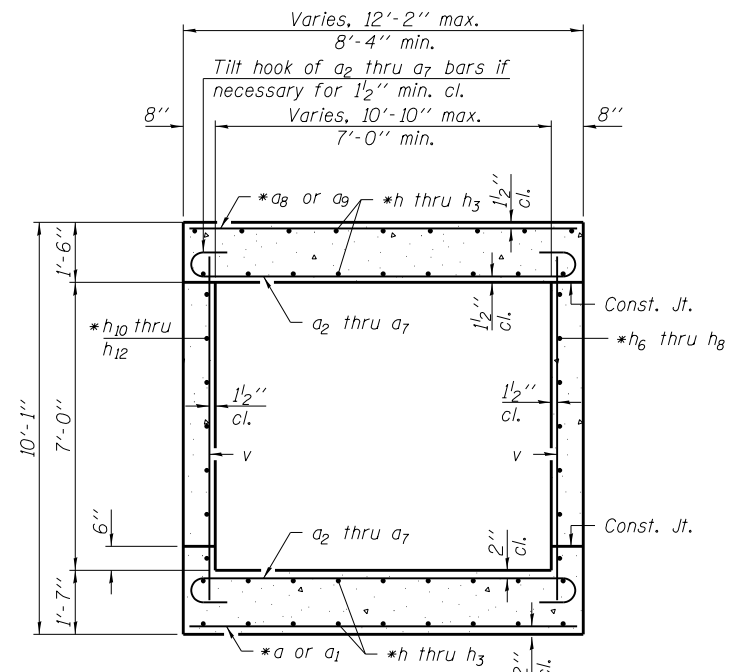
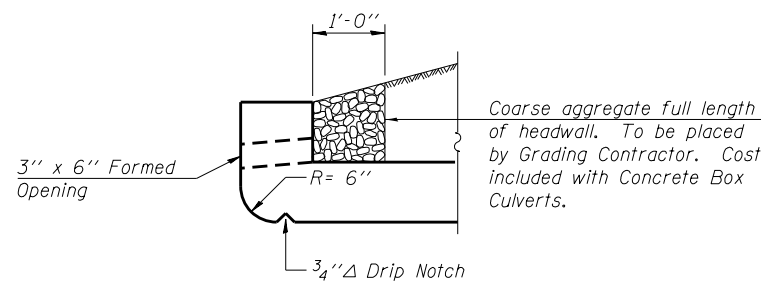


**LONGITUDINAL SECTION**  
(36" φ pipe not shown for clarity, see detail)

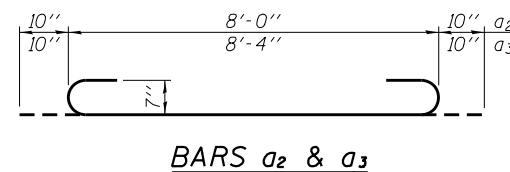


**SECTION THRU BARREL**  
(Looking downstream)

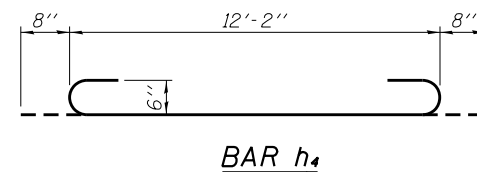
- \* Cut or bend bars to fit.
- \*\* Elev. 616.87, N. Corner
- Elev. 617.31, S. Corner
- \*\*\* 8-#5 h<sub>6</sub> bars at 12" cts., N. Wall
- 8-#5 h<sub>10</sub> bars at 12" cts., S. Wall
- \*\*\*\* 8-#5 h<sub>7</sub> bars at 12" cts., N. Wall
- 8-#5 h<sub>11</sub> bars at 12" cts., S. Wall
- \*\*\*\*\* 8-#5 h<sub>8</sub> bars at 12" cts., N. Wall
- 8-#5 h<sub>12</sub> bars at 12" cts., S. Wall



