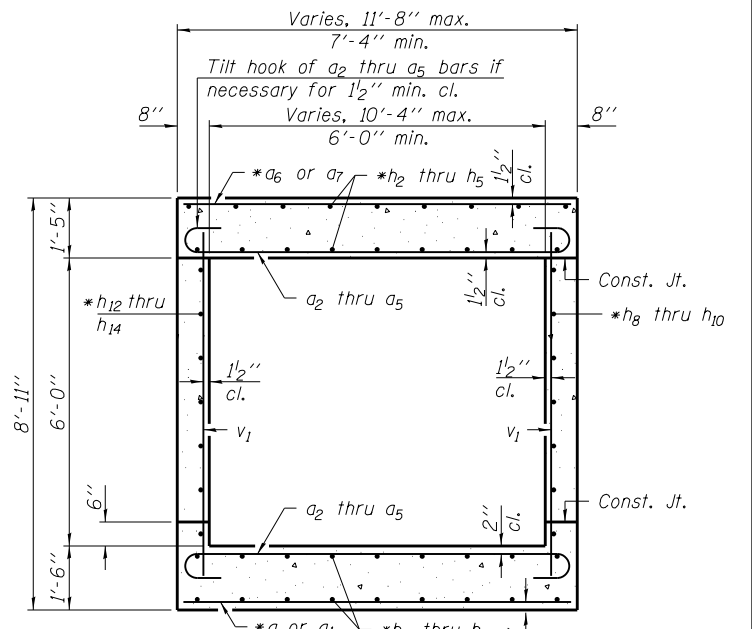


LONGITUDINAL SECTION
(24" ϕ pipe not shown for clarity, see detail)

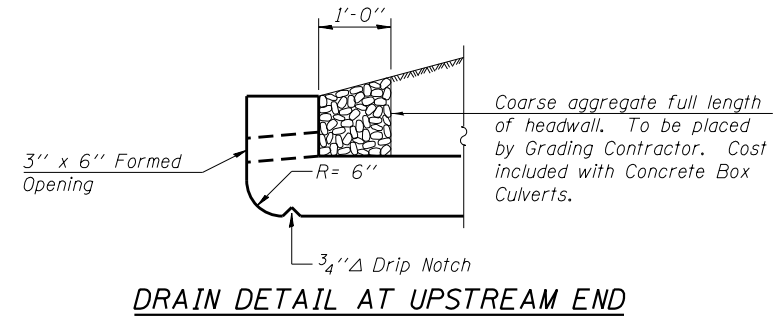


SECTION THRU BARREL
(Looking downstream)

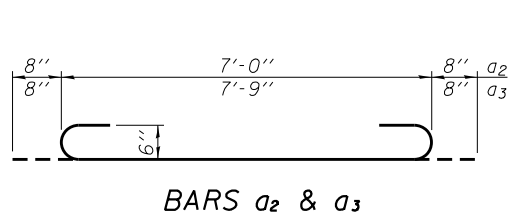
- * Cut or bend bars to fit.
- ** Elev. 622.81, N. Corner
Elev. 622.75, S. Corner
- *** 7-#5 h_8 bars at 12" cts., N. Wall
7-#5 h_{12} bars at 12" cts., S. Wall
- **** 7-#5 h_9 bars at 12" cts., N. Wall
7-#5 h_{13} bars at 12" cts., S. Wall
- ***** 7-#5 h_{10} bars at 12" cts., N. Wall
7-#5 h_{14} bars at 12" cts., S. Wall

BILL OF MATERIAL

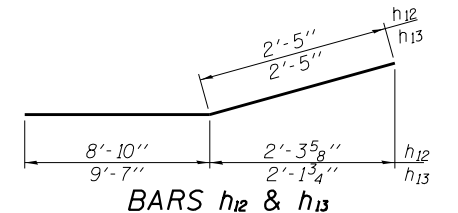
Bar	No.	Size	Length	Shape
a	10	#4	7'-0"	—
a ₁	6	#4	9'-6"	—
a ₂	52	#6	8'-4"	U
a ₃	4	#6	9'-1"	U
a ₄	6	#6	10'-1"	U
a ₅	10	#6	10'-6"	U
a ₆	13	#5	7'-0"	—
a ₇	8	#5	9'-4"	—
b	19	#5	32'-9"	—
b ₁	23	#6	45'-6"	—
d	54	#4	5'-4"	J
h	1	#4	43'-0"	—
h ₁	10	#4	17'-5"	—
h ₂	32	#5	9'-0"	—
h ₃	32	#5	20'-6"	—
h ₄	4	#5	6'-2"	—
h ₅	4	#5	2'-6"	—
h ₆	4	#7	16'-7"	U
h ₇	4	#6	14'-4"	—
h ₈	7	#5	10'-3"	—
h ₉	7	#5	11'-0"	—
h ₁₀	7	#5	12'-11"	—
h ₁₁	6	#4	22'-8"	—
h ₁₂	7	#5	11'-3"	—
h ₁₃	7	#5	12'-0"	—
h ₁₄	7	#5	4'-7"	—
h ₁₅	6	#4	20'-6"	—
s	2	#4	32'-1"	□
s ₁	15	#4	5'-9"	□
v	52	#5	10'-7"	J
v ₁	58	#5	8'-7"	—
v ₂	66	#6	12'-0"	J
x	16	#4	4'-3"	—
x ₁	16	#4	5'-6"	—
x ₂	6	#4	18'-9"	—
x ₃	4	#5	6'-4"	—
Reinforcement Bars		Pound	8,200	
Concrete Box Culverts		Cu. Yd.	74.8	



