06-14-13 LETTING ITEM 046

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	1

CONTRACT NO. 63833

26+1 = (27)

INDEX OF SHEETS

1		COVER SHEET AND INDEX OF SHEETS
2		GENERAL NOTES AND HIGHWAY STANDARDS
3		SUMMARY OF QUANTITIES
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8-11		SUGGESTED TRAFFIC CONTROL PLAN
12-13		PAVEMENT MARKING PLAN
14-16		PROJECT DETAILS
17	BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
18	BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
19	BD-32	BUTT JOINT AND HMA TAPER DETAILS
20	TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
21	TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
22	TC-14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
23	TC-16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
24	TC-22	ARTERIAL ROAD INFORMATION SIGN
25	TC-26	DRIVEWAY ENTRANCE SIGNING
26	TS-07	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

TRAFFIC DATA

ADT:
ST. CHARLES ROAD 17,300 VPD (2008)

POSTED SPEED

30 MPH (EXISTING)
30 MPH (PROPOSED)

DESIGN SPEED

35 MPH (EXISTING)
35 MPH (PROPOSED)

FUNCTIONAL CLASSIFICATION

MINOR ARTERIAL - URBAN

PROJECT IS LOCATED IN THE CITY OF ELMHURST

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU ROUTE 1397 (ST CHARLES ROAD)
FAP 0344 (IL ROUTE 83) TO FAU 2672 (SPRING ROAD)
RESURFACING
SECTION 13-00181-00-RS
PROJECT M-4003(159)
CITY OF ELMHURST
DUPAGE COUNTY
JOB NO. C-91-211-13

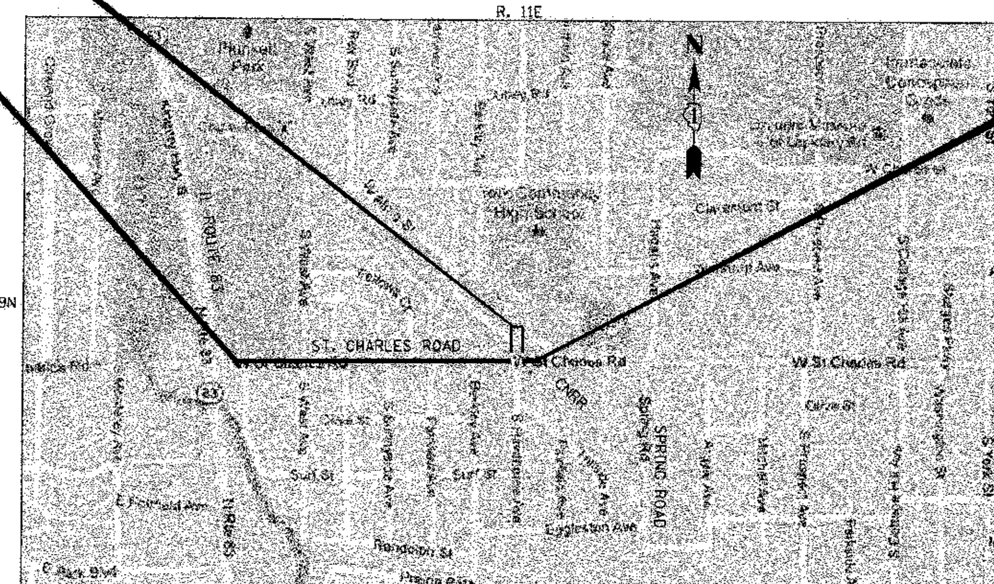
PROJECT OMISSION STATION 126+02 TO STATION 126+36

PROJECT BEGINS STATION 104+00

PROJECT ENDS STATION 127+08

R. 11E

T. 39N



YORK TOWNSHIP
SECTIONS 2, 3, 10, AND 11

CITY OF ELMHURST

3RD P.M.

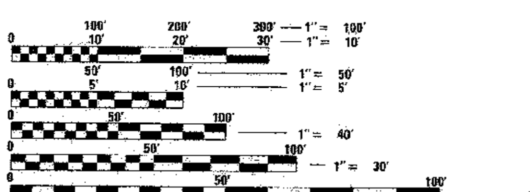
LOCATION MAP

NOT TO SCALE

PROJECT LENGTH (GROSS / NET)
2,308 FT (0.44 MILES) (GROSS)

OMISSION = 34 FT (0.01 MILES)
TOTAL PROJECT LENGTH = 2,274 FT (0.43 MILES) (NET)

LOCATION OF SECTION INDICATED THUS:-



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 63833

TranSystems
1475 EAST WOODFIELD ROAD, SUITE 600
SCHAUMBURG, ILLINOIS 60173
(847) 605-9600

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E., (847) 705-4406, SCHAUMBURG, IL

DAVID W. BLOCK
062-050966
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

4-1-13

DAVID W. BLOCK, P.E.
NO. 062-050966
EXP. DATE 11/30/13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 4-2-13, 2013
CITY OF ELMHURST, CITY ENGINEER

PASSED April 9, 2013
C.J. Holt
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW April 10, 2013
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

GENERAL NOTES

1. ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2012.

2. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.

3. PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR SHALL VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, THE CONTRACTOR MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANYWORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT WITH THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS/HER OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.

4. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.

5. BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 AND THE CITY OF ELMHURST AT 630-530-3020 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).

6. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY OR ROW WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.

7. SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.

8. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THEIR AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

9. OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.

10. PCC SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

11. QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD, PRIOR TO REMOVAL.

12. THE CONTRACTOR IS REQUIRED TO USE A PAVER SKI WHEN PLACING BITUMINOUS LIFTS.

13. THE CONTRACTOR SHALL COORDINATE PAVING OPERATIONS FOR BOTH HMA LEVEL BINDER AND SURFACE COURSES SO THAT THE LONGITUDINAL JOINTS ARE CLOSED AND COMPACTED AT THE END OF EACH DAY. PAVING OPERATIONS SHALL BE SCHEDULED SO THAT ADJACENT LANES ARE PAVED IN THE SAME DIRECTION AS THE INITIAL LANE MINIMIZING THE TIME THE EDGE OF A PAVEMENT MAT IS ALLOWED TO COOL.

14. THE CONTRACTOR SHALL USE 2 CHANGEABLE MESSAGE SIGNS AT LOCATIONS TO BE DETERMINED BY THE ENGINEER FOR A PERIOD FROM ONE WEEK PRIOR TO THE START OF CONSTRUCTION TO THE CONCLUSION OF THE PROJECT.

15. THE CONTRACTOR SHALL LIMIT ALL WORK EAST OF THE CN RAILROAD TRACKS TO BETWEEN THE HOURS OF 8:30 AM AND 2:30 PM TO MINIMIZE THE TRAFFIC IMPACTS TO YORK HIGH SCHOOL.

BOXED ITEMS INDICATE WORK NOT PAID FOR SEPARATELY BUT TO BE INCLUDED IN ANOTHER PAY ITEM OR THE CONTRACT.

SIGNING AND STRIPING

1. SEE IDOT STANDARD DETAIL 780001, DISTRICT ONE DETAIL TC-13 AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.

2. THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SIGNS THAT INTERFERE WITH CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.25.

TRAFFIC CONTROL

1. SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION.

2. THE CONTRACTOR SHALL SCHEDULE CONSTRUCTION ACTIVITIES SO THAT TWO LANES OF TRAFFIC SHALL REMAIN OPEN AT ALL TIMES.

3. "ROAD CONSTRUCTION AHEAD" SIGNS SHALL BE POSTED ON ALL SIDE STREETS FROM BOTH DIRECTIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE TRAFFIC CONTROL AND PROTECTION (SPECIAL).

ALL UTILITY ADJUSTMENTS AT THE WEST AVENUE INTERSECTION (STA. 109+00 TO STA. 111+00) SHALL BE COMPLETED AT NIGHT BETWEEN THE HOURS OF 7:00 PM AND 7:00 AM. A HIGH-EARLY-STRENGTH CONCRETE MIXTURE WITH A CURE TIME OF 12 HOURS SHALL BE USED.

STORM SEWERS, WATER MAINS, AND UTILITIES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF ANY UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.

2. THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN IF NOT SHOWN ON THE PLANS. ALL UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S EXPENSE.

4. ALL UTILITY OWNERS AND THE ENGINEER SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.

5. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENNANCES THAT MUST BE KEPT IN OPERATION.

6. ALL LOOSE MATERIAL DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.15.

7. THE EXISTING FRAMES AND LIDS SHALL REMAIN AS PROPERTY OF THE CITY OF ELMHURST. ALL OLD FRAMES AND LIDS NOT BEING REUSED SHALL BE REMOVED FROM PARKWAYS BY THE CONTRACTOR, DELIVERED TO AND STOCKPILED AT THE CITY MUNICIPAL SERVICE FACILITY WITHIN SEVEN (7) DAYS OF THEIR REMOVAL. THE UTILITY DEPARTMENT YARD IS LOCATED AT THE NORTH END OF THE WASTE WATER TREATMENT PLANT FACILITY AT 625 S. IL ROUTE 83. THIS WORK SHALL BE INCLUDED IN THE COST OF FRAMES AND LIDS, TYPE 1, CLOSED LID.

8. FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) SHALL INCLUDE REPLACEMENT OF EXISTING BROKEN ADJUSTMENT RINGS AND PATCHING INSIDE THE STRUCTURES BETWEEN PIPES AND STRUCTURES WITH HYDRAULIC CEMENT AT LOCATIONS AS DIRECTED BY THE ENGINEER. IF THE STRUCTURE IS A COMBINATION SEWER OR SANITARY MANHOLE THEN CHIMNEY SEALS SHALL BE PROVIDED.

9. ALL DRAINAGE STRUCTURE ADJUSTMENTS AND FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) SHALL USE PCC. HMA WILL NOT BE ALLOWED. EACH JOINT SHALL BE SEALED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS AS DIRECTED PER ARTICLE 602.02.

MISCELLANEOUS

1. MATERIALS RESULTING FROM THE REMOVAL OF CONCRETE SURFACES, UTILITY STRUCTURE ADJUSTMENTS, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IF THE CONTRACTOR DOES NOT REMOVE THESE MATERIALS AT THE REQUEST OF THE ENGINEER, THE ENGINEER WILL HIRE A CONTRACTOR TO HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL BE BILLED (CHARGED) ACCORDINGLY.

2. THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS/HER YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYING OF HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL.

4. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS, COMMERCIAL PROPERTY OWNERS, AND THE ENGINEER WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO SIDEWALK, DRIVEWAY, AND/OR CURB AND GUTTER REPLACEMENT. AT LOCATIONS WHERE THE SIDEWALK, DRIVEWAY, AND/OR CURB AND GUTTER IS SCHEDULED TO BE REMOVED, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER 48 HOURS PRIOR TO REMOVING THE SIDEWALK, DRIVEWAY, OR CURB AND GUTTER. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES. THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE A DRIVEWAY FOR MORE THAN 48 HOURS UNDER ANY CIRCUMSTANCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE BARRICADES TO PREVENT TRAFFIC FROM USING THE DRIVEWAYS DURING THIS PERIOD.

5. WHEN REMOVING PAVEMENT, CURB AND GUTTER, SIDEWALK, AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES AND BUILDING FOUNDATIONS WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

6. WHEN THE PCC SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK ADJACENT TO THE DRIVEWAY SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. THE CROSS SLOPE OF ANY SIDEWALK EXTENDING THROUGH A DRIVEWAY IS TO BE A MAXIMUM OF 1V:50H. ALL SIDEWALK THAT DOES NOT EXTEND THROUGH A DRIVEWAY WILL BE PAID FOR AS PCC SIDEWALK, 5".

7. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.

8. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE WITH THE CANADIAN NATIONAL RAILROAD WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE CANADIAN NATIONAL RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD ROW MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-01	DIAGONAL CURB RAMPS
424011-01	CORNER PARALLEL CURB RAMPS
424021-01	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-03	FRAME AND LIDS TYPE 1
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-05	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL TURN LANE
701602-06	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-08	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
720006-03	SIGN PANEL ERECTION DETAILS
780001-03	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

FILE NAME = D:\CH12\13082\Road\Sheets\13082-2-GENNOTES.dgn	USER NAME = JLSTO	DESIGNED - JLB	REVISED -
		DRAWN - JLB	REVISED -
	PLOT SCALE = 50.000 '/ in.	CHECKED - DWB	REVISED -
#MODELNAME#	PLOT DATE = 4/3/2013	DATE - 04/03/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD RESURFACING
GENERAL NOTES AND HIGHWAY STANDARDS

SCALE: NTS | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	2
		CONTRACT NO. 63833		
		ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES

*	CODE NO	PAY ITEM	UNIT	QUANTITY	0005 ROADWAY 70% STP 30% LA	0005 ROADWAY 100% LA
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	59	59	
*	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	154	154	
*	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	5	5	
*	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	5	5	
*	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	5	5	
*	25200110	SODDING, SALT TOLERANT	SQ YD	154	154	
*	25200200	SUPPLEMENTAL WATERING	UNIT	5	5	
	28000510	INLET FILTERS	EACH	16	16	
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	41	41	
	31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	516	516	
	35800100	PREPARATION OF BASE	SQ YD	12,070	12,070	
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	2,414	2,414	
	40600300	AGGREGATE (PRIME COAT)	TON	49	49	
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	25	25	
	40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	664	664	
	40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	118	118	
	40600990	TEMPORARY RAMP	SQ YD	244	244	
	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1,352	1,352	
	42001300	PROTECTIVE COAT	SQ YD	3,412	3,412	
	42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	31	31	
	42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	53	53	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	3,951	3,951	
	42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	SQ FT	267	267	
	42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	420	420	
	42400800	DETECTABLE WARNINGS	SQ FT	333	333	
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	115	115	
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	460	460	
	44000600	SIDEWALK REMOVAL	SQ FT	4,638	4,638	
	44201749	CLASS D PATCHES, TYPE I, 9 INCH	SQ YD	151	151	
	44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	151	151	
	44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	151	151	
	44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	151	151	
	60266600	VALVE BOXES TO BE ADJUSTED	EACH	4	4	
	60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5	5	
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	260	260	
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	200	200	
	67100100	MOBILIZATION	L SUM	1	1	
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	1,917	1,917	
	70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	924	924	
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	10,130	10,130	
	70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	3,426	3,426	
	70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	422	422	
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	816	816	
	70300510	PAVEMENT MARKING TAPE, TYPE III - LETTERS AND SYMBOLS	SQ FT	219	219	

SUMMARY OF QUANTITIES

*	CODE NO	PAY ITEM	UNIT	QUANTITY	C005 ROADWAY 70% STP 30% LA	0005 ROADWAY 100% LA
	70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	3,049	3,049	
	70300540	PAVEMENT MARKING TAPE, TYPE III 6"	FOOT	1,204	1,204	
	70300570	PAVEMENT MARKING TAPE, TYPE III 24"	FOOT	122	122	
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,015	1,015	
*	78005100	EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	462	462	
*	78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	5,065	5,065	
*	78005130	EPOXY PAVEMENT MARKING - LINE 6"	FOOT	1,713	1,713	
*	78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	211	211	
*	78005180	EPOXY PAVEMENT MARKING - LINE 24"	FOOT	408	408	
*	81028350	UNDERGROUND CONDUIT, PVC, 2" DIA.	FOOT	159		159
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	698	698	
	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	2	2	
	X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	2	2	
	X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	11,952	11,952	
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	26	26	
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
	70106800	CHANGEABLE MESSAGE SIGN	CAL mo	4	4	
*	X7830068	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS	SQ FT	462		462
*	X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	5,065		5,065
*	X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	1,713		1,713
*	X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	211		211
*	X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	408		408
*	X8140215	HEAVY-DUTY HANDHOLE TO BE ADJUSTED	EACH	2		2
	XX000445	SAWCUT AND SEAL NEW JOINTS	FOOT	3,414		3,414
	Z0004522	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"	SQ YD	12	12	
	Z0004530	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8"	SQ YD	19	19	
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
	Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	8	8	
	Z0023202	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	16	16	
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	164	164	
	Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1	

