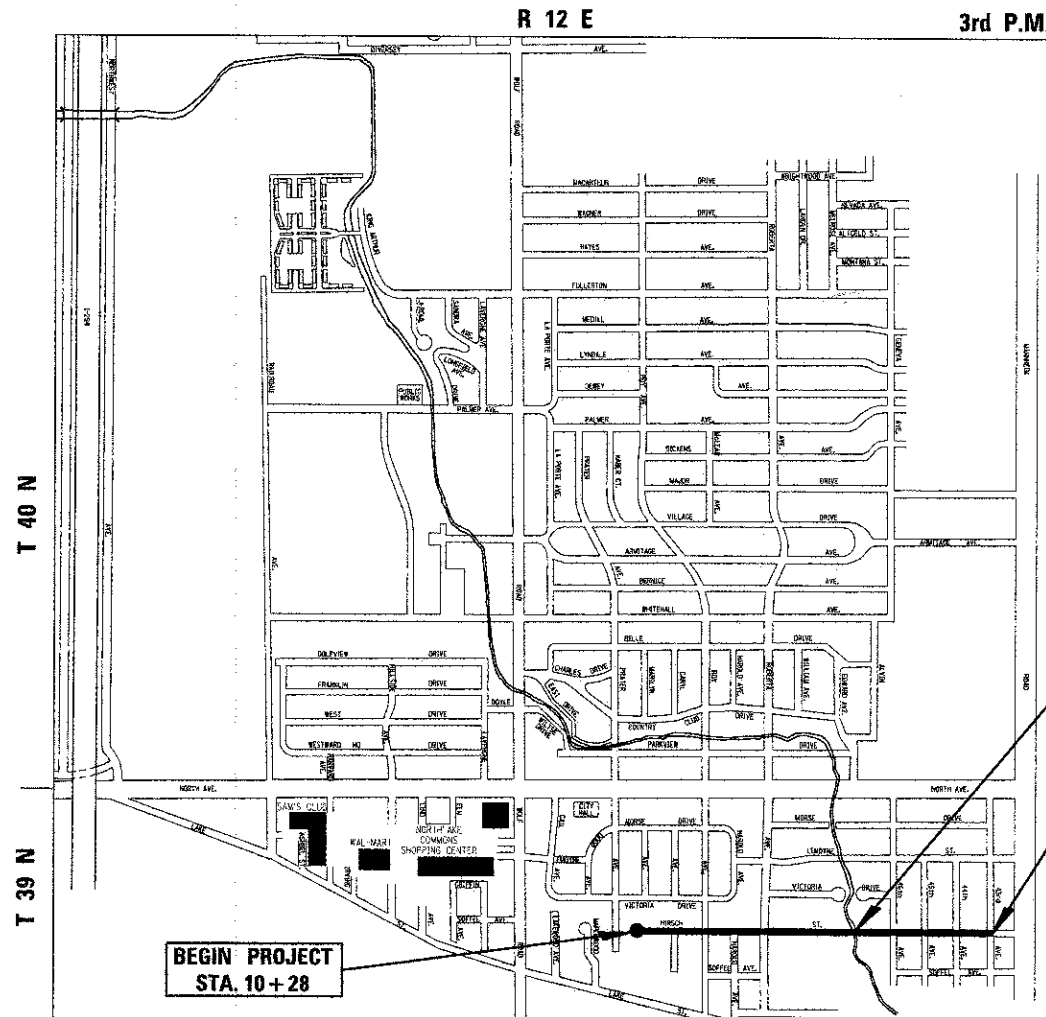
**CHRISTOPHER B. BURKE** ENGINEERING LTD.  
 9575 West Higgins Road, Suite 600  
 Rosemont, Illinois 60018 (847) 823-0500

**TRAFFIC DATA**  
 CLASSIFICATION - RESIDENTIAL COLLECTOR  
 ADT (2011) = 1900  
 SPEED LIMIT = 25 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123



S.N. 016-7615  
 STA. 33+33

END PROJECT  
 STA. 47+06

BEGIN PROJECT  
 STA. 10+28

**LOCATION MAP - PROVISO TOWNSHIP**

GROSS LENGTH OF PROJECT = 3678 LINEAL FEET (0.70 MI.)  
 NET LENGTH OF PROJECT = 3462 LINEAL FEET (0.66 MI.)

CONTRACT NO. 63831

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

APPROVED 3/25 20 13

*[Signature]*  
 MAYOR  
 CITY OF NORTHLAKE

PASSED APRIL 15 20 13

*[Signature]*  
 DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR  
 BID BASED ON  
 LIMITED REVIEW April 18 20 13

*[Signature]*  
 DEPUTY DIRECTOR OF HIGHWAYS  
 REGION ONE ENGINEER



March 25, 2013

*[Signature]*  
**ANDREW M. PUFUNDT**  
 ILLINOIS REGISTRATION No. 062-061729 ENGINEER  
 EXPIRATION DATE: 11/30/15

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4906, SCHAMBURG, IL

**GENERAL NOTES**

**SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS**

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2012; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2013; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", SSTCI; THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JULY 2009 SIXTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE LAWS AND GOVERNMENT AGENCY REGULATIONS AND RULES; AUTHORITIES HAVING JURISDICTION; OSHA REGULATIONS AND RULES; AND ANY APPLICABLE RULES AND REGULATIONS OF THE STATE OF ILLINOIS OR COOK COUNTY AGENCIES, FURTHERMORE, AND AS RELATED TO THE WORK, THE CONTRACTOR SHALL GIVE NOTICES AND COMPLY WITH APPLICABLE LAWS, ORDINANCES, RULES, REGULATIONS, AND LAWFUL ORDERS OF ALL PUBLIC AUTHORITIES BEARING ON THE SAFETY OF PERSONS OR PROPERTY OR THEIR PROTECTION FROM DAMAGE, INJURY OR LOSS.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

**UTILITIES**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)

**STAKING**

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED, AND SHALL BE AS INDICATED ON THE PLANS. ELEVATIONS SHOWN AT POINT OF CURVE, ETC. IS EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

**WATER, STORM SEWER AND SANITARY SEWER**

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE COMPLETED PER ARTICLE 107.15 OF THE STANDARD SPECIFICATIONS.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AT THEIR EXPENSE.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS WITHOUT PRIOR AUTHORIZATION FROM THE CITY WATER DEPARTMENT. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

COST TO CONNECT EXISTING OR PROPOSED STORM SEWER TO EXISTING OR PROPOSED DRAINAGE STRUCTURE SHALL BE INCLUDED IN THE COST FOR STORM SEWER.

CONTRACTOR SHALL PROVIDE STRUCTURE LAYOUT INFORMATION TO THE ENGINEER PRIOR TO ORDERING ALL PROPOSED DRAINAGE STRUCTURES. THIS INFORMATION WILL BE DETERMINED IN THE FIELD WITH THE ENGINEER. COST TO DO THIS INVESTIGATIVE WORK SHALL IN INCLUDED IN THE COST FOR THE DRAINAGE STRUCTURE.

**GENERAL NOTES**

1. THE CITY OF NORTHLAKE AND ENGINEER SHALL BE NOTIFIED BY THE CONTRACTOR IN WRITING AT LEAST (3) FULL WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION.

2. THE CONTRACTOR SHALL PROVIDE ALL NECESSARY PROTECTION FOR EXISTING UTILITIES IN CONFORMANCE WITH THE AFFECTED UTILITY COMPANIES REQUIREMENTS AS MAY BE REQUIRED TO PERFORM THE WORK OF THIS CONTRACT.

3. BEFORE BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE LINE AND GRADES SHOWN ON THE CONTRACT DRAWINGS. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONTRACT DRAWINGS, THE CONTRACTOR SHALL IMMEDIATELY REPORT SAME TO THE ENGINEER PRIOR TO PERFORMING WORK. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF WORK AS REQUIRED.

4. THE CONTRACT DOCUMENTS ARE NOT INTENDED TO SHOW EVERY AND ALL DETAILS OF WORK TO BE PERFORMED OR EQUIPMENT TO BE SUPPLIED. THE INTENT OF THE CONTRACT DOCUMENTS IS TO ILLUSTRATE THE CONCEPTUAL DESIGN AND LAYOUT. THE CONTRACTOR SHALL BE KNOWLEDGEABLE AND REGULARLY ENGAGED IN THE TYPE OF WORK DESCRIBED BY THESE CONTRACT DOCUMENTS, AND SHALL BE RESPONSIBLE FOR UNDERSTANDING THEIR INTENT. ADDITIONAL WORK TO BE PERFORMED OR ITEM OF EQUIPMENT TO BE SUPPLIED WHICH IS NOT SPECIFICALLY CALLED FOR BY THESE CONTRACT DOCUMENTS BUT WHICH IS NECESSARY TO PROVIDE A COMPLETE AND SUCCESSFUL WORKING SYSTEM SHALL BE INCLUDED IN THE COST OF THE CONTRACT AT NO ADDITIONAL COST TO THE OWNER.

5. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ALL MATERIAL QUANTITIES AND APPRAISE HIMSELF/HERSELF OF ALL CONDITIONS. NO CLAIMS FOR ADDITIONAL COMPENSATION FOR INDIVIDUAL PAY ITEMS WILL BE RECOGNIZED DUE TO THE CONTRACTOR'S FAILURE TO UNDERSTAND THE SCOPE OF WORK.

6. THE WORK PERFORMED UNDER THIS CONTRACT SHALL IN NO WAY INTERFERE WITH THE NORMAL OPERATION OF ANY EXISTING UTILITY SERVICE. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ITEMS OF EQUIPMENT REQUIRED TO MAINTAIN SUCH NORMAL OPERATION AT NO ADDITIONAL COST TO THE OWNER. THE COST ASSOCIATED FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE CONTRACT.

7. CERTAIN INFORMATION SHOWN ON THESE DRAWINGS HAS BEEN OBTAINED FROM DRAWINGS OF RECORD. CONTRACTOR SHALL VERIFY SUCH INFORMATION PRIOR TO ACTUAL START OF WORK. WHERE DISCREPANCIES ARE DISCOVERED THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER. FAILURE BY THE CONTRACTOR TO IMMEDIATELY NOTIFY THE ENGINEER OF SUCH DISCREPANCIES SHALL RESULT IN THE CONTRACTOR BEARING THE FULL BURDEN OF ALL RISKS/COSTS ATTRIBUTED TO THE DISCOVERED DISCREPANCY.

8. SOIL EROSION PROTECTION SHALL BE IN ACCORDANCE WITH IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL. ALL DISTURBED AREAS (NOT IMPERVIOUS IN NATURE) SHALL BE FINE GRADED, TOP SOIL RESTORED (MIN 4 INCHES) AND SEED/MULCH APPLIED UNLESS OTHERWISE SPECIFIED ON THE PLANS.

9. ALL REMOVAL OR EXCAVATION ITEMS BEING DISPOSED OF AT AN UNCONTAMINATED SOIL FILL OPERATION OR CLEAN CONSTRUCTION AND DEMOLITION DEBRIS (CCDD) FILL SITE SHALL MEET THE REQUIREMENTS OF PUBLIC ACT 96-1416. ALL COSTS ASSOCIATED WITH MEETING THESE REQUIREMENTS SHALL BE INCLUDED IN THE UNIT PRICE COST FOR THE ASSOCIATED REMOVAL OR EXCAVATION ITEMS IN THE CONTRACT. THESE COSTS SHALL INCLUDE BUT ARE NOT LIMITED TO ALL REQUIRED TESTING, LAB ANALYSIS, CERTIFICATION BY A LICENSED PROFESSIONAL ENGINEER, AND STATE OR LOCAL TIPPING FEES.

**MISCELLANEOUS**

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION. THE COST TO PROVIDE ACCESS SHALL BE PAID FOR AND INCLUDED IN THE PAY ITEM TEMPORARY ACCESS (PRIVATE ENTRANCE).