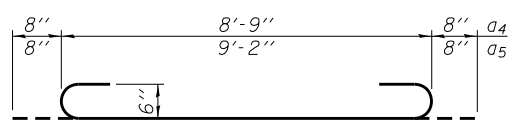
DRAIN DETAIL AT UPSTREAM END



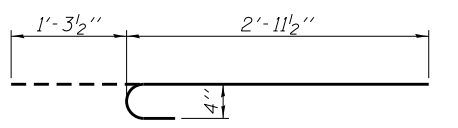
BARS a₂ & a₃



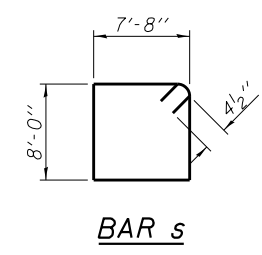
BARS h₁₂ & h₁₃



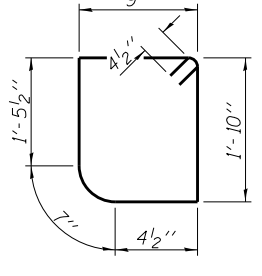
BARS a₄ & a₅



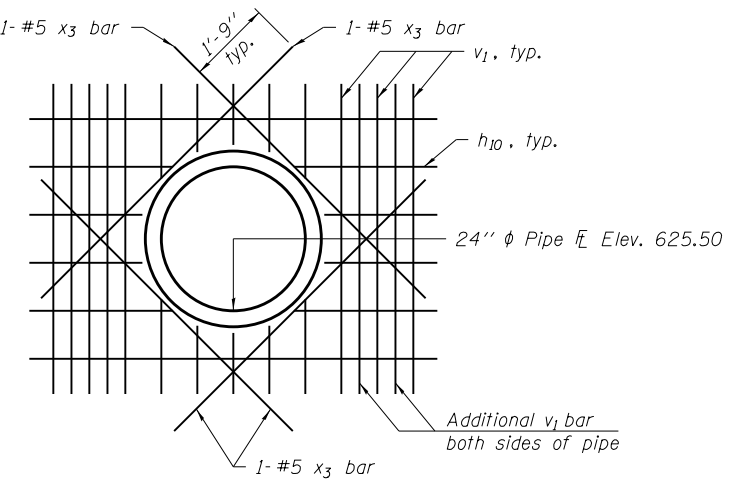
BAR x



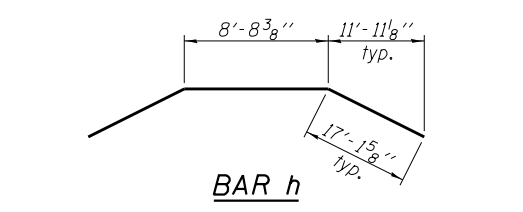
BAR s



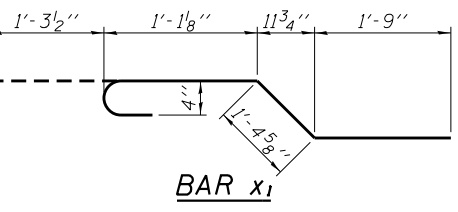
BAR s₁



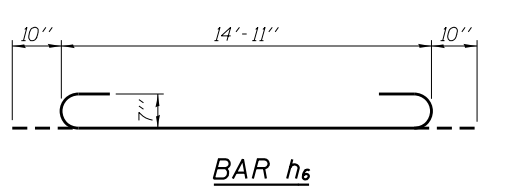
PIPE CULVERT THRU WALL DETAIL



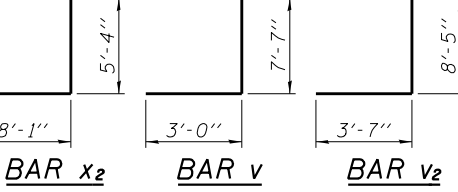
BAR h



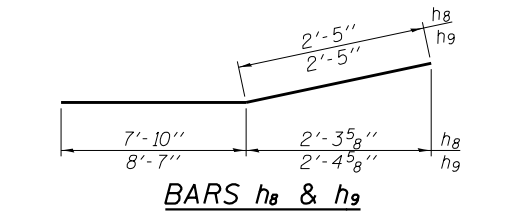
BAR x₁



BAR h₆



BARS x₂ v v₂



BARS h₈ & h₉

Notes:
Cut bars to provide 2" min. clearance to pipe.
For Downstream Apron Details, see sheet 1 of 3.
For Plan of Sloped Tapered Inlet, see sheet 2 of 3.
For flowline elevations not shown, see roadway cross-section.

LAST SAVED = 1/23/2013 11:46:28 AM
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PLOT SCALE = 0:2.0000 '1' = 1"	CHECKED -	DRAWN E.M. Lagemann	REVISED -
PLOT DATE = 1/23/2013 11:46:28 AM	DATE -		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HORNER & SHIRIN, INC.
ENGINEERS

SLOPED TAPERED INLET DETAILS
CULVERT AT STA. 2089+50.12

SCALE: SHEET NO. 3 OF 3 SHEETS STA. 2089+33.39 TO STA. 2089+79.02

F.A.P. RTE. 312	SECTION 68-WRS-1	COUNTY MONROE	TOTAL SHEETS 760	SHEET NO. 446
CONTRACT NO. 76817				ILLINOIS FED. AID PROJECT