* SPECIALTY ITEM

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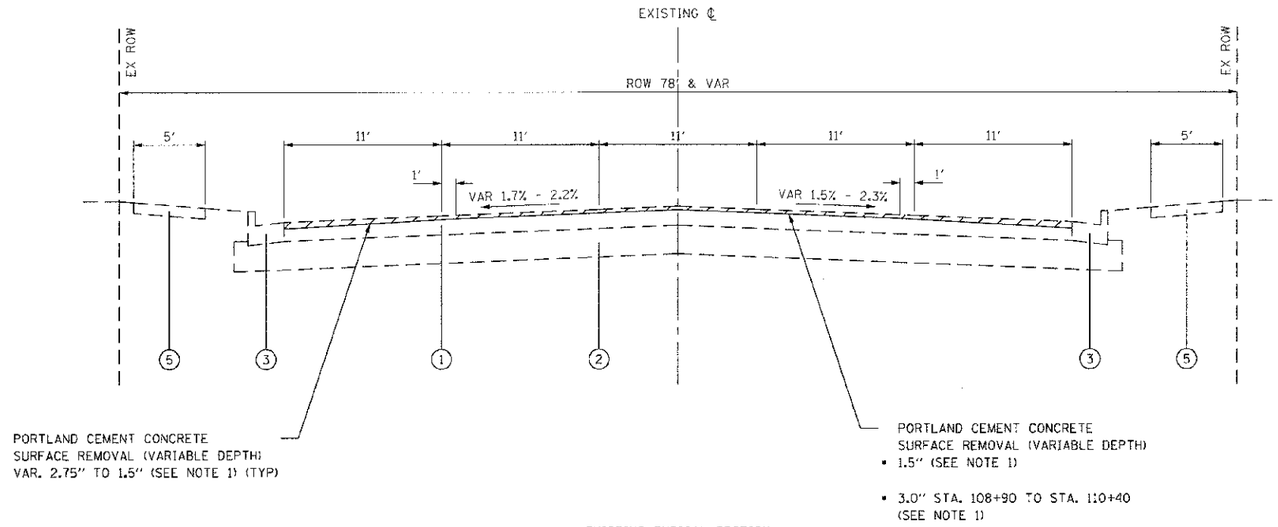
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD RESURFACING
SUMMARY OF QUANTITIES

SCALE: NTS | SHEET 1 OF 1 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	3
		CONTRACT NO. 63833		

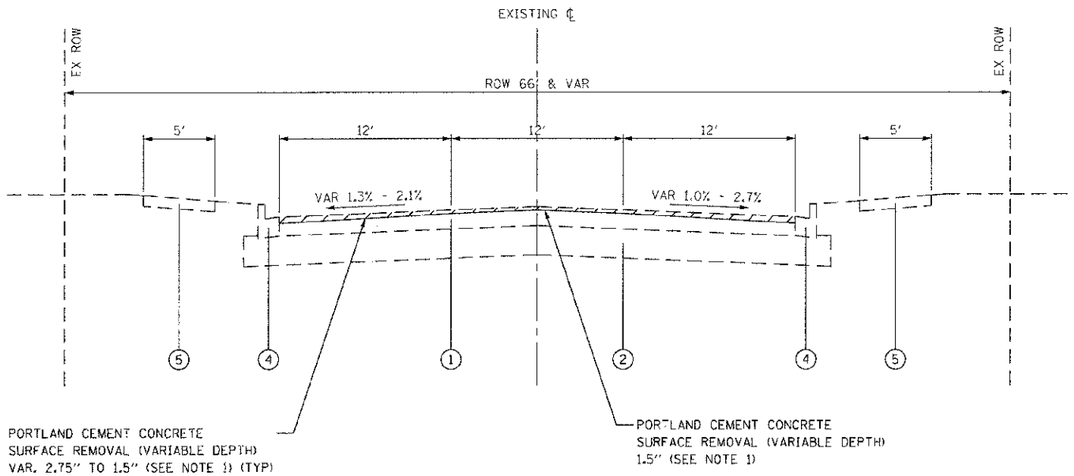
ILLINOIS FED. AID PROJECT



EXISTING TYPICAL SECTION
STA. 104+00 TO STA. 114+16, ST. CHARLES ROAD

LEGEND

(1) EXISTING PCC PAVEMENT, 8"
(2) EXISTING AGGREGATE SUBGRADE, 12"
(3) EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.24
(4) EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
(5) EXISTING PCC SIDEWALK, 5"
(6) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
(7) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
(8) CLASS D PATCHES, 9" (AS DIRECTED BY ENGINEER)
(9) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY ENGINEER)
(10) COMBINATION CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
(11) COMBINATION CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
(12) SIDEWALK REMOVAL
PCC SIDEWALK, 5" OR PCC SIDEWALK, 6" OR PCC SIDEWALK, 8"
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
(13) SUBBASE GRANULAR MATERIAL, TYPE B 4"
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
(14) SODDING, SALT TOLERANT
TOPSOIL FURNISH AND PLACE, 4"
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)



EXISTING TYPICAL SECTION
STA. 114+16 TO STA. 126+02
OMISSION STA. 126+02 TO STA. 126+36
STA. 126+36 TO STA. 127+08
ST. CHARLES ROAD

NOTES

1. THE CONTRACTOR SHALL MILL A VARIABLE DEPTH OF PAVEMENT BETWEEN THE EDGE OF PAVEMENT AND 12' FROM THE EDGE OF PAVEMENT. THE MILL DEPTH SHALL BE 2.75" AT THE EDGE OF PAVEMENT AND 1.5" AT 12' FROM THE EDGE OF PAVEMENT. A CONSTANT DEPTH OF 1.5" SHALL BE REMOVED BETWEEN THE 12' EDGE OF PAVEMENT OFFSETS. ALL REMOVAL WILL BE PAID FOR AS PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH).

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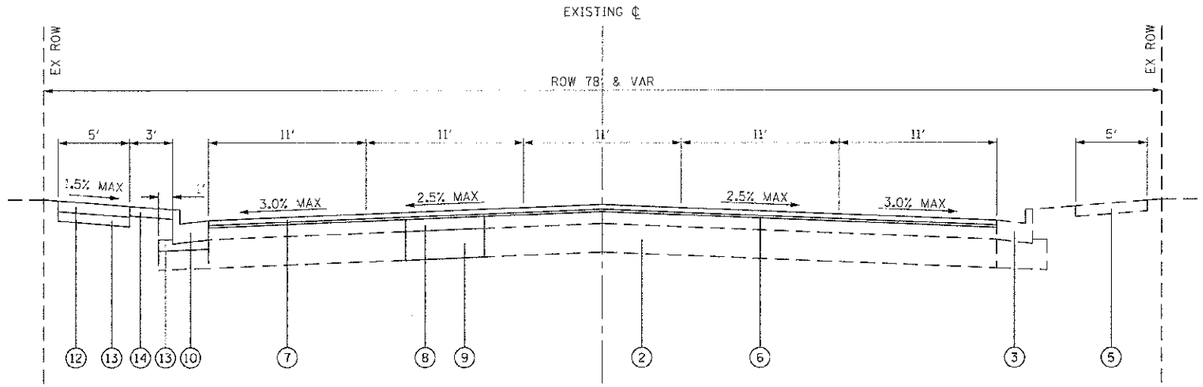
	PLOT DATE = 4/3/2013	DATE - 04/03/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD RESURFACING
TYPICAL SECTIONS

SCALE: NTS | SHEET 1 OF 2 SHEETS | STA. | TO STA.

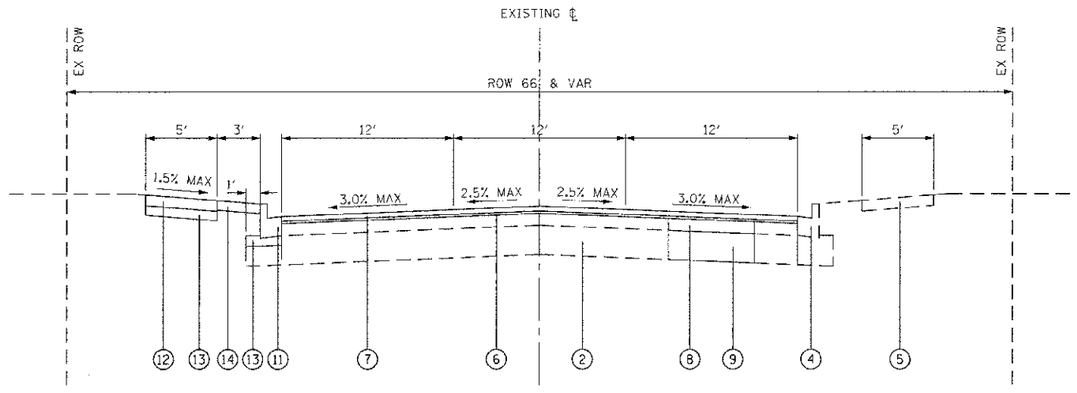
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	4
		CONTRACT NO. 63833		
	ILLINOIS FED. AID PROJECT			



PROPOSED TYPICAL SECTION
STA. 104+00 TO STA. 114+16, ST. CHARLES ROAD

LEGEND

1. EXISTING PCC PAVEMENT, 8"
2. EXISTING AGGREGATE SUBGRADE, 12"
3. EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.24
4. EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
5. EXISTING PCC SIDEWALK, 5"
6. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
7. HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
8. CLASS D PATCHES, 9" (AS DIRECTED BY ENGINEER)
9. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
   AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY ENGINEER)
10. COMBINATION CURB AND GUTTER REMOVAL
    COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
    (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
11. COMBINATION CURB AND GUTTER REMOVAL
    COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
    (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
12. SIDEWALK REMOVAL
    PCC SIDEWALK, 5" OR PCC SIDEWALK, 6" OR PCC SIDEWALK, 8"
    (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
13. SUBBASE GRANULAR MATERIAL, TYPE B 4"
    (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
14. SODDING, SALT TOLERANT
    TOPSOIL FURNISH AND PLACE, 4"
    (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)



PROPOSED TYPICAL SECTION
STA. 114+16 TO STA. 126+02
OMISSION STA. 126+02 TO STA. 126+36
STA. 126+36 TO STA. 127+08
ST. CHARLES ROAD

NOTES

1. THE CONTRACTOR SHALL MILL A VARIABLE DEPTH OF PAVEMENT BETWEEN THE EDGE OF PAVEMENT AND 12' FROM THE EDGE OF PAVEMENT. THE MILL DEPTH SHALL BE 2.75" AT THE EDGE OF PAVEMENT AND 1.5" AT 12' FROM THE EDGE OF PAVEMENT. A CONSTANT DEPTH OF 1.5" SHALL BE REMOVED BETWEEN THE 12' EDGE OF PAVEMENT OFFSETS. ALL REMOVAL WILL BE PAID FOR AS PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH).
2. ALL PROPOSED ROADWAY CROSS SLOPES ARE ADA COMPLIANT.

THE CONTRACTOR SHALL MILL BEFORE PATCHING.

MIXTURE TYPE	AIR VOIDS @ Ndes
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5mm), 2"	4% @ 70 GYRATIONS
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% @ 50 GYRATIONS
CLASS D PATCHES (HMA BINDER IL-19 VM), 9" (IN 3 LIFTS)	4% @ 70 GYRATIONS
HOT-MIX ASPHALT DRIVEWAYS 6"	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5mm) 2"	4% @ 50 GYRATIONS
HMA BASE COURSE (HMA BINDER IL - 19mm) 4" (IN 2 LIFTS)	4% @ 50 GYRATIONS
HOT-MIX ASPHALT DRIVEWAYS 8"	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5mm) 2"	4% @ 50 GYRATIONS
HMA BASE COURSE (HMA BINDER IL - 19mm) 6" (IN 2 LIFTS)	4% @ 50 GYRATIONS

NOTES: 1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2) THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE THE SPECIAL PROVISIONS.

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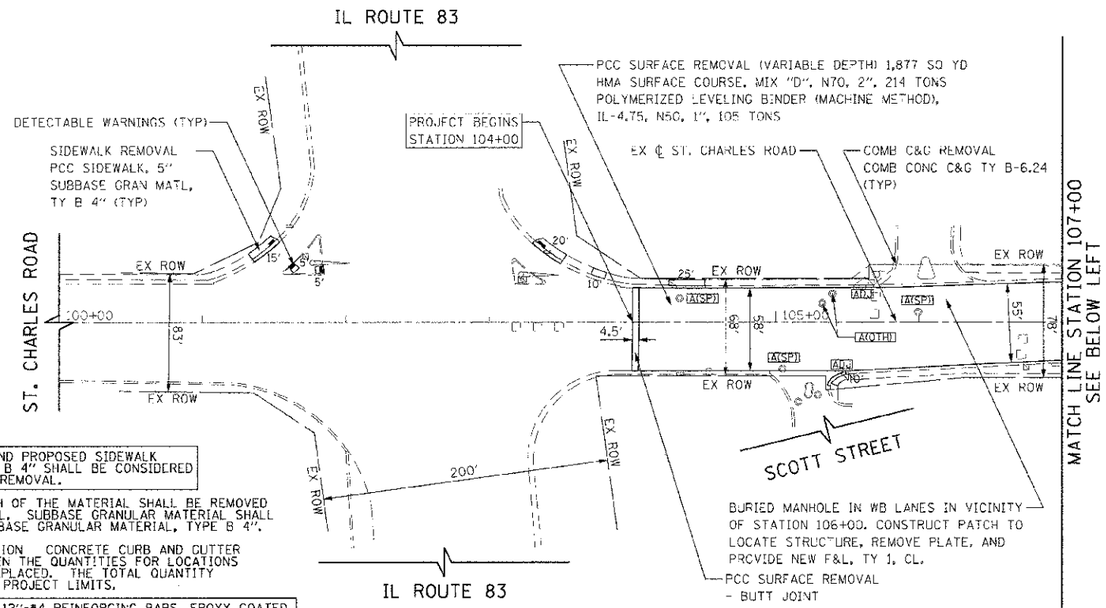
		DATE - 04/03/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD RESURFACING
TYPICAL SECTIONS

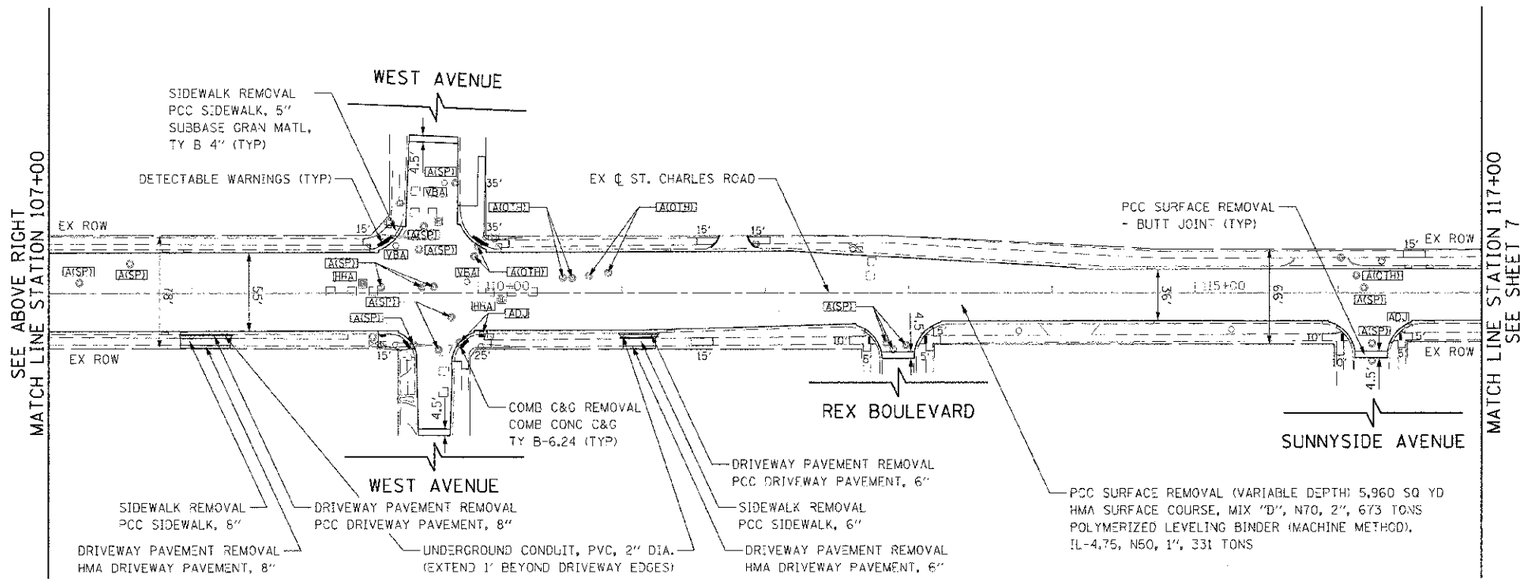
SCALE: NTS | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	5
		CONTRACT NO. 63833		
	ILLINOIS FED. AID PROJECT			



NOTES:

1) CLASS D PATCHES, 9" AS DIRECTED BY ENGINEER.
2) THE ADJUSTMENTS OF DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED.
3) REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER AND PROPOSED SIDEWALK LOCATIONS IN ORDER TO INSTALL THE PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4" SHALL BE CONSIDERED INCLUDED IN THE COSTS OF COMBINATION CURB AND GUTTER REMOVAL AND SIDEWALK REMOVAL.
4) IF UNSUITABLE MATERIAL IS ENCOUNTERED UNDER AN EXISTING DRIVEWAY, A 4" DEPTH OF THE MATERIAL SHALL BE REMOVED AND DISPOSED OF AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. SUBBASE GRANULAR MATERIAL SHALL BE INSTALLED IN PLACE OF THE REMOVED MATERIAL AND SHALL BE PAID FOR AS SUBBASE GRANULAR MATERIAL, TYPE B 4".
5) AN ADDITIONAL QUANTITY OF COMBINATION CURB AND GUTTER REMOVAL AND COMBINATION CONCRETE CURB AND GUTTER (TYPES B-6.12 AND B-6.24) WHICH IS NOT SHOWN ON THE PLANS HAS BEEN INCLUDED IN THE QUANTITIES FOR LOCATIONS WHERE CURB AND GUTTER IS ADJACENT TO PATCHING AREAS AND WILL NEED TO BE REPLACED. THE TOTAL QUANTITY IS EQUAL TO 10% OF THE TOTAL LENGTH OF EXISTING CURB AND GUTTER WITHIN THE PROJECT LIMITS.
6) ALL PROPOSED SIDEWALK ADJACENT TO EXISTING SIDEWALK SHALL BE TIED WITH TWO 12"-#4 REINFORCING BARS, EPOXY COATED. THIS WORK SHALL BE INCLUDED IN THE COST OF THE PCC SIDEWALK PAY ITEM OF THE CORRESPONDING DEPTH.
7) ALL SIDE ROADS SHALL BE MILLED AT A CONSISTENT 3" DEPTH. ALL REMOVAL WILL BE PAID FOR AS PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH).