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED TO REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

TREE ROOT PRUNING, COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, DRIVEWAY PAVEMENT REMOVAL, PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH, PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH AND 7 INCH, AND CLASS D PATCHES 6" ARE NOT SHOWN ON THE PLANS BUT WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLANNED QUANTITY.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

DETECTABLE WARNINGS FOR THE DISABLED SHALL BE INSTALLED AT ALL INTERSECTING STREETS, COMMERCIAL DRIVEWAYS, AND ALLEYS AS DIRECTED BY THE ENGINEER (SEE IDOT STD. 424001 INCLUDED IN THE SPECIFICATIONS).

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES. THIS WORK SHALL BE PERFORMED PER ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.

PER ARTICLE 107.20, MAILBOXES WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED, TEMPORARILY RELOCATED, AND REPLACED UPON COMPLETION OF THE PROPOSED IMPROVEMENTS AS DIRECTED BY THE ENGINEER.

PROPOSED CONCRETE CURB AND GUTTER SHALL BE TRANSITIONED TO EXISTING CURB AND GUTTER OVER A LENGTH OF 5 FEET. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.

ALL UNDERGROUND, DRIVEWAY, CONCRETE, AND LANDSCAPE RESTORATION WORK IS TO BE COMPLETED BEFORE THE SURFACE COURSE CAN BE INSTALLED.

THE HMA SURFACE COURSE USED TO REPLACE DRIVEWAYS WILL BE PAID FOR PER TON.

PROTECTIVE COAT FOR ALL CONCRETE SURFACES SHALL BE CONSIDERED INCLUDED IN THE COST OF THE RESPECTIVE PAY ITEM.

ANY DEFACED WORK SHALL BE CORRECTED OR REPLACED BY THE CONTRACTOR AT HIS SOLE EXPENSE PRIOR TO FINAL PAYMENT. THE CITY WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE PER CONDITIONS OF ARTICLE 107.30 OF THE STANDARD SPECIFICATIONS.

FOR HOT-MIX ASPHALT SURFACE REMOVAL, THE EXISTING ASPHALT SURFACE SHALL BE REMOVED TO THE DEPTH SPECIFIED. THE GRINDINGS SHALL BE REMOVED FROM THE SITE AND THE SURFACE MECHANICALLY BROOMED UNTIL THE SURFACE IS COMPLETELY FREE OF ANY LOOSE MATERIAL AND DEBRIS. GRINDING OF THE EXISTING CONCRETE BASE MAY BE REQUIRED TO ESTABLISH THE SPECIFIED DEPTH. CONCRETE GRINDING SHALL BE CONSIDERED INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL.

CLASS D PATCHES SHALL CONSIST OF REMOVAL AND REPLACEMENT OF EXISTING PAVEMENT AT LOCATIONS DIRECTED BY THE ENGINEER. AFTER MILLING IS COMPLETE, THE EXISTING PAVEMENT INCLUDING THE BASE AND HMA SURFACE SHALL BE REMOVED TO A DEPTH OF SIX (6) INCHES AND REPLACED WITH SIX (6) INCHES OF HMA MIX. THE SURFACE OF THE PATCH SHALL MEET THE SURFACE OF THE GROUND OF THE HOT-MIX ASPHALT SURFACE REMOVAL. ALL HOLES, SOFT PLACES AND OTHER DEFECTS IN THE SUBBASE OR SUBGRADE SHALL BE CORRECTED BY THE CONTRACTOR BY REMOVING THE UNSUITABLE MATERIAL, ADDING MORE HMA MIX AS SPECIFIED IN SECTION 406 OF THE STANDARD SPECIFICATIONS.

FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EXPOXY COATED, UNLESS NOTED ON THE PLAN.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR JOB SITE SAFETY AS WELL AS SUPERVISION / DIRECTION AND MEANS / METHODS OF CONSTRUCTION.

COST TO CONNECT EXISTING OR PROPOSED STORM SEWER TO EXISTING OR PROPOSED DRAINAGE STRUCTURE SHALL BE INCLUDED IN COST FOR STORM SEWER.

COST TO REMOVE EXISTING SPEED BUMP IS INCLUDED IN THE COST FOR HOT-MIX ASPHALT REMOVAL.

COST FOR CONSTRUCTION OF SPEED BUMP IS INCLUDED IN THE COST FOR HOT-MIX ASPHALT SURFACE COURSE. THE REPLACEMENT OF THE SPEED BUMP SHALL INCLUDE WORK TO GRIND THE PAVEMENT AFTER THE SURFACE COURSE HAS BEEN INSTALLED FOR A BUTT-JOINT. CONSTRUCTION OF THE SPEED BUMP WILL REQUIRE HANDWORK. THE SPEED BUMP SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAILS AS SHOWN IN THE PLANS.

**HIGHWAY STANDARDS**

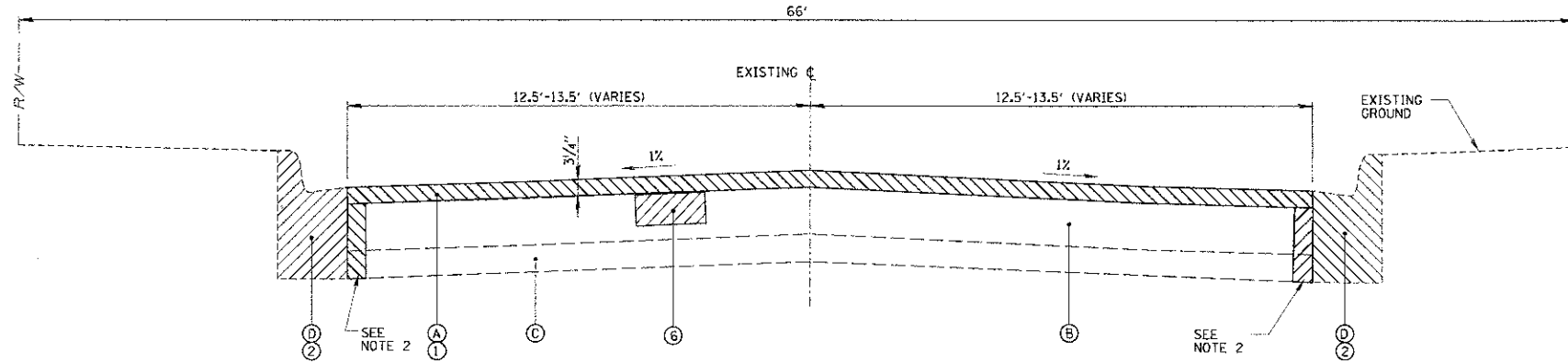
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALK
424006-01	DIAGONAL CURB RAMPS FOR SIDEWALKS
424021-01	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602301-03	INLET - TYPE A
602401-03	MANHOLE - TYPE A
604001-03	FRAME AND LIDS TYPE 1
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-06	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
780001-03	TYPICAL PAVEMENT MARKINGS
B.L.R. 17-4	TRAFFIC CONTROL DEVICES - DAY LABOR CONSTRUCTION
B.L.R. 18-5	TRAFFIC CONTROL DEVICES - DAY LABOR MAINTENANCE

**SUMMARY OF QUANTITIES**

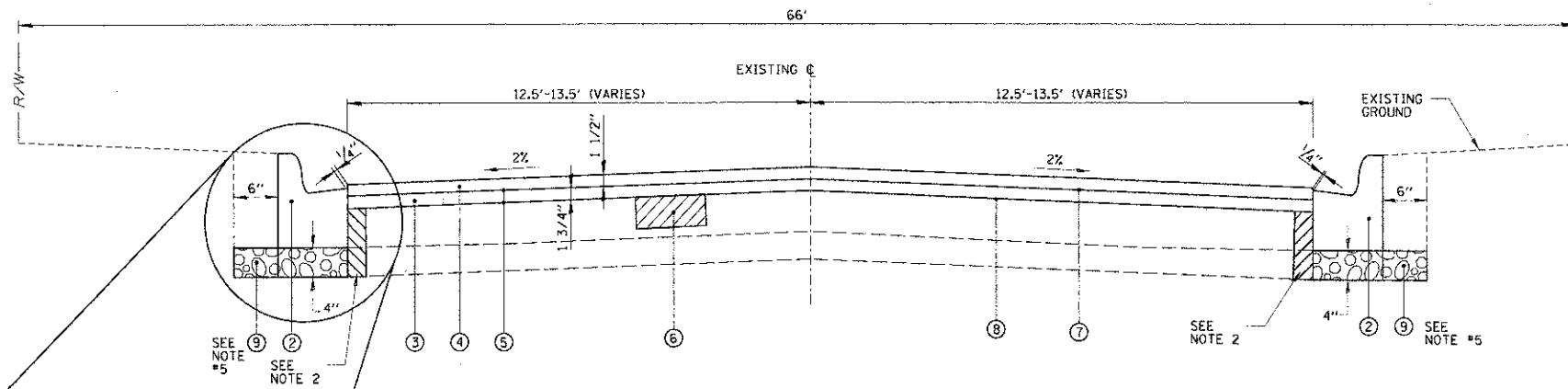
LOCATION OF WORK			HIRSCH STREET
FUNDING SOURCE			80% FED. 20% LOCAL
SUMMARY OF QUANTITIES			STP - LAFO
ITEM #	ITEM	UNIT	CONSTRUCTION TYPE CODE
			OOO5
20101200	TREE ROOT PRUNING	EACH	5
20900150	TRENCH BACKFILL	CU YD	15
21101615	TOPSOIL, FURNISH AND PLACE 4"	SQ YD	750
25200110	SODDING, SALT TOLERANT	SQ YD	750
28000510	INLET FILTERS	EACH	29
31101100	SUB-BASE GRANULAR MATERIAL, TYPE B	CU YD	30
35501287	HOT-MIX ASPHALT BASE COURSE, 2 1/4"	SQ YD	150
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	2250
40600300	AGGREGATE (PRIME COAT)	TON	29
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	20
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	1200
40600895	CONSTRUCTING TEST STRIP	EACH	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	215
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1045
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	25
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2750
42400400	PORTLAND CEMENT CONCRETE SIDEWALK 7 INCH	SQ FT	250
42400800	DETECTABLE WARNINGS	SQ FT	380
44000100	PAVEMENT REMOVAL	SQ YD	25
44000162	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4"	SQ YD	11450
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	175
44000600	SIDEWALK REMOVAL	SQ FT	3000
44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	425
44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	425
44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	425
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	425
44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	11450
550A0040	STORM SEWERS, CLASS A, TYPE 1 10"	FOOT	45
56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	2
58100200	WATERPROOFING MEMBRANE SYSTEM	SQ YD	200
58300100	PORTLAND CEMENT MORTAR FAIRING COURSE	FOOT	600
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	2
60219400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1

LOCATION OF WORK			HIRSCH STREET
FUNDING SOURCE			80% FED. 20% LOCAL
SUMMARY OF QUANTITIES			STP - LAFO
ITEM #	ITEM	UNIT	CONSTRUCTION TYPE CODE
			OOO5
80234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	1
80250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	18
60258200	MANHOLES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1
60269600	VALVE BOXES TO BE ADJUSTED	EACH	8
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1
60500040	REMOVING MANHOLES	EACH	1
60500050	REMOVING CATCH BASINS	EACH	2
87100100	MOBILIZATION	L SUM	1
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1075
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	40
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	210
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	20
X7010216	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1
XX004096	REMOVE AND RE-INSTALL CONCRETE FLARED END SECTIONS	EACH	1
Z0013798	CONSTRUCTION LAYOUT	L SUM	1
Z0018100	DRAINAGE STRUCTURE ADJUSTMENT (SPECIAL)	EACH	1
Z0019400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	9
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1750
* Z0012752	CONCRETE STRUCTURE REPAIR	CU FT	100
* Z0032700	KEYWAY REPAIR	FOOT	600