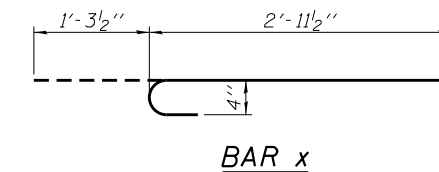
**DRAIN DETAIL AT UPSTREAM END**



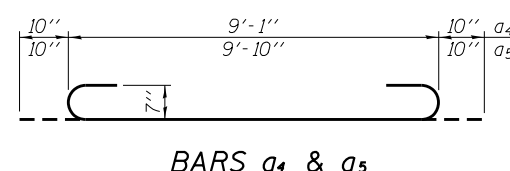
**BARS a<sub>2</sub> & a<sub>3</sub>**



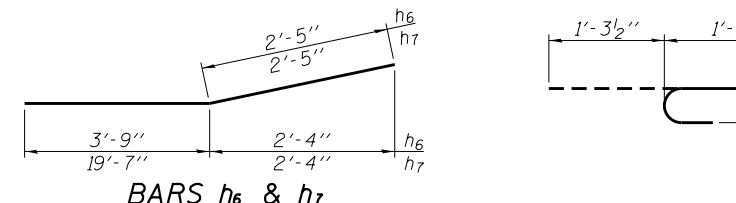
**BAR h<sub>4</sub>**



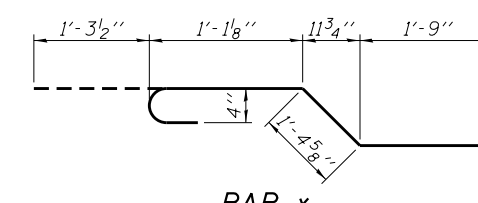
**BAR x**



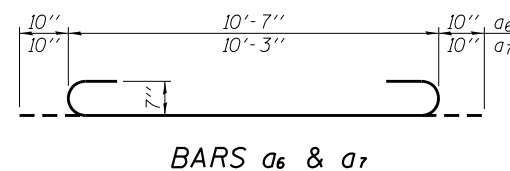
**BARS a<sub>4</sub> & a<sub>5</sub>**



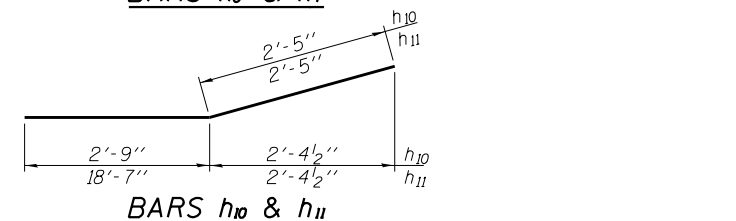
**BARS h<sub>6</sub> & h<sub>7</sub>**



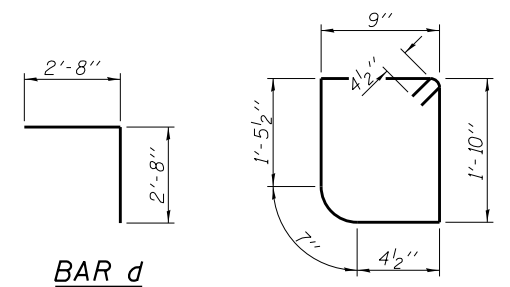
**BAR x<sub>1</sub>**



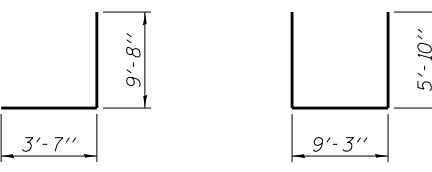
**BARS a<sub>6</sub> & a<sub>7</sub>**



**BARS h<sub>10</sub> & h<sub>11</sub>**



**BAR d**



**BAR v<sub>1</sub>**

**BAR x<sub>2</sub>**

**BAR s**

Notes:  
Cut bars to provide 2" min. clearance to pipe.  
For Plan of Sloped Tapered Inlet, see sheet 1 of 2.  
For flowline elevations not shown, see roadway cross-sections.

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a	12	#4	8'-0"	—
a <sub>1</sub>	5	#4	11'-0"	—
a <sub>2</sub>	58	#7	9'-8"	U
a <sub>3</sub>	4	#7	10'-0"	U
a <sub>4</sub>	4	#7	10'-9"	U
a <sub>5</sub>	4	#7	11'-6"	U
a <sub>6</sub>	4	#7	12'-3"	U
a <sub>7</sub>	4	#7	11'-11"	U
a <sub>8</sub>	16	#5	8'-0"	—
a <sub>9</sub>	7	#5	11'-0"	—
b	17	#6	39'-3"	—
d	27	#4	5'-4"	J
h	44	#5	4'-0"	—
h <sub>1</sub>	44	#5	30'-8"	—
h <sub>2</sub>	4	#5	3'-9"	—
h <sub>3</sub>	4	#5	5'-6"	—
h <sub>4</sub>	4	#6	13'-6"	U
h <sub>5</sub>	4	#6	11'-5"	—
h <sub>6</sub>	8	#5	6'-2"	—
h <sub>7</sub>	8	#5	22'-0"	—
h <sub>8</sub>	8	#5	7'-1"	—
h <sub>9</sub>	7	#4	13'-8"	—
h <sub>10</sub>	8	#5	5'-2"	—
h <sub>11</sub>	8	#5	21'-0"	—
h <sub>12</sub>	8	#5	11'-0"	—
h <sub>13</sub>	7	#4	23'-0"	—
s	12	#4	5'-9"	D
v	93	#5	9'-9"	—
v <sub>1</sub>	56	#6	13'-3"	J
x	20	#4	4'-3"	—
x <sub>1</sub>	18	#4	5'-6"	—
x <sub>2</sub>	6	#4	20'-11"	—
x <sub>3</sub>	8	#5	7'-6"	—
Reinforcement Bars	Pound	7,890		
Concrete Box Culverts	Cu. Yd.	68.0		

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

HORNER &  
SHIRIN, INC.  
ENGINEERS

SLOPED TAPERED INLET DETAILS  
CULVERT AT STA. 2074+48.31

SCALE: SHEET NO. 2 OF 2 SHEETS STA. 2074+05.94 TO STA. 2074+65.32

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
312	68-WRS-1	MONROE	760	443
CONTRACT NO. 76817			ILLINOIS FED. AID PROJECT	