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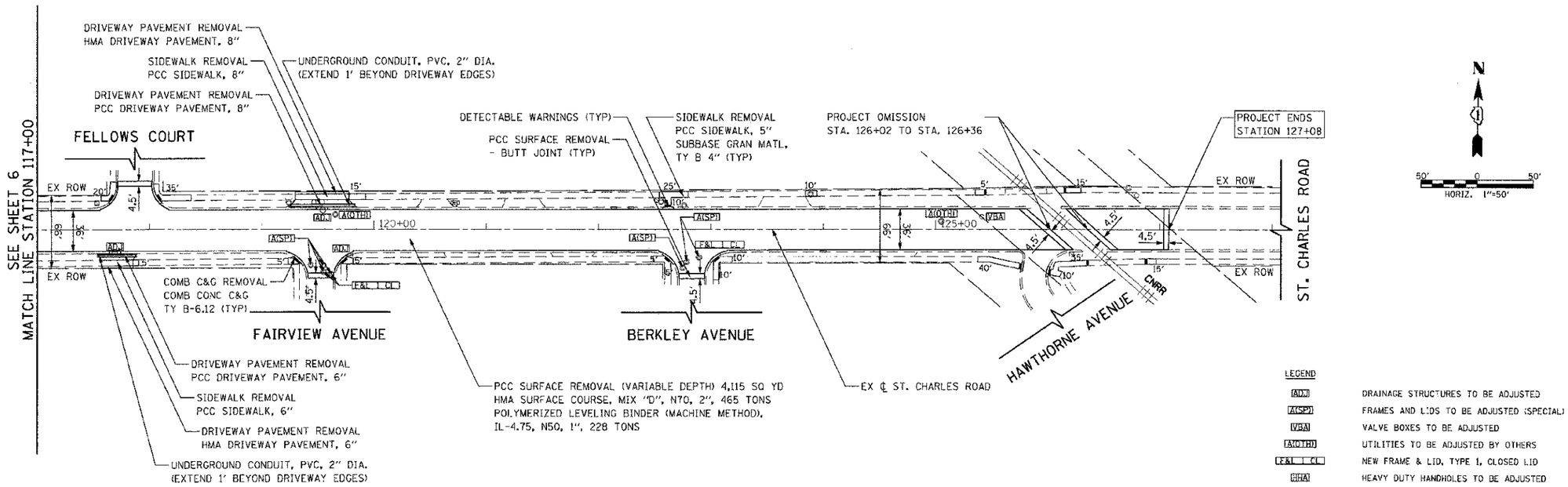
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD RESURFACING
ROADWAY PLAN

SCALE: 1"=50' | SHEET 1 OF 2 SHEETS | STA. 104+00 TO STA. 117+00

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	6
		CONTRACT NO. 63833		
	ILLINOIS FED. AID PROJECT			



NOTES: 1) CLASS D PATCHES, 9" AS DIRECTED BY ENGINEER.

2) THE ADJUSTMENTS OF DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED.

3) REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER AND PROPOSED SIDEWALK LOCATIONS IN ORDER TO INSTALL THE PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4" SHALL BE CONSIDERED INCLUDED IN THE COSTS OF COMBINATION CURB AND GUTTER REMOVAL AND SIDEWALK REMOVAL.

4) IF UNSUITABLE MATERIAL IS ENCOUNTERED UNDER AN EXISTING DRIVEWAY, A 4" DEPTH OF THE MATERIAL SHALL BE REMOVED AND DISPOSED OF AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. SUBBASE GRANULAR MATERIAL SHALL BE INSTALLED IN PLACE OF THE REMOVED MATERIAL AND SHALL BE PAID FOR AS SUBBASE GRANULAR MATERIAL, TYPE B 4".

5) AN ADDITIONAL QUANTITY OF COMBINATION CURB AND GUTTER REMOVAL AND COMBINATION CONCRETE CURB AND GUTTER (TYPES B-6.12 AND B-6.24) WHICH IS NOT SHOWN ON THE PLANS HAS BEEN INCLUDED IN THE QUANTITIES FOR LOCATIONS WHERE CURB AND GUTTER IS ADJACENT TO PATCHING AREAS AND WILL NEED TO BE REPLACED. THE TOTAL QUANTITY IS EQUAL TO 10% OF THE TOTAL LENGTH OF EXISTING CURB AND GUTTER WITHIN THE PROJECT LIMITS.

6) ALL PROPOSED SIDEWALK ADJACENT TO EXISTING SIDEWALK SHALL BE TIED WITH TWO 12"-#4 REINFORCING BARS, EPOXY COATED. THIS WORK SHALL BE INCLUDED IN THE COST OF THE PCC SIDEWALK PAY ITEM OF THE CORRESPONDING DEPTH.

7) ALL SIDE ROADS SHALL BE MILLED AT A CONSISTENT 3" DEPTH. ALL REMOVAL WILL BE PAID FOR AS PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH).

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

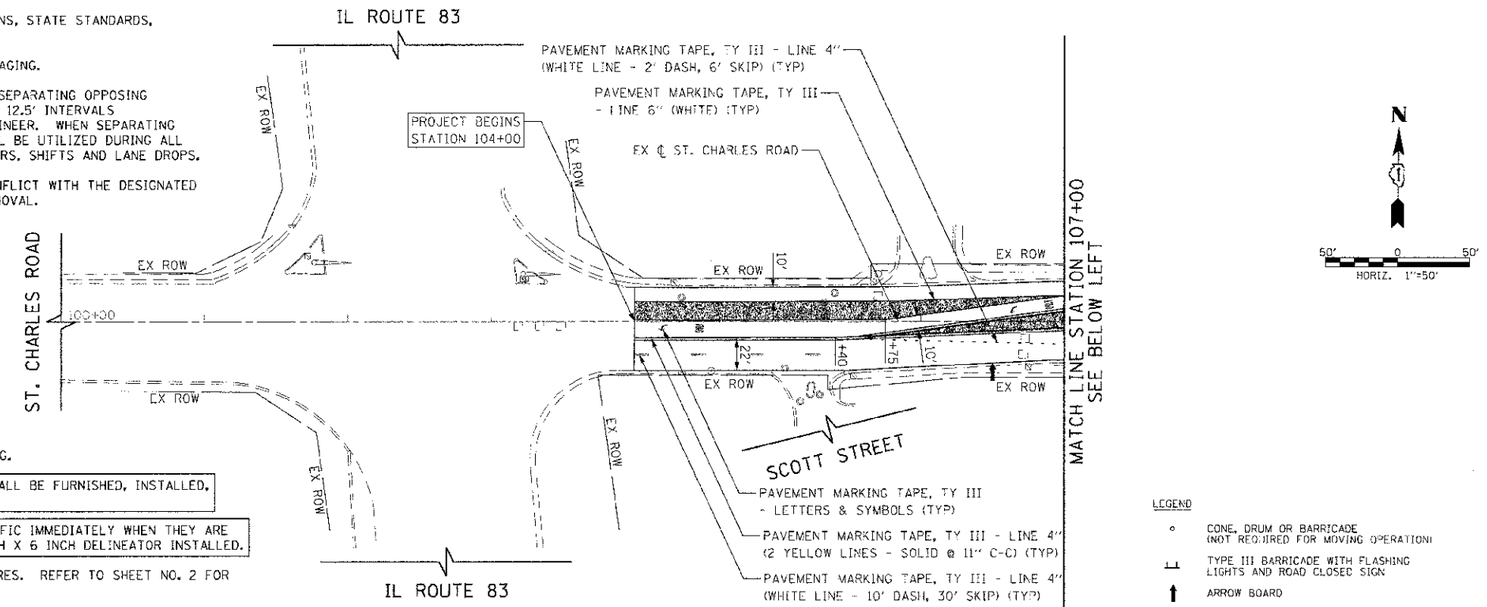
ST. CHARLES ROAD RESURFACING
ROADWAY PLAN

SCALE: 1"=50' | SHEET 2 OF 2 SHEETS | STA. 117+00 TO STA. 127+08

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	7
		CONTRACT NO. 63833		
		ILLINOIS FED. AID PROJECT		

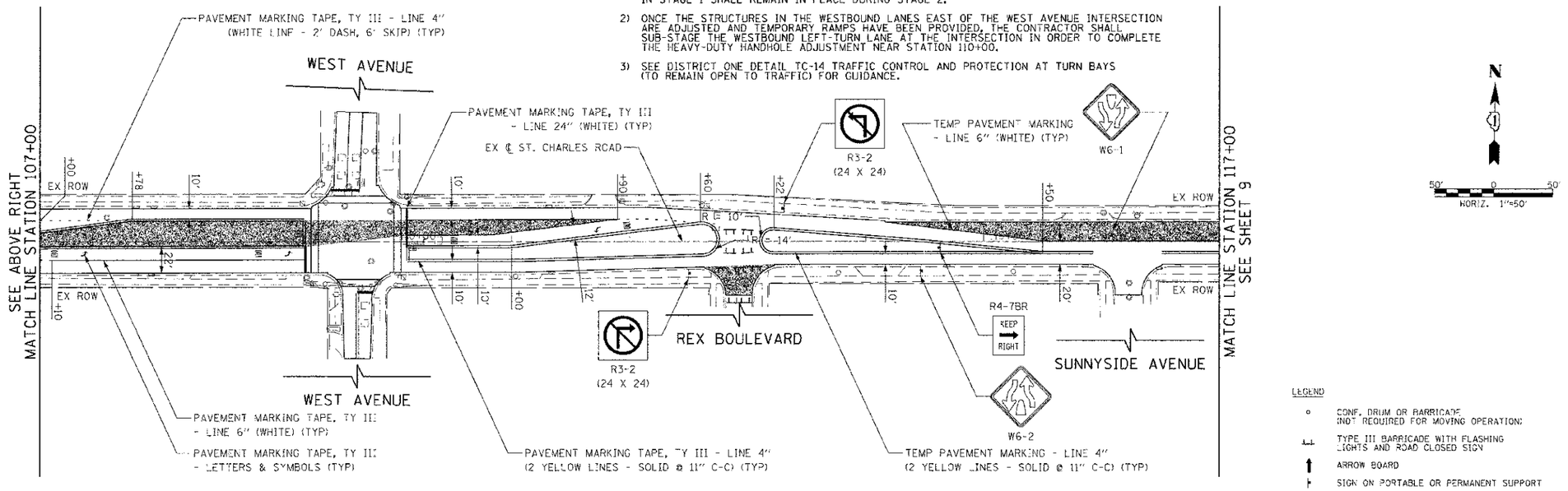
MAINTENANCE OF TRAFFIC – GENERAL NOTES (ALL STAGES)

1. THE CONTRACTOR SHALL MAINTAIN TRAFFIC IN ACCORDANCE WITH THE PLANS, SPECIAL PROVISIONS, STATE STANDARDS, STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.
2. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE IN CONSTRUCTION STAGING.
3. TYPE II BARRICADES SHALL BE EQUIPPED WITH MONODIRECTIONAL STEADY BURN LIGHTS. WHEN SEPARATING OPPOSING LANES OF TRAFFIC, DELINEATORS AND BARRICADES SHALL BE PLACED AT 25' INTERVALS AND AT 12.5' INTERVALS WITHIN TAPER AND CURVE SECTIONS AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. WHEN SEPARATING TRAFFIC FROM CONSTRUCTION, THIS SPACING MAY BE DOUBLED. BARRICADES, 2' IN WIDTH SHALL BE UTILIZED DURING ALL STAGES OF TRAFFIC CONTROL. DIRECTION INDICATOR BARRICADES SHALL BE USED AT ALL TAPERS, SHIFTS AND LANE DROPS.
4. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE ALL EXISTING PAVEMENT MARKINGS WHICH CONFLICT WITH THE DESIGNATED TRAFFIC CONTROL PLAN. THIS WORK SHALL BE PAID FOR AS WORK ZONE PAVEMENT MARKING REMOVAL.
5. ALL TEMPORARY PAVEMENT MARKINGS SHOWING DETERIORATION AFTER 7 DAYS SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER.
6. THE FURNISHING, INSTALLING, AND RELOCATION OF ALL TRAFFIC SIGNS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THE STANDARD SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL). ALL CONFLICTING TRAFFIC SIGNS SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THIS SHALL ALSO BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).
7. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PAY ITEM FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL) UNLESS OTHERWISE INDICATED ON THE PLANS OR IN THE SPECIAL PROVISIONS.
8. ALL TRAFFIC CONTROL WARNING SIGNS AND ASSOCIATED SIGNING MOUNTED WITH THE WARNING SIGNS SHALL HAVE BLACK LEGENDS AND BORDERS ON FLUORESCENT ORANGE REFLECTIVE SHEETING.
9. ALL CONSTRUCTION SIGNS, BARRICADES, AND OTHER DEVICES REQUIRED TO CONTROL TRAFFIC SHALL BE FURNISHED, INSTALLED, AND MAINTAINED BY THE CONTRACTOR.
10. ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED, COVERED, OR TURNED AWAY FROM THE TRAFFIC IMMEDIATELY WHEN THEY ARE NO LONGER NECESSARY. WHEN A SIGN IS COVERED, ITS POST SHALL HAVE A REFLECTIVE 3 INCH X 6 INCH DELINEATOR INSTALLED.
11. SUGGESTED TRAFFIC CONTROL SHEETS DO NOT SHOW ALL THE SIGNS REQUIRED FOR LANE CLOSURES. REFER TO SHEET NO. 2 FOR THE APPROPRIATE HIGHWAY STANDARDS FOR ADDITIONAL SIGNS TO BE PLACED.
12. THESE SUGGESTED TRAFFIC CONTROL PLANS ARE TO ALLOW FOR THE UTILITY STRUCTURE ADJUSTMENTS AND CURING. OTHER WORK OPERATIONS MAY BE COMPLETED AS COORDINATED WITH THE ENGINEER. THE CONTRACTOR SHALL IMPLEMENT APPROPRIATE TRAFFIC CONTROL HIGHWAY STANDARDS FOR PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, DRIVEWAY AND SIDEWALK REMOVAL AND REPLACEMENT, HMA RESURFACING, MILLING, AND OTHER WORK ITEMS.



13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING ALL AFFECTED RESIDENTS OF TEMPORARY SIDE ROAD CLOSURES. UTILITY ADJUSTMENTS IN THE SIDE STREET INTERSECTIONS WILL NEED TO BE COMPLETED IN STAGES AS SHOWN.

NOTES:
1) DURING STAGE 1, THE CONTRACTOR SHALL PLACE PAVEMENT MARKING TAPE, TYPE III BETWEEN IL ROUTE 83 AND REX BOULEVARD AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) EAST OF REX BOULEVARD AND ALONG WEST AVENUE. WHEN SWITCHING THE LANE CONFIGURATIONS TO STAGE 2, THE CONTRACTOR SHALL REMOVE THE PAVEMENT MARKING TAPE BETWEEN IL ROUTE 83 AND REX BOULEVARD AND REPLACE THOSE MARKINGS WITH TEMPORARY PAVEMENT MARKINGS (PAINT). THE EXISTING TEMPORARY PAVEMENT MARKINGS (PAINT) THAT WERE PLACED IN STAGE 1 SHALL REMAIN IN PLACE DURING STAGE 2.
2) ONCE THE STRUCTURES IN THE WESTBOUND LANES EAST OF THE WEST AVENUE INTERSECTION ARE ADJUSTED AND TEMPORARY RAMPS HAVE BEEN PROVIDED, THE CONTRACTOR SHALL SUB-STAGE THE WESTBOUND LEFT-TURN LANE AT THE INTERSECTION IN ORDER TO COMPLETE THE HEAVY-DUTY HANDHOLE ADJUSTMENT NEAR STATION 110+00.
3) SEE DISTRICT ONE DETAIL TC-14 TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) FOR GUIDANCE.