\* INDICATES SPECIALTY ITEM



EXISTING TYPICAL SECTION  
STA. 10+28 TO STA. 47+06  
NOT TO SCALE



PROPOSED TYPICAL SECTION  
STA. 10+28 TO STA. 47+06  
NOT TO SCALE

LEGEND

- (A) EXISTING ASPHALT PAVEMENT (+/- 3.5" TO 7.5")
- (B) EXISTING HMA OR P.C.C. BASE (+/- 0" TO 7")
- (C) EXISTING AGGREGATE BASE (+/- 0" TO 15")
- (D) EXISTING COMBINATION CURB AND GUTTER (TYPE VARIES - TYPE M-4, 12)
- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4"
- (2) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (3) LEVELING BINDER (MACHINE METHOD), N50 - 1 3/4" MIN.
- (4) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (5) BITUMINOUS MATERIALS (PRIME COAT)
- (6) CLASS D PATCHES, TYPE VARIES, 6"
- (7) AREA REFLECTIVE CRACK CONTROL TREATMENT
- (8) AGGREGATE (PRIME COAT)
- (9) SUBBASE GRANULAR MATERIAL, TYPE B

NOTES

1. CONTRACTOR SHALL MILL PAVEMENT BEFORE PATCHING.
2. ANY PAVEMENT REMOVAL REQUIRED FOR CONSTRUCTION OF COMBINATION CONCRETE CURB AND GUTTER SHALL BE REPLACED WITH P.C.C. (CLASS S1) AND SHALL BE VIBRATED IN PLACE. COST FOR PAVEMENT REMOVAL AND P.C.C. (CLASS S1) SHALL BE INCLUDED IN UNIT PRICE FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
3. SEE CURB AND GUTTER REMOVAL AND REPLACEMENT DETAIL REGARDING PAYMENT FOR LANDSCAPE RESTORATION (SHEET 13).
4. THE EXISTING PAVEMENT CROSS SLOPE IS APPROXIMATELY 1%. ADDITIONAL QUANTITY OF LEVELING BINDER HAS BEEN INCLUDED IN THE CONTRACT TO RAISE THE ROADWAY CROWN TO ESTABLISH A 2% CROSS SLOPE. GRINDING AND PAVING OPERATIONS SHALL BE ADJUSTED ACCORDINGLY.
5. SUBBASE GRANULAR MATERIAL, TYPE B INSTALLED BENEATH CONCRETE CURB AND GUTTER SHALL BE INSTALLED PER THE DETAIL AND SHALL BE INCLUDED IN COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (SEE DETAIL SHEET 13).
6. COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE PERFORMED AT LOCATIONS AS DIRECTED BY THE ENGINEER.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE ITEM	AIR VOIDS Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 1 1/2"	4% @ 50 GYR
LEVELING BINDER (MACHINE METHOD), N50, 1 1/2"	4% @ 50 GYR
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 3" (2 LIFTS)	4% @ 50 GYR
PATCHING	
CLASS D PATCHES, 6" (HMA BINDER IL-19 MM) NOTE: SAW CUT PATCHES PRIOR TO REMOVAL	4% @ 70 GYR

- NOTES:
1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/50 YD<sup>3</sup>IN.
  2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

**SCHEDULES OF QUANTITIES**

**COMBINATION CONCRETE CURB AND GUTTER  
REMOVAL AND REPLACEMENT**

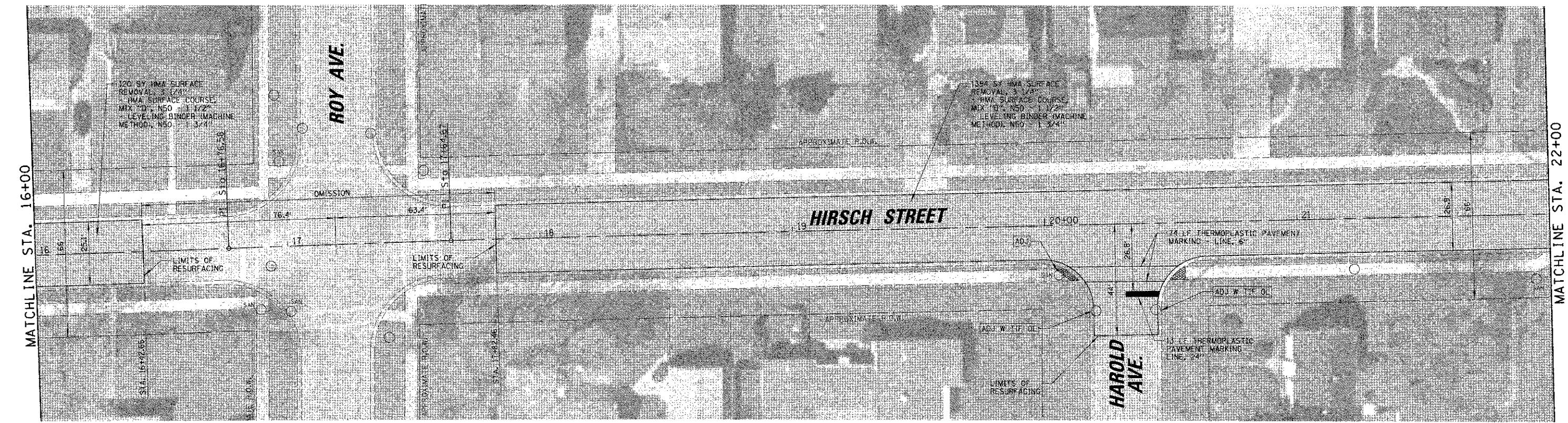
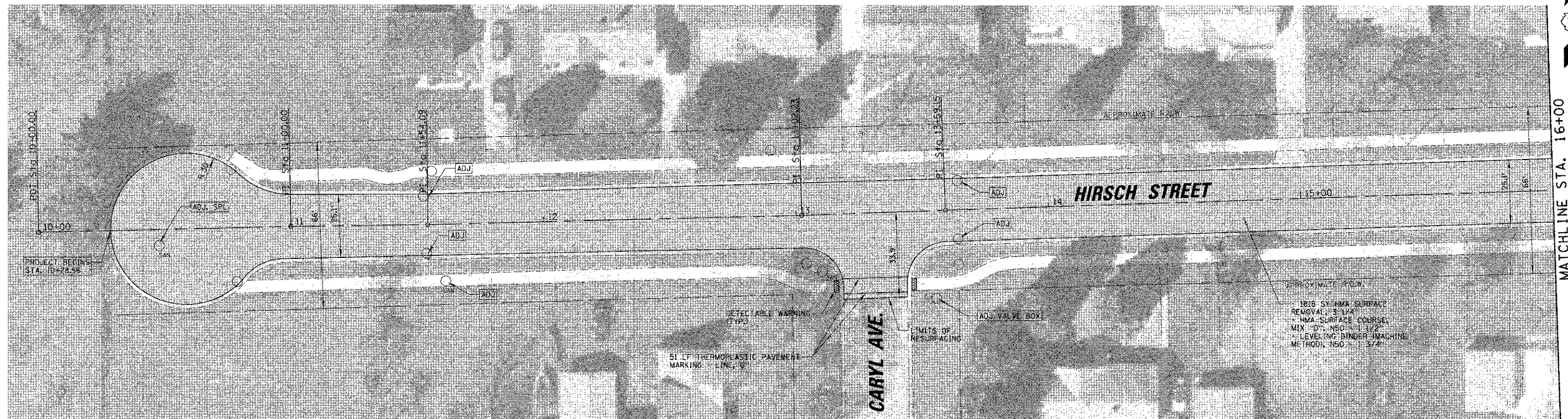
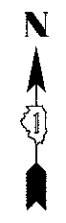
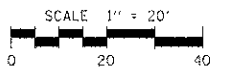
FROM STA.	TO STA.	LOR R	LENGTH (FT)
10+63	10+78	L	15
11+47	11+98	L	51
13+57	14+00	L	43
16+30	17+00	L	103
17+34	17+70	L	84
18+35	18+67	L	32
19+36	19+77	L	41
21+47	21+54	L	7
21+68	23+34	L	202
24+91	25+08	L	10
28+35	28+58	L	23
31+35	31+60	L	25
32+37	32+51	L	14
34+87	35+05	L	18
35+50	35+66	L	16
36+32	36+55	L	27
36+82	37+12	L	43
38+35	38+53	L	18
39+68	39+89	L	41
40+20	40+30	L	10
41+63	41+86	L	23
43+10	43+23	L	22
43+48	43+59	L	11
44+70	45+04	L	34
45+17	45+59	L	42
46+40	46+55	L	22
11+45	11+57	R	12
13+14	13+14	R	18
13+42	13+72	R	47
14+82	14+92	R	10
15+87	16+02	R	15
16+32	17+05	R	110
17+34	17+72	R	61
20+00	20+20	R	35
20+45	20+57	R	36
28+40	28+49	R	9
30+38	30+43	R	5
31+00	31+14	R	14
31+42	31+31	R	17
32+40	32+52	R	12
32+65	32+96	R	31
34+82	35+48	R	66
36+32	36+53	R	39
36+78	37+00	R	43
39+45	39+90	R	65
40+15	40+38	R	38
42+25	42+39	R	14
43+00	43+23	R	47
45+12	45+22	R	10
		TOTAL =	1731

NOTE: LIMITS OF CURB REMOVAL AND REPLACEMENT ARE APPROXIMATE AND TO BE MARKED IN THE FIELD BY THE ENGINEER. LIMITS ARE SUBJECT TO CHANGE.

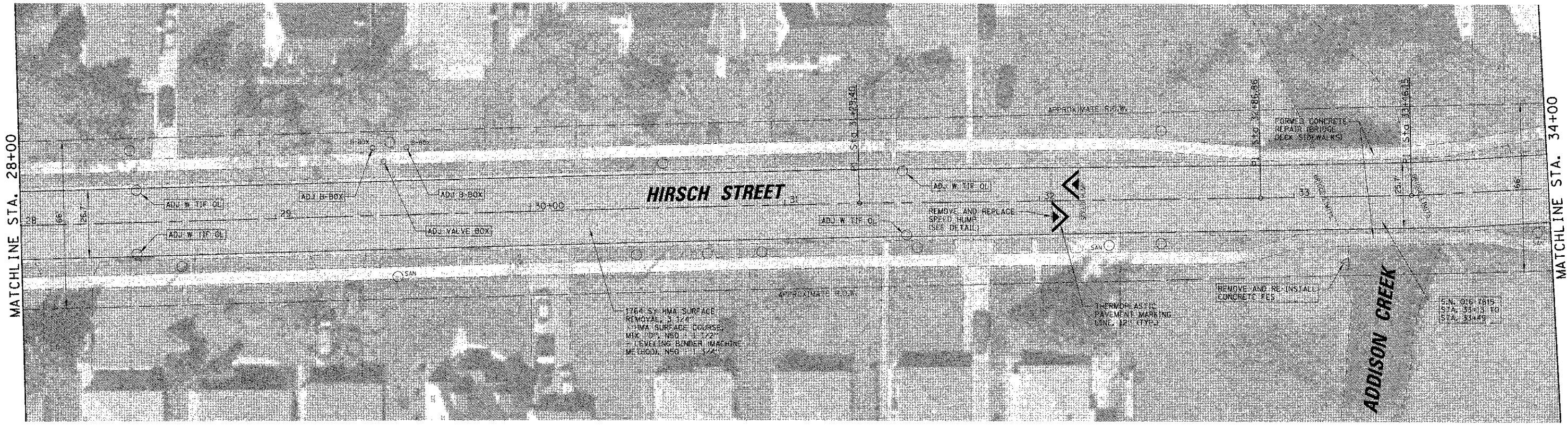
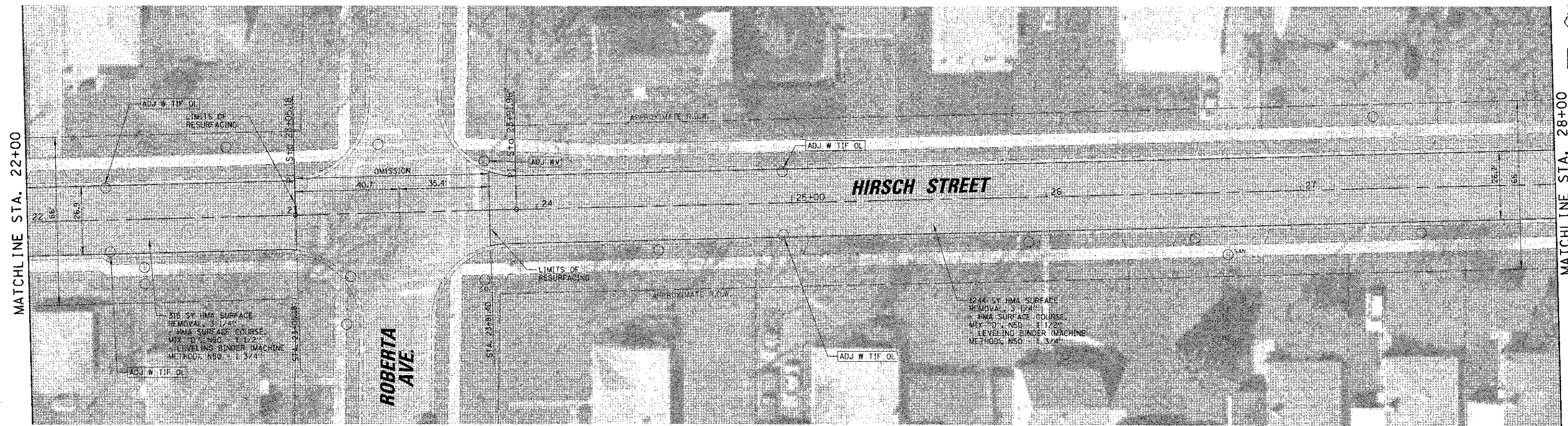
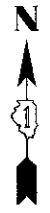
**DRIVEWAY REMOVAL AND REPLACEMENT**