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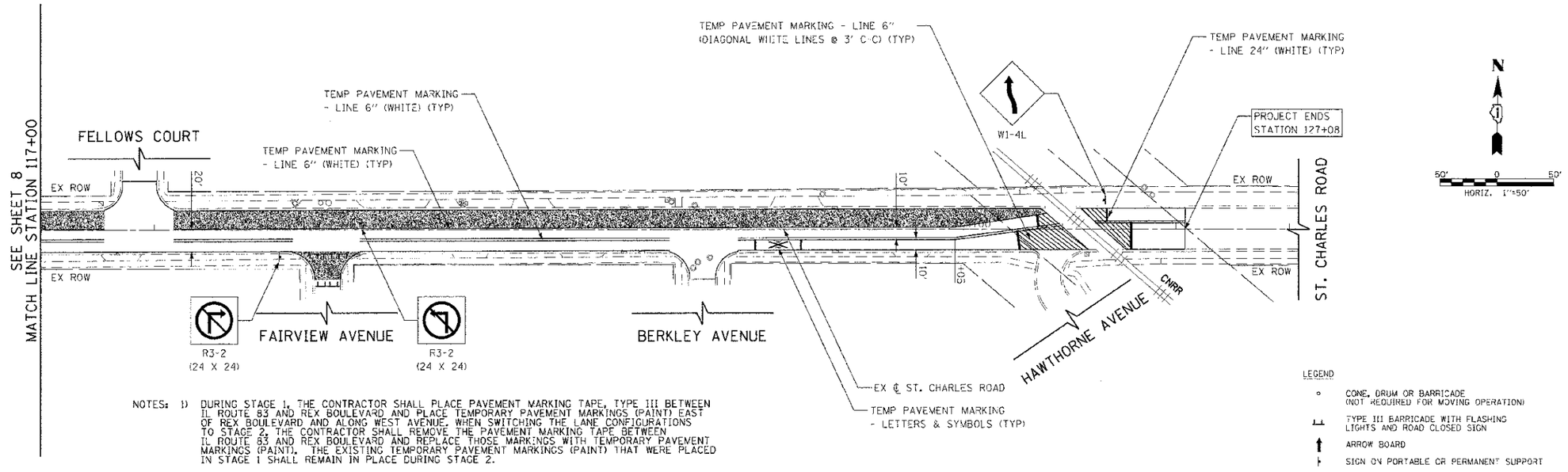
$MODELNAME$	PLOT DATE = 4/3/2013	DATE - 04/03/2013	REVISED -	

ST. CHARLES ROAD RESURFACING
SUGGESTED TRAFFIC CONTROL PLAN (STAGE 1)

SCALE: 1"=50' | SHEET 1 OF 4 SHEETS | STA. 104+00 TO STA. 117+00

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	8
		CONTRACT NO. 63833		
		ILLINOIS FED. AID PROJECT		



NOTES: 1) DURING STAGE 1, THE CONTRACTOR SHALL PLACE PAVEMENT MARKING TAPE, TYPE III BETWEEN IL ROUTE 83 AND REX BOULEVARD AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) EAST OF REX BOULEVARD AND ALONG WEST AVENUE. WHEN SWITCHING THE LANE CONFIGURATIONS TO STAGE 2, THE CONTRACTOR SHALL REMOVE THE PAVEMENT MARKING TAPE BETWEEN IL ROUTE 83 AND REX BOULEVARD AND REPLACE THOSE MARKINGS WITH TEMPORARY PAVEMENT MARKINGS (PAINT). THE EXISTING TEMPORARY PAVEMENT MARKINGS (PAINT) THAT WERE PLACED IN STAGE 1 SHALL REMAIN IN PLACE DURING STAGE 2.

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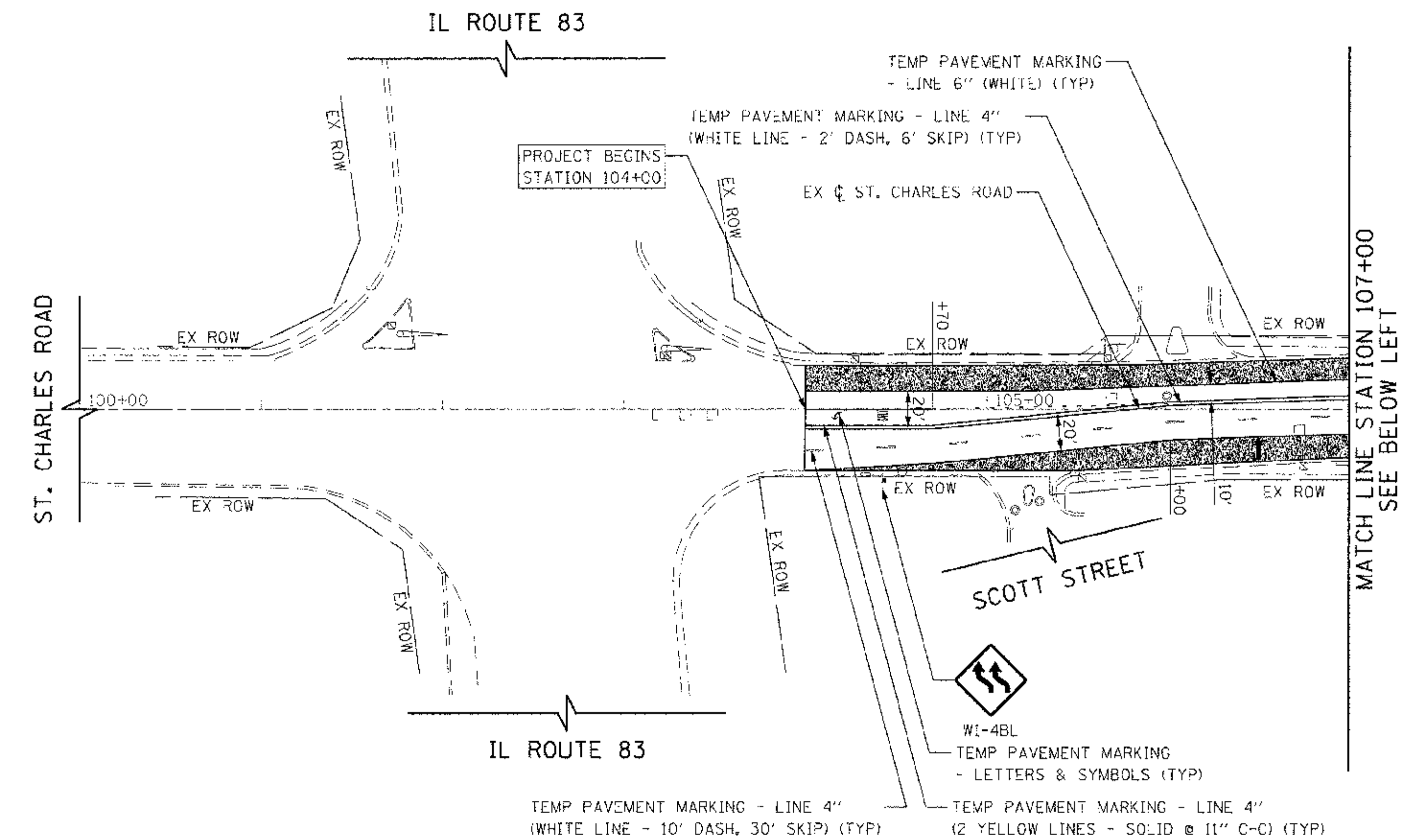
#MODELNAME#		DATE - 04/03/2013	REVISED -

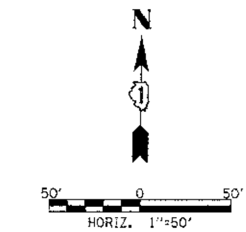
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD RESURFACING
SUGGESTED TRAFFIC CONTROL PLAN (STAGE 1)

SCALE: 1"=50' | SHEET 2 OF 4 SHEETS | STA. 117+00 TO STA. 127+08

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	9
		CONTRACT NO. 63833		
	ILLINOIS FED. AID PROJECT			

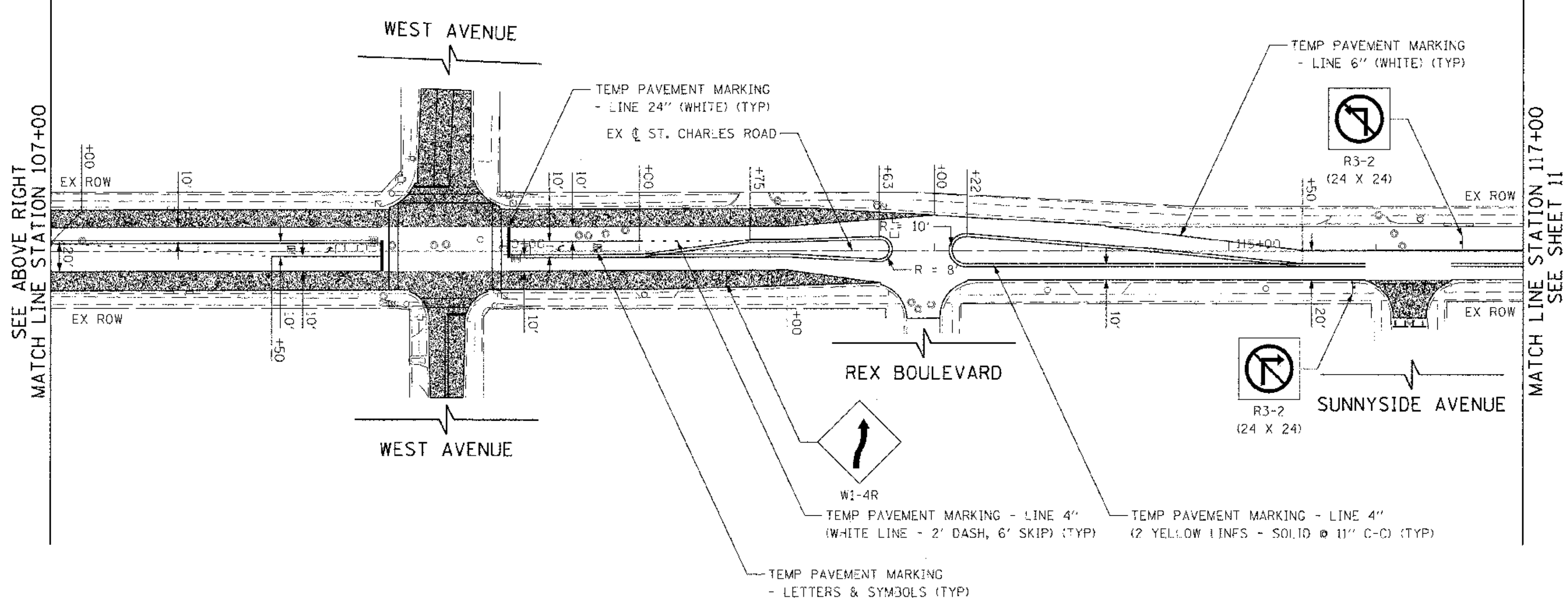


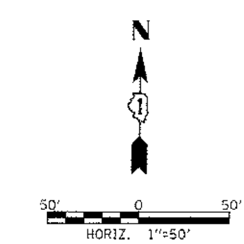


LEGEND

∘	CONE, DRUM OR BARRICADE (NOT REQUIRED FOR MOVING OPERATION)
⊥⊥	TYPE III BARRICADE WITH FLASHING LIGHTS AND ROAD CLOSED SIGN
↑	ARROW BOARD
⊢	SIGN ON PORTABLE OR PERMANENT SUPPORT
▨	WORK ZONE AREA

NOTES: 1) DURING STAGE 1, THE CONTRACTOR SHALL PLACE PAVEMENT MARKING TAPE, TYPE III BETWEEN IL ROUTE 83 AND REX BOULEVARD AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) EAST OF REX BOULEVARD AND ALONG WEST AVENUE. WHEN SWITCHING THE LANE CONFIGURATIONS TO STAGE 2, THE CONTRACTOR SHALL REMOVE THE PAVEMENT MARKING TAPE BETWEEN IL ROUTE 83 AND REX BOULEVARD AND REPLACE THOSE MARKINGS WITH TEMPORARY PAVEMENT MARKINGS (PAINT). THE EXISTING TEMPORARY PAVEMENT MARKINGS (PAINT) THAT WERE PLACED IN STAGE 1 SHALL REMAIN IN PLACE DURING STAGE 2.





LEGEND

∘	CONE, DRUM OR BARRICADE (NOT REQUIRED FOR MOVING OPERATION)
⊥⊥	TYPE III BARRICADE WITH FLASHING LIGHTS AND ROAD CLOSED SIGN
↑	ARROW BOARD
⊢	SIGN ON PORTABLE OR PERMANENT SUPPORT
▨	WORK ZONE AREA

FILE NAME = O:\C-12\0080\Road\Sheets\0080-10-MOT-STAGE2-1.dgn	USER NAME = USER	DESIGNED - JLB	REVISED -
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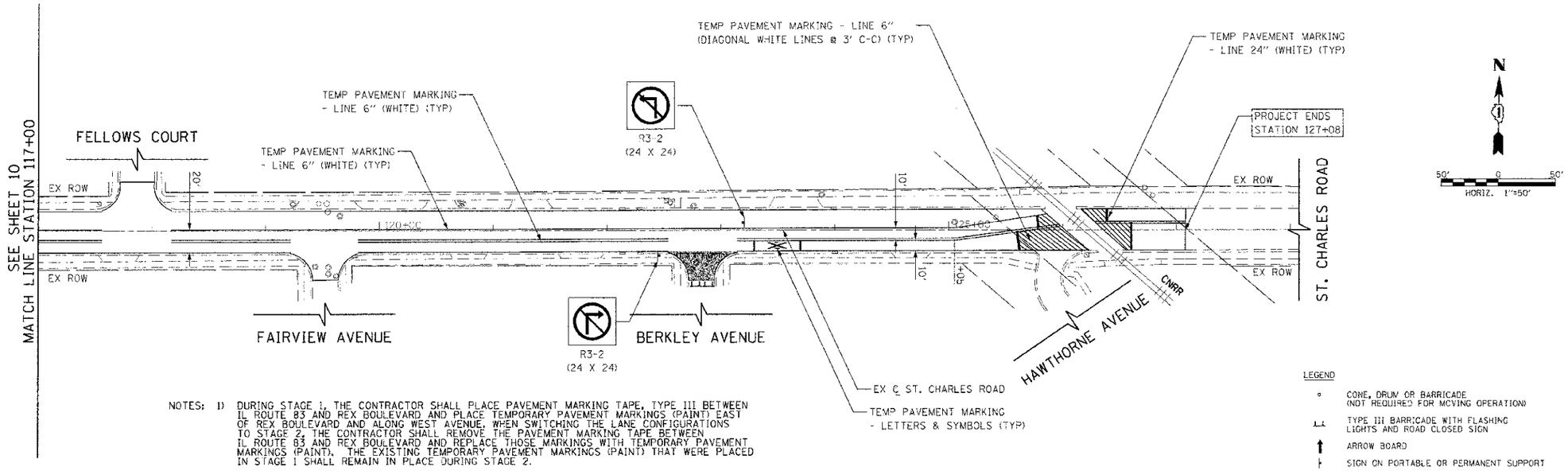
$MODELNAME$	PLOT DATE = 4/3/2013	DATE - 04/03/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD RESURFACING
SUGGESTED TRAFFIC CONTROL PLAN (STAGE 2)

SCALE: 1"=50' | SHEET 3 OF 4 SHEETS | STA. 104+00 TO STA. 117+00

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	10
		CONTRACT NO. 63833		
	ILLINOIS FED. AID PROJECT			



NOTES: 1) DURING STAGE 1, THE CONTRACTOR SHALL PLACE PAVEMENT MARKING TAPE, TYPE III BETWEEN IL ROUTE 83 AND REX BOULEVARD AND PLACE TEMPORARY PAVEMENT MARKINGS (PAINT) EAST OF REX BOULEVARD AND ALONG WEST AVENUE, WHEN SWITCHING THE LANE CONFIGURATIONS TO STAGE 2. THE CONTRACTOR SHALL REMOVE THE PAVEMENT MARKING TAPE BETWEEN IL ROUTE 83 AND REX BOULEVARD AND REPLACE THOSE MARKINGS WITH TEMPORARY PAVEMENT MARKINGS (PAINT). THE EXISTING TEMPORARY PAVEMENT MARKINGS (PAINT) THAT WERE PLACED IN STAGE 1 SHALL REMAIN IN PLACE DURING STAGE 2.