AT STA.	LOR R	TYPE	AREA (SY)
11+85	L	BIT	13.9
13+85	L	BIT	8.3
19+50	L	PCC	9.4
21+80	L	BIT	9.4
22+05	L	BIT	9.4
28+55	L	BIT	8.3
35+00	L	BIT	8.3
44+85	L	BIT	8
45+45	L	PCC	6.2
24+90	R	BIT	11.1
34+90	R	BIT	7.8
35+40	R	BIT	9.4
42+30	R	BIT	33.3
		TOTAL =	142.8

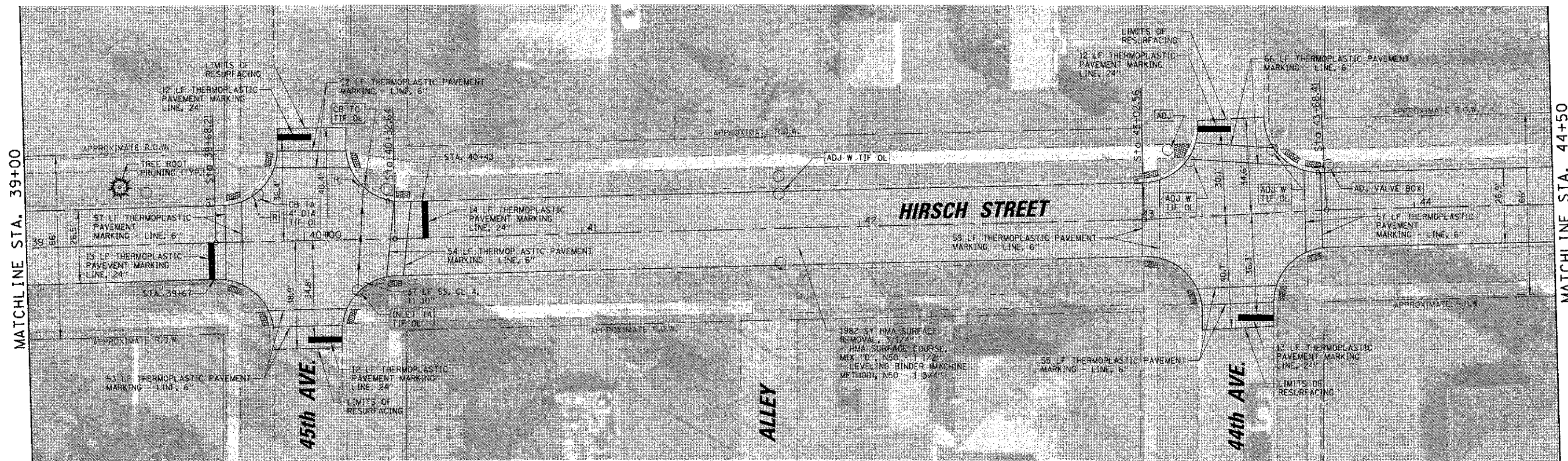
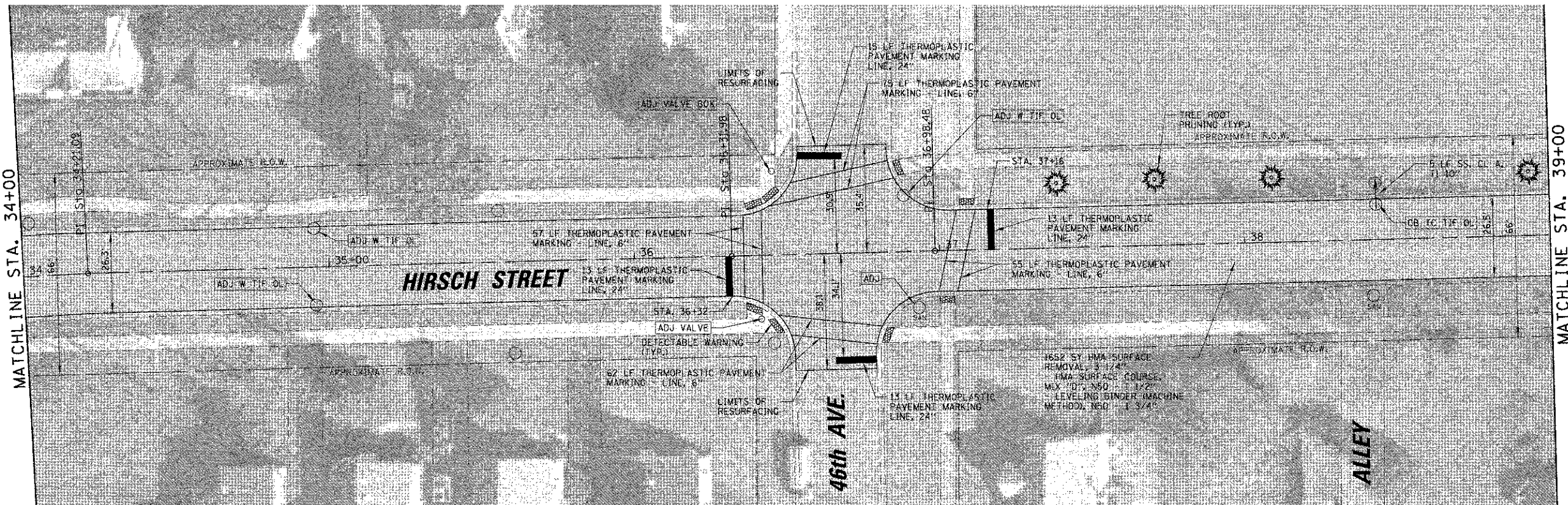
NOTE: AREAS FOR DRIVEWAY REMOVAL AND REPLACEMENT ARE APPROXIMATE AND TO BE MARKED IN THE FIELD BY THE ENGINEER. LIMITS ARE SUBJECT TO CHANGE.



FILE NAME: \\NORTHLAKE\94823202\2\N\civil\p\hirsch.dwg USER NAME: apufundt DRAWN: EDT CHECKED: MEW PLOT SCALE: 20' PLOT DATE: 3/22/2013	DESIGNED: AMP DATE: 03/25/13	REVISED: - REVISED: - REVISED: - REVISED: -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLANS</b> <b>HIRSCH STREET</b>		F.A.I. RTE.: 1033	SECTION: 12-00079-06-RS	COUNTY: COOK	TOTAL SHEET NO.: 17 SHEETS: 6
				SCALE: _____	SHEET NO. _____ OF _____ SHEETS	STA. _____ TO STA. _____	ILLINOIS FED. AID PROJECT CONTRACT NO. 63831		

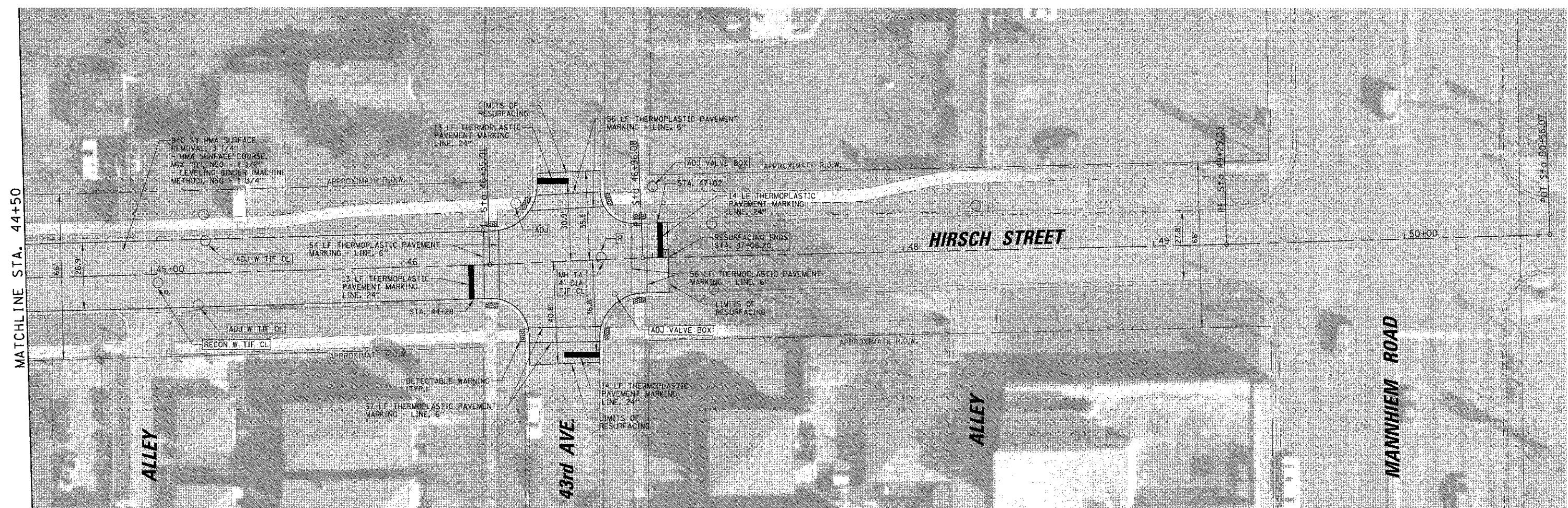
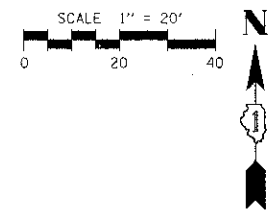


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	DRAWN - EDT	REVISED -			CONTRACT NO. 63831		ILLINOIS, FED. AID PROJECT		
	CHECKED - MEW	REVISED -			SCALE: SHEET NO. OF SHEETS STA. TO STA.				
	DATE - 03/25/13	REVISED -							