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$MODELNAME$	PLOT DATE = 4/3/2013	DATE - 04/03/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD RESURFACING
SUGGESTED TRAFFIC CONTROL PLAN (STAGE 2)

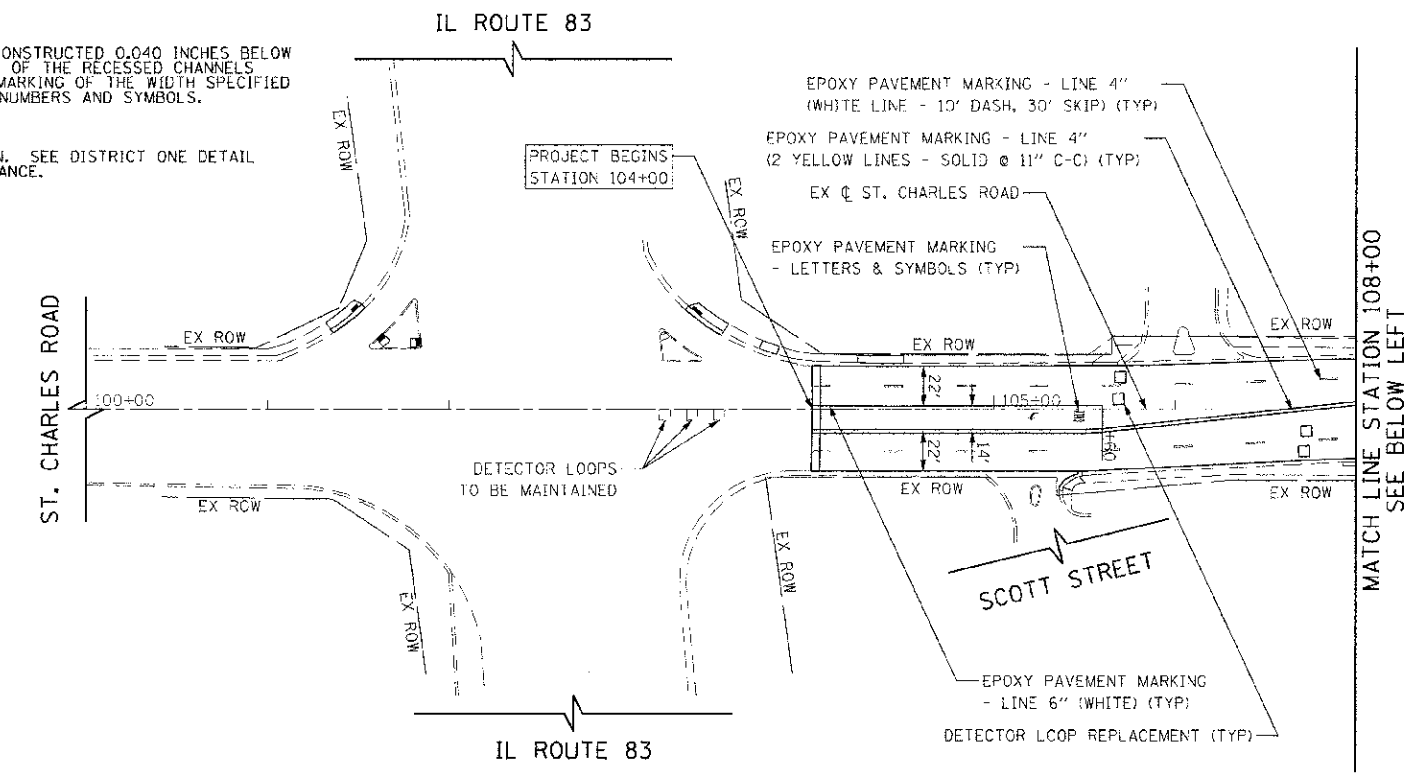
SCALE: 1"=50' | SHEET 4 OF 4 SHEETS | STA. 117+00 TO STA. 127+08

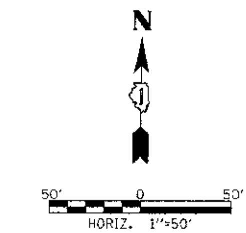
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	11
		CONTRACT NO. 63833		
	ILLINOIS FED. AID PROJECT			

NOTES: 1) EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.

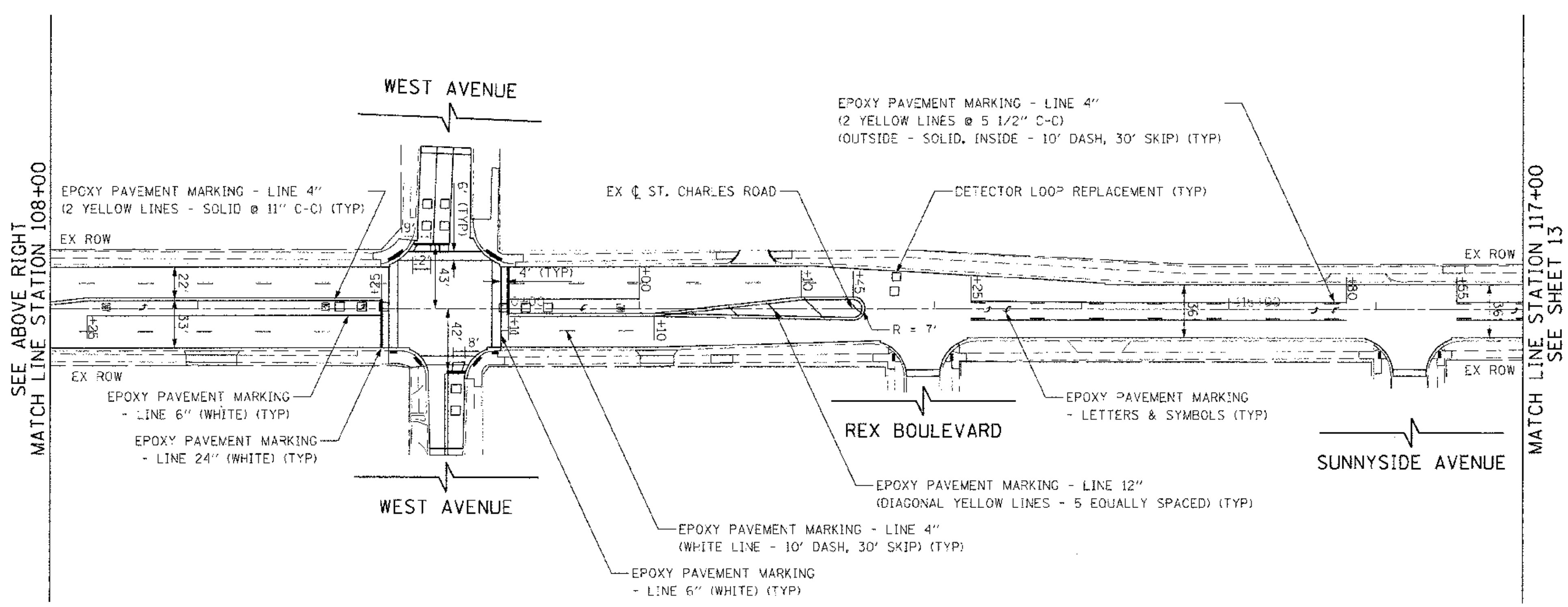
2) SEE DISTRICT ONE DETAIL TC-13 TYPICAL PAVEMENT MARKINGS FOR GUIDANCE.

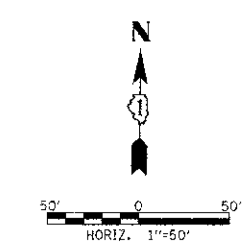
3) ALL DETECTOR LOOPS SHALL BE DIMENSIONED AS 6' BY 6' UNLESS OTHERWISE SHOWN. SEE DISTRICT ONE DETAIL TS-07 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING FOR GUIDANCE.





PAVEMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS
EPOXY PAVEMENT MARKING - LINE 4"	GROOVING FOR RECESSED PAVEMENT MARKING 5"
EPOXY PAVEMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAVEMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAVEMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"



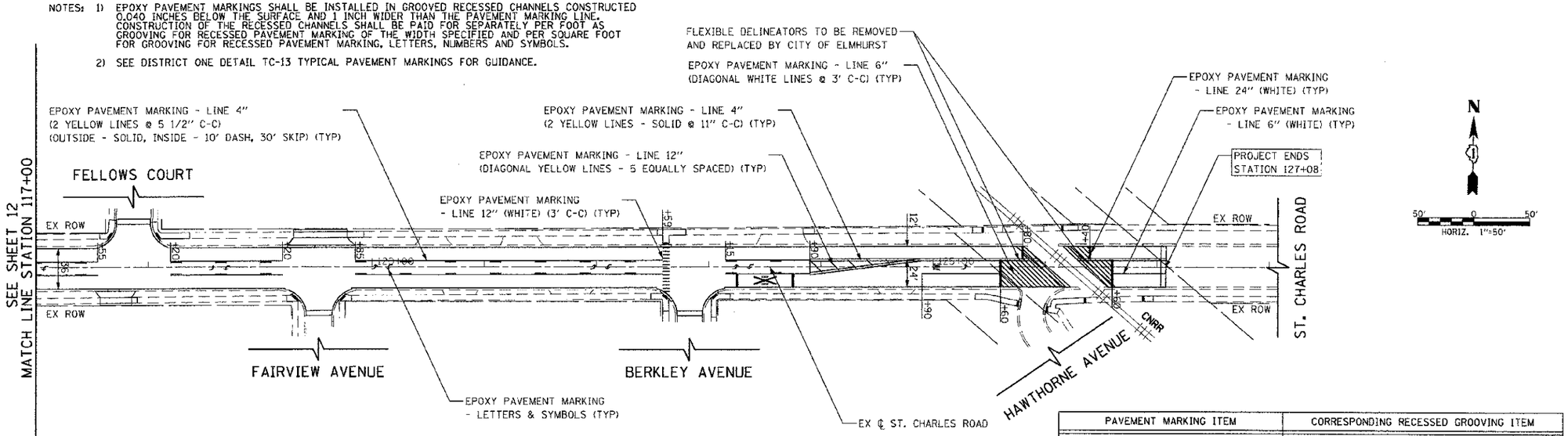


FILE NAME = G:\CI\2\0090\Road\Sheets\0000-12-PM-1.dgn	USER NAME = USER.	DESIGNED - JLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. CHARLES ROAD RESURFACING PAVEMENT MARKING PLAN
	PLOT SCALE = 50.020 ' / in.	DRAWN - JLB	REVISED -		SCALE: 1"=50'
	PLOT DATE = 4/3/2013	CHECKED - DWB	REVISED -		SHEET 1 OF 2 SHEETS
$MODELNAME$		DATE - 04/03/2013	REVISED -		STA. 104+00 TO STA. 117+00

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	12
		CONTRACT NO. 63833		
ILLINOIS FED. AID PROJECT				

NOTES: 1) EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.

2) SEE DISTRICT ONE DETAIL TC-13 TYPICAL PAVEMENT MARKINGS FOR GUIDANCE.



PAVEMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS
EPOXY PAVEMENT MARKING - LINE 4"	GROOVING FOR RECESSED PAVEMENT MARKING 5"
EPOXY PAVEMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAVEMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAVEMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"

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$MODELNAME$	PLOT DATE = 4/3/2013	DATE 04/03/2013	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD RESURFACING
PAVEMENT MARKING PLAN

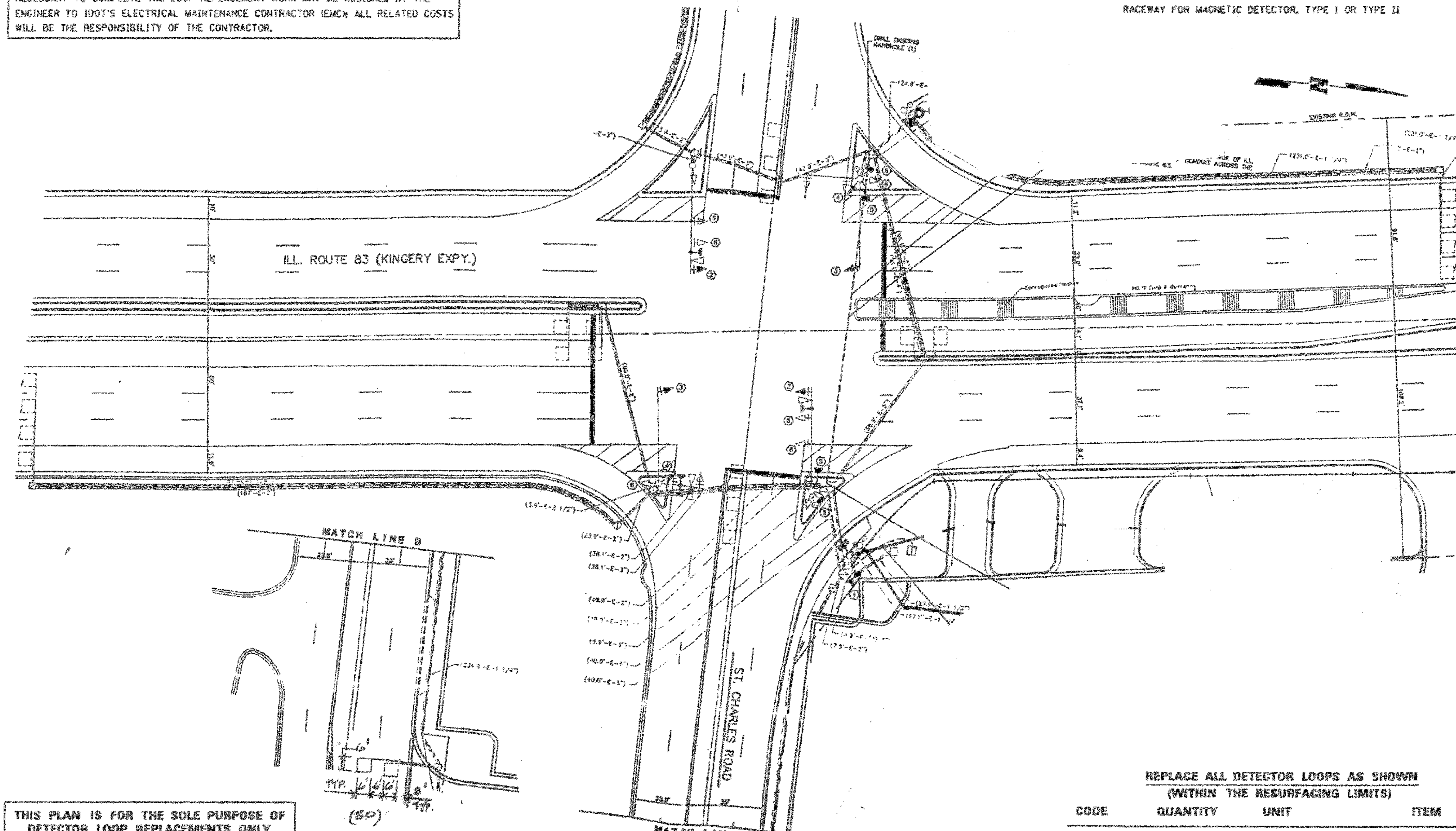
SCALE: 1"=50' | SHEET 2 OF 2 SHEETS | STA. 117+00 | TO STA. 127+08

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	13
		CONTRACT NO. 63833		
		ILLINOIS FED. AID PROJECT		

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		-|▷
SIGNAL HEAD		-▷
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	- - - -	———
DETECTOR LOOP	□	⬚
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		▭—
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		═══ "C"



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	80	FOOT	DETECTOR LOOP, REPLACEMENT

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m:\pw.work\pwidot\plawcroom\d0112618\f...sher.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/22/2011	DATE -	REVISED -

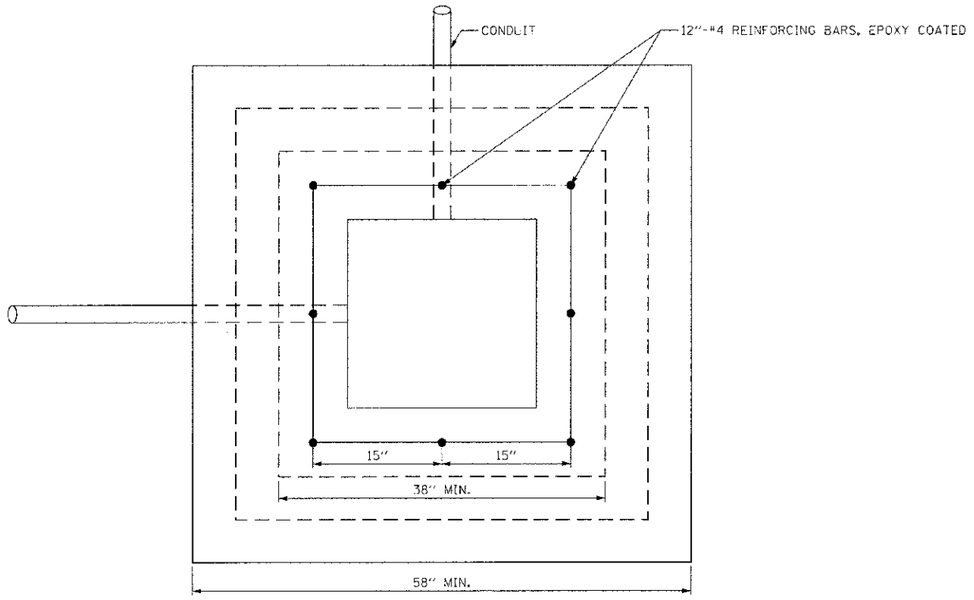
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE – DETECTOR LOOP REPLACEMENT
ILL. ROUTE 83 @ ST. CHARLES RD.