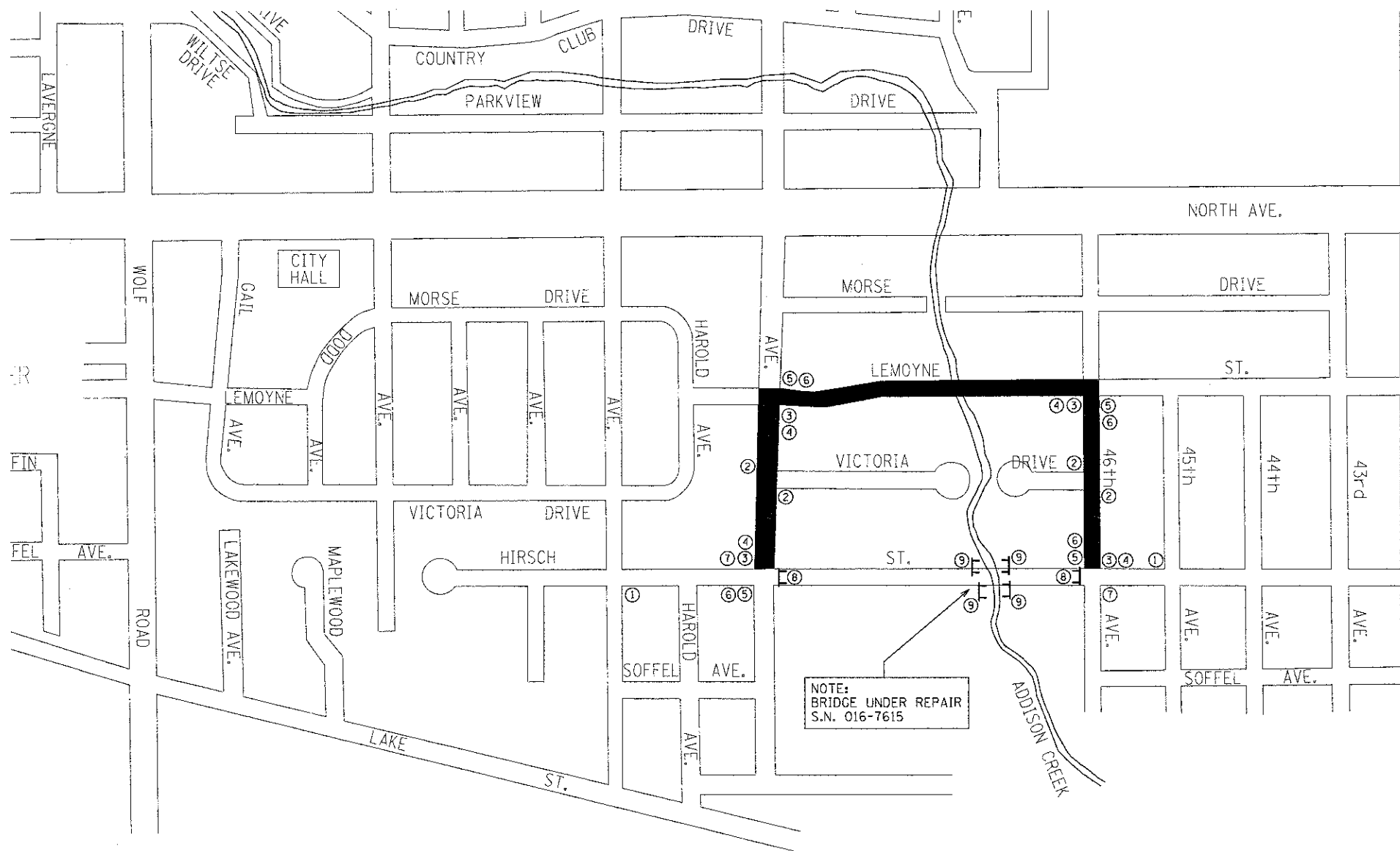


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	PLT: SCALE = 20'	DRAWN: EDT	REVISED:			1033	12-00079-00-RS	COOK	17	8
	PLT DATE = 3/28/2013	CHECKED: MEW	REVISED:			CONTRACT NO. 63831				
		DATE: 03/25/13	REVISED:			ILLINOIS FED. AID PROJECT				








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NONORTH: AKF\940832\0121\00\va\p1a4	h.00121ent	DRAWN - EDT	REVISED -			1033	12-00079-00-R5	COOK	17	9	
PL07 SCALE = 2"		CHECKED - MEW	REVISED -			CONTRACT NO. 63831					
PL07 DATE = 3/20/2013		DATE - 03/25/13	REVISED -			ILLINOIS FED. AID PROJECT					
						SCALE:	SHEET NO.	OF	SHEETS	STA. TO STA.	












**S.N. 016-7615: LOADING HS-15**

**LEGEND**

-  OTHER DETOUR SIGNS. NUMBER DENOTES TYPE.
-  TYPE III BARRICADE WITH AMBER FLASHING LIGHTS.
-  DETOUR ROUTE

**SCHEDULE OF SIGNS**

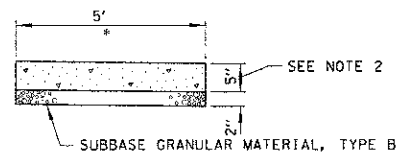
SIGN NO.	SIGN TYPE	QUANTITY
1	 W20-2 48X48	2
2	 M4-9 30X21	4
3	 M4-9 R 30X21	4
4	 M4-9 R 30X21	4
5	 M4-9 L 30X21	4
6	 M4-9 L 30X21	4
7	 M4-8a 24X18	2
8	 R11-4 60X30	2
9	 R11-2 48X30	4

**DETOUR SIGNAGE**

1. CONTRACTOR SHALL FURNISH AND INSTALL ALL TRAFFIC CONTROL DETOUR SIGNAGE IN ACCORDANCE WITH THE PLAN. IF THE SIGNAGE IS NOT ADEQUATE, THE ENGINEER MAY REQUIRE ADDITIONAL DETOUR SIGNAGE TO BE POSTED BY THE CONTRACTOR TO MAINTAIN TRAFFIC FLOW. THIS WORK WILL BE PAID FOR UNDER THE TRAFFIC CONTROL AND PROTECTION PAY ITEMS.

**CONSTRUCTION SEQUENCE FOR WORK OVER BRIDGE**

- CONTRACTOR SHALL SUBMIT TO THE ENGINEER FOR APPROVAL THE STRUCTURAL ASSESSMENT REPORTS FOR CONTRACTOR'S MEANS AND METHODS (GBSP 67) FOR WORK ON THE BRIDGE. WEIGHT RESTRICTIONS MAY LIMIT MEANS AND METHODS, INCLUDING BUT NOT LIMITED TO THE TYPE OF EQUIPMENT USED, MOBILIZATION/DELIVERY OF EQUIPMENT, AND TRUCK ROUTE. NO ADDITIONAL PAYMENT WILL BE MADE TO ACCOMMODATE THESE RESTRICTIONS.
- SET UP TRAFFIC CONTROL AND DETOUR ROUTE.
- GRIND EXISTING ASPHALT PAVEMENT OVER THE TOP OF THE DECK BEAMS. THE TYPE OF EQUIPMENT USED TO REMOVE THE ASPHALT MAY BE LIMITED DUE TO WEIGHT LIMITS. PER ENGINEER'S DIRECTION, USE OF A BOBCAT OR HANDWORK MAY BE REQUIRED TO REMOVE THE EXISTING ASPHALT. NO JACKHAMMERING WILL BE PERMITTED ON THE BRIDGE DECK. USE OF RADIANT OR DIRECT HEAT WILL NOT BE PERMITTED. THIS WORK WILL BE PAID FOR AS HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4" AND NO ADDITIONAL COMPENSATION WILL BE PROVIDED FOR WORK REQUIRED TO REMOVE THE FULL DEPTH OF THE EXISTING ASPHALT.  
  
ANY DAMAGE DONE TO THE CONCRETE BEAMS OR BRIDGE STRUCTURE AS A RESULT OF THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND AT NO ADDITIONAL COST TO THE CITY.
- EXPOSE AND SWEEP THE TOP OF THE DECK BEAMS CLEAN FOR INSPECTION.
- ENGINEER WILL INSPECT THE TOP OF THE CONCRETE BEAMS TO DETERMINE WHETHER ANY FURTHER REHABILITATION WORK IS REQUIRED PRIOR TO PLACING THE WATERPROOFING MEMBRANE. NO ADDITIONAL COMPENSATION OR CONTRACT TIME WILL BE PERMITTED TO THE CONTRACTOR WHILE THE DECK BEAMS ARE INSPECTED AND REHABILITATED.
- REMOVAL OF EXISTING GROUT, PREPARATION, AND INSTALLATION OF REPAIR MATERIAL SHALL BE INCLUDED IN THE COST OF KEYWAY REPAIR. THE EXTENT OF REMOVAL OF EXISTING GROUT SHALL BE AS DIRECTED BY THE ENGINEER.
- PLACE WATERPROOFING MEMBRANE.
- PLACE LEVELING BINDER - 1 3/4". THICKNESS OF LEVELING BINDER MAY BE ADJUSTED (3/4" MIN.) TO MAKE SURE PROPER DRAINAGE IS MAINTAINED. DRAINAGE PATTERN SHALL MATCH PRE-CONSTRUCTION CONDITIONS.
- INSTALL SURFACE COURSE - 1 1/2".

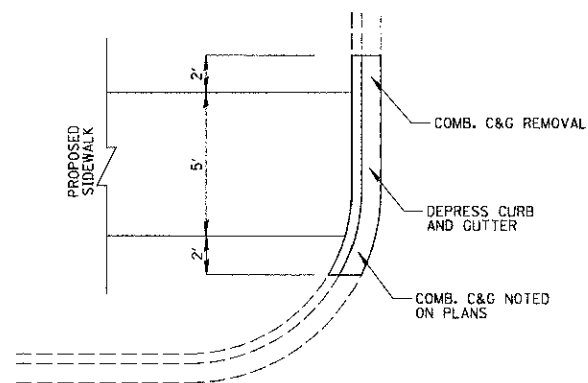


CROSS SLOPE 2% OR AS SHOWN ON CROSS SECTIONS

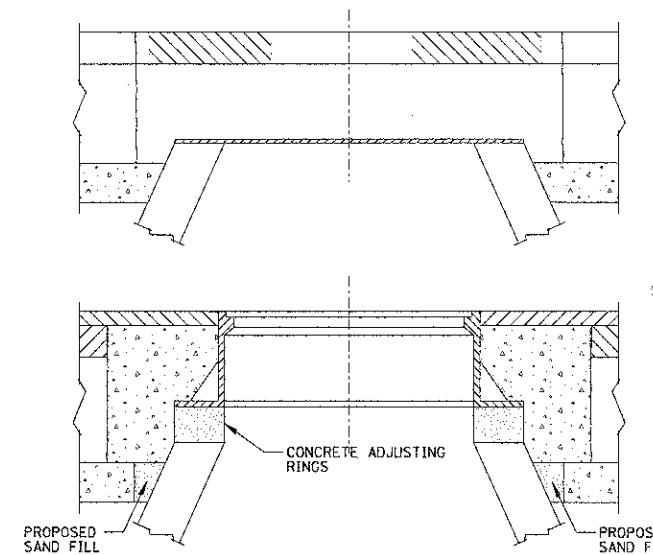
**NOTES:**

1. ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK TO MEET CURRENT ADA REQUIREMENTS SHALL BE INCLUDED IN THE COST FOR P.C.C. SIDEWALK.
2. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.
3. ALL LANDSCAPE RESTORATION (TOPSOIL, SEEDING, CLASS I, MULCH METHOD 3) SHALL BE INCLUDED IN COST FOR P.C.C. SIDEWALK.