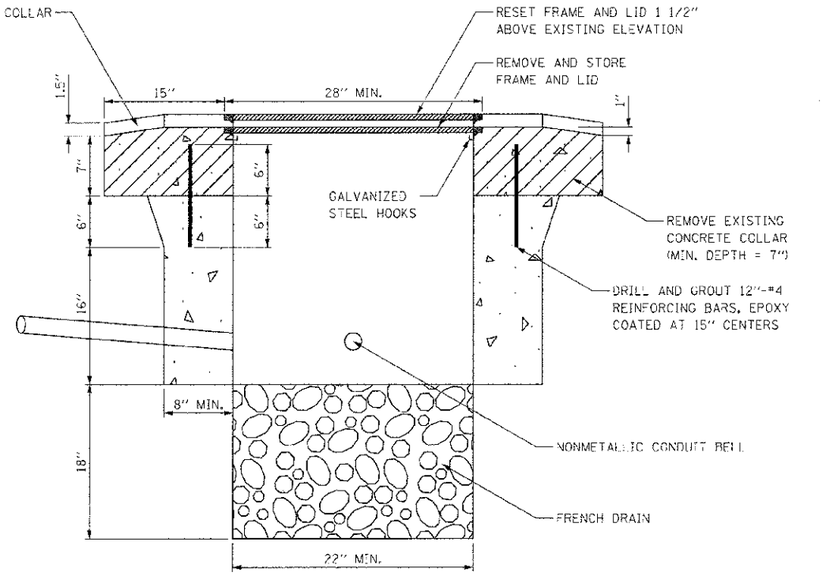
SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	13A
		CONTRACT NO. 63833		

FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT

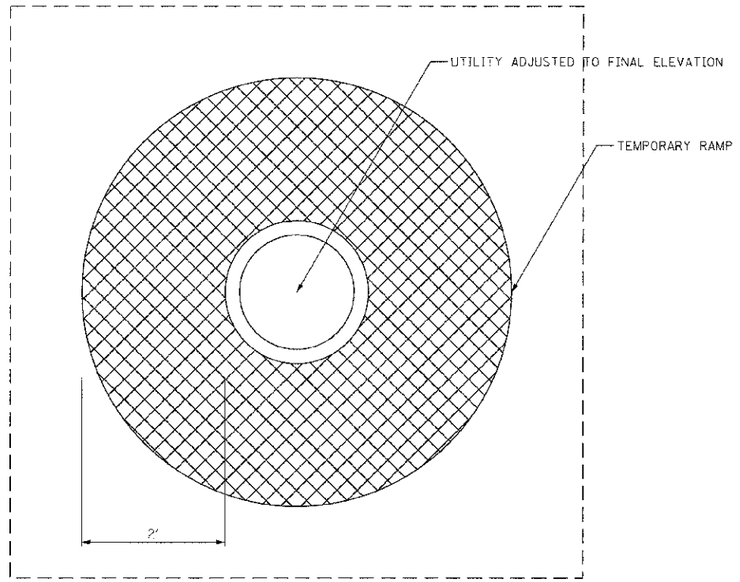


PLAN

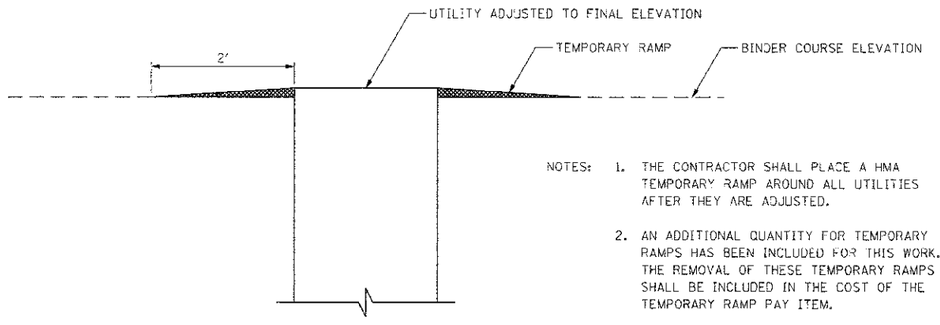


ELEVATION

HEAVY DUTY HANDHOLES TO BE ADJUSTED



PLAN



NOTES:
1. THE CONTRACTOR SHALL PLACE A HMA TEMPORARY RAMP AROUND ALL UTILITIES AFTER THEY ARE ADJUSTED.
2. AN ADDITIONAL QUANTITY FOR TEMPORARY RAMPS HAS BEEN INCLUDED FOR THIS WORK. THE REMOVAL OF THESE TEMPORARY RAMPS SHALL BE INCLUDED IN THE COST OF THE TEMPORARY RAMP PAY ITEM.

ELEVATION

TEMPORARY RAMPS

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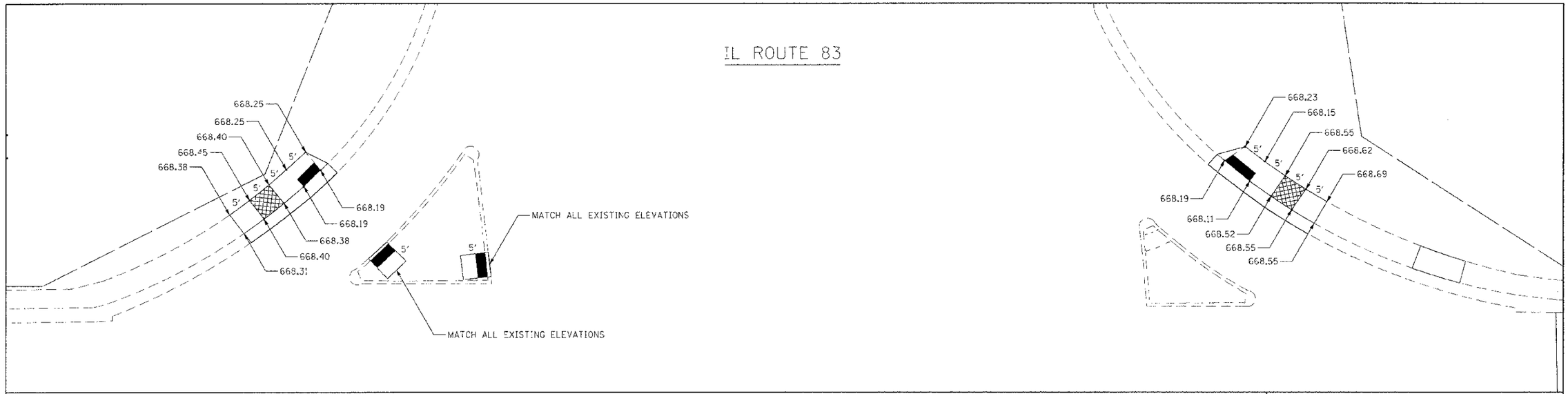
#MODELNAME#	PLOT DATE = 4/3/2013	DATE - 04/03/2013	REVISED -

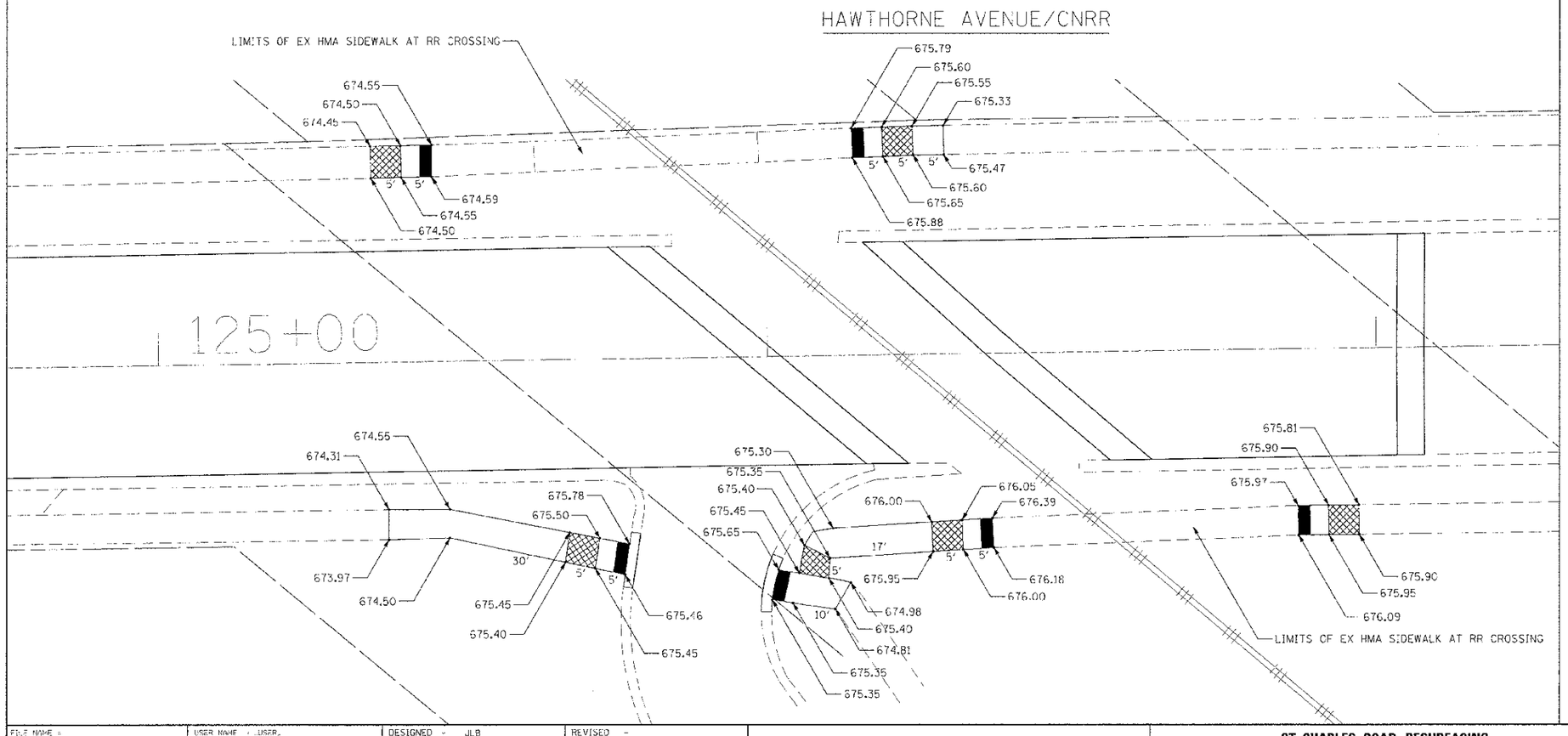
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD RESURFACING
PROJECT DETAILS

SCALE: NTS | SHEET 1 OF 3 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	14
		CONTRACT NO. 63833		
		ILLINOIS FED. AID PROJECT		





NOTES: 1) THE CONTRACTOR SHALL USE THE EXISTING SIDEWALK ELEVATIONS AT EACH INTERSECTION CORNER AS THE VERTICAL CONTROL FOR CONSTRUCTING THE PROPOSED ELEVATIONS AT THE RESPECTIVE LOCATION. NO OTHER TEMPORARY BENCHMARKS ARE PROVIDED.

LEGEND:
- DETECTABLE WARNINGS
- ADA LANDING AREA

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$MODELNAME$	PLOT DATE = 4/3/2013	DATE - 04/03/2013	REVISED -

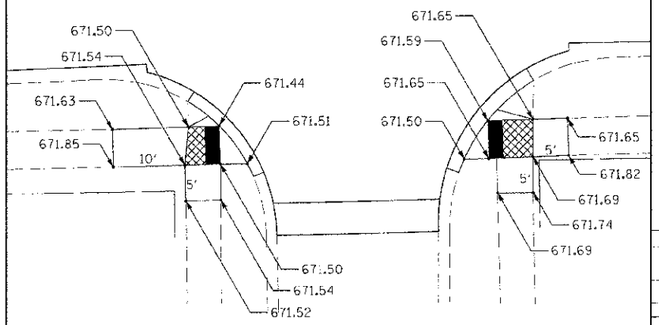
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD RESURFACING
CURB RAMP AND DETECTABLE WARNING DETAILS

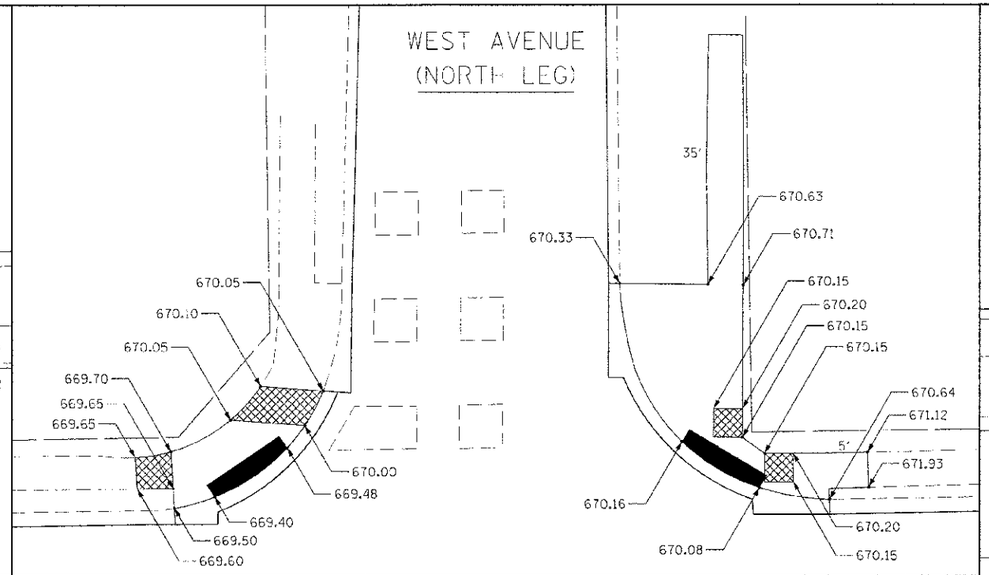
SCALE: 1"=10' | SHEET 2 OF 3 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-03-RS	DUPAGE	26	15
		CONTRACT NO. 63833		
	ILLINOIS FED. AID PROJECT			

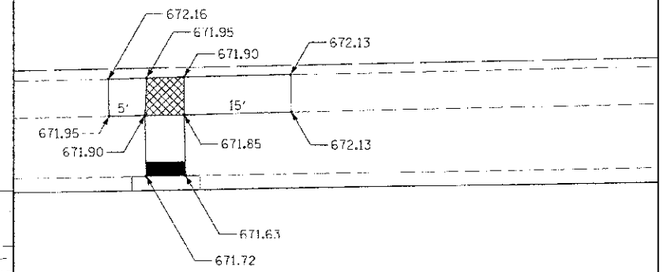
REX BOULEVARD



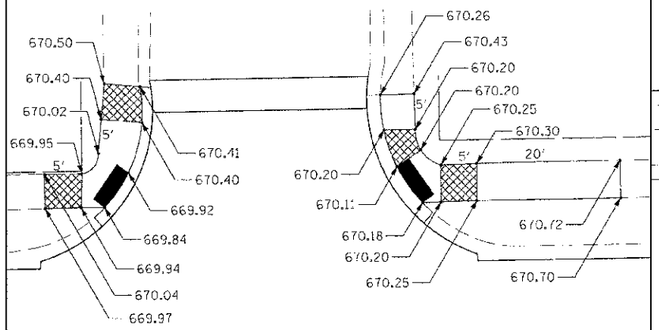
WEST AVENUE (NORTH LEG)



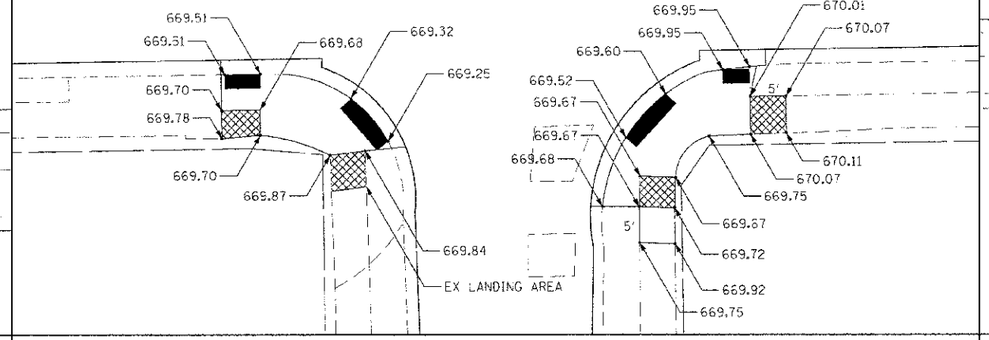
BERKLEY AVENUE

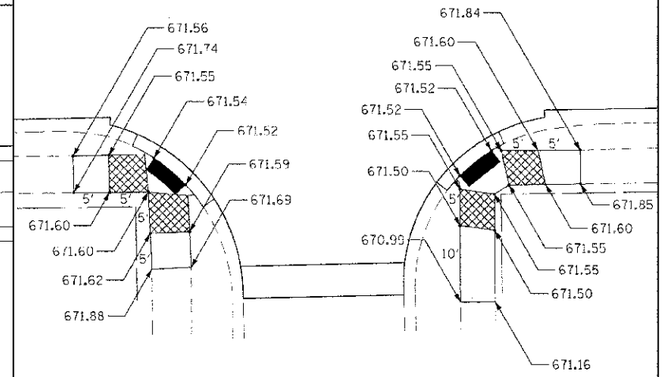


FELLOWS COURT

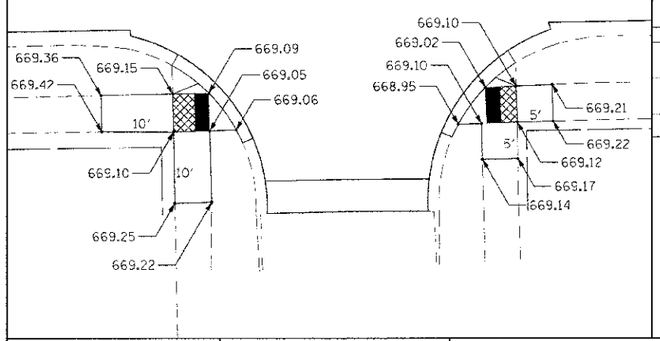


WEST AVENUE (SOUTH LEG)

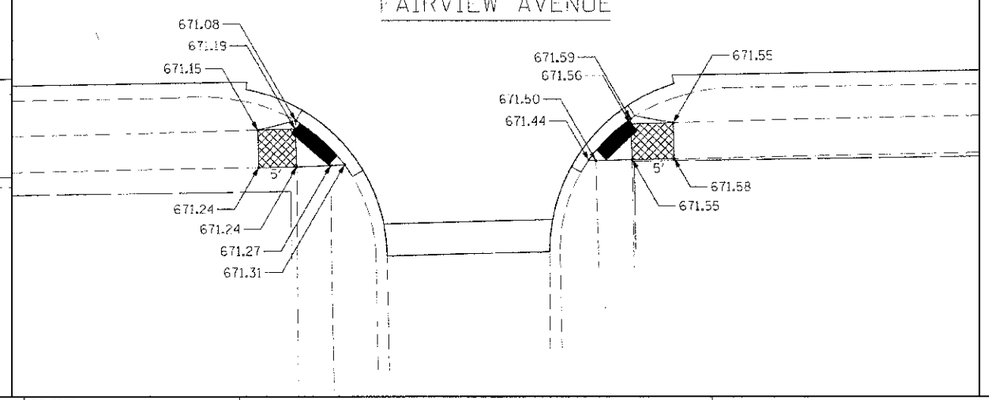




SUNNYSIDE AVENUE



FAIRVIEW AVENUE



NOTES: 1) THE CONTRACTOR SHALL USE THE EXISTING SIDEWALK ELEVATIONS AT EACH INTERSECTION CORNER AS THE VERTICAL CONTROL FOR CONSTRUCTING THE PROPOSED ELEVATIONS AT THE RESPECTIVE LOCATION. NO OTHER TEMPORARY BENCHMARKS ARE PROVIDED.

LEGEND: DETECTABLE WARNINGS

ADA LANDING AREA

FILE NAME =	USER NAME = USER	DESIGNED - JLB	REVISED -
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	PLOT SCALE = 50.000 ' / in.	CHECKED - DWB	REVISED -
#MODELNAME#	PLOT DATE = 4/3/2013	DATE - 04/03/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

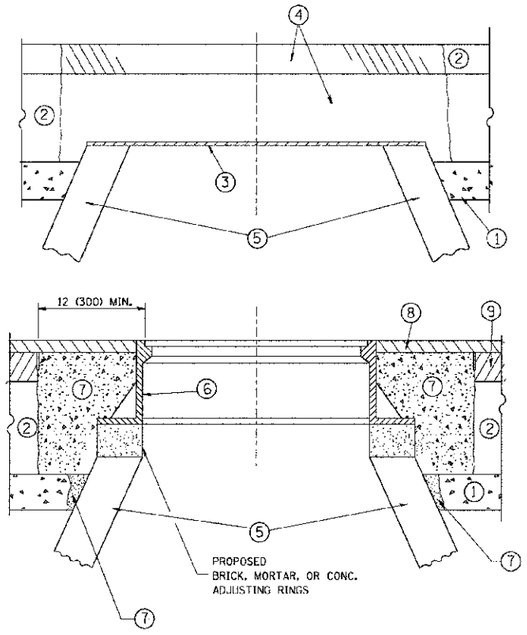
ST. CHARLES ROAD RESURFACING
CURB RAMP AND DETECTABLE WARNING DETAILS

SCALE: 1"=10' | SHEET 3 OF 3 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	16

CONTRACT NO. 63833

ILLINOIS FED. AID PROJECT



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-1* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

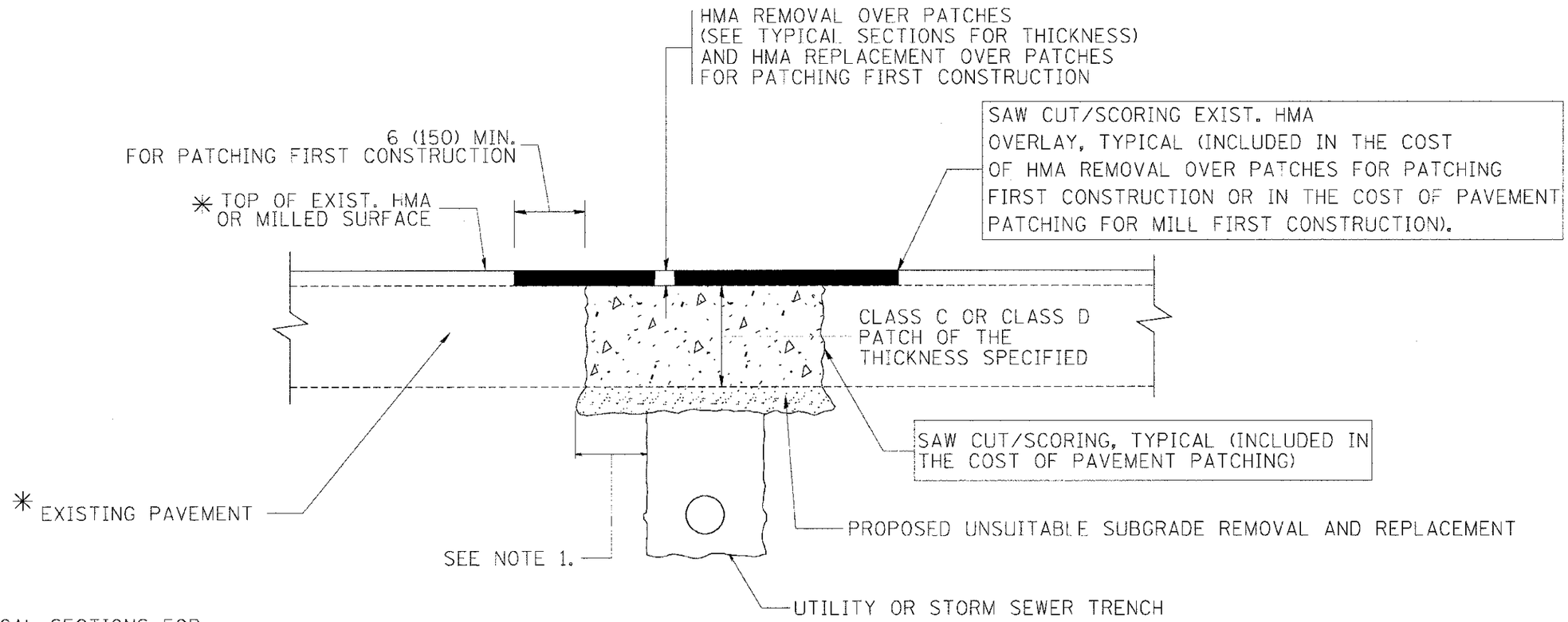
FILE NAME = c:\pw_work\pwidot\bauerdl\d0186315\bd08.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1988.5000 ' / in	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	17
	BD600-03 (BD-8)	CONTRACT NO. 63833		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

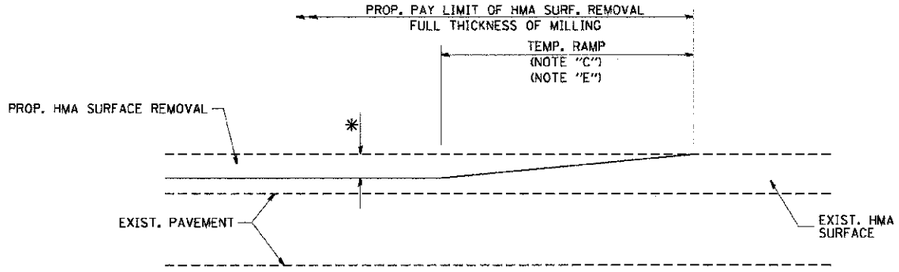
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\proj_pcts\dists\d22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
		DRAWN -	REVISED - R. BORO 01-01-07		
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - R. BORO 09-04-07		
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

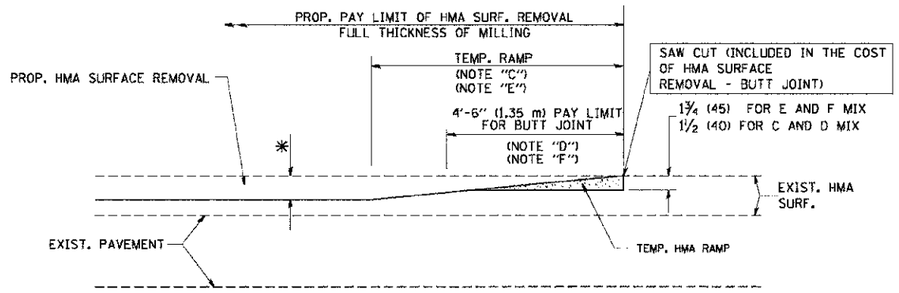
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	18
	BD400-04 (BD-22)	CONTRACT NO. 63833		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

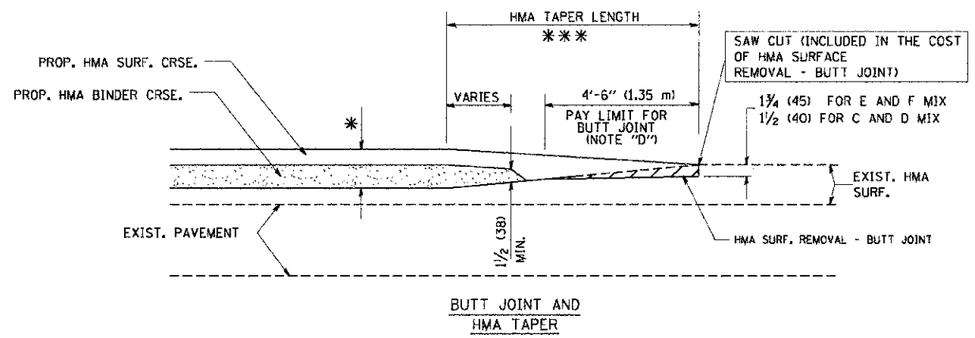


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

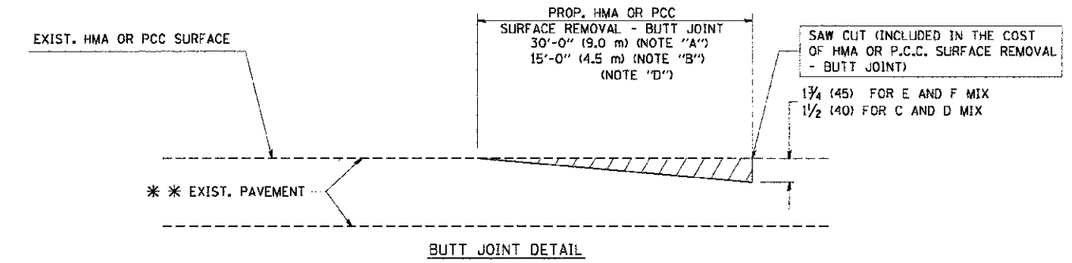
OPTION 2

TYPICAL TEMPORARY RAMP

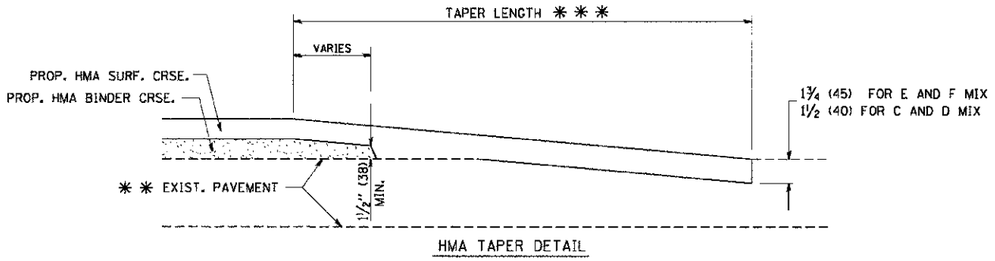


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

* * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

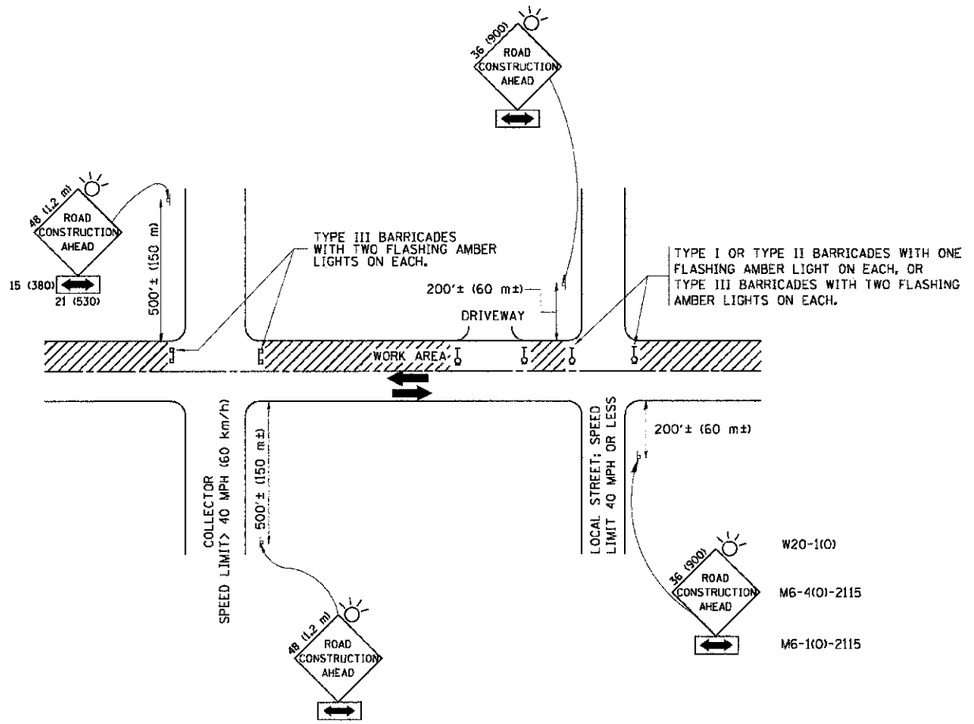
BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = w:\dgnstd\22x34\bd32.dgn	USER NAME = geglenabt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS
		DRAWN -	REVISED - A. ABBAS 03-21-97		
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA.
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07		

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	19
	BD400-05 BD32	CONTRACT NO. 63833		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (W6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (W6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

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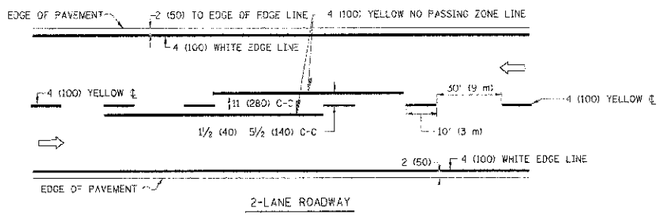
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 '/ IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2000	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

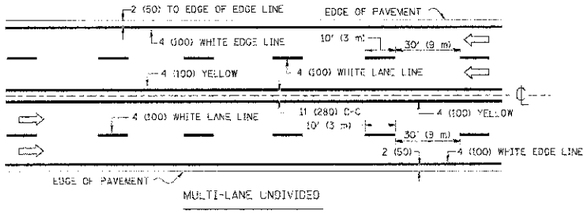
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

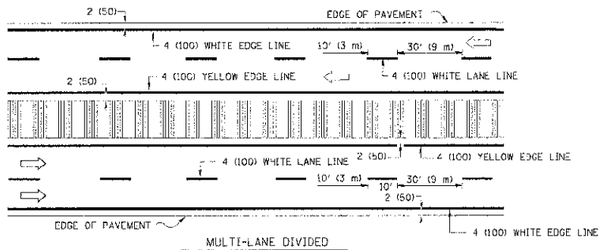
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	20
	TC-10	CONTRACT NO. 63833		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