**P.C.C. SIDEWALK DETAIL**



**PROPOSED DEPRESSED CURB & GUTTER**  
EXISTING CURB & GUTTER NOT DEPRESSED  
NOT TO SCALE



**CONSTRUCTION PROCEDURES**

**STAGE I (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12" OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36" DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1" THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

**STAGE II (AFTER PAVEMENT MILLING)**

- A) REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602 AND 603 OF THE STANDARD SPECIFICATIONS.

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

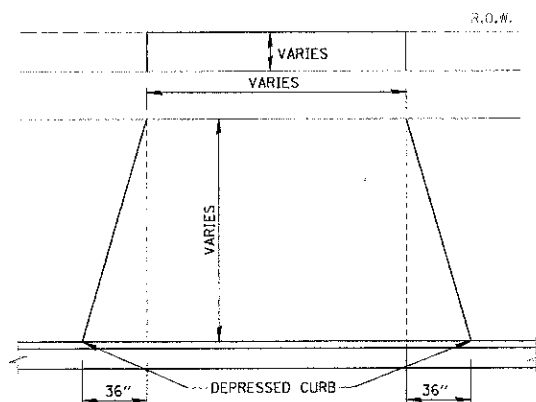
**BASIS OF PAYMENT**

DRAINAGE STRUCTURE ADJUSTMENT (SPECIAL).

**NOTES**

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

**DRAINAGE STRUCTURE ADJUSTMENT (SPECIAL)**



**DETAIL OF DRIVEWAY**

THIS TYPICAL DRIVE LAYOUT IS FOR BOTH CONCRETE AND ASPHALT DRIVES

- P.C.C. DRIVE - 7" P.C.C. DRIVEWAY PAVEMENT  
2" SUB-BASE GRANULAR MATERIAL, TYPE B
- ASPHALT DRIVE - 2" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50  
2 1/4" HOT-MIX ASPHALT BASE COURSE  
2" SUB-BASE GRANULAR MATERIAL, TYPE B

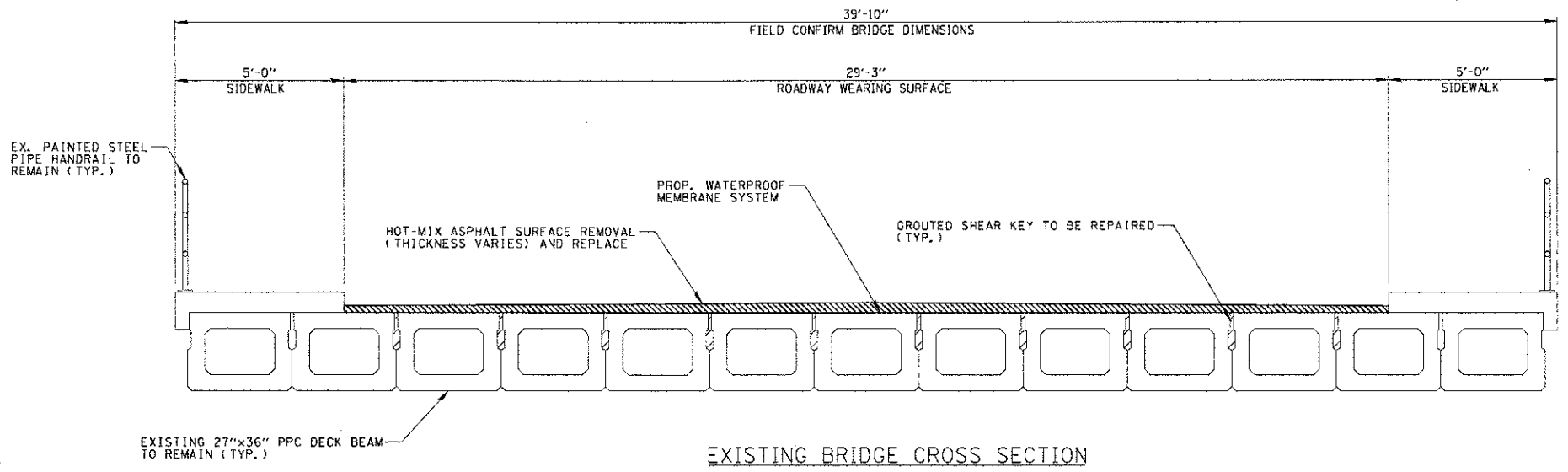
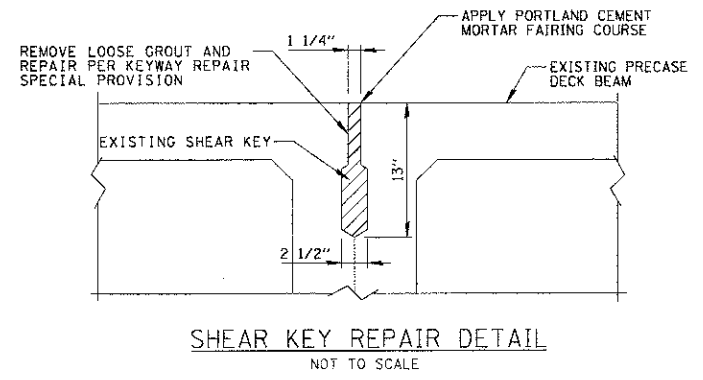
**NOTES:**

1. PROPOSED SIDEWALK THROUGH DRIVEWAY SHALL BE 7 INCHES OF PORTLAND CEMENT CONCRETE ON 2 INCH SUB-BASE GRANULAR MATERIAL, TYPE B.

2. THE HMA SURFACE COURSE USED TO REPLACE DRIVEWAYS WILL BE PAID FOR PER TON. THE NUMBER OF DRIVEWAYS TO BE REPLACED WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

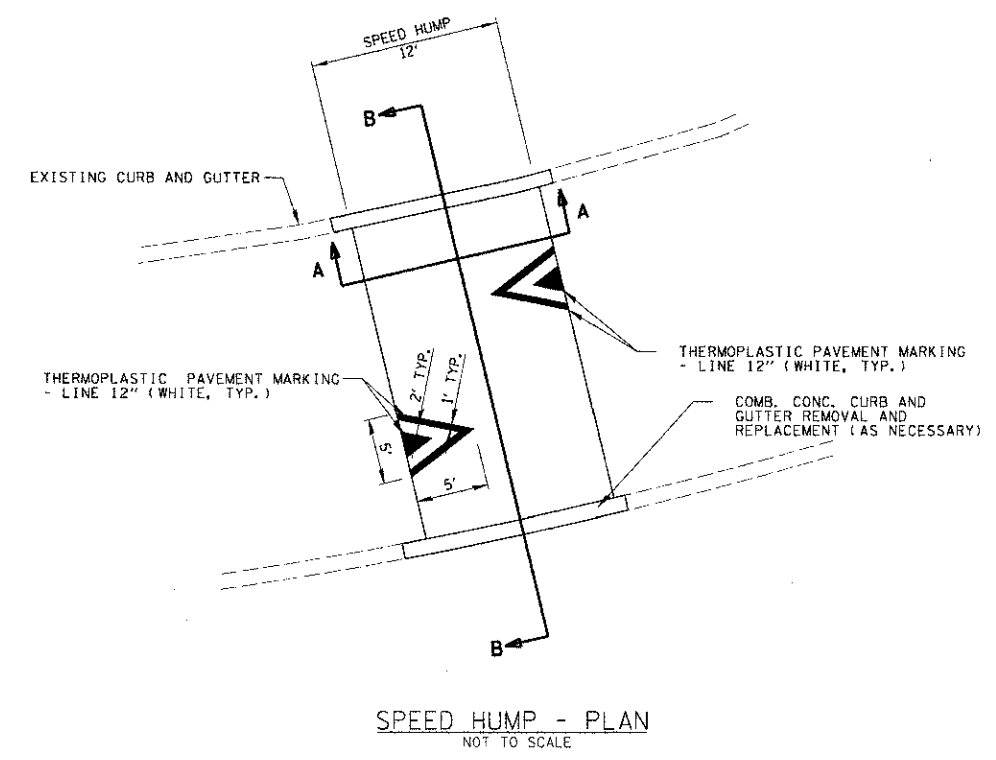
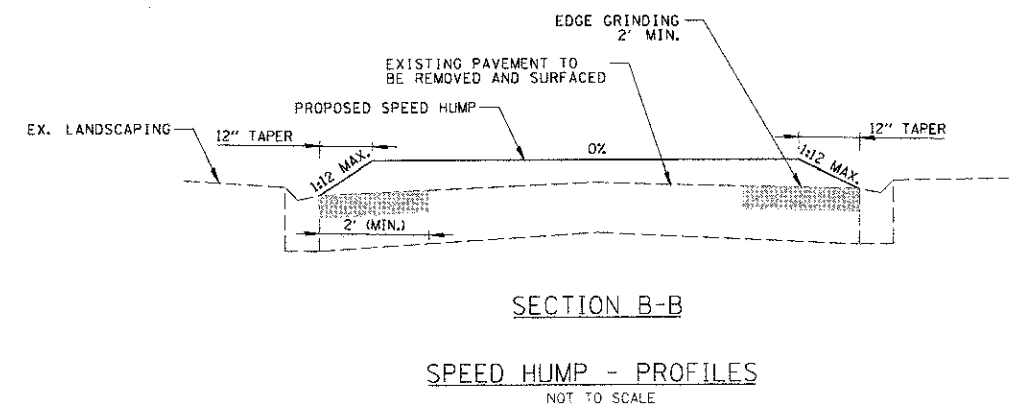
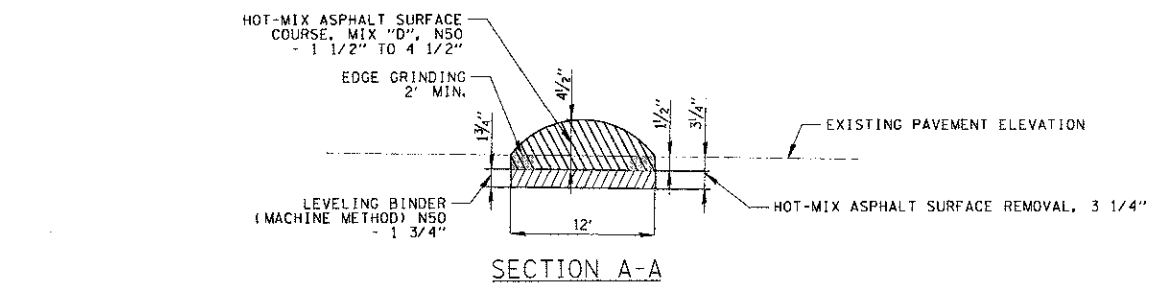
3. ALL LANDSCAPE RESTORATION (TOPSOIL, SEEDING, CLASS I, MULCH METHOD 3) SHALL BE INCLUDED IN COST FOR DRIVEWAY REPLACEMENT.

FILE NAME =	USPR NAME = agufund-	DESIGNED - AMP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CONSTRUCTION DETAILS</b>	F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISIONS	DATE	DATE	DATE			1033	12-00079-00-RS	COOK	17	11
SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT							
			CONTRACT NO. 63831							



S.N. 016-7615  
STA. 33+13 TO STA. 33+49

NOTE: THE ILLINOIS DEPARTMENT OF TRANSPORTATION IS NOT THE OWNER OF RECORD FOR THIS BRIDGE. THOSE SEEKING HISTORIC AS-BUILT OR OTHER RECORD PLANS AND DOCUMENTS MUST CONTACT THE OWNER OF RECORD TO MAKE ARRANGEMENTS FOR ACCESS TO THIS INFORMATION.