2-LANE ROADWAY



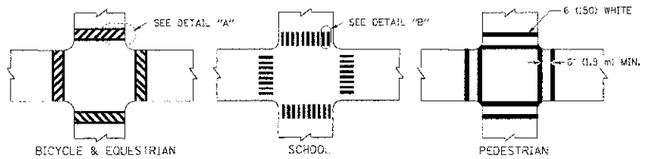
MULTI-LANE UNDIVIDED

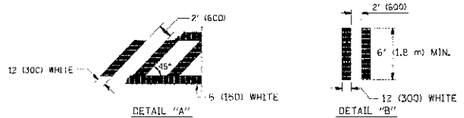


MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

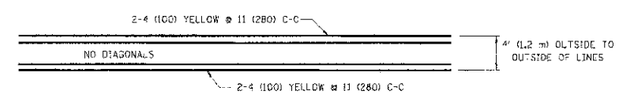
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

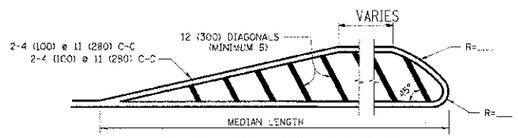




TYPICAL CROSSWALK MARKING



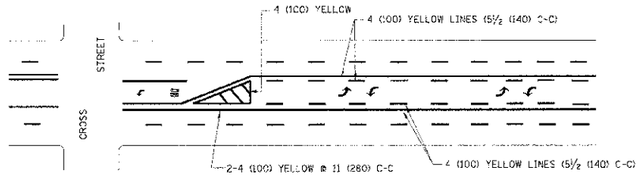
4' (1.2 m) WIDE MEDIANS ONLY



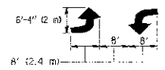
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

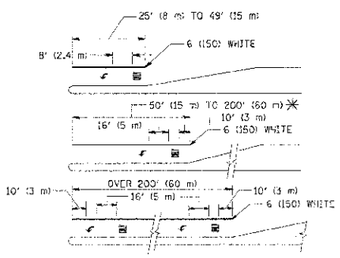


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED. WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

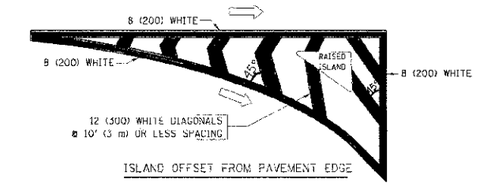


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

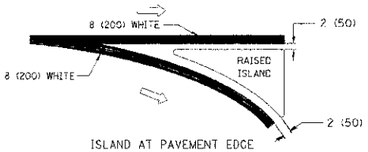
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "RR"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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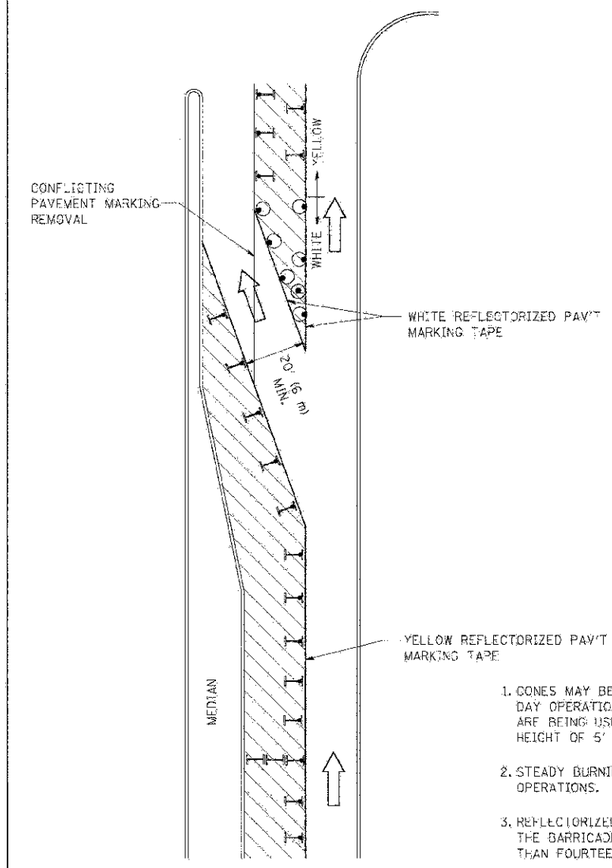
		DRAWN	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50.000 '/ IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

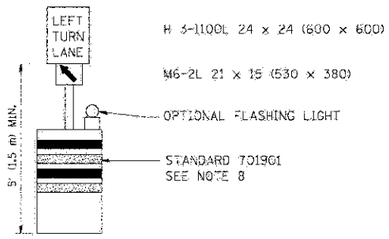
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TC STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	21
	TC-13	CONTRACT NO. 63833		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		





GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED (TEMPORARY PAVEMENT) MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-109 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

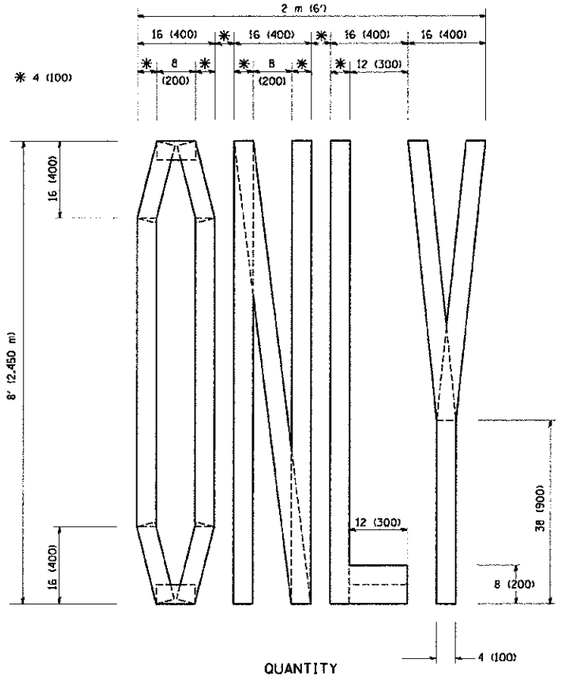
FILE NAME =	USER NAME = dravsh.origin	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
c:\pw_work\DPW\D0179\D0179D01\Model\tc14.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 1.9999 ' / IN.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 5/14/2009	REVISED - T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

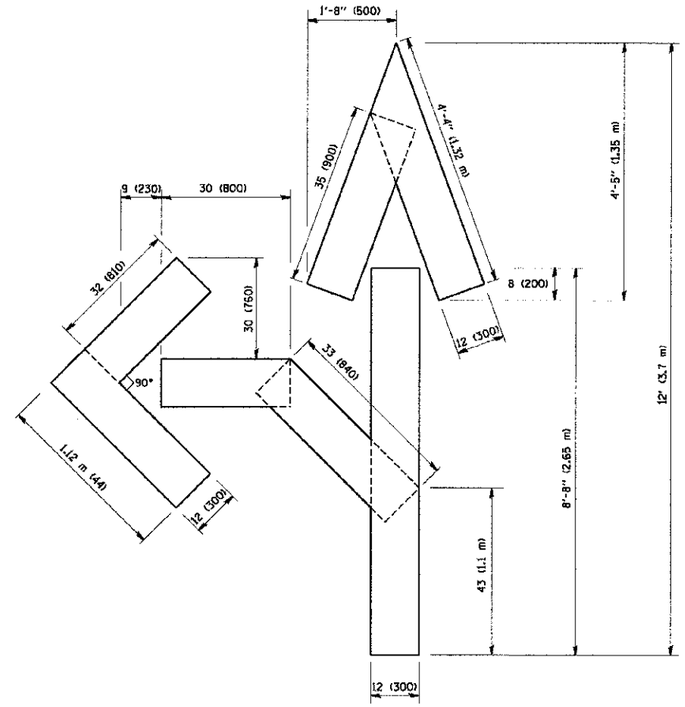
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TC STA.

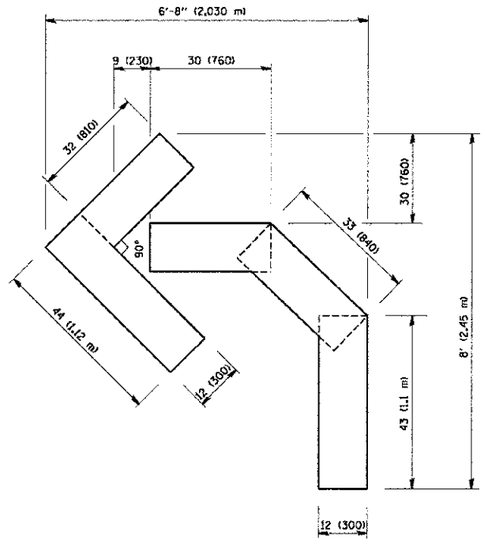
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	22
	TC-14	CONTRACT NO. 63833		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



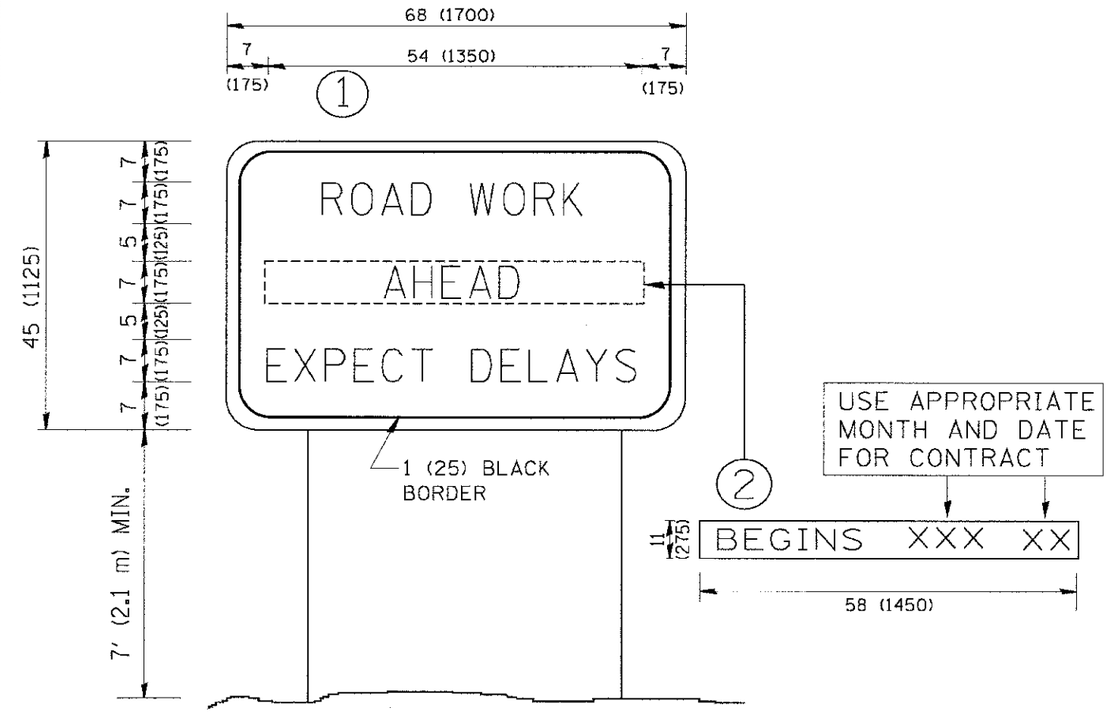
QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\tc16.dgn	USER NAME = gegliamobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING	F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -T. RAMMACHER 11-04-97			1397	13-00181-00-RS	DUPAGE	26	23
	PLOT SCALE = 50.2030 ' / IN.	CHECKED	REVISED T. RAMMACHER 03-02-98				TC-16	CONTRACT NO. 63833		
	PLOT DATE = 7/4/2038	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

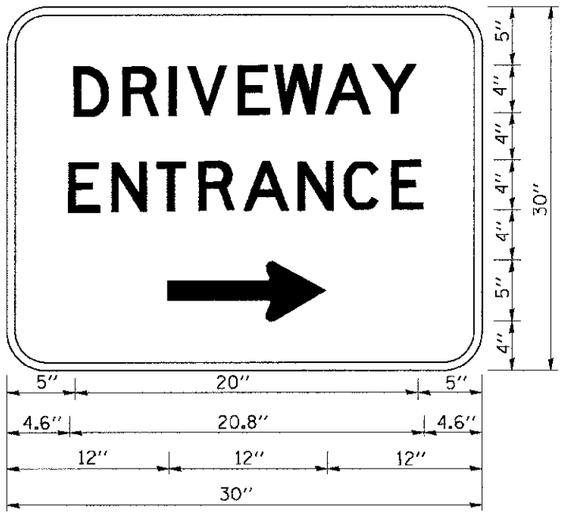
FILE NAME = W:\diststd\22x34\to22.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-16-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	24
	TC-22	CONTRACT NO. 63833		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

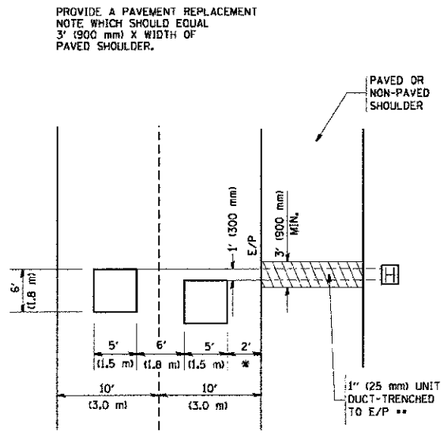
NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\diastd\22x34\tc26.dgn	USER NAME = gaghannbt	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING
		DRAWN -	REVISED -		
	PLOT SCALE = 60.000 ' / IN.	CHECKED -	REVISED		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA.
	PLOT DATE = 1/4/2008	DATE -	REVISED -		

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00101-00-RS	DUPAGE	26	25
	TC-26	CONTRACT NO. 63833		
FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

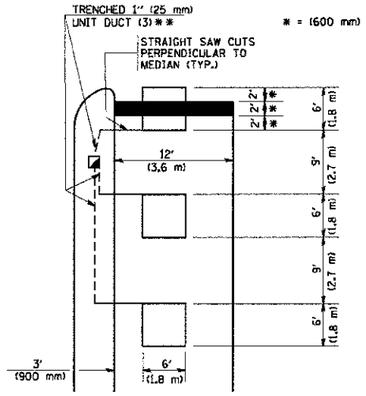


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

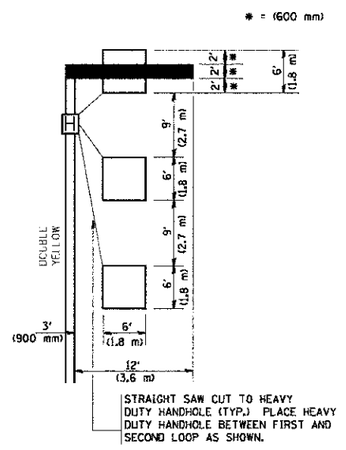
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

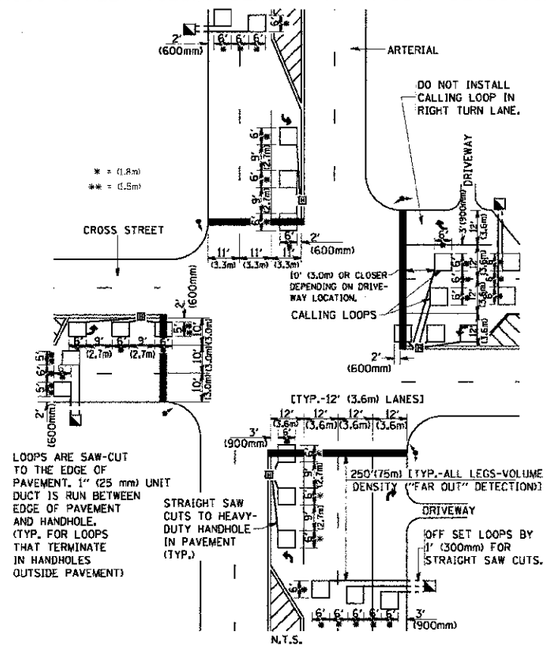
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



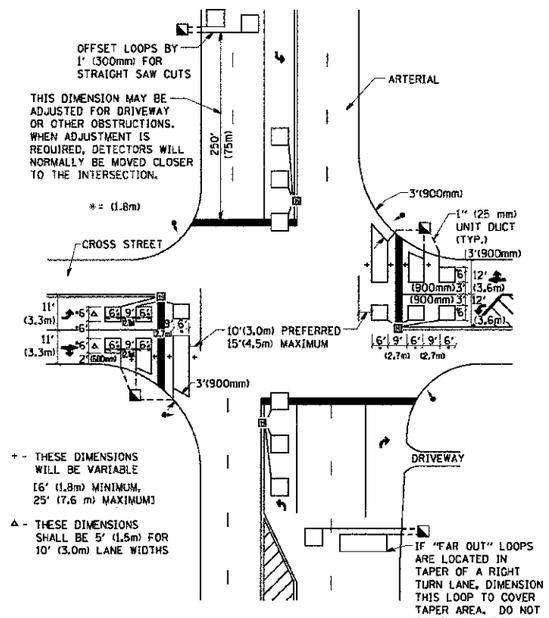
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn	USER NAME = gaglianobr	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - R.K.F.	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	13-00181-00-RS	DUPAGE	26	26
	TS-07	CONTRACT NO. 63833		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		