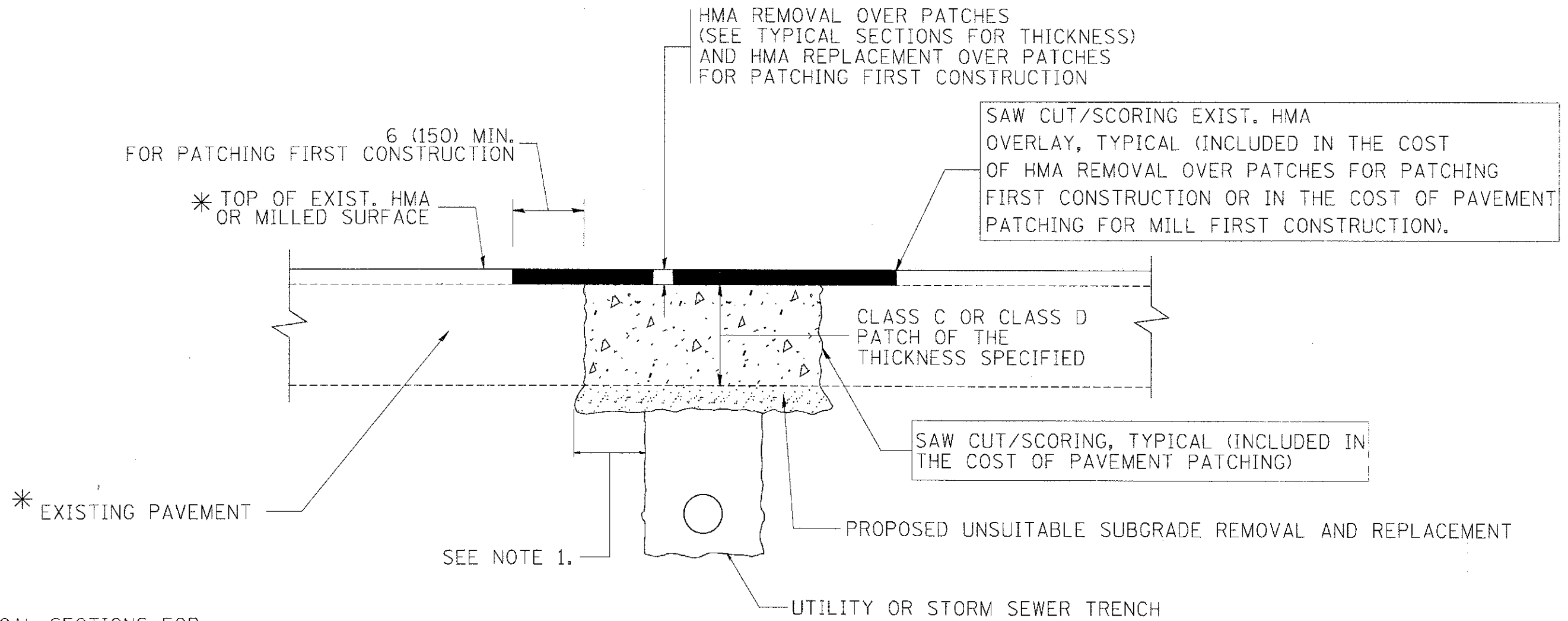
FILE NAME:	USER NAME:	DESIGNED - AMP	REVISED -
PROJECT NAME:	PROJECT NO.:	DRAWN - EDT	REVISED -
PROJECT SCALE:	PROJECT DATE:	CHECKED - MEW	REVISED -
		DATE - 03/25/13	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CONSTRUCTION DETAILS</b>			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1033	12-00079-00-RS	COOK	17	12
CONTRACT NO. 63831				
ILLINOIS FED. AID PROJECT				





\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

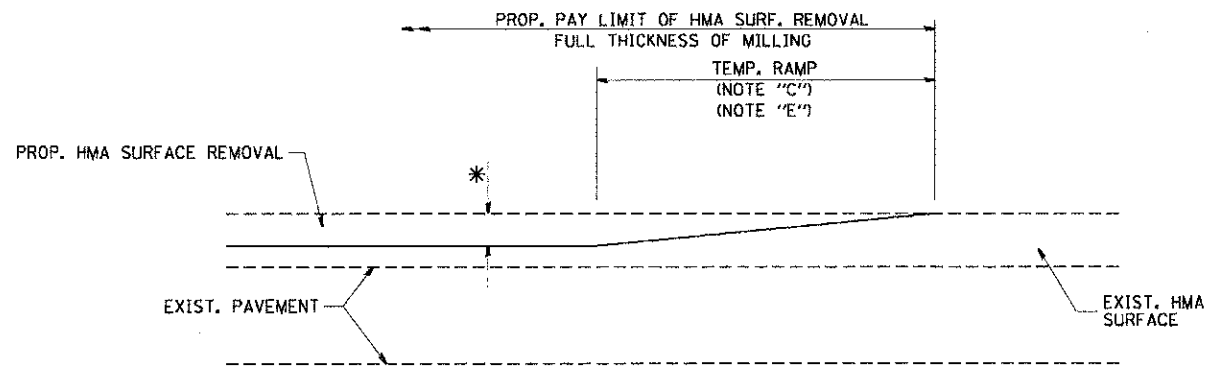
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

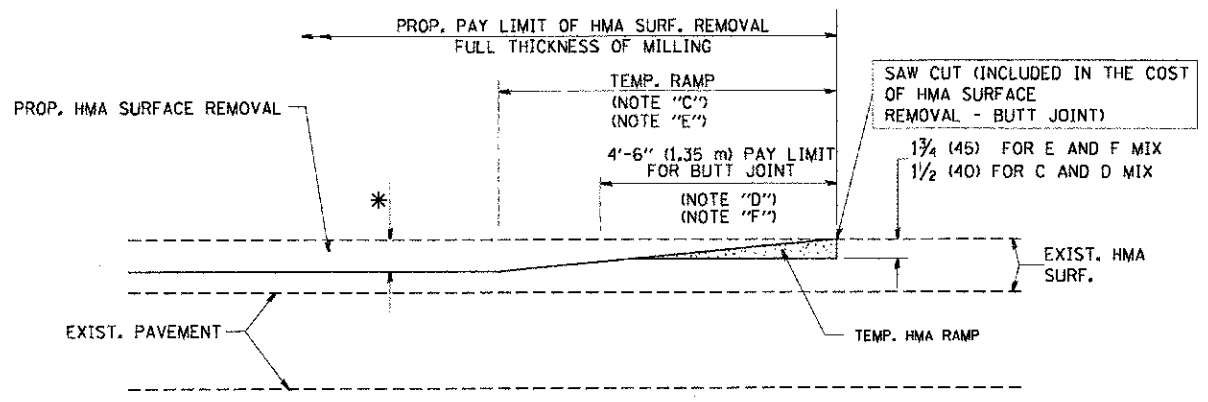
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\distssd22x34\bd22.dgn	USER NAME = beurd1	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	12-00079-00-RS	COOK	17	14	
		PLOT SCALE = 50.000' / IN.	REVISED - R. BORO 09-04-07						<b>BD400-04 (BD-22)</b>				
		PLOT DATE = 10/27/2000	REVISED - K. ENG 10-27-08						FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



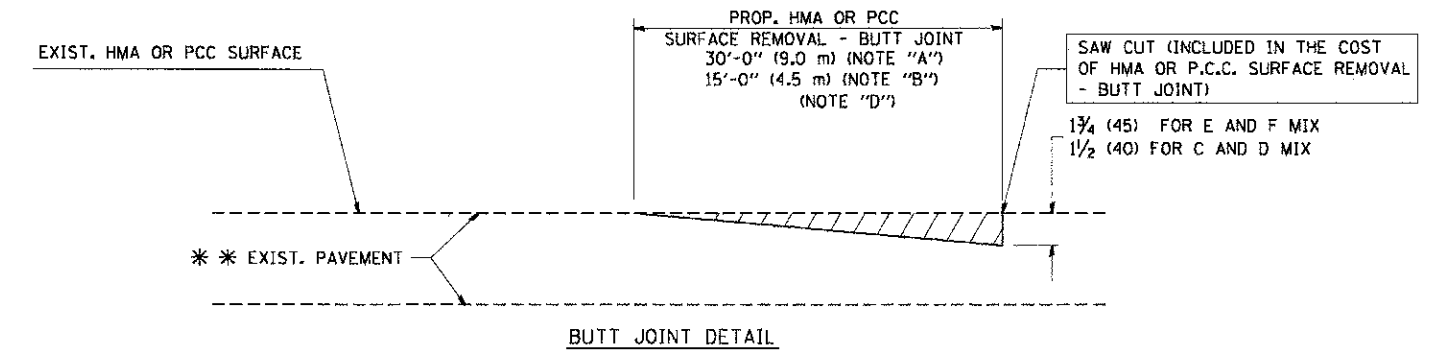
**MILLED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

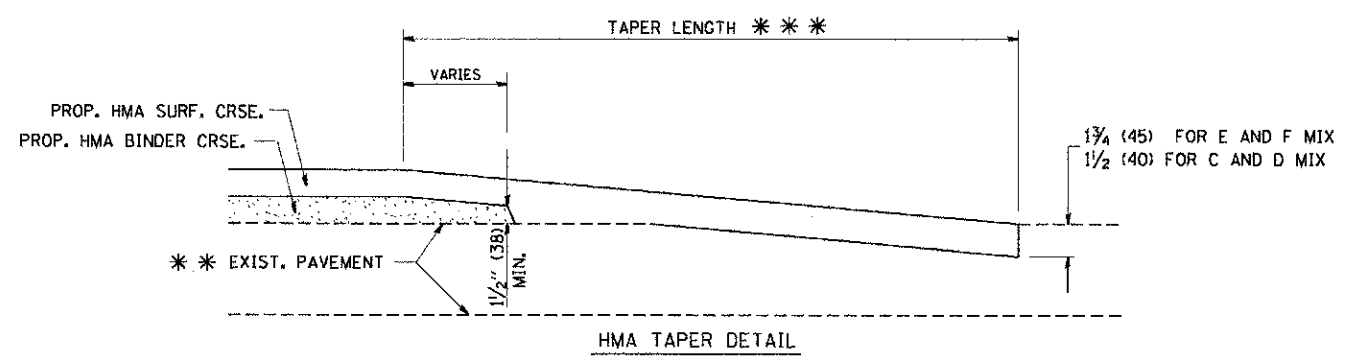


**HMA CONSTRUCTED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**  
**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

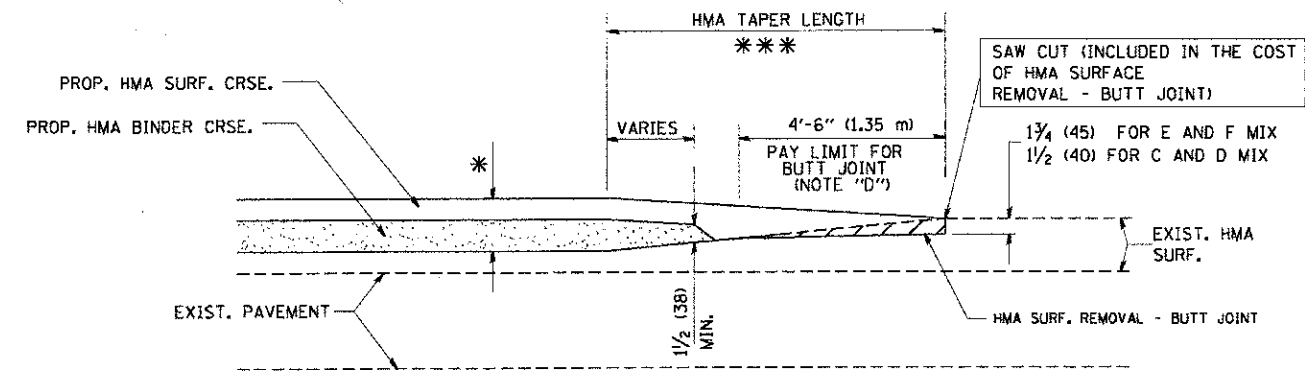
\* \* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**  
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**BUTT JOINT AND HMA TAPER**

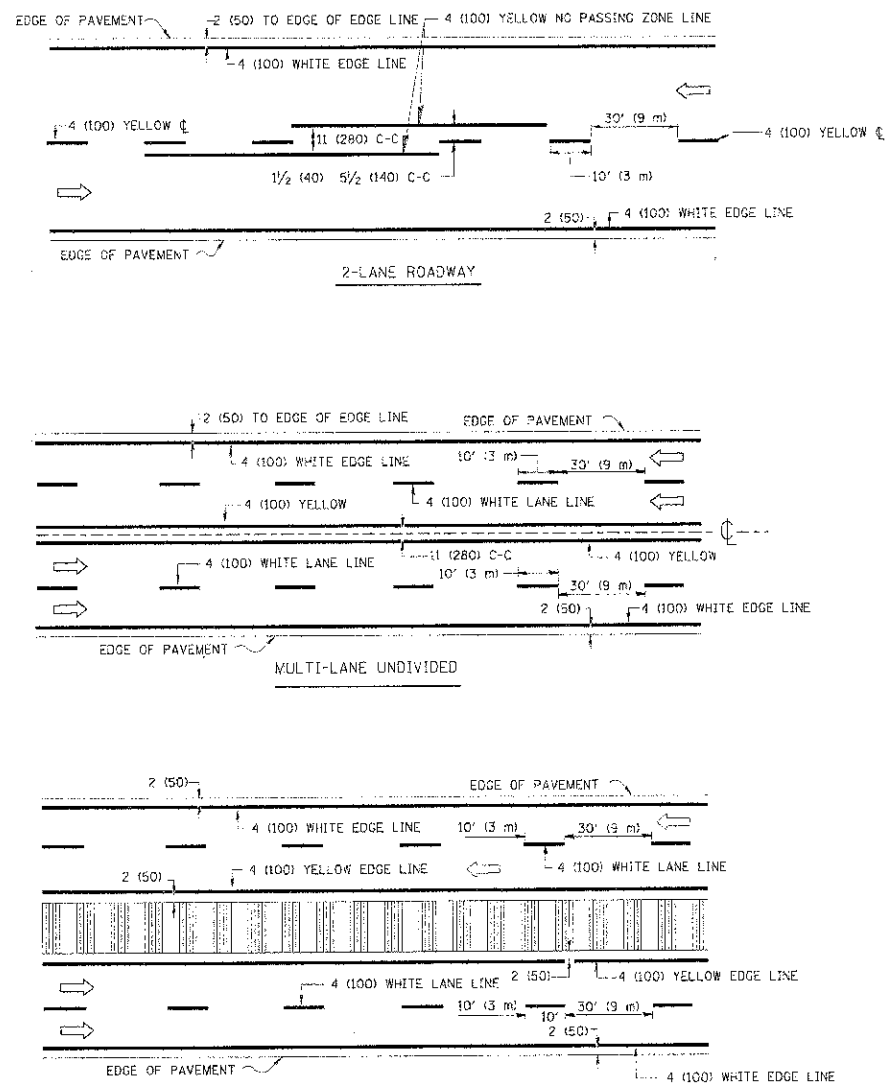
**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME = W:\d\state\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 5/8"=1'-0" IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

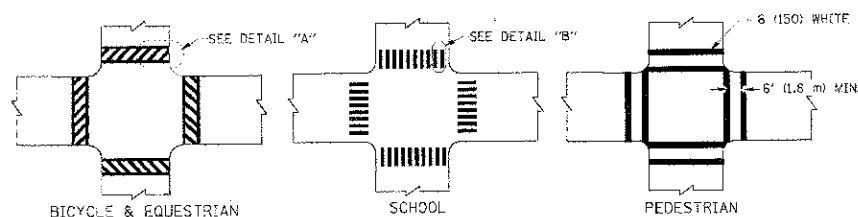
<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	12-00079-00-RS	COOK	17	15
<b>BD400-05 BD32</b>			<b>CONTRACT NO. 63831</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

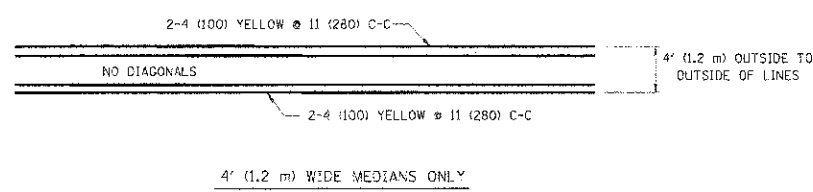


TYPICAL LANE AND EDGE LINE MARKING

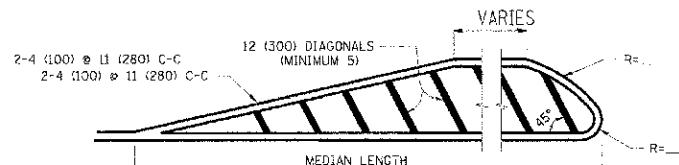
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE



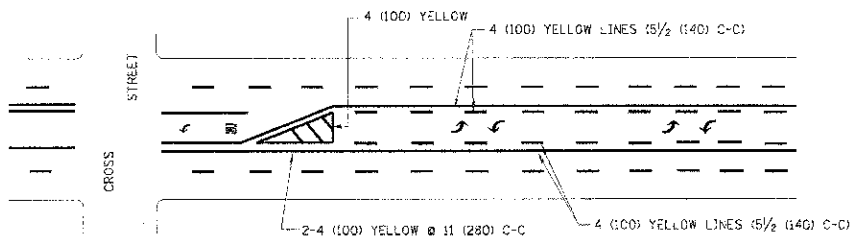
TYPICAL CROSSWALK MARKING



4' (1.2 m) WIDE MEDIANS ONLY

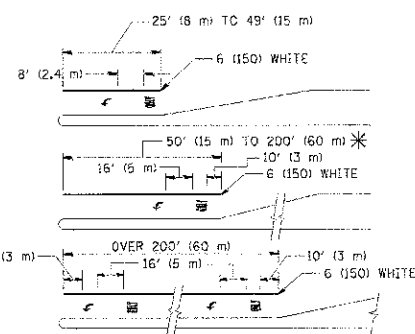


MEDIANS OVER 4' (1.2 m) WIDE



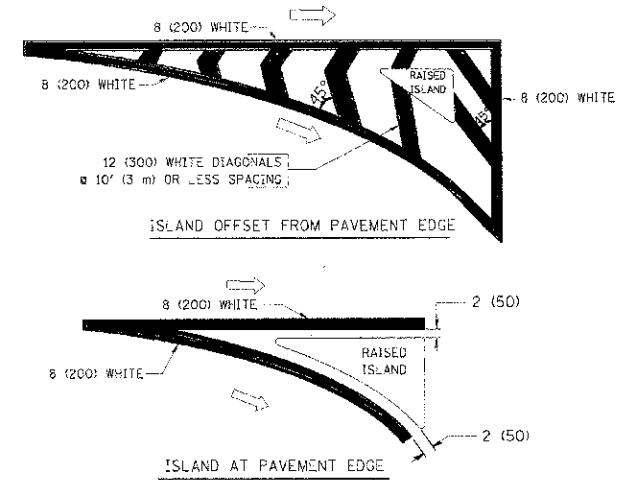
MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

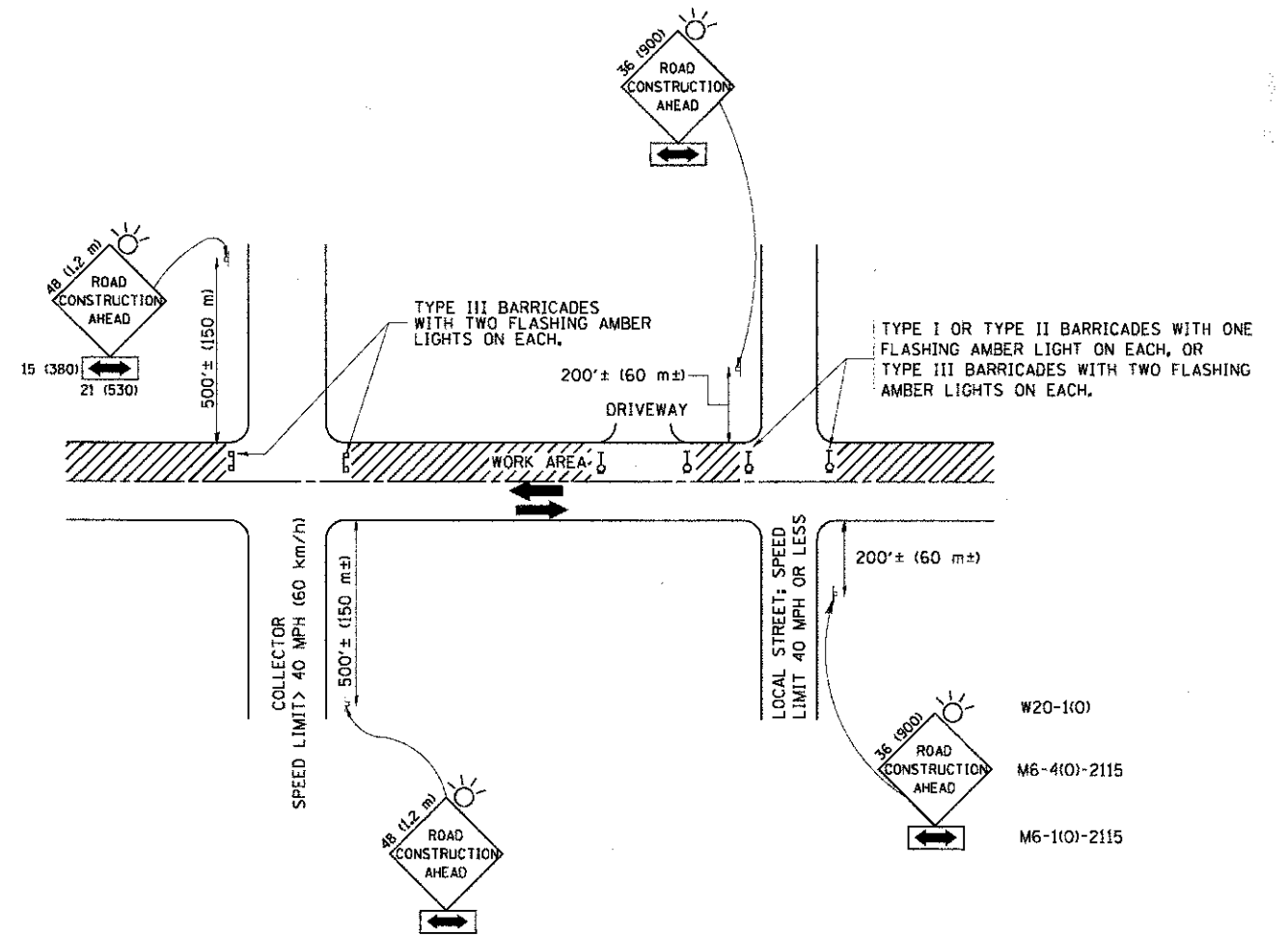
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 8' (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS; 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (23 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME: c:\pwork\work\proj\dtd\drawn\031909\031909.dwg	USER NAME: d_rammacher	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE TYPICAL PAVEMENT MARKINGS</b>	F.A. RTE.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 1/8" = 1'-0"	CHECKED	REVISED - C. JUCIJS 09-09-09				12-00079-00-RS	COOK	17	16	
PLOT DATE = 9/9/2009	DATE	REVISED				TC-13			CONTRACT NO. 63831	
	03-19-90					FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT			





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\dot\std\22x34\td12.dgn	USER NAME = geglennob	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.820' / 1M	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	12-00079-00-RS	COOK	17	17
TC-10		CONTRACT NO.	63831	
FